

# CITY OF AURORA AURORA, ILLINOIS

## CONSTRUCTION PLANS FOR

# AURORA MUNICIPAL AIRPORT

### AIRFIELD PAVEMENT REHABILITATION, PHASE 3 (AREA 1)

ILLINOIS PROJECT: ARR-4352  
 S.B.G. PROJECT: 3-17-SBGP-156

JANUARY 10, 2020



**811** Know what's below.  
Call before you dig.

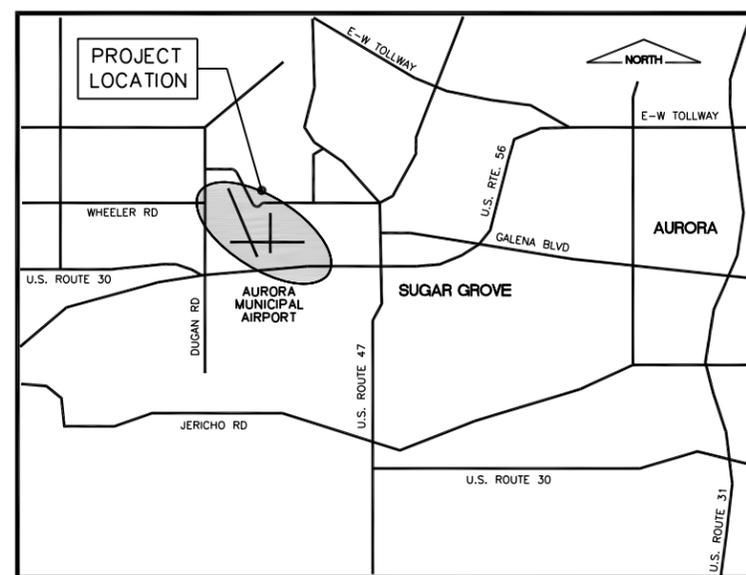
J.U.L.I.E.  
JOINT UTILITY LOCATING  
INFORMATION FOR EXCAVATORS  
www.illinois1call.com

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

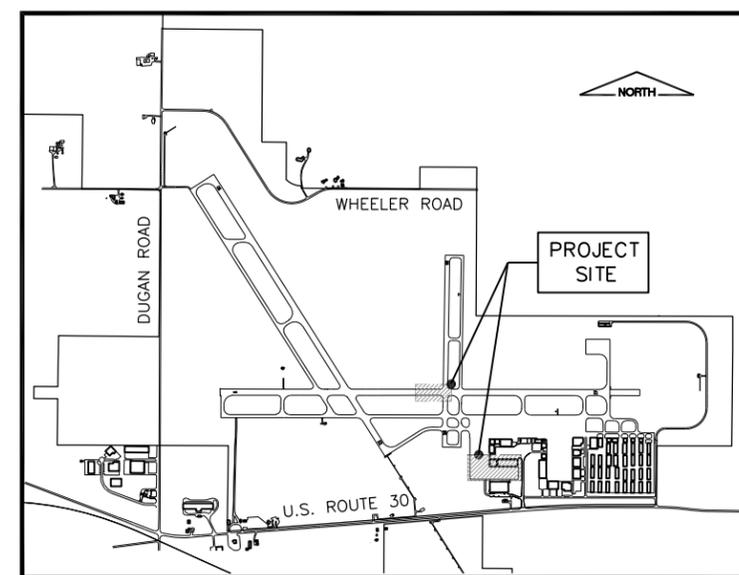
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 AT 811**

AURORA MUNICIPAL AIRPORT

SECTION: 17 AND 18	COUNTY: KANE
RANGE: 7 EAST	U.S. ROUTE 30
TOWNSHIP: 38 NORTH	SUGAR GROVE TOWNSHIP



LOCATION MAP



SITE PLAN




**CITY OF AURORA , ILLINOIS**

APPROVED BY  MAYOR  
 RICHARD C. IRVIN

DATE 1/7 2020

APPROVED BY  AIRPORT MANAGER  
 STEPHEN K. ANDRAS, P.E.

DATE JANUARY 8 2020



190285-02

CRAWFORD MURPHY & TILLY, INC.  
 CONSULTING ENGINEERS



ANTONIO R. MARIN, P.E.  
 062-057994  
 STATE OF ILLINOIS  
 EXPIRES 11/30/2021

SUBMITTED BY  ANTONIO R. MARIN, P.E.

DATE JANUARY 6, 2020

**NOTES**

- SPECIAL ATTENTION IS NECESSARY WHEN WORKING NEAR FAA POWER AND CONTROL CABLES. ANY FAA UTILITY THAT IS DAMAGED OR CUT DURING CONSTRUCTION SHALL BE REPAIRED IMMEDIATELY. **FAA REQUIRES THAT ANY DAMAGED CABLE BE REPLACED IN ITS ENTIRETY, FROM POWER/CONTROL SOURCE TO THE EQUIPMENT/SERVICE. SPLICES OF ANY KIND WILL NOT BE PERMITTED.** EXPOSURES OF ANY FAA CABLES MUST BE DONE BY HAND DIGGING OR HYDRO-EXCAVATION. NO ADDITIONAL COMPENSATION WILL BE MADE FOR LOCATING, REPLACEMENT OR REPAIR OF FAA FACILITIES OR CABLES BUT, SHALL BE INCIDENTAL TO THE CONTRACT.
- WHEN FAA CABLES ARE REQUIRED TO BE LOCATED, OR THE CONTRACTOR IS PLANNING ON WORKING ON OR AROUND FAA CABLES, CONDUITS OR EQUIPMENT, A 10 WORKING DAY ADVANCED NOTICE SHALL BE GIVEN TO THE FAA BEFORE ANY SUCH MARKINGS ARE REQUIRED. ONCE FAA MARKS THE CABLES, THE CONTRACTOR WILL BE REQUIRED TO SURVEY THE FAA UTILITIES SO THEY CAN BE REPLACED DURING CONSTRUCTION WITHOUT REMARKING BY THE FAA. THIS SHALL BE INCIDENTAL TO THE CONTRACT. THE FAA PERSONNEL ARE ONLY AVAILABLE FROM 9 AM TO 3 PM, MONDAY THROUGH FRIDAY WITH ADVANCED NOTICE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DAMAGE DONE BY HIS HAULING AND CONSTRUCTION EQUIPMENT. ANY WORK NECESSARY TO CORRECT DAMAGED WORK AND EXISTING PAVEMENT SHALL BE PERFORMED BY THE CONTRACTOR AND AT THE EXPENSE OF THE CONTRACTOR.
- THE RESIDENT ENGINEER IN CONSULTATION WITH THE AIRPORT SHALL IDENTIFY ALL CRACKS TO BE REPAIRED AND OR SEALED. CONTRACTOR SHALL ADJUST THEIR OPERATIONS AS NECESSARY FOR THE IDENTIFIED WORK. NO ADDITIONAL COMPENSATION SHALL BE MADE.
- CONTRACTOR SHALL DISPOSE OF ALL PAVEMENT REMOVAL AND OTHER MISCELLANEOUS CONSTRUCTION DEBRIS OFF OF AIRPORT PROPERTY.
- PAVEMENT REMOVAL AND REPLACEMENT AREAS SHALL BE LAID OUT BY THE RESIDENT ENGINEER IN THE FIELD DURING CONSTRUCTION.
- INITIAL SAWCUT SHALL BE MADE AS SOON AS POSSIBLE. SAWING SHALL BE STOPPED IF SPALLING OF THE SAWCUT IS NOTED AND RESUMED AFTER ADDITIONAL CURE TIME.
- DRY CUTTING OF INITIAL CUT AND OF THE FINAL JOINT RESERVOIR SHALL NOT BE ALLOWED. ONLY WET SAWING METHODS SHALL BE EMPLOYED ON THIS PROJECT.
- CONTRACTOR SHALL INSTALL DOWEL BARS INTO THE EXISTING CONCRETE PAVEMENT ON ALL EXISTING SIDES, COST INCLUDED IN NEW PCC PAVEMENT.
- JOINT BETWEEN NEW AND EXISTING PCC PAVEMENT SHALL BE SAWED AND SEALED. COST INCLUDED IN NEW PCC PAVEMENT.
- THE EXISTING PAVEMENT TO BE REMOVED SHALL BE SAWED FULL DEPTH AROUND THE PERIMETER OF THE REMOVAL LIMITS. COST OF SAWCUTTING AND DISPOSAL OF PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM. ANY DAMAGE TO PAVEMENT BEYOND THE LIMITS SHOWN ON THE PLANS SHALL BE REMOVED AND REPLACED BY THE CONTRACTOR AT HIS EXPENSE. THESE AREAS SHALL BE SAW CUT TO A UNIFORM WIDTH.
- ALL PAVEMENT MARKINGS SHALL BE RESTORED TO MATCH THE EXISTING MARKINGS PRIOR TO PAVEMENT REMOVAL. MARKING SHALL BE PAID FOR ON A SQUARE FOOT BASIS AS DETAILED IN THE SPECIFICATION.
- EXISTING CONSTRUCTION JOINTS ARE KEYED. WHEN NEW DOWELS ARE INSTALLED AT AN EXISTING KEYED CONSTRUCTION JOINT, THE JOINT SHALL BE CLEANED AND PATCHED IN ACCORDANCE WITH THE PAVEMENT JOINTING DETAILS. THE COSTS SHALL BE INCIDENTAL TO THE NEW PCC PAVEMENT.
- IN AREAS OF NEW 9" PCC PAVEMENT INSTALLATION, THE CONTRACTOR SHALL REGRADE AND COMPACT THE EXISTING AGGREGATE BASE COURSE TO THE SATISFACTION OF THE RESIDENT ENGINEER. ANY ADDITIONAL AGGREGATE FILL OR EXCAVATION NECESSARY TO MEET THE BASE GRADE SHALL BE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND WILL NOT BE MEASURED PAYMENT.
- PCC REMOVAL SHALL INCLUDE THE EXISTING HMA BASE. HOT MIX ASPHALT/ASPAHLT TREATED PERMEABLE BASES REMOVED UNDER PCC PAVEMENT SHALL NOT BE MEASURED SEPARATELY FOR PAYMENT BUT SHALL BE INCIDENTAL TO THE PCC PAVEMENT REMOVAL PAY ITEM.
- CONTRACTOR SHALL MATCH EXISTING PCC THICKNESS OF REMAINING PANELS (9" MIN.)

**EXISTING CONDITIONS LEGEND**

- EXISTING BASE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
- EXISTING STAKE MOUNTED MEDIUM INTENSITY TAXIWAY LIGHT
- EXISTING BASE MOUNTED HIGH INTENSITY RUNWAY LIGHT
- EXISTING STAKE MOUNTED HIGH INTENSITY RUNWAY LIGHT
- EXISTING IN PAVEMENT MOUNTED HIGH INTENSITY RUNWAY LIGHT
- EXISTING ELEVATED RETROREFLECTIVE MARKER
- EXISTING VISUAL APPROACH SLOPE INDICATOR (VASI)
- EXISTING RUNWAY END IDENTIFIER LIGHT (REIL)
- EXISTING APPROACH LIGHTING SYSTEM
- EXISTING AIRFIELD GUIDANCE SIGN
- EXISTING WINDCONE
- EXISTING WINDCONE WITH SEGMENTED CIRCLE
- EXISTING ELECTRICAL TRANSFORMER
- EXISTING ELECTRICAL/STORM/SANITARY/TELEPHONE MANHOLE OR EXISTING WATER VALVE VAULT
- EXISTING ELECTRICAL HANDHOLE
- EXISTING STORM INLET
- EXISTING SLOPE BOX
- EXISTING FLARED END SECTION
- EXISTING HEADWALL
- EXISTING CLEANOUT STRUCTURE
- EXISTING FLUSH MOUNTED FIRE HYDRANT
- EXISTING WATER VALVE
- EXISTING UTILITY PEDESTAL
- EXISTING CONDUIT/DUCT BANK
- EXISTING FAA CABLES
- EXISTING RUNWAY END IDENTIFIER LIGHTING CIRCUIT
- EXISTING COMED SERVICE TO FAA ILS SYSTEM
- EXISTING STORM SEWER
- EXISTING UNDERDRAIN
- EXISTING WATERMAIN
- EXISTING SANITARY SEWER
- EXISTING TELEPHONE LINES
- EXISTING ELECTRIC LINE
- EXISTING GAS LINE
- EXISTING FENCE

**SUMMARY OF QUANTITIES**

ITEM	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
<b>BASE BID</b>				
AR150510	ENGINEER'S FIELD OFFICE	LS	1	
AR152410	UNCLASSIFIED EXCAVATION	CY	85	
AR208515	POROUS GRANULAR EMBANKMENT	CY	85	
AR209604	CRUSHED AGG. BASE COURSE - 4"	SY	1,050	
AR501509	9" PCC PAVEMENT	SY	1,050	
AR501530	PCC TEST BATCH	EACH	1	
AR501900	REMOVE PCC PAVEMENT	SY	1,050	
AR510510	TIE DOWN	EACH	4	
AR510515	GROUND ROD	EACH	3	
AR605542	CLEAN & SEAL EXPANSION JOINTS	LF	760	
AR751980	RECONSTRUCT INLET	EACH	2	
AR800009	PCC SPALL REPAIR	SF	10	
<b>ADDITIVE ALTERNATE NO. 1 - RUNWAY 9/27 PCC PANEL REPLACEMENT</b>				
AS501900	REMOVE PCC PAVEMENT	SY	85	
AS620520	PAVEMENT MARKING - WATERBORNE	SF	65	
AS800075	9" PCC PAVEMENT - HIGH EARLY MIX	SY	85	
<b>ADDITIVE ALTERNATE NO. 2 - WEST APRON PCC PANEL REPLACEMENT</b>				
AT162506	CLASS E FENCE 6'	LF	40	
AT162900	REMOVE CLASS E FENCE	LF	40	
AT209604	CRUSHED AGGREGATE BASE COURSE-4"	SY	42	
AT501509	9" PCC PAVEMENT	SY	42	
AT501900	REMOVE PCC PAVEMENT	SY	42	
AT751995	RECONSTRUCT UTILITY STRUCTURE	EACH	1	

**INDEX TO SHEETS**

- COVER SHEET
- INDEX TO SHEETS, SUMMARY OF QUANTITIES AND GENERAL NOTES
- SITE PLAN/PROJECT CONTROL
- SEQUENCE OF CONSTRUCTION PER AC 150/5370-2G (LATEST EDITION) - SHEET 1
- SEQUENCE OF CONSTRUCTION PER AC 150/5370-2G (LATEST EDITION) - SHEET 2
- SEQUENCE OF CONSTRUCTION GENERAL NOTES
- SEQUENCE OF CONSTRUCTION DETAILS
- TYPICAL SECTIONS/PAVEMENT REHABILITATION DETAILS
- PROPOSED IMPROVEMENTS
- PAVEMENT JOINTING DETAILS
- STORM WATER POLLUTION PREVENTION NOTES AND DETAILS
- RECONSTRUCT INLET DETAILS AND NOTES

IL CONTRACT: **AU070**  
 IL LETTING ITEM: **02A**  
 IL PROJECT: **ARR-4352**  
 S.B.G. PROJECT: **3-17-SBGP-156**

SURVEY BOOK #    BOOK #

**REVISIONS**

NUMBER	BY	DATE

0    1    2  
 THIS BAR IS EQUAL TO 2"  
 AT FULL SCALE (34X22).

AURORA MUNICIPAL AIRPORT  
 AURORA, ILLINOIS  
 AIRFIELD PAVEMENT REHABILITATION, PHASE 3 (AREA 1)

INDEX TO SHEETS  
 SUMMARY OF QUANTITIES  
 AND GENERAL NOTES

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CHECKED BY:        ARM

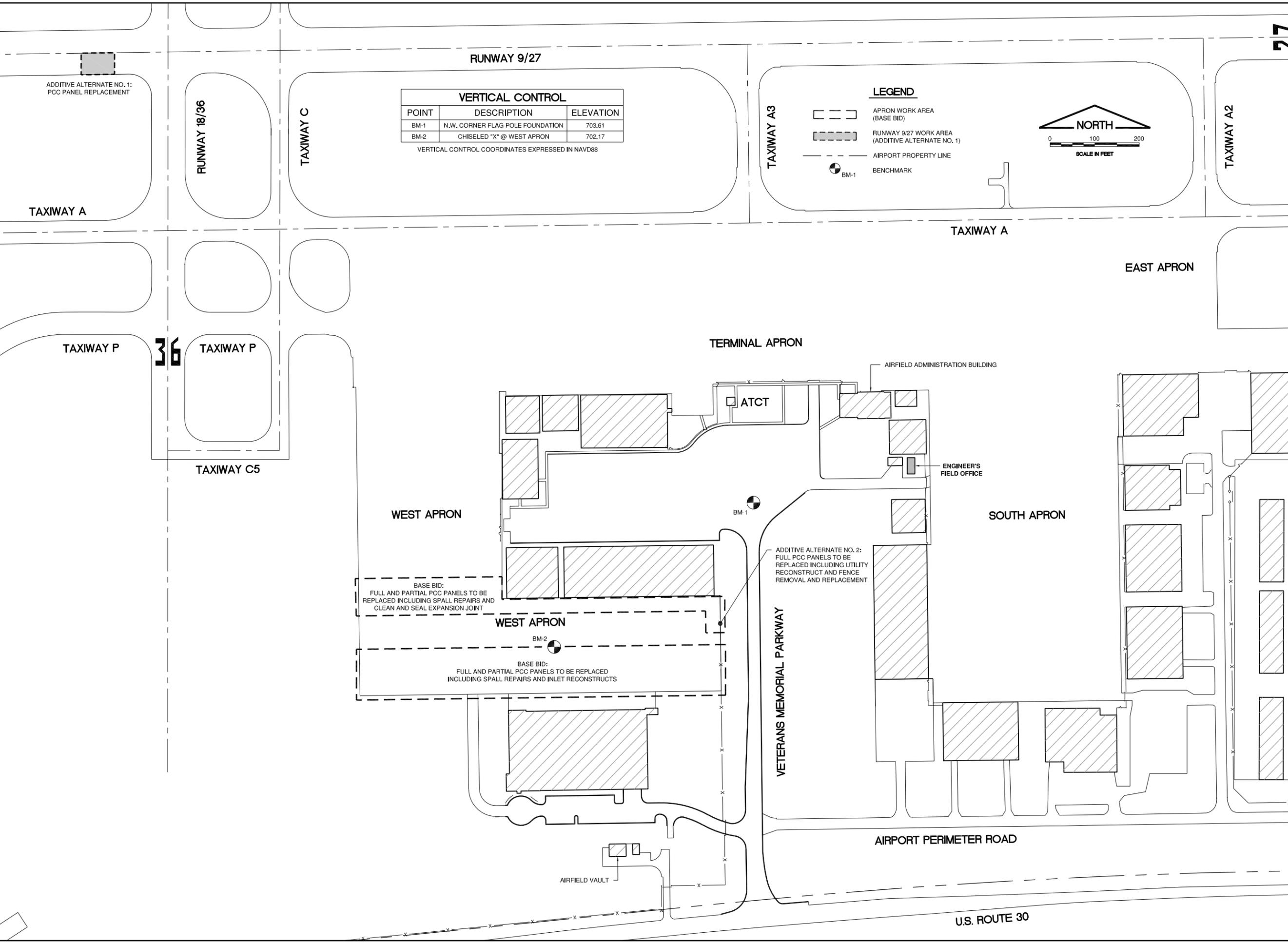
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DATE:                    1/10/2020

JOB No:                190285-02-00

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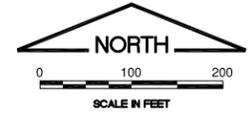
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 LAYOUT: Layout1  
 IMAGE FILES: Aurora Municipal Airport Logo FINAL PKG.smg  
 REF: DWG: Rehab\_02.dwg  
 PLOT: 190285-02-00.dwg



VERTICAL CONTROL		
POINT	DESCRIPTION	ELEVATION
BM-1	N.W. CORNER FLAG POLE FOUNDATION	703.61
BM-2	CHISELED "X" @ WEST APRON	702.17

VERTICAL CONTROL COORDINATES EXPRESSED IN NAVD88

- LEGEND**
- APRON WORK AREA (BASE BID)
  - RUNWAY 9/27 WORK AREA (ADDITIVE ALTERNATE NO. 1)
  - AIRPORT PROPERTY LINE
  - BENCHMARK



IL. CONTRACT: **AU070**  
 IL. LETTING ITEM: **02A**  
 IL. PROJECT: **ARR-4352**  
 S.B.G. PROJECT: **3-17-SBGP-156**

SURVEY BOOK #

REVISIONS		
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0 1 2  
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**AURORA MUNICIPAL AIRPORT**  
**AURORA, ILLINOIS**  
**AIRFIELD PAVEMENT REHABILITATION, PHASE 3 (AREA 1)**  
**SITE PLAN/PROJECT CONTROL**

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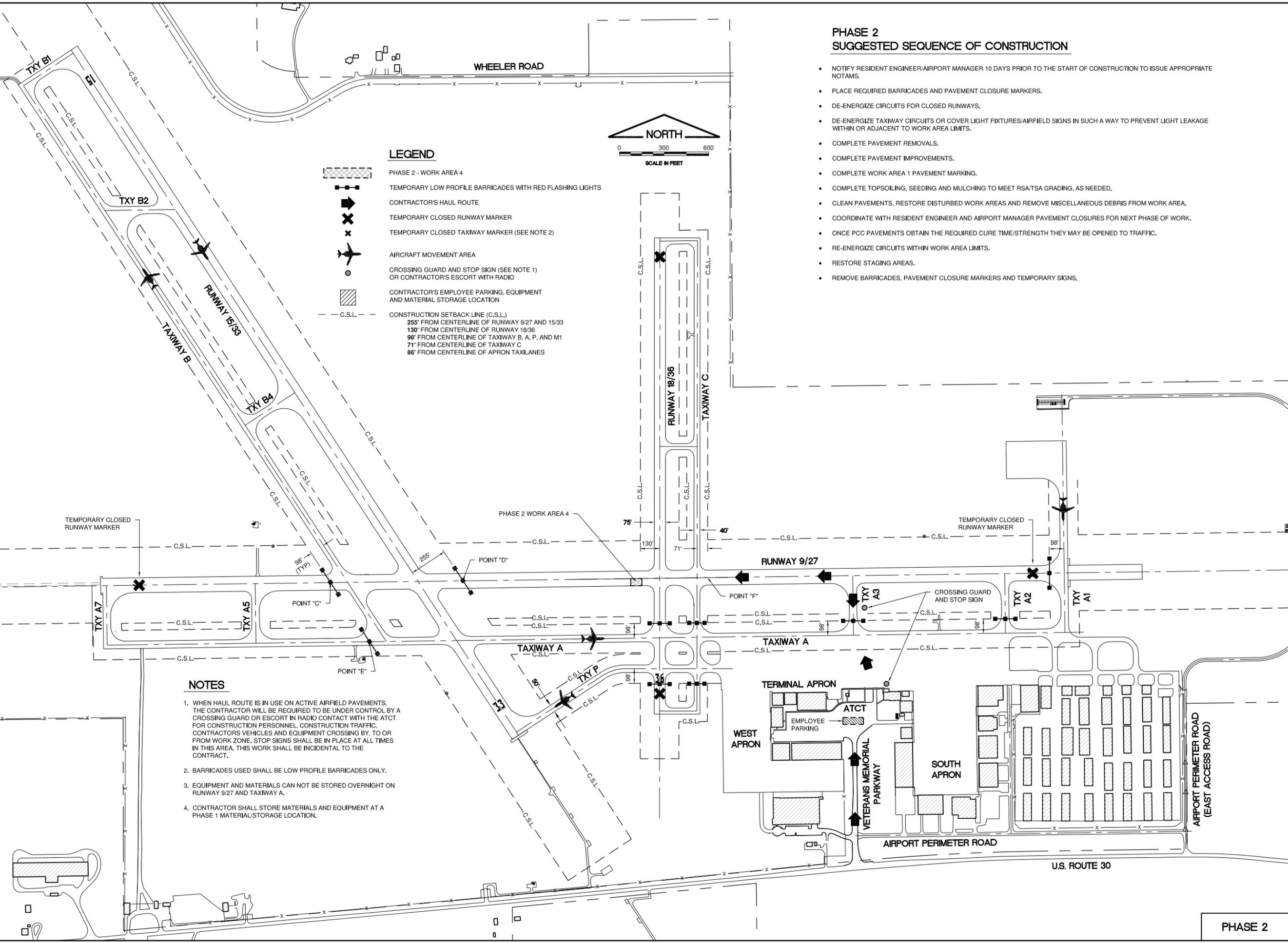
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JOB No:	190285-02-00

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DATE: Friday, January 10, 2020 6:28:44 PM  
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 UPDATE BY: Topy Mierin  
 LAYOUT: Layout1  
 REF DWG: Final grading  
 MAP: 19028502-00\_Pave\Phase3\Draw  
 IMAGE FILES: Aurora Municipal Airport Logo FINAL PMS.smg



**PHASE 2  
SUGGESTED SEQUENCE OF CONSTRUCTION**

- NOTIFY RESIDENT ENGINEER/AIRPORT MANAGER 10 DAYS PRIOR TO THE START OF CONSTRUCTION TO ISSUE APPROPRIATE NOTAMS.
- PLACE REQUIRED BARRICADES AND PAVEMENT CLOSURE MARKERS.
- DE-ENERGIZE CIRCUITS FOR CLOSED RUNWAYS.
- DE-ENERGIZE TAXIWAY CIRCUITS OR COVER LIGHT FIXTURES/AIRFIELD SIGNS IN SUCH A WAY TO PREVENT LIGHT LEAKAGE WITHIN OR ADJACENT TO WORK AREA LIMITS.
- COMPLETE PAVEMENT REMOVALS.
- COMPLETE PAVEMENT IMPROVEMENTS.
- COMPLETE WORK AREA 1 PAVEMENT MARKING.
- COMPLETE TOPSOILING, SEEDING AND MULCHING TO MEET RSA/TSA GRADING, AS NEEDED.
- CLEAN PAVEMENTS, RESTORE DISTURBED WORK AREAS AND REMOVE MISCELLANEOUS DEBRIS FROM WORK AREA.
- COORDINATE WITH RESIDENT ENGINEER AND AIRPORT MANAGER PAVEMENT CLOSURES FOR NEXT PHASE OF WORK.
- ONCE PCC PAVEMENTS OBTAIN THE REQUIRED CURE TIME/STRENGTH THEY MAY BE OPENED TO TRAFFIC.
- RE-ENERGIZE CIRCUITS WITHIN WORK AREA LIMITS.
- RESTORE STAGING AREAS.
- REMOVE BARRICADES, PAVEMENT CLOSURE MARKERS AND TEMPORARY SIGNS.

**LEGEND**

- PHASE 2 - WORK AREA 4
- TEMPORARY LOW PROFILE BARRICADES WITH RED FLASHING LIGHTS
- CONTRACTOR'S HAUL ROUTE
- TEMPORARY CLOSED RUNWAY MARKER
- TEMPORARY CLOSED TAXIWAY MARKER (SEE NOTE 2)
- AIRCRAFT MOVEMENT AREA
- CROSSING GUARD AND STOP SIGN (SEE NOTE 1) OR CONTRACTOR'S ESCORT WITH RADIO
- CONTRACTOR'S EMPLOYEE PARKING, EQUIPMENT AND MATERIAL STORAGE LOCATION
- C.S.L. - CONSTRUCTION SETBACK LINE (C.S.L.)  
 255' FROM CENTERLINE OF RUNWAY 9/27 AND 15/33  
 130' FROM CENTERLINE OF RUNWAY 18/36  
 98' FROM CENTERLINE OF TAXIWAY B, A, P, AND M1  
 71' FROM CENTERLINE OF TAXIWAY C  
 86' FROM CENTERLINE OF APRON TAXILANES

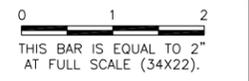
**NOTES**

1. WHEN HAUL ROUTE IS IN USE ON ACTIVE AIRFIELD PAVEMENTS, THE CONTRACTOR WILL BE REQUIRED TO BE UNDER CONTROL BY A CROSSING GUARD OR ESCORT IN RADIO CONTACT WITH THE ATCT FOR CONSTRUCTION PERSONNEL, CONSTRUCTION TRAFFIC, CONTRACTORS VEHICLES AND EQUIPMENT CROSSING BY, TO OR FROM WORK ZONE. STOP SIGNS SHALL BE IN PLACE AT ALL TIMES IN THIS AREA. THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT.
2. BARRICADES USED SHALL BE LOW PROFILE BARRICADES ONLY.
3. EQUIPMENT AND MATERIALS CAN NOT BE STORED OVERNIGHT ON RUNWAY 9/27 AND TAXIWAY A.
4. CONTRACTOR SHALL STORE MATERIALS AND EQUIPMENT AT A PHASE 1 MATERIAL/STORAGE LOCATION.

IL CONTRACT: **AU070**  
 IL LETTING ITEM: **02A**  
 IL PROJECT: **ARR-4352**  
 S.B.G. PROJECT: **3-17-SBGP-156**

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE



AURORA MUNICIPAL AIRPORT  
 AURORA, ILLINOIS  
 AIRFIELD PAVEMENT REHABILITATION, PHASE 3 (AREA 1)  
**SEQUENCE OF CONSTRUCTION  
 PER AC 150/5370-2G (LATEST EDITION)  
 SHEET 2**

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APPROVED BY:	DKP
DATE:	1/10/2020
JOB No:	190285-02-00

**PHASE 2**

**FINAL**  
 SHEET 5 OF 12 SHEETS

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XREF DWG: 02airn\_19228502.dwg  
MANAGE FILES: Aurora Municipal Airport Logo FINAL PMS.png  
UPDATE BY: Tony Marfin  
LAYOUT: Layout1

### GENERAL NOTES

- THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE PROPOSED IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT MANAGER AND RESIDENT ENGINEER AND BE APPROVED BY THE DIVISION OF AERONAUTICS AND FEDERAL AVIATION ADMINISTRATION.
- ALL OPERATIONS SHALL BE IN CONFORMANCE WITH AC 150/5370-2G (LATEST EDITION) OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION.
- CONTRACTOR'S EQUIPMENT SHALL BE STORED IN THE EQUIPMENT AND MATERIAL STORAGE/STAGING AREA WHEN CONSTRUCTION IS NOT IN PROGRESS.
- THE AIRPORT MANAGER IN CONSULTATION WITH THE RESIDENT ENGINEER SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT SAFETY.
- ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A HAUL ROAD OR STORAGE AREA BY THE CONTRACTOR SHALL BE PROTECTED, AS NEEDED, THROUGH THE USE OF STEEL PLATES OR OTHER MEANS ACCEPTABLE TO THE RESIDENT ENGINEER AND AIRPORT IN ORDER TO PREVENT DAMAGE TO THE PAVEMENT. ALL DAMAGED PAVEMENT SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT MANAGER. THE COST OF PROTECTING, MAINTAINING, REPAIRING OR CONSTRUCTING THESE PAVEMENTS AND AREAS SHALL BE INCIDENTAL TO THE CONTRACT. EXISTING AREAS OUTSIDE THE PROJECT LIMITS WHICH ARE DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT MANAGER.
- THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING TAXIWAYS, APRONS AND RUNWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE AIRPORT MANAGER.
- WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE AS APPROVED BY THE RESIDENT ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY. CONTRACTOR'S WORK HOURS SHALL BE IN ACCORDANCE WITH LOCAL ORDINANCES.
- THE CONTRACTOR SHALL PROVIDE PORTABLE FLOOD LIGHTING FOR NIGHTTIME CONSTRUCTION. SUFFICIENT UNITS SHALL BE PROVIDED SO THAT WORK AREAS ARE ILLUMINATED TO A LEVEL OF FIVE HORIZONTAL FOOT CANDLES. THE LIGHTING LEVELS SHALL BE CALCULATED AND MEASURED IN ACCORDANCE WITH THE CURRENT STANDARDS OF THE ILLUMINATION ENGINEERING SOCIETY. LIGHTS SHALL BE POSITIONED SO AS NOT TO INTERFERE WITH AIRPORT OPERATIONS.
- THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES. WHEN ACTIVE AIRFIELD PAVEMENTS ARE UTILIZED AS HAUL ROADS BY THE CONTRACTOR, MATERIAL TRACKED ON TO THE PAVEMENT SHALL BE CONTINUALLY REMOVED WITH SAID SWEEPER. THIS SWEEPING SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- MATERIALS REMOVED FROM THE PROJECT WILL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS NOTED OTHERWISE.
- PAYMENT FOR TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO BARRICADES, SIGNING, TAXIWAY AND RUNWAY CLOSED MARKERS, AIR OPERATIONS AREA (A.O.A.) LATHE AND RIBBON, ETC. SHALL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. BARRICADES SHALL BE WEIGHED TO PREVENT BLOWING OVER. BARRICADES SHALL HAVE A STEADY BURN OR FLASHING RED LIGHT. BARRICADE INSTALLATION WILL BE REQUIRED PRIOR TO ACCESS TO THE A.O.A. BY CONTRACTOR'S WORKERS, EQUIPMENT OR MATERIAL. SIGNS SHALL BE PLACED AT EACH TAXIWAY/RUNWAY CLOSURE LOCATION AND SHALL BE ATTACHED TO THE BARRICADES. EACH BARRICADE LOCATION SHALL CONSIST OF ONE "DO NOT ENTER" SIGN AND ONE "AIRCRAFT MOVEMENT AREA" SIGN. SIGNS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- THE CONTRACTOR SHALL CONTACT THE AIRPORT MANAGER THROUGH THE RESIDENT ENGINEER TEN (10) WORKING DAYS IN ADVANCE OF THE START OF CONSTRUCTION SO THAT THE APPROPRIATE NOTAMS MAY BE ISSUED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ALL CONSTRUCTION ACCESS GATES CLOSED DURING NON WORKING HOURS. THE CONTRACTOR SHALL PROVIDE A SIGN AT THE ACCESS GATE SAYING "AUTHORIZED PERSONNEL ONLY". THE CONTRACTOR SHALL CLOSE AND LOCK THE ACCESS GATE UPON LEAVING THE SITE. THROUGHOUT THE DURATION OF THE CONTRACT, ANY DAMAGES TO THE ACCESS ROAD, ACCESS GATE OR FENCING ADJACENT TO THE PROJECT SHALL BE REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE RESIDENT ENGINEER. ALL COST RELATING TO CONTRACTOR'S ACCESS AND SECURITY SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.
- CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS AND HAVE BEACON LIGHTS ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION. SEE FLAG DETAIL.
- IN THE CASE OF AN EMERGENCY, CONTRACTOR SHALL NOTIFY AIRPORT MANAGER AND THE RESIDENT ENGINEER IMMEDIATELY.
- DURING ADVERSE WEATHER, THE CONTRACTOR SHALL MAKE PROVISIONS FOR ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK.
- THE TALLEST PIECE OF CONSTRUCTION EQUIPMENT IS ANTICIPATED TO BE AN ASPHALT/STONE TRUCK WHICH HAS A MAXIMUM HEIGHT OF 25 FEET IN A DUMP POSITION.
- IF RUNWAY NUMERALS ARE PRESENT DURING CONSTRUCTION THEN CONTRACTOR SHALL PLACE CLOSED RUNWAY MARKER OVER NUMERALS AS DETAILED, OTHERWISE PLACE RUNWAY CLOSED MARKER IN TURF AT ENDS OF RUNWAY AS DETAILED.
- AURORA MUNICIPAL AIRPORT WILL BE IN OPERATION DURING THE CONSTRUCTION OF THIS PROJECT. COORDINATION OF WORK WITH THE AIRPORT IS MANDATORY SO AS TO MINIMIZE IMPACTS ON AIRPORT OPERATIONS.
- APPROXIMATE LOCATION OF HAUL ROUTES ON THE AIRPORT SITE ARE SHOWN ON THE GENERAL PROJECT LAYOUT AND THE PHASING PLANS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO COORDINATE OFF-SITE HAUL ROUTES (STATE HIGHWAYS, COUNTY ROADS OR CITY STREETS) WITH THE APPROPRIATE OWNER WHO HAS JURISDICTION OVER THE AFFECTED ROUTE. ON-SITE ROADS USED AS HAUL ROUTES SHALL BE MAINTAINED BY THE CONTRACTOR AND SHALL BE RESTORED AT THE CONTRACTOR'S EXPENSE TO THEIR ORIGINAL CONDITION UPON COMPLETION OF BEING USED AS A HAUL ROUTE. THE BEFORE AND AFTER CONDITION OF ON-SITE HAUL ROUTES SHALL BE JOINTLY INSPECTED AND DETERMINED BY THE CONTRACTOR AND THE ENGINEER. FENCING, DRAINAGE, GRADING AND OTHER MISCELLANEOUS CONSTRUCTION REQUIRED TO CONSTRUCT TEMPORARY HAUL ROUTES OR ACCESS POINTS ON THE AIRPORT WILL BE THE CONTRACTOR'S TOTAL RESPONSIBILITY AND SHALL BE APPROVED BY THE ENGINEER PRIOR TO THE WORK. ALL ON-SITE ACCESS ROADS TO AIRPORT FACILITIES SHALL REMAIN OPEN, UNLESS OTHERWISE SHOWN, AND MAINTAINED AT ALL TIMES.
- MOBILIZATION/EQUIPMENT STORAGE AREA WILL BE MADE AVAILABLE FOR CONTRACTOR'S MOBILIZATION AND STORAGE AS SHOWN ON THE PLANS. THIS AREA SHALL BE RESTORED TO THE ORIGINAL CONDITION UPON COMPLETION OF THE PROJECT AT THE CONTRACTOR'S EXPENSE.

- LOCATION OF KNOWN EXISTING AIRPORT UNDERGROUND CABLES ARE SHOWN ON THE PLANS AND MUST BE VERIFIED BY THE CONTRACTOR. REPAIR OF DAMAGED CABLE MUST BE STARTED IMMEDIATELY AND CONTINUED UNTIL COMPLETED. ALL SUCH REPAIRS SHALL BE IN ACCORDANCE WITH THE SPECIFICATIONS, OR AS DIRECTED BY THE OWNER OF THE CABLE OR FACILITY, AND SHALL BE AT THE CONTRACTOR'S EXPENSE. IF FAA CABLES ARE DAMAGED, REPAIRS SHALL BE DONE FROM PREVIOUS EXISTING TERMINATION POINT TO NEXT EXISTING TERMINATION POINT IN ACCORDANCE WITH FAA REQUIREMENTS AND IN THE PRESENCE OF A FAA REPRESENTATIVE. THE OWNER MAY ELECT TO HAVE THE REPAIR PERFORMED BY OTHERS IN WHICH CASE THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYING THE INCURRED COSTS OF REPAIRS.
- COORDINATION MEETINGS - THE CONTRACTOR SHALL CONDUCT WEEKLY COORDINATION MEETINGS TO DISCUSS WORK AREAS AND SCHEDULING, ETC. WITH THE RESIDENT ENGINEER, AIRPORT OPERATIONS, FAA, AND OTHER APPROPRIATE OFFICIALS. MINUTES FROM THE WEEKLY MEETINGS SHALL BE PREPARED BY THE CONTRACTOR, FURNISHED TO ALL ATTENDEES PRIOR TO THE SUBSEQUENT MEETING, AND KEPT ON FILE AT THE FIELD OFFICE. THE COORDINATION MEETING COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- THE CONTRACTOR SHALL PROVIDE THE PHONE NUMBERS OF THREE PERSONNEL, INCLUDING THE PROJECT SUPERINTENDING, WHO MAY BE CONTACTED IN AN EMERGENCY. PERSONNEL SHALL BE ON CALL 24 HOURS PER DAY FOR MAINTAINING AIRPORT HAZARD LIGHTING AND BARRICADES.
- DRAINAGE MODIFICATIONS SHALL BE SEQUENCED TO PROVIDE POSITIVE DRAINAGE AT ALL TIMES AT NO ADDITIONAL COST TO THE CONTRACT.
- CONTRACTOR PERSONNEL, VEHICLES, EQUIPMENT AND BARRICADES SHALL NOT BE ALLOWED WITHIN THE TAXIWAY OBJECT FREE AREA (TOFA) OF ACTIVE TAXIWAYS AND THE RUNWAY SAFETY AREA (RSA) OF ACTIVE RUNWAYS UNLESS OTHERWISE APPROVED BY THE AIRPORT DIRECTOR IN CONSULTATION WITH THE RESIDENT ENGINEER.
- CONTRACTOR SHALL STORE EQUIPMENT AND MATERIALS IN SUCH A MANNER AS NOT TO VIOLATE FEDERAL AVIATION ADMINISTRATION PART 77 IMAGINARY SURFACES OR RUNWAY AND TAXIWAY SAFETY AREAS.
- ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER ELECTRICAL CABLES SHALL REMAIN IN SERVICE AT ALL TIMES. ALL EXISTING LIGHTING AND VAULT EQUIPMENT SHALL REMAIN IN SERVICE UNTIL PROPOSED IMPROVEMENTS ARE INSTALLED AND OPERATIONAL. UNLESS OTHERWISE APPROVED BY THE RESIDENT ENGINEER, ANY CABLES DAMAGED BY THE CONTRACTOR SHALL BE IMMEDIATELY REPAIRED AT HIS EXPENSE. ANY NECESSARY TEMPORARY JUMPER CABLES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. CONTRACTOR IS REFERRED TO SECTION 70-17 OF THE STANDARD SPECIFICATIONS AND THE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAVE BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER OR THE DESIGN ENGINEER ASSUME ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED THAT THE LOCATIONS, SIZE AND TYPE MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE RESIDENT ENGINEER AND THE AIRPORT DIRECTOR. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT DIRECTOR.
- ALL AIRFIELD LIGHTING AND LIGHTING GUIDANCE SYSTEMS (NAVAIDS) LOCATED WITHIN AND IMMEDIATELY ADJACENT TO THE CONTRACTOR'S WORK ZONE SHALL BE CHECKED FOR OPERATIONAL CONDITION PRIOR TO THE DEPARTURE FROM THE AIRPORT WITH THE AIRPORT MANAGER. ANY DEFICIENCIES IN THESE SYSTEMS DUE TO THE ACTS OF CONTRACTOR OR HIS SUBCONTRACTORS, SUPPLIERS OR CONSULTANTS SHALL BE REPAIRED IMMEDIATELY.
- CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS AND TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP OR I.D.O..

### AIRFIELD LIGHTS AND SIGNS NOTES

- CONTRACTOR SHALL COVER ALL AIRFIELD SIGNS AND TAXIWAY LIGHTS ON CLOSED TAXIWAYS UNTIL THE TAXIWAY IS RE-OPENED FOR AIRCRAFT USE. THE METHOD AND MATERIALS USED TO COVER THE SIGNS AND LIGHTS SHALL MEET THE ENGINEER'S AND AIRPORT'S APPROVAL. COST INCIDENTAL TO THE CONTRACT. REMOVING LAMPS FROM ENERGIZED FIXTURES AS A MEANS TO REMOVE THE LIGHTS OR FIXTURES FROM SERVICE SHALL NOT BE ACCEPTABLE.
- CONTRACTOR SHALL TURN OFF RUNWAY EDGE LIGHTING REGULATOR AND LOCK-OUT/TAG-OUT CIRCUIT BREAKER AND CUT OUT INSIDE THE ELECTRICAL VAULT. DURING ALL RUNWAY CLOSURES, CONTRACTOR SHALL COORDINATE ACCESS TO THE VAULT WITH THE AIRPORT MANAGER/RESIDENT ENGINEER PRIOR TO RE-OPENING THE RUNWAY. THE CONTRACTOR SHALL COORDINATE WITH AIRPORT MANAGER/RESIDENT ENGINEER TO RE-ENERGIZE THE RUNWAY CIRCUIT.

PHASE	WORK AREA	ALLOWABLE WORK PERIOD	OPERATIONAL STATUS/RESTRICTIONS
1	1	AIRCRAFT RESTRICTIONS WITHIN WORK AREA. CAN NOT BE DONE CONCURRENTLY WITH WORK AREA 2	ALL RUNWAYS AND TAXIWAYS - OPEN APRONS SOUTH AND TERMINAL - OPEN WEST APRON - PARTIAL CLOSURE
	2	AIRCRAFT RESTRICTIONS WITHIN WORK AREA. CAN NOT BE DONE CONCURRENTLY WITH WORK AREA 1 AND 3	ALL RUNWAYS AND TAXIWAYS - OPEN APRONS SOUTH AND TERMINAL - OPEN WEST APRON - PARTIAL CLOSURE
	3	AIRCRAFT RESTRICTIONS WITHIN WORK AREA. CAN NOT BE DONE CONCURRENTLY WITH WORK AREA 2	ALL RUNWAYS AND TAXIWAYS - OPEN APRONS SOUTH AND TERMINAL - OPEN WEST APRON - PARTIAL CLOSURE
2	4	CONTRACTOR LIMITED TO 5 CALENDAR DAYS TO COMPLETE WORK CAN BE CONCURRENT W/ PHASE 1	RUNWAY 15/33 - OPEN TAXIWAYS B, C, M1 - OPEN TAXIWAY A AND C - PARTIAL CLOSURE RUNWAY 9/27 AND 18/36 - CLOSED TAXIWAY A3 - CLOSED

### LIMITATIONS ON CONSTRUCTION WITHIN RUNWAY SAFETY AREA (RSA) AND TAXIWAY/TAXILANE OBJECT FREE AREA (TOFA)

#### RUNWAYS:

THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT DIRECTOR TEN (10) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS. WORK SHALL BE EXPEDITED IN THESE AREAS AND AT THE END OF EACH WORKING DAY THESE AREAS SHALL BE SMOOTHLY GRADED TO ALLOW THE RUNWAY TO BE REOPENED UNLESS OTHERWISE SHOWN ON THE PLANS. AT LEAST ONE OF THE RUNWAYS SHALL REMAIN IN OPERATION AT ALL TIMES UNLESS OTHERWISE APPROVED BY THE AIRPORT DIRECTOR IN CONSULTATION WITH THE RESIDENT ENGINEER. IF NECESSARY STEEL PLATES SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR TO COVER ANY OPEN TRENCHES OR EXCAVATION WITHIN THE RSA IF DURING RUNWAY CLOSURE AN EMERGENCY IS DECLARED. THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE RUNWAY OF ALL VEHICLES, MEN AND EQUIPMENT. REFERENCE TABLE ON PREVIOUS SHEET FOR SAFETY AREA WIDTHS.

#### TAXIWAYS/TAXILANES:

ANY WORK WITHIN TAXIWAY OBJECT FREE AREA (TOFA) WILL REQUIRE A TAXIWAY CLOSURE. WORK WITHIN THE TOFA SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE TOFA. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT DIRECTOR FIVE (5) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS. REFERENCE TABLE ON PREVIOUS SHEET FOR OBJECT FREE AREA WIDTHS. NO DROP-OFFS OR OPEN EXCAVATIONS WILL BE ALLOWED WITHIN THE TAXIWAY SAFETY AREAS OF OPEN TAXIWAYS.

### CONTRACTOR CROSSING RUNWAY/TAXIWAY/TAXILANE/APRON AIR OPERATIONS AREA (A.O.A.)

- ANYTIME THE CONTRACTOR IS REQUIRED TO UTILIZE OR CROSS ACTIVE AIRFIELD PAVEMENTS FOR ACCESS TO AND FROM THE WORK ZONE, A CROSSING GUARD OR ESCORT IN RADIO CONTACT WITH THE CONTROL TOWER SHALL BE FURNISHED BY THE CONTRACTOR FOR MOVEMENTS OF VEHICLES OR EQUIPMENT TO AND FROM THE WORK ZONE. THE RADIO OPERATOR SHALL BE FAMILIAR WITH AIRPORT GROUND CONTROL PROCEDURES AND DEMONSTRATE KNOWLEDGE OF SAME TO THE AIRPORT. THE AIRPORT RESERVES THE RIGHT TO APPROVE THE CROSSING GUARDS. THE CONTRACTOR SHALL PROVIDE THEIR OWN RADIOS. THIS COST SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PAYMENT OF MUNICIPAL FINES (\$500 PER OCCURRENCE) DUE TO AIRFIELD INCURSIONS BY HIS EMPLOYEES, SUBCONTRACTORS, SUPPLIERS, CONSULTANTS AND/OR AGENTS.
- ANY PAVEMENT DAMAGED BY CONTRACTOR'S OPERATIONS SHALL BE REPAIRED IMMEDIATELY BY HIM TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT DIRECTOR AT NO ADDITIONAL COST TO THE OWNER. PAVEMENT SHALL BE CONTINUALLY SWEEPED TO PROVIDE DEBRIS FREE SURFACE DURING ALL HAUL ROAD OPERATIONS. THIS COST SHALL NOT BE PAID SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- WORK WITHIN THE A.O.A. SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE A.O.A. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE MEN AND EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT DIRECTOR TEN (10) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS.

DESIGN AIRCRAFT APPROACH CATEGORY: D  
DESIGN AIRPORT GROUP: III

GROUND CONTROL FREQUENCY: 121.70  
AIR CONTROL FREQUENCY: 120.60  
MAXIMUM ANTICIPATED HEIGHT  
OF CONSTRUCTION EQUIPMENT: 25'

### CONSTRUCTION POINT TABLE

POINT	NEAREST ACTIVE RUNWAY	LATITUDE	LONGITUDE	ELEVATION
A	CENTERLINE RUNWAY 18/36	41°46'00.91"	88°28'10.28"	701.3
B	CENTERLINE RUNWAY 18/36	41°46'01.02"	88°28'12.25"	700.9
C	CENTERLINE RUNWAY 15/33	41°46'12.79"	88°28'47.05"	695.1
D	CENTERLINE RUNWAY 15/33	41°46'12.92"	88°28'35.28"	695.9
E	CENTERLINE RUNWAY 15/33	41°46'08.87"	88°28'43.62"	693.3
F	CENTERLINE RUNWAY 18/36	41°46'00.00"	88°28'09.27"	701.8

CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE STANDARD SPECIFICATIONS SECTION 50-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS. IT IS ANTICIPATED THE FOLLOWING PROJECTS MAY BE UNDER CONSTRUCTION CONCURRENTLY WITH THIS PROJECT. NO ADDITIONAL COMPENSATION SHALL BE CONSIDERED FOR ANY EFFORTS TO COORDINATE AND ACCESS TO THE SITE DUE TO ADJACENT CONSTRUCTION.

- PHASE 2 : AIRFIELD PAVEMENT REHABILITATION INCLUDING GENERAL AVIATION APRON AND TAXIWAY P

IL CONTRACT: **AU070**  
IL LETTING ITEM: **02A**  
IL PROJECT: **ARR-4352**  
S.B.G. PROJECT: **3-17-SBGP-156**

SURVEY BOOK #    BOOK #

### REVISIONS

NUMBER	BY	DATE

0    1    2  
THIS BAR IS EQUAL TO 2"  
AT FULL SCALE (34X22).

AURORA MUNICIPAL AIRPORT  
AURORA, ILLINOIS  
AIRFIELD PAVEMENT REHABILITATION, PHASE 3 (AREA 1)

SEQUENCE OF CONSTRUCTION  
GENERAL NOTES

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DESIGN BY: ARM  
DRAWN BY: JRO  
CHECKED BY: ARM  
APPROVED BY: DKP  
DATE: 1/10/2020  
JOB No: 190285-02-00

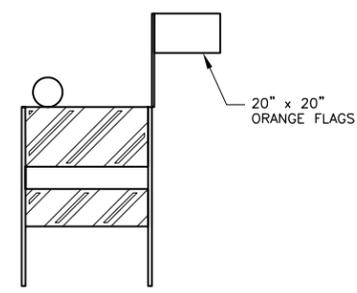
**FINAL**

XREF DWG: Detail\_01.dwg

IMAGE FILES: Aurora Municipal Airport Logo FINAL PMS.png

UPDATE BY: Tony Mafrin  
LAYOUT: Layout

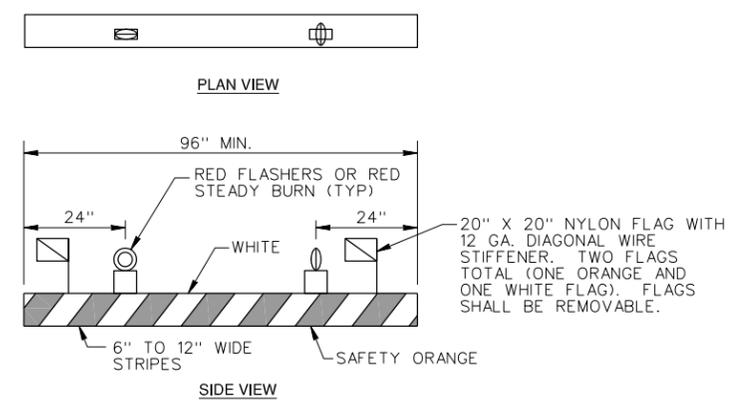
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FILE: K:\Aurora\19228502-00\_Pavement\Aurora\Draw\Sheets\19228502-7-Sequence of Construction Details.dwg



**FLASHER BARRICADE DETAIL - IDOT TYPE 2  
(WORK AREAS 1 - 3)**  
NOT TO SCALE

**FLASHER BARRICADE NOTES**

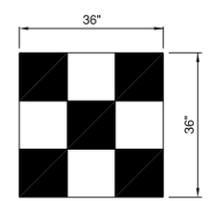
1. FLASHERS TO BE BATTERY OPERATED. LENS TO BE RED AND BE ABLE TO ROTATE 90 DEGREES.
2. SANDBAGS TO BE PLACED ON EACH SUPPORT BRACE AS REQUIRED TO PREVENT DISPLACEMENT BY WIND, JET OR PROP BLAST.
3. CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING BARRICADES AT ALL TIMES TO THE SATISFACTION OF THE AIRPORT.
4. NO SEPARATE PAYMENT WILL BE MADE FOR THIS ITEM. COSTS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
5. PLACE BARRICADES AT 4' INTERVALS.
6. ALTERNATE FLASHER LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.



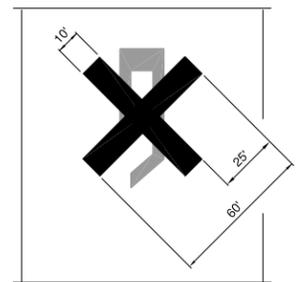
**AIRSIDE LOW PROFILE LIGHTED BARRICADE  
(WORK AREAS 1 - 4)**  
NOT TO SCALE

**BARRICADE NOTES**

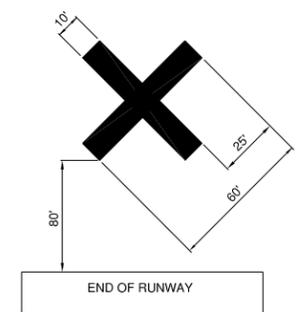
1. FLASHER OR STEADY BURN LIGHTS SHALL BE BATTERY OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
2. FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
3. BARRICADES TO BE PLACED WITH A MAXIMUM OF 4' SPACING END TO END UP TO THE EDGE OF PAVEMENT ALONG OPERATIONAL PAVEMENT ADJACENT TO CONSTRUCTION AS DIRECTED BY THE RESIDENT ENGINEER. ALTERNATE FLASHER OR STEADY BURN LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
4. FLASHER OR STEADY BURN LIGHTS SHALL BE SECURED TO THE BARRICADES, AS APPROVED BY THE RESIDENT ENGINEER.
5. BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF IT COMPONENTS, AND WEIGHTED TO AVOID BEING BLOWN OVER.
6. BARRICADES SHALL BE OF A COMMERCIAL DESIGN AND SHALL MEET CURRENT FAA REQUIREMENTS.
7. PLACE ALL BARRICADES OUTSIDE RUNWAY SAFETY AREAS AND OUTSIDE TAXIWAY OBJECT FREE AREAS.
8. ALL COST ASSOCIATED WITH THE LOW PROFILE BARRICADES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.



**CONSTRUCTION EQUIPMENT  
AND TRUCK SIGNAL FLAG**  
NOT TO SCALE



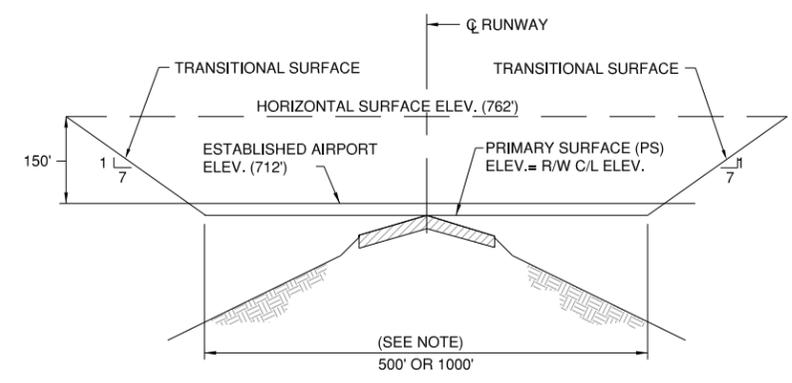
**ON PAVEMENT  
CLOSED RUNWAY MARKER DETAIL**  
NOT TO SCALE



**OFF PAVEMENT  
CLOSED RUNWAY MARKER DETAIL**  
NOT TO SCALE

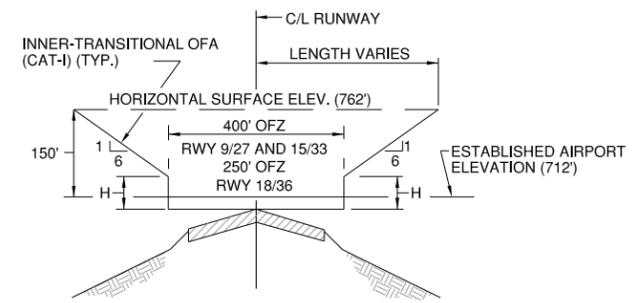
**CLOSED RUNWAY MARKER DETAIL NOTES**

1. CLOSED RUNWAY MARKERS SHALL BE YELLOW.
2. MARKERS SHALL BE MATERIAL APPROVED BY THE ENGINEER.
3. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION
4. MARKERS ON PAVEMENT SHALL BE PLACED OVER EXISTING RUNWAY NUMERALS AS SHOWN.
5. COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
6. DURING VARIOUS PHASES OF WORK, IT WILL BE NECESSARY TO CLOSE RUNWAYS TO AIR TRAFFIC ON A TEMPORARY BASIS AS COORDINATED WITH THE AIRPORT AND TOWER PERSONNEL. THE CONTRACTOR SHALL MARK THE RUNWAYS TO BE CLOSED BY PLACING A YELLOW CROSS AT THE LOCATION AND DIMENSIONS DETAILED ON THIS SHEET. THE CROSSES ARE SHOWN ON THE RESPECTIVE RUNWAYS ACCORDING TO THE VARIOUS PHASES OF WORK AS DELINEATED IN THE SUGGESTED SEQUENCE OF CONSTRUCTION.



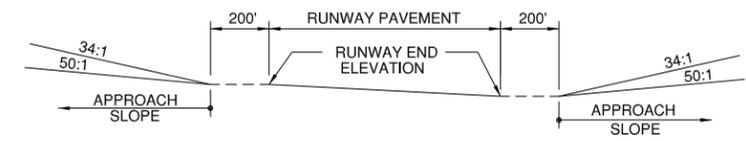
**TYPICAL SECTION  
F.A.R. PART 77 IMAGINARY SURFACES**  
NO SCALE

**NOTE:**  
IMAGINARY SURFACE REQUIREMENTS FOR EXISTING ACTIVE RUNWAYS (R/W) ARE SIMILAR EXCEPT PRIMARY SURFACE (PS) DIMENSIONS VARY  
R/W 18-36 500' PS (250' LT & RT OF C)  
R/W 9-27, 15/33 1000' PS (500' LT & RT OF C)



**TYPICAL SECTION  
OBSTACLE FREE ZONE (OFZ)**  
NO SCALE

RUNWAY	TYPE OF RUNWAY	H (FEET)
9-27	PRECISION (CAT I)	43
15-33 18-36	NON-PRECISION	150



**TYPICAL PROFILE  
F.A.R. PART 77 IMAGINARY SURFACES**  
NO SCALE

RUNWAY END	ELEVATION	APPROACH SLOPE
9	707	50:1
27	706	34:1
18	700	20:1
36	700	20:1
15	712	34:1
33	699	34:1

IL CONTRACT: **AU070**  
IL LETTING ITEM: **02A**  
IL PROJECT: **ARR-4352**  
S.B.G. PROJECT: **3-17-SBGP-156**

SURVEY BOOK #    BOOK #

REVISIONS		
NUMBER	BY	DATE

0    1    2  
THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**AURORA MUNICIPAL AIRPORT**  
**AURORA, ILLINOIS**  
**AIRFIELD PAVEMENT REHABILITATION, PHASE 3 (AREA 1)**  
**SEQUENCE OF CONSTRUCTION GENERAL DETAILS**

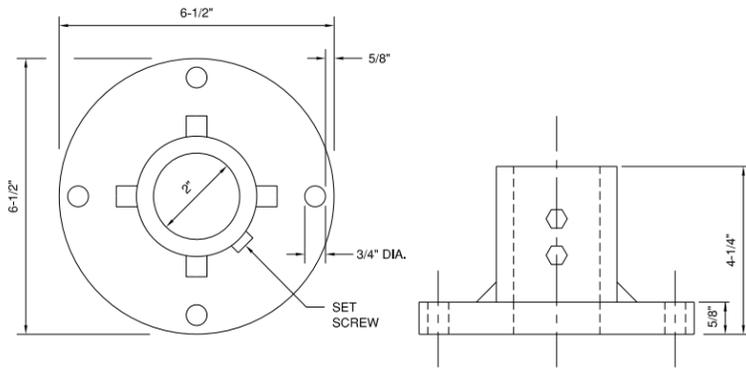
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**AURORA**  
MUNICIPAL AIRPORT

DESIGN BY:	ARM
DRAWN BY:	JRO
CHECKED BY:	ARM
APPROVED BY:	DKP
DATE:	1/10/2020
JOB No:	190285-02-00

DATE: Friday, January 10, 2020 6:29:10 PM  
 FILE: K:\Aurora\19028502-00\_PaveRehab\Draw\Sheets\19028502-8\_Typical Sections.dwg  
 UPDATE BY: Tony Mearn  
 LAYOUT: Layout  
 IMAGE FILES: Aurora Municipal Airport Logo FINAL PMS.spc  
 XREF DWG: Detail\_anchoring

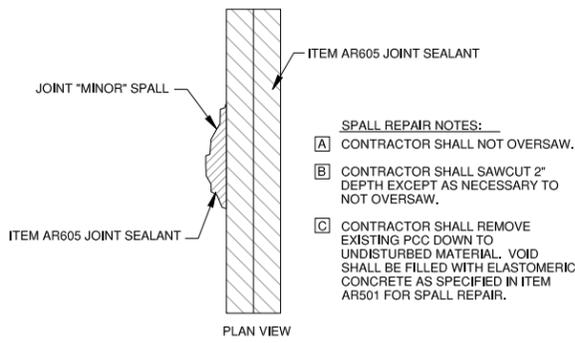


**CHAIN LINK FENCE FLOOR FLANGE**

NOT TO SCALE

**NOTES**

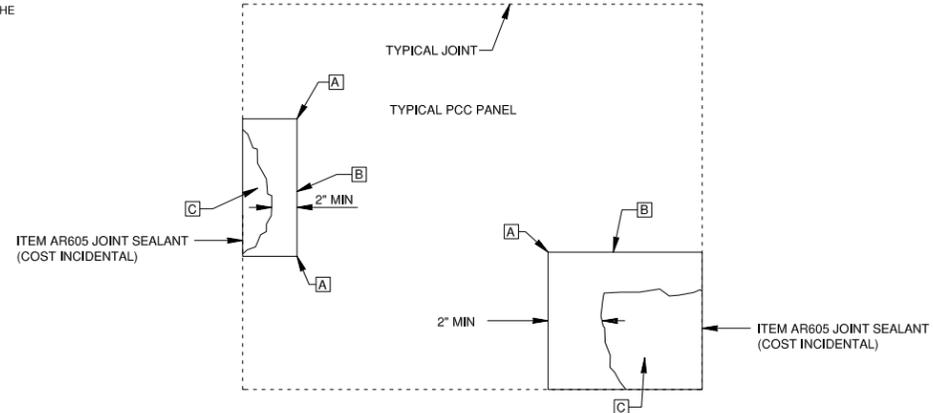
- FLOOR FLANGE SHALL BE GALVANIZED IN ACCORDANCE WITH PROJECT SPECIFICATIONS.
- EACH FLANGE SHALL REQUIRE A MINIMUM OF 2 SET SCREWS TO SECURE FENCE POST.
- EACH FLANGE SHALL BE ANCHORED TO THE PCC PAVEMENT WITH 4 - ZINC PLATED 1/2" x 4-1/4" WEDGE ANCHORS.
- CONTRACTOR MAY SUBMIT ALTERNATE/MODIFIED FLOOR FLANGES FOR REVIEW AND APPROVAL BY THE RESIDENT ENGINEER.



**MINOR SPALL REPAIR DETAIL**

NOT TO SCALE

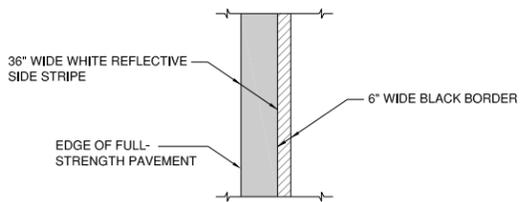
NOTE: FOR MINOR SPALLS ADJACENT TO PCC PAVEMENTS, CONTRACTOR SHALL FILL IN WITH JOINT SEALANT AT NO ADDITIONAL COST TO THE CONTRACT.



**PLAN VIEW SPALL REPAIR DETAIL**

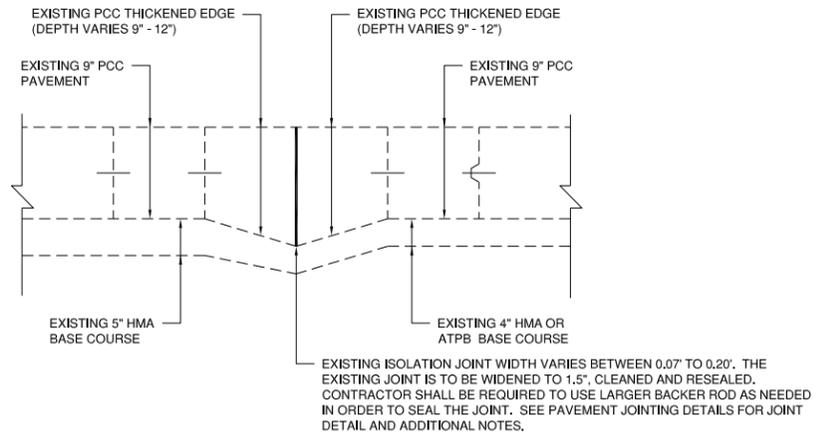
NOT TO SCALE

NOTE: SPALL REPAIR MATERIAL SHALL CONFORM TO THE REQUIREMENTS OF ASTM C881, TYPE III



**RUNWAY EDGE MARKING CONTINUOUS**

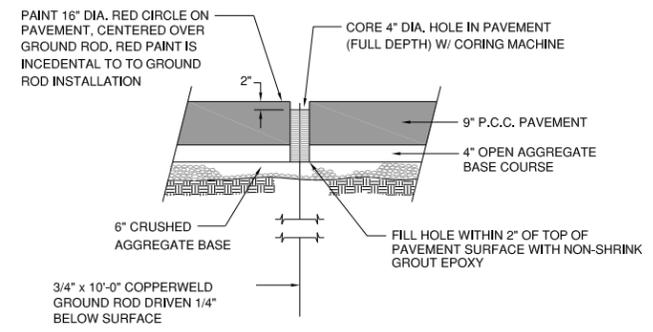
NOT TO SCALE



**WEST APRON - ISOLATION JOINT TYPICAL SECTION A-A**

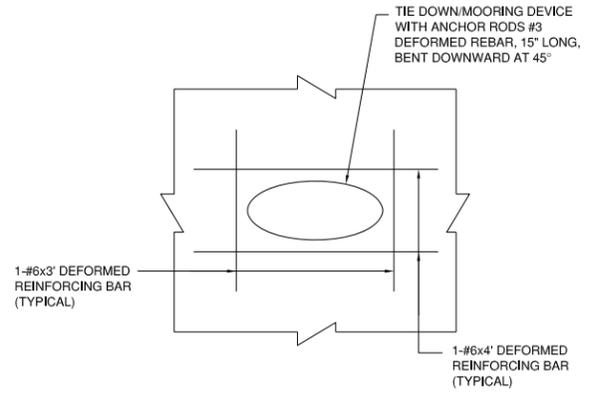
NOT TO SCALE

NOTE: SEE SHEET 9 FOT TYPICAL SECTION A-A



**GROUND ROD DETAIL**

NOT TO SCALE



**TIE DOWN REINFORCING DETAIL**

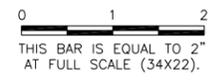
NOT TO SCALE

**TIE DOWN NOTES**

- THESE #6 REINFORCING BARS SHOULD BE PLACED 3" FROM MOORING DEVICE AND 4.5" BELOW PAVEMENT SURFACE.
- THE ENDS OF REINFORCING BARS SHOULD BE PLACED 3" FROM PAVING JOINTS TO PROVIDE COVER.
- MOORING EYES SHALL BE DUCTILE OR GREY IRON, RATED FOR AIRPORT USE AND SHALL HAVE A MINIMUM HEIGHT OF 3.5'.

IL. CONTRACT: **AU070**  
 IL. LETTING ITEM: **02A**  
 IL. PROJECT: **ARR-4352**  
 S.B.G. PROJECT: **3-17-SBGP-156**

REVISIONS		
NUMBER	BY	DATE



AURORA MUNICIPAL AIRPORT  
 AURORA, ILLINOIS  
 AIRFIELD PAVEMENT REHABILITATION, PHASE 3 (AREA 1)

**TYPICAL SECTIONS/  
 PAVEMENT REHABILITATION DETAILS**

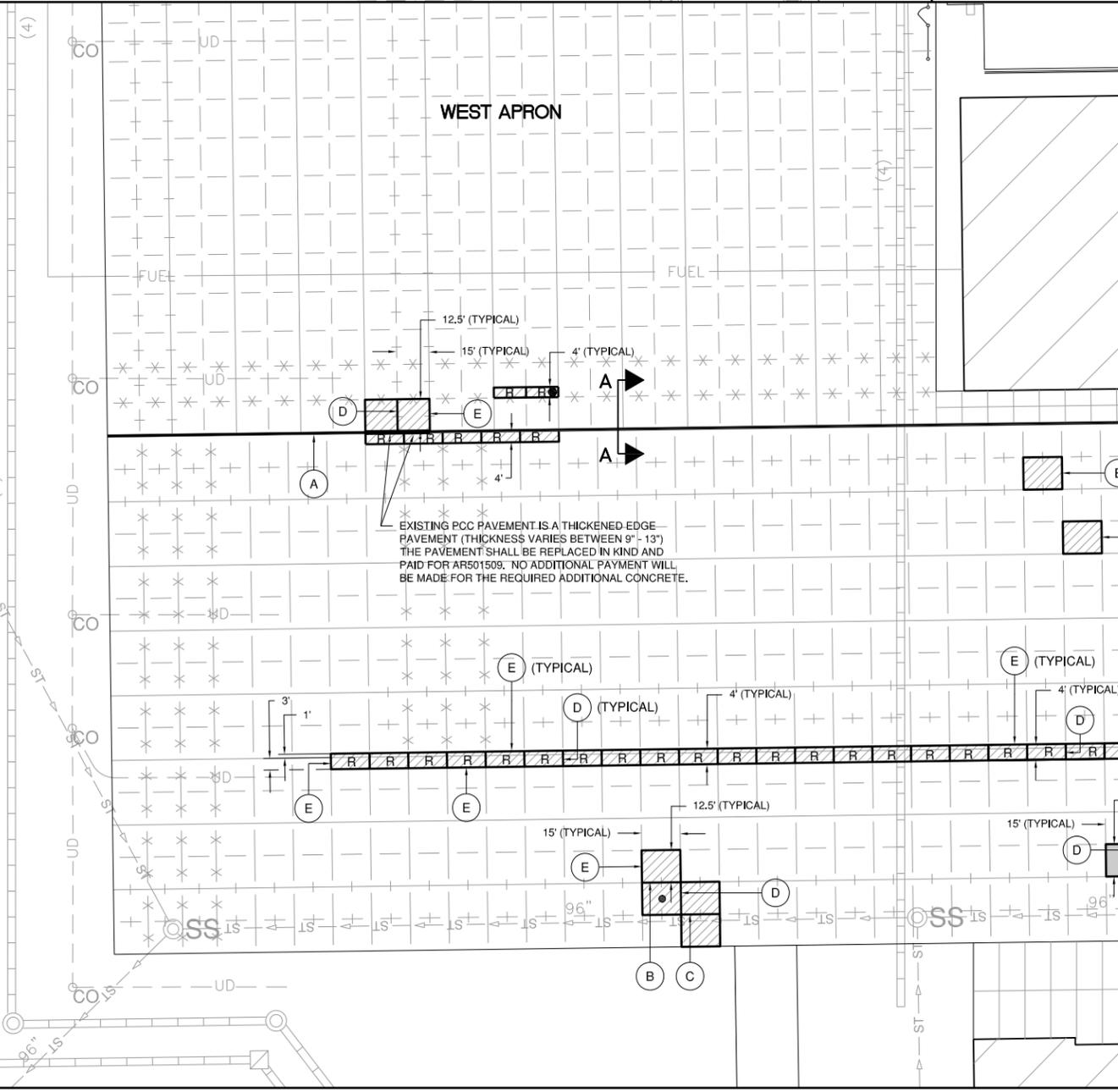
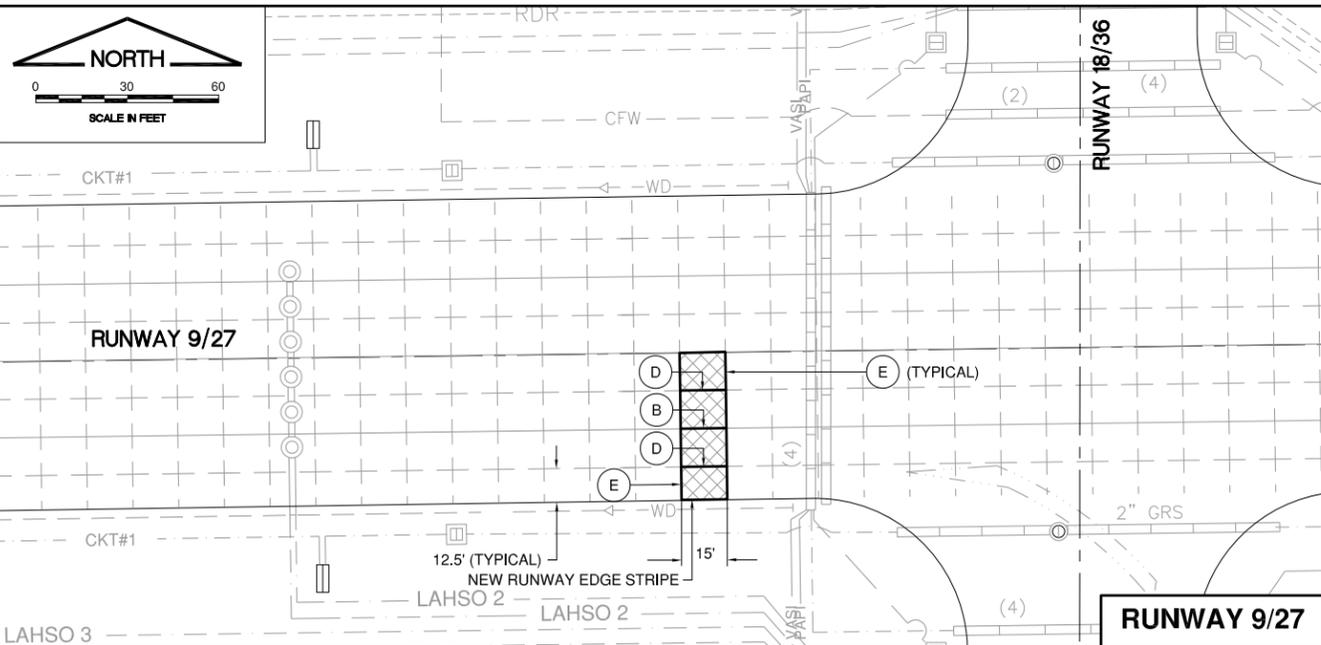
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DRAWN BY:	JRO
CHECKED BY:	ARM
APPROVED BY:	DKP
DATE:	1/10/2020
JOB No:	190285-02-00

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DATE: Friday, January 10, 2020 6:29:20 PM  
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 UPDATE BY: Tony Merin  
 LAYOUT: Layout1  
 IMAGE FILES: Aurora Municipal Airport Logo FINAL PMS.smg  
 REF: DWG: 190285-02-00.dwg  
 190285-02-00.dwg



### LEGEND

- EXISTING 9"± PCC PAVEMENT AND 4"± HMA BASE OR ASPHALT TREATED PERMEABLE BASE COURSE TO BE REMOVED (AR501900) AND REPLACED WITH 9" PCC PAVEMENT (AR501509) AND 4" CRUSHED AGGREGATE BASE COURSE (AR209604)(BASE BID)
- EXISTING 9"± PCC PAVEMENT AND 4"± ASPHALT TREATED PERMEABLE BASE COURSE TO BE REMOVED (AR501900) AND REPLACED WITH 9" PCC PAVEMENT (AR501509), 4" CRUSHED AGGREGATE BASE COURSE (AR209604) AND 12" POROUS GRANULAR EMBANKMENT (AR208515)(BASE BID)
- EXISTING 9"± PCC PAVEMENT AND 4"± HMA BASE COURSE TO BE REMOVED (AT501900) AND REPLACED WITH 9" PCC PAVEMENT (AT501509), 4" CRUSHED AGGREGATE BASE COURSE (AT209604)(ADDITIVE ALTERNATE NO. 2)
- EXISTING 9"± PCC PAVEMENT TO BE REMOVED (AS501900) AND REPLACED WITH 9" PCC PAVEMENT (AS800075)(ADDITIVE ALTERNATE NO. 1)
- EXISTING INLET/UTILITY AND BOX OUT TO BE RECONSTRUCTED (AR751980/AT751995)
- WIDEN, CLEAN AND SEAL ISOLATION JOINT
- NEW TIEDOWN (AR510510)
- NEW GROUND ROD (AR510515)
- NEW TYPE B - HINGED CONTRACTION JOINT (TIED)
- NEW TYPE C - DOWELED CONTRACTION JOINT
- NEW TYPE D - DUMMY CONTRACTION JOINT
- NEW TYPE E - DOWELED CONTRACTION JOINT
- REINFORCED PANEL

### NOTE

1. NEW ISOLATION JOINT INSTALLED ADJACENT OR WITHIN NEW PCC PAVEMENT SHALL BE INCIDENTAL TO NEW PCC PAVEMENT.
2. SEE SHEET 2 FOR ADDITIONAL NOTES.

THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.

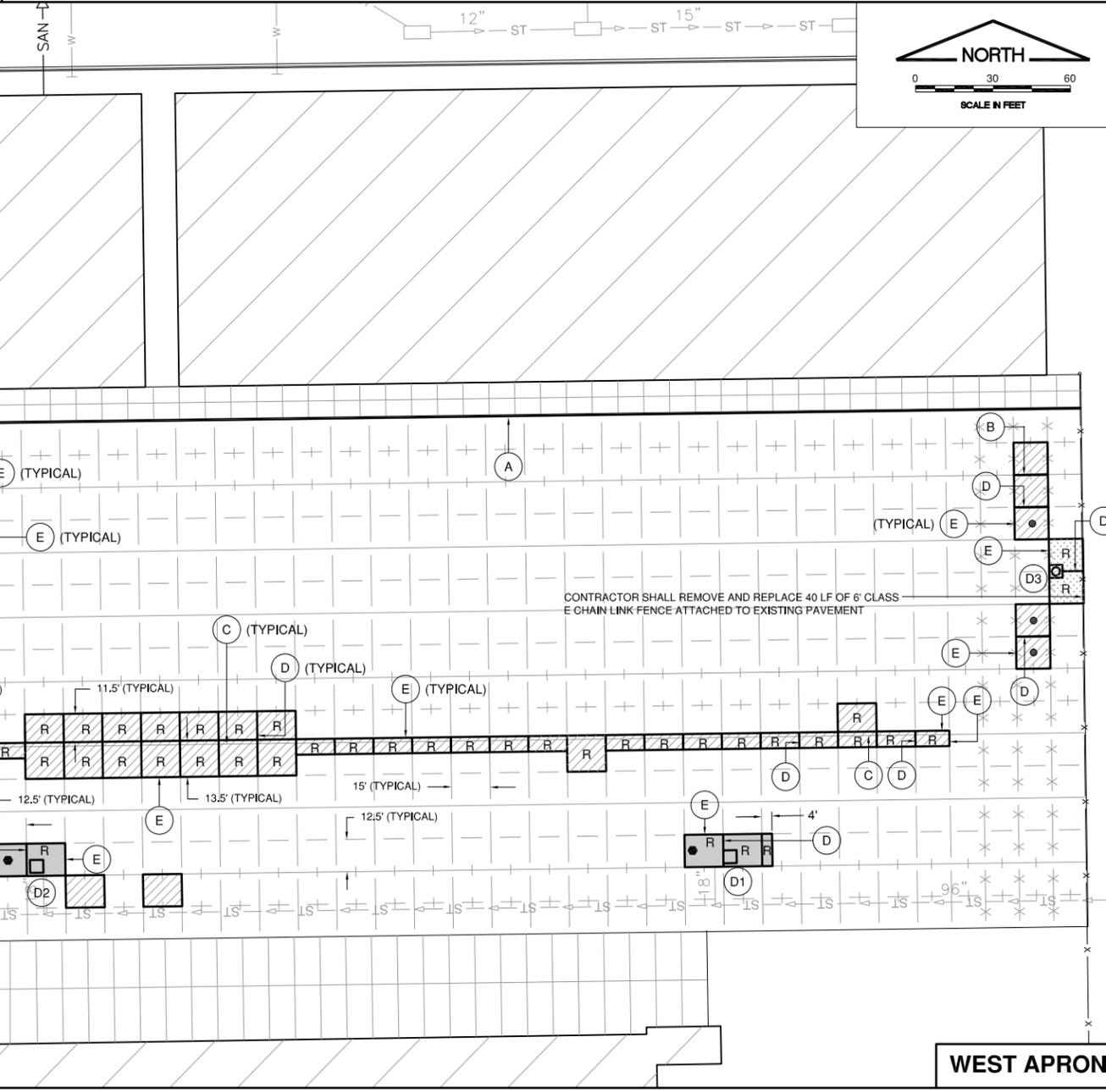
### KEY MAP

IL CONTRACT: **AU070**  
 IL LETTING ITEM: **02A**  
 IL PROJECT: **ARR-4352**  
 S.B.G. PROJECT: **3-17-SBGP-156**

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2  
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).



AURORA MUNICIPAL AIRPORT  
 AURORA, ILLINOIS  
 AIRFIELD PAVEMENT REHABILITATION, PHASE 3 (AREA 1)

## PROPOSED IMPROVEMENTS

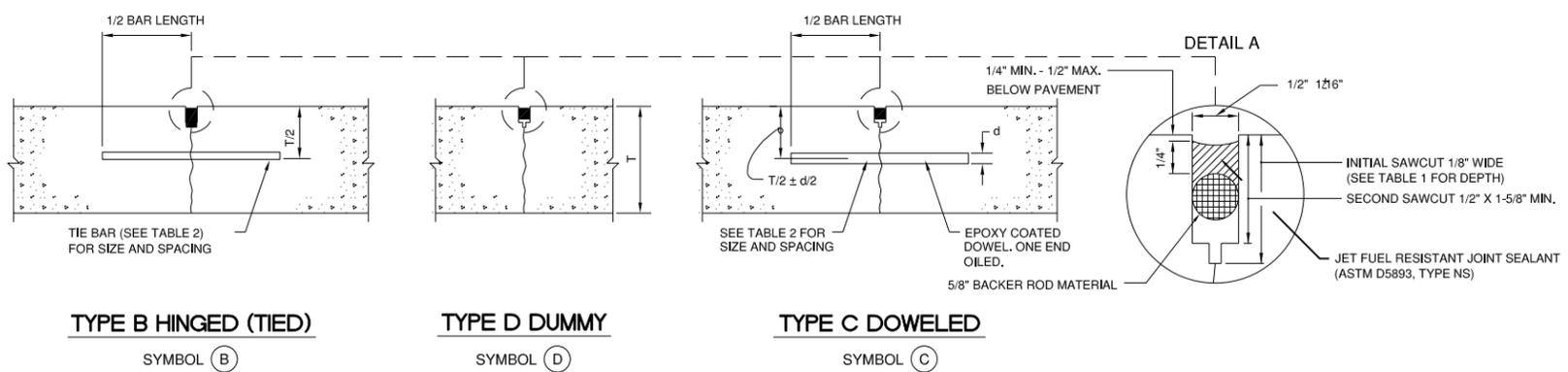
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 APPROVED BY: DKP  
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SHEET 9 OF 12 SHEETS

XREF DWG: Detail - Jointing  
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 LAYOUT: Layout  
 DATE: Friday, January 10, 2020 6:29:27 PM  
 FILE: K:\Aurora\192285-00\_PaveRehab\DrawSheets\192285-11 - Jointing Details.dwg



**TYPE B HINGED (TIED)**  
SYMBOL (B)

**TYPE D DUMMY**  
SYMBOL (D)

**TYPE C DOWELED**  
SYMBOL (C)

**JOINT NOTES**

- ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
- THE INITIAL SAWCUT FOR ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS SHALL BE SAWS AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT. SAWING OF LONGITUDINAL CONTRACTION JOINTS ADJACENT TO THE THICKENED EDGES SHALL BE GIVEN PRIORITY OVER OTHER LONGITUDINAL JOINT SAWING.
- ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY, WHICH WILL INSURE THAT THEY WILL REMAIN PARALLEL TO THE PAVEMENT LANES. THE DOWEL BAR ASSEMBLIES SHALL BE APPROVED BY THE ENGINEER PRIOR TO INSTALLATION.
- ALL TIE BARS AND MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR PLACED BY OTHER APPROVED METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT.
- TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH ASTM A615 OR ASTM A616, EXCEPT THAT RAIL STEEL BARS, GRADE 50 OR 60 SHALL NOT BE USED FOR THE BARS THAT ARE TO BE BENT OR RESTRAIGHTENED DURING CONSTRUCTION. TIE BARS DESIGNATED AS GRADE 40 IN ASTM A615 CAN BE USED FOR CONSTRUCTION REGARDING BENT BARS.
- THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSION OF THE SECOND SAWCUT WILL NOT BE ALLOWED.
- JOINTS SHALL BE DRY AND CLEAN BEFORE SEALING OPERATIONS BEGIN.
- COST OF ALL JOINT SAWING, CLEANING AND SEALING OF NEW CONCRETE PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ASSOCIATED PAY ITEM AND NO SEPARATE PAYMENT SHALL BE MADE.
- SHOULD THE POURING OPERATIONS REQUIRE THE INSERTION OF AN INTERMEDIATE HEADER, A DOWEL BASKET ASSEMBLY OR OTHER APPROVED METHOD OF DOWEL BAR PLACEMENT SHALL BE REQUIRED.
- DOWEL BASKET ASSEMBLIES MEETING IDOT APPROVAL MAY BE PROPOSED BY THE CONTRACTOR TO BE APPROVED BY THE ENGINEER. DOWELS IN THE APPROVED BASKET ASSEMBLIES SHALL CONFORM TO TABLE 2.

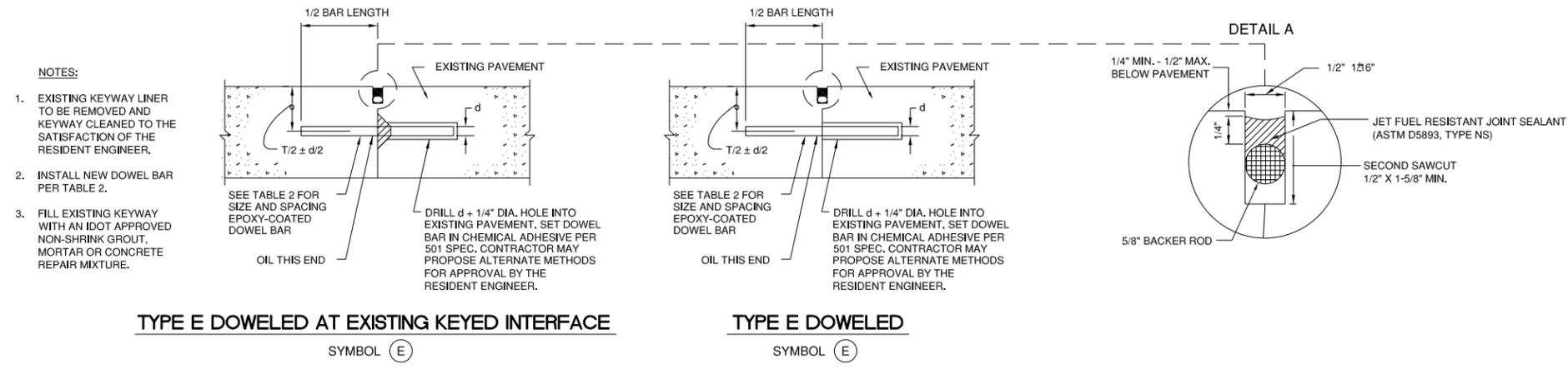
IL CONTRACT: **AU070**  
 IL LETTING ITEM: **02A**  
 IL PROJECT: **ARR-4352**  
 S.B.G. PROJECT: **3-17-SBGP-156**

SURVEY BOOK #

REVISIONS		
NUMBER	BY	DATE

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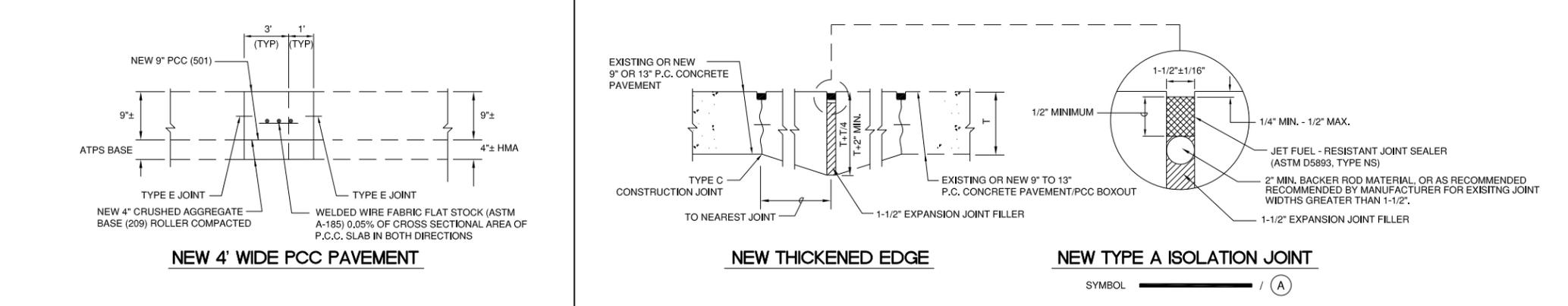
**CONTRACTION JOINTS**



**TYPE E DOWELED AT EXISTING KEYED INTERFACE**  
SYMBOL (E)

**TYPE E DOWELED**  
SYMBOL (E)

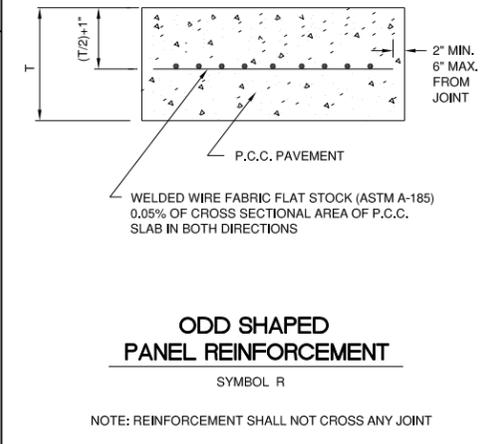
**CONSTRUCTION JOINTS**



**NEW 4' WIDE PCC PAVEMENT**

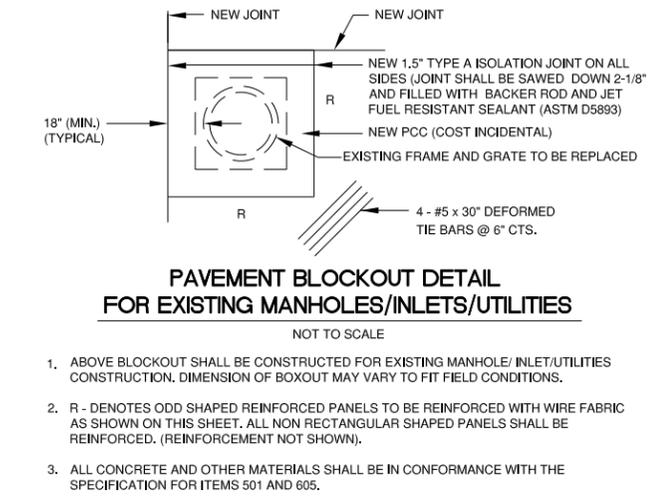
**NEW THICKENED EDGE**

**NEW TYPE A ISOLATION JOINT**  
SYMBOL / (A)



**ODD SHAPED PANEL REINFORCEMENT**  
SYMBOL R

NOTE: REINFORCEMENT SHALL NOT CROSS ANY JOINT



**PAVEMENT BLOCKOUT DETAIL FOR EXISTING MANHOLES/INLETS/UTILITIES**  
NOT TO SCALE

- ABOVE BLOCKOUT SHALL BE CONSTRUCTED FOR EXISTING MANHOLE/ INLET/UTILITIES CONSTRUCTION. DIMENSION OF BOXOUT MAY VARY TO FIT FIELD CONDITIONS.
- R - DENOTES ODD SHAPED REINFORCED PANELS TO BE REINFORCED WITH WIRE FABRIC AS SHOWN ON THIS SHEET. ALL NON RECTANGULAR SHAPED PANELS SHALL BE REINFORCED. (REINFORCEMENT NOT SHOWN).
- ALL CONCRETE AND OTHER MATERIALS SHALL BE IN CONFORMANCE WITH THE SPECIFICATION FOR ITEMS 501 AND 605.

**NOTES**

- THE CONTRACTOR SHALL VISIT THE PROJECT SITE AND ACQUAINT HIMSELF WITH THE REQUIRED WORK. CONTRACTOR SHALL BE FAMILIAR TO THE SITE AND REQUIRED WORK PRIOR TO BID. SITE VISITS SHALL BE COORDINATED WITH THE AIRPORT MANAGER AT (630) 256-3120.
- PRIOR TO ORDERING NEW MATERIAL, CONTRACTOR SHALL FIELD MEASURE FOR JOINTING MATERIAL SIZES AND AMOUNT. MEASUREMENTS SHALL ALSO DETERMINE BACKER ROD SIZE AND QUANTITIES. ALSO PER NEW JOINTS DETAILS, DEPTH OF NEW JOINT SEALANT SHALL BE AS RECOMMENDED BY MANUFACTURER. NO EXTRA COMPENSATION WILL BE MADE FOR EXCESS MATERIALS NOT USED.
- FOR VARYING JOINT WIDTHS AND DEPTHS, THERE SHALL BE NO ADDITIONAL COST FOR DEEPER SAWCUTS, LARGER BACKER RODS AND SEALANT MATERIAL USED.

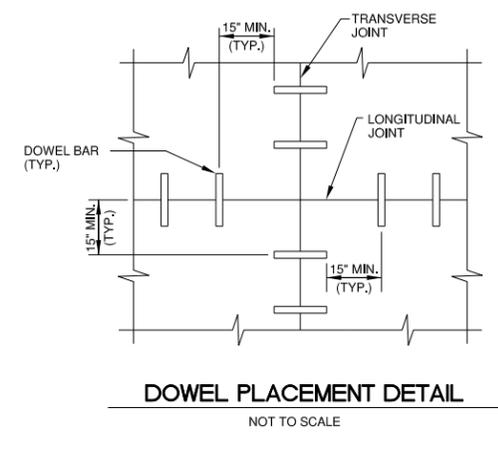
**TABLE 1**

PAVEMENT THICKNESS T - INCHES	DEPTH OF CONTRACTION JOINT INITIAL SAW CUT T, INCHES T=(T/4) ±1/4"
9	2.25"
10	2.50"
11	2.75"
12	3"
13	3.25"

**TABLE 2**

PAVEMENT THICKNESS T - INCHES	DOWEL BAR DETAILS			TIE BAR DETAILS		
	DIA. (d)	LENGTH	SPACING	BAR SIZE	LENGTH	SPACING
8" - 12"	1"	19"	12"	#5	30"	30"

**DIMENSION TABLES**



**DOWEL PLACEMENT DETAIL**  
NOT TO SCALE

**AURORA MUNICIPAL AIRPORT**  
**AURORA, ILLINOIS**  
**AIRFIELD PAVEMENT REHABILITATION, PHASE 3 (AREA 1)**  
**PAVEMENT JOINTING DETAILS**

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 DATE: 1/10/2020  
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**FINAL**

SHEET 10 OF 12 SHEETS

# STORM WATER POLLUTION PREVENTION NOTES

THE FOLLOWING IS TO ESTABLISHED BEST MANAGEMENT PRACTICES TO BE INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

## SITE DESCRIPTION

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF RECONSTRUCTING EXISTING HMA PAVEMENTS, PCC PAVEMENT REPLACEMENT, TOPSOIL PLACEMENT, SEEDING AND MULCHING AT AURORA MUNICIPAL AIRPORT.

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS EXCAVATION AND GRADING:

1. INSTALL TEMPORARY EROSION CONTROL MEASURES.
2. INSTALL CONCRETE WASHOUT AT CONTRACTOR STAGING AREA.
3. REMOVE EXISTING PAVEMENTS.
4. CONSTRUCT NEW PAVEMENTS.
5. FINAL GRADING AND OTHER MISCELLANEOUS ITEMS.
6. PLACEMENT OF PERMANENT EROSION CONTROL, SUCH AS SEEDING AND MULCH IF REQUIRED.

## AREA OF CONSTRUCTION SITE

THE TOTAL AREA OF THE CONSTRUCTION SITE THAT WILL BE DISTURBED BY EXCAVATION, GRADING AND OTHER ACTIVITIES IS LESS THAN ONE (1) ACRE.

## DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION SITE DRAINS INTO STORM SEWERS THAT OUTLET INTO THE WELCH CREEK.

## SEDIMENTATION AND EROSION CONTROL NOTES

THE RESIDENT ENGINEER WILL PERFORM PERIODIC INSPECTION OF THE SITE TO IDENTIFY POTENTIAL SEDIMENT AND EROSION ISSUES.

ALL SOIL EROSION AND SEDIMENT CONTROL PRACTICES ARE REFERENCED FROM THE ILLINOIS URBAN MANUAL, UNLESS OTHERWISE STATED.

THE DRAWINGS, SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE SEEDING AND MULCHING AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED WITHIN SEVEN (7) DAYS OF DISTURBANCE.

## DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

1. WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.
2. EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTORS EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN FOURTEEN DAYS.
3. AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:
  - A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.
  - B. EXCAVATED AREAS AND EMBANKMENT AREAS SHALL BE PERMANENTLY SEEDED IMMEDIATELY AFTER FINAL GRADING. IF NOT, THEY SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S COST, IF NO CONSTRUCTION ACTIVITY IN THE AREA IS PLANNED FOR SEVEN DAYS.
4. CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.
5. SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF OFF SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCIDENTAL TO THE COST OF THE PROJECT.
6. THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCIDENTAL TO THE CONTRACT.

## DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

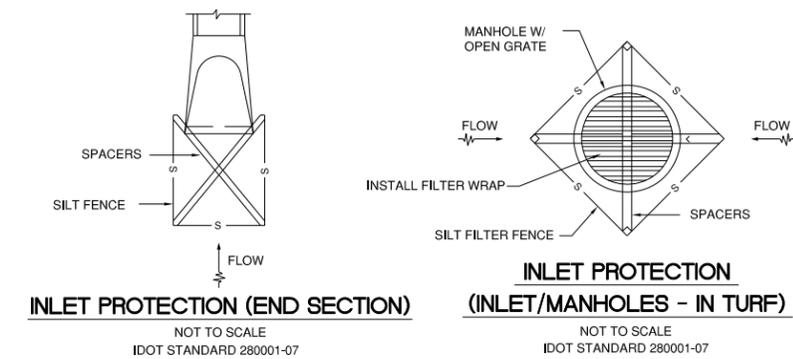
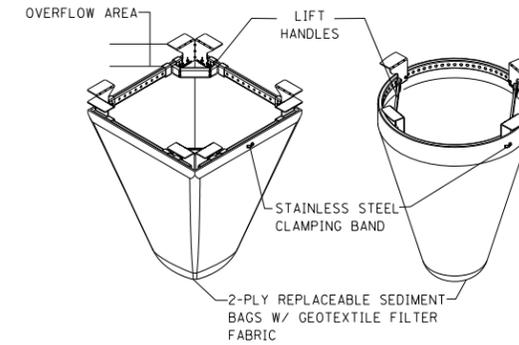
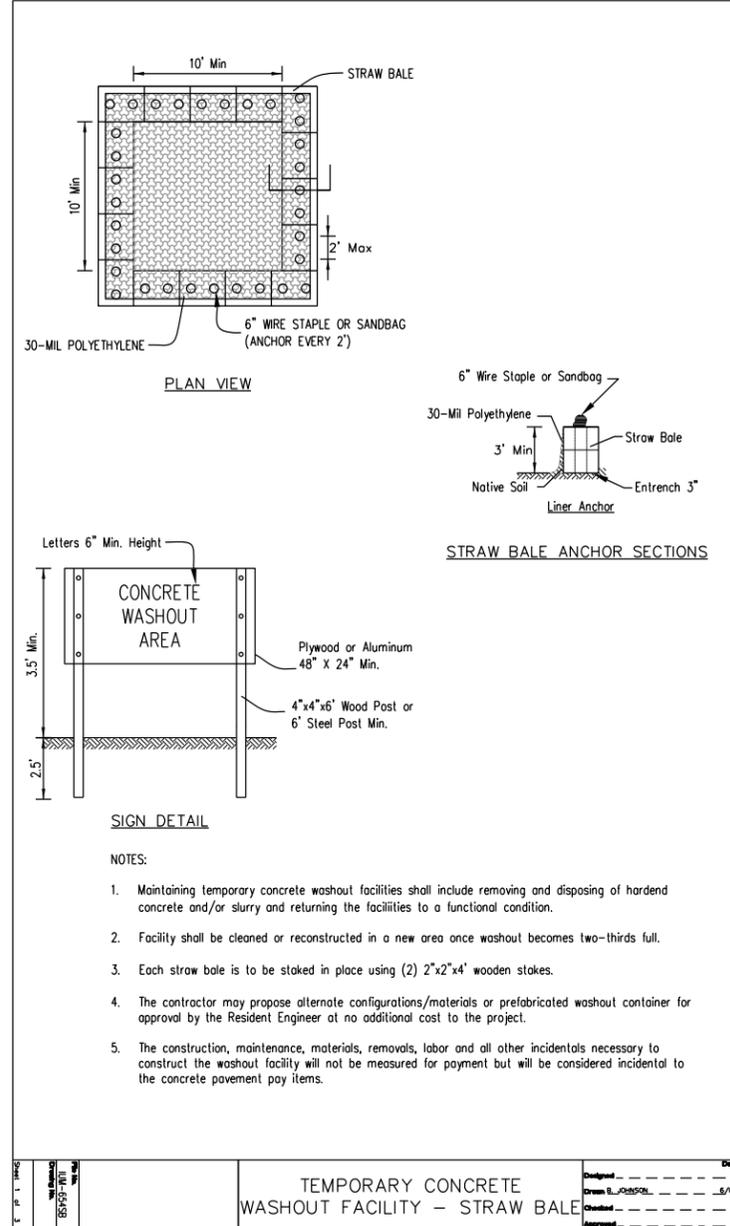
TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED.

ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RESEEDED.

## MAINTENANCE AFTER CONSTRUCTION

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

AUTOCAD2006



## NOTES FOR INLET PROTECTION DETAILS

1. FILTER WRAP TO BE PLACED IN ALL SLOPE BOX INLETS, INLETS, MANHOLES, TRENCH DRAINS AND CATCH BASINS LOCATED IN PAVED AREAS AND NONPAVED AREAS.
2. FABRIC SHALL BE IN CONFORMANCE WITH ARTICLE 1080.03 OF THE IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION 2016.
3. FABRIC SHALL OVERLAY FRAME BY 2" (MIN.).
4. CONTRACTOR SHALL CLEAR DEBRIS AND SILT AS REQUIRED FROM FABRIC TO MAINTAIN DRAINAGE THROUGH THE STRUCTURE.
5. FABRIC SHALL REMAIN IN PLACE UNTIL COMPLETION OF PAVEMENT REHABILITATION.
6. COST OF INLET PROTECTION, FILTER WRAP AND MAINTENANCE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

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 IL LETTING ITEM: **02A**  
 IL PROJECT: **ARR-4352**  
 S.B.G. PROJECT: **3-17-SBGP-156**

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**AURORA MUNICIPAL AIRPORT**  
**AURORA, ILLINOIS**  
**AIRFIELD PAVEMENT REHABILITATION, PHASE 3 (AREA 1)**

**STORM WATER POLLUTION PREVENTION**  
**NOTES AND DETAILS**

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