

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

F.A.P. DATE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2018-010-RS-SW	COOK	61	1
ILLINOIS			CONTRACT NO. 62G80	

D-91-341-18



FOR INDEX OF SHEETS, SEE SHEET NO. 2

THIS PROJECT IS LOCATED IN THE CITY OF BLUE ISLAND & VILLAGE OF ALSIP

TRAFFIC DATA:  
2015 ADT: 32,800  
POSTED SPEED LIMIT: 30 MPH

DESIGN DESIGNATION:  
FAP 344 (127TH ST./BURR OAK AVE): OTHER ARTERIAL

# PROPOSED HIGHWAY PLANS

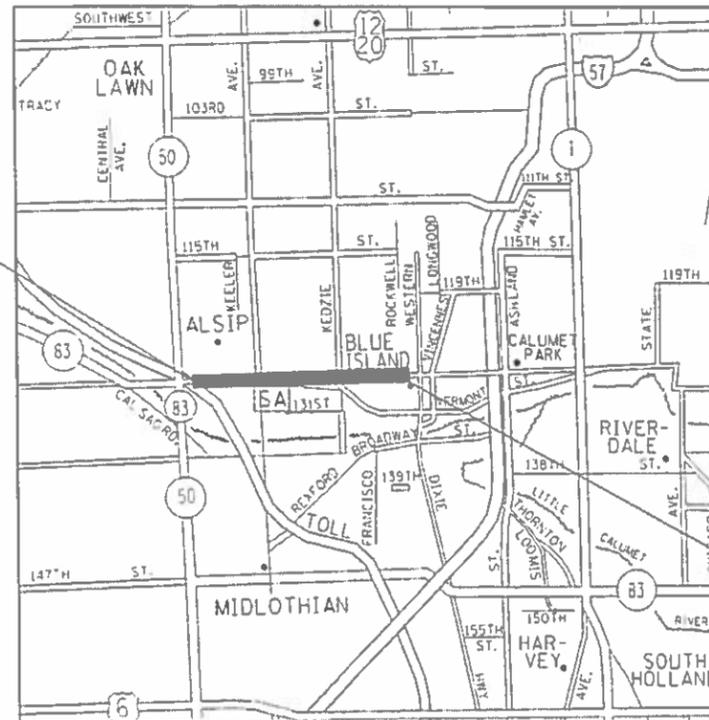
FAP ROUTE 344 (127TH ST./BURR OAK AVE.)  
EO I-294 TO ARTESIAN AVE.  
SECTION: 2018-010-RS-SW  
PROJECT: NHPP-5RPA(310)  
RESURFACING (3P), PEDESTRIAN RAMPS  
COOK COUNTY

C-91-264-18

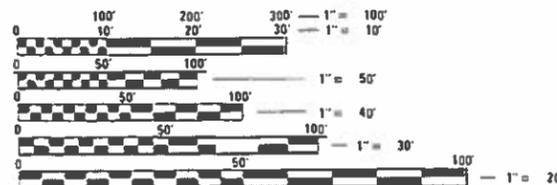
R 13 E

PROJECT BEGINS:  
STA. 22+31

T 37 N



PROJECT ENDS:  
STA. 159+20



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

PROJECT ENGINEER: DANIEL WILGREEN (847)-705-4240  
PROJECT MANAGER: FAWAD AQUEEL (847)-705-4247

GROSS LENGTH = 13,689 FT. = 2.39 MILE  
NET LENGTH = 13,443 FT. = 2.55 MILE

CONTRACT NO. 62G80

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED February 5, 2019

*Anthony J. Danzig* REGIONAL ENGINEER  
Feb 11 2019

*Paul P. Chaf* ENGINEER OF DESIGN AND ENVIRONMENT  
Feb 11 2019  
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

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**STATE STANDARDS**

STANDARD NO.	DESCRIPTION
000001-07	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-11	PERPENDICULAR CURB RAMPS
424011-04	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
442101-09	CLASS B PATCHES
424021-05	DEPRESSED CORNER FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
604001-04	FRAME AND LIDS TYPE 1
604086-03	FRAME AND GRATE TYPE 23
606001-07	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
606301-04	PC CONCRETE ISLANDS AND MEDIANS
606306-04	CORRUGATED PC CONCRETE MEDIANS
701101-05	OFF-RD OPERATIONS, MULTILANE, 15' (4.5 m) TO 24' (600 mm) FROM PAVEMENT EDGE
701427-05	LANE CLOSURE, MULTILANE, DAY OPERATIONS ONLY, FOR SPEEDS <= 40 MPH

**STATE STANDARDS (CONTINUED)**

STANDARD NO.	DESCRIPTION
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701602-10	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701611-01	URBAN HALF ROAD CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-04	SIGN PANEL ERECTION DETAILS
728001-01	TELESCOPING STEEL SIGN SUPPORT
814001-03	HANDHOLES

**GENERAL NOTES**

NOTE NO.	DESCRIPTION
1	BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION AT 8-1-1 OR (800) 892-0123 OR "C.U.A.N." (CHICAGO UTILITY ALERT NETWORK) AT (312) 744-7000 FOR FIELD LOCATION OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES (48 HOUR NOTIFICATION IS REQUIRED).
2	IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS
3	TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER ITEMS OF WORK TO EXISTING CURBS AND GUTTERS AND CONDITIONS IN THE FIELD UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
4	THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE CITY OF BLUE ISLAND
5	IN VARIOUS LOCATIONS THROUGHOUT THE PROJECT, EXISTING ASPHALT OVERLAYS THE EXISTING CONCRETE GUTTER FLAG. REMOVAL OF ASPHALT ON THE GUTTER FLAG SHALL BE INCLUDED IN THE UNIT COST OF "HMA SURFACE REMOVAL VARIABLE DEPTH"
6	THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
7	SAW CUTTING OF PAVEMENTS, SIDEWALK, CURB & GUTTER, ETC. SHALL BE TO FULL DEPTH AND SHALL RESULT IN A CLEAN STRAIGHT EDGE ON THE PORTION REMAINING. ALL SAW CUTTING SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEM REMOVED
8	DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS
9	THE CONTRACTOR SHALL USE CARE IN REMOVING OR EXCAVATING NEAR ALL EXISTING ITEMS WHICH WILL REMAIN. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
10	WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL, BETWEEN PASSES OF THE MILLING MACHING SHALL NOT EXCEED 1.5 INCHES WHERE THE SPEED LIMIT IS 40 MPH OR LESS. WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRAD DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF MILLING IS SLOPED A MINIMUM OF 1:3 (V:H)
11	UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURES AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS, OVERNIGHT CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING.

**GENERAL NOTES (CONTINUED)**

NOTE NO.	DESCRIPTION
12	BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE ALL EXISTING PAVEMENT MARKING LINES AND RAISED REFLECTIVE PAVEMENT MARKERS IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKING SHALL BE AS DIRECTED BY THE ENGINEER.
13	PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SRUFACES.
14	THE CONTRACTOR SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO BEGINNING WORK
15	THE RESIDENT ENGINEER SHALL CONTACT THE AREA TRAFFIC FIELD ENGINEER, PATRICE HARRIS AT PATRICE.HARRIS@ILLINOIS.GOV AT LEAST TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
16	ALL PAVEMENT PATCHING, CURB AND GUTTER REMOVAL AND REPLACEMENT, DRAINAGE ADJUSTMENT LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER
17	BUTT JOINTS WILL BE INSTALLED AT THE END OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE THE "BUTT JOINT AND HMA TAPER DETAILS" INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED
18	MATCH EXISTING PAVEMENT AT THE PROJECT LIMITS.
19	ALL DAMAMGE TO EXISTING PAVEMENT MARKINGS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACOTORS EXPENSE.
20	THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS IF UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY
21	THE LOCATION OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, AND ANY OTHER PUBLIC OR PRIVATE UTILITIES AS SHOWN ON THE PLANS IS APPROXIMATE AND THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR.
22	THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITES EVEN THOUGH THEY MIGHT NOT BE SHOWN ON THE PLANS IS APPROXIMATE AND THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR.
23	ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES WHICH OBSTRUCTS THE NATURAL FLOW OF WATER SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WILL NOT BE PAID FOR SEPERATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.
24	EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPOLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPERATE PAY ITEM HAS BEEN PROVIDED.
25	FOR WORK OUTSIDE THE LIMITS OF THE BRIDGE APPROACH PAVEMENT, ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS AND TIE BARS IN PAVEMENT, SHOULDERS, CURB, GUTTER, COMBINATION CURB AND GUTTER, MEDIAN, AND CHAIR SUPPORTS FOR CRC PAVEMENT,SHALL BE EPOXY COATED
26	CONTRACTOR SHALL MILL & PAVE UP TO R.R. CROSSING PANELS
27	PROPOSED SIDEWALK RAMPS SHALL CONFORM TO CURRENT ADA REQUIREMENTS AND APPLICABLE STATE HIGHWAY STANDARDS OR AS DETERMINED BY THE ENGINEER
28	THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT
29	THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN ACCESS AT ALL TIMES DURING CONSTRUCTION
30	LONGITUDINAL JOINT SEALANT SHALL BE APPLIED OVER POLYMERIZED LEVELING BINDER

REV. - MS

FILE NAME =	USER NAME = curryw	DESIGNED -	REVISED - 11-22-16 D.W.	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>127TH STREET - EAST OF 294 TO ARTESIAN AVE INDEX OF SHEETS, STATE STANDARDS &amp; GENERAL NOTES</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw\ill084EBIDINTEG.illinois.gov\PI\DOT Documents\DOT Offices\District 1\Projects\013418\BRAN\Note\Design\013418-shr-geo\note.dgn	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -			344	2018-010-R5-SW	COOK	61	2	
Default	PLOT DATE = 12/10/2018	DATE -	REVISED -			CONTRACT NO. 62G80					
						SCALE:	SHEET	OF	SHEETS	STA.	TO

URBAN

URBAN

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		0005 80% FED 20% STATE				
20200100	EARTH EXCAVATION	CU YD	49	49				
20200200	ROCK EXCAVATION	CU YD	3	3				
21101625	TOPSOIL FURNISH AND PLACE, 6"	SO YD	475	475				
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	9	9				
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	9	9				
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	9	9				
25200110	SODDING, SALT TOLERANT	SO YD	475	475				
25200200	SUPPLEMENTAL WATERING	UNIT	22	22				
30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SO YD	2836	2836				
35501308	HOT-MIX ASPHALT BASE COURSE, 6"	SO YD	14	14				
35501316	HOT-MIX ASPHALT BASE COURSE, 8"	SO YD	37	37				
35501317	HOT-MIX ASPHALT BASE COURSE, 8 1/4"	SO YD	567	567				
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	60878	60878				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	135.3	135.3				
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4, 75, N50	TON	3788	3788				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	850	850				
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	5.8	5.8				

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		0005 80% FED 20% STATE				
42001300	PROTECTIVE COAT	SO YD	3444.5	3444.5				
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SO YD	67	67				
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	7355	7355				
42400800	DETECTABLE WARNINGS	SO FT	659	659				
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SO YD	90190	90190				
44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	67	67				
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	1233	1233				
44000600	SIDEWALK REMOVAL	SO FT	7107	7107				
44003100	MEDIAN REMOVAL	SO FT	25523	25523				
44201803	CLASS D PATCHES, TYPE II, 13 INCH	SO YD	995	995				
44201807	CLASS D PATCHES, TYPE III, 13 INCH	SO YD	609	609				
44201809	CLASS D PATCHES, TYPE IV, 13 INCH	SO YD	826	826				
56104900	WATER VALVES 6"	EACH	2	2				
56105000	WATER VALVES 8"	EACH	2	2				
56109210	WATER VALVES TO BE ADJUSTED	EACH	3	3				REV. - MS
56500600	DOMESTIC WATER SERVICE BOXES TO BE ADJUSTED	EACH	3	3				
56500800	DOMESTIC WATER SERVICE BOXES	EACH	2	2				* = SPECIALTY ITEMS

URBAN

URBAN

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		0005 80% FED 20% STATE					
60208230	CATCH BASINS, TYPE C, TYPE 23 FRAME AND GRATE	EACH	5	5					
60250400	CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID	EACH	1	1					
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	1	1					
60255800	MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	1	1					
60257900	MANHOLES TO BE RECONSTRUCTED	EACH	2	2					
60262700	INLETS TO BE RECONSTRUCTED	EACH	3	3					
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	17	17					
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	33	33					
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6. 12	FOOT	733	733					
60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6. 24	FOOT	500	500					
60620000	CONCRETE MEDIAN, TYPE SB-6. 24	SO FT	21526.6	21526.6					
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	49	49					
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	5	5					

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		0005 80% FED 20% STATE					
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	LSUM	1	1					
* 66901002	ON-SITE MONITORING OF REGULATED SUBSTANCES	CAL DA	10	10					
* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	LSUM	1	1					
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	12	12					
67100100	MOBILIZATION	L SUM	1	1					
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1					
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1					
70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	L SUM	1	1					
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1					
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1					
70300100	SHORT TERM PAVEMENT MARKING	FOOT	21575	21575					REV. - MS
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	7192	7192					* = SPECIALTY ITEMS

URBAN

URBAN

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		0005 80% FED 20% STATE					
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	1452	1452					
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	67856	67856					
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	6700	6700					
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	1021	1021					
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	912	912					
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	10788	10788					
72400200	REMOVE SIGN PANEL ASSEMBLY - TYPE B	EACH	1	1					
72400310	REMOVE SIGN PANEL - TYPE 1	SO FT	7	7					
* 72400600	RELOCATE SIGN PANEL ASSEMBLY - TYPE B	EACH	1	1					
* 72400710	RELOCATE SIGN PANEL - TYPE 1	SO FT	7	7					
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	1452	1452					
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	67856	67856					
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	6700	6700					

SUMMARY OF QUANTITIES			TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		0005 80% FED 20% STATE					
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1021	1021					
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	912	912					
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	1324	1324					
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	1324	1324					
* 85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	7	7					
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	2753	2753					
* 89500400	RELOCATE EXISTING PEDESTRIAN PUSH-BUTTON	EACH	14	14					
* 89502376	REBUILD EXISTING HANDHOLE	EACH	7	7					
X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1	1					
X0325207	TELEVISION INSPECTION OF SEWER	FOOT	250	250					
X0327611	REMOVE AND REINSTALL BRICK PAVER	SO FT	64	64					
X4060004	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, N80	TON	8838.6	8838.6					

\* = SPECIALTY ITEMS



### HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS (%) @ Ndes	OMP
-PAVEMENT RESURFACING-		
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5 N80	3.5% @ 80 GYR.	PFP
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	3.5% @ 50 GYR.	OCP
-HMA BASE COURSE WIDENING-		
HOT-MIX ASPHALT BASE COURSE, IL-19.0, N90	4% @ 90 GYR.	QC/OA
-PATCHING-		
CLASS D PATCH (HMA BINDER IL-19 mm)	4% @ 70 GYR.	QC/OA
-DRIVEWAYS-		
HMA SURFACE COURSE, MIX "D", N50 (IL 9.5mm), 2"	4% @ 50 GYR.	QC/OA
HMA BASE COURSE (HMA BINDER IL-19 mm); PE - 6", CE -8"	4% @ 50 GYR.	QC/OA

**OMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/OA);  
QUALITY CONTROL FOR PERFORMANCE (OCP); PAY FOR PERFORMANCE (PFP)**

#### LEGEND (EXISTING)

- Ⓐ EXIST. PCC BASE COURSE, 9''(±)
- Ⓑ EXIST. HOT-MIX ASPHALT SURFACE COURSE (BEFORE MILLING), 1 1/2''(±)
- Ⓒ EXIST. CONCRETE CURB AND GUTTER
- Ⓓ EXIST. SUB-BASE GRANULAR MATERIAL TYPE B (4'')
- Ⓔ EXIST. CONC. MEDIAN 10''
- Ⓕ EXIST. HOT-MIX ASPHALT BINDER COURSE (BEFORE MILLING), 4 1/2''(±)

#### LEGEND (PROPOSED)

- ① PROP. HOT-MIX ASPHALT SURFACE REMOVAL - 2 1/2''
- ② PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5 N80, 1 3/4''
- ③ PROP. POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 3/4''
- ④ PROP. CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (LOCATIONS TO BE DETERMINED BY THE ENGINEER)
- \*⑤ CONCRETE MEDIAN REMOVAL AND REPLACEMENT STA. 80+00 TO STA. 96+00
- \*⑥ AGGREGATE SUBGRADE IMPROVEMENT 12'' (TO BE DETERMINED BY THE ENGINEER)
- ⑦ CONCRETE MEDIAN REMOVAL STA. 107+67 TO STA. 113+33
- ⑧ PROP. HMA BASE COURSE, IL-19.0, N90, 8 1/4''

#### HMA MIXTURE NOTES:

QUALITY MANAGEMENT PROGRAM (OMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQYD/IN

THE "AC TYPE" FOR POLYMERIZED HMA MIXTURES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

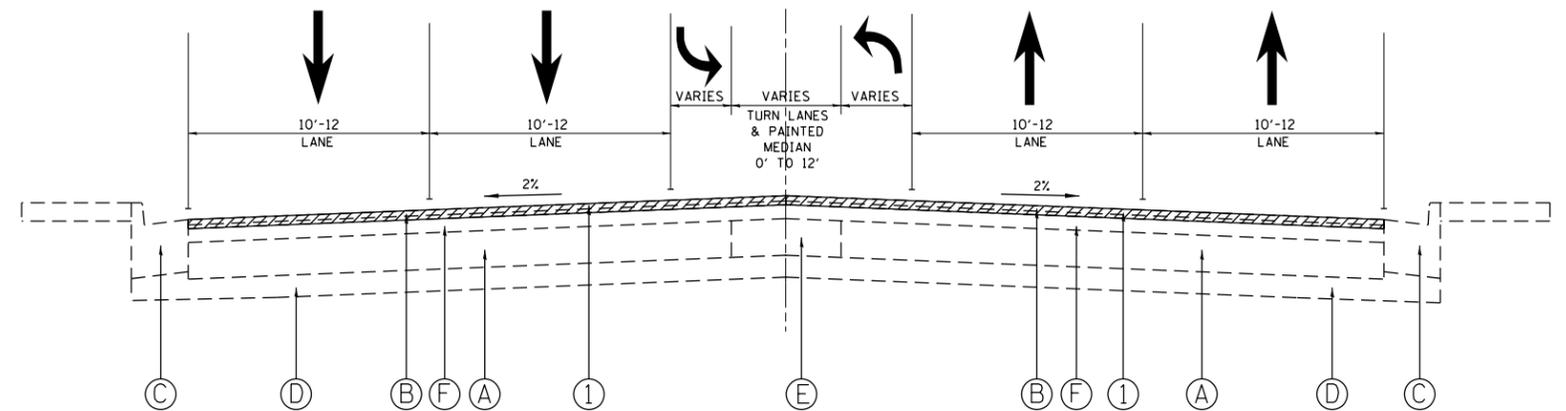
FOR USE OF RECYCLED MATERIALS SEE DISTRICT ONE SPECIAL PROVISIONS

MILLING SHALL BE DONE PRIOR TO PATCHING

#### NOTE:

WHERE EXISTING ASPHALT OVERLAYS THE EXISTING CONCRETE GUTTER FLAG, PLEASE SEE GENERAL NOTES: NOTE NO. 5

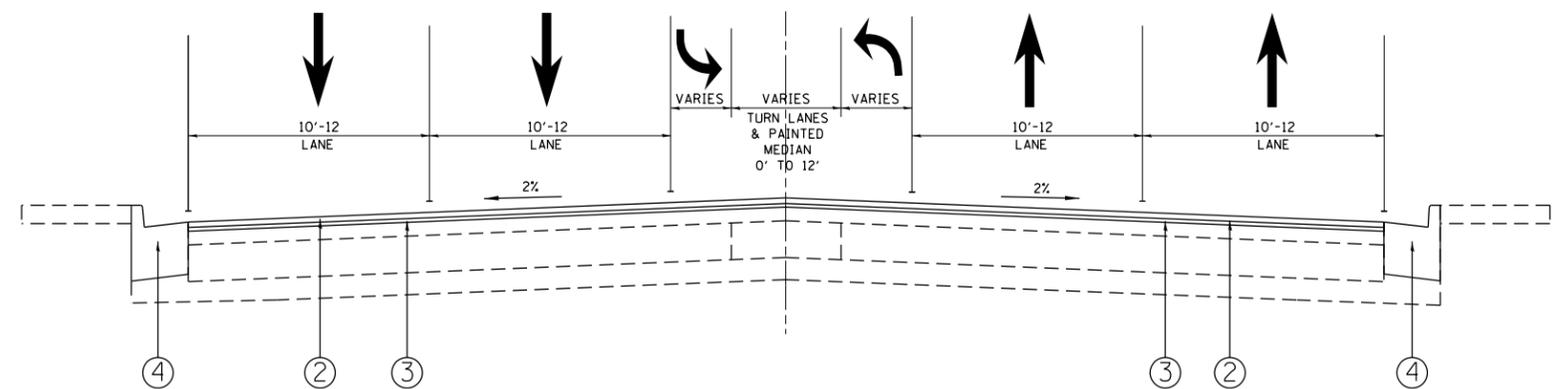
EXISTING SUB-BASE GRANULAR MATERIAL SHALL BE RE-USED ONSITE IN AGGREGATE SUB-GRADE IMPROVEMENT (AS DIRECTED BY THE ENGINEER)



EXISTING TYPICAL SECTION

127TH ST. KEDZIE AVE.

STATION: 20+00 TO 35+00      STATION: 29+92 TO 37+10  
 50+19 TO 80+00  
 96+00 TO 107+67  
 113+33 TO 127+39  
 143+00 TO 159+09



PROPOSED TYPICAL SECTION

127TH ST. KEDZIE AVE.

STATION: 20+00 TO 35+00      STATION: 29+92 TO 37+10  
 50+19 TO 80+00  
 96+00 TO 107+67  
 113+33 TO 127+39  
 143+00 TO 159+09

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS  
127TH STREET(BURR OAK AVENUE) EAST OF I-294 TO ARTESIAN AVENUE

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2018-010-R5-SW	COOK	61	7
CONTRACT NO. 62G80				

SCALE: SHEET OF SHEETS STA. TO STA.

ILLINOIS FED. AID PROJECT

MODEL: Default  
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PROJECT: 127th Street  
OFFICE: DISTRICT 1  
PROJECT NO: 127th Street  
DATE: 1/24/2019

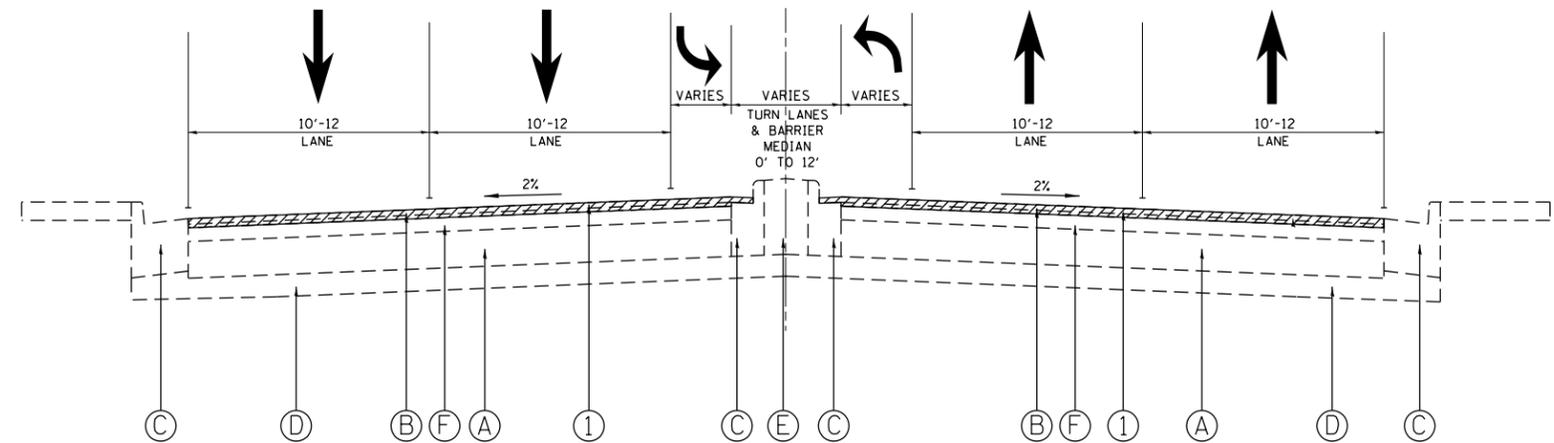
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	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 1/24/2019	DATE -	REVISED -

LEGEND (EXISTING)

- Ⓐ EXIST. PCC BASE COURSE, 9''(±)
- Ⓑ EXIST. HOT-MIX ASPHALT SURFACE COURSE (BEFORE MILLING), 1 1/2''(±)
- Ⓒ EXIST. CONCRETE CURB AND GUTTER
- Ⓓ EXIST. SUB-BASE GRANULAR MATERIAL TYPE B (4'')
- Ⓔ EXIST. CONC. MEDIAN 10''
- Ⓕ EXIST. HOT-MIX ASPHALT BINDER COURSE (BEFORE MILLING), 4 1/2''(±)

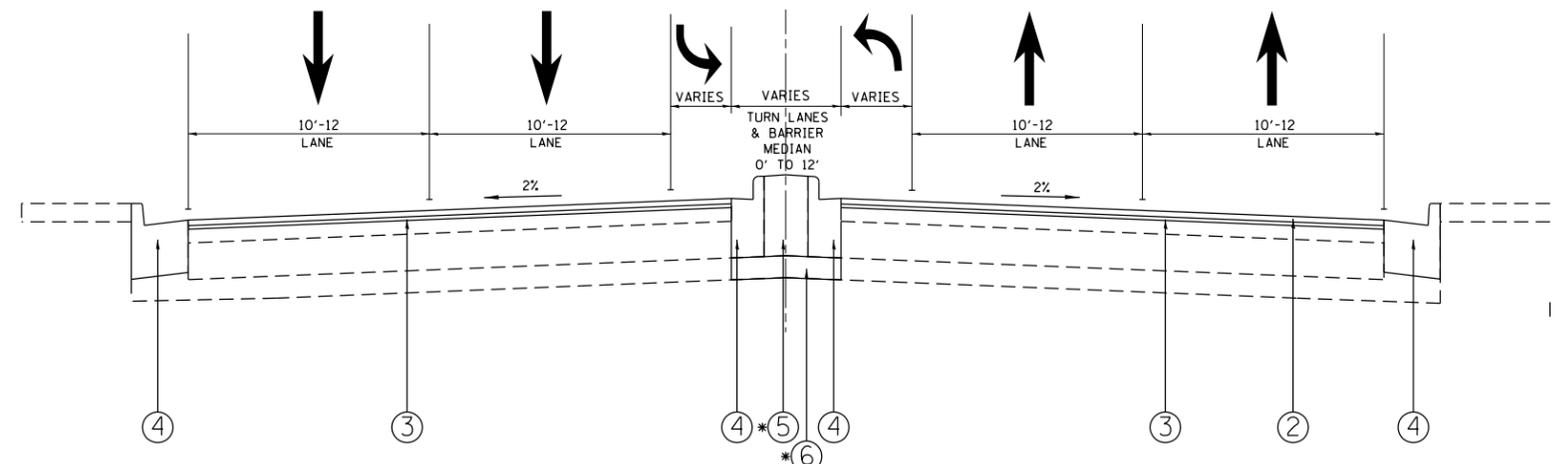
LEGEND (PROPOSED)

- ① PROP. HOT-MIX ASPHALT SURFACE REMOVAL - 2 1/2''
- ② PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5 N80, 1 3/4''
- ③ PROP. POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 3/4''
- ④ PROP. CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (LOCATIONS TO BE DETERMINED BY THE ENGINEER)
- \*⑤ CONCRETE MEDIAN REMOVAL AND REPLACEMENT STA. 80+00 TO STA. 96+00
- \*⑥ AGGREGATE SUBGRADE IMPROVEMENT 12'' (TO BE DETERMINED BY THE ENGINEER)
- ⑦ CONCRETE MEDIAN REMOVAL STA. 107+67 TO STA. 113+33
- ⑧ PROP. HMA BASE COURSE, IL-19.0, N90, 8 1/4''



EXISTING TYPICAL SECTION  
127TH ST.

STATION:  
80+00 TO 96+00



PROPOSED TYPICAL SECTION  
127TH ST.

STATION:  
80+00 TO 96+00

**NOTE:**

WHERE EXISTING ASPHALT OVERLAYS THE EXISTING CONCRETE GUTTER FLAG, PLEASE SEE GENERAL NOTES: NOTE NO. 5

EXISTING SUB-BASE GRANULAR MATERIAL SHALL BE RE-USED ONSITE IN AGGREGATE SUB-GRADE IMPROVEMENT (AS DIRECTED BY THE ENGINEER)

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PLOT DATE = 1/24/2019	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

TYPICAL SECTIONS	
127TH STREET(BURR OAK AVENUE) EAST OF I-294 TO ARTESIAN AVENUE	
SCALE:	SHEET OF SHEETS STA. TO STA.

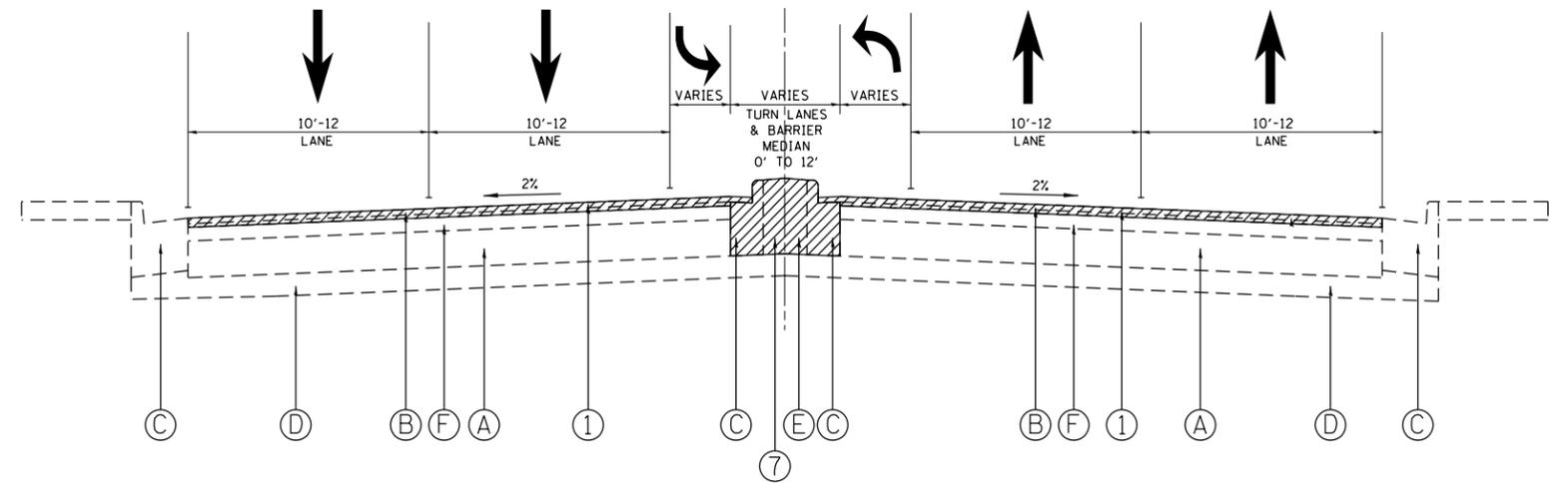
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2018-010-R5-SW	COOK	61	8
CONTRACT NO. 62G80				
ILLINOIS FED. AID PROJECT				

LEGEND (EXISTING)

- Ⓐ EXIST. PCC BASE COURSE, 9''(±)
- Ⓑ EXIST. HOT-MIX ASPHALT SURFACE COURSE (BEFORE MILLING), 1½''(±)
- Ⓒ EXIST. CONCRETE CURB AND GUTTER
- Ⓓ EXIST. SUB-BASE GRANULAR MATERIAL TYPE B (4'')
- Ⓔ EXIST. CONC. MEDIAN 10''
- Ⓕ EXIST. HOT-MIX ASPHALT BINDER COURSE (BEFORE MILLING), 4½''(±)

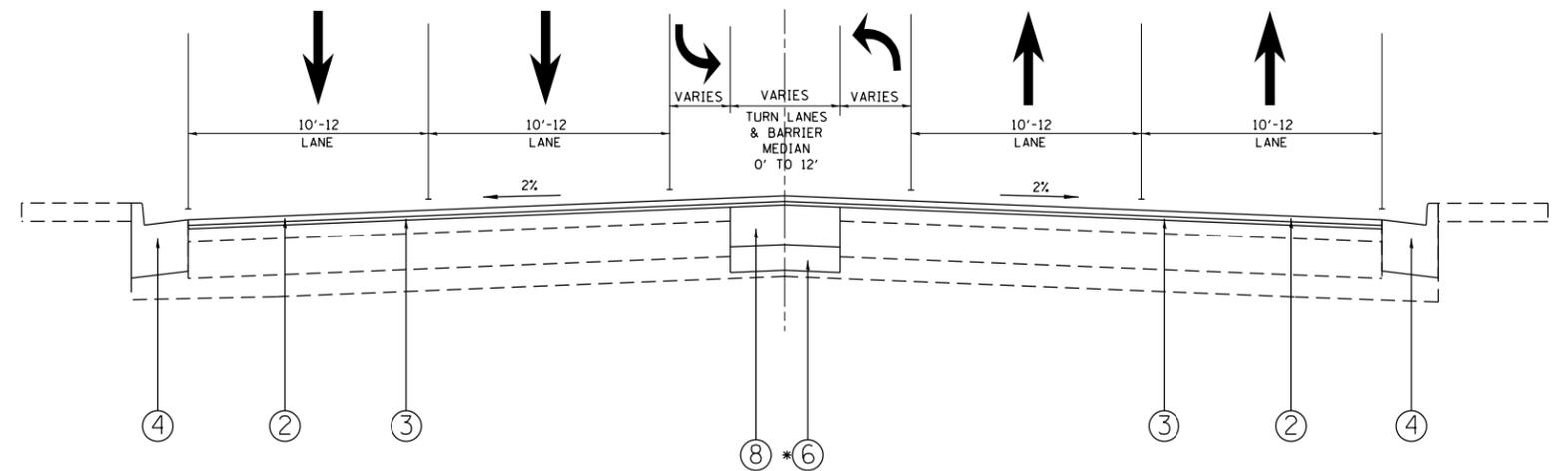
LEGEND (PROPOSED)

- ① PROP. HOT-MIX ASPHALT SURFACE REMOVAL - 2½''
- ② PROP. POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5 N80, 1 ¾''
- ③ PROP. POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, ¾''
- ④ PROP. CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (LOCATIONS TO BE DETERMINED BY THE ENGINEER)
- \*⑤ CONCRETE MEDIAN REMOVAL AND REPLACEMENT STA. 80+00 TO STA. 96+00
- \*⑥ AGGREGATE SUBGRADE IMPROVEMENT 12'' (TO BE DETERMINED BY THE ENGINEER)
- ⑦ CONCRETE MEDIAN REMOVAL STA. 107+67 TO STA. 113+33
- ⑧ PROP. HMA BASE COURSE, IL-19.0, N90, 8¼''



EXISTING TYPICAL SECTION  
127TH ST.

STATION:  
107+67 TO 113+33



PROPOSED TYPICAL SECTION  
127TH ST.

STATION:  
107+67 TO 113+33

**NOTE:**

WHERE EXISTING ASPHALT OVERLAYS THE EXISTING CONCRETE GUTTER FLAG, PLEASE SEE GENERAL NOTES: NOTE NO. 5

EXISTING SUB-BASE GRANULAR MATERIAL SHALL BE RE-USED ONSITE IN AGGREGATE SUB-GRADE IMPROVEMENT (AS DIRECTED BY THE ENGINEER)

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PLOT DATE = 1/24/2019	DATE -	REVISED -

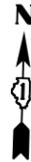
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS  
127TH STREET(BURR OAK AVENUE) EAST OF I-294 TO ARTESIAN AVENUE**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2018-010-R5-SW	COOK	61	9
CONTRACT NO. 62G80				
ILLINOIS FED. AID PROJECT				



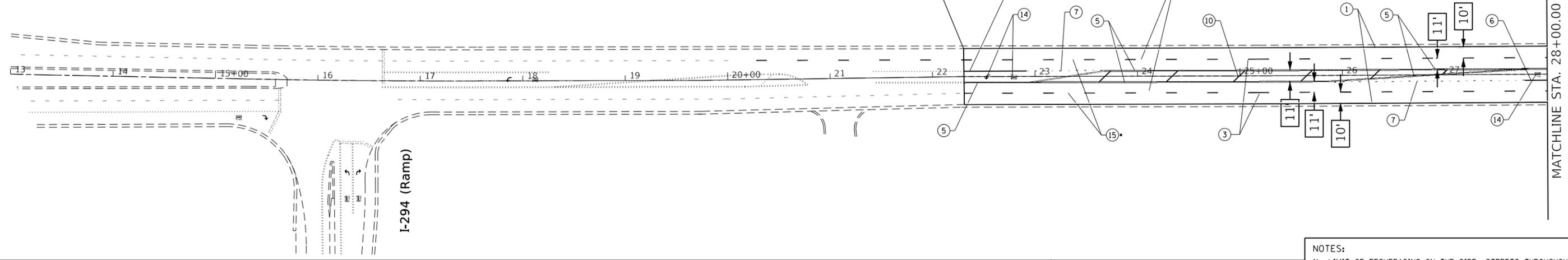


**PROJECT BEGINS  
STA. 22 + 31**

BURR OAK  
CEMETARY

**127TH STREET  
(BURR OAK AVENUE)**

HMA SURFACE REMOVAL, 2 1/2"  
POLYMERIZED LEVELING BINDER (MACHINE METHOD),  
1L-4.75, N50 3/4"  
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE,  
STONE MATRIX ASPHALT, 9.5, N80 1 3/4"



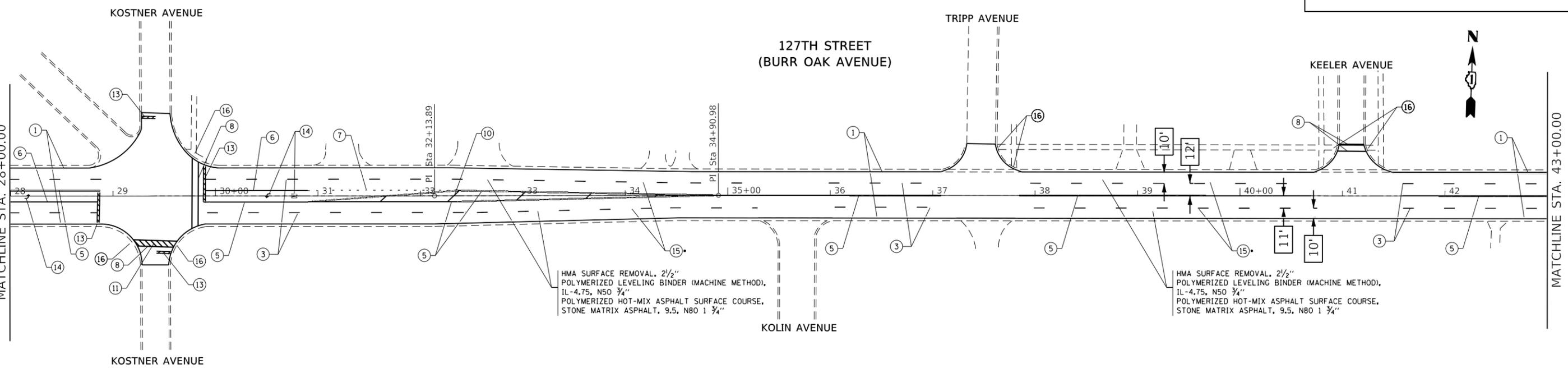
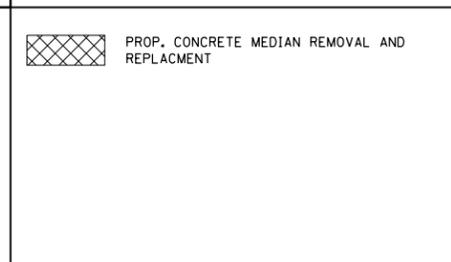
MATCHLINE STA. 28+00.00

**PAVEMENT MARKING LEGEND**

**ROADWAY LEGEND**

- NOTES:**
- 1) LIMIT OF RESURFACING ON THE SIDE- STREETS THROUGHOUT THE PROJECT SHALL BE TO THE RADIUS RETURN, UNLESS OTHERWISE SHOWN ON THE PLANS, OR AS DIRECTED BY THE RESIDENT ENGINEER / TECHNICIAN.
  - 2) ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC PAVEMENT MARKINGS (OF THE EXTRUDED TYPE).
  - 3) ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH TC-13 "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" STANDARD DETAIL.
  - 4) ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH TC-11 "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" STANDARD DETAIL.
  - 5) FOR PROPOSED CURB RAMP IMPROVEMENTS SEE "CURB RAMP FOR SIDEWALKS" PLANS.
  - 6) MEDIAN BETWEEN STA. 107+66 TO STA. 113+33 TO BE REMOVED AND REPLACED WITH HMA PAVEMENT, SEE TYPICAL SECTIONS.

- |   |  |  |
|---|--|--|
| ① THERMOPLASTIC PVMT. MARKING 4", LINE, WHITE (TYP.)                  | ⑧ THERMOPLASTIC PVMT. MARKING 6", CROSSWALK-PEDESTRIAN, WHITE (TYP.)<br>-SEE DISTRICT DETAIL TC-13 FOR SPACING | ⑬ THERMOPLASTIC PVMT. MARKING 24", STOP BAR, WHITE (TYP.)  |
| ② THERMOPLASTIC PVMT. MARKING 4", LINE, YELLOW (TYP.)                 | ⑨ THERMOPLASTIC PVMT. MARKING 8", LINE, WHITE (TYP.)   | ⑭ THERMOPLASTIC PVMT. MARKING, LETTERS & SYMBOLS, WHITE (TYP.)   |
| ③ THERMOPLASTIC PVMT. MARKING 4", 10' LINE - 30' SPACE, WHITE (TYP.)  | ⑩ THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, YELLOW (TYP.)<br>- SEE DISTRICT DETAIL TC-13 FOR SPACING         | ⑮ RAISED REFLECTIVE PAVEMENT MARKER (TYP.)   |
| ④ THERMOPLASTIC PVMT. MARKING 4", 10' LINE - 30' SPACE, YELLOW (TYP.) | ⑪ THERMOPLASTIC PVMT. MARKING 12", DIAGONALS, WHITE (TYP.)<br>- SEE DISTRICT DETAIL TC-13 FOR SPACING          | ⑯ ADA RAMP IMPROVEMENT LOCATION  |
| ⑤ THERMOPLASTIC PVMT. MARKING 4", DOUBLE YELLOW (TYP.)                | ⑫ THERMOPLASTIC PVMT. MARKING 12", CROSSWALK-SCHOOL, WHITE (TYP.)<br>- SEE DISTRICT DETAIL TC-13 FOR SPACING   | ⑰ THERMOPLASTIC PAVEMENT MARKING 6", R.R. DIAGONALS WHITE (TYP.)<br>-SEE DISTRICT DETAIL TC-23 FOR SPACING |
| ⑥ THERMOPLASTIC PVMT. MARKING 6", LINE, WHITE (TYP.)                  |  | ⑱ CONCRETE MEDIAN REMOVAL AND REPLACEMENT  |
| ⑦ THERMOPLASTIC PVMT. MARKING 6", 2' LINE - 6' SPACE, WHITE (TYP.)    |  | ⑲ THERMOPLASTIC PAVEMENT MARKING 12", CHEVRON, WHITE<br>-SEE TC-13 FOR SPACING                             |



MATCHLINE STA. 43+00.00

HMA SURFACE REMOVAL, 2 1/2"  
POLYMERIZED LEVELING BINDER (MACHINE METHOD),  
1L-4.75, N50 3/4"  
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE,  
STONE MATRIX ASPHALT, 9.5, N80 1 3/4"

HMA SURFACE REMOVAL, 2 1/2"  
POLYMERIZED LEVELING BINDER (MACHINE METHOD),  
1L-4.75, N50 3/4"  
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE,  
STONE MATRIX ASPHALT, 9.5, N80 1 3/4"

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USER NAME = curryw	DESIGNED BY ALLEN CURRY, P.E.	REVISED -
DRAWN -	REVISIONS -	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 1/24/2019	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLANS**  
**127TH STREET (BURR OAK AVENUE) EAST OF I-294 TO ARTESIAN AVENUE**  
SCALE: 1"= 50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 344	SECTION 2018-010-R5-SW	COUNTY COOK	TOTAL SHEETS 61	SHEET NO. 11
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62G80	

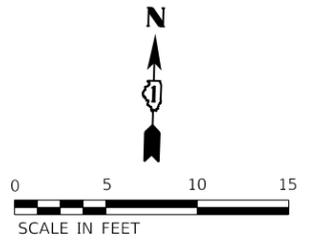




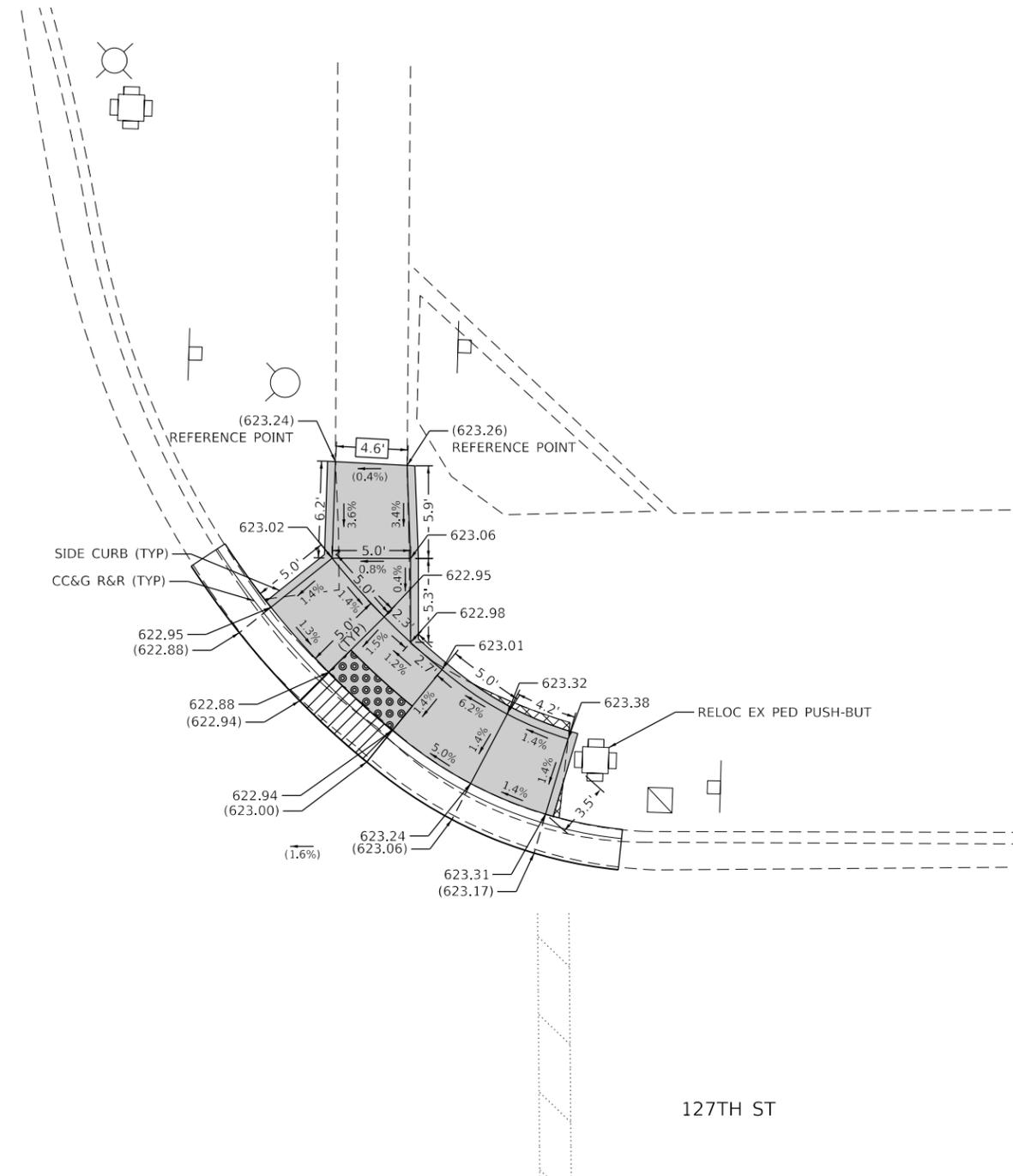








KOSTNER AVE



NOTE: REFER TO ROADWAY AND PAVEMENT MARKING PLANS FOR PROPOSED PAVEMENT MARKINGS.

REFERENCE BENCHMARK 39 EL 625.27 (NOT SHOWN)  
 BENCHMARK: SW BOLT ON FH  
 LOCATION: SW COR 127TH AND KOSTNER AVE

**LEGEND**

XX.XX'	EXISTING LENGTH
( )	EXISTING ELEVATION / SLOPE
	PORTLAND CEMENT CONCRETE SIDEWALK 5"
	DETECTABLE WARNINGS
	SIDEWALK REMOVAL REPLACE WITH TOPSOIL AND SOD
	DEPRESSED CURB AND GUTTER

MODEL: D:\dgn\11111111.DWG  
 FILE NAME: 20180111.DWG  
 USER: RGA  
 DATE: 11/21/2018  
 PROJECT: ADA RAMP AT 127TH ST AND KOSTNER AVE

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PLOT SCALE = 10.0000' / in.	CHECKED - RG	REVISED -
PLOT DATE = 11/21/2018	DATE - 11/21/2018	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

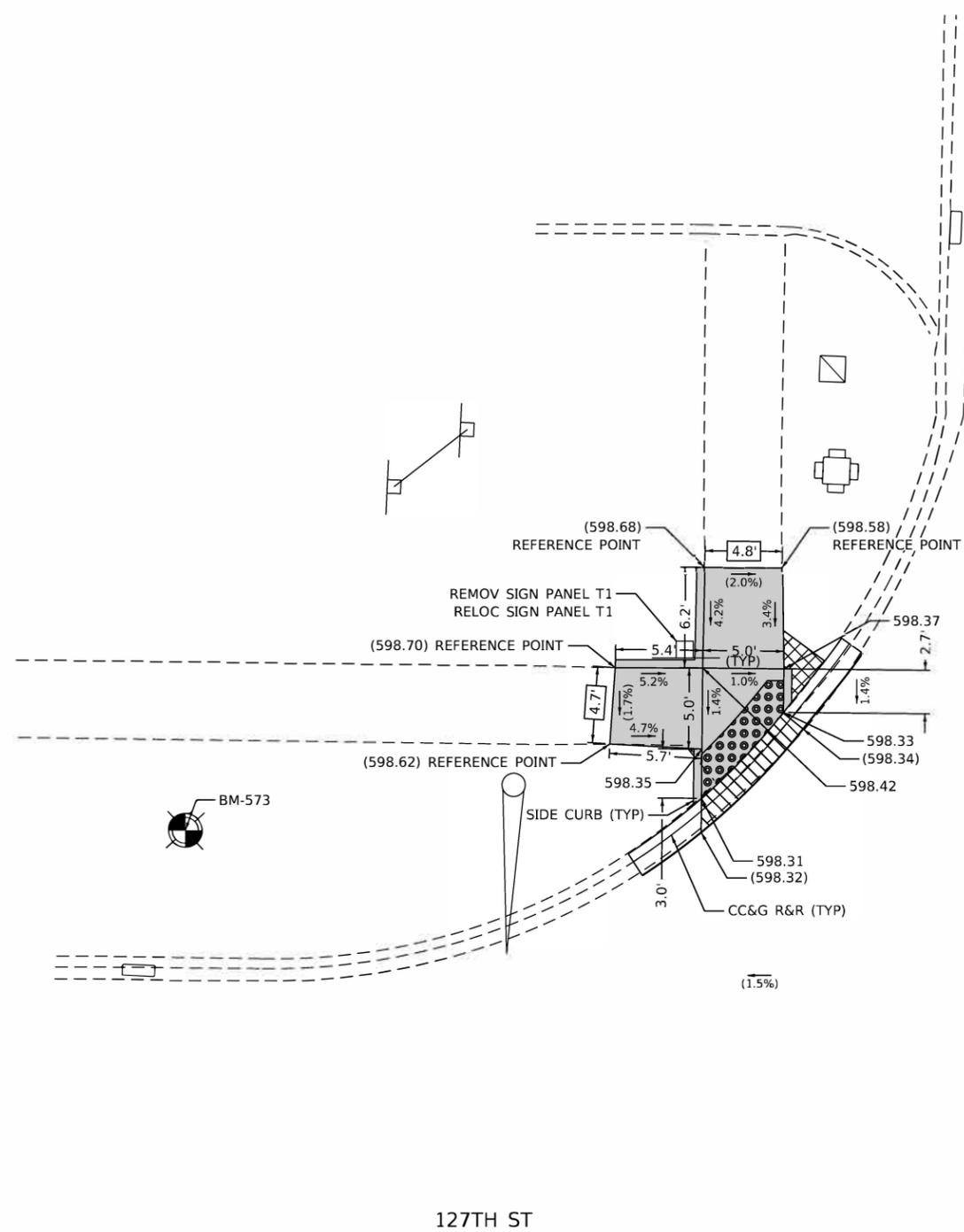
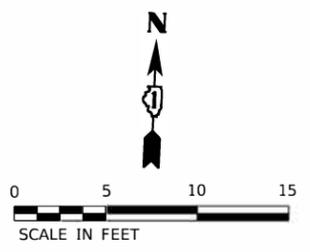
<b>ADA RAMP DETAILS 127TH ST AT KOSTNER AVE</b>			
SCALE:	SHEET	OF	SHEETS
	STA.		TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344A	2018-010-R5-SW	COOK	61	17
			CONTRACT NO. 62G80	
		ILLINOIS FED. AID PROJECT		





PULASKI RD



127TH ST

NOTE: REFER TO ROADWAY AND PAVEMENT MARKING PLANS FOR PROPOSED PAVEMENT MARKINGS.

REFERENCE BENCHMARK 573 EL 599.37  
 BENCHMARK: NE BOLT ON ST LIGHT  
 LOCATION: NW COR 127TH AND PULASKI RD

**LEGEND**

XX.XX'	EXISTING LENGTH
( )	EXISTING ELEVATION / SLOPE
[Solid Grey Box]	PORTLAND CEMENT CONCRETE SIDEWALK 5"
[Dotted Pattern Box]	DETECTABLE WARNINGS
[Cross-hatched Box]	SIDEWALK REMOVAL REPLACE WITH TOPSOIL AND SOD
[Diagonal Hatched Box]	DEPRESSED CURB AND GUTTER

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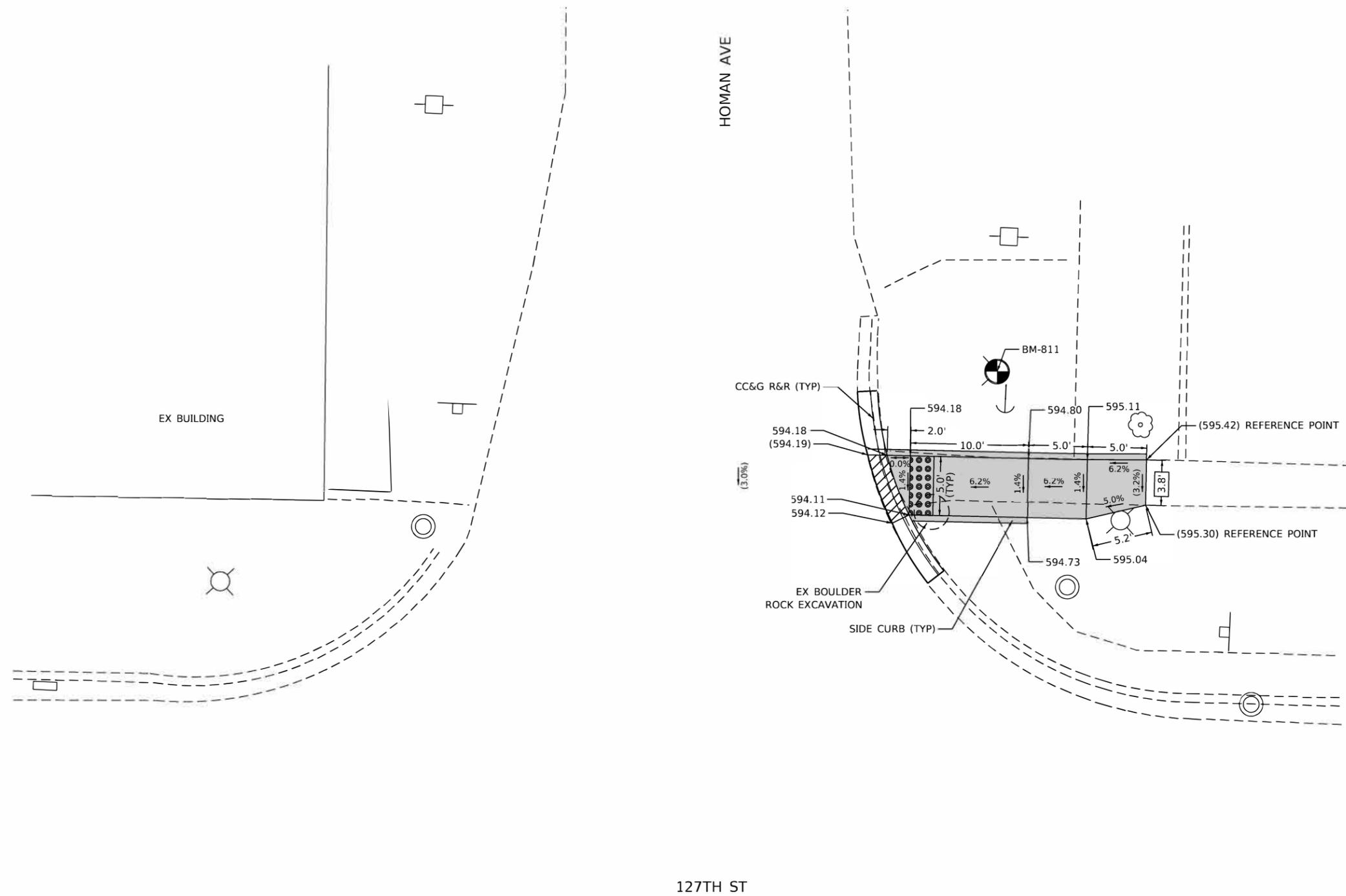
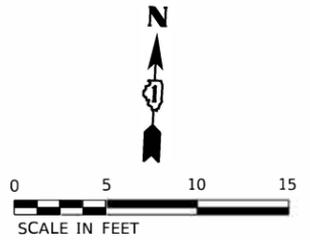
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**ADA RAMP DETAILS  
 127TH ST AT PULASKI RD**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344A	2018-010-R5-SW	COOK	61	20
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62G80	





127TH ST

NOTE: REFER TO ROADWAY AND PAVEMENT MARKING PLANS FOR PROPOSED PAVEMENT MARKINGS.

**LEGEND**

XX.XX'	EXISTING LENGTH
( )	EXISTING ELEVATION / SLOPE
[Solid Grey Box]	PORTLAND CEMENT CONCRETE SIDEWALK 5"
[Grid Pattern Box]	DETECTABLE WARNINGS
[Cross-hatch Pattern Box]	SIDEWALK REMOVAL REPLACE WITH TOPSOIL AND SOD
[Diagonal Line Pattern Box]	DEPRESSED CURB AND GUTTER

REFERENCE BENCHMARK 811 EL 596.56  
 BENCHMARK: NW BOLT ON FH  
 LOCATION: NW COR 127TH AND HOMAN AVE

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PLOT DATE = 11/21/2018	DATE - 11/21/2018	REVISED +

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

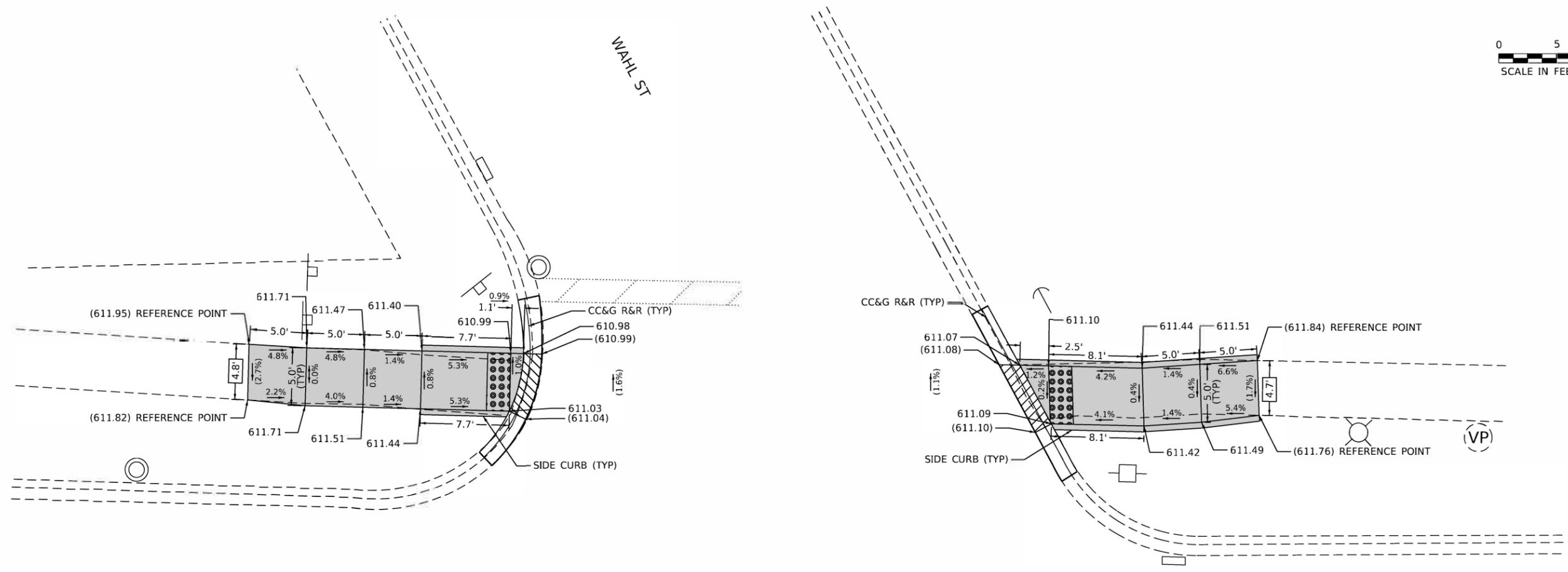
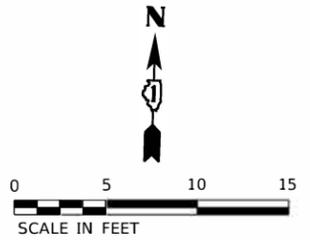
<b>ADA RAMP DETAILS        127TH ST AT HOMAN AVE</b>			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344A	2018-010-RS-SW	COOK	61	22
CONTRACT NO. 62G80				
ILLINOIS FED. AID PROJECT				









127TH ST

NOTE: REFER TO ROADWAY AND PAVEMENT MARKING PLANS FOR PROPOSED PAVEMENT MARKINGS.

**LEGEND**

XX.XX'	EXISTING LENGTH
( )	EXISTING ELEVATION / SLOPE
[Solid Grey Box]	PORTLAND CEMENT CONCRETE SIDEWALK 5"
[Dotted Box]	DETECTABLE WARNINGS
[Cross-hatched Box]	SIDEWALK REMOVAL REPLACE WITH TOPSOIL AND SOD
[Diagonal Line Box]	DEPRESSED CURB AND GUTTER

REFERENCE BENCHMARK 1426 EL 613.34 (NOT SHOWN)  
 BENCHMARK: SE BOLT ON FH  
 LOCATION: NW COR 127TH AND WAHL ST

MODEL: D:\dgn\11 DOT D1 127th Street ADA Ramp\11 DOT D1 127th Street ADA Ramp\11 DOT D1 127th Street ADA Ramp.dgn  
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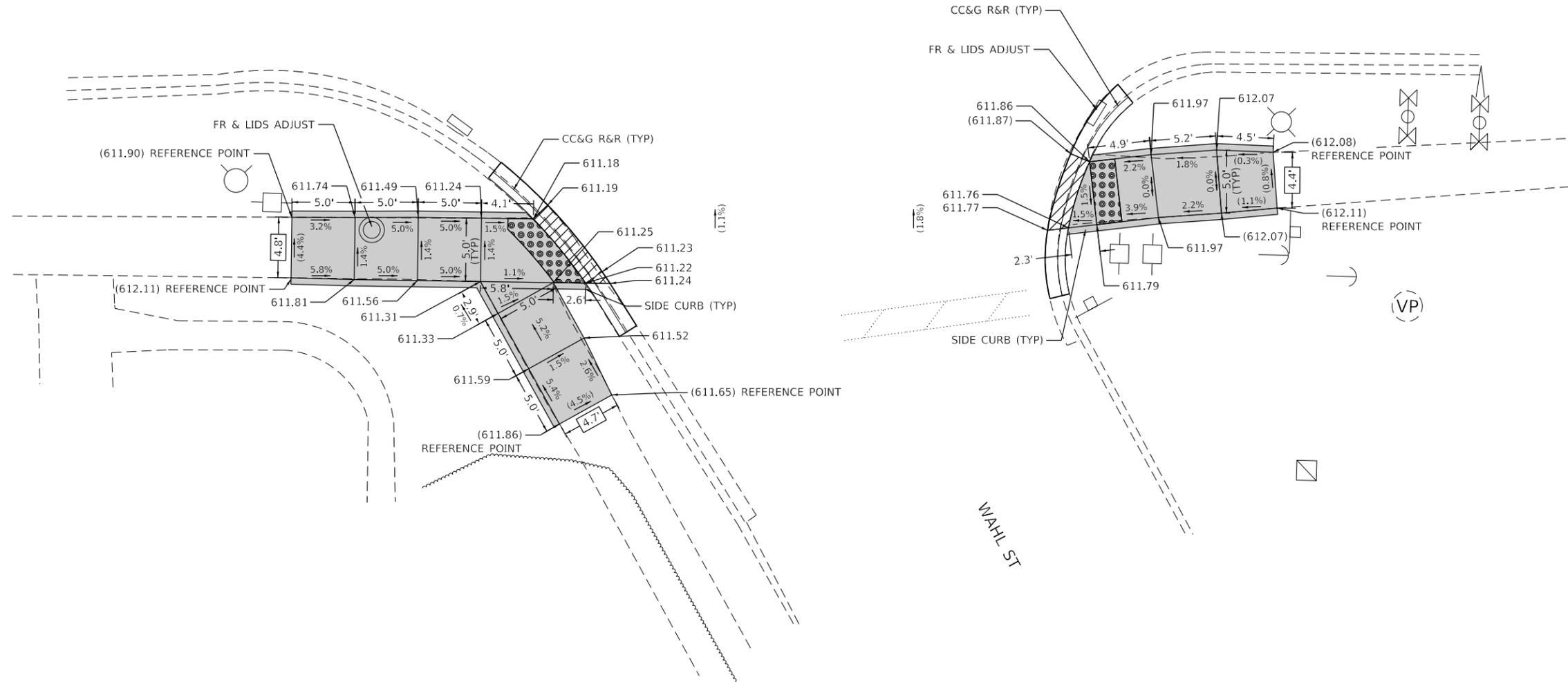
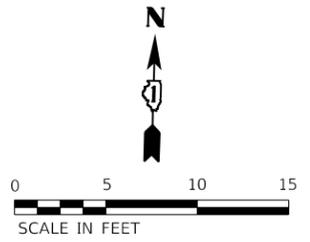
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ADA RAMP DETAILS  
127TH ST AT WAHL ST**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344A	2018-010-R5-SW	COOK	61	26
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62G80	

127TH ST



NOTE: REFER TO ROADWAY AND PAVEMENT MARKING PLANS FOR PROPOSED PAVEMENT MARKINGS.

**LEGEND**

XX.XX'	EXISTING LENGTH
( )	EXISTING ELEVATION / SLOPE
[Solid Grey Box]	PORTLAND CEMENT CONCRETE SIDEWALK 5"
[Grid Pattern Box]	DETECTABLE WARNINGS
[Cross-hatch Pattern Box]	SIDEWALK REMOVAL REPLACE WITH TOPSOIL AND SOD
[Diagonal Line Pattern Box]	DEPRESSED CURB AND GUTTER

REFERENCE BENCHMARK 1426 EL 613.34  
 BENCHMARK: SE BOLT ON FH  
 LOCATION: SE COR 127TH AND WAHL ST

MODEL: D:\p\11\127th Street ADA Ramps\DCN\Design\127th Street ADA Ramps\DCN\Detail\ADA-12.dgn

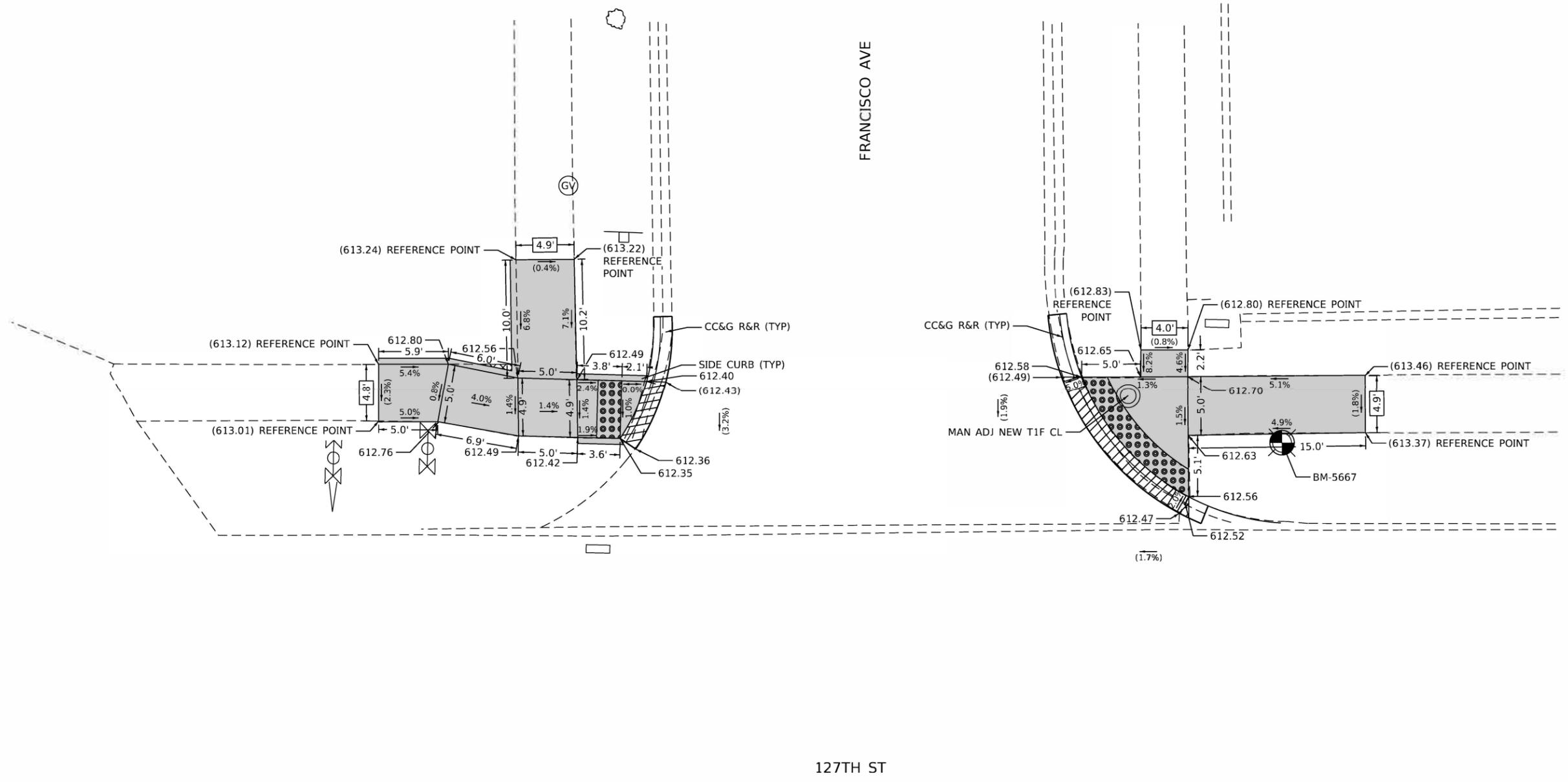
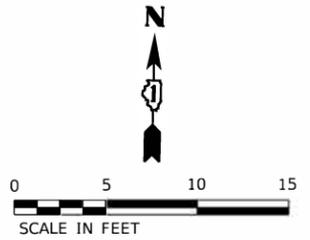
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	DRAWN - CS / AN	REVISED -
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PLOT DATE = 11/21/2018	DATE - 11/21/2018	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**ADA RAMP DETAILS  
 127TH ST AT WAHL ST**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344A	2018-010-R5-SW	COOK	61	27
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62G80	



NOTE: REFER TO ROADWAY AND PAVEMENT MARKING PLANS FOR PROPOSED PAVEMENT MARKINGS.

**LEGEND**

XX.XX'	EXISTING LENGTH
( )	EXISTING ELEVATION / SLOPE
[Solid Grey Box]	PORTLAND CEMENT CONCRETE SIDEWALK 5"
[Dotted Box]	DETECTABLE WARNINGS
[Cross-hatched Box]	SIDEWALK REMOVAL REPLACE WITH TOPSOIL AND SOD
[Diagonal-hatched Box]	DEPRESSED CURB AND GUTTER

REFERENCE BENCHMARK 5667 EL 613.41  
 BENCHMARK: NW BOLT OF LP BASE  
 LOCATION: NE COR 127TH AND FRANCISCO AVE

MODEL: D:\ref\11 DOT D1 127th Street ADA Ramp\DOT\Design\Pre\11\127th Street ADA Ramp\DOT\Design\Pre\11\127th Street ADA Ramp\DOT\Design\Pre\11\127th Street ADA Ramp.dgn  
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DRAWN - CS / AN	REVISIONS -	
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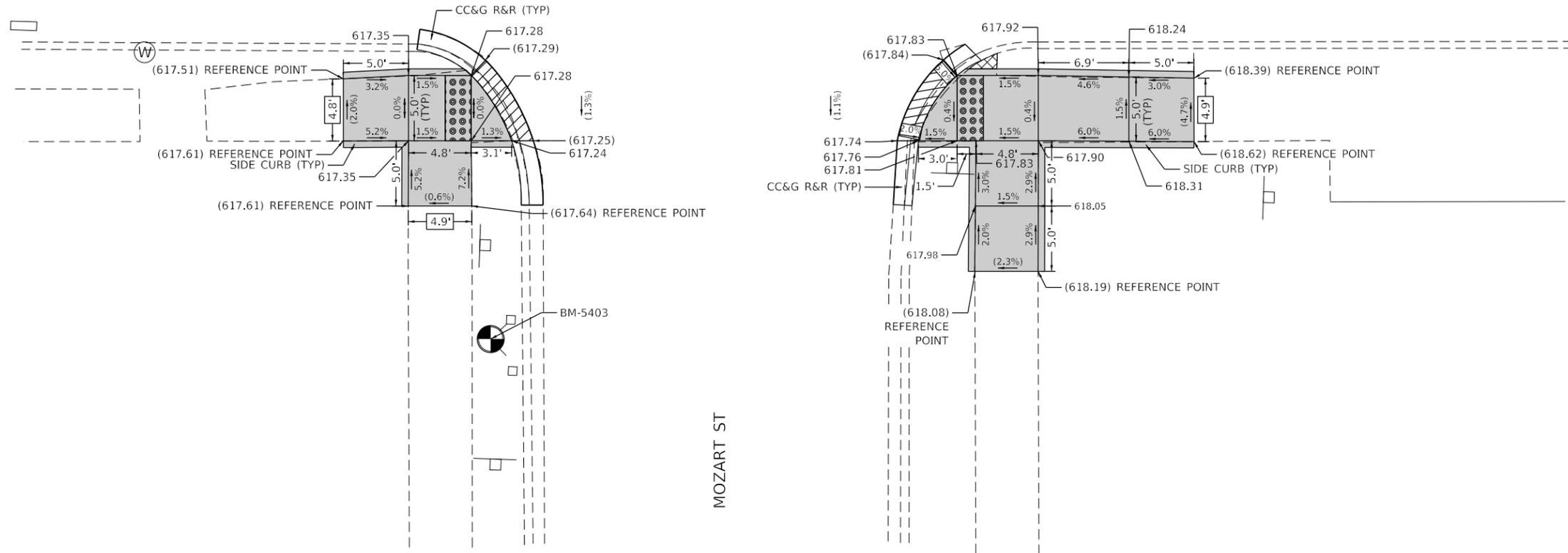
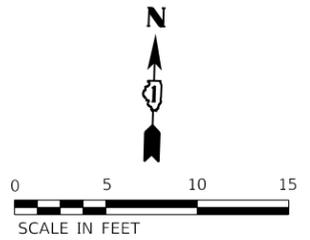
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**ADA RAMP DETAILS  
 127TH ST AT FRANCISCO AVE**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344A	2018-010-R5-SW	COOK	61	28
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62G80	

127TH ST



NOTE: REFER TO ROADWAY AND PAVEMENT MARKING PLANS FOR PROPOSED PAVEMENT MARKINGS.

REFERENCE BENCHMARK 5403 EL 618.65  
 BENCHMARK: SW BOLT ON FH  
 LOCATION: SW COR 127TH AND MOZART ST

**LEGEND**

- XX.XX' EXISTING LENGTH
- ( ) EXISTING ELEVATION / SLOPE
- PORTLAND CEMENT CONCRETE SIDEWALK 5"
- DETECTABLE WARNINGS
- ▨ SIDEWALK REMOVAL  
REPLACE WITH TOPSOIL AND SOD
- ▩ DEPRESSED CURB AND GUTTER

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**ADA RAMP DETAILS  
 127TH ST AT MOZART ST**

USER NAME = RGall	DESIGNED - CS / AN	REVISED -
PLOT SCALE = 10.0000' / in.	DRAWN - CS / AN	REVISED -
PLOT DATE = 11/21/2018	CHECKED - RG	REVISED -
	DATE - 11/21/2018	REVISED -

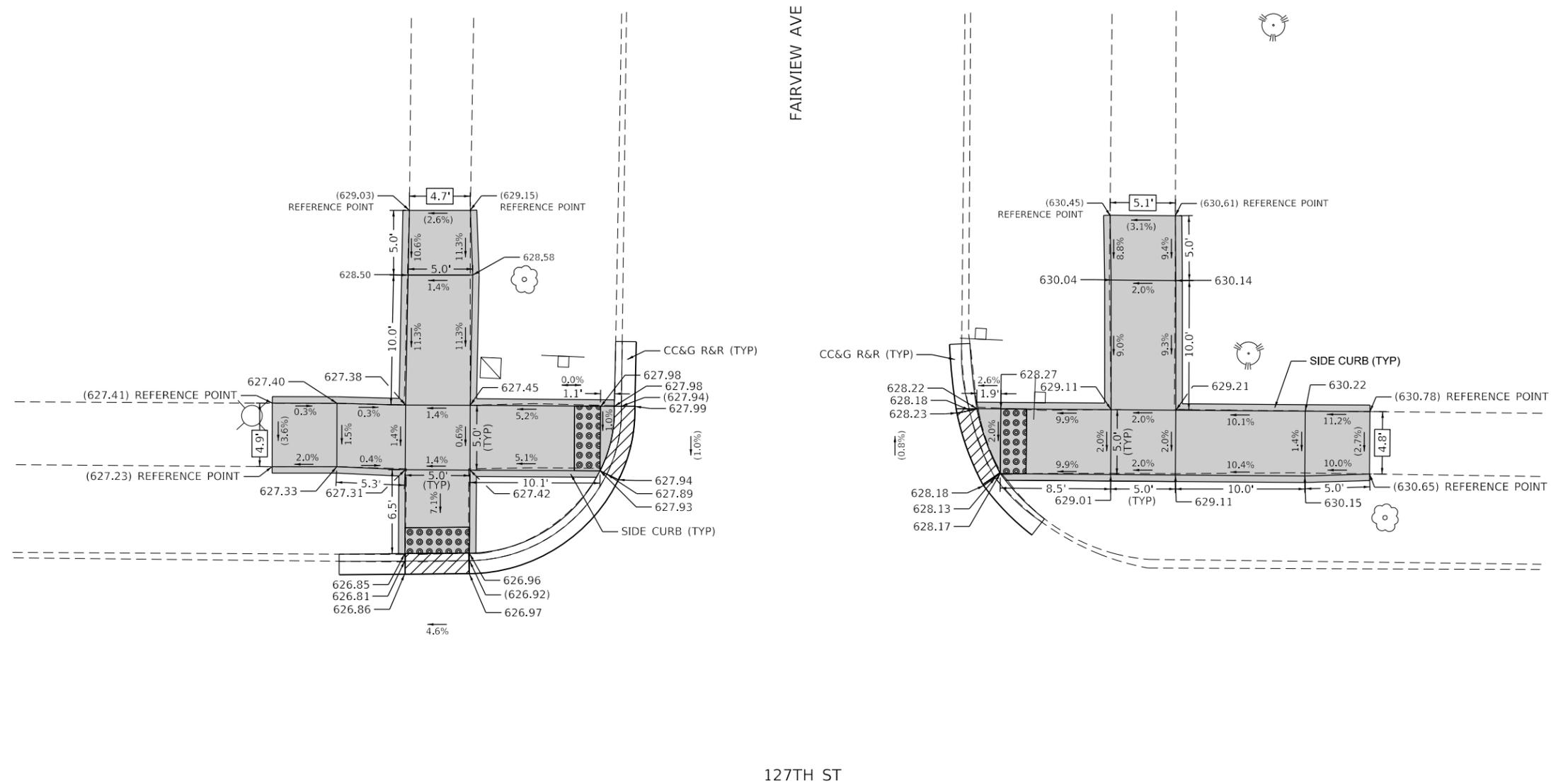
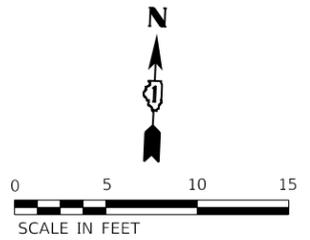
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344A	2018-010-R5-SW	COOK	61	29
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62G80	

MODEL: D:\p\h\11 DOT D1 PFB 183408 WO 11 127th Street ADA Ramps\DC\Design\PreDrawn\PreDrawn\183408-15.dgn  
 FILE NAME: 2018010-R5-SW







NOTE: REFER TO ROADWAY AND PAVEMENT MARKING PLANS FOR PROPOSED PAVEMENT MARKINGS.

REFERENCE BENCHMARK 4962 EL 630.80 (NOT SHOWN)  
 BENCHMARK: NW BOLT ON FH  
 LOCATION: NW COR 127TH AND FAIRVIEW AVE

**LEGEND**

XX.XX'	EXISTING LENGTH
( )	EXISTING ELEVATION / SLOPE
[Solid Grey Box]	PORTLAND CEMENT CONCRETE SIDEWALK 5"
[Dotted Box]	DETECTABLE WARNINGS
[Cross-hatched Box]	SIDEWALK REMOVAL REPLACE WITH TOPSOIL AND SOD
[Diagonal-hatched Box]	DEPRESSED CURB AND GUTTER

MODEL: D:\p\h\1  
 FILE NAME: 2013015.11 IDOT D1 PFB 163408 WO 11 127th Street ADA Ramps\DC\Design\Pre\Drawings\163408-18.dgn

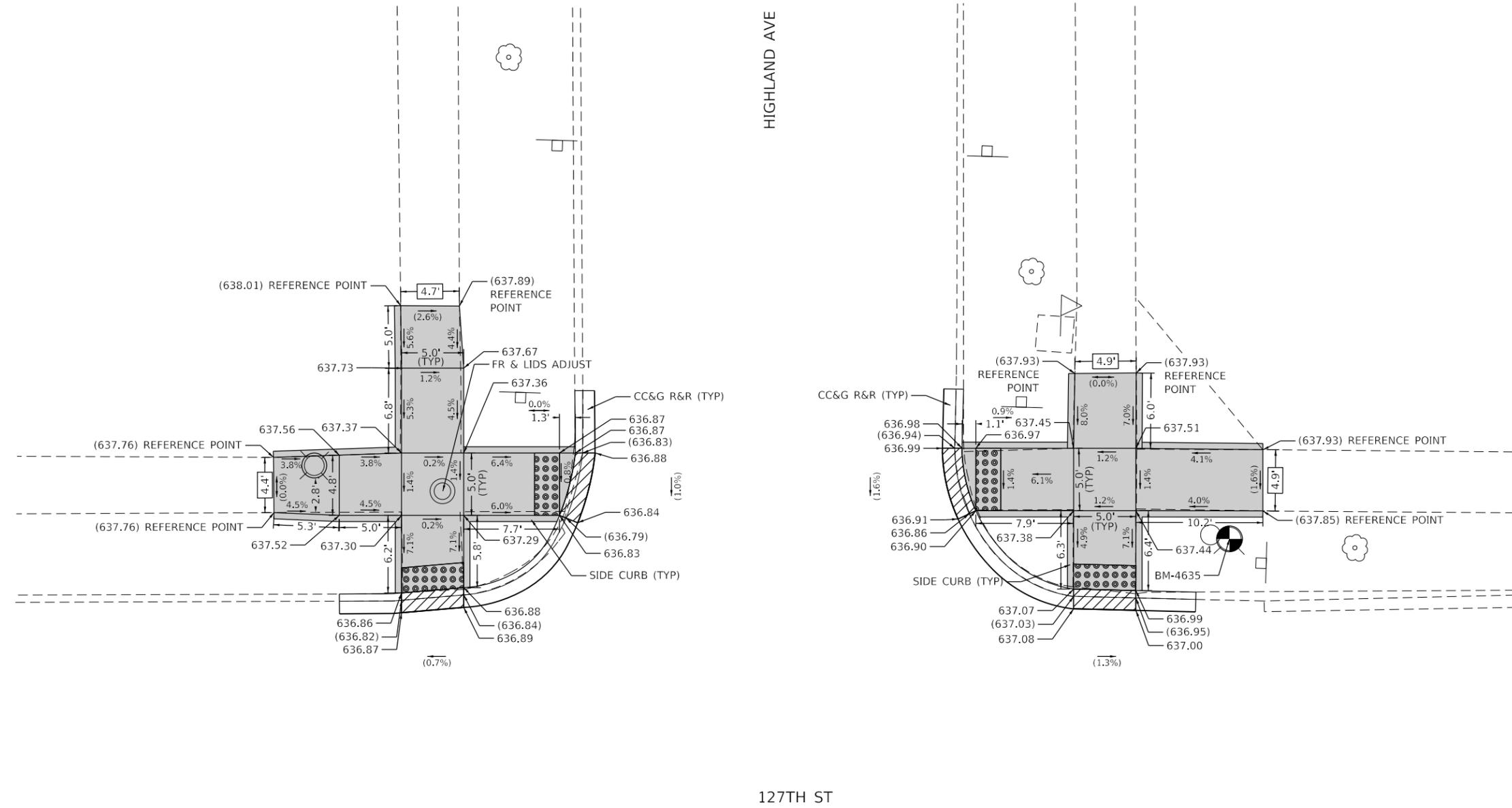
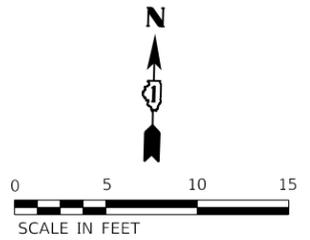
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**ADA RAMP DETAILS  
 127TH ST AT FAIRVIEW AVE**

USER NAME = RGall	DESIGNED - CS / AN	REVISED -
PLOT SCALE = 10.0000 ' / in.	DRAWN - CS / AN	REVISED -
PLOT DATE = 11/21/2018	CHECKED - RG	REVISED -
	DATE - 11/21/2018	REVISED -

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344A	2018-010-R5-SW	COOK	61	32
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62G80	



127TH ST

HIGHLAND AVE

NOTE: REFER TO ROADWAY AND PAVEMENT MARKING PLANS FOR PROPOSED PAVEMENT MARKINGS.

**LEGEND**

XX.XX'	EXISTING LENGTH
( )	EXISTING ELEVATION / SLOPE
[Solid Grey Box]	PORTLAND CEMENT CONCRETE SIDEWALK 5"
[Dotted Pattern Box]	DETECTABLE WARNINGS
[Cross-hatch Pattern Box]	SIDEWALK REMOVAL REPLACE WITH TOPSOIL AND SOD
[Diagonal Line Pattern Box]	DEPRESSED CURB AND GUTTER

REFERENCE BENCHMARK 4635 EL 639.05  
 BENCHMARK: NE BOLT ON FH  
 LOCATION: NE CORNER 127TH AND HIGHLAND AVE

MODEL: D:\p\11 DOT D1 PFB 163408.WD 11 127th Street ADA Ramps\DCN\Design\PreDrawn\PreDrawn\11127thStreetADA-19.dgn  
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USER NAME = RGall	DESIGNED - CS / AN	REVISED -
	DRAWN - CS / AN	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - RG	REVISED -
PLOT DATE = 11/21/2018	DATE - 11/21/2018	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

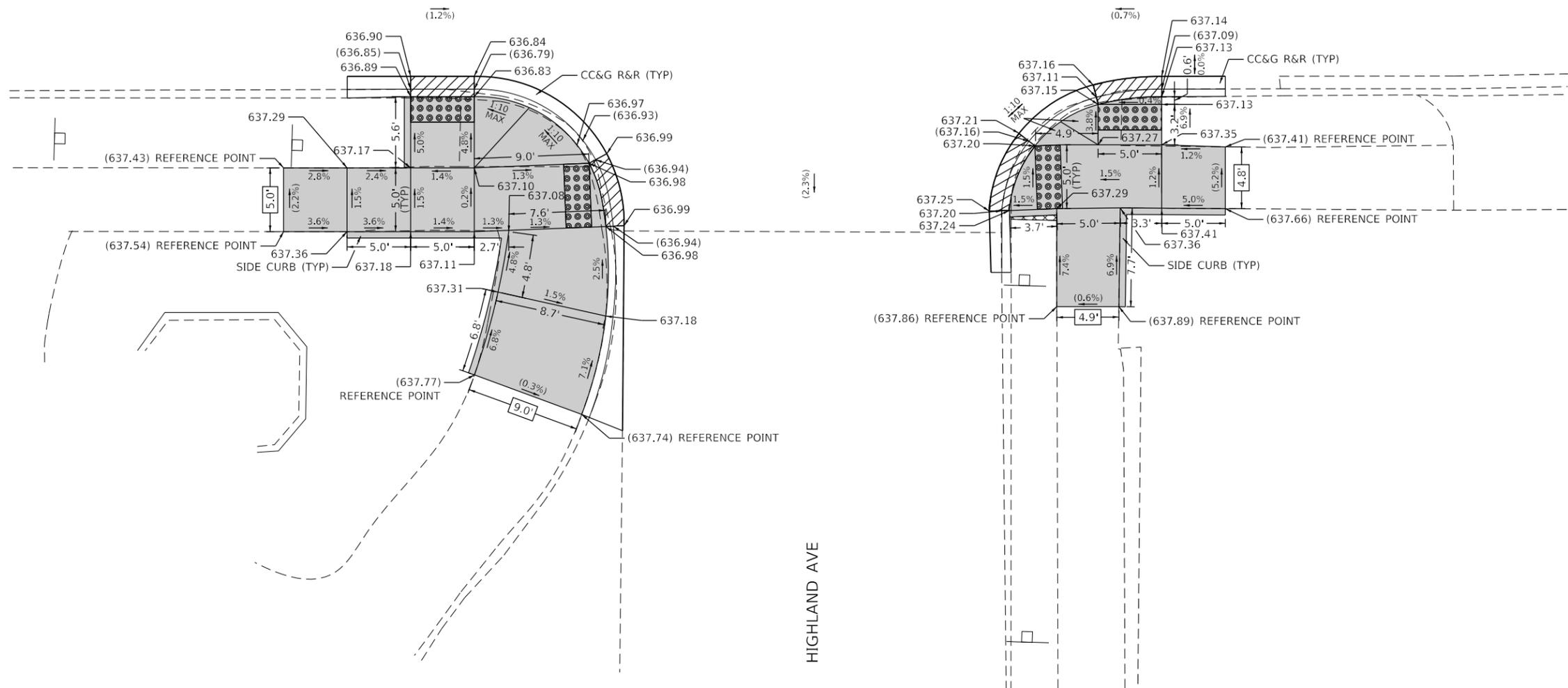
**ADA RAMP DETAILS  
 127TH ST AT HIGHLAND AVE**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344A	2018-010-R5-SW	COOK	61	33
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62G80	



127TH ST



NOTE: REFER TO ROADWAY AND PAVEMENT MARKING PLANS FOR PROPOSED PAVEMENT MARKINGS.

**LEGEND**

XX.XX'	EXISTING LENGTH
( )	EXISTING ELEVATION / SLOPE
	PORTLAND CEMENT CONCRETE SIDEWALK 5"
	DETECTABLE WARNINGS
	SIDEWALK REMOVAL REPLACE WITH TOPSOIL AND SOD
	DEPRESSED CURB AND GUTTER

REFERENCE BENCHMARK 4635 EL 639.05 (NOT SHOWN)  
 BENCHMARK: NE BOLT ON FH  
 LOCATION: NE COR 127TH AND HIGHLAND AVE

MODEL: D:\p\11\127th Street ADA Ramps\DCG\Design\11\127th Street ADA Ramps\DCG\Design\11\127th Street ADA Ramps\DCG\Detail\ADA-20.dgn  
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USER NAME = RGall	DESIGNED - CS / AN	REVISED -
	DRAWN - CS / AN	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - RG	REVISED -
PLOT DATE = 11/21/2018	DATE - 11/21/2018	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

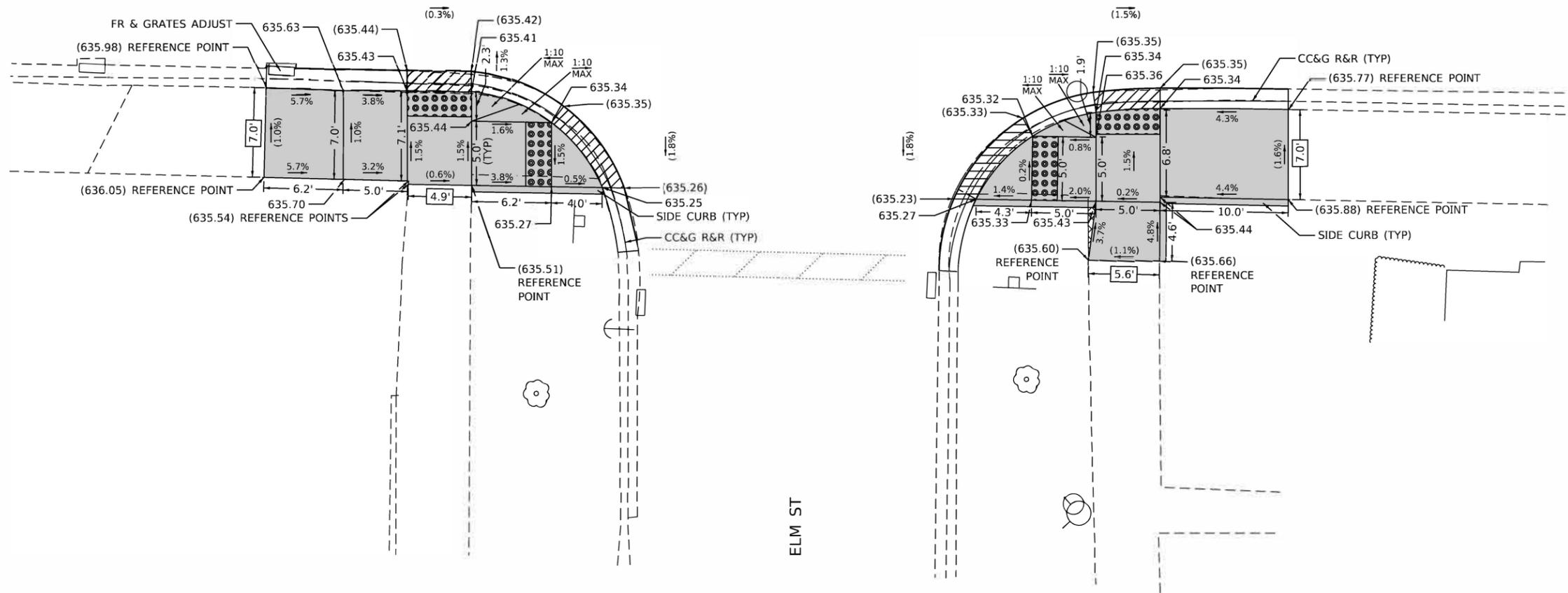
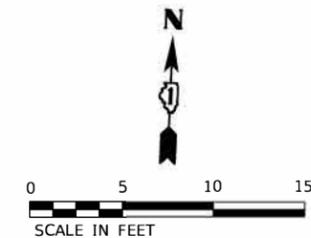
**ADA RAMP DETAILS  
 127TH ST AT HIGHLAND AVE**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344A	2018-010-R5-SW	COOK	61	34
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62G80	



127TH ST



NOTE: REFER TO ROADWAY AND PAVEMENT MARKING PLANS FOR PROPOSED PAVEMENT MARKINGS.

**LEGEND**

XX.XX'	EXISTING LENGTH
( )	EXISTING ELEVATION / SLOPE
[Solid Grey Box]	PORTLAND CEMENT CONCRETE SIDEWALK 5"
[Grid Pattern Box]	DETECTABLE WARNINGS
[Cross-hatch Pattern Box]	SIDEWALK REMOVAL REPLACE WITH TOPSOIL AND SOD
[Diagonal Line Pattern Box]	DEPRESSURED CURB AND GUTTER

REFERENCE BENCHMARK 4399 EL 637.47 (NOT SHOWN)  
 BENCHMARK: NE BOLT ON FH  
 LOCATION: NE COR 127TH AND ELM ST

MODEL: D:\p1\127th Street ADA Ramp\127th Street ADA Ramp\127th Street ADA Ramp\127th Street ADA Ramp.dwg  
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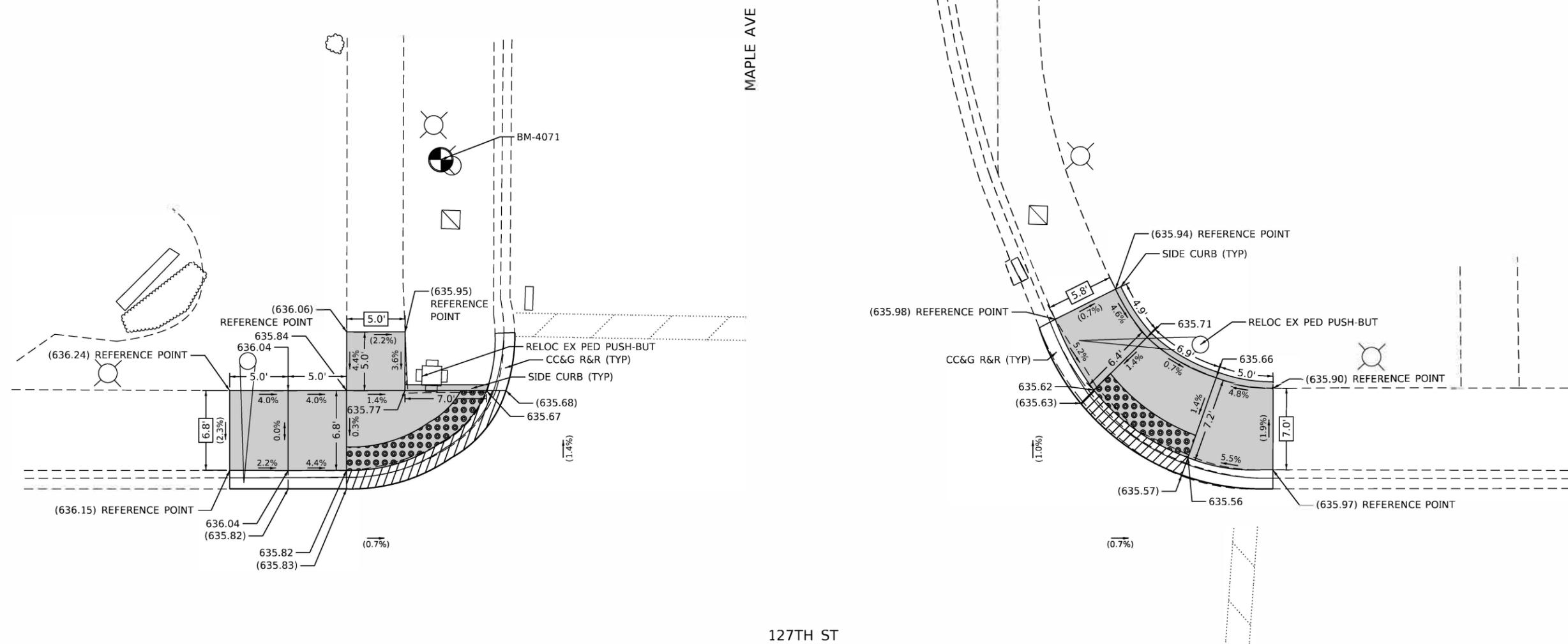
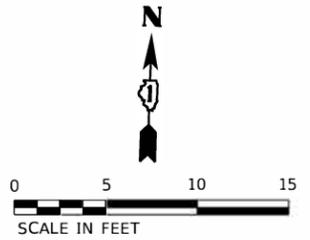
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	DRAWN - CS / AN	REVISED -
PLOT SCALE = 10.0000" / in.	CHECKED - RG	REVISED +
PLOT DATE = 11/21/2018	DATE - 11/21/2018	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**ADA RAMP DETAILS  
 127TH ST AT ELM ST**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344A	2018-010-RS-SW	COOK	61	36
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62G80	



NOTE: REFER TO ROADWAY AND PAVEMENT MARKING PLANS FOR PROPOSED PAVEMENT MARKINGS.

REFERENCE BENCHMARK 4071 EL 637.63  
 BENCHMARK: NW BOLT ON FH  
 LOCATION: NW COR 127TH AND MAPLE ST

**LEGEND**

XX.XX'	EXISTING LENGTH
( )	EXISTING ELEVATION / SLOPE
[Solid Grey Box]	PORTLAND CEMENT CONCRETE SIDEWALK 5"
[Dotted Box]	DETECTABLE WARNINGS
[Cross-hatched Box]	SIDEWALK REMOVAL REPLACE WITH TOPSOIL AND SOD
[Diagonal Line Box]	DEPRESSURED CURB AND GUTTER

MODEL: D:\defult  
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DRAWN - CS / AN	CHECKED - RG	REVISED -
PLOT SCALE = 10.0000 ' / in.	DATE - 11/21/2018	REVISED +
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**ADA RAMP DETAILS  
 127TH ST AT MAPLE AVE**

SCALE:      SHEET      OF      SHEETS      STA.      TO      STA.

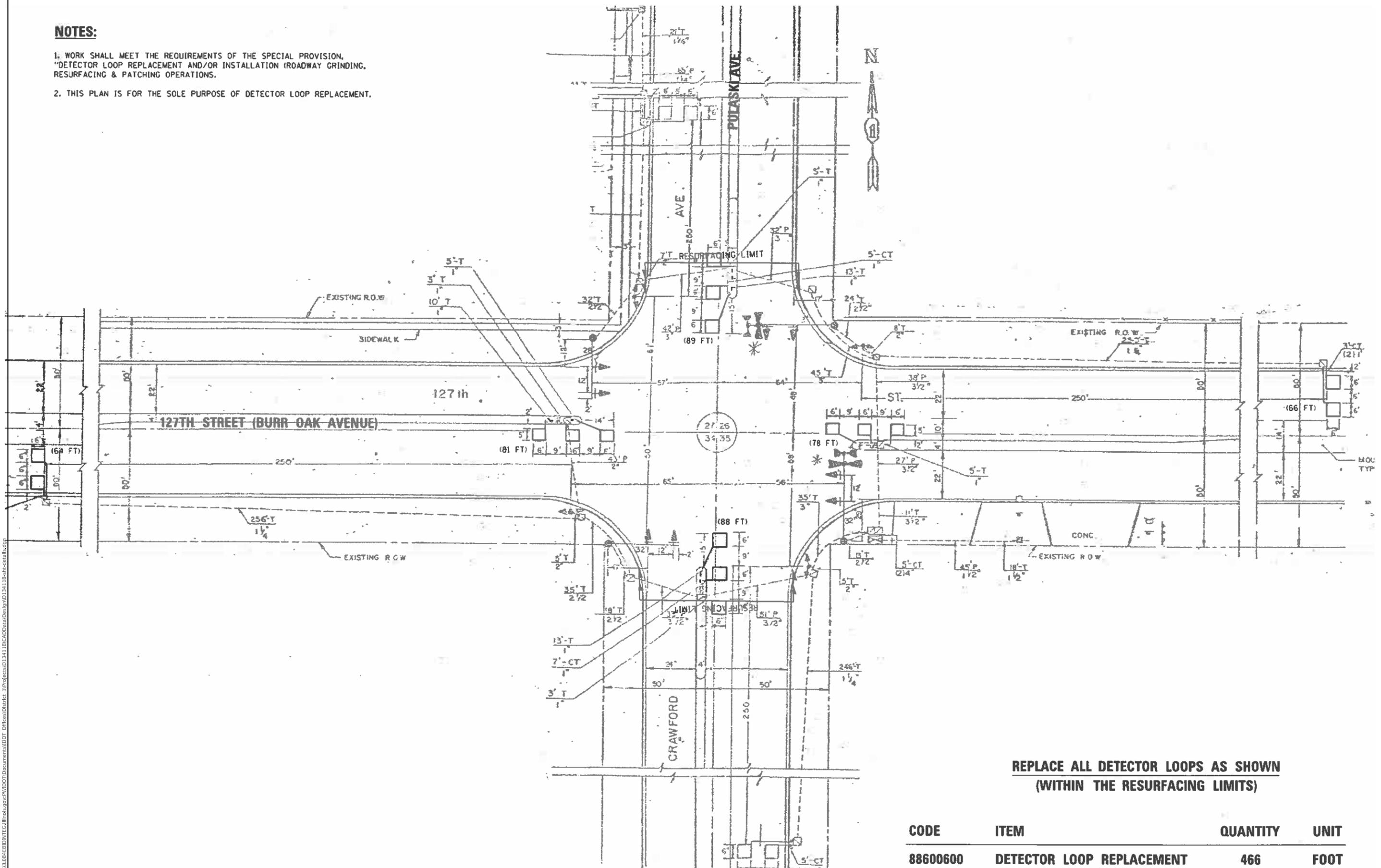
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344A	2018-010-R5-SW	COOK	61	37
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62G80	





**NOTES:**

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



**REPLACE ALL DETECTOR LOOPS AS SHOWN  
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	466	FOOT

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETECTOR LOOP REPLACEMENT PLAN  
127TH ST. (BURR OAK AVE.) AT PULASKI ROAD**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2018-010-R5-SW	COOK	61	40
CONTRACT NO. 62G80				
ILLINOIS FED. AID PROJECT				

USER NAME = curryw	DESIGNED -	REVISED -
PLOT SCALE = 100,0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/6/2018	CHECKED -	REVISED -
	DATE -	REVISED -

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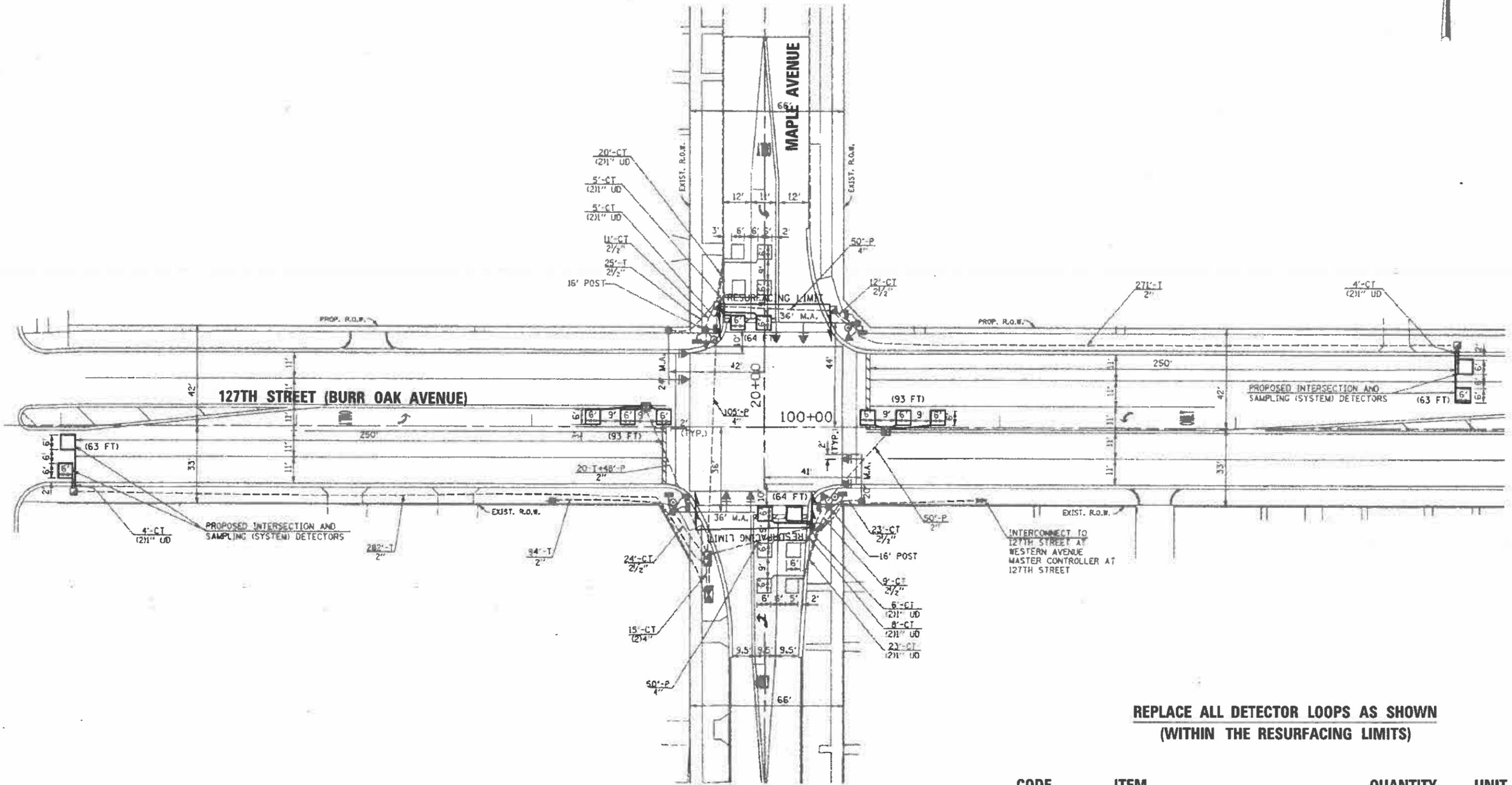






**NOTES:**

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



**REPLACE ALL DETECTOR LOOPS AS SHOWN  
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	440	FOOT

USER NAME = curryw	DESIGNED -	REVISED -
	DRAWN -	REVISED -
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PLOT DATE = 12/6/2018	DATE -	REVISED -

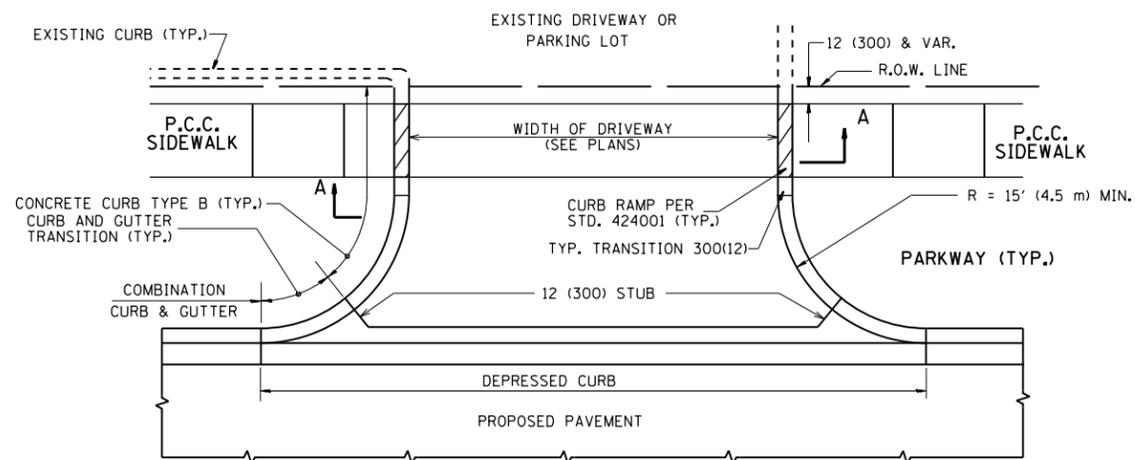
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETECTOR LOOP REPLACEMENT PLAN  
127TH ST. (BURR OAK AVE.) AT MAPLE AVENUE**

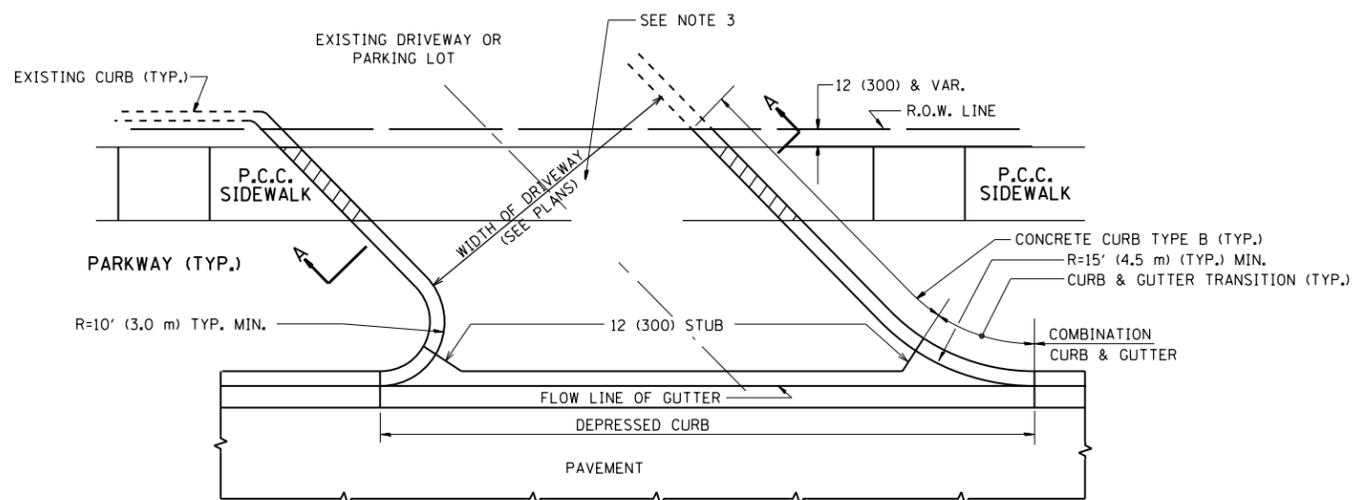
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2018-010-RS-SW	COOK	61	44
CONTRACT NO. 62G80				
ILLINOIS FED. AID PROJECT				

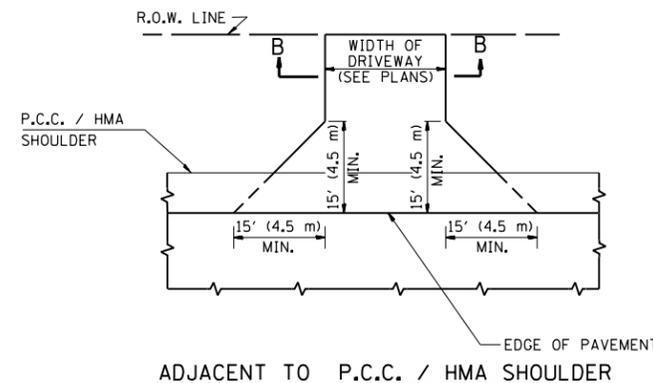
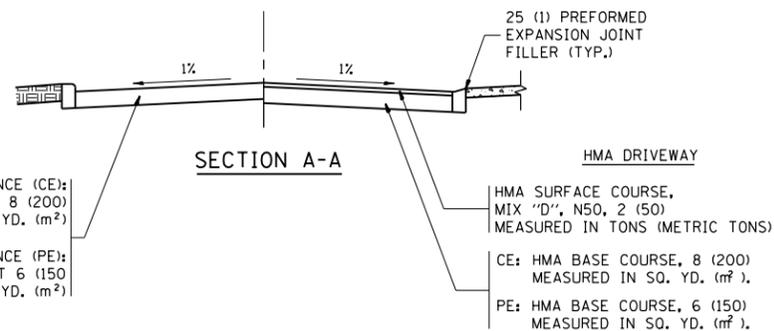
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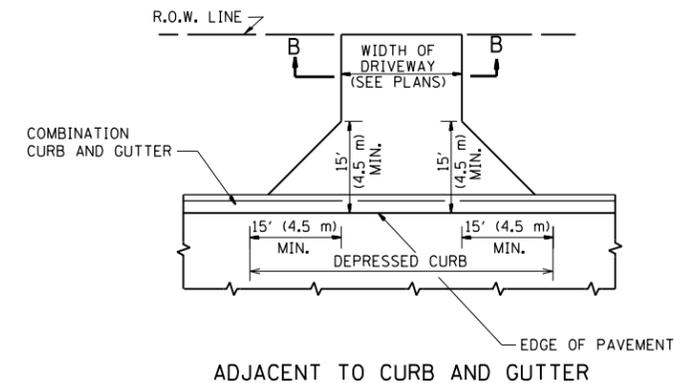
WITH CONCRETE CURB, TYPE B



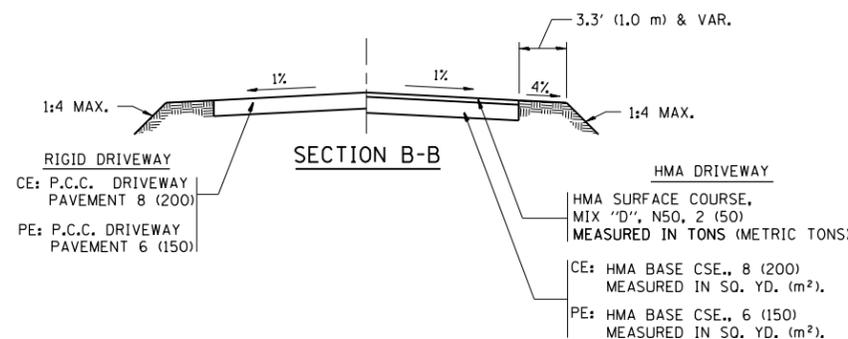
WITH CONCRETE CURB, TYPE B



ADJACENT TO P.C.C. / HMA SHOULDER



ADJACENT TO CURB AND GUTTER



RURAL FIELD ENTRANCE (FE)

**GENERAL NOTES:**

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

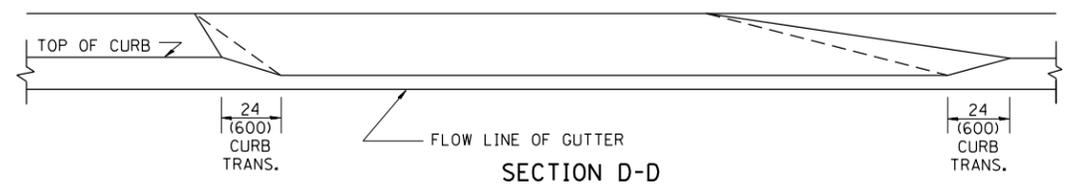
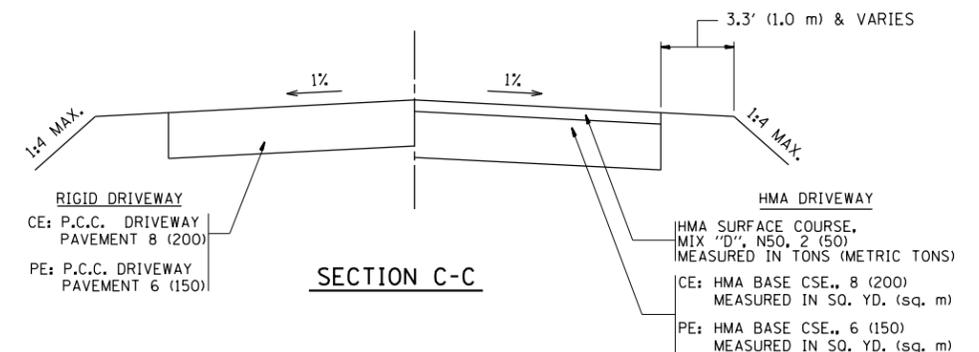
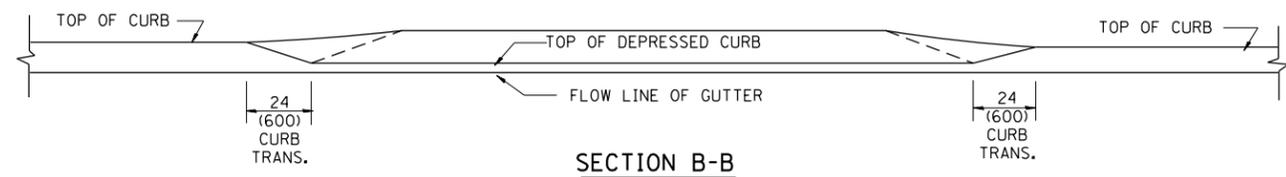
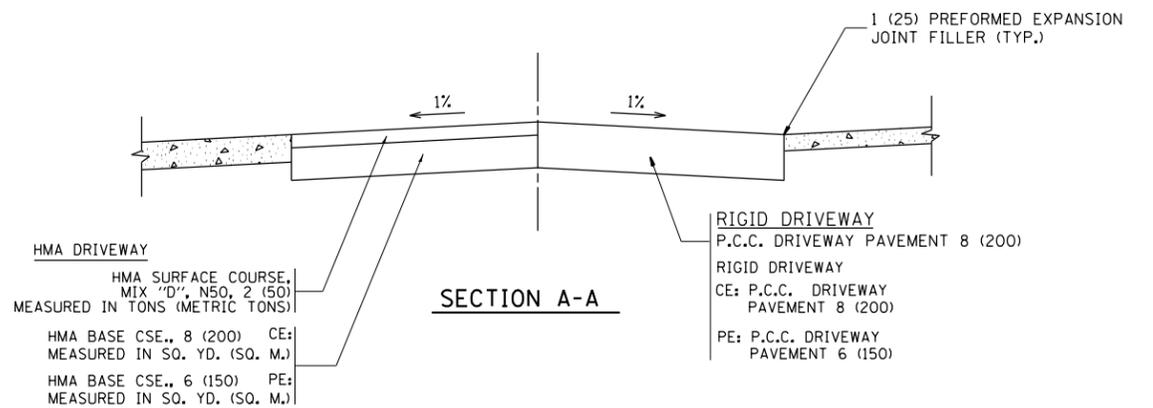
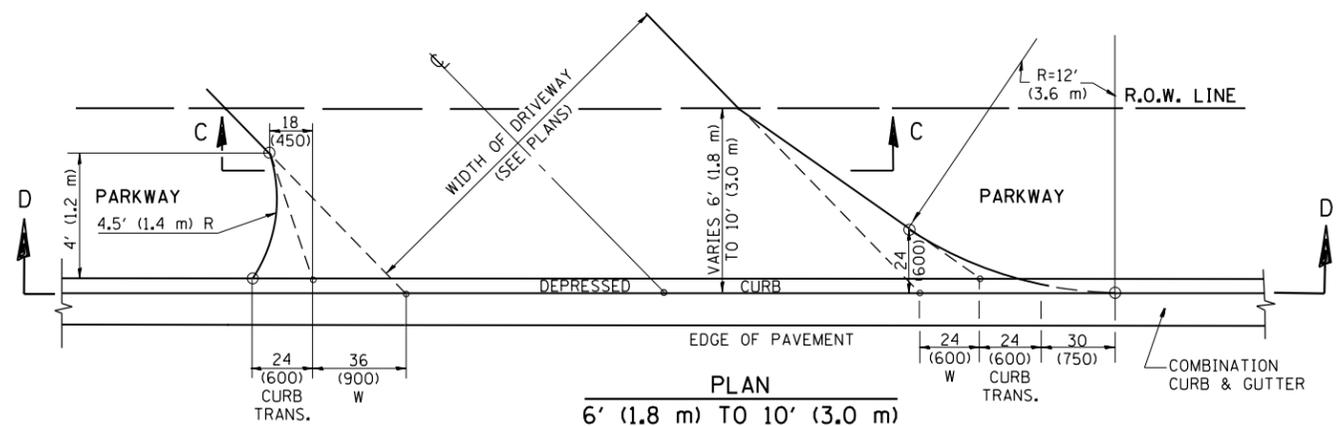
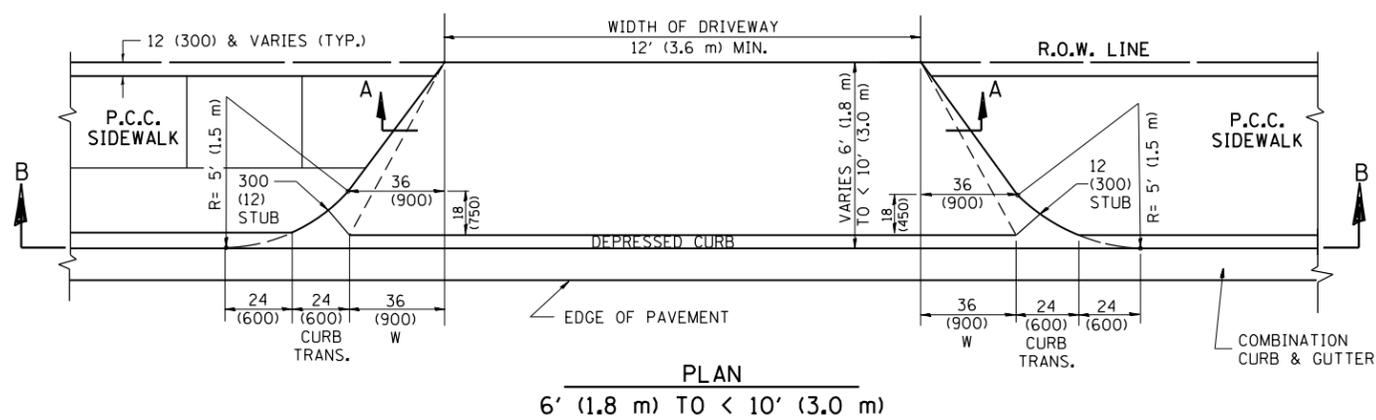
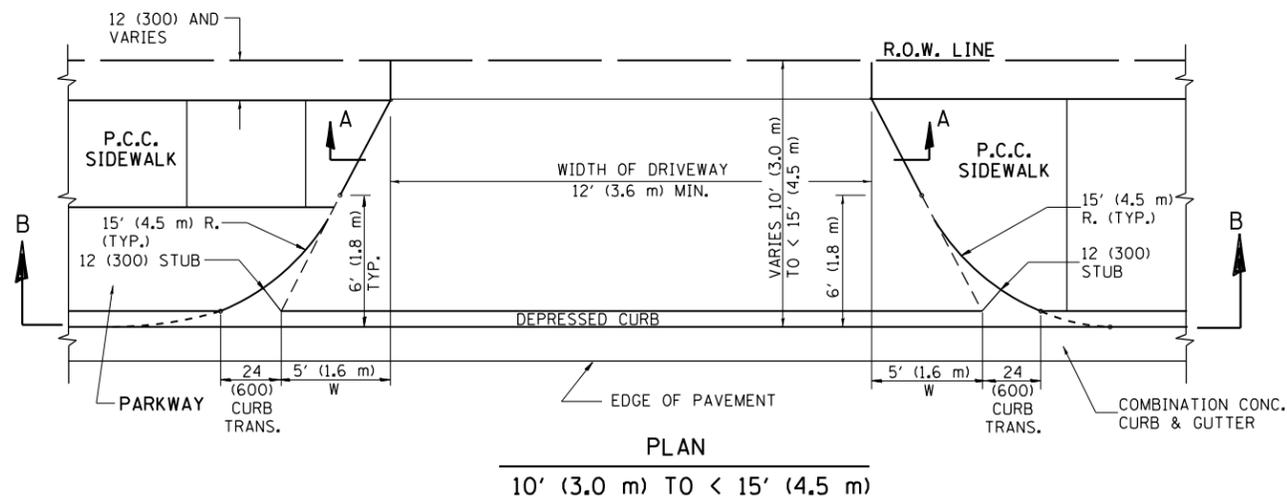
WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

FILE NAME =	USER NAME = currywaw	DESIGNED - R. SHAH	REVISED - P. LaFLUER 04-15-03
pw:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI34\BROWN\Design\DistStd.dgn		CHECKED -	REVISED - R. BORO 01-01-07
		DATE - 11-04-95	REVISED - R. BORO 06-11-08
			REVISED - R. BORO 09-06-11

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2018-010-RS-SW	COOK	61	45
BD0156-07 (BD-01)		CONTRACT NO. 62680		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**GENERAL NOTES**

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

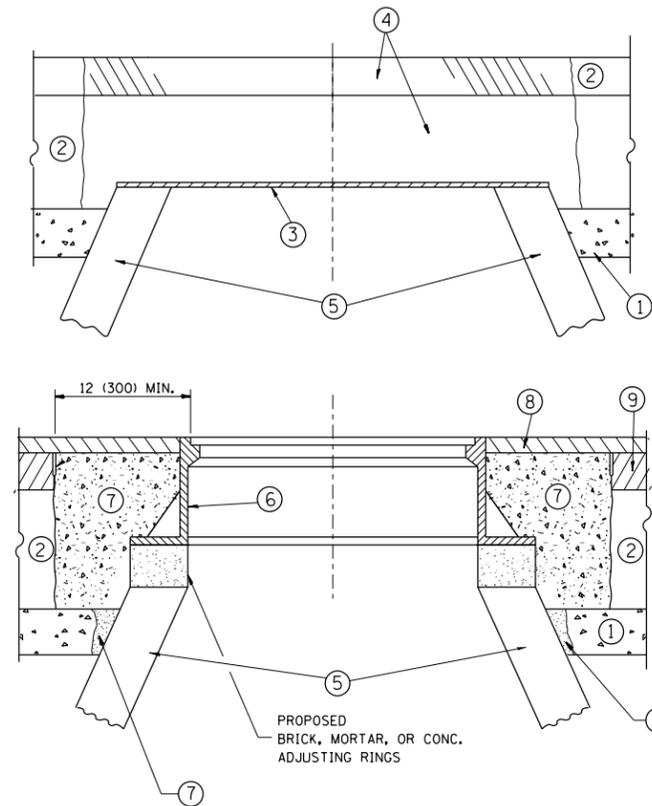
"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

FILE NAME =	USER NAME = currywaw	DESIGNED - R. SHAH	REVISED - M. GOMEZ 04-06-01
p:\1\084EBIDINTEG.illinois.gov\PI\DOT\Documents\IDOT Offices\District 1\Projects\0134\BROWNS\Design\DistStd.dgn		CHECKED -	REVISED - P. LaFLEUR 04-15-03
		DATE - 11-06-95	REVISED - R. BORO 01-01-07
			REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DRIVEWAY DETAILS</b>		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)		344	2018-010-RS-SW	COOK	61	46
SCALE: NONE		BD400-02 (BD-02)		CONTRACT NO. 62G80		
SHEET NO. 1 OF 1 SHEETS		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

\* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1\* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

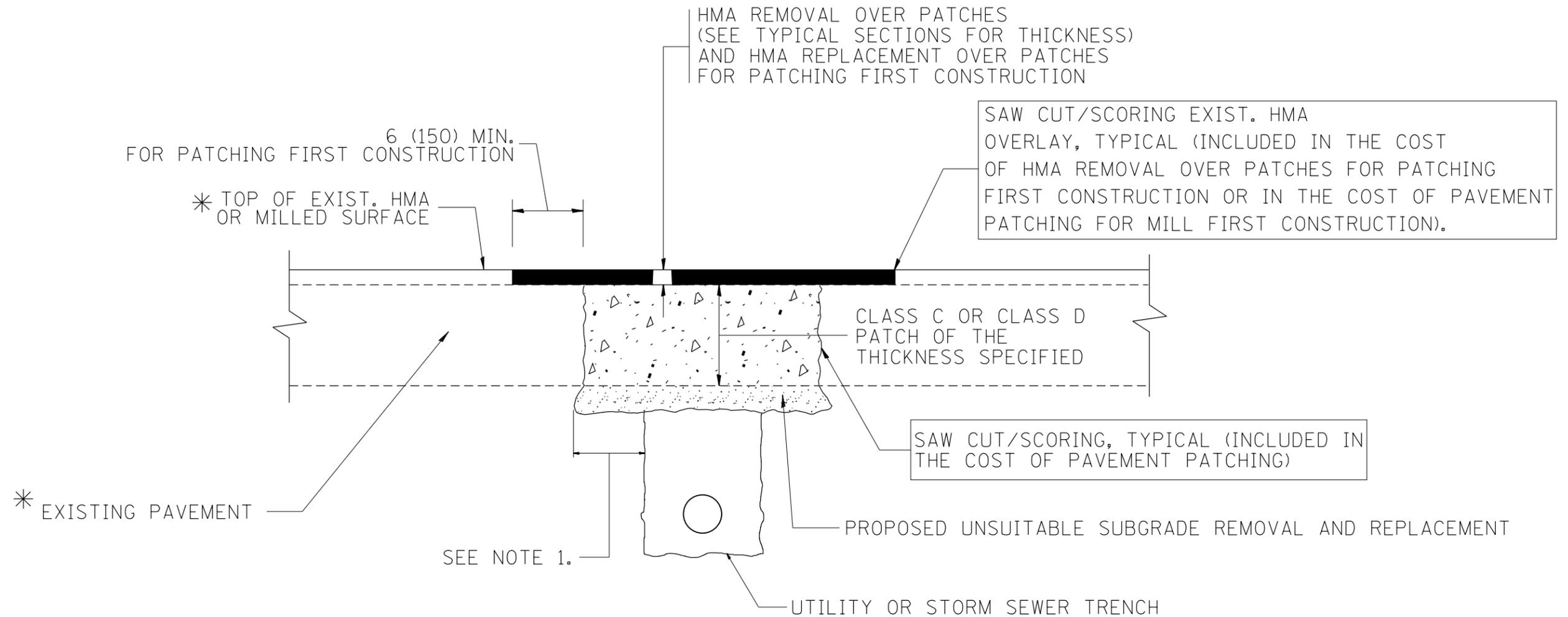
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pw:\IL\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI34\BRAW\Notes\Design\DistStd.dgn			REVISED - R. BORO 01-01-07
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 12/6/2018	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR  
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2018-010-RS-SW	COOK	61	47
BD600-03 (BD-8)		CONTRACT NO. 62G80		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

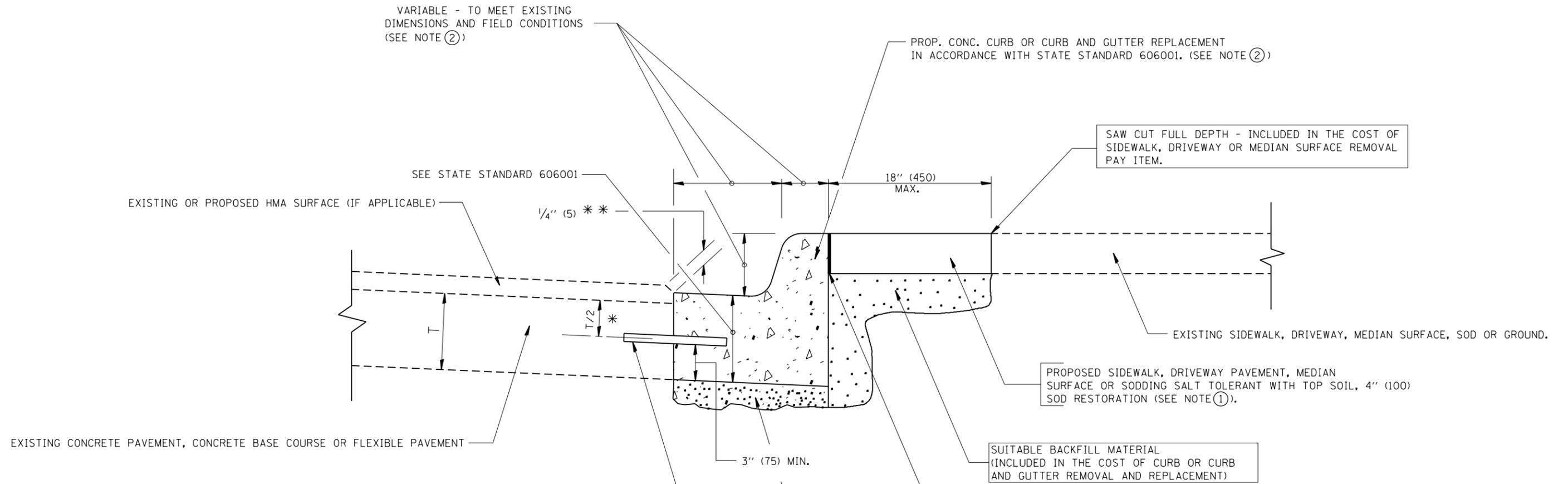
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = curryw	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\11084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\1341\BROWNS\Design\DistStd.dgn	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 01-01-07					344	2018-010-RS-SW	COOK	61	48
	PLOT DATE = 12/6/2018	DATE - 10-25-94	REVISED - R. BORO 09-04-07		BD400-04 (BD-22)			CONTRACT NO. 62680				
			REVISED - K. ENG 10-27-08		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT			



\* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

\* \* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

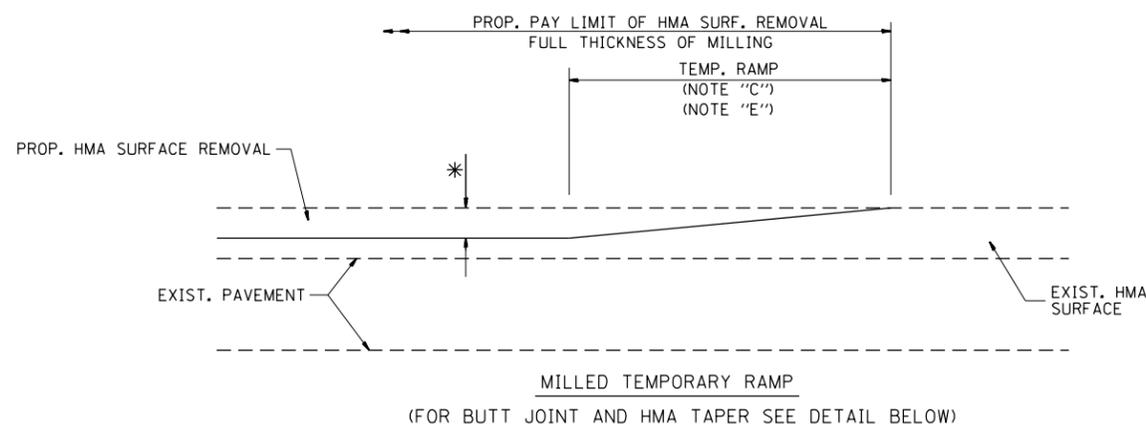
PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE 3).

**BASIS OF PAYMENT:**  
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

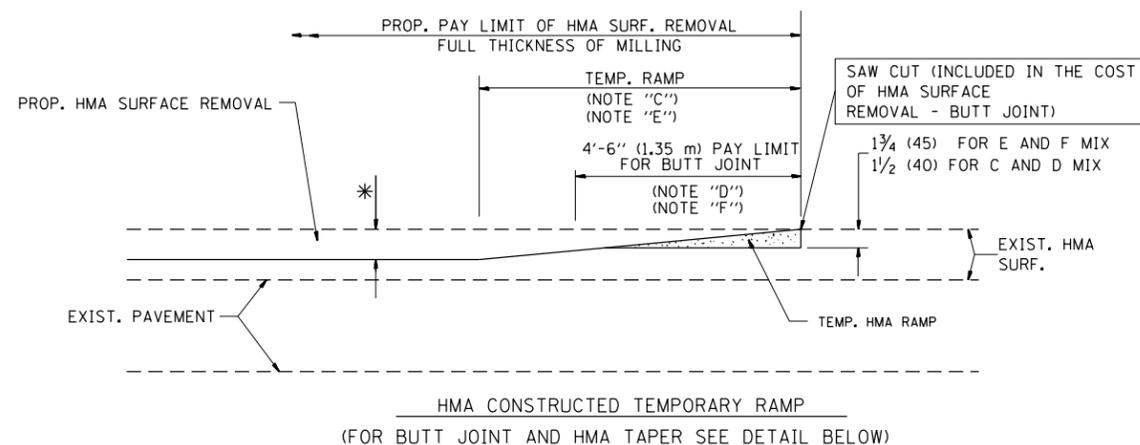
# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = currywaw	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>	F.A.P. R.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw:\IL\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI341\BROW\Design\DistStd.dgn	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. ABBAS 03-21-97			344	2018-010-RS-SW	COOK	61	49	
PLOT DATE = 12/6/2018	DATE - 03-11-94	REVISED - M. GOMEZ 01-22-01	REVISED - R. BORO 12-15-09			<b>BD600-06 (BD-24)</b>		<b>CONTRACT NO. 62680</b>			
						FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT					

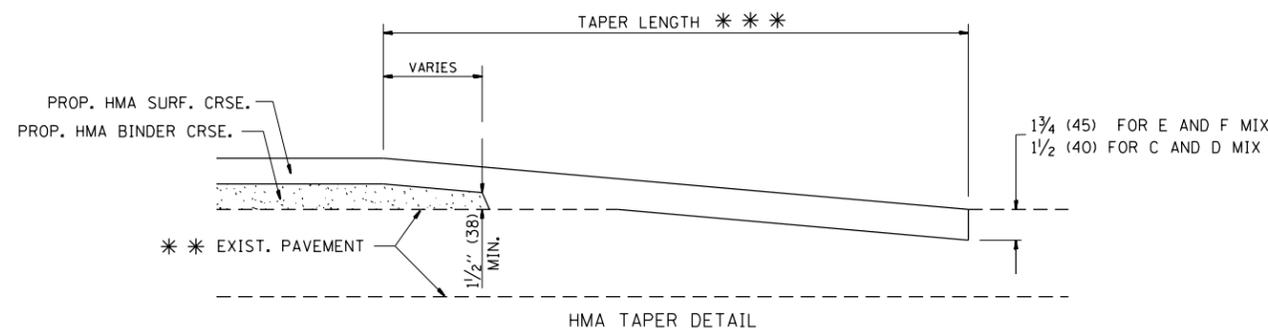
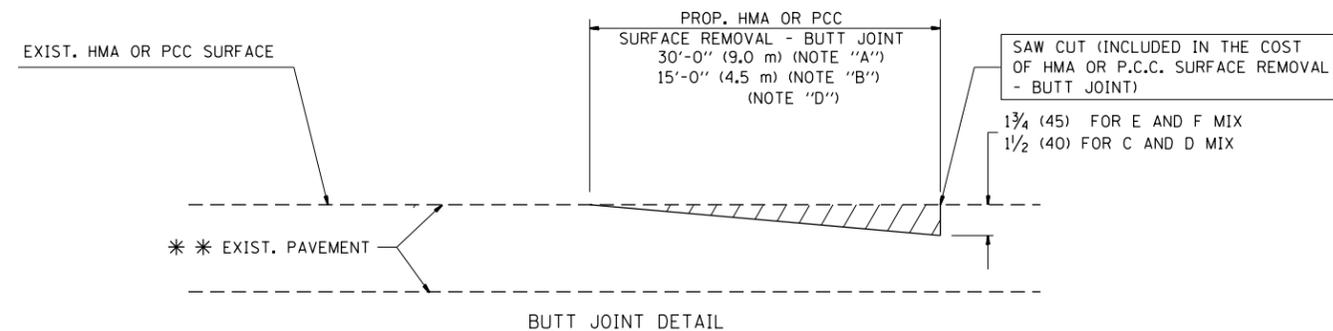


**OPTION 1**



**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\* \* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

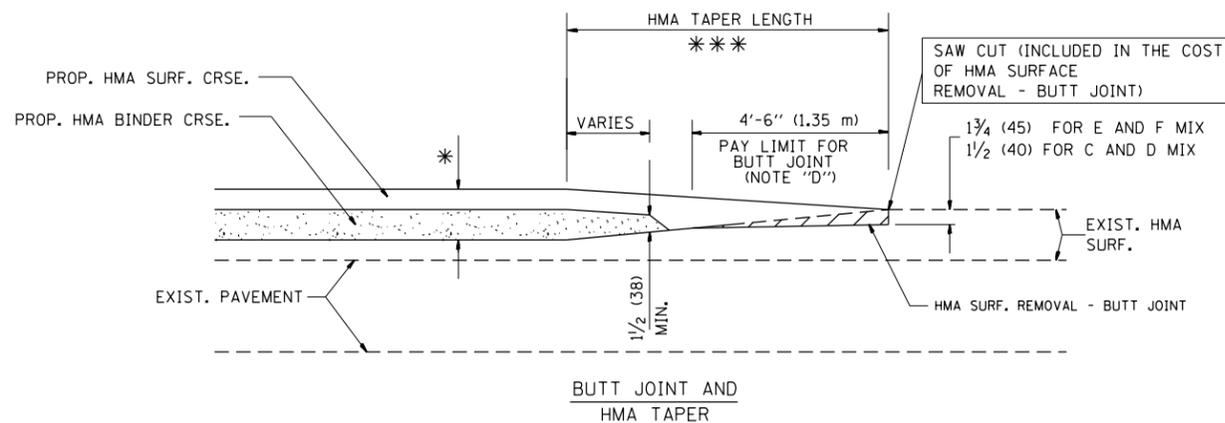
**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \* \* \* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

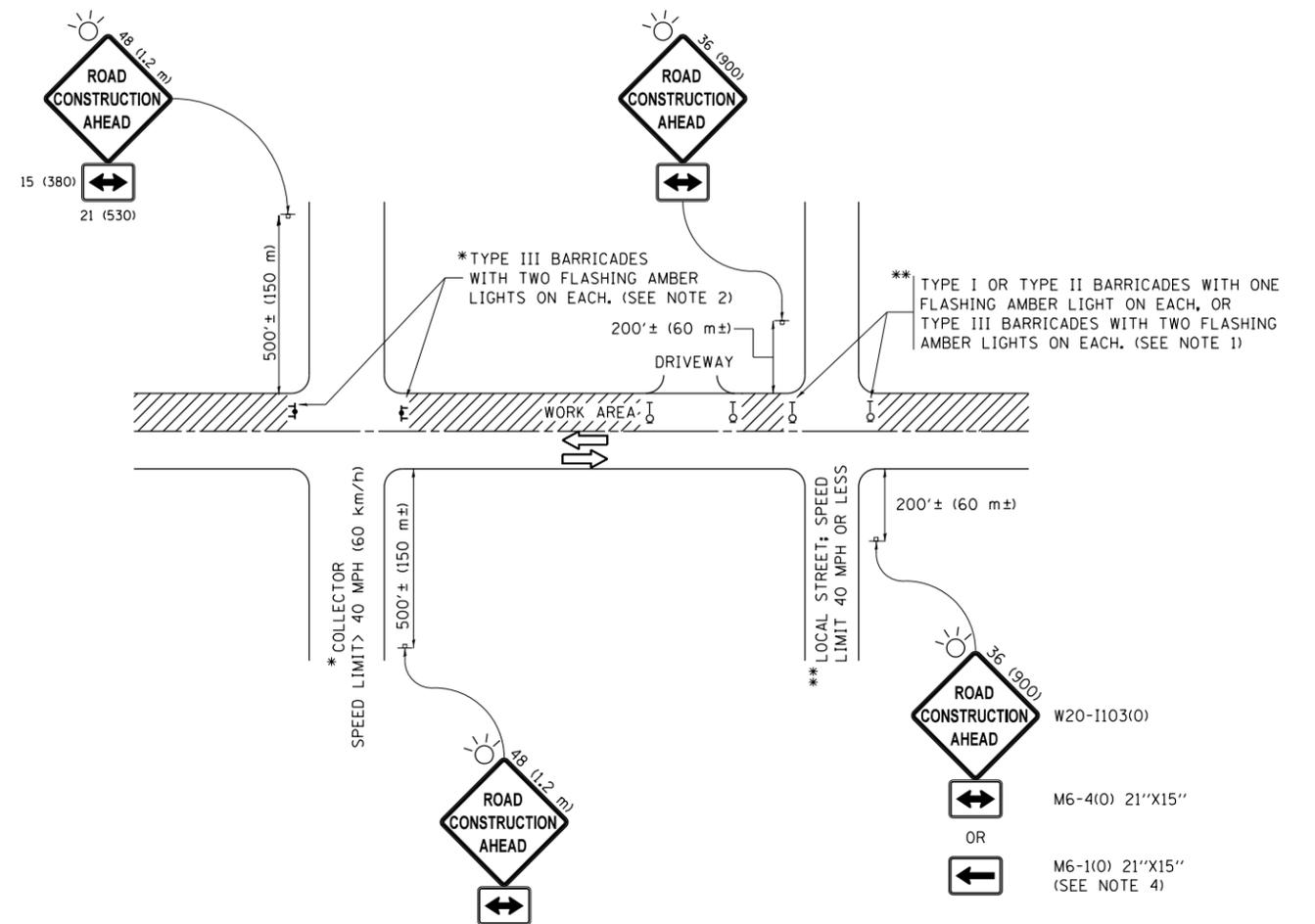
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	PLOT SCALE = 100.0000' / in.	DATE - 06-13-90	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 12/6/2018		REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND  
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2018-010-RS-SW	COOK	61	50
BD400-05 BD32		CONTRACT NO. 62880		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**NOTES:**

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

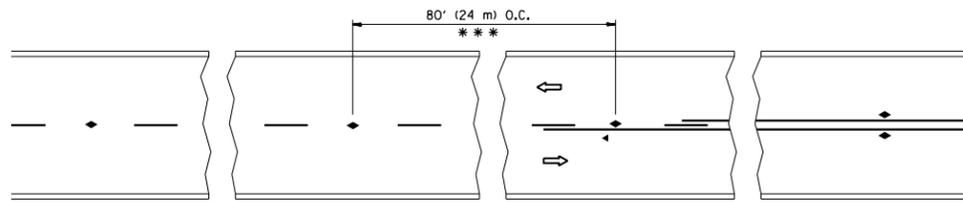
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Default	PLOT SCALE = 100.0000' / in.	DATE - 06-89	REVISED - A. SCHUETZE 07-01-13
	PLOT DATE = 12/6/2018		REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

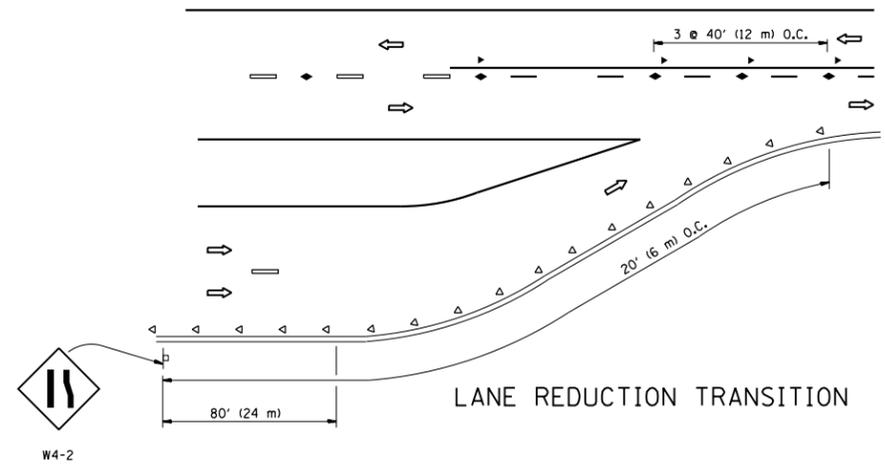
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2018-010-RS-SW	COOK	61	51
<b>TC-10</b>			<b>CONTRACT NO. 62G80</b>	
ILLINOIS FED. AID PROJECT				

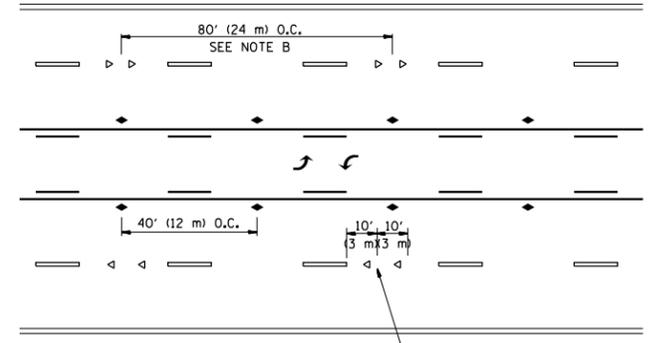


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

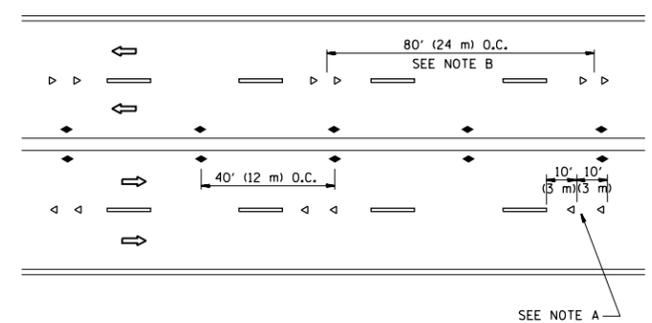
TWO-LANE/TWO-WAY



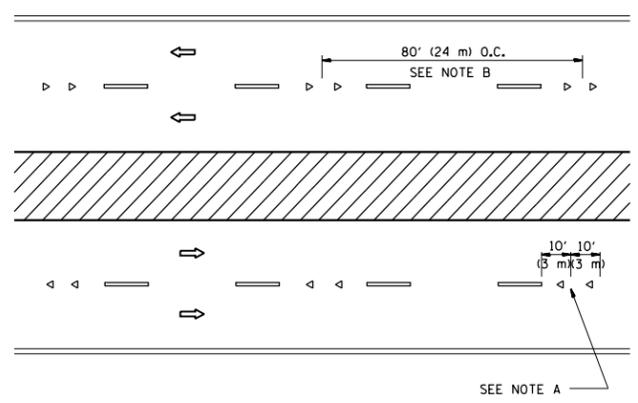
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

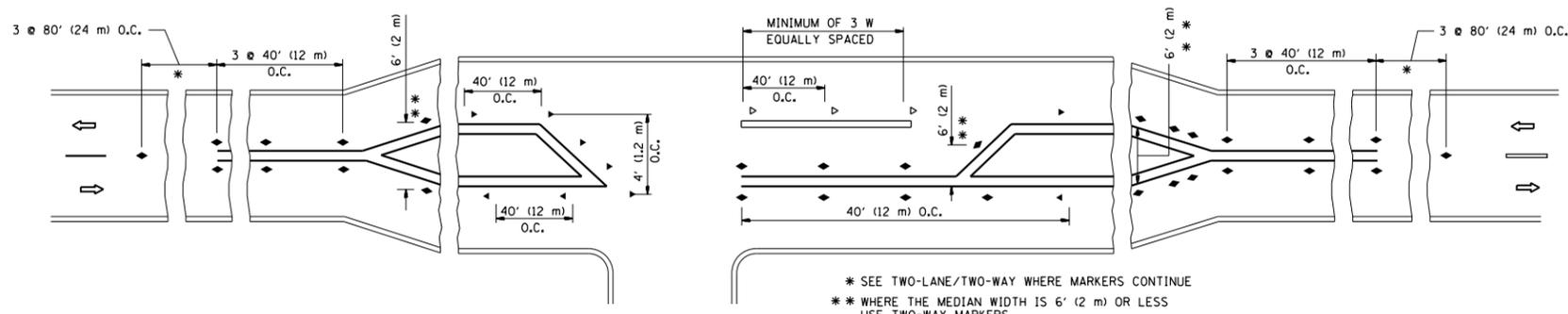
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

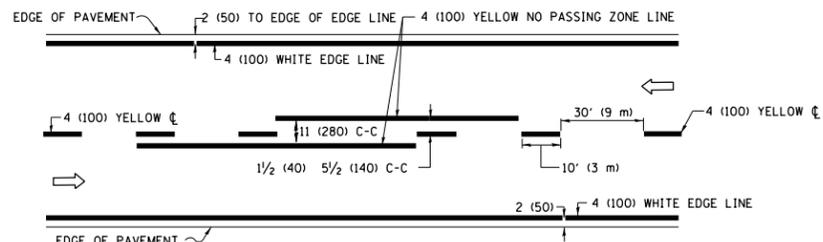


LEFT TURN

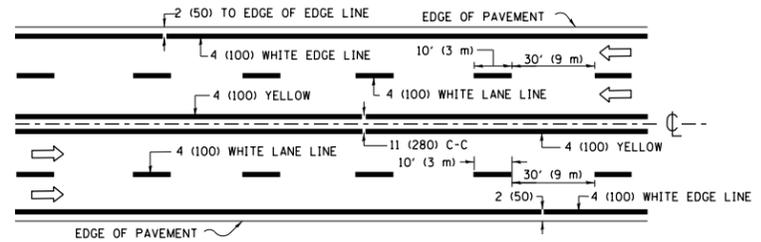
\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

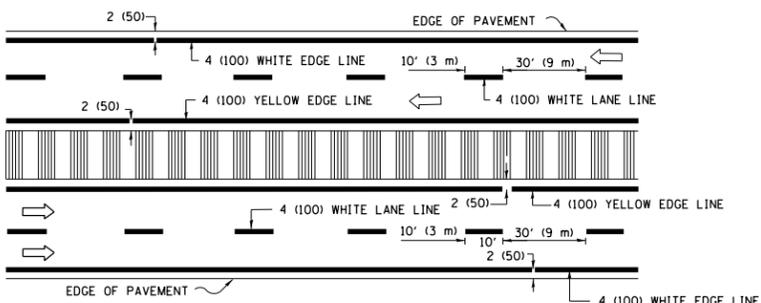
FILE NAME =	USER NAME = currywaw	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\11\084EBIDINTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\DI34\BRAW\Design\DistStd.dgn	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - T. RAMMACHER 03-12-99					344	2018-010-RS-SW	COOK	61	52
PLOT DATE = 12/6/2018	DATE -	REVISED - T. RAMMACHER 01-06-00	REVISED - C. JUCIUS 09-09-09		<b>TC-11</b>			<b>CONTRACT NO. 62G80</b>				
					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**2-LANE ROADWAY**

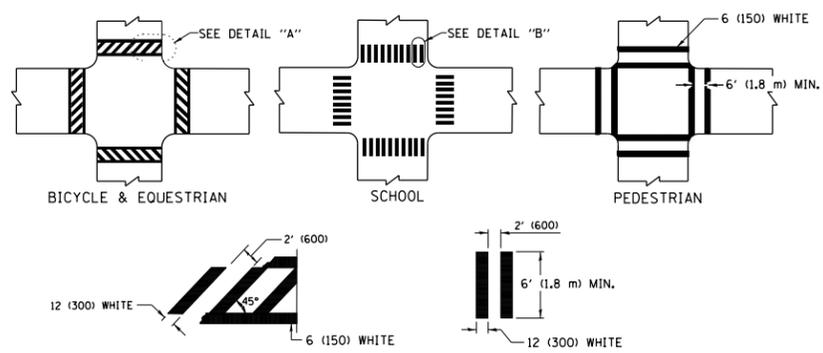


**MULTI-LANE UNDIVIDED**



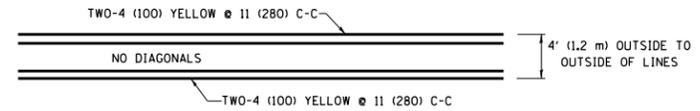
**MULTI-LANE DIVIDED WITH MEDIAN**

**TYPICAL LANE AND EDGE LINE MARKING**

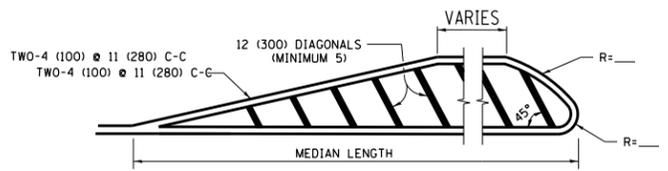


**TYPICAL CROSSWALK MARKING**

\* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

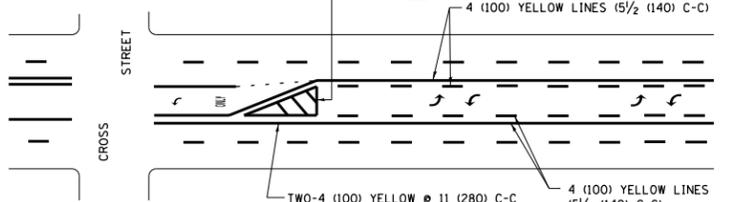


**4' (1.2 m) WIDE MEDIANS ONLY**



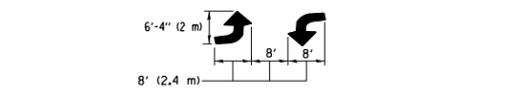
**MEDIANS OVER 4' (1.2 m) WIDE**

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



**MEDIAN WITH TWO-WAY LEFT TURN LANE  
TYPICAL PAINTED MEDIAN MARKING**

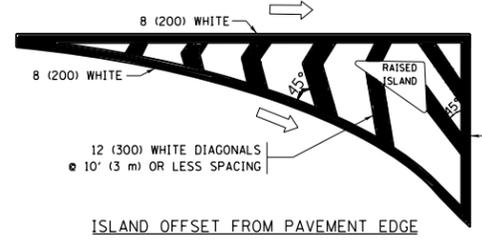
A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



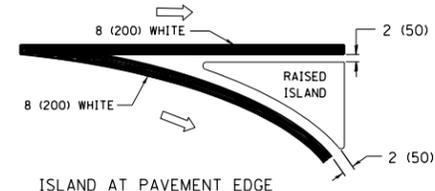
**TYPICAL LEFT (OR RIGHT) TURN LANE**

**TYPICAL TURN LANE MARKING**

FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)  
\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

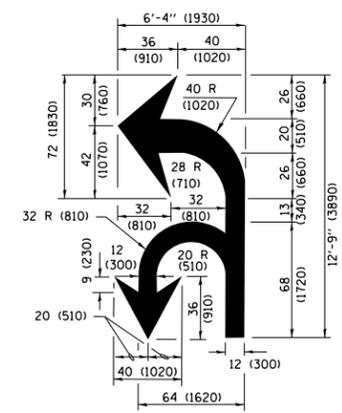


**ISLAND OFFSET FROM PAVEMENT EDGE**

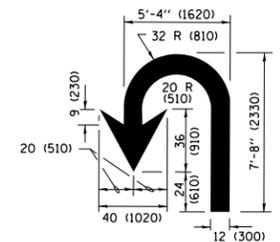


**ISLAND AT PAVEMENT EDGE**

**TYPICAL ISLAND MARKING**



**COMBINATION LEFT AND U-TURN**



**U-TURN**

**LANE REDUCTION TRANSITION**

\* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT, OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

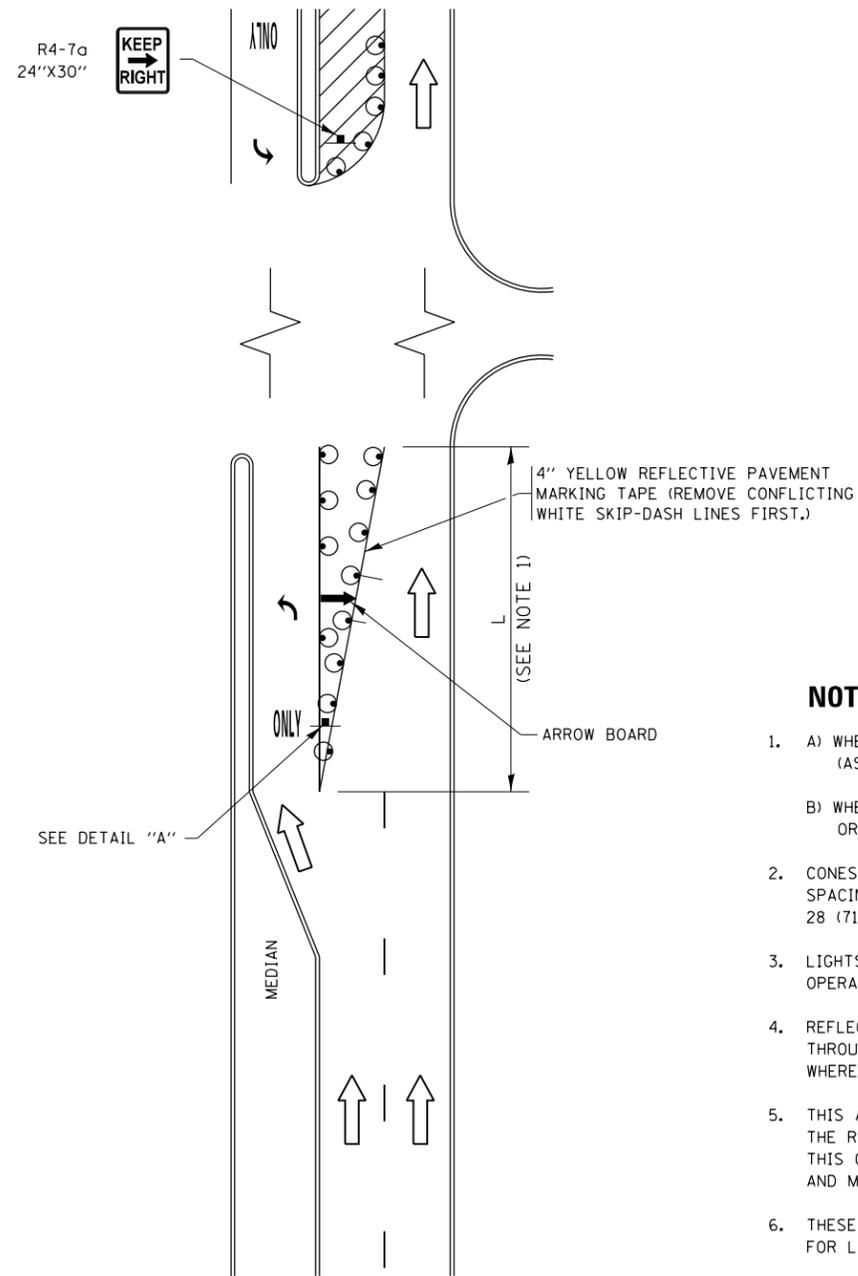
FILE NAME =	USER NAME = currywaw	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
pw\1\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\0134\BROWN\Design\DistStd.dgn			REVISED - C. JUCIUS 07-01-13
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - C. JUCIUS 12-21-15
	PLOT DATE = 12/6/2018	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DISTRICT ONE</b>			
<b>TYPICAL PAVEMENT MARKINGS</b>			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

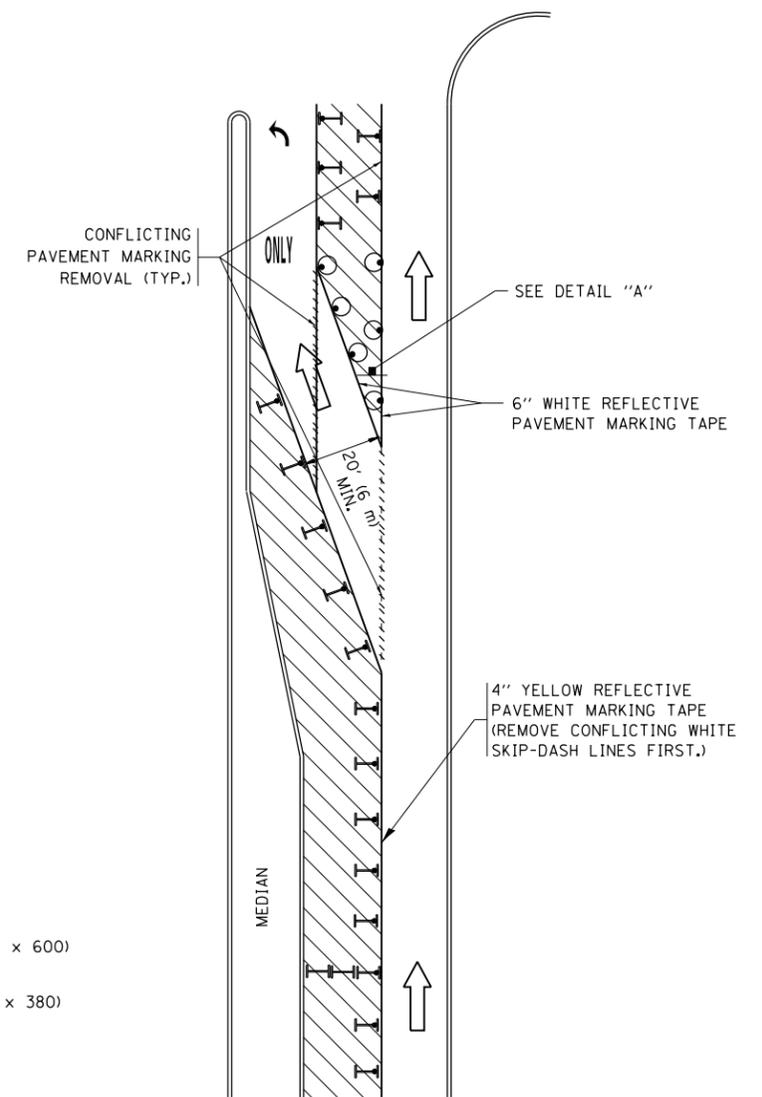
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2018-010-RS-SW	COOK	61	53
<b>TC-13</b>		<b>CONTRACT NO. 62G80</b>		
ILLINOIS FED. AID PROJECT				

# TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER



**FIGURE 1**

# TURN BAY ENTRANCE WITHIN A LANE CLOSURE



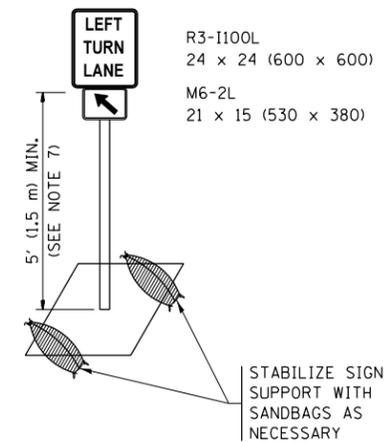
**FIGURE 2**

## LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

## NOTES:

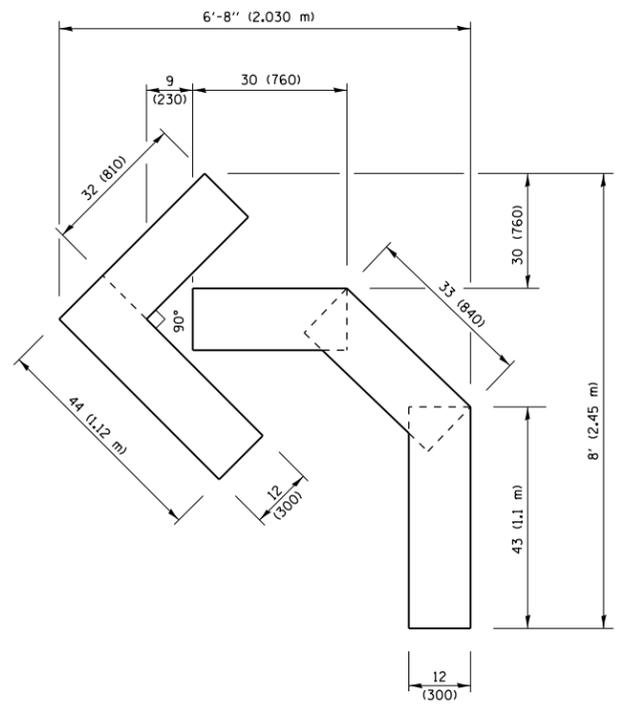
1. A) WHEN "L" IS  $\leq$  THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.  
B) WHEN "L" IS  $>$  THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



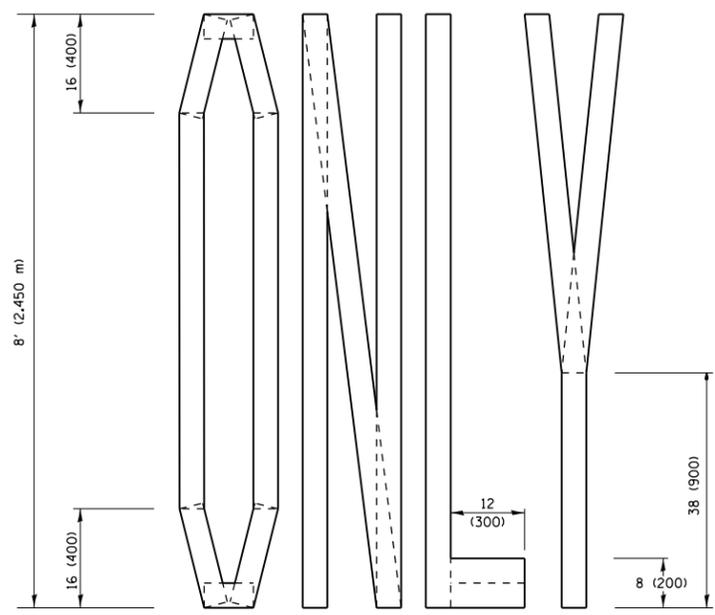
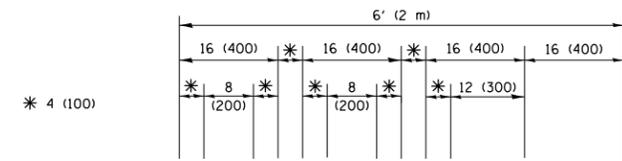
**DETAIL A**

All dimensions are in inches (millimeters) unless otherwise shown.

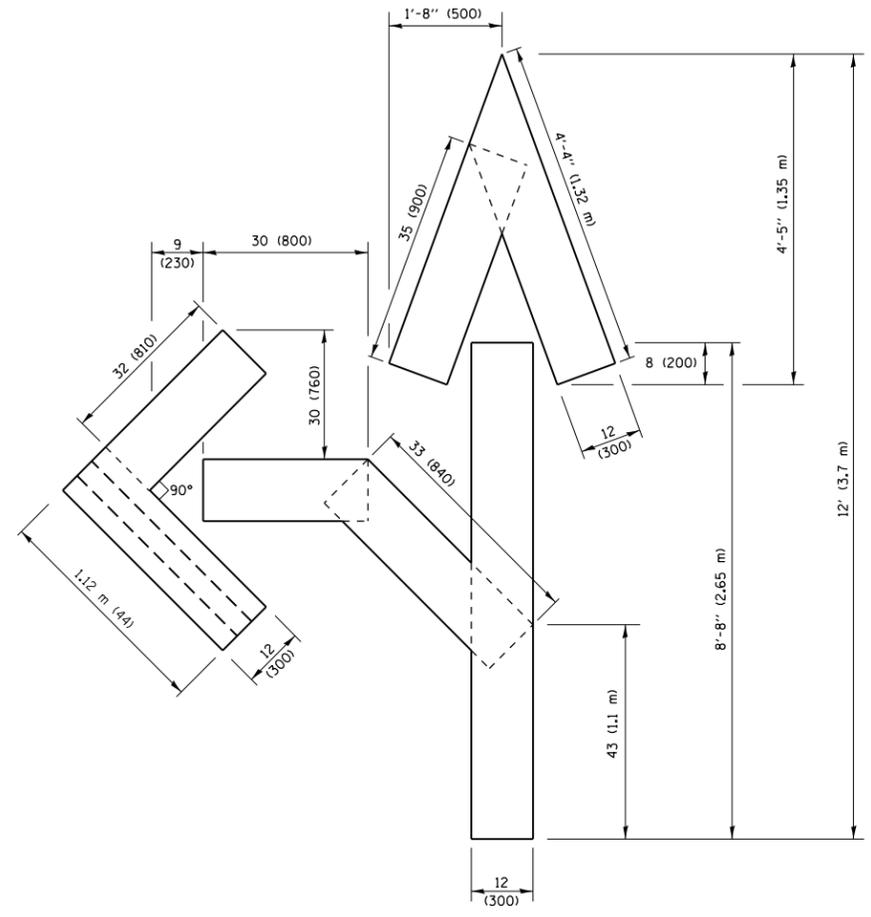
FILE NAME =	USER NAME = curryw	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)</b>	F.A.P. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\0134\REVISED Design\084EBIDINTEG.DWG	REVISED - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13	344			2018-010-RS-SW	COOK	61	54	
Default	REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16	<b>TC-14</b>			<b>CONTRACT NO. 62G80</b>				
PLOT SCALE = 100.0000' / in.	REVISED - T. RAMMACHER 01-06-00	REVISED -	ILLINOIS FED. AID PROJECT							
PLOT DATE = 12/6/2018				SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.	



**QUANTITY**  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.41 sq. m)

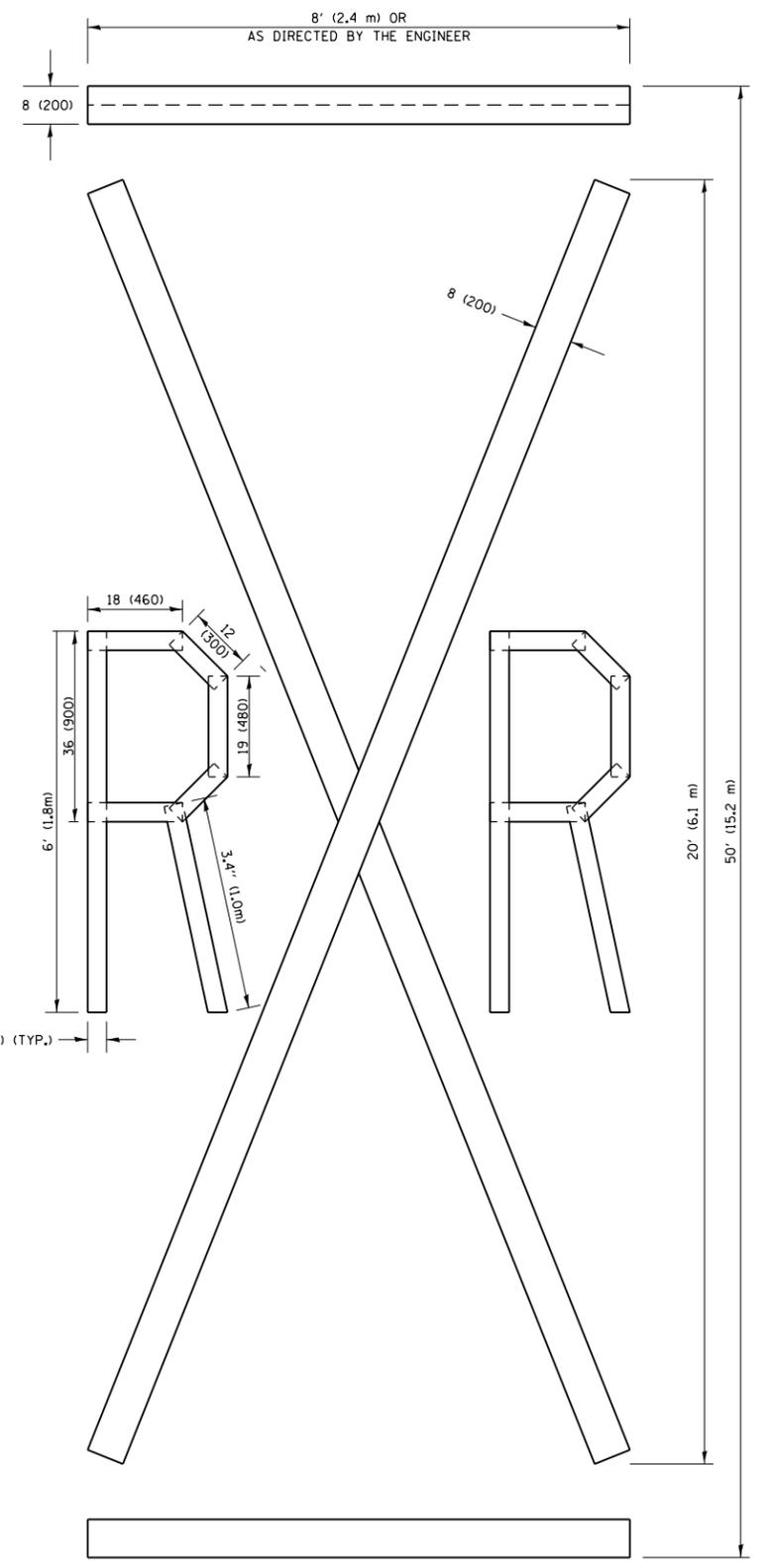


**QUANTITY**  
 4 (100) LINE = 64.1 ft. (19.5 m)  
 21.4 sq. ft. (1.99 sq. m)



**QUANTITY**  
 4 (100) LINE = 82.5 ft. (25.1 m)  
 27.5 sq. ft. (2.53 sq. m)

**NOTE:**  
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



**QUANTITY**  
 4 (100) LINE = 225.9 ft. (68.9 m)  
 75.3 sq. ft. (6.99 sq. m)

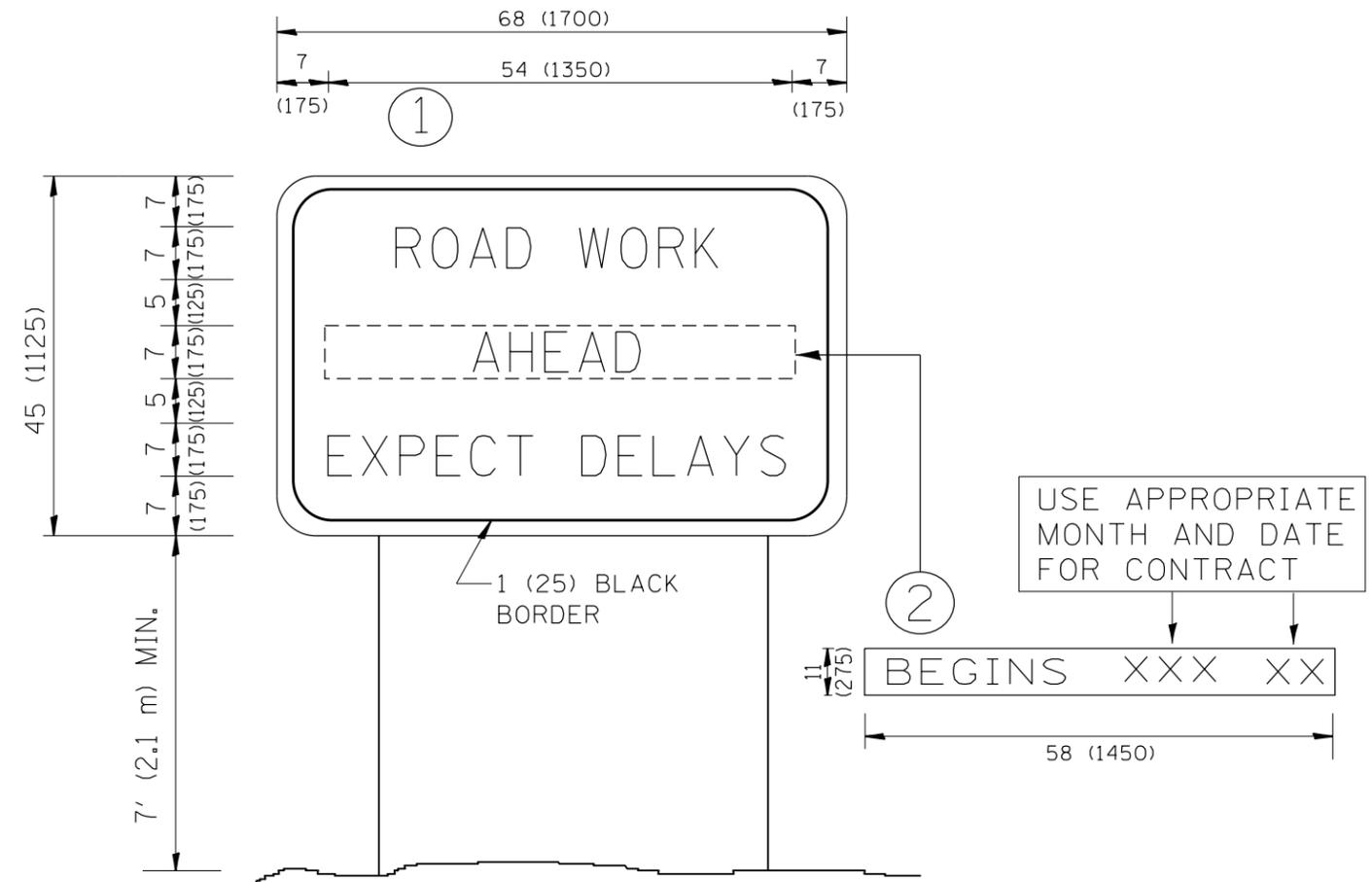
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = curryw	DESIGNED -	REVISED -T. RAMMACHER 03-02-98
p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 1\Projects\DI34\BROWNS\Design\DistStd.dgn		CHECKED -	REVISED -E. GOMEZ 08-28-00
		DATE -	REVISED -E. GOMEZ 08-28-00
			REVISED -A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2018-010-RS-SW	COOK	61	55
<b>TC-16</b>		<b>CONTRACT NO. 62680</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

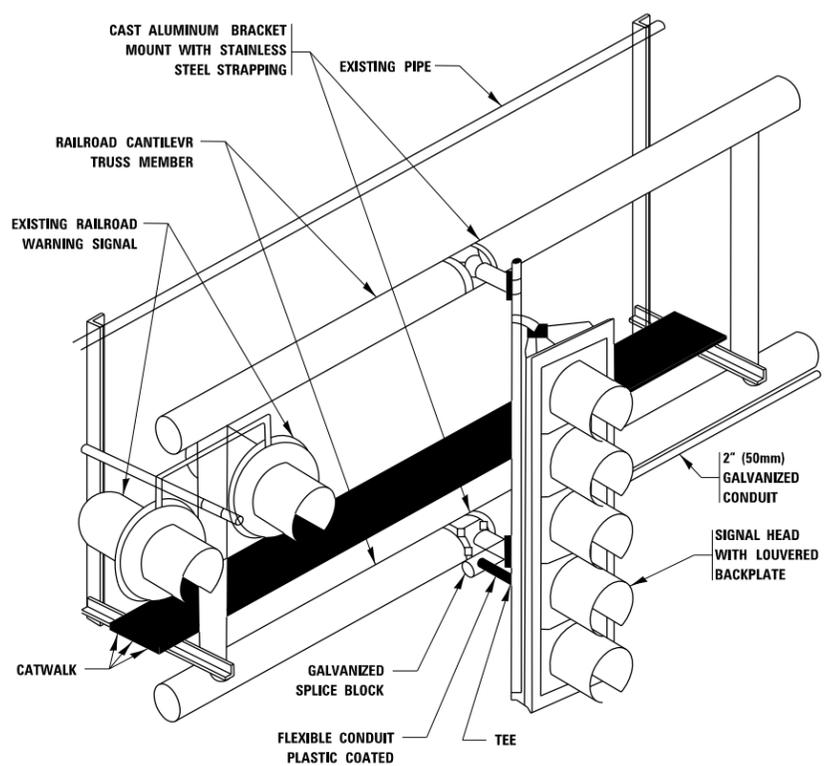
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p:\11\084EBIDINTEG.illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\DI34\BROW\Notes\Design\DistStd.dgn		CHECKED -	REVISED - R. MIRS 12-11-97
		PLOT SCALE = 100.0000' / 1in.	REVISED - T. RAMMACHER 02-02-99
		DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD  
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2018-010-RS-SW	COOK	61	56
TC-22		CONTRACT NO. 62680		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



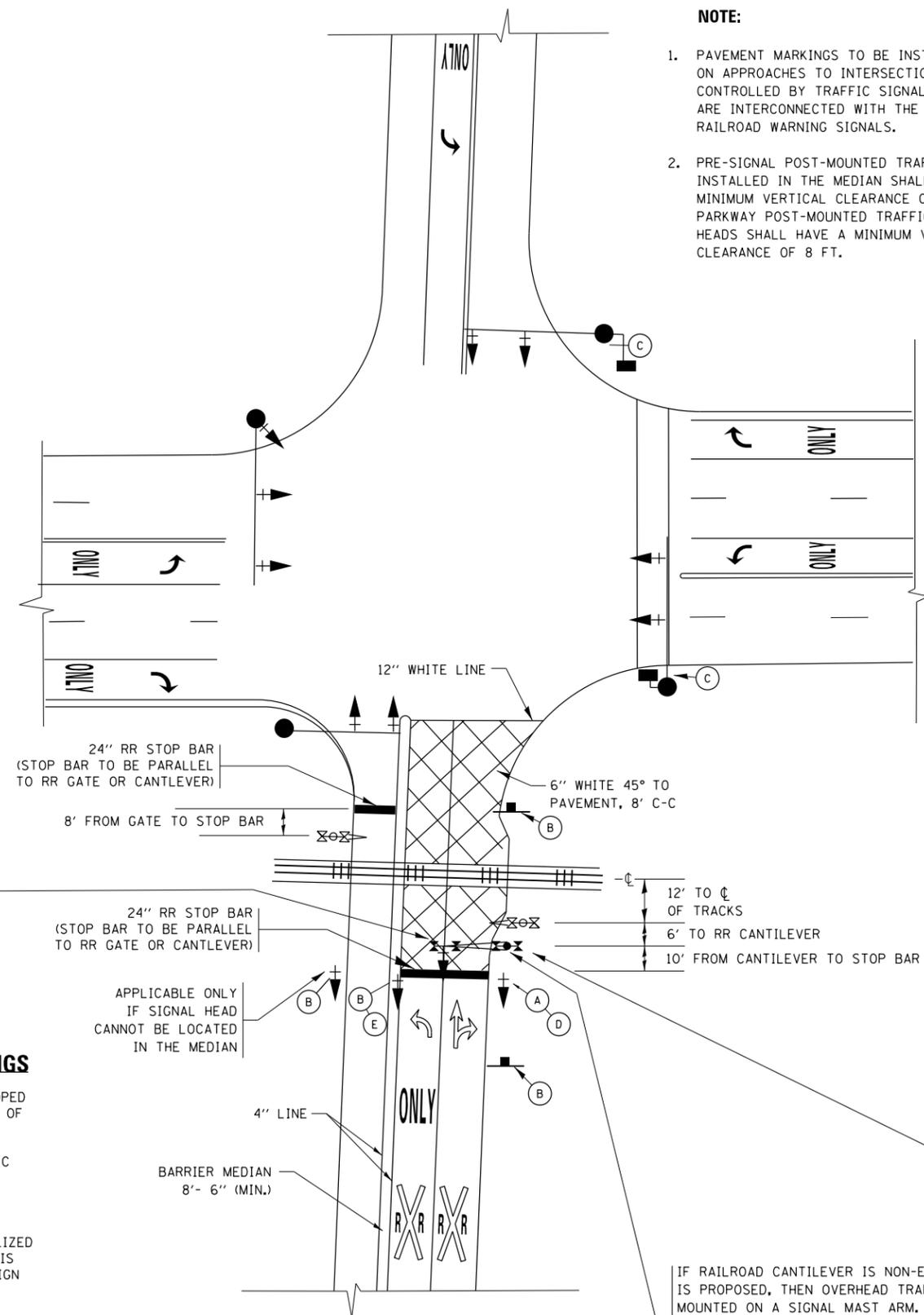
**RAILROAD CANTILEVER SIGNAL HEAD MOUNTING**  
 USE NON-CONDUCTIVE SPACERS BETWEEN THE TRAFFIC SIGNAL EQUIPMENT AND THE RAILROAD CANTILEVER TO PREVENT DISSIMILAR METAL CORROSION  
 N.T.S.

**SIGNING AND PAVEMENT MARKING AT RAILROAD CROSSINGS**

SIGNING AND PAVEMENT MARKING TRAFFIC CONTROL STANDARD (TC-23) HAS BEEN DEVELOPED IN CONSULTATION WITH THE ILLINOIS COMMERCE COMMISSION AND THE U.S. DEPARTMENT OF TRANSPORTATION'S GRADE CROSSING SAFETY TASK FORCE. THIS STANDARD PROVIDES INFORMATION ON UPDATES TO THE PAVEMENT MARKING AND SIGNING DETAILS IN ORDER TO INCORPORATE CHANGES ADOPTED IN THE 2009 NATIONAL MANUAL ON UNIFORM TRAFFIC CONTROL DEVICE (MUTCD). THESE NEW DETAILS HAVE BEEN STUDIED AND TESTED BY THE DEPARTMENT AND ACCEPTED BY THE ILLINOIS COMMERCE COMMISSION.

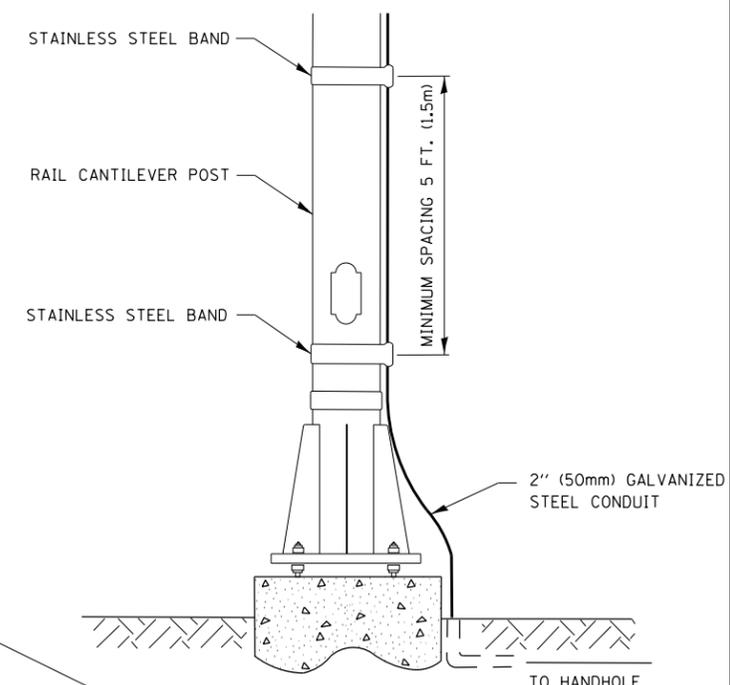
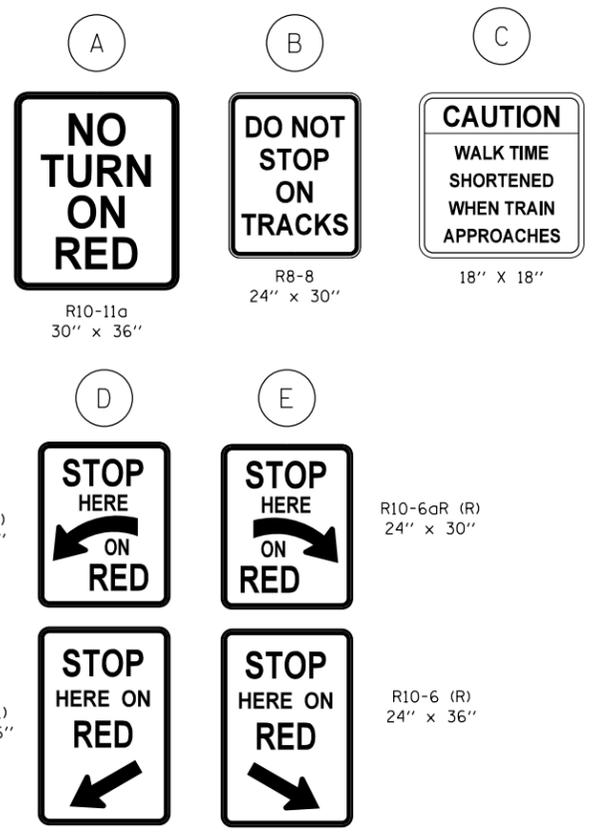
THIS APPLIES TO PROJECTS WHICH INCLUDE RAILROAD INTERCONNECTED TRAFFIC SIGNALS, WITH OR WITHOUT PRE-SIGNALS. THIS STANDARD ALSO APPLIES TO NON-SIGNALIZED INTERSECTIONS THAT ARE WITHIN 81 FEET OF A RAILROAD GRADE CROSSING. THE ILLINOIS SUPPLEMENT TO THE MUTCD SHOULD BE CONSULTED FOR ADDITIONAL INFORMATION ON SIGN REQUIREMENTS AT NON-SIGNALIZED INTERSECTIONS NEAR RAILROAD GRADE CROSSINGS.

THESE DETAILS WILL BE INCLUDED IN A FUTURE UPDATE TO THE BUREAU OF OPERATIONS TRAFFIC POLICIES AND PROCEDURES MANUAL.



**SIGNALIZED INTERSECTION WITH NEAR-SIDE TRAFFIC SIGNAL**

- NOTE:**
- PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
  - PRE-SIGNAL POST-MOUNTED TRAFFIC SIGNAL HEADS INSTALLED IN THE MEDIAN SHALL HAVE A MINIMUM VERTICAL CLEARANCE OF 4.5 FT. PARKWAY POST-MOUNTED TRAFFIC SIGNAL HEADS SHALL HAVE A MINIMUM VERTICAL CLEARANCE OF 8 FT.



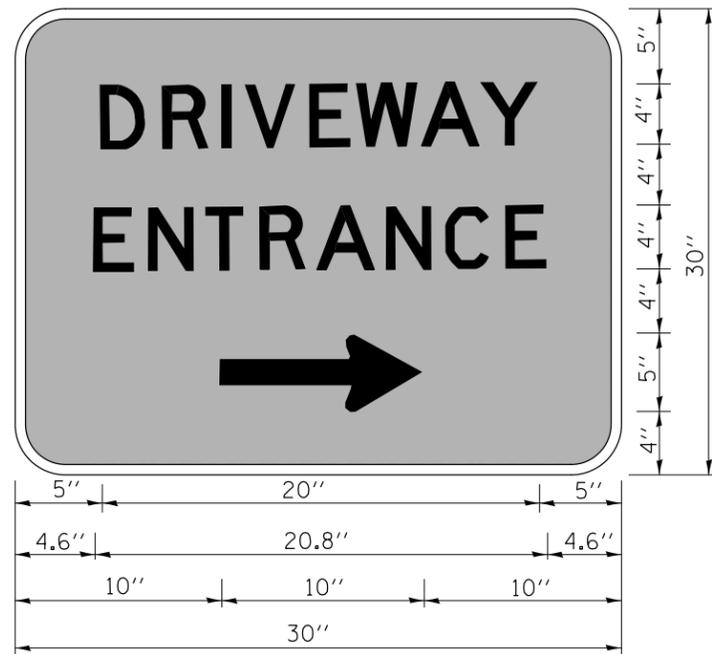
**SIGNAL CONDUIT CONNECTION TO RAIL CANTILEVER DETAIL**

USE NON-CONDUCTIVE SPACERS BETWEEN THE TRAFFIC SIGNAL EQUIPMENT AND THE RAILROAD CANTILEVER TO PREVENT DISSIMILAR METAL CORROSION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

IF RAILROAD CANTILEVER IS NON-EXISTANT AND NONE IS PROPOSED, THEN OVERHEAD TRAFFIC SIGNAL TO BE MOUNTED ON A SIGNAL MAST ARM. SIGNAL MAST ARM AND SIGNAL HEADS SHALL BE INSTALLED AS CLOSE AS PRACTICABLE TO THE RAILROAD TRACKS WITHOUT OBSTRUCTING ANY RAILROAD WARNING DEVICES. SIGNAL MAST ARM SHALL BE AT LEAST 12 FT. FROM NEAREST RAIL.

FILE NAME =	USER NAME = currywaw	DESIGNED -	REVISED - 02-25-11	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING TREATMENT FOR RAILROAD CROSSINGS</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBID\INTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI34\BROWN\Design\DistStd.dgn	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - 04-26-12			344	2018-010-RS-SW	COOK	61	57
Default	PLOT DATE = 12/6/2018	DATE -	REVISED - A.R. 07-11-16			<b>TC-23</b>		<b>CONTRACT NO. 62880</b>		
			REVISED -			ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED  
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

**NOTES:**

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE  
 PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN)  
 SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY  
 AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE  
 FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME =	USER NAME = curryw	DESIGNED -	REVISED - C. JUCIUS 02-15-07
p:\11\084EBIDINTEG\illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\DI34\BROW\Notes\Design\DistStd.dgn		11/13/2018	REVISED -
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 12/6/2018	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**DRIVEWAY ENTRANCE SIGNING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
344	2018-010-RS-SW	COOK	61	58
<b>TC-26</b>			<b>CONTRACT NO. 62680</b>	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

# TRAFFIC SIGNAL LEGEND

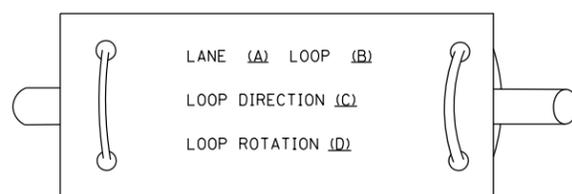
(NOT TO SCALE)

ITEM	EXISTING	PROPOSED	ITEM	EXISTING	PROPOSED	ITEM	EXISTING	PROPOSED
CONTROLLER CABINET			HANDHOLE -SQUARE -ROUND	 	 	SIGNAL HEAD -(P) PROGRAMMABLE SIGNAL HEAD		
COMMUNICATION CABINET			HEAVY DUTY HANDHOLE -SQUARE -ROUND	 	 	SIGNAL HEAD WITH BACKPLATE -(P) PROGRAMMABLE SIGNAL HEAD -(RB) RETROREFLECTIVE BACKPLATE		
MASTER CONTROLLER			DOUBLE HANDHOLE			PEDESTRIAN SIGNAL HEAD AT RAILROAD INTERSECTIONS		
MASTER MASTER CONTROLLER			JUNCTION BOX			PEDESTRIAN SIGNAL HEAD WITH COUNTDOWN TIMER		
UNINTERRUPTABLE POWER SUPPLY			RAILROAD CANTILEVER MAST ARM			ILLUMINATED SIGN "NO LEFT TURN"/"NO RIGHT TURN"	 	 
SERVICE INSTALLATION -(P) POLE MOUNTED			RAILROAD FLASHING SIGNAL			NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE. ALL DETECTOR LOOP CABLE TO BE SHIELDED		
SERVICE INSTALLATION -(G) GROUND MOUNTED -(GM) GROUND MOUNTED METERED	 	 	RAILROAD CROSSING GATE			GROUND CABLE IN CONDUIT, NO. 6 SOLID COPPER (GREEN)		
TELEPHONE CONNECTION			RAILROAD CROSSBUCK			ELECTRIC CABLE IN CONDUIT, TRACER NO. 14 1/C		
STEEL MAST ARM ASSEMBLY AND POLE			RAILROAD CONTROLLER CABINET			COAXIAL CABLE		
ALUMINUM MAST ARM ASSEMBLY AND POLE			UNDERGROUND CONDUIT (UC), GALVANIZED STEEL			VENDOR CABLE		
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE			TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE			COPPER INTERCONNECT CABLE, NO. 18, 3 PAIR TWISTED, SHIELDED		
SIGNAL POST -(BM) BARREL MOUNTED - TEMPORARY			SYSTEM ITEM	S	SP	FIBER OPTIC CABLE -NO. 62.5/125, MM12F -NO. 62.5/125, MM12F SM12F -NO. 62.5/125, MM12F SM24F	  	  
WOOD POLE			INTERSECTION ITEM	I	IP	GROUND ROD -(C) CONTROLLER -(M) MAST ARM -(P) POST -(S) SERVICE	   	   
GUY WIRE			REMOVE ITEM		R			
SIGNAL HEAD			RELOCATE ITEM		RL			
SIGNAL HEAD WITH BACKPLATE			ABANDON ITEM		A			
SIGNAL HEAD OPTICALLY PROGRAMMED			CONTROLLER CABINET AND FOUNDATION TO BE REMOVED		RCF			
FLASHER INSTALLATION -(FS) SOLAR POWERED	 	 	MAST ARM POLE AND FOUNDATION TO BE REMOVED		RMF			
PEDESTRIAN SIGNAL HEAD			SIGNAL POST AND FOUNDATION TO BE REMOVED		RPF			
PEDESTRIAN PUSH BUTTON -(APS) ACCESSIBLE PEDESTRIAN PUSH BUTTON	 	 	DETECTOR LOOP, TYPE I					
RADAR DETECTION SENSOR			PREFORMED DETECTOR LOOP					
VIDEO DETECTION CAMERA			SAMPLING (SYSTEM) DETECTOR					
RADAR/VIDEO DETECTION ZONE			INTERSECTION AND SAMPLING (SYSTEM) DETECTOR					
PAN, TILT, ZOOM (PTZ) CAMERA			QUEUE AND SAMPLING (SYSTEM) DETECTOR					
EMERGENCY VEHICLE LIGHT DETECTOR			WIRELESS DETECTOR SENSOR					
CONFIRMATION BEACON			WIRELESS ACCESS POINT					
WIRELESS INTERCONNECT								
WIRELESS INTERCONNECT RADIO REPEATER								

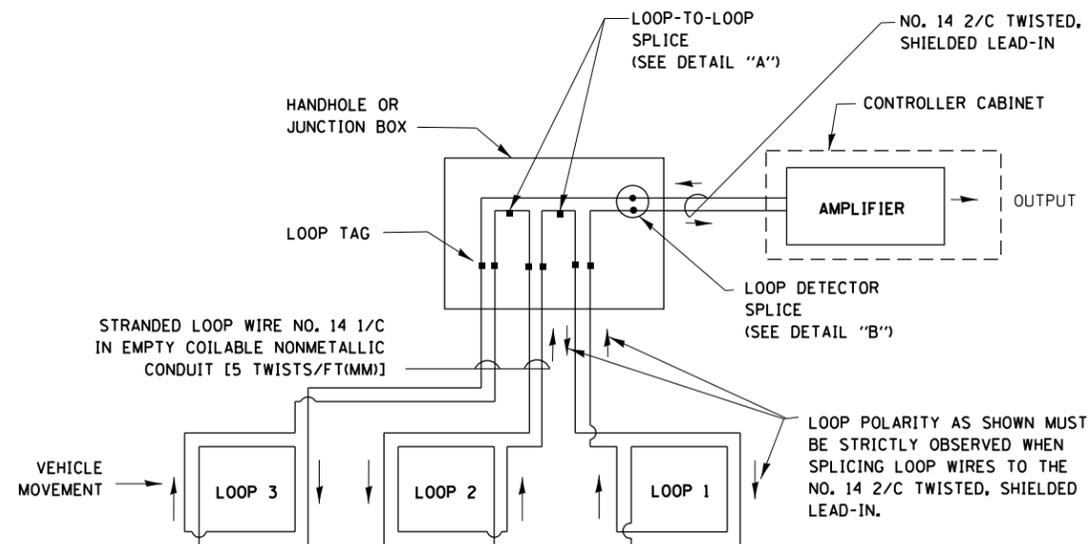
**LOOP DETECTOR NOTES**

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

**LOOP LEAD-IN CABLE TAG**

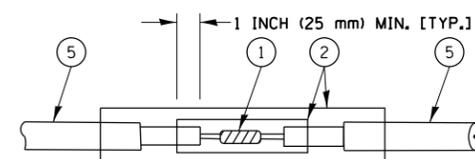


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

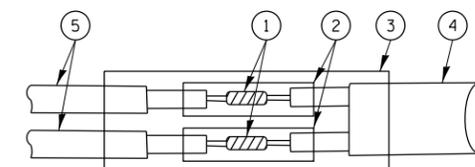


**DETECTOR LOOP WIRING SCHEMATIC**

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

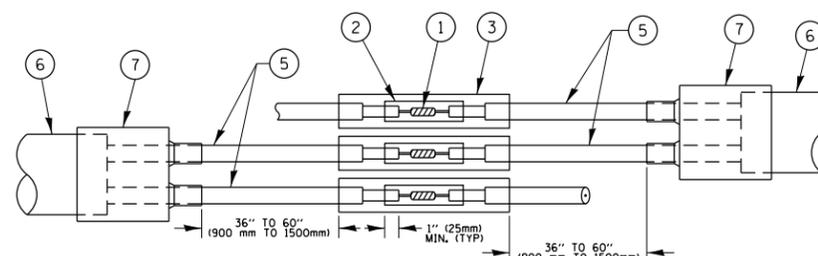


DETAIL "A"  
LOOP-TO-LOOP SPLICE

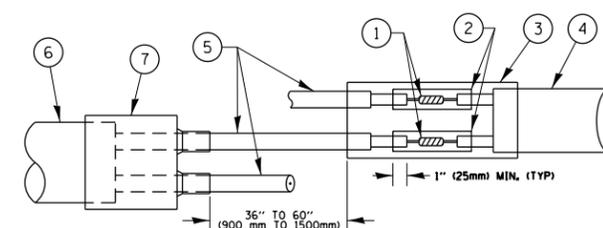


DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

**TYPE I LOOP**



DETAIL "A"  
LOOP-TO-LOOP SPLICE



DETAIL "B"  
LOOP-TO-CONTROLLER SPLICE

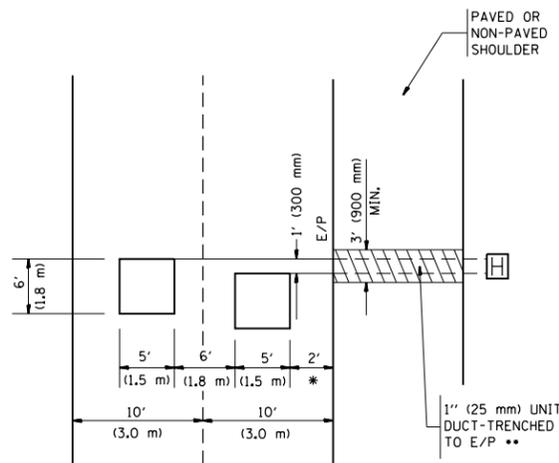
**PREFORMED LOOP**

**LOOP DETECTOR SPLICE**

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PREFORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

**LOOPS NEXT TO SHOULDERS**

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



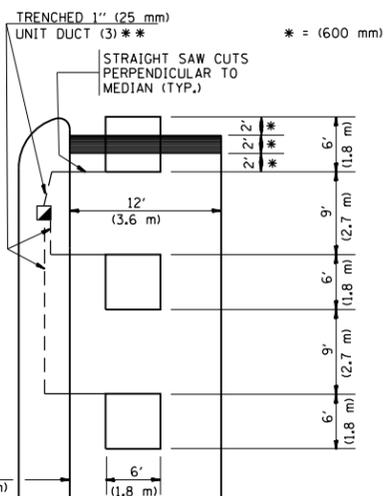
\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

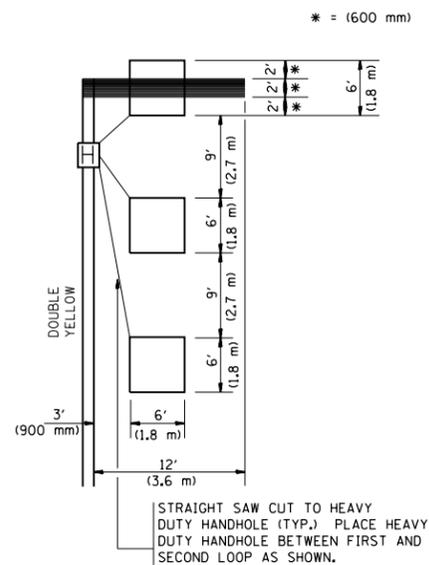


\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

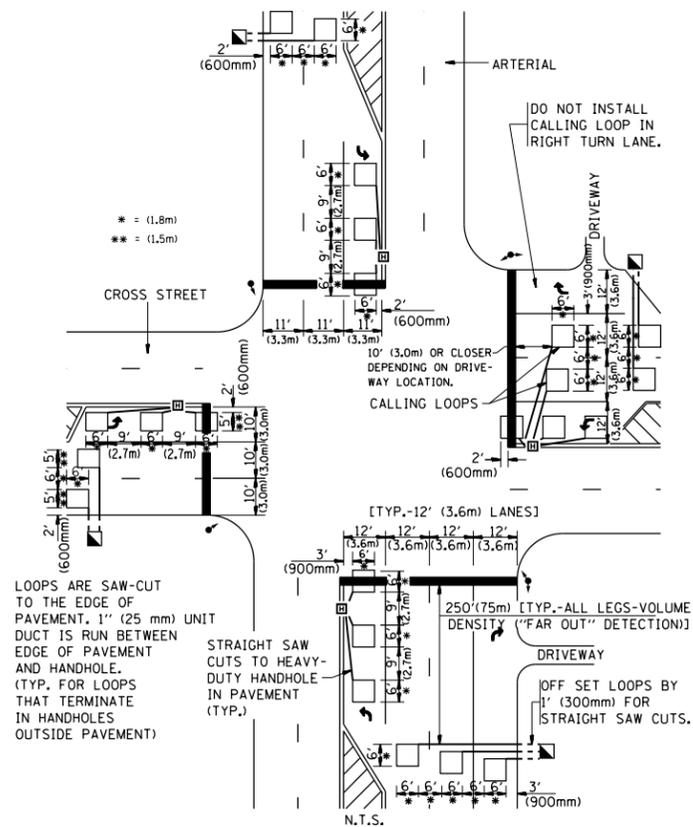
**LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)



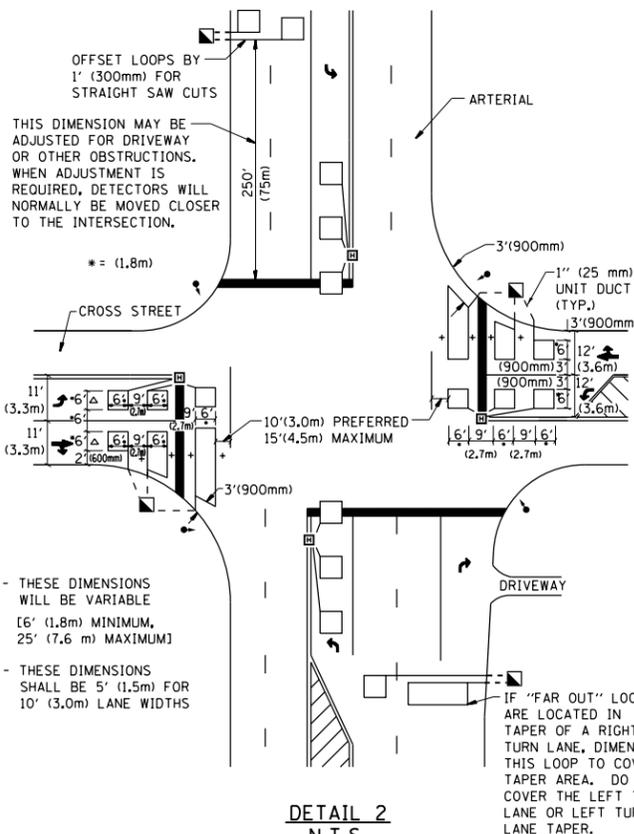
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1**  
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2**  
N.T.S.

**NOTES:**

**VEHICLES LOOP DETECTORS**

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

**PLACEMENT OF DETECTORS**

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

**NOTE:**

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = currywaw	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING</b>			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBID\INTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\DI34\BRAW\Notes\Design\DistStd.dgn		CHECKED - R.K.F.	REVISED -					344	2018-010-RS-SW	COOK	61	61
PLOT SCALE = 100.0000' / in.		DATE -	REVISED -					<b>TS-07</b>		<b>CONTRACT NO. 62C80</b>		
PLOT DATE = 12/6/2018			REVISED -					FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
				SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.					