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# CONSTRUCTION PLANS FOR WILLARD AIRPORT

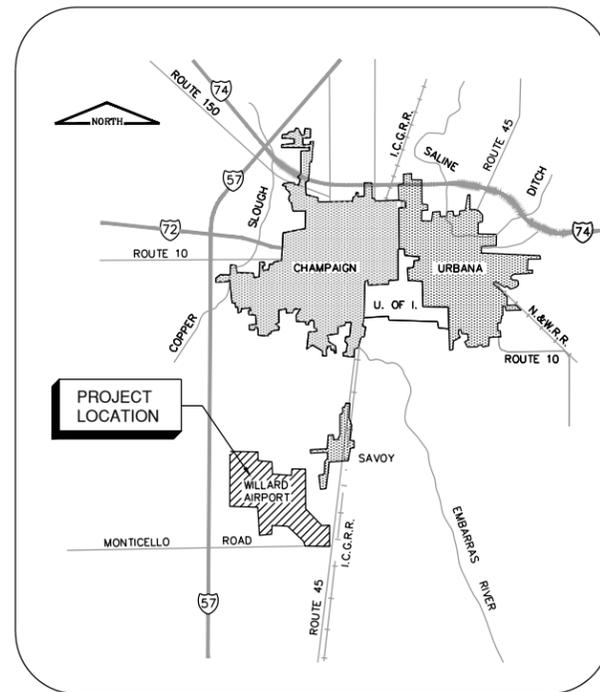
## UNIVERSITY OF ILLINOIS SAVOY, IL

### IL. PROJ. NO: CMI-5043

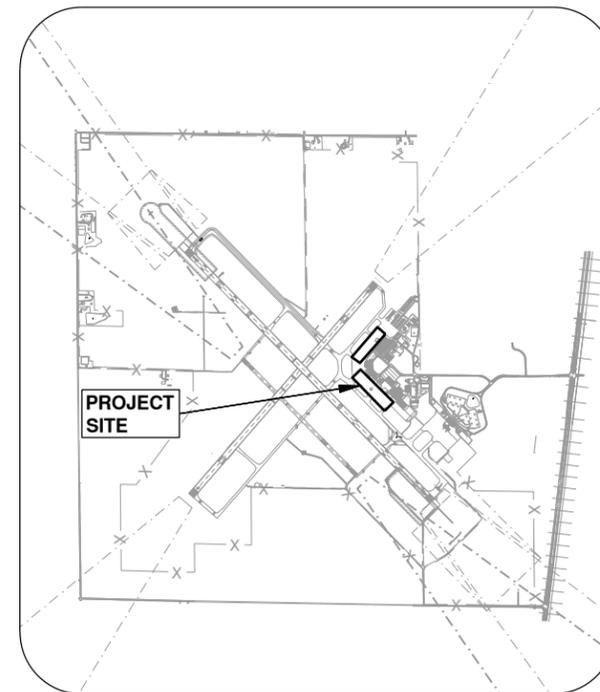
# REHABILITATE GENERAL AVIATION APRONS

SUMMARY OF QUANTITIES			
<b>BASE BID</b>			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY
AR150520	MOBILIZATION	L SUM	1
AR401610	BITUMINOUS SURFACE COURSE	TON	190
AR401650	BITUMINOUS PAVEMENT MILLING	SQ YD	1,275
AR401655	BUTT JOINT CONSTRUCTION	SQ YD	350
AR501509	9" PCC PAVEMENT	SQ YD	1,800
AR501510	10" PCC PAVEMENT	SQ YD	625
AR501530	PCC TEST BATCH	EACH	1
AR501900	REMOVE PCC PAVEMENT	SQ YD	2,425
AR501922	PCC SPALL REPAIR	SQ FT	60
AR510901	REMOVE TIE DOWN - TYPE A	EACH	9
AR603510	BITUMINOUS TACK COAT	GAL	250
AR620520	PAVEMENT MARKING - WATERBORNE	SQ FT	2,375
<b>ADDITIVE ALTERNATE #1 - SELECT PCC PANEL REPAIR</b>			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY
AS150520	MOBILIZATION	L SUM	1
AS501510	10" PCC PAVEMENT	SQ YD	400
AS501900	REMOVE PCC PAVEMENT	SQ YD	400
<b>ADDITIVE ALTERNATE #2 - SELECT PCC PANEL REPAIR 2</b>			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY
AT150520	MOBILIZATION	L SUM	1
AT501510	10" PCC PAVEMENT	SQ YD	150
AT501900	REMOVE PCC PAVEMENT	SQ YD	150
<b>ADDITIVE ALTERNATE #3 - MISC. SPALL REPAIR</b>			
ITEM NO.	DESCRIPTION	UNIT	QUANTITY
AU501922	PCC SPALL REPAIR	SQ FT	160
AU510511	TIE DOWN - TYPE A	EACH	9
AU605510	JOINT SEALING FILLER	FOOT	11,800

MARCH 07, 2025



LOCATION MAP



SITE PLAN

GROUND CONTROL RADIO FREQUENCY - 121.8  
 ATIS FREQUENCY - 124.85  
 MAXIMUM HEIGHT OF EQUIPMENT  
 ABOVE GROUND IS 25 FT.

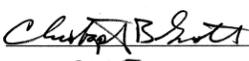
UNIVERSITY OF ILLINOIS



APPROVED   
 PRINTED Timothy A. Bannon, A.A.E.  
 DATE January 10, 2025



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SUBMITTED BY   
 DATE January 10, 2025

CMT JOB NUMBER: 21001968



CALL J.U.L.I.E.  
 BEFORE EXCAVATING  
 1-800-892-0123  
 UNIVERSITY OF ILLINOIS - WILLARD AIRPORT  
 TOWNSHIP: T 18 N  
 RANGE: R 8 E  
 COUNTY: CHAMPAIGN  
 SECTION 2, 3, 10 AND 11

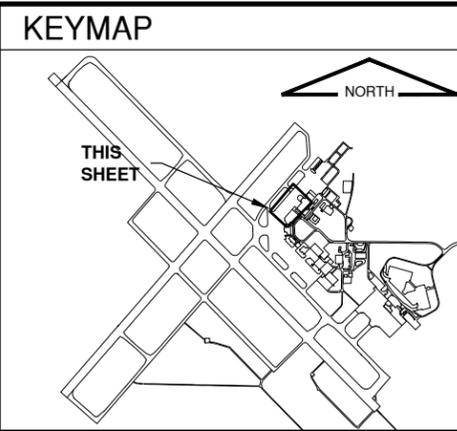
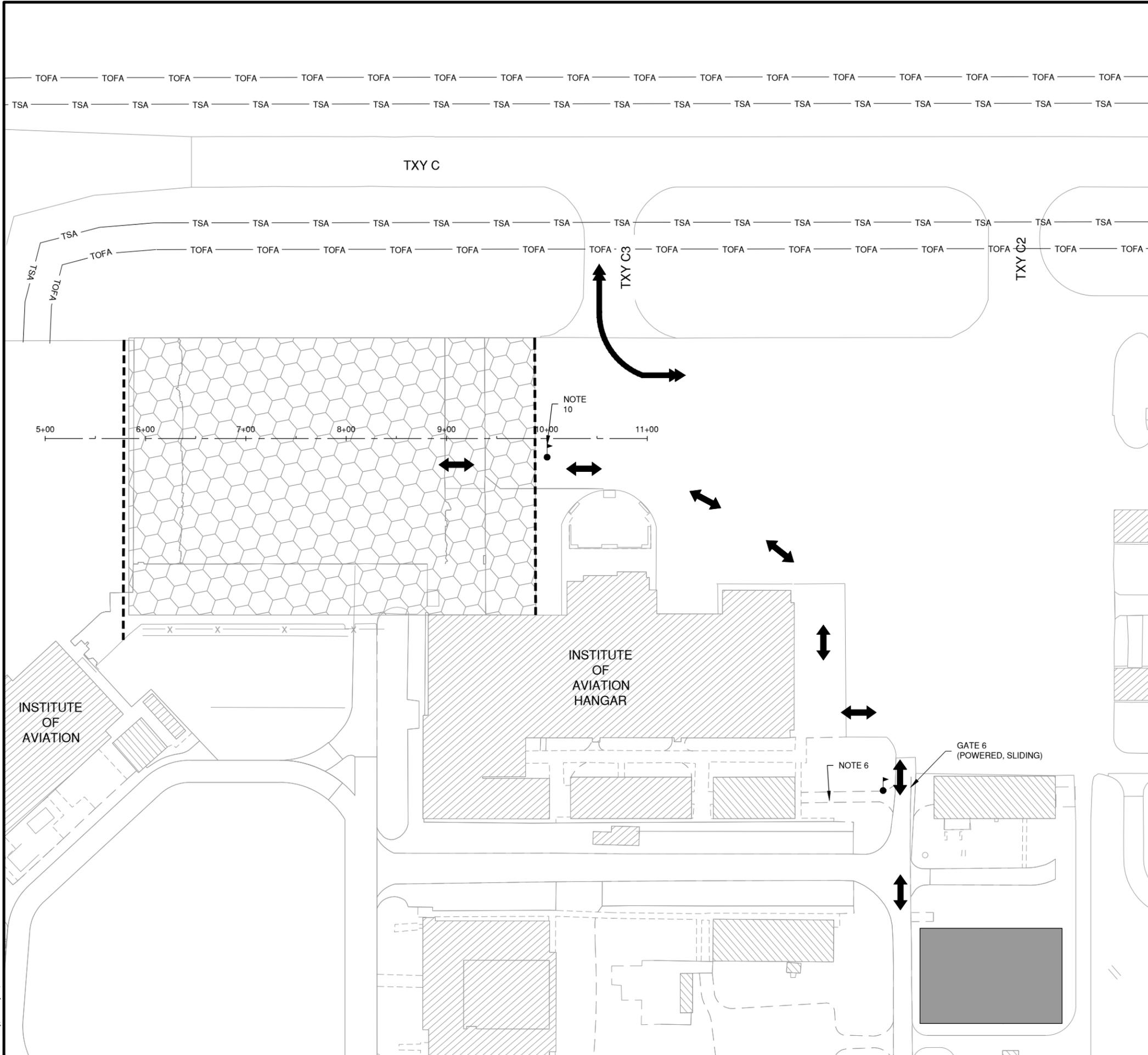
**TAXIWAY A**  
 AIRPLANE DESIGN GROUP - ADG III  
 TAXIWAY DESIGN GROUP III  
 DESIGN APPROACH CATEGORY - C



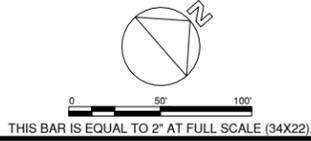








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**LEGEND**

- CONTRACTORS STAGING AND STORAGE AREA
- WORK AREA 1
- CONTRACTORS ACCESS ROUTE
- AIRCRAFT TAXI ROUTE
- GATE GUARD / FLAGGER
- BEAM BARRICADES

100%  
MARCH 07, 2025

REHABILITATE GENERAL AVIATION APRONS



UNIVERSITY OF ILLINOIS WILLARD AIRPORT SAVOY, IL

**NOTES**

1. THE CONTRACTOR SHALL PLACE ALL BARRICADES AS SHOWN PRIOR TO STARTING WORK ON ANY WORK AREA.
2. THE AIRPORT WILL REQUIRE 7 DAYS NOTICE PRIOR TO INITIATING WORK IN ANY WORK AREAS TO COORDINATE WITH THE INSTITUTE OF AVIATION. THIS WORK AREA IS INTENDED TO BE SCHEDULED AROUND THE INSTITUTE OF AVIATION'S OPERATIONS.
3. WORK AREAS MAY BE ACTIVE CONCURRENTLY WITH THE APPROVAL OF THE AIRPORT.
4. STAGING AND STORAGE AREA SHALL BE RESTORED TO ORIGINAL CONDITION PRIOR TO PROJECT COMPLETION.
5. COORDINATION WITH THE INSTITUTE OF AVIATION AND ALL INVOLVED PARTIES WILL BE REQUIRED WHEN REPAINTING EXISTING MARKINGS.
6. RPR FIELD OFFICE LOCATION TO BE APPROVED BY AIRPORT PRIOR TO PLACEMENT.
7. CONTRACTOR SHALL CONTINUOUSLY SWEEP ACCESS ROUTE AND KEEP FREE OF F.O.D.
8. GATE GUARDS SHALL BE USED IF POWERED GATE IS TO REMAIN OPEN FOR AN EXTENDED PERIOD OF TIME.
9. CONSTRUCTION TRAFFIC SHALL YIELD TO AIRCRAFT AND AIRPORT OPERATIONS AT ALL TIMES.
10. FLAGGER DURING PAVING / HIGH TRAFFIC DAYS TO DIRECT TRAFFIC.

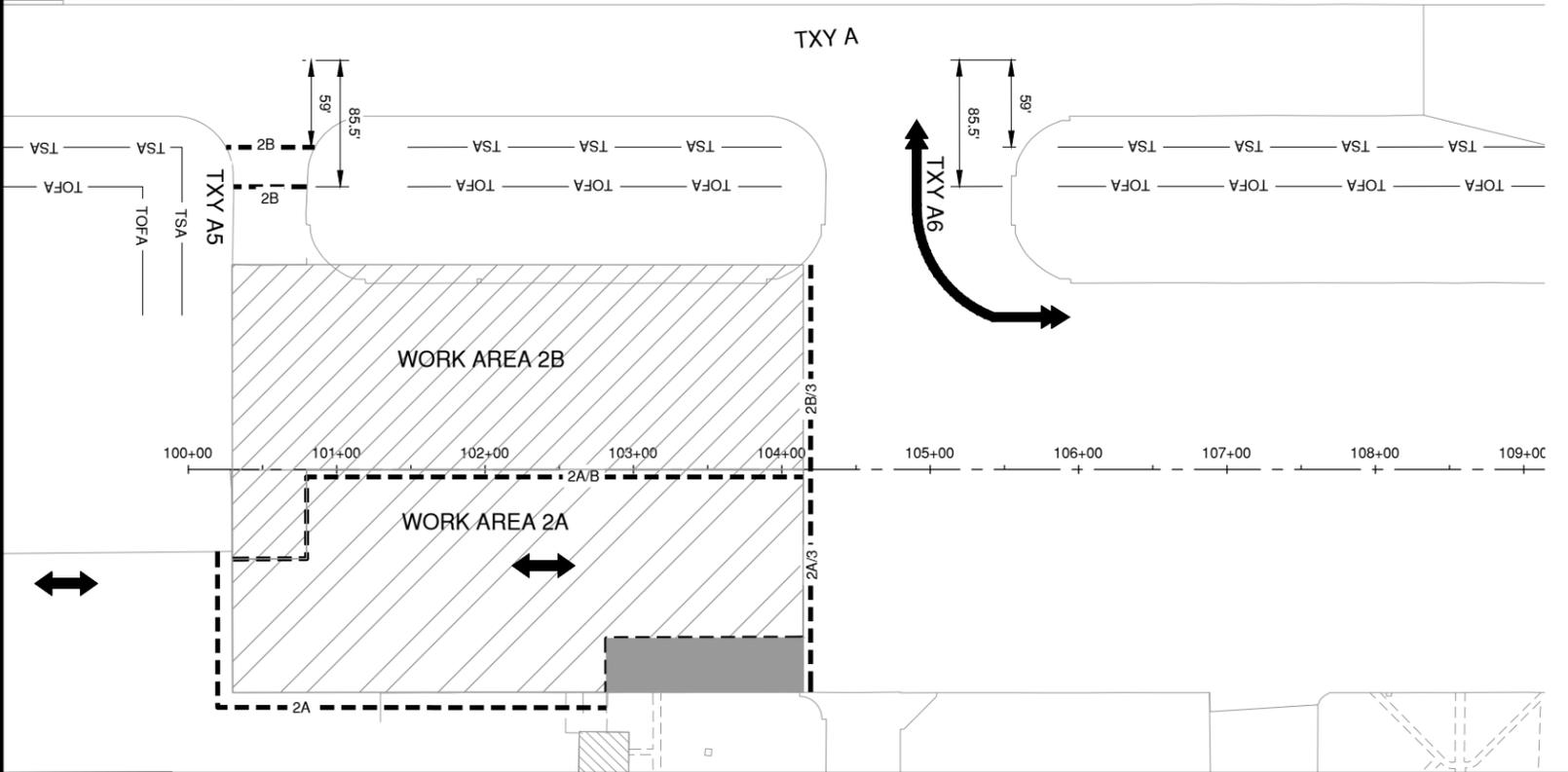
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IL PROJ. NO: CMI-5043	
CMT PROJECT NO: 21001968	
CAD DWG FILE: 21001968-GC100.DWG	
DESIGNED BY: MRK	
DRAWN BY: MRK	
CHECKED BY: MJD	
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SHEET TITLE  
**CONSTRUCTION ACTIVITY PLAN 1**

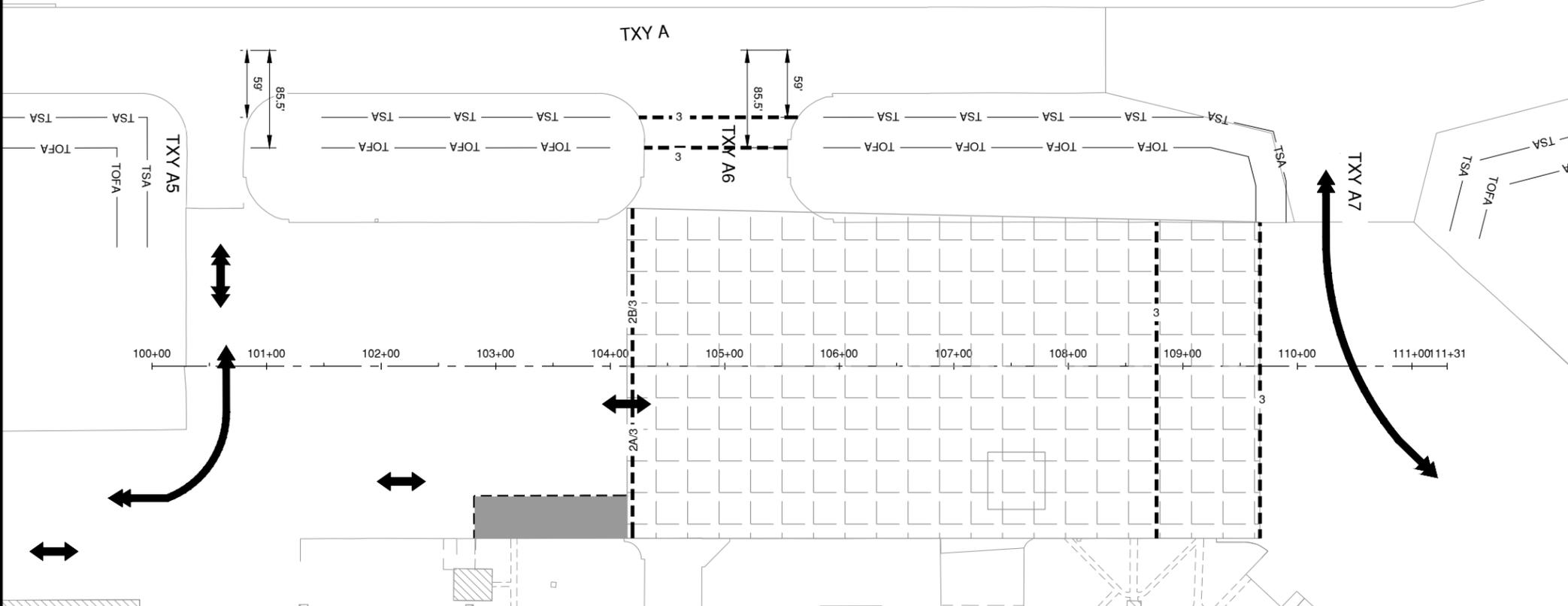
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Date: Friday, January 17, 2025 10:42:45 AM

**WORK AREA 2A / 2B**



LOCATION	PAVEMENT STATUS			
	1	2A	2B	3
RUNWAY 14L/32R	OPEN	OPEN	OPEN	OPEN
RUNWAY 4/22	OPEN	OPEN	OPEN	OPEN
TAXIWAY A	OPEN	OPEN	OPEN	OPEN
TAXIWAY A5	OPEN	OPEN	CLOSED	OPEN
TAXIWAY A6	OPEN	OPEN	OPEN	CLOSED
TAXIWAY A7	OPEN	OPEN	OPEN	OPEN
TAXIWAY C	OPEN	OPEN	OPEN	OPEN
TAXIWAY C3	OPEN	OPEN	OPEN	OPEN
TAXIWAY C4	OPEN	OPEN	OPEN	OPEN

**WORK AREA 3**



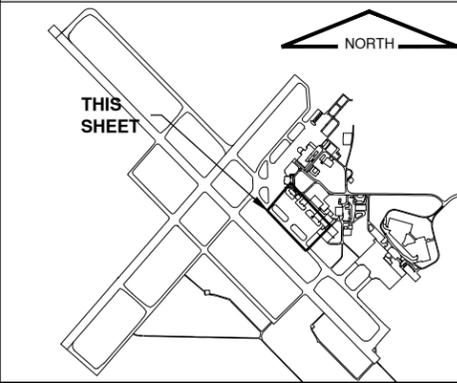
**WORK AREA 2A NOTES**

1. WORK ITEMS TO BE COMPLETED DURING THIS PHASE SHALL INCLUDE FULL DEPTH PANEL REMOVAL AND REPLACEMENT, INSTALLATION OF TIE DOWNS, JOINT SEALING, AND PAVEMENT MARKINGS.
2. PHASE 2A IS A HIGH PRIORITY WORK AREA. SCHEDULING THIS WORK MAY REQUIRE  $\frac{24}{7}$  OPERATIONS TO COMPLETE.
3. PHASE 2A SHALL BE COMPLETED IN 7 CALENDAR DAYS.

**WORK AREA 2B NOTES**

1. WORK ITEMS TO BE COMPLETED DURING THIS PHASE SHALL INCLUDE FULL DEPTH PANEL REMOVAL AND REPLACEMENT, INSTALLATION OF TIE DOWNS, JOINT SEALING, AND PAVEMENT MARKINGS.
2. WORK IN THIS PHASE REQUIRES THE CLOSURE OF TAXIWAY A5.

**KEYMAP**



**LEGEND**

- CONTRACTORS STAGING AND STORAGE AREA
- WORK AREA 2A
- WORK AREA 2B
- WORK AREA 3
- CONTRACTORS ACCESS ROUTE
- AIRCRAFT TAXI ROUTE
- BEAM BARRICADE / WORK AREA TXY A5/A6

**NOTES**

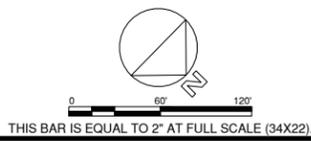
1. THE CONTRACTOR SHALL PLACE ALL BARRICADES AS SHOWN PRIOR TO STARTING WORK ON ANY WORK AREA.
2. THE AIRPORT WILL REQUIRE 7 DAYS NOTICE PRIOR TO INITIATING WORK IN ANY WORK AREAS TO COORDINATE WITH THE INSTITUTE OF AVIATION & ISSUE NOTAMS.
3. WORK AREAS MAY BE ACTIVE CONCURRENTLY WITH THE APPROVAL OF THE AIRPORT.
4. STAGING AND STORAGE AREA SHALL BE RESTORED TO ORIGINAL CONDITION. PRIOR TO PROJECT COMPLETION.

**WORK AREA 3 NOTES**

1. WORK ITEMS TO BE COMPLETED DURING THIS PHASE SHALL INCLUDE FULL DEPTH PANEL REMOVAL AND REPLACEMENT, INSTALLATION OF TIE DOWNS, JOINT SEALING, AND PAVEMENT MARKINGS.
2. WORK IN THIS PHASE REQUIRES THE CLOSURE OF TAXIWAY A6.
3. BARRICADE SPACING AT STATION 104+20 SHALL HAVE AN INTERVAL TO ALLOW FOR ARFF ACCESS.



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**REHABILITATE GENERAL AVIATION APRONS**



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WILLARD AIRPORT  
SAVOY, IL

MARK	DATE	DESCRIPTION

AIP PROJ. NO:	
IL PROJ. NO: CMI-5043	
CMT PROJECT NO: 21001968	
CAD DWG FILE: 21001968-GC100.DWG	
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**CONSTRUCTION ACTIVITY PLAN 2**

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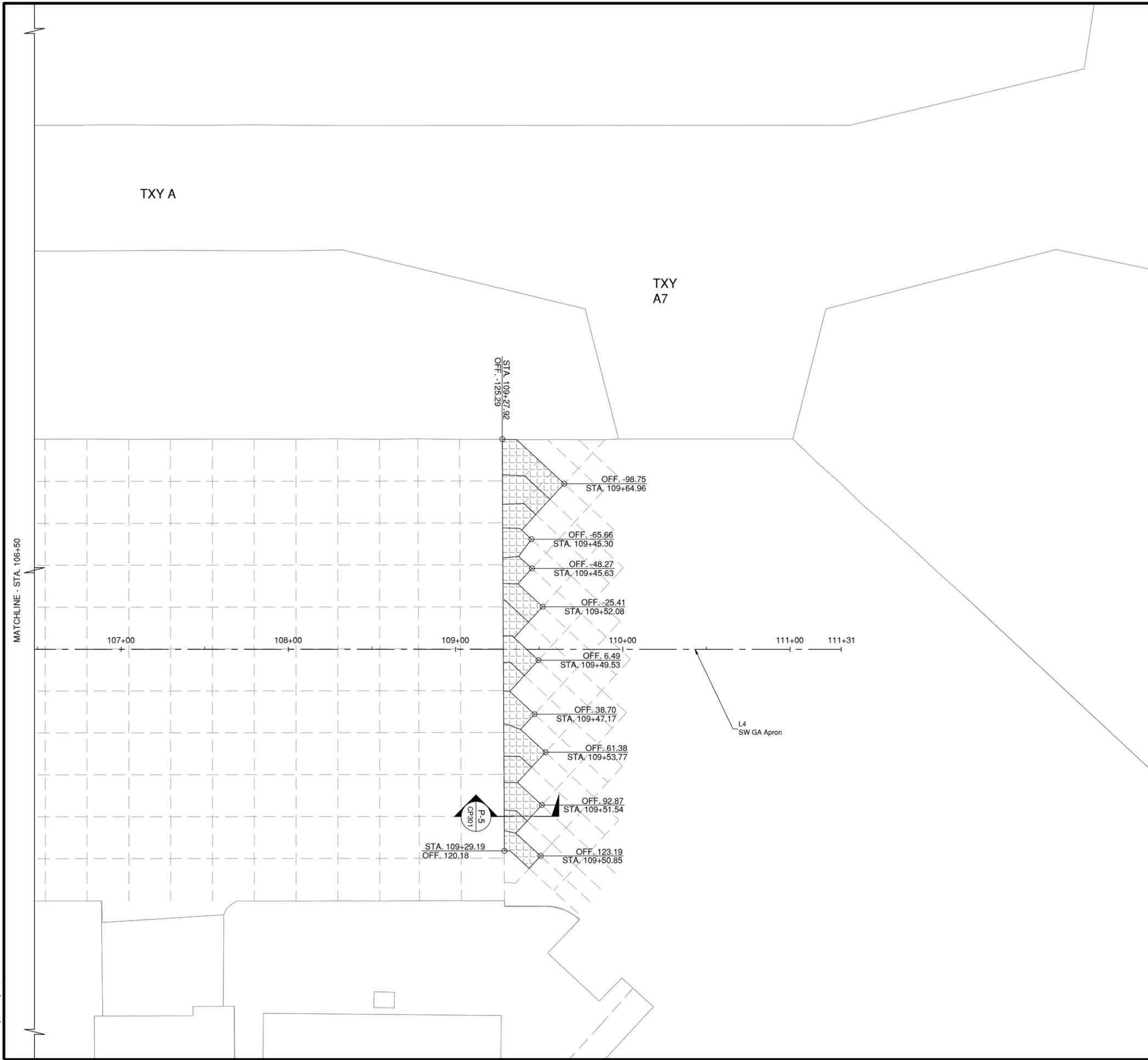




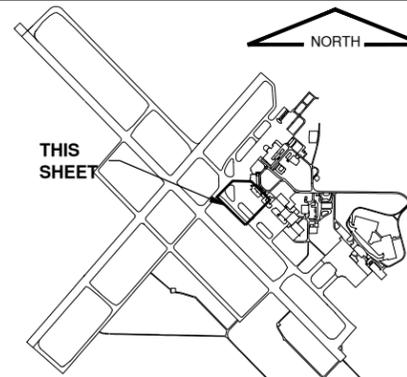




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**KEYMAP**



**LEGEND**

-  NEW 9" & VAR. PCC PANEL REPLACEMENT
-  NEW 10" & VAR. PCC PANEL REPLACEMENT
-  TYPICAL SECTION

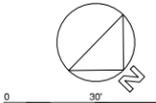
**NOTES**

1. CONTRACTOR SHALL REGRADE AND RECOMPACT BASE TO THE SATISFACTION OF THE RPR IF EXISTING BASE IS DISTURBED AFTER PAVEMENT REMOVAL.
2. FINAL PANEL LOCATIONS TO BE DETERMINED BY THE RESIDENT ENGINEER.
3. THE REMOVAL / CONSTRUCTION LIMITS MAY BE MODIFIED TO FACILITATE MORE UNIFORM REMOVAL AND REPLACEMENT. RE AND CONTRACTOR WILL DETERMINE IN THE FIELD THE MINIMUM LIMITS OF REMOVAL.



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THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

100%  
MARCH 07, 2025

REHABILITATE GENERAL AVIATION APRONS

OWNER



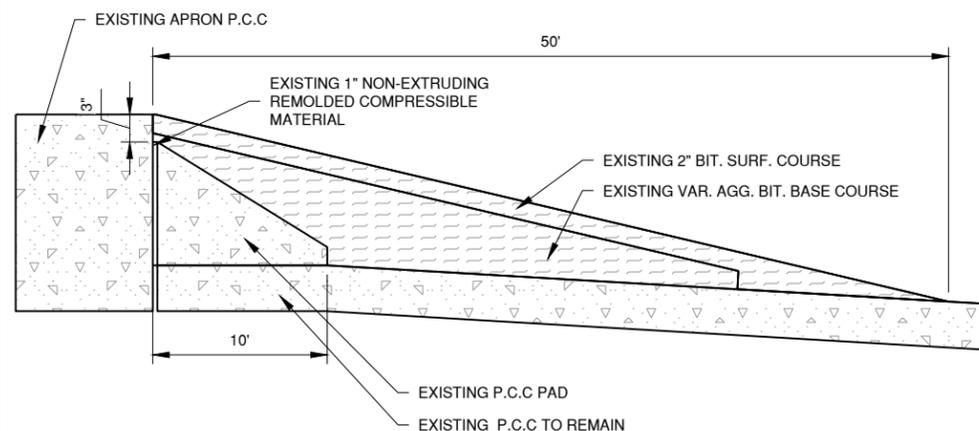
UNIVERSITY OF ILLINOIS  
WILLARD AIRPORT  
SAVOY, IL

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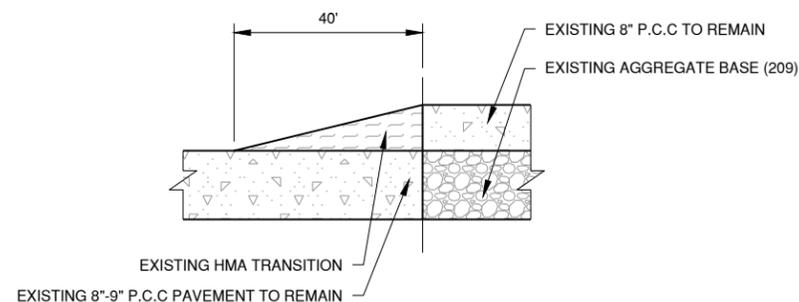
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DESIGNED BY: MRK	
DRAWN BY: MRK	
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SHEET TITLE  
**PROPOSED IMPROVEMENTS 3**

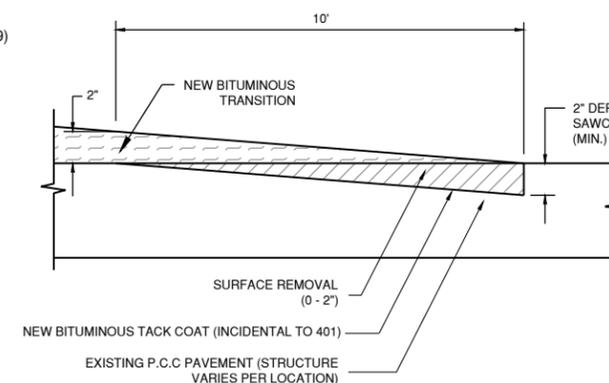
CP103  
SHEET 14 OF 26



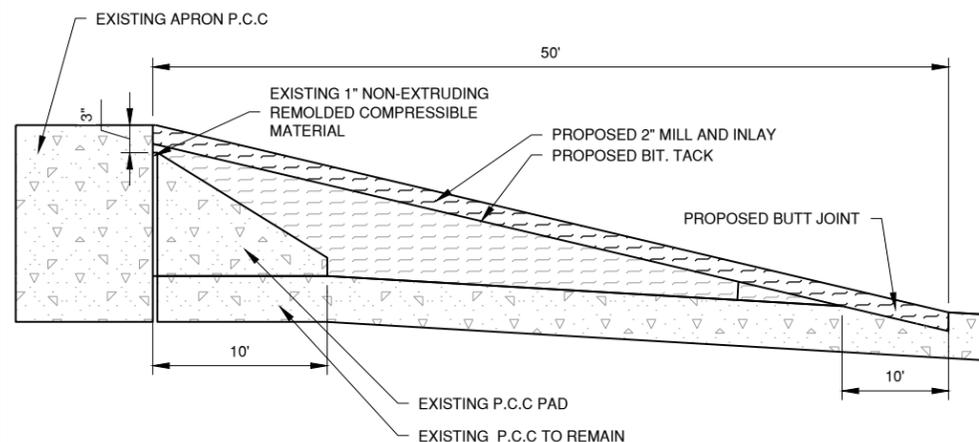
**EXISTING TYPICAL SECTION HMA TRANSITION**  
N.T.S. **E-1**  
CD101



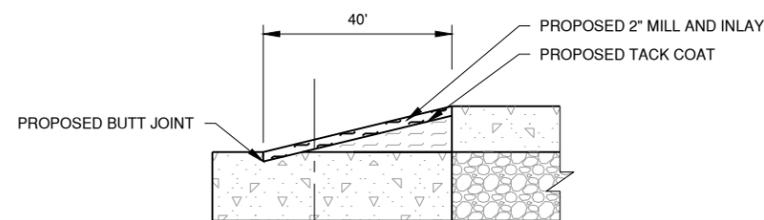
**EXISTING TYPICAL SECTION HMA TRANSITION**  
N.T.S. **E-2**  
CD101



**BUTT JOINT CONSTRUCTION DETAIL (AR401655)**  
NOT TO SCALE



**PROPOSED TYPICAL SECTION HMA TRANSITION**  
N.T.S. **P-1**  
CP101



**PROPOSED TYPICAL SECTION HMA TRANSITION**  
N.T.S. **P-2**  
CP101

**EXISTING CONDITIONS NOTES**

1. PAVEMENT STRUCTURES SHOWN WERE TAKEN FROM RECORD DRAWINGS AND ARE CONSIDERED REPRESENTATIVE OF THE "AS CONSTRUCTED" PAVEMENT SECTION WITH SOME VARIABILITY FROM THE THICKNESS INDICATED TO BE EXPECTED. THERE WILL BE NO ADDITIONAL PAYMENT TO THE CONTRACTOR DUE TO VARIATIONS IN SIZE OR QUANTITY OF EXISTING FEATURES.
2. CONTRACTOR SHALL REGRADE AND RECOMPACT BASE TO THE SATISFACTION OF THE RPR IF EXISTING BASE IS DISTURBED AFTER PAVEMENT REMOVAL.

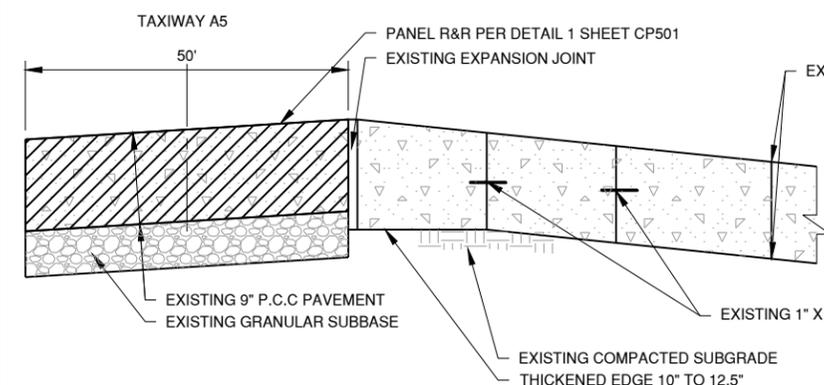
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MARCH 07, 2025

REHABILITATE GENERAL  
AVIATION APRONS

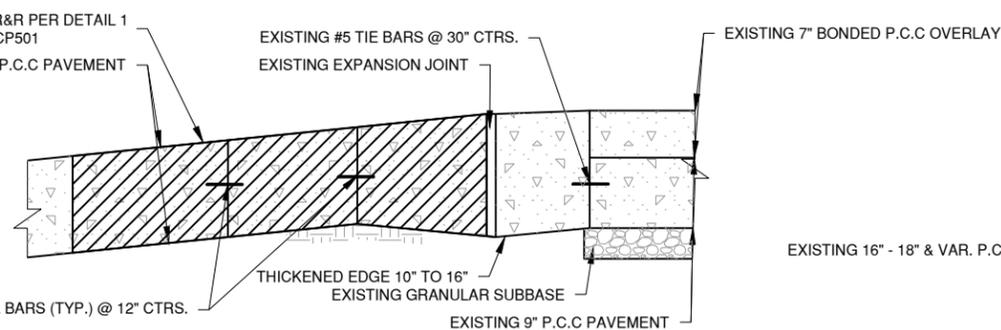
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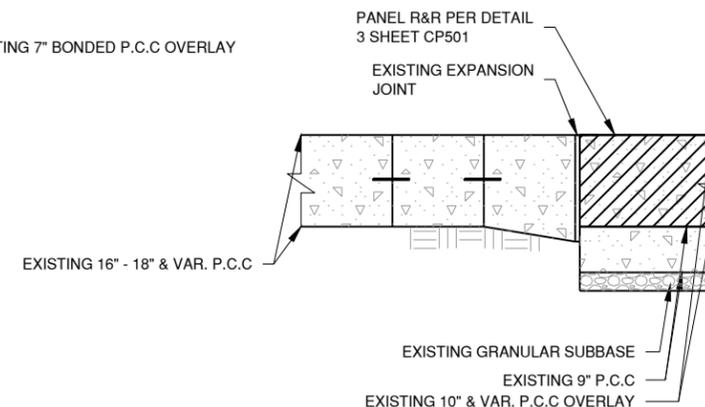
UNIVERSITY OF ILLINOIS  
WILLARD AIRPORT  
SAVOY, IL



**PROPOSED TYPICAL SECTION GA APRON**  
N.T.S. **P-3**  
CD101



**PROPOSED TYPICAL SECTION GA APRON**  
N.T.S. **P-4**  
CD101

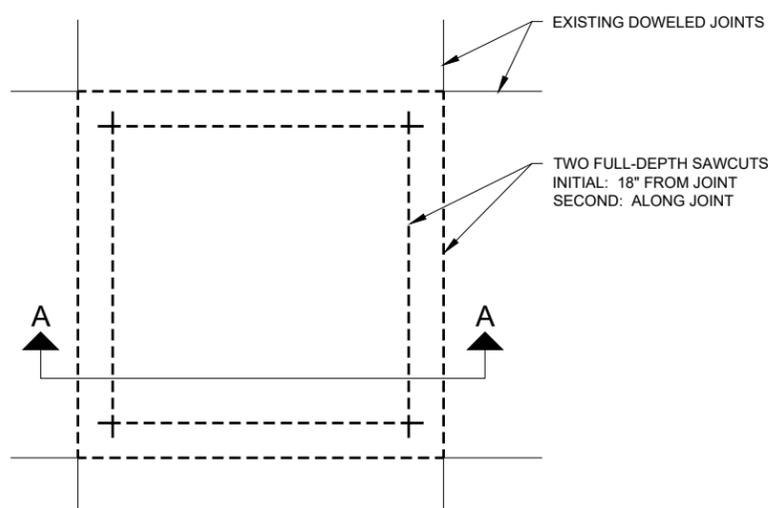


**PROPOSED TYPICAL SECTION GA APRON**  
N.T.S. **P-5**  
CD101

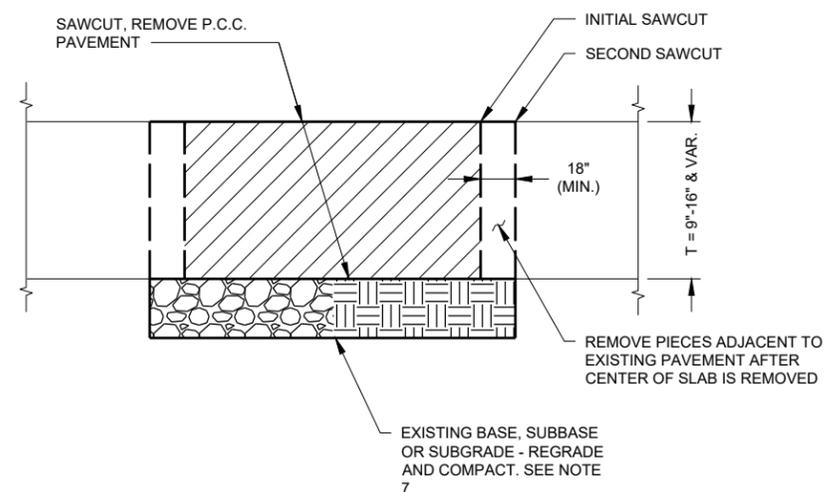
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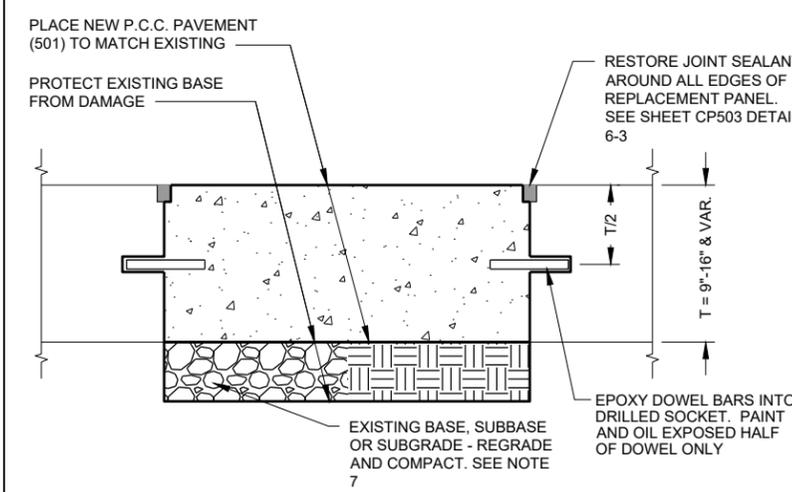
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**TYPICAL SECTIONS**



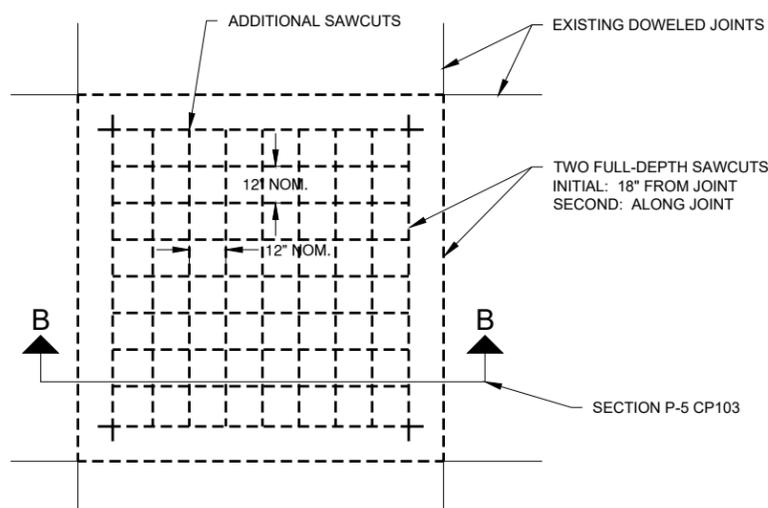
**1 FULL PANEL DEMOLITION SAWCUT DETAIL**  
N.T.S.



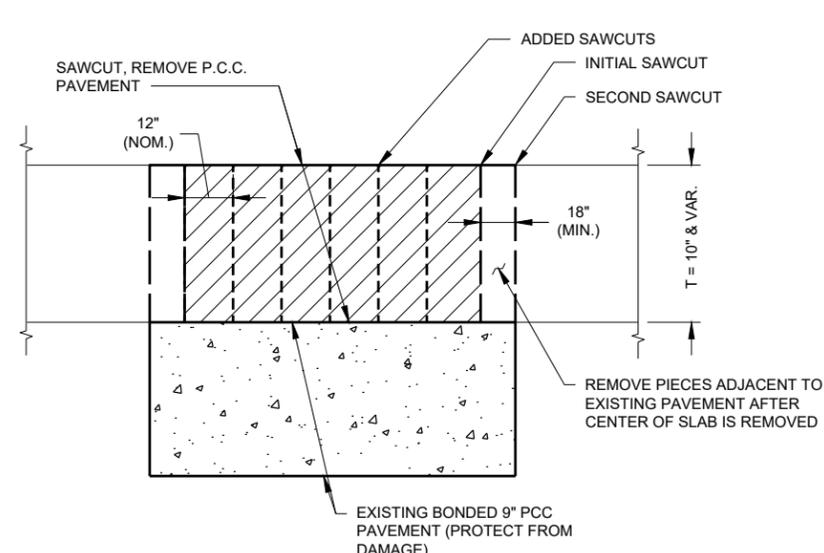
**2 PCC PANEL - REMOVAL DETAIL (SECTION A-A)**  
N.T.S.



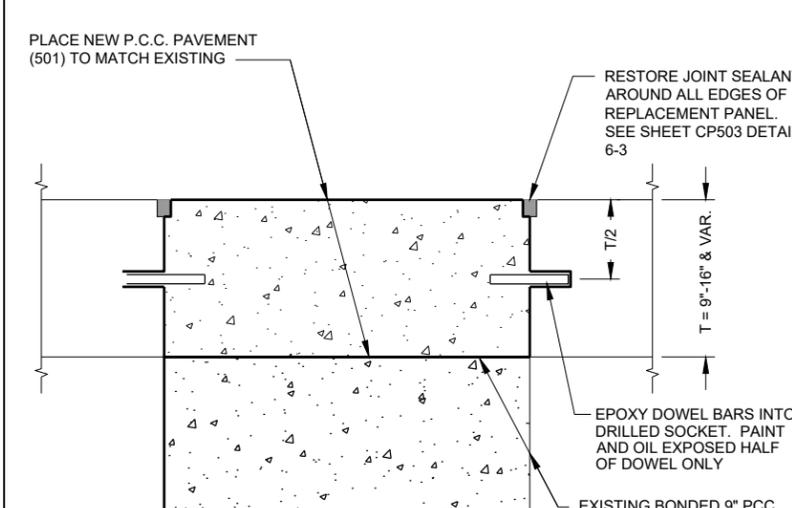
**5 PCC PANEL - PLACEMENT DETAIL**  
N.T.S.



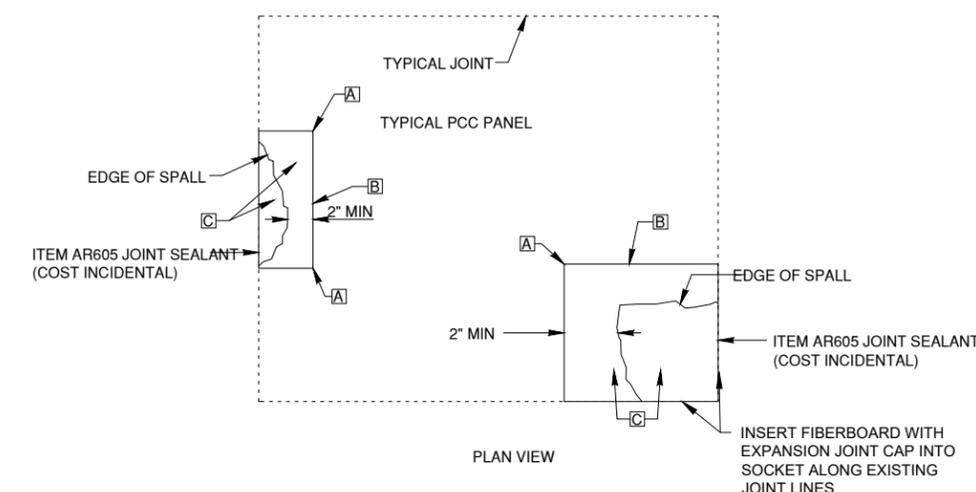
**3 FULL PANEL DEMOLITION W/ BONDED PCC**  
N.T.S.



**4 BONDED OVERLAY - REMOVAL DETAIL (SECTION B-B)**  
N.T.S.



**6 PCC PANEL - PLACEMENT DETAIL**  
N.T.S.



**7 PCC SPALL REPAIR**  
N.T.S.

**SPALL REPAIR NOTES**

- A** A. CONTRACTOR SHALL NOT OVERSAW. WORK SHALL BE ACCORDING TO STEPS STATED IN 501-3.24.
  - B** B. CONTRACTOR SHALL SAWCUT 3" DEPTH EXCEPT AS NECESSARY TO NOT OVERSAW.
  - C** C. CONTRACTOR SHALL REMOVE EXISTING PCC DOWN TO UNDISTURBED MATERIAL. VOID SHALL BE FILLED WITH ELASTOMERIC CONCRETE AS SPECIFIED IN ITEM 501 FOR SPALL REPAIR.
1. SAWCUTS MAY BE MADE A MAXIMUM OF 24 HOURS PRIOR TO PAVEMENT REMOVAL. HOWEVER, ALL SAWCUT CLURRY AND DUST MUST BE REMOVED BY THE END OF THE WORK SHIFT.
  2. MINIMUM DISTANCE BETWEEN ADJACENT EDGE SPALL PATCHES IS 2 FEET. IF LESS SPACE IS REQUIRED, PATCHES SHOULD BE COMBINED AND CONTINUOUS. IF DISTANCE BETWEEN SPALL REPAIR AND SLAB CORNER IS LESS THAN 18 INCHES, EXTEND PATCH TO SLAB CORNER
  3. SEE JOINT SEALING DETAILS ON SHEET CP503.
  4. SPALL WIDTH SHALL NOT EXCEED 24"
  5. SHOULD THE CONTRACTOR'S OPERATION DAMAGE ADJACENT PAVEMENT, THE CONTRACTOR SHALL REPAIR AS DIRECTED BY THE RESIDENT ENGINEER AT THE CONTRACTOR'S EXPENSE.
  6. WORK SHOWN SHALL BE INCIDENTAL TO PAY ITEM 501922- PCC SPALL REPAIR

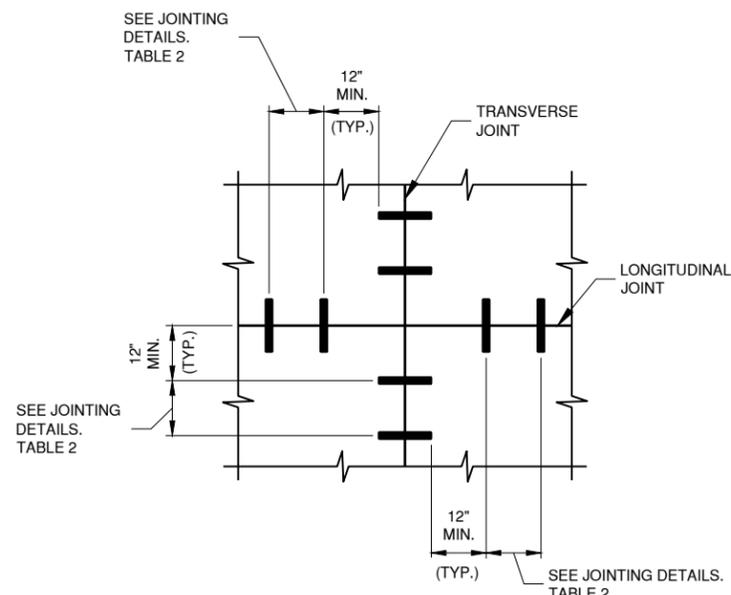
**REMOVE AND REPLACE PCC PANEL NOTES**

1. DEMOLITION SHALL FOLLOW DETAILS 1-4. PCC REPLACEMENT SHALL FOLLOW DETAIL 5-6.
2. TO PROTECT UNDERLYING ELECTRICAL CONDUITS FROM DAMAGE, GUILLOTINE OR HOE RAM SHALL NOT BE USED TO BREAK PAVEMENT IN THE REMOVAL AREA.
3. FOR MULTIPLE PANEL REPLACEMENTS REQUIRING A TYPE B OR C CONTRACTION JOINT, SAW AND SEAL PER DETAIL 6-2 SHEET CP503.
4. SAW CUT OVERCUTS SHALL BE THOROUGHLY CLEANED AND COMPLETELY SEALED.
5. WHERE A LONGITUDINAL CRACK EXISTS IN AN ADJACENT PANEL, PLACE 2 LAYERS OF TWO #4 REBAR, 3' LONG AND CENTERED ON THE CRACK, AS SHOWN IN DETAIL 3.
6. PANEL REMOVAL AND REPLACEMENT SHALL BE AS SHOWN ON SHEET CP101-CP103 AND AS DIRECTED BY THE RESIDENT ENGINEER.
7. CONTRACTOR SHALL REPLACE BASE COURSE AGGREGATE LOSS DUE TO PAVEMENT REMOVALS W/ P-209 AGGREGATE & COMPACT TO SATISFACTION OF THE RPR.

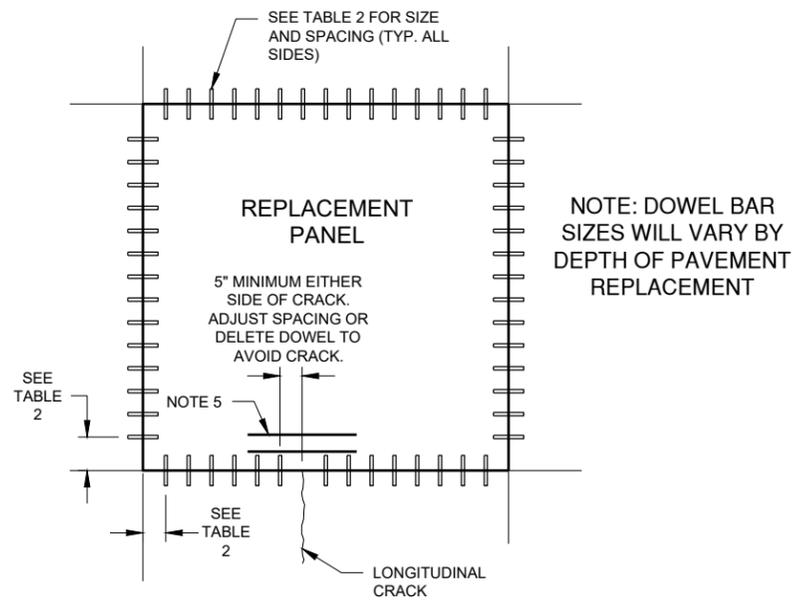
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CMT PROJECT NO: 21001968	
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DRAWN BY: MRK	
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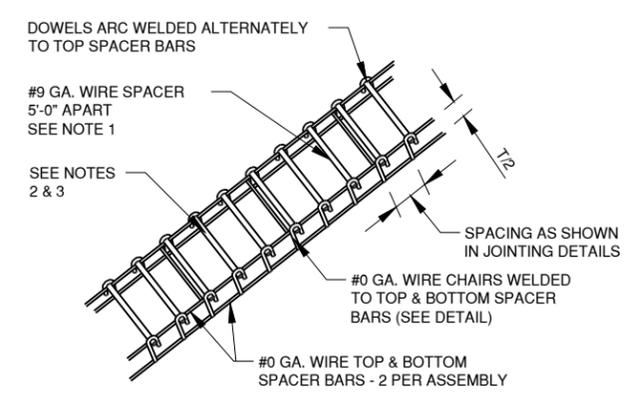
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**PAVING DETAILS 2**



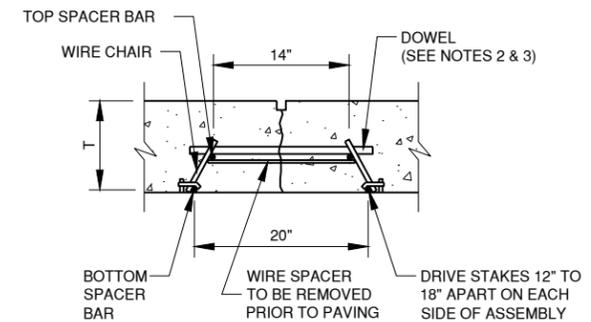
**1 DOWEL PLACEMENT**  
N.T.S.



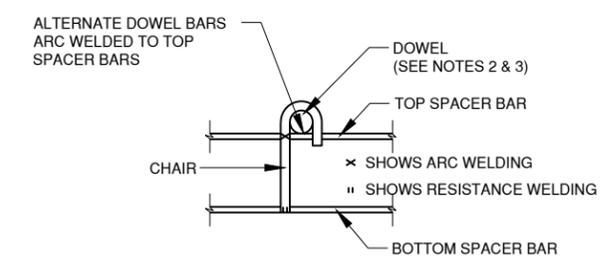
**2 DOWEL BAR LAYOUT**  
N.T.S.



**3 DOWEL BASKET ASSEMBLY**  
N.T.S.



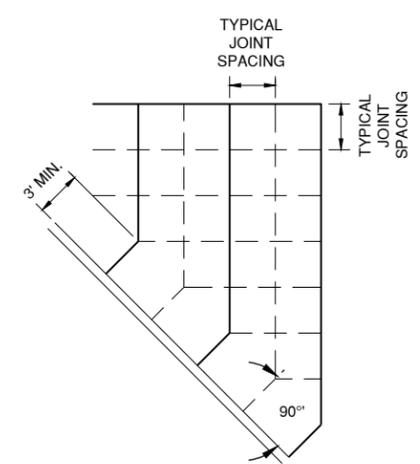
**4 DOWEL BAR INSTALLATION**  
N.T.S.



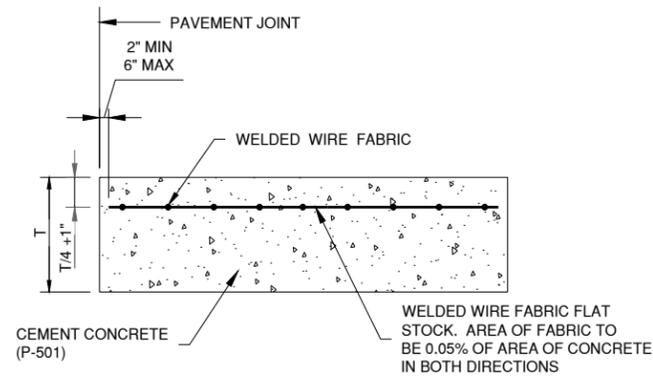
**5 TYPICAL DOWEL BASKET ELEVATION SHOWING CHAIR**  
N.T.S.

**DOWEL BASKET NOTES**

- #9 GA. WIRE SPACER BAR ARC WELDED TO THE BOTTOM OF TOP SPACER BAR. (MAY BE MECHANICALLY ATTACHED IN LIEU OF WELDING) 3 REQUIRED PER UNIT. THIS WIRE MUST BE CUT OR REMOVED PRIOR TO PAVING.
- DOWEL BAR DIAMETER, LENGTH & SPACING SHALL BE AS SHOWN IN TABLE 2.
- DOWELS SHALL BE EPOXY COATED FULL LENGTH OF DOWEL. IMMEDIATELY PRIOR TO PAVING, THE FREE END OF EACH DOWEL SHALL BE LUBRICATED OR OILED, FOR HALF THE LENGTH OF THE DOWEL.



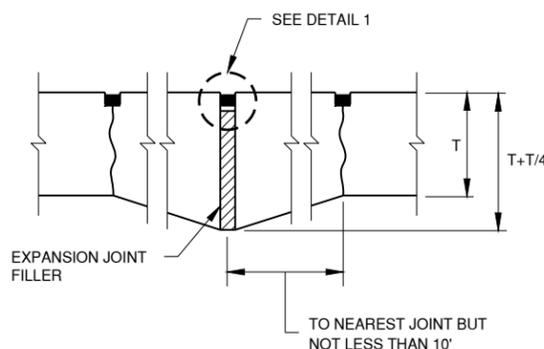
**6 JOINTING AT SKEWED EDGE**  
N.T.S.



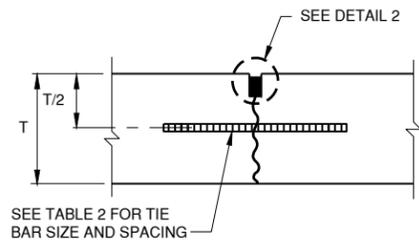
**7 ODD SHAPE PANEL REINFORCEMENT**  
N.T.S.  
SYMBOL R

ALL ODD SHAPED PANELS AND PANELS HAVING A L/W RATIO GREATER THAN 1.25

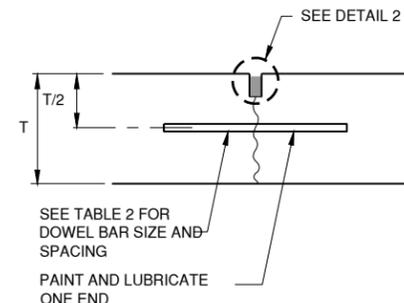
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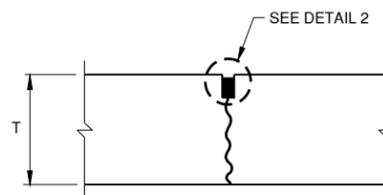
**1 TYPE A THICKENED ISOLATION**  
N.T.S. SYMBOL — A —



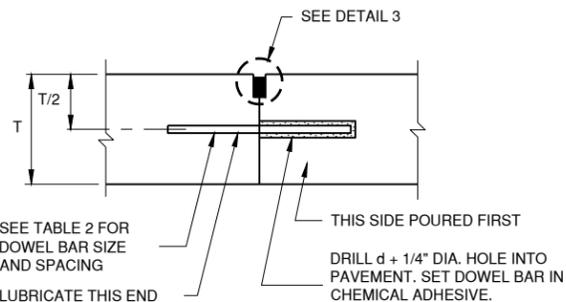
**2 TYPE B HINGED (TIED) CONTRACTION**  
N.T.S. SYMBOL — B —



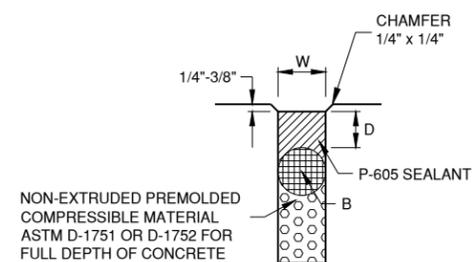
**3 TYPE C DOWELED CONTRACTION**  
N.T.S. SYMBOL — C —



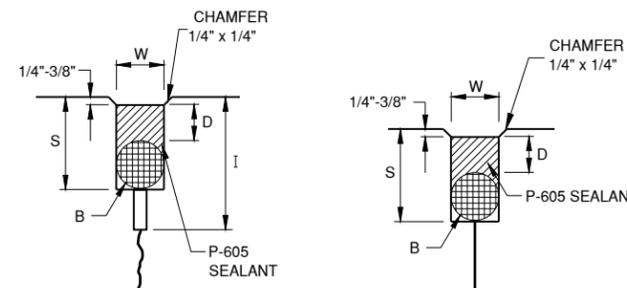
**4 TYPE D DUMMY CONTRACTION**  
N.T.S. SYMBOL — D —



**5 JOINT SEALING DETAILS**  
N.T.S. SYMBOL — E —



**DETAIL 1**



**DETAIL 2**

**DETAIL 3**

**6 JOINT SEALING DETAILS**  
N.T.S.

100%  
MARCH 07, 2025

REHABILITATE GENERAL  
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PAVEMENT THICKNESS T - INCHES	DEPTH OF CONTRACTION JOINT INITIAL SAW CUT I, INCHES I=(T/3) ±1/4"
5	1.67"
6	2.00"
7	2.33"
8	2.67"
9	3.00"
10	3.33"
11	3.67"
12	4.00"
13	4.33"
14	4.67"
15	5.00"
16	5.33"
17	5.67"
18	6.00"

PAVEMENT THICKNESS T - INCHES	DOWEL BAR DETAILS			TIE BAR DETAILS		
	DIA.	LENGTH	SPACING	BAR SIZE	LENGTH	SPACING
5	5/8"	12"	12"	#4	24"	30"
6	3/4"	18"	12"	#5	30"	30"
7	3/4"	18"	12"	#5	30"	30"
8	1"	18"	12"	#5	30"	30"
9	1"	18"	12"	#5	30"	30"
10	1"	18"	12"	#5	30"	30"
11	1"	18"	12"	#5	30"	30"
12	1"	18"	12"	#5	30"	30"
13	1 - 1/4"	20"	15"	#5	30"	30"
14	1 - 1/4"	20"	15"	#5	30"	30"
15	1 - 1/4"	20"	15"	#5	30"	30"
16	1 - 1/4"	20"	15"	#5	30"	30"
17	1 - 1/2"	20"	18"	#5	30"	30"
18	1 - 1/2"	20"	18"	#5	30"	30"

	DETAIL 1	DETAIL 2	DETAIL 3
W=WIDTH OF SEALANT RESERVOIR (IN.)	3/4 +/- 1/8	1/2	1/2
D=DEPTH OF SEALANT RESERVOIR (IN.)	3/4 +/- 1/8	1/4	1/4
B=BACKER ROD DIAMETER (IN.)	7/8	5/8	5/8
S=SECOND SAWCUT DEPTH (IN.) MINIMUM	N/A	1-3/8	1-3/8

**JOINT NOTES**

- ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A CHAMFER OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A CHAMFER > 1/4" WILL NOT BE ACCEPTABLE.
- THE INITIAL SAWCUT FOR ALL LONGITUDINAL & TRANSVERSE CONTRACTION JOINTS SHALL BE SAWED AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT.
- ALL TIE BARS & MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER APPROVED METHODS TO PREVENT SHIFTING DURING & AFTER CONCRETE PLACEMENT.
- TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH THE SPECIFICATIONS.
- THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSIONS OF THE SECOND SAWCUT WILL NOT BE ALLOWED.

MARK | DATE | DESCRIPTION

AIP PROJ. NO:  
IL PROJ. NO: CMI-5043  
CMT PROJECT NO: 21001968  
CAD DWG FILE: 21001968-CP500.DWG  
DESIGNED BY: MRK  
DRAWN BY: MRK  
CHECKED BY: MJD  
APPROVED BY: CBG  
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**JOINTING DETAILS**









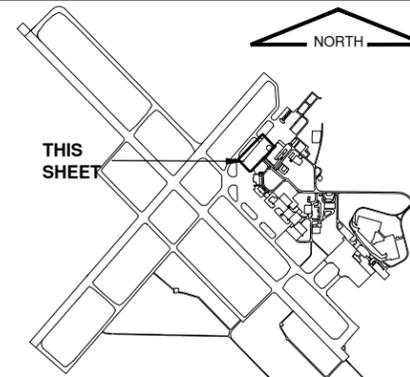


TXY C

TXY C4

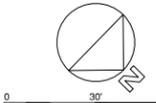
TXY C3

KEYMAP



License No. 184-000613

CONSULTANTS



THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

LEGEND

- NEW MARKING
- EXISTING MARKING
- NEW TIE DOWN LAYOUT MARKING
- NEW TIE DOWN - TYPE A - AA3

100%  
MARCH 07, 2025

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NOTES

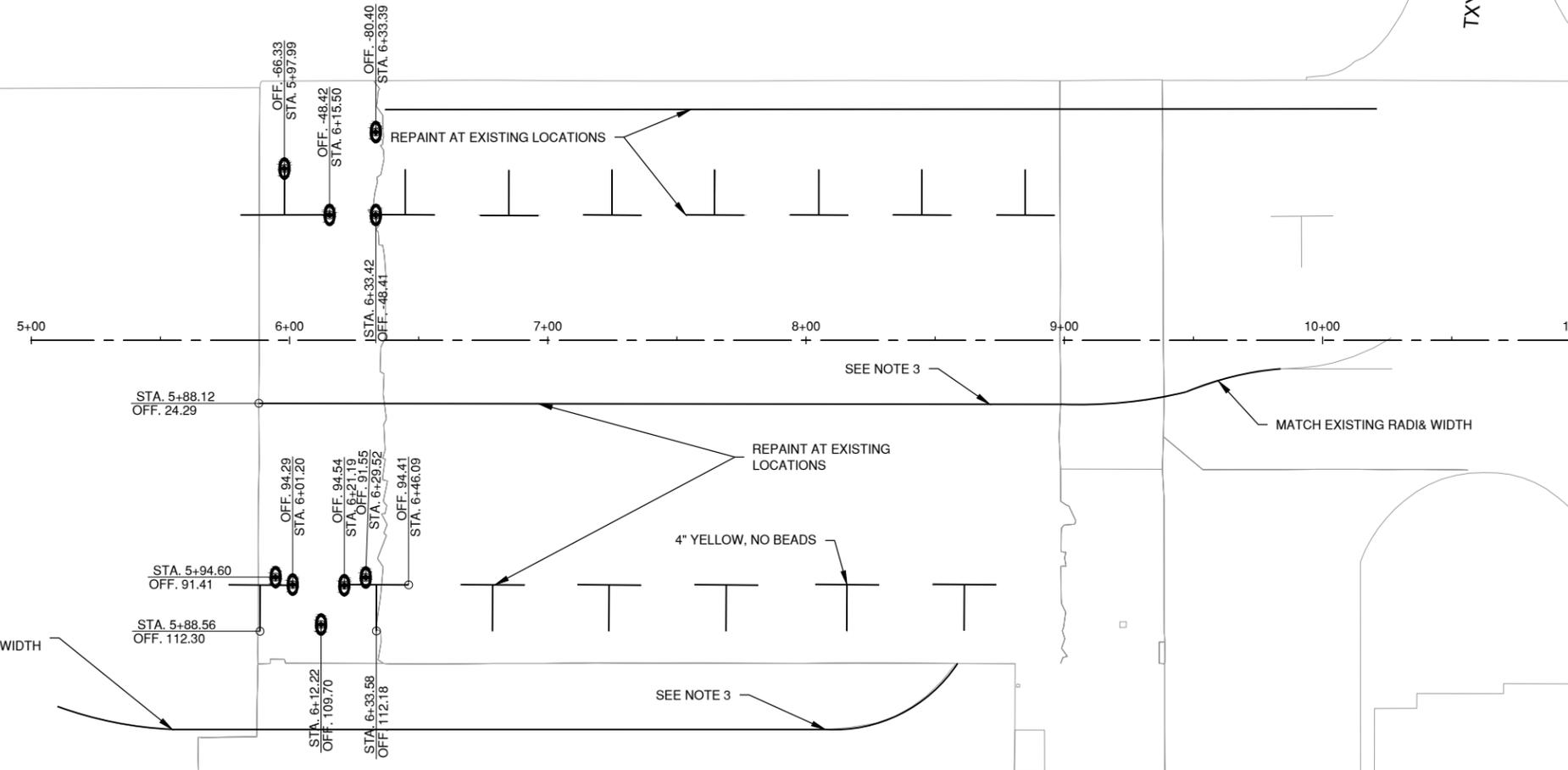
1. MATCH EXISTING TAXIWAY CENTERLINES.
2. EXISTING MARKINGS TO BE REPAINTED SHALL BE PREPARED ACCORDING TO SPECIFICATIONS PRIOR TO REMARKING.
3. TAXIWAY CENTERLINES SHALL BE SOLID YELLOW, 6" IN WIDTH AND WILL INCLUDE A 3" BLACK BORDER.

MARK | DATE | DESCRIPTION

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IL PROJ. NO: CMI-5043	
CMT PROJECT NO: 21001968	
CAD DWG FILE: 21001968-CM100.DWG	
DESIGNED BY: MRK	
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SHEET TITLE  
MARKING AND TIE DOWN PLAN 1

CM101  
SHEET 24 OF 26



REPAINT EXISTING CL  
MATCH EXISTING RADI & WIDTH

REPAINT AT EXISTING LOCATIONS

4" YELLOW, NO BEADS

MATCH EXISTING RADI & WIDTH

SEE NOTE 3

SEE NOTE 3

INSTITUTE OF AVIATION HANGAR

INSTITUTE OF AVIATION

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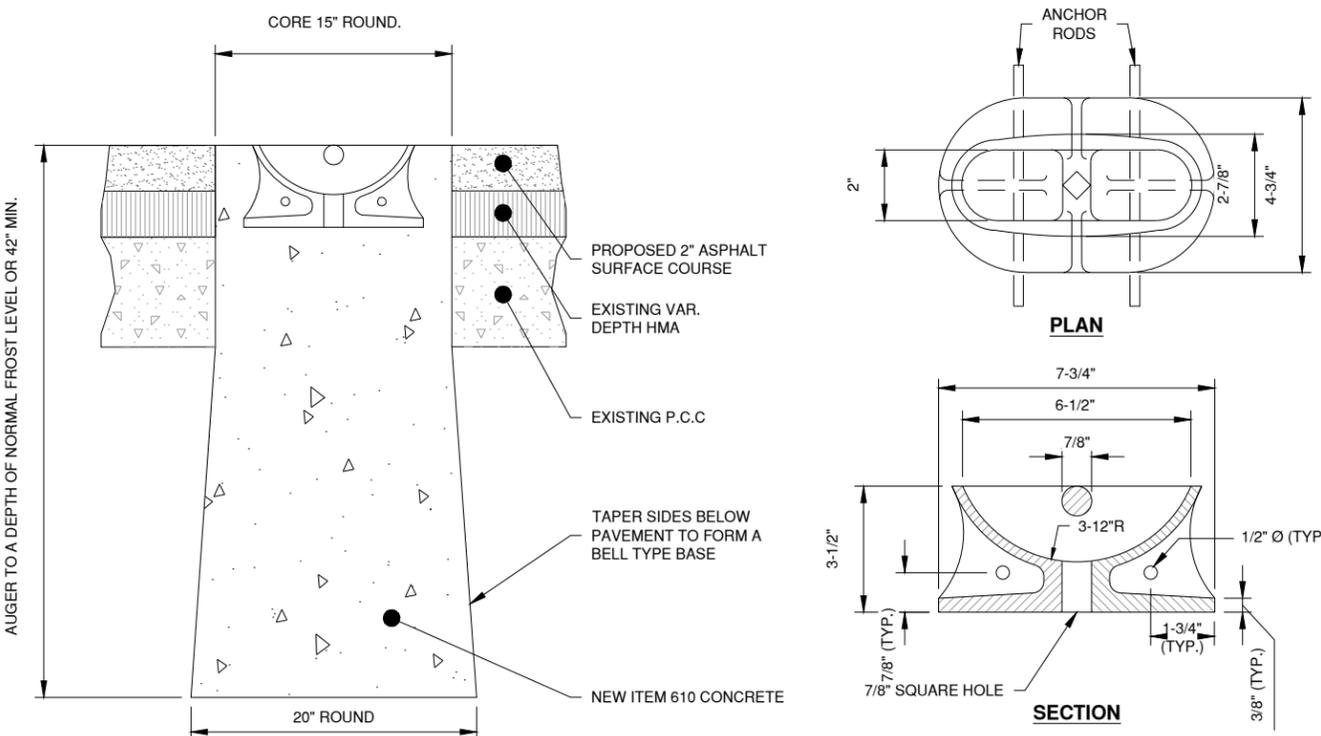


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WILLARD AIRPORT  
SAVOY, IL

MARK	DATE	DESCRIPTION

AIP PROJ. NO:	
IL PROJ. NO: CMI-5043	
CMT PROJECT NO: 21001968	
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DESIGNED BY: MRK	
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CHECKED BY: MJD	
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SHEET TITLE  
**MARKING AND TIE  
DOWN DETAILS**



**1 TIE DOWN DETAIL (TYPE A)**  
N.T.S.

**2 TAXIWAY CENTERLINE DETAIL**  
N.T.S.

**3 TIE DOWN LAYOUT DETAIL**  
N.T.S.

**NOTES:**

1. MOORING CASTINGS SHALL BE NEENAH SEMI-STEEL AIRPORT MOORING EYE CATALOG NO. R3490 OR APPROVED EQUIVALENT.
2. ANCHOR RODS TO BE NO. 3 DEFORMED REINFORCING STEEL 15" LONG AND SHALL BE BENT DOWNWARD AT 45°.
3. TYPE A MOORING EYES SHALL BE INSTALLED AFTER PAVING OPERATIONS ARE COMPLETE BY CORING THROUGH NEW PAVEMENT.

**NOTE:**

1. ALL DIMENSIONS AND SPACINGS ARE TO BE FIELD VARIED PRIOR TO CONSTRUCTION. PROPOSED DIMENSIONS AND SPACINGS SHALL MATCH EXISTING CONDITIONS.

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