F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
--	06-00084-00-RS	DUPAGE	14	1
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

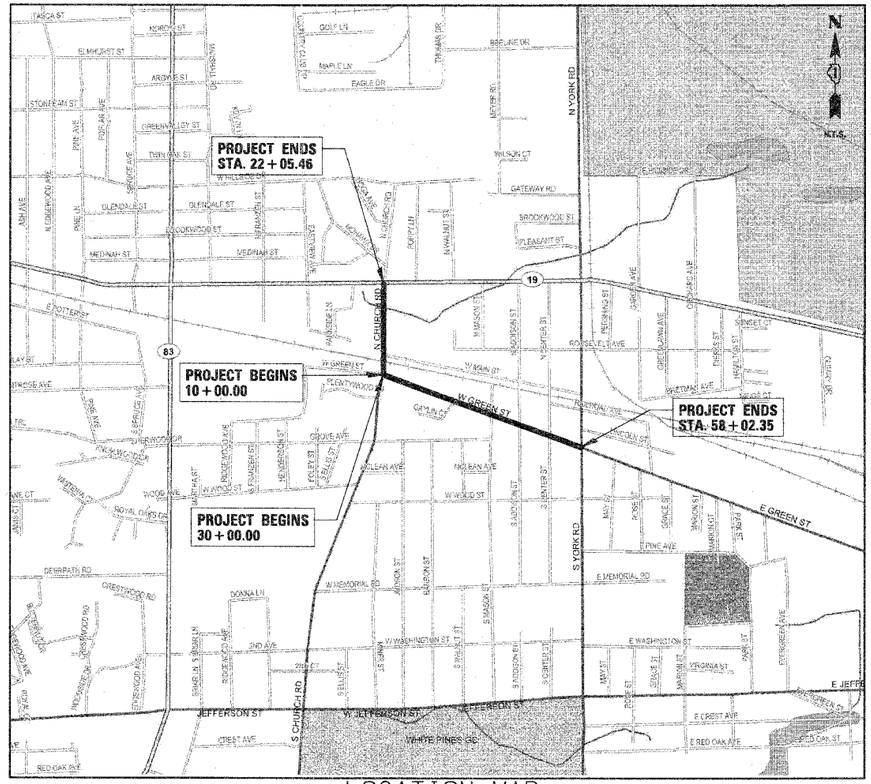
CONTRACT NO. 83887

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PLANS FOR PROPOSED FEDERAL AID HIGHWAY

GREEN STREET (FAU 3533)
FROM CHURCH ROAD (FAU 2667) TO YORK ROAD (FAU 2678)
CHURCH ROAD (FAU 2667)
FROM GREEN STREET (FAU 3533) TO IRVING PARK ROAD (FAU 1321)
RESURFACING
L.A. SECTION No. 06-00084-00-RS
PROJECT No. M-8003 (628)
VILLAGE OF BENSENVILLE
DUPAGE COUNTY
C-91-300-06

INDEX OF SHEETS

RANGE 11E



LOCATION MAP
N.T.S.

GROSS LENGTH OF PROJECT = 3800 FEET (0.72 MI)
NET LENGTH OF PROJECT = 3800 FEET (0.72 MI)

TRAFFIC DATA (GREEN STREET)
ADT (YEAR) = 9200 (2004)
SPEED LIMIT = 25 MPH

TRAFFIC DATA (CHURCH ROAD)
ADT (YEAR) = 9100 (2004)
SPEED LIMIT = 25 MPH

DESIGN DESIGNATION: COLLECTOR

LOCATION OF SECTION INDICATED THUS: -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

APPROVED 11/15 2006
VILLAGE ENGINEER, VILLAGE OF BENSENVILLE

PASSED DECEMBER 28 2006
CHRISTOPHER HOLT
DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS

RELEASING FOR BID BASED ON LIMITED REVIEW DEC. 28 2006
DEPUTY DIRECTOR OF HIGHWAYS, REGION ONE ENGINEER

100' 200' 300' — 1"= 100'
0 10' 20' 30' — 1"= 10'
0 50' 100' — 1"= 50'
0 50' 100' — 1"= 40'
0 50' 100' — 1"= 30'
0 50' 100' — 1"= 20'

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123

CONTRACT NO. 83887

CHRISTOPHER B. BURKE ENGINEERING LTD.
9575 West Higgins Road, Suite 600
Rosemont, Illinois 60018 (847) 823-0500

LEE M. FELL
062-053708
LICENSED PROFESSIONAL ENGINEER
ILLINOIS

ENGINEER 11/13/06 DATE

LEE M. FELL
ILLINOIS REGISTRATION No. 062-053708
EXPIRATION DATE: 11/30/2007

IDOT FEDERAL-AID DESIGN ENGINEER: KEVIN STALLWORTH 847-705-4169

N:\BENSENVILLE\20691BG01\CIVIL\20691BG01.CVR

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	06-00084-00-RS	DUPAGE	14	2
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 83887

CHRISTOPHER B. BURKE ENGINEERING LTD.
9575 West Higgins Road, Suite 600
Rosemont, Illinois 60018
(847) 823-0500

SURVEYED
PLOTTED
ALIGNMENT CHECKED
ALL ELEVATIONS CONNECTED
CADD FILE NAME
PLAN
NOTE BOOK
NO.
DATE
BY

PROFILE
SURVEYED
PLOTTED
GRADES CHECKED
HORIZONTAL
STRUCTURE NOTATIONS CHKD.
NOTE BOOK
NO.
DATE
BY

SUMMARY OF QUANTITIES

IDOT ITEMS	ITEMS	UNIT	CHURCH ROAD (SEC 1) I000-2A QUANTITY	CHURCH ROAD NON-PARTICIPATING QUANTITY	GREEN STREET (SEC 2) I000-2A QUANTITY	GREEN STREET NON-PARTICIPATING QUANTITY	PROJECT TOTALS I000-2A QUANTITY	PROJECT TOTALS NON-PARTICIPATING QUANTITY	PROJECT TOTALS TOTAL QUANTITY
*21101615	TOPSOIL FURNISH AND PLACE, 4"	SY	120	0	314	0	434	0	434
*25200110	SODDING, SALT TOLERANT	SY	120	0	314	0	434	0	434
25200200	SUPPLEMENTAL WATERING	UNIT	5	0	10	0	15	0	15
35101800	AGGREGATE BASE COURSE, TYPE B 6"	SY	190	0	0	0	190	0	190
40200800	AGGREGATE SURFACE COURSE, TYPE B	TON	100	0	100	0	200	0	200
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GAL	455	0	1100	0	1555	0	1555
40600300	AGGREGATE (PRIME COAT)	TON	10	0	20	0	30	0	30
40600625	LEVELING BINDER (MACHINE METHOD), N50	TON	210	0	500	0	710	0	710
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	436	0	1000	0	1436	0	1436
42300300	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 7 INCH	SY	130	0	11	0	141	0	141
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SF	0	0	2300	0	2300	0	2300
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SY	0	0	192	0	192	0	192
44000200	DRIVEWAY PAVEMENT REMOVAL	SY	4641	0	10909	0	15550	0	15550
42400800	DETECTABLE WARNINGS	SF	320	0	68	0	388	0	388
44000600	SIDEWALK REMOVAL	SF	0	0	2300	0	2300	0	2300
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	250	0	630	0	880	0	880
44300100	AREA REFLECTIVE CRACK CONTROL TREATMENT	SY	4641	0	10909	0	15550	0	15550
60258200	MANHOLES TO BE RECONSTRUCTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	0	0	3	0	3	0	3
67100100	MOBILIZATION	L SUM	0.5	0	0.5	0	1	0	1
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L. SUM	0.5	0	0.5	0	1	0	1
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L. SUM	0.5	0	0.5	0	1	0	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L. SUM	0.5	0	0.5	0	1	0	1
*78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SF	230	0	50	0	280	0	280
*78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	2250	0	6100	0	8350	0	8350
*78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	335	0	1850	0	2185	0	2185
*78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	32	0	200	0	232	0	232
*78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	80	0	220	0	300	0	300
*88600600	DETECTOR LOOP REPLACEMENT	FOOT	280	0	330	0	610	0	610
X0300624	RAILROAD PROTECTIVE LIABILITY INSURANCE (SPECIAL)	EACH	1	0	0	0	1	0	1
XX000667	SUB-BASE GRANULAR MATERIAL, TYPE B 2"	SY	0	0	256	0	256	0	256
XX003535	AGGREGATE BASE COURSE, TYPE B, 2"	SY	130	0	11	0	141	0	141
*Z0017500	DRAINAGE & UTILITY STRUCTURE ADJUSTMENT (SPECIAL)	EACH	25	0	75	0	100	0	100
XX006786	CLASS D PATCHES, SPECIAL, 7-INCHES	SY	455	0	1100	0	1555	0	1555

* SPECIALTY ITEMS

REVISIONS NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITES
AND STORM SEWER SCHEDULE

SCALE NTS
DRAWN BY JCR
DATE 12/20/2006
CHECKED BY LMF

12/20/2006 N:\BENSENVILLE\06918G01\Civil\06918G01_QUA1.QUA

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	06-00084-00-RS	DUPAGE	14	3
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 83887

GENERAL NOTES

UTILITIES

THE CONTRACTOR SHALL COOPERATE WITH THE VILLAGE IN ANY UNDERGROUND UTILITY CONSTRUCTION WHICH THE VILLAGE MAY WANT TO PLACE DURING THE CONTRACTOR'S OPERATIONS.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING FACILITIES SO THAT THE UTILITIES AND THEIR APPURTENANCES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE STANDARD SPECIFICATIONS.

THE LOCATIONS OF EXISTING DRAINAGE STRUCTURES, STORM AND SANITARY SEWERS, WATER SERVICE LINES AND OTHER UTILITY LINES ARE APPROXIMATE, AND THE VILLAGE DOES NOT GUARANTEE THEIR ACCURACY. THEIR EXACT HORIZONTAL AND VERTICAL LOCATIONS ARE TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR AT HIS OWN EXPENSE.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER OR THE VILLAGE. THIS WORK SHALL BE AT THE CONTRACTOR'S EXPENSE.
BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, GAS AND CABLE TELEVISION FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED.)

STAKING

THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE VILLAGE, HIS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

WATER, STORM SEWER AND SANITARY SEWER

WHENEVER DURING CONSTRUCTION OPERATIONS ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS. THE WORK SPECIFIED ABOVE WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCIDENTAL TO THE CONTRACT.

ANY EXISTING OR PROPOSED STORM SEWER DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR AND INCIDENTAL TO THE CONTRACT.

THE CONTRACTOR SHALL NOT OPEN OR SHUT ANY WATER VALVES OR FIRE HYDRANTS. CONTACT WATER DEPARTMENT FOR THEM TO TURN VALVES OR OPERATE HYDRANTS. UNAUTHORIZED USE SHALL SUBJECT THE OFFENDER TO ARREST AND PROSECUTION.

MISCELLANEOUS

ACCESS: THE CONTRACTOR SHALL PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT, EXCEPT FOR PERIODS OF SHORT DURATION. THE COST TO PROVIDE ACCESS SHALL BE PAID FOR AND INCLUDED IN THE ITEM AGGREGATE SURFACE COURSE, TYPE B.

DIMENSIONS: IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.

ALL SAWCUTTING SHALL BE INCLUDED TO REMOVAL ITEMS AND SHALL BE PERFORMED PRIOR TO BEGINNING REMOVAL. ANY ITEMS OF WORK REMOVED PRIOR TO SAWCUTTING WILL NOT BE MEASURED FOR PAYMENT.

PAVEMENT PATCHING, CURB AND GUTTER REMOVAL AND REPLACEMENT, SIDEWALK REMOVAL AND REPLACEMENT, DRIVEWAY REMOVAL AND REPLACEMENT, STRUCTURES TO BE ADJUSTED, DETECTOR LOOP REPLACEMENT, AND CLASS D PATCHES WILL BE DETERMINED BY THE ENGINEER IN THE FIELD AND WILL NOT EXCEED THE PLANNED QUANTITY.

THE THICKNESSES OF BITUMINOUS MIXTURES SHOWN IN THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACES OR BASIS ON WHICH THEY ARE TO BE PLACED. PLAN THICKNESSES SHOULD BE CONSIDERED THE MINIMUM THICKNESS PERMITTED.

DETECTABLE WARNINGS FOR THE HANDICAPPED SHALL BE INSTALLED AT ALL INTERSECTING STREETS, DRIVEWAYS, AND ALLEYS AS DIRECTED BY THE ENGINEER (SEE IDOT STD. 424001-04 INCLUDED IN THE SPECIFICATIONS).

PAVEMENT GRADES: THE ELEVATIONS INDICATED ON THE PLANS ARE FINISHED GRADES OF PROPOSED PAVEMENT OR SURFACE COURSE, UNLESS OTHERWISE INDICATED.

RELOCATING EXISTING SIGNS: EXISTING SIGNS WHICH ARE IN CONFLICT WITH PROPOSED IMPROVEMENTS SHALL BE REMOVED AND REINSTALLED UPON COMPLETION OF CONFLICTING IMPROVEMENTS IN ACCORDANCE WITH THE ILLINOIS DEPARTMENT OF TRANSPORTATION "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" AND THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION". STOP SIGNS, SPEED LIMIT SIGNS, AND STREET NAME SIGNS SHALL BE UP AND VISIBLE AT ALL TIMES. THIS WORK SHALL BE INCLUDED TO THE PAY ITEM TRAFFIC CONTROL AND PROTECTION.

MAILBOXES WHICH ARE IN CONFLICT WITH PROPOSED IMPROVEMENTS SHALL BE REMOVED, TEMPORARILY RELOCATED, AND REPLACED UPON COMPLETION OF THE PROPOSED IMPROVEMENTS AS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE CONSIDERED INCLUDED TO THE PAY ITEM TRAFFIC CONTROL AND PROTECTION.

PAY ITEMS IN THE SUMMARY OF QUANTITIES HAVE BEEN ESTIMATED. IF, IN THE ENGINEER'S OPINION, THE WORK IS NOT REQUIRED, THE ITEM WILL BE DEDUCTED FROM THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

PROPOSED CONCRETE CURB AND GUTTER SHALL BE TRANSITIONED TO EXISTING CURB AND GUTTER OVER A LENGTH OF 5 FEET. THIS WORK SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT FOR COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT.

CONTRACTOR SHALL NOT PLACE SOD UNTIL THE TEMPERATURE IS 80° OR LESS AND THE FORECAST FOR THE NEXT 7 DAYS SHOWS TEMPERATURES OF 80° OR LESS. IF ALL OTHER PAY ITEMS ARE COMPLETED, THE CONTRACTOR WILL NOT BE CHARGED WORKING DAYS FOR DELAYS IN PARKWAY RESTORATION DUE TO TEMPERATURE.

HIGHWAY STANDARDS

000001-04	STANDARD SYMBOLS, ABBREVIATIONS, AND PATTERNS
424001-04	CURB RAMPS FOR SIDEWALKS
442201-02	CLASS C AND D PATCHES
606001-03	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701501-03	URBAN LANE CLOSURE, 2L, 2W UNDIVIDED
701701-04	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-03	LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
702001-06	TRAFFIC CONTROL DEVICES
780001-01	TYPICAL PAVEMENT MARKINGS
886001	DETECTOR LOOP INSTALLATIONS
B.L.R. 17-3	TRAFFIC CONTROL DEVICES DAY LABOR CONSTRUCTION
B.L.R. 22-4	TRAFFIC CONTROL DEVICES DAY LABOR MAINTENANCE

REVISIONS NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

GENERAL NOTES AND IDOT STANDARDS

SCALE N.T.S. DRAWN BY JCR

DATE 11/10/2006 CHECKED BY LMF

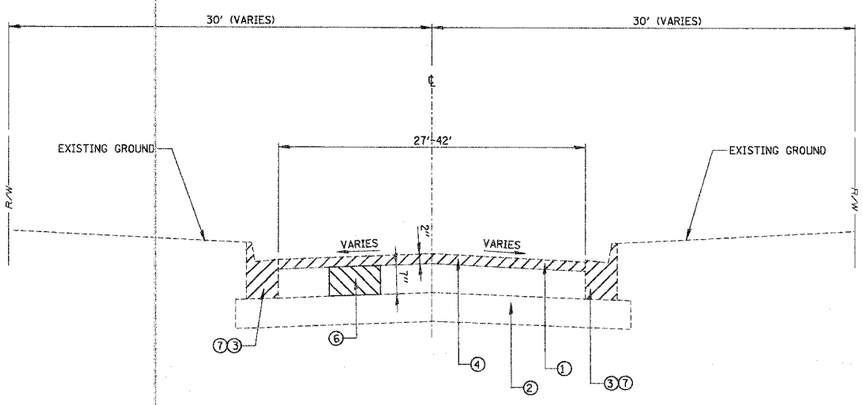
CHRISTOPHER B. BURKE ENGINEERING LTD.
9575 West Higgins Road, Suite 600
Rosemont, Illinois 60018
(847) 823-0500

PROFILE SAWCUT/ PLOTTED/ IMAGES CHECKED/ ALL XREFS ATTACHED/ STRUCTURE NUMBERS SHOWN/ NOTE BOOK/ NO.

DATE / BY / NOTE BOOK / NO. / SURVEYOR PLOTTED / ALIGNMENT CHECKED / ALL XREFS ATTACHED / CADD FILE NAME

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	06-00084-00-RS	DUPAGE	14	4
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 83887



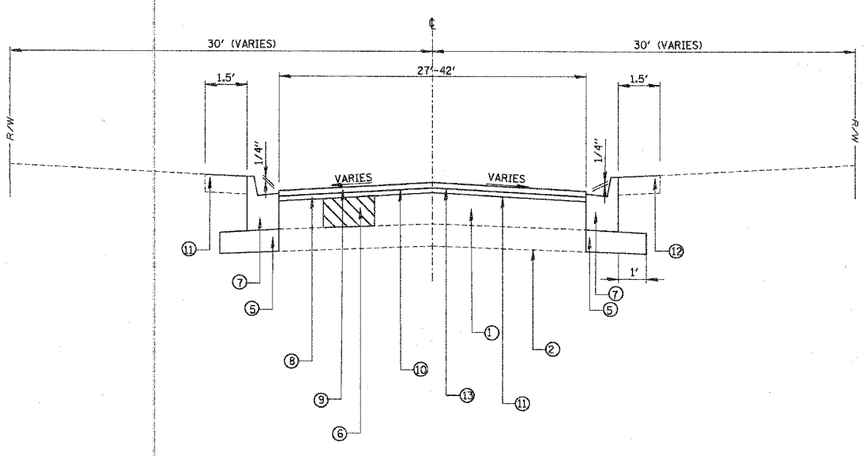
EXISTING TYPICAL SECTION

CHURCH RD. (STA. 10+00.00 - STA. 22+05.46)
GREEN ST. (STA. 30+00.00 - STA. 45+93.07)

LEGEND

1. EXISTING BITUMINOUS PAVEMENT, (11")
2. EXISTING AGGREGATE BASE, (4")
3. EXISTING CURB AND GUTTER
4. HOT-MIX ASPHALT SURFACE REMOVAL, 2"
5. *PROPOSED SUB-BASE GRANULAR MATERIAL, TYPE B, 4"
6. CLASS D PATCHES, SPECIAL, 7"
7. COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)
8. PROPOSED LEVELING BINDER (MACHINE METHOD), N50 - 3/4" MIN.
9. PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N50 - 1 1/2"
10. PROPOSED BITUMINOUS MATERIAL (PRIME COAT)
11. PROPOSED AGGREGATE (PRIME COAT)
12. PROPOSED SODDING, SALT TOLERANT
13. PROPOSED AREA REFLECTIVE CRACK CONTROL TREATMENT

* INCIDENTAL TO COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT
(SEE DETAIL ON SHEET 11 "CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT")



PROPOSED TYPICAL SECTION

CHURCH RD. (STA. 10+00.00 - STA. 22+05.46)
GREEN ST. (STA. 30+00.00 - STA. 45+93.07)

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		
ITEM	AC-TYPE	VOIDS
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	PG 64-22	4%@50GYR.
LEVELING BINDER (MACHINE METHOD), N50	PG 64-22/ 58-22	4%@50GYR.
CLASS D PATCHES, SPECIAL, 7"	PG 64-22/ 58-22	4%@70GYR.

NOTE:
1. THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SY/IN.
2. WHEN RAP EXCEEDS 20% THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS

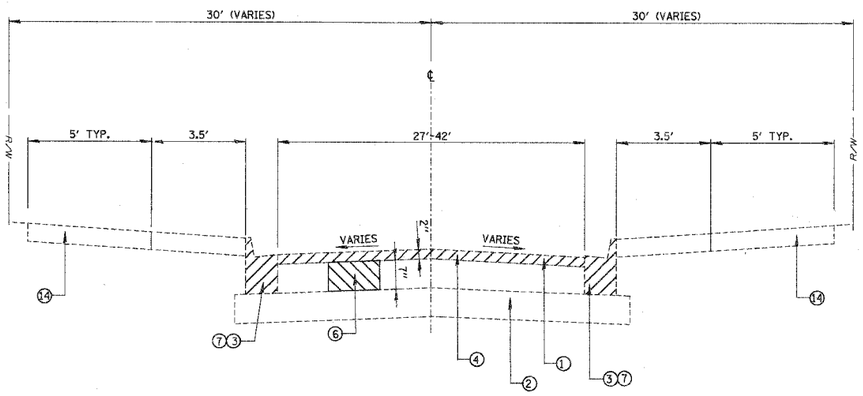
SCALE NTS — DRAWN BY JCR
DATE 11/20/2006 — CHECKED BY LMF

CHRISTOPHER B. BURKE ENGINEERING LTD.
9575 West Higgins Road, Suite 600
Rosemont, Illinois 60018
(847) 823-0500

PLAN — SURVEYED — PLOTTED — ALIGNMENT CHECKED — ELEVATIONS CHECKED — CAD FILE NAME — BY — DATE
NOTE BOOK — NO.
PROFILE — SURVEYED — PLOTTED — GRADES CHECKED — R.R.A. NO. — STRUCTURE NOTATIONS CHKD — BY — DATE

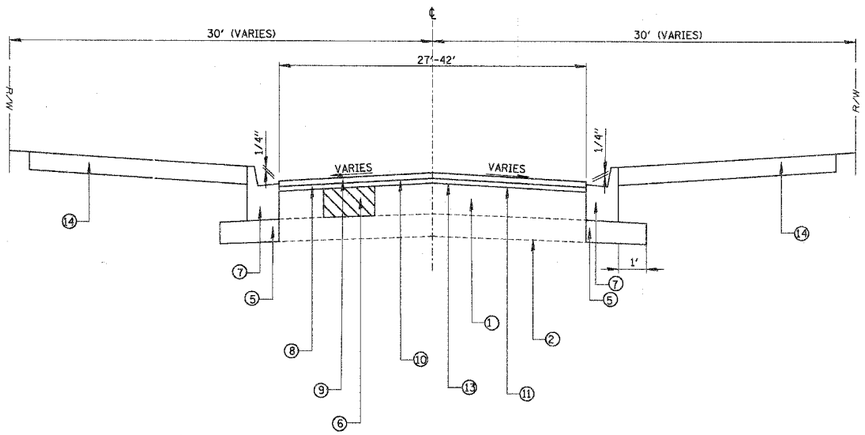
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	06-00084-00-RS	DUPAGE	14	5
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 83887



EXISTING TYPICAL SECTION

GREEN ST. (STA. 45+ 93.07 - STA. 58+02.35)



PROPOSED TYPICAL SECTION

GREEN ST. (STA. 45+ 93.07 - STA. 58+02.35)

LEGEND

1. EXISTING BITUMINOUS PAVEMENT, (11")
2. EXISTING AGGREGATE BASE, (4")
3. EXISTING CURB AND GUTTER
4. HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"
5. *PROPOSED SUB-BASE GRANULAR MATERIAL, TYPE B, 4"
6. CLASS D PATCHES, SPECIAL, 7"
7. COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)
8. PROPOSED LEVELING BINDER (MACHINE METHOD), N50 - 3/4" MIN.
9. PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D" N50 - 1 1/2"
10. PROPOSED BITUMINOUS MATERIAL (PRIME COAT)
11. PROPOSED AGGREGATE (PRIME COAT)
12. PROPOSED SODDING, SALT TOLERANT
13. PROPOSED AREA REFLECTIVE CRACK CONTROL TREATMENT
14. EXISTING SIDEWALK

* INCIDENTAL TO COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT

(SEE DETAIL ON SHEET 11 "CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT")

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		
ITEM	AC-TYPE	VOIDS
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	PG 64-22	4%@50GYR.
LEVELING BINDER (MACHINE METHOD), N50	PG 64-22/ 58-22	4%@50GYR.
CLASS D PATCHES, SPECIAL, 7"	PG 64-22/ 58-22	4%@70GYR.

NOTE:
1. THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SY/IN.
2. WHEN RAP EXCEEDS 20% THE NEW ASPHALT BINDER IN THE MIX SHALL BE PG 58-22.

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS

SCALE NTS — DRAWN BY JCR

DATE 11/20/2006 — CHECKED BY LMF

CHRISTOPHER B. BURKE ENGINEERING LTD.
9575 West Higgins Road, Suite 600
Rosemont, Illinois 60018
(847) 823-0500

PLAN — NOTE BOOK — NO. — DATE — BY — WORKTOP PLOTTED ALIGNMENT CHECKED GRADES CHECKED PROJECT FILE NAME

PROFILE — NOTE BOOK — NO. — DATE — BY — WORKTOP PLOTTED GRADES CHECKED DRAIN. NOTES STRUCTURE NOTATNS CHKD

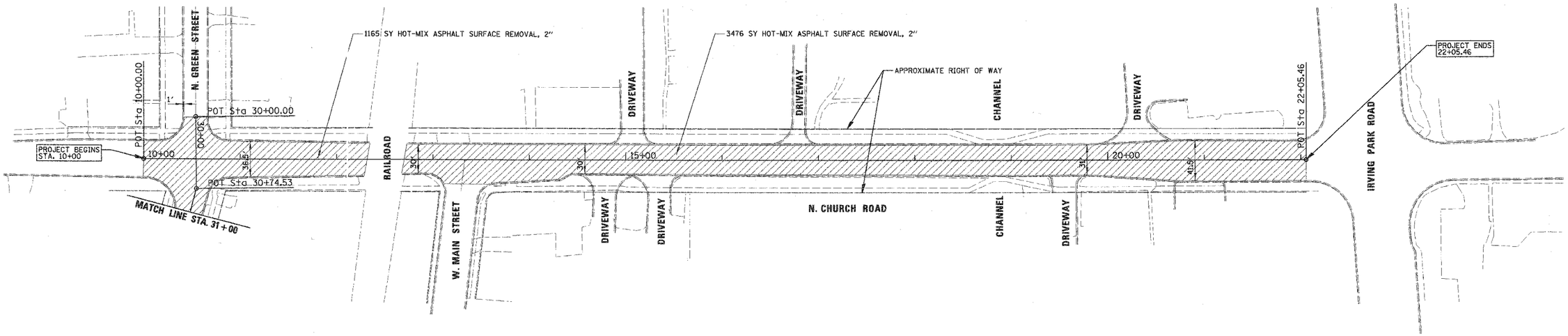
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	06-00084-00-RS	DUPAGE	14	6
STA. 10+00.00	TO STA. 22+05.46			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 83887

CHRISTOPHER B. BURKE ENGINEERING LTD.
9575 West Higgins Road, Suite 600
Rosemont, Illinois 60018
(847) 823-0500

PLAN
NOTE HOOK NO. ____
SURVEYED
PLOTTED
ELEVATIONS CHECKED
RT. OF WAY CHECKED
CADD FILE NAME
BY
DATE

PROFILE
NOTE 90K NO. ____
SURVEYED
PLOTTED
GRADES CHECKED
R.R. NOTED
STRUCTURE NOTATNS VERIFIED
BY
DATE



NOTE:
1. BUTT JOINTS WILL BE AT SIDE STREETS AND AT LIMITS OF THE PROJECT. ALL BUTT JOINTS JOINTS ARE INCIDENTAL TO THE SURFACE COURSE.
2. ALL EXISTING CURB AND GUTTER IS B-6.12.

LEGEND

HOT-MIX ASPHALT SURFACE REMOVAL, 2"

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

CHURCH ROAD

EXISTING CONDITIONS AND REMOVAL PLAN

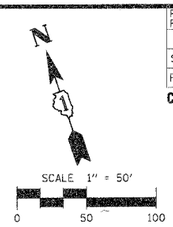
SCALE 1"=50' DRAWN BY JCR

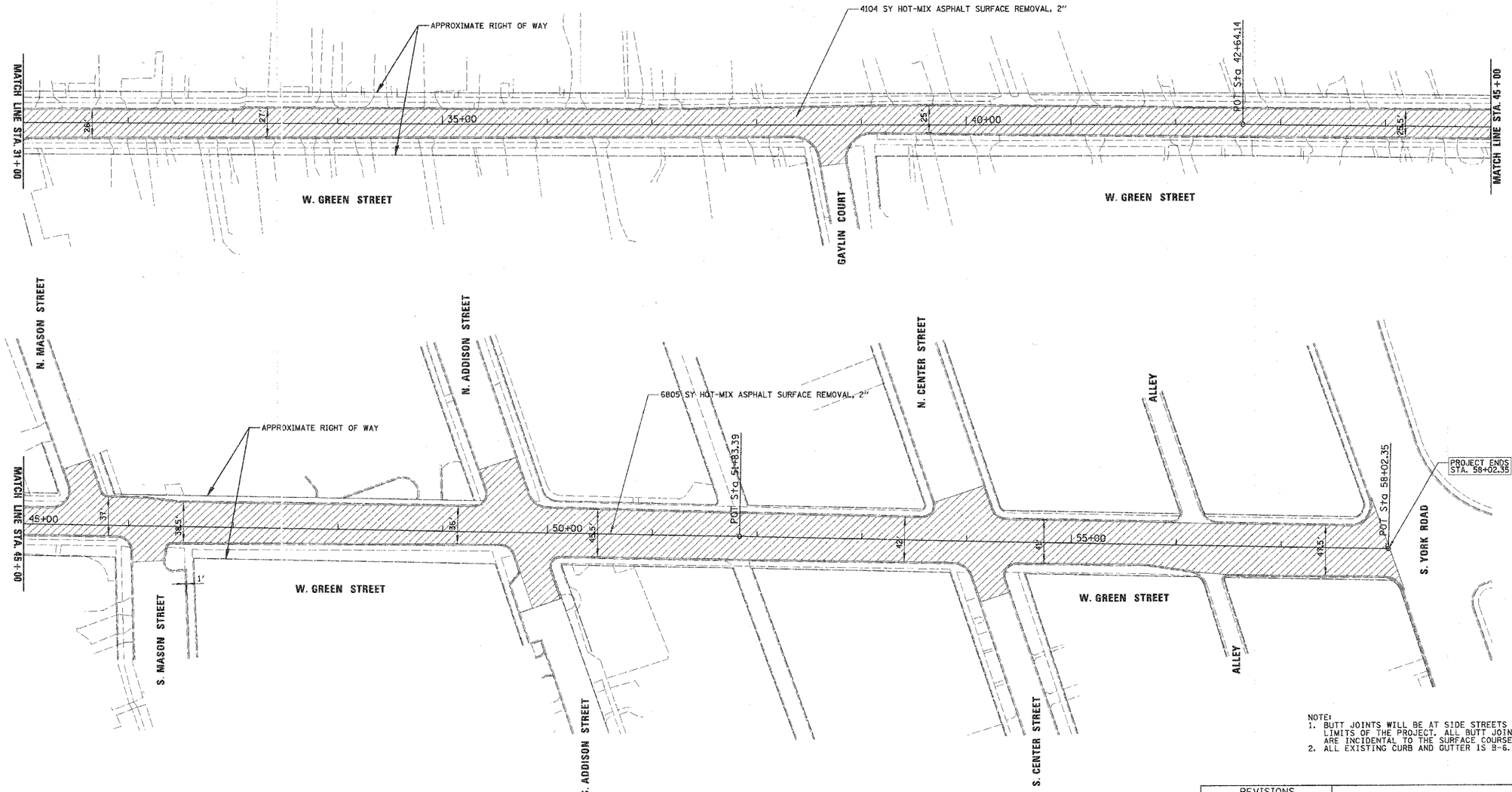
DATE 11/13/2006 CHECKED BY LMF

11/13/2006 N:\BENSENVILLE\0691BG01\CIVIL\0691BG01_CHURCH_EXIST.PLN

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	06-00084-00-RS	DUPAGE	14	7
STA. 31+00.00		TO STA. 58+02.35		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 83887





NOTE:
1. BUTT JOINTS WILL BE AT SIDE STREETS AND AT LIMITS OF THE PROJECT. ALL BUTT JOINTS JOINTS ARE INCIDENTAL TO THE SURFACE COURSE.
2. ALL EXISTING CURB AND GUTTER IS B-6.12.

LEGEND

HOT-MIX ASPHALT SURFACE REMOVAL, 2"

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

GREEN STREET

EXISTING CONDITIONS AND REMOVAL PLAN

SCALE 1"=50' DRAWN BY JCR

DATE 11/13/2006 CHECKED BY LMF

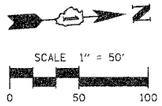
CHRISTOPHER B. BURKE ENGINEERING LTD.
9575 West Higgins Road, Suite 600
Rosemont, Illinois 60018
(847) 823-0500

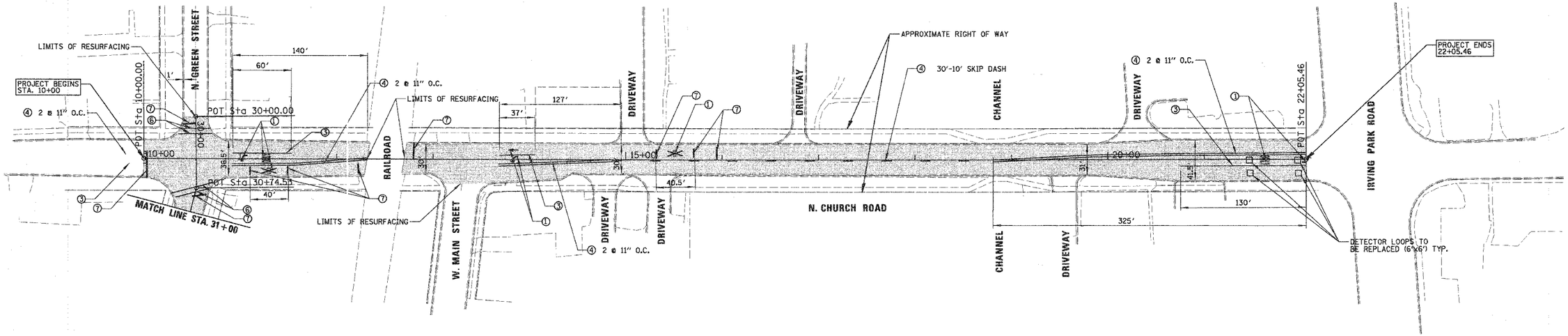
PLAN | NOTE BOOK | NO. | SURVEYED | PLOTTED | FIELD CHECKED | PLAN NOTED | PROJECTED | BY | DATE | PROFILE | SHOWN | PLOTTED | FIELD CHECKED | PLAN NOTED | PROJECTED | NOTE BOOK | NO. | BY | DATE

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	06-00084-00-RS	DUPAGE	14	8
STA. 10+00.00		TO STA. 22+05.46		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 83087

LEGEND
1. THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
2. THERMOPLASTIC PAVEMENT MARKINGS - LINE 4" (WHITE)
3. THERMOPLASTIC PAVEMENT MARKINGS - LINE 6" (WHITE)
4. THERMOPLASTIC PAVEMENT MARKINGS - LINE 4" (YELLOW)
5. THERMOPLASTIC PAVEMENT MARKINGS - LINE 8" (WHITE)
6. THERMOPLASTIC PAVEMENT MARKINGS - LINE 12" (WHITE)
7. THERMOPLASTIC PAVEMENT MARKINGS - LINE 24" (WHITE)
8. THERMOPLASTIC PAVEMENT MARKINGS - LINE 12" (YELLOW)





LEGEND

-LEVELING BINDER (MACHINE METHOD), N50 - 3/4" (MIN.)
-HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 1 1/2 "

REVISIONS NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

CHURCH ROAD

PROPOSED PLAN

SCALE 1"=50' DRAWN BY JCR

DATE 11/10/2006 CHECKED BY LMF

11/10/2006 N:\BENSENVILLE\06918601\CIVIL\06918601_CHURCH_PRO.dgn

CHRISTOPHER B. BURKE ENGINEERING LTD.
9575 West Higgins Road, Suite 600
Rosemont, Illinois 60018
(847) 823-0500

PLAN | SURVEYED | BY | DATE
NOTE BOOK | PLOTTED | CHANGE MADE | CHECKED
NO. | RT. OF WAY CHECKED | CADD FILE NAME

PROFILE | SURFACED | BY | DATE
NOTE BOOK | PLOTTED | CHANGE MADE | CHECKED
NO. | NUM. NO'S | STRUCTURE NOTATING | CHKD

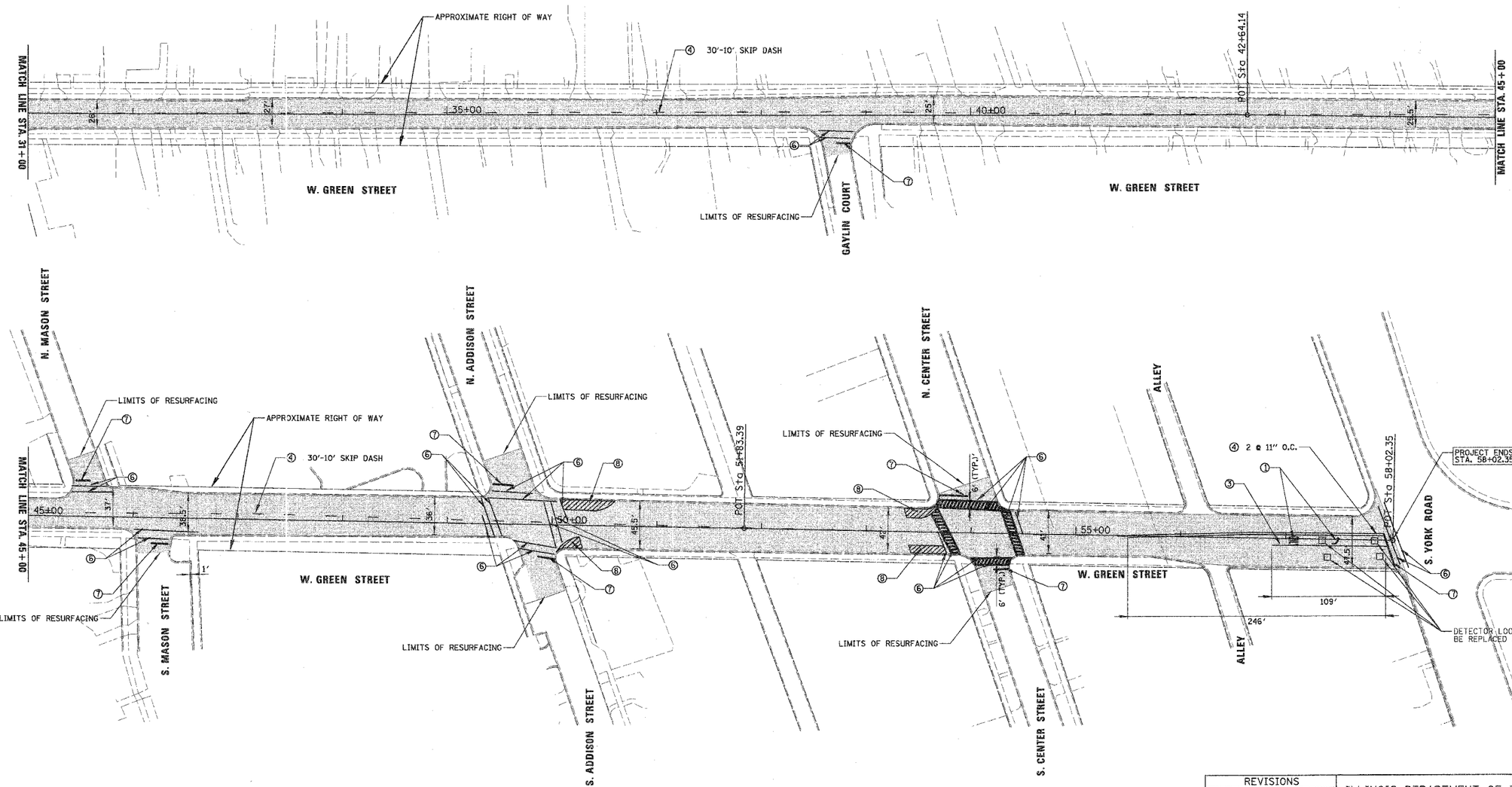
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	06-00084-00-RS	DUPAGE	14	9
STA. 31+00.00	TO STA. 58+02.35			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 83887

LEGEND
① THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS
② THERMOPLASTIC PAVEMENT MARKINGS - LINE 4" (WHITE)
③ THERMOPLASTIC PAVEMENT MARKINGS - LINE 6" (WHITE)
④ THERMOPLASTIC PAVEMENT MARKINGS - LINE 4" (YELLOW)
⑤ THERMOPLASTIC PAVEMENT MARKINGS - LINE 8" (WHITE)
⑥ THERMOPLASTIC PAVEMENT MARKINGS - LINE 12" (WHITE)
⑦ THERMOPLASTIC PAVEMENT MARKINGS - LINE 24" (WHITE)
⑧ THERMOPLASTIC PAVEMENT MARKINGS - LINE 12" (YELLOW)

N

SCALE 1" = 50'
0 50 100



LEGEND

-LEVELING BINDER (MACHINE METHOD), N50 - 3/4" (MIN.)
-HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 - 1 1/2 "

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

GREEN STREET
PROPOSED PLAN

SCALE 1"=50' DRAWN BY JCR
DATE 11/10/2006 CHECKED BY LMF

CHRISTOPHER B. BURKE ENGINEERING LTD.
9575 West Higgins Road, Suite 600
Rosemont, Illinois 60018
(847) 823-0500

PLAN: SURVEYED, DRAWN, ALIGNMENT CHECKED, P.I. OF R.W. CHECKED, PROJECT NOTATIONS CHKD, NOTE BOOK, NO.
PROFILE: SURVEYED, DRAWN, GRADES CHECKED, P.I. NOTES, NOTATIONS CHKD, NOTE BOOK, NO.

11/10/2006 N:\BENSENVILLE\06918G01\CIVIL\06918G01_GREEN_PRO.dgn

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	06-00084-00-RS	DUPAGE	14	10
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 83887

CONSTRUCTION PROCEDURES

STAGE I (BEFORE PAVEMENT MILLING)

A) REMOVE A MINIMUM OF 12" OF THE PAVEMENT FROM AROUND THE STRUCTURE.
B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
C) COVER THE STRUCTURE OPENING WITH A 36" DIAMETER METAL PLATE.
D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½" THICK HOT-MIX MATERIAL APPROVED BY THE ENGINEER.

STAGE II (AFTER PAVEMENT MILLING)

A) REMOVE THE HOT-MIX MATERIAL AND CRUSHED STONE.
B) INSTALL THE FRAME AND LID: ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602 AND 603 OF THE STANDARD SPECIFICATIONS.

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

STRUCTURE TO BE ADJUSTED.

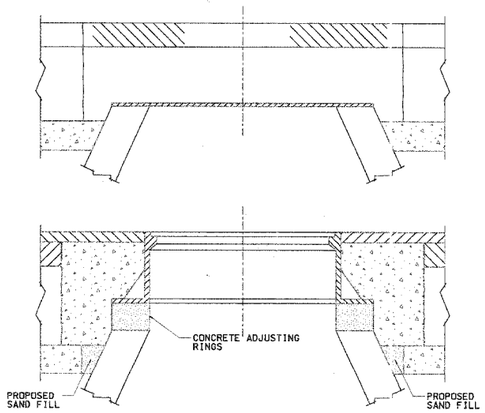
NOTES

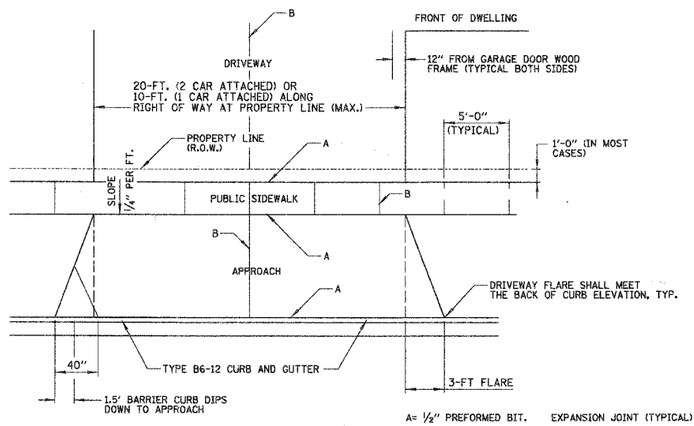
1. EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

2. IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

3. THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING





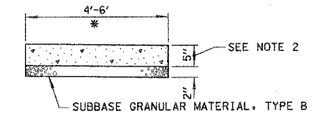
A= ½" PREFORMED BIT. EXPANSION JOINT (TYPICAL)

B= TOLLED OR SAWED CONTRACTION JOINTS

DRIVEWAY WITH A TYPE B-6.12 CURB AND GUTTER

GENERAL NOTES:

1. DRIVEWAY SHALL HAVE A MIN. SLOPE OF 2% AND MAX. SLOPE OF 8%.
2. APPROACH SHALL HAVE A MIN. SLOPE OF 2% AND MAX. OF 10%.
3. ALL AGGREGATE SUB-BASE SHALL BE MECHANICALLY COMPACTED. (95% PROCTOR)
4. PUBLIC SIDEWALK SHALL BE 7" THICK P.C. CONCRETE AT DRIVEWAY. (NO WIRE MESH)
5. MINIMUM THICKNESS FOR APPROACH. (NO WIRE MESH)
   A. 7" THK. P.C. CONCRETE ON 2" AGGREGATE BASE COURSE TYPE B OR
   B. 3" THK. HOT-MIX ASPHALT SURFACE ON 6" AGGREGATE BASE COURSE TYPE B



* CROSS SLOPE 2% OR AS SHOWN ON CROSS SECTIONS

NOTES:

1. ALL REQUIRED EARTH EXCAVATION TO CONSTRUCT P.C.C. SIDEWALK SHALL BE INCIDENTAL TO THE P.C.C. SIDEWALK 5 INCH.

2. WHEN FORMS ARE REMOVED FROM THE SIDEWALK EITHER THE SIDEWALK SHALL BE BARRICADED OR BACKFILLED WITHIN 24 HOURS.

P.C.C. SIDEWALK DETAIL

REVISIONS	
NAME	DATE

ILLINOIS DEPARTMENT OF TRANSPORTATION

CONSTRUCTION DETAILS

SCALE N.T.S. DRAWN BY JCR

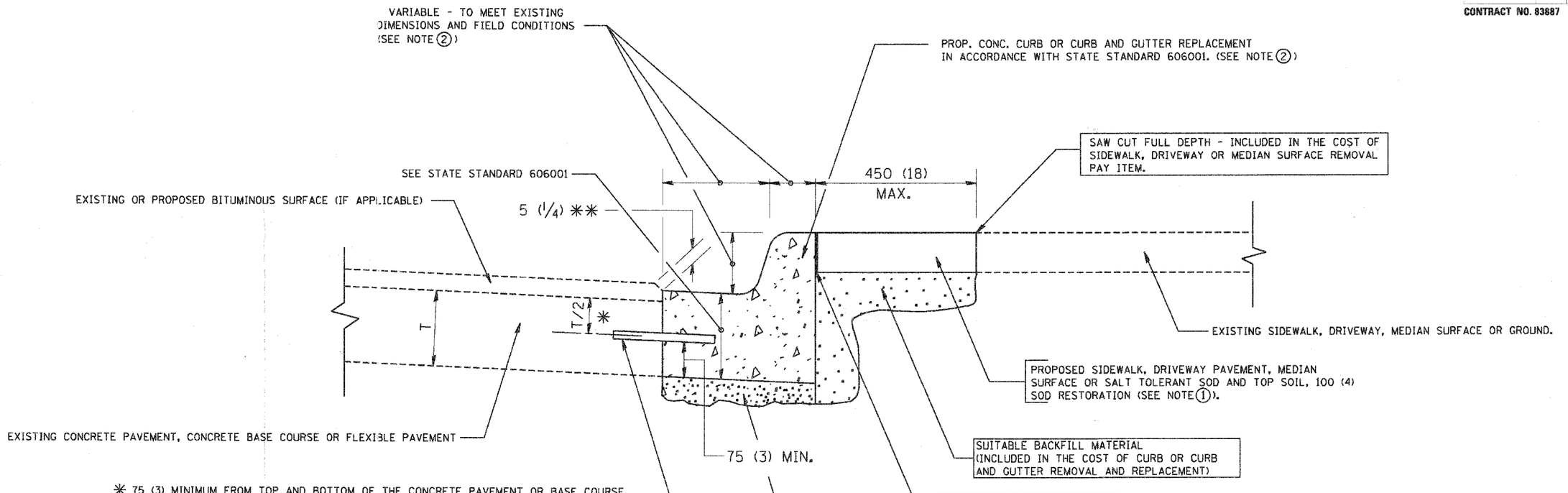
DATE 11/13/2006 CHECKED BY LMF

CHRISTOPHER B. BURKE ENGINEERING LTD.
9575 West Higgins Road, Suite 600
Rosemont, Illinois 60018
(847) 823-0500

11/13/2006 N:\BENSENVILLE\0691BG01\CIVIL\0691BG01_DET1.PLN

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	06-00084-00-RS	DUPAGE	14	11
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 83887



* 75 (3) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: (1) SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SALT TOLERANT SOD AND TOP SOIL, 100 (4) RESTORATION WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

(2) CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

(3) FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

(4) LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

(5) THE COST OF BITUMINOUS SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

(6) THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

(7) THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER METER (FOOT) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

REVISIONS NAME	DATE
M. DE YONG	05/28/91
A. HOUSEH	03/11/94
R. SHAH	02/24/95
R. SHAH	03/02/95
R. SHAH	08/19/96
R. SHAH	09/12/96
R. SHAH	09/19/96
R. SHAH	10/03/96
A. ABBAS	03/21/97
M. GOMEZ	01/22/01

ILLINOIS DEPARTMENT OF TRANSPORTATION

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

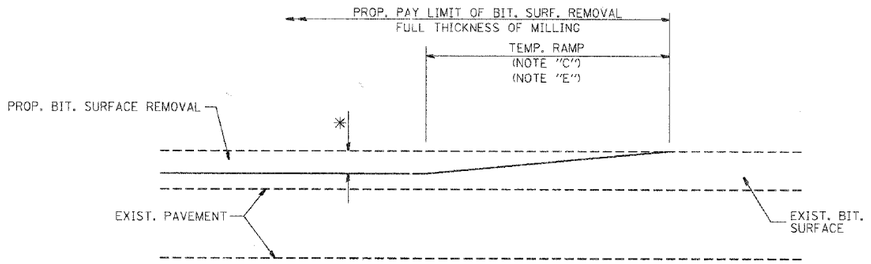
SCALE: VERT. HORIZ. DRAWN BY

DATE: 2/15/2006 CHECKED BY

11/10/2006 N:\BENSENVILLE\D691BG01\CIVIL\D691BG01_DET3.pln

F.A. RTE.	SECTION	COUNTY	TOTAL SHEET'S	SHEET NO.
-	06-00084-00-RS	DUPAGE	14	12
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

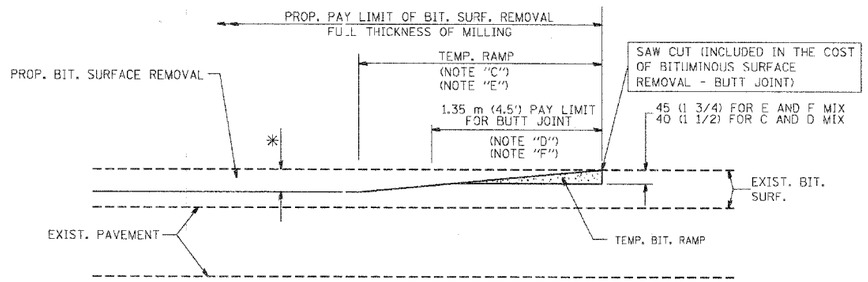
CONTRACT NO. 83887



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND BIT. TAPER SEE DETAIL BELOW)

OPTION 1

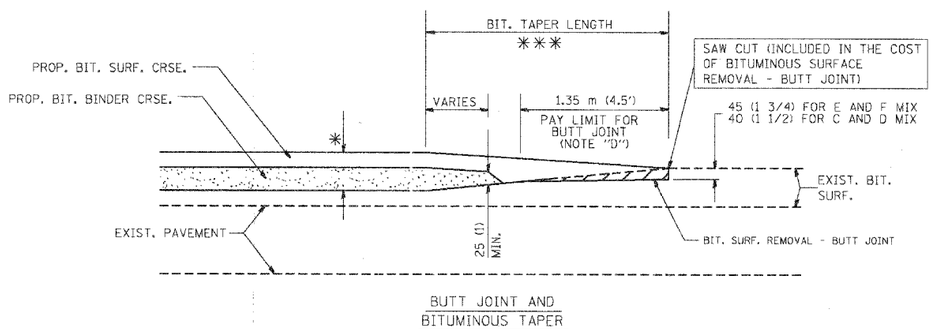


BITUMINOUS CONSTRUCTED TEMPORARY RAMP

(FOR BUTT JOINT AND BIT. TAPER SEE DETAIL BELOW)

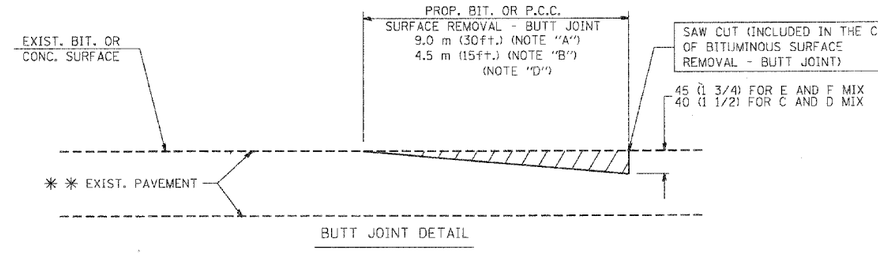
OPTION 2

TYPICAL TEMPORARY RAMP

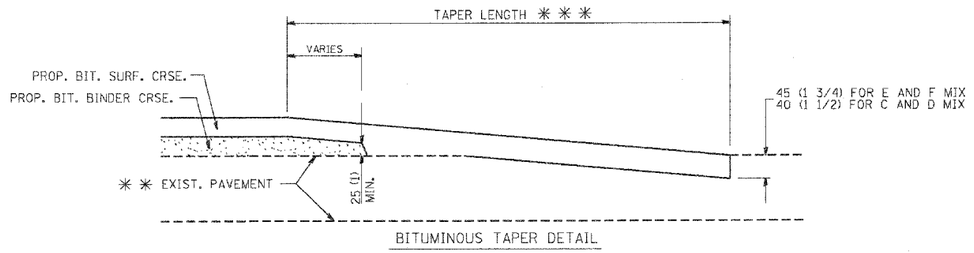


BUTT JOINT AND BITUMINOUS TAPER

TYPICAL BUTT JOINT AND BITUMINOUS TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



BITUMINOUS TAPER DETAIL

TYPICAL BUTT JOINT AND BITUMINOUS TAPER FOR RESURFACING ONLY

✳✳ PC CONCRETE, BITUMINOUS OR BITUMINOUS RESURFACED PAVEMENT.

NOTES

A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.

B: MINOR SIDE ROADS.

C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING BITUMINOUS SURFACE.

D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED BITUMINOUS COURSES.

E: TAPER THE TEMP. RAMP AT A RATE OF 900 (3 ft.) PER INCH OF MILLING THICKNESS.

F: INSTALLATION AND REMOVAL OF THE 1.35 m (4.5') TEMP. BIT. RAMP WILL BE PAID AS "BITUMINOUS SURFACE REMOVAL - BUTT JOINT".

G: SEE ARTICLE 406.18 AND 406.24 OF THE STANDARD SPECIFICATIONS FOR "BITUMINOUS AND PCC SURFACE REMOVAL, BUTT JOINT".

✳ SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

✳✳✳ 6.1 m (20') PER 25 (1) RESURFACING (NOTE "A")
3.0 m (10') PER 25 (1) RESURFACING (NOTE "B")

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR PER SQUARE METER (SQUARE YARD.) AS "BITUMINOUS SURFACE REMOVAL - BUTT JOINT" OR AS "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

REVISIONS	
NAME	DATE
M. DE YONG	6-13-90
M. DE YONG	7-3-90
M. DE YONG	3-27-92
R. SHAH	09/09/94
R. SHAH	10/25/94
A. ABBAS	03/21/97
M. GOMEZ	04/06/01

ILLINOIS DEPARTMENT OF TRANSPORTATION
VILLAGE OF BENSENVILLE

BUTT JOINT AND BITUMINOUS TAPER DETAILS

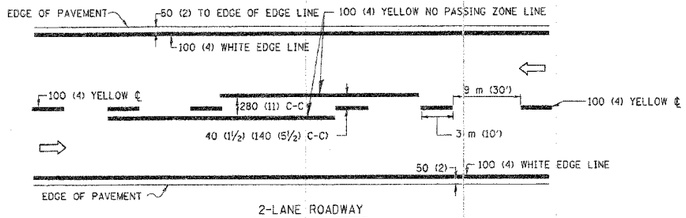
SCALE: VERT.
HORIZ.
DATE: 2/15/2006

DRAWN BY
CHECKED BY

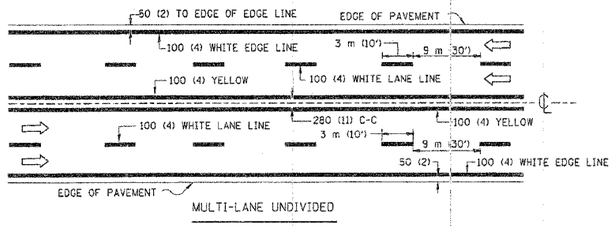
11/10/2006 N:\BENSENVILLE\0691BG01\CIVIL\0691BG01_DET2.PLN

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	06-00084-00-RS	DUPAGE	14	13
STA.	TO STA.			
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

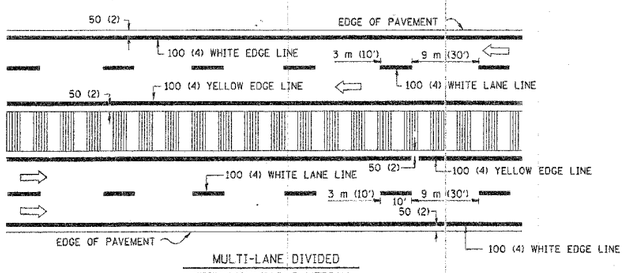
CONTRACT NO. 83887



2-LANE ROADWAY



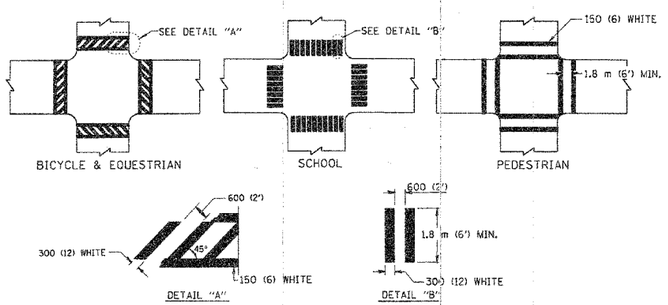
MULTI-LANE UNDIVIDED



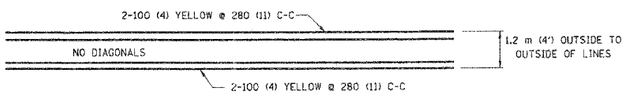
MULTI-LANE DIVIDED
WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

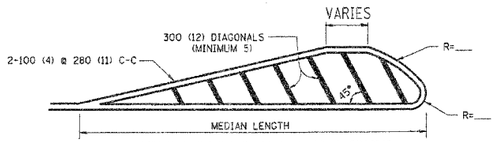
TYPICAL LANE AND EDGE LINE MARKING



TYPICAL CROSSWALK MARKING



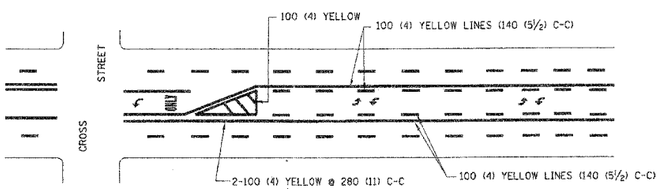
1.2 m (4') WIDE MEDIANS ONLY



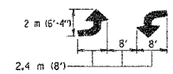
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 15 m (50') C-C (LESS THAN 50 km/h (30 MPH))
25 m (75') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH))
45 m (150') C-C (MORE THAN 70 km/h (45 MPH))

MEDIANS OVER 1.2 m (4') WIDE

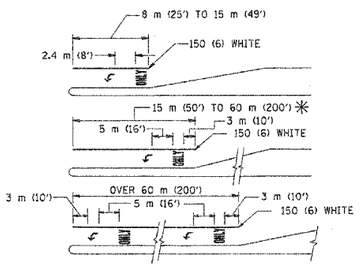


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED. WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 60 m (200') TO 90 m (300') INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

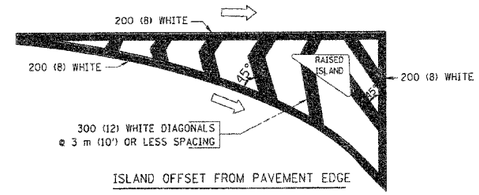


FULL SIZE LETTERS 2.4 m (8') AND ARROWS SHALL BE USED.
↰ AREA = 1.5 m^2 (15.6 SQ. FT.) ONLY AREA = 1.9 m^2 (20.8 SQ. FT.)

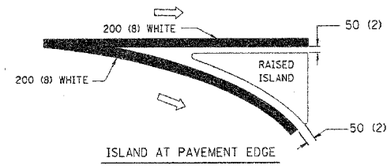
✳ TURN LANES IN EXCESS OF 120 m (400') IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



ISLAND OFFSET FROM PAVEMENT EDGE



ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	100 (4)	SKIP-DASH	YELLOW	3 m (10') LINE WITH 9 m (30') SPACE
CENTERLINE ON MULTI-LANE UNDIVEDED PAVEMENT	2 @ 100 (4)	SOLID	YELLOW	280 (11) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	100 (4) 2 @ 100 (4)	SOLID SOLID	YELLOW YELLOW	140 (5½) C-C FROM SKIP-DASH CENTERLINE 280 (11) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	100 (4) 125 (5) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	3 m (10') LINE WITH 9 m (30') SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	600 (2') LINE WITH 1.8 m (6') SPACE
EDGE LINES	100 (4)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	150 (6) LINE; FULL SIZE LETTERS & SYMBOLS (2.4 m (8'))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 100 (4) EACH DIRECTION 2.4 m (8') LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	3 m (10') LINE WITH 9 m (30') SPACE FOR SKIP-DASH; 140 (5½) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 150 (6) 300 (12) @ 45° 300 (12) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 1.8 m (6') APART 600 (2') APART 600 (2') APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	600 (24)	SOLID	WHITE	PLACE 1.2 m (4') IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 100 (4) WITH 300 (12) DIAGONALS @ 45° NO DIAGONALS USED FOR 1.2 m (4') WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	280 (11) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	200 (8) WITH 300 (12) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 4.5 m (15') C-C (LESS THAN 50 km/h (30 MPH)) 6 m (20') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH)) 9 m (30') C-C (OVER 70 km/h (45 MPH))
RAILROAD CROSSING	600 (24) TRANSVERSE LINES; "RR" IS 1.8 m (6') LETTERS; 400 (16) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=0.33 m^2 (3.6 SQ. FT.) EACH "X"=5.0 m^2 (54.0 SQ. FT.)
SHOULDER DIAGONALS	300 (12) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	15 m (50') C-C (LESS THAN 50 km/h (30 MPH)) 25 m (75') C-C (50 km/h (30 MPH) TO 70 km/h (45 MPH)) 45 m (150') C-C (OVER 70 km/h (45 MPH))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in millimeters (inches) unless otherwise shown.

REVISIONS	
NAME	DATE
EVERS	03-19-90
T. RAMMACHER	10-27-94
ALEX HOUSEH	10-09-96
ALEX HOUSEH	10-17-96
T. RAMMACHER	01-06-00

ILLINOIS DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

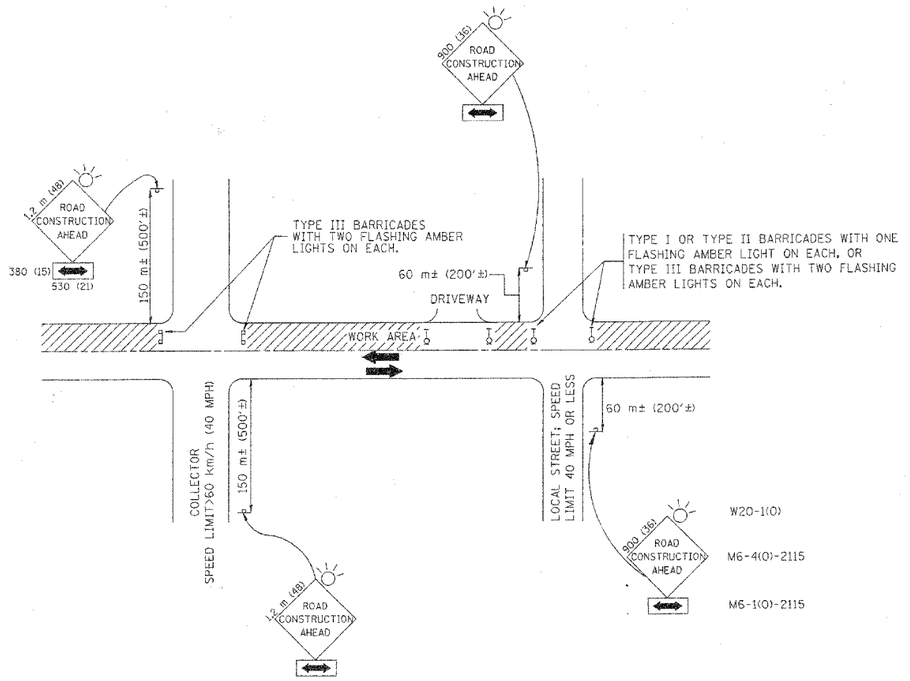
SCALE: NONE — DRAWN BY CADD

DATE 07/25/05 — CHECKED BY

11/10/2006 N:\BENSENVILLE\D691BG01\Civil\D691BG01_DET3.pln

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
-	06-00084-00-RS	DUPAGE	14	14
STA.		TO STA.		
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

CONTRACT NO. 83887



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 60 km/h (40 MPH) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

   a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 900x900 (36x36) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 60 m (200') IN ADVANCE OF THE MAIN ROUTE.

   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 60 km/h (40 MPH) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

   a) ONE **ROAD CONSTRUCTION AHEAD** SIGN 1.2 m x 1.2 m (48x48) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 150 m (500') IN ADVANCE OF THE MAIN ROUTE.

   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

REVISIONS	
NAME	DATE
LHA	6/89
T. RAMMACHER	09/06/94
J. OBERLE	10/18/95
A. HOUSEH	03/06/96
A. HOUSEH	10/15/96
T. RAMMACHER	01/06/00

ILLINOIS DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE | DRAWN BY

DATE: 07/25/05 | CHECKED BY

11/10/2006 N:\BENSENVILLE\06918G01\Civil\06918G01_DET3.prn