

143

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2019-120-RS&SW	COOK	# 28	1
* OR 308 & FAU 2885			ILLINOIS	CONTRACT NO. 62J66

\* 28 + 1 = 29 TOTAL SHEETS

D-91-233-20



FOR INDEX OF SHEETS, SEE SHEET NO. 2

# PROPOSED HIGHWAY PLANS

O.R. 308 / F.A.U. 2885: CANAL STREET  
 ARCHER AVENUE TO 28TH STREET  
 SECTION 2019-120-RS&SW  
 PROJECT STP-HSXV(975)  
 DESIGNED OVERLAY & ADA IMPROVEMENTS  
 COOK COUNTY  
 C-91-013-20

**TRAFFIC DATA**

EXISTING ADT = 11500 (2014)  
 POSTED SPEED LIMIT = 30 MPH

THE IMPROVEMENT IS LOCATED IN  
THE CITY OF CHICAGO

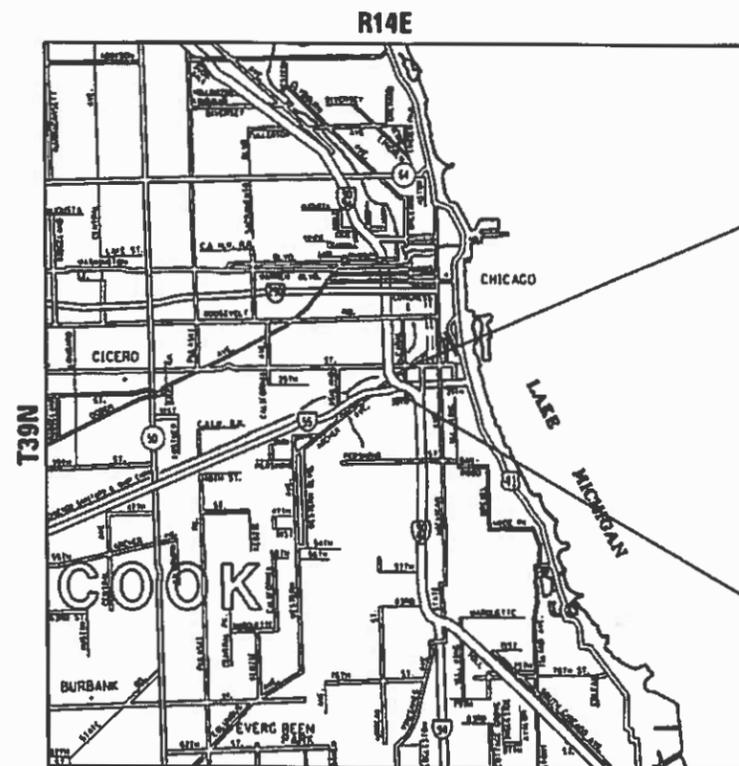


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD  
 ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT  
 CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS  
 ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

C.U.A.N.  
 CHICAGO UTILITY ALERT NETWORK  
 1-312-744-7000

PROJECT ENGINEER: ALAIN MIDY (847) 221-3056  
 PROJECT MANAGER: FAWAD AQUEEL (847) 705-4247

CONTRACT NO. 62J66



PROJECT ENDS  
STA. 41 + 80

OMISSION 1  
FROM STA. 20 + 36  
TO STA. 21 + 16

OMISSION 2  
FROM STA. 26 + 49  
TO STA. 28 + 30

PROJECT BEGINS  
STA. 13 + 59

SOUTH CHICAGO TOWNSHIP

GROSS LENGTH = 2821 FT. = 0.5343 MILE  
 NET LENGTH = 2560 FT. = 0.4848 MILE

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

SUBMITTED January 15, 2020

Anthony A. D'Amico REGIONAL ENGINEER

Jan 31, 2020

Jan 31, 2020 ENGINEER OF DESIGN AND ENVIRONMENT

Jan 31, 2020 DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY  
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**STATE STANDARDS**

701006-05	OFF-RD OPERATIONS, 2L, 2W, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
701011-04	OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701101-05	OFF-RD OPERATIONS, MULTILANE, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS < 40 MPH
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701611-01	URBAN HALF ROAD CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES

**GENERAL NOTES – CITY OF CHICAGO**

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "C.U.A.N." (CHICAGO UTILITY ALERT NETWORK) AT (312)744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. (48 HOUR NOTIFICATION IS REQUIRED).
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE CITY OF CHICAGO.
- PERFORATED LIDS SHALL BE PLACED ON ALL MANHOLES AND CATCH BASINS.
- SIDEWALK HANDICAPPED RAMPS SHALL NOT BE CONSTRUCTED DIRECTLY OVER EXISTING OR PROPOSED DRAINAGE STRUCTURES.
- ALL BROKEN, CRACKED, WORN OR OTHERWISE DAMAGED OR BICYCLE UNSAFE FRAMES AND LIDS ON SEWER STRUCTURES, SHALL BE REPLACED WITH NEW DEPARTMENT OF SEWERS' STANDARD FRAMES AND LIDS.
- OPEN LID DRAINAGE STRUCTURES SHALL NOT BE CLOSED, COVERED OR OTHERWISE OBSTRUCTED DURING CONSTRUCTION OF THIS ROADWAY WITHOUT THE WRITTEN PERMISSION FROM THE CITY OF CHICAGO.
- CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY OF CHICAGO AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
- CTA OPERATES BUSES ON ARCHER AVENUE, 26TH STREET, AND CANAL STREET. CTA BUSES REQUIRE 11-FT MINIMUM LANE WIDTHS. PLEASE CONTACT CTA TRAFFIC PLANNING TO DISCUSS IMPACTS SHOULD THERE BE A DISRUPTION TO BUS ROUTING OR ANY BUS STOP LOCATIONS. AT LEAST TWO (2) WEEKS NOTICE IS REQUIRED TO BE PROVIDED FOR ANY BUS SERVICE CHANGES. CTA'S POINT OF CONTACT IS JACK CHALABIAN, COORDINATOR, TRAFFIC PLANNING, VIA EMAIL AT JCHALABIAN@TRANSITCHICAGO.ORG OR AT (312) 681-4176.
- FOR ANY FRAME AND COVER ADJUSTMENTS OF COMED MANHOLES, PLEASE CONTACT CONNOR AUSTELL AT (773) 509-3746 OR VIA EMAIL AT CONNOR.AUSTELL@COMED.COM AT LEAST SIX (6) WEEKS PRIOR TO THE START OF CONSTRUCTION.

**GENERAL NOTES**

- TEN (10) FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTER AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
- ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
- ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT, WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
- SIDEWALK REMOVAL AND P.C.C. SIDEWALK 5" LOCATIONS SHALL BE DETERMINED BY THE ENGINEER.
- THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.
- THE ENGINEER SHALL CONTACT EMAD ALHUSSEINI, AREA TRAFFIC FIELD ENGINEER, AT EMAD.ALHUSSEINI@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- THESE PLANS HAVE BEEN PREPARED FROM NOTES RECEIVED FROM THE BUREAU OF CONSTRUCTION.
- THE THICKNESS OF THE HMA MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA MIXTURE IS PLACED.
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF PLATED STRUCTURES BY STATION AND OFFSET LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT.
- THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.
- FOR FRAMES AND LIDS ADJUSTMENT WITHOUT MILLING, REUSE EXISTING FRAME AND LID UNLESS OTHERWISE SPECIFIED IN THE PLANS.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
- WHEN THE MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1/2 INCHES (40 mm) WHERE THE SPEED LIMIT IS 40 MPH (80 km/h) OR LESS AND 1 INCH (25 mm) WHERE THE SPEED LIMIT IS GREATER THAN 40 MPH (80 km/h), WITH WRITTEN APPROVAL OF THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 mm) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H) OR A NOTCHED LONGITUDINAL WEDGE IS USED.
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) ACCORDING TO THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- OVERNIGHT LANE CLOSURES SHALL NOT BE ALLOWED FOR REHABILITATION PROJECTS INVOLVING DAYTIME MILLING AND RESURFACING OPERATIONS AND CLASS D PATCHING UNLESS OTHER CONDITIONS WARRANT EXTENDED LANE CLOSURES AS DETERMINED AND APPROVED IN WRITING BY THE ENGINEER OR AS PROVIDED FOR IN THE CONTRACT SPECIFICATIONS.

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**INDEX OF SHEETS, STANDARDS AND GENERAL NOTES  
CANAL ST. – ARCHER AVE. TO 28TH ST.**

SCALE: SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	2019-120-R5&SW	COOK	28	2
* OR 308 & FAU 2885		CONTRACT NO. 62J66		
ILLINOIS		FED. AID PROJECT		

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	0005 20% STATE 80% FED.	CONSTRUCTION TYPE CODE					SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	0005 20% STATE 80% FED.	CONSTRUCTION TYPE CODE							
CODE NO	ITEM	UNIT			CODE NO	ITEM	UNIT	CODE NO	ITEM	UNIT	CODE NO	ITEM			UNIT	CODE NO	ITEM	UNIT				
20200100	EARTH EXCAVATION	CU YD	38	38								44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	22	22						
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	12	12								44000600	SIDEWALK REMOVAL	SO FT	4500	4500						
25200110	SODDING, SALT TOLERANT	SO YD	12	12								44201749	CLASS D PATCHES, TYPE I, 9 INCH	SO YD	50	50						
25200200	SUPPLEMENTAL WATERING	UNIT	1	1								44201753	CLASS D PATCHES, TYPE II, 9 INCH	SO YD	200	200						
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	10308	10308								44201757	CLASS D PATCHES, TYPE III, 9 INCH	SO YD	150	150						
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	23	23								44201759	CLASS D PATCHES, TYPE IV, 9 INCH	SO YD	125	125						
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	339	339								60250200	CATCH BASINS TO BE ADJUSTED	EACH	3	3						
40602985	HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70	TON	1283	1283								60255500	MANHOLES TO BE ADJUSTED	EACH	3	3						
40604062	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	TON	1283	1283								60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	10	10						
42001300	PROTECTIVE COAT	SO YD	500	500								* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	38	38						
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SO YD	22	22								* 66900530	SOIL DISPOSAL ANALYSIS	EACH	4	4						
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	4500	4500								* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	LSUM	1	1						
44000161	HOT-MIX ASPHALT SURFACE REMOVAL, 3"	SO YD	15270	15270								* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	LSUM	1	1						
												* 66901006	REGULATED SUBSTANCES MONITORING	CAL DA	12	12						
												67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6						

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**CANAL ST. - ARCHER AVE. TO 28TH ST.  
SUMMARY OF QUANTITIES**

SCALE: SHEET NO. 1 OF 2 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	2019-120-RS&SW	COOK	28	3
	* OR 308 & FAU 2885		CONTRACT NO. 62J66	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		0005 20% STATE 80% FED.						CODE NO	ITEM		UNIT	0005 20% STATE 80% FED.	100% STATE			
67100100	MOBILIZATION	L SUM	1	1						X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1	1				
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1						* X4240800	DETECTABLE WARNINGS (SPECIAL)	SO FT	215	215				
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1						△ X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	200		200			
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1						X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SO FT	5716	5716				
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1						Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	750	750				
70300100	SHORT TERM PAVEMENT MARKING	FOOT	1332	1332						△ Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	1		1			
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	111	111						Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	51.4	51.4				
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	1734	1734						Z0033700	LONGITUDINAL JOINT SEALANT	FOOT	2485	2485				
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	396	396														
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	2470	2470														
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	333	333														
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	1762	1762														
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	396	396														
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	379	379														

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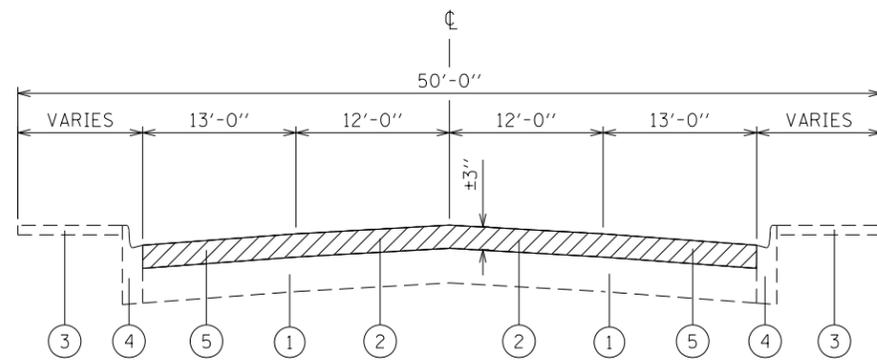
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**CANAL ST. - ARCHER AVE. TO 28TH ST.  
SUMMARY OF QUANTITIES**

SCALE: SHEET NO. 2 OF 2 SHEETS STA. TO STA.

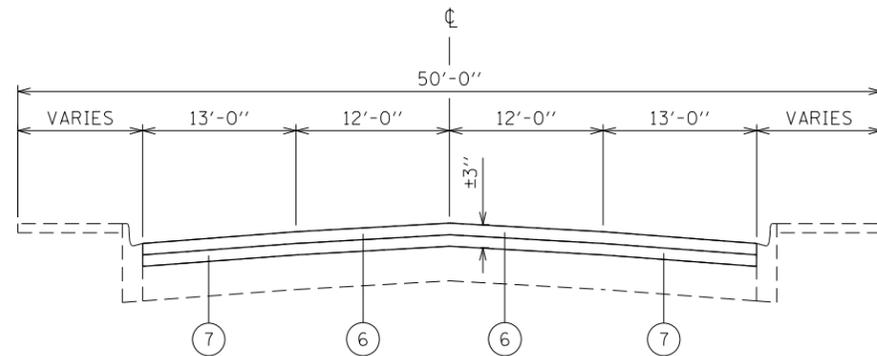
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	2019-120-RS&SW	COOK	28	4
* OR 308 & FAU 2885			CONTRACT NO. 62J66	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

\* SPECIALTY ITEMS    △ NON-PARTICIPATING ITEMS



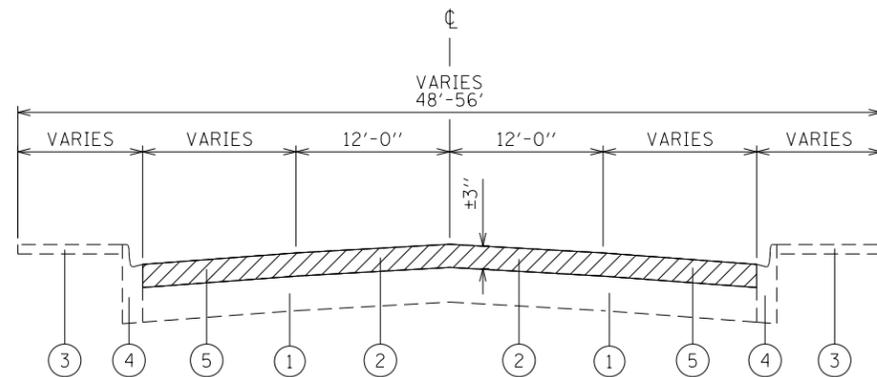
**EXISTING CANAL STREET**

STA. 13+59 TO STA. 20+36



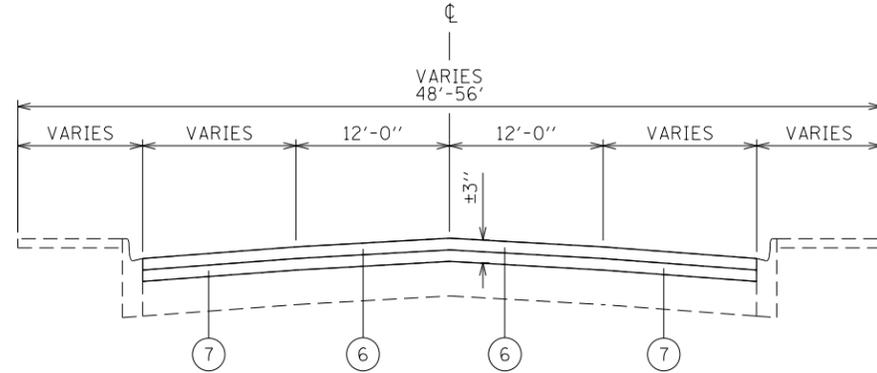
**PROPOSED CANAL STREET**

STA. 13+59 TO STA. 20+36



**EXISTING CANAL STREET**

STA. 21+16 TO STA. 26+49  
STA. 28+30 TO STA. 41+03



**PROPOSED CANAL STREET**

STA. 21+16 TO STA. 26+49  
STA. 28+30 TO STA. 41+03

CONTRACTOR SHALL MILL FIRST BEFORE PATCHING

**LEGEND**

- ① EXISTING PCC PAVEMENT, ±9"
- ② EXISTING HOT-MIX ASPHALT PAVEMENT, ±3"
- ③ EXISTING CONCRETE SIDEWALK
- ④ EXISTING COMB. CONC. CURB AND GUTTER
- ⑤ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 3"
- ⑥ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N70, 1/2"
- ⑦ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 1/2"

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		QMP
MIXTURE TYPE	AIR VOIDS @ Ndes	
<b>PAVEMENT RESURFACING</b>		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N70, 1/2"	4% @ 70 Gyr.	QC/QA
HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 1/2"	4% @ 70 Gyr.	QC/QA
<b>PATCHING</b>		
CLASS D PATCH (HMA BINDER IL-19 mm), 10 INCH	4% @ 70 Gyr.	QC/QA
QMP Designations: Quality Control/Quality Assurance (QC/QA); Quality Control for Performance (QCP); Pay for Performance (PFP)		

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 16-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

FOR USE OF RECYCLED MATERIAL SEE SPECIAL PROVISIONS.

QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

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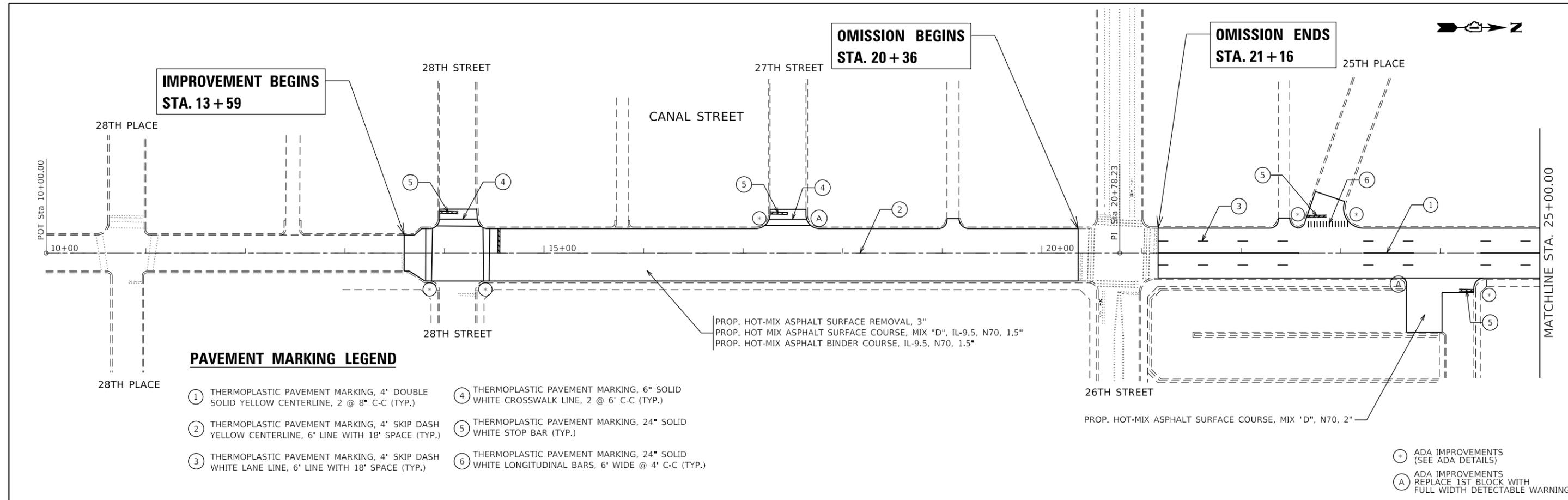
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS  
CANAL ST. - ARCHER AVE. TO 28TH ST.**

SCALE: SHEET 1 OF 1 SHEETS STA. TO STA.

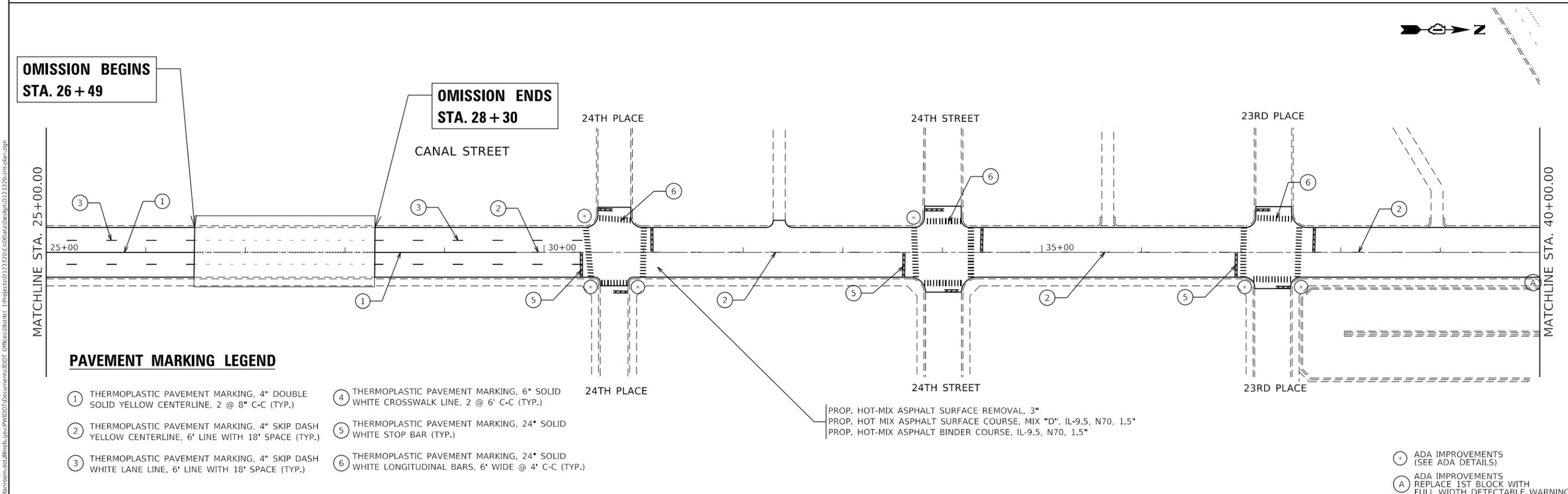
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	2019-120-R5&SW	COOK	28	5
* OR 308 & FAU 2885		CONTRACT NO. 62J66		
		ILLINOIS	FED. AID PROJECT	



**PAVEMENT MARKING LEGEND**

- ① THERMOPLASTIC PAVEMENT MARKING, 4" DOUBLE SOLID YELLOW CENTERLINE, 2 @ 8" C-C (TYP.)
- ② THERMOPLASTIC PAVEMENT MARKING, 4" SKIP DASH YELLOW CENTERLINE, 6' LINE WITH 18' SPACE (TYP.)
- ③ THERMOPLASTIC PAVEMENT MARKING, 4" SKIP DASH WHITE LANE LINE, 6' LINE WITH 18' SPACE (TYP.)
- ④ THERMOPLASTIC PAVEMENT MARKING, 6" SOLID WHITE CROSSWALK LINE, 2 @ 6' C-C (TYP.)
- ⑤ THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE STOP BAR (TYP.)
- ⑥ THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE LONGITUDINAL BARS, 6' WIDE @ 4' C-C (TYP.)

- ⊛ ADA IMPROVEMENTS (SEE ADA DETAILS)
- Ⓐ ADA IMPROVEMENTS REPLACE 1ST BLOCK WITH FULL WIDTH DETECTABLE WARNING



**PAVEMENT MARKING LEGEND**

- ① THERMOPLASTIC PAVEMENT MARKING, 4" DOUBLE SOLID YELLOW CENTERLINE, 2 @ 8" C-C (TYP.)
- ② THERMOPLASTIC PAVEMENT MARKING, 4" SKIP DASH YELLOW CENTERLINE, 6' LINE WITH 18' SPACE (TYP.)
- ③ THERMOPLASTIC PAVEMENT MARKING, 4" SKIP DASH WHITE LANE LINE, 6' LINE WITH 18' SPACE (TYP.)
- ④ THERMOPLASTIC PAVEMENT MARKING, 6" SOLID WHITE CROSSWALK LINE, 2 @ 6' C-C (TYP.)
- ⑤ THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE STOP BAR (TYP.)
- ⑥ THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE LONGITUDINAL BARS, 6' WIDE @ 4' C-C (TYP.)

- ⊛ ADA IMPROVEMENTS (SEE ADA DETAILS)
- Ⓐ ADA IMPROVEMENTS REPLACE 1ST BLOCK WITH FULL WIDTH DETECTABLE WARNING

MODEL: Default  
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 PROJECT: 2019-120-R5&SW  
 DRAWING: 2019-120-R5&SW-01  
 DATE: 2/10/2020

USER NAME = rothjp	DESIGNED -	REVISED -
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PLOT DATE = 2/10/2020	CHECKED -	REVISED -
	DATE -	REVISED -

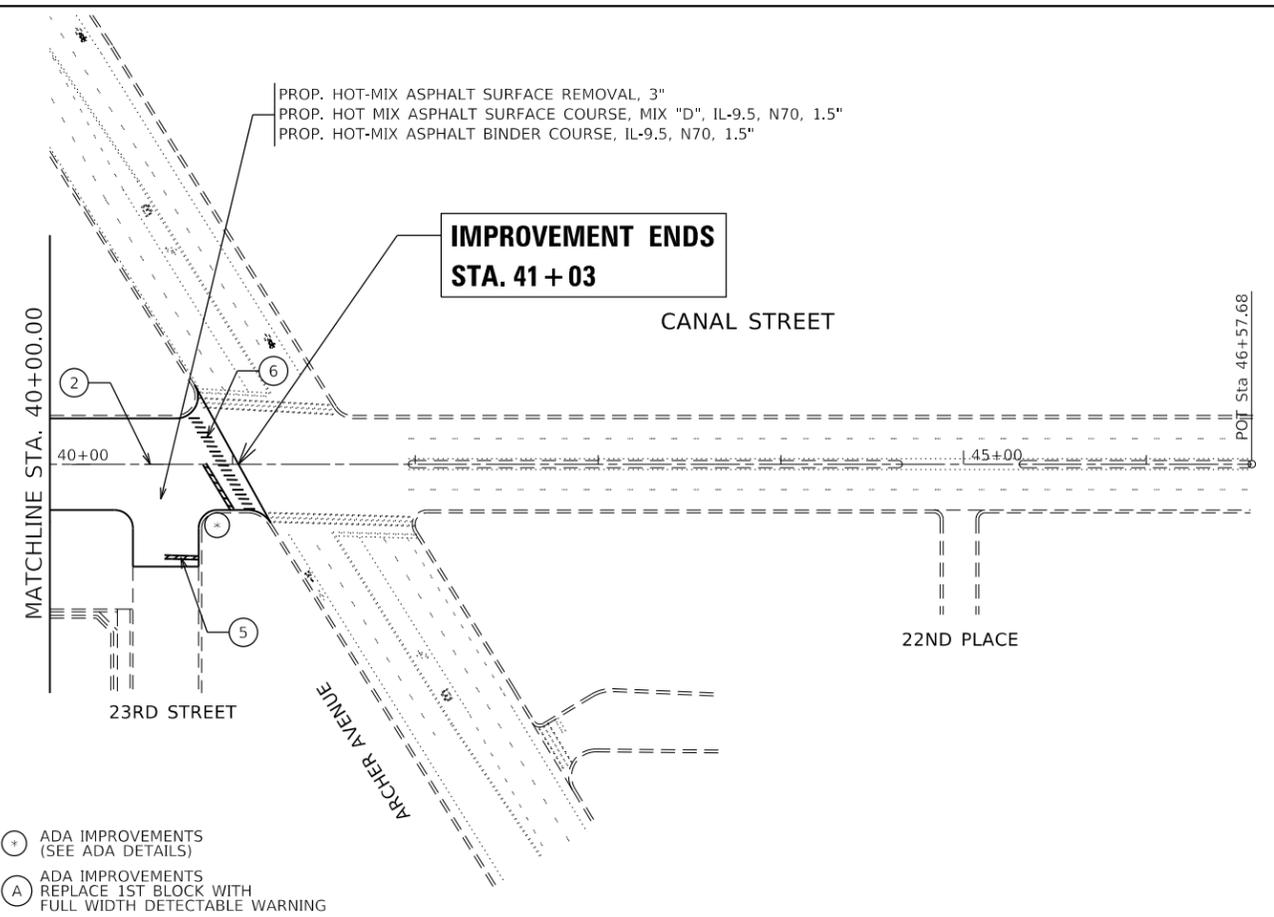
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED ROADWAY PLAN  
CANAL ST. - ARCHER AVE. TO 28TH ST.**

SCALE: 1" = 50' SHEET 1 OF 2 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	2019-120-R5&SW	COOK	28	6
* OR 308 & FAU 2885			CONTRACT NO. 62J66	

ILLINOIS FED. AID PROJECT



- ① THERMOPLASTIC PAVEMENT MARKING, 4" DOUBLE SOLID YELLOW CENTERLINE, 2 @ 8" C-C (TYP.)
- ② THERMOPLASTIC PAVEMENT MARKING, 4" SKIP DASH YELLOW CENTERLINE, 6' LINE WITH 18' SPACE (TYP.)
- ③ THERMOPLASTIC PAVEMENT MARKING, 4" SKIP DASH WHITE LANE LINE, 6' LINE WITH 18' SPACE (TYP.)
- ④ THERMOPLASTIC PAVEMENT MARKING, 6" SOLID WHITE CROSSWALK LINE, 2 @ 6' C-C (TYP.)
- ⑤ THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE STOP BAR (TYP.)
- ⑥ THERMOPLASTIC PAVEMENT MARKING, 24" SOLID WHITE LONGITUDINAL BARS, 6' WIDE @ 4' C-C (TYP.)

- (\*) ADA IMPROVEMENTS (SEE ADA DETAILS)
- (A) ADA IMPROVEMENTS REPLACE 1ST BLOCK WITH FULL WIDTH DETECTABLE WARNING

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PLOT DATE = 2/10/2020	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

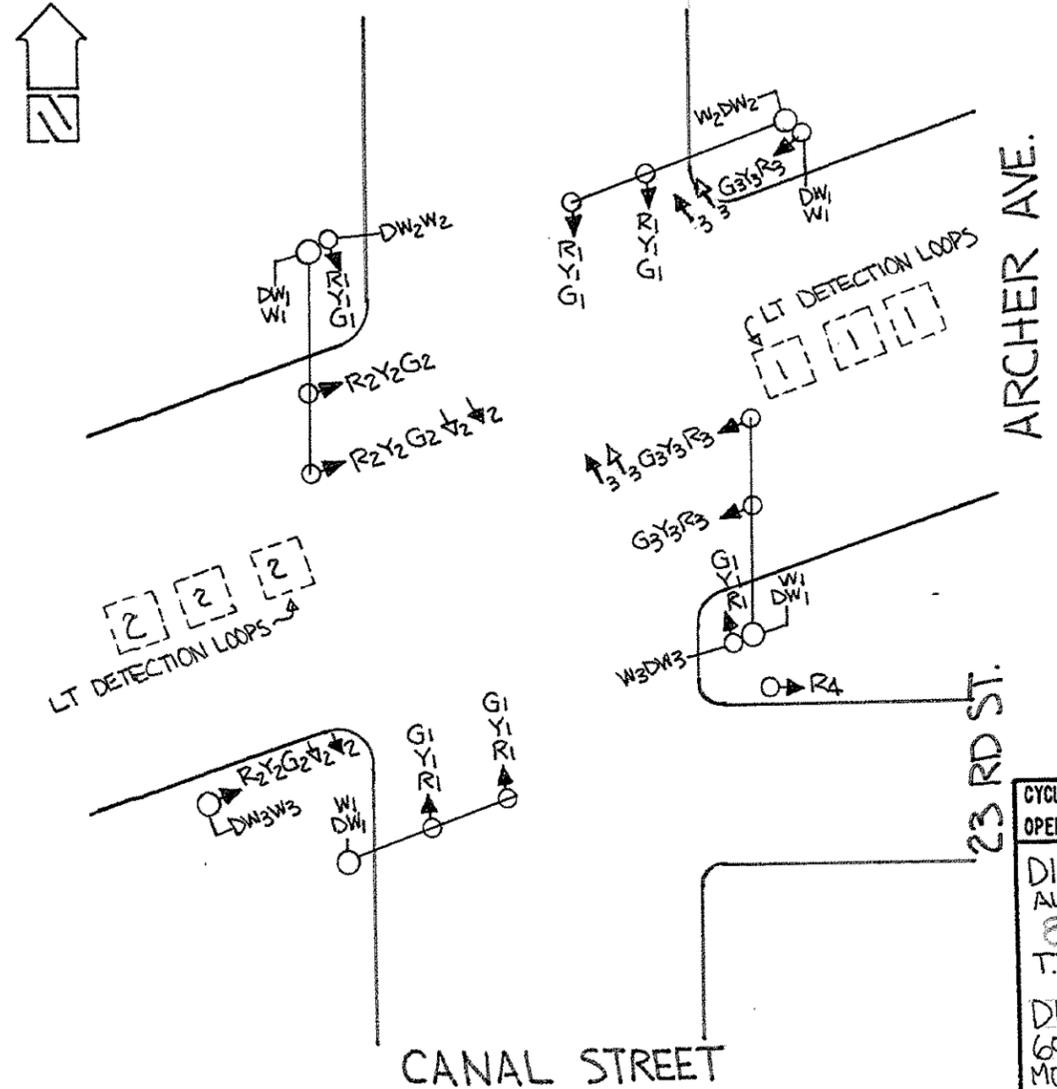
**EXISTING AND PROPOSED ROADWAY PLAN  
CANAL STREET (ARCHER AVENUE TO 28TH STREET)**

SCALE: 1"= 50'      SHEET 2 OF 2 SHEETS      STA.      TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	2019-120-R5&SW	COOK	28	7
* OR 308 & FAU 2885		CONTRACT NO. 62J66		
		ILLINOIS	FED. AID PROJECT	

DRAWN: **KFI**  
 DATE: 3-6-96  
 CHECKED:  
 DATE:

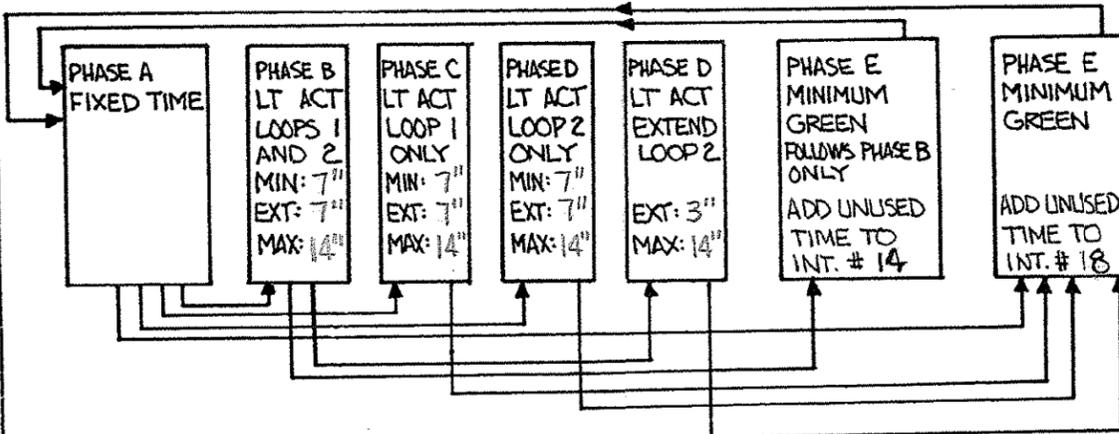
**CDOT / BOT**  
 TRAFFIC SIGNAL TIMING SCHEDULE  
**CANAL ST. & ARCHER AVE.**  
 APPROVED: **K. HOO**  
 TRAFFIC ENGINEER  
 CITY TRAFFIC ENGINEER



	A				B	C1	D1	D2	E1	E2												
	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	
CANAL ST. G1																						
" " Y1																						
" " R1																						
ARCHER (SWB) G2																						
" " Y2																						
" " R2																						
" " GLTA ← 2																						
" " YLTA ← 2																						
ARCHER (NEB) G3																						
" " Y3																						
" " R3																						
" " GLTA ← 3																						
" " YLTA ← 3																						
23 RD ST. R4																						
WALK W1																						
DONT WALK DW1																						
WALK W2																						
DONT WALK DW2																						
WALK W3																						
DONT WALK DW3																						
SECONDS	10	17	3	1	*	*3	*3	3*3	3	2	1	2	3	1	2	1	2	3	1			

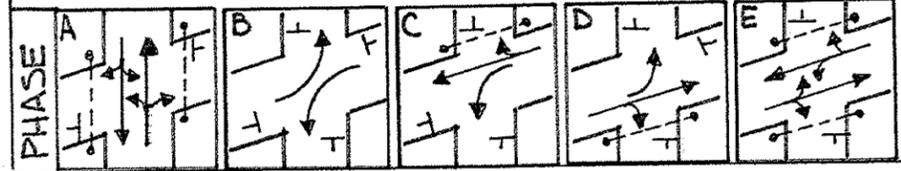
CYCLES, T. T. OFFSETS, HOURS OF OPERATION, SPECIAL FEATURES:  
 DIAL #1  
 ALL OTHER TIMES  
 85" CYCLE  
 T.T. OFFSET: 53"  
 DIAL #2  
 600 AM TO 1000 AM  
 MONDAY THRU FRIDAY  
 85" CYCLE  
 T.T. OFFSET: 0"  
 DIAL #3  
 300 PM TO 700 PM  
 MON - FRI  
 85" CYCLE  
 T.T. OFFSET: 17"

FLASH OPERATION: R1R2R3R4  
 W/DW - OFF  
 ARROWS - OFF



DATE INSTALLED:  
 07-31-04  
 REVISED  
 5-20-04  
 (Detour timing)

**FOR INFORMATION ONLY**



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PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 1/13/2020	DATE -	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

DETECTOR LOOP DETAILS (FOR INFORMATION ONLY)  
 CANAL STREET (ARCHER AVENUE TO 28TH STREET)  
 SCALE: 1"=50' SHEET 1 OF 2 SHEETS STA. TO STA.

F.A. RTE. *	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	2019-120-R56SW	COOK	28	8
* OR 308 & FAU 2885		CONTRACT NO.		
ILLINOIS		FED. AID PROJECT		

DRAWN: E. DLEE  
 DATE: 5-8-88  
 CHECKED: \_\_\_\_\_  
 DATE: \_\_\_\_\_

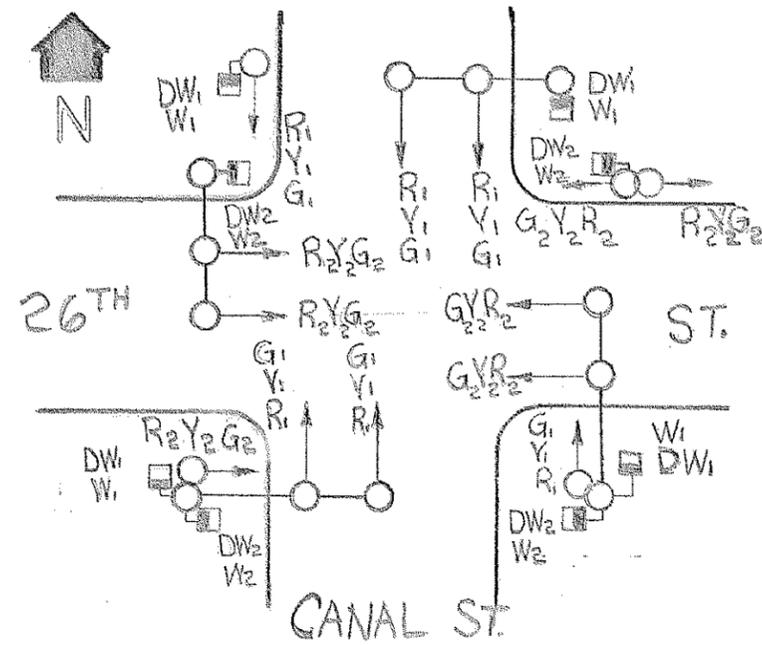
CITY OF CHICAGO - DEPARTMENT OF PUBLIC WORKS  
 BUREAU OF TRAFFIC ENGINEERING AND OPERATIONS  
**TRAFFIC SIGNAL TIMING SCHEDULE**

TR: 1/4 | 1/2 | 1  
 APPROVED: K. H...  
 TRAFFIC ENGINEER  
 CITY TRAFFIC ENGINEER

CANAL ST. & 26<sup>TH</sup> ST.

PAGE 1 OF 1

CYCLES, T. T. OFFSETS, HOURS OF OPERATION, SPECIAL FEATURES:  
 DIAL No. 1 Avg.  
 ALL OTHER TIMES  
 65" CYCLE  
 T. T. OFFSET 12"  
 DIAL No. 2 A.M.  
 6<sup>00</sup> AM to 10<sup>00</sup> AM  
 MON. THRU FRI.  
 65" CYCLE  
 T. T. OFFSET 37"  
 DIAL No. 3 P.M.  
 3<sup>00</sup> PM to 7<sup>00</sup> PM  
 MON. THRU FRI.  
 T. T. OFFSET 12"  
 FLASH OPERATION: R<sub>1</sub>, R<sub>2</sub>  
 W<sub>1</sub>, DW OFF



SIGNAL INDICATION	PHASE:	REPEAT																DATE
		A				B				REPEAT								
	INTERVAL:	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	
CANAL ST. G <sub>1</sub>		█	█	█	█													
Y <sub>1</sub>																		
R <sub>1</sub>						█	█	█	█	█	█	█	█	█	█	█	█	
26 <sup>TH</sup> ST G <sub>2</sub>																		
Y <sub>2</sub>																		
R <sub>2</sub>																		
WALK W <sub>1</sub>		█	█	█	█	█	█	█	█	█	█	█	█	█	█	█	█	
DONT WALK DW <sub>1</sub>		█	█	█	█	█	█	█	█	█	█	█	█	█	█	█	█	
WALK W <sub>2</sub>																		
DONT WALK DW <sub>2</sub>																		
DIAL No. 1 ELAPSED TM.		19	29	32	33	52	61	64	65									INST: 9-26-88
SEC. PER UNIT		19	10	3	1	19	9	3	1									
DIAL No. 2 ELAPSED TM.		14	24	27	28	52	61	64	65									
SEC. PER UNIT		14	10	3	1	24	9	3	1									
DIAL No. 3 ELAPSED TM.		24	34	37	38	52	61	64	65									
SEC. PER UNIT		24	10	3	1	14	9	3	1									

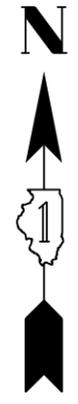
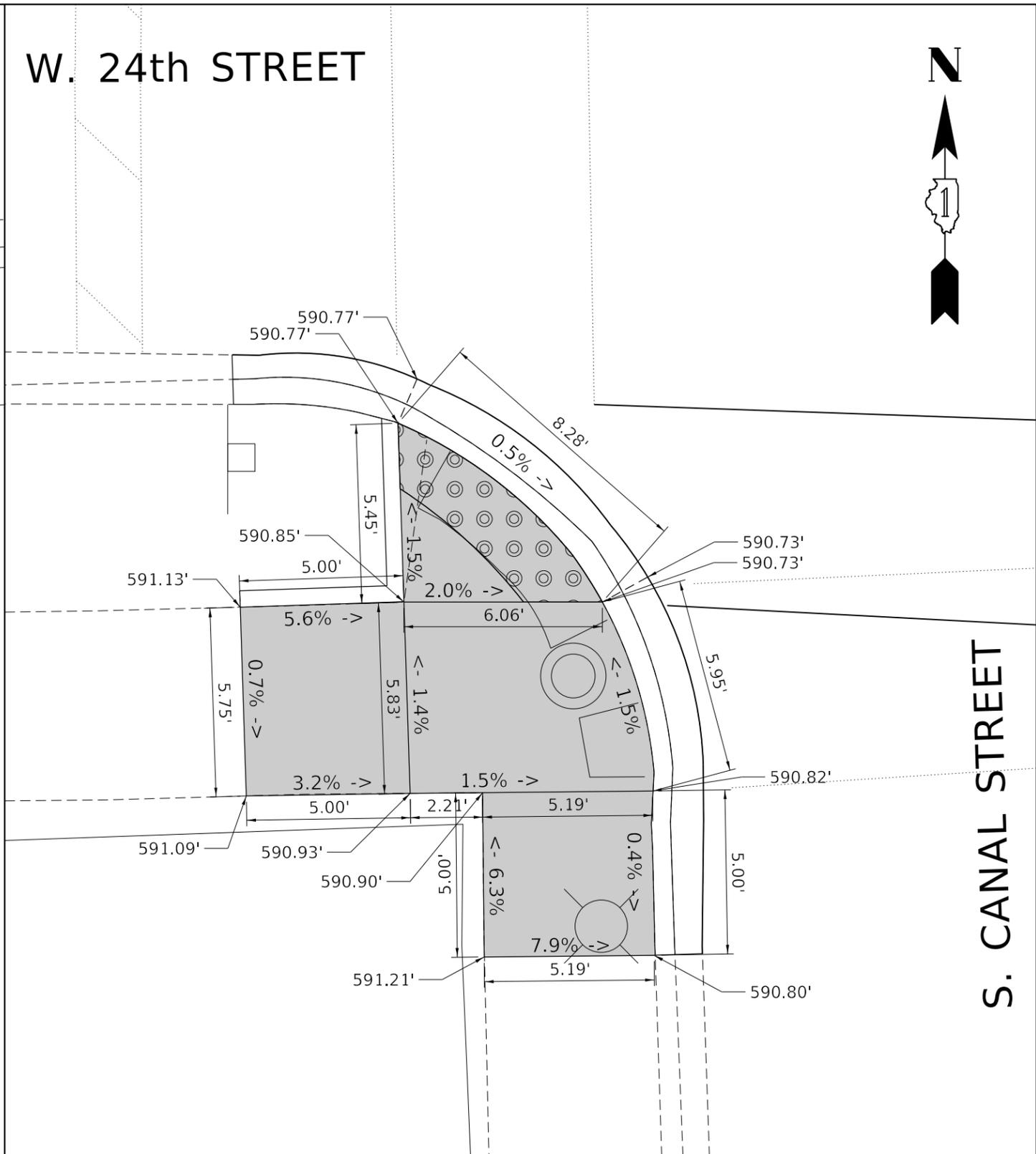
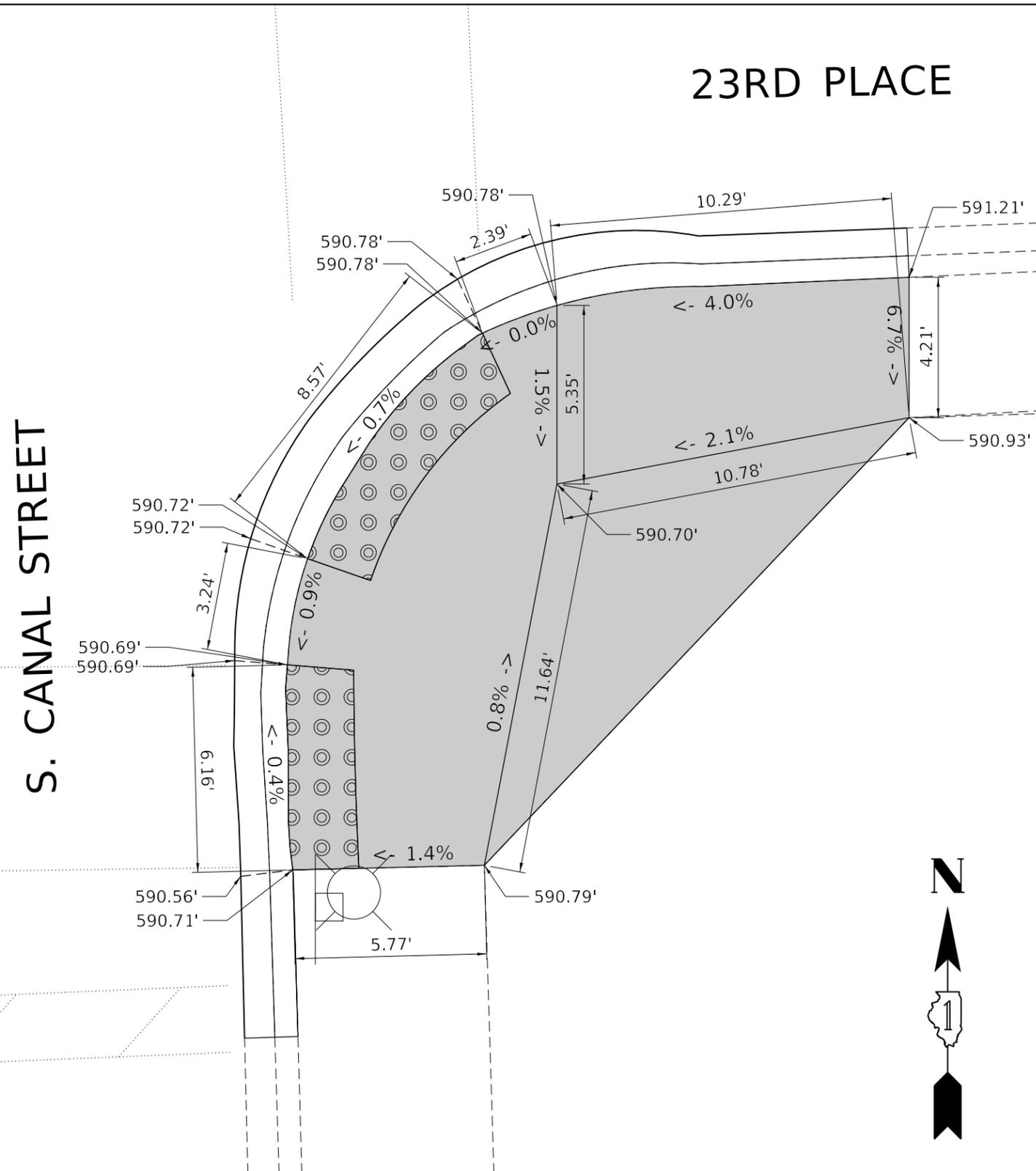
REV. 8-8-85, J.A.M.  
DPW - RTED. FORN TSING

**FOR INFORMATION ONLY**

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REFERENCE BENCHMARK ELEV  
 BENCHMARK :  
 LOCATION :

**LEGEND**

- xx.xx' EXISTING LENGTH
- PROPOSED SIDE CURB
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL  
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 591.98  
 BENCHMARK : WEST BOLT ON F.H.  
 LOCATION : N. WEST CORNER OF CANAL ST. & 24TH ST.

USER NAME = rothjp	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 2/10/2020	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

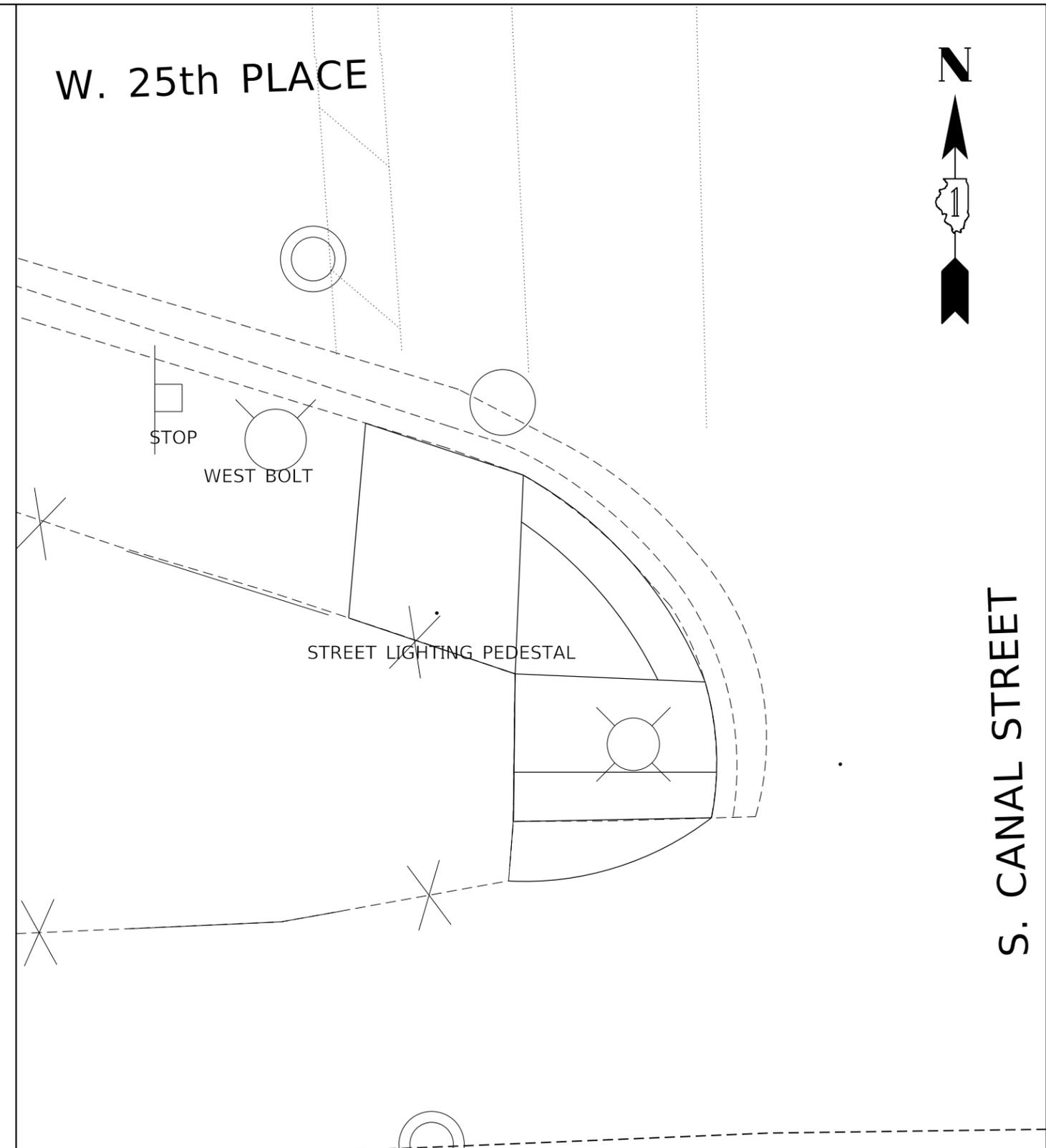
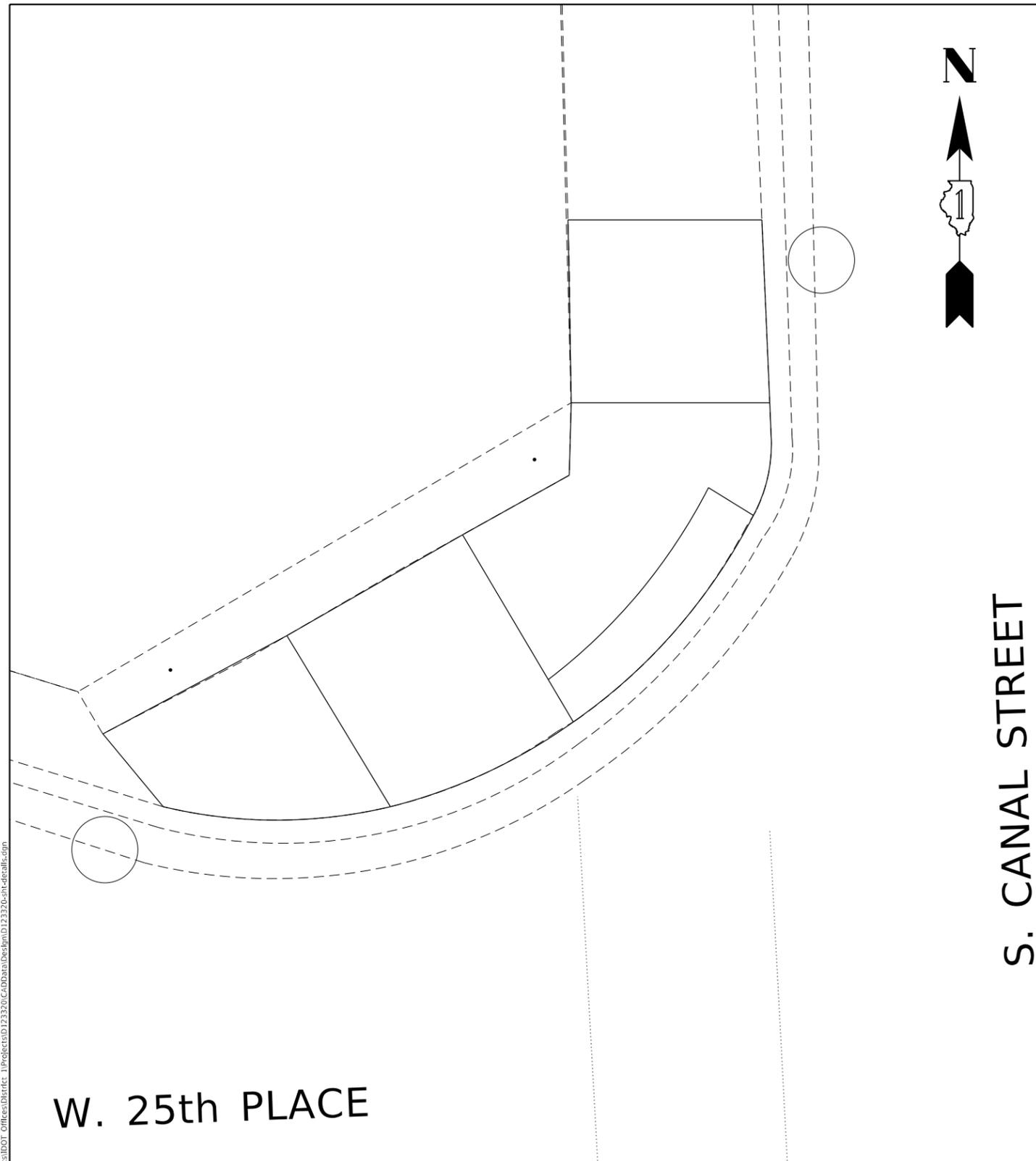
**ADA IMPROVEMENT DETAILS**  
**CANAL STREET (ARCHER AVENUE TO 28TH STREET)**  
 SCALE: SHEET 2 OF 7 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	2019-120-R5&SW	COOK	28	11
* O.R. 308 / F.A.U. 2885		CONTRACT NO. 62J66		
ILLINOIS FED. AID PROJECT				





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W. 25th PLACE

W. 25th PLACE

S. CANAL STREET

S. CANAL STREET

REFERENCE BENCHMARK ELEV  
 BENCHMARK : WEST BOLT ON F.H.  
 LOCATION : S. WEST CORNER @ CANAL ST. & 25TH PL.

REFERENCE BENCHMARK ELEV  
 BENCHMARK : WEST BOLT ON F.H.  
 LOCATION : S. WEST CORNER @ CANAL ST. & 25TH PL.

**LEGEND**

- xx.xx' EXISTING LENGTH
- PROPOSED SIDE CURB
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL  
REPLACE W/TOPSOIL & SOD

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ADA IMPROVEMENT DETAILS  
CANAL STREET (ARCHER AVENUE TO 28TH STREET)**

USER NAME = rothjp	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 2/10/2020	DATE -	REVISED -

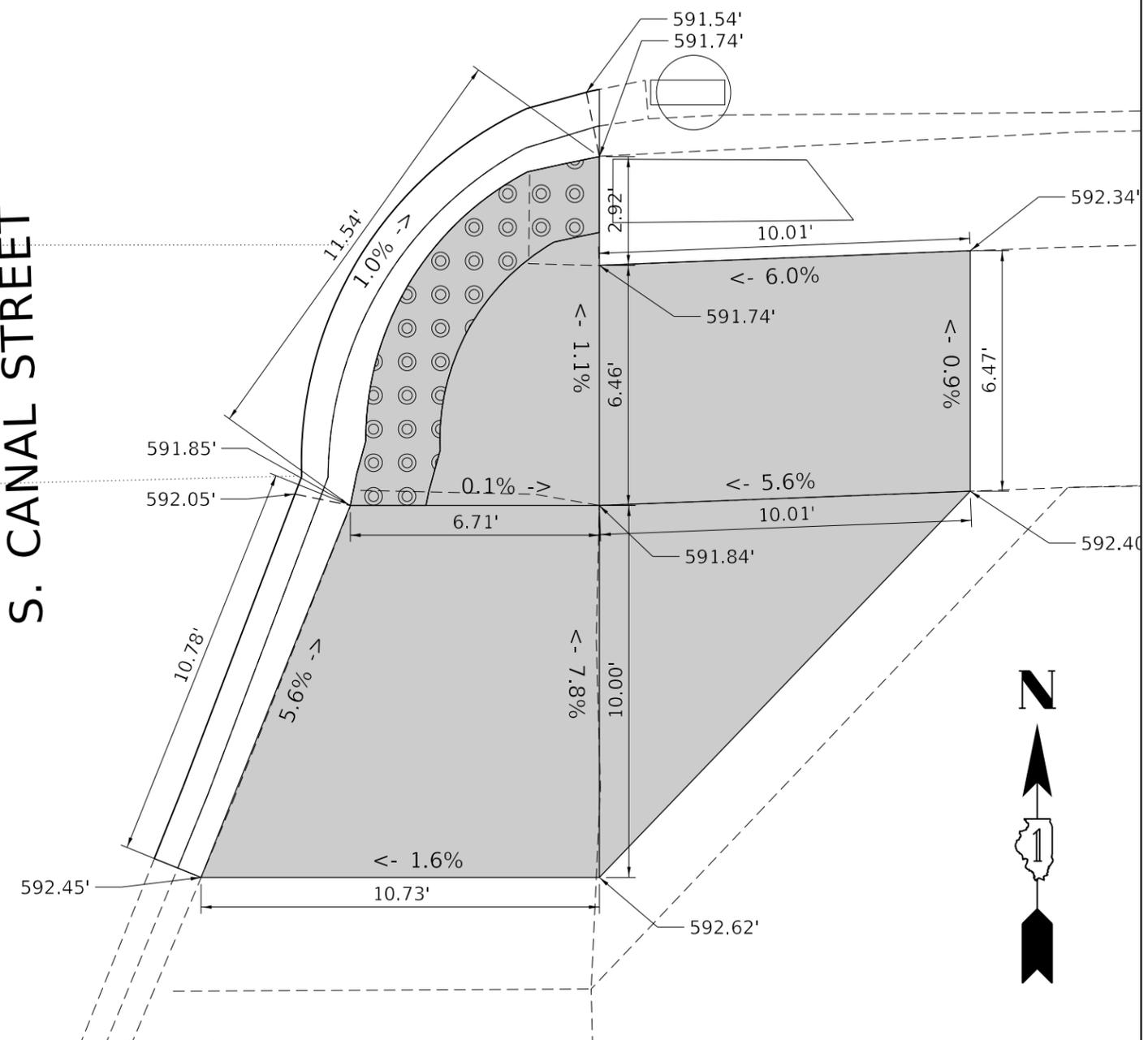
SCALE: SHEET 5 OF 7 SHEETS STA. TO STA.

F.A.* RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	2019-120-R5&SW	COOK	28	13a
* O.R. 308 / F.A.U. 2885			CONTRACT NO. 62J66	
ILLINOIS FED. AID PROJECT				



# W. 28th STREET

# S. CANAL STREET



REFERENCE BENCHMARK ELEV 593.68  
 BENCHMARK : X-CUT ON N. EAST BOLT OF F.H.  
 LOCATION : N. WEST CORNER OF W 28TH ST. & CANAL ST.

## LEGEND

- EXISTING LENGTH
- PROPOSED SIDE CURB
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL  
REPLACE W/TOPSOIL & SOD

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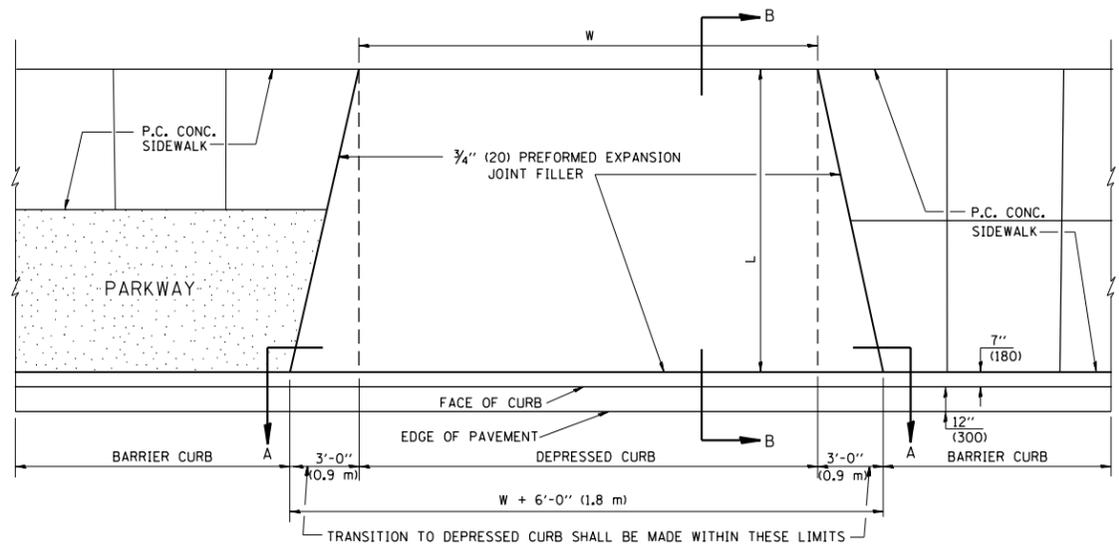
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PLOT DATE = 2/10/2020	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**ADA IMPROVEMENT DETAILS  
 CANAL STREET (ARCHER AVENUE TO 28TH STREET)**

SCALE: SHEET 7 OF 7 SHEETS STA. TO STA.

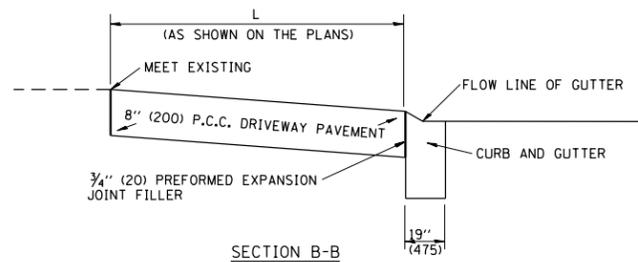
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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* O.R. 308 / F.A.U. 2885		CONTRACT NO. 62J66		
ILLINOIS FED. AID PROJECT				



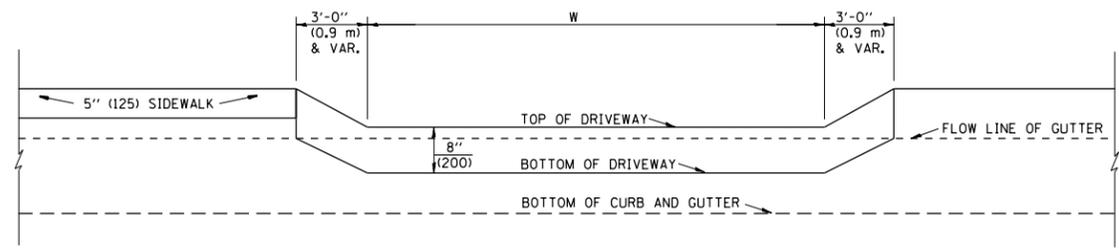
PLAN VIEW

NOTES:

1. EXPANSION JOINTS SHALL BE CONSTRUCTED AS SHOWN ON THE DETAILS FOR P.C.C. SIDEWALK.
2. THE CURB BETWEEN ADJACENT DRIVEWAYS SHALL BE FULL HEIGHT FOR A DISTANCE OF AT LEAST FOUR FEET (1.2 METERS)
3. P.C. CONCRETE DRIVEWAYS SHALL BE CONSTRUCTED AT LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
4. 3/4" (20) PREFORMED EXPANSION JOINTS WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO P.C.C. DRIVEWAY PAVEMENT 8" (200).
5. COMBINATION CONC. CURB AND GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE TRANSITION CURB AND GUTTER.

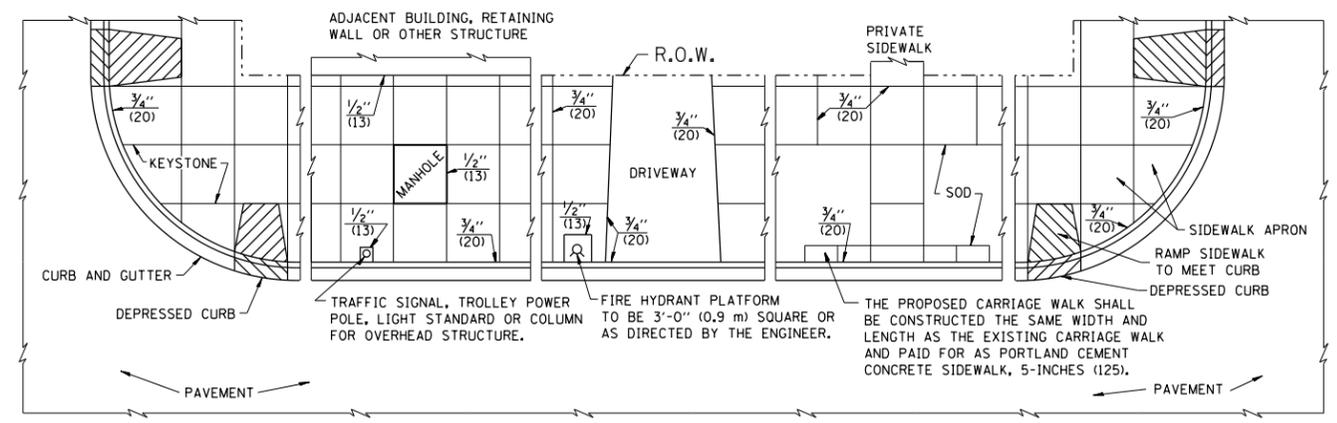


SECTION B-B



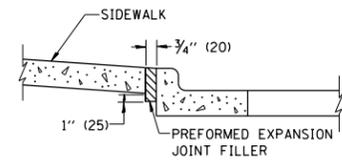
SECTION A-A

P.C.C. DRIVEWAY PAVEMENT DETAIL



NOTES:

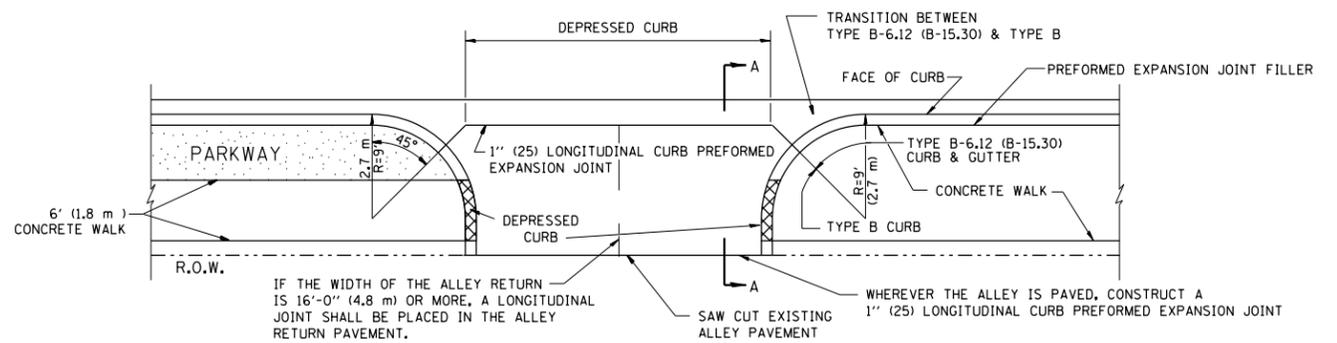
1. ONE-HALF INCH THICK EXPANSION JOINTS SHALL BE PLACED BETWEEN THE SIDEWALK AND ALL STRUCTURES SUCH AS LIGHT STANDARDS, TRAFFIC LIGHT STANDARDS, MANHOLES, WHICH EXTEND THROUGH THE SIDEWALK.
2. 3/4" (20) THICK EXPANSION JOINTS SHALL BE PLACED AT INTERVALS OF NOT MORE THAN 100 FEET (30 METERS) IN THE SIDEWALK, WHERE THE SIDEWALK IS CONSTRUCTED ADJACENT TO PAVEMENT OR CURB HAVING EXPANSION JOINTS, THE EXPANSION JOINTS IN THE SIDEWALK SHALL BE PLACED OPPOSITE THE EXISTING EXPANSION JOINTS AS NEARLY AS PRACTICABLE. EXPANSION JOINTS SHALL ALSO BE PLACED WHERE THE SIDEWALK ABUTS EXISTING SIDEWALKS, BETWEEN DRIVEWAY PAVEMENT AND SIDEWALK, AND BETWEEN SIDEWALK AND CURBS WHERE THE SIDEWALK ABUTS A CURB.



SLOPE FOR SIDEWALK  
1" (25) IN 3'-0" (0.9 m) IN CHICAGO

PORTLAND CEMENT CONCRETE SIDEWALK DETAILS

NOTES: NO EXTRA COMPENSATION SHALL BE ALLOWED FOR THE GUTTER FLARE



SECTION A-A

ALLEY RETURN DETAIL

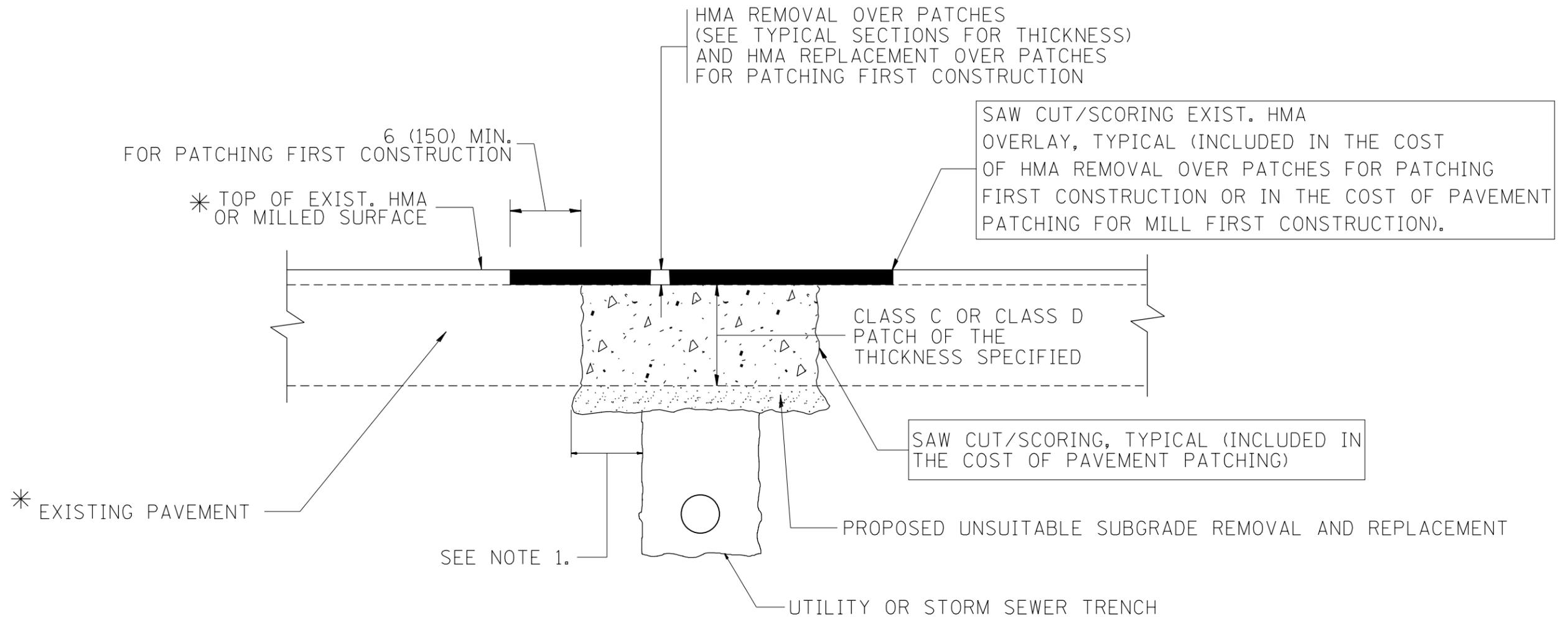
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		DATE - 06-13-90	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO	
DETAILS FOR P.C. CONCRETE DRIVEWAY, ALLEY RETURN AND SIDEWALK	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2019-120-R5&S	COOK	28	16
	BD400-03 (BD-17)	CONTRACT NO. 62J66		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

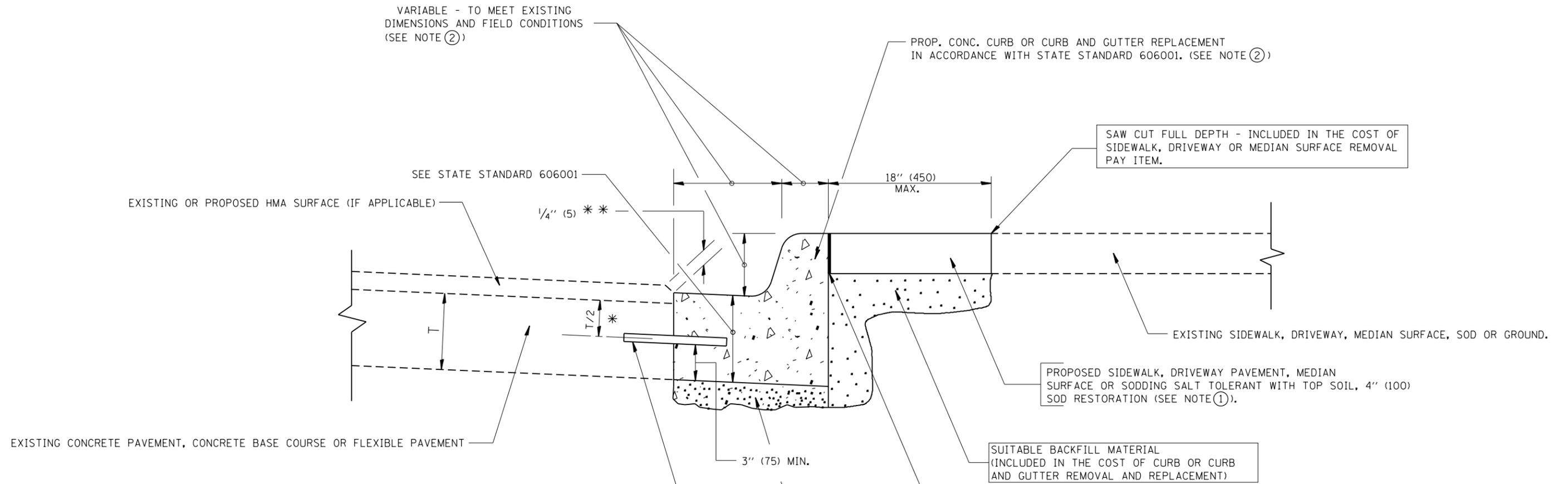
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = rothjp	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT</b>			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\012331\Drawings\Design\DistStd.dgn	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 01-01-07					2019-120-R5&SW	COOK	28	17	
	PLOT DATE = 1/13/2020	DATE - 10-25-94	REVISED - R. BORO 09-04-07		<b>BD400-04 (BD-22)</b>		<b>CONTRACT NO. 62J66</b>					
			REVISED - K. ENG 10-27-08		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT			



- \* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
  - \*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.
- NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
- SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.
- ② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED
  - ③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.
  - ④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.
  - ⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
  - ⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.
  - ⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.
  - ⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

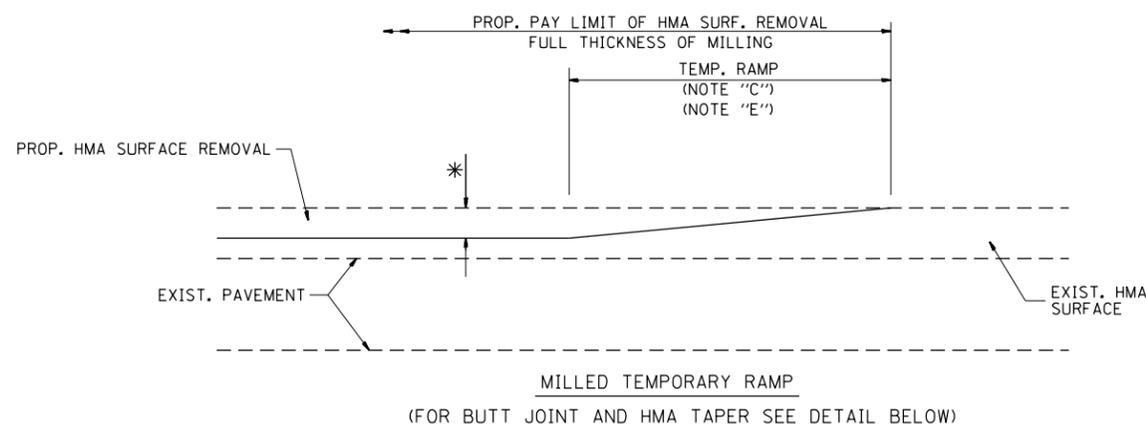
- PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)
- UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.
- REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.
- REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.
- PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

**BASIS OF PAYMENT:**  
 THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

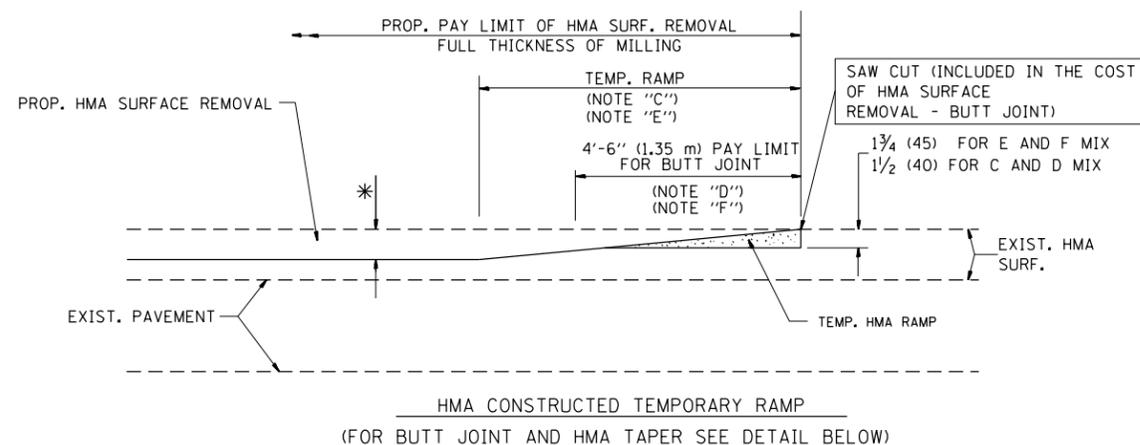
# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = rothjp	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>	F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\012331\Drawings\Design\DistStd.dgn			REVISED - A. ABBAS 03-21-97			•	2019-120-R5&SW	COOK	28	18
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - M. GOMEZ 01-22-01				<b>BD600-06 (BD-24)</b>		<b>CONTRACT NO. 62J66</b>		
PLOT DATE = 1/13/2020	DATE - 03-11-94	REVISED - R. BORO 12-15-09				SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT

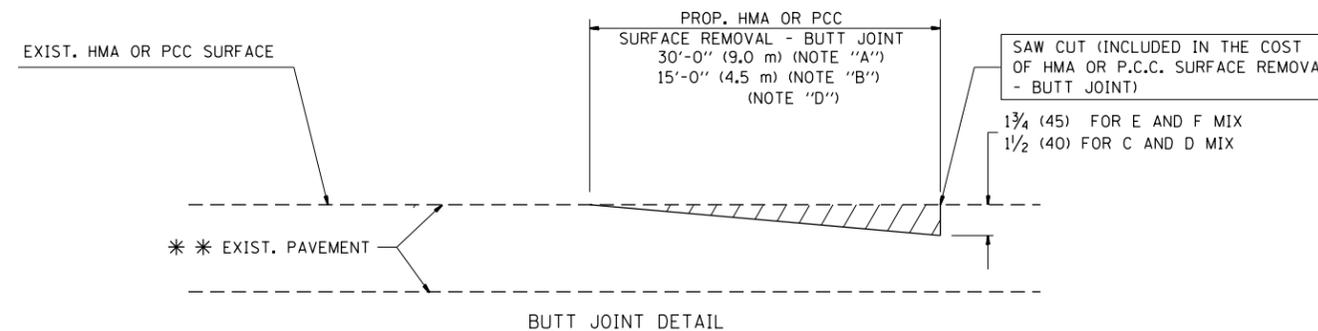


**OPTION 1**

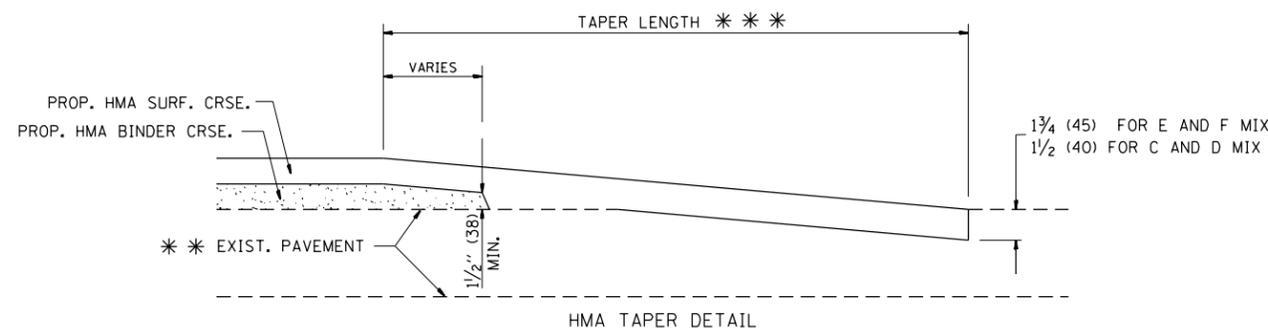


**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

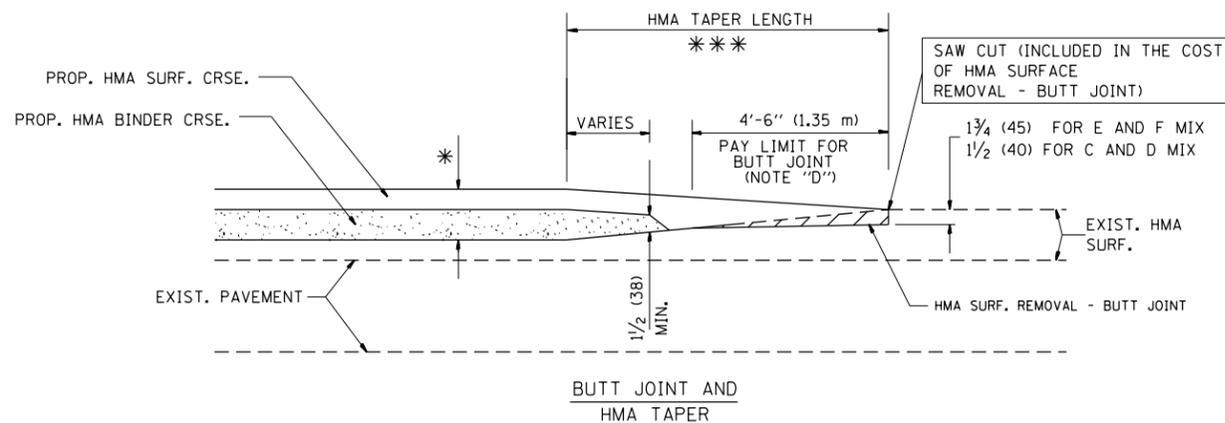
**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

FILE NAME =	USER NAME = rothjp	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
pw:\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\012331\DRAWING\Design\DistStd.dgn			REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/13/2020	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

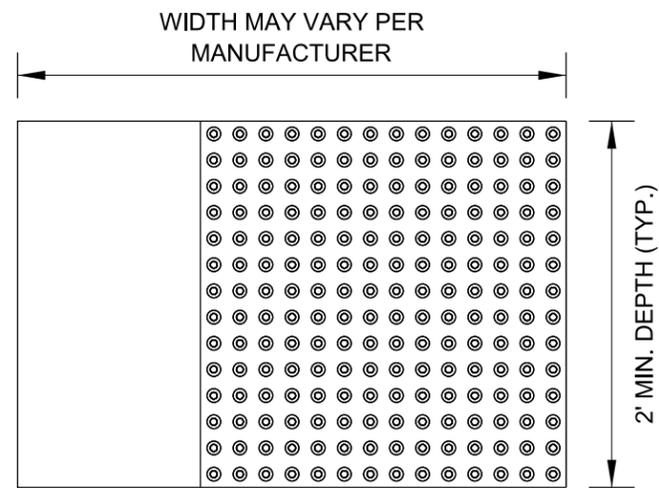
**BUTT JOINT AND  
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

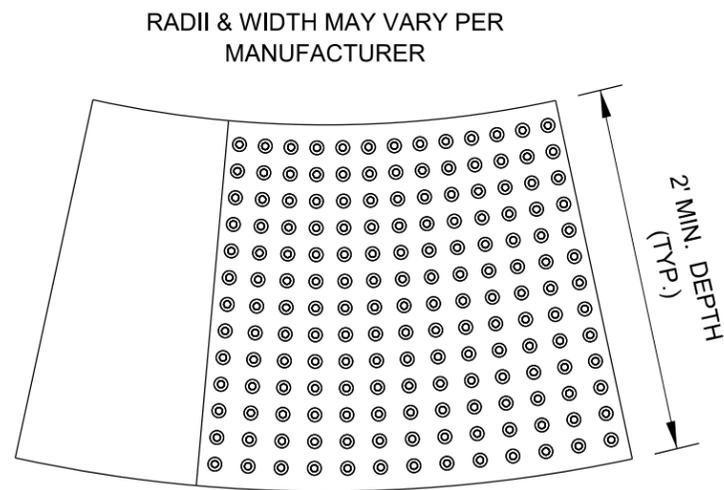
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
*	2019-120-RS&SW	COOK	28	19
BD400-05 BD32			CONTRACT NO. 62J66	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



### STRAIGHT DETECTABLE WARNING UNITS



### RADIAL DETECTABLE WARNING UNITS

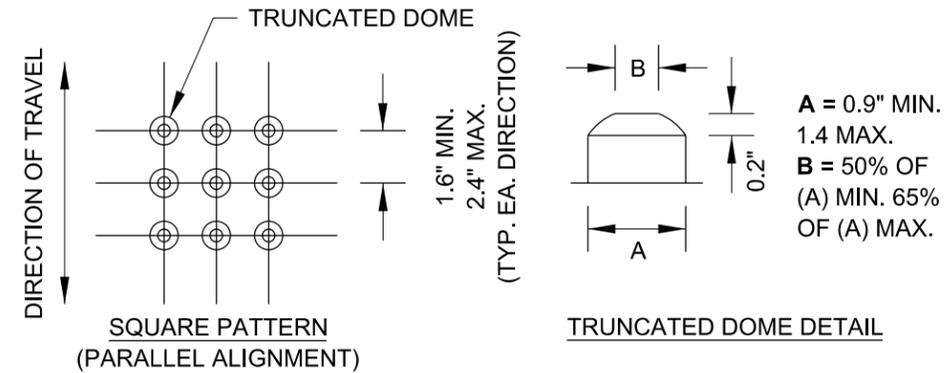


### DETECTABLE WARNING UNIT SIZES

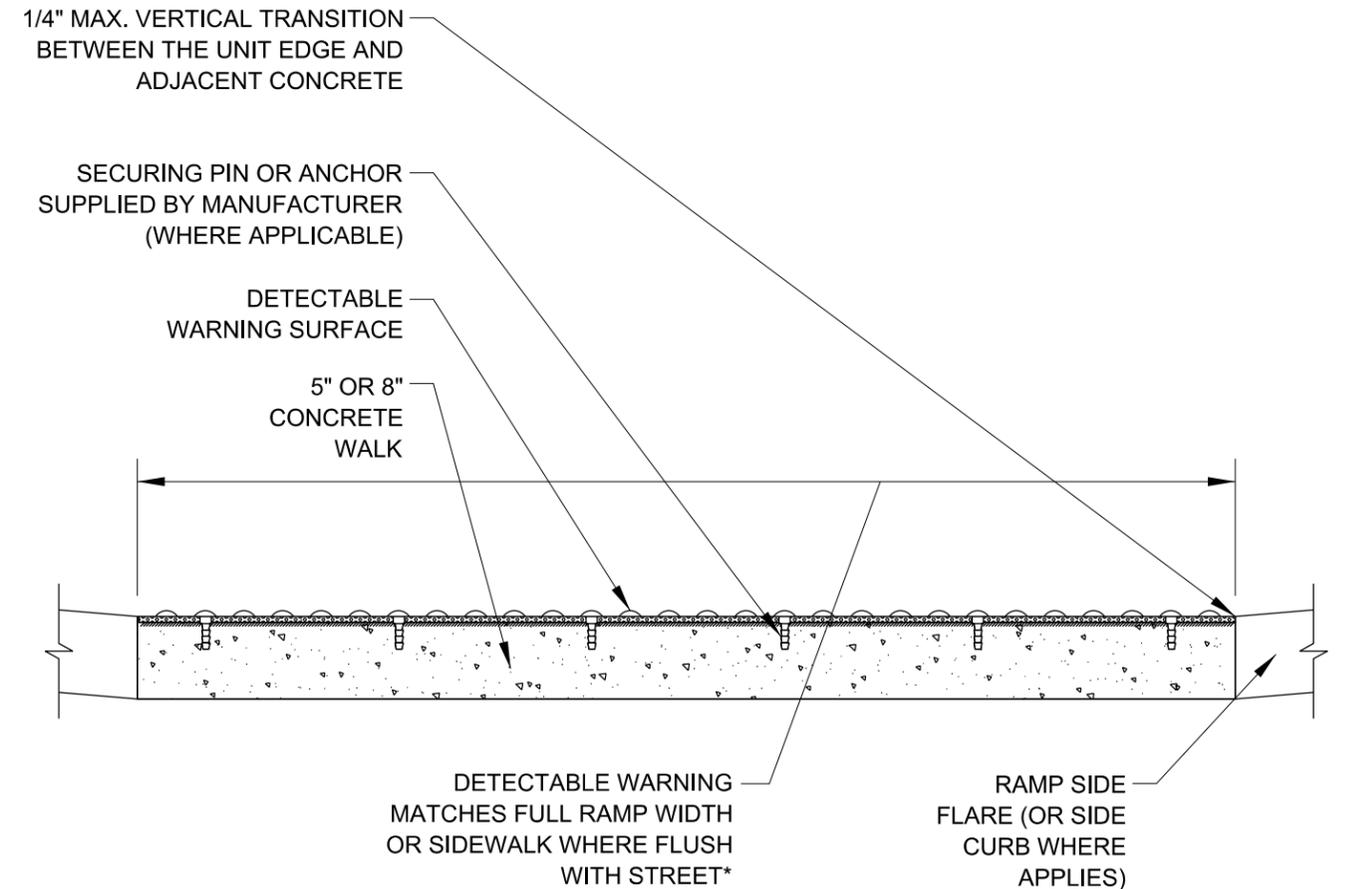
- VERIFY ALL DIMENSIONS WITH THE PRODUCT MANUFACTURER.
- IF USING RADIAL UNITS, VERIFY THAT THE CURB RADIUS MATCHES AVAILABLE UNIT RADII WITH THE PRODUCT MANUFACTURER.

### GENERAL NOTE:

THE ROWS OF DOMES IN THE DETECTABLE WARNING MATERIAL MUST BE ALIGNED WITH THE PATH OF WHEELCHAIR TRAVEL WHICH IS REQUIRED TO BE PERPENDICULAR TO THE GRADE BREAK AT THE BOTTOM OF THE RAMP TO PERMIT TRACKING BETWEEN DOME ROWS. ON BLENDED TRANSITIONS OR FLUSH TRANSITIONS, WHERE RADIAL UNITS ARE SITUATED ABOUT THE CURB RADIUS, DOME ORIENTATION IS NOT SIGNIFICANT.



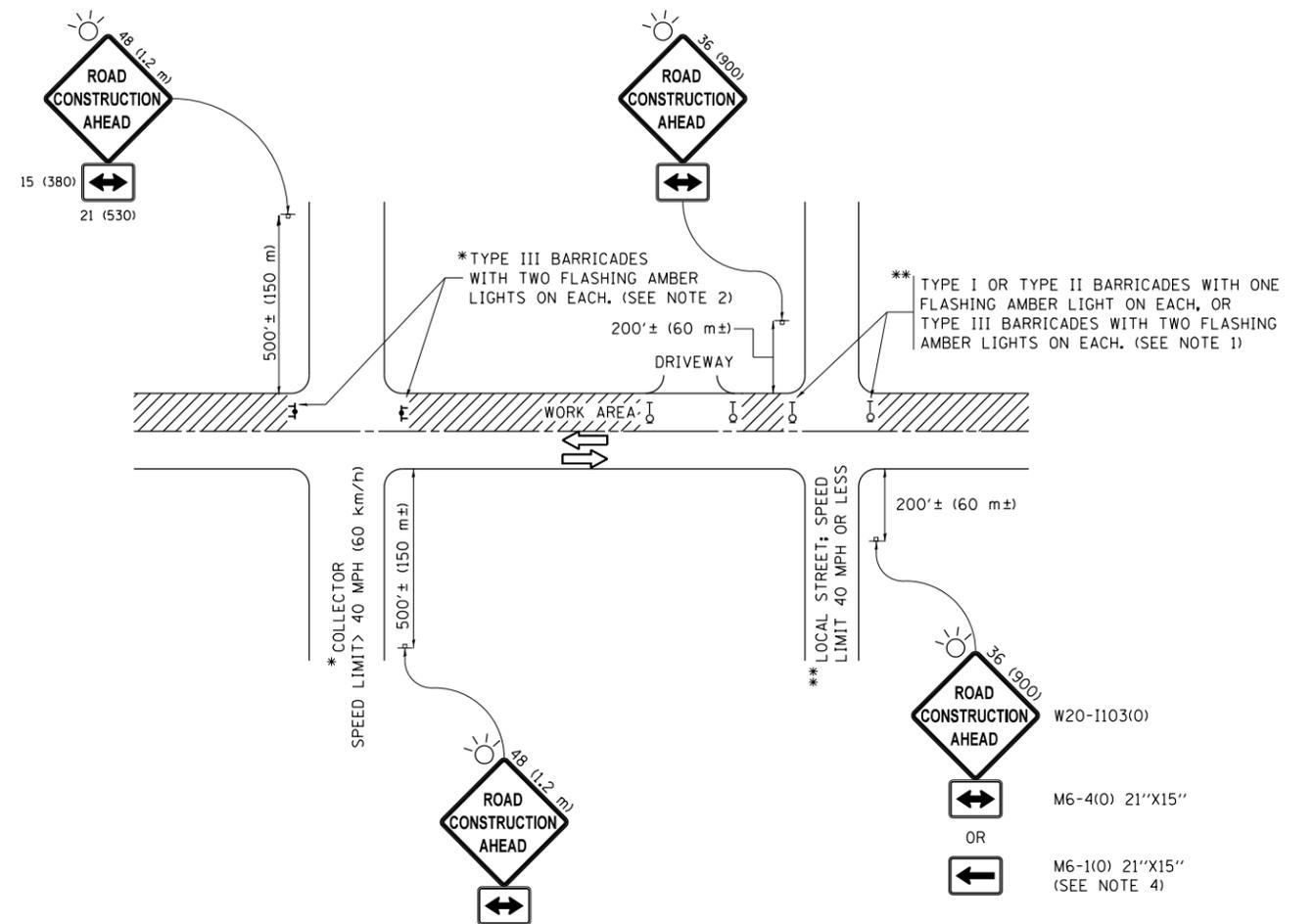
### UNIT PATTERN & DOME DETAIL



\*A BORDER OF 2 INCHES OR LESS AROUND THE DETECTABLE WARNING SURFACE IS ACCEPTABLE IF REQUIRED FOR PROPER INSTALLATION OF THE DETECTABLE WARNING SURFACE PRODUCT

### DETECTABLE WARNING UNIT SECTION

FILE NAME =	USER NAME = rothjp	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CITY OF CHICAGO DETECTABLE WARNINGS</b>			F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw:\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\012331\DRAWING\Design\DistStd.dgn		CHECKED -	REVISED -		SCALE: NONE	SHEET 1	OF 1 SHEETS	STA.	TO STA.	2019-120-RS&SW	COOK	28	21
Default	PLOT SCALE = 100.0000' / in.	DATE - 06-20-2017	REVISED -					<b>BD 58</b>		CONTRACT NO. 62J66			
	PLOT DATE = 1/13/2020				ILLINOIS FED. AID PROJECT								



**NOTES:**

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

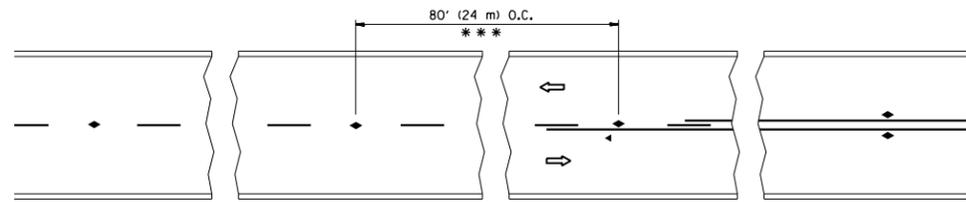
FILE NAME =	USER NAME = rothjp	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
pw:\planroom.dot.illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\012331\DRAWING\Design\DistStd.dgn			REVISED - T. RAMMACHER 01-06-00
Default	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
	PLOT DATE = 1/13/2020	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

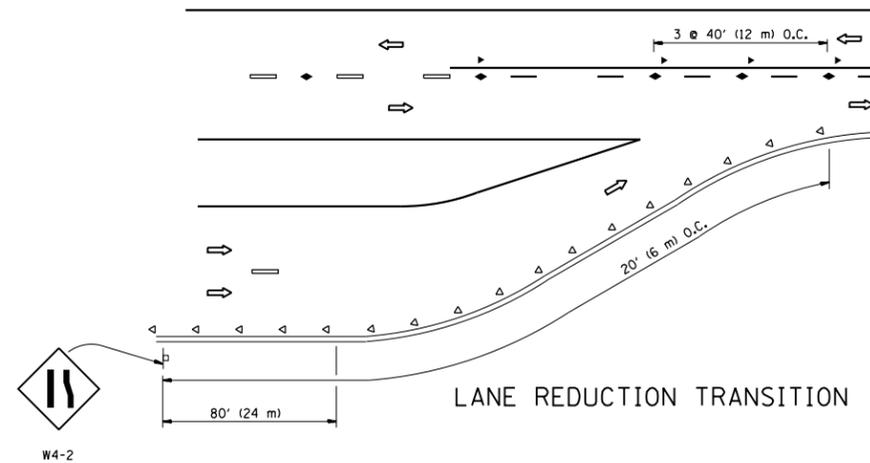
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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<b>TC-10</b>			<b>CONTRACT NO. 62J66</b>	
ILLINOIS FED. AID PROJECT				

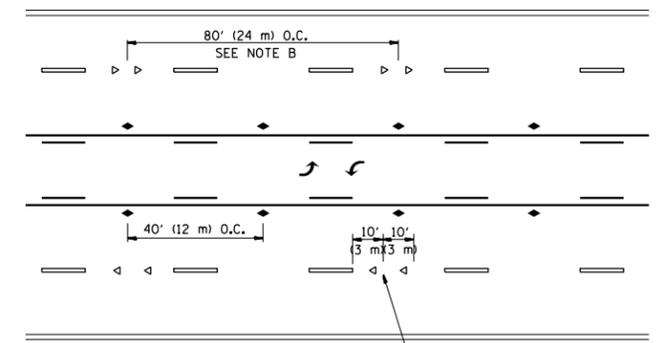


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

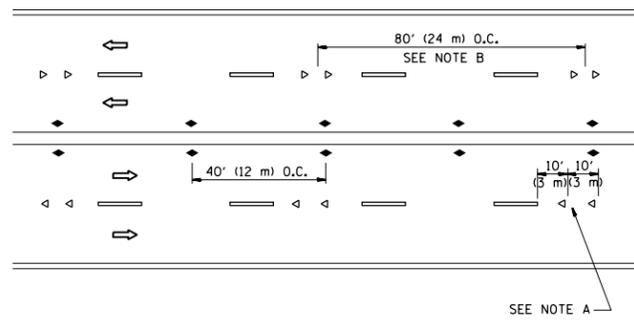
TWO-LANE/TWO-WAY



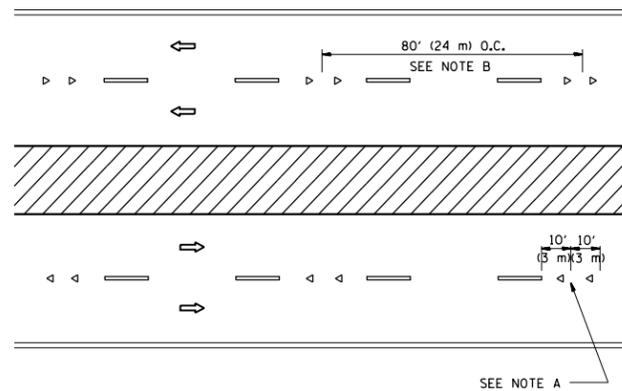
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

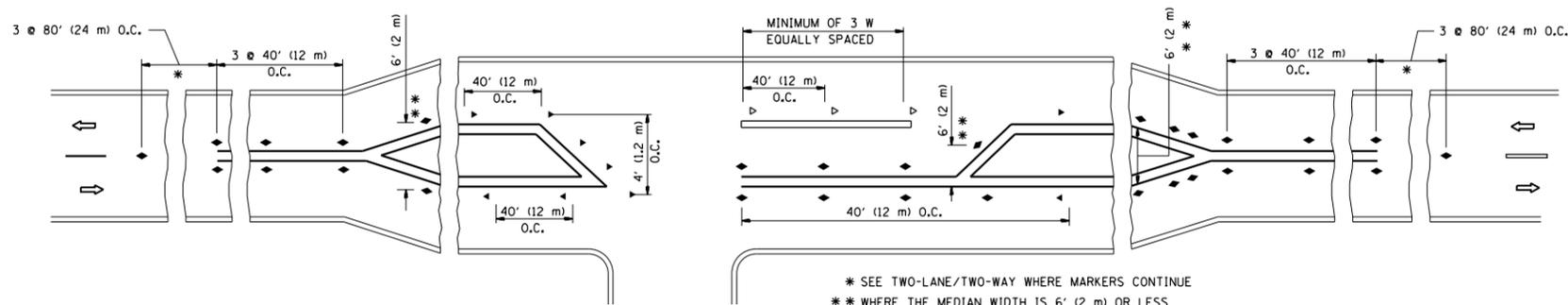
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

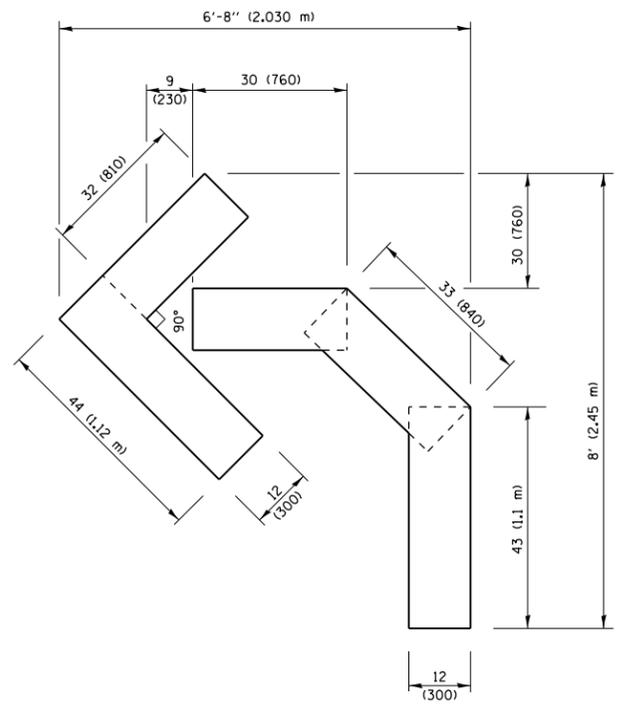
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pw:\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\012331\DRAWING\Design\DistStd.dgn		DRAWN -	REVISED - T. RAMMACHER 03-12-99
	PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED - T. RAMMACHER 01-06-00
	PLOT DATE = 1/13/2020	DATE -	REVISED - C. JUCIUS 09-09-09

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

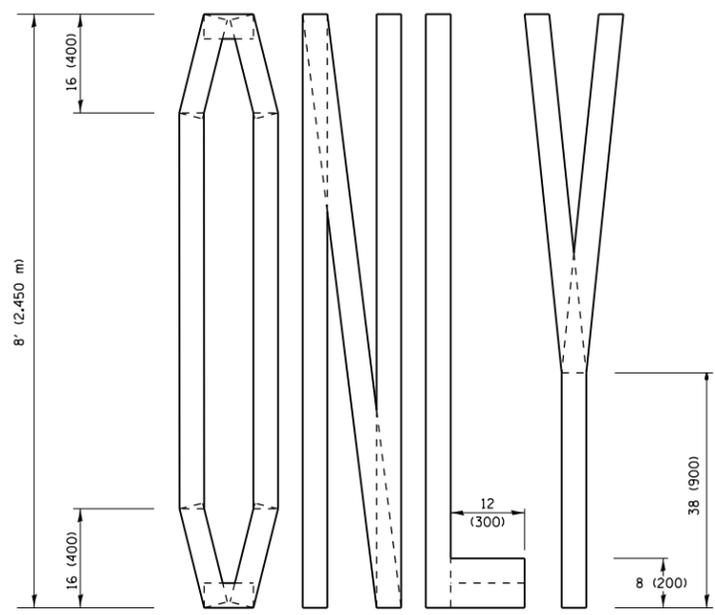
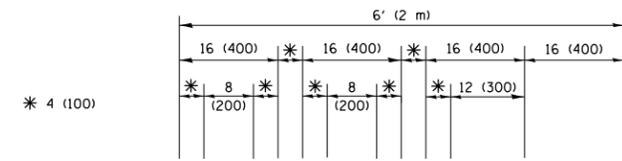
TYPICAL APPLICATIONS  
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

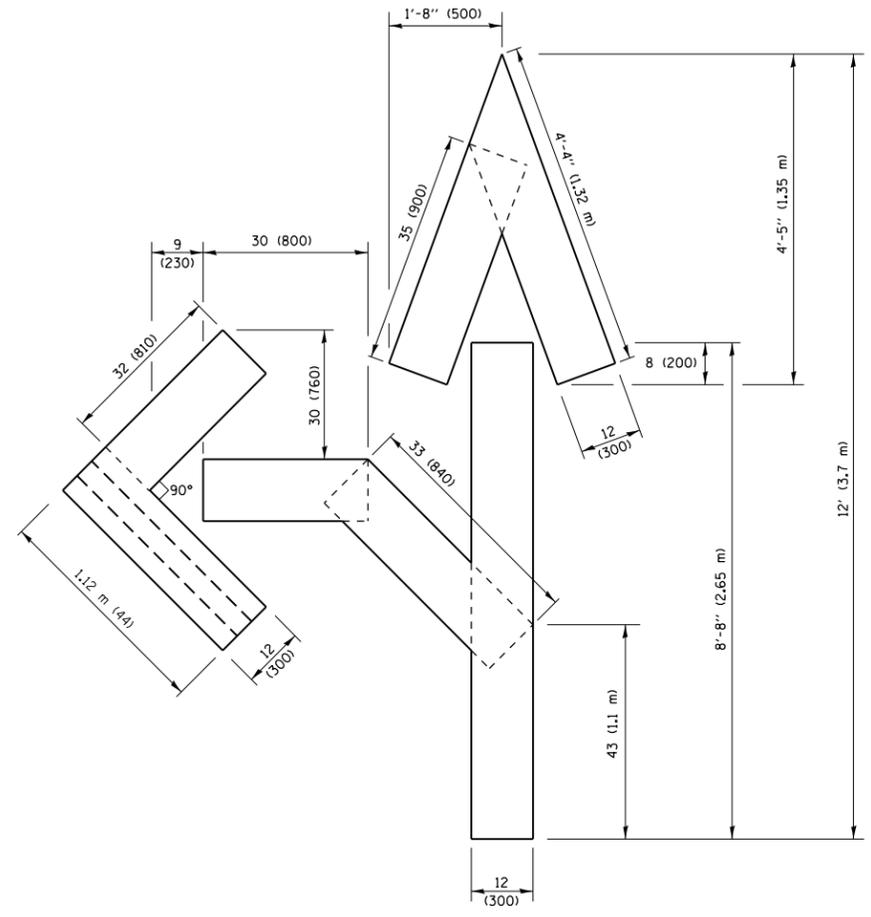
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2019-120-RS&SW	COOK	28	23
TC-11		CONTRACT NO. 62J66		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**QUANTITY**  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.41 sq. m)

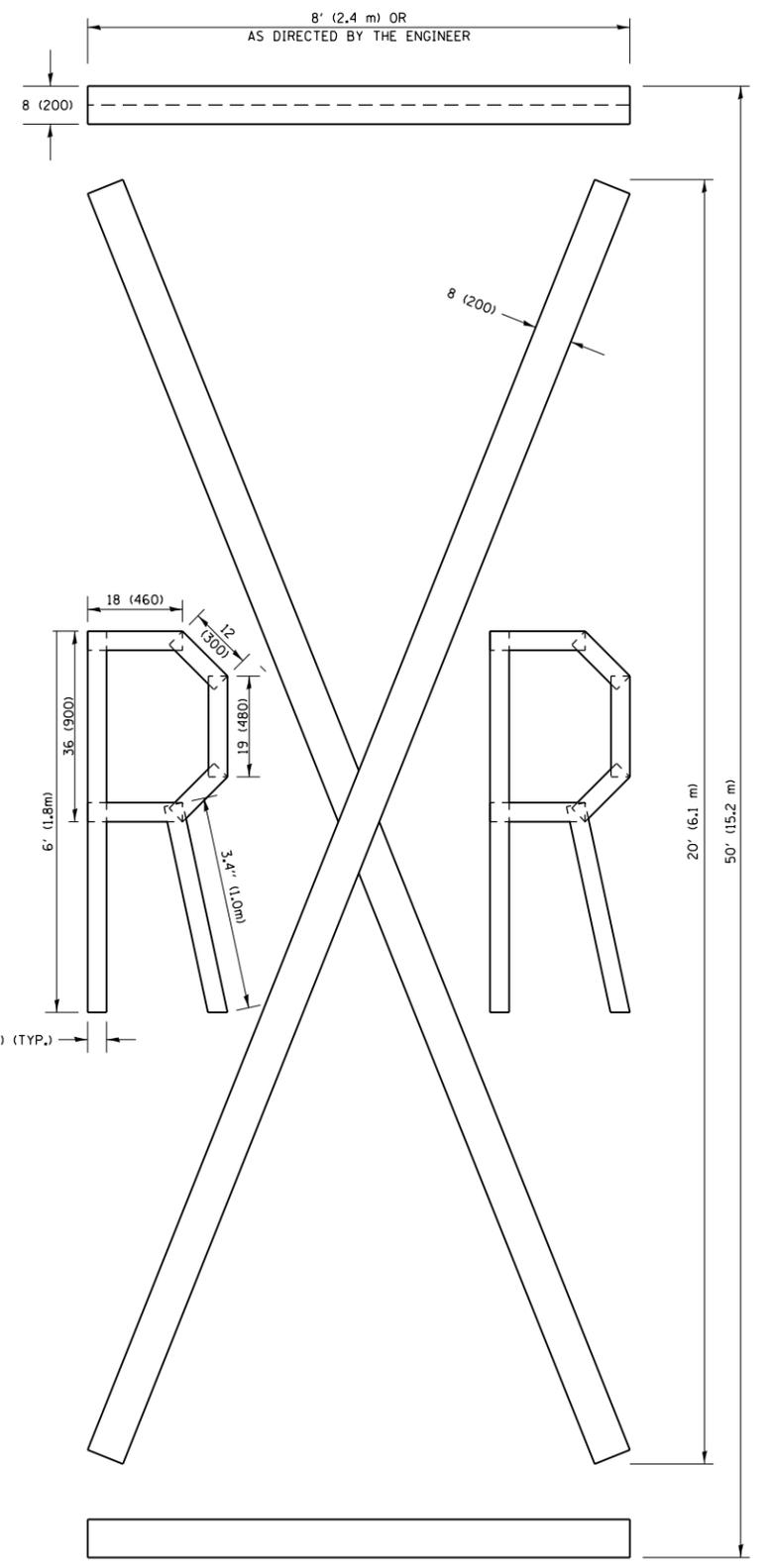


**QUANTITY**  
 4 (100) LINE = 64.1 ft. (19.5 m)  
 21.4 sq. ft. (1.99 sq. m)



**QUANTITY**  
 4 (100) LINE = 82.5 ft. (25.1 m)  
 27.5 sq. ft. (2.53 sq. m)

**NOTE:**  
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



**QUANTITY**  
 4 (100) LINE = 225.9 ft. (68.9 m)  
 75.3 sq. ft. (6.99 sq. m)

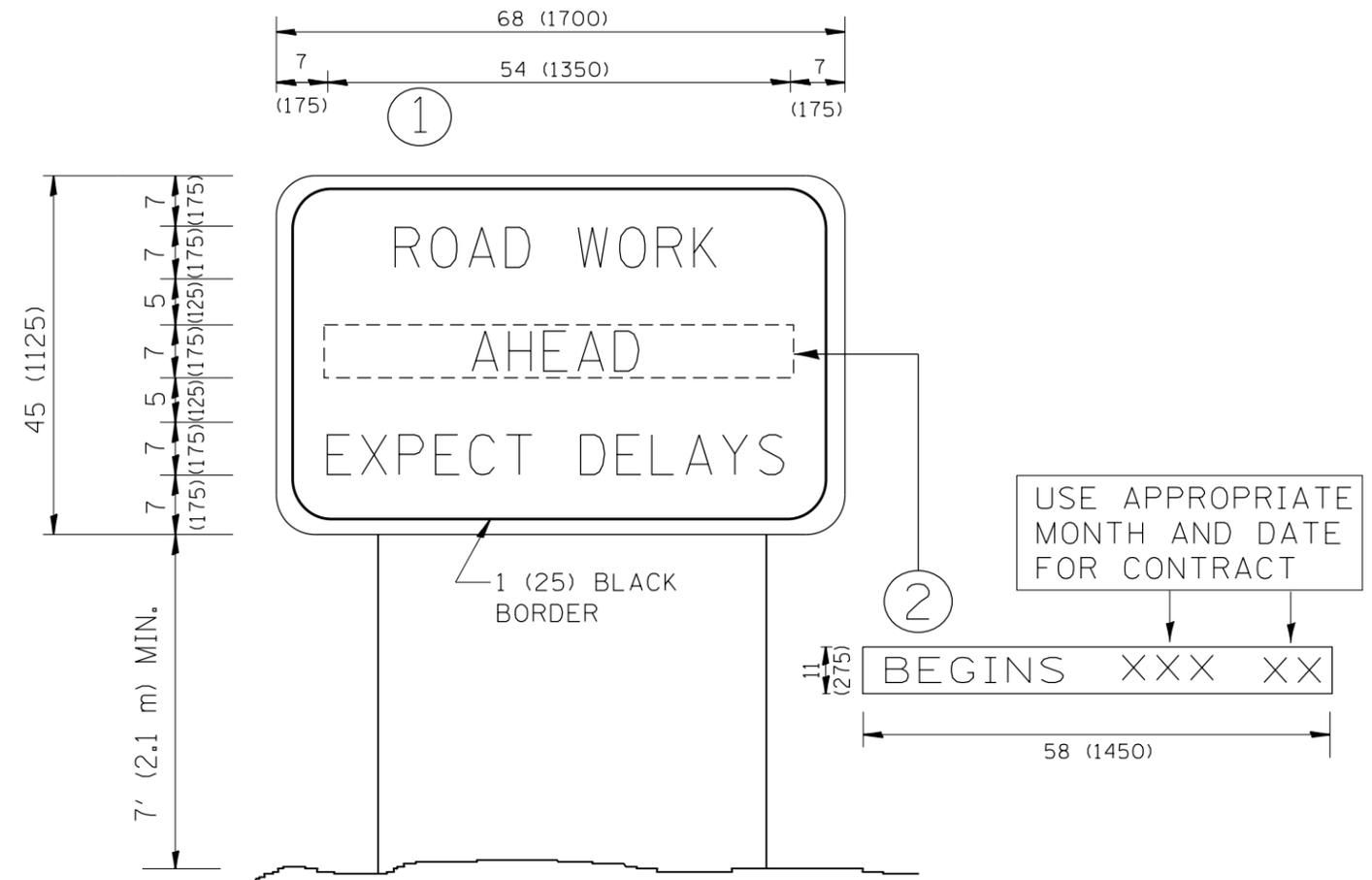
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = rothjp	DESIGNED -	REVISED -T. RAMMACHER 03-02-98
pw:\planroom.dot\illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\012331\Drawings\Design\DistStd.dgn		CHECKED -	REVISED -E. GOMEZ 08-28-00
		DATE -	REVISED -E. GOMEZ 08-28-00
			REVISED -A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2019-120-RS&SW	COOK	28	24
<b>TC-16</b>			<b>CONTRACT NO. 62J66</b>	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

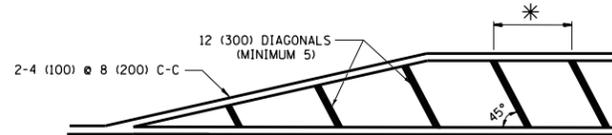
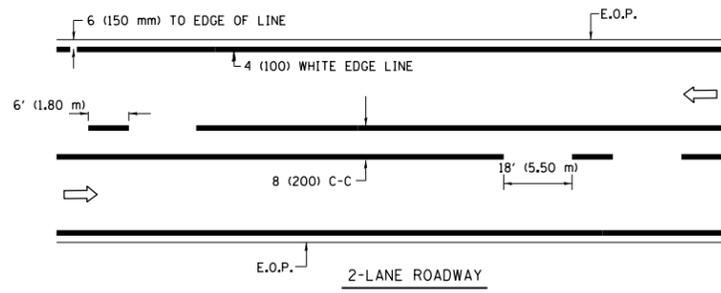
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pw:\planroom.dot\illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\012331\DRAWING\Design\DistStd.dgn		DRAWN -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 1/13/2020	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD  
INFORMATION SIGN**

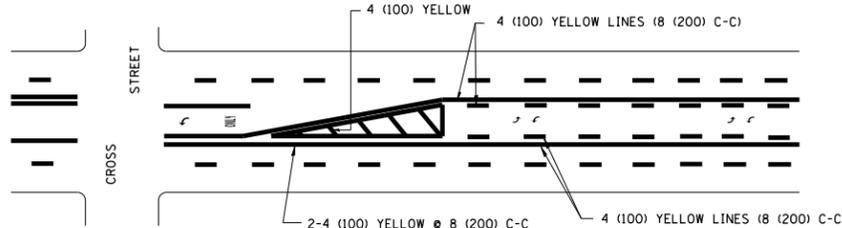
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
•	2019-120-R5&SW	COOK	28	25
TC-22			CONTRACT NO. 62J66	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

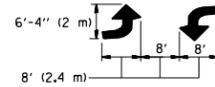


\* FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.  
 \* DIAGONAL LINE SPACING: 20' (6.1 m) C-C

**PAINTED MEDIANS**

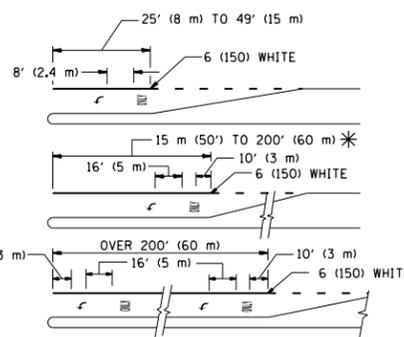
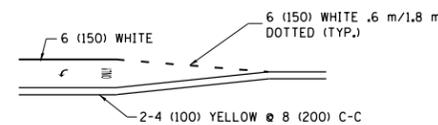


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



**MEDIAN WITH TWO-WAY LEFT TURN LANE**

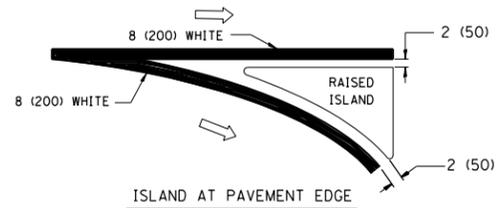
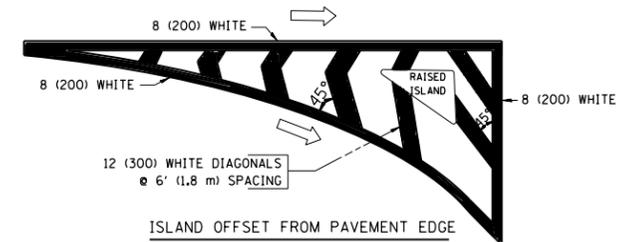
**TYPICAL PAINTED MEDIAN MARKING**



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
 AREA = 15.8 SQ. FT. (1.47 m<sup>2</sup>) ONLY AREA = 22.9 SQ. FT. (2.13 m<sup>2</sup>)  
 \* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

**TYPICAL LEFT (OR RIGHT) TURN LANE**

**TYPICAL TURN LANE MARKING**

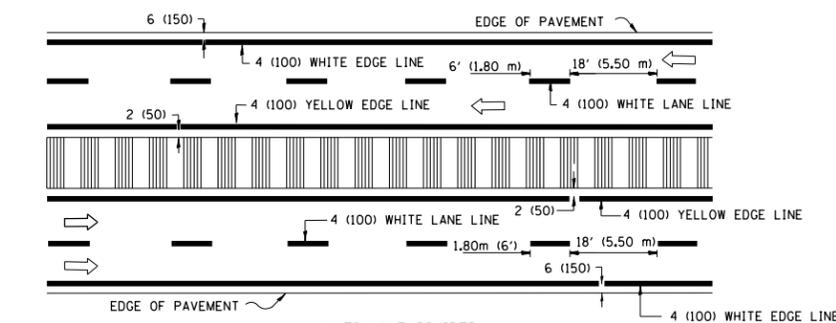
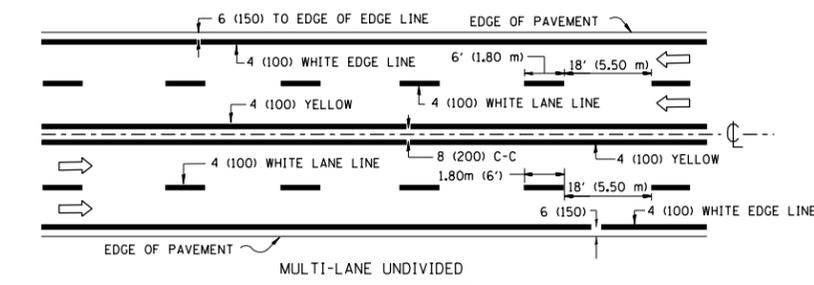


**TYPICAL ISLAND MARKING**

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	8 (200) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	8 (200) C-C
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4 m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4 m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	6' (1.8 m) LINE WITH 18' (5.50 m) SPACE FOR SKIP-DASH; 8 (200) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL & PEDESTRIAN)	12 (300) @ 45° 24 (600) @ 90°	SOLID SOLID	WHITE WHITE	2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	8 (200) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 20' (6.1 m) (LESS THAN 30 MPH (50 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )

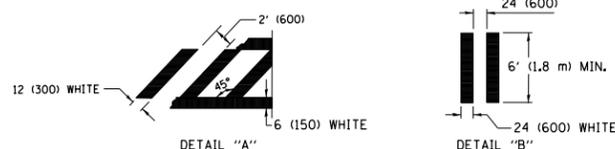
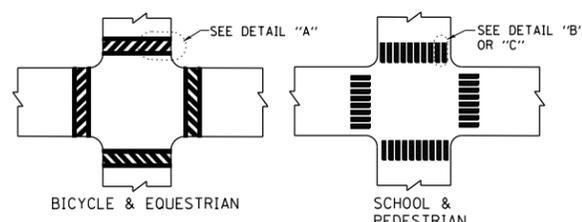
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STREET MARKING STANDARDS, PRINTED BY CITY OF CHICAGO, DEPARTMENT OF TRANSPORTATION, BUREAU OF TRAFFIC.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

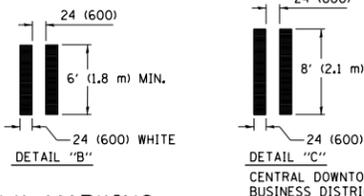


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

**TYPICAL LANE AND EDGE LINE MARKING**



**TYPICAL CROSSWALK MARKING**

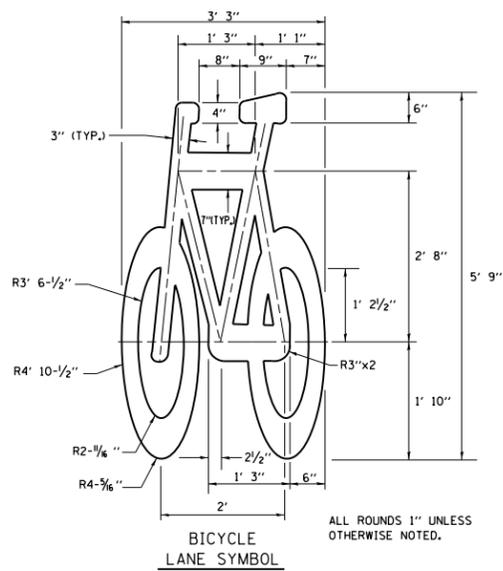
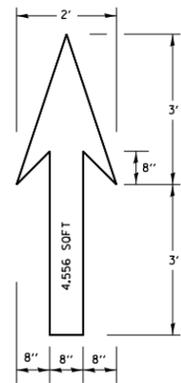


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pw:\planning\dot.illinois.gov\PWIDOT\Documents\IDOT Offices\District 1\Projects\012331\Drawings\Design\DistStd.dgn		CHECKED -	REVISED - K. ENG 02-28-12
		DATE -	REVISED -
			REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

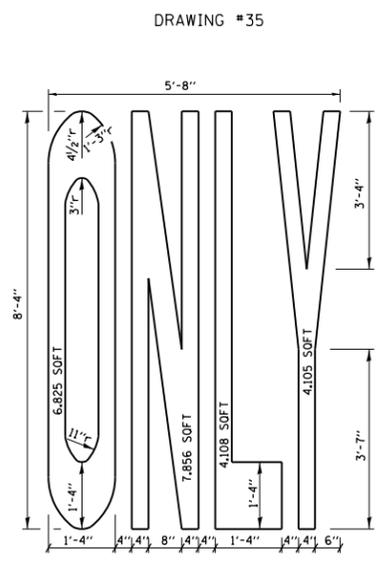
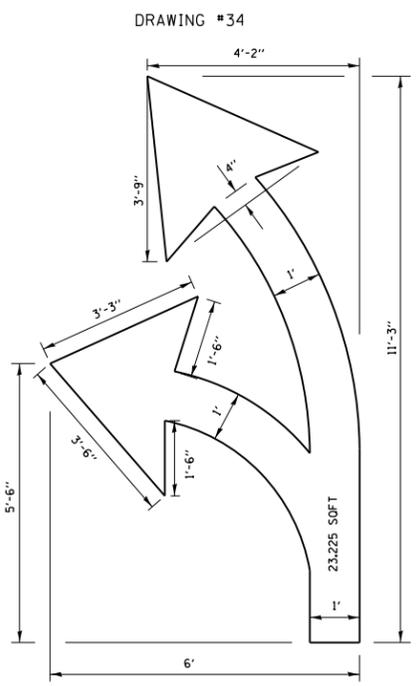
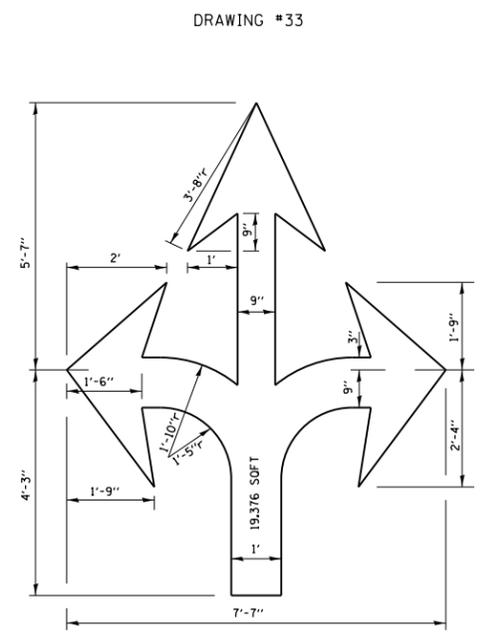
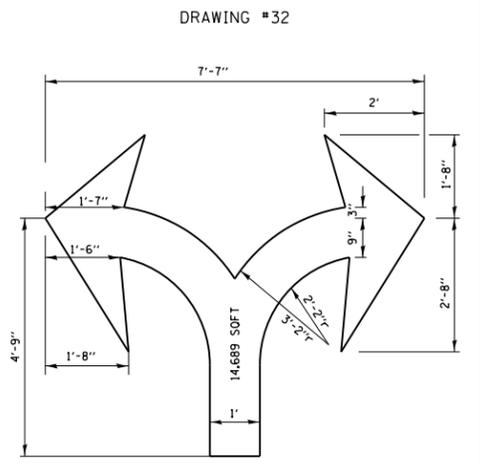
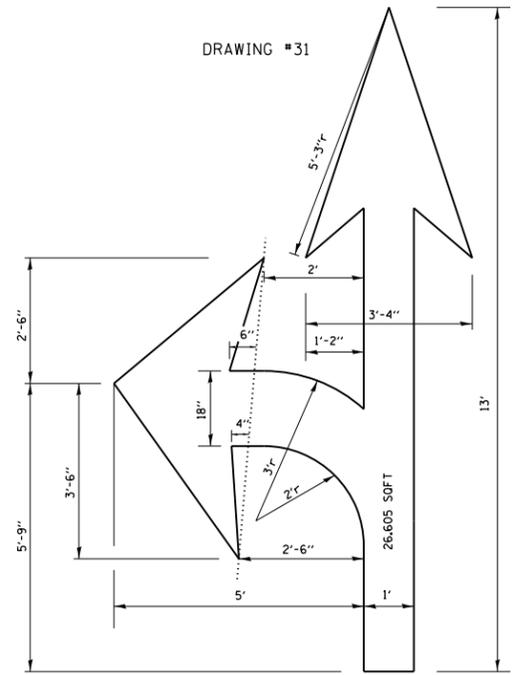
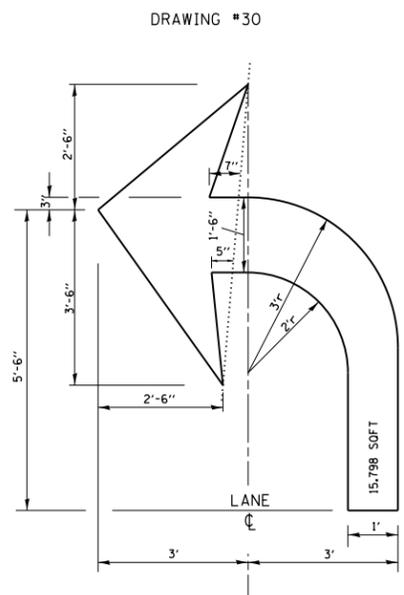
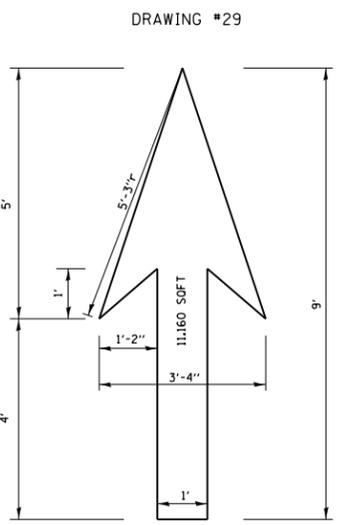
<b>CITY OF CHICAGO</b>			
<b>TYPICAL PAVEMENT MARKINGS</b>			
SCALE: NONE	SHEET NO. 1 OF 3 SHEETS	STA. TO STA.	

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2019-120-RS&SW	COOK	28	26
<b>TC-24</b>		<b>CONTRACT NO. 62J66</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**NOTE:**  
 1.) FOR BIKE LANE SYMBOLS ONLY, USE PRE-FORMED THERMOPLASTIC WITH A MINIMUM THICKNESS OF 90 MILS, MINIMUM SKID RESISTANCE VALUE OF 60 BPN, & A MINIMUM INDEX OF REFRACTION OF 1.50.  
 2.) THE RESIDENT ENGINEER SHALL CONTACT MR. BEN GOMBERG AT 312-744-8093 AT LEAST ONE CALENDAR WEEK PRIOR TO INSTALLING BIKE LANE SYMBOLS.

**TYPICAL BIKE LANE SYMBOLS**  
 DRAWING #28



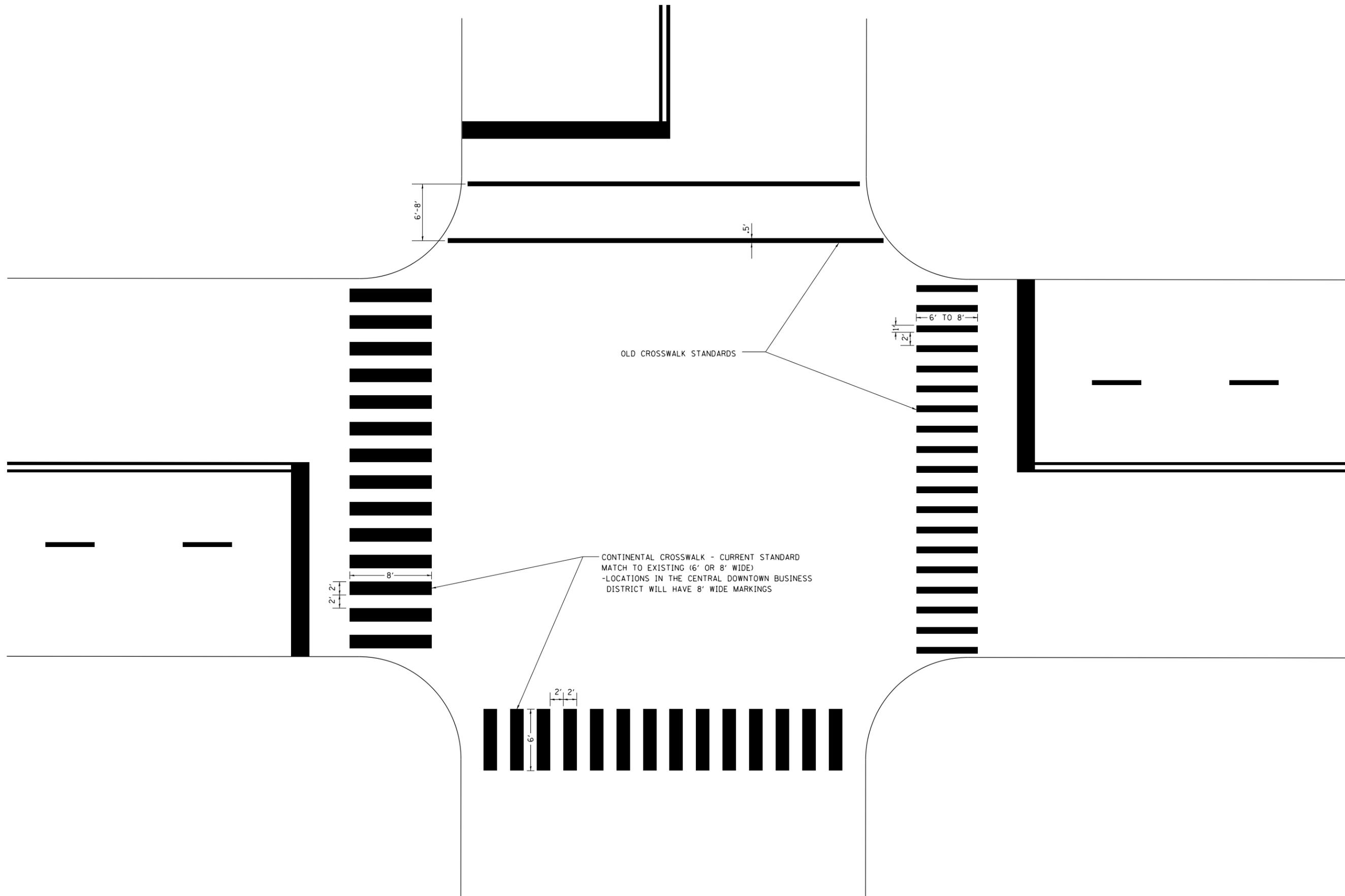
**NOTE:**  
 ALL MARKINGS SHALL BE SOLID WHITE UNLESS OTHERWISE NOTED IN THE PLANS

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PLOT SCALE = 100.0000' / in.		DATE -	REVISED -
PLOT DATE = 1/13/2020			

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

<b>CITY OF CHICAGO</b>			
<b>TYPICAL PAVEMENT MARKINGS</b>			
SCALE: NONE	SHEET NO. 2 OF 3 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	2019-120-R5&SW	COOK	28	27
<b>TC-24</b>		<b>CONTRACT NO. 62J66</b>		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



CONTINENTAL CROSSWALK - CURRENT STANDARD  
 MATCH TO EXISTING (6' OR 8' WIDE)  
 -LOCATIONS IN THE CENTRAL DOWNTOWN BUSINESS  
 DISTRICT WILL HAVE 8' WIDE MARKINGS

OLD CROSSWALK STANDARDS

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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 1/13/2020	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>CITY OF CHICAGO</b>			
<b>TYPICAL PAVEMENT MARKINGS</b>			
SCALE: NONE	SHEET NO. 3 OF 3 SHEETS	STA.	TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
•	2019-120-RS&SW	COOK	28	28
<b>TC-24</b>			<b>CONTRACT NO. 62J66</b>	
FED. ROAD DIST. NO. 1   ILLINOIS FED. AID PROJECT				