03-07-2025 LETTING ITEM 022

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ2	DUPAGE	44	1
	ILLINOIS	CONTRACT NO. 62T31		

* 44 + 1 = 45 TOTAL SHEETS

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

PROPOSED
HIGHWAY PLANS

FAP ROUTE 870: IL ROUTE 53
OVER GINKO WAY (MORTON ARBORETUM) AND
OVER EAST BRANCH DUPAGE RIVER
SECTION: FAP 0870 22 BJ2
PROJECT: NHPP–V2QK(631)
BRIDGE JOINT REPAIR, BRIDGE DECK OVERLAY
DUPAGE COUNTY

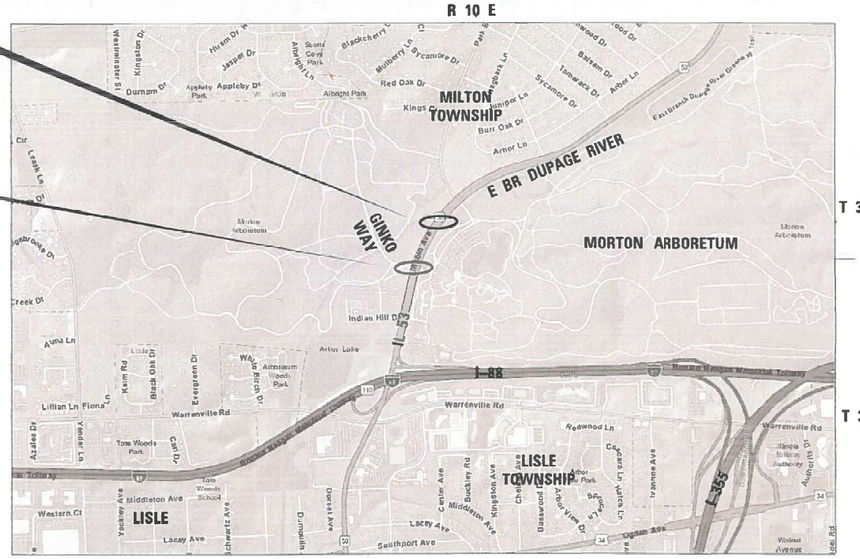
C–91–336–22

D–91–282–22

LOCATION OF SECTION INDICATED THUS: –

PROJECT LOCATION
IL–53 OVER
EAST BRANCH DUPAGE RIVER
NB STRUCTURE NO. 022–0078
SB STRUCTURE NO. 022–0079

PROJECT LOCATION
IL–53 OVER
GINKO WAY (MORTON ARBORETUM)
NB STRUCTURE NO. 022–0080
SB STRUCTURE NO. 022–0081



FOR INDEX OF SHEETS AND HIGHWAY STANDARDS, SEE SHEET NO. 2

THIS PROJECT IS LOCATED IN UNINCORPORATED DUPAGE COUNTY

TRAFFIC DATA

2023 ADT = 22,700 VPD
POSTED SPEED LIMIT = 45 MPH
PRINCIPAL ARTERIAL

0 100' 200' 300' — 1" = 100'
0 10' 20' 30' — 1" = 10'
0 50' 100' — 1" = 50'
0 50' 100' — 1" = 40'
0 50' 100' — 1" = 30'
0 50' 100' — 1" = 20'

FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E. JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION 1–800–892–0123 OR 811

MEADE ELECTRIC CO. DISTRICT ONE ELECTRICAL MAINTENANCE CONTRACTOR LOCATES IDOT ELECTRICAL EQUIPMENT AND UNDERGROUND CABLES 773–287–7672

PROJECT ENGINEER: LUKASZ POCIECHA (847) 705–4255
PROJECT MANAGER: VESELIN VELICHKOV

CONTRACT NO. 62T31

GROSS LENGTH = 3,713 FT. = 0.703 MILES
NET LENGTH = 341 FT. = 0.065 MILES

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CONTACT: ALEXANDER LANE (312) 477–0620

LICENSED PROFESSIONAL ENGINEER
ALEXANDER CARL LANE
062-063261
OF ILLINOIS

ALEXANDER CARL LANE, P.E.
IL LIC. NO. 062–063261
EXP: 11/30/2025
DATE: 12/05/2024

THIS SEAL AND SIGNATURE PERTAINS TO SHEETS 1 TO 13

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED Dec 12 20 24
REGION 1 ENGINEER
January 31 20 25
ENGINEER OF DESIGN AND ENVIRONMENT
January 31 20 25
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

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INDEX OF SHEETS

STATE STANDARDS

STANDARD NO.	DRAWING NAME
000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
630001-13	STEEL PLATE BEAM GUARDRAIL
701101-05	OFF-RD MOVING OPERATION, 2L, 2W, DAY ONLY
701106-02	OFF-RD OPERATIONS, MULTILANE, MORE THAN 15' (4.5 m) AWAY
701423-10	LANE CLOSURE, MULTILANE, WITH BARRIER, FOR SPEEDS ≥ 45 MPH TO 55 MPH
701426-09	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS ≥ 45 MPH
701428-01	TRAFFIC CONTROL SETUP AND REMOVAL FREEWAY/EXPRESSWAY
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701601-09	URBAN LANE CLOSURE MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701701-10	URBAN LANE CLOSURE MULTILANE INTERSECTION
701901-10	TRAFFIC CONTROL DEVICES
704001-08	TEMPORARY CONCRETE BARRIER
001006	DECIMEL OF AN INCH AND OF A FOOT
001002-02	AREAS OF REINFORCEMENT BARS
420001-10	PAVEMENT JOINTS
542301-03	PRECAST REINFORCEMENT CONCRETE FLARED END SECTION
631031-18	TRAFFIC BARRIER TERMINAL, TYPE 6
780001-04	TYPICAL PAVEMENT MARKINGS
781001-04	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
782006-01	GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS

HMA TABLE

HOT-MIX ASPHALT MIXTURE REQUIRMENTS		QUALITY MANAGEMENT PROGRAM (QMP)
MIXTURE TYPE	AIR VOIDS @ Ndes	
HMA OVERLAY ON APPROACH		
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", IL-9.5, N70, 2"	4% @ 70 Gyr	QC/QA
BUTT JOINT & SHOULDERS		
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", IL-9.5, N70, 1-3/4"	4% @ 70 Gyr	QC/QA
QMP DESIGNATION: QUALITY CONTROL/ QUALITY ASSURANCE (QC/QA): QUALITY CONTROL FOR PERFORMANCE (QCP): PAY FOR PERFORMANCE (PFP)		

MIXTURE REQUIREMENT NOTES:

1. THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY RECLAIMED MATERIALS SPECIFICATIONS.

GENERAL NOTES

1. THE EXACT LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE ORDERING ANY MATERIALS AND STARTING ANY WORK. FOR LOCATIONS OF UTILITIES, LOCALLY OWNED EQUIPMENT, LEASED ENFORCEMENT CAMERA SYSTEM FACILITIES AND IDOT UNDERGROUND FACILITIES, CONTACT THE LOCAL COUNTIES, MUNICIPALITIES AND IDOT FOR LOCATES.
2. MEADE ELECTRIC COMPANY, THE IDOT DISTRICT ONE ELECTRICAL MAINTENANCE CONTRACTOR, LOCATES IDOT ELECTRICAL EQUIPMENT AND UNDERGROUND CABLES. CALL 773-287-7672 FOR THE INITIAL LOCATE. REQUEST FOR LOCATES OF PREVIOUSLY MARKED FACILITIES MAY BE AT THE CONTRACTOR'S EXPENSE.
3. IN ADDITION TO FIELD REVIEW AND AERIAL DATA, PLAN DIMENSIONS AND DETAILS RELATIVE TO THE EXISTING FACILITIES HAVE BEEN TAKEN FROM EXISTING PLANS AND ARE SUBJECT TO CONSTRUCTION VARIATIONS. PLAN DIMENSIONS AND DETAILS RELATIVE TO EXISTING PLANS ARE SUBJECT TO ROUTINE VARIATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY EXISTING DIMENSIONS AND DETAILS AFFECTING NEW CONSTRUCTION AND MAKE NECESSARY APPROVED ADJUSTMENTS PRIOR TO CONSTRUCTION OR ORDERING MATERIALS. SUCH VARIATIONS SHALL NOT BE CAUSE FOR ADDITIONAL COMPENSATION FOR A CHANGE IN THE SCOPE OF WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE BID PRICE FOR THE WORK.
4. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
5. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO BEGINNING CONSTRUCTION AND ORDERING MATERIALS.
6. THE CONTRACTOR SHALL TAKE ALL NECESSARY PRECAUTIONS FOR THE PROTECTION OF EXISTING PLANT MATERIAL FOR WHICH THE CONTRACT DOES NOT PROVIDE REMOVAL. THE PROTECTION OF EXISTING PLANT MATERIAL AND THE REPAIR OR REPLACEMENT OF EXISTING PLANT MATERIAL DAMAGED BY THE CONTRACTOR SHALL BE DONE IN ACCORDANCE WITH THE REQUIREMENTS OF SECTION 201 OF THE STANDARD SPECIFICATIONS.
7. THE DEPARTMENT HAS NOT OBTAINED ANY PERMITS FOR OFFSITE BORROW, WASTE, USE (BWU) AREAS. PRIOR TO WORKING IN BWU AREAS, IF THE CONTRACTOR CHOOSES TO USE ACTIVITIES REQUIRING PERMITS IT IS THE CONTRACTOR'S RESPONSIBILITY TO SECURE THE PROPER PERMITS. IN ADDITION TO THE BORROW REVIEW (BDE 2289) AND USE/WASTE REVIEW (BDE 2290) SUBMITTALS, THE CONTRACTOR SHALL SUBMIT AN EROSION AND SEDIMENT CONTROL (ESC) PLAN FOR EVERY BWU SITE TO THE DEPARTMENT FOR ACCEPTANCE. GUIDELINES FOR ACCEPTABLE BWU PRACTICES CAN BE FOUND IN SECTION 11.G.1. THE COST OF ALL MATERIALS AND LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS TO PREPARE AND IMPLEMENT ESC PLANS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE CONTRACT UNIT PRICES AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
8. THE RESIDENT ENGINEER SHALL CONTACT PATRICE HARRIS, AREA TRAFFIC FIELD TECHNICIAN, AT PATRICE.HARRIS@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
9. THE DEPARTMENT HAS DETERMINED THAT IN STREAM WORK IS NOT REQUIRED FOR THE WORK SPECIFIED IN THIS CONTRACT. THE DEPARTMENT HAS NOT OBTAINED A USACE PERMIT. IF THE CONTRACTOR CHOOSES TO USE ACTIVITIES REQUIRING AN USACE PERMIT, IT IS THE CONTRACTOR'S RESPONSIBILITY TO SECURE THE PROPER USACE PERMITS. THE COST OF ALL MATERIALS AND LABOR NECESSARY TO SECURE AND COMPLY WITH A USACE PERMIT FOR CONTRACTOR'S ACTIVITIES WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
10. THESE PLANS HAVE BEEN PREPARED FROM NOTES RECEIVED FROM I.D.O.T. FIELD MAINTENANCE ENGINEERS.
11. THE CONTRACTOR SHALL USE CARE IN REMOVING OR EXCAVATING NEAR ALL EXISTING ITEMS WHICH WILL REMAIN. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
12. SAW CUTTING PRIOR TO ANY REMOVAL ITEMS NOTED ON THE PLANS OR DIRECTED BY THE ENGINEER SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ITEMS BEING REMOVED.
13. FOR WORK OUTSIDE THE LIMITS OF BRIDGE APPROACH PAVEMENT. ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS AND TIE BARS IN PAVEMENT SHALL BE EPOXY COATED UNLESS NOTED ON THE PLANS.
14. DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS-RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTENT)" SHOWN IN PLANS.
15. THE CENTERLINE IS FOR INFORMATION ONLY.
16. THE CONTRACTOR SHALL NOTIFY VINCE GORTNER, SENIOR DIRECTOR OF GUEST EXPERIENCE & SAFETY, THE MORTON ARBORETUM, AT 630-725-2099 OR VGORTNER@MORTONARB.ORG A MINIMUM OF TWO (2) WEEKS PRIOR TO BEGINNING CONSTRUCTION.
17. THE PROJECT HAS A PERMIT FOR SECTION 404 OF THE CLEAN WATER ACT AND SHALL FOLLOW THE CONDITIONS OF THE PERMIT PROGRAM.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES
IL-53 OVER MORTON ARBORETUM & EAST BRANCH DUPAGE RIVER

SCALE: | SHEET OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ2	DUPAGE	44	2
		CONTRACT NO. 62T31		
	ILLINOIS | FED. AID PROJECT			

REV-SEP

			URBAN	CONSTRUCTION CODE				
				0059 SN 022-0078 SN 022-0079		0059 SN 022-0080 SN 022-0081		
PAY ITEM NUMBER	DESIGNATION	UNIT	TOTAL QUANTITY	80% FEDERAL 20% STATE	100% STATE	80% FEDERAL 20% STATE	100% STATE	
20200100	EARTH EXCAVATION	CU YD	106	106				
28100107	STONE RIPRAP, CLASS A4	SQ YD	578	578				
28200200	FILTER FABRIC	SQ YD	580	580				
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	115	92		23		
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	580	580				
40604172	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "E", N70	TON	26	20		6		
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	469			469		
50102400	CONCRETE REMOVAL	CU YD	26.4	26.4				
50104650	SLOPE WALL REMOVAL	SQ YD	578	578				
50157300	PROTECTIVE SHIELD	SQ YD	407			407		
50201101	COFFERDAM (TYPE 1) (LOCATION - 1)	EACH	1	1				
50300255	CONCRETE SUPERSTRUCTURE	CU YD	26.3	26.3				
50300300	PROTECTIVE COAT	SQ YD	1,888	1,888				
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	1,888	1,439		449		
50800515	BAR SPLICERS	EACH	48	48				
52000110	PREFORMED JOINT STRIP SEAL	FOOT	184	184				
54213663	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 18"	EACH	1	1				
63302700	REMOVE AND REERECT TRAFFIC BARRIER TERMINALS, TYPE 6	EACH	1	1				
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	110	110				
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	2	2				
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	L SUM	1	1				
* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	L SUM	1	1				
* 66901006	REGULATED SUBSTANCES MONITORING	CAL DA	3	3				
67100100	MOBILIZATION	L SUM	1	1				

* = SPECIALTY ITEM

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
IL-53 OVER MORTON ARBORETUM & EAST BRANCH DUPAGE RIVER

SCALE: | SHEET OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ2	DUPAGE	44	3
		CONTRACT NO. 62T31		
		ILLINOIS FED. AID PROJECT		

REV-SEP

			URBAN	CONSTRUCTION CODE				
				0059 SN 022-0078 SN 022-0079		0059 SN 022-0080 SN 022-0081		
PAY ITEM NUMBER	DESIGNATION	UNIT	TOTAL QUANTITY	80% FEDERAL 20% STATE	100% STATE	80% FEDERAL 20% STATE	100% STATE	
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	268	134		134		
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	2,936	1,468		1,468		
70307120	TEMPORARY PAVEMENT MARKING - LINE 4" - TYPE IV TAPE	FOOT	12,006	6,003		6,003		
70307130	TEMPORARY PAVEMENT MARKING - LINE 6" - TYPE IV TAPE	FOOT	2,296	1,148		1,148		
70307160	TEMPORARY PAVEMENT MARKING - LINE 12"- TYPE IV TAPE	FOOT	184	92		92		
70307210	TEMPORARY PAVEMENT MARKING - LINE 24"- TYPE IV TAPE	FOOT	142	71		71		
70400100	TEMPORARY CONCRETE BARRIER	FOOT	1,050	525		525		
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	950	475		475		
70600255	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 2	EACH	4	2		2		
70600322	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE, NARROW), TEST LEVEL 2	EACH	4	2		2		
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	255	128		128		
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	10,228	5,114		5,114		
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	2,224	1,112		1,112		
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	184	92		92		
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	142	71		71		
* 78004635	PREFORMED PLASTIC PAVEMENT MARKING, TYPE D - LINE 7"	FOOT	130	65		65		
* 78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	1,035	788		247		
* 78009006	MODIFIED URETHANE PAVEMENT MARKING - LINE 6"	FOOT	73	22		51		
* 78011040	GROOVING FOR RECESSED PAVEMENT MARKING 8"	FOOT	129	98		31		
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	8	4		4		
* 78200011	BARRIER WALL REFLECTORS, TYPE C	EACH	38	19		19		

* = SPECIALTY ITEM

MODEL: Default
FILE NAME: p:\p-20\20-4094-00 IDOT Various Phase 2 (IFB 196 Item 16)\WO 36 62T31 (Closed)\DGN\CADD Sheets\62T31-sht-SOQ-02.dgn

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PLOT DATE = 12/12/2024	DATE - 07/28/2023	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
IL-53 OVER MORTON ARBORETUM & EAST BRANCH DUPAGE RIVER

SCALE: | SHEET OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ2	DUPAGE	44	4
		CONTRACT NO. 62T31		
	ILLINOIS	FED. AID PROJECT		

REV-SEP

			URBAN	CONSTRUCTION CODE				
				0059 SN 022-0078 SN 022-0079		0059 SN 022-0080 SN 022-0081		
PAY ITEM NUMBER	DESIGNATION	UNIT	TOTAL QUANTITY	80% FEDERAL 20% STATE	100% STATE	80% FEDERAL 20% STATE	100% STATE	
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	8	4		4		
78300202	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	5,584	2,792		2,792		
X4400503	COMBINATION CURB AND GUTTER REMOVAL AND REPLACEMENT GREATER THAN 10 FEET	FOOT	274	137		137		
X6700407	ENGINEER'S FIELD OFFICE, TYPE A (D1)	CAL MO	12	6		6		
X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	0.5		0.5		
X7830050	RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REMOVAL	EACH	154	77		77		
Z0001700	APPROACH SLAB REPAIR (FULL DEPTH)	SQ YD	16	16				
Z0006016	BRIDGE DECK LATEX CONCRETE OVERLAY, 2 3/4 INCHES	SQ YD	1,871	1,430		441		
Z0012130	BRIDGE DECK SCARIFICATION 3/4"	SQ YD	1,435	1,435				
Z0012142	BRIDGE DECK SCARIFICATION 2 1/4"	SQ YD	449			449		
Z0012754	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	127	92		35		
Z0018051	DRAINAGE SCUPPERS TO BE ADJUSTED	EACH	6	6				
Z0029090	DIAMOND GRINDING (BRIDGE SECTION)	SQ YD	1,871	1,430		441		
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	72	36		36		
Z0041895	POLYMER CONCRETE	CU FT	25	10.0		15.0		
* X7830052	RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REPLACEMENT	EACH	154	77		77		
Z0065700	SLOPE WALL REPAIR	SQ YD	2	2				
Z0073510	TEMPORARY TRAFFIC SIGNAL TIMING	EACH	1	1				

* = SPECIALTY ITEM

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	DRAWN - ACL	REVISED -		
PLOT SCALE = 20.0000 ' / in.	CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.
PLOT DATE = 12/12/2024	DATE - 07/28/2023	REVISED -		

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ2	DUPAGE	44	5
		CONTRACT NO. 62T31		
	ILLINOIS FED. AID PROJECT			

MAINTENANCE OF TRAFFIC GENERAL NOTES

1. THE CONTRACTOR SHALL CONTACT THE IDOT DISTRICT 1 ARTERIAL TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

2. CONTRACTOR SHALL REMOVE OR COVER ALL W21-1 (WORKERS PRESENT) AND W20-7 (FLAGGER) SIGNS, WITHIN AN HOUR, WHEN WORKERS ARE NOT PRESENT OR IF THE SIGN IS NOT APPLICABLE.

3. TYPE C REFLECTORS SHALL BE CONSIDERED INCLUDED IN THE COST OF TEMPORARY CONCRETE BARRIER

4. WORK OVER GINKGO WAY SHALL TAKE PLACE AT NIGHT. LOOSE DEBRIS SHALL BE CLEANED AND CLEARED BEFORE TRAFFIC RESUMES ON GINKGO WAY.

5. PAVEMENT MARKING TAPE, TYPE IV (AND/OR TYPE III) SHOWN ON THE PLANS FOR ANY CONSTRUCTION STAGE THAT THE CONTRACTOR PROPOSES TO EXTEND OVER THE WINTER PERIOD SHALL MEAN MODIFIED URETHANE PAVEMENT MARKING AND WILL BE PAID FOR THEIR RESPECTIVE CONTRACT UNIT PRICE.

6. THE "ROAD CONSTRUCTION AHEAD" SIGNS SHALL REMAIN INSTALLED UNTIL THE COMPLETION OF THE PROJECT OR WHEN NO ROADWAY HAZARDS REMAIN WITHIN THE WORK ZONE.

7. CHANGEABLE MESSAGE SIGNS SHALL BE INSTALLED TWO WEEKS PRIOR TO ALL TRAFFIC STAGE CHANGES ON EACH APPROACH OF THE EFFECTED ROADWAY TO WARN MOTORISTS OF THE UPCOMING EVENT. THE SIGN MESSAGES SHALL BE REVISED TWO WEEKS THEREAFTER WITH MESSAGES WARNING TRAFFIC OF POTENTIAL TRAFFIC DELAYS, QUEUING AND/OR WITH MESSAGES NOTIFYING TRAFFIC TO USE ALTERNATE ROUTES. THE SIGN LOCATIONS AND MESSAGES SHALL BE DETERMINED BY THE ENGINEER.

8. ALL EXISTING LANE LINE PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKER REFLECTORS LOCATED WITHIN TEMPORARY LANE CLOSURE TAPERS, LANE SHIFT TAPERS OR IN LOCATIONS THAT CONFLICT WITH THE TEMPORARY PAVEMENT MARKING TAPE USED FOR STAGING SHALL BE REMOVED VIA WATER BLASTING WITH VACUUM RECOVERY IF THE STAGING WILL REMAIN IN PLACE FOR MORE THAN 14 DAYS. THE EXISTING PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKER REFLECTORS THAT WERE REMOVED SHALL BE RESTORED IN KIND AFTER THE COMPLETION OF THE STAGING.

9. TEMPORARY CONCRETE BARRIER SHALL BE INSTALLED IN ACCORDANCE WITH SECTION 704 OF THE IDOT STANDARD SPECIFICATIONS. ALL TEMPORARY CONCRETE BARRIER APPROACH AND DEPARTING END UNITS SHALL BE ANCHORED TO THE PAVEMENT USING SIX ANCHOR PINS AS SHOWN IN IDOT STANDARD 704001. PINNING OF ADDITIONAL BARRIER UNITS WITH THREE ANCHOR PINS ON THE TRAFFIC SIDE HOLES WITHIN THE INSTALLATION SHALL BE REQUIRED WHEN EQUIPMENT, VEHICLES, MATERIALS, FIXED OBJECTS, OR A DROP-OFF IS LOCATED WITHIN 24" BEHIND THE BARRIER. THE 24" OF CLEAR PAVEMENT MEASUREMENT SHALL BE FROM THE BASE OF THE NON-TRAFFIC SIDE OF THE BARRIER. TRAFFIC SIDE PINNED BARRIER SHALL HAVE A MINIMUM OF 6" OF CLEAR PAVEMENT BEHIND THE BARRIER. WHERE BOTH PINNED AND UNPINNED BARRIER UNITS ARE USED IN A CONTINUOUS INSTALLATION, A TRANSITION SHALL BE PROVIDED BETWEEN THEM. THE TRANSITION FROM PINNED TO UNPINNED BARRIER SHALL CONSIST OF TWO ANCHOR PINS INSTALLED IN THE END HOLES ON THE TRAFFIC SIDE OF THE FIRST BARRIER BEYOND THE PINNED SECTION AND ONE ANCHOR PIN INSTALLED IN THE MIDDLE HOLE OF THE TRAFFIC SIDE OF THE SECOND BARRIER BEYOND THE PINNED SECTION. THE THIRD BARRIER BEYOND THE PINNED SECTION SHALL THEN BE UNPINNED.

IL 53 CONSTRUCTION STAGING

MAINTENANCE OF TRAFFIC - STAGE 1

CONSTRUCTION:
1. COMPLETE BRIDGE DECK AND JOINT REPAIRS AND OVERLAY OF THE EAST SIDE OF THE BRIDGES.

MAINTENANCE OF TRAFFIC:
1. CLOSE THE EAST LANE OF TRAFFIC AND SHIFT TRAFFIC TO THE WEST LANE. 1 LANE IN EACH DIRECTION SHALL BE MAINTAINED.
2. ONE WAY / ONE LANE OPERATION ON GINKGO WAY IN COORDINATION WITH THE MORTON ARBORETUM.

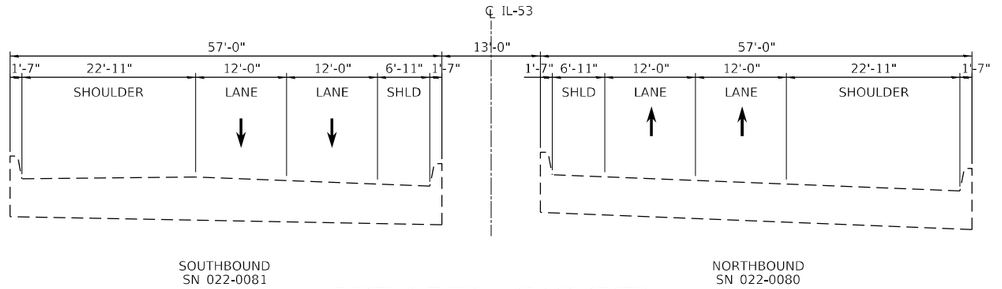
MAINTENANCE OF TRAFFIC - STAGE 2

CONSTRUCTION:
1. COMPLETE BRIDGE DECK AND JOINT REPAIRS AND OVERLAY OF THE WEST SIDE OF THE BRIDGES.

MAINTENANCE OF TRAFFIC:
1. CLOSE THE WEST LANE OF TRAFFIC AND SHIFT TRAFFIC TO THE EAST LANE. 1 LANE IN EACH DIRECTION SHALL BE MAINTAINED.
2. ONE WAY / ONE LANE OPERATION ON GINKGO WAY IN COORDINATION WITH THE MORTON ARBORETUM.

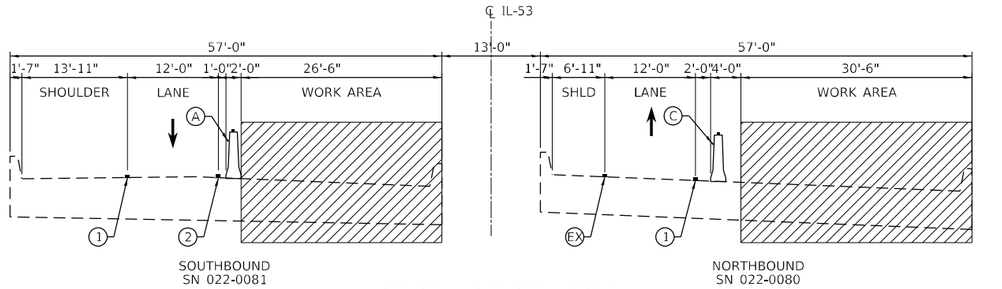
WORK ZONE SPEED LIMIT:

	EXISTING POSTED SPEED	PROPOSED POSTED SPEED
STAGE 1	45 MPH	45 MPH
STAGE 2	45 MPH	45 MPH



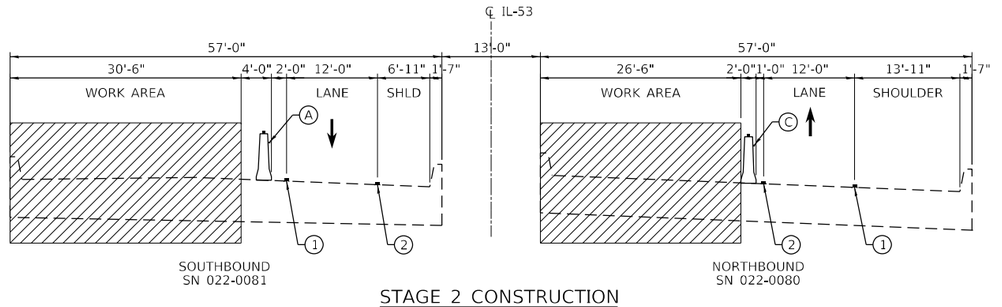
EXISTING TYPICAL CROSS SECTION

BRIDGES OVER GINKGO WAY



STAGE 1 CONSTRUCTION

BRIDGES OVER GINKGO WAY



STAGE 2 CONSTRUCTION

BRIDGES OVER GINKGO WAY

MOT TYPICAL LEGEND:

- WORK AREA
- DIRECTION OF TRAVEL
- TEMPORARY CONCRETE BARRIER PER STD 704001
- TEMPORARY PAVEMENT MARKING
- TYPE II BARRICADE, DRUM, OR VERTICAL BARRICADE
- (A) MONO-DIRECTIONAL AMBER TYPE C REFLECTORS PER STD 782006
- (C) MONO-DIRECTIONAL CRYSTAL TYPE C REFLECTORS PER STD 782006
- (EX) EXISTING PAVEMENT MARKING
- (1) TEMPORARY PAVEMENT MARKING TAPE, TYPE IV 4" (WHITE) (70300904)
- (2) TEMPORARY PAVEMENT MARKING TAPE, TYPE IV 4" (YELLOW) (70300904)

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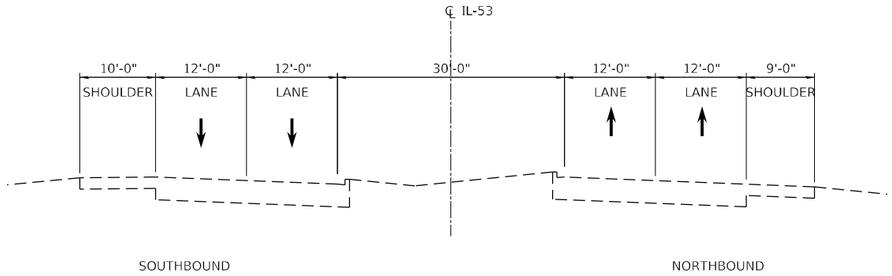
PLOT DATE = 1/8/2025	DATE - 07/28/2023	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLANS
GENERAL NOTES AND TYPICAL SECTIONS

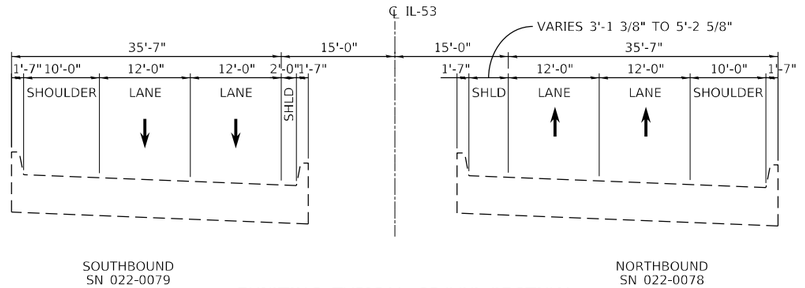
SCALE: NTS | SHEET OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ2	DUPAGE	44	6
		CONTRACT NO. 62T31		
	ILLINOIS FED. AID PROJECT			



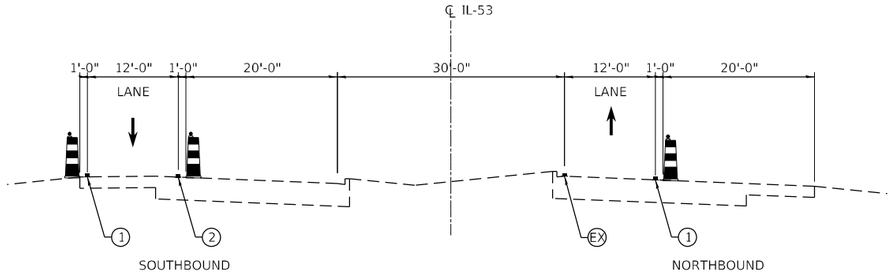
EXISTING TYPICAL CROSS SECTION

BETWEEN BRIDGES



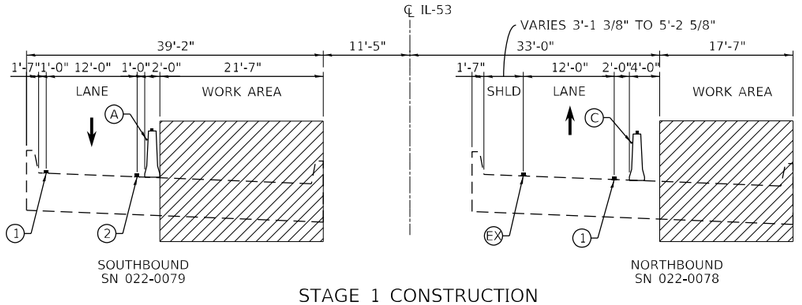
EXISTING TYPICAL CROSS SECTION

BRIDGES OVER EAST BRANCH DUPAGE RIVER



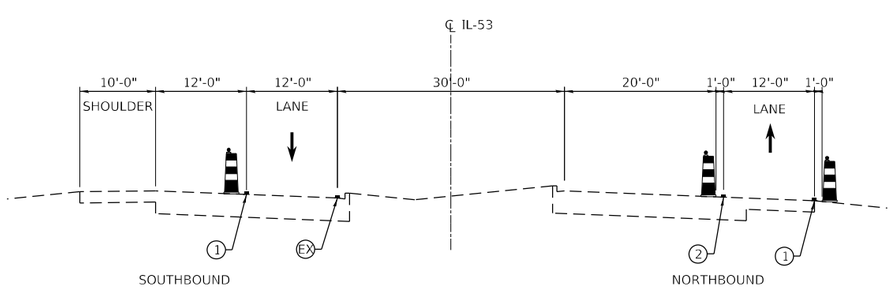
STAGE 1 CONSTRUCTION

BETWEEN BRIDGES



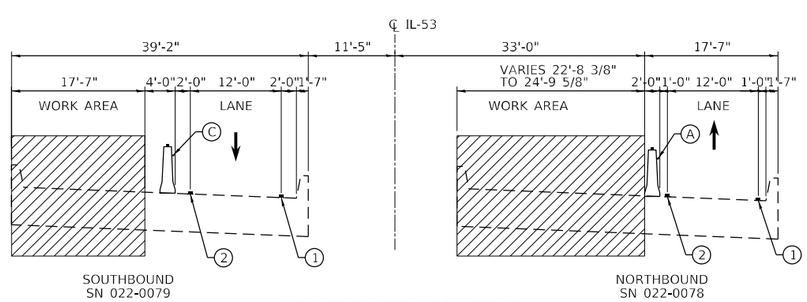
STAGE 1 CONSTRUCTION

BRIDGES OVER EAST BRANCH DUPAGE RIVER



STAGE 2 CONSTRUCTION

BETWEEN BRIDGES



STAGE 2 CONSTRUCTION

BRIDGES OVER EAST BRANCH DUPAGE RIVER

MOT TYPICAL LEGEND:

- WORK AREA
- DIRECTION OF TRAVEL
- TEMPORARY CONCRETE BARRIER PER STD 704001
- TEMPORARY PAVEMENT MARKING
- TYPE II BARRICADE, DRUM, OR VERTICAL BARRICADE
- (A) MONO-DIRECTIONAL AMBER TYPE C REFLECTORS PER STD 782006
- (C) MONO-DIRECTIONAL CRYSTAL TYPE C REFLECTORS PER STD 782006
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PLOT DATE = 1/3/2025	DATE - 07/28/2023	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLANS
TYPICAL SECTIONS

SCALE: NTS | SHEET OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ2	DUPAGE	44	6A
		CONTRACT NO. 62T31		
	ILLINOIS	FED. AID PROJECT		

MODEL: Default
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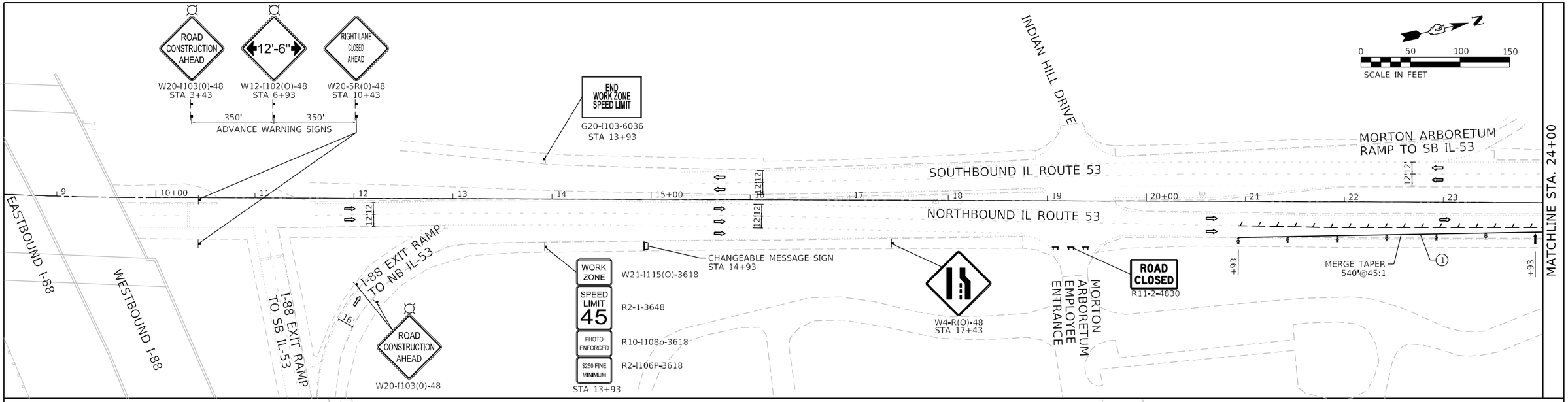

MOT SYMBOL LEGEND

- DRUM OR BARRICADE
- DIRECTION INDICATOR BARRICADE
- WORK AREA
- TYPE III BARICADE
- DIRECTION OF TRAFFIC
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- ARROW BOARD
- TEMPORARY CONCRETE BARRIER
- IMPACT ATTENUATOR, FULLY REDIRECTIVE, NARROW, TEST LEVEL 2
- PAVEMENT MARKING REMOVAL
- FLAGGER WITH TRAFFIC CONTROL SIGN

MOT PROPOSED LEGEND

1. TEMPORARY PAVEMENT MARKING TAPE, TYPE IV 4" (WHITE)
2. TEMPORARY PAVEMENT MARKING TAPE, TYPE IV 4" (YELLOW)
3. TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS, TYPE IV (WHITE)
4. TEMPORARY PAVEMENT MARKING TAPE, TYPE IV 6" (WHITE)
5. TEMPORARY PAVEMENT MARKING TAPE, TYPE IV 6" (WHITE) (2' LINE - 6' SPACE)

NOTES:
1. REMOVE CONFLICTING PAVEMENT MARKINGS USING PAVEMENT MARKING REMOVAL - WATERBLASTING

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PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -	
PLOT DATE = 1/8/2025	DATE - 07/28/2023	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLANS
STAGE 1

SCALE: NTS | SHEET OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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		CONTRACT NO. 62T31		
		ILLINOIS	FED. AID PROJECT	

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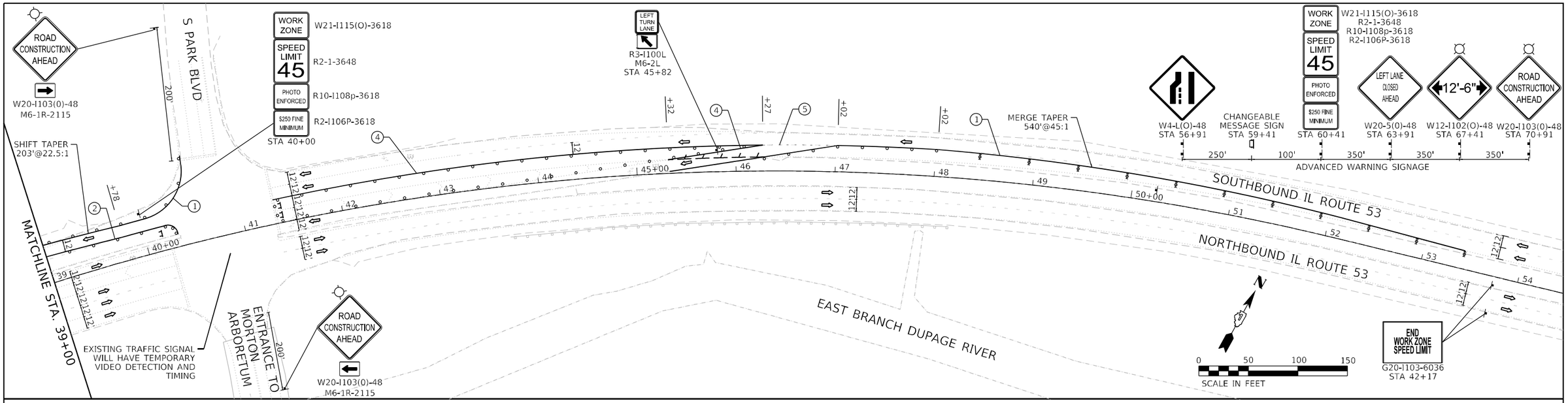
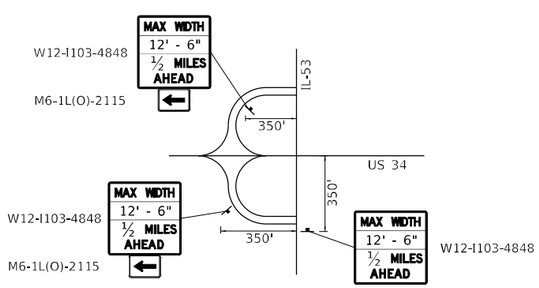
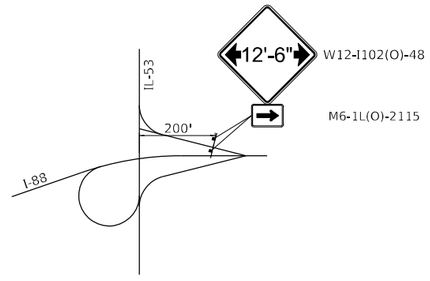
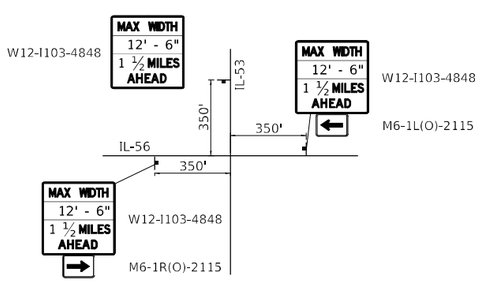


ADVANCED WIDTH RESTRICTION SIGNS







MOT SYMBOL LEGEND

- DRUM OR BARRICADE
- DIRECTION INDICATOR BARRICADE
- WORK AREA
- TYPE III BARICADE
- DIRECTION OF TRAFFIC
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- ARROW BOARD
- TEMPORARY CONCRETE BARRIER
- IMPACT ATTENUATOR, FULLY REDIRECTIVE, NARROW, TEST LEVEL 2
- PAVEMENT MARKING REMOVAL
- FLAGGER WITH TRAFFIC CONTROL SIGN

MOT PROPOSED LEGEND

1. TEMPORARY PAVEMENT MARKING TAPE, TYPE IV 4" (WHITE)
2. TEMPORARY PAVEMENT MARKING TAPE, TYPE IV 4" (YELLOW)
3. TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS, TYPE IV (WHITE)
4. TEMPORARY PAVEMENT MARKING TAPE, TYPE IV 6" (WHITE)
5. TEMPORARY PAVEMENT MARKING TAPE, TYPE IV 6" (WHITE) (2' LINE - 6' SPACE)

NOTES:
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PLOT DATE = 1/8/2025	DATE - 07/28/2023	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLANS
STAGE 1

SCALE: 1" = 50' | SHEET OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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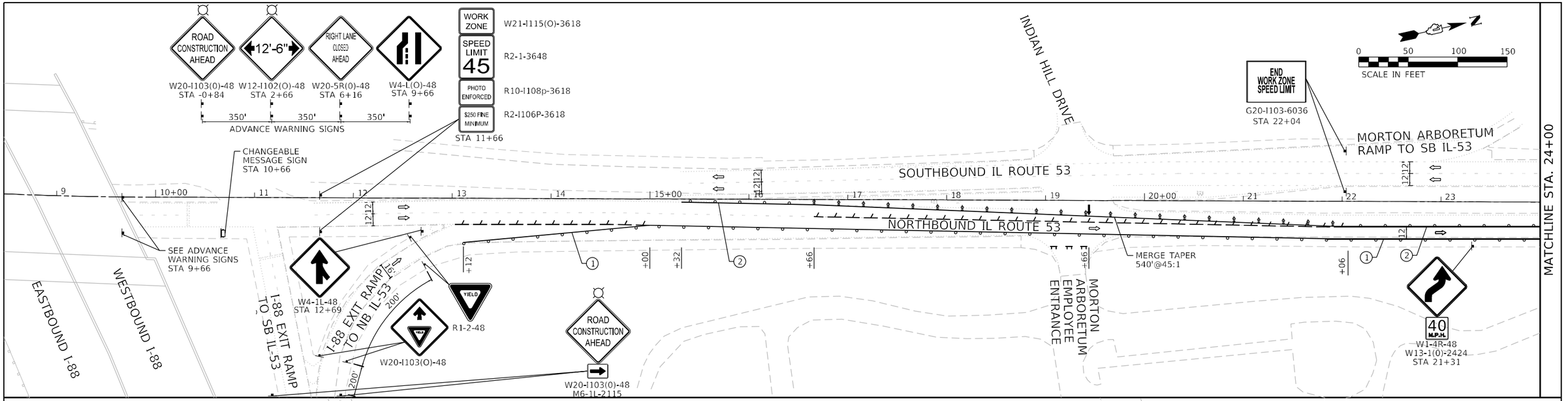
		CONTRACT NO. 62T31		
	ILLINOIS	FED. AID PROJECT		



MOT SYMBOL LEGEND

- DRUM OR BARRICADE
- DIRECTION INDICATOR BARRICADE
- WORK AREA
- TYPE III BARICADE
- DIRECTION OF TRAFFIC
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- ARROW BOARD
- TEMPORARY CONCRETE BARRIER
- IMPACT ATTENUATOR, FULLY REDIRECTIVE, NARROW, TEST LEVEL 2
- PAVEMENT MARKING REMOVAL
- FLAGGER WITH TRAFFIC CONTROL SIGN

MOT PROPOSED LEGEND

1. TEMPORARY PAVEMENT MARKING TAPE, TYPE IV 4" (WHITE)
2. TEMPORARY PAVEMENT MARKING TAPE, TYPE IV 4" (YELLOW)
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NOTES:
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PLOT DATE = 1/8/2025	DATE - 07/28/2023	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLANS
STAGE 2

SCALE: 1" = 50' | SHEET OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ2	DUPAGE	44	9
		CONTRACT NO. 62T31		
		ILLINOIS	FED. AID PROJECT	

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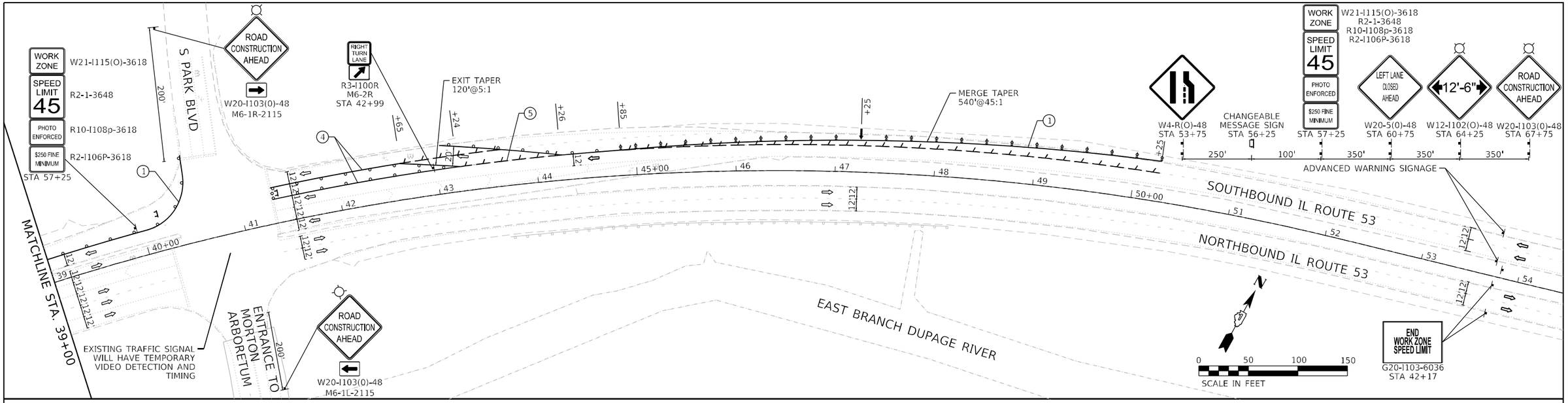
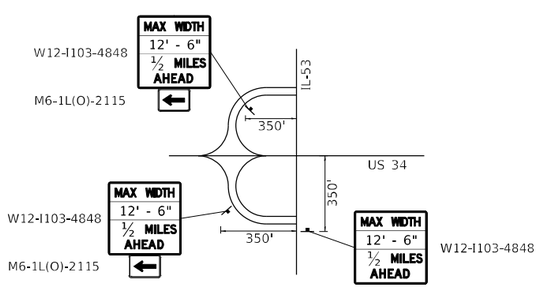
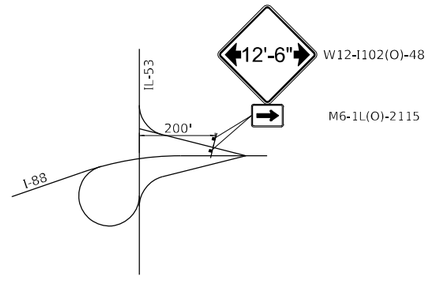
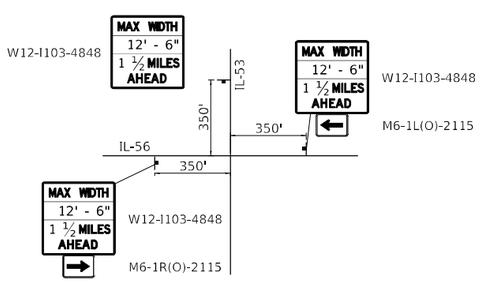


ADVANCED WIDTH RESTRICTION SIGNS







MOT SYMBOL LEGEND

- DRUM OR BARRICADE
- DIRECTION INDICATOR BARRICADE
- WORK AREA
- TYPE III BARICADE
- DIRECTION OF TRAFFIC
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- ARROW BOARD
- TEMPORARY CONCRETE BARRIER
- IMPACT ATTENUATOR, FULLY REDIRECTIVE, NARROW, TEST LEVEL 2
- PAVEMENT MARKING REMOVAL
- FLAGGER WITH TRAFFIC CONTROL SIGN

MOT PROPOSED LEGEND

1. TEMPORARY PAVEMENT MARKING TAPE, TYPE IV 4" (WHITE)
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5. TEMPORARY PAVEMENT MARKING TAPE, TYPE IV 6" (WHITE) (2' LINE - 6' SPACE)

NOTES:
1. REMOVE CONFLICTING PAVEMENT MARKINGS USING PAVEMENT MARKING REMOVAL - WATERBLASTING

MODEL: Default
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	DRAWN - ACL	REVISED -	
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -	
PLOT DATE = 1/8/2025	DATE - 07/28/2023	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

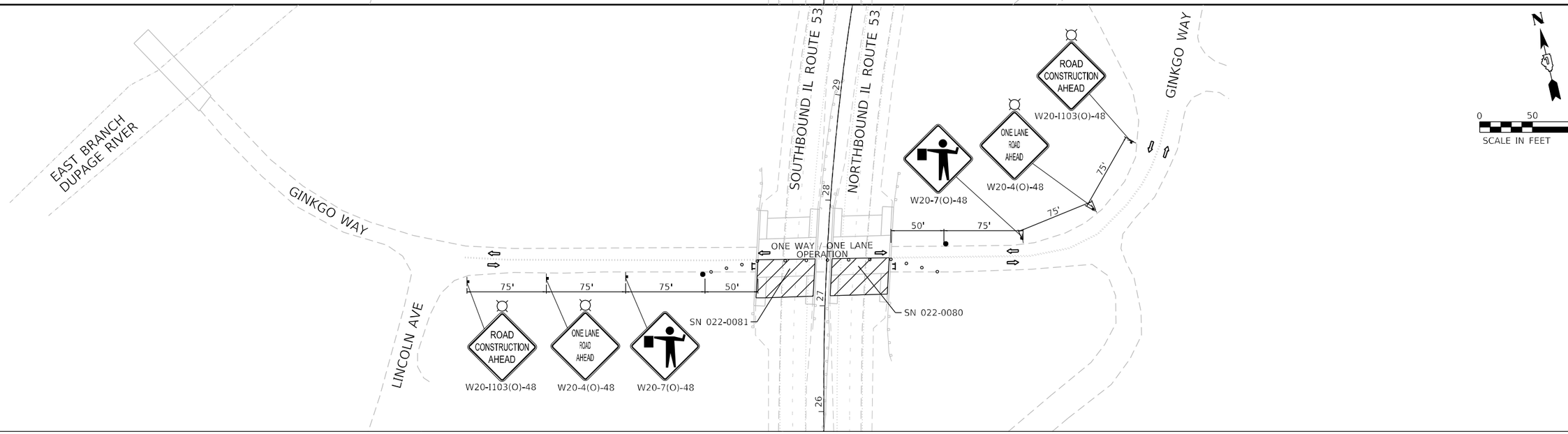
TRAFFIC CONTROL PLANS
STAGE 2

SCALE: 1" = 50' | SHEET OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ2	DUPAGE	44	10
		CONTRACT NO. 62T31		
	ILLINOIS	FED. AID PROJECT		

TYPICAL SECTION

GINKGO WAY UNDER IL 53



MOT SYMBOL LEGEND

- DRUM OR BARRICADE
- DIRECTION INDICATOR BARRICADE
- WORK AREA
- TYPE III BARICADE
- DIRECTION OF TRAFFIC
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- ARROW BOARD
- TEMPORARY CONCRETE BARRIER
- IMPACT ATTENUATOR, FULLY REDIRECTIVE, NARROW, TEST LEVEL 2
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- FLAGGER WITH TRAFFIC CONTROL SIGN

MOT PROPOSED LEGEND

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4. TEMPORARY PAVEMENT MARKING TAPE, TYPE IV 6" (WHITE)
5. TEMPORARY PAVEMENT MARKING TAPE, TYPE IV 6" (WHITE) (2' LINE - 6' SPACE)

NOTES:

1. WORK OVER GINKO WAY SHALL TAKE PLACE AT NIGHT. LOOSE DEBRIS SHALL BE CLEANED AND CLEARED BEFORE TRAFFIC RESUMES ON GINKO WAY.

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USER NAME = ALane	DESIGNED - ACL	REVISED -
	DRAWN - ACL	REVISED -
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 1/24/2025	DATE - 07/28/2023	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL PLANS
STAGE 1 & 2 GINKO WAY

SCALE: 1" = 50' | SHEET OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ2	DUPAGE	44	11
		CONTRACT NO. 62T31		
	ILLINOIS	FED. AID PROJECT		

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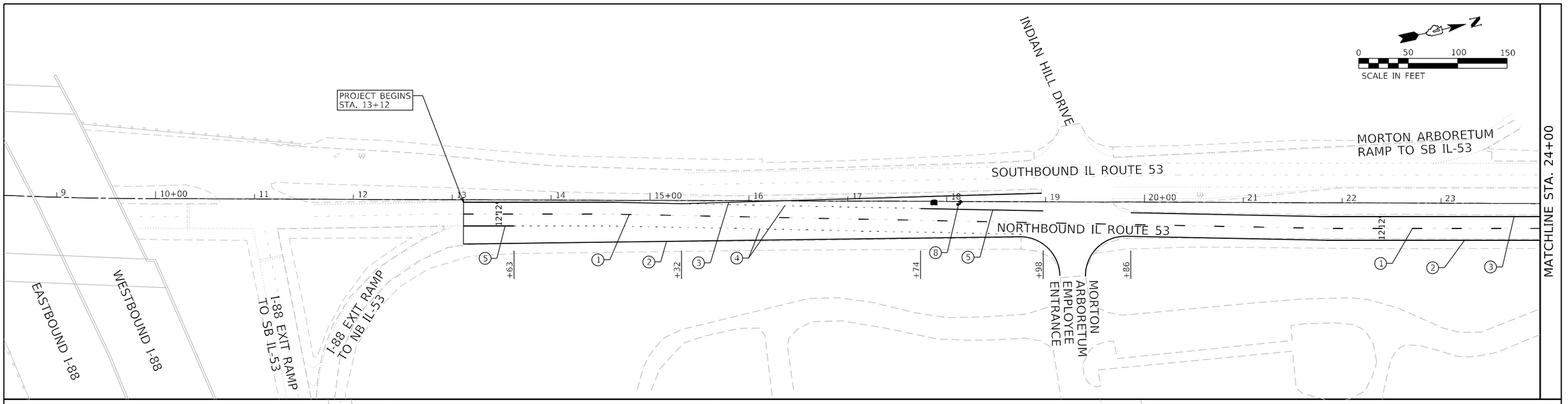
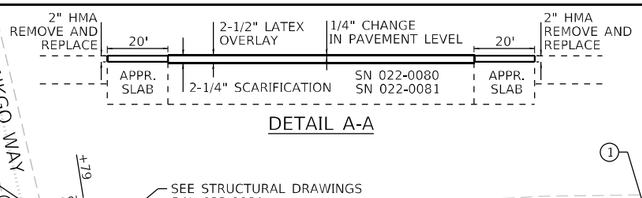
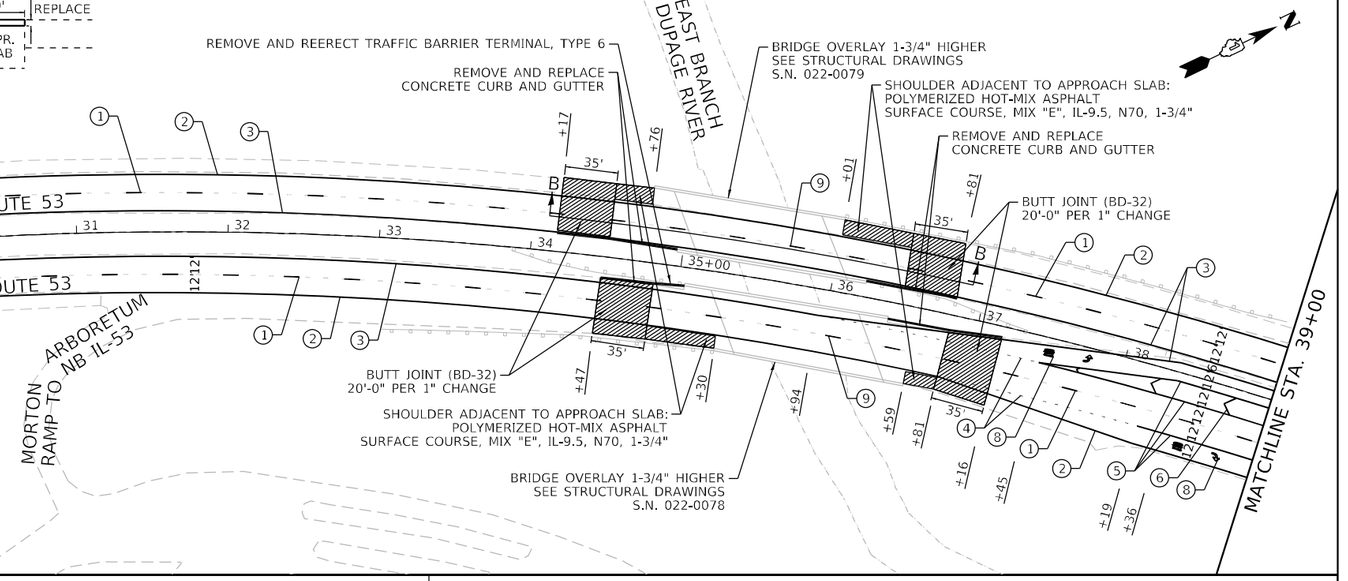




DETAIL A-A



PAVEMENT MARKING LEGEND

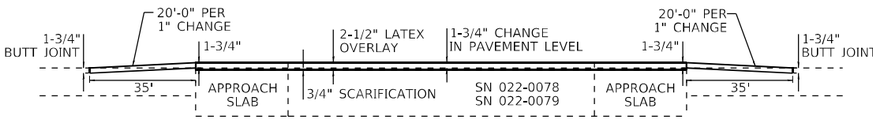
1. LINE 4", SKIP-DASH, YELLOW (10' LINE - 30' SPACE) - LANE LINE
2. LINE 4", SOLID, WHITE - EDGE LINE
3. LINE 4", SOLID, YELLOW - EDGE LINE
4. LINE 6", SKIP-DASH, WHITE (2' LINE - 6' SPACE) - DOTTED LINES
5. LINE 6", SOLID, WHITE - TURN LINE
6. LINE 12", SOLID, WHITE - PAINTED MEDIANS DIAGONALS (SEE DISTRICT DETAIL TC-13 FOR SPACING)
7. LINE 24", SOLID, WHITE - STOP LINE
8. LETTERS & SYMBOLS, SOLID, WHITE - TURN LANE MARKINGS
9. PREFORMED PLASTIC PAVEMENT MARKING, TYPE D, LINE 7", SKIP-DASH, (10' LINE - 30' SPACE), (1.5" BLACK, 4" WHITE, 1.5" BLACK) ON GROOVING FOR RECESSED PAVEMENT MARKING 8"

LEGEND

HMA OVERLAY

NOTES

1. ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC PAVEMENT MARKINGS (OF THE EXTRUDED TYPE) ON HMA PAVEMENT AND MODIFIED URETHANE ON CONCRETE PAVEMENT UNLESS OTHERWISE NOTED.
2. ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH TC-13 "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" STANDARD DETAIL.
3. ALL RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH TC-11 "TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" STANDARD DETAIL.



DETAIL B-B

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PLOT DATE = 1/8/2025	DATE - 07/28/2023	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY AND PAVEMENT MARKING PLANS

SCALE: 1" = 50' | SHEET OF SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ2	DUPAGE	44	12
		CONTRACT NO. 62T31		
	ILLINOIS	FED. AID PROJECT		

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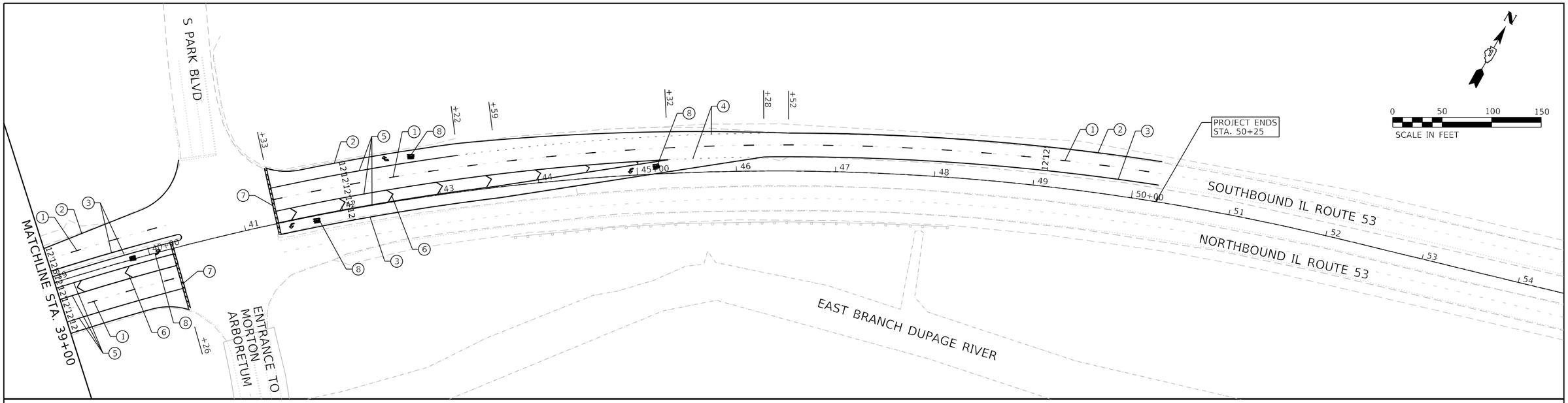

PAVEMENT MARKING LEGEND

1. LINE 4", SKIP-DASH, YELLOW (10' LINE - 30' SPACE) - LANE LINE
2. LINE 4", SOLID, WHITE - EDGE LINE
3. LINE 4", SOLID, YELLOW - EDGE LINE
4. LINE 6", SKIP-DASH, WHITE (2' LINE - 6' SPACE) - DOTTED LINES
5. LINE 6", SOLID, WHITE - TURN LINE
6. LINE 12", SOLID, WHITE - PAINTED MEDIANS DIAGONALS (SEE DISTRICT DETAIL TC-13 FOR SPACING)
7. LINE 24", SOLID, WHITE - STOP LINE
8. LETTERS & SYMBOLS, SOLID, WHITE - TURN LANE MARKINGS
9. PREFORMED PLASTIC PAVEMENT MARKING, TYPE D, LINE 7", SKIP-DASH, (10' LINE - 30' SPACE), (1.5" BLACK, 4" WHITE, 1.5" BLACK) ON GROOVING FOR RECESSED PAVEMENT MARKING 8"

LEGEND

HMA OVERLAY

NOTES

1. ALL FINAL PAVEMENT MARKINGS SHALL BE THERMOPLASTIC PAVEMENT MARKINGS (OF THE EXTRUDED TYPE) ON HMA PAVEMENT AND MODIFIED URETHANE ON CONCRETE PAVEMENT UNLESS OTHERWISE NOTED.
2. ALL FINAL PAVEMENT MARKINGS SHALL BE PLACED IN ACCORDANCE WITH TC-13 "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" STANDARD DETAIL.
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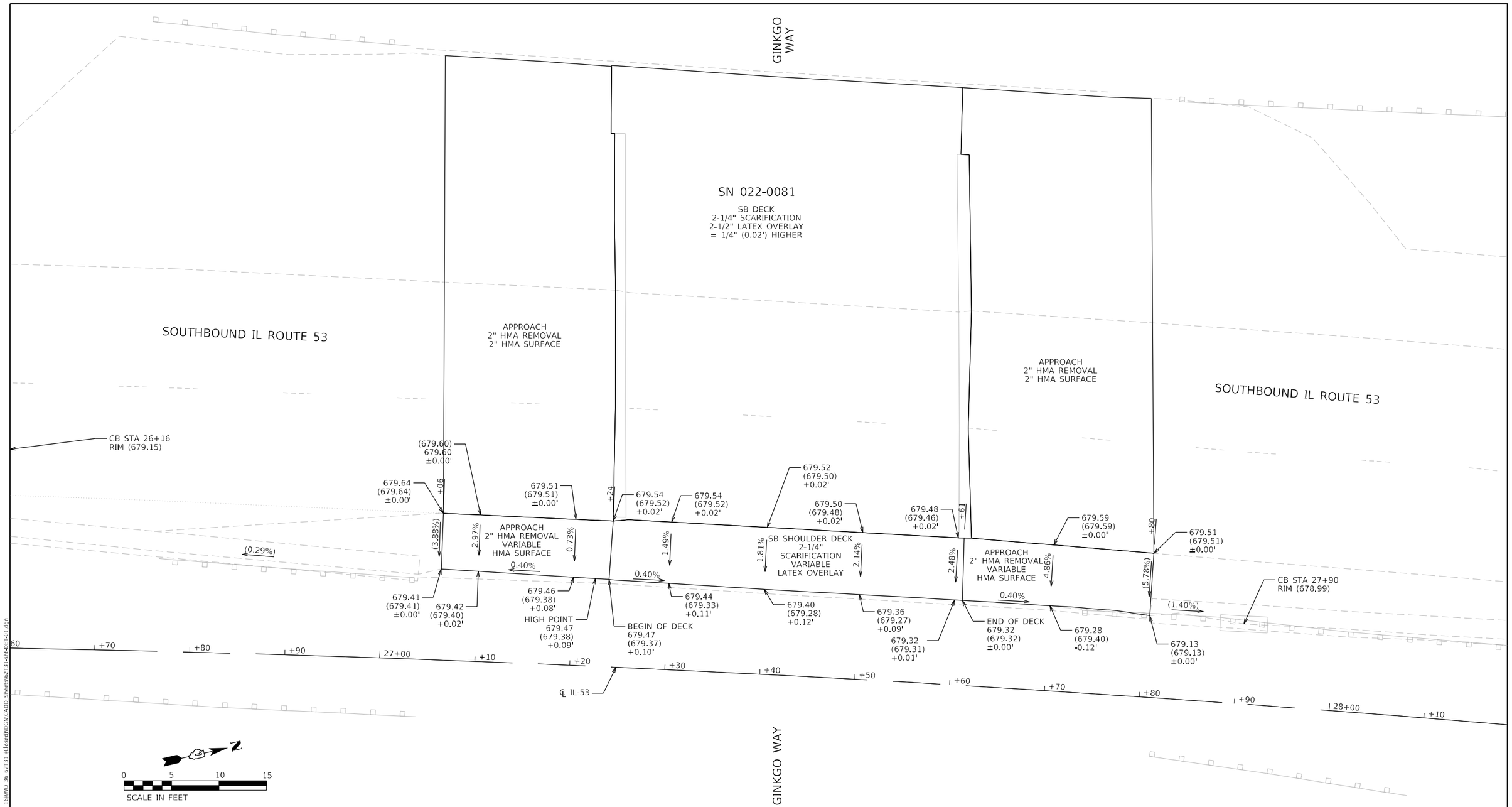
PLOT DATE = 12/12/2024	DATE - 07/28/2023	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY AND PAVEMENT MARKING PLANS

SCALE: 1" = 50' | SHEET OF SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ2	DUPAGE	44	13
		CONTRACT NO. 62T31		
	ILLINOIS	FED. AID PROJECT		



LEGEND

679.40	PROPOSED ELEVATION
(679.28)	EXISTING ELEVATION
+0.12'	ELEVATION DIFFERENCE
(3.88%)	EXISTING SLOPE
2.97%	PROPOSED SLOPE

MODEL: Default
FILE NAME: P:\P-20-20-4094-00 IDOT Various Phase 2 (PTB 196 Item 16)\WO 36 62T31 (Closed)\DGN\CADD Sheets\62T31-sht-DET-01.dgn

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PLOT DATE = 1/8/2025	DATE - 07/28/2023	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INSIDE SHOULDER SLOPE CORRECTION DETAIL
S.N. 022-0081

SCALE: 1" = 5' | SHEET OF SHEETS | STA. | TO STA.

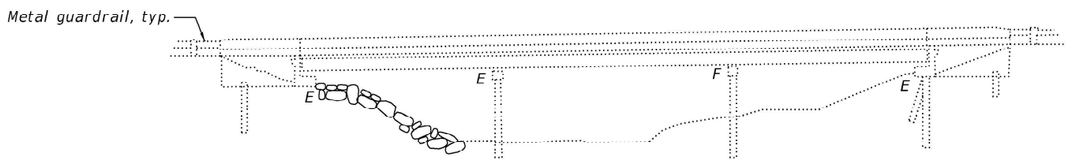
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ2	DUPAGE	44	14
		CONTRACT NO. 62T31		
	ILLINOIS FED. AID PROJECT			

Benchmark: Disk set on top of northwest retaining wall of southbound Route 53 bridge over east branch of Du Page River, elevation 680.24

Existing Structure: SN 022-0078 & 022-0079 were built as F.A. Route 61, Section 534X-RB at Sta. 28+00.00 1959. The deck was replaced and the bridge widened in 1991 as FAU Route 2578, Section 534X-R-1 & 534X-RB(BR & R) 89 at Station 967+00.00. Superstructures consist of concrete deck on three span continuous non-composite rolled steel beams. Substructures consist of stub abutments on concrete piles and concrete pile bent piers. Length is 100'-0½" (SB) and 101'-6" (NB) back-to-back abutments. Southbound width is 39'-2" and northbound width varies from 40'-3¾" to 42'-4⅝" out-to-out deck.

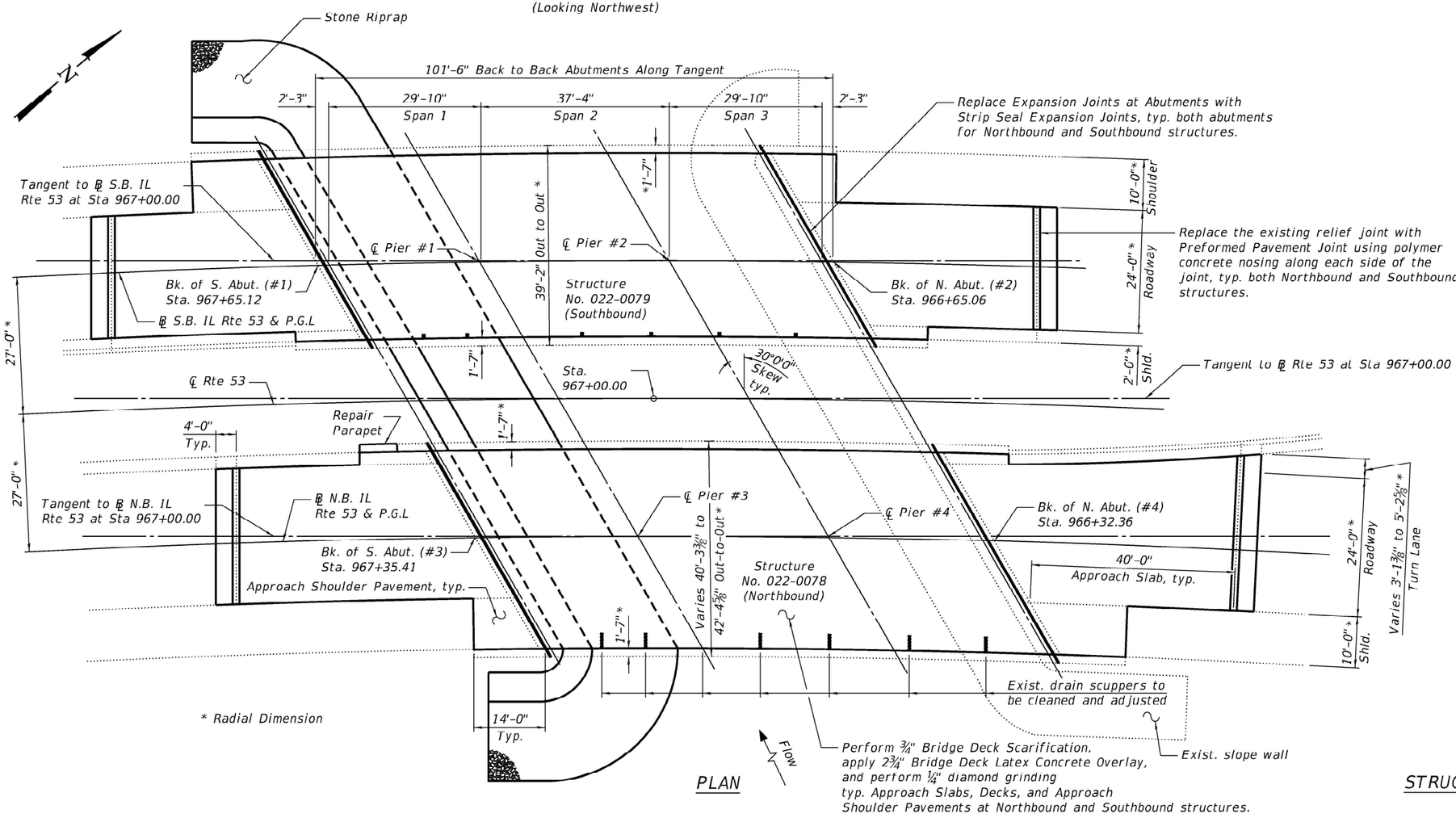
Traffic will be maintained using Stage Construction.

No Salvage.



ELEVATION

(Looking Northwest)



* Radial Dimension

PLAN

Pankaj Kumar

PANKAJ KUMAR, S.E.
NO. 081-007577
EXP. DATE: 11/30/2024

INDEX OF SHEETS

DESIGN SPECIFICATIONS

2002 AASHTO Standard Specifications for Highway Bridges (17th Edition)

LOADING HS20-44

No future wearing surface allowed

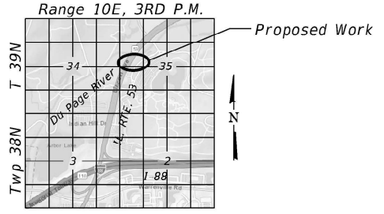
DESIGN STRESSES

New Construction

f'c = 3,500 psi (Substructure)
f'c = 4,000 psi (Superstructure)
fy = 60,000 psi (Reinforcement)

Existing Construction

f'c = 3,500 psi (Concrete)
fy = 60.000 psi (Reinforcement)



LOCATION SKETCH

GENERAL PLAN AND ELEVATION
IL. RTE 53 OVER
EAST BRANCH DU PAGE RIVER
FAP 870 SECTION FAP 0870 22 BJ2
DU PAGE COUNTY
STATION 967+00.00
STRUCTURE NOS. 022-0078(NB) & 022-0079(SB)

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PLOT DATE = 12/12/2024	CHECKED - SPK	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHEET 1 OF 17 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ2	DUPAGE	44	15
		CONTRACT NO. 62T31		
		ILLINOIS	FED. AID PROJECT	

SCOPE OF WORK

1. Perform ¾" bridge deck scarification on the bridge decks approach slabs, and approach shoulder pavements.
2. Perform approach slab repairs as required.
3. Remove bridge deck expansion joints at both abutments and install new preformed strip seal joints.
4. Apply 2¾" bridge deck latex concrete overlay on the bridge decks, approach slabs, and approach shoulder pavements.
5. Perform ¼" diamond grinding on the bridge decks, approach slabs, and approach shoulder pavements.
6. Perform bridge deck grooving (longitudinal) on the bridge decks and approach slabs.
7. Apply protective coat to the reconstructed top and inside surfaces of parapets, barriers, and concrete overlay areas.
8. Replace the existing relief joints with Preformed Pavement Joints.
9. Clean and adjust bridge deck scuppers.
10. Perform structural repair of concrete on the abutments and pier
11. Repair parapet on wing wall.
12. Repair north slope wall.
13. Replace south slope wall with stone riprap.

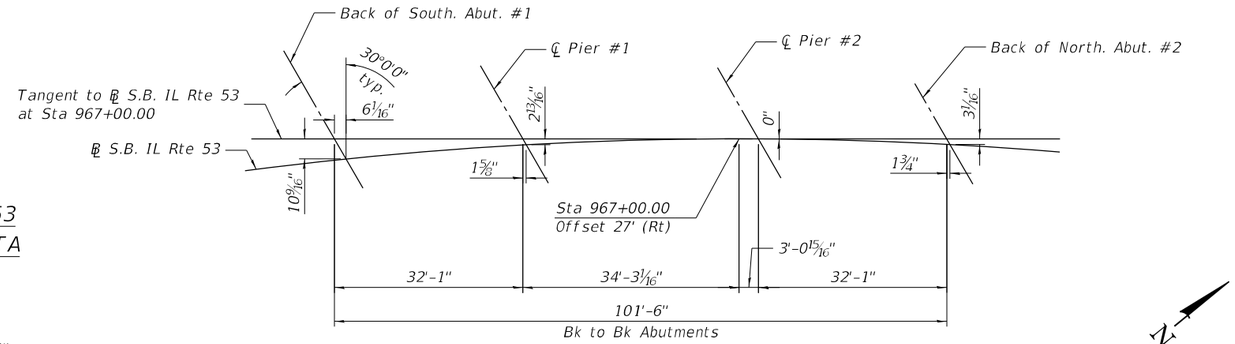
GENERAL NOTES

1. Reinforcement bars designated (E) shall be epoxy coated.
2. Plan dimensions and details relative to the existing structure have been taken from existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work; however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
3. All structural steel for expansion joints shall be AASHTO M270 Grade 36.
4. Prior to pouring the new concrete deck, all heavy or loose rust, loose mill scale, and other loose detrimental foreign material shall be removed from the surfaces in contact with concrete(SSPC – SP3 standards). Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be paid for according to Article 109.04 of the Standard Specifications.
5. Layout of the slope protection system may be varied to suit ground conditions in the field as directed by the Engineer.

TOTAL BILL OF MATERIAL

	ITEM	UNIT	SUPER	SUB	TOTAL
	Stone Riprap, Class A4	Sq Yd		577	577
	Filter Fabric	Sq Yd		580	580
	Concrete Removal	Cu Yd	26.4		26.4
	Slope Wall Removal	Sq Yd		577	577
	Concrete Superstructure	Cu Yd	26.3		26.3
*	Protective Coat	Sq Yd	1,439		1,439
	Reinforcement Bars, Epoxy Coated	Pound	2,960		2,960
	Bar Splicers	Each	48		48
	Preformed Joint Strip Seal	Foot	184		184
	Precast Reinforced Concrete Flared End Sections 18"	Each		1	1
	Bridge Deck Grooving (Longitudinal)	Sq Yd	1,080		1,080
	Approach Slab Repair (Full Depth)	Sq Yd	16		16
	Bridge Deck Latex Concrete Overlay, 2¾ Inches	Sq Yd	1,430		1,430
	Bridge Deck Scarification, ¾"	Sq Yd	1,435		1,435
	Structural Repair of Concrete (Depth Equal to or Less than 5 Inches)	Sq Ft		92	92
	Drainage Scuppers To Be Adjusted	Each	6		6
	Diamond Grinding (Bridge Section)	Sq Yd	1,331		1,331
	Polymer Concrete	Cu Ft	9.7		9.7
	Slope Wall Repair	Sq Yd		2	2

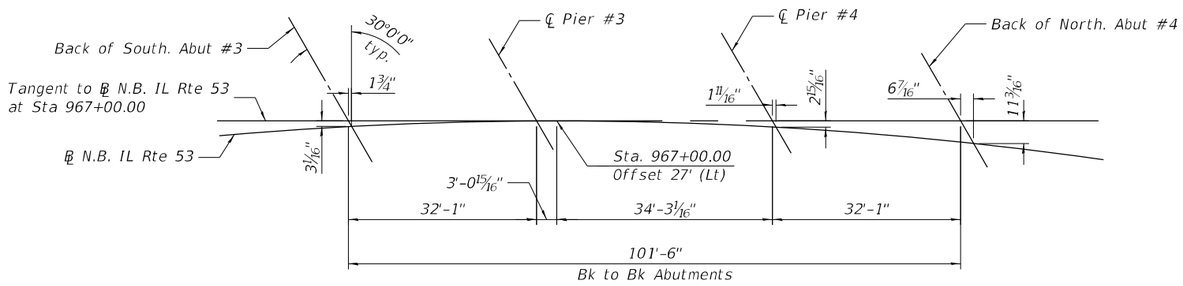
* Quantity is for the top of the deck, approach slabs, and approach shoulder pavements and top and roadway face of new concrete for the parapets.



OFFSET SKETCH – S.B. STRUCTURE

℄ ROUTE 53 CURVE DATA

E = 401.32'
S.E. = 0.038%
R = 2438.12'
L = 2624.11'
Δ = 61°-40'-00"
D = 2°-21'
T = 1455.33'
P.I. Sta. 963+60.39
P.C. Sta. 949+05.06
P.T. Sta. 975+29.17



OFFSET SKETCH – N.B. STRUCTURE

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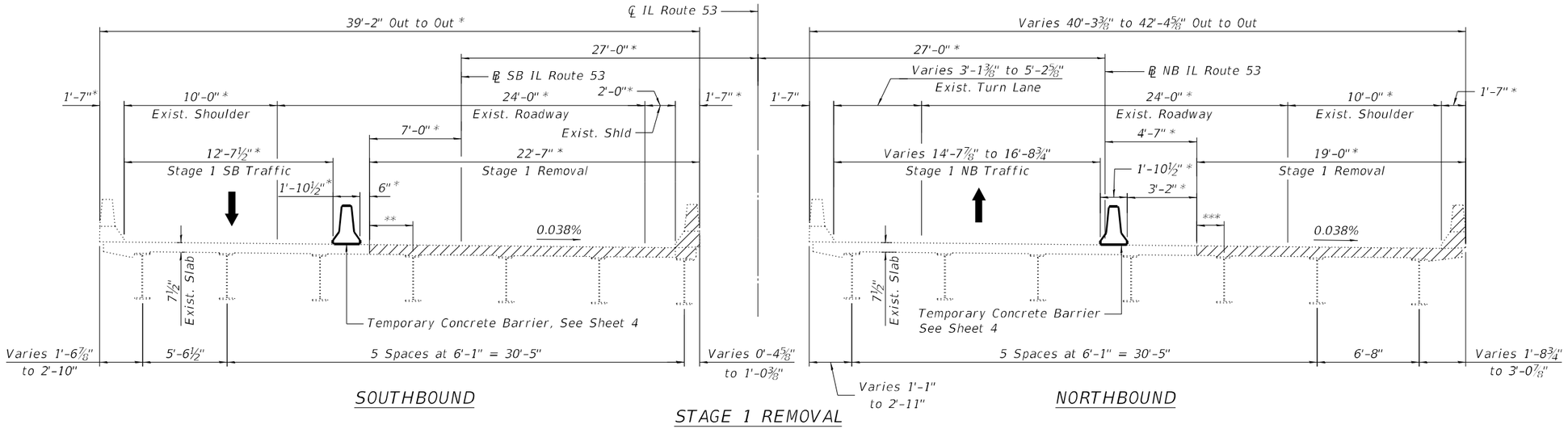
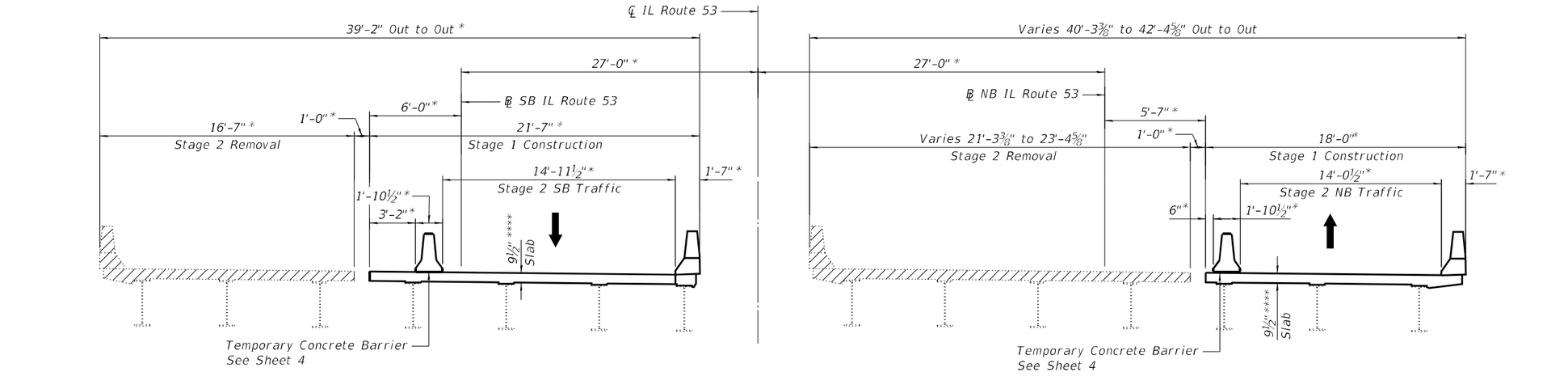
PLOT DATE = 12/12/2024	CHECKED - SPK	REVISED -	

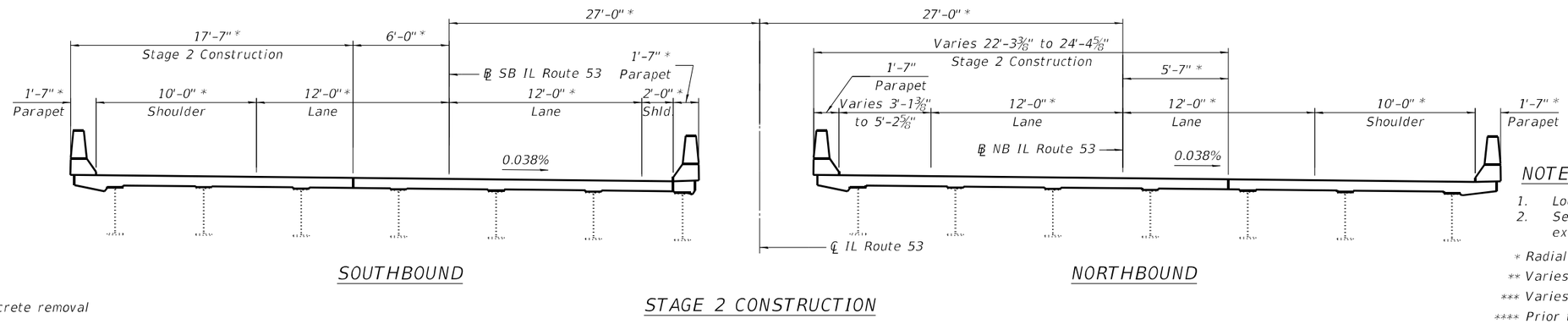
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES, SCOPE OF WORK, & BILL OF MATERIAL
S.N. 022–0078(NB) & S.N. 022–0079(SB)
SHEET 2 OF 17 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ2	DUPAGE	44	16
		CONTRACT NO. 62T31		
	ILLINOIS FED. AID PROJECT			







Hatched area indicates concrete removal

NOTES

1. Looking north for all sections.
2. Sections are shown near the expansion joints.

* Radial Dimension
** Varies from 2'-0⅞" to 2'-11¼"
*** Varies from 2'-2⅛" to 2'-11½"
**** Prior to grinding

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGE CONSTRUCTION DETAILS
S.N. 022–0078(NB) & S.N. 022–0079(SB)

SHEET 3 OF 17 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ2	DUPAGE	44	17
		CONTRACT NO. 62T31		
	ILLINOIS	FED. AID PROJECT		

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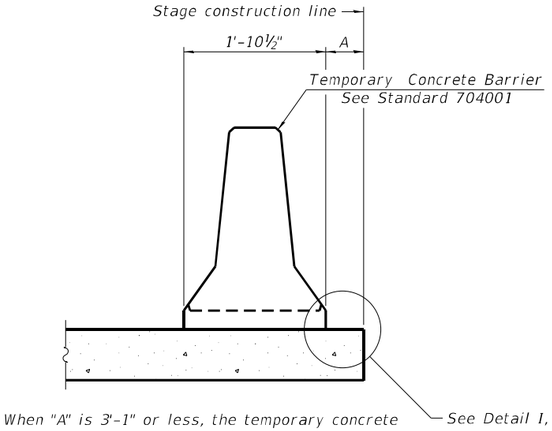
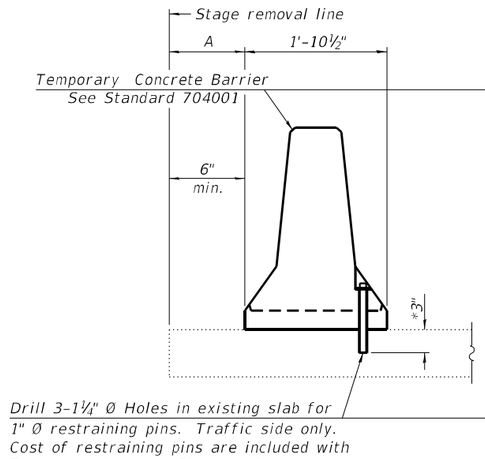


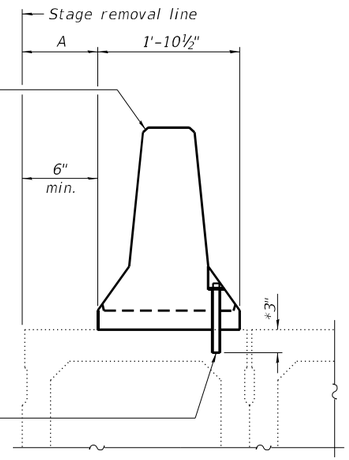
When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM



Drill 3-1¼" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

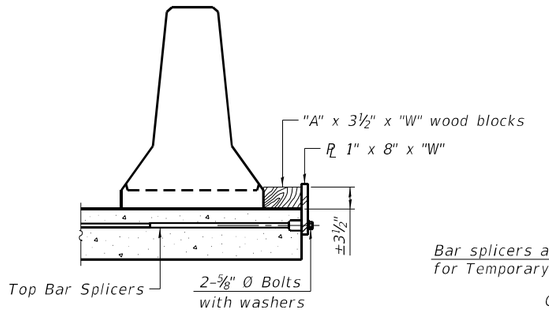
EXISTING SLAB



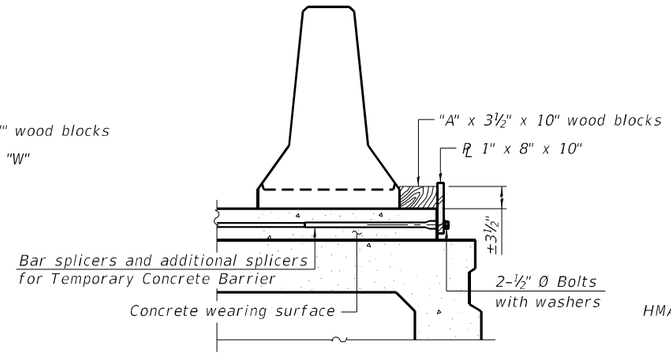
* When hot-mix asphalt wearing surface is present, embedment shall be 3" plus the wearing surface depth.

EXISTING DECK BEAM

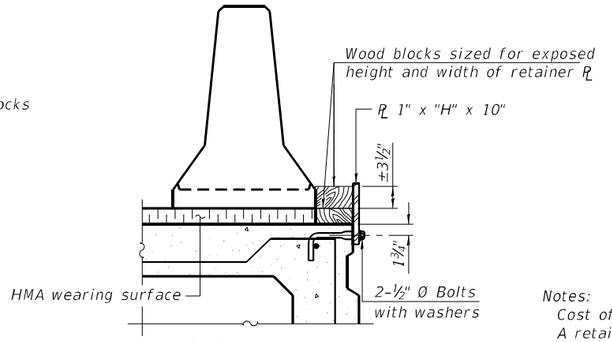
SECTIONS THRU SLAB OR DECK BEAM



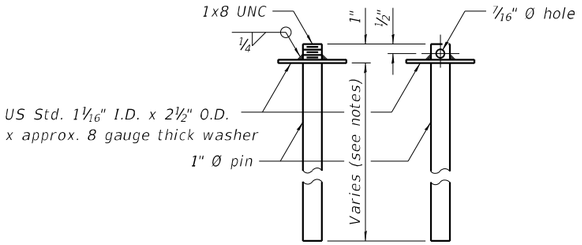
DETAIL I



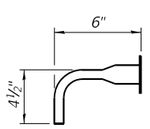
DETAIL II



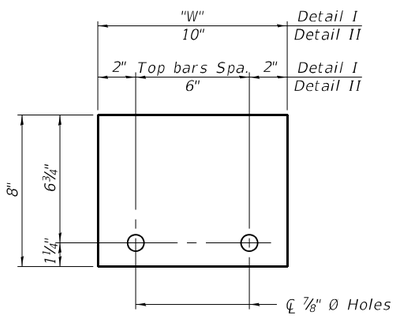
DETAIL III



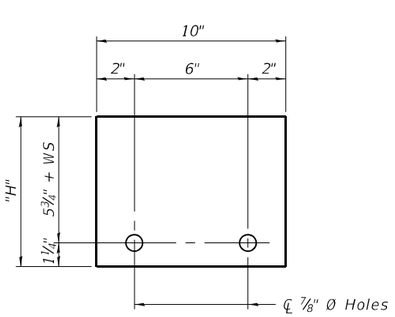
RESTRAINING PIN



BAR SPLICER FOR #4 BAR - DETAIL III



STEEL RETAINER PL 1" x 8" x "W"
(Detail I and II)



STEEL RETAINER PL 1" x "H" x 10"
(Detail III)

Notes:

Cost of retainer assembly is included with Temporary Concrete Barrier.

A retainer assembly shall be located at the approximate ℄ of each temporary concrete barrier.

The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.

When the 'A' dimension is less than 1½", the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.

Detail I - Installation for a new bridge deck or bridge slab.

Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.

Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

RAILING CRITERIA

NCHRP 350 Test Level	3
Railing Weight (plf)	440

R-27

10-12-2021

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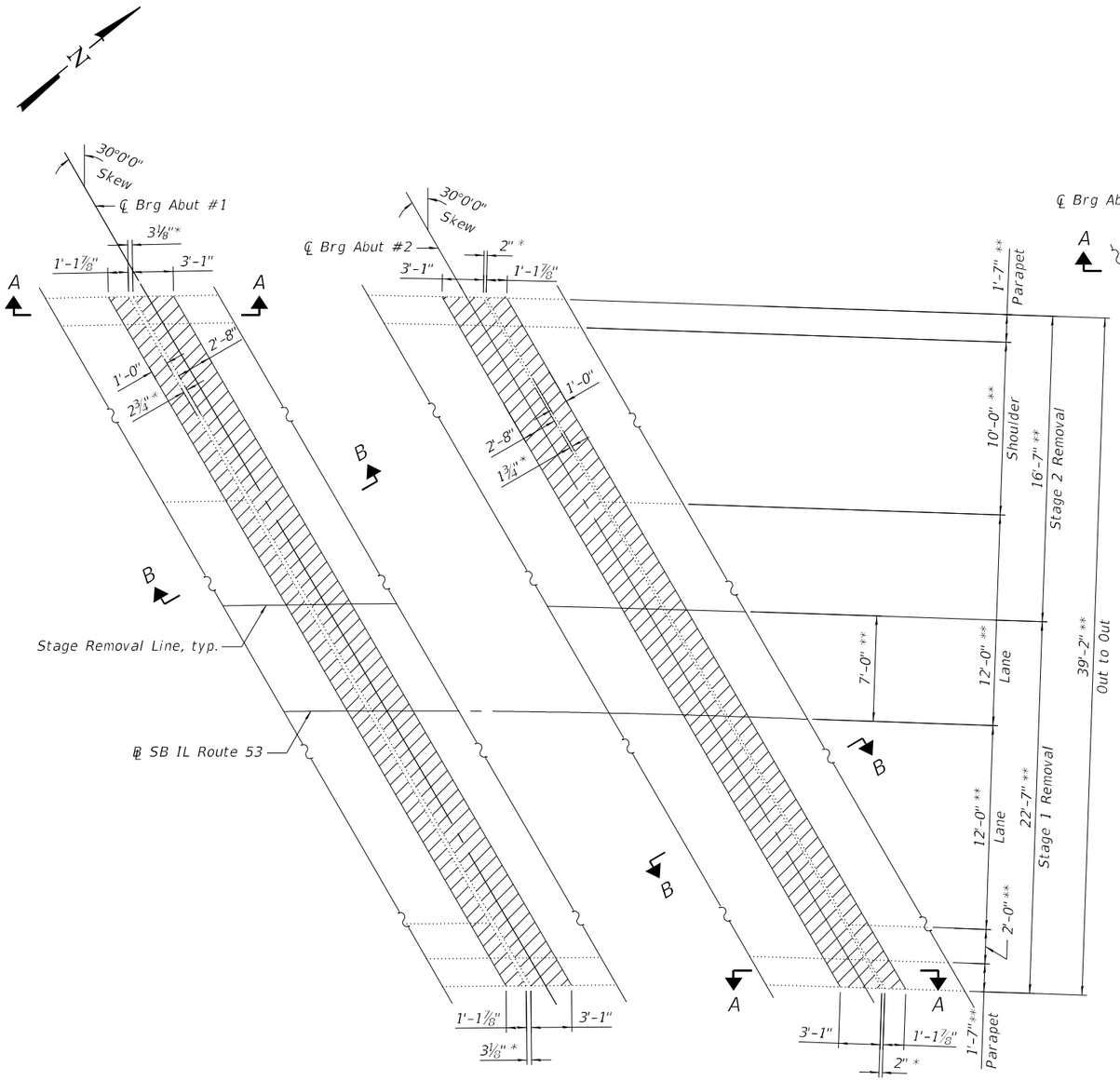
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

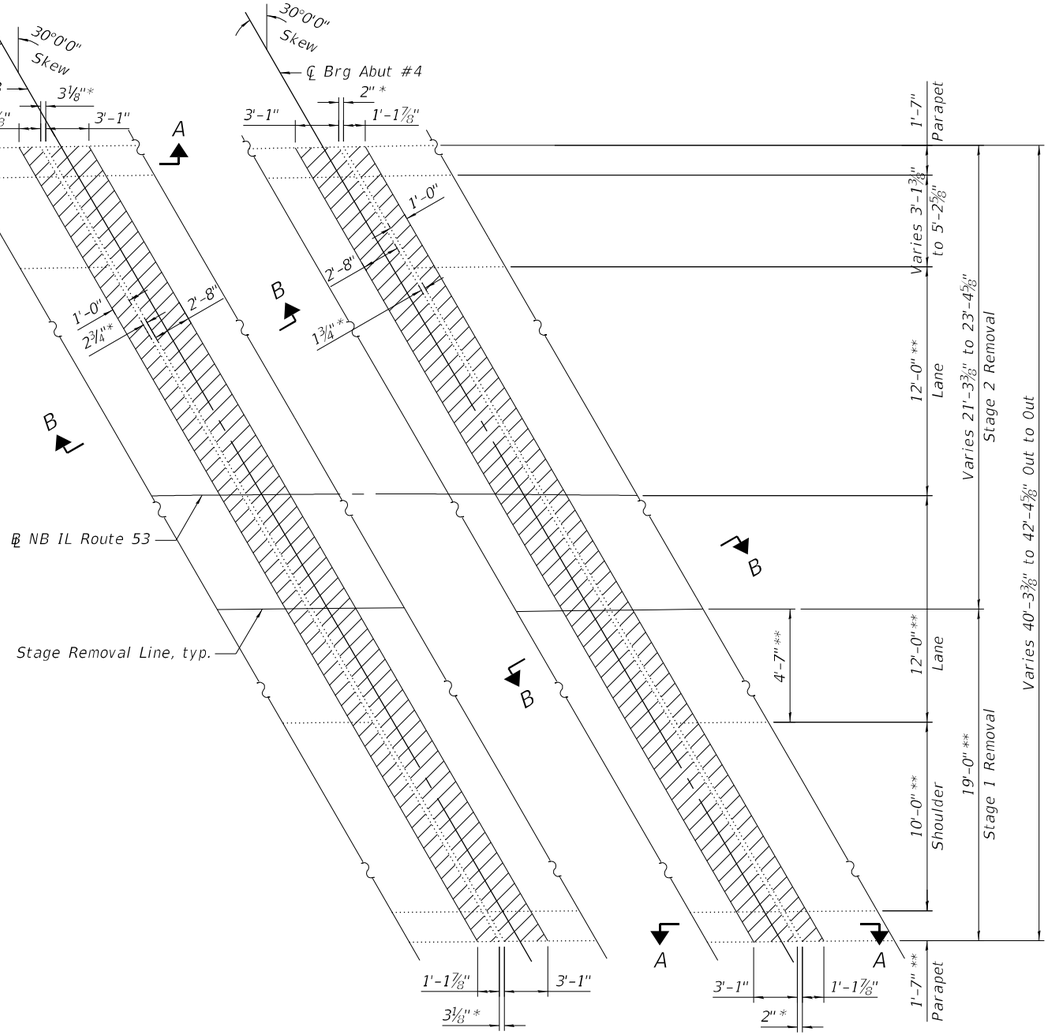
TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION
S.N. 022-0078(NB) & S.N. 022-0079(SB)

SHEET 4 OF 17 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ2	DUPAGE	44	18
		CONTRACT NO. 62T31		
	ILLINOIS FED. AID PROJECT			



SOUTHBOUND DECK



NORTHBOUND DECK

PLAN

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Concrete Removal	Cu Yd	26.2
Bridge Deck Scarification, ¾"	Sq Yd	815

NOTES

1. Hatched areas denote Concrete Removal.
2. See Sheet 6, for Sections A-A and B-B.

* At 50° F
** Radial
*** Curved

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ABUTMENT AND DECK CONCRETE REMOVAL
S.N. 022-0078(NB) & S.N. 022-0079(SB)
SHEET 5 OF 17 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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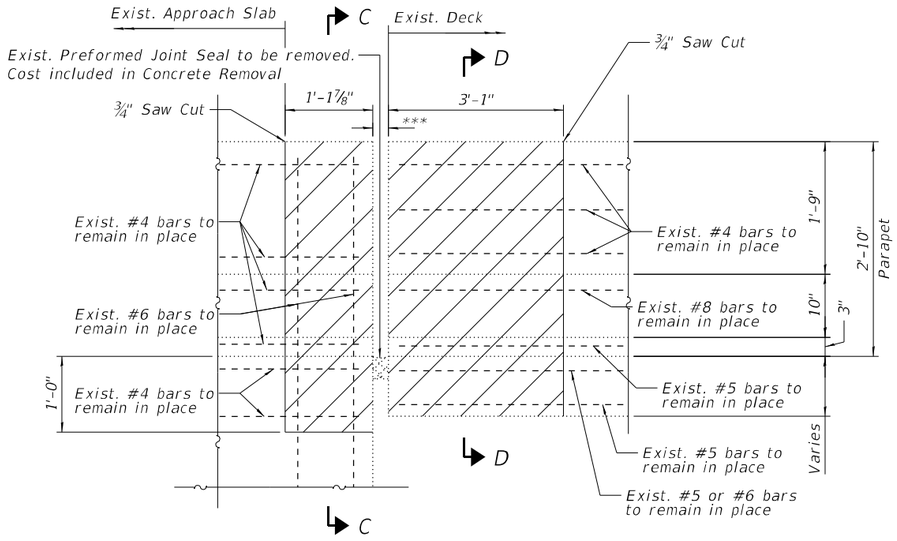
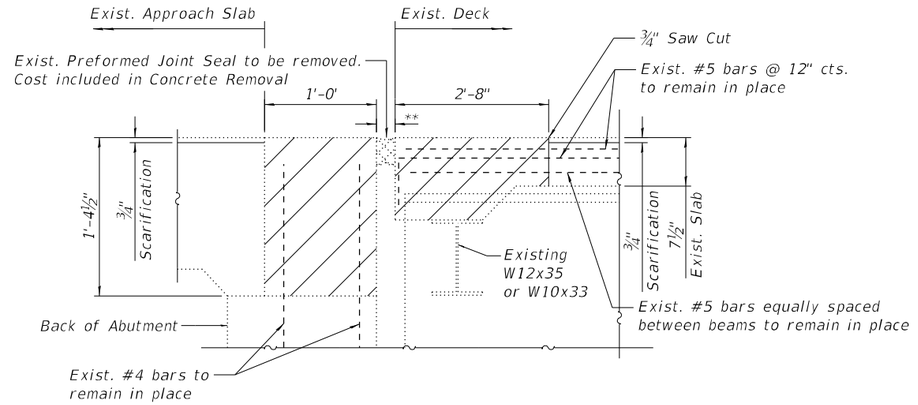
		CONTRACT NO. 62T31		
	ILLINOIS	FED. AID PROJECT		

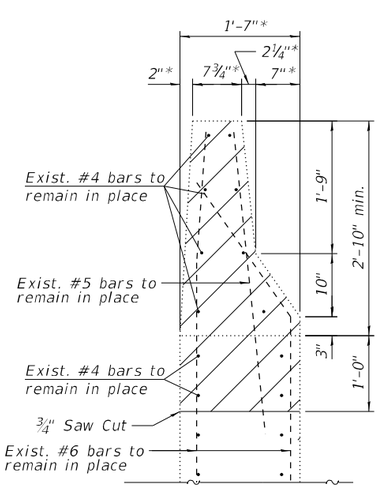


SECTION A-A

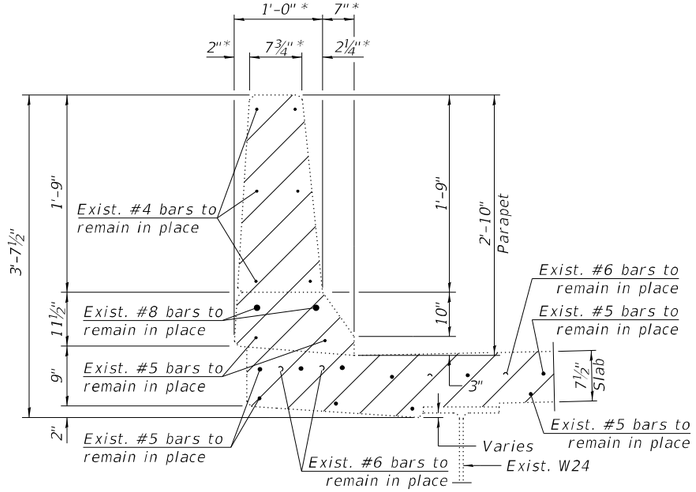


SECTION B-B

(Dimensions are at right angle)



SECTION C-C



SECTION D-D

NOTE

1. Hatch area indicates limits of concrete removal.
2. Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or archorage sysem. Cost included with Concrete Removal.

* Radial for both Southbound structure's parapets and Northbound structure's east parapet.

** 1¾" at 50° F at north abutments. 2¾" at 50° F at south abutments.

*** 2" at 50° F at north abutments. 3⅛" at 50° F at south abutments.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ABUTMENT AND DECK CONCRETE REMOVAL SECTIONS
S.N. 022-0078(NB) & S.N. 022-0079(SB)

SHEET 6 OF 17 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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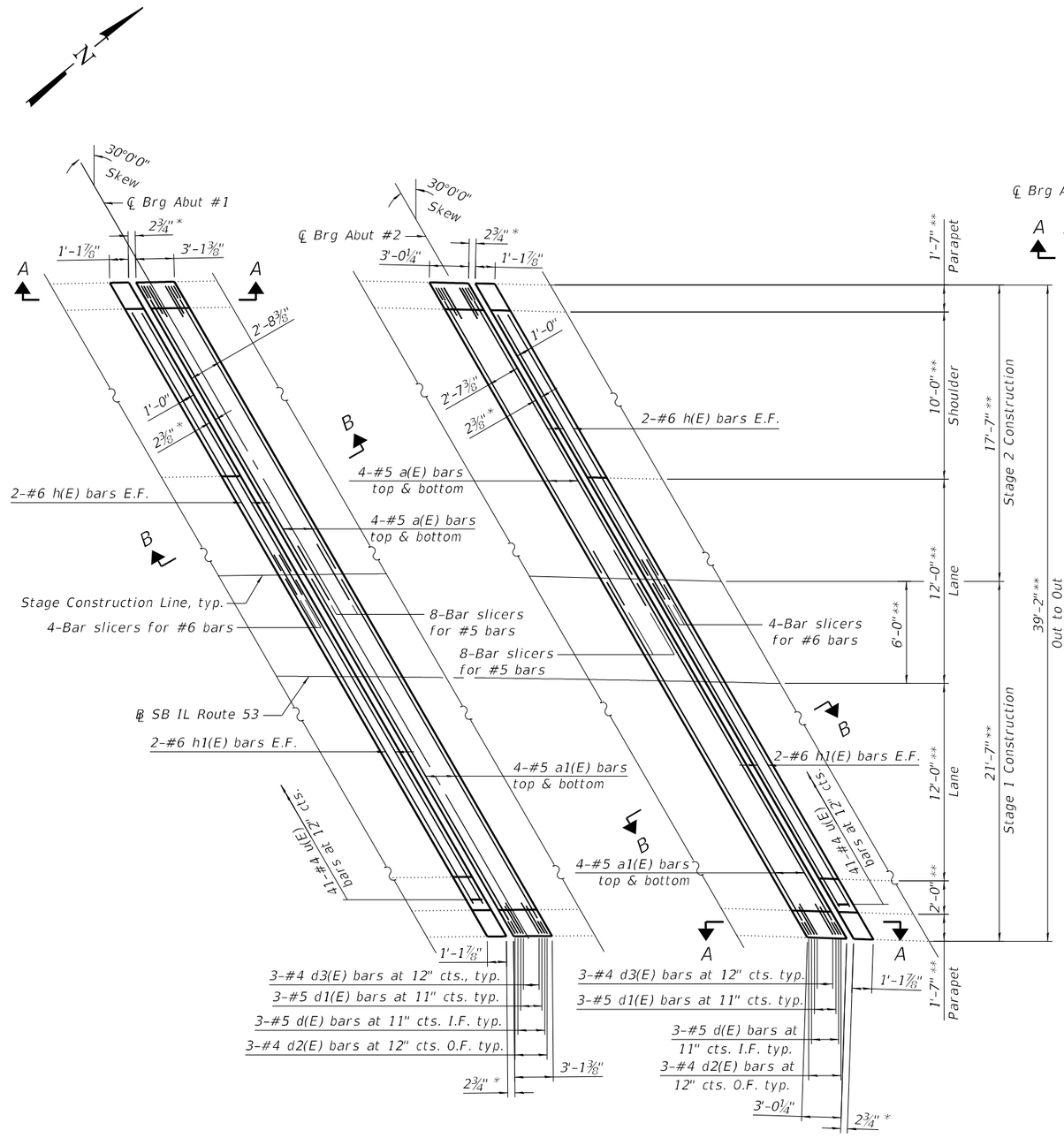
		CONTRACT NO. 62T31		
	ILLINOIS	FED. AID PROJECT		



SOUTHBOUND DECK

NORTHBOUND DECK

NOTE

1. See Sheet 8, for Sections A-A and B-B

* At 50° F

** Radial

PLAN

MODEL: Default
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ABUTMENT AND DECK ALTERATIONS
S.N. 022-0078(NB) & S.N. 022-0079(SB)
SHEET 7 OF 17 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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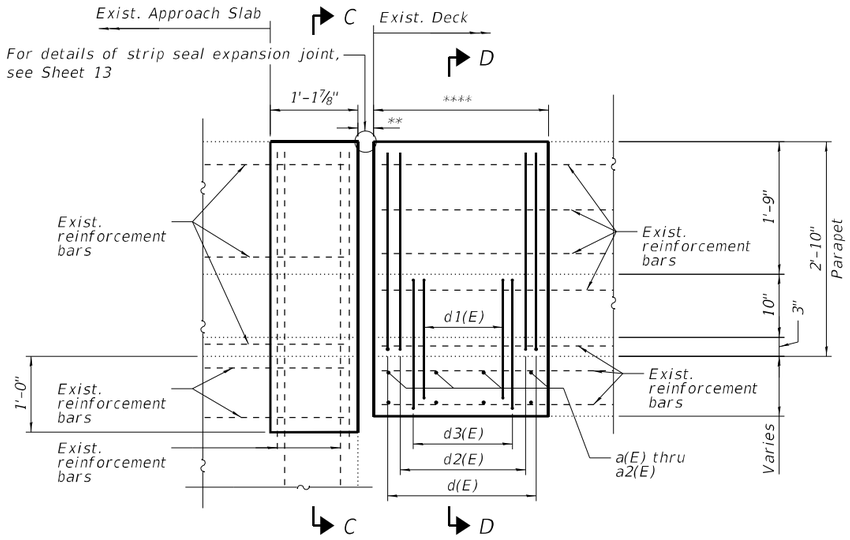
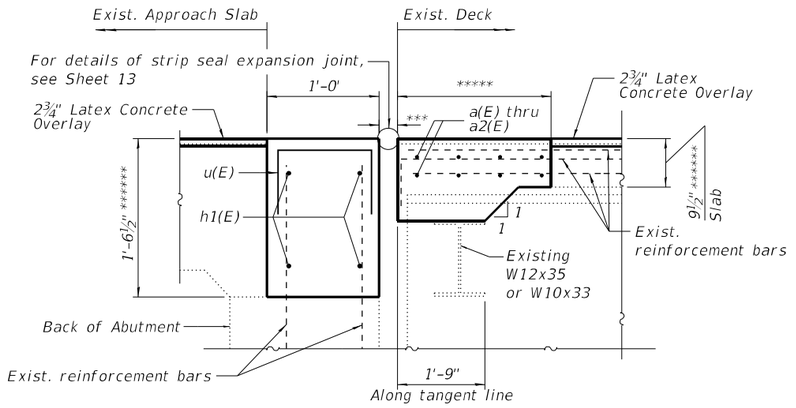


SECTION A-A

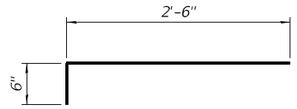


SECTION B-B

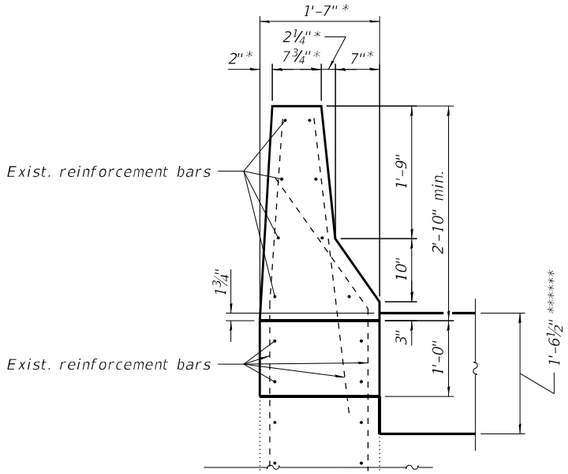
(Dimensions are at right angle)

BILL OF MATERIAL

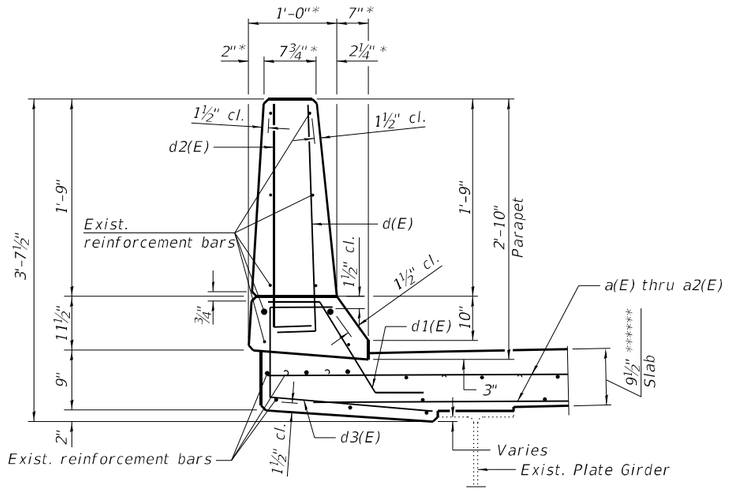
Bar	No.	Size	Length	Shape
a(E)	32	#5	19'-10"	
a1(E)	16	#5	24'-5"	
a2(E)	16	#5	25'-9"	
d(E)	12	#5	3'-0"	
d1(E)	12	#5	2'-7"	
d2(E)	12	#4	3'-0"	
d3(E)	12	#4	2'-11"	
h(E)	16	#6	18'-2"	
h1(E)	8	#6	22'-9"	
h2(E)	8	#6	24'-1"	
u(E)	170	#4	2'-0"	
Concrete Superstructure			Cu Yd	26.1
Reinforcement Bars, Epoxy Coated			Pound	2,960



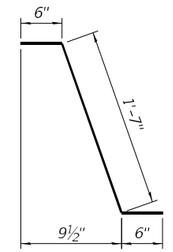
BARS d(E) & d2(E)



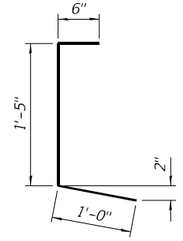
SECTION C-C



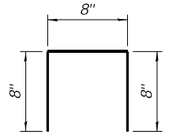
SECTION D-D



BAR d1E)



BAR d3(E)



BAR u(E)

* Radial for both Southbound structure's parapets and Northbound structure's east parapet.

** 2¾" at 50° F

*** 2⅞" at 50° F

**** 3'-0¼" at north end of deck. 3'-1⅜" at south end of deck.

***** 2'-7⅜" at north end of deck. 2'-8⅜" at south end of deck.

****** Prior to grinding

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ABUTMENT AND DECK DETAILS
S.N. 022-0078(NB) & S.N. 022-0079(SB)
SHEET 8 OF 17 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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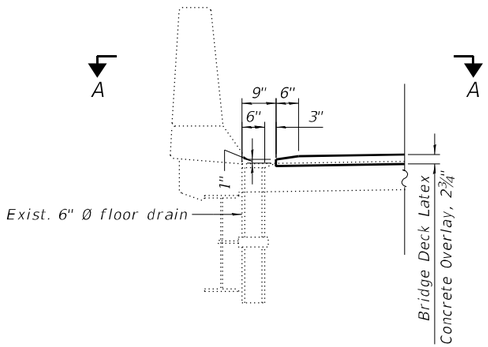
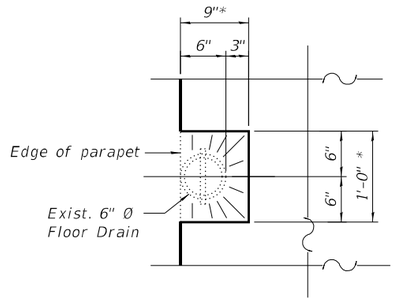


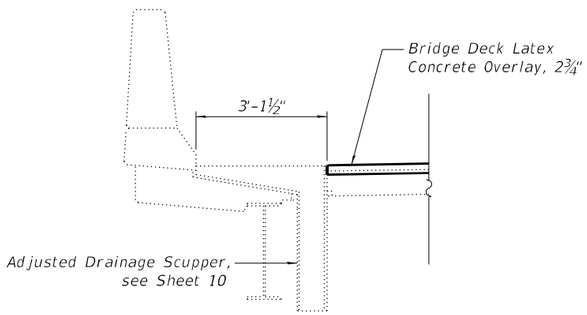
SECTION THRU FLOOR DRAINS

(6 Locations)



SECTION A-A

** At existing drains slope to drain*



SECTION THROUGH DRAINAGE SCUPPER

(6 Locations)

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Protective Coat	Sq Yd	815
Bridge Deck Grooving (Longitudinal)	Sq Yd	527
Bridge Deck Latex Concrete Overlay, 2¾"	Sq Yd	815
Diamond Grinding (Bridge Section)	Sq Yd	727

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DECK DETAILS AT DRAINAGE STRUCTURES
S.N. 022-0078(NB) & S.N. 022-0079(SB)

SHEET 9 OF 17 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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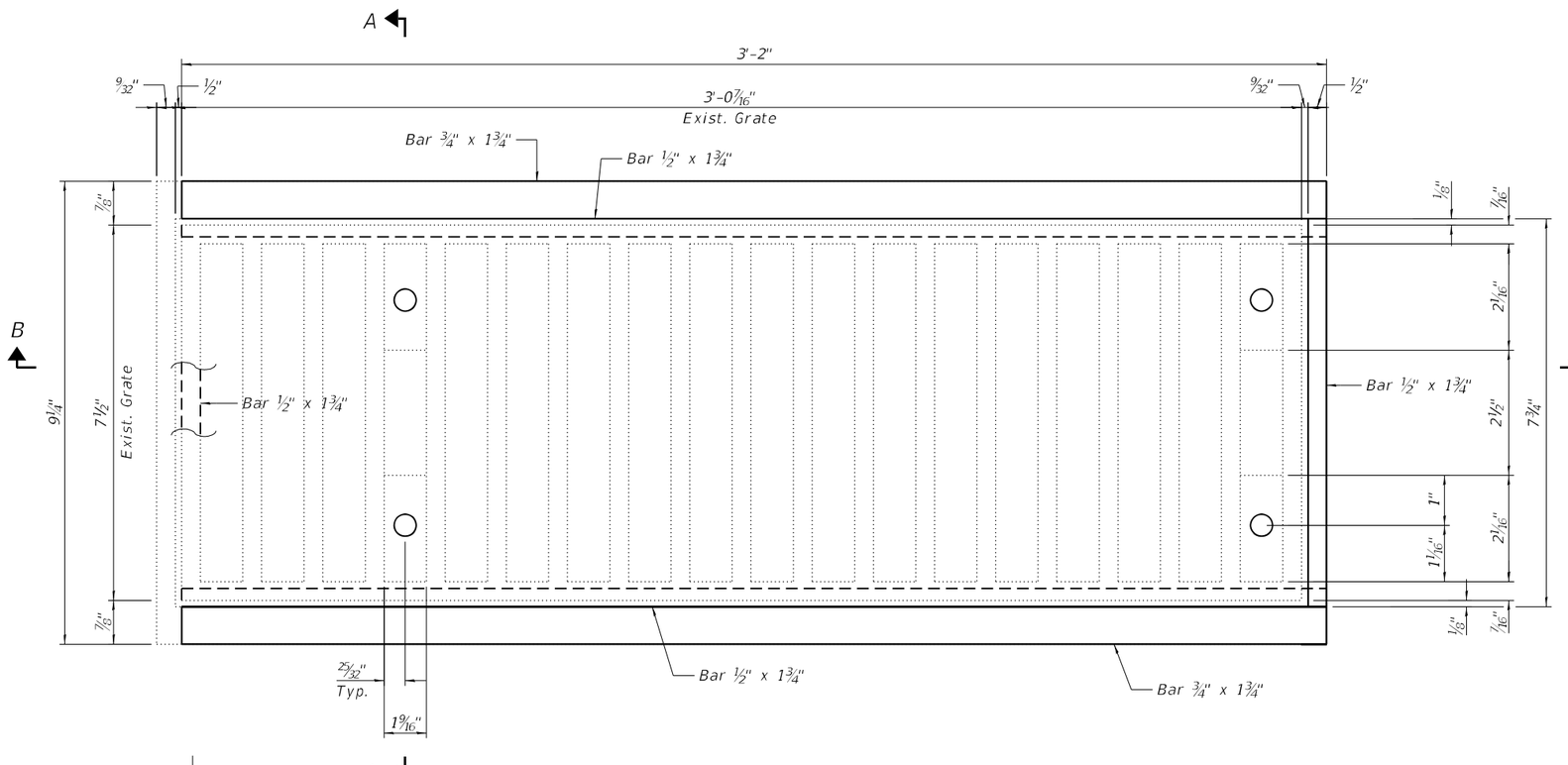
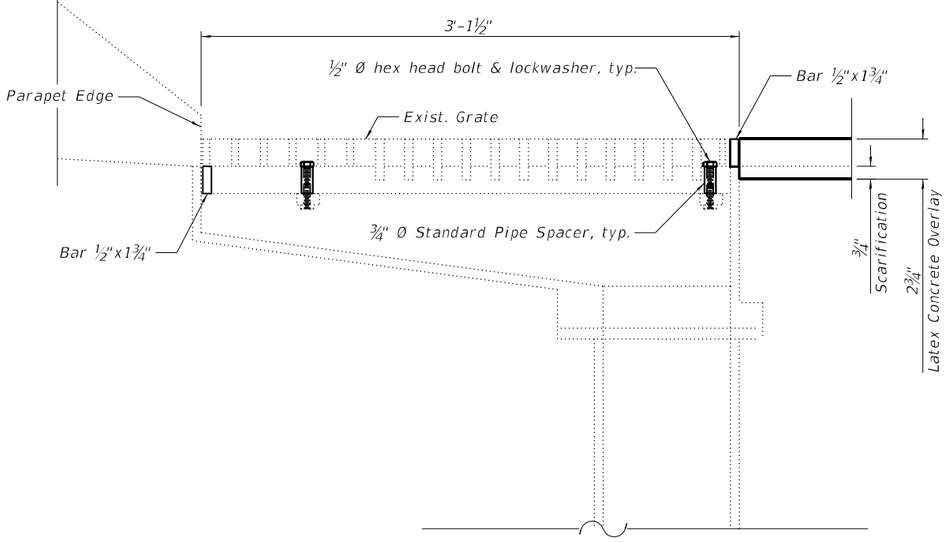
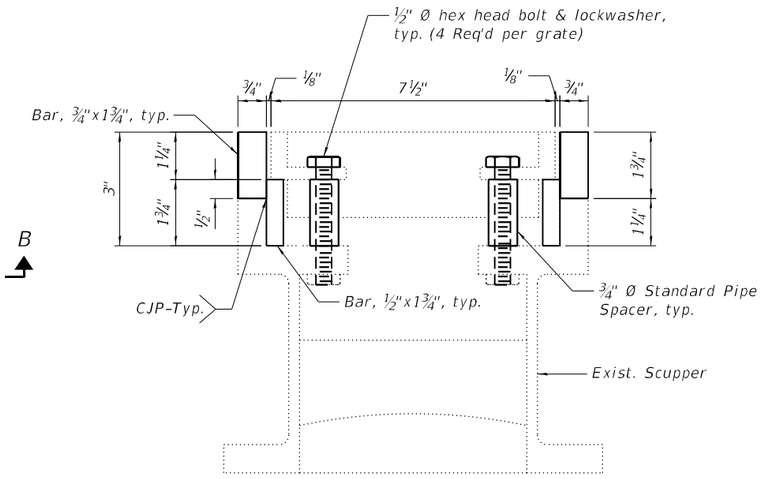
		CONTRACT NO. 62T31		
	ILLINOIS	FED. AID PROJECT		



TYPICAL CAST IRON DRAINAGE SCUPPER PLAN



SECTION B-B



SECTION A-A

NOTES

1. The Contractor shall field verify existing dimensions and details of the existing scuppers and make necessary adjustments prior to the construction and ordering of material for adjusting drainage scupper.
2. All new bars to be structural steel and conform to AASHTO M-270, Grade 36.
3. All pipes to be galvanized and conform to ASTM A53 Grade B steel.
4. The adjusting bars shall be galvanized according to AASHTO M111 and ASTM A385.
5. Bolts, washers, and nuts shall conform to the requirements of ASTM A 307 and shall be galvanized according to AASHTO M232.
6. The Contractor shall ensure that no damage is done to existing grates to be reused. If grate is damaged, Contractor to replace the grate at his own cost.
7. All labor and material necessary to remove the existing grate, clean the existing scupper, furnish and install adjusting bars, and reinstall the existing grate are included in the cost of Drainage Scuppers to be Adjusted.
8. All new steel bars are to be welded together to form a rectangular frame before galvanizing.

CJP = Complete Joint Penetration

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Drainage Scupper to be Adjusted	Each	6

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRAINAGE SCUPPER MODIFICATIONS
S.N. 022-0078(NB) & S.N. 022-0079(SB)

SHEET 10 OF 17 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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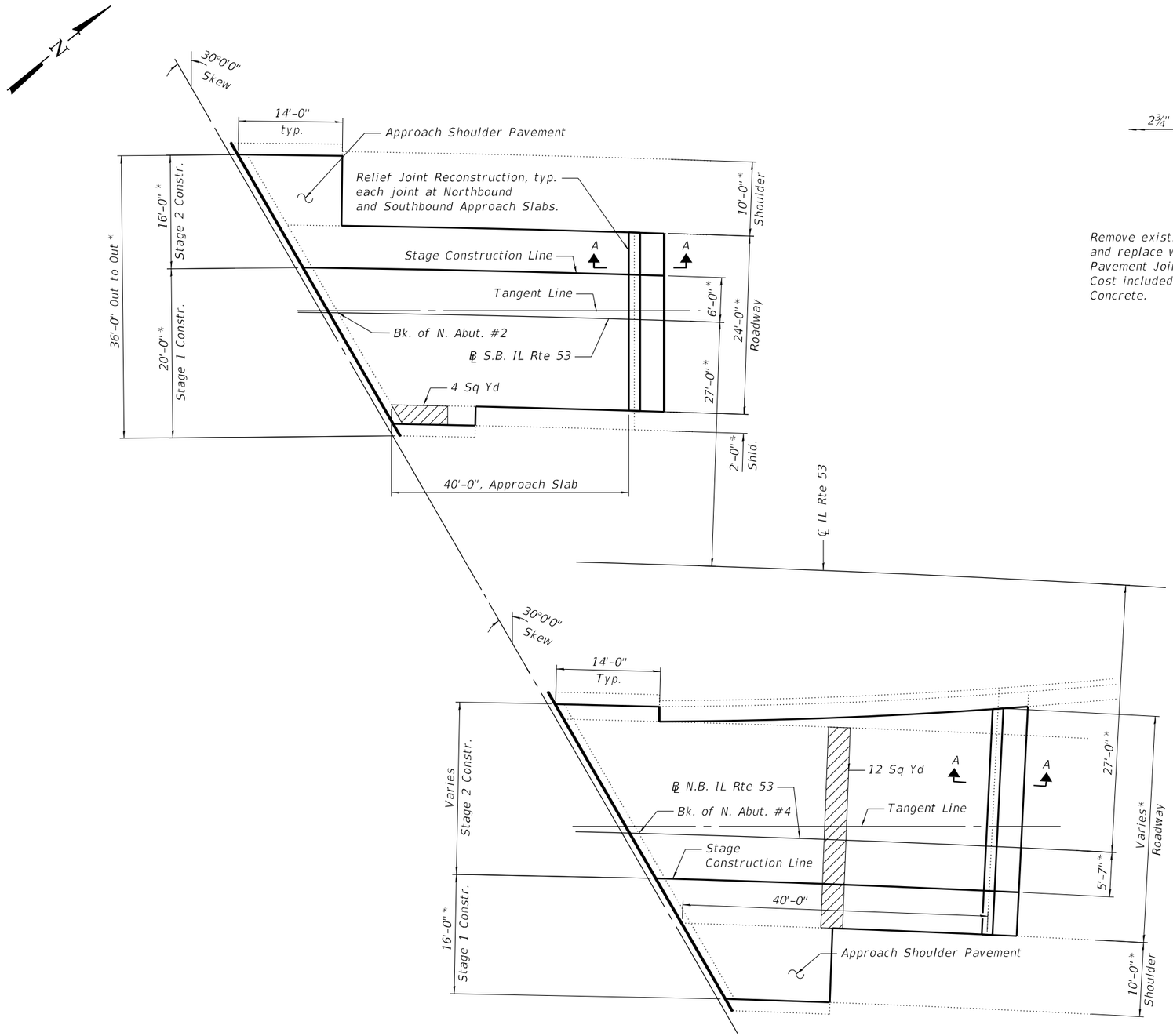
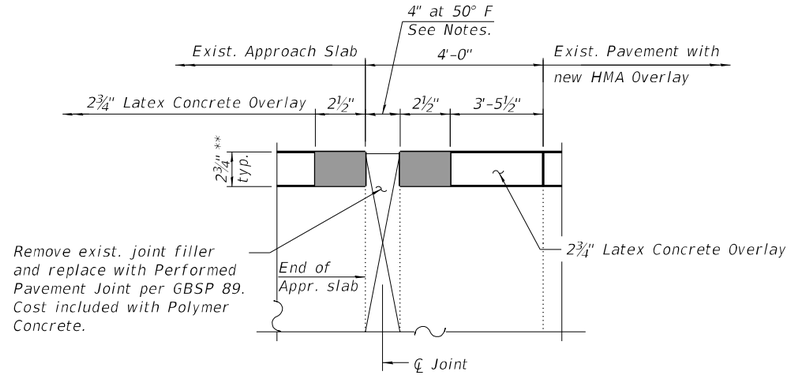
		CONTRACT NO. 62T31		
	ILLINOIS FED. AID PROJECT			



NORTH APPROACH SLABS

(Similar South Approach Slabs)



SECTION A-A

NOTES

1. Quantity and limits of Approach Slab Repairs shown herein are estimated from a survey completed by the District. Actual limits shall be determined in the field by the Engineer and noted in the As-Built Plans.
2. The joint opening shall be adjusted for temperature per Article 520.04 of the Standard Specifications.

* Radial

** Prior to grinding

LEGEND

Approach Slab Repair (Full Depth),
4 Sq Yd (Southbound)
12 Sq Yd (Northbound)

Polymer Concrete

BILL OF MATERIAL **

ITEM	UNIT	TOTAL
Protective Coat	Sq Yd	624
Bridge Deck Grooving (Longitudinal)	Sq Yd	553
Approach Slab Repair (Full Depth)	Sq Yd	16
Bridge Deck Latex Concrete Overlay, 2¾"	Sq Yd	615
Bridge Deck Scarification, ¾"	Sq Yd	620
Diamond Grinding (Bridge Section)	Sq Yd	604
Polymer Concrete	Cu Ft	9.7

** Includes 4 approach slabs

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APPROACH SLAB REPAIR PLAN
S.N. 022-0078(NB) & S.N. 022-0079(SB)
SHEET 11 OF 17 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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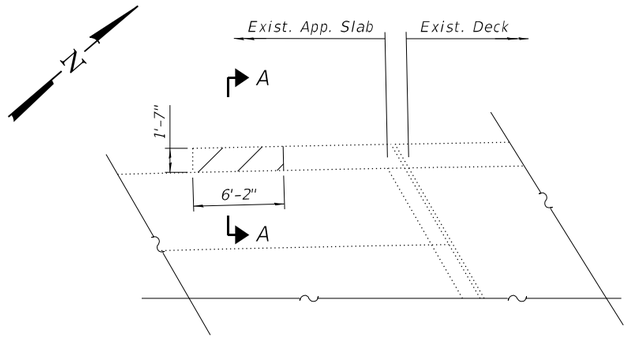
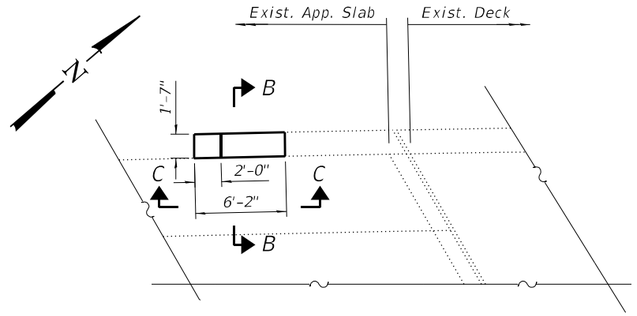
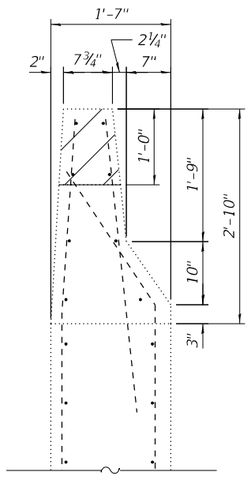


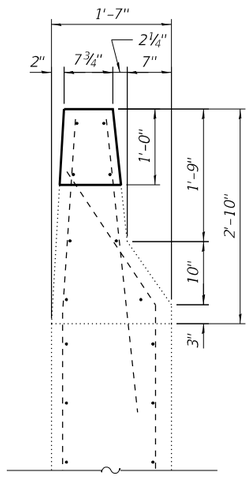
WEST PARAPET REMOVAL PLAN



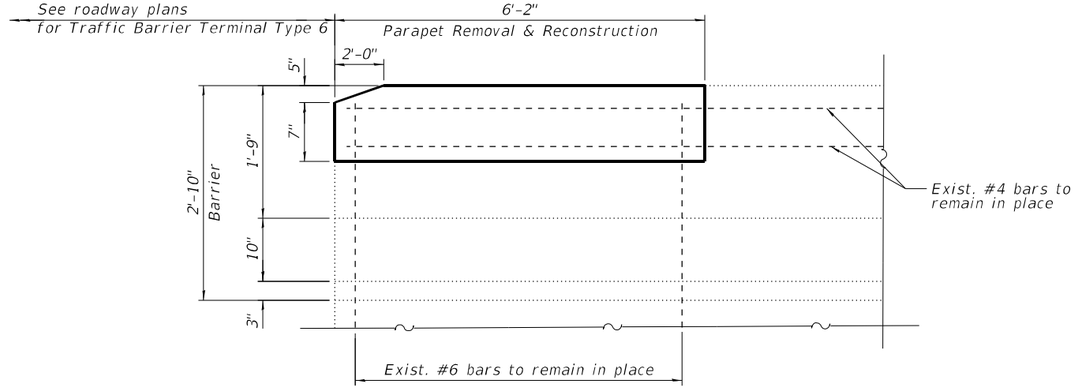
WEST PARAPET RECONSTRUCTION PLAN



SECTION A-A



SECTION B-B



SECTION C-C

LEGEND

Concrete Removal

NOTE

1. Parapet repair shall be paid for as Concrete Superstructure.
2. Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or archorage system. Cost included with Concrete Removal.

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Concrete Removal	Cu Yd	0.2
Concrete Superstructure	Cu Yd	0.2

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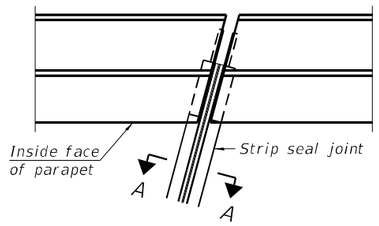
PLOT DATE = 12/12/2024	CHECKED - SPK	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

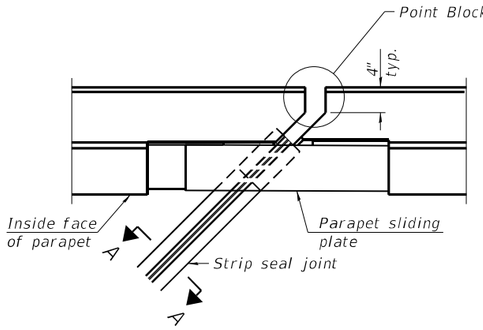
SOUTH ABUTMENT (#3) WEST WINGWALL
S.N. 022-0078(NB) & S.N. 022-0079(SB)

SHEET 12 OF 17 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ2	DUPAGE	44	26
		CONTRACT NO. 62T31		
	ILLINOIS FED. AID PROJECT			

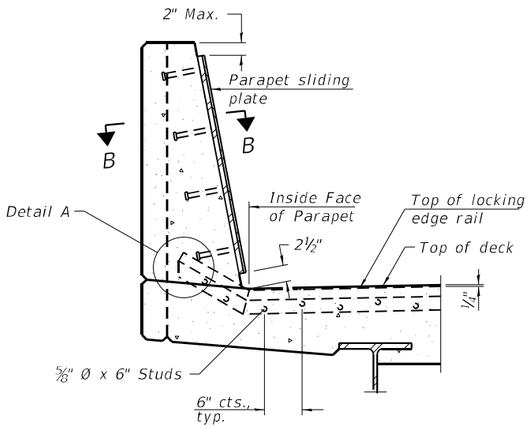


FOR SKEWS ≤ 30°



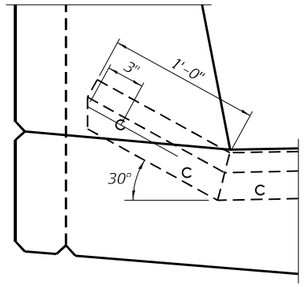
FOR SKEWS > 30°

PLAN AT PARAPET

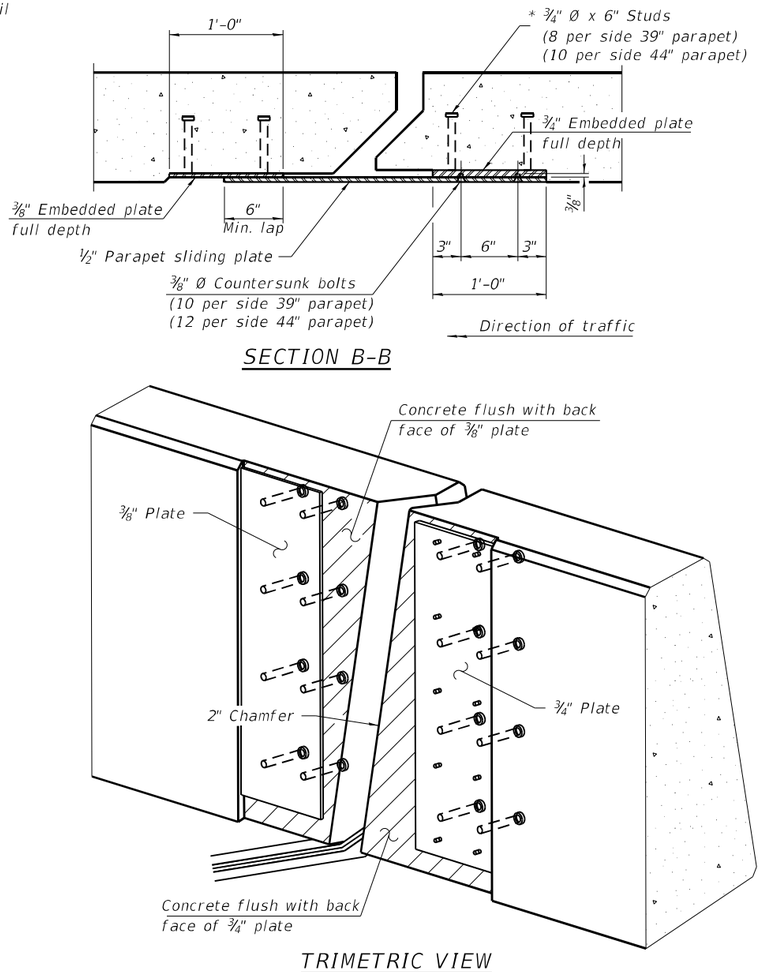


SECTION AT PARAPET

(Skews > 30° shown. Skews ≤ 30° similar except as shown in plan view.)



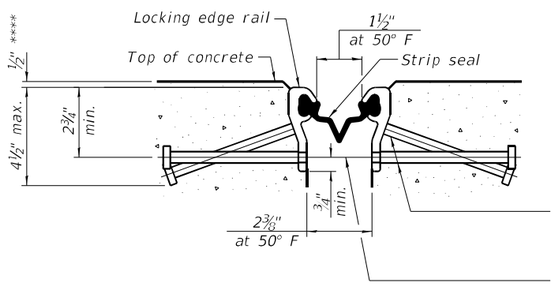
DETAIL A



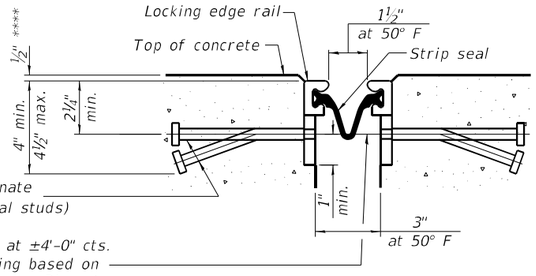
TRIMETRIC VIEW

(Showing embedded plates only)

SECTION B-B



SHOWING ROLLED RAIL JOINT



SHOWING WELDED RAIL JOINT

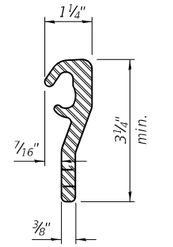
* ⅝" Ø x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)

⅜" ϕ threaded rods in 7/16" ϕ holes at ±4'-0" cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

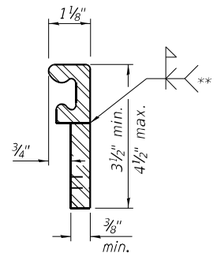
SECTION A-A

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

**** Prior to grinding



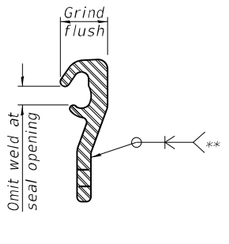
ROLLED (EXTRUDED) RAIL



WELDED RAIL

LOCKING EDGE RAILS

** Back gouge not required if complete joint penetration is verified by mock-up.



LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

Notes:

The strip seal shall be made continuous and shall have a minimum thickness of ¼". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the 4½" maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.

The manufacturer's recommended installation methods shall be followed.

All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

The Maximum space between locking edge rail segments shall be 3/16" and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.

Cost of parapet sliding plates, embedded plates, and anchorage studs included with Preformed Joint Strip Seal.

39" constant slope barrier shown, 44" constant slope barrier similar as noted.

The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required. One exception to this would be the strip seal joint at the end of the precast bridge approach slab. For these cases the pavement connector length shall be adjusted, not the length of the bridge approach slab.

BILL OF MATERIAL

	Item	Unit	Total
***	Preformed Joint Strip Seal	Foot	184

*** 91' for Southbound and 93' for Northbound.

MODEL: Default
FILE NAME: P:\P-20\20-4094-00 IDOT Various Phase 2 (PTB 196 Item 16)\WO 36 62T31 (Closed)\DGN\CADD Sheets\62T31-std-STR-10 78679 ExpJtDet.dgn

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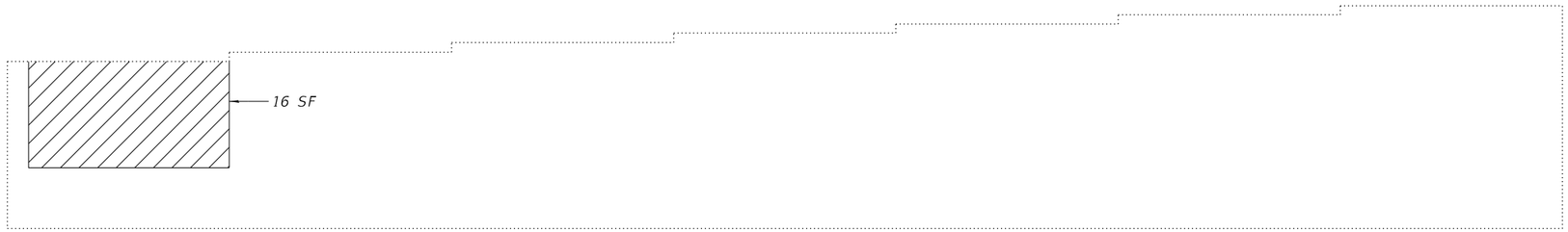
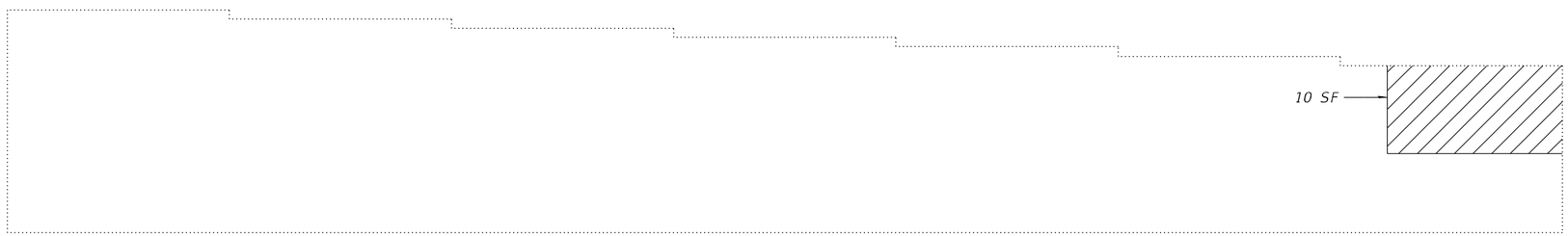
PREFORMED JOINT STRIP SEAL
S.N. 022-0078(NB) & S.N. 022-0079(SB)
SHEET 13 OF 17 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ2	DUPAGE	44	27
		CONTRACT NO. 62T31		
	ILLINOIS	FED. AID PROJECT		



ELEVATION – SOUTH ABUTMENT (#1)

(Looking South)



ELEVATION – NORTH ABUTMENT (#2)

(Looking North)

NOTE

1. Quantities and limits of Structural Repair of Concrete shown herein are estimated from a survey completed by the District. Actual limits shall be determined in the field by the Engineer and noted in the As-Built Plans.

LEGEND

Structural Repair Of Concrete
(Depth Equal To Or Less Than 5 Inches)

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Structural Repair Of Concrete (Depth Equal To Or Less Than 5 Inches)	Sq Ft	26

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOUTH ABUTMENT (#1) & NORTH ABUTMENT (#2) REPAIRS
S.N. 022–0078(NB) & S.N. 022–0079(SB)

SHEET 14 OF 17 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ2	DUPAGE	44	28
		CONTRACT NO. 62T31		
	ILLINOIS	FED. AID PROJECT		

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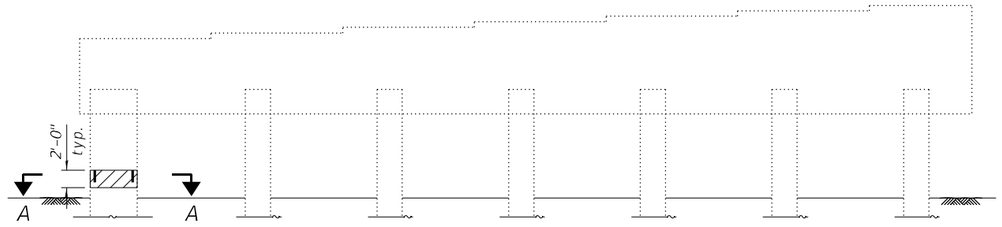
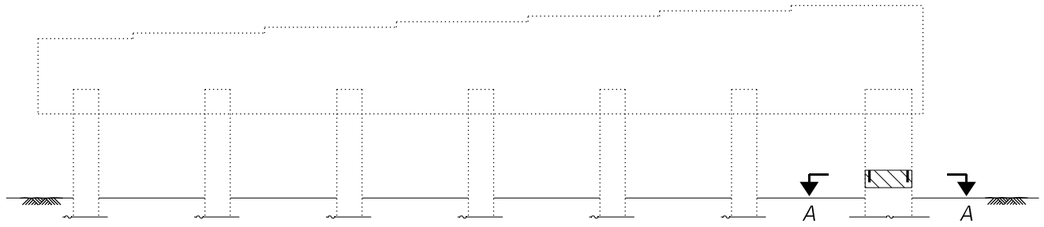


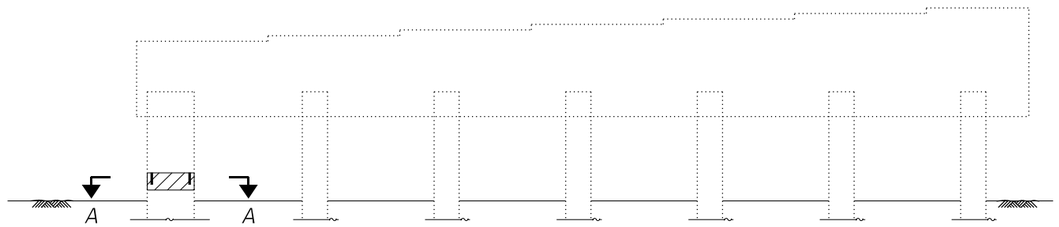
PIER #3



PIER #1

NORTH ELEVATION

Looking South



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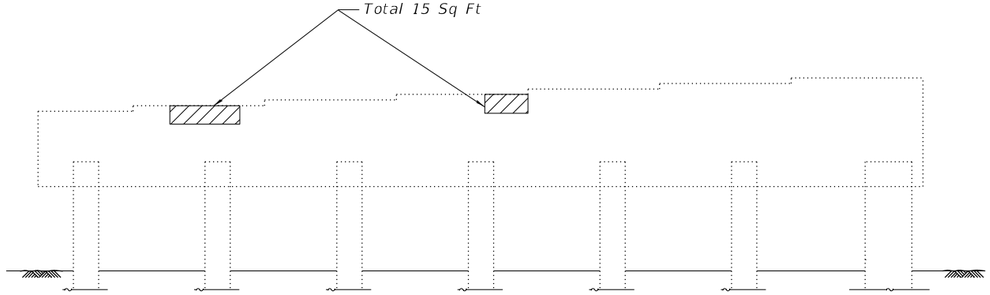
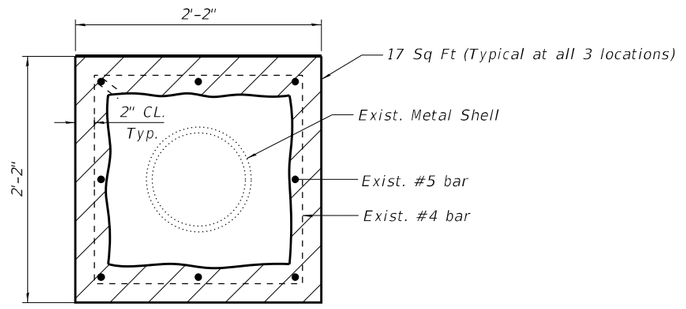


PIER #2

NORTH ELEVATION

Looking South



SECTION A-A

NOTE

1. Quantities and limits of Structural Repair of Concrete shown herein are estimated from a survey completed by the District. Actual limits shall be determined in the field by the Engineer and noted in the As-Built Plans.

LEGEND

Structural Repair Of Concrete
(Depth Equal To Or Less Than 5 Inches)

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Structural Repair Of Concrete (Depth Equal To Or Less Than 5 Inches)	Sq Ft	66

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIERS REPAIRS
S.N. 022-0078(NB) & S.N. 022-0079(SB)
SHEET 15 OF 17 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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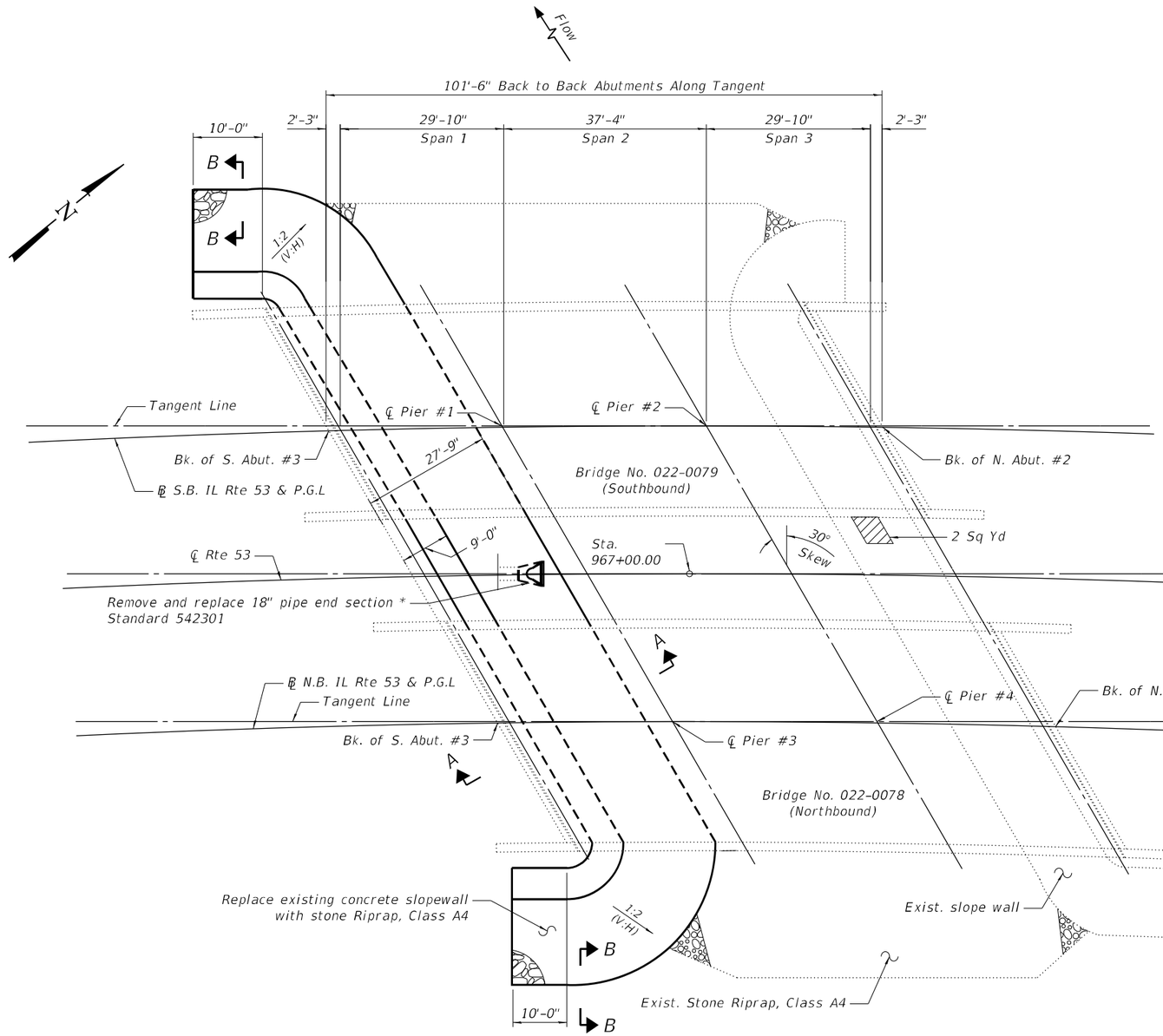
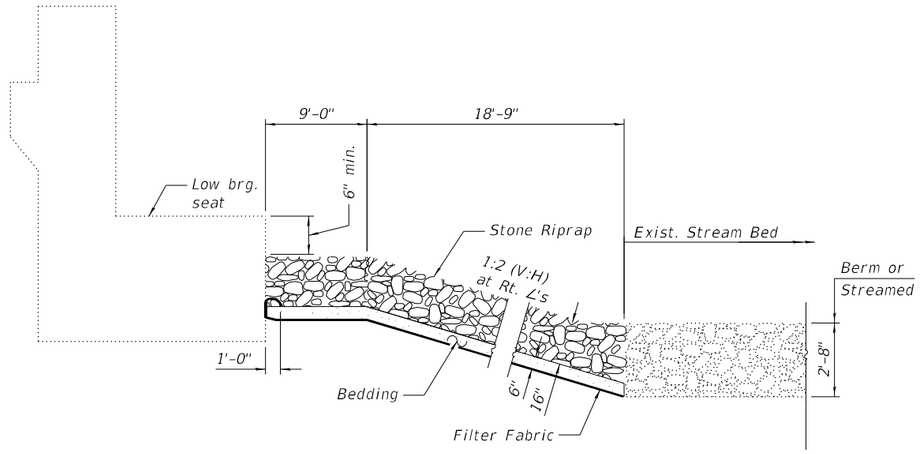
		CONTRACT NO. 62T31		
	ILLINOIS	FED. AID PROJECT		

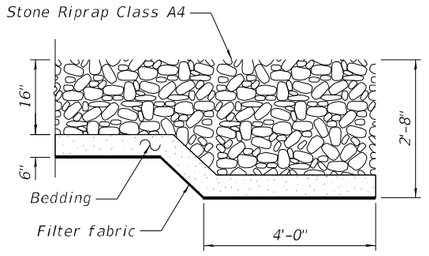


PLAN

* Contractor to verify pipe size before ordering end section.



SECTION A-A



SECTION B-B

LEGEND

Slope Wall Repair

NOTE

1. Cost of removing and disposing of existing flared end section included in the cost of Slope Wall Removal.

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Stone Riprap, Class A4	Sq Yd	577
Filter Fabric	Sq Yd	580
Slope Wall Removal	Sq Yd	577
Precast Reinforced Concrete Flared End Sections 18"	Each	1
Slope Wall Repair	Sq Yd	2

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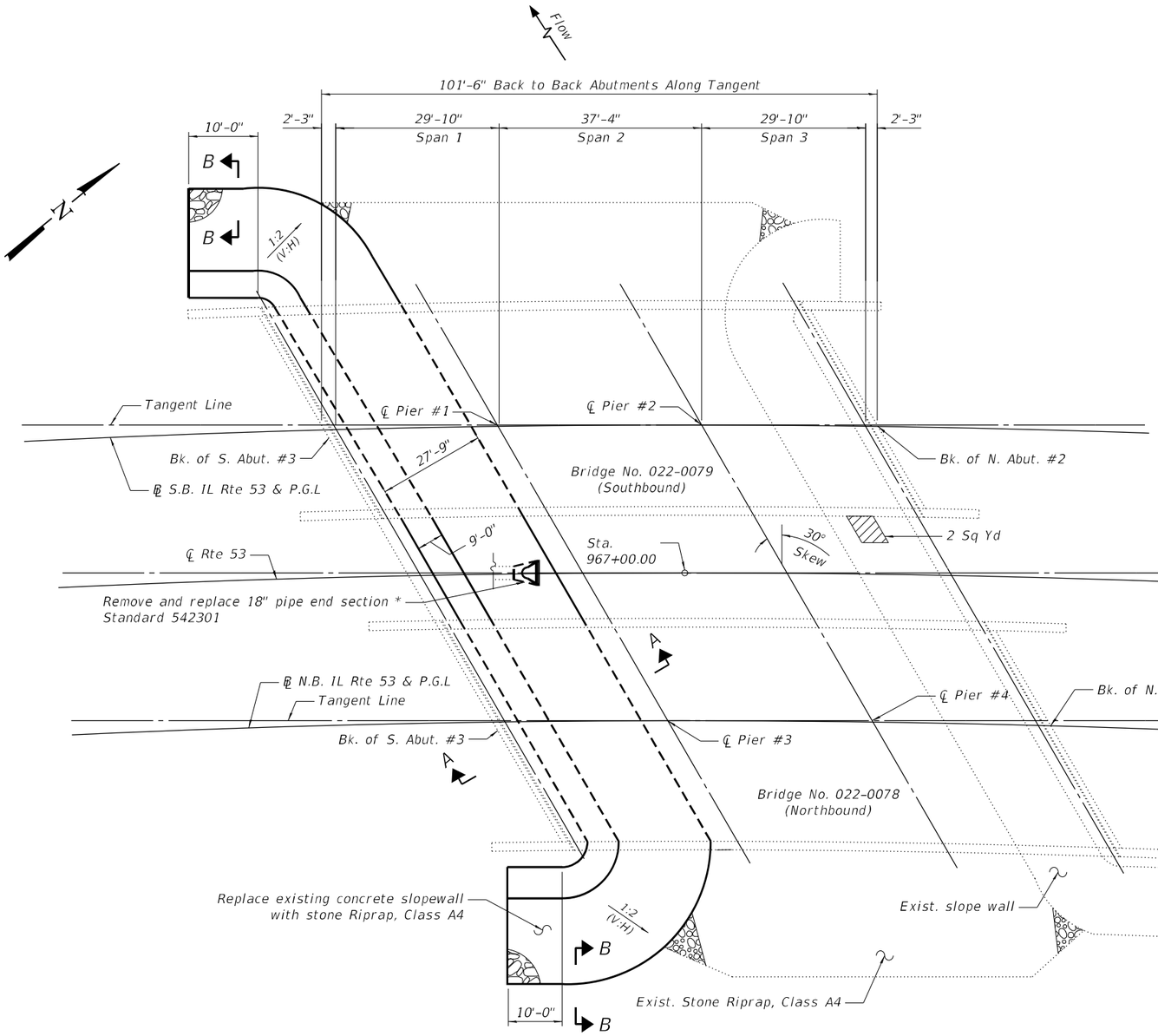
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

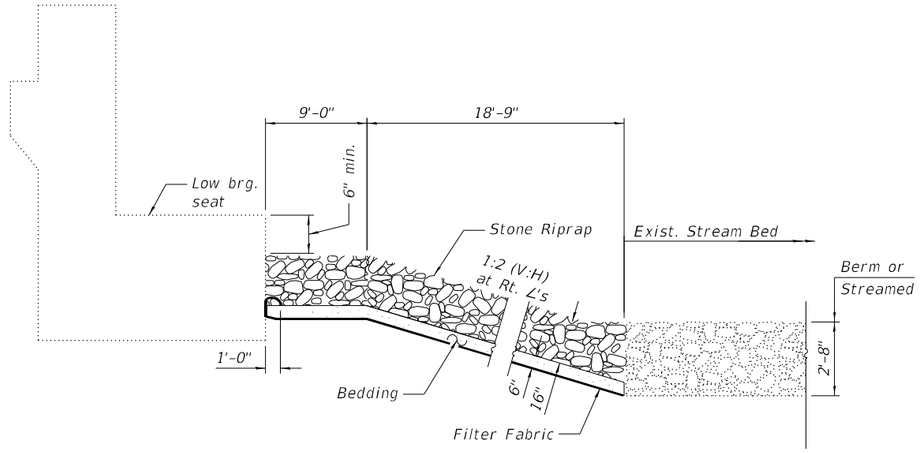
SLOPE WALLS REPAIR & REPLACEMENT
S.N. 022-0078(NB) & S.N. 022-0079(SB)
SHEET 16 OF 17 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ2	DUPAGE	44	30
		CONTRACT NO. 62T31		
	ILLINOIS FED. AID PROJECT			



PLAN

* Contractor to verify pipe size before ordering end section.



SECTION A-A

SECTION B-B

NOTE

1. Cost of removing and disposing of existing flared end section included in the cost of Slope Wall Removal.

LEGEND

Slope Wall Repair

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Stone Riprap, Class A4	Sq Yd	577
Filter Fabric	Sq Yd	580
Slope Wall Removal	Sq Yd	577
Precast Reinforced Concrete Flared End Sections 18"	Each	1
Slope Wall Repair	Sq Yd	2

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SLOPE WALLS REPAIR & REPLACEMENT
S.N. 022-0078(NB) & S.N. 022-0079(SB)

SHEET 16 OF 17 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ2	DUPAGE	44	31
		CONTRACT NO. 62T31		
	ILLINOIS	FED. AID PROJECT		

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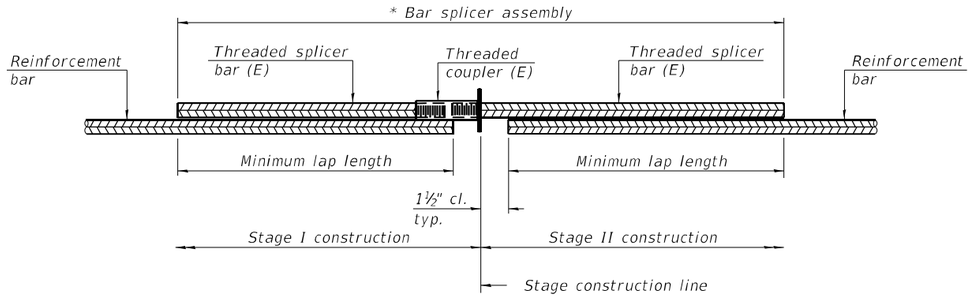


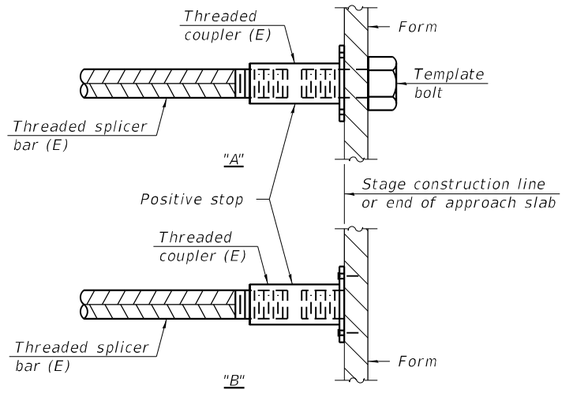
STANDARD BAR SPLICER ASSEMBLY PLAN

(All components shall be provided from one supplier)

Threaded splicer bar length = min. lap length + 1½" + thread length

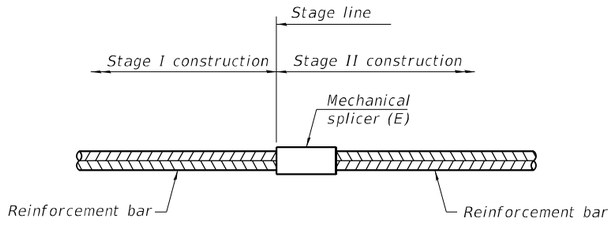
** Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.*

Location	Bar size	No. assemblies required	Minimum lap length
SB Abutments	#6	8	4'-0"
SB Deck	#5	16	3'-6"
NB Abutments	#6	8	4'-0"
NB Deck	#5	16	3'-6"



INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
(E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required

Notes:
Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
All reinforcement shall be lapped and tied to the splicer bars.
Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1 1-1-2020

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PLOT DATE = 1/8/2025	CHECKED - SPK	REVISED -	

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DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
S.N. 022-0078(NB) & S.N. 022-0079(SB)

SHEET 17 OF 17 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ2	DUPAGE	44	32
		CONTRACT NO. 62T31		
	ILLINOIS FED. AID PROJECT			

SCOPE OF WORK

1. Remove existing wearing surface and scarify deck $\frac{1}{4}$".
2. Place polymer concrete on both sides of the expansion joints.
3. Remove bridge deck expansion joints at both abutments and install new preformed pavement joints.
4. Apply $2\frac{3}{4}$" bridge deck latex concrete overlay on the bridge decks.
5. Perform diamond grinding and bridge deck grooving on the bridge decks.
6. Apply protective coat to the concrete overlay areas.
7. Perform structural repair of concrete on the abutments.

TOTAL BILL OF MATERIAL

	ITEM	UNIT	SUPER	SUB	TOTAL
	Protective Shield	Sq Yd	407		407
*	Protective Coat	Sq Yd	449		449
	Bridge Deck Grooving (Longitudinal)	Sq Yd	203		203
	Bridge Deck Latex Concrete Overlay, 2 3/4 Inches	Sq Yd	441		441
	Bridge Deck Scarification 2 1/4"	Sq Yd	449		449
	Structural Repair of Concrete (Depth Greater Than 5 Inches)	Sq Ft		35	35
	Diamond Grinding (Bridge Section)	Sq Yd	417		417
	Polymer Concrete	Cu Ft	14.9		14.9

* Protective Coat to be applied to the top of the deck

GENERAL NOTE

1. Plan dimensions and details relative to the existing structure have been taken from existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work; however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

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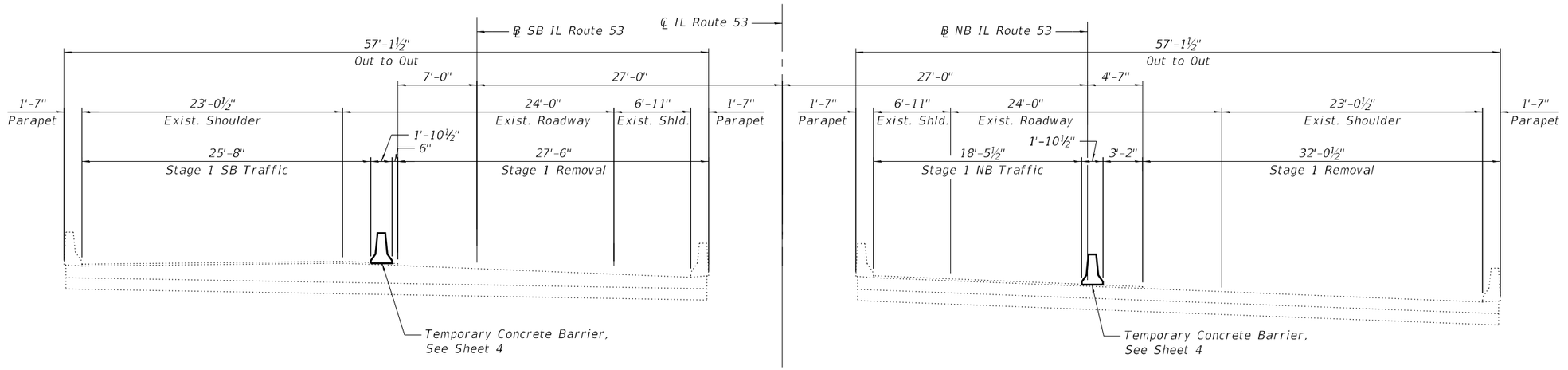
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES, SCOPE OF WORK, & BILL OF MATERIAL
S.N. 022–0080(NB) & S.N. 022–0081(SB)

SHEET 2 OF 6 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ2	DUPAGE	44	33
		CONTRACT NO. 62T31		
	ILLINOIS	FED. AID PROJECT		

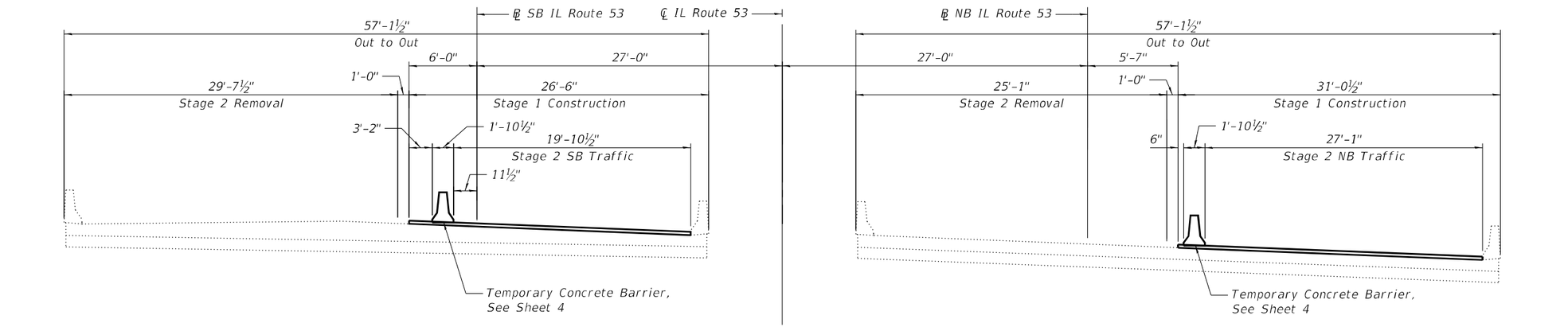


SOUTHBOUND

STAGE 1 REMOVAL

(Looking North)

NORTHBOUND

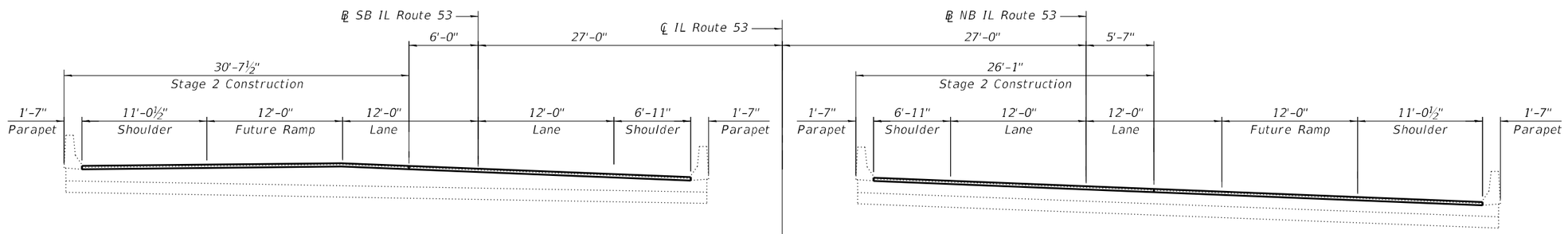


SOUTHBOUND

STAGE 1 CONSTRUCTION AND STAGE 2 REMOVAL

(Looking North)

NORTHBOUND



SOUTHBOUND

STAGE 2 CONSTRUCTION

(Looking North)

NORTHBOUND

MODEL: Default
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1 South Wacker | Suite 2650 | Chicago, IL 60606
P 312.419.6500 | F 312.419.5594 | www.infrastruct-eng.com

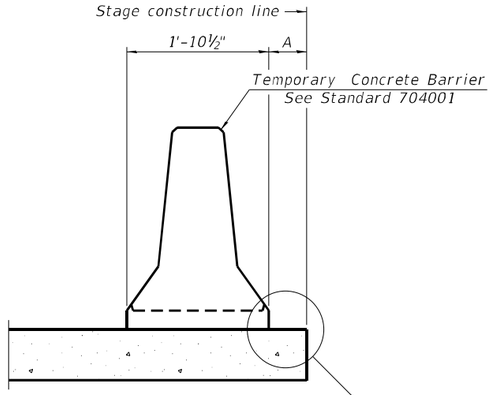
USER NAME = ALane	DESIGNED - PK	REVISED -	
	CHECKED - SPK	REVISED -	
PLOT SCALE = 10.6667 ' / in.	DRAWN - PK	REVISED -	
PLOT DATE = 12/12/2024	CHECKED - SPK	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGE CONSTRUCTION DETAILS
S.N. 022–0080(NB) & S.N. 022–0081(SB)

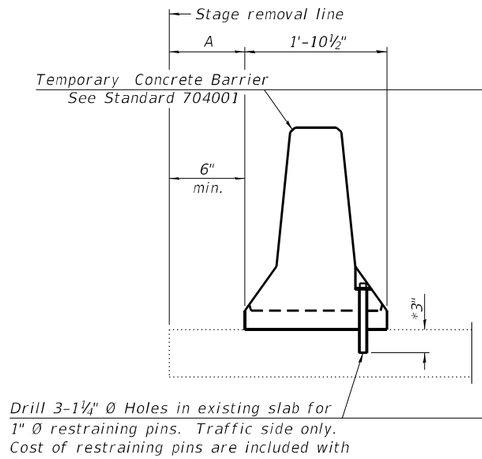
SHEET 3 OF 6 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ2	DUPAGE	44	34
		CONTRACT NO. 62T31		
	ILLINOIS FED. AID PROJECT			



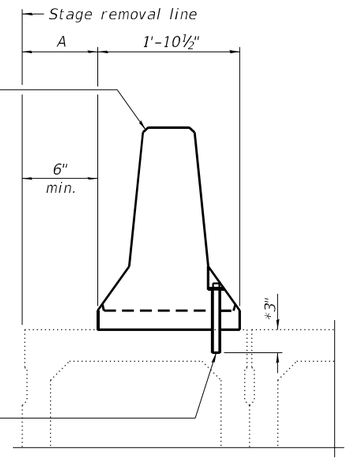
When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM



Drill 3-1¼" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

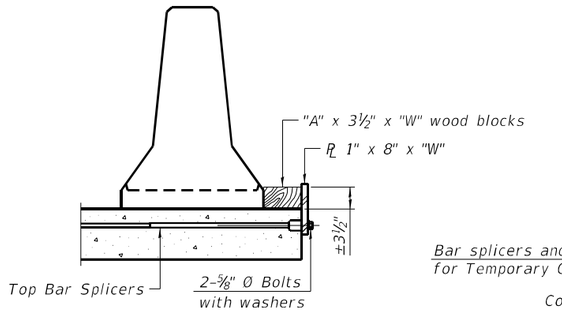
EXISTING SLAB



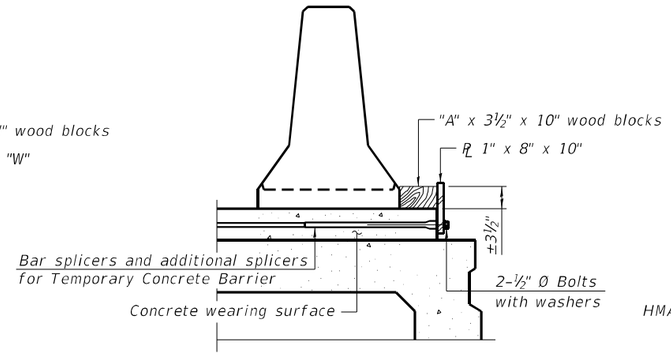
* When hot-mix asphalt wearing surface is present, embedment shall be 3" plus the wearing surface depth.

EXISTING DECK BEAM

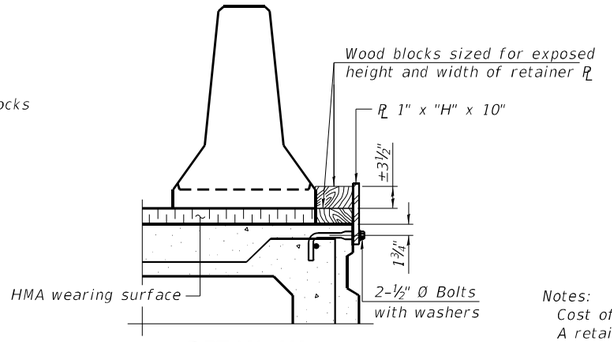
SECTIONS THRU SLAB OR DECK BEAM



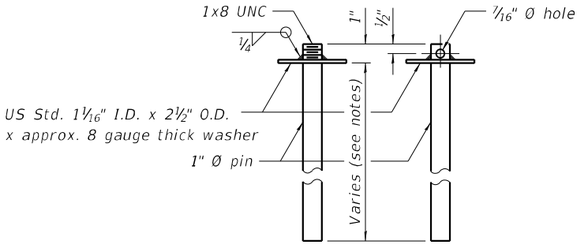
DETAIL I



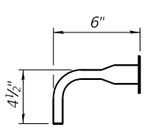
DETAIL II



DETAIL III



RESTRAINING PIN



BAR SPLICER FOR #4 BAR - DETAIL III

Notes:

Cost of retainer assembly is included with Temporary Concrete Barrier.

A retainer assembly shall be located at the approximate ℄ of each temporary concrete barrier.

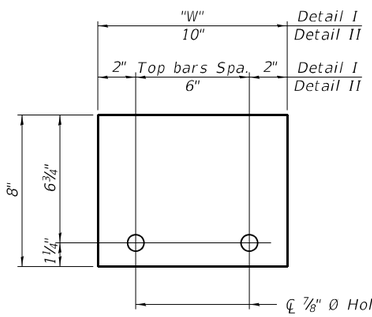
The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.

When the 'A' dimension is less than 1½", the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.

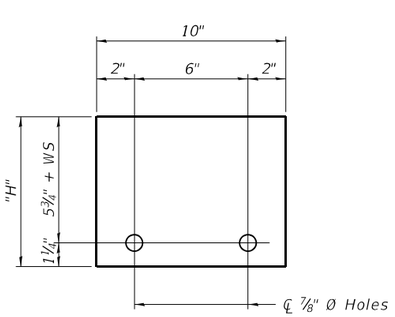
Detail I - Installation for a new bridge deck or bridge slab.

Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.

Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.



STEEL RETAINER PL 1" x 8" x "W"
(Detail I and II)



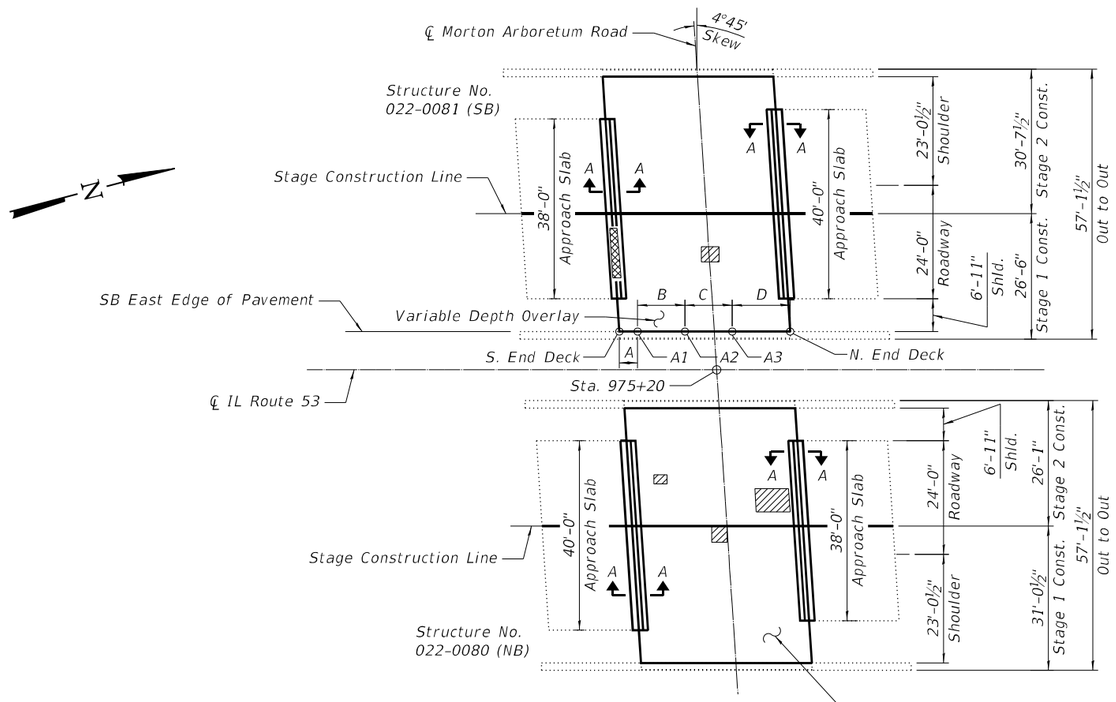
STEEL RETAINER PL 1" x "H" x 10"
(Detail III)

RAILING CRITERIA

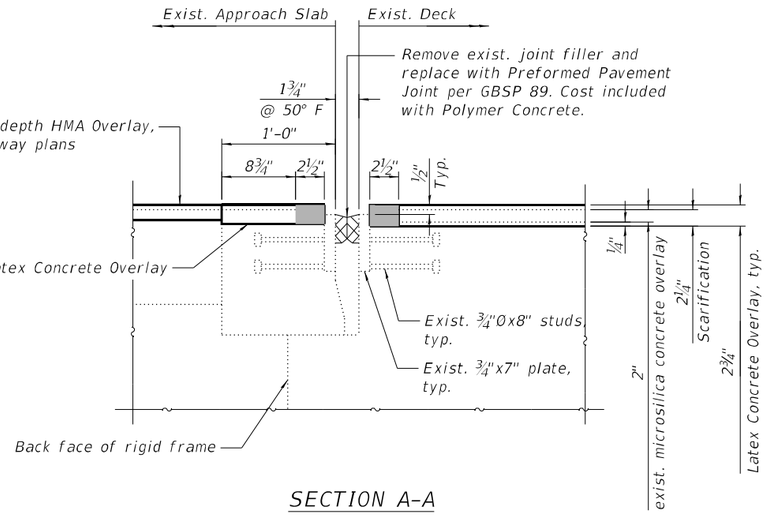
NCHRP 350 Test Level	3
Railing Weight (plf)	440

R-27

10-12-2021



DECK PLAN



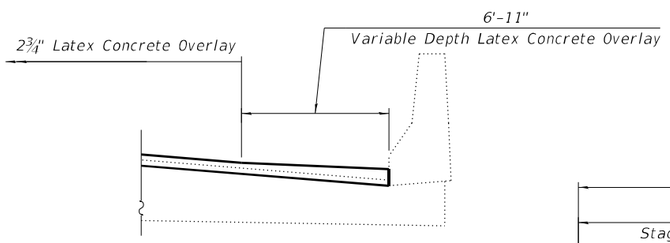
SECTION A-A

VARIABLE DIMENSION TABLE

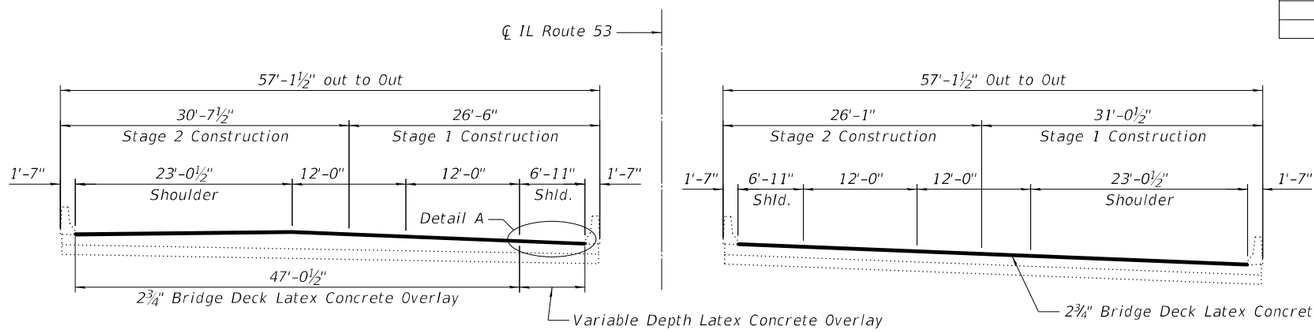
Location	A	B	C	D
SB East Edge of Pavement	5'-0"	10'-0"	10'-0"	11'-0"

SB EAST EDGE OF PAVEMENT

Location	Overlay Thickness
S. End Deck	3½"
A1	3⅝"
A2	3¾"
A3	3¼"
N. End Deck	2¼"



DETAIL A



SOUTHBOUND

NORTHBOUND

CROSS SECTION

(Looking North)

NOTES

1. Quantity and limits of Deck Slab and Approach Slab Repairs shown herein are estimated from a survey completed by the District. Actual limits shall be determined in the field by the Engineer and noted in the As-Built Plans.
2. Areas of Approach Slab Repair (Partial Depth) and Deck Slab Repair (Partial Depth) are Provide for information only and shall be included in the cost of Bridge Deck Latex Concrete Overlay, 2¾".
3. Cost of variable depth latex concrete overlay shall be included in the cost of Bridge Deck Latex Concrete Overlay, 2¾".

LEGEND

- Deck Slab Repair (Partial Depth) total 9 Sq Yd
- Approach Slab Repair (Partial Depth) total 2 Sq Yd
- Polymer Concrete

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Protective Coat	Sq Yd	449
Bridge Deck Grooving (Longitudinal)	Sq Yd	203
Bridge Deck Latex Concrete Overlay, 2 3/4 Inches	Sq Yd	441
Bridge Deck Scarification 2 1/4"	Sq Yd	449
Polymer Concrete	Cu Ft	14.9

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USER NAME = ALane	DESIGNED - PK	REVISED -	
	CHECKED - SPK	REVISED -	
PLOT SCALE = 32.0000 ' / in.	DRAWN - PK	REVISED -	
PLOT DATE = 12/12/2024	CHECKED - SPK	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DECK REPAIR PLANS
S.N. 022-0080(NB) & S.N. 022-0081(SB)
SHEET 5 OF 6 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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		CONTRACT NO. 62T31		
	ILLINOIS	FED. AID PROJECT		

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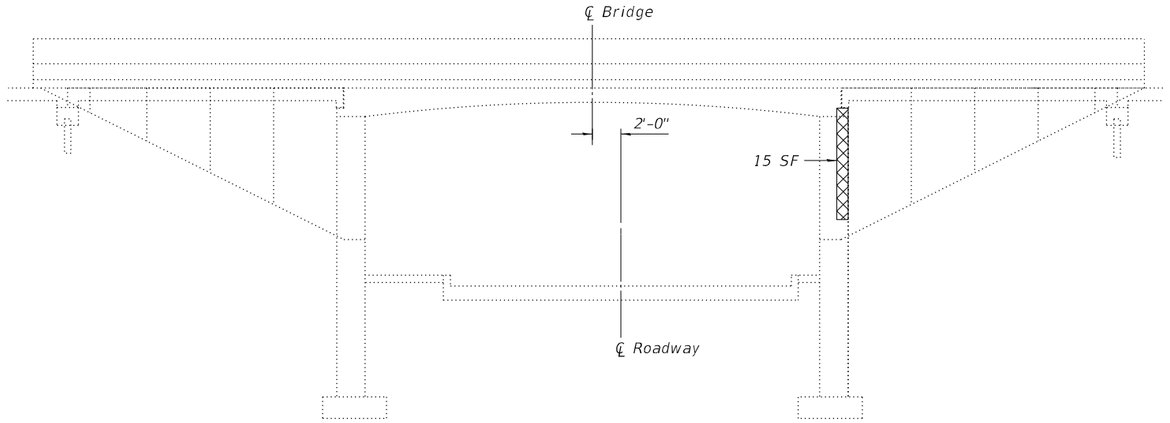
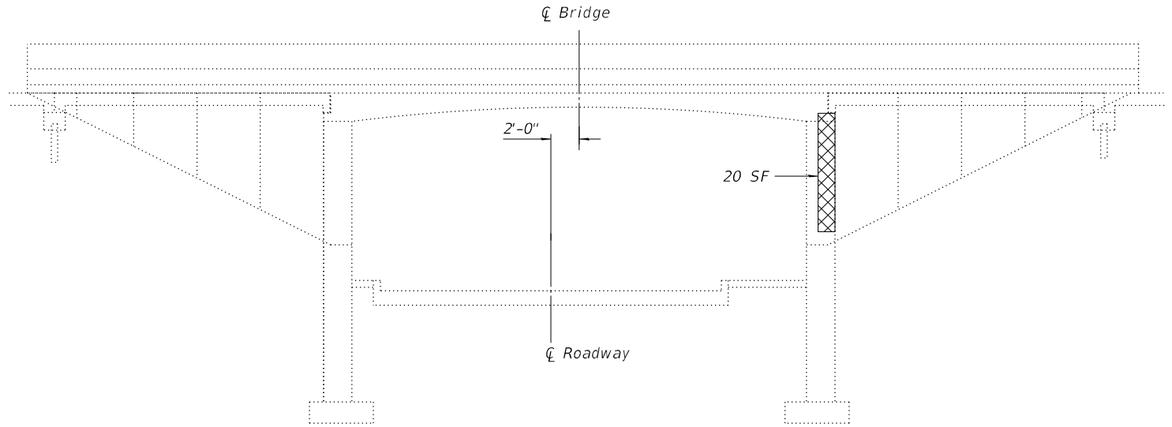


SOUTHBOUND - EAST ELEVATION

(Looking West)



SOUTHBOUND - WEST ELEVATION

(Looking East)

NOTE

1. *Quantities and limits of Structural Repair of Concrete shown herein are estimated from a survey completed by the District. Actual limits shall be determined in the field by the Engineer and noted in the As-Built Plans.*

LEGEND

	Structural Repair Of Concrete (Depth Greater Than 5 Inches)

BILL OF MATERIAL

ITEM	UNIT	TOTAL
Structural Repair Of Concrete (Depth Greater Than 5 Inches)	Sq Ft	35

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USER NAME = ALane	DESIGNED - PK	REVISED -	
	CHECKED - SPK	REVISED -	
PLOT SCALE = 32.0000 ' / in.	DRAWN - PK	REVISED -	
PLOT DATE = 12/12/2024	CHECKED - SPK	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

RIGID FRAME WALL REPAIRS
S.N. 022-0080(NB) & S.N. 022-0081(SB)
SHEET 6 OF 6 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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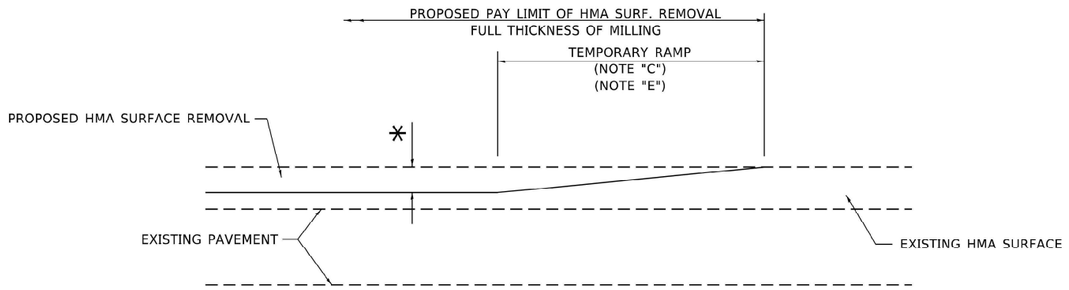
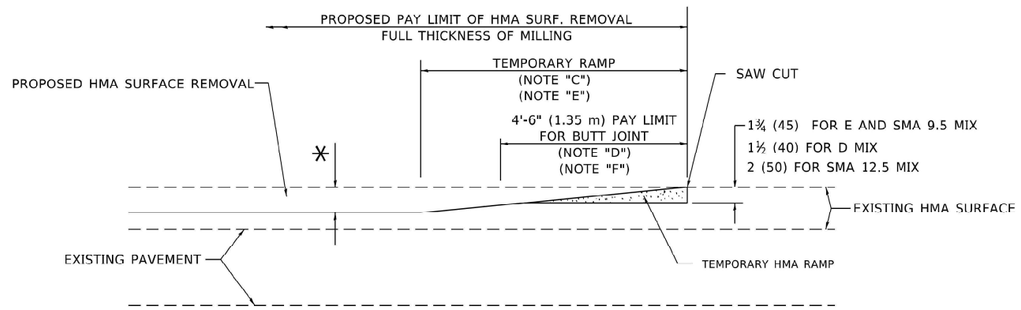
		CONTRACT NO. 62T31		
	ILLINOIS	FED. AID PROJECT		



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

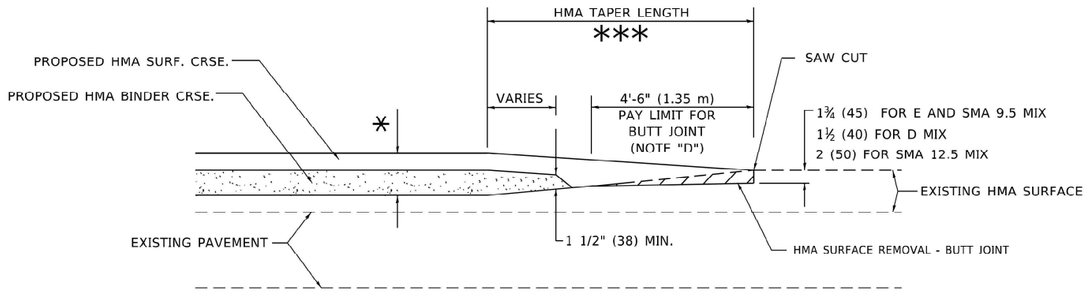


HMA CONSTRUCTED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

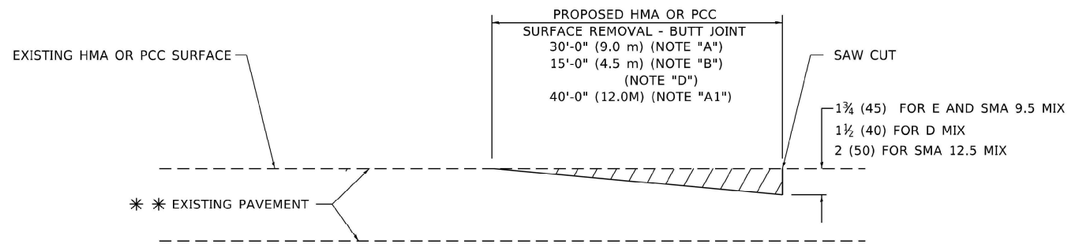
OPTION 2

TYPICAL TEMPORARY RAMP

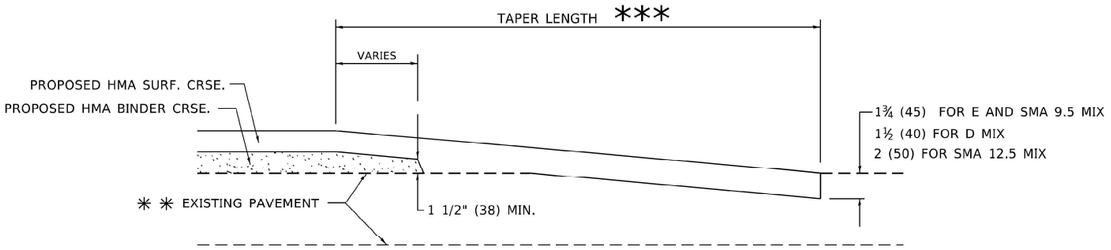


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

GENERAL NOTES

A. MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.

A1. INTERSTATES

B. MINOR SIDE ROADS.

C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.

D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.

E. TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.

* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.

F. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".

*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

1. THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".

2. THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: Default
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USER NAME = demanchelt	DESIGNED - M. DE YONG	REVISED - A. ABBAS 03-21-97	
	DRAWN -	REVISED - M. GOMEZ 04-06-01	
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - R. DORO 01-01-07	
PLOT DATE = 2/2/2022	DATE - 06-13-90	REVISED - K. SMITH 02-01-22	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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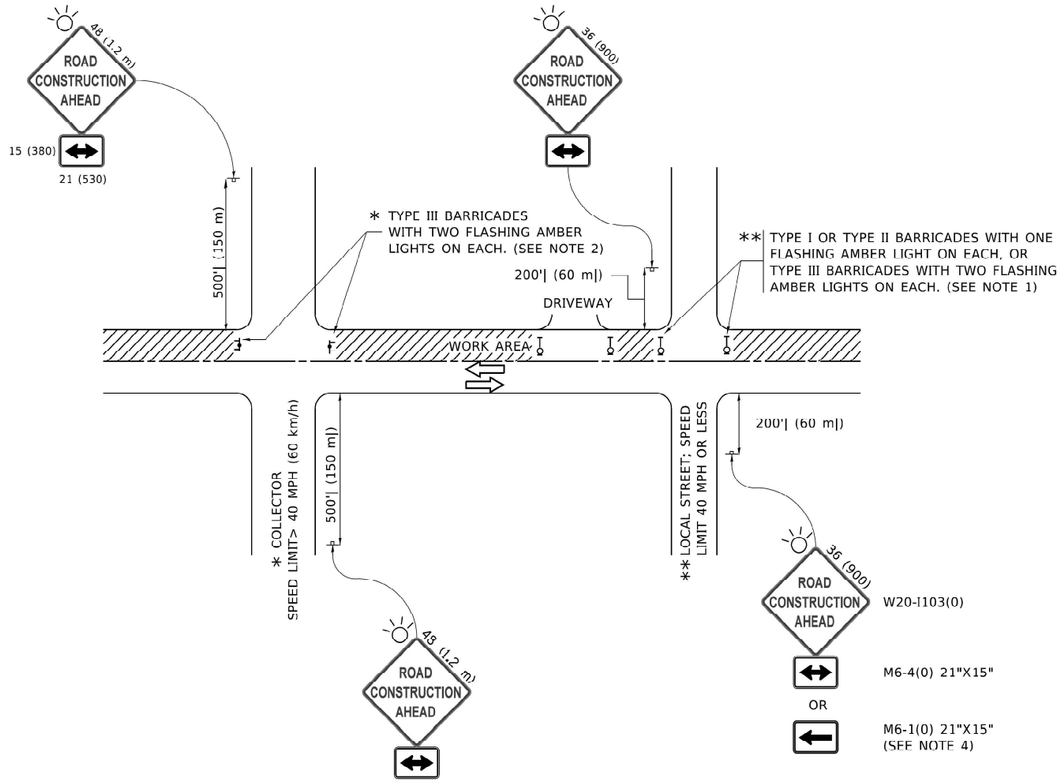
	BD400-05 BD-32	CONTRACT NO. 62T31		
	ILLINOIS FED. AID PROJECT			



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
   a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
   a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
   b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME = footemj	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
	DRAWN -	REVISED - T. RAMMACHER 01-06-00
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
PLOT DATE = 3/4/2019	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ2	DUPAGE	44	39
	TC-10	CONTRACT NO. 62T31		
	ILLINOIS FED. AID PROJECT			

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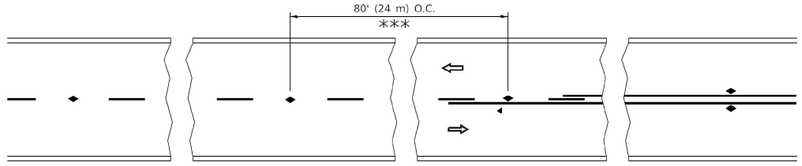
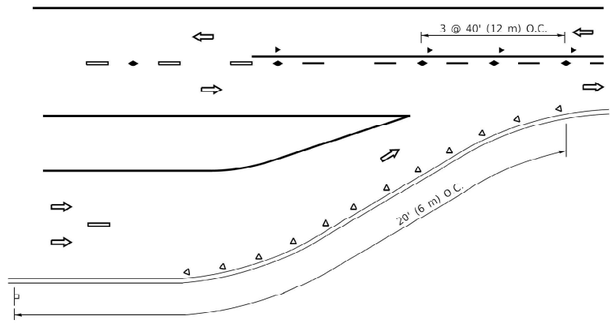


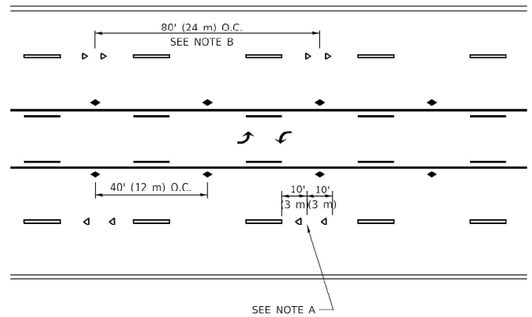
*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

TWO–LANE/TWO–WAY

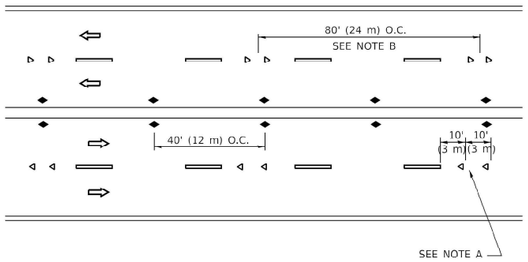
SEE FIGURE 3B-14 MUTCD



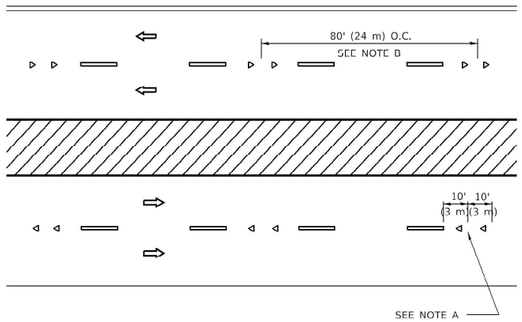
LANE REDUCTION TRANSITION



TWO–WAY LEFT TURN



MULTI–LANE/UNDIVIDED



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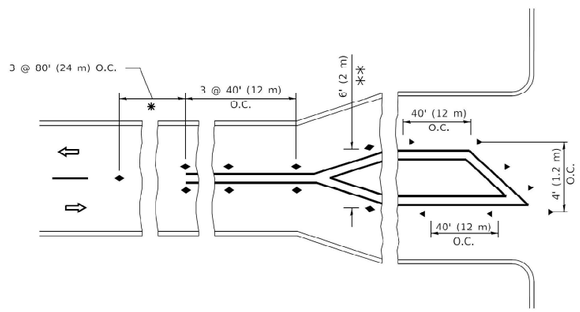

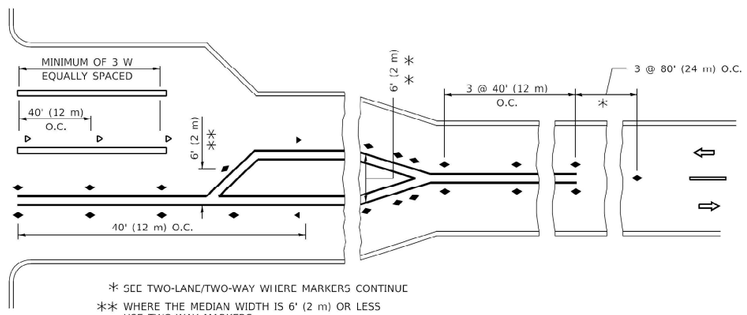

* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE

** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

TURN LANES

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

- YELLOW STRIPE
- WHITE STRIPE
- ONE-WAY AMBER MARKER
- ONE-WAY CRYSTAL MARKER (W/O)
- TWO-WAY AMBER MARKER

LANE MARKER NOTES

A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.

B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: Default
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	DRAWN -	REVISED - T. RAMMACHER 01-06-00	
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED - C. JUCIUS 09-09-09	
PLOT DATE = 3/4/2019	DATE -	REVISED - C. JUCIUS 07-01-13	

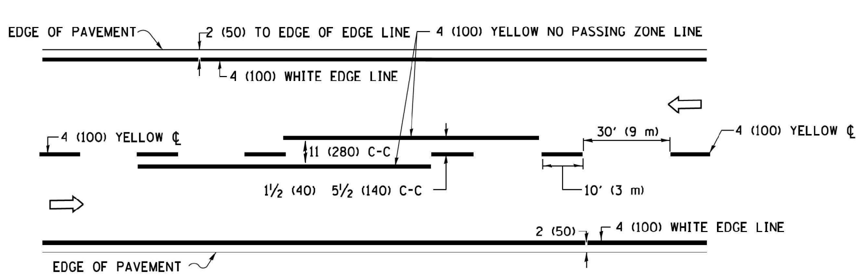
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW–PLOW RESISTANT)

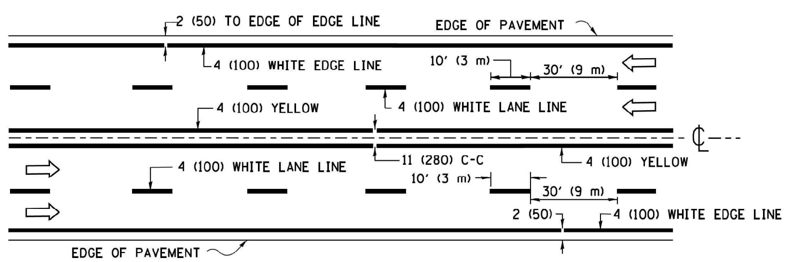
SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ2	DUPAGE	44	40
	TC–11	CONTRACT NO. 62T31		
	ILLINOIS FED. AID PROJECT			

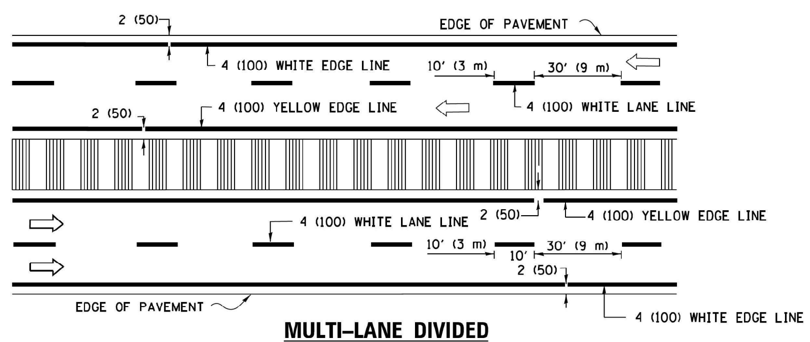
tc11.dgn 3/4/2019 12:29:08 AM | footemj



2-LANE ROADWAY



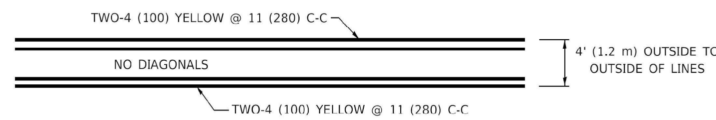
MULTI-LANE UNDIVIDED



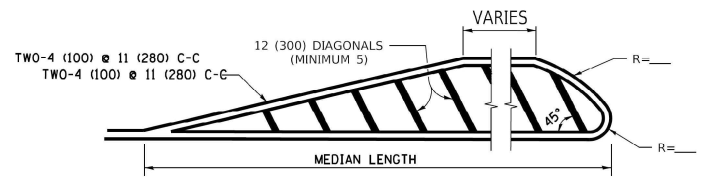
MULTI-LANE DIVIDED

WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING



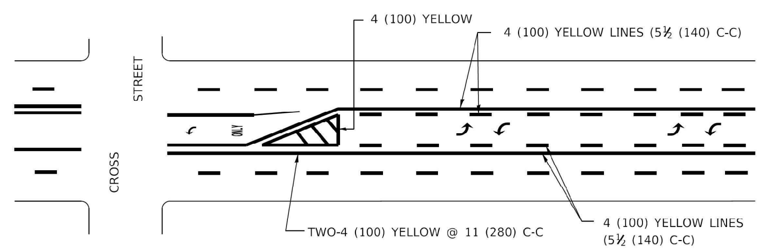
4' (1.2 m) WIDE MEDIANS ONLY



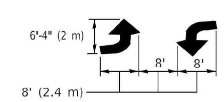
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

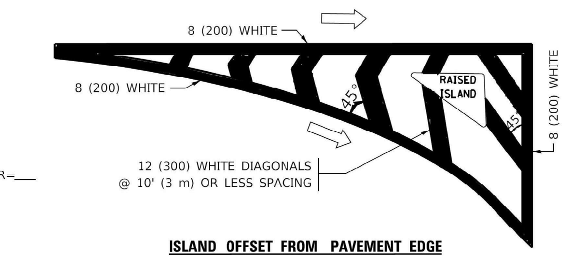


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.

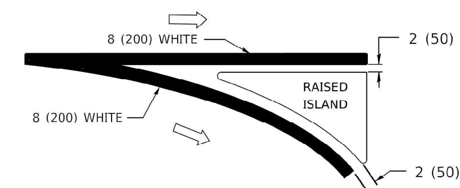


MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

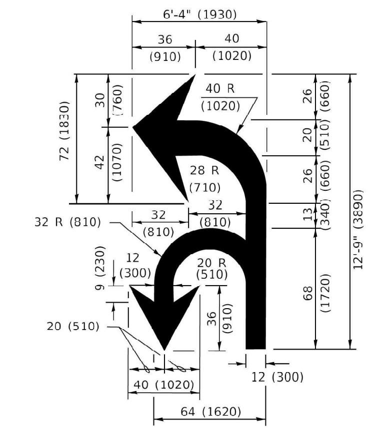


ISLAND OFFSET FROM PAVEMENT EDGE

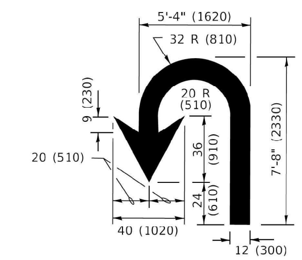


ISLAND AT PAVEMENT EDGE

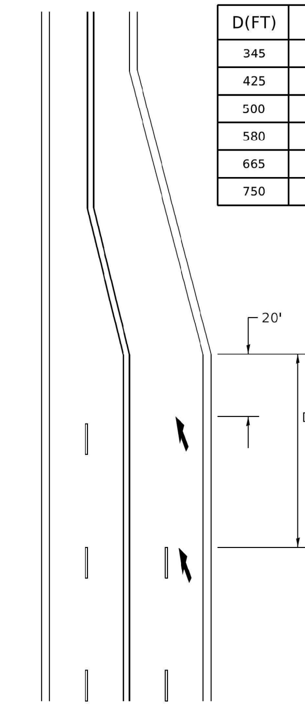
TYPICAL ISLAND MARKING



COMBINATION LEFT AND U-TURN



U-TURN



D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

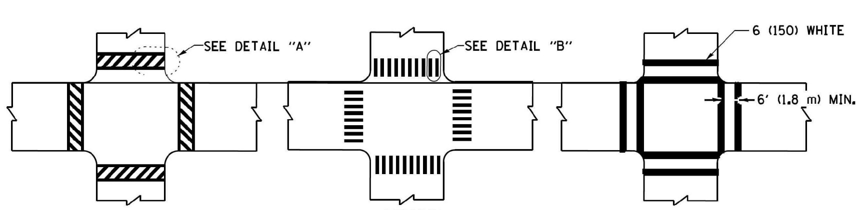
LANE REDUCTION TRANSITION

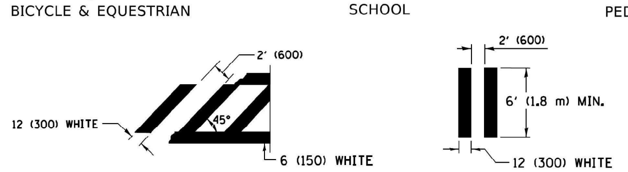
* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m^2) EACH "X"=54.0 SQ. FT. (5.0 m^2)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

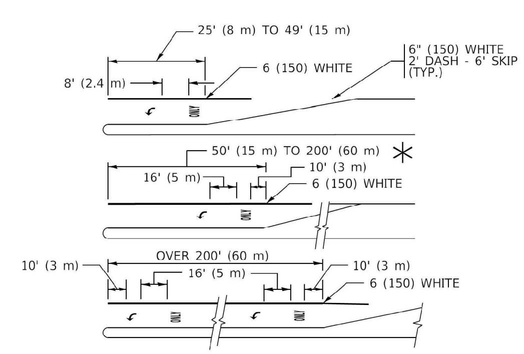




DETAIL "A" DETAIL "B"

TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
↰ AREA = 15.6 SQ. FT. (1.5 m^2) ONLY AREA = 20.8 SQ. FT. (1.9 m^2)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

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	DRAWN -	REVISED - C. JUCIUS 07-01-13
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED - C. JUCIUS 12-21-15
PLOT DATE = 3/4/2019	DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE | SHEET 1 OF 2 SHEETS | STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ2	DUPAGE	44	41
	TC-13	CONTRACT NO. 62T31		
	ILLINOIS	FED. AID PROJECT		

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

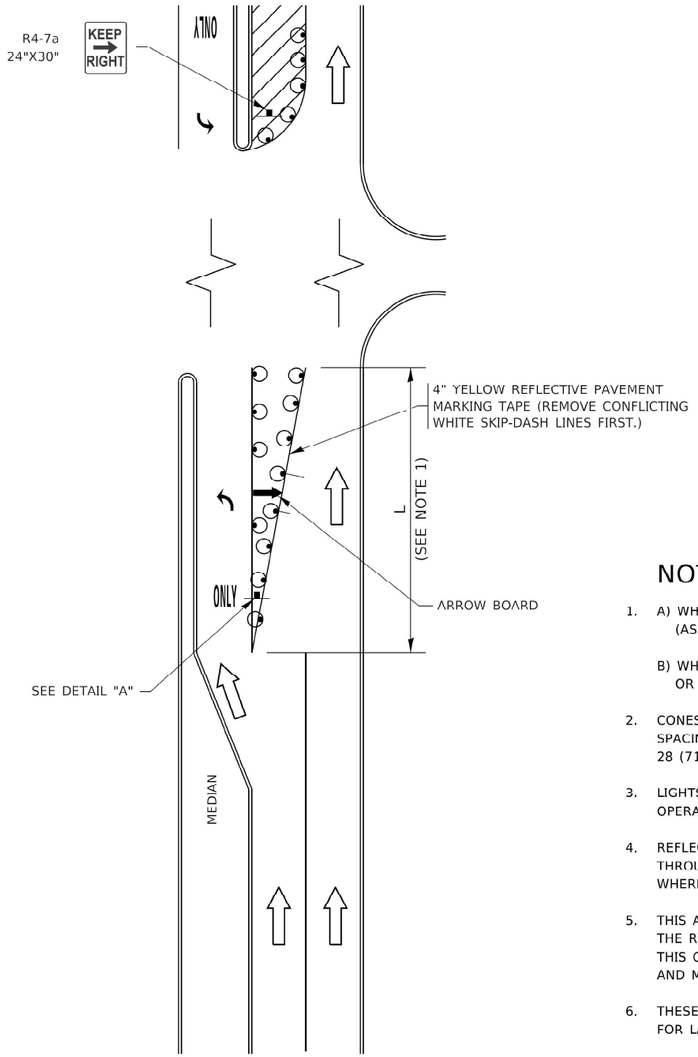


FIGURE 1

LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

NOTES:

1. A) WHEN "L" IS ≤ THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.

   B) WHEN "L" IS > THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.

2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.

3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.

4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.

5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-I100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.

6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.

7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.

8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

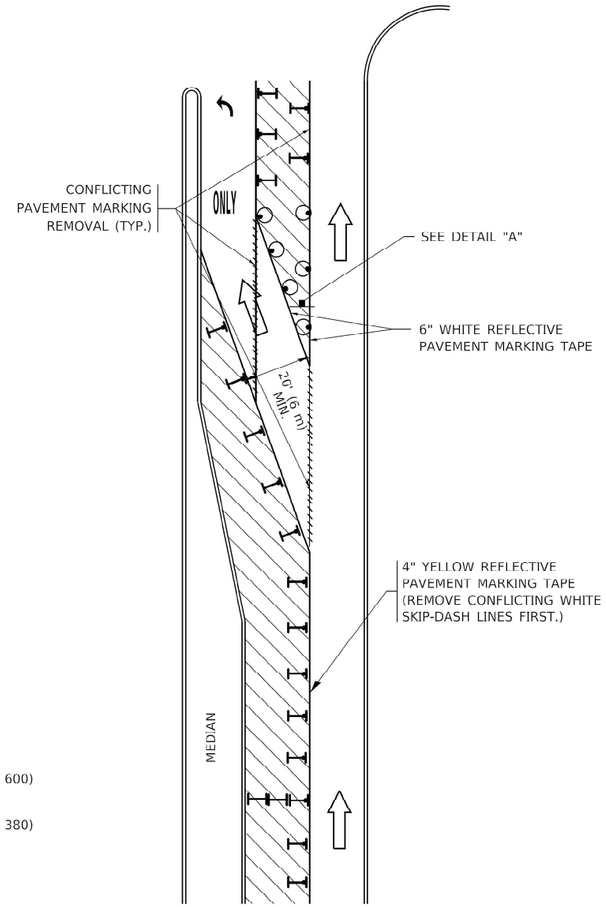
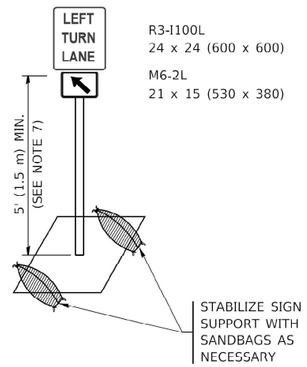


FIGURE 2



DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

USER NAME = footemj	DESIGNED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)	
	DRAWN - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13			
PLOT SCALE = 50.0000 ' / in.	CHECKED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16			
PLOT DATE = 3/4/2019	DATE - T. RAMMACHER 01-06-00	REVISED -		SCALE: NONE / SHEET 1 OF 1 SHEETS / STA. / TO STA.	

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ2	DUPAGE	44	42
	TC-14	CONTRACT NO. 62T31		
	ILLINOIS / FED. AID PROJECT			

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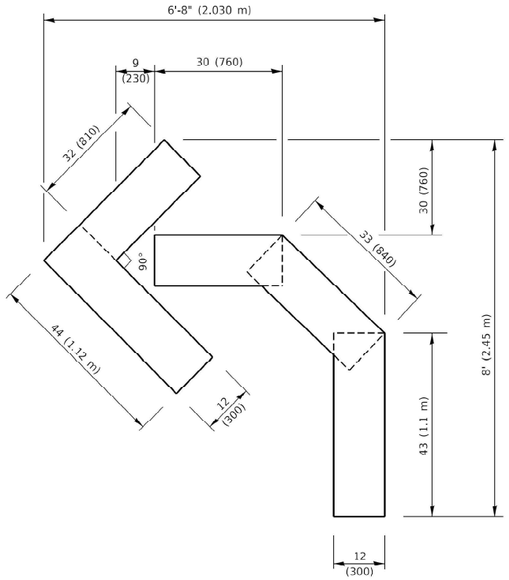
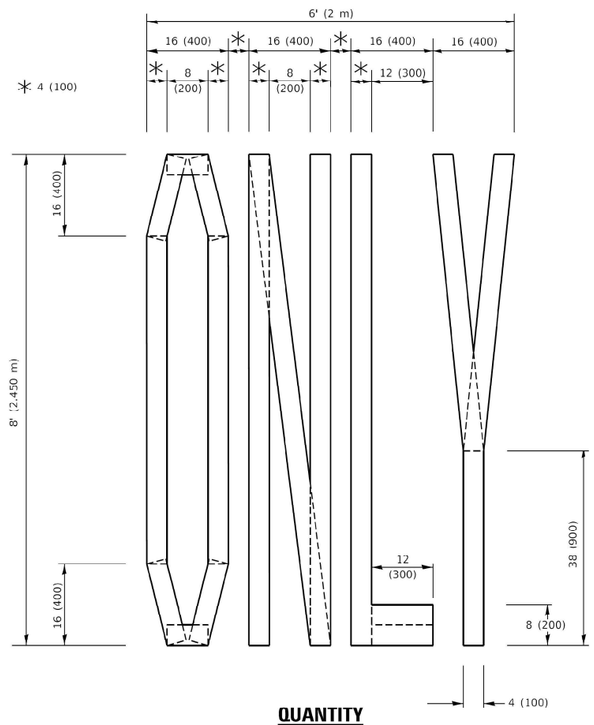
sic14.dgn 3/4/2019 12:36:01 AM | footemj



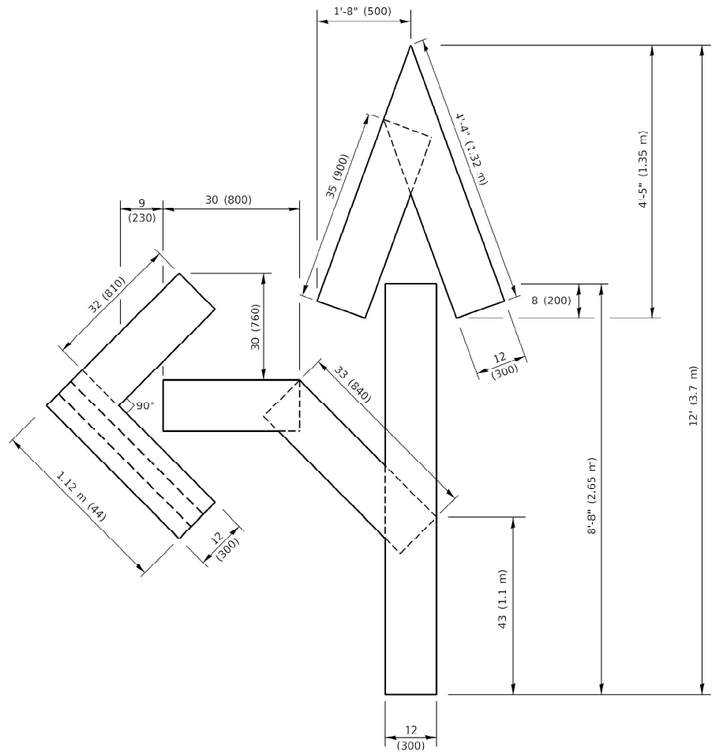
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

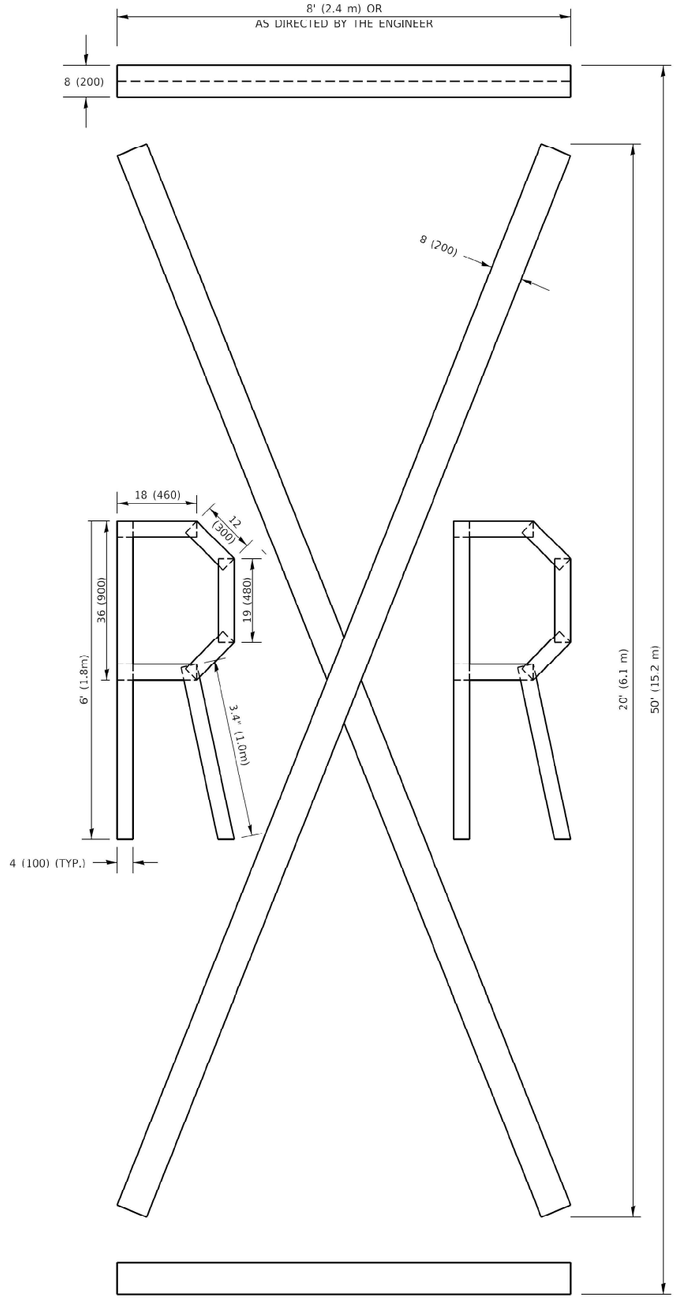


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

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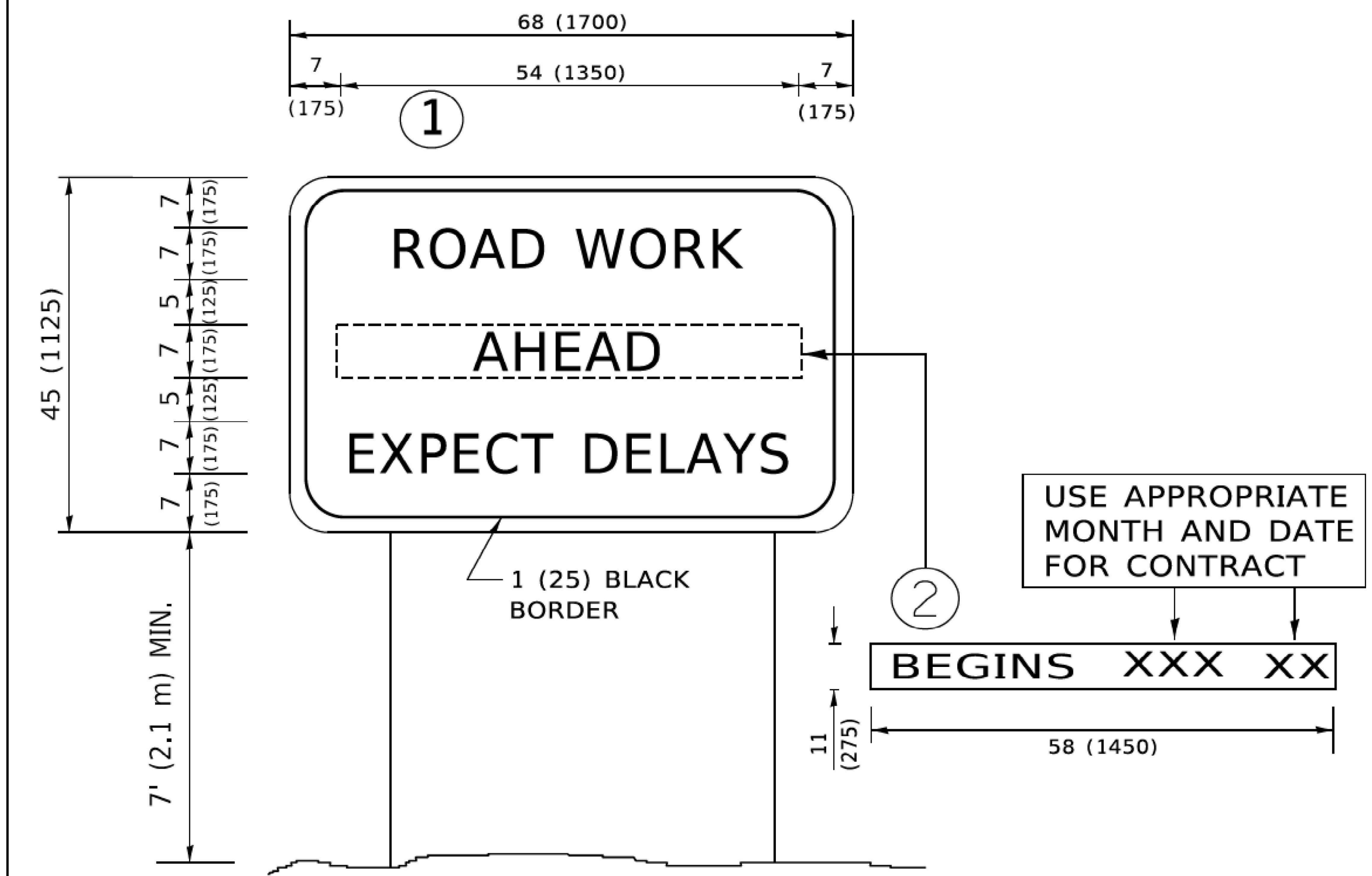
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED - E. GOMEZ 08-28-00	
PLOT DATE = 3/4/2019	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ2	DUPAGE	44	43
	TC-16	CONTRACT NO. 62T31		
		ILLINOIS	FED. AID PROJECT	



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN (1) WITH INSTALLED PANEL (2) ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL (2) SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

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	DRAWN -	REVISED -	R. MIRS 12-11-97
PLOT SCALE = 50.0000 ' / In.	CHECKED -	REVISED -	T. RAMMACHER 02-02-99
PLOT DATE = 3/4/2019	DATE -	REVISED -	C. JUCIUS 01-31-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ARTERIAL ROAD INFORMATION SIGN

SCALE: NONE | SHEET 1 OF 1 SHEETS | STA. | TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
870	FAP 0870 22 BJ2	DUPAGE	44	44
	TC–22	CONTRACT NO. 62T31		
	ILLINOIS	FED. AID PROJECT		