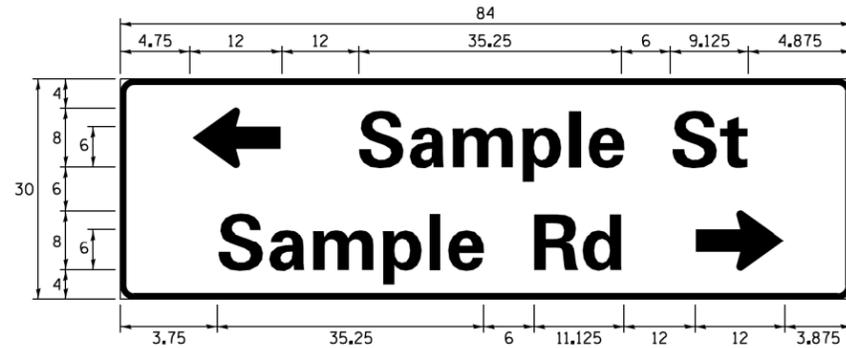
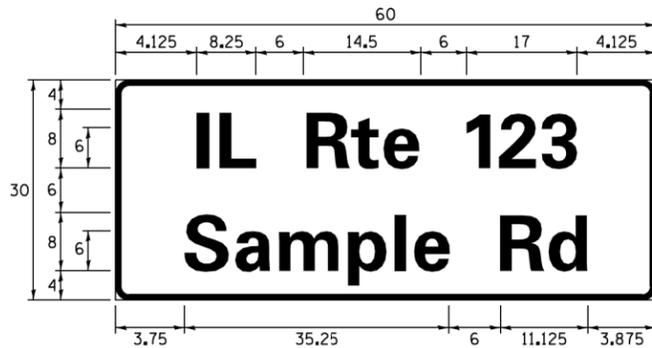
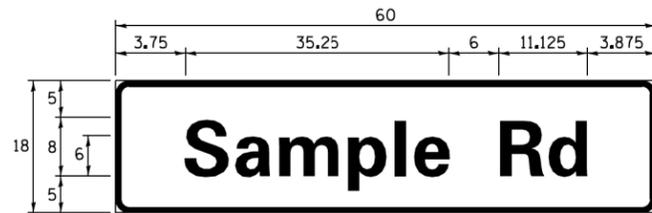


SIGN PANEL – TYPE 1 OR TYPE 2



DESIGN SERIES	AREA (SQ FT)	SIGN PANEL TYPE	SHEETING TYPE	QTY. REQUIRED
D OR C	-	1 OR 2	ZZ	-

ALL DIMENSIONS ARE IN INCHES EXCEPT NOTED OTHERWISE

COMMON STREET NAME ABBREVIATIONS AND WIDTHS

NAME	ABBREVIATION	WIDTH (INCH)	
		SERIES "C"	SERIES "D"
AVENUE	Ave	15.000	18.250
BOULEVARD	Blvd	17.125	20.000
CIRCLE	Cir	11.125	13.000
COURT	Ct	8.250	9.625
DRIVE	Dr	8.625	10.125
HIGHWAY	Hwy	18.375	22.000
ILLINOIS	IL	7.000	8.250
LANE	Ln	9.125	10.750
PARKWAY	Pkwy	23.375	27.375
PLACE	Pl	7.125	7.750
ROAD	Rd	9.625	11.125
ROUTE	Rte	12.625	14.500
STREET	St	8.000	9.125
TERRACE	Ter	12.625	14.625
TRAIL	Tr	7.750	9.125
UNITED STATES	US	10.375	12.250

GENERAL NOTES

- WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 877001, 877002, 877006, 877011 AND 877012, AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" x 8'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
- ALL SIGNS SHALL CONSIST OF A WHITE LEGEND AND BORDER (TYPE ZZ SHEETING) ON A GREEN BACKGROUND (TYPE ZZ SHEETING)
- THE SIGN LENGTH SHALL BE IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHALL NOT EXCEED 8'-0". ALL BORDERS SHALL BE 3/4" WIDE. CORNER RADIUS SHALL BE 1-7/8". THE SPACING BETWEEN THE WORDS SHOULD BE 6", IF POSSIBLE, BUT MAY BE REDUCED TO 5" WHEN SPACING IS CRITICAL. A MINIMUM OF 2-1/2" SHALL BE INCLUDED BETWEEN THE WORD AND THE RIGHT AND LEFT EDGES OF THE SIGN.
- A PREFERRED METHOD FOR THE SIGN DESIGN IS TO USE SERIES "D" LETTER ON A ONE-LINE SIGN 18" IN HEIGHT AND A MAXIMUM OF 8'-0" IN WIDTH. IF SERIES "D" DOES NOT FIT ON A 8'-0" SIGN, THEN SERIES "C" SHOULD BE TRIED. IF SERIES "C" DOES NOT FIT ON A 8'-0" SIGN, A 30" HIGH TWO-LINE SIGN CAN BE USED. THE CROSSROAD DESIGNATION AS TO STREET, AVENUE, ETC. SHOULD BE SPELLED OUT ON THE SECOND LINE, IF THE ABBREVIATION CANNOT FIT ON THE FIRST LINE.
- LED ILLUMINATED STREET NAME SIGNS CAN BE USED IN PLACE OF REGULAR SIGN PANELS BUT ANY SPECIAL WORDING AND SYMBOLGY MUST BE APPROVED BY THE DEPARTMENT. GENERAL DESIGN REQUIREMENT AS LISTED ABOVE (COLOR, FONT, SIZE, ETC.) MUST BE FOLLOWED.
- SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS.

LOCAL SUPPLIERS:

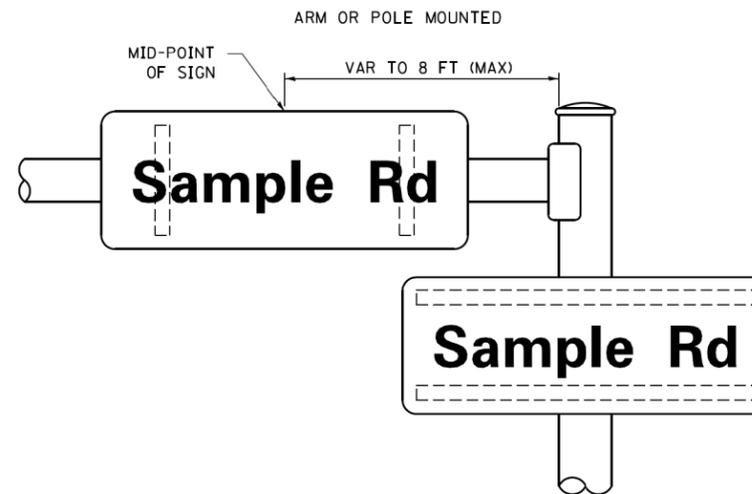
- J.O. HERBERT COMPANY, INC
MIDLOTHIAN, VA
- WESTERN REMAC, INC.
WOODRIDGE, IL

PARTS LISTING:

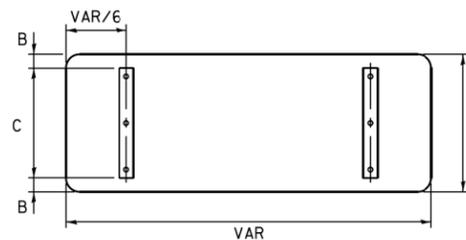
- SIGN CHANNEL PART #HPN053 (MED. CHANNEL)
SIGN SCREWS 1/4" x 14 x 1" H.W.H. #3
SELF TAPPING WITH NEOPRENE WASHER
PART #HPN034 (UNIVERSAL)
BRACKETS CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING

OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BACKET OF THE ABOVE PRODUCT.

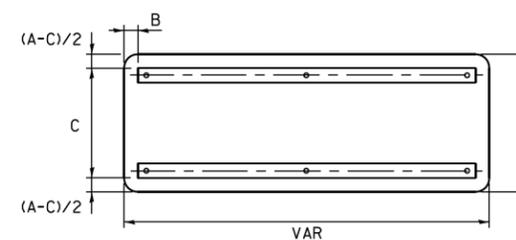
MOUNTING LOCATION



SUPPORTING CHANNELS



A	B	C
18"	2"	14"
30"	2"	24"

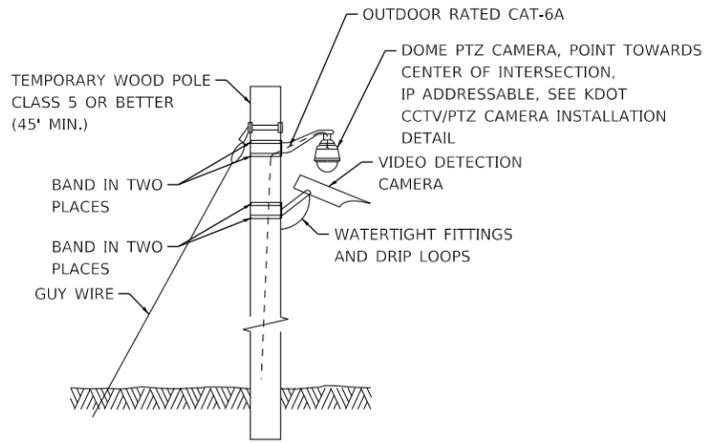


A	B	C
18"	2"	12"
30"	2"	22"

STANDARD ALPHABETS SPACING CHART

(8") UPPER CASE AND (6") LOWER CASE

FHWA SERIES "C"				FHWA SERIES "D"			
CHARACTER	LEFT SPACING (INCH)	WIDTH (INCH)	RIGHT SPACING (INCH)	CHARACTER	LEFT SPACING (INCH)	WIDTH (INCH)	RIGHT SPACING (INCH)
A	0.240	5.122	0.240	A	0.240	6.804	0.240
B	0.880	4.482	0.480	B	0.960	5.446	0.400
C	0.720	4.482	0.720	C	0.800	5.446	0.800
D	0.880	4.482	0.720	D	0.960	5.446	0.800
E	0.880	4.082	0.480	E	0.960	4.962	0.400
F	0.880	4.082	0.240	F	0.960	4.962	0.240
G	0.720	4.482	0.720	G	0.800	5.446	0.800
H	0.880	4.482	0.880	H	0.960	5.446	0.960
I	0.880	1.120	0.880	I	0.960	1.280	0.960
J	0.240	4.082	0.880	J	0.240	5.122	0.960
K	0.880	4.482	0.480	K	0.960	5.604	0.400
L	0.880	4.082	0.240	L	0.960	4.962	0.240
M	0.880	5.284	0.880	M	0.960	6.244	0.960
N	0.880	4.482	0.880	N	0.960	5.446	0.960
O	0.720	4.722	0.720	O	0.800	5.684	0.800
P	0.880	4.482	0.720	P	0.960	5.446	0.240
Q	0.720	4.722	0.720	Q	0.800	5.684	0.800
R	0.880	4.482	0.480	R	0.960	5.446	0.400
S	0.480	4.482	0.480	S	0.400	5.446	0.400
T	0.240	4.082	0.240	T	0.240	4.962	0.240
U	0.880	4.482	0.880	U	0.960	5.446	0.960
V	0.240	4.962	0.240	V	0.240	6.084	0.240
W	0.240	6.084	0.240	W	0.240	7.124	0.240
X	0.240	4.722	0.240	X	0.400	5.446	0.400
Y	0.240	5.122	0.240	Y	0.240	6.884	0.240
Z	0.480	4.482	0.480	Z	0.400	5.446	0.400
a	0.320	3.842	0.640	a	0.400	4.562	0.720
b	0.720	4.082	0.480	b	0.800	4.802	0.480
c	0.480	4.002	0.240	c	0.480	4.722	0.240
d	0.480	4.082	0.720	d	0.480	4.802	0.800
e	0.480	4.082	0.320	e	0.480	4.722	0.320
f	0.320	2.480	0.160	f	0.320	2.882	0.160
g	0.480	4.082	0.720	g	0.480	4.802	0.800
h	0.720	4.082	0.640	h	0.800	4.722	0.720
i	0.720	1.120	0.720	i	0.800	1.280	0.800
j	0.000	2.320	0.720	j	0.000	2.642	0.800
k	0.720	4.322	0.160	k	0.800	5.122	0.160
l	0.720	1.120	0.720	l	0.800	1.280	0.800
m	0.720	6.724	0.640	m	0.800	7.926	0.720
n	0.720	4.082	0.640	n	0.800	4.722	0.720
o	0.480	4.082	0.480	o	0.480	4.882	0.480
p	0.720	4.082	0.480	p	0.800	4.802	0.480
q	0.480	4.082	0.720	q	0.480	4.802	0.800
r	0.720	2.642	0.160	r	0.800	3.042	0.160
s	0.320	3.362	0.240	s	0.320	3.762	0.240
t	0.080	2.882	0.080	t	0.080	3.202	0.080
u	0.640	4.082	0.720	u	0.720	4.722	0.800
v	0.160	4.722	0.160	v	0.160	5.684	0.160
w	0.160	7.524	0.160	w	0.160	9.046	0.160
x	0.000	5.202	0.000	x	0.000	6.244	0.000
y	0.160	4.962	0.160	y	0.160	6.004	0.160
z	0.240	3.362	0.240	z	0.240	4.002	0.240
1	0.720	1.680	0.880	1	0.800	2.000	0.960
2	0.480	4.482	0.480	2	0.800	5.446	0.800
3	0.480	4.482	0.480	3	1.440	5.446	0.800
4	0.240	4.962	0.720	4	0.160	6.004	0.960
5	0.480	4.482	0.480	5	0.800	5.446	0.800
6	0.720	4.482	0.720	6	0.800	5.446	0.800
7	0.240	4.482	0.720	7	0.560	5.446	0.560
8	0.480	4.482	0.480	8	0.800	5.446	0.800
9	0.480	4.482	0.480	9	0.800	5.446	0.800
0	0.720	4.722	0.720	0	0.800	5.684	0.800
-	0.240	2.802	0.240	-	0.240	2.802	0.240



TEMPORARY PTZ CAMERA & VIDEO DETECTION MOUNTING DETAIL

REMOVAL AND RELOCATION NOTES

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 1 EACH CONTROLLER CABINET
- 14 EACH SIGNAL HEAD, 1-FACE, 3-SECTION
- 6 EACH TRAFFIC SIGNAL BACKPLATE
- 4 EACH STEEL MAST ARM ASSEMBLY AND POLE
- 3 EACH SIGNAL POST
- 1 EACH SERVICE INSTALLATION
- 1 EACH ENCODER
- 1 EACH PTZ JOYSTICK
- 1 EACH PTZ CONTROLLER EQUIPMENT
- 1 EACH PTZ ASSEMBLY AND EXTENSION PIPE

THE FOLLOWING EXISTING TRAFFIC SIGNAL EQUIPMENT SHALL BE REMOVED BY THE CONTRACTOR AND SHALL REMAIN THE PROPERTY OF THE AGENCY LISTED BELOW. THE CONTRACTOR SHALL SAFELY STORE AND ARRANGE A PICK UP OF ALL EQUIPMENTS TO BE RETURNED TO THE LISTED AGENCY AS PER THE TRAFFIC SIGNAL SPECIFICATIONS.

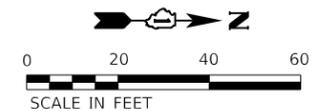
AGENCY: KANE COUNTY DIVISION OF TRANSPORTATION

- 3 EACH LIGHT DETECTOR
- 3 EACH CONFIRMATION BEACON
- 1 EACH EMERGENCY VEHICLE LIGHT DETECTOR AMPLIFIER
- 1 EACH PAN, TILT, ZOOM CAMERA
- 1 EACH CONTROLLER
- 1 EACH ETHERNET SWITCH

KANE COUNTY EXISTING INFORMATION TECHNOLOGY DOUBLE HANDHOLE TO REMAIN IN PLACE, SEE TEMPORARY INTERCONNECT PLANS

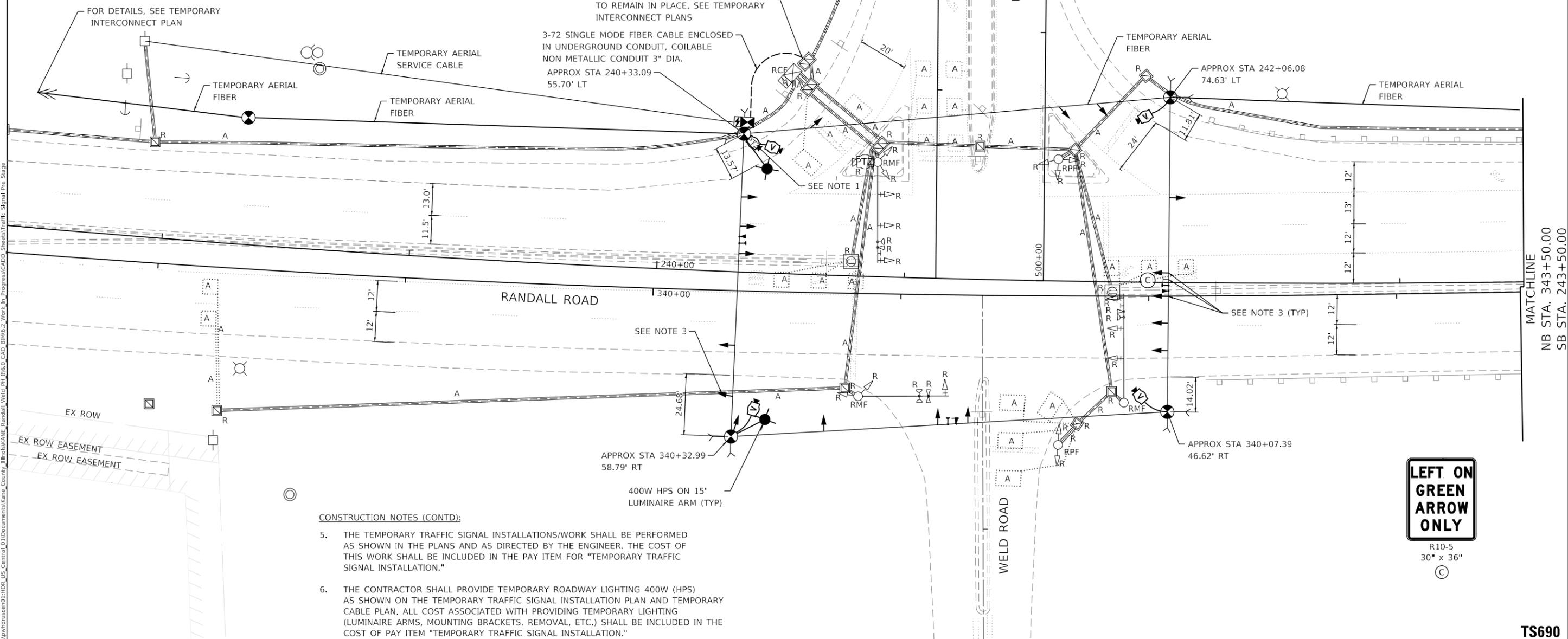
3-72 SINGLE MODE FIBER CABLE ENCLOSED IN UNDERGROUND CONDUIT, COILABLE NON METALLIC CONDUIT 3" DIA. APPROX STA 240+33.09 55.70' LT

NOTE:
THE LIGHT DETECTORS AND LIGHT DETECTOR AMPLIFIER FOR THIS PROJECT SHALL BE "3 COM OPTICOM" TO MEET THE LOCAL FIRE DEPARTMENT REQUIREMENTS.



CONSTRUCTION NOTES:

1. PTZ CAMERA SHOULD BE INSTALLED AS PER KANE COUNTY DIVISION OF TRANSPORTATION STANDARD SHOULD BE SAFELY RELOCATED TO THE PERMANENT SIGNAL AS SHOWN ON THE PROPOSED TRAFFIC PLAN.
2. ALL REMOVALS SHOWN ON THE TEMPORARY TRAFFIC SIGNAL PLAN SHALL BE SEQUENCED ACCORDING TO THEIR RESPECTIVE STAGE OF CONSTRUCTION OR AS DIRECTED BY THE ENGINEER.
3. ALL TEMPORARY SIGNAL HEADS AND SIGNS ASSOCIATED WITH STAGE CONSTRUCTION SHALL BE BAGGED TILL THE START OF STAGE 1 AND SHALL BE UNBAGGED ACCORDINGLY AS PER THE RESPECTIVE STAGE OF CONSTRUCTION AS SHOWN ON THE STAGE 1, 2 AND STAGE 2A PLANS.
4. THE ENTIRE CENTRALIZED TRAFFIC COMMUNICATIONS SYSTEM MUST BE COMMUNICATING ON THE DAY OF TEMPORARY TRAFFIC SIGNAL IS PLACED IN SERVICE. THE TEMPORARY SIGNALS MUST INCLUDE ALL THE NECESSARY CONTROL AND COMMUNICATIONS EQUIPMENT REQUIRED BY DOT TO MAINTAIN ALL EXISTING COMMUNICATIONS THROUGH OUT CONSTRUCTION. ALL COSTS FOR EQUIPMENT AND LABOR FOR MEETING THIS REQUIREMENT SHALL BE INCIDENTAL TO THE CONTRACT UNIT PRICE FOR "TEMPORARY TRAFFIC SIGNAL INSTALLATION". CONTRACTOR SHALL BE ALLOWED TWO WORKING DAYS FOR REQUIRED COORDINATION WITH THE COUNTY'S FIBER OPTIC COMMUNICATION CONSULTANT, NTI FIBER OPTICS.



CONSTRUCTION NOTES (CONTD):

5. THE TEMPORARY TRAFFIC SIGNAL INSTALLATIONS/WORK SHALL BE PERFORMED AS SHOWN IN THE PLANS AND AS DIRECTED BY THE ENGINEER. THE COST OF THIS WORK SHALL BE INCLUDED IN THE PAY ITEM FOR "TEMPORARY TRAFFIC SIGNAL INSTALLATION."
6. THE CONTRACTOR SHALL PROVIDE TEMPORARY ROADWAY LIGHTING 400W (HPS) AS SHOWN ON THE TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND TEMPORARY CABLE PLAN. ALL COST ASSOCIATED WITH PROVIDING TEMPORARY LIGHTING (LUMINAIRE ARMS, MOUNTING BRACKETS, REMOVAL, ETC.) SHALL BE INCLUDED IN THE COST OF PAY ITEM "TEMPORARY TRAFFIC SIGNAL INSTALLATION."



MODEL: Default FILE: \\nas01\projects\hndescen01\H08_US_Centrol_01\Documents\Kane_County_Illinois\KANE_Randall_Void_Plan\6.0_CAD\BIM6.2_Work_In_Progress\CADD_Sheets\Traffic_Signals_Pre_Stage



USER NAME = lln44	DESIGNED - IS	REVISED -
PLOT SCALE = 40,0000' / in.	DRAWN - IS	REVISED -
PLOT DATE = 01/03/2019	CHECKED - ST	REVISED -
	DATE - 01-03-19	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION
AND REMOVAL PLAN - RANDALL ROAD AT US 20 RAMP C**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	102
CONTRACT NO. 61F57				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

TS690

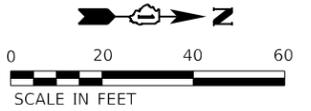
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CONSTRUCTION NOTES:

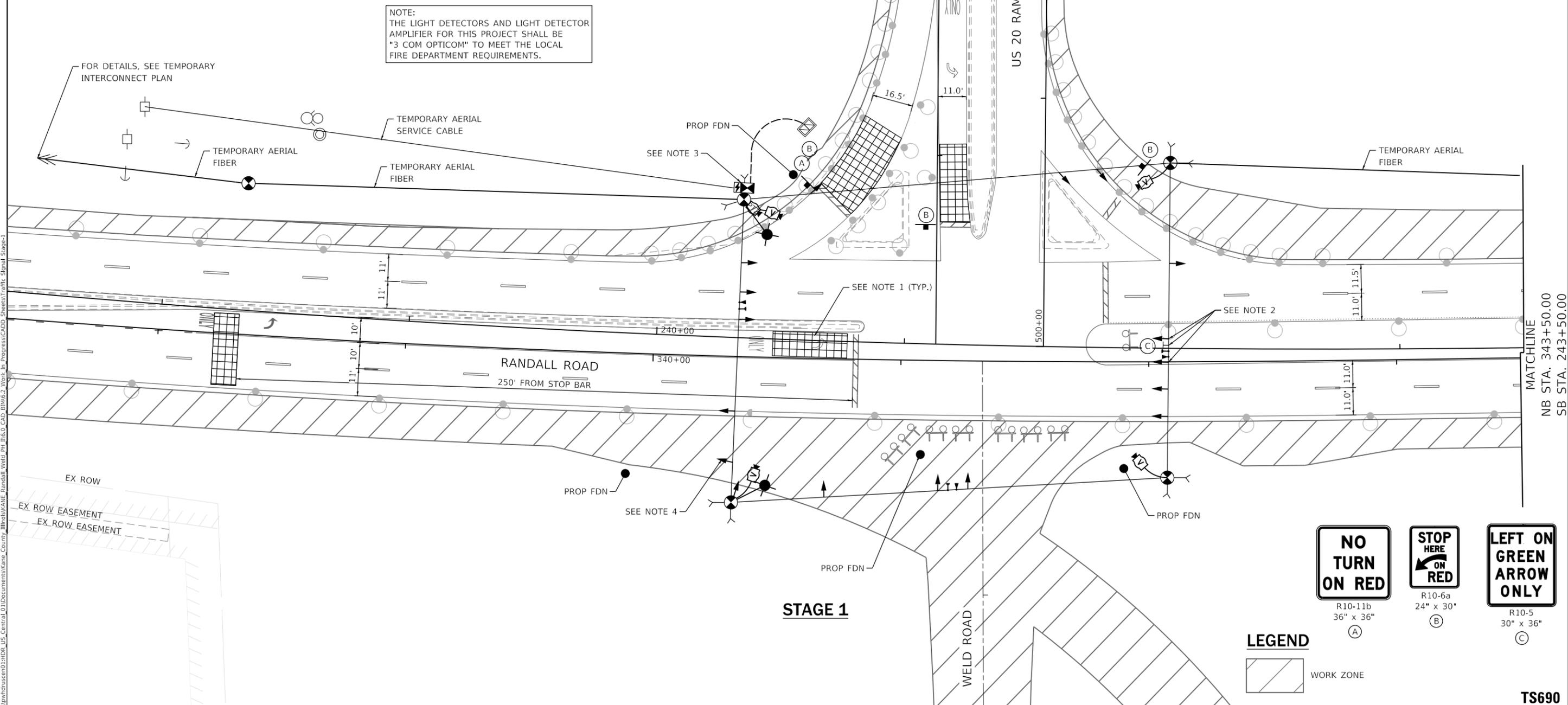
1. THE VIDEO DETECTION ZONES SHOWN ON THE PLANS ARE FOR CONSTRUCTION STAGE 1 AND SHALL BE REDEFINED FOR EACH CONSTRUCTION STAGE THEREAFTER AS A PART OF "TEMPORARY TRAFFIC SIGNAL INSTALLATION" WORK.
2. SIGNAL HEADS AND SIGNAGE ASSOCIATED WITH RANDALL ROAD LEFT TURNING MOVEMENTS SHALL BE BAGGED OR REMOVED WHEN THE APPROPRIATE TRAFFIC MOVEMENTS ARE PROHIBITED DUE TO ONGOING WORK IN STAGE 2 AND STAGE 2A.
3. THE ENTIRE CENTRALIZED TRAFFIC COMMUNICATIONS SYSTEM MUST BE COMMUNICATING ON THE DAY OF TEMPORARY TRAFFIC SIGNAL IS PLACED IN SERVICE. THE TEMPORARY SIGNALS MUST INCLUDE ALL THE NECESSARY CONTROL AND COMMUNICATIONS EQUIPMENT REQUIRED BY DOT TO MAINTAIN ALL EXISTING COMMUNICATIONS THROUGH OUT CONSTRUCTION. ALL COSTS FOR EQUIPMENT AND LABOR FOR MEETING THIS REQUIREMENT SHALL BE INCIDENTAL TO THE CONTRACT UNIT PRICE FOR "TEMPORARY TRAFFIC SIGNAL INSTALLATION". CONTRACTOR SHALL BE ALLOWED TWO WORKING DAYS FOR REQUIRED COORDINATION WITH THE COUNTY'S FIBER CONSULTANT, NTI FIBER OPTICS.

CONSTRUCTION NOTES (CONTD):

4. SIGNAL HEAD TO BE BAGGED AT THE START OF STAGE 1 AND REMAIN BAGGED THROUGH OUT STAGE 1. SIGNAL HEAD TO BE UNBAGGED AT THE START OF STAGE 2.
5. THE TEMPORARY TRAFFIC SIGNAL INSTALLATIONS/WORK SHALL BE PERFORMED AS SHOWN IN THE PLANS AND AS DIRECTED BY THE ENGINEER. THE COST OF THIS WORK SHALL BE INCLUDED IN THE PAY ITEM FOR "TEMPORARY TRAFFIC SIGNAL INSTALLATION."
6. THE CONTRACTOR SHALL PROVIDE TEMPORARY ROADWAY LIGHTING 400W (HPS) AS SHOWN ON THE TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN AND TEMPORARY CABLE PLAN. ALL COST ASSOCIATED WITH PROVIDING TEMPORARY LIGHTING (LUMINAIRE ARMS, MOUNTING BRACKETS, REMOVAL, ETC.) SHALL BE INCLUDED IN THE COST OF PAY ITEM "TEMPORARY TRAFFIC SIGNAL INSTALLATION."
7. ALL TEMPORARY SIGNAL HEADS SHALL BE ALIGNED WITH THE APPROACHING LANE PAVEMENT MARKINGS ACCORDINGLY, AS PER THEIR RESPECTIVE STAGE OF CONSTRUCTION AND AS DIRECTED BY THE ENGINEER.



NOTE:
THE LIGHT DETECTORS AND LIGHT DETECTOR AMPLIFIER FOR THIS PROJECT SHALL BE "3 COM OPTICOM" TO MEET THE LOCAL FIRE DEPARTMENT REQUIREMENTS.



NO TURN ON RED R10-11b 36" x 36" (A)	STOP HERE ON RED R10-6a 24" x 30" (B)	LEFT ON GREEN ARROW ONLY R10-5 30" x 36" (C)
--	---	--

LEGEND



STAGE 1

MODEL: Default; FILE: \\nas01\proj\19032019\01\1108_US_Centrol_01\Documents\Kane_County_Ill\proj\KANE_Randall_Void_Plan_016.0_CAD\BIM6.2_Work_In_Progress\CADD_Sheets\Traffic_Signals_Stage-1



USER NAME = lln44	DESIGNED - IS	REVISED -
PLOT SCALE = 40.0000' / in.	DRAWN - IS	REVISED -
PLOT DATE = 01/03/2019	CHECKED - ST	REVISED -
	DATE - 01-03-19	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION
MOT STAGE 1 - RANDALL ROAD AT US 20 RAMP C**

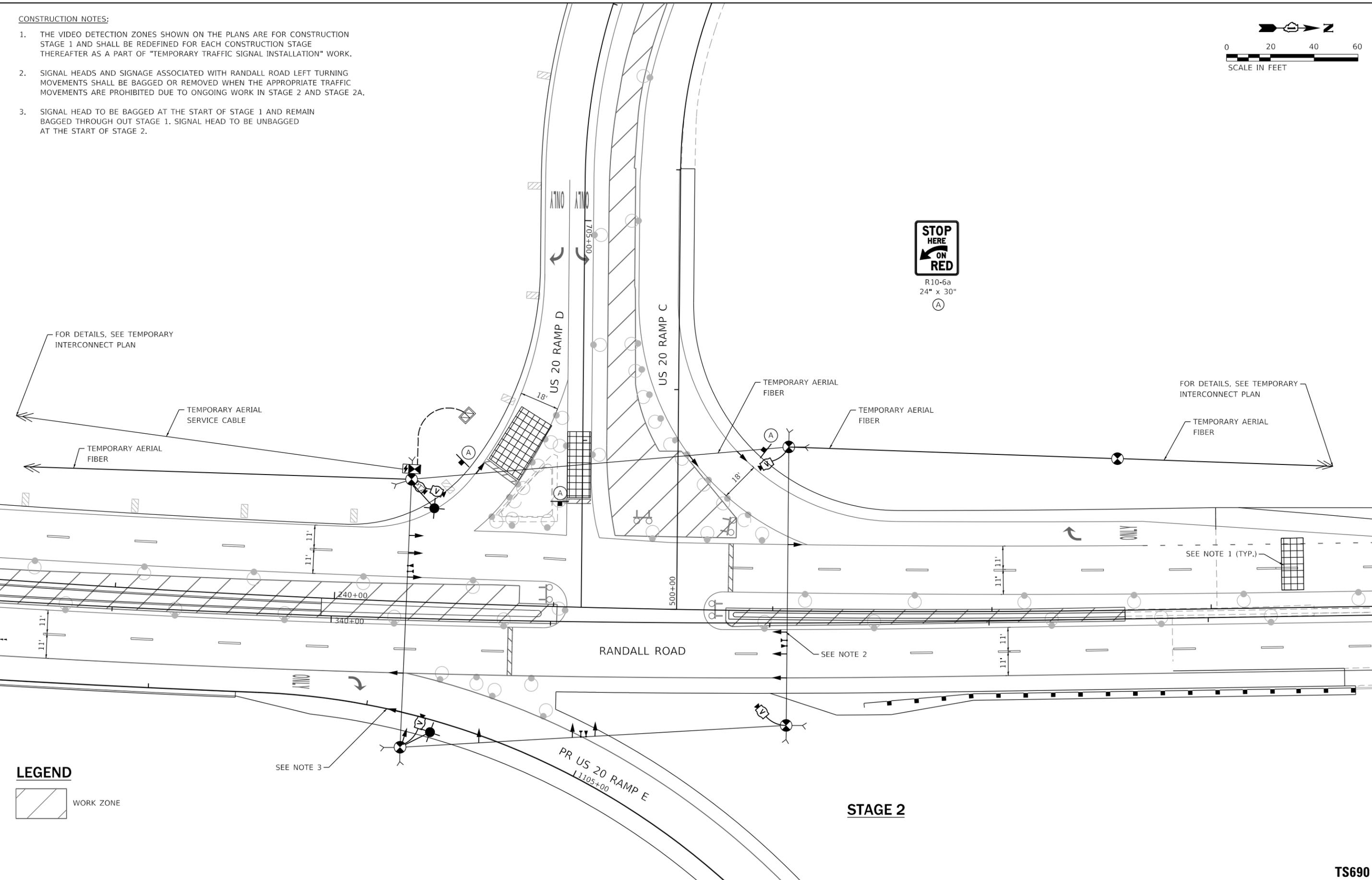
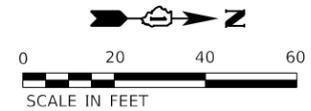
F.A.P. RTE. 336/345	SECTION 14-00446-00-CH	COUNTY KANE	TOTAL SHEETS 354	SHEET NO. 104
CONTRACT NO. 61F57				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

TS690

MATCHLINE
NB STA. 343+50.00
SB STA. 243+50.00

CONSTRUCTION NOTES:

1. THE VIDEO DETECTION ZONES SHOWN ON THE PLANS ARE FOR CONSTRUCTION STAGE 1 AND SHALL BE REDEFINED FOR EACH CONSTRUCTION STAGE THEREAFTER AS A PART OF "TEMPORARY TRAFFIC SIGNAL INSTALLATION" WORK.
2. SIGNAL HEADS AND SIGNAGE ASSOCIATED WITH RANDALL ROAD LEFT TURNING MOVEMENTS SHALL BE BAGGED OR REMOVED WHEN THE APPROPRIATE TRAFFIC MOVEMENTS ARE PROHIBITED DUE TO ONGOING WORK IN STAGE 2 AND STAGE 2A.
3. SIGNAL HEAD TO BE BAGGED AT THE START OF STAGE 1 AND REMAIN BAGGED THROUGH OUT STAGE 1. SIGNAL HEAD TO BE UNBAGGED AT THE START OF STAGE 2.



LEGEND



MODEL: Default
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 PROJECT: 14-00446-00-CH
 SHEET: 1 OF 2
 DATE: 01/03/2019
 DRAWN: IS
 CHECKED: ST
 DESIGNED: IS
 REVISIONS:

LIN ENGINEERING, LTD.
Consulting Engineers
Westmont, Illinois

USER NAME = lin44	DESIGNED - IS	REVISED -
DRAWN - IS	REVISIONS	
PLOT SCALE = 40.0000' / in.	CHECKED - ST	REVISED -
PLOT DATE = 01/03/2019	DATE - 01-03-19	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION
MOT STAGE 2 - RANDALL ROAD AT US 20 RAMP C**

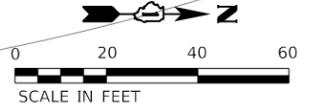
SCALE: 1"=20' SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.P. RTE. 336/345	SECTION 14-00446-00-CH	COUNTY KANE	TOTAL SHEETS 354	SHEET NO. 107
CONTRACT NO. 61F57				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

TS690

EX ROW

NOTE:
THE LIGHT DETECTORS AND LIGHT DETECTOR
AMPLIFIER FOR THIS PROJECT SHALL BE
"3 COM OPTICOM" TO MEET THE LOCAL
FIRE DEPARTMENT REQUIREMENTS.



5'-UC-2"
18' POST
APPROX STA 701+41.52,
13.69' RT
9' X 6' QUEUE AND SAMPLING
(SYSTEM) DETECTOR

232'-UC-3"

US 20 RAMP C

US 20 RAMP D

US 20 RAMP C

US 20 RAMP D

INTERSECTION AND SAMPLING
(SYSTEM) DETECTORS

119'-UC-3"

FROM STOP BAR

MATCHLINE STA 705+30
SEE PREVIOUS SHEET

NOTE:

1. EACH DETECTOR LOOP SHALL HAVE ITS OWN 1" COILABLE NON-METALLIC CONDUIT BETWEEN THE EDGE OF PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND AS STATED IN THE TRAFFIC SIGNAL SPECIFICATIONS.

MODEL: Default
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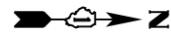
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DRAWN - IS	REVISIONS -	
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PLOT DATE = 01/03/2019	DATE - 01-03-19	REVISIONS -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

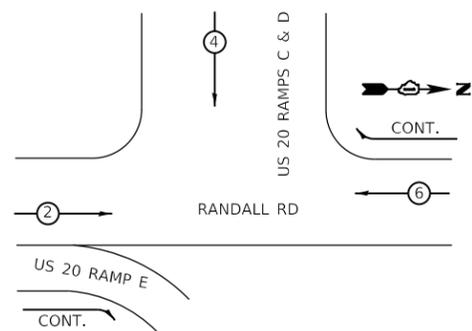
PROPOSED TRAFFIC SIGNAL PLAN RANDALL ROAD	
SCALE: 1=20'	SHEET 2 OF 2 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	111
CONTRACT NO. 61F57				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

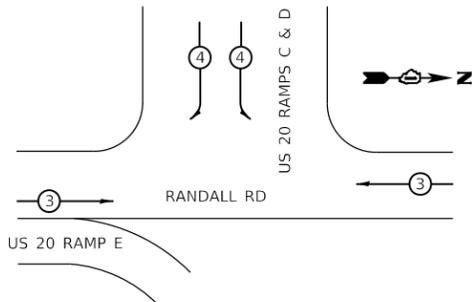
TS690



PROPOSED CONTROLLER SEQUENCE



PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE

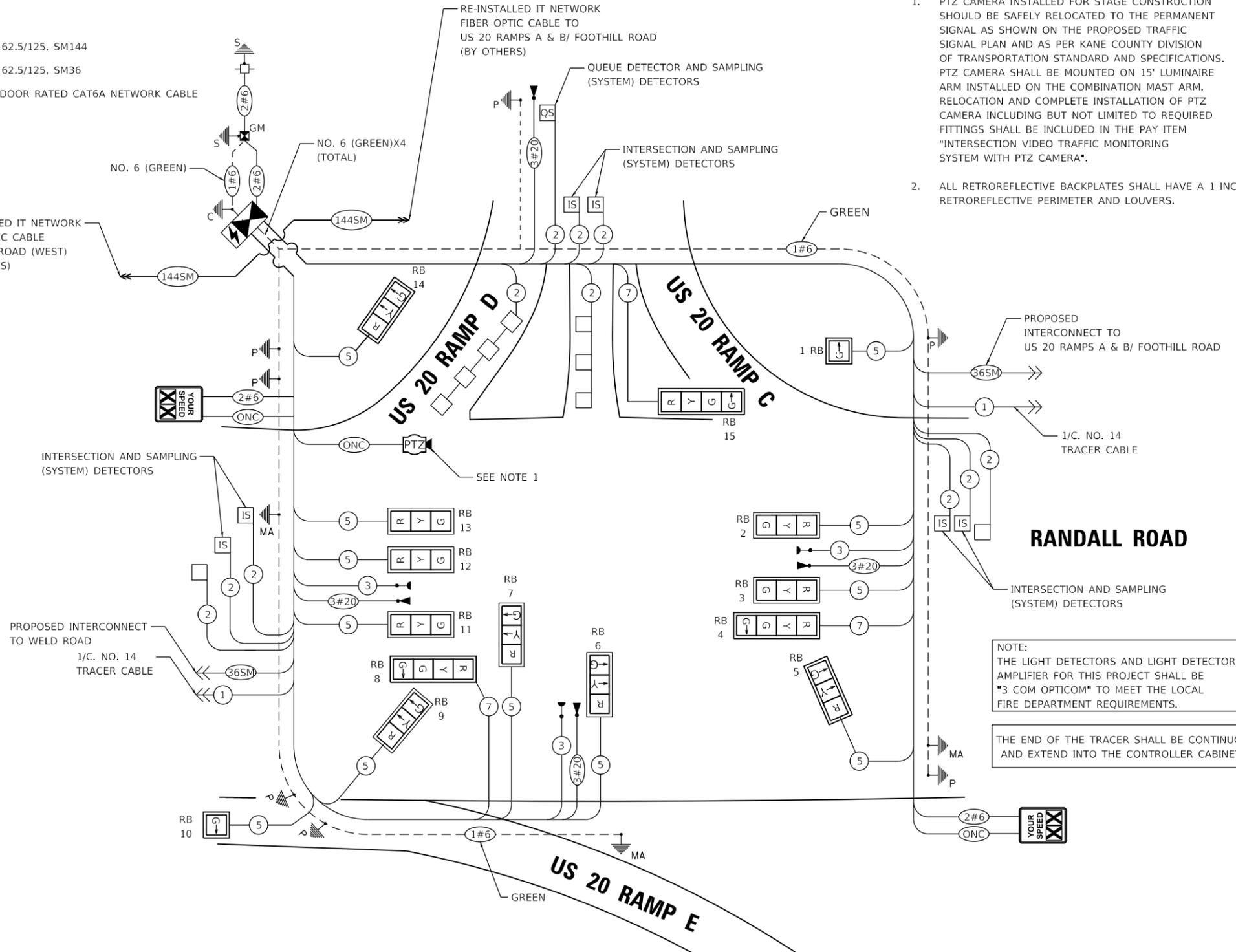


NOTE:
THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE "NTCIP", COMPATIBLE WITH KANE COUNTY CENTRAL SYSTEM SOFTWARE

LEGEND

- (144SM) NO. 62.5/125, SM144
- (36SM) NO. 62.5/125, SM36
- (ONC) OUTDOOR RATED CAT6A NETWORK CABLE

REINSTALLED IT NETWORK FIBER OPTIC CABLE TO WELD ROAD (WEST) (BY OTHERS)



NOTES

1. PTZ CAMERA INSTALLED FOR STAGE CONSTRUCTION SHOULD BE SAFELY RELOCATED TO THE PERMANENT SIGNAL AS SHOWN ON THE PROPOSED TRAFFIC SIGNAL PLAN AND AS PER KANE COUNTY DIVISION OF TRANSPORTATION STANDARD AND SPECIFICATIONS. PTZ CAMERA SHALL BE MOUNTED ON 15' LUMINAIRE ARM INSTALLED ON THE COMBINATION MAST ARM. RELOCATION AND COMPLETE INSTALLATION OF PTZ CAMERA INCLUDING BUT NOT LIMITED TO REQUIRED FITTINGS SHALL BE INCLUDED IN THE PAY ITEM "INTERSECTION VIDEO TRAFFIC MONITORING SYSTEM WITH PTZ CAMERA".
2. ALL RETROREFLECTIVE BACKPLATES SHALL HAVE A 1 INCH RETROREFLECTIVE PERIMETER AND LOUVERS.

NOTE:
THE LIGHT DETECTORS AND LIGHT DETECTOR AMPLIFIER FOR THIS PROJECT SHALL BE "3 COM OPTICOM" TO MEET THE LOCAL FIRE DEPARTMENT REQUIREMENTS.

THE END OF THE TRACER SHALL BE CONTINUOUS AND EXTEND INTO THE CONTROLLER CABINET.

TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. OF LAMPS	WATTAGE		PERCENT % OPERATION	TOTAL WATTAGE
		INCAND.	LED		
SIGNAL (RED)	13	-	10	50	65.0
(YELLOW)	13	-	13	5	8.5
(GREEN)	13	-	10	45	58.5
ARROW (EXCEPT FOR FYA)	5	-	10	10	5.0
FLASHING YELLOW ARROW (FYA)	-	-	12	30	-
PEDESTRIAN SIGNAL	-	-	20	100	-
CONTROLLER	1	-	100	100	100.0
UPS	1	-	25	100	25.0
RADAR SPEED SIGN	2	-	15	100	30.0
VIDEO DETECTION SYSTEM	-	-	150	100	-
ILLUM. STREET NAME SIGN	-	-	120	50	-
LUMINAIRE (COMBO POLE)	-	-	-	50	-
PTZ / SURVEILLANCE CAMERA	1	-	60	100	60.0
FLASHING BEACON (SIGNAL)	-	-	-	50	-
ENERGY COSTS TO:				TOTAL =	352.0

CABLE PLAN (NOT TO SCALE)

ILLINOIS DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAY/DISTRICT 1
201 WEST CENTER COURT/SCHAUMBURG, ILLINOIS 60196-1096

ENERGY SUPPLY: CONTACT: _____
PHONE: _____
COMPANY: _____



USER NAME = lin44	DESIGNED - IS	REVISED -
PLOT SCALE = 40,0000 ' / in.	DRAWN - IS	REVISED -
PLOT DATE = 01/03/2019	CHECKED - ST	REVISED -
	DATE - 01-03-19	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

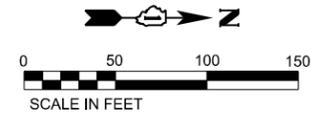
CABLE PLAN RANDALL ROAD

SCALE: N.T.S. SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	112
CONTRACT NO. 61F57				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

TS690

MODEL: Default FILE: \\lin\c:\p\lin\p\h\res\01\h08_US_Central_01\Documents\Kane_County_Illinois\KANE_Randall_Weld_Rd_IL6.0_CAD\BIM6.2_Work_In_Progress\CADD_Sheets\Proposed_Traffic_Signal_Cable_Plan



EXISTING KANE IT HANDHOLE TO REMAIN AND BE USED FOR TEMPORARY SPLICES AND SLACK STORAGE. CONTRACTOR SHALL INSTALL AT LEAST 13 FEET OF SLACK FROM EACH CABLE RUN WITHIN THIS EXISTING HANDHOLE. CONTRACTOR SHALL ALSO INSTALL 72 SINGLE MODE FIBER CABLE (ADSS, ALL DIELECTRIC SELF SUPPORTING TYPE) BETWEEN KANE IT HANDHOLE AND TEMP SIGNAL CABINET. ALL TERMINATIONS IN EXISTING KANE IT HANDHOLE AND SIGNAL CABINET SHALL BE DONE BY COUNTY'S FIBER OPTIC COMMUNICATION CONSULTANT. CONTRACTOR SHALL BE RESPONSIBLE FOR ALL HARDWARE AND CONFIGURATION BEGINNING WITH THE FIBER OPTIC JUMPERS CONNECTING AT THE ST FIBER OPTIC INTERFACE PANEL.

OVERSIZED FIBERGLASS HANDHOLE (SPECIAL HH), 48"x48"x48", OPEN BOTTOM, INSTALLED AT FIRST HANDHOLE WHICH IS AN EXISTING TO REMAIN LOCATION; INSTALL 2 INCH CONDUIT BETWEEN EXISTING HH AND SPECIAL HH. DISCONNECT FIBER CABLE AT SIGNAL CABINET AT RANDALL/US 20 RAMP D/C AND PULL CABLE BACK TO THIS FIBERGLASS HANDHOLE, AND STORE ALL SLACK IN THIS HH. CONTRACTOR TO INSTALL NEW OUTDOOR RATED 72 SM FIBER CABLE (ADSS, ALL DIELECTRIC SELF-SUPPORTING TYPE) FROM THIS SPECIAL HH AND STRUNG AERIALY FROM WOOD POLES BACK TO TEMP SIGNAL CABINET POLE. LEAD CABLE INTO THE EXISTING KANE IT HANDHOLE. THE EXISTING FIBER OPTIC CABLE SHALL BE SPLICED TO THE TEMPORARY AERIAL 72 SM FIBER OPTIC CABLE IN THIS SPECIAL HANDHOLE. TEMPORARY CABLE TERMINATIONS TO BE DONE BY COUNTY'S FIBER OPTIC COMMUNICATION CONSULTANT. CONTRACTOR SHALL COORDINATE WITH COUNTY'S FIBER OPTIC COMMUNICATION CONSULTANT ON WHEN THE SPLICING WORK CAN OCCUR TO THE PREFERENCE OF COUNTY'S FIBER OPTIC CONSULTANT.

3-72 SINGLE MODE FIBER CABLE ENCLOSED IN UNDERGROUND CONDUIT, COILABLE NON METALLIC CONDUIT 3" DIA.

APPROX. SB STA 237+20.64, 46.88' LT

6'-UC-2"

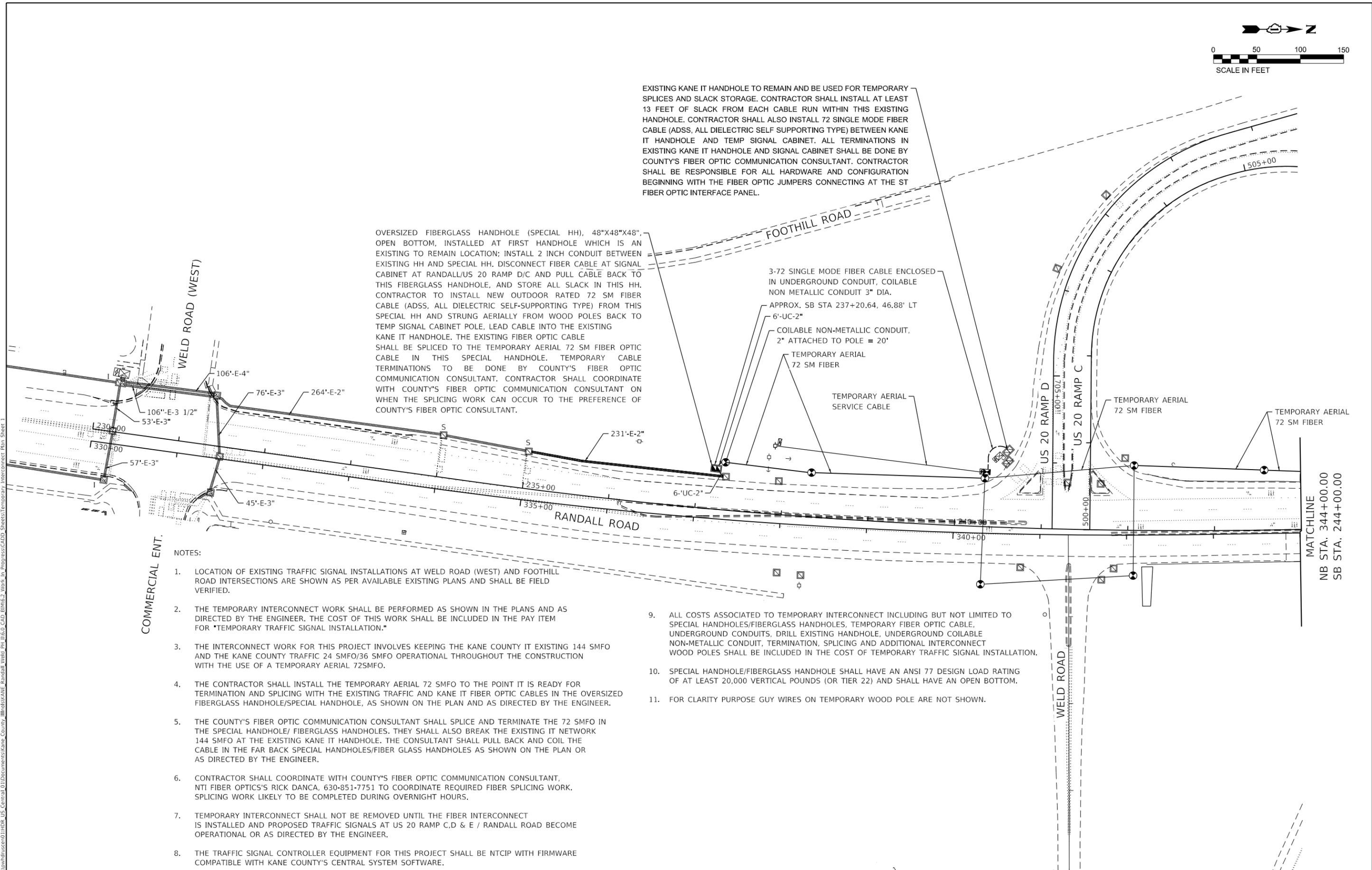
COILABLE NON-METALLIC CONDUIT, 2" ATTACHED TO POLE = 20'

TEMPORARY AERIAL 72 SM FIBER

TEMPORARY AERIAL SERVICE CABLE

TEMPORARY AERIAL 72 SM FIBER

TEMPORARY AERIAL 72 SM FIBER



COMMERCIAL ENT.

NOTES:

1. LOCATION OF EXISTING TRAFFIC SIGNAL INSTALLATIONS AT WELD ROAD (WEST) AND FOOTHILL ROAD INTERSECTIONS ARE SHOWN AS PER AVAILABLE EXISTING PLANS AND SHALL BE FIELD VERIFIED.
2. THE TEMPORARY INTERCONNECT WORK SHALL BE PERFORMED AS SHOWN IN THE PLANS AND AS DIRECTED BY THE ENGINEER. THE COST OF THIS WORK SHALL BE INCLUDED IN THE PAY ITEM FOR "TEMPORARY TRAFFIC SIGNAL INSTALLATION."
3. THE INTERCONNECT WORK FOR THIS PROJECT INVOLVES KEEPING THE KANE COUNTY IT EXISTING 144 SMFO AND THE KANE COUNTY TRAFFIC 24 SMFO/36 SMFO OPERATIONAL THROUGHOUT THE CONSTRUCTION WITH THE USE OF A TEMPORARY AERIAL 72SMFO.
4. THE CONTRACTOR SHALL INSTALL THE TEMPORARY AERIAL 72 SMFO TO THE POINT IT IS READY FOR TERMINATION AND SPLICING WITH THE EXISTING TRAFFIC AND KANE IT FIBER OPTIC CABLES IN THE OVERSIZED FIBERGLASS HANDHOLE/SPECIAL HANDHOLE, AS SHOWN ON THE PLAN AND AS DIRECTED BY THE ENGINEER.
5. THE COUNTY'S FIBER OPTIC COMMUNICATION CONSULTANT SHALL SPLICE AND TERMINATE THE 72 SMFO IN THE SPECIAL HANDHOLE/ FIBERGLASS HANDHOLES. THEY SHALL ALSO BREAK THE EXISTING IT NETWORK 144 SMFO AT THE EXISTING KANE IT HANDHOLE. THE CONSULTANT SHALL PULL BACK AND COIL THE CABLE IN THE FAR BACK SPECIAL HANDHOLES/FIBER GLASS HANDHOLES AS SHOWN ON THE PLAN OR AS DIRECTED BY THE ENGINEER.
6. CONTRACTOR SHALL COORDINATE WITH COUNTY'S FIBER OPTIC COMMUNICATION CONSULTANT, NTL FIBER OPTICS'S RICK DANCA, 630-851-7751 TO COORDINATE REQUIRED FIBER SPLICING WORK. SPLICING WORK LIKELY TO BE COMPLETED DURING OVERNIGHT HOURS.
7. TEMPORARY INTERCONNECT SHALL NOT BE REMOVED UNTIL THE FIBER INTERCONNECT IS INSTALLED AND PROPOSED TRAFFIC SIGNALS AT US 20 RAMP C,D & E / RANDALL ROAD BECOME OPERATIONAL OR AS DIRECTED BY THE ENGINEER.
8. THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE NTCIP WITH FIRMWARE COMPATIBLE WITH KANE COUNTY'S CENTRAL SYSTEM SOFTWARE.

9. ALL COSTS ASSOCIATED TO TEMPORARY INTERCONNECT INCLUDING BUT NOT LIMITED TO SPECIAL HANDHOLES/FIBERGLASS HANDHOLES, TEMPORARY FIBER OPTIC CABLE, UNDERGROUND CONDUITS, DRILL EXISTING HANDHOLE, UNDERGROUND COILABLE NON-METALLIC CONDUIT, TERMINATION, SPLICING AND ADDITIONAL INTERCONNECT WOOD POLES SHALL BE INCLUDED IN THE COST OF TEMPORARY TRAFFIC SIGNAL INSTALLATION.
10. SPECIAL HANDHOLE/FIBERGLASS HANDHOLE SHALL HAVE AN ANSI 77 DESIGN LOAD RATING OF AT LEAST 20,000 VERTICAL POUNDS (OR TIER 22) AND SHALL HAVE AN OPEN BOTTOM.
11. FOR CLARITY PURPOSE GUY WIRES ON TEMPORARY WOOD POLE ARE NOT SHOWN.

MATCHLINE
NB STA. 344+00.00
SB STA. 244+00.00

MODEL: Default; FILE: \\hpc\p\proj\hpc\hpc\01\H08_US_Central_01\Documents\Kane_County_Ill\hpc\KANE_Randall_Void_PN_IL03_CAD_01M6_2_Work_In_Progress\CADD_Sheets\Temporary Interconnect Plan Sheet 1

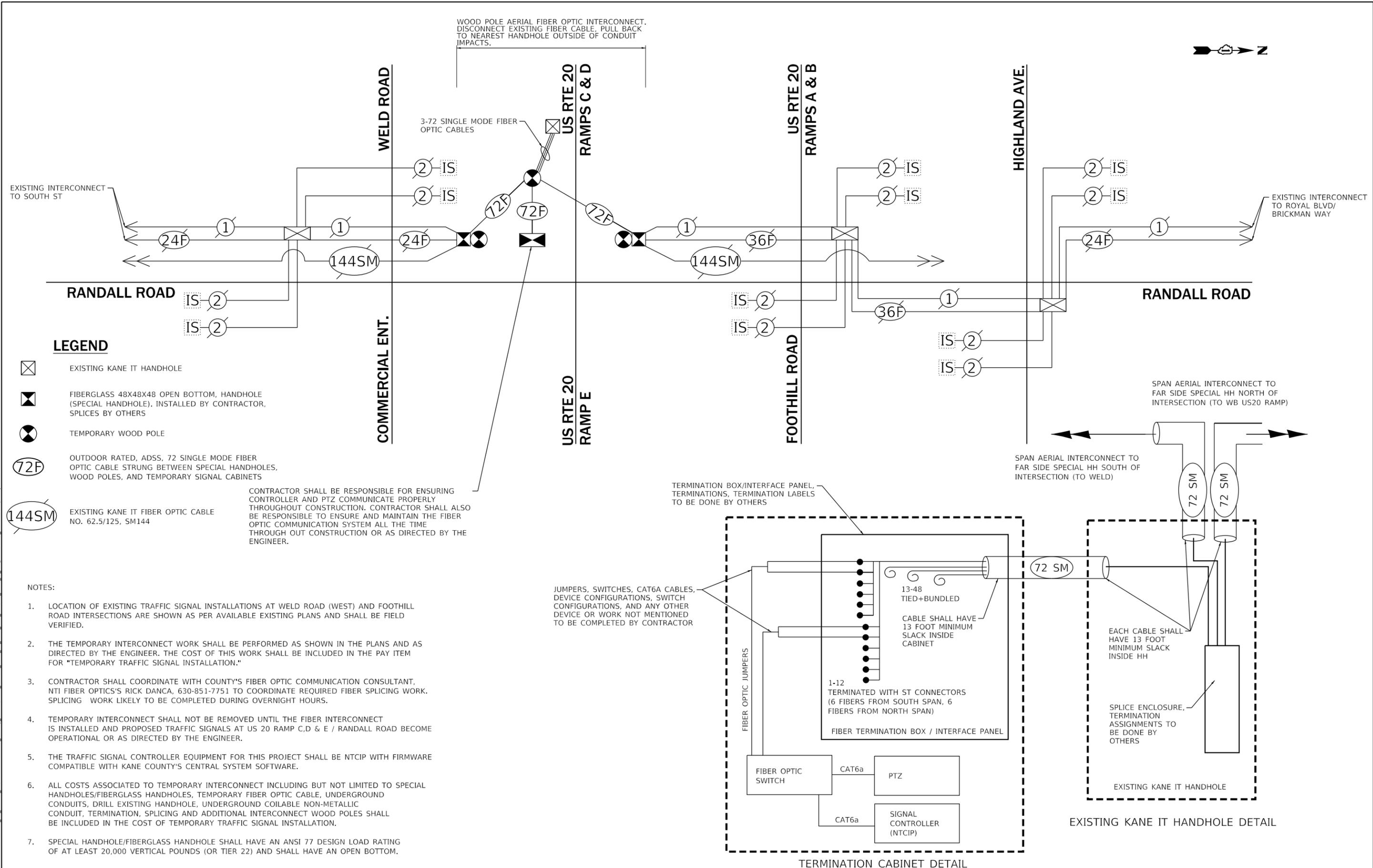


USER NAME = lin44	DESIGNED - IS	REVISED -
	DRAWN - IS	REVISED -
PLOT SCALE = 100,0002' / in.	CHECKED - ST	REVISED -
PLOT DATE = 01/03/2019	DATE - 01-03-19	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY INTERCONNECT PLAN	
RANDALL ROAD	
SCALE: 1" = 50'	SHEET 1 OF 2 SHEETS
STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	114
CONTRACT NO. 61F57				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



LEGEND

- EXISTING KANE IT HANDHOLE
- FIBERGLASS 48X48X48 OPEN BOTTOM, HANDHOLE (SPECIAL HANDHOLE). INSTALLED BY CONTRACTOR, SPLICES BY OTHERS
- TEMPORARY WOOD POLE
- OUTDOOR RATED, ADSS, 72 SINGLE MODE FIBER OPTIC CABLE STRUNG BETWEEN SPECIAL HANDHOLES, WOOD POLES, AND TEMPORARY SIGNAL CABINETS
- EXISTING KANE IT FIBER OPTIC CABLE NO. 62.5/125, SM144

CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING CONTROLLER AND PTZ COMMUNICATE PROPERLY THROUGHOUT CONSTRUCTION. CONTRACTOR SHALL ALSO BE RESPONSIBLE TO ENSURE AND MAINTAIN THE FIBER OPTIC COMMUNICATION SYSTEM ALL THE TIME THROUGH OUT CONSTRUCTION OR AS DIRECTED BY THE ENGINEER.

NOTES:

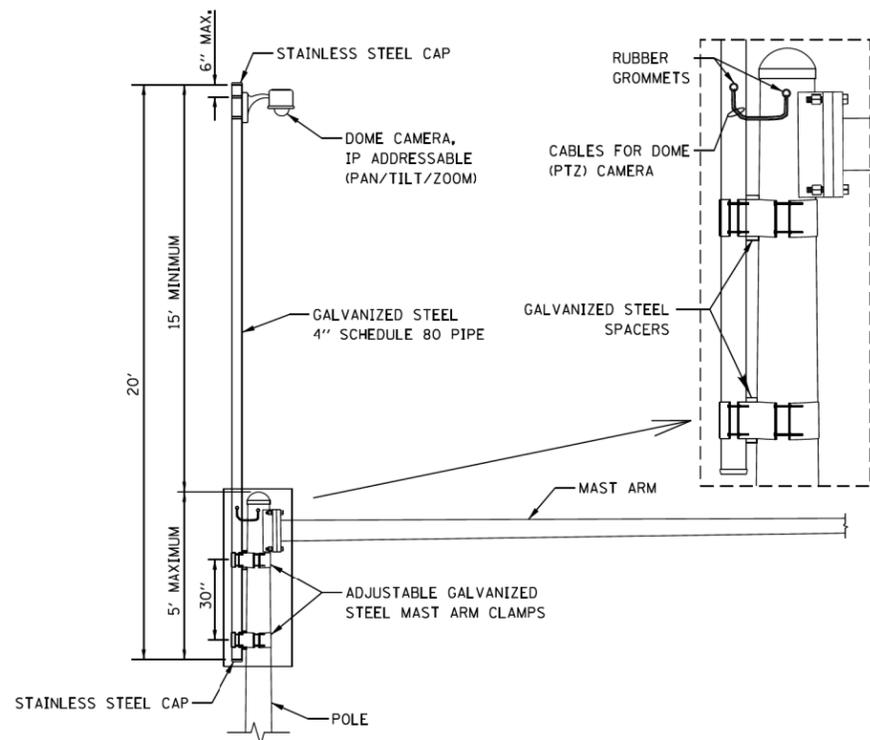
1. LOCATION OF EXISTING TRAFFIC SIGNAL INSTALLATIONS AT WELD ROAD (WEST) AND FOOTHILL ROAD INTERSECTIONS ARE SHOWN AS PER AVAILABLE EXISTING PLANS AND SHALL BE FIELD VERIFIED.
2. THE TEMPORARY INTERCONNECT WORK SHALL BE PERFORMED AS SHOWN IN THE PLANS AND AS DIRECTED BY THE ENGINEER. THE COST OF THIS WORK SHALL BE INCLUDED IN THE PAY ITEM FOR "TEMPORARY TRAFFIC SIGNAL INSTALLATION."
3. CONTRACTOR SHALL COORDINATE WITH COUNTY'S FIBER OPTIC COMMUNICATION CONSULTANT, NFI FIBER OPTICS'S RICK DANCA, 630-851-7751 TO COORDINATE REQUIRED FIBER SPLICING WORK. SPLICING WORK LIKELY TO BE COMPLETED DURING OVERNIGHT HOURS.
4. TEMPORARY INTERCONNECT SHALL NOT BE REMOVED UNTIL THE FIBER INTERCONNECT IS INSTALLED AND PROPOSED TRAFFIC SIGNALS AT US 20 RAMP C,D & E / RANDALL ROAD BECOME OPERATIONAL OR AS DIRECTED BY THE ENGINEER.
5. THE TRAFFIC SIGNAL CONTROLLER EQUIPMENT FOR THIS PROJECT SHALL BE NTCIP WITH FIRMWARE COMPATIBLE WITH KANE COUNTY'S CENTRAL SYSTEM SOFTWARE.
6. ALL COSTS ASSOCIATED TO TEMPORARY INTERCONNECT INCLUDING BUT NOT LIMITED TO SPECIAL HANDHOLES/FIBERGLASS HANDHOLES, TEMPORARY FIBER OPTIC CABLE, UNDERGROUND CONDUITS, DRILL EXISTING HANDHOLE, UNDERGROUND COILABLE NON-METALLIC CONDUIT, TERMINATION, SPLICING AND ADDITIONAL INTERCONNECT WOOD POLES SHALL BE INCLUDED IN THE COST OF TEMPORARY TRAFFIC SIGNAL INSTALLATION.
7. SPECIAL HANDHOLE/FIBERGLASS HANDHOLE SHALL HAVE AN ANSI 77 DESIGN LOAD RATING OF AT LEAST 20,000 VERTICAL POUNDS (OR TIER 22) AND SHALL HAVE AN OPEN BOTTOM.

USER NAME = lin44	DESIGNED - IS	REVISED -
	DRAWN - IS	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED - ST	REVISED -
PLOT DATE = 01/03/2019	DATE - 01-03-19	REVISED -

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	116
CONTRACT NO. 61F57				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

GENERAL NOTES FOR ALL PTZ (PAN, TILT, ZOOM) CAMERA INSTALLATIONS

1. CABLE SLACK SHALL MEET IDOT REQUIREMENTS EXCEPT WHERE SLACK IS PROVIDED IN HANDHOLES AND DOUBLE HANDHOLES. SLACK SHALL BE 15' MIN. IN ALL HANDHOLES FOR PTZ RELATED CABLES.
2. PTZ CAMERAS SHALL BE IP (INTERNET PROTOCOL) ADDRESSABLE. CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATION WITH KDOT TRAFFIC IN REQUESTING AND PROGRAMMING THE IP ADDRESS DESIRED BY KDOT TRAFFIC INTO THE PTZ EQUIPMENT.
3. CAMERA SHALL HAVE A MINIMUM THIRTY (30x) MECHANICAL ZOOM AND MINIMUM TWELVE (12x) DIGITAL ZOOM. VIDEO QUALITY SHALL BE H.D. TV QUALITY (1080p MINIMUM).
4. PTZ CAMERA SHALL BE HIGH POE (POWER OVER ETHERNET, 60W) UNLESS OTHERWISE STATED. A POE EXTENDER SHALL BE INSTALLED WITHIN AN APPROPRIATELY SPACED AUXILIARY CABINET AS TO NOT EXCEED 300 FEET CONSECUTIVE CABLE LENGTH AS MEASURED ALONG THE CABLE (INCLUDING CABLE SLACK). COST OF AUXILIARY CABINET AND REQUISITE ADDITIONAL CABLES, FIBER OPTIC, JUMPERS, ETC. SHALL BE INCLUDED IN THE COST OF THE PTZ CAMERA BEING INSTALLED.
5. PTZ CABLES SHALL BE OUTDOOR RATED CAT6A UNLESS OTHERWISE RECOMMENDED BY EQUIPMENT MANUFACTURER AND APPROVED BY THE ENGINEER.
6. CONTRACTOR SHALL PROVIDE AN ITEMIZED INVENTORY OF THE PTZ CAMERA EQUIPMENT AND CABINET COMPONENTS CONTAINING MODEL, SERIAL, AND PART NUMBERS. THIS INVENTORY TO BE FURNISHED DURING ACCEPTANCE OF SYSTEM OR FINAL TRANSFER OF SYSTEM MAINTENANCE.
7. RUBBER GROMITS SHALL BE INSTALLED IN ALL HOLES / OPENINGS USED TO RUN PTZ EQUIPMENT CABLE.
8. THE CONTRACTOR SHALL CONTACT KANE COUNTY TRAFFIC PRIOR TO INSTALLING THE PTZ CAMERA AND ASSOCIATED WIRING TO RECEIVE FINAL APPROVAL ON THE CAMERA LOCATION.

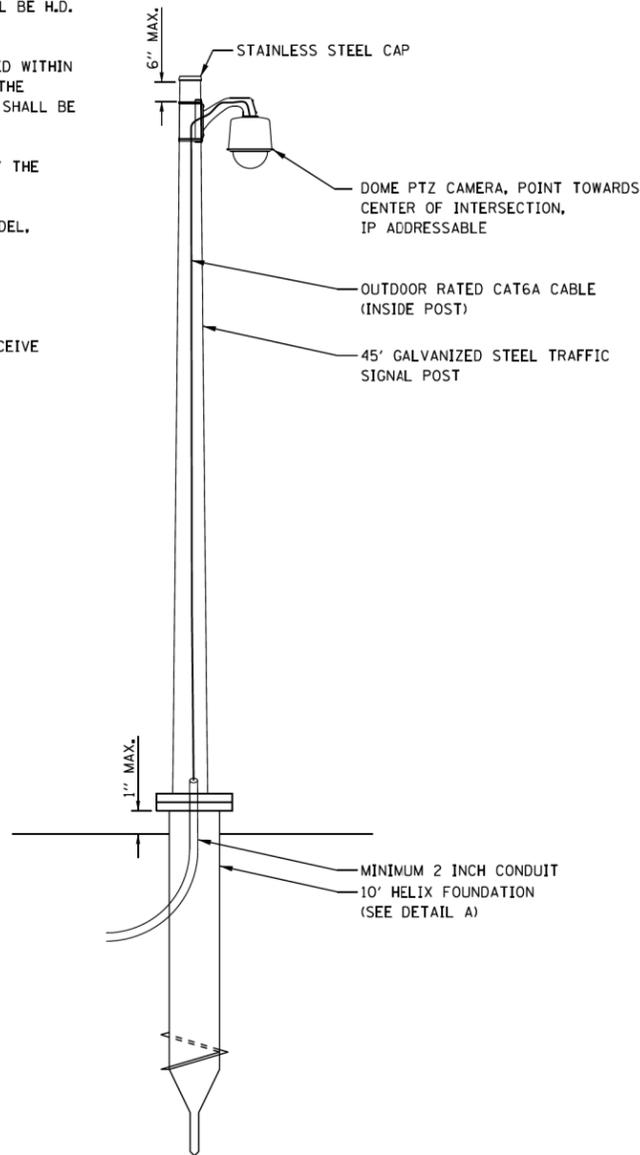


**CAMERA MOUNTING ASSEMBLY DETAIL:
PIPE / POLE EXTENSION**

(NOT TO SCALE)

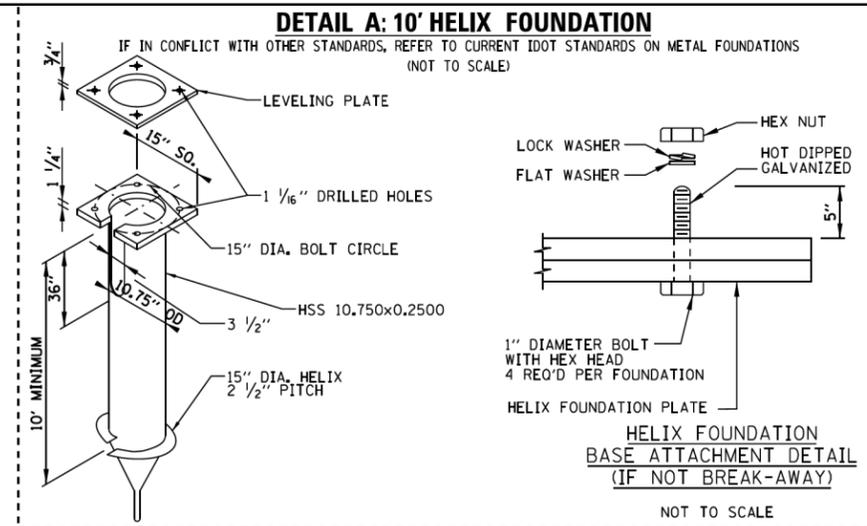
NOTES:

- THE MAST ARM IS TAPERED.
- INSTALL EXTENSION POLE VERTICAL AND PLUMB BY MODIFYING/INSTALLING BRACKETS AS NECESSARY. ADDITIONAL SPACERS REQUIRED ARE INCLUDED IN THE COST OF THE CAMERA MOUNTING ASSEMBLY OF THE TYPE SPECIFIED.
- SPACERS ARE TO BE INTEGRATED OR MANUFACTURED WITH THE MAST ARM BRACKETS.



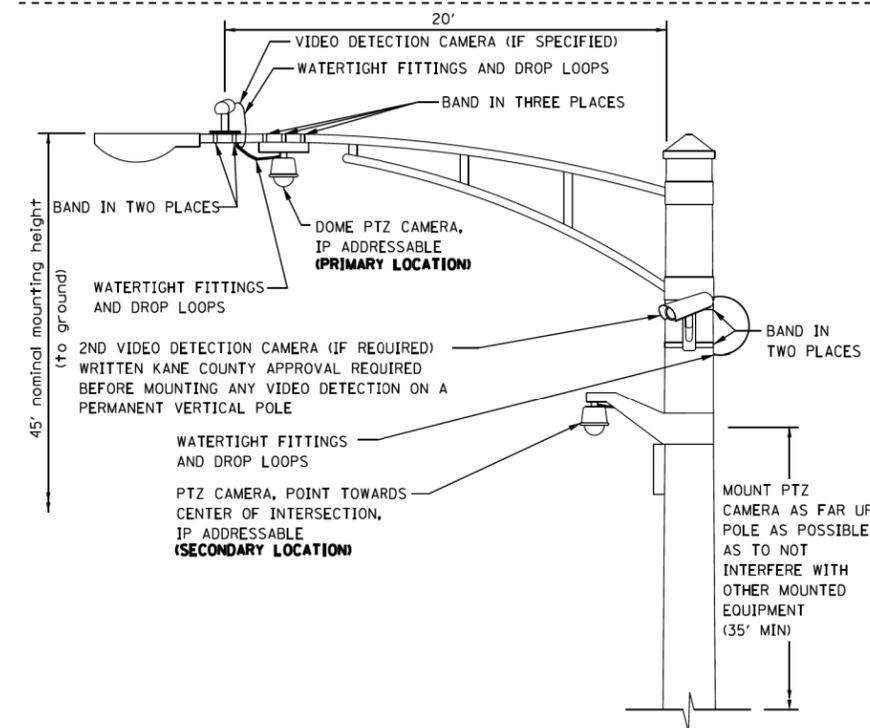
**CAMERA MOUNTING ASSEMBLY DETAIL:
STAND-ALONE 45' POST**

(NOT TO SCALE)



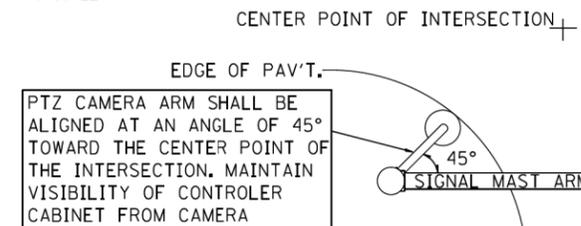
**HELIX FOUNDATION
BASE ATTACHMENT DETAIL
(IF NOT BREAK-AWAY)**

(NOT TO SCALE)



**CAMERA MOUNTING ASSEMBLY DETAIL:
COMBINATION MAST ARMS**

(NOT TO SCALE)



PTZ CAMERA MOUNTING DETAILS

(SECONDARY LOCATION ONLY)
(NOT TO SCALE)

FILE NAME = E:\M12\Phase 2\Project Information\Kane County PTZ Camera detail\PTZ Detail.dgn

USER NAME = jln44	DESIGNED - SDZ 12/31/14	REVISED - SDZ 02/23/15
	DRAWN - SDZ 12/31/14	REVISED - SDZ 08/22/16
PLOT SCALE = 50.0000' / 1" = 1/4"	CHECKED - DRAFT	REVISED -
PLOT DATE = 8/30/2018	DATE - 01-16-2015	REVISED -

**KANE COUNTY
DIVISION OF TRANSPORTATION**

CCTV / PTZ CAMERA INSTALLATION DETAILS

SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
										CONTRACT NO.	
										ILLINOIS FED. AID PROJECT	

USER NAME = jln44	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 2.0000' / 1" = 1/4"	CHECKED -	REVISED -
PLOT DATE = 01/03/2019	DATE - 01-03-19	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
										CONTRACT NO. 61F57	
										FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	

LIGHTING AND ELECTRICAL LEGEND – IDOT

SYMBOL	DESCRIPTION
	EXISTING IDOT HIGH MAST LIGHTING UNIT TO REMAIN
	PROPOSED IDOT LIGHTING UNIT MOUNTED ON BREAKAWAY TRANSFORMER BASE, 47.5 FT M.H. (U.N.O.), 12FT MAST ARM, 250W HPS LUMINAIRE (240VAC)
	EXISTING IDOT LIGHTING CONTROLLER
	EXISTING UNDERGROUND UNIT DUCT TO REMAIN
	PROPOSED UNIT DUCT IN UNDERGROUND CONDUIT, SIZE, AND TYPE AS NOTED

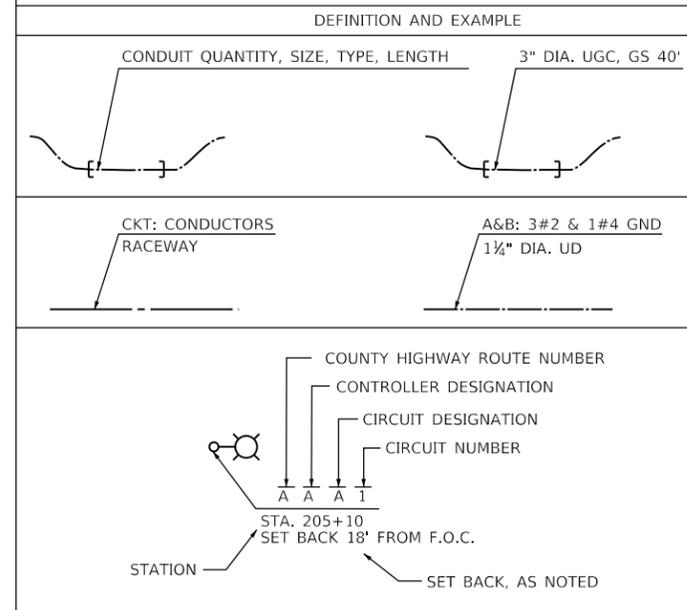
LIGHTING AND ELECTRICAL LEGEND – KDOT

SYMBOL	DESCRIPTION
	EXISTING KANE COUNTY LIGHTING UNIT TO BE REMOVED
	EXIST, KANE COUNTY COMBINATION TRAFFIC SIGNAL AND LUMINAIRE LIGHTING UNIT
	EXISTING KANE COUNTY LIGHTING UNIT TO REMAIN
	PROPOSED KANE COUNTY LIGHTING UNIT MOUNTED ON BREAKAWAY TRANSFORMER BASE, 40FT M.H. (U.N.O.), 8FT MAST ARM, 250W HPS (240VAC)
	TEMPORARY LIGHTING UNIT, 60FT WOOD POLE, 15FT MAST ARM, 250W HPS LUMINAIRE
	EXISTING KANE COUNTY LIGHTING CONTROLLER
	EXISTING HANDHOLE
	EXISTING UNDERGROUND UNIT DUCT TO REMAIN
	PROPOSED UNDERGROUND CONDUIT, SIZE, AND TYPE AS NOTED
	TEMPORARY AERIAL CABLE, 3-1/C NO. 4 WITH MESSENGER WIRE

ABBREVIATIONS

ABBREVIATION	DESCRIPTION
AC	ALTERNATING CURRENT
A/C	AERIAL CABLE
B.O.C.	BACK OF CURB
CAL MO	CALENDAR MONTH
CB	CIRCUIT BREAKER
CKT	CIRCUIT
CM	CENTIMETER
CP	CONTROL PANEL
CT	CURRENT TRANSFORMER
DA	DAVIT ARM
DC	DIRECT CURRENT
DIA	DIAMETER
DP	DISTRIBUTION PANEL
E	EXISTING UNIT TO REMAIN
ECA	ELECTRIC CABLE ASSEMBLY
E.O.P.	EDGE OF PAVEMENT
F.O.C.	FACE OF CURB
FT	FEET OR FOOT
FU	FUSE
GND	GROUND
HID	HIGH INTENSITY DISCHARGE
JB	JUNCTION BOX
KVA	KILOVOLT-AMPERE
KW	KILOWATTS
LED	LIGHT EMITTING DIODE
M	METER
MA	MAST ARM
MC	MULTI-CONDUCTOR
MM	MILLIMETER
M.H.	MOUNTING HEIGHT
MW	MESSENGER WIRE
NO. #	NUMBER
N.T.S.	NOT TO SCALE
P	PROPOSED
PB	PUSH BUTTON
PNL	PANEL
PVC	POLYVINYL CHLORIDE
PVCC RGC	PVC COATED RIGID GALVANIZED CONDUIT
PT	POTENTIAL TRANSFORMER
R	EXISTING UNIT TO BE REMOVED (OWNER SALVAGED U.N.O.)
RR	EXISTING UNIT TO BE REMOVED AND REINSTALLED
RECP	RECEPTACLE
RGC	RIGID GALVANIZED CONDUIT
SEL SW	SELECTOR SWITCH
SPARE	SPARE
SPACE	SPACE
SS	STAINLESS STEEL
STA	STATION
T/F	TOP OF FOUNDATION
UD	UNIT DUCT
U.N.O.	UNLESS NOTED OTHERWISE
UGC, GS	UNDERGROUND CONDUIT, GALVANIZED STEEL
VAC	VOLTS ALTERNATING CURRENT
WP	WOOD POLE
XFMR	TRANSFORMER
HPS	HIGH PRESSURE SODIUM
LPS	LOW PRESSURE SODIUM
LTFM	LIQUID TIGHT FLEXIBLE METALLIC

CALL-OUT SAMPLE



GENERAL NOTES

1. THE ELECTRICAL WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE LATEST CODES, STANDARDS AND THE IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION ADOPTED APRIL 1, 2016, AND SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS.
2. MAINTENANCE OF LIGHTING SYSTEM PAY ITEM SHALL INCLUDE MAINTENANCE OF IDOT AND KANE COUNTY LIGHTING SYSTEMS AS DESCRIBED IN THE SPECIAL PROVISION.
3. MEADE ELECTRIC CO. DISTRICT ONE ELECTRICAL MAINTENANCE CONTRACTOR LOCATES IDOT ELECTRICAL EQUIPMENT AND UNDERGROUND CABLES (773-287-7672).

IDOT-D1 STANDARDS:

STANDARD NO.	TITLE
BE-301	LIGHT POLE FOUNDATION 40' TO 47 1/2' M.H. 15" BOLT CIRCLE
BE-400	ALUMINUM LIGHT POLE, 47'-6" MOUNTING HEIGHT
BE-401	ALUMINUM LIGHT POLE, 40'-0" MOUNTING HEIGHT
BE-701	LUMINAIRE SAFETY CABLE ASSEMBLY
BE-702	MISC. ELECTRICAL DETAILS SHEET A
BE-800	TEMPORARY LIGHT POLE DETAILS
BE-801	TEMPORARY AERIAL CABLE INSTALLATION

INDEX OF DRAWINGS

STANDARD NO.	TITLE
E-01	LEGEND, ABBREVIATIONS, GENERAL NOTES, AND SCHEDULE OF QUANTITIES
E-02	PROPOSED LIGHTING PLAN, IDOT
E-03	EXISTING LIGHTING CONTROLLER "KX" WIRING DIAGRAM, IDOT
E-04	EXISTING LIGHTING REMOVAL AND TEMPORARY LIGHTING PLAN, KDOT
E-05	PROPOSED LIGHTING PLAN, KDOT
E-06	EXISTING LIGHTING CONTROLLER WIRING DIAGRAM, KDOT
E-07 TO E-08	LIGHTING DETAILS, KDOT
E-09 TO E-15	IDOT D1 BE STANDARDS

SCHEDULE OF QUANTITIES

ITEM	UNIT	IDOT QTY.	KDOT QTY.	TOTAL QTY.
UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	122	-	122
UNDERGROUND CONDUIT, PVC, 2" DIA.	FOOT	-	1845	1845
UNIT DUCT, 600V, 3-1C NO.2, 1/C NO.4 GROUND, (XLP-TYPE USE), 1 1/4" DIA. POLYETHYLENE	FOOT	730	-	730
ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 6	FOOT	-	5895	5895
AERIAL CABLE, 3-1/C NO. 4 WITH MESSENGER WIRE	FOOT	-	480	480
LUMINAIRE, SODIUM VAPOR, HORIZONTAL MOUNT, 250 WATT	EACH	3	12	15
LIGHT POLE, ALUMINUM, 40 FT. M.H., 8 FT. MAST ARM	EACH	-	8	8
LIGHT POLE, ALUMINUM, 47.5 FT. M.H., 12 FT. MAST ARM	EACH	3	-	3
LIGHT POLE, WOOD, 60 FOOT, CLASS 4, WITH 15FT MAST ARM	EACH	-	4	4
LIGHT POLE FOUNDATION, 24" DIAMETER	FOOT	28.5	76	104.5
BREAKAWAY DEVICE, TRANSFORMER BASE, 15 INCH BOLT CIRCLE	EACH	3	8	11
REMOVAL OF TEMPORARY LIGHTING UNIT	EACH	-	4	4
REMOVAL OF LIGHTING UNIT, NO SALVAGE	EACH	-	7	7
REMOVAL OF POLE FOUNDATION	EACH	-	7	7
INTERCEPT EXISTING CONDUIT	EACH	-	2	2
LUMINAIRE SAFETY CABLE ASSEMBLY	EACH	3	8	11
MAINTENANCE OF LIGHTING SYSTEM	CAL MO	7.5	7.5	15

E-01

MODEL: Default
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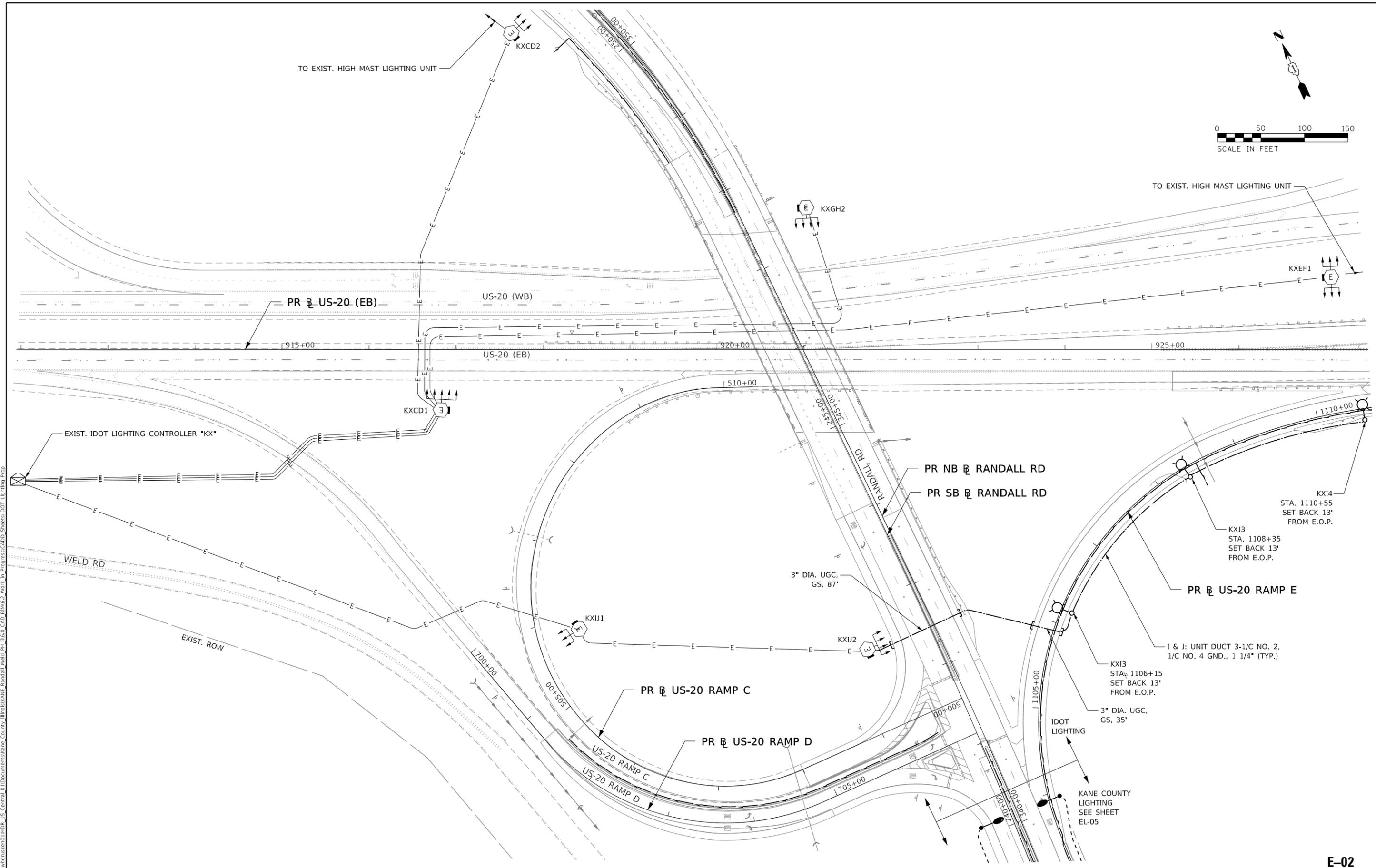
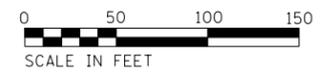
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PLOT DATE = Thursday, January 03, 2019 11:38:39	DATE - 01-03-19	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

LEGEND, ABBREVIATIONS, GENERAL NOTES, AND SCHEDULE OF QUANTITIES

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	122
				CONTRACT NO. 61F57
FED. ROAD DIST. NO. 1		ILLINOIS		FED. AID PROJECT



E-02

MODEL: Default
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USER NAME = vgvskas	DESIGNED - VG	REVISED -
DRAWN - MG	REVISIONS	
CHECKED - KP	REVISIONS	
DATE - 01-03-19	REVISIONS	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

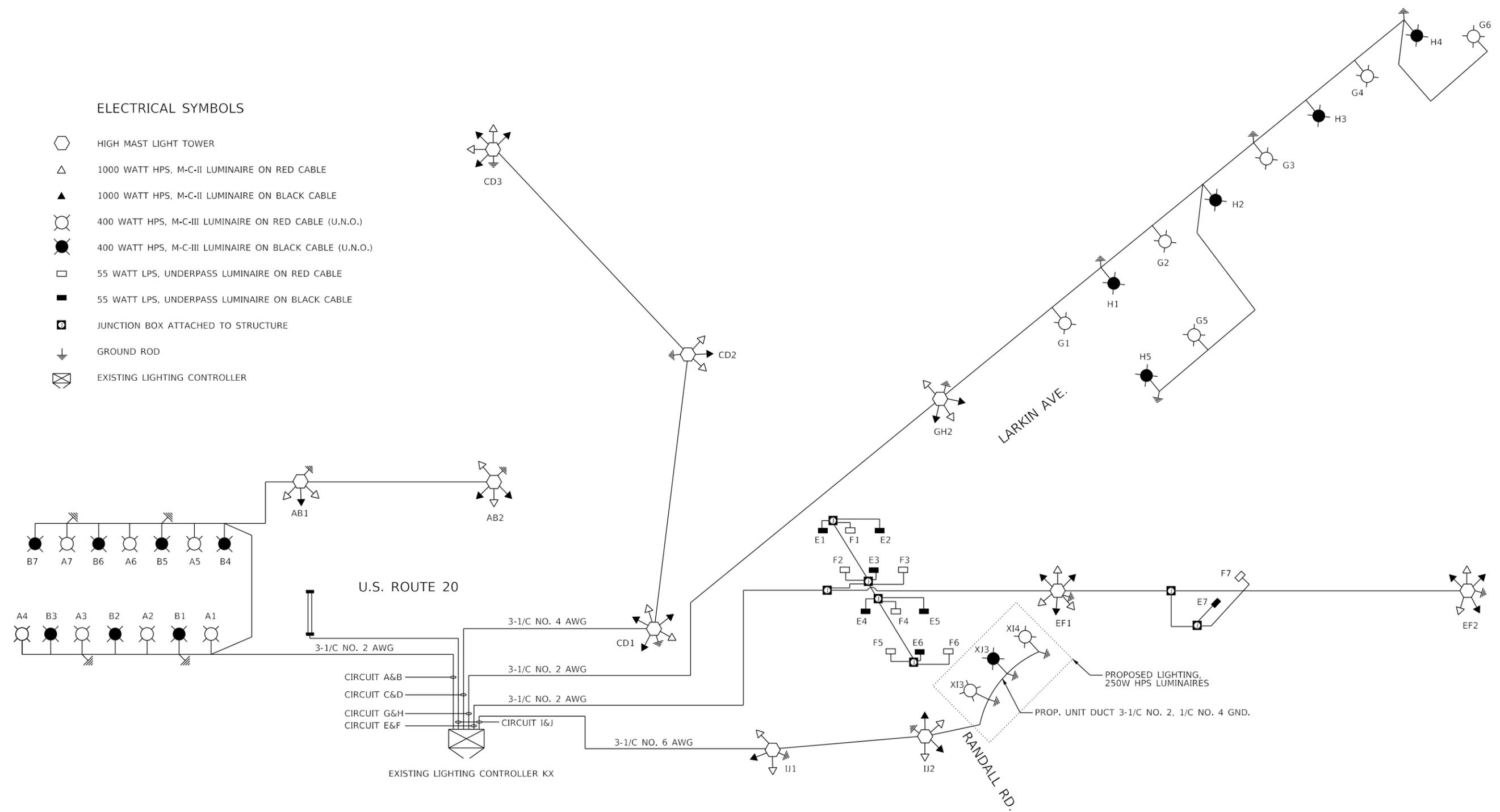
PROPOSED LIGHTING PLAN
IDOT

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	123
CONTRACT NO. 61F57				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

ELECTRICAL SYMBOLS

-  HIGH MAST LIGHT TOWER
-  1000 WATT HPS, M-C-II LUMINAIRE ON RED CABLE
-  1000 WATT HPS, M-C-II LUMINAIRE ON BLACK CABLE
-  400 WATT HPS, M-C-III LUMINAIRE ON RED CABLE (U.N.O.)
-  400 WATT HPS, M-C-III LUMINAIRE ON BLACK CABLE (U.N.O.)
-  55 WATT LPS, UNDERPASS LUMINAIRE ON RED CABLE
-  55 WATT LPS, UNDERPASS LUMINAIRE ON BLACK CABLE
-  JUNCTION BOX ATTACHED TO STRUCTURE
-  GROUND ROD
-  EXISTING LIGHTING CONTROLLER



MODEL: Default
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 SHEET: E-03
 DATE: 12/14/2018 09:54:58



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PLOT DATE = Friday, December 14, 2018 09:54:58	DATE - 01-03-19	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING LIGHTING CONTROLLER "KX" WIRING DIAGRAM
IDOT

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	124
CONTRACT NO. 61F57				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

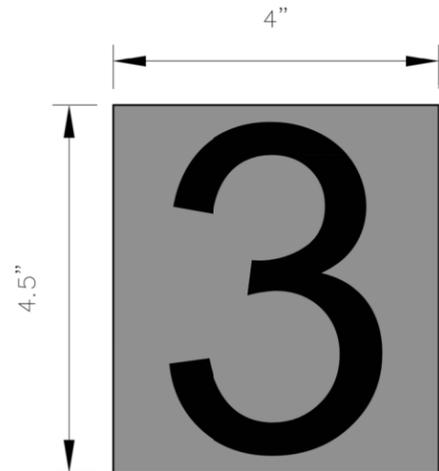
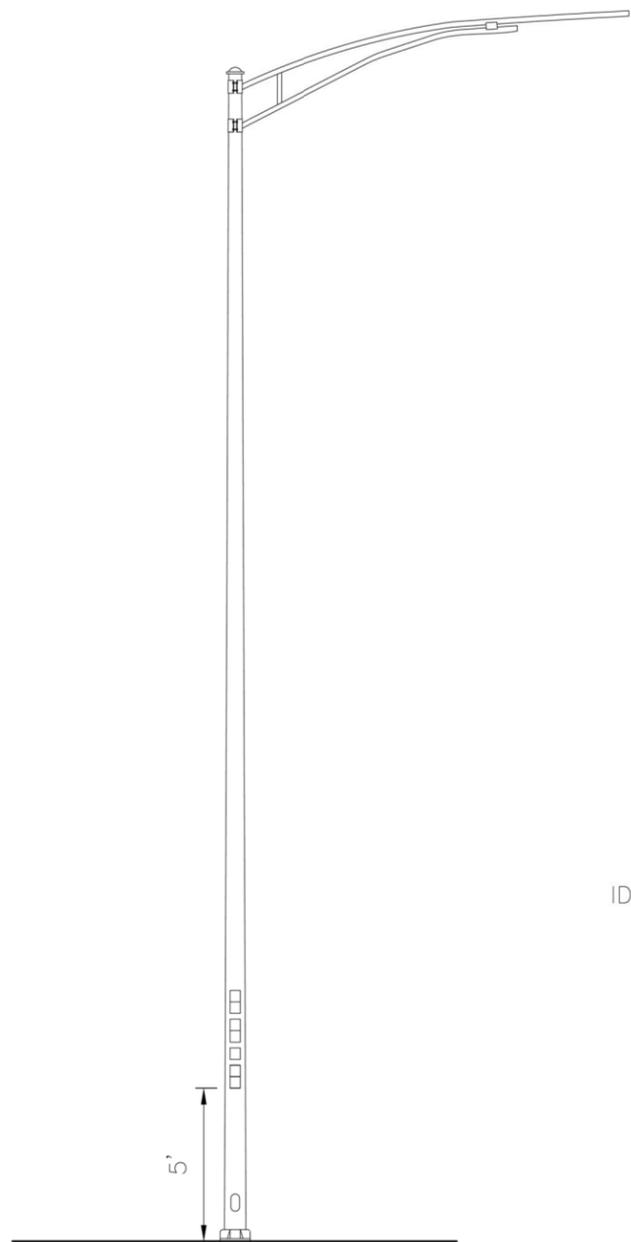
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REVISED
 FILED
 PLOT SCALE
 USER NAME

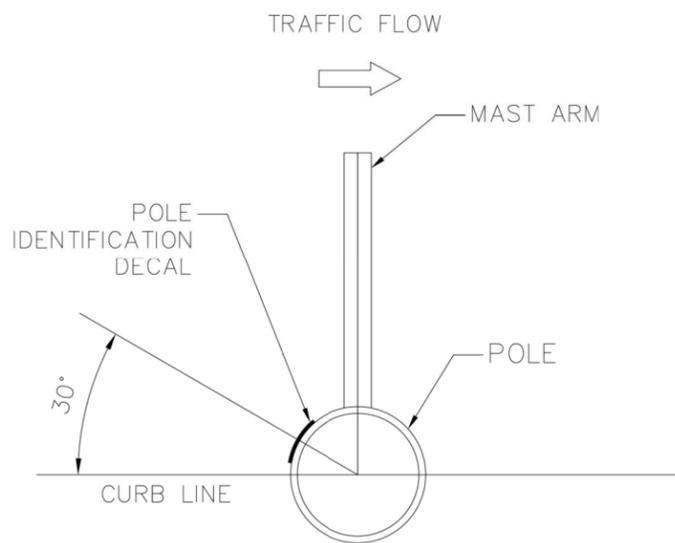
NOTE:
 PLACE BOTTOM OF THE BOTTOM DECAL AT 5'
 ABOVE GROUND LEVEL. PLACE DECALS
 MOVING UP THE POLE FOLLOWING THE
 DECAL SPACING DETAIL TO THE RIGHT.

MOUNTING HEIGHT DETAIL

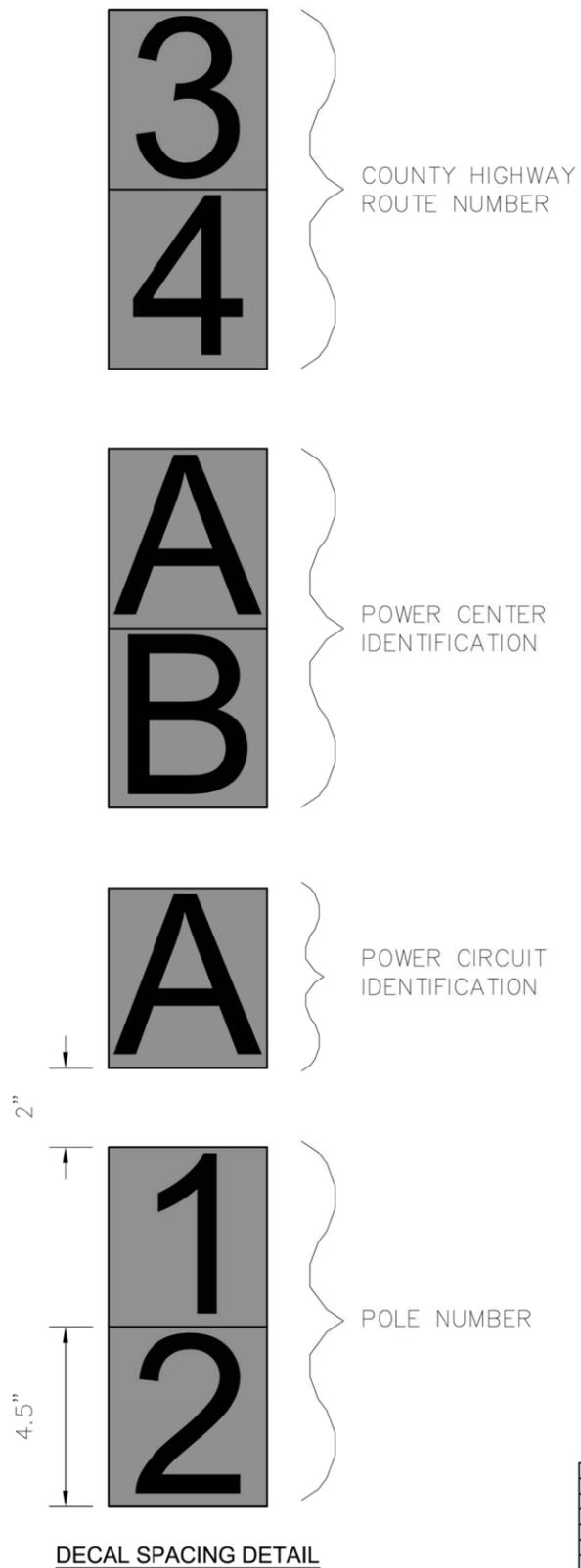


NOTE:
 ALL DECALS SHALL BE BLACK LETTERS
 ON ORANGE BACKGROUND

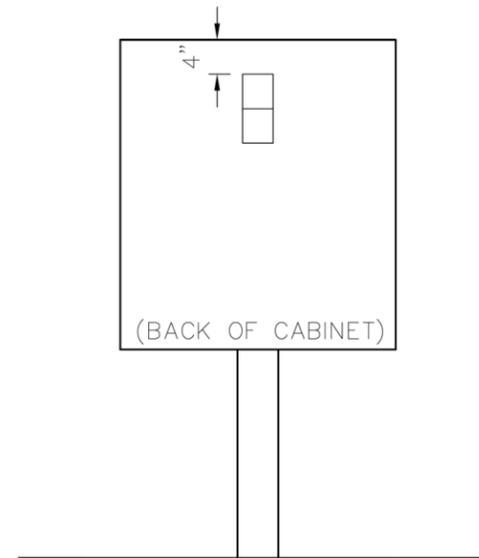
DECAL DETAIL



POLE IDENTIFICATION DECAL
 ORIENTATION DETAIL

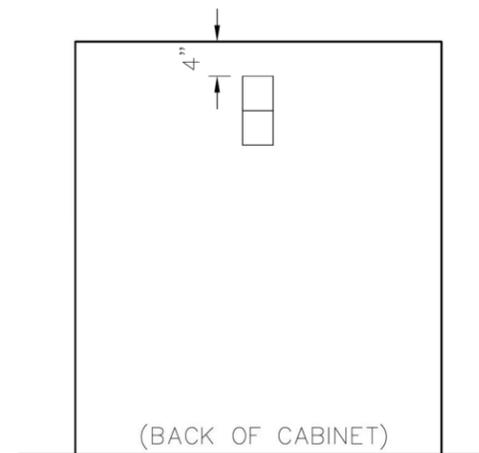


DECAL SPACING DETAIL



PEDESTAL MOUNTED CABINET
 DECAL LOCATION DETAIL

NOTE:
 DECALS ON CABINET SHALL BE THE
 POWER CENTER IDENTIFICATION ONLY.



GROUND MOUNTED CABINET
 DECAL LOCATION DETAIL

REVISIONS	
NAME	DATE
HLR	11/12

**LIGHT POLE DECAL
 STANDARD
 KDOT**

SCALE: N.T.S.
 DATE: 3/16/2012

DRAWN BY: BF
 CHECKED BY: RG



USER NAME = vjgurskas	DESIGNED -	REVISED -
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

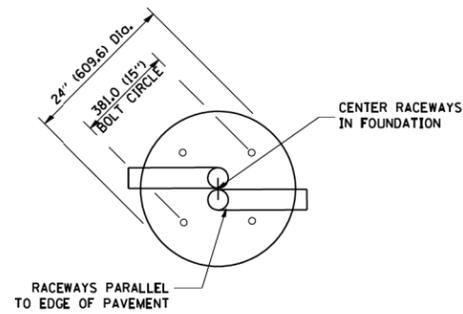
LIGHTING DETAILS
 KDOT

SCALE: N.T.S. SHEET OF SHEETS STA. TO STA.

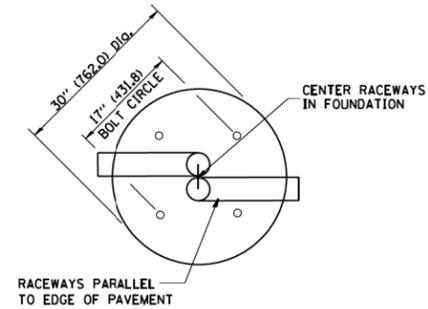
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	129
CONTRACT NO. 61F57				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LIGHT POLE FOUNDATION DEPTH TABLE
40 FT. (12.192 m) TO 47.5 FT. (14.478 m) MOUNTING HEIGHT

SOIL CONDITIONS	DESIGN DEPTH "D" OF FOUNDATION	
	SINGLE ARM POLE	TWIN ARM POLE
SOFT CLAY O _u = 0.375 TON/SO. FT.	13'-0" (3.96 m)	15'-0" (4.57 m)
MEDIUM CLAY O _u = 0.75 TON/SO.FT	9'-6" (2.93 m)	10'-9" (3.23 m)
STIFF CLAY O _u = 1.50 TON/SO. FT.	7'-0" (2.13 m)	8'-0" (2.44 m)
LOOSE SAND φ = 34°	9'-0" (2.74 m)	10'-0" (3.05 m)
MEDIUM SAND φ = 37.5°	8'-3" (2.52 m)	9'-0" (2.74 m)
DENSE SAND φ = 40°	7'-9" (2.36 m)	9'-0" (2.74 m)



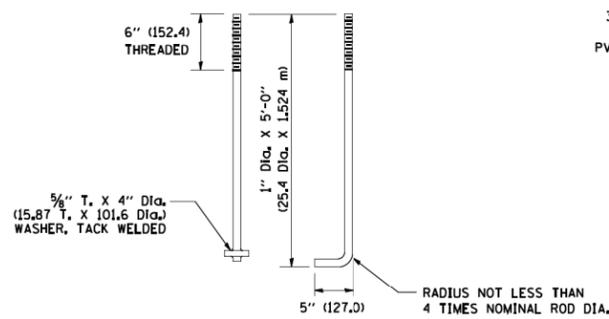
TOP VIEW



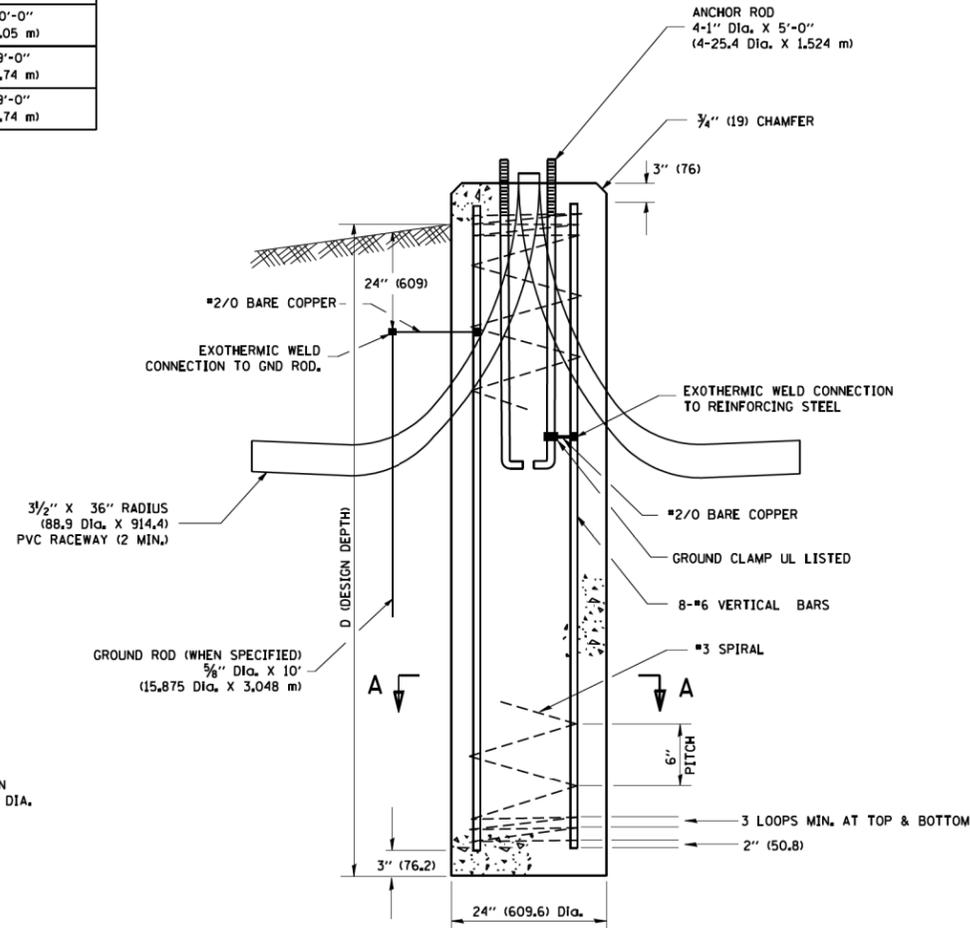
TOP VIEW

NOTES

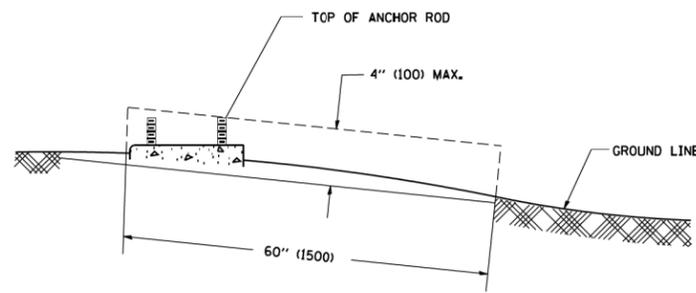
- ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
- THE ANCHOR RODS AND RACEWAYS SHALL BE PROPERLY SECURED IN PLACE BEFORE THE CONCRETE IS PLACED.
- THE FOUNDATION SHALL NOT PROTRUDE MORE THAN 100MM (4 IN.) ABOVE THE FINISHED GRADE WITHIN A 60 IN. (1.5 m) CHORD ACROSS THE FOUNDATION, WITH ANCHOR RODS INCLUDED, IN ACCORDANCE WITH AASHTO GUIDELINES. IF THE FOUNDATION HEIGHT, INCLUDING ANCHOR RODS, EXTENDS BEYOND THESE SPECIFIED LIMITS, THE FOUNDATION SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE. SEE FOUNDATION EXTENSION DETAIL.
- THE HOLE FOR THE FOUNDATION SHALL BE MADE BY DRILLING WITH AN AUGER, OF THE SAME DIAMETER AS THE FOUNDATION. IF SOIL CONDITIONS REQUIRE THE USE OF A LINER TO FORM THE HOLE, THE LINER SHALL BE WITHDRAWN AS THE CONCRETE IS DEPOSITED.
- THE TOP OF THE FOUNDATION SHALL BE CONSTRUCTED LEVEL. A LINER OR FORM SHALL BE USED TO PRODUCE A UNIFORM SMOOTH SIDE TO THE TOP OF THE FOUNDATION. FOUNDATION TOP SHALL BE CHAMFERED 3/4-IN. (20 mm).
- THE CONCRETE SHALL BE CLASS S1. CONCRETE SHALL CURE ACCORDING TO ARTICLE 1020.13 BEFORE LIGHT POLES ARE INSTALLED.
- THE ANCHOR ROD SHALL BE A HOOK ROD TYPE. COLD BENDING OF THE ANCHOR ROD WILL NOT BE ALLOWED. THE RADIUS OF THE HOOK BEND SHALL NOT BE LESS THAN 4 TIMES THE NOMINAL DIAMETER OF THE ANCHOR ROD. A TACK WELDED ANCHOR ROD MAY BE SUBSTITUTED WITH THE APPROVAL OF THE ENGINEER.
- THE ANCHOR RODS SHALL BE ACCORDING TO ASTM F1554 GRADE 725 (GRADE 105). NUTS SHALL BE HEXAGON NUTS ACCORDING TO ASTM A 194 2H OR ASTM A 563 DH, AND WASHERS SHALL BE ACCORDING TO ASTM F 436.
- ANCHOR RODS, NUTS AND WASHERS SHALL BE COMPLETELY GALVANIZED BY EITHER THE HOT-DIPPED PROCESS CONFORMING WITH AASHTO M 232, THE MECHANICAL PLATING METHOD CONFORMING TO AASHTO M 298, CLASS 50 WITH A MAXIMUM COATING THICKNESS OF 150 (6 MILS) OR THE ELECTROLYTIC PROCESS ACCORDING TO ASTM F 1136.
- THE ANCHOR RODS SHALL BE THREADED A MINIMUM OF 6 INCHES (150 mm) WITH A MINIMUM OF 3 INCHES (75 mm) OF THREADED ANCHOR ROD EMBEDDED IN THE FOUNDATION.
- ANCHOR RODS SHALL PROJECT 2 3/4" (69.9 mm) ABOVE THE TOP OF THE FOUNDATION, IF BREAKAWAY COUPLINGS ARE SPECIFIED. THE CONTRACTOR SHALL CAREFULLY COORDINATE THE ANCHOR ROD PROJECTION WITH THE INSTALLATION REQUIREMENTS OF THE BREAKAWAY COUPLINGS.
- THE CONTRACTOR SHALL USE A #3 SPIRAL AT 6" (152.4 mm) PITCH OR MAY SUBSTITUTE #3 TIES AT 12" (304.8 mm) O.C. WITH THE APPROVAL OF THE ENGINEER.
- THE CABLE TRENCHES AND FOUNDATION SHALL BE BACK FILLED AND COMPACTED AS SPECIFIED BEFORE THE LIGHT POLE IS ERECTED.
- THE RACEWAYS SHALL PROJECT 1" (25.4 mm) ABOVE THE TOP OF THE FOUNDATION.



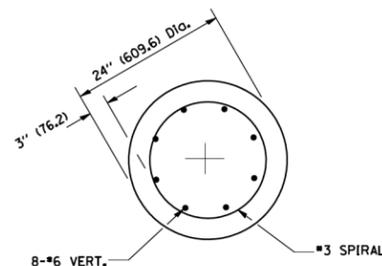
ANCHOR ROD DETAIL



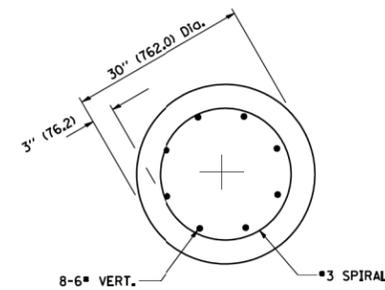
FOUNDATION DETAIL



FOUNDATION EXTENSION DETAIL



SECTION A-A



SECTION A-A

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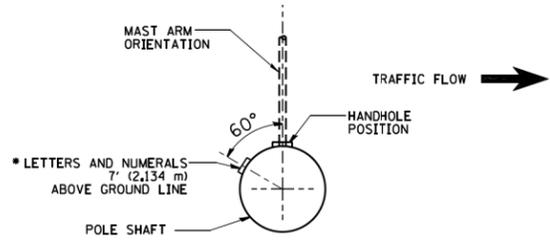
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DRAWN -
CHECKED -
DATE -

REVISED - 04-22-02
REVISED -
REVISED -
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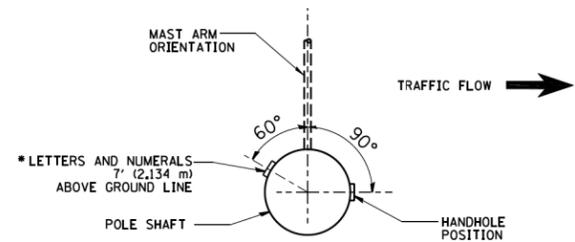
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

LIGHT POLE FOUNDATION
40' (12.192 m) TO 47 1/2' (14.478 m) M.H. 15" (381 mm) BOLT CIRCLE
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

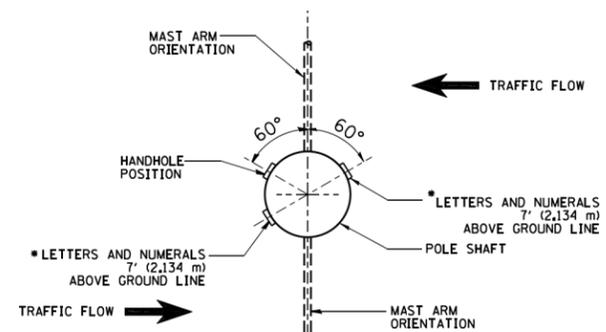
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	130
BE-301		CONTRACT NO. 61F57		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



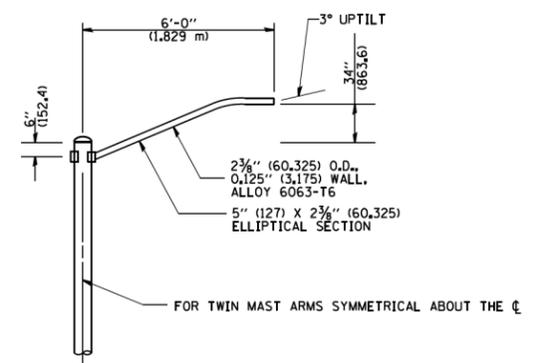
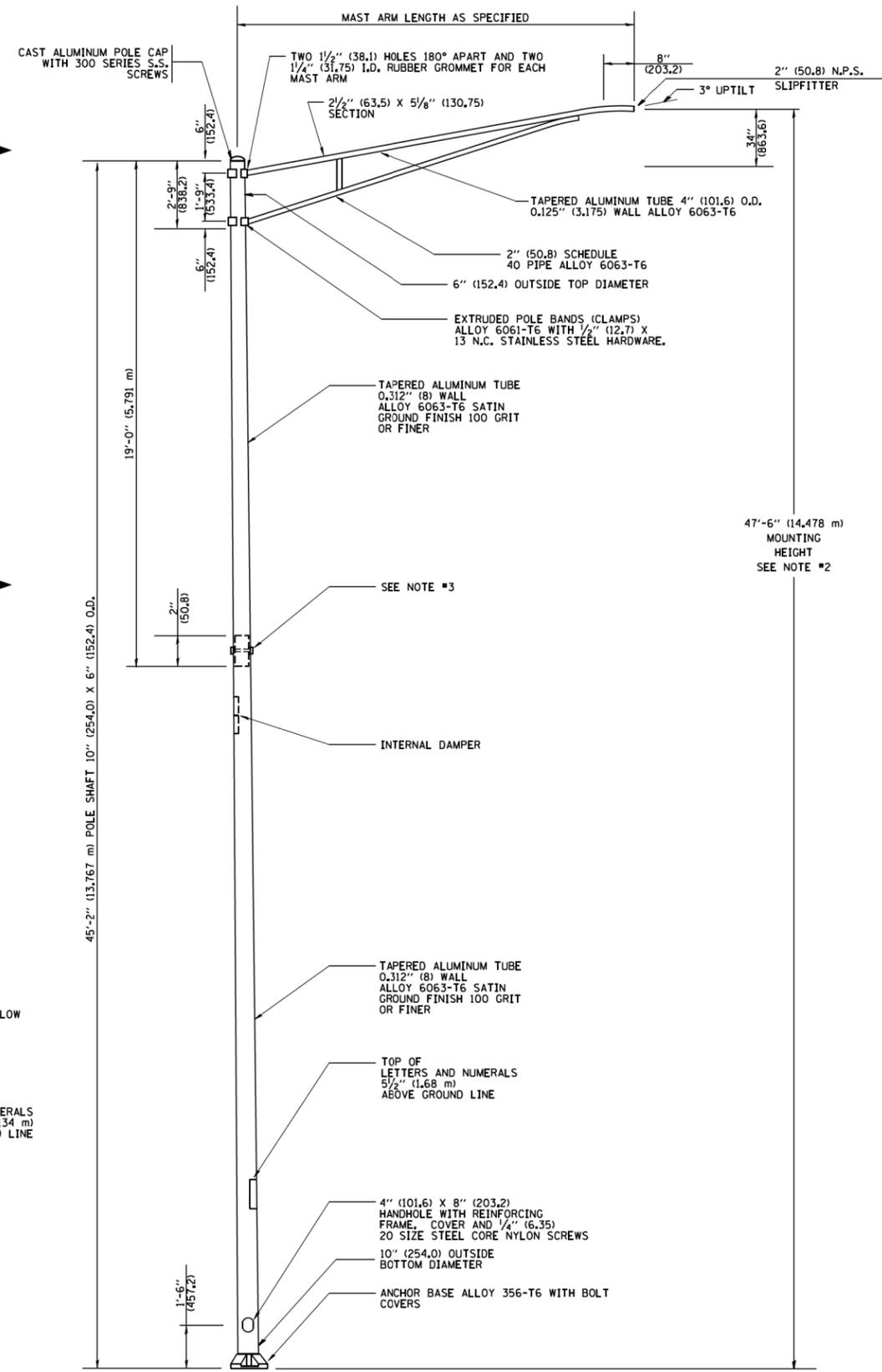
POSITION OF HANDHOLE AND POLE NUMBER FOR SINGLE MAST ARM POLES MOUNTED ON BRIDGE PARAPET OR BARRIER WALL



POSITION OF HANDHOLE AND POLE NUMBER FOR SINGLE MAST ARM POLES

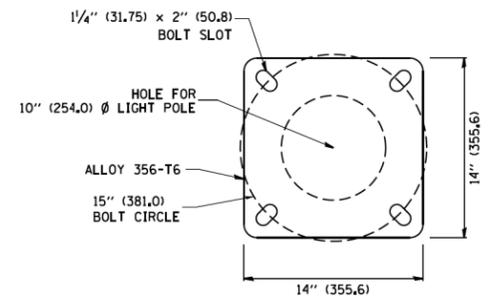


POSITION OF HANDHOLE AND POLE NUMBER FOR TWIN MAST ARM POLES

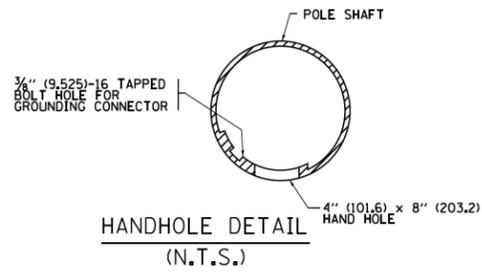


6' (1.8 m) SINGLE MEMBER MAST ARM (N.T.S.)

- NOTES:
1. ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
 2. MOUNTING HEIGHT IS DEFINED AS THE DISTANCE FROM THE CENTERLINE OF THE TENON TO THE BOTTOM OF THE ANCHOR BASE.
 3. TWO PIECE SHAFT WILL BE MATCHED MARKED AND INTERCHANGEABLE BETWEEN DIFFERENT UNITS. FIELD DRILLING OF THE HOLES WILL NOT BE ALLOWED.
 4. THE LIGHT POLE WILL MEET AASHTO DESIGN CRITERIA AS SPECIFIED.
 5. THE INSTALLING CONTRACTOR WILL PROVIDE A UL LISTED GROUNDING CONNECTOR, BURNDY K2C23, T&B SP4DL OR APPROVED EQUAL.
 6. LIGHT POLES WILL NOT BE INSTALLED WITHOUT MAST ARMS AND LUMINAIRES.
 7. LIGHT POLES WILL BE SET PLUMB ON THE FOUNDATION WITHOUT THE USE OF LEVELING NUTS, WASHERS OR SHIMS.
 8. LIGHTING UNIT IDENTIFICATION NUMBERS SHALL BE INSTALLED BEFORE THE LIGHTING UNIT IS ENERGIZED.

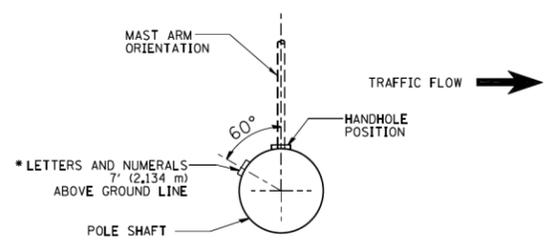


LIGHT POLE BASE PLATE DETAIL
15 INCH (381.0) BOLT CIRCLE

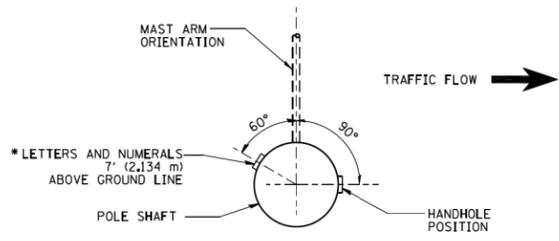


HANDHOLE DETAIL (N.T.S.)

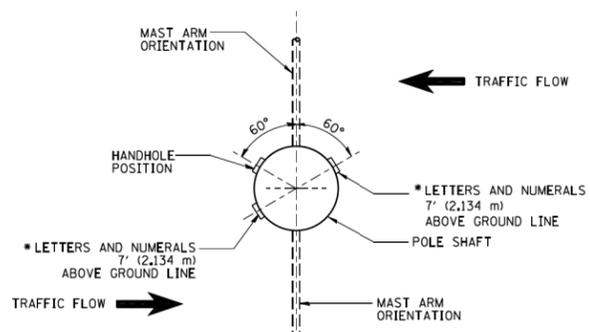
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pw\1\084EBID\INTEG\illinois.gov\FWIDOT\Documents\DOT Offices\District 1\Projects\Dist 1\CADData\CADsheets\be400.dgn		DRAWN	REVISED - R. TOMSONS 09-03-03		47'-6" (14.478 m) MOUNTING HEIGHT			336/345	14-00446-00-CH	KANE	354	131
Default		CHECKED -	REVISED - R. TOMSONS 01-18-13		SCALE:	SHEET 1	OF 1	SHEETS	STA.	TO STA.	BE-400 CONTRACT NO. 61F57	
		DATE -	REVISED - R. TOMSONS 03-18-15		ILLINOIS/FED. AID PROJECT							



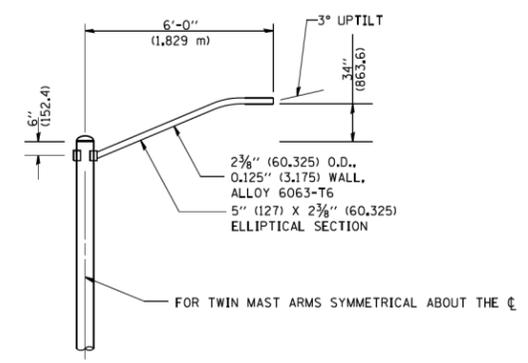
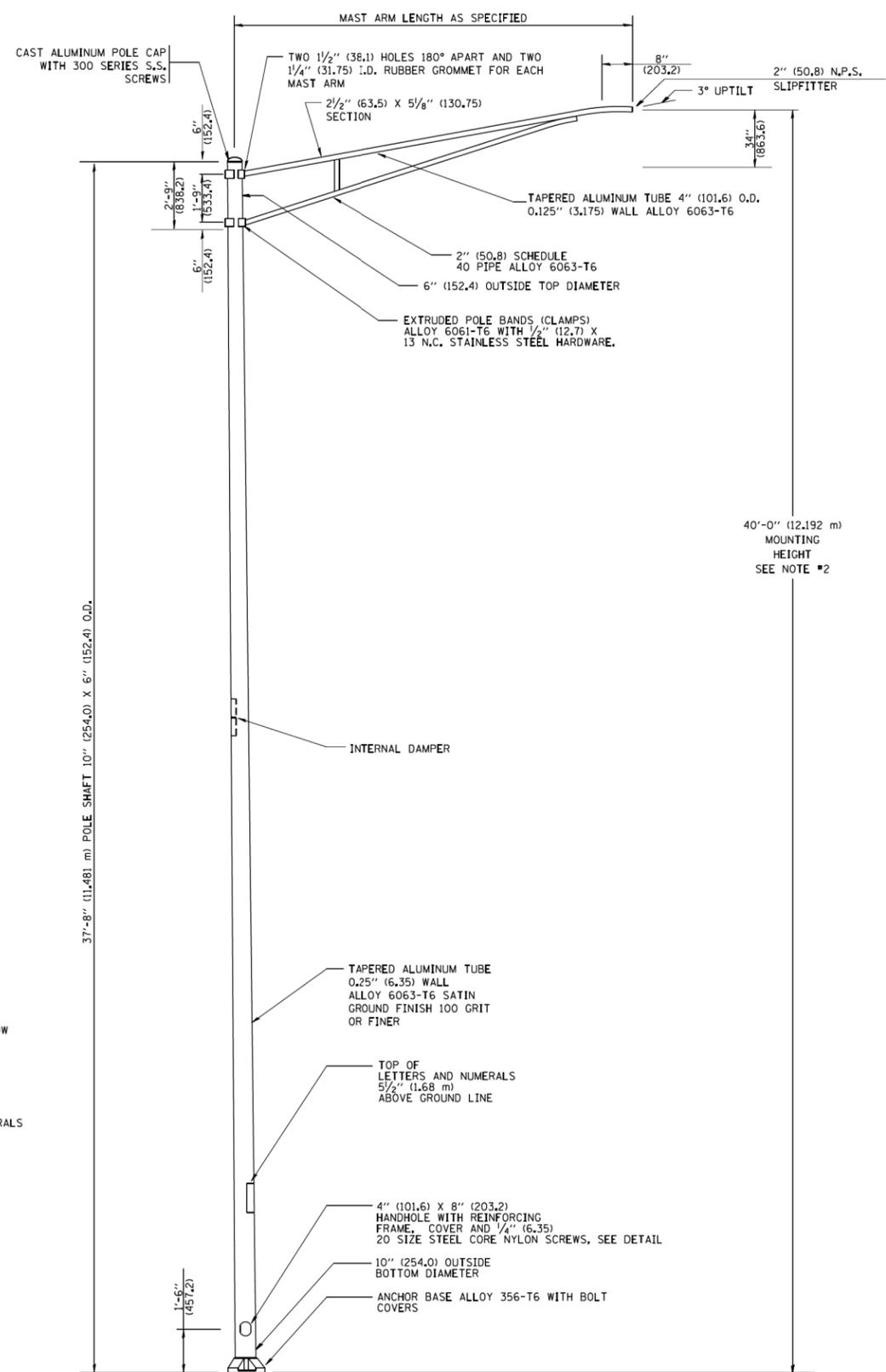
POSITION OF HANDHOLE AND POLE NUMBER FOR SINGLE MAST ARM POLES MOUNTED ON BRIDGE PARAPET OR BARRIER WALL



POSITION OF HANDHOLE AND POLE NUMBER FOR SINGLE MAST ARM POLES

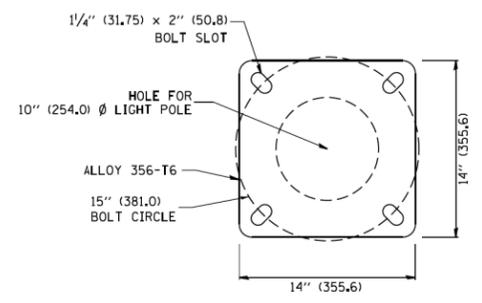


POSITION OF HANDHOLE AND POLE NUMBER FOR TWIN MAST ARM POLES

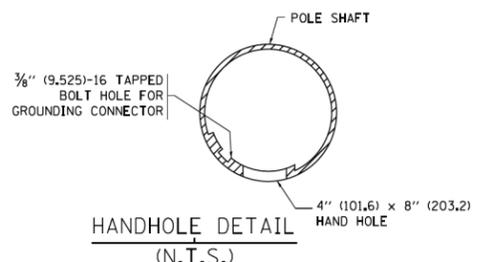


6' (1,8 m) SINGLE MEMBER MAST ARM (N.T.S.)

- NOTES:**
1. ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
 2. MOUNTING HEIGHT IS DEFINED AS THE DISTANCE FROM THE CENTERLINE OF THE TENON TO THE BOTTOM OF THE ANCHOR BASE.
 3. THE LIGHT POLE WILL MEET AASHTO DESIGN CRITERIA AS SPECIFIED.
 4. THE INSTALLING CONTRACTOR WILL PROVIDE A UL LISTED GROUNDING CONNECTOR, BURNDY K2C23, T&B SP4DL OR APPROVED EQUAL.
 5. LIGHT POLES WILL NOT BE INSTALLED WITHOUT MAST ARMS AND LUMINAIRES.
 6. LIGHT POLES WILL BE SET PLUMB ON THE FOUNDATION WITHOUT THE USE OF LEVELING NUTS, WASHERS OR SHIMS.
 7. LIGHTING UNIT IDENTIFICATION NUMBERS SHALL BE INSTALLED BEFORE THE LIGHTING UNIT IS ENERGIZED.

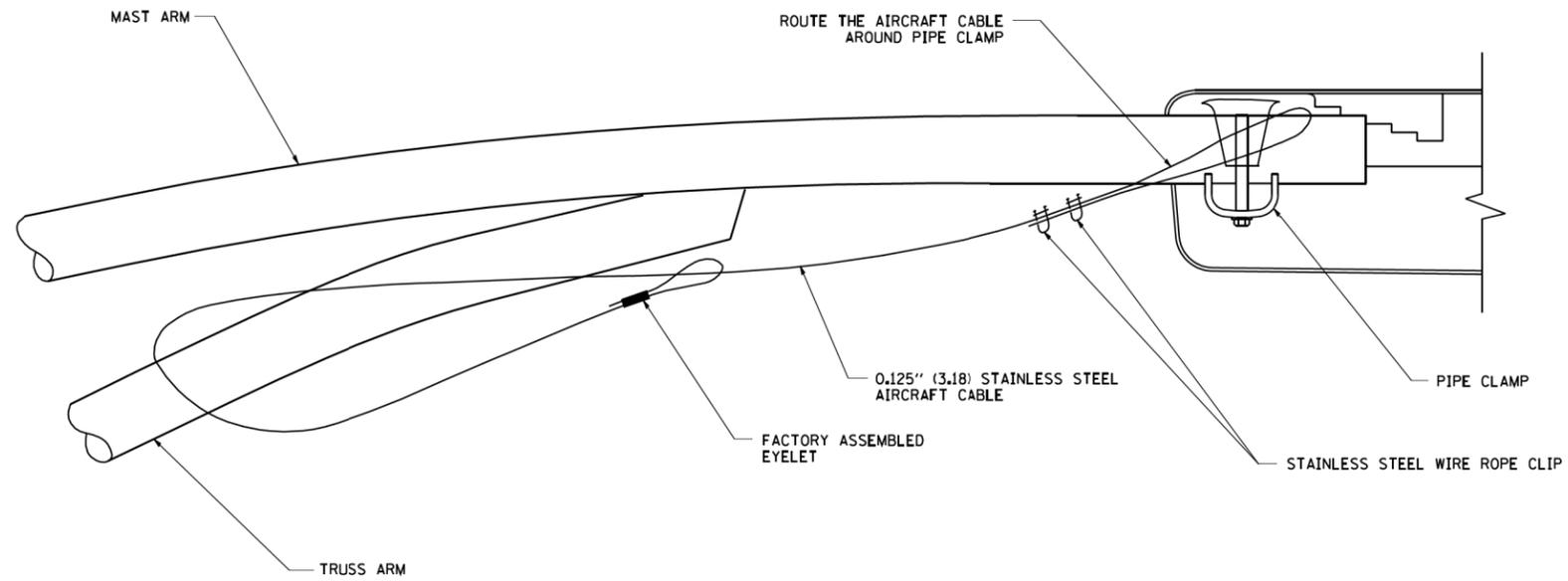


LIGHT POLE BASE PLATE DETAIL
15 INCH (381.0) BOLT CIRCLE

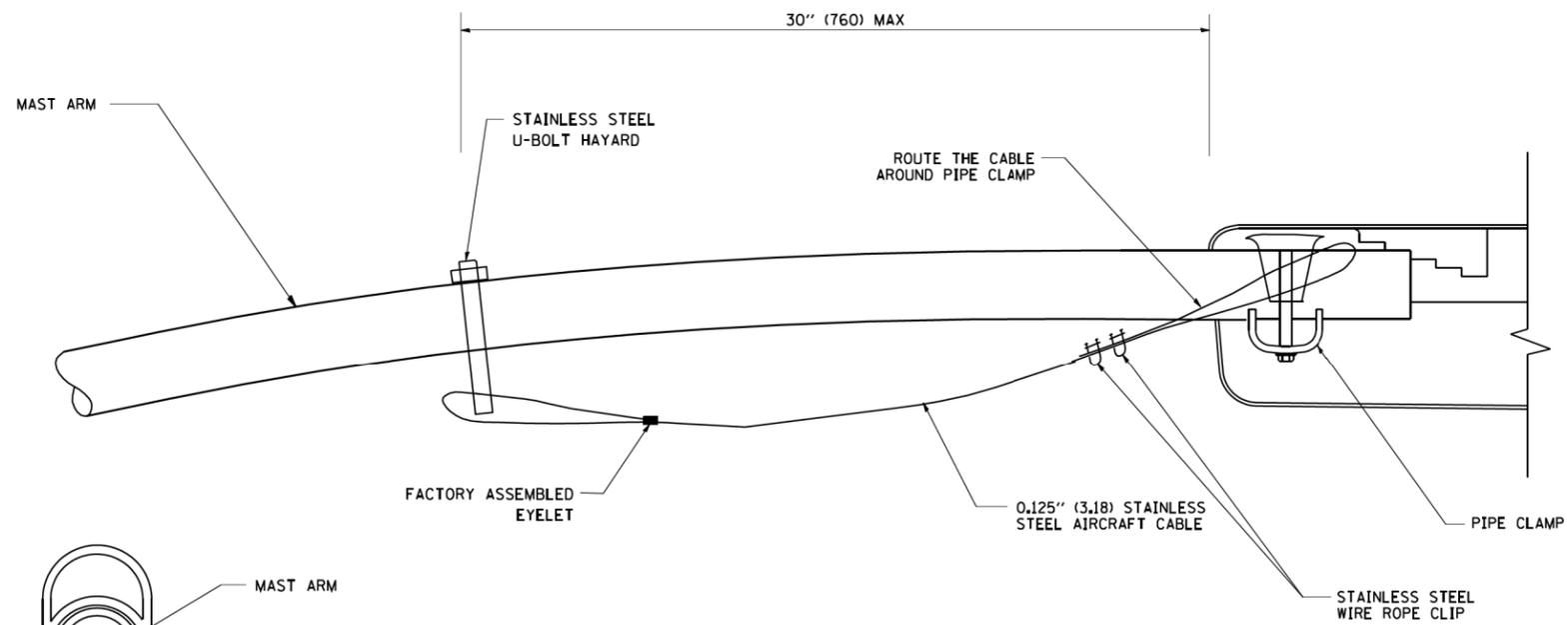


HANDHOLE DETAIL (N.T.S.)

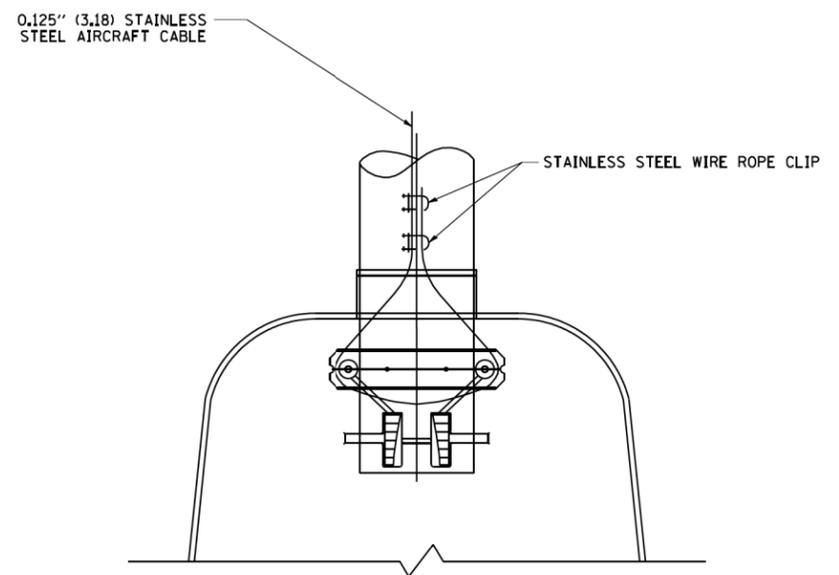
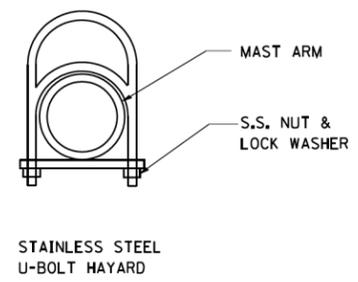
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ca\pwwork\pwwork\dtrvakosgn\d0188315\ba01.dgn		DRAWN -	REVISED - R. TOMSONS 09-02-03		336/345	14-00446-00-CH	KANE	354	132			
PLOT SCALE = 50,822 ' / in.		CHECKED -	REVISED - R. TOMSONS 01-18-13		BE-401			CONTRACT NO. 61F57				
PLOT DATE = 2/27/2013		DATE -	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



SIDE VIEW (TRUSS ARM)
N.T.S.



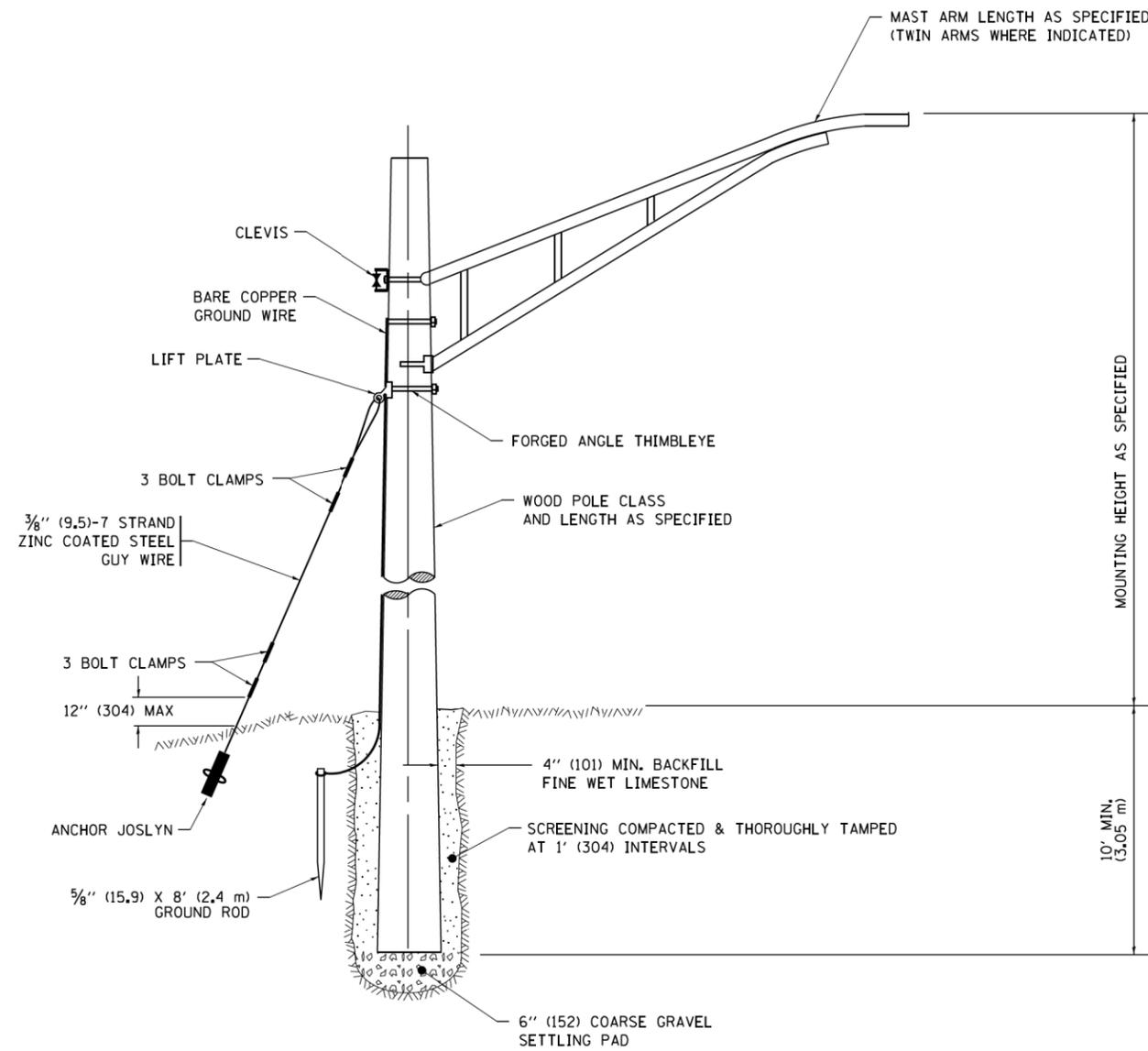
SIDE VIEW (SINGLE MEMBER OR DAVIT ARM)
N.T.S.



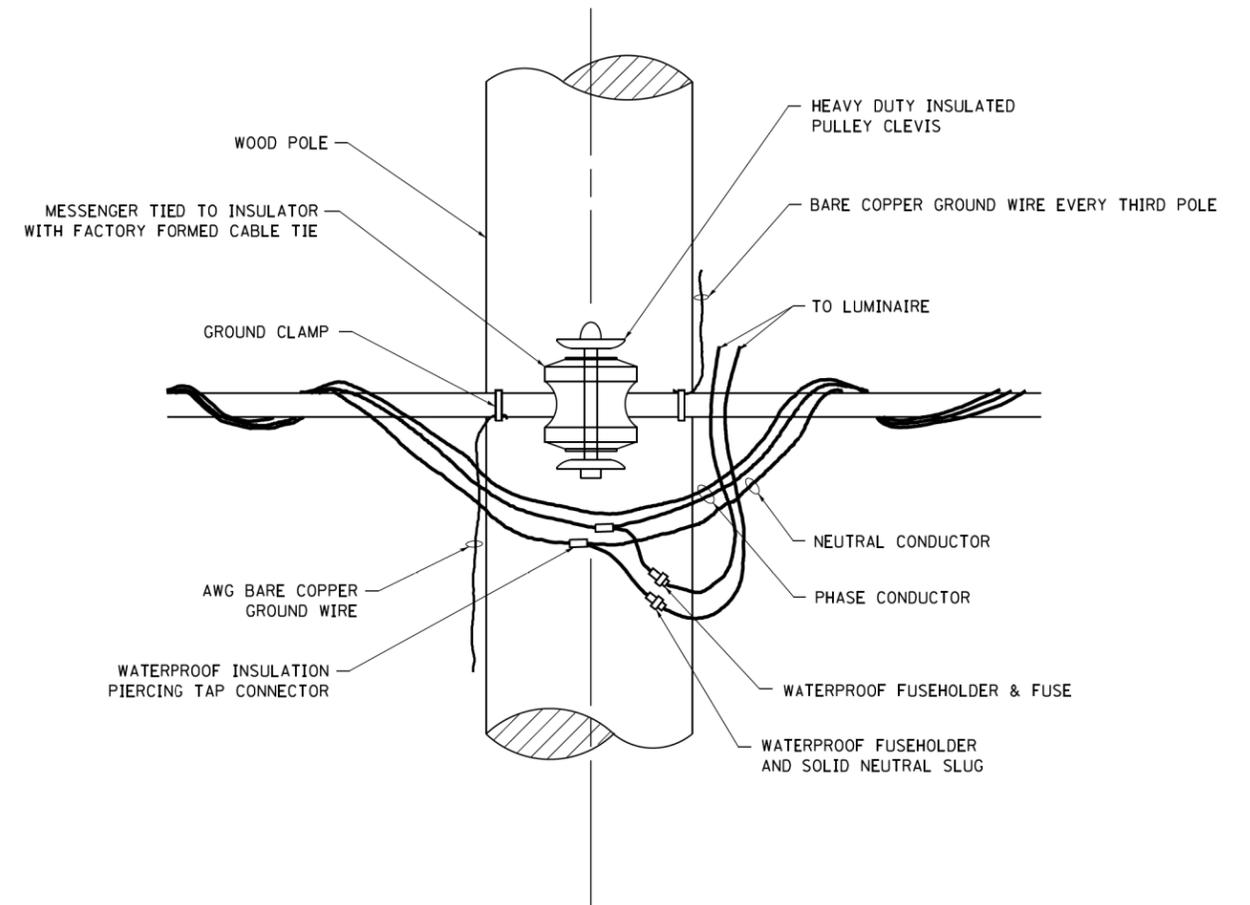
BOTTOM VIEW
N.T.S.

- NOTES:**
1. ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.
 2. CONTRACTOR SHALL ADJUST THE WIRE CLIP TO ELIMINATE ANY SLACK FROM THE WIRE ROPE.
 3. THE 0.125" (3.18) STAINLESS STEEL AIRCRAFT CABLE SHALL REMAIN VISIBLE FROM THE GROUND LEVEL.
 4. THE BREAKING STRENGTH OF THE CABLE SHALL BE 1700 LBS. MIN.

FILE NAME = W:\diststd\22x34\be701.dgn	USER NAME = gegl1enobt	DESIGNED - DRAWN -	REVISED - 08-08-03 REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	LUMINAIRE SAFETY CABLE ASSEMBLY			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50.000 ' / IN. PLOT DATE = 1/4/2008	CHECKED - DATE -			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	336/345	14-00446-00-CH	KANE	354
								BE-701		CONTRACT NO. 61F57		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT												



TEMPORARY LIGHT POLE DETAIL



TEMPORARY LIGHT POLE ATTACHMENT DETAIL

NOTE:

1. ALL DIMENSIONS IN INCHES (MILLIMETERS) UNLESS OTHERWISE INDICATED.
2. MAST ARM SHALL BE RATED FOR THE SPECIFIED MOUNTING HEIGHT.

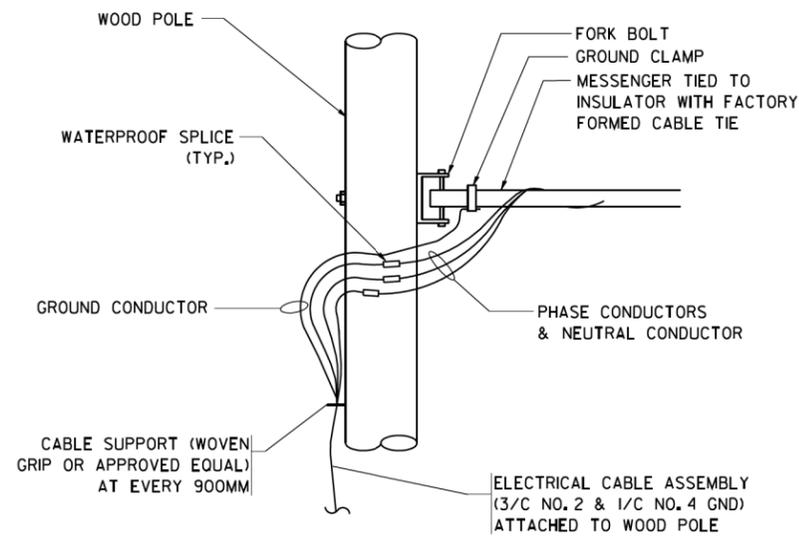
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pw\1\084EBID\INTEG\illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\Dist		DRAWN	REVISED - R.T. 07-26-16
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Default	PLOT DATE = 9/1/2016	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

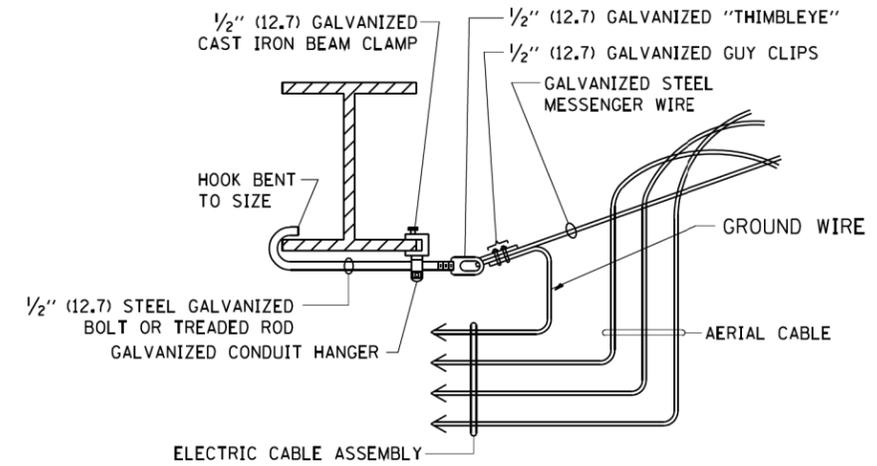
TEMPORARY LIGHT POLE DETAILS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	135
BE-800		CONTRACT NO. 61F57		
ILLINOIS FED. AID PROJECT				



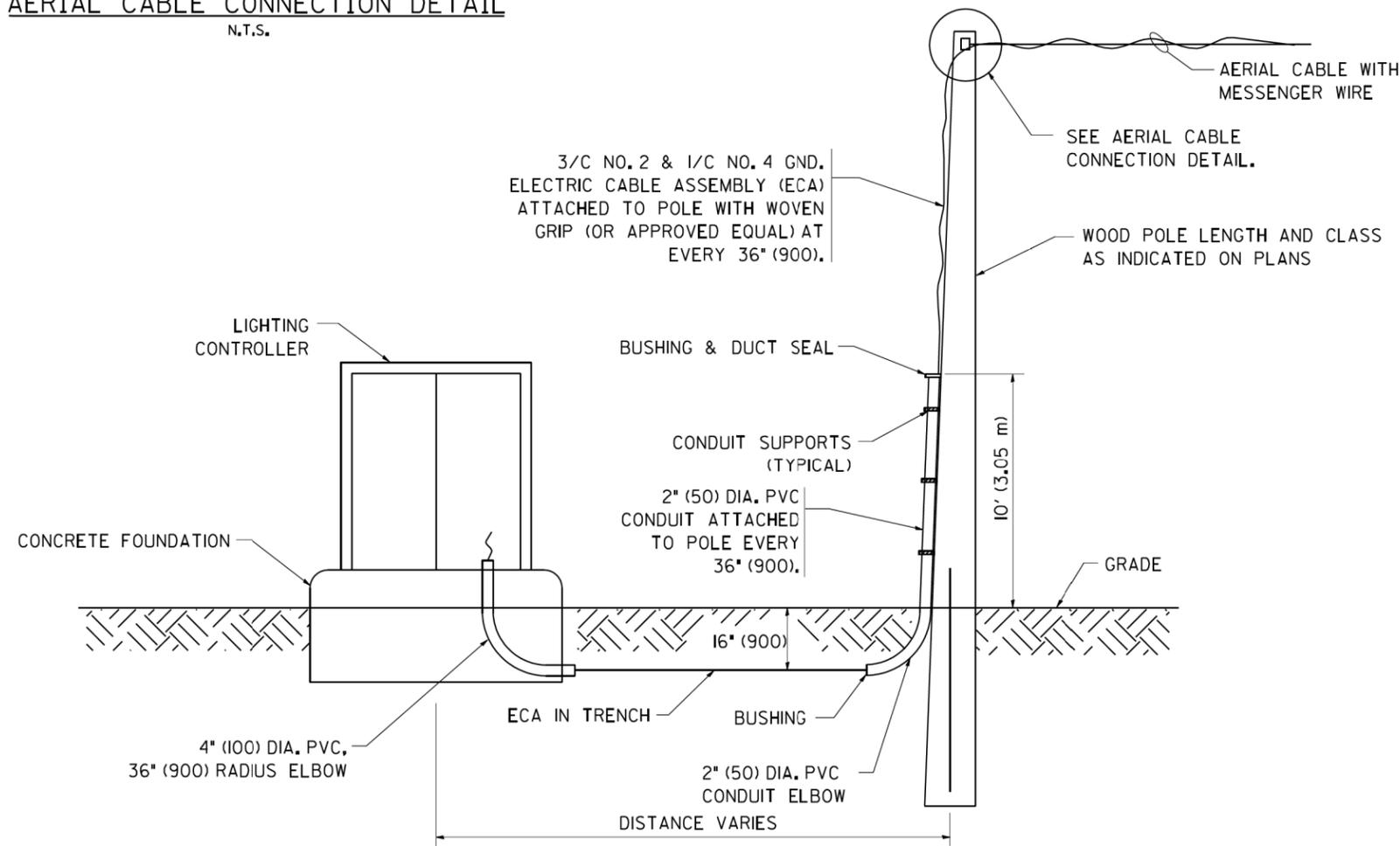
AERIAL CABLE CONNECTION DETAIL
N.T.S.



AERIAL CABLE ATTACHED TO STRUCTURE
NOT TO SCALE

NOTES:

1. ALL DIMENSIONS IN INCHES (MILLIMETERS) UNLESS OTHERWISE INDICATED.
2. SEE PROPOSED LIGHTING PLAN FOR CONDUIT, CABLE AND ROUTING.
3. THE CONTRACTOR SHALL PROVIDE INTERMEDIATE SUPPORTS TO MAINTAIN MINIMUM CLEARANCES. REFER TO AERIAL CABLE ATTACHED TO STRUCTURE DETAIL.
4. COST OF SPLICES AND MOUNTING HARDWARE SHALL BE INCLUDED IN THE UNIT PRICE FOR AERIAL CABLE.



WOOD POLE TO LIGHTING CONTROLLER WIRING CONNECTION DETAIL
N.T.S.

FILE NAME = W:\diststd\22x34\be801.dgn	USER NAME = geglennob	DESIGNED -	REVISED - 08-08-03	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TEMPORARY AERIAL CABLE INSTALLATION			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
	PLOT SCALE = 50.000' / IN.	DRAWN -	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	336/345	14-00446-00-CH	KANE	354	136
	PLOT DATE = 1/4/2008	CHECKED -	REVISED -						BE-801		CONTRACT NO. 61F57		
		DATE -	REVISED -						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

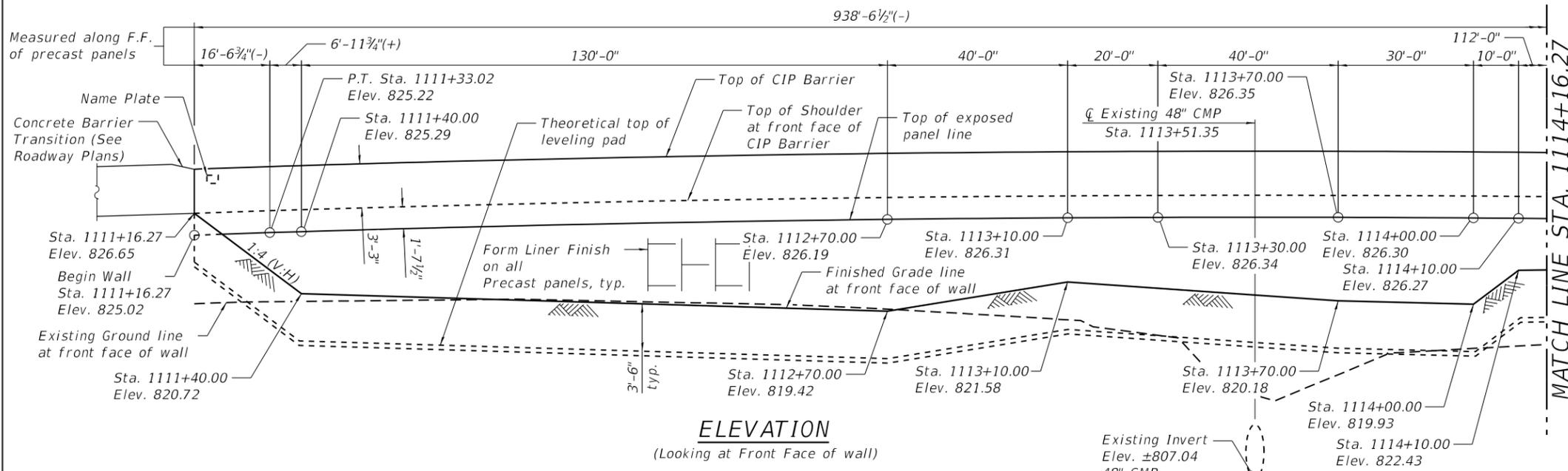
Bench Mark: Chiseled "□" in concrete headwall, center of headwall for culvert for Otter Creek on the south side of Elgin Bypass/US 20; Elev. 811.858 (NAVD 88).

Existing Structure: None.

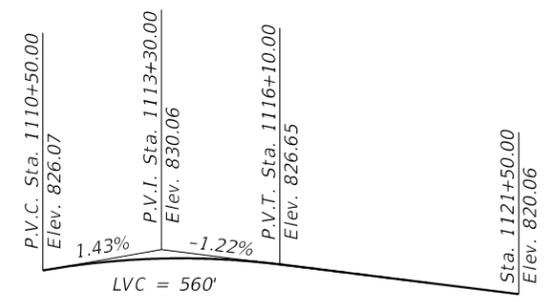
Traffic Control: Traffic to be maintained on Eastbound US 20 during construction. See Maintenance of Traffic Plans.

DESIGN SPECIFICATIONS
AASHTO LRFD Bridge Design Specifications, 8th Edition

DESIGN STRESSES
FIELD UNITS
f'c = 4,000 psi
fy = 60,000 psi (Reinforcement)
PRECAST UNITS
f'c = 4,500 psi (Precast Panels)



ELEVATION
(Looking at Front Face of wall)



PROFILE GRADE
(Along Centerline of Ramp E)

CURVE DATA
(PR_RAMP_E3)
P.I. Sta. = 1110+04.97
Δ = 23° 03' 55" (Rt.)
D = 8° 52' 59"
R = 645.00'
T = 131.61'
L = 259.65'
E = 13.29'
e = 6.0%
T.R. = 130.00'
S.E. Run = 390.00'
P.C. Sta. = 1108+73.36
P.T. Sta. = 1111+33.02
S.E. Rt. Shldr. Transition from Sta. 1110+03.02 (-6%) to Sta. 1111+33.02 (-4%)

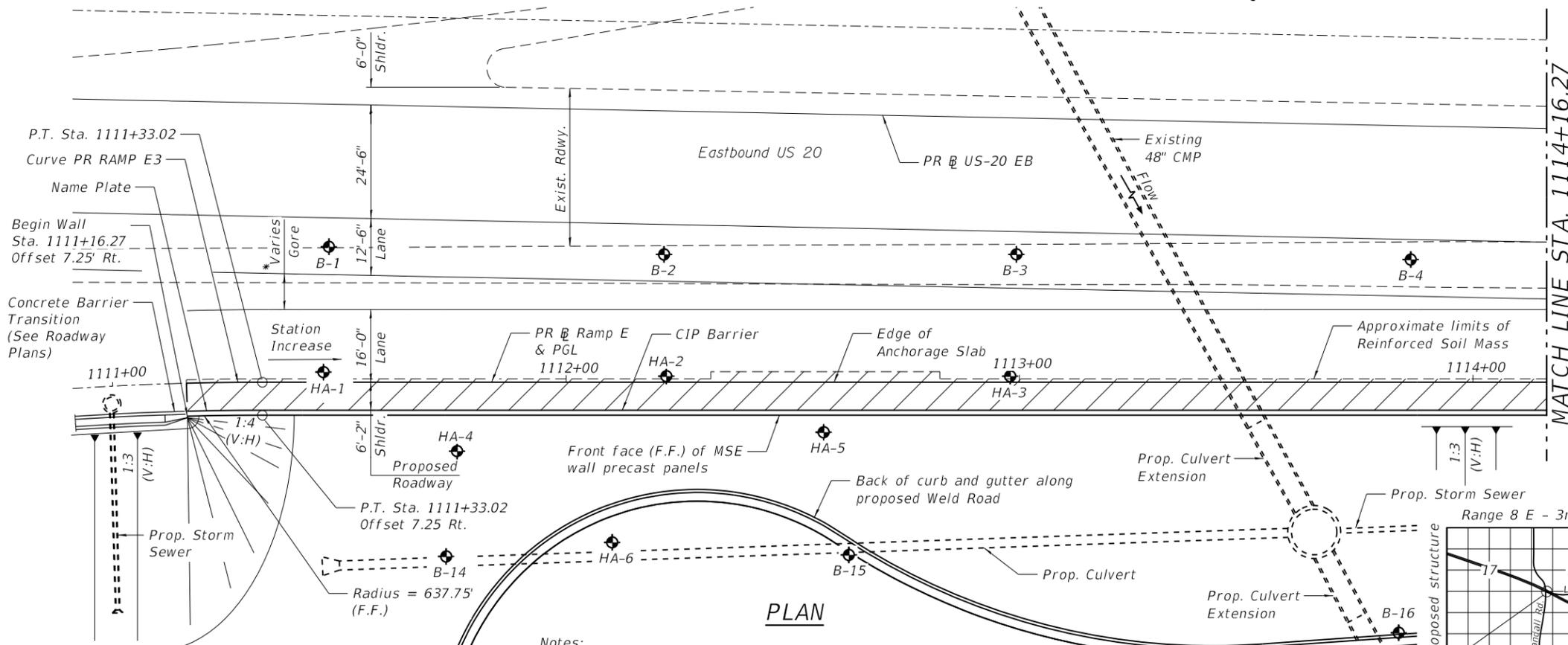
INDEX OF SHEETS

1. General Plan and Elevation (1 of 3)
2. General Plan and Elevation (2 of 3)
3. General Plan and Elevation (3 of 3)
4. General Notes
5. Typical Section and Details
6. Anchorage Slab General Plan (1 of 3)
7. Anchorage Slab General Plan (2 of 3)
8. Anchorage Slab General Plan (3 of 3)
9. Anchorage Slab and Wall Details
10. Parapet Slipforming Option
- 11-18. Soil Boring Logs



BY: *Marcin Krok* DATE: 12-05-2018
HDR, INC.
DWGS_137-154
LICENSE EXPIRES NOV. 30, 2020

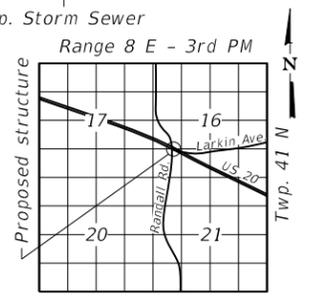
"I certify that to the best of my knowledge, information and belief, this retaining wall design is structurally adequate for the design loading shown on the plans. The design is an economical one for the style of structure and complies with the requirements of the 'AASHTO LRFD Bridge Design Specifications', edition as shown on the plans."



PLAN

LEGEND

Reinforced Soil Mass



LOCATION SKETCH

Notes:
Wall offsets are measured from the PR Ramp E to the front face of precast panels.
See Drainage Plans for drainage details.
See Roadway Typical Sections and SGR for estimated location of Unsuitable Material removal.

* 8'-7" to 0'-0" from Sta. 1111+16.27 to Sta. 1115+33.26

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DATE = 1/3/19	CHECKED - MJK	REVISIONS

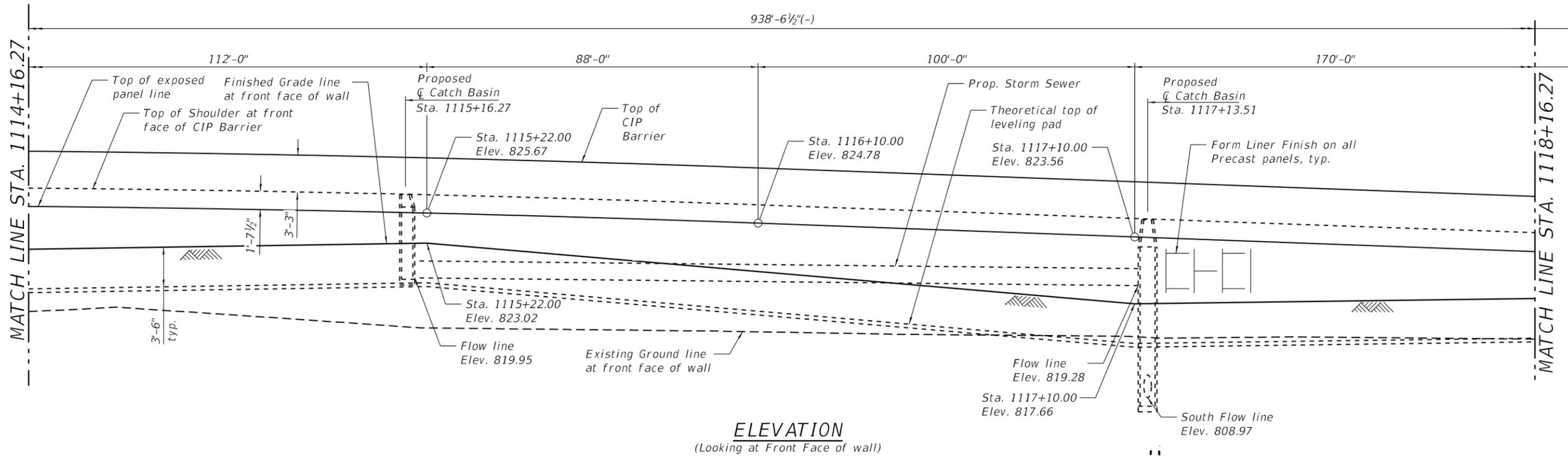
KANE COUNTY DEPARTMENT OF TRANSPORTATION

GENERAL PLAN AND ELEVATION (1 OF 3) STRUCTURE NO. 045-2108

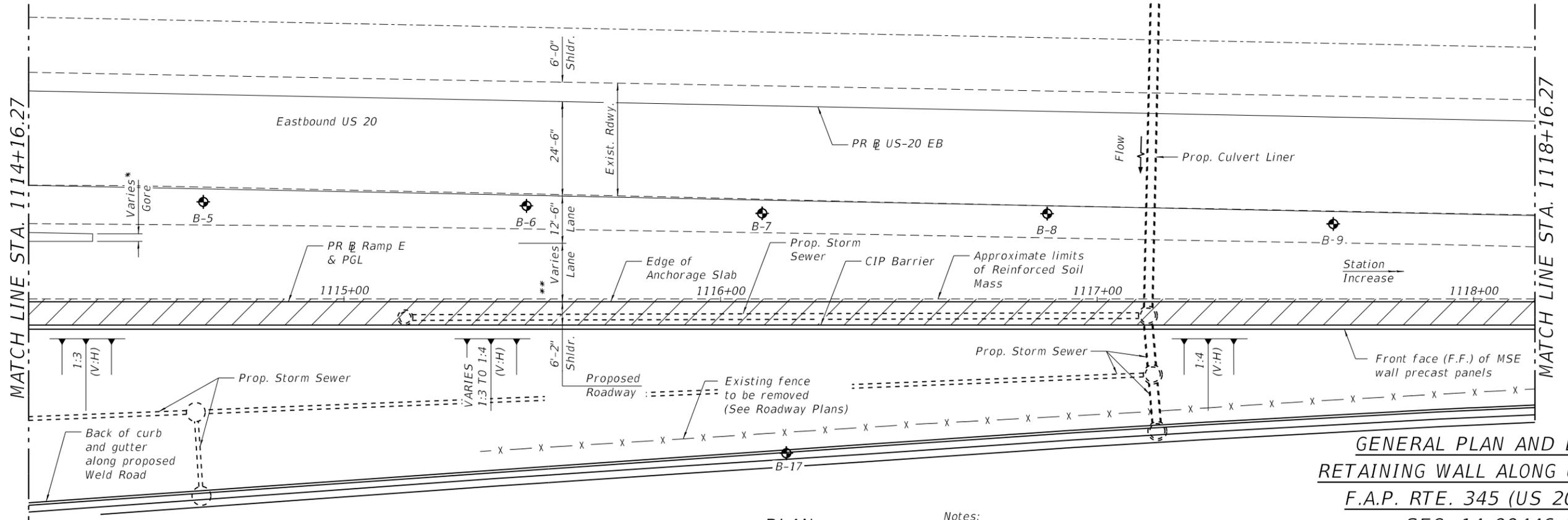
SHEET 1 OF 18 SHEETS

F.A.P. RTE. 336/345	SECTION 14-00446-00-CH	COUNTY KANE	TOTAL SHEETS 354	SHEET NO. 137
CONTRACT NO. 61F57				

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT



ELEVATION
(Looking at Front Face of wall)



PLAN

* 8'-7" to 0'-0" from Sta. 1111+16.27 to Sta. 1115+33.26
 ** 16'-0" to 5'-6 3/4" (+) from Sta. 1115+33.26 to Sta. 1120+55.00

Notes:
 Wall offsets are measured from the PR & Ramp E to the front face of precast panels.
 See Drainage Plans for drainage details.
 See Roadway Typical Sections and SGR for estimated location of Unsuitable Material removal.

**GENERAL PLAN AND ELEVATION
 RETAINING WALL ALONG US 20 RAMP E
 F.A.P. RTE. 345 (US 20/WELD RD)
 SEC. 14-00446-00-CH
 KANE COUNTY
 STATION 1111+16.27 TO 1120+55.00
 STRUCTURE NO. 045-2108**

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HDR
 HDR
 8550 W. BRYN MAWR AVE.
 CHICAGO, IL 60631

USER NAME =	MKROK
DESIGNED -	BJC
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REVISOR -	
REVISIONS -	
DATE =	1/3/19

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REVISOR -	
REVISIONS -	

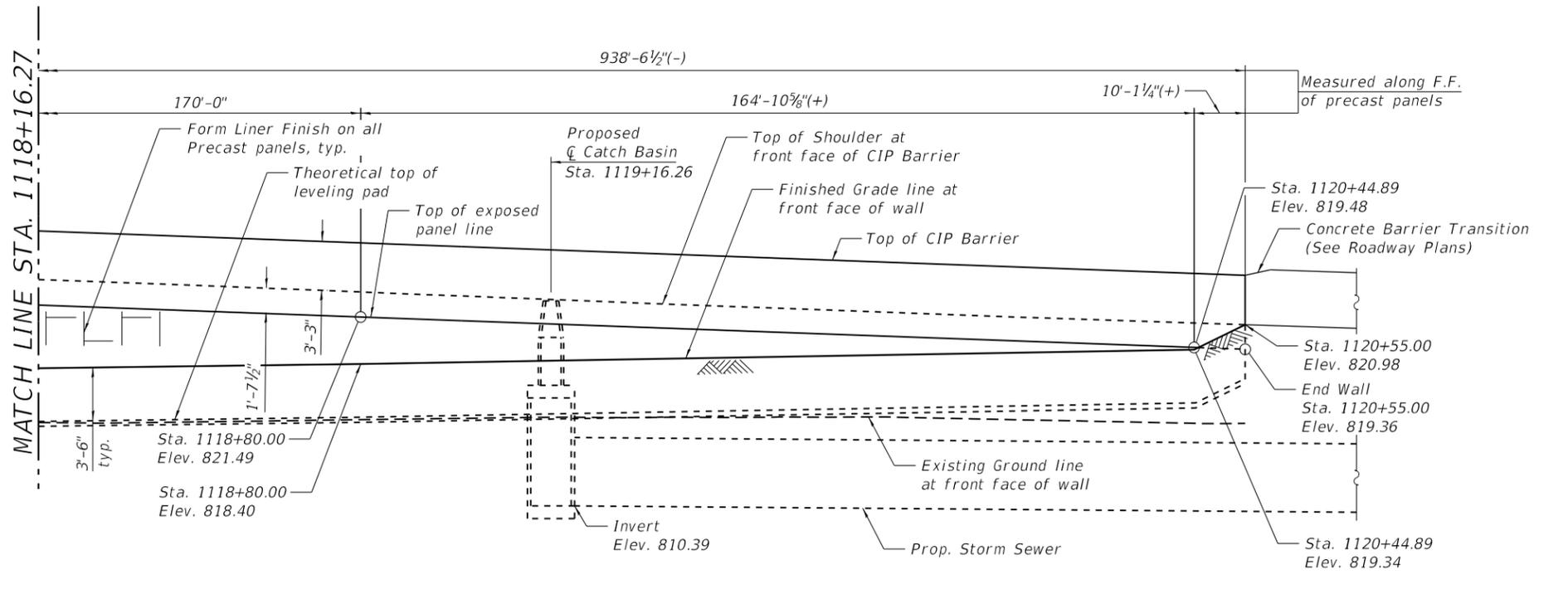
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REVISIONS -	

**KANE COUNTY
 DEPARTMENT OF TRANSPORTATION**

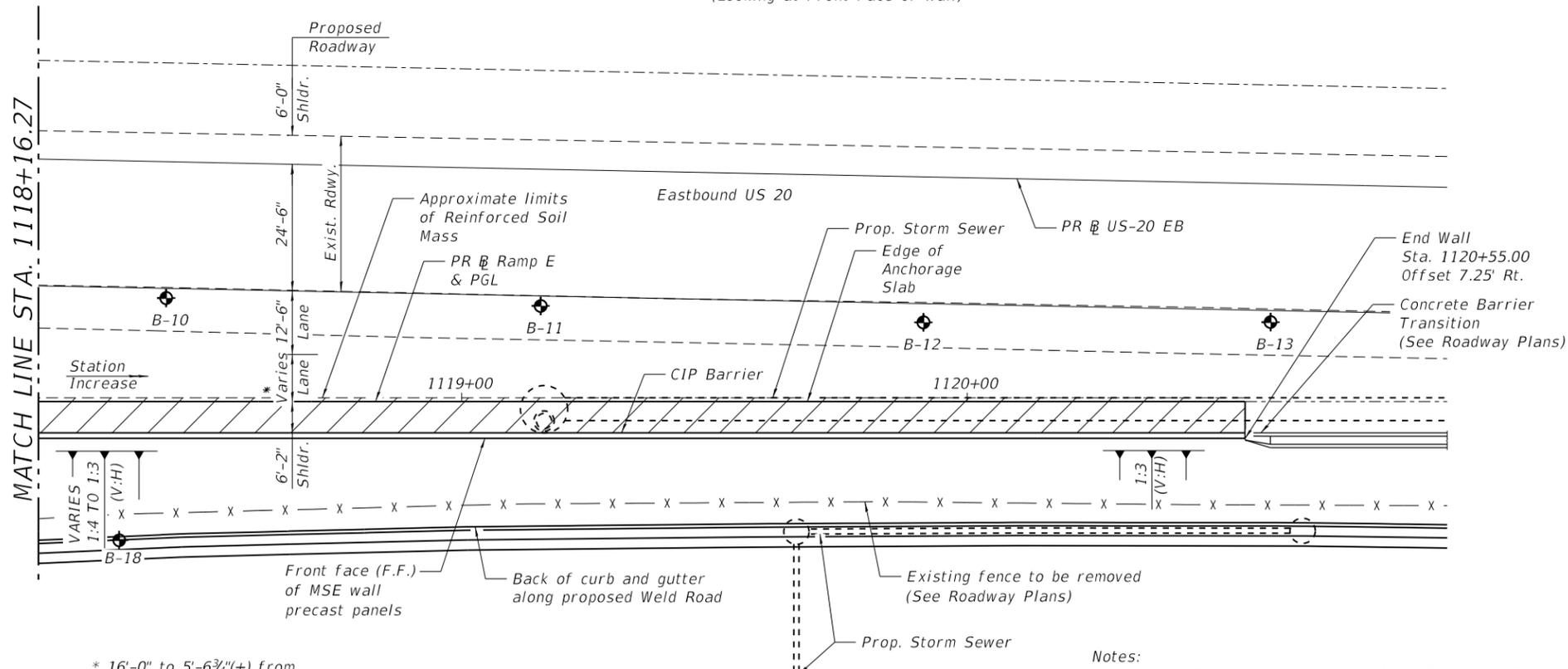
**GENERAL PLAN AND ELEVATION (2 OF 3)
 STRUCTURE NO. 045-2108**

SHEET 2 OF 18 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	138
CONTRACT NO. 61F57				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



ELEVATION
(Looking at Front Face of wall)



PLAN

* 16'-0" to 5'-6 3/4" (+) from Sta. 1115+33.26 to Sta. 1120+55.00

Notes:
Wall offsets are measured from the PR @ Ramp E to the front face of precast panels.
See Drainage Plans for drainage details.
See Roadway Typical Sections and SGR for estimated location of Unsuitable Material removal.

**GENERAL PLAN AND ELEVATION
RETAINING WALL ALONG US 20 RAMP E
F.A.P. RTE. 345 (US 20/WELD RD)
SEC. 14-00446-00-CH
KANE COUNTY
STATION 1111+16.27 TO 1120+55.00
STRUCTURE NO. 045-2108**

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**KANE COUNTY
DEPARTMENT OF TRANSPORTATION**

**GENERAL PLAN AND ELEVATION (3 OF 3)
STRUCTURE NO. 045-2108**

SHEET 3 OF 18 SHEETS

F.A.P. RTE. 336/345	SECTION 14-00446-00-CH	COUNTY KANE	TOTAL SHEETS 354	SHEET NO. 139
CONTRACT NO. 61F57				

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

GENERAL NOTES:

1. Reinforcement bars designated (E) shall be epoxy coated.
2. Wall stations and offsets are given to the front face (FF) of the wall and are measured from the Ramp E baseline, except as noted. FF of the wall is to be considered edge of panel or form liner.
3. Slipforming of the parapet is allowed.
4. Protective coat shall be applied to top of Anchorage Slab and top and traffic face of barrier rail.
5. Form Liner Textured Surface shall be ashlar stone pattern with 1" maximum relief and 3/4" minimum relief.
6. Anti-Graffiti Protection System shall be applied to the exposed face of precast panels, underside of anchorage slab overhang and outside vertical faces of anchorage slab and barrier rail.

TOTAL BILL OF MATERIAL

ITEM	UNIT	TOTAL
Structure Excavation	Cu. Yd.	575
Concrete Superstructure	Cu. Yd.	466
Form Liner Textured Surface	Sq. Ft.	8,013
Protective Coat	Sq. Yd.	1,072
Reinforcement Bars, Epoxy Coated	Pound	65,840
Name Plates	Each	1
Mechanically Stabilized Earth Retaining Wall	Sq. Ft.	7,079
Anti-Graffiti Protection System	Sq. Ft.	8,691

STATION 1111+16.27
 BUILT 201 BY
 KANE COUNTY
 F.A.P. RT. 345
 SEC. 14-00446-00-CH
 STRUCTURE NO. 045-2108

NAME PLATE
 See Std. 515001

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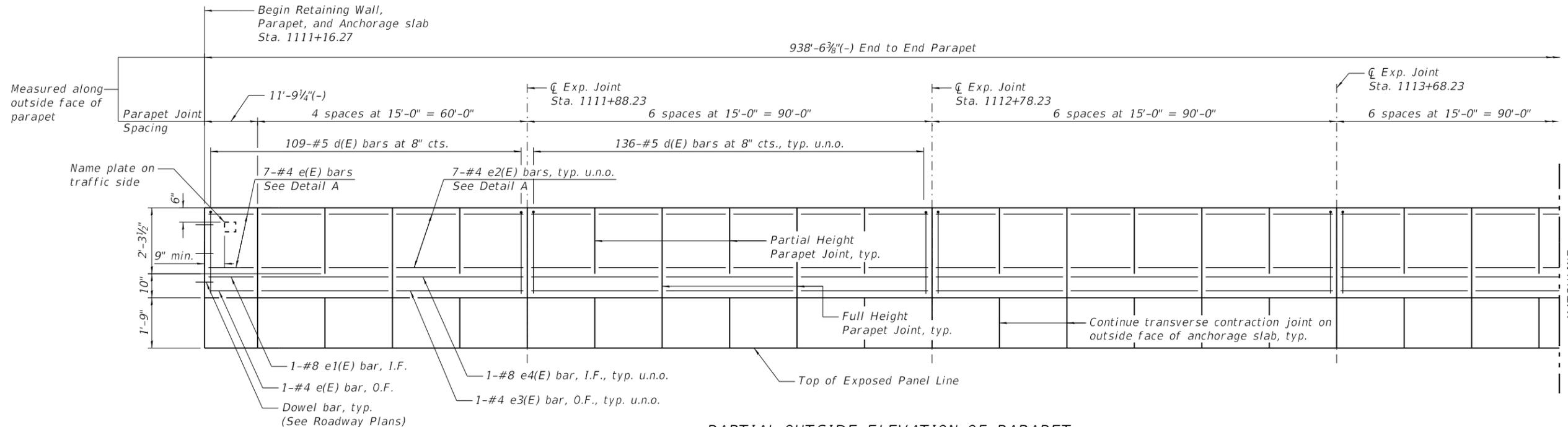
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**KANE COUNTY
 DEPARTMENT OF TRANSPORTATION**

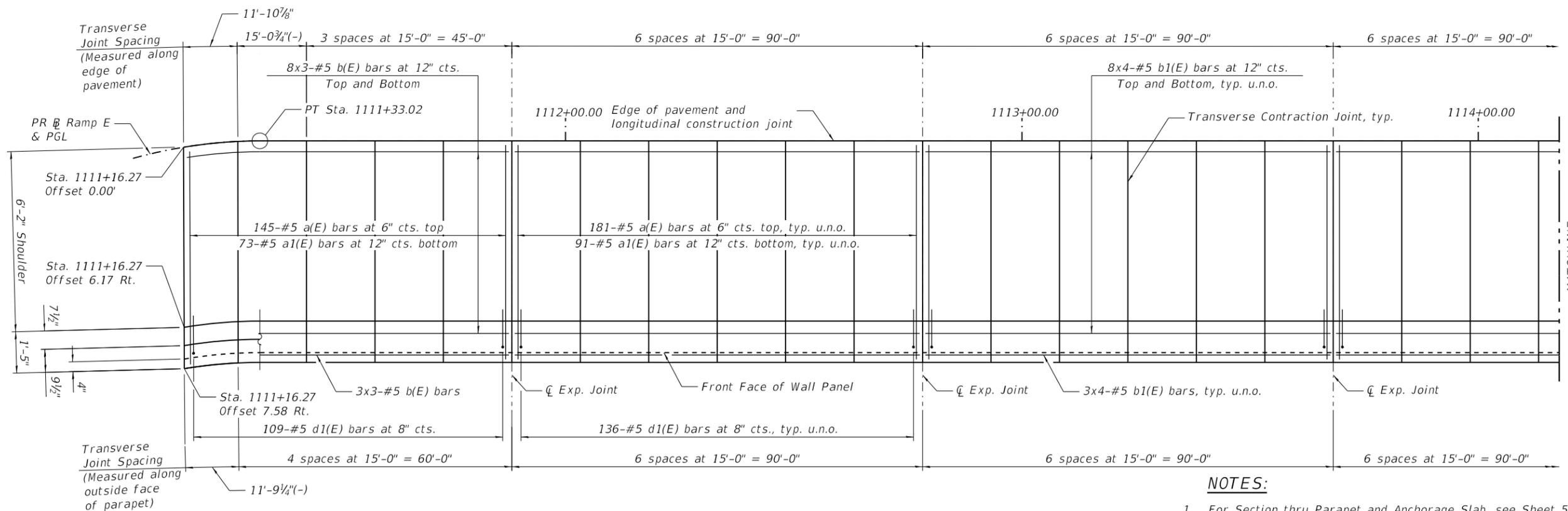
**GENERAL NOTES
 STRUCTURE NO. 045-2108**

SHEET 4 OF 18 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	140
CONTRACT NO. 61F57				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



PARTIAL OUTSIDE ELEVATION OF PARAPET



PARTIAL PLAN - PARAPET AND ANCHORAGE SLAB

NOTES:

1. For Section thru Parapet and Anchorage Slab, see Sheet 5.
2. Stations and offsets on this sheet measured from the Ramp E baseline.
3. Bars indicated thus 8x4-#5 etc. indicates 8 lines of bars with 4 lengths per line.
4. For Bill of Materials, See Sheet 9

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**KANE COUNTY
DEPARTMENT OF TRANSPORTATION**

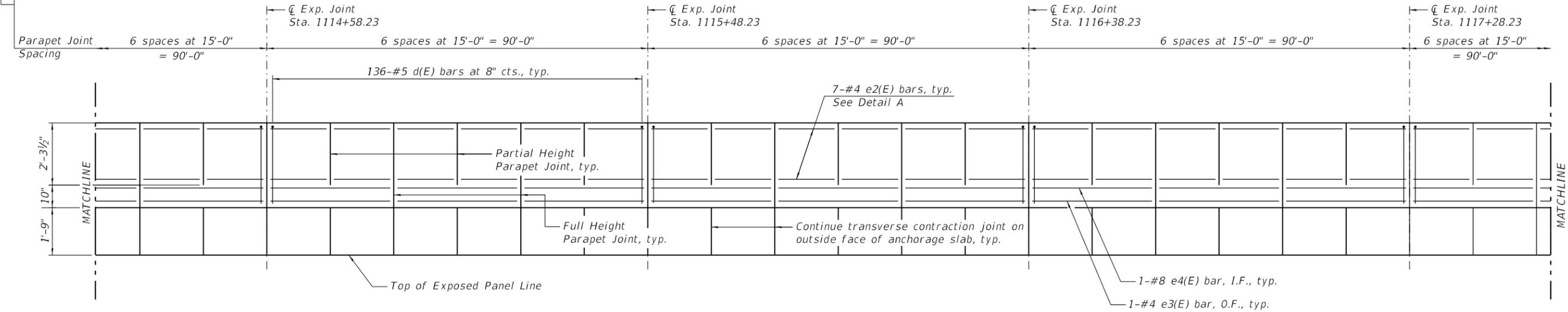
**ANCHORAGE SLAB GENERAL PLAN (1 OF 3)
STRUCTURE NO. 045-2108**

SHEET 6 OF 18 SHEETS

F.A.P. RTE. 336/345	SECTION 14-00446-00-CH	COUNTY KANE	TOTAL SHEETS 354	SHEET NO. 142
CONTRACT NO. 61F57				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

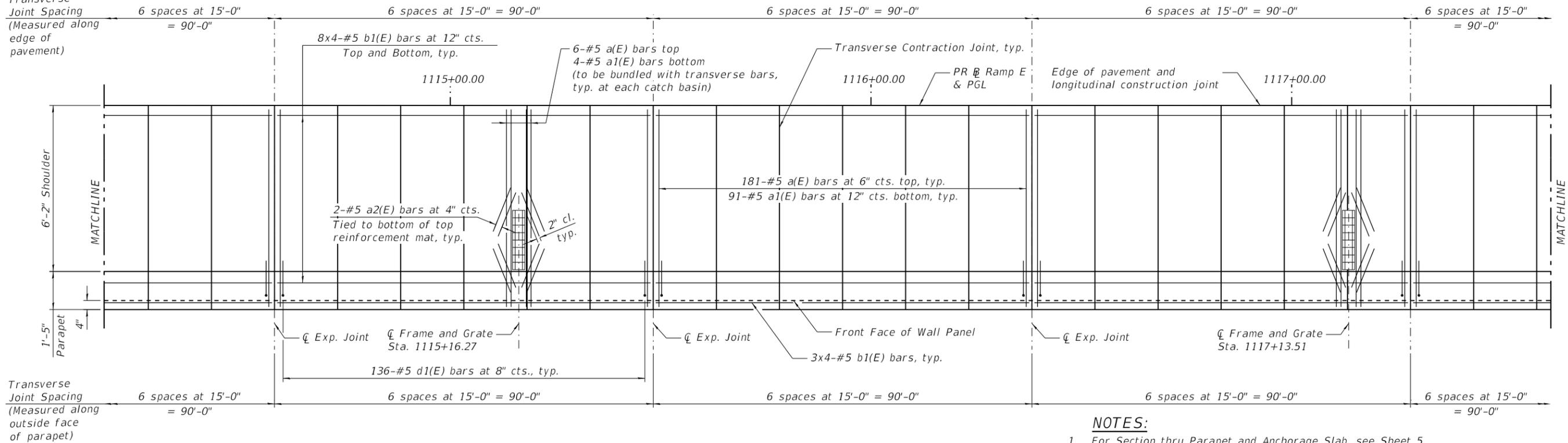
Measured along
outside face of
parapet

938'-6³/₈"(-) End to End Parapet



PARTIAL OUTSIDE ELEVATION OF PARAPET

Transverse
Joint Spacing
(Measured along
edge of
pavement)



PARTIAL PLAN - PARAPET AND ANCHORAGE SLAB

NOTES:

1. For Section thru Parapet and Anchorage Slab, see Sheet 5.
2. Stations and offsets on this sheet are measured from the Ramp E baseline.
3. Bars indicated thus 8x4-#5 etc. indicates 8 lines of bars with 4 lengths per line.
4. Cut transverse and longitudinal bars as necessary to provide 2" clearance to drainage structure.
5. For Bill of Materials, see Sheet 9.
6. For Frame and Grate Details see Drainage Plans.

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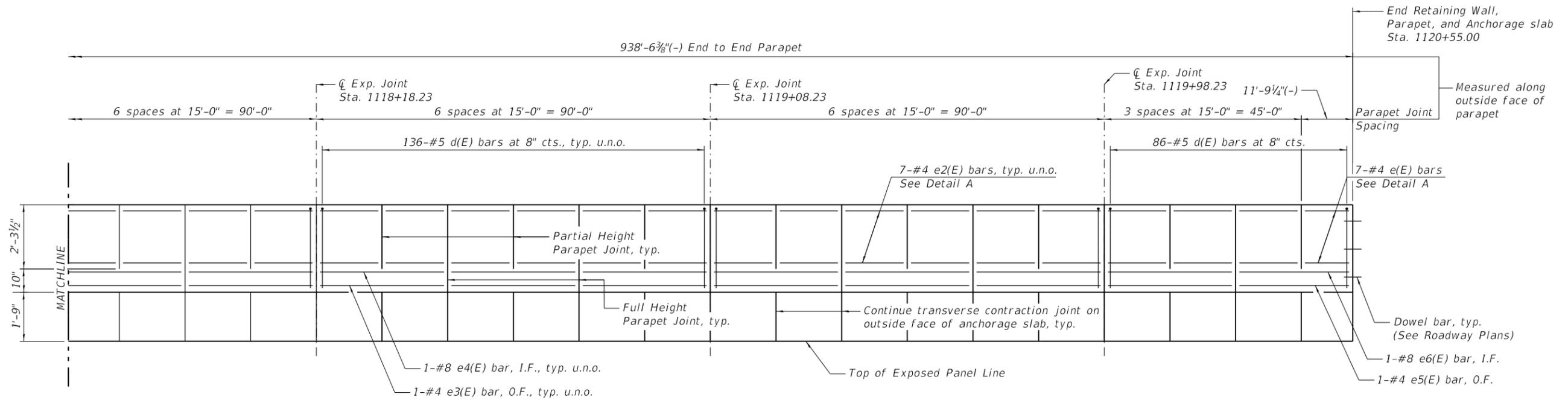
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HDR
8550 W. BRYN MAWR AVE.
CHICAGO, IL 60631

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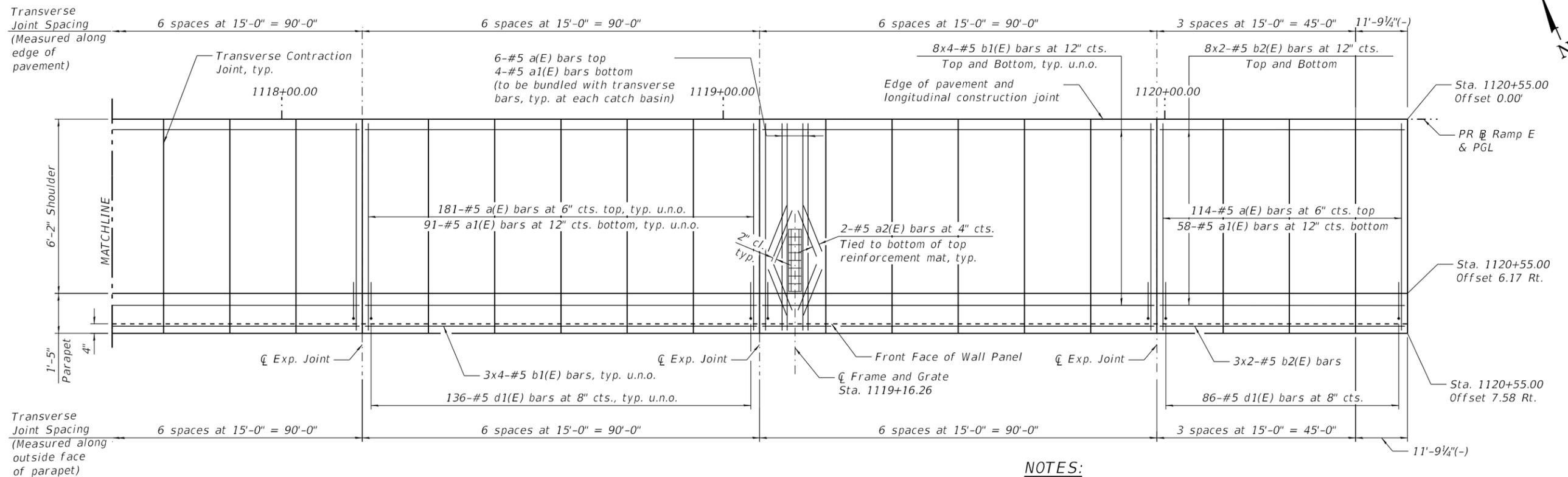
**KANE COUNTY
DEPARTMENT OF TRANSPORTATION**

**ANCHORAGE SLAB GENERAL PLAN (2 OF 3)
STRUCTURE NO. 045-2108**

F.A.P. RTE. 336/345	SECTION 14-00446-00-CH	COUNTY KANE	TOTAL SHEETS 354	SHEET NO. 143
CONTRACT NO. 61F57				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



PARTIAL OUTSIDE ELEVATION OF PARAPET



PARTIAL PLAN - PARAPET AND ANCHORAGE SLAB

NOTES:

1. For Section thru Parapet and Anchorage Slab, see Sheet 5.
2. Stations and offsets on this sheet are measured from the Ramp E baseline.
3. Bars indicated thus 8x4-#5 etc. indicates 8 lines of bars with 4 lengths per line.
4. Cut transverse and longitudinal bars as necessary to provide 2" clearance to drainage structure.
5. For Bill of Materials, see Sheet 9.
6. For Frame and Grate Details see Drainage Plans.

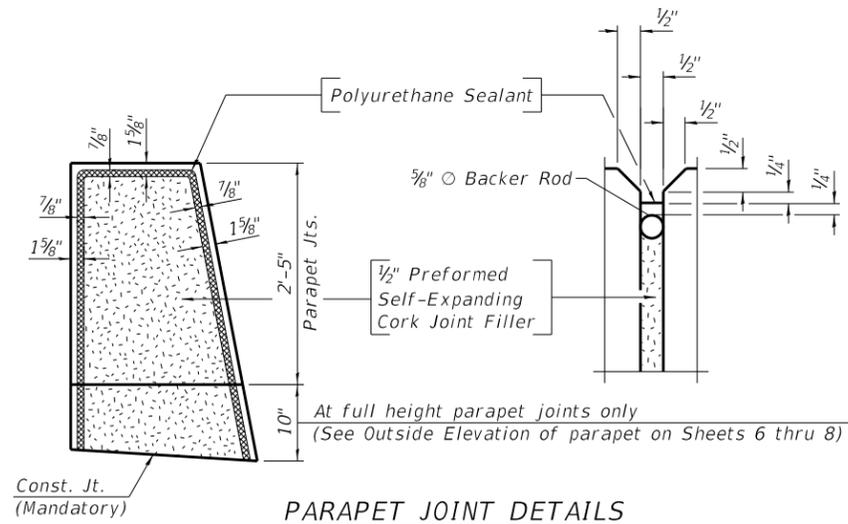
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KANE COUNTY
DEPARTMENT OF TRANSPORTATION

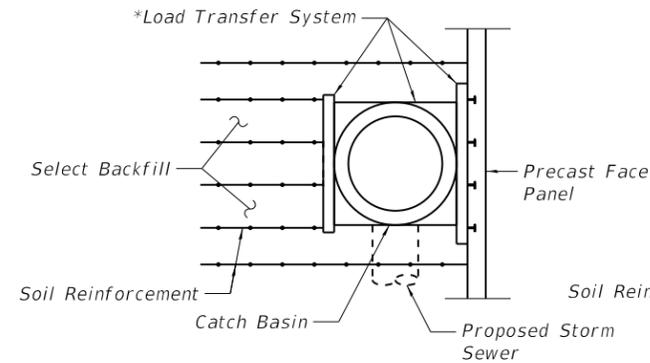
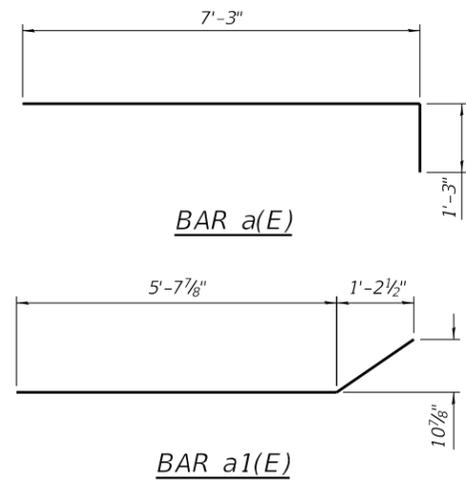
ANCHORAGE SLAB GENERAL PLAN (3 OF 3)
STRUCTURE NO. 045-2108
 SHEET 8 OF 18 SHEETS

F.A.P. RTE. 336/345	SECTION 14-00446-00-CH	COUNTY KANE	TOTAL SHEETS 354	SHEET NO. 144
CONTRACT NO. 61F57				
FED. ROAD DIST. NO. 1		ILLINOIS	FED. AID PROJECT	

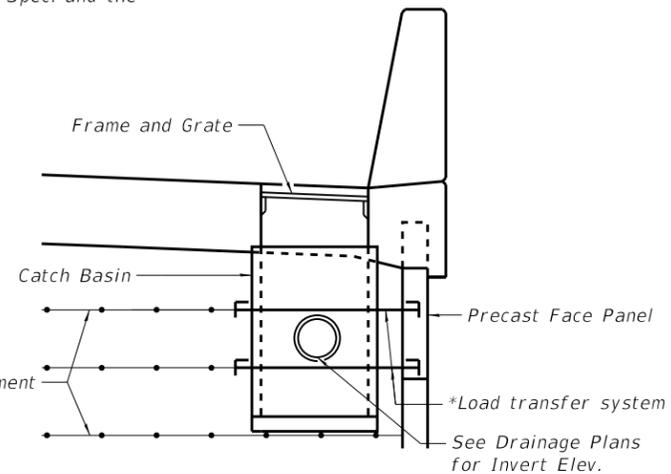


PARAPET JOINT DETAILS

The polyurethane Sealant shall be according to Article 1050.04 of the Std. Spec. and the color shall be gray

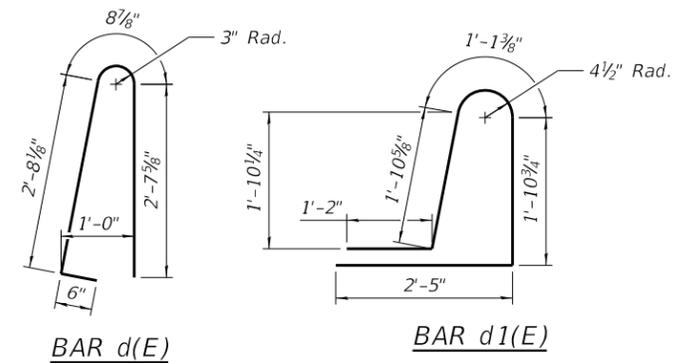


ANCHORAGE SLAB INLET PLAN



ANCHORAGE SLAB INLET SECTION

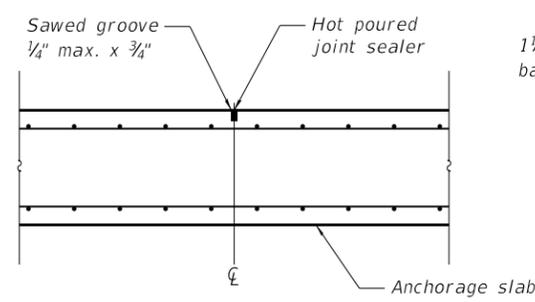
*M.S.E. supplier to design load transfer system to accommodate concrete pipe and catch basin.



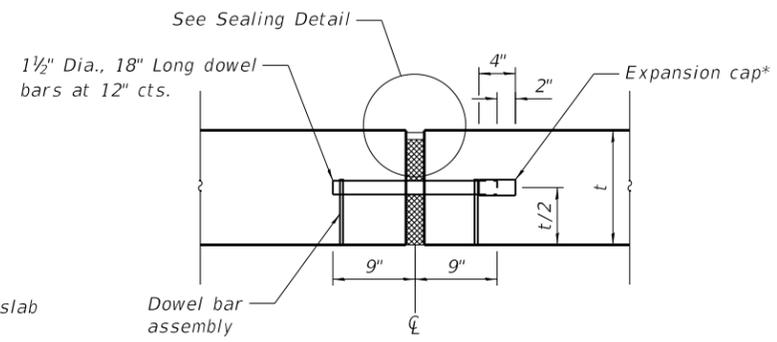
RETAINING WALL BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	1906	#5	8'-6"	—
a1(E)	962	#5	7'-2"	—
a2(E)	24	#5	2'-0"	—
b(E)	33	#5	26'-3"	—
b1(E)	396	#5	25'-1"	—
b2(E)	22	#5	30'-0"	—
d(E)	1419	#5	6'-7"	—
d1(E)	1419	#5	8'-6"	—
e(E)	15	#4	11'-6"	—
e1(E)	1	#8	11'-6"	—
e2(E)	427	#4	14'-9"	—
e3(E)	30	#4	29'-9"	—
e4(E)	30	#8	29'-9"	—
e5(E)	1	#4	26'-6"	—
e6(E)	1	#8	26'-6"	—
Reinforcement Bars, Epoxy Coated			Pound	65,840
Concrete Superstructure			Cu. Yd.	466
Protective Coat			Sq. Yd.	1,072

MIN BAR LAP
#5 bars - 3'-6"



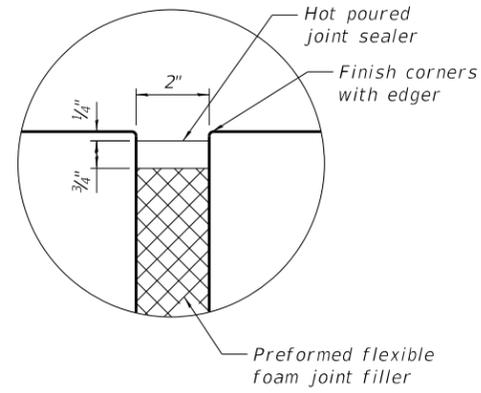
TRANSVERSE CONTRACTION JOINT



ANCHORAGE SLAB EXPANSION JOINT

*Expansion caps shall be installed on the exposed end of each dowel bar once the header has been removed and the joint filler material has been installed.

Expansion joint and dowel bars included in the cost of Concrete Superstructure



SEALING DETAIL

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DATE = 1/3/19	DRAWN - BJC	REVISED -
	CHECKED - MJK	REVISED -

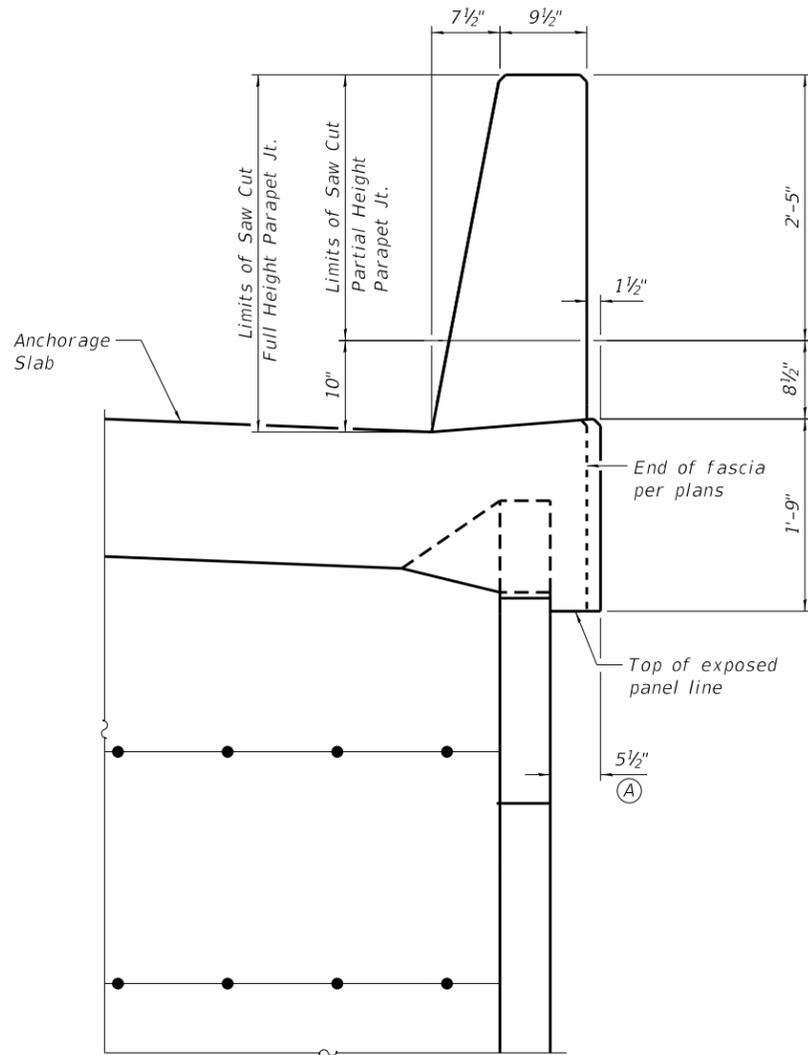
**KANE COUNTY
DEPARTMENT OF TRANSPORTATION**

**ANCHORAGE SLAB AND WALL DETAILS
STRUCTURE NO. 045-2108**

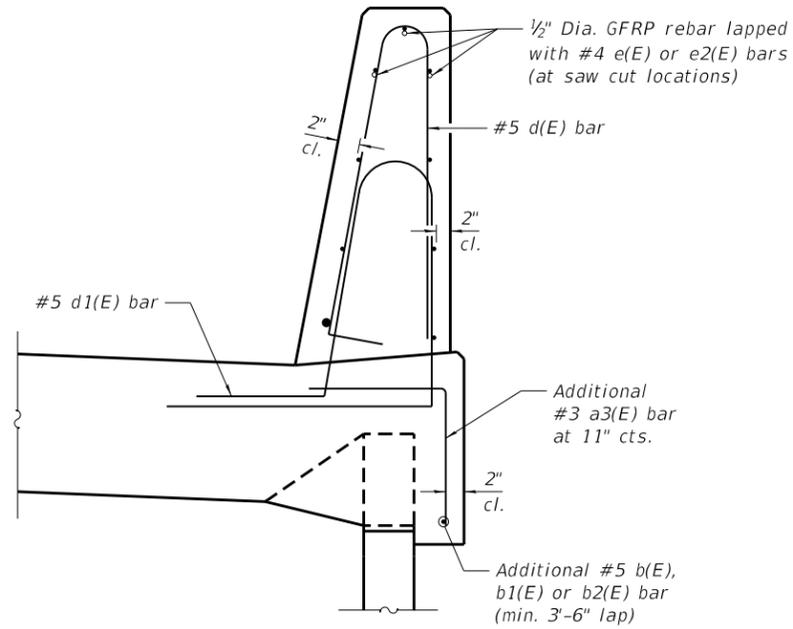
SHEET 9 OF 18 SHEETS

F.A.P. RTE. 336/345	SECTION 14-00446-00-CH	COUNTY KANE	TOTAL SHEETS 354	SHEET NO. 145
CONTRACT NO. 61F57				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		

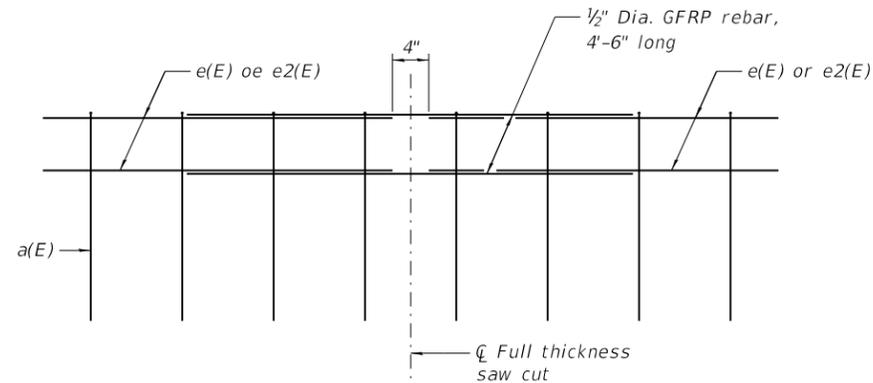
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SECTION THRU PARAPET AND ANCHORAGE SLAB



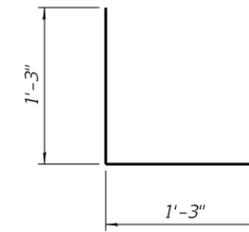
SECTION
 (Showing reinforcement clearances for slip forming and additional reinforcement)



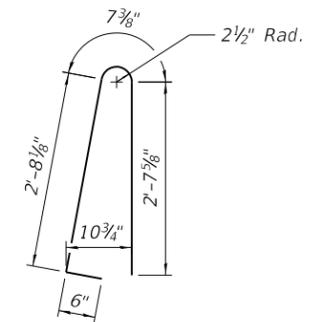
GFRP REBAR STIFFENING DETAIL

GENERAL NOTES

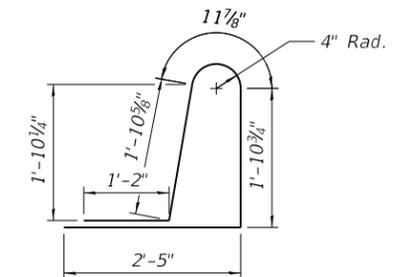
1. All dimensions shall remain the same as shown on typical details, except dimension A, which is to be revised as shown. Additional concrete needed to revise dimension A equals 0.01 cu. yds./ft
2. Full thickness saw cut at all joint locations in lieu of cork joint filler



#3 a3(E) BAR



ALTERNATE BAR #5-d(E)



ALTERNATE BAR #5-d1(E)



HDR
 8550 W. BRYN MAWR AVE.
 CHICAGO, IL 60631

USER NAME = MKROK	DESIGNED - BJC	REVISED -
CHECKED - MJK	REVISIONS -	
PLOT SCALE = N.T.S.	DRAWN - BJC	REVISED -
DATE = 1/3/19	CHECKED - MJK	REVISED -

**KANE COUNTY
 DEPARTMENT OF TRANSPORTATION**

**PARAPET SLIPFORMING OPTION
 STRUCTURE NO. 045-2108**

SHEET 10 OF 18 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	146
CONTRACT NO. 61F57				
FED. ROAD DIST. NO. 1	ILLINOIS	FED. AID PROJECT		



SOIL BORING LOG

ROUTE US Route 20 DESCRIPTION Retaining Wall Along US 20 Ramp LOGGED BY D.C.

SECTION 14-00446-00CH LOCATION US Route 20

COUNTY Kane County DRILLING METHOD Hollow Stem Auger HAMMER TYPE Automatic

Table with columns for D, B, U, M, O, I, S, T (Depth, Bulge, Unconfined Compressive Strength, Moisture, Organic Content, SPT) and Soil Description. Includes data for approximately 8 inches of ASPHALT, 6 inches of CONCRETE, and various soil layers like A-1: Brown clayey SAND, A-6, A-7: Brown and gray silty CLAY, A-1: Brown SAND with gravel, A-2: Gray silty SAND, and A-6, A-7: Brown silty CLAY.

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer) The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206) BBS, from 137 (Rev. 8-99)



SOIL BORING LOG

ROUTE US Route 20 DESCRIPTION Retaining Wall Along US 20 Ramp LOGGED BY D.C.

SECTION 14-00446-00CH LOCATION US Route 20

COUNTY Kane County DRILLING METHOD Hollow Stem Auger HAMMER TYPE Automatic

Table with columns for D, B, U, M, O, I, S, T and Soil Description. Includes data for approximately 8 inches of ASPHALT, 6 inches of CONCRETE, and various soil layers like A-6, A-7: Brown and gray silty CLAY, A-1: Brown SAND with gravel, A-2: Gray silty SAND, and A-6, A-7: Brown silty CLAY.

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer) The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206) BBS, from 137 (Rev. 8-99)



SOIL BORING LOG

ROUTE US Route 20 DESCRIPTION Retaining Wall Along US 20 Ramp LOGGED BY D.C.

SECTION 14-00446-00CH LOCATION US Route 20

COUNTY Kane County DRILLING METHOD Hollow Stem Auger HAMMER TYPE Automatic

Table with columns for D, B, U, M, O, I, S, T and Soil Description. Includes data for approximately 9 inches of ASPHALT, 6 inches of CONCRETE, and various soil layers like A-1: Brown SAND with gravel, A-2: Gray silty SAND, and A-1: Brown SAND with gravel.

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer) The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206) BBS, from 137 (Rev. 8-99)

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HDR logo and address: 8550 W. BRYN MAWR AVE. CHICAGO, IL 60631

Table with columns for USER NAME, DESIGNED, CHECKED, PLOT SCALE, DATE, REVISED, DRAWN, and REVISIONS.

KANE COUNTY DEPARTMENT OF TRANSPORTATION

SOIL BORING LOGS STRUCTURE NO. 045-2108 SHEET 11 OF 18 SHEETS

Table with columns for F.A.P. RTE., SECTION, COUNTY, TOTAL SHEETS, SHEET NO., and CONTRACT NO.



SOIL BORING LOG

Page 1 of 1
Date 7/14/16

ROUTE US Route 20 DESCRIPTION Retaining Wall Along US 20 Ramp LOGGED BY D.C.

SECTION 14-00446-00CH LOCATION US Route 20

COUNTY Kane County DRILLING METHOD Hollow Stem Auger HAMMER TYPE Automatic

Table with columns for STRUCT. NO., BORING NO., Station, Offset, Ground Surface Elev., and soil data columns (D, B, U, M, O, I, S, T).

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)
BBS, from 137 (Rev. 8-99)



SOIL BORING LOG

Page 1 of 1
Date 7/15/16

ROUTE US Route 20 DESCRIPTION Retaining Wall Along US 20 Ramp LOGGED BY D.C.

SECTION 14-00446-00CH LOCATION US Route 20

COUNTY Kane County DRILLING METHOD Hollow Stem Auger HAMMER TYPE Automatic

Table with columns for STRUCT. NO., BORING NO., Station, Offset, Ground Surface Elev., and soil data columns (D, B, U, M, O, I, S, T).

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)
BBS, from 137 (Rev. 8-99)



SOIL BORING LOG

Page 1 of 1
Date 7/15/16

ROUTE US Route 20 DESCRIPTION Retaining Wall Along US 20 Ramp LOGGED BY D.C.

SECTION 14-00446-00CH LOCATION US Route 20

COUNTY Kane County DRILLING METHOD Hollow Stem Auger HAMMER TYPE Automatic

Table with columns for STRUCT. NO., BORING NO., Station, Offset, Ground Surface Elev., and soil data columns (D, B, U, M, O, I, S, T).

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)
BBS, from 137 (Rev. 8-99)

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USER NAME = MKROK
DESIGNED - BJC
CHECKED - MJK
REVISOR -
PLOT SCALE = N.T.S.
DRAWN - BJC
REVISOR -
DATE = 1/3/19
CHECKED - MJK
REVISOR -

KANE COUNTY
DEPARTMENT OF TRANSPORTATION

SOIL BORING LOGS
STRUCTURE NO. 045-2108
SHEET 12 OF 18 SHEETS

Table with columns: F.A.P. RTE., SECTION, COUNTY, TOTAL SHEETS, SHEET NO., CONTRACT NO. 61F57, FED. ROAD DIST. NO. 1, ILLINOIS, FED. AID PROJECT



SOIL BORING LOG

Page 1 of 1
Date 7/19/16

ROUTE US Route 20 DESCRIPTION Retaining Wall Along US 20 Ramp LOGGED BY D.C.

SECTION 14-00446-00CH LOCATION US Route 20

COUNTY Kane County DRILLING METHOD Hollow Stem Auger HAMMER TYPE Automatic

Table with columns for STRUCT. NO., BORING NO., Station, Offset, Ground Surface Elev., and soil data columns (D, B, U, M, P, O, S, T, H, S, Qu, T). Includes soil descriptions like ASPHALT, CONCRETE, and SAND with gravel.

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)
BBS, from 137 (Rev. 8-99)



SOIL BORING LOG

Page 1 of 1
Date 7/20/16

ROUTE US Route 20 DESCRIPTION Retaining Wall Along US 20 Ramp LOGGED BY D.C.

SECTION 14-00446-00CH LOCATION US Route 20

COUNTY Kane County DRILLING METHOD Hollow Stem Auger HAMMER TYPE Automatic

Table with columns for STRUCT. NO., BORING NO., Station, Offset, Ground Surface Elev., and soil data columns (D, B, U, M, P, O, S, T, H, S, Qu, T). Includes soil descriptions like ASPHALT, CONCRETE, and SAND with gravel.

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)
BBS, from 137 (Rev. 8-99)



SOIL BORING LOG

Page 1 of 1
Date 7/20/16

ROUTE US Route 20 DESCRIPTION Retaining Wall Along US 20 Ramp LOGGED BY D.C.

SECTION 14-00446-00CH LOCATION US Route 20

COUNTY Kane County DRILLING METHOD Hollow Stem Auger HAMMER TYPE Automatic

Table with columns for STRUCT. NO., BORING NO., Station, Offset, Ground Surface Elev., and soil data columns (D, B, U, M, P, O, S, T, H, S, Qu, T). Includes soil descriptions like ASPHALT, CONCRETE, and SAND with gravel.

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)
BBS, from 137 (Rev. 8-99)

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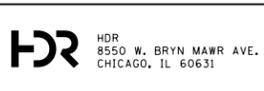


Table with columns for USER NAME, DESIGNED, CHECKED, PLOT SCALE, DATE, and REVISIONS.

KANE COUNTY DEPARTMENT OF TRANSPORTATION

SOIL BORING LOGS STRUCTURE NO. 045-2108

Table with columns for F.A.P. RTE., SECTION, COUNTY, TOTAL SHEETS, SHEET NO., and CONTRACT NO.



SOIL BORING LOG

Page 1 of 1
Date 7/21/16

ROUTE US Route 20 DESCRIPTION Retaining Wall Along US 20 Ramp LOGGED BY D.C.
SECTION 14-00446-00CH LOCATION US Route 20
COUNTY Kane County DRILLING METHOD Hollow Stem Auger HAMMER TYPE Automatic

Table with columns for STRUCT. NO., BORING NO., and soil data columns (D, B, U, M, etc.). Includes soil descriptions like 'A-3: Brown SAND with gravel, trace fines; Medium dense' and 'A-6, A-7: Brown and gray silty CLAY'.

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)
BBS, from 137 (Rev. 8-99)



SOIL BORING LOG

Page 1 of 1
Date 4/25/18

ROUTE US Route 20 DESCRIPTION Phase II Retaining Wall along US 20 LOGGED BY J.W.
SECTION 14-00446-00CH LOCATION US 20 & Randall Road Interchange, Elgin, IL
COUNTY Kane County DRILLING METHOD Hollow Stem Auger HAMMER TYPE Automatic

Table with columns for STRUCT. NO., BORING NO., and soil data columns (D, B, U, M, etc.). Includes soil descriptions like 'TOPSOIL: Black and brown silty clay with roots' and 'Gray GRAVELY SAND to SANDY GRAVEL'.

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)
BBS, from 137 (Rev. 8-99)



SOIL BORING LOG

Page 1 of 1
Date 4/25/18

ROUTE US Route 20 DESCRIPTION Phase II Retaining Wall along US 20 LOGGED BY J.W.
SECTION 14-00446-00CH LOCATION US 20 & Randall Road Interchange, Elgin, IL
COUNTY Kane County DRILLING METHOD Hollow Stem Auger HAMMER TYPE Automatic

Table with columns for STRUCT. NO., BORING NO., and soil data columns (D, B, U, M, etc.). Includes soil descriptions like 'TOPSOIL: black and brown silty clay' and 'Medium stiff to stiff, gray SILT to SILTY LOAM'.

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)
BBS, from 137 (Rev. 8-99)

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SOIL BORING LOG

Page 1 of 1
Date 5/22/18

ROUTE US Route 20 DESCRIPTION Phase II Retaining Wall along US 20 LOGGED BY J.W.
SECTION 14-00446-00CH LOCATION US 20 & Randall Road Interchange, Elgin, IL
COUNTY Kane County DRILLING METHOD Hollow Stem Auger HAMMER TYPE Automatic

Table with columns for STRUCT. NO., BORING NO., Station, Offset, Ground Surface Elev., and soil test results (D, B, U, M, P, O, S, T, H, S, Qu, T) for various soil types like Sandy gravel, Silty clay, and Silty loam.

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)
BBS, from 137 (Rev. 8-99)



SOIL BORING LOG

Page 1 of 1
Date 4/20/18

ROUTE US Route 20 DESCRIPTION Phase II Retaining Wall along US 20 LOGGED BY J.W.
SECTION 14-00446-00CH LOCATION US 20 & Randall Road Interchange, Elgin, IL
COUNTY Kane County DRILLING METHOD Hollow Stem Auger HAMMER TYPE Automatic

Table with columns for STRUCT. NO., BORING NO., Station, Offset, Ground Surface Elev., and soil test results for various soil types like Silty clay, Silty loam, and Sandy loam.

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)
BBS, from 137 (Rev. 8-99)



SOIL BORING LOG

Page 1 of 1
Date 4/20/18

ROUTE US Route 20 DESCRIPTION Phase II Retaining Wall along US 20 LOGGED BY J.W.
SECTION 14-00446-00CH LOCATION US 20 & Randall Road Interchange, Elgin, IL
COUNTY Kane County DRILLING METHOD Hollow Stem Auger HAMMER TYPE Automatic

Table with columns for STRUCT. NO., BORING NO., Station, Offset, Ground Surface Elev., and soil test results for various soil types like Silty clay, Silty loam, and Sandy loam.

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)
BBS, from 137 (Rev. 8-99)

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Table with columns for USER NAME, DESIGNED, CHECKED, PLOT SCALE, DATE, REVISED, DRAWN, and REVISIONS.

KANE COUNTY DEPARTMENT OF TRANSPORTATION

SOIL BORING LOGS STRUCTURE NO. 045-2108 SHEET 16 OF 18 SHEETS

Table with columns for F.A.P. RTE., SECTION, COUNTY, TOTAL SHEETS, SHEET NO., and CONTRACT NO.



SOIL BORING LOG

Page 1 of 1
Date 10/6/16

ROUTE US Route 20 DESCRIPTION Phase II Retaining Wall along US 20 LOGGED BY J.K
SECTION 14-00446-00CH LOCATION US 20 & Randall Road Interchange, Elgin, IL
COUNTY Kane County DRILLING METHOD Humboldt DCP + Hand Auger HAMMER TYPE Humboldt DCP

Table with columns: STRUCT. NO., BORING NO., Station, Offset, Ground Surface Elev., D E L T H, B L W, U C S Qu, M O I S T, Surface Water Elev., Stream Bed Elev., Groundwater Elev., First Encounter, Upon Completion, After. Includes soil log data for approximately 9 inches of TOPSOIL and Brown SAND with gravel.

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)
BBS, from 137 (Rev. 8-99)



SOIL BORING LOG

Page 1 of 1
Date 10/6/16

ROUTE US Route 20 DESCRIPTION Phase II Retaining Wall along US 20 LOGGED BY J.K
SECTION 14-00446-00CH LOCATION US 20 & Randall Road Interchange, Elgin, IL
COUNTY Kane County DRILLING METHOD Humboldt DCP + Hand Auger HAMMER TYPE Humboldt DCP

Table with columns: STRUCT. NO., BORING NO., Station, Offset, Ground Surface Elev., D E L T H, B L W, U C S Qu, M O I S T, Surface Water Elev., Stream Bed Elev., Groundwater Elev., First Encounter, Upon Completion, After. Includes soil log data for approximately 10 inches of TOPSOIL and Brown SAND with gravel.

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)
BBS, from 137 (Rev. 8-99)



SOIL BORING LOG

Page 1 of 1
Date 10/6/16

ROUTE US Route 20 DESCRIPTION Phase II Retaining Wall along US 20 LOGGED BY J.K
SECTION 14-00446-00CH LOCATION US 20 & Randall Road Interchange, Elgin, IL
COUNTY Kane County DRILLING METHOD Humboldt DCP + Hand Auger HAMMER TYPE Humboldt DCP

Table with columns: STRUCT. NO., BORING NO., Station, Offset, Ground Surface Elev., D E L T H, B L W, U C S Qu, M O I S T, Surface Water Elev., Stream Bed Elev., Groundwater Elev., First Encounter, Upon Completion, After. Includes soil log data for approximately 8 inches of TOPSOIL and Brown SAND with gravel.

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206)
BBS, from 137 (Rev. 8-99)

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HDR logo and address: 8550 W. BRYN MAWR AVE., CHICAGO, IL 60631

Table with columns: USER NAME, DESIGNED, CHECKED, DRAWN, DATE, REVISED. Values include MKROK, BJC, MJK, BJC, 1/3/19.

KANE COUNTY DEPARTMENT OF TRANSPORTATION

SOIL BORING LOGS STRUCTURE NO. 045-2108 SHEET 17 OF 18 SHEETS

Table with columns: F.A.P. RTE., SECTION, COUNTY, TOTAL SHEETS, SHEET NO., CONTRACT NO. Values include 336/345, 14-00446-00-CH, KANE, 354, 153, 61F57.



SOIL BORING LOG

Date 5/29/18

ROUTE US Route 20 DESCRIPTION Phase II Retaining Wall along US 20 LOGGED BY T.R.

SECTION 14-00446-00CH LOCATION US 20 & Randall Road Interchange, Elgin, IL

COUNTY Kane County DRILLING METHOD Hand Auger HAMMER TYPE Humboldt DCP

Table with columns: STRUCT. NO., BORING NO., Station, Offset, Ground Surface Elev., D, E, P, T, H, S, B, L, O, W, U, C, S, M, O, I, S, T, Qu, Surface Water Elev., Stream Bed Elev., Groundwater Elev., First Encounter, Upon Completion, After. Includes data for boring 045-2108 and 045-2108.

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer) The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206) BBS, from 137 (Rev. 8-99)



SOIL BORING LOG

Date 5/29/18

ROUTE US Route 20 DESCRIPTION Phase II Retaining Wall along US 20 LOGGED BY T.R.

SECTION 14-00446-00CH LOCATION US 20 & Randall Road Interchange, Elgin, IL

COUNTY Kane County DRILLING METHOD Hand Auger HAMMER TYPE Humboldt DCP

Table with columns: STRUCT. NO., BORING NO., Station, Offset, Ground Surface Elev., D, E, P, T, H, S, B, L, O, W, U, C, S, M, O, I, S, T, Qu, Surface Water Elev., Stream Bed Elev., Groundwater Elev., First Encounter, Upon Completion, After. Includes data for boring 045-2108 and 045-2108.

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer) The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206) BBS, from 137 (Rev. 8-99)



SOIL BORING LOG

Date 5/29/18

ROUTE US Route 20 DESCRIPTION Phase II Retaining Wall along US 20 LOGGED BY T.R.

SECTION 14-00446-00CH LOCATION US 20 & Randall Road Interchange, Elgin, IL

COUNTY Kane County DRILLING METHOD Hand Auger HAMMER TYPE Humboldt DCP

Table with columns: STRUCT. NO., BORING NO., Station, Offset, Ground Surface Elev., D, E, P, T, H, S, B, L, O, W, U, C, S, M, O, I, S, T, Qu, Surface Water Elev., Stream Bed Elev., Groundwater Elev., First Encounter, Upon Completion, After. Includes data for boring 045-2108 and 045-2108.

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer) The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206) BBS, from 137 (Rev. 8-99)

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USER NAME - MKROK DESIGNED - BJC REVISIONS - CHECKED - MJK REVISIONS - DRAWN - BJC REVISIONS - DATE - 1/3/19

KANE COUNTY DEPARTMENT OF TRANSPORTATION

SOIL BORING LOGS STRUCTURE NO. 045-2108

Table with columns: F.A.P. RTE., SECTION, COUNTY, TOTAL SHEETS, SHEET NO., CONTRACT NO. 61F57



SOIL BORING LOG

Page 1 of 1

Date 5/22/18

ROUTE US Route 20 DESCRIPTION Phase II Weld Road LOGGED BY J.W.

SECTION 14-00446-00CH LOCATION US 20 & Randall Road Interchange, Elgin, IL

COUNTY Kane DRILLING METHOD Hollow Stem Auger HAMMER TYPE Automatic

STRUCT. NO.	DEPTH	DESCRIPTION	U	M	Surface Water Elev.
Station	ft	(ft) (6")	(tsf)	(%)	N/A ft
	821.73	Black and dark brown SILTY CLAY Possible topsoil			N/A ft
		Stiff, Black, brown and gray SILTY CLAY [A-6] Possible fill	2.5 P	12	N/A ft
	818.90	Stiff, Black and dark gray SILTY CLAY [A-6]	3.3 P	25	
	816.40	Stiff, Greenish-brown SILTY CLAY [A-6]	1.0- 2.5 P	13	
	813.90	Stiff, Brown LOAM [A-4]	2.3 B	11	
	812.40	End of boring at approximately 10 feet below existing grade. No free groundwater encountered during drilling operations.			
	-20				

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206) BBS, from 137 (Rev. 8-99)



SOIL BORING LOG

Page 1 of 1

Date 5/22/18

ROUTE US Route 20 DESCRIPTION Phase II Weld Road LOGGED BY J.W.

SECTION 14-00446-00CH LOCATION US 20 & Randall Road Interchange, Elgin, IL

COUNTY Kane DRILLING METHOD Hollow Stem Auger HAMMER TYPE Automatic

STRUCT. NO.	DEPTH	DESCRIPTION	U	M	Surface Water Elev.
Station	ft	(ft) (6")	(tsf)	(%)	N/A ft
	813.63	Black and dark brown SILTY CLAY [A-6] Possible topsoil			N/A ft
		Medium stiff, Black and brown SILTY CLAY [A-6] Possible fill	1.2 B	28	N/A ft
	810.80	Soft to medium stiff, Brown, mottled, SILTY CLAY LOAM [A-6]	1.5 B	34	
	806.30	Gray, mottled SILTY CLAY to SILTY CLAY LOAM [A-4 to A-6]	0.3 B	27	
	803.80	Medium dense, Grayish-brown SANDY LOAM to LOAM [A-4]	0.8 P	11	
	801.30	Medium dense, Grayish-brown SAND with gravel [A-1-b]		10	
	799.80	End of boring at approximately 15 feet below existing grade. No free groundwater encountered during drilling operations.			
	-20				

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206) BBS, from 137 (Rev. 8-99)

MODEL: Default
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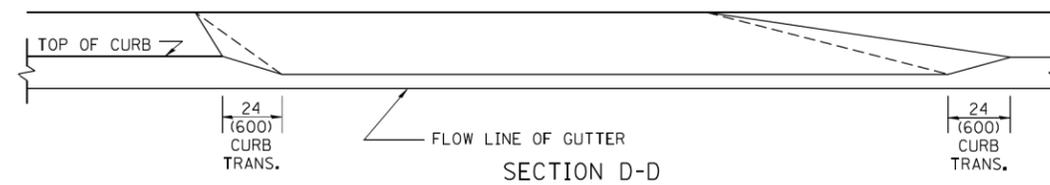
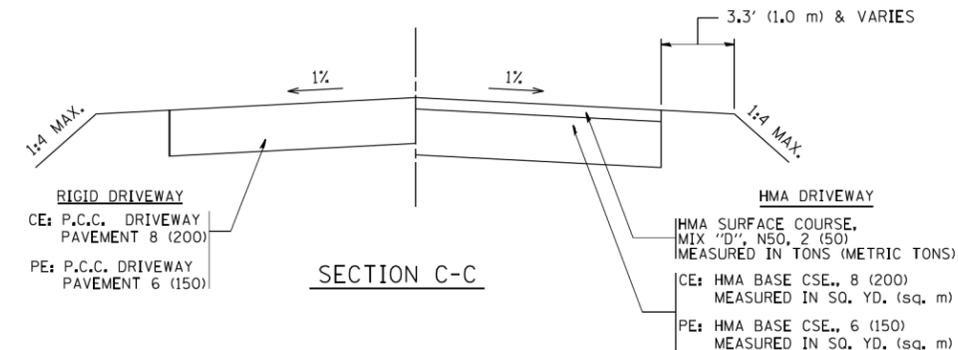
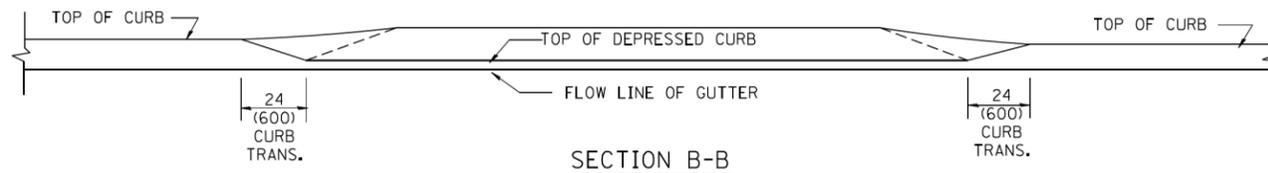
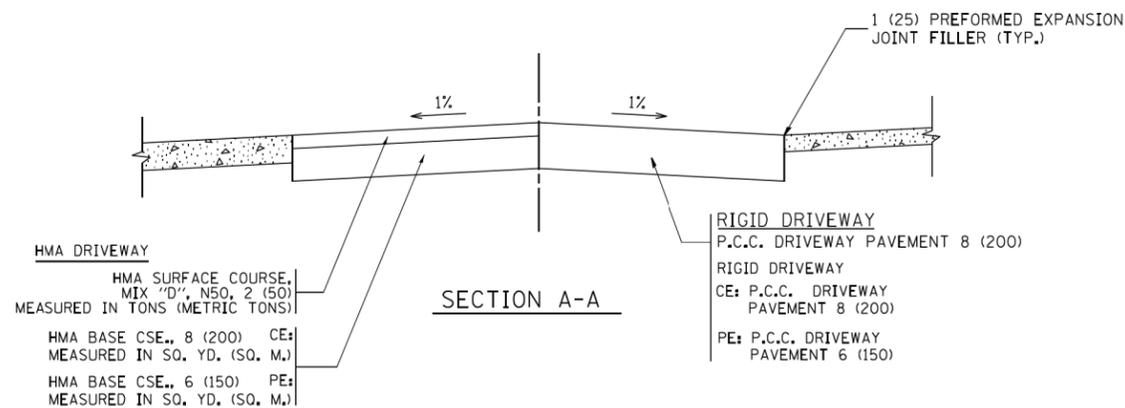
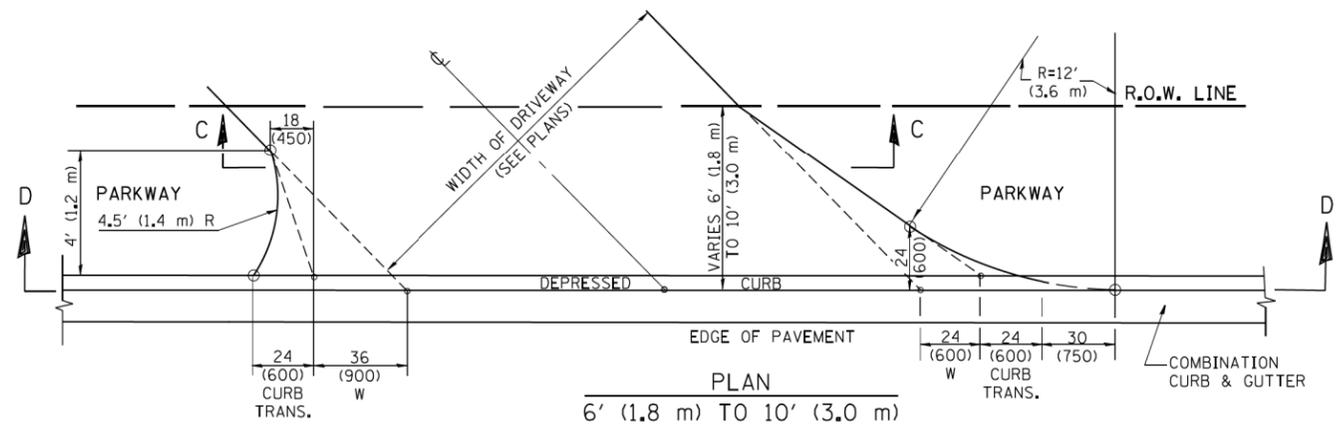
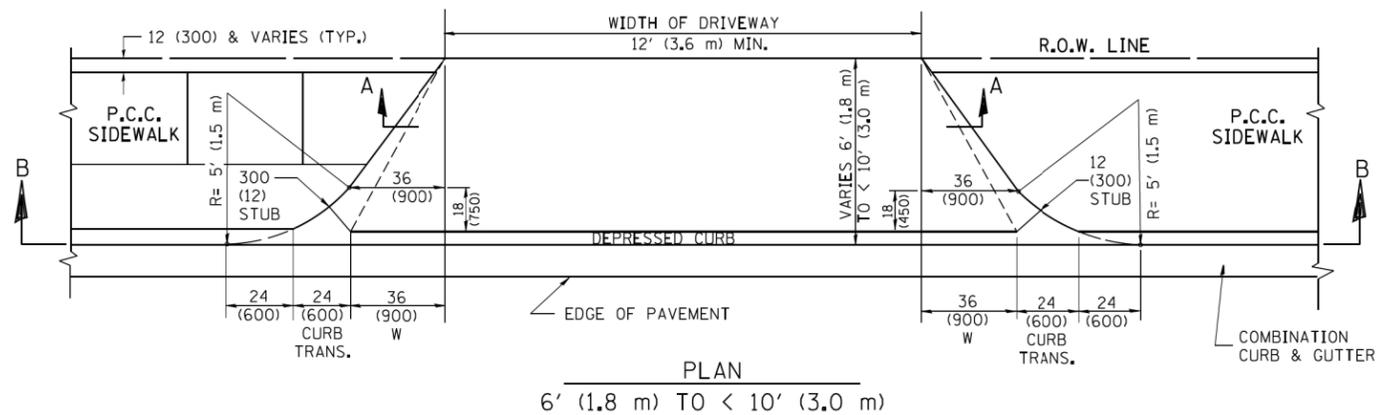
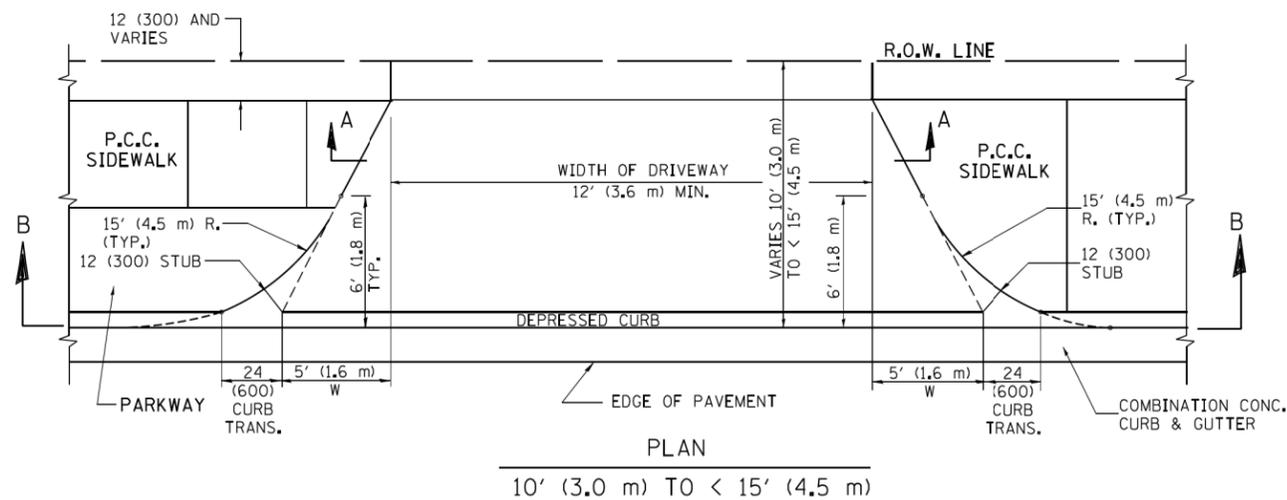
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PLOT SCALE = 0:2.0000 '"/in.	CHECKED - AKS	REVISED -
PLOT DATE = 01/03/2019	DATE - 01-03-19	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOIL BORING LOGS

SCALE: N/A SHEET 3 OF 4 SHEETS STA. N/A TO STA. N/A

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	157
CONTRACT NO. 61F57				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

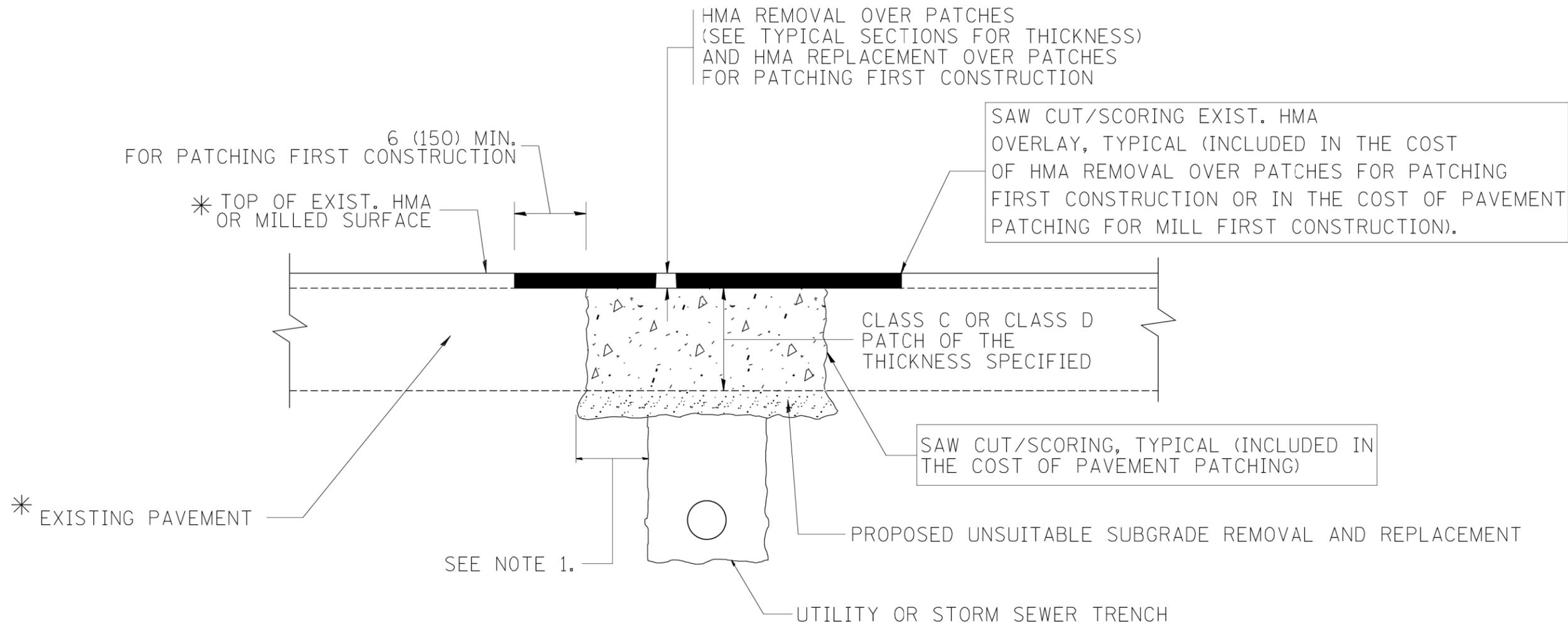
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

FILE NAME =	USER NAME = lcyss	DESIGNED - R. SHAH	REVISED - M. GOMEZ 04-05-01
ca:\p\work\p\dot\leyss\d0108315\bd02.dgn		DRAWN -	REVISED - P. LOFLEUR 04-15-03
	PLOT SCALE = 50.0000' / 1"	CHECKED -	REVISED - R. BORO 01-01-07
	PLOT DATE = 10/28/2011	DATE - 11-06-95	REVISED - R. BORO 09-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DRIVEWAY DETAILS	
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	159
BD400-02 (BD-02)		CONTRACT NO. 61F57		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = c:\projects\diststd22x34\bd22.dgn	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT			F.A.P. RTE. 336/345	SECTION 14-00446-00-CH	COUNTY KANE	TOTAL SHEETS 354	SHEET NO. 161
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - R. BORO 09-04-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	BD400-04 (BD-22)		CONTRACT NO. 61F57	
	PLOT DATE = 10/27/2008	DATE - 10-25-94	REVISED - K. ENG 10-27-08		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

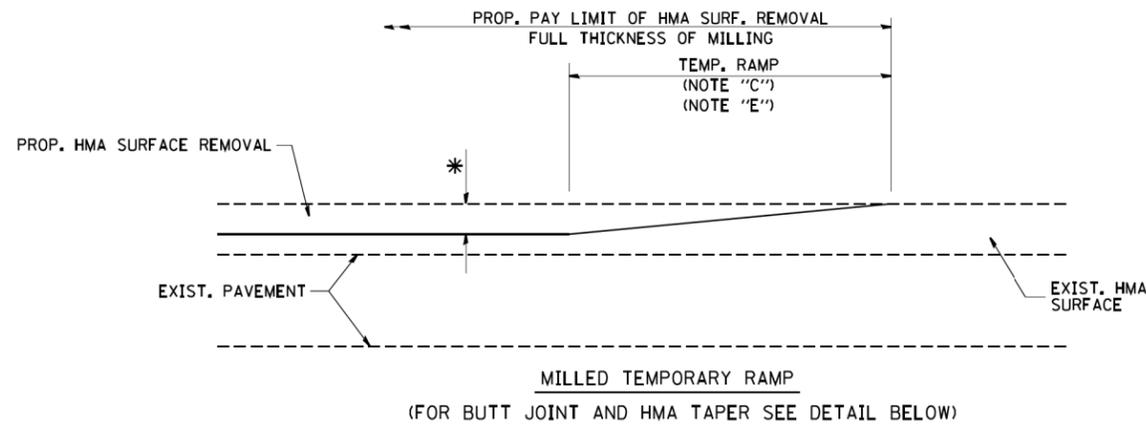
⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

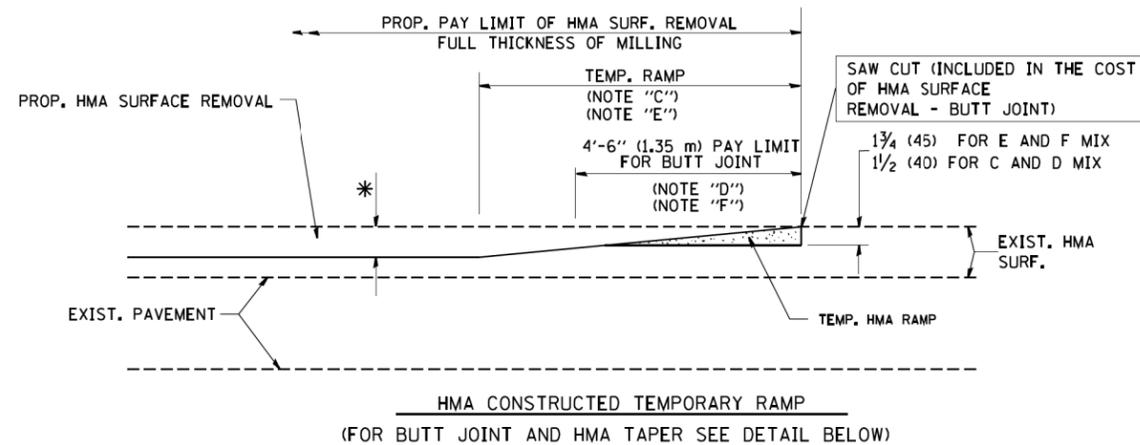
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = drivkosgn	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\p\work\p\dot\drivkosgn\d0108315\bc24.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	336/345	14-00446-00-CH	KANE	354	162
		PLOT SCALE = 50.000' / IN.	REVISED - M. GOMEZ 01-22-01						BD600-06 (BD-24)		CONTRACT NO. 61F57		
		PLOT DATE = 12/15/2009	REVISED - R. BORO 12-15-09						FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

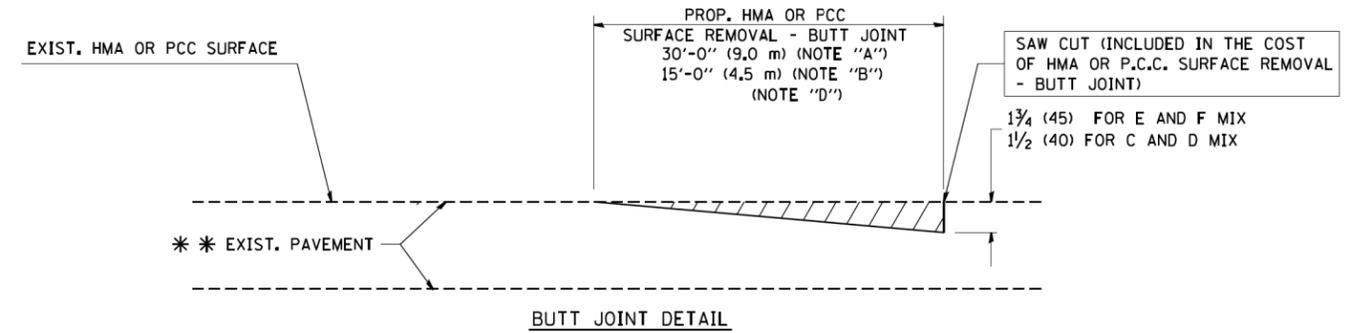


OPTION 1

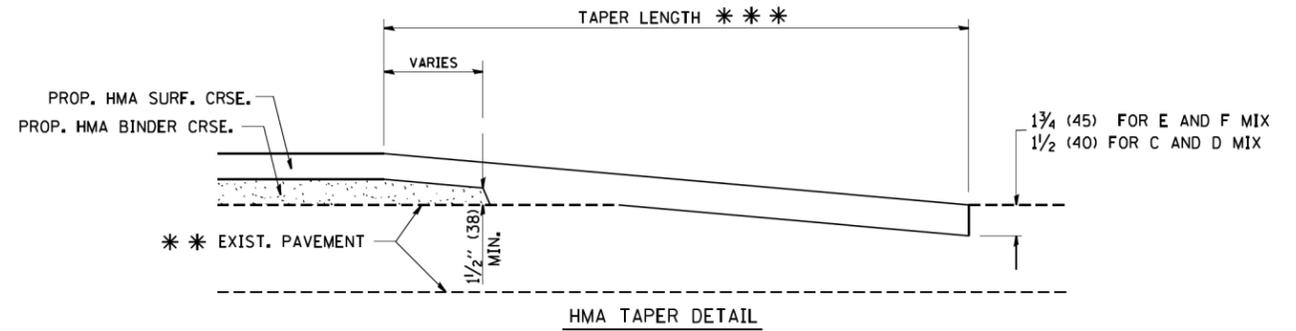


OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

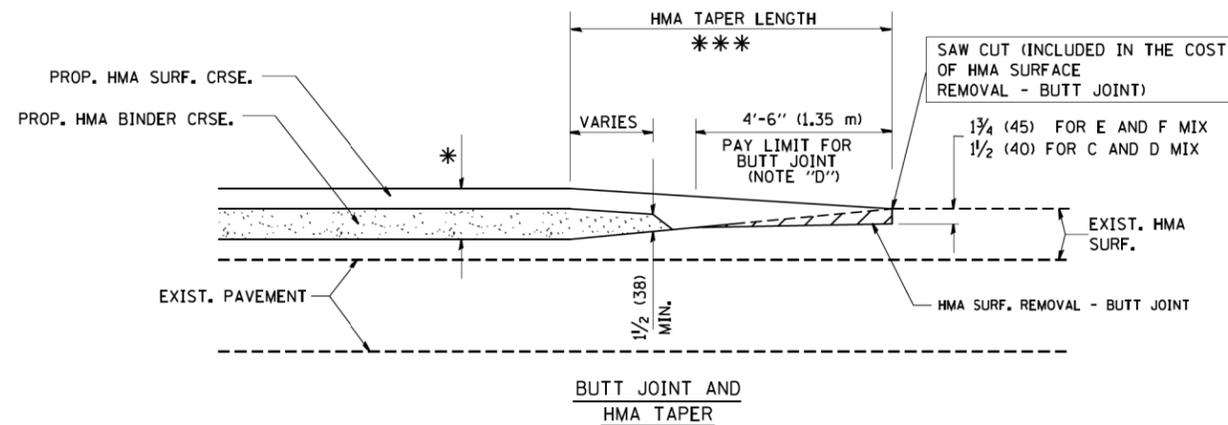
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



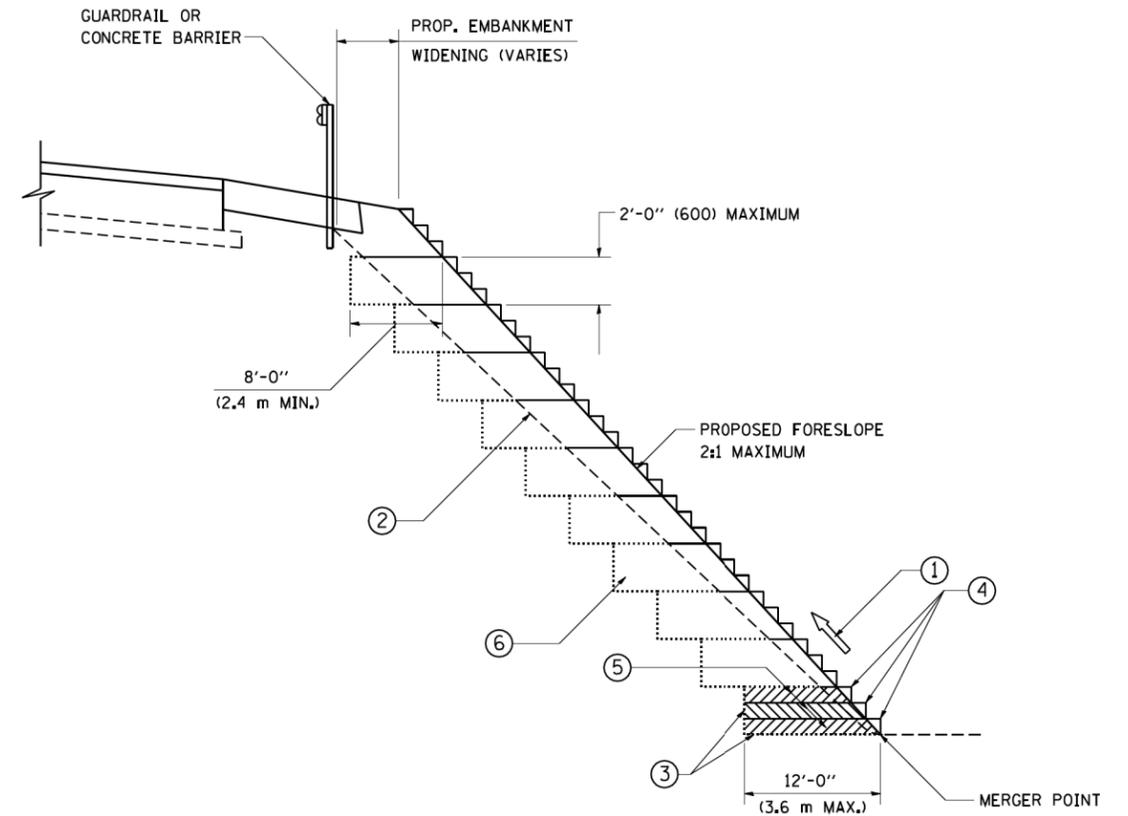
TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

FILE NAME = W:\diststd\22x34\bd32.dgn	USER NAME = gegl1enobt	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUTT JOINT AND HMA TAPER DETAILS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	163
BD400-05 BD32		CONTRACT NO. 61F57		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**TYPICAL BENCHING DETAIL
FOR EMBANKMENT**

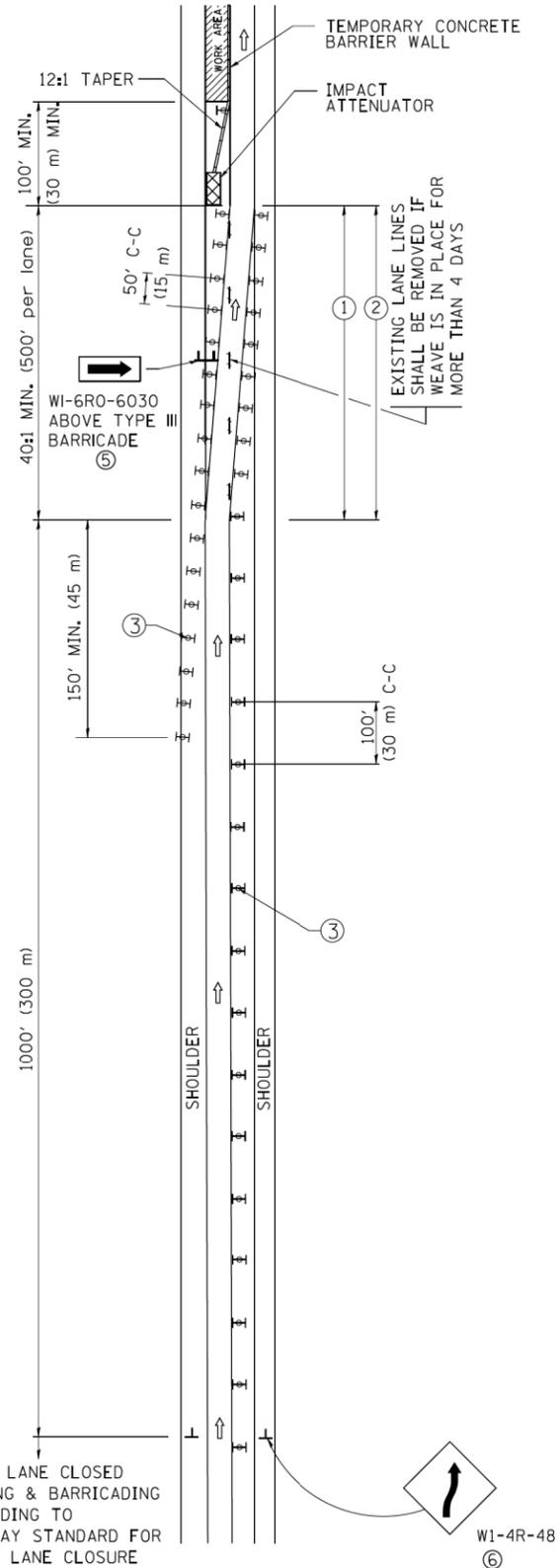
NOTES:

- ① CONSTRUCT SUCCEEDING BENCH CUTS AND EMBANKMENT PLACEMENT AND COMPACTION FROM BOTTOM TO TOP IN STAIRSTEP FASHION.
- ② EXISTING FORESLOPE PREPARED IN ACCORDANCE WITH ARTICLE 205.03 OF THE STANDARD SPECIFICATIONS.
- ③ BENCH CUT EXISTING SLOPE TYPICAL FOR EACH STEP.
- ④ TRIM TO FINAL SLOPE.
- ⑤ EQUAL 8-INCH (200) LIFTS OF EMBANKMENT COMPACTED IN ACCORDANCE WITH ARTICLE 205.05 OF THE STANDARD SPECIFICATIONS.
- ⑥ EXCAVATION OF BENCH CUTS WITHIN EXISTING EMBANKMENT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER CUBIC METER OR CUBIC YARD FOR "EARTH EXCAVATION". THIS PRICE WILL INCLUDE ALL LABOR AND MATERIAL, NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- ⑦ SLOPES SHALL BE BENCHED ACCORDING TO THIS DETAIL WHEN THE SLOPE IS STEEPER THAN 4:1 AND THE HEIGHT IS GREATER THAN 5' (1.5 m).

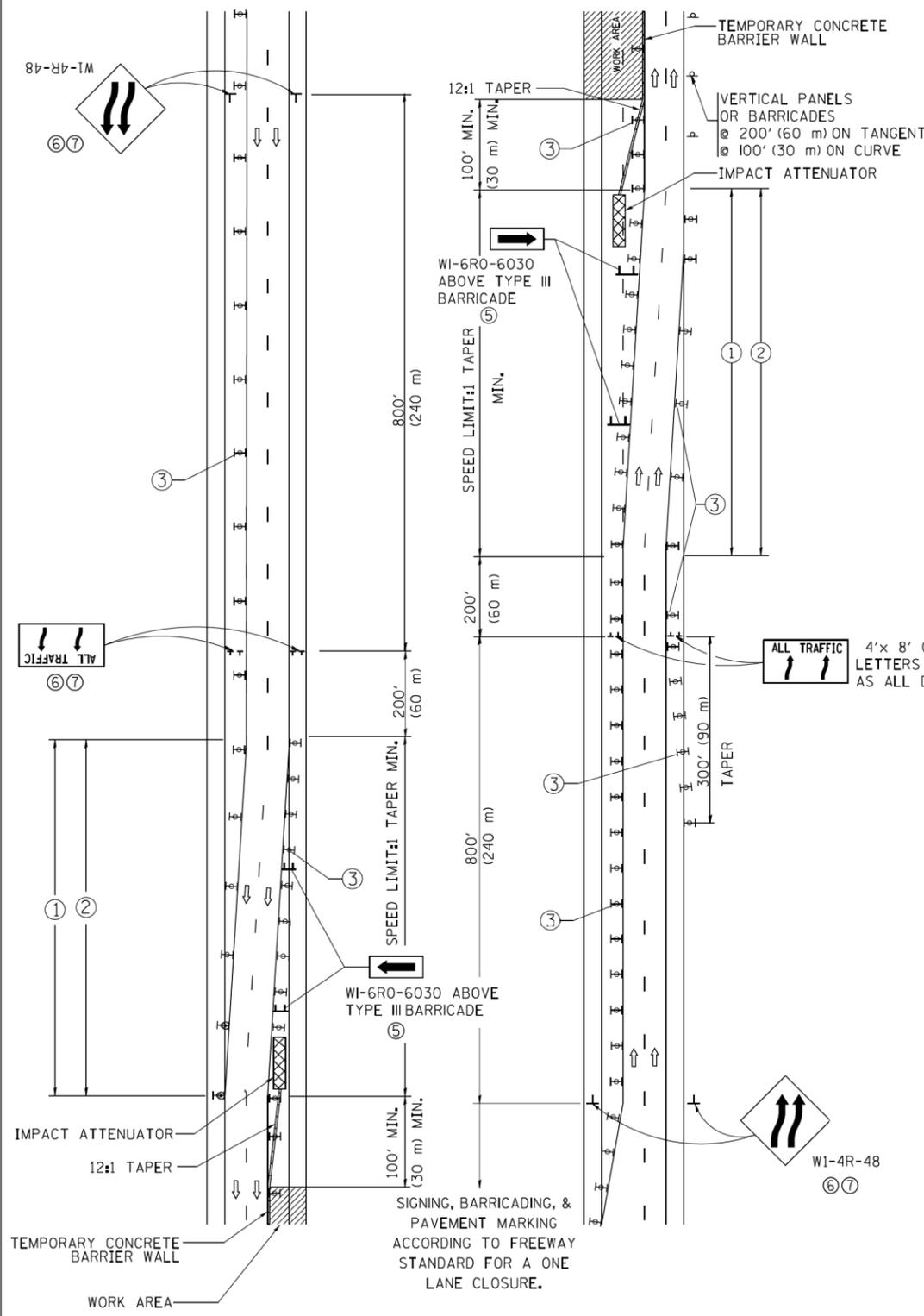
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\bd51.dgn	USER NAME = gegl1enobt	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BENCHING DETAIL FOR EMBANKMENT WIDENING		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50.0000' / IN.	DRAWN - CADD	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	14-00446-00-CH	KANE	354	165
	PLOT DATE = 1/4/2008	CHECKED - S.E.B.	REVISED -					BD-51	CONTRACT NO. 61F57		
		DATE - 06-16-04	REVISED -					FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

SINGLE LANE WEAVE



MULTI-LANE WEAVE



GENERAL NOTES

- ① EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED. PAVEMENT MARKING REMOVAL SHALL NOT BE REQUIRED FOR SINGLE LANE WEAVES UNDER 4 DAYS IN DURATION.
- ② CONTINUOUS REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 300' (90 m) ALONG SIDE THE WORK AREA WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS. THE LEFT EDGE LINE SHALL BE YELLOW AND THE RIGHT EDGE LINE SHALL BE WHITE. FOR MULTI-LANE WEAVES LANE LINES SHALL BE 5 INCH, 10'-30' (3 m-9 m) SKIP DASH, WHITE.
- ③ PLASTIC DRUMS WITH STEADY BURN LIGHTS AT 50' (15 m) C-C SPACING IN TAPERS AND 100' (30 m) C-C SPACING IN TANGENTS.
- ④ ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- ⑤ TYPE III BARRICADES MAY BE OMITTED FOR SINGLE-LANE WEAVES UNDER 24-HOURS IN DURATION. W1-6 SIGNS WILL STILL BE REQUIRED. IF THE WIDTH OF OFFSET IS LESS THAN 6' THEN THE TYPE III BARRICADE WITH ATTACHED ARROW SIGN PANEL CAN BE ELIMINATED IN THE TAPER AREAS.
- ⑥ WHEN THE LENGTH OF THE SHIFTED SEGMENT (DISTANCE BETWEEN WEAVE POINTS) IS LESS THAN 1500', DOUBLE REVERSE CURVE SIGNS (W24-1) SHOULD BE USED INSTEAD OF THE REVERSE CURVE (W1-4) SIGNS. ARROWS ON THE 4'X8' "ALL TRAFFIC" SIGNS SHALL BE THE SAME SHAPE.
- ⑦ THE NUMBER OF ARROWS ON THESE SIGNS SHALL MATCH THE NUMBER OF LANES OPEN TO TRAFFIC.

SYMBOLS

- DIRECTION OF TRAFFIC
- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- TYPE II BARRICADE OR DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- TEMPORARY CONCRETE BARRIER WALL
- IMPACT ATTENUATOR
- W1-4R-48
- W24-1-48

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

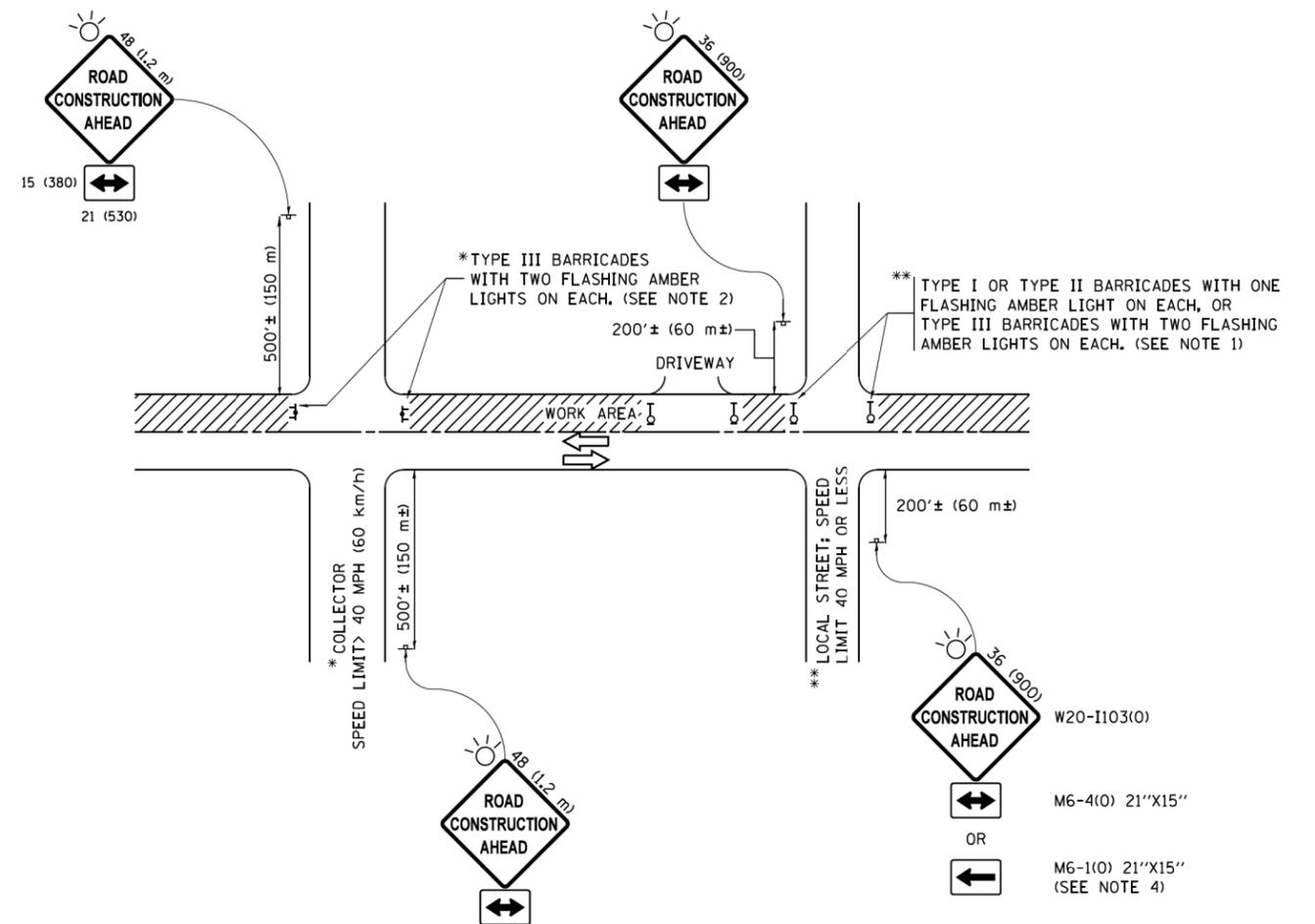
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	PLOT SCALE = 50.000' / in.	CHECKED -	REVISED - SPB 12-09
	PLOT DATE = 7/1/2013	DATE - 02-87	REVISED - MD 06-13

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS FOR FREEWAY SINGLE & MULTI-LANE WEAVE

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	166
TC-09		CONTRACT NO. 61F57		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

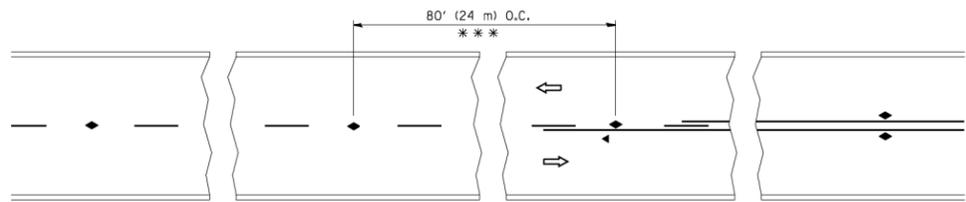
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	PLOT DATE = 9/15/2016	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

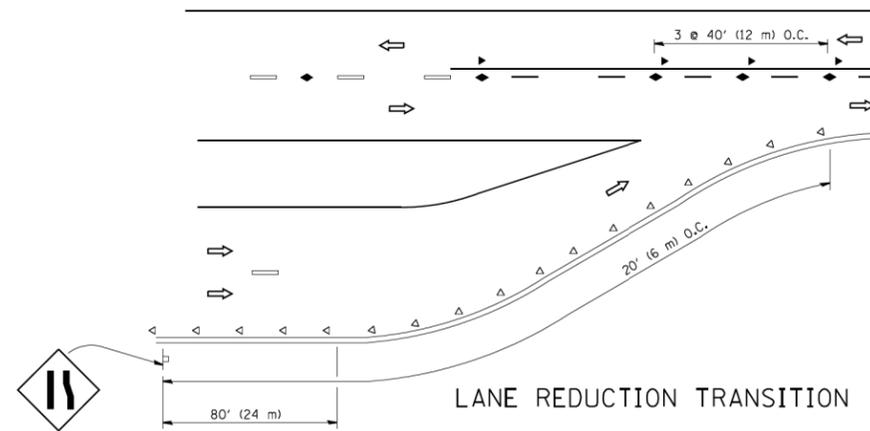
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-10			CONTRACT NO. 61F57	
ILLINOIS FED. AID PROJECT				

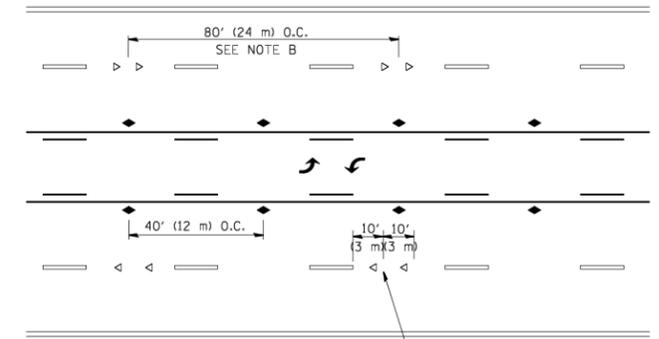


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

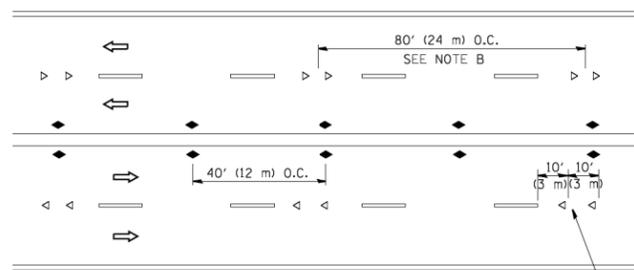
TWO-LANE/TWO-WAY



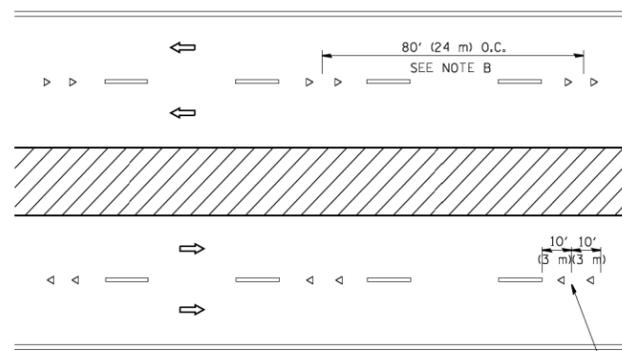
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

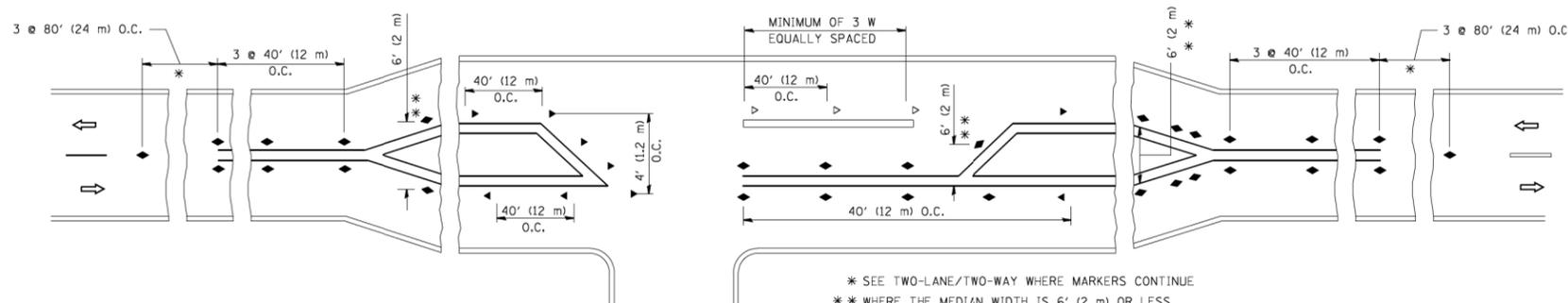
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

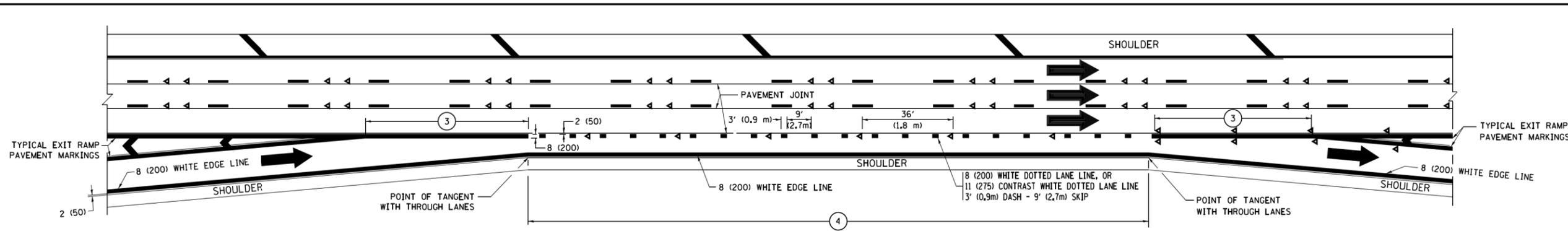
All dimensions are in inches (millimeters) unless otherwise shown.

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	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - T. RAMMACHER 01-06-00
	PLOT DATE = 3/2/2011	DATE -	REVISED - C. JUCIUS 09-09-09

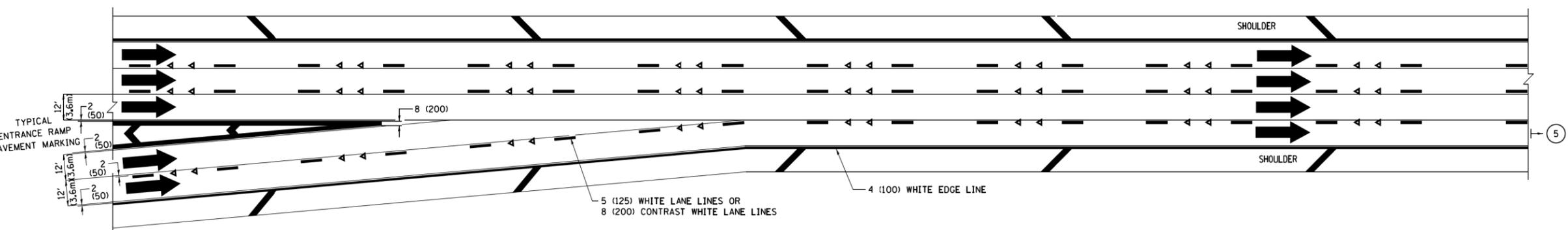
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS	
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STA.	TO STA.

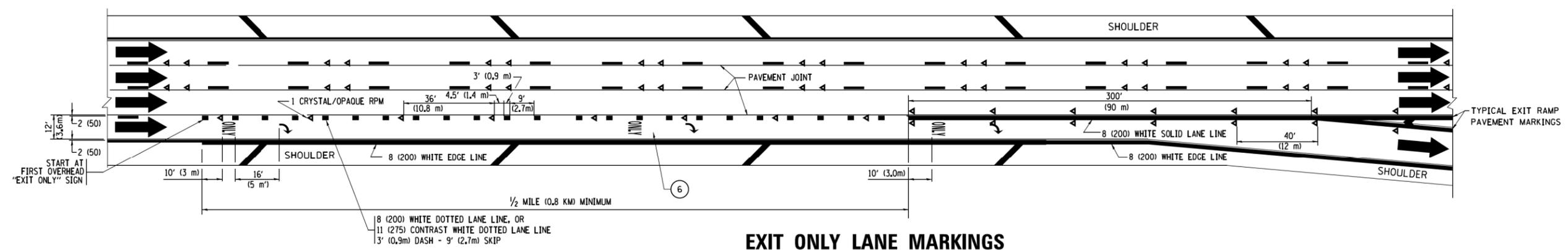
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-11		CONTRACT NO. 61F57		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



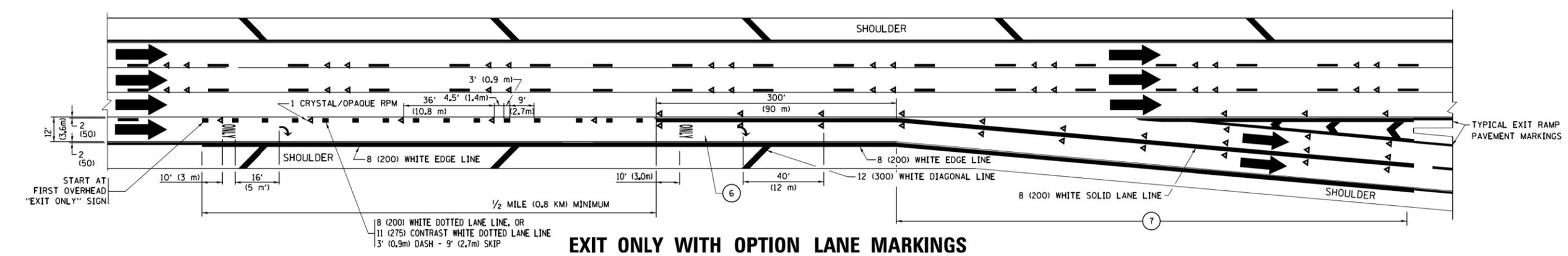
AUXILIARY LANE MARKINGS



TWO LANE ENTRANCE RAMP WITH MERGE MARKINGS



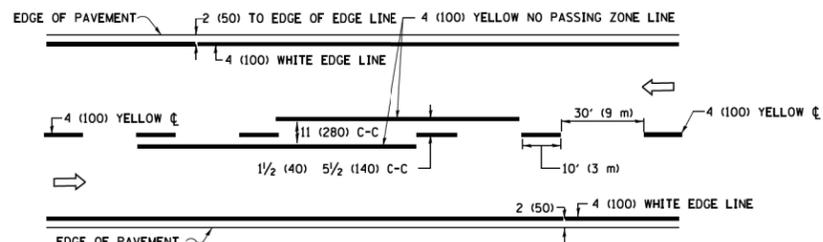
EXIT ONLY LANE MARKINGS



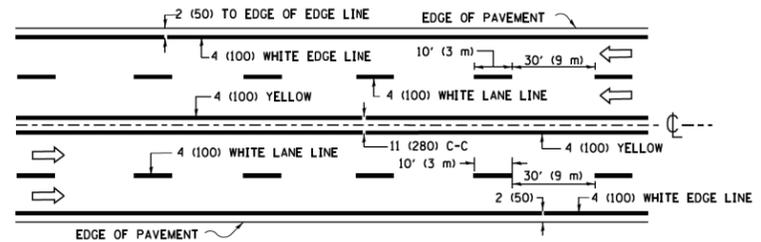
EXIT ONLY WITH OPTION LANE MARKINGS

- NOTES:**
- ③ OMIT WHEN LENGTH OF AUXILIARY LANE IS LESS THAN 500' (150 m).
 - ④ 8-INCH WIDE DOTTED LANE LINE MARKINGS SHALL BE USED WHEN THE LENGTH OF THE AUXILIARY LANE IS 2 MILES OR LESS.
 - ⑤ FOR TWO-LANE ENTRANCE RAMP, IF RIGHT LANE ENDS, USE TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS.
 - ⑥ ONLY AND ARROWS EQUALLY SPACED, 500' (150 m) MAXIMUM SPACING. FULL SIZE LETTERS AND ARROW SHALL BE USED.
 - ⑦ CONTINUE 8" SOLID LANE LINE THROUGH EXIT TO END OF PAVED GORE.

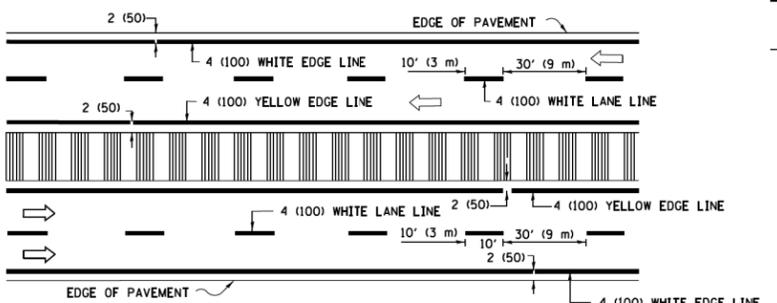
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pw\1\084EBID\INTEG\illinois.gov\FWIDOT\Documents\IDOT Offices\District 1\Projects\Dist 1\084EBID\CAD\CAD sheets\to12.dgn		DRAWN	REVISED - S.P.B. 01-07		SCALE: NONE	SHEET 2	OF 2	SHEETS	STA.	TO STA.	336/345	14-00446-00-CH	KANE	354	170
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		PLOT DATE = 9/6/2017	DATE - 01-90		REVISED - M.D. 09-17				ILLINOIS FED. AID PROJECT						



2-LANE ROADWAY

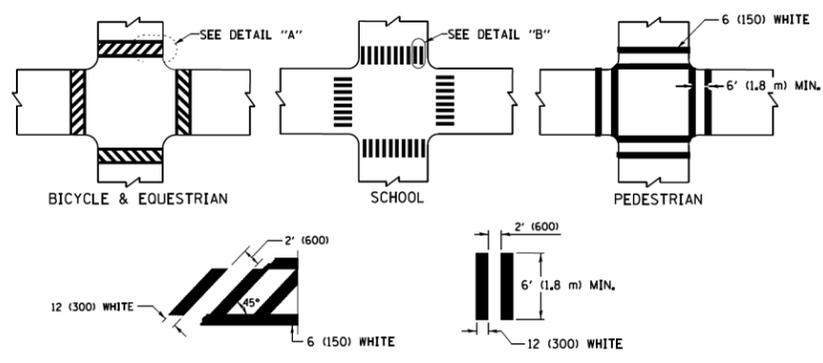


MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

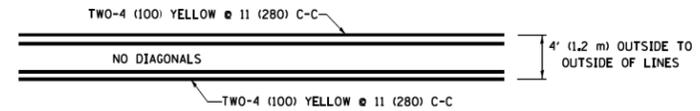


DETAIL "A"

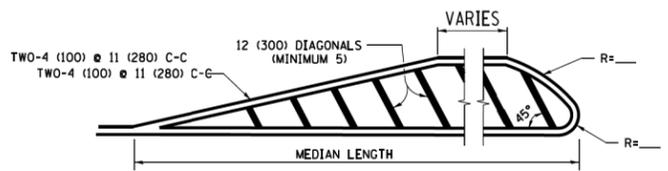
DETAIL "B"

TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



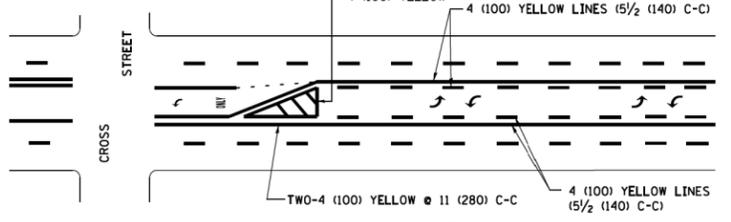
4' (1.2 m) WIDE MEDIANS ONLY



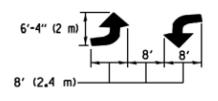
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

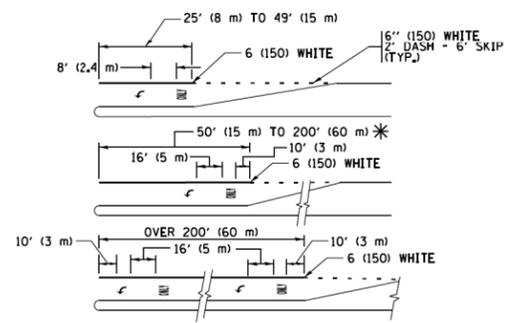


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

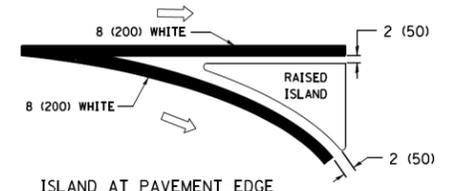
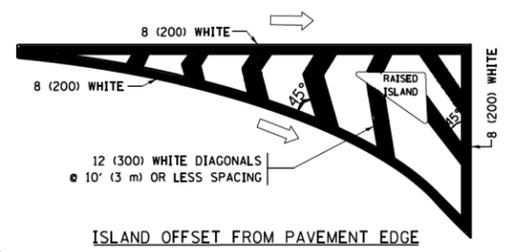


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

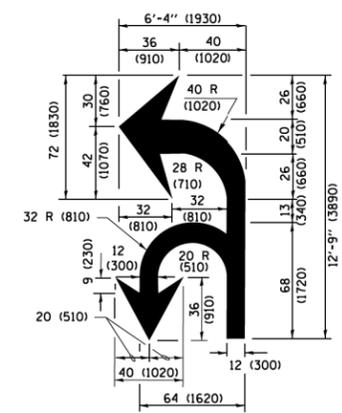
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

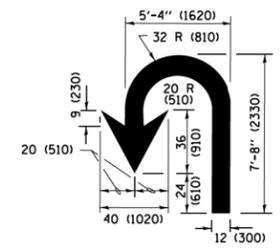
TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS; 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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	PLOT DATE = 6/23/2017	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	171
TC-13		CONTRACT NO. 61F57		
ILLINOIS FED. AID PROJECT				

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

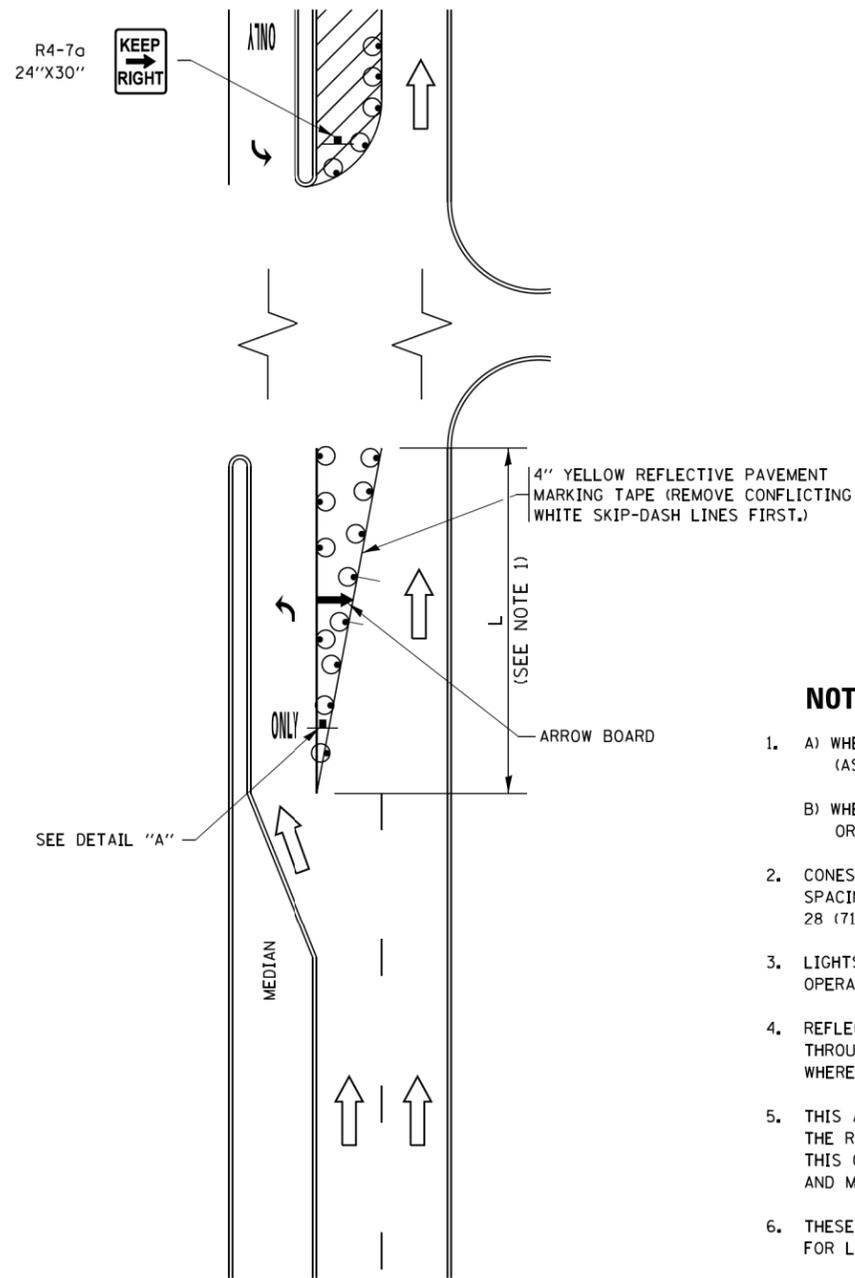
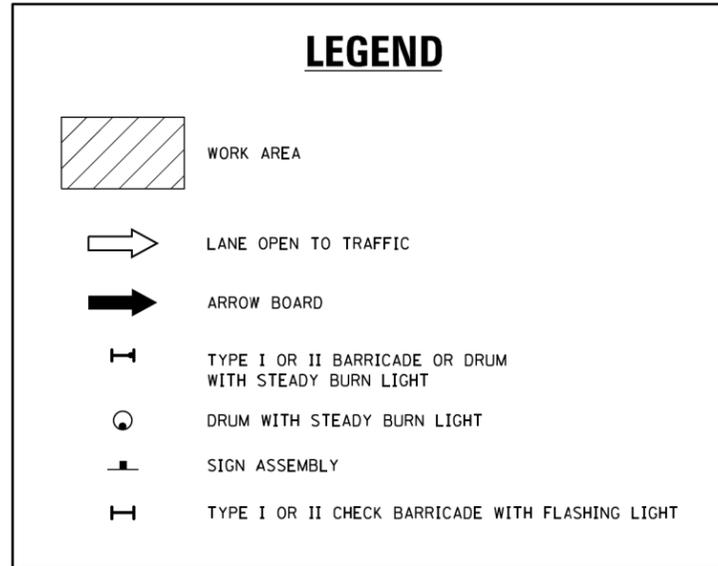


FIGURE 1



NOTES:

1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-I100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PRE REQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

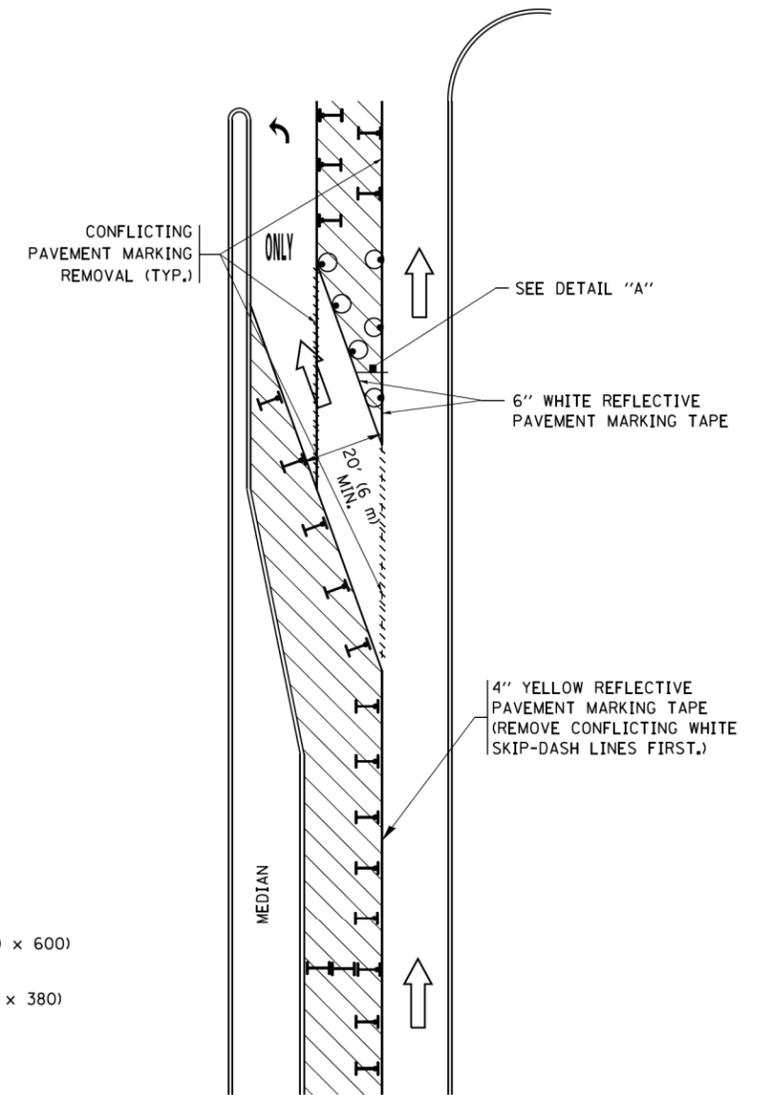
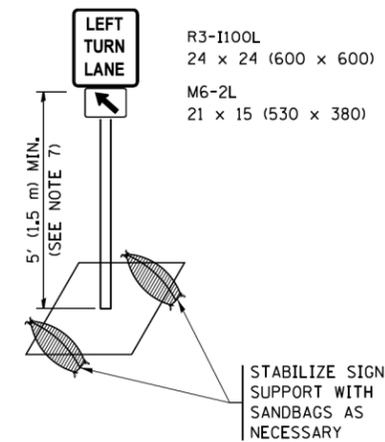


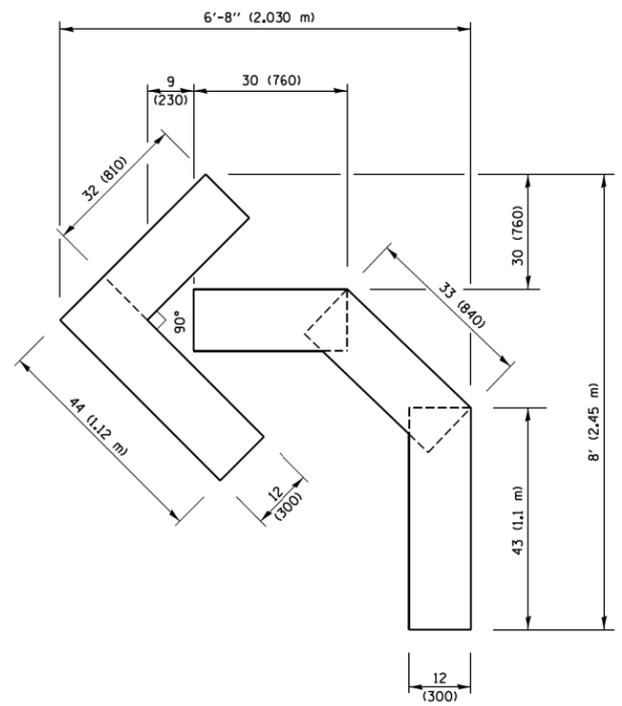
FIGURE 2



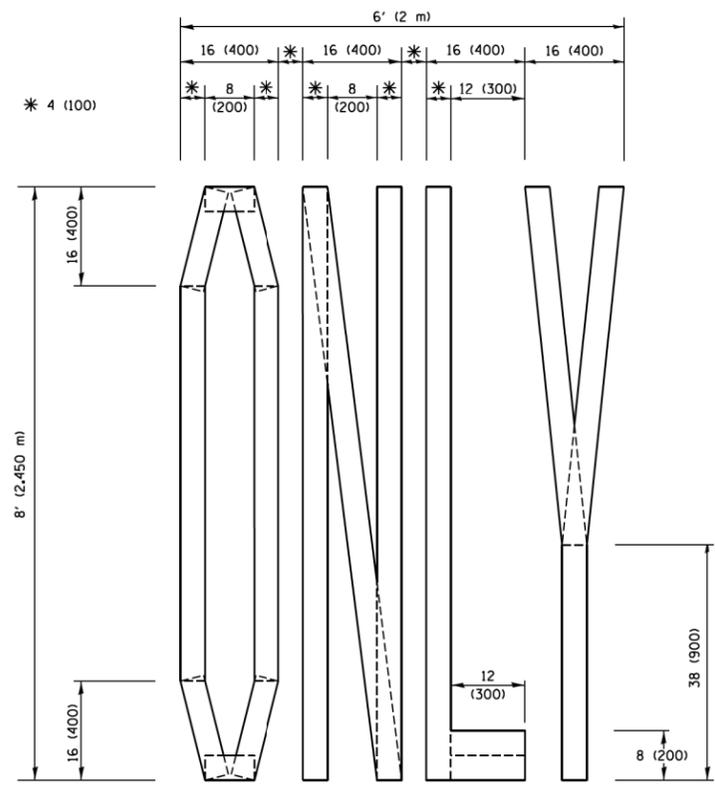
DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

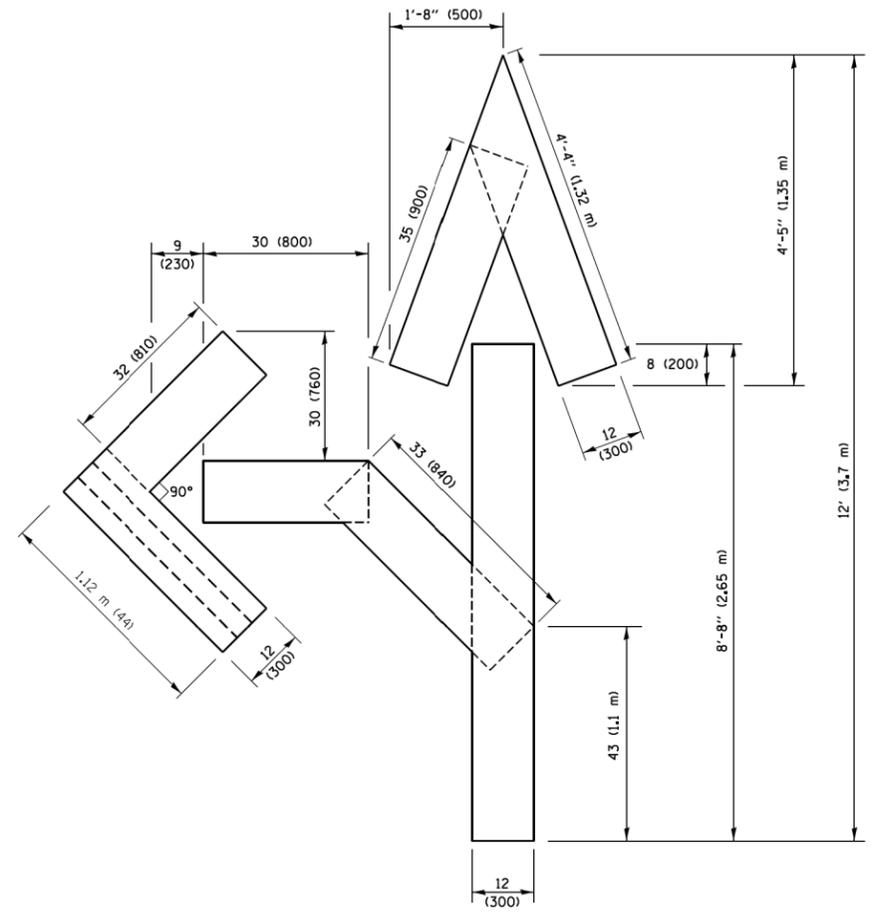
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Default	Default	REVISED - A. HOUSEH 10-07-95	REVISED - A. SCHUETZE 07-01-13					336/345	14-00446-00-CH	KANE	354	172
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	PLOT DATE = 9/15/2016	REVISED - T. RAMMACHER 01-06-00	REVISED -							ILLINOIS FED. AID PROJECT		



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.41 sq. m)

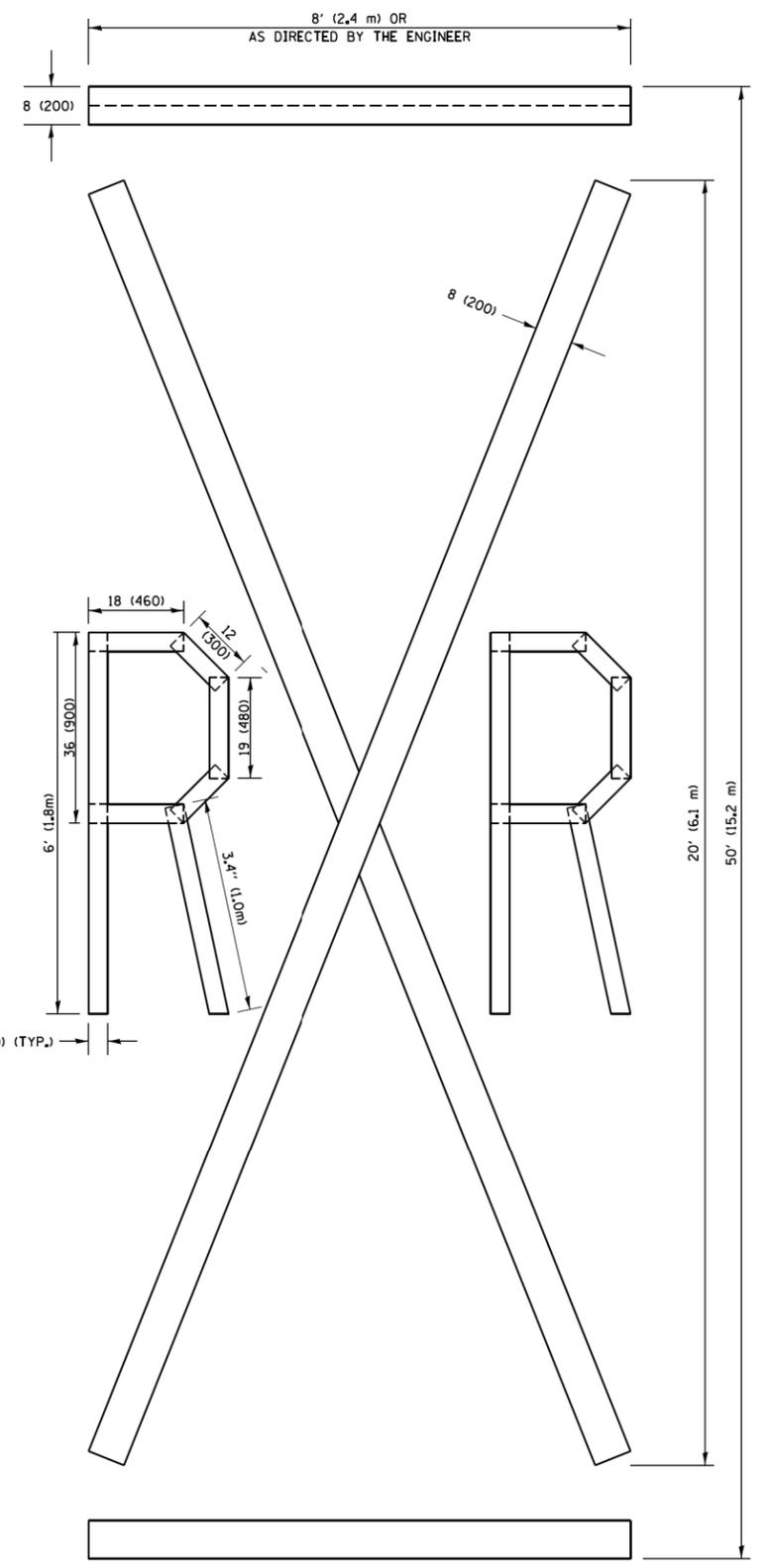


QUANTITY
 4 (100) LINE = 64.1 ft. (19.5 m)
 21.4 sq. ft. (1.99 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.1 m)
 27.5 sq. ft. (2.53 sq. m)

NOTE:
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY
 4 (100) LINE = 225.9 ft. (68.9 m)
 75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

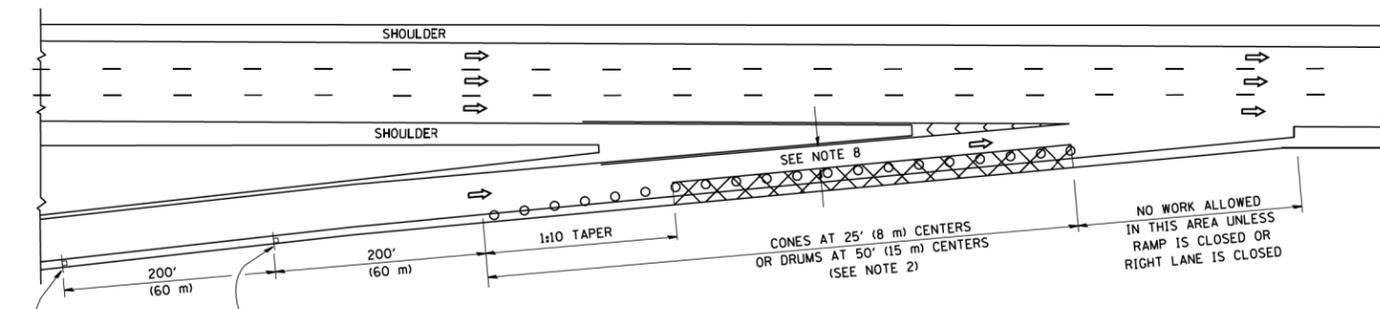
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

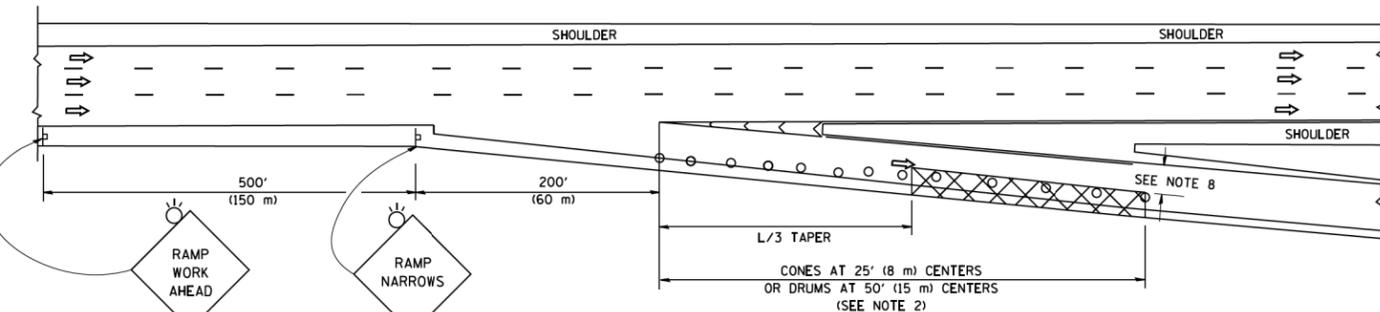
SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS			
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

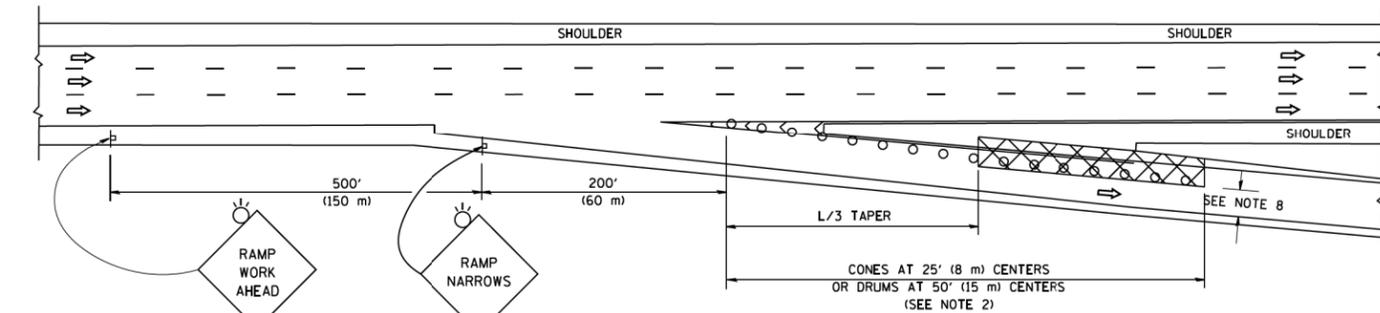
PARTIAL RAMP CLOSURE DETAILS



TYPICAL ENTRANCE RAMP



TYPICAL EXIT RAMP



TYPICAL EXIT RAMP

SYMBOLS

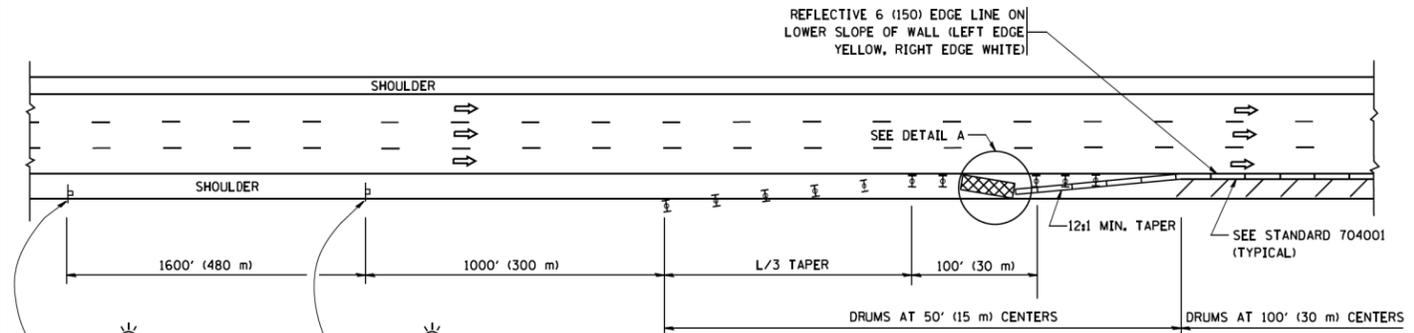
- ACTIVE WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- FLAGGER WITH CONTROL SIGN
- TYPE II BARRICADE OR DRUM
- CONE, DRUM OR BARRICADE
- IMPACT ATTENUATOR OF TYPE AND TEST LEVEL SPECIFIED

GENERAL NOTES

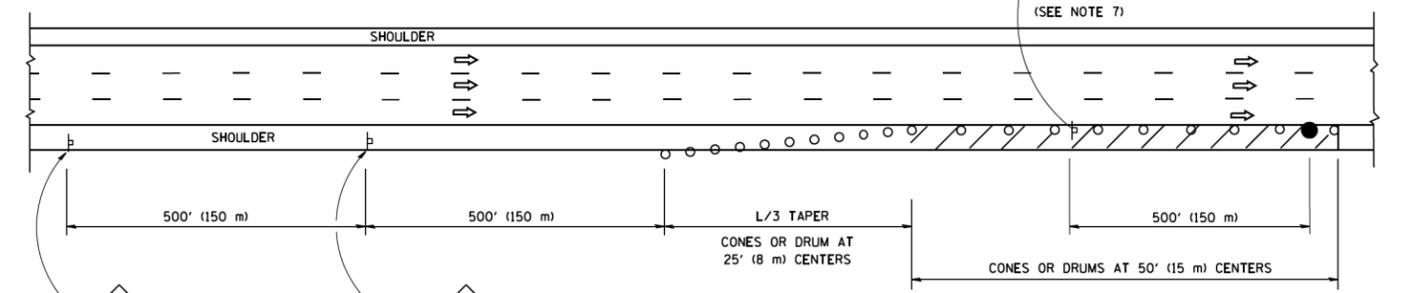
1. THE "L" DISTANCE EQUALS:

SPEED LIMIT	FORMULAS
45 mph (80 km/h) OR GREATER	METRIC ENGLISH L=0.65(W)(S) L=(W)(S)
W = WIDTH OF OFFSET IN FEET (METERS) S = NORMAL POSTED SPEED MPH (KM/H)	
2. TYPE II BARRICADES OR DRUMS ARE REQUIRED FOR ALL NIGHTTIME CLOSURES. TYPE II BARRICADES OR DRUMS WITH MONODIRECTIONAL STEADY BURN LIGHTS ARE REQUIRED FOR DELINEATING OBSTACLES, EXCAVATIONS, OR HAZARDS EXCEEDING 100 FT (30m) IN LENGTH AT NIGHT.
3. ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
4. FLASHING LIGHTS SHALL BE USED DURING THE HOURS OF DARKNESS AND SHALL BE INSTALLED ABOVE THE FIRST TWO SETS OF SIGNS.
5. THE IMPACT ATTENUATOR, TEMPORARY IS NOT REQUIRED WHEN THE TEMPORARY CONCRETE BARRIER WALL IS PROTECTED BY OR IS TIED INTO THE EXISTING GUARDRAIL. IF OFFSET IS LESS THAN 5 FEET USE NARROW USE TYPE DEVICE TO MEET NCHRP350/MASH.
6. AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL FREEWAY CLOSURES.
7. THE FLAGGER AND FLAGGER SIGN ARE REQUIRED AT THE ABOVE WORK SITES WHEN:
 - a. FOUR OR MORE WORK VEHICLES ENTER THE TRAFFIC LANES IN A ONE HOUR PERIOD.
 - b. THE WORK ACTIVITY REQUIRES FREQUENT ENCROACHMENT INTO THE LANE OPEN TO TRAFFIC.
 THE FLAGGER SHALL BE STATIONED APPROXIMATELY 100' (30 m) TO 200' (60 m) IN ADVANCE OF THE WORKERS.
8. 12' MIN. WIDTH TANGENT SECTION
16' MIN. WIDTH CURVE SECTION.

SHOULDER CLOSURE DETAILS

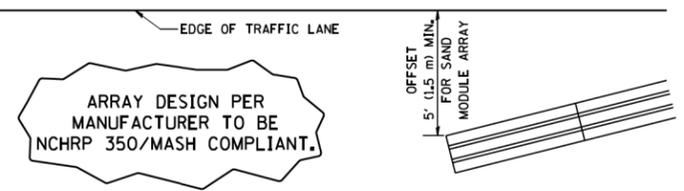


PERMANENT SHOULDER CLOSURE



DAYTIME SHOULDER CLOSURE

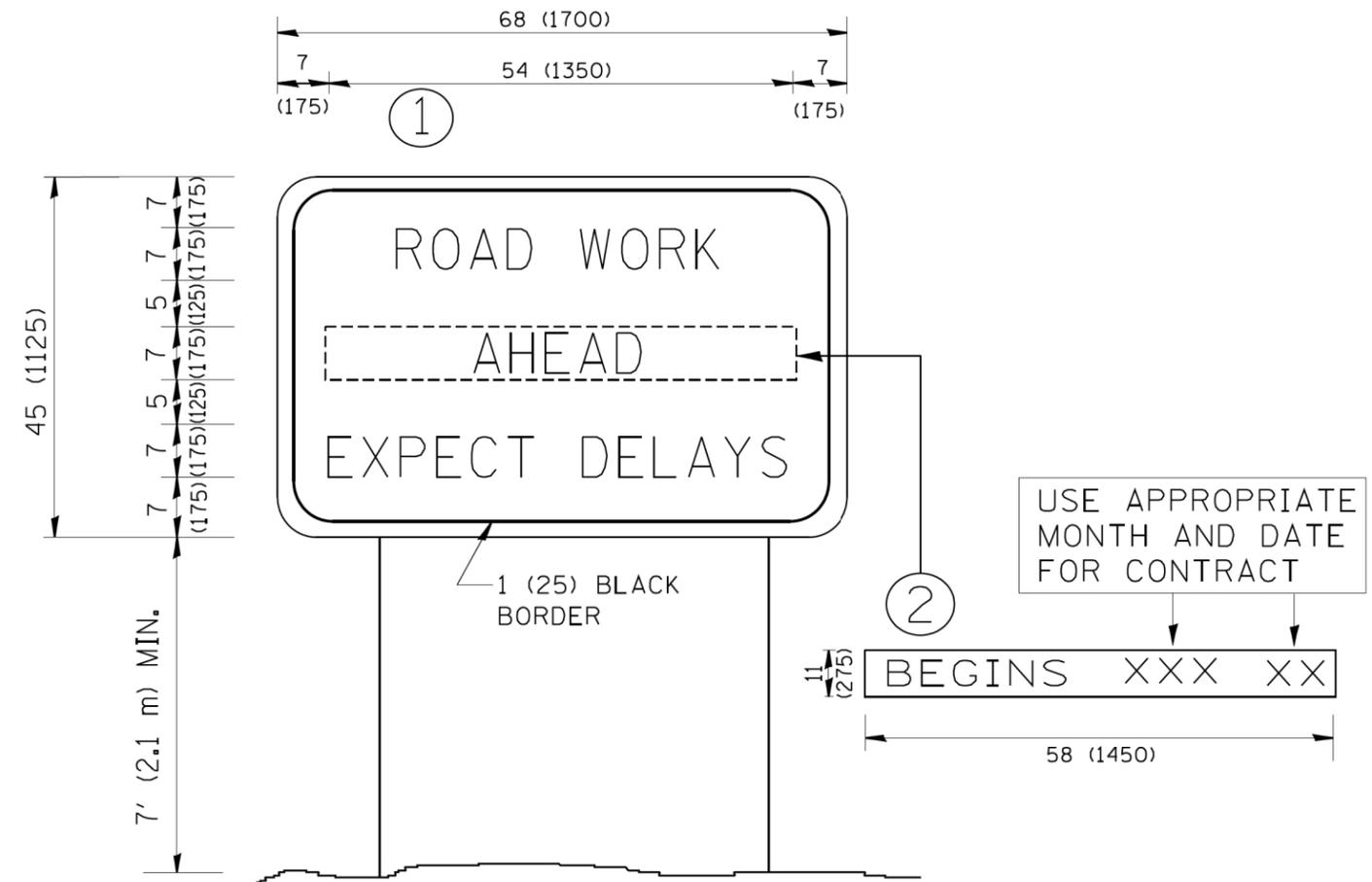
THIS DETAIL IS USED WHERE:
1. VEHICLES, EQUIPMENT, WORKERS OR THEIR ACTIVITIES ENCROACH IN AN AREA CLOSER THAN 15' (4.5 m) TO THE EDGE OF PAVEMENT FOR A PERIOD IN EXCESS OF 15 MINUTES.



DETAIL "A"
IMPACT ATTENUATOR, TEMPORARY
(SEE NOTE 5)

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = footemj	DESIGNED -	REVISED - S.P.B. 01-07	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL DETAILS FOR FREEWAY SHOULDER CLOSURES AND PARTIAL RAMP CLOSURES	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\1\084EBID\INTEG\illinois.gov\FWIDOT\Documents\IDOT Offices\District 1\Projects\Dist 1\Projects\CADData\CAD\11-96\11-96-17.dgn	DRAWN -	REVISED - S.P.B. 12-09	336/345			14-00446-00-CH	KANE	354	174	
Default	CHECKED -	REVISED - M.D. 06-13	TC-17			CONTRACT NO. 61F57				
PLOT SCALE = 50,0000' / in.	DATE - 11-96	REVISED - M.D. 01-18	SCALE: NONE			SHEET 1	OF 1 SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT	



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\tc22.dgn	USER NAME = gegl1enobt	DESIGNED -	REVISED - R. MIRS 09-15-97
		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 50.000 ' / IN.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE. 336/345	SECTION 14-00446-00-CH	COUNTY KANE	TOTAL SHEETS 354	SHEET NO. 175
TC-22		CONTRACT NO. 61F57		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE
 PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN)
 SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY
 AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE
 FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME =	USER NAME = gaglianob	DESIGNED -	REVISED - C. JUCIUS 02-15-07
ca\pwork\pwork\gaglianob\d0108315\tp6.dgn		DRAWN -	REVISED -
		CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

DRIVEWAY ENTRANCE SIGNING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	176
TC-26			CONTRACT NO. 61F57	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	179
		ILLINOIS	CONTRACT NO. 61F57	

FOR INDEX OF SHEETS, SEE SHEET NO. 2
FOR LIST OF HIGHWAY STANDARDS, SEE SHEET NO. 2

DESIGN DESIGNATION:

RANDALL ROAD	=	5110 (40) OTHER PRINCIPAL ARTERIAL (SRA) 5.96 (FD-20)
US ROUTE 20	=	3910 (40) FREEWAY 7.90 (FD-20)
US ROUTE 20 RAMPS C, D & E	=	1025 (40) FREEWAY RAMP 2.85 (FD-20)
WELD ROAD	=	180 (40) FRONTAGE ROAD 0.43 (FD-20)

ADT:		2015	2040
RANDALL ROAD	=	54,000	60,000
US ROUTE 20 (EB)	=	17,900	19,700
US ROUTE 20 & RAMPS C, D & E	=	12,625	15,000

DESIGN SPEED:

RANDALL ROAD	=	45 MPH
US ROUTE 20	=	60 MPH
WELD ROAD	=	45 MPH

POSTED SPEED:

RANDALL ROAD	=	45 MPH
US ROUTE 20	=	55 MPH
WELD ROAD	=	45 MPH

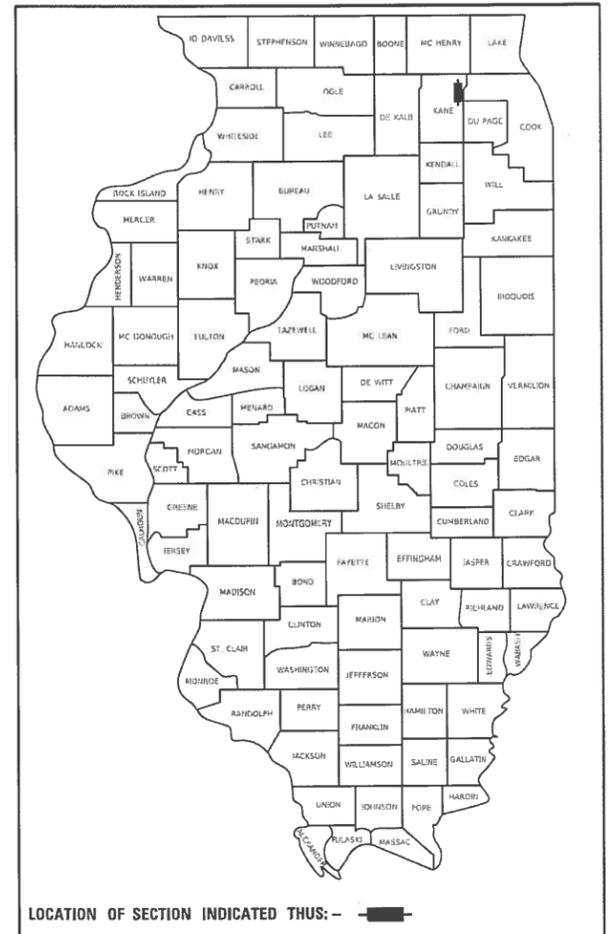
STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION

PLANS FOR PROPOSED
FEDERAL AID HIGHWAY

F.A.P. 336 (COUNTY ROUTE 34) (RANDALL ROAD)
AT F.A.P. 345 (US ROUTE 20)/WELD ROAD
INTERSECTION IMPROVEMENTS
SECTION 14-00446-00-CH
PROJECT YD9C(562)
KANE COUNTY

C-91-137-18



LOCATION OF SECTION INDICATED THUS: -

VOL. 1: GENERAL PLANS, ROADWAY, MOT, EROSION CONTROL, DRAINAGE, UTILITY, PAVEMENT MARKING SIGNING AND LANDSCAPING, TRAFFIC SIGNAL, ROADWAY LIGHTING, STRUCTURES, SOIL BORINGS, STANDARD DRAWINGS

VOL. 2: CROSS SECTIONS

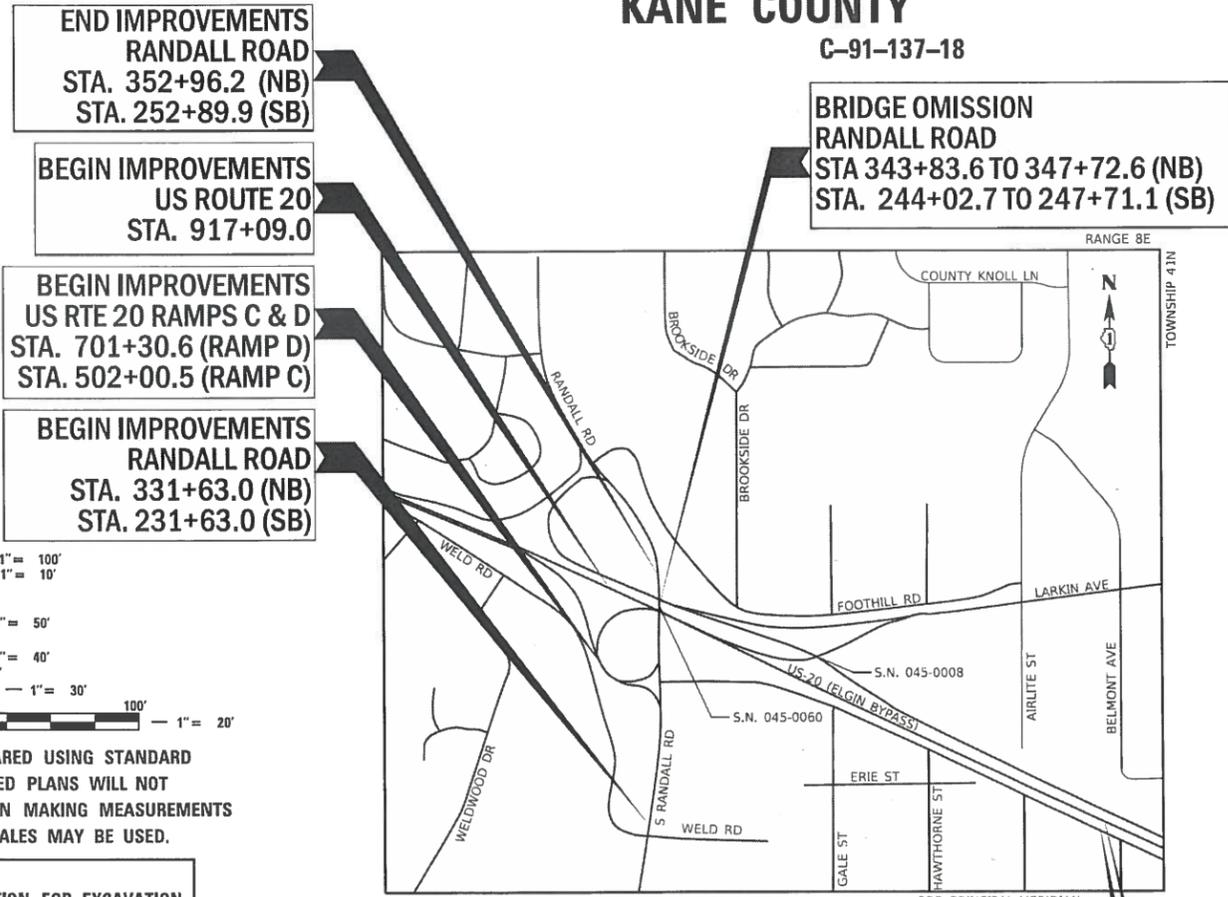
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

Approved: December 5, 2018
Director of Transportation/County Engineer
Kane County Division of Transportation

Passed: DECEMBER 20, 2018
CHRISTOPHER HOLT
District 1 Engineer of Local Roads & Streets

Releasing for Bid
Based on Limited
Review: December 20, 2018
Anthony J. Quigley
Regional Engineer

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS



PROJECT LOCATED IN
CITY OF ELGIN

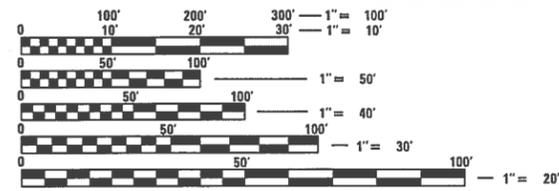
LOCATION MAP
SCALE = N.T.S.

RANDALL ROAD:	GROSS LENGTH = 2133 FT. = 0.404 MILE
	NET LENGTH = 1744 FT. = 0.330 MILE
US ROUTE 20:	GROSS LENGTH = 4023 FT. = 0.762 MILE
	NET LENGTH = 4023 FT. = 0.762 MILE
TOTAL:	GROSS LENGTH = 6156 FT. = 1.166 MILE
	NET LENGTH = 5767 FT. = 1.092 MILE

END IMPROVEMENTS
US ROUTE 20
STA. 957+32.0

END IMPROVEMENTS
WELD ROAD
STA. 73+16.3

- BY: David Wong DATE: 12-05-2018
HDR, INC.
DWGS: 1-27, 32-63, 86-88, 155-178
LICENSE EXPIRES NOV. 30, 2019
- BY: Adam Sattler DATE: 12-05-2018
HDR, INC.
DWGS: 89-94, 179-354
LICENSE EXPIRES NOV. 30, 2019
- BY: Marc J. Krak DATE: 12-05-2018
HDR, INC.
DWGS: 137-154
LICENSE EXPIRES NOV. 30, 2020
- BY: Fred Lin DATE: 12-05-2018
LIN ENGINEERING, LTD.
DWGS: 28-31, 64-85, 95-121
LICENSE EXPIRES NOV. 30, 2019
- BY: Aron Prajapati DATE: 12-05-2018
SINGH & ASSOCIATES, INC.
DWGS: 122-136
LICENSE EXPIRES NOV. 30, 2019



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

MEADE ELECTRIC CO. DISTRICT ONE ELECTRICAL MAINTENANCE CONTRACTOR LOCATES IDOT ELECTRICAL EQUIPMENT AND UNDERGROUND CABLES 773-287-7672.

HDR
HDR, Inc.
8550 W. Bryn Mawr Ave., Suite 900
Chicago, IL 60631
773-380-7900
DESIGN FIRM REGISTRATION NUMBER 184.001070

CONTRACT NO. 61F57

FEDERAL AID PROGRAM ENGINEER: CARMEN E. RAMOS P.E., SCHAUMBURG, IL

50 45 40 35 30 25 20 15 10 5 0 5 10 15 20 25 30 35 40 45 50 55 60 65 70 75 80 85 90 95 100

THE UTILITY INFORMATION SHOWN ON THE CROSS SECTIONS ARE FROM THE BEST AVAILABLE INFORMATION PROVIDED TO HDR, INC. ACTUAL HORIZONTAL AND VERTICAL LOCATIONS MAY VARY. THE HORIZONTAL LOCATION AND DEPTHS OF THE UTILITIES SHOWN ARE ASSUMED AND SHOULD NOT BE CONSIDERED ACCURATE UNLESS NOTED OTHERWISE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE LOCATION OF ALL UTILITIES.

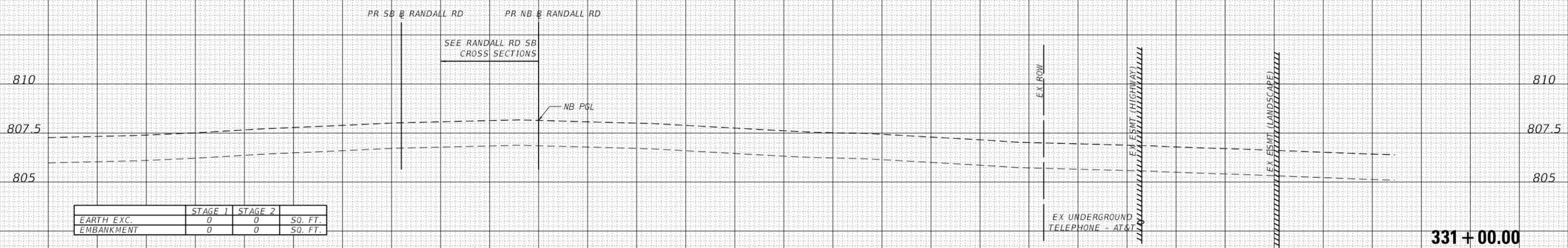
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UNLESS OTHERWISE NOTED CROSS SECTIONS ARE SHOWN PERPENDICULAR TO THE ALIGNMENT INDICATED IN THE TITLEBLOCK. CROSS SECTIONS AT A SKEW ARE LABELED WITH THE SKEW ANGLE RELATIVE TO NORMAL.

DATE	
BY	
FINISHED SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK NO.	
AREAS CHECKED	



DATE	
BY	
ORIGINAL SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK NO.	
AREAS CHECKED	



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PLOT DATE = 01/03/2019	DATE - 01-03-19	REVISED -

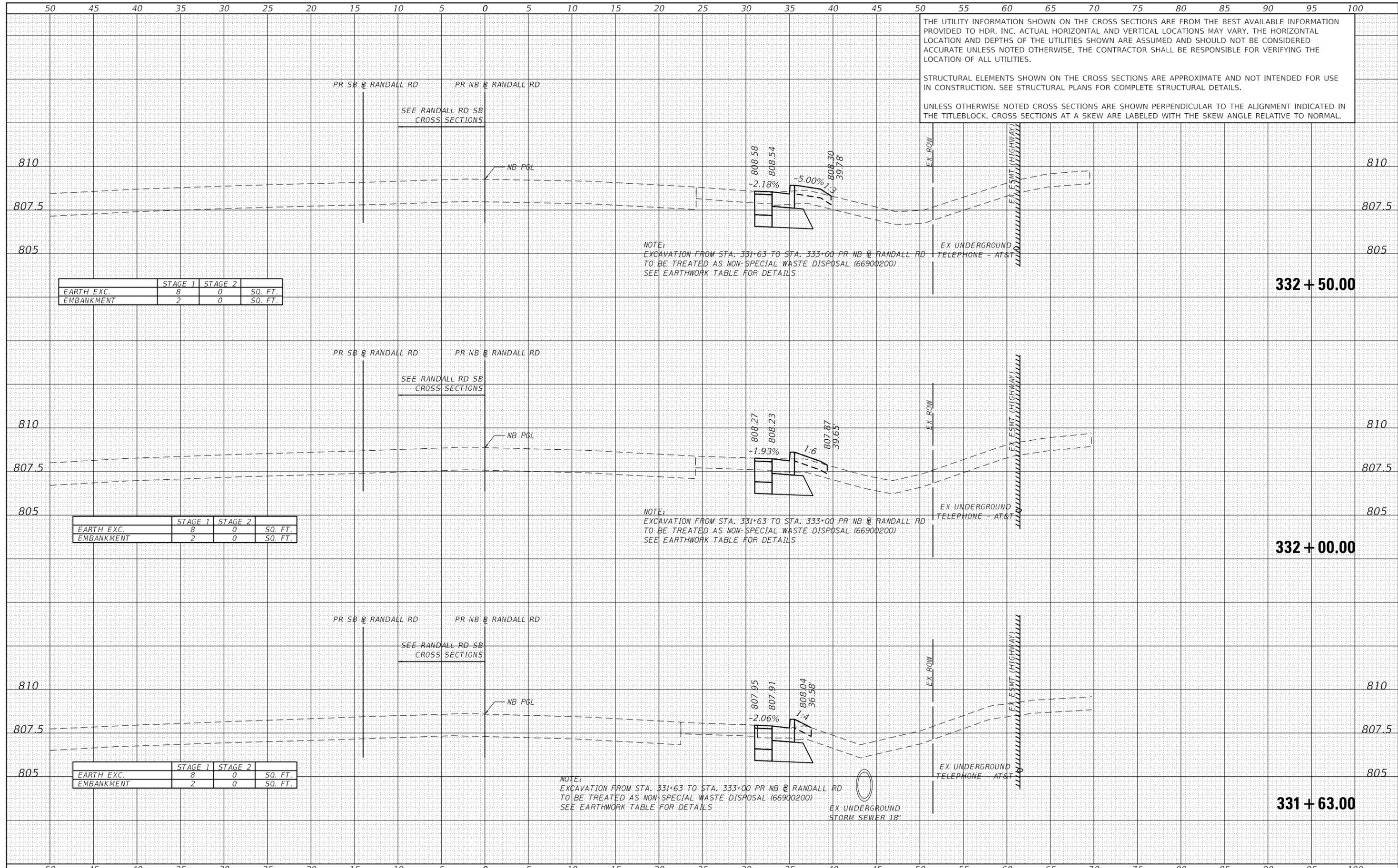
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS RANDALL ROAD - NORTHBOUND	
SCALE: 1"=5'/1"=2.5'	SHEET 1 OF 22 SHEETS
STA. 331+00.00	TO STA. 331+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	181
CONTRACT NO. 61F57				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

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FINAL SURVEY	
NOTE BOOK	
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PLOTTED	
TEMPLATE	
AREAS CHECKED	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	



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	STAGE 1	STAGE 2	
EARTH EXC.	8	0	SQ. FT.
EMBANKMENT	2	0	SQ. FT.

	STAGE 1	STAGE 2	
EARTH EXC.	8	0	SQ. FT.
EMBANKMENT	2	0	SQ. FT.

	STAGE 1	STAGE 2	
EARTH EXC.	8	0	SQ. FT.
EMBANKMENT	2	0	SQ. FT.

NOTE:
EXCAVATION FROM STA. 331+63 TO STA. 333+00 PR NB @ RANDALL RD
TO BE TREATED AS NON-SPECIAL WASTE DISPOSAL (66900200)
SEE EARTHWORK TABLE FOR DETAILS



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FILE NAME = 5 Scale X5 - Randall Rd NB	DRAWN - AKS	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - DWW	REVISED -
PLOT DATE = 01/03/2019	DATE - 01-03-19	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
RANDALL ROAD - NORTHBOUND

SCALE: 1"=51/1"=2.5' SHEET 2 OF 22 SHEETS STA. 331+63.00 TO STA. 332+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	182
CONTRACT NO. 61F57				

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

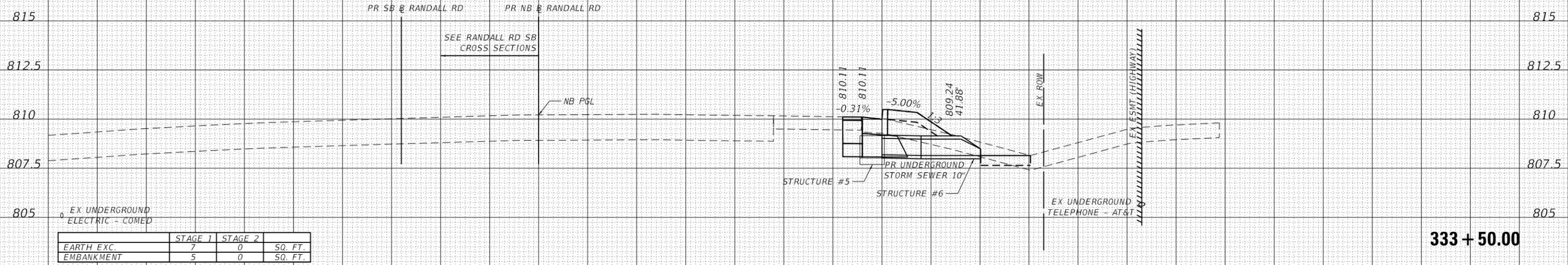
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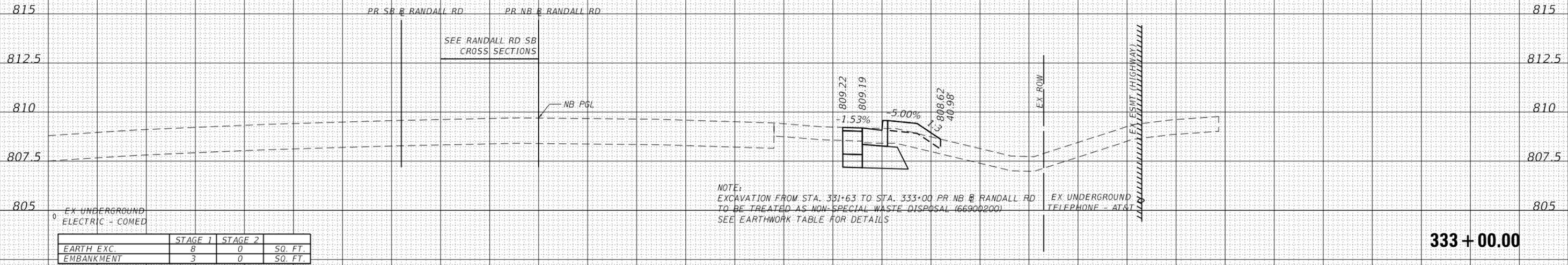
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DATE	
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FINISHED SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK AREAS CHECKED	



333 + 50.00

DATE	
BY	
NO.	
ORIGINAL SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK AREAS CHECKED	



NOTE:
EXCAVATION FROM STA. 331+63 TO STA. 333+00 PR NB @ RANDALL RD
TO BE TREATED AS NON-SPECIAL WASTE DISPOSAL (66900200)
SEE EARTHWORK TABLE FOR DETAILS

333 + 00.00

50 45 40 35 30 25 20 15 10 5 0 5 10 15 20 25 30 35 40 45 50 55 60 65 70 75 80 85 90 95 100



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FILE NAME = 5 Scale X5 - Randall Rd NB	DRAWN - AKS	REVISED -
PLOT SCALE = 10.0000 ' / in.	CHECKED - DWW	REVISED -
PLOT DATE = 01/03/2019	DATE - 01-03-19	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS RANDALL ROAD - NORTHBOUND	
SCALE: 1"=5'/1"=2.5'	SHEET 3 OF 22 SHEETS
STA. 333+00.00	TO STA. 333+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	183
CONTRACT NO. 61F57				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

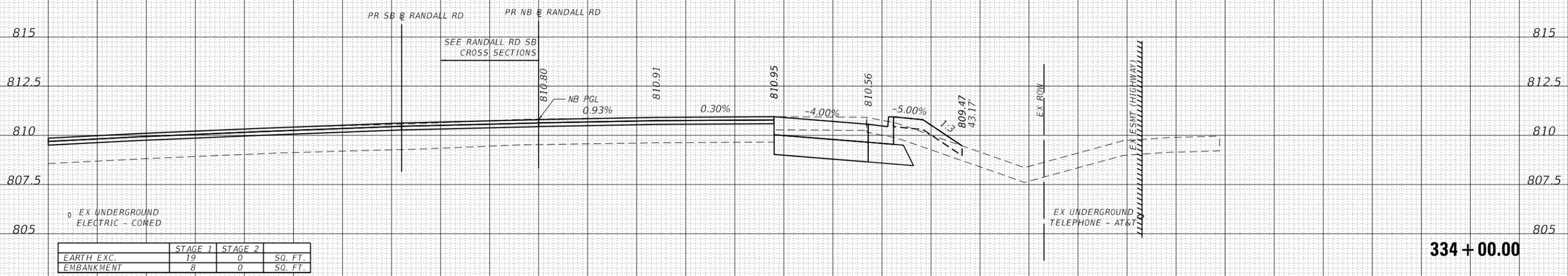
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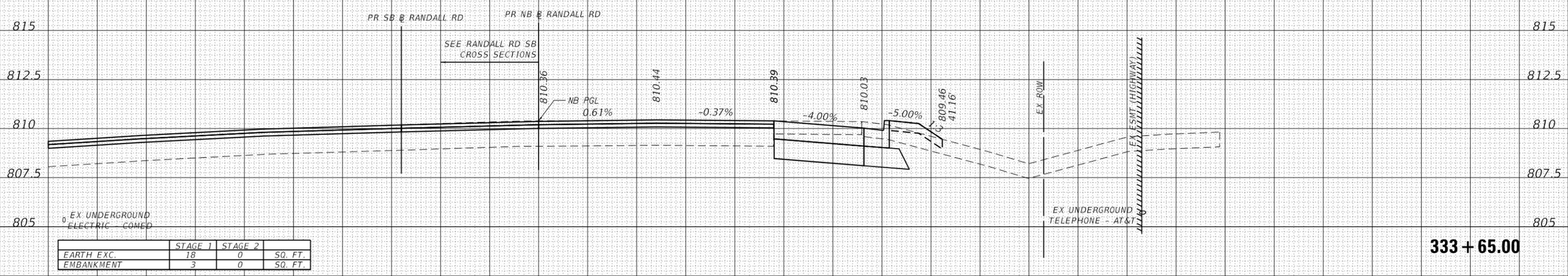
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DATE	
BY	
FINISHED SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	



DATE	
BY	
ORIGINAL SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	



50 45 40 35 30 25 20 15 10 5 0 5 10 15 20 25 30 35 40 45 50 55 60 65 70 75 80 85 90 95 100



USER NAME = RROGERS	DESIGNED - AKS	REVISED -
FILE NAME = 5 Scale X5 - Randall Rd NB	DRAWN - AKS	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - DWW	REVISED -
PLOT DATE = 01/03/2019	DATE - 01-03-19	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
RANDALL ROAD - NORTHBOUND

SCALE: 1"=51/1"=2.5' SHEET 4 OF 22 SHEETS STA. 333+65.00 TO STA. 334+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	184
CONTRACT NO. 61F57				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

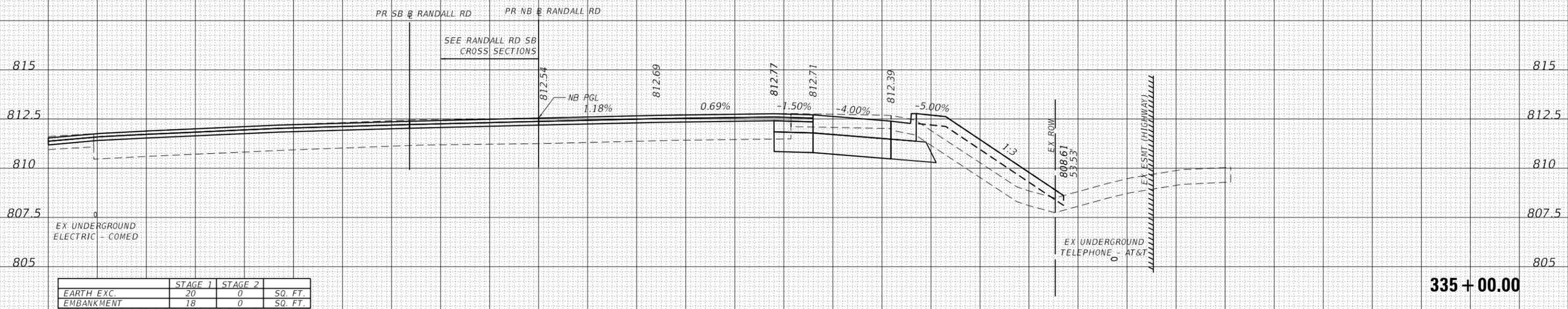
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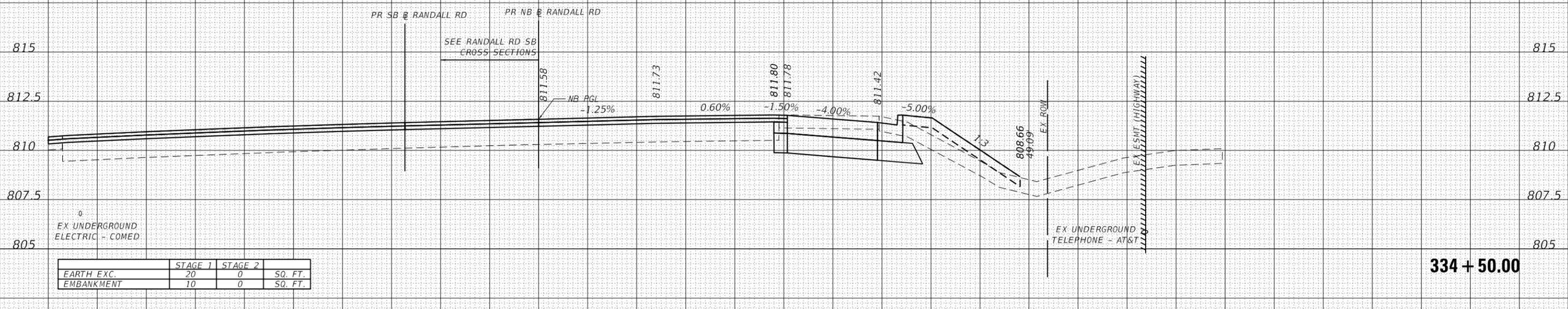
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DATE	
BY	
NO.	
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PLOTTED TEMPLATE	
NOTE BOOK AREAS CHECKED	



DATE	
BY	
NO.	
ORIGINAL SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK AREAS CHECKED	



50 45 40 35 30 25 20 15 10 5 0 5 10 15 20 25 30 35 40 45 50 55 60 65 70 75 80 85 90 95 100



USER NAME = RROGERS	DESIGNED - AKS	REVISED -
FILE NAME = 5 Scale X5 - Randall Rd NB	DRAWN - AKS	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - DWW	REVISED -
PLOT DATE = 01/03/2019	DATE - 01-03-19	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

CROSS SECTIONS RANDALL ROAD - NORTHBOUND	
SCALE: 1"=51/1"=2.5'	SHEET 5 OF 22 SHEETS
STA. 334+50.00	TO STA. 335+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	185
CONTRACT NO. 61F57				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

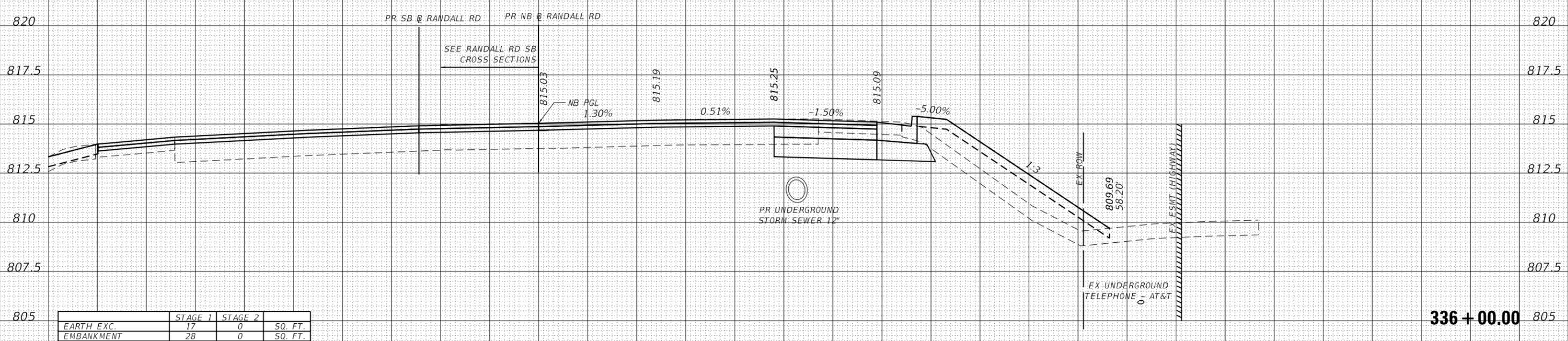
50 45 40 35 30 25 20 15 10 5 0 5 10 15 20 25 30 35 40 45 50 55 60 65 70 75 80 85 90 95 100

THE UTILITY INFORMATION SHOWN ON THE CROSS SECTIONS ARE FROM THE BEST AVAILABLE INFORMATION PROVIDED TO HDR, INC. ACTUAL HORIZONTAL AND VERTICAL LOCATIONS MAY VARY. THE HORIZONTAL LOCATION AND DEPTHS OF THE UTILITIES SHOWN ARE ASSUMED AND SHOULD NOT BE CONSIDERED ACCURATE UNLESS NOTED OTHERWISE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE LOCATION OF ALL UTILITIES.

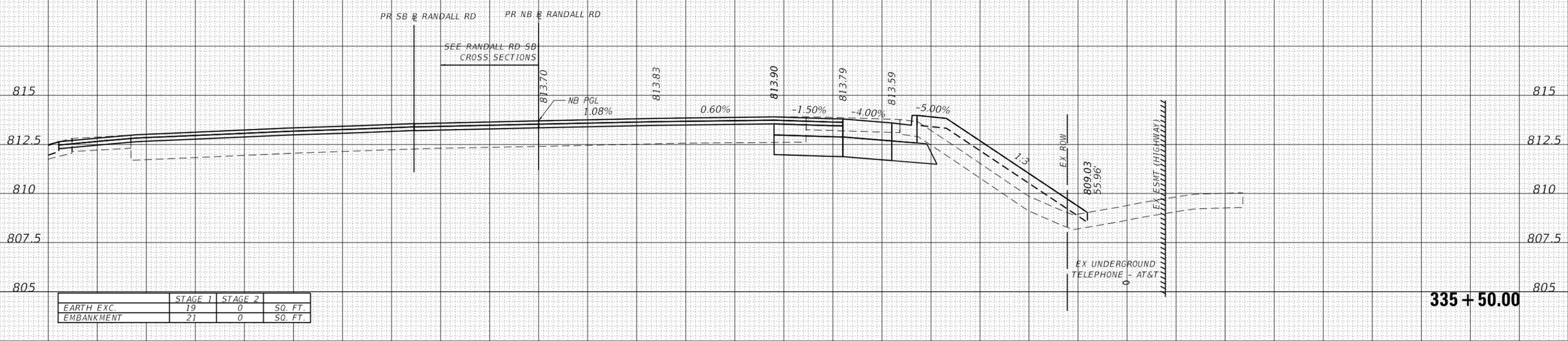
STRUCTURAL ELEMENTS SHOWN ON THE CROSS SECTIONS ARE APPROXIMATE AND NOT INTENDED FOR USE IN CONSTRUCTION. SEE STRUCTURAL PLANS FOR COMPLETE STRUCTURAL DETAILS.

UNLESS OTHERWISE NOTED CROSS SECTIONS ARE SHOWN PERPENDICULAR TO THE ALIGNMENT INDICATED IN THE TITLEBLOCK. CROSS SECTIONS AT A SKEW ARE LABELED WITH THE SKEW ANGLE RELATIVE TO NORMAL.

DATE	
BY	
FINISHED SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	



DATE	
BY	
ORIGINAL SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	



50 45 40 35 30 25 20 15 10 5 0 5 10 15 20 25 30 35 40 45 50 55 60 65 70 75 80 85 90 95 100



USER NAME = RROGERS	DESIGNED - AKS	REVISED -
FILE NAME = 5 Scale X5 - Randall Rd NB	DRAWN - AKS	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - DWW	REVISED -
PLOT DATE = 01/03/2019	DATE - 01-03-19	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
RANDALL ROAD - NORTHBOUND

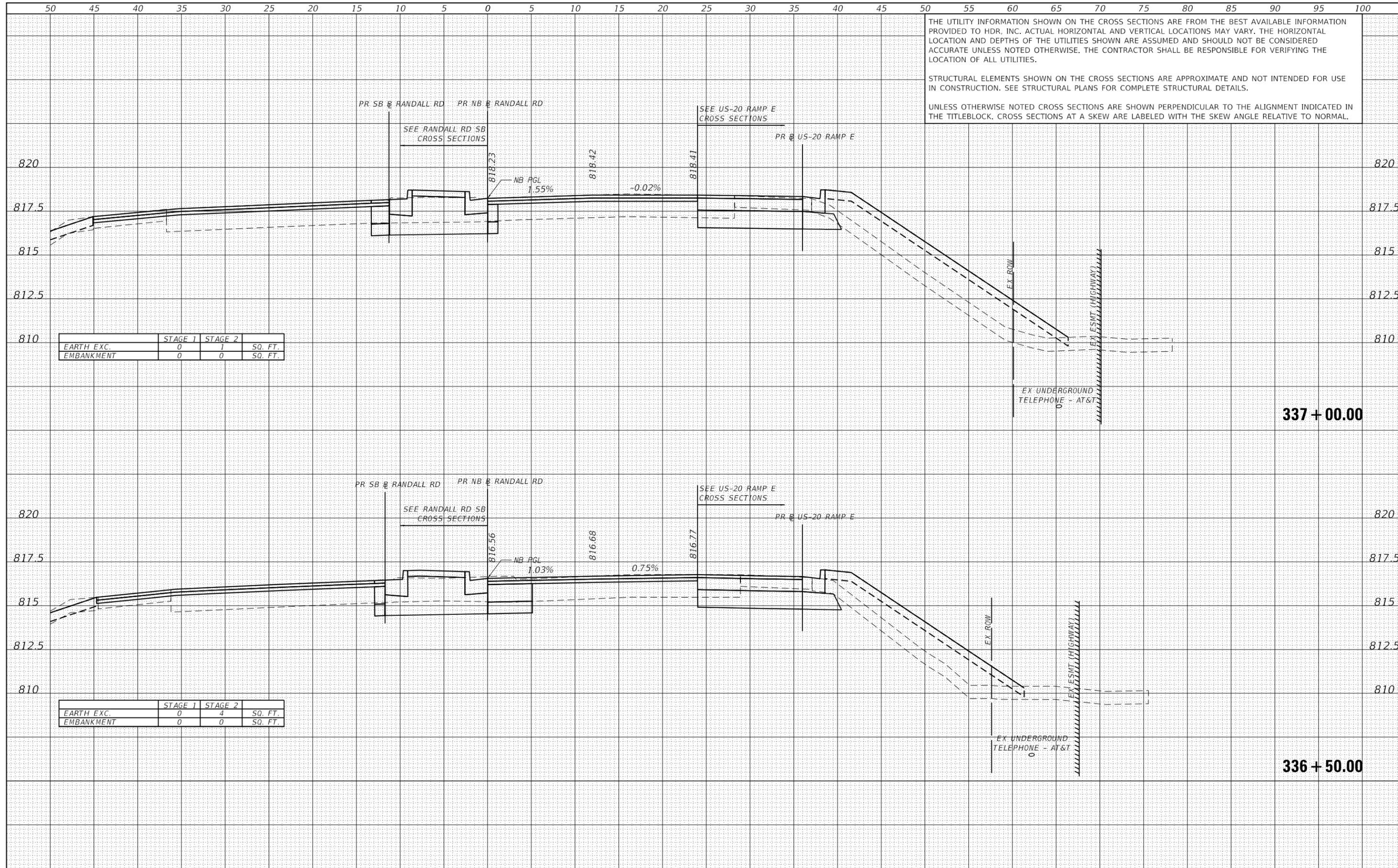
SCALE: 1"=5 1/2' 1"=2.5'

SHEET 6 OF 22 SHEETS STA. 335+50.00 TO STA. 336+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
336/345	14-00446-00-CH	KANE	354	186	
FED. ROAD DIST. NO. 1				ILLINOIS	FED. AID PROJECT
CONTRACT NO. 61F57					

DATE	
BY	
FINISHED SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	

DATE	
BY	
ORIGINAL SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	



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	STAGE 1	STAGE 2	SQ. FT.
EARTH EXC.	0	1	
EMBANKMENT	0	0	

	STAGE 1	STAGE 2	SQ. FT.
EARTH EXC.	0	4	
EMBANKMENT	0	0	



USER NAME = RR0GERS
 FILE NAME = 5 Scale X5 - Randall Rd NB
 PLOT SCALE = 10.0000' / in.
 PLOT DATE = 01/03/2019

DESIGNED - AKS
 DRAWN - AKS
 CHECKED - DWW
 DATE - 01-03-19

REVISED -
 REVISED -
 REVISED -
 REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
 RANDALL ROAD - NORTHBOUND
 SCALE: 1"=5'/1"=2.5' SHEET 7 OF 22 SHEETS STA. 336+50.00 TO STA. 337+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	187
CONTRACT NO. 61F57				

FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

50 45 40 35 30 25 20 15 10 5 0 5 10 15 20 25 30 35 40 45 50 55 60 65 70 75 80 85 90 95 100

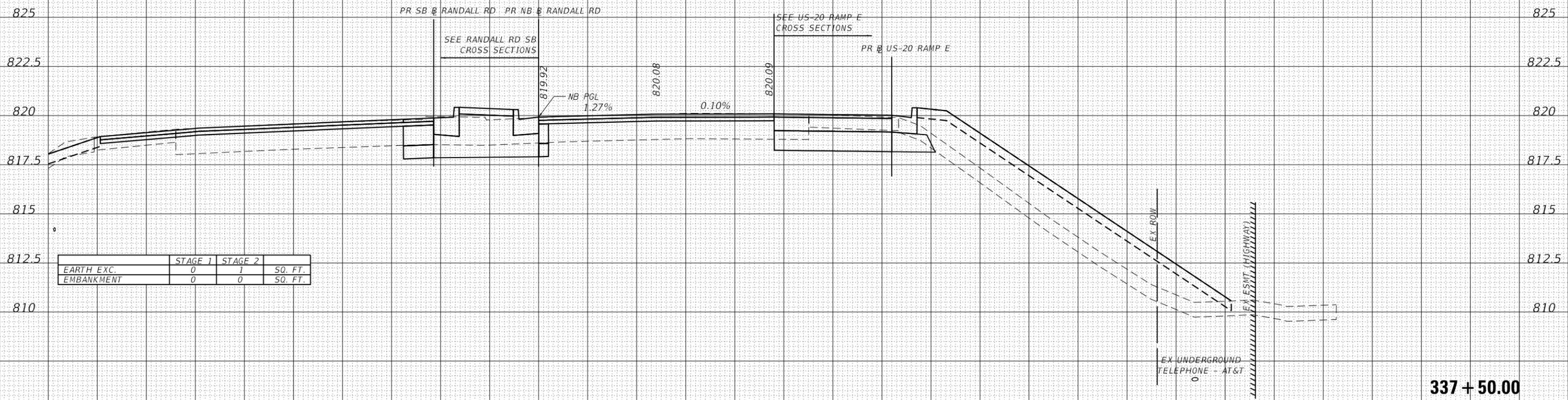
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DATE	
BY	
FINISHED SURVEY	
PLOTTED TEMPLATE	
AREAS CHECKED	
NO.	

DATE	
BY	
ORIGINAL SURVEY	
PLOTTED TEMPLATE	
AREAS CHECKED	
NO.	



337 + 50.00

50 45 40 35 30 25 20 15 10 5 0 5 10 15 20 25 30 35 40 45 50 55 60 65 70 75 80 85 90 95 100



USER NAME = RR0GERS	DESIGNED - AKS	REVISED -
FILE NAME = 5 Scale X5 - Randall Rd NB	DRAWN - AKS	REVISED -
PLOT SCALE = 10.0000 ' / in.	CHECKED - DWW	REVISED -
PLOT DATE = 01/03/2019	DATE - 01-03-19	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS RANDALL ROAD - NORTHBOUND	
SCALE: 1"=5'/1"=2.5'	SHEET 8 OF 22 SHEETS
STA. 337+50.00	TO STA. 337+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	188
CONTRACT NO. 61F57				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

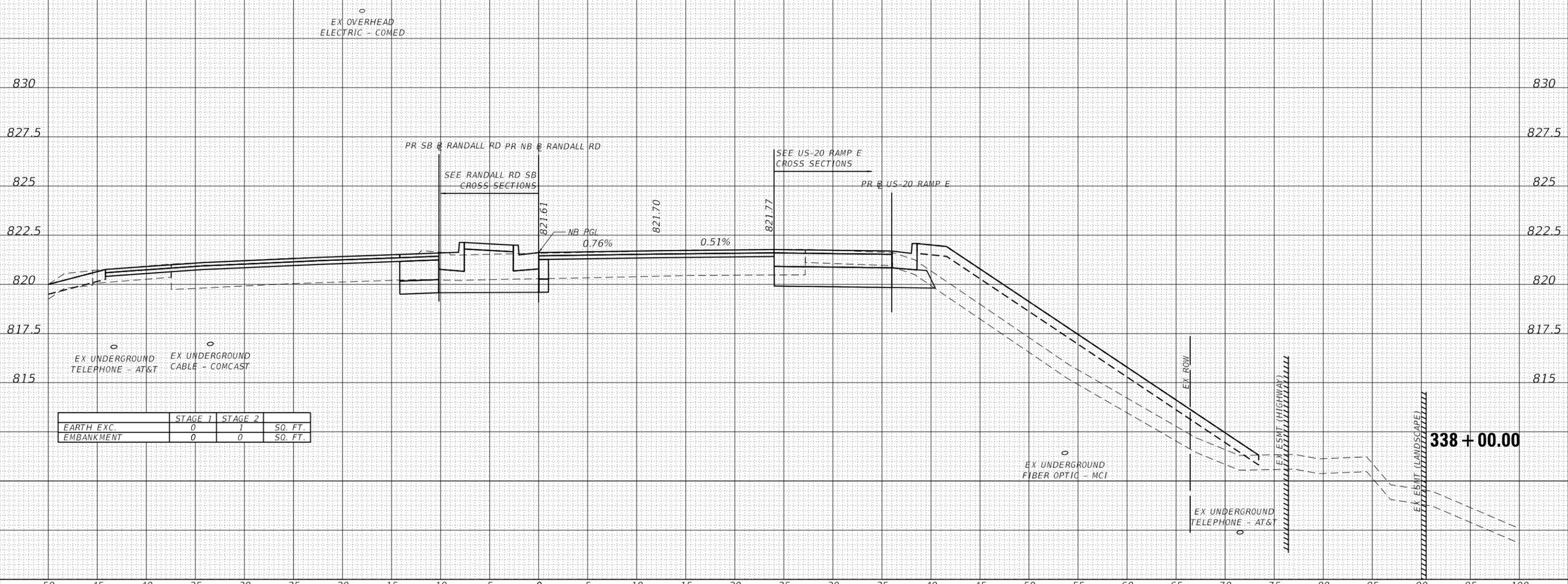
50 45 40 35 30 25 20 15 10 5 0 5 10 15 20 25 30 35 40 45 50 55 60 65 70 75 80 85 90 95 100

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DATE	
BY	
NO.	
ORIGINAL SURVEY	
NOTE BOOK	
AREAS CHECKED	
NO.	
FINAL SURVEY	
NOTE BOOK	
AREAS CHECKED	
NO.	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS CHECKED	
NO.	



DATE	
BY	
NO.	
ORIGINAL SURVEY	
NOTE BOOK	
AREAS CHECKED	
NO.	
FINAL SURVEY	
NOTE BOOK	
AREAS CHECKED	
NO.	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS CHECKED	
NO.	

50 45 40 35 30 25 20 15 10 5 0 5 10 15 20 25 30 35 40 45 50 55 60 65 70 75 80 85 90 95 100



USER NAME = RROGERS	DESIGNED - AKS	REVISED -
FILE NAME = 5 Scale X5 - Randall Rd NB	DRAWN - AKS	REVISED -
PLOT SCALE = 10.0000 ' / in.	CHECKED - DWW	REVISED -
PLOT DATE = 01/03/2019	DATE - 01-03-19	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS
RANDALL ROAD - NORTHBOUND**

SCALE: 1"=51/1"=2.5' SHEET 9 OF 22 SHEETS STA. 338+00.00 TO STA. 338+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	189
CONTRACT NO. 61F57				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

50 45 40 35 30 25 20 15 10 5 0 5 10 15 20 25 30 35 40 45 50 55 60 65 70 75 80 85 90 95 100

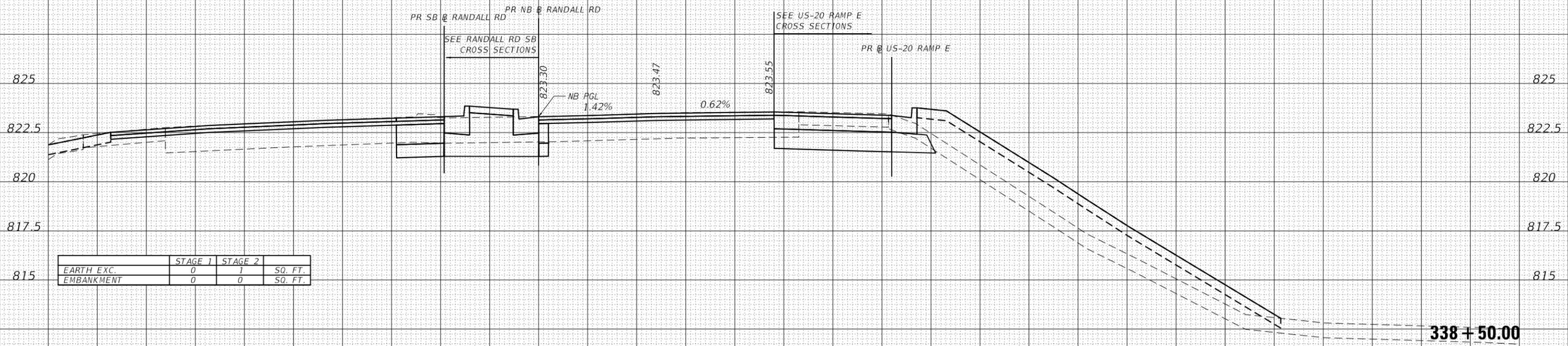
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DATE	
BY	
FINISHED SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK NO.	
AREAS CHECKED	

DATE	
BY	
ORIGINAL SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK NO.	
AREAS CHECKED	



	STAGE 1	STAGE 2	SQ. FT.
EARTH EXC.	0	1	
EMBANKMENT	0	0	

50 45 40 35 30 25 20 15 10 5 0 5 10 15 20 25 30 35 40 45 50 55 60 65 70 75 80 85 90 95 100



USER NAME = RROGERS	DESIGNED - AKS	REVISED -
FILE NAME = 5 Scale X5 - Randall Rd NB	DRAWN - AKS	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - DWW	REVISED -
PLOT DATE = 01/03/2019	DATE - 01-03-19	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
RANDALL ROAD - NORTHBOUND
SCALE: 1"=51'1"=2.5' SHEET 10 OF 22 SHEETS STA. 338+50.00 TO STA. 338+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	190
CONTRACT NO. 61F57				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

50 45 40 35 30 25 20 15 10 5 0 5 10 15 20 25 30 35 40 45 50 55 60 65 70 75 80 85 90 95 100

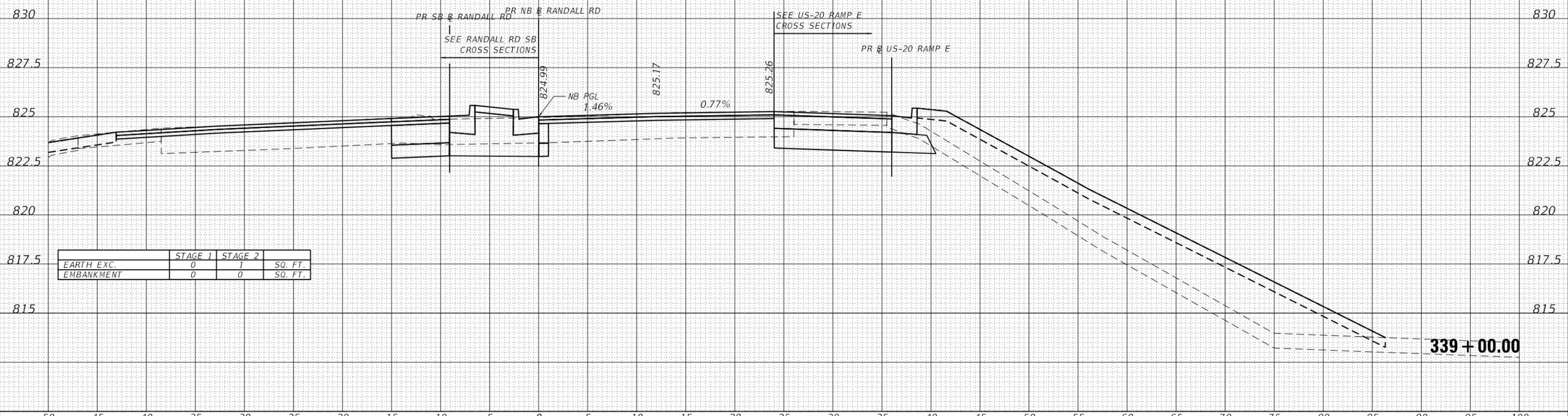
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DATE	
BY	
FINISHED SURVEY	
NOTE BOOK	
NO.	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	

DATE	
BY	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	



50 45 40 35 30 25 20 15 10 5 0 5 10 15 20 25 30 35 40 45 50 55 60 65 70 75 80 85 90 95 100



USER NAME = RR0GERS	DESIGNED - AKS	REVISED -
FILE NAME = 5 Scale X5 - Randall Rd NB	DRAWN - AKS	REVISED -
PLOT SCALE = 10.0000 ' / in.	CHECKED - DWW	REVISED -
PLOT DATE = 01/03/2019	DATE - 01-03-19	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

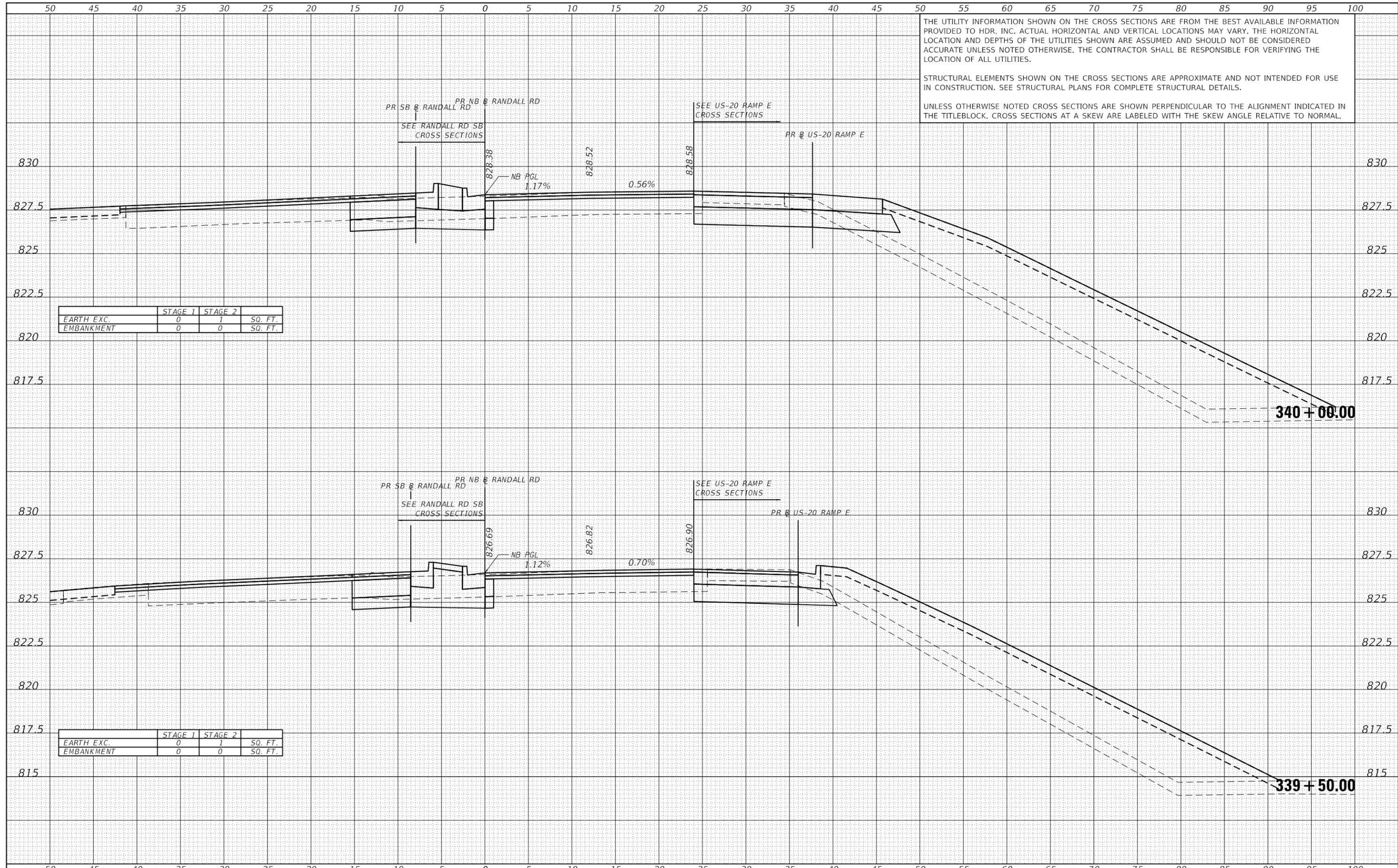
**CROSS SECTIONS
RANDALL ROAD - NORTHBOUND**

SCALE: 1"=5'/1"=2.5' SHEET 11 OF 22 SHEETS STA. 339+00.00 TO STA. 339+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	191
CONTRACT NO. 61F57				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

DATE	
BY	
NO.	
FINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
AREAS CHECKED	TEMPLATE
	AREAS CHECKED

DATE	
BY	
NO.	
ORIGINAL SURVEY	SURVEYED
NOTE BOOK	PLOTTED
AREAS CHECKED	TEMPLATE
	AREAS CHECKED



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	STAGE 1	STAGE 2	
EARTH EXC.	0	1	SQ. FT.
EMBANKMENT	0	0	SQ. FT.

	STAGE 1	STAGE 2	
EARTH EXC.	0	1	SQ. FT.
EMBANKMENT	0	0	SQ. FT.



USER NAME =	RR ROGERS	DESIGNED -	AKS	REVISED -	
FILE NAME =	5 Scale X5 - Randall Rd NB	DRAWN -	AKS	REVISED -	
PLOT SCALE =	10.0000' / in.	CHECKED -	DWW	REVISED -	
PLOT DATE =	01/03/2019	DATE	01-03-19	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS	
RANDALL ROAD - NORTHBOUND	
SCALE: 1"=51/1"=2.5'	SHEET 12 OF 22 SHEETS
STA. 339+50.00	TO STA. 340+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	192
CONTRACT NO. 61F57				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

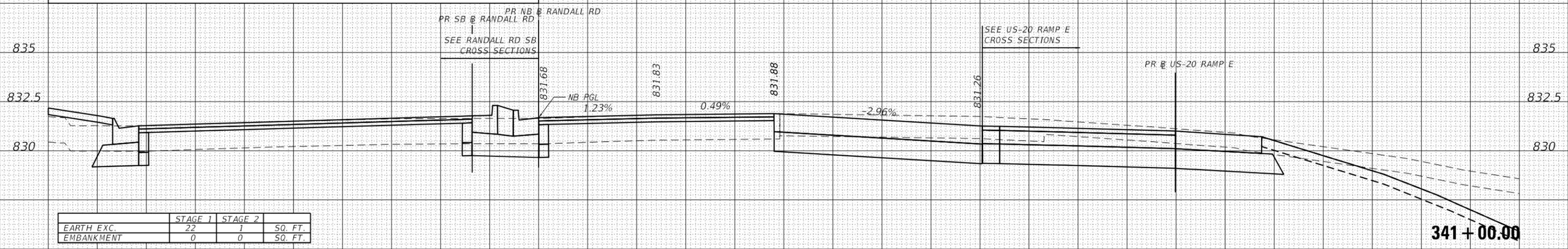
50 45 40 35 30 25 20 15 10 5 0 5 10 15 20 25 30 35 40 45 50 55 60 65 70 75 80 85 90 95 100

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DATE	
BY	
FINISHED SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	



	STAGE 1	STAGE 2	SQ. FT.
EARTH EXC.	22	1	
EMBANKMENT	0	0	

DATE	
BY	
ORIGINAL SURVEY	
PLOTTED	
TEMPLATE	
NOTE BOOK	
AREAS CHECKED	
NO.	



	STAGE 1	STAGE 2	SQ. FT.
EARTH EXC.	5	1	
EMBANKMENT	0	0	

50 45 40 35 30 25 20 15 10 5 0 5 10 15 20 25 30 35 40 45 50 55 60 65 70 75 80 85 90 95 100



USER NAME = RROGERS	DESIGNED - AKS	REVISED -
FILE NAME = 5 Scale X5 - Randall Rd NB	DRAWN - AKS	REVISED -
PLOT SCALE = 10.0000 ' / in.	CHECKED - DWW	REVISED -
PLOT DATE = 01/03/2019	DATE - 01-03-19	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS		TOTAL SHEETS		SHEET NO.	
RANDALL ROAD - NORTHBOUND		354		193	
SCALE: 1"=5'/1"=2.5'		SHEET 13 OF 22 SHEETS		STA. 340+50.00 TO STA. 341+00.00	

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	193
CONTRACT NO. 61F57				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

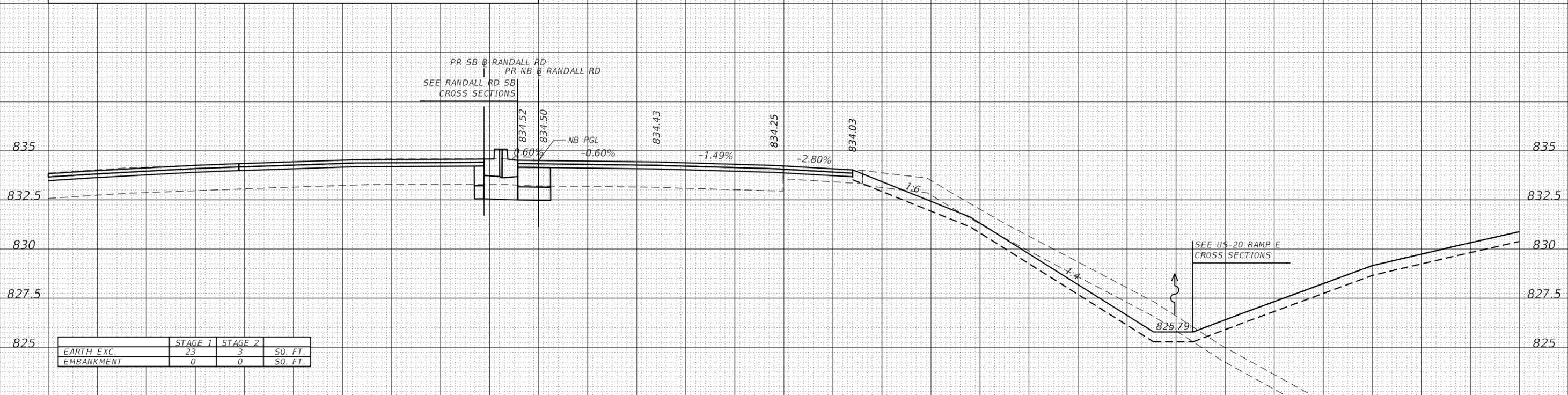
50 45 40 35 30 25 20 15 10 5 0 5 10 15 20 25 30 35 40 45 50 55 60 65 70 75 80 85 90 95 100

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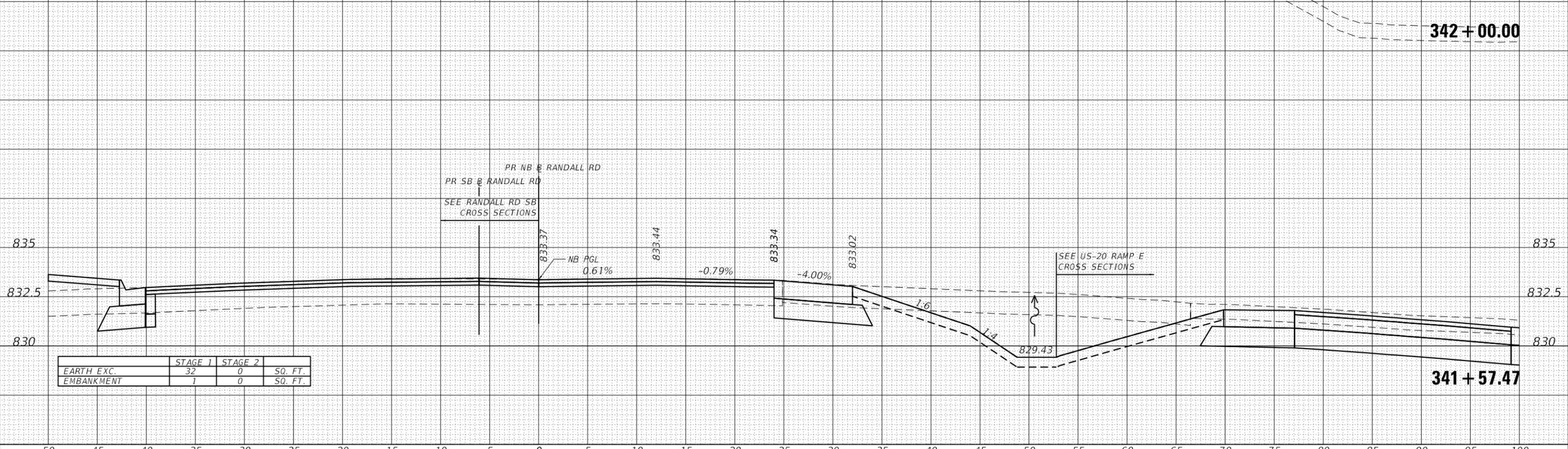
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DATE	
BY	
FINAL SURVEY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
NO.	



DATE	
BY	
ORIGINAL SURVEY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
NO.	



<p>HDR 8550 W. BRYN MAWR AVE. CHICAGO, IL 60631</p>	USER NAME = RR0GERS FILE NAME = 5 Scale X5 - Randall Rd NB PLOT SCALE = 10.0000' / in. PLOT DATE = 01/03/2019	DESIGNED - AKS DRAWN - AKS CHECKED - DWW DATE - 01-03-19	REVISED - REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CROSS SECTIONS RANDALL ROAD - NORTHBOUND	SCALE: 1"=5 1/2"=2.5' SHEET 15 OF 22 SHEETS STA. 341+57.47 TO STA. 342+00.00	F.A.P. RTE. SECTION COUNTY TOTAL SHEETS SHEET NO. 336/345 14-00446-00-CH KANE 354 195
	CONTRACT NO. 61F57 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						

50 45 40 35 30 25 20 15 10 5 0 5 10 15 20 25 30 35 40 45 50 55 60 65 70 75 80 85 90 95 100

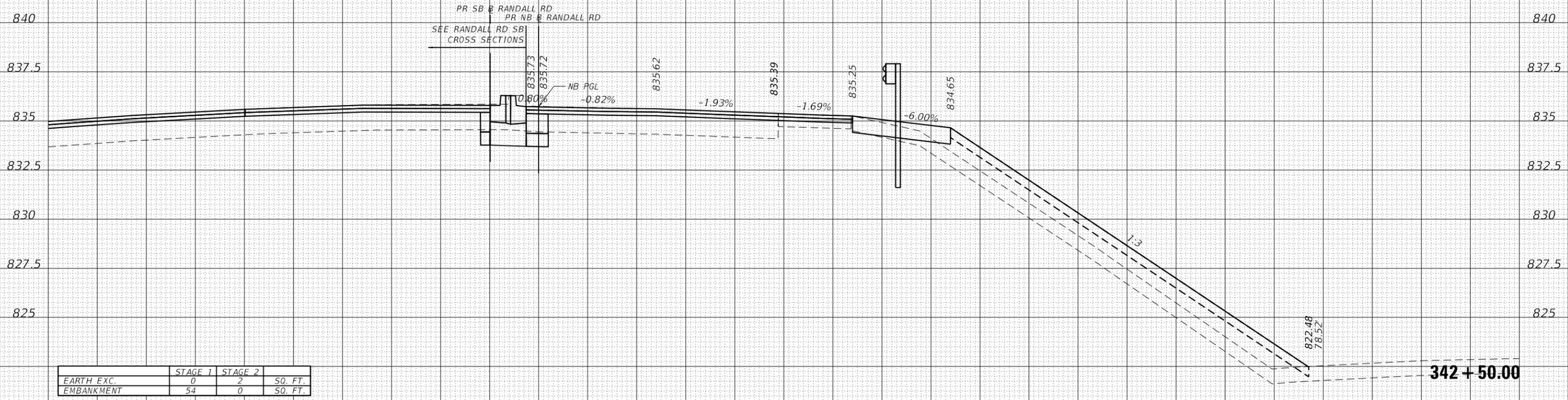
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DATE	
BY	
FINISHED SURVEY	
NOTE BOOK	
NO.	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	

DATE	
BY	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	



50 45 40 35 30 25 20 15 10 5 0 5 10 15 20 25 30 35 40 45 50 55 60 65 70 75 80 85 90 95 100



USER NAME = RR00GERS	DESIGNED - AKS	REVISED -
FILE NAME = 5 Scale X5 - Randall Rd NB	DRAWN - AKS	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - DWW	REVISED -
PLOT DATE = 01/03/2019	DATE - 01-03-19	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
RANDALL ROAD - NORTHBOUND

SCALE: 1"=5'/1"=2.5' SHEET 16 OF 22 SHEETS STA. 342+50.00 TO STA. 342+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	196
CONTRACT NO. 61F57				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

50 45 40 35 30 25 20 15 10 5 0 5 10 15 20 25 30 35 40 45 50 55 60 65 70 75 80 85 90 95 100

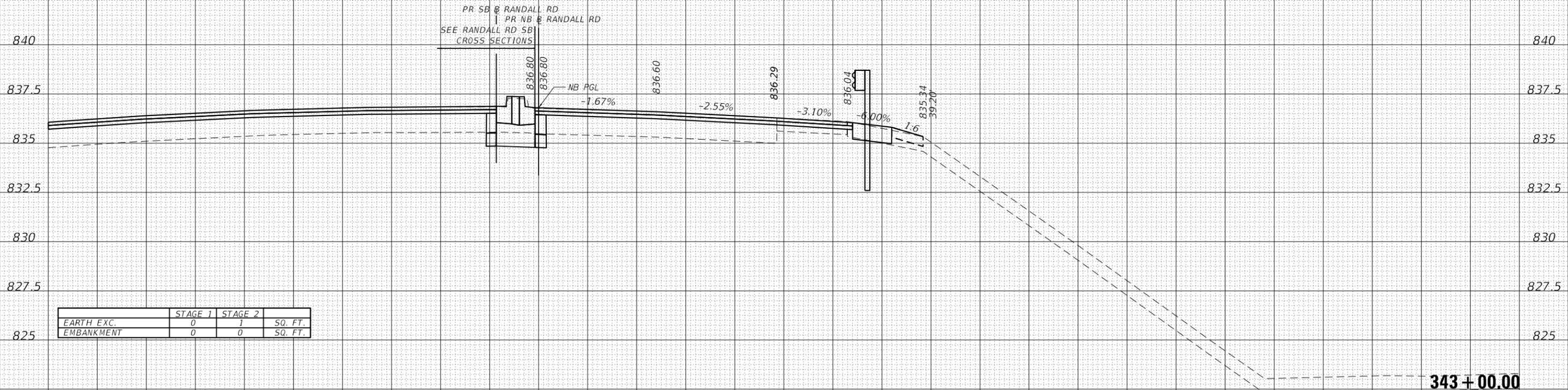
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DATE	
BY	
FINISHED SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK NO.	
AREAS CHECKED	

DATE	
BY	
ORIGINAL SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK NO.	
AREAS CHECKED	



	STAGE 1	STAGE 2	
EARTH EXC.	0	1	SQ. FT.
EMBANKMENT	0	0	SQ. FT.

50 45 40 35 30 25 20 15 10 5 0 5 10 15 20 25 30 35 40 45 50 55 60 65 70 75 80 85 90 95 100



USER NAME = RR0GERS	DESIGNED - AKS	REVISED -
FILE NAME = 5 Scale X5 - Randall Rd NB	DRAWN - AKS	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - DWW	REVISED -
PLOT DATE = 01/03/2019	DATE - 01-03-19	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS	
RANDALL ROAD - NORTHBOUND	
SCALE: 1"=5'/1"=2.5'	SHEET 17 OF 22 SHEETS
STA. 343+00.00	TO STA. 343+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	197
CONTRACT NO. 61F57				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

50 45 40 35 30 25 20 15 10 5 0 5 10 15 20 25 30 35 40 45 50 55 60 65 70 75 80 85 90 95 100

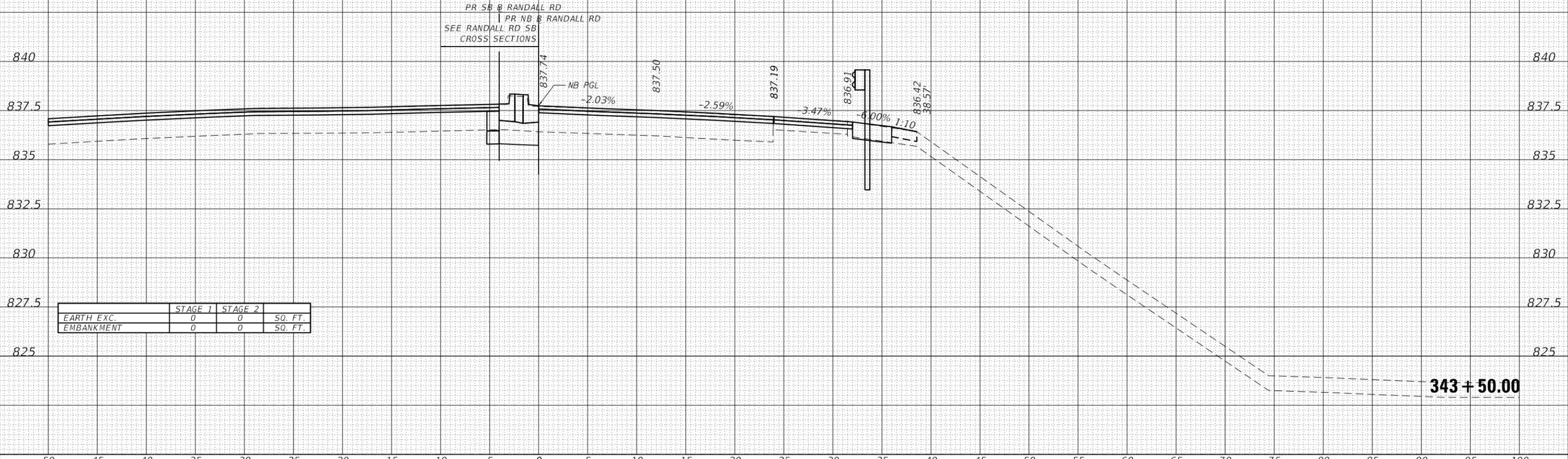
THE UTILITY INFORMATION SHOWN ON THE CROSS SECTIONS ARE FROM THE BEST AVAILABLE INFORMATION PROVIDED TO HDR, INC. ACTUAL HORIZONTAL AND VERTICAL LOCATIONS MAY VARY. THE HORIZONTAL LOCATION AND DEPTHS OF THE UTILITIES SHOWN ARE ASSUMED AND SHOULD NOT BE CONSIDERED ACCURATE UNLESS NOTED OTHERWISE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING THE LOCATION OF ALL UTILITIES.

STRUCTURAL ELEMENTS SHOWN ON THE CROSS SECTIONS ARE APPROXIMATE AND NOT INTENDED FOR USE IN CONSTRUCTION. SEE STRUCTURAL PLANS FOR COMPLETE STRUCTURAL DETAILS.

UNLESS OTHERWISE NOTED CROSS SECTIONS ARE SHOWN PERPENDICULAR TO THE ALIGNMENT INDICATED IN THE TITLEBLOCK. CROSS SECTIONS AT A SKEW ARE LABELED WITH THE SKEW ANGLE RELATIVE TO NORMAL.

DATE	
BY	
FINISHED SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK AREAS CHECKED	
NO.	

DATE	
BY	
ORIGINAL SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK AREAS CHECKED	
NO.	



50 45 40 35 30 25 20 15 10 5 0 5 10 15 20 25 30 35 40 45 50 55 60 65 70 75 80 85 90 95 100



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FILE NAME = 5 Scale X5 - Randall Rd NB	DRAWN - AKS	REVISED -
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PLOT DATE = 01/03/2019	DATE - 01-03-19	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS
RANDALL ROAD - NORTHBOUND**

SCALE: 1"=51/1"=2.5' SHEET 18 OF 22 SHEETS STA. 343+50.00 TO STA. 343+50.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	198
CONTRACT NO. 61F57				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

50 45 40 35 30 25 20 15 10 5 0 5 10 15 20 25 30 35 40 45 50 55 60 65 70 75 80 85 90 95 100

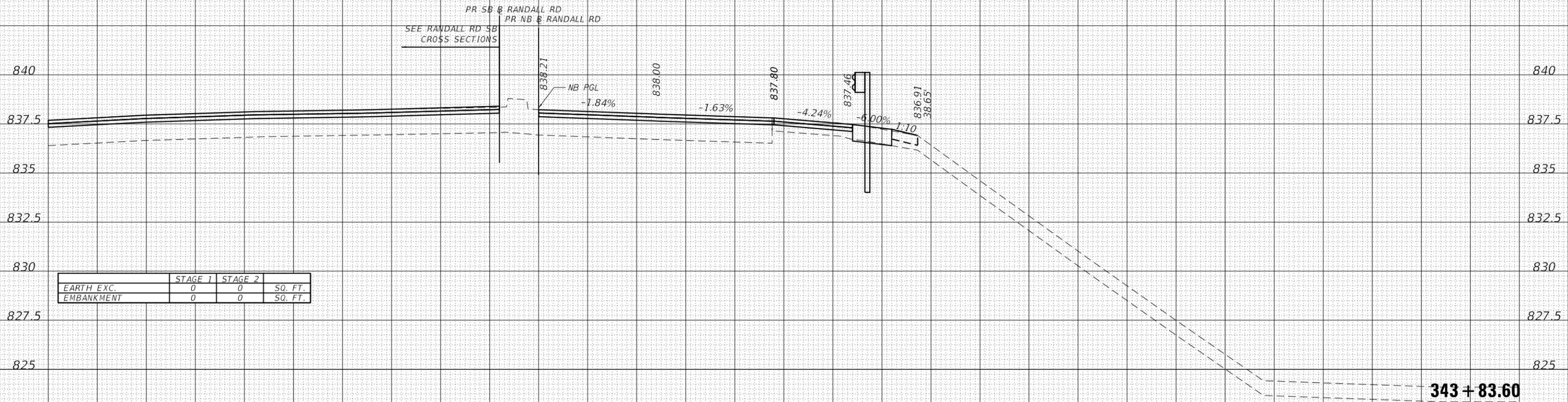
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DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
FINAL SURVEY	
NOTE BOOK	
NO.	

DATE	
BY	
SURVEYED	
PLOTTED	
TEMPLATE	
AREAS	
CHECKED	
ORIGINAL SURVEY	
NOTE BOOK	
NO.	



	STAGE 1	STAGE 2	
EARTH EXC.	0	0	SQ. FT.
EMBANKMENT	0	0	SQ. FT.

50 45 40 35 30 25 20 15 10 5 0 5 10 15 20 25 30 35 40 45 50 55 60 65 70 75 80 85 90 95 100



USER NAME = RROGERS	DESIGNED - AKS	REVISED -
FILE NAME = 5 Scale X5 - Randall Rd NB	DRAWN - AKS	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - DWW	REVISED -
PLOT DATE = 01/03/2019	DATE - 01-03-19	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
RANDALL ROAD - NORTHBOUND

SCALE: 1"=5'/1"=2.5' SHEET 19 OF 22 SHEETS STA. 343+83.60 TO STA. 343+83.60

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	199
CONTRACT NO. 61F57				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

50 45 40 35 30 25 20 15 10 5 0 5 10 15 20 25 30 35 40 45 50 55 60 65 70 75 80 85 90 95 100

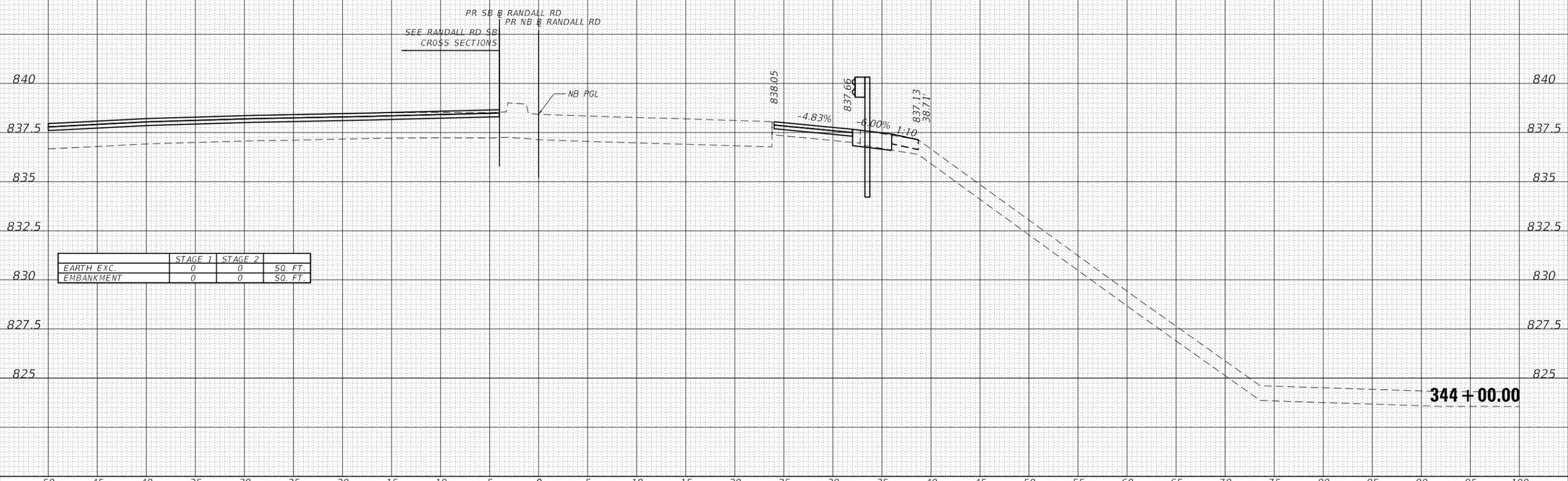
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BY	
FINISHED SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK NO.	
AREAS CHECKED	

DATE	
BY	
ORIGINAL SURVEY	
PLOTTED TEMPLATE	
NOTE BOOK NO.	
AREAS CHECKED	



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
RANDALL ROAD - NORTHBOUND

SCALE: 1"=5'/1"=2.5' SHEET 20 OF 22 SHEETS STA. 344+00.00 TO STA. 344+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
336/345	14-00446-00-CH	KANE	354	200
CONTRACT NO. 61F57				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				