

STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION

PROPOSED HIGHWAY PLANS

ROUTE F.A.U. 2710: DES PLAINES RIVER ROAD
FOSTER AVE. TO IL 171 (1st AVENUE)
DESIGNED OVERLAY, NEW SHOULDERS AND
ADA IMPROVEMENTS

SECTION NO.: 2019-098-RS-SW & SR
PROJECT: STP-9SBX (149)
COOK COUNTY
C-91-027-20

F.A.U. RITE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	2019-098-RS-SW&SR	COOK	54	1
FED. ROAD DIST. NO. 1		ILLINOIS CONTRACT NO. 62J88		

D-91-247-20



FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT IS LOCATED IN THE VILLAGES OF
SCHILLER PARK, FRANKLIN PARK
AND RIVER GROVE

TRAFFIC DATA:

2018 ADT - 12800 TO 27700 VPD
SPEED LIMIT - 35 TO 45 MPH

OMISSIONS:

NORTH CENTRAL RAILROAD
STA. 82+91 TO STA. 83+55

AT BELMONT AVE.
STA. 87+49 TO STA. 107+64

AT IRVING PARK RD.
STA. 159+39 TO STA. 162+53

PROJECT ENDS
STA. 238+91

PROJECT BEGINS
STA. 14+89



LEYDEN TOWNSHIP

GROSS LENGTH OF PROJECT - 22402 FEET - 4.24 MILES
NET LENGTH OF PROJECT - 20109 FEET - 3.80 MILES



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: VESELIN VELICHKOV (847) 705-4432
PROJECT MANAGER: FAWAD AQUEEL (847) 705-4247

CONTRACT NO. 62J88

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED December 13, 2019

Anthony J. Puzio (CAS)
REGIONAL ENGINEER

Jan 31, 2020
ENGINEER OF DESIGN AND ENVIRONMENT

Jan 31, 2020
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

INDEX OF SHEETS	
SHEET NO.	DESCRIPTION:
1	COVER SHEET
2	INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES
3-5	SUMMARY OF QUANTITIES
6-13	EXISTING AND PROPOSED TYPICAL SECTIONS
14-21	PROPOSED ROADWAY AND PAVEMENT MARKING PLANS - DES PLAINES RIVER ROAD
22	DRAINAGE STRUCTURE REPAIR PLAN - LAWRENCE AVE
23-31	ADA SIDEWALK RAMP DETAILS
32	DETAILS FOR FRAMES AND LIDS ADJUSTMENTS WITH MILLING (BD-08)
33	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)
34	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)
35	BUTT JOINT AND HMA TAPER DETAILS (BD-32)
36	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (TC-10)
37	TYPICAL APPLICATION FOR RAISED REFLECTIVE PAVEMENT MARKERS (TC-11)
38	DISTRICT ONE TYPICAL PAVEMENT MARKINGS (TC-13)
39	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TC-14)
40	SHORT-TREM PAVEMENT MARKING LETTERS AND SYMBOLS (TC-16)
41	TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING TREATMENT FOR RAILROAD CROSSINGS (TC-23)
42	DRIVEWAY ENTRANCE SIGNING (TC-26)
43	DISTRICT ONE - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING (TS-07)
44-53	DETECTOR LOOP LOCATION DETAILS
54	ARTERIAL ROAD INFORMATION SIGNING (TC-22)

STATE STANDARDS	
000001-07	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-11	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424006-04	DIAGONAL CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
604001-05	FRAME AND LIDS, TYPE 1
604006-05	FRAME AND GRATE, TYPE 3
606001-07	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701006-05	OFF ROAD OPERATIONS, 2L, 2W, 15' (4.5M) TO 24" (600MM) FROM PAVEMENT EDGE
701011-04	OFF-ROAD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701101-05	OFF ROAD OPERATIONS, MULTILANE, 15' (4.5M) TO 24" (600MM) FROM PAVEMENT EDGE
701301-04	LANE CLOSURE 2L, 2W, SHORT-TIME OPERATIONS
701311-03	LANE CLOSURE 2L, 2W, MOVING OPERATION DAY ONLY
701426-09	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION FOR SPEEDS GREATER THAN OR EQUAL TO 45MPH
701427-05	LANE CLOSURE, MULTILANE, OR INTERMITTENT OR MOVING OPERATION FOR SPEEDS LESS THAN OR EQUAL TO 40 MPH
701602-10	URBAN LANE CLOSURE, MULTILANE, 2W WITH BI-DIRECTIONAL LEFT TURN LANE
701611-01	URBAN HALF ROAD CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRANSVERSABLE MEDIAN
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	LANE CLOSURE, MULTILANE, 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUTS FOR DETECTION LOOPS

GENERAL NOTES:

BEFORE STARTING ANY EXCAVATION THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR "CUAN" (CHICAGO UTILITY ALERT NETWORK) AT 312-744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES, (48 HOURS NOTIFICATION IS REQUIRED).

10 FEET (3 METERS) TRANSITION SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTERS AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITION SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH THE UTILITY COMPANIES AND THE VILLAGES OF SCHILLER PARK, NORRIDGE, FRANKLIN PARK AND RIVER GROVE.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.

ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (OR COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED ON THE PLANS)), WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND EXISTING CONDITIONS IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENT SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT IN PART OF THIS CONTRACT.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

WHEN THE MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1/2 INCHES (40 mm) WHERE THE SPEED LIMIT IS 40 MPH (80 km/ h) OR LESS AND 1 INCH (25 mm) WHERE THE SPEED LIMIT IS GREATER THAN 40 MPH (80 km/ h). WITH WRITTEN APPROVAL OF THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 mm) MAY BE ALLOWED IF THE EDGE OF THE MILLING MACHINE IS SLOPED A MINIMUM (1:3).

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL CENTER SUPERVISOR FOR ARTERIALS AT KALPANA.KANNAN-HOSADURGA @ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH THE "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" DETAIL. (TC-13)

RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH THE "TYPICAL APPLICATIONS FOR RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" DETAIL. (TC-11)

THE RESIDENT ENGINEER SHALL CONTACT KYLIE VOGRIN, AREA TRAFFIC FIELD ENGINEER, AT KYLIE.VOGRIN@ILLINOIS.GOV, A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN ACCESS AT ALL TIMES DURING CONSTRUCTION.

THE CONTRACTOR SHALL CONTACT THE ROADSIDE DEVELOPMENT UNIT AT (847) 705-4171 AT LEAST 2 WEEKS PRIOR TO BEGINNING ANY FORESTRY WORK FOR LAYOUT.

FILE NAME =	USER NAME = Velichkovv	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS, STATE STANDARDS, AND GENERAL NOTES FAU 2710 (DES PLAINES RIVER RD.)—FOSTER AVE. TO IL 171 (1ST AVE.)	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
pw\plnroom.dot.illinois.gov\PIDOT\Documents\DOT Offices\District 1\Projects\124720\Drawings\Design\124720-sht-pln.dgn		CHECKED -	REVISED -			2710	2019-098-RS-SW&SR	COOK	54	2	
PLOT SCALE = 100.0000' / in.		DATE -	REVISED -			CONTRACT NO. 62J88					
PLOT DATE = 12/13/2019						SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	(ILLINOIS) FED. AID PROJECT	

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		80% FEDERAL 20% STATE 0005	100% STATE			
20200100	EARTH EXCAVATION	CU YD	22	22				
20200600	EXCAVATING AND GRADING EXISTING SHOULDER	UNIT	117	117				
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	331	326	5			
25003210	INTERSEEDING, CLASS 2A	ACRE	7	7				
25200110	SODDING, SALT TOLERANT	SO YD	331	326	5			
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	82747	82747				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	184	184				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	908	908				
40602985	HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70	TON	13730	13730				
40604060	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	8	8				
40604062	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	TON	1655	1655				
40605026	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80	TON	12014	12014				
42001300	PROTECTIVE COAT	SO YD	1100	1100				

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		80% FEDERAL 20% STATE 0005	100% STATE			
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	2180	2180				
42400800	DETECTABLE WARNINGS	SO FT	149	149				
44000164	HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4"	SO YD	122588	122588				
44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	36	36				
44000600	SIDEWALK REMOVAL	SO FT	2431	2431				
44201815	CLASS D PATCHES, TYPE II, 14 INCH	SO YD	1226	1226				
44201819	CLASS D PATCHES, TYPE III, 14 INCH	SO YD	736	736				
44201821	CLASS D PATCHES, TYPE IV, 14 INCH	SO YD	491	491				
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	459	459				
60200105	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	2		2			
60250400	CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID	EACH	5	5				
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	12	12				
60250500	CATCH BASINS TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	8	8				
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	34	34				
60261530	INLETS TO BE ADJUSTED WITH NEW TYPE 23 FRAME AND GRATE	EACH	5	5				
60500080	REMOVING CATCH BASINS TO MAINTAIN FLOW	EACH	2		2			
60266600	VALVE BOXES TO BE ADJUSTED	EACH	1	1				
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	78	78				
67100100	MOBILIZATION	L SUM	1	1				
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	8	8				

FILE NAME =	USER NAME = veltchkovv	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES FAU 2710 (DES PLAINES RIVER RD.)—FOSTER AVE. TO IL 171 (1ST AVE.)	SCALE:	SHEET NO.	OF	SHEETS	STA.	TO	STA.	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\planroom\dtd\illinois\gov\PI\DOT\Documents\DOT\Office\District\Projects\DI24720\CADD\td\Design\DI24720-01\DRAWN	PLLOT SCALE = 100.0000' / 1in	CHECKED -	REVISED -			2710	2019-098-R5-SW&SR	COOK	54	3							
PLLOT DATE = 12/13/2019	DATE -	REVISED -								CONTRACT NO. 62J88							
												FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE						
CODE NO	ITEM	UNIT	TOTAL QUANTITIES URBAN	80% FEDERAL 20% STATE 0005	100% STATE					CODE NO	ITEM	UNIT	TOTAL QUANTITIES URBAN	80% FEDERAL 20% STATE 0005	100% STATE				
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	LSUM	1	1						70300100	SHORT TERM PAVEMENT MARKING	FOOT	15398	15398					
* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	LSUM	1	1						70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	2567	2567					
* 66901006	REGULATED SUBSTANCES MONITORING	CAL DA	30	30						70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	515	515					
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6						70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	49412	49412					
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1						70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	4061	4061					
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1						70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	601	601					
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1						70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	1082	1082					
70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	L SUM	1	1						70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	1116	1116					
70102634	TRAFFIC CONTROL AND PROTECTION, STANDARD 701611	L SUM	1	1					* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	515	515						
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1					* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	49412	49412						
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1					* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	4061	4061						
									* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	601	601						
																	* SPECIALTY ITEMS		
																	<input type="checkbox"/> NON PARTICIPATING ITEMS		

FILE NAME =	USER NAME = Vellchokov	DESIGNED -	REVISED -
pw:\planroom\dtd\illinois\gov\PI\DOT\Documents\DOT\Projects\District\Projects\024720\CAD\Date\Design\024720-31\DRAWN			
PLOT SCALE = 100.0000' / 1in	CHECKED -	REVISED -	
PLOT DATE = 12/13/2019	DATE -	REVISED -	

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES			
FAU 2710 (DES PLAINES RIVER RD.)—FOSTER AVE. TO IL 171 (1ST AVE.)			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	2019-098-RS-SW&SR	COOK	54	4
CONTRACT NO. 62J88				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		80% FEDERAL 20% STATE 0005	100% STATE			
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1082	1082				
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	1116	1116				
* 78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	3748	3748				
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	1539	1539				
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	1385	1385				
* 85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	3	3				
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	3068	3068				
* 89500400	RELOCATE EXISTING PEDESTRIAN PUSH-BUTTON	EACH	1	1				
* 89502376	REBUILD EXISTING HANDHOLE	EACH	3	3				
X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1	1				
X2020110	GRADING AND SHAPING SHOULDERS	UNIT	115	115				
* X2700003	GROOVING FOR RECESSED PAVEMENT MARKING 8"	FOOT	5778	5778				

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		80% FEDERAL 20% STATE 0005	100% STATE			
* X2700004	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 7"	FOOT	2030	2030				
X4421790	CLASS D PATCHES, TYPE II, 12 INCH (SPECIAL)	SO YD	16	16				
X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	1140	1140				
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	21	21				
X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SO FT	2566	2566				
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	1414	1384	30			
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	56		56			
Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	51.4	51.4				
Z0018600	DRAINAGE STRUCTURES TO BE RECONSTRUCTED	EACH	3	3				
Z0033700	LONGITUDINAL JOINT SEALANT	FOOT	62168	62168				
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1				
Ø Z0076600	TRAINEES	HOUR	500	500				
Ø Z0076604	TRAINEES - TRAINING PROGRAM GRADUATES	HOUR	500	500				

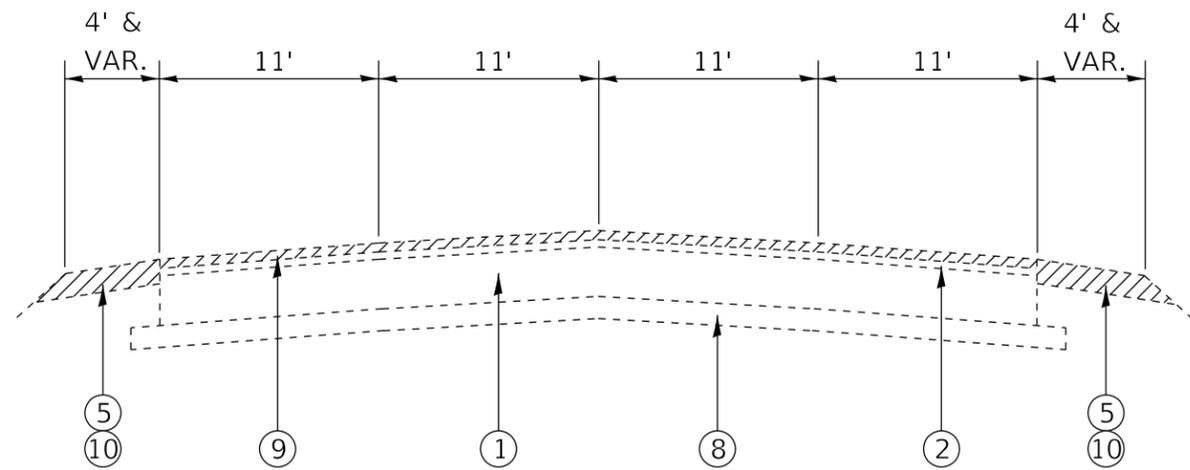
* SPECIALTY ITEMS
 NON PARTICIPATING ITEMS
 Ø 0042

FILE NAME =	USER NAME = Veltchkovv	DESIGNED -	REVISED -
pw:\planroom\dtd\illinois\gov\PI\DOT\Documents\DOT\	Offices\District\Projects\024720\CAD\Drawn\Design\024720-84	DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / 1in	CHECKED -	REVISED -
	PLOT DATE = 12/14/2019	DATE -	REVISED -

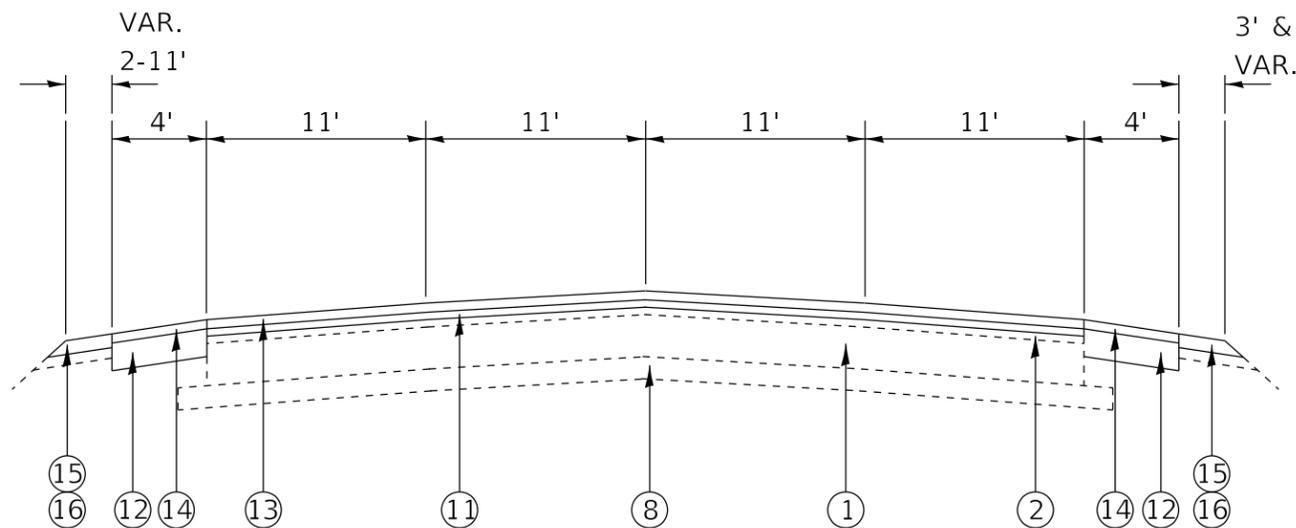
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES
FAU 2710 (DES PLAINES RIVER RD.)—FOSTER AVE. TO IL 171 (1ST AVE.)**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	2019-098-R5-SW&SR	COOK	54	5
CONTRACT NO. 62J88				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



EXISTING TYPICAL SECTION
 STA. 14+88.7 TO 16+59
 STA. 44+11 TO 61+64
 STA. 74+27 TO 82+91
 STA. 144+15 TO 151+33



PROPOSED TYPICAL SECTION
 STA. 14+88.7 TO 16+59
 STA. 44+11 TO 61+64
 STA. 74+27 TO 82+91
 STA. 144+15 TO 151+33

LEGEND:

- ① EXISTING P.C.C. PAVEMENT ±8"
- ② EXISTING HOT-MIX ASPHALT SURFACE ±3" (AFTER MILLING)
- ③ EXISTING P.C.C. BARRIER MEDIAN
- ④ EXISTING HOT-MIX ASPHALT BARRIER MEDIAN
- ⑤ EXISTING AGGREGATE SHOULDER
- ⑥ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ⑦ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.12
- ⑧ EXISTING STABILIZED SUB-BASE
- ⑨ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 3¾"
- ⑩ PROPOSED AGGREGATE SHOULDER REMOVAL
- ⑪ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 2"
- ⑫ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 6¼"
- ⑬ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, MIX "F", 9.5, N80, 1¾"
- ⑭ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1¾"
- ⑮ PROPOSED GRADING AND SHAPING SHOULDERS
- ⑯ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)

NOTES:

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE QUANTITIES IS 112 LBS./SQ. YD./ IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

THE MILLING SHALL BE DONE PRIOR TO PATCHING

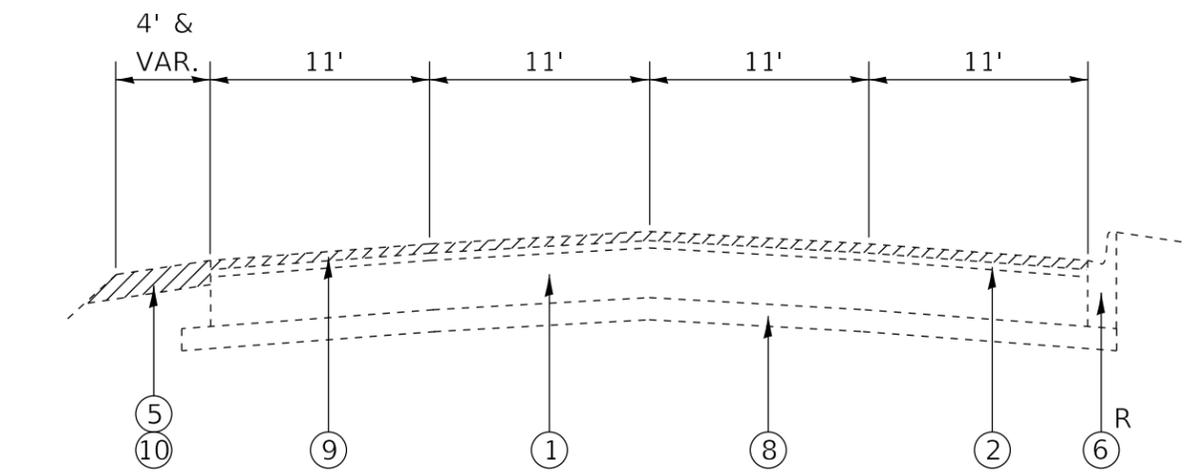
THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE HMA BINDER IL-9.5

QUALITY MANAGEMENT PROGRAM (OMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE

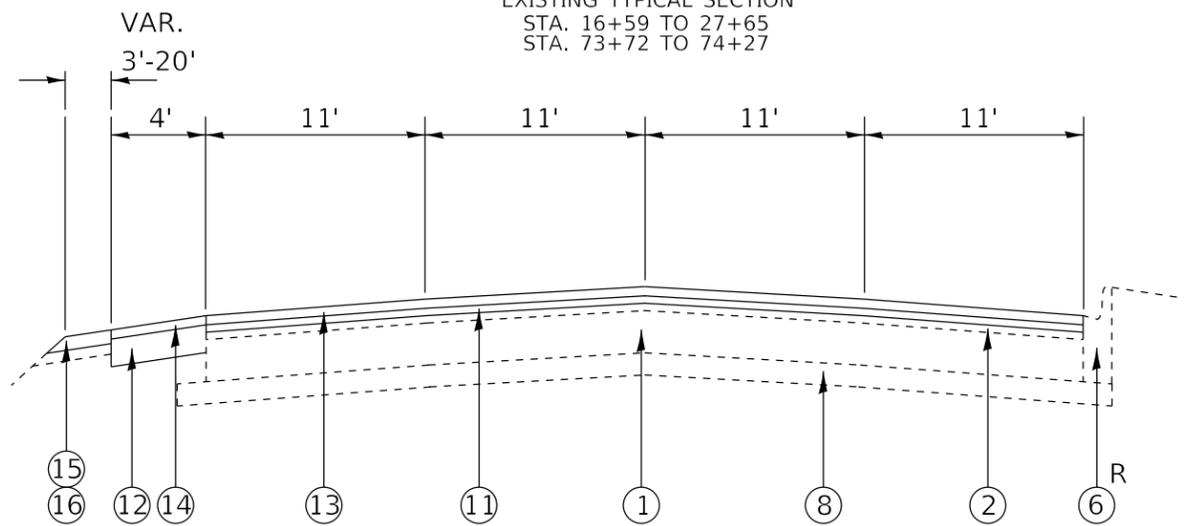
FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS

MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ Ndes	OMP
PATCHING		
CLASS "D" PATCHES (HMA BINDER COURSE, IL-19MM)	4% @ 70 GYR.	QC / OA
HMA SHOULDER, 8"		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	4% @ 70 GYR.	QC / OA
HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70	4% @ 70 GYR.	QC / OA
PAVEMENT RESURFACING		
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, MIX "F", 9.5, N80	3.5% @ 80 GYR.	PPF
HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70	4% @ 70 GYR.	OCP
OMP DESIGNATION: QUALITY CONTROL FOR PERFORMANCE (OCP); QUALITY CONTROL/ QUALITY ASSURANCE (QC/ OA); PAY FOR PERFORMANCE (PPF)		



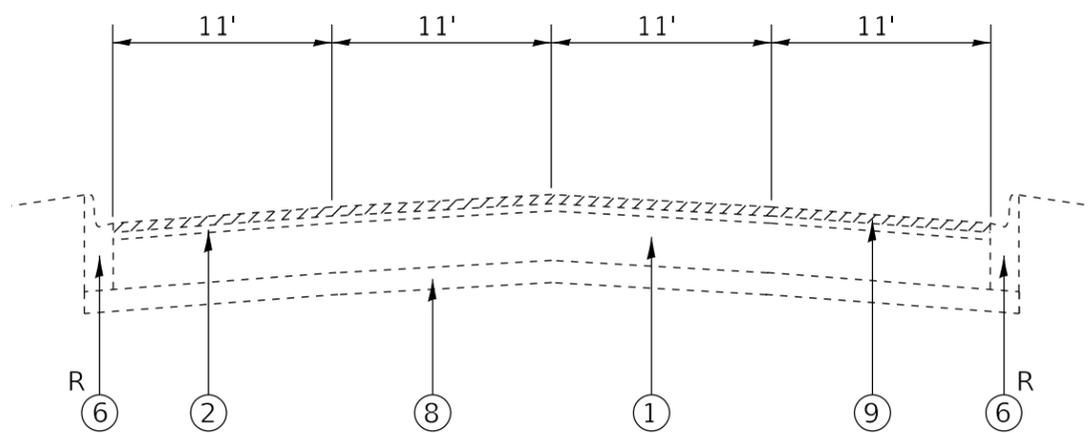
EXISTING TYPICAL SECTION
STA. 16+59 TO 27+65
STA. 73+72 TO 74+27



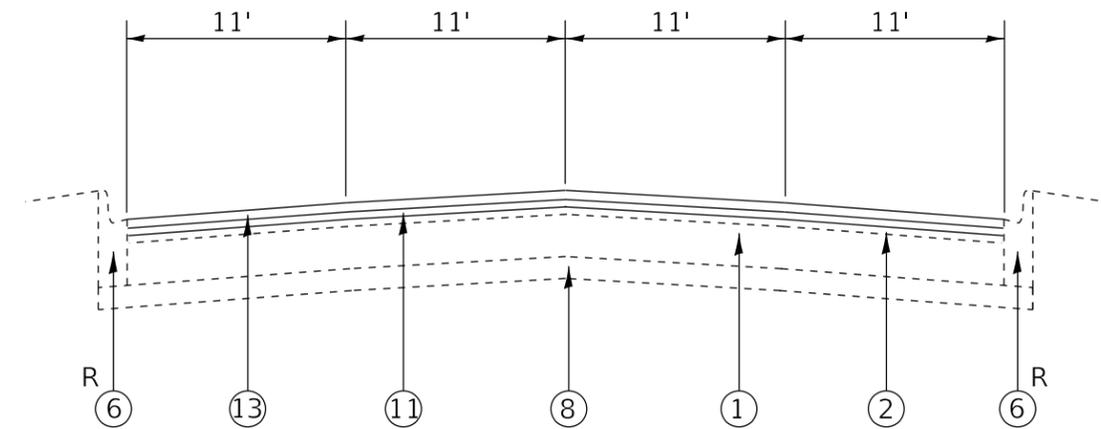
PROPOSED TYPICAL SECTION
STA. 16+59 TO 27+65
STA. 73+72 TO 74+27

LEGEND:

- ① EXISTING P.C.C. PAVEMENT ±8"
- ② EXISTING HOT-MIX ASPHALT SURFACE ±3" (AFTER MILLING)
- ③ EXISTING P.C.C. BARRIER MEDIAN
- ④ EXISTING HOT-MIX ASPHALT BARRIER MEDIAN
- ⑤ EXISTING AGGREGATE SHOULDER
- ⑥ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ⑦ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.12
- ⑧ EXISTING STABILIZED SUB-BASE
- ⑨ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 3¾"
- ⑩ PROPOSED AGGREGATE SHOULDER REMOVAL
- ⑪ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 2"
- ⑫ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 6¼"
- ⑬ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, MIX "F", 9.5, N80, 1¾"
- ⑭ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1¾"
- ⑮ PROPOSED GRADING AND SHAPING SHOULDERS
- ⑯ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)



EXISTING TYPICAL SECTION
STA. 169+49 TO 182+30
STA. 199+79 TO 203+38
STA. 217+37 TO 229+55



PROPOSED TYPICAL SECTION
STA. 169+49 TO 182+30
STA. 199+79 TO 203+38
STA. 217+37 TO 229+55

FILE NAME =	USER NAME = Velichkovv	DESIGNED -	REVISED -
p:\planroom.dot\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI24720\Drawings\Design\DI24720-sht-plan.dgn		CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

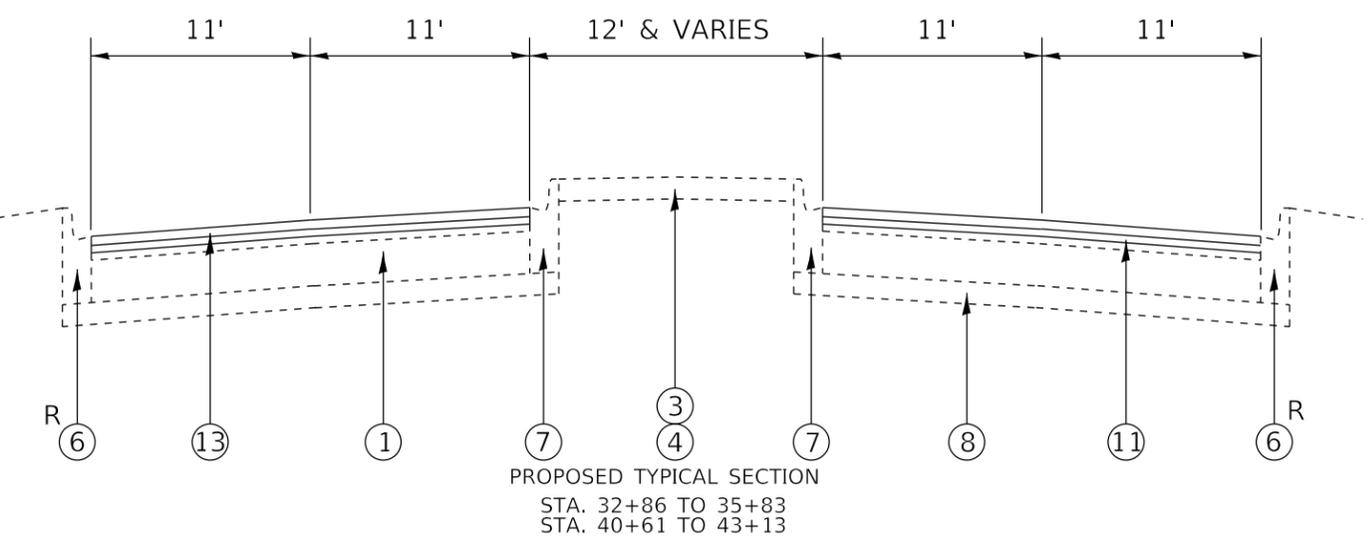
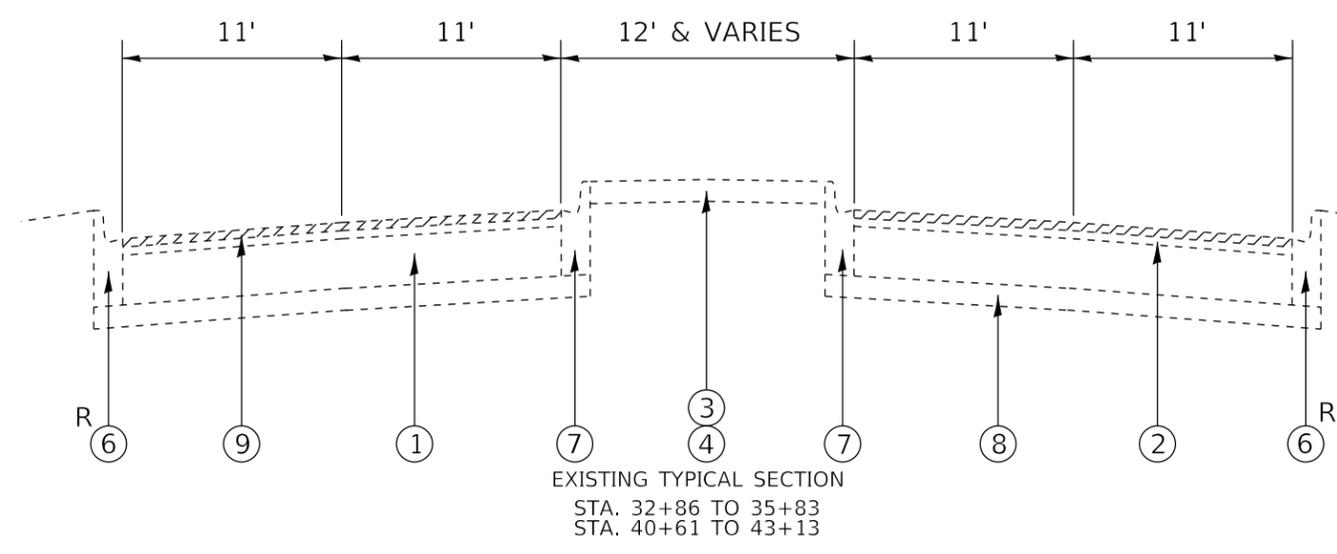
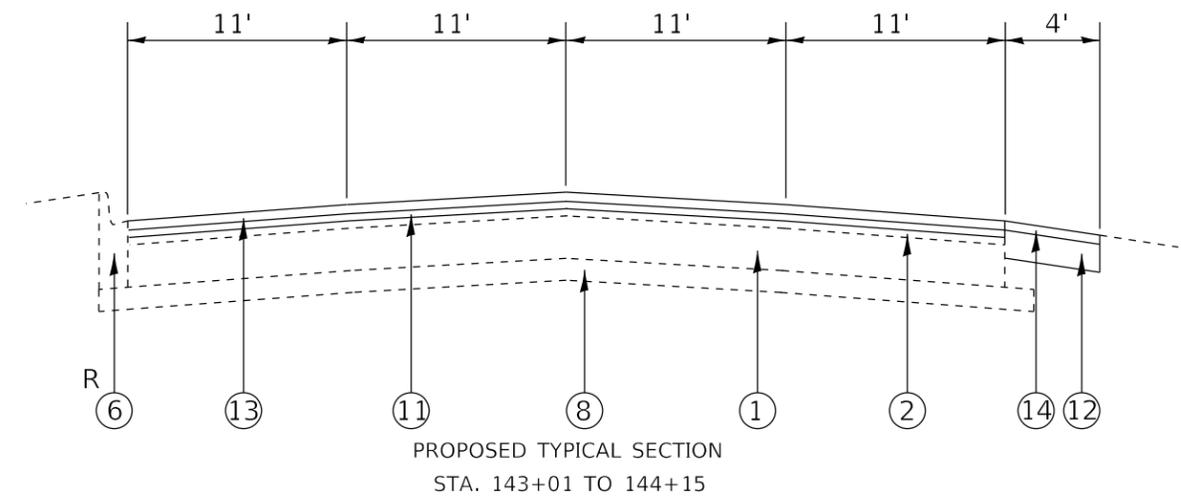
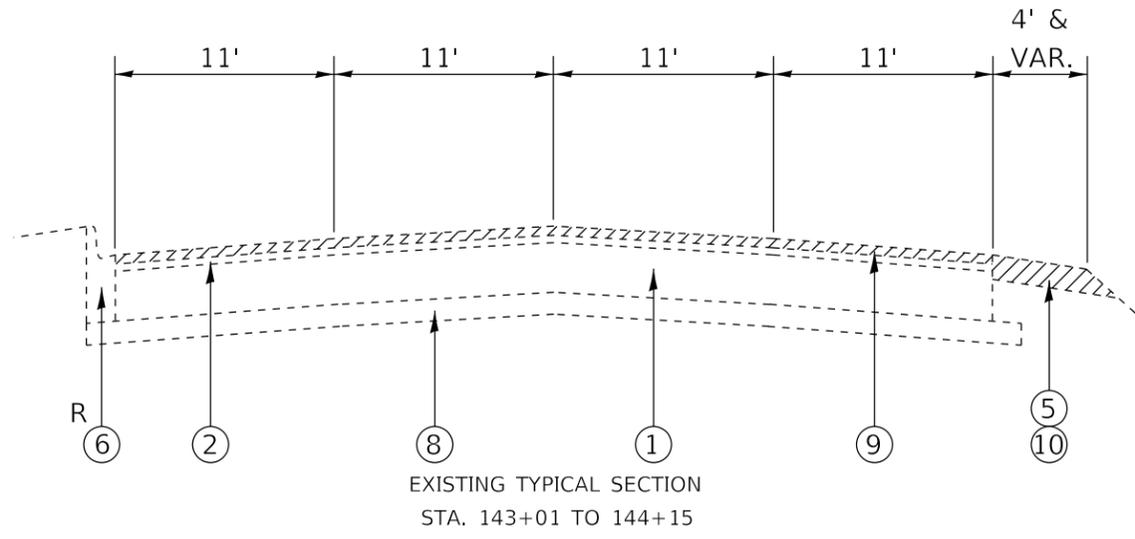
**EXISTING AND PROPOSED TYPICAL SECTIONS
FAU 2710 /DES PLAINES RIVER RD.—FOSTER AVE. TO IL 171 (1ST AVE.)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

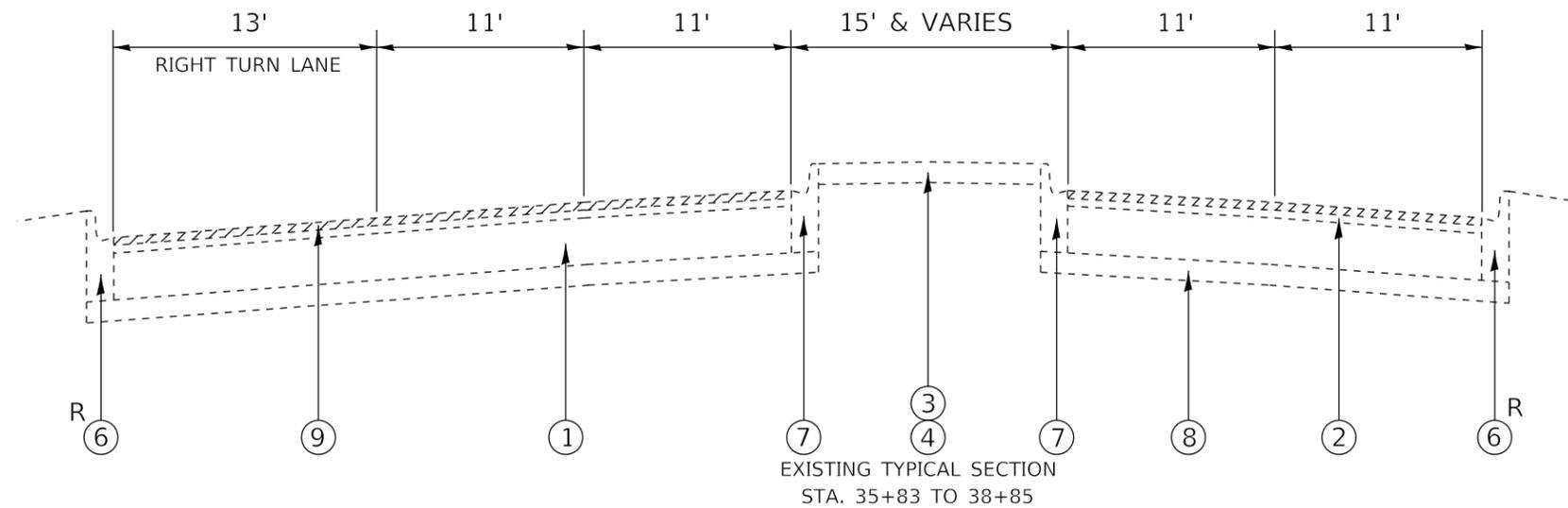
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	2019-098-RS-SW&SR	COOK	54	7
CONTRACT NO. 62J88				
ILLINOIS FED. AID PROJECT				

LEGEND:

- ① EXISTING P.C.C. PAVEMENT ±8"
- ② EXISTING HOT-MIX ASPHALT SURFACE ±3" (AFTER MILLING)
- ③ EXISTING P.C.C. BARRIER MEDIAN
- ④ EXISTING HOT-MIX ASPHALT BARRIER MEDIAN
- ⑤ EXISTING AGGREGATE SHOULDER
- ⑥ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ⑦ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.12
- ⑧ EXISTING STABILIZED SUB-BASE
- ⑨ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 3¾"
- ⑩ PROPOSED AGGREGATE SHOULDER REMOVAL
- ⑪ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 2"
- ⑫ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 6¼"
- ⑬ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, MIX "F", 9.5, N80, 1¾"
- ⑭ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1¾"
- ⑮ PROPOSED GRADING AND SHAPING SHOULDERS
- ⑯ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)



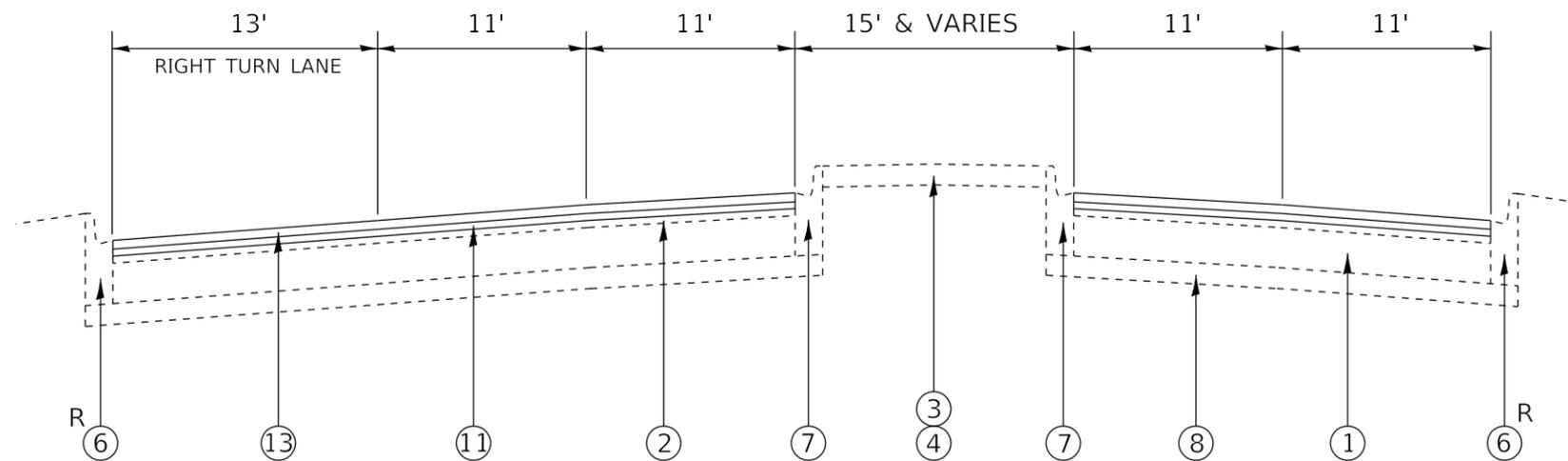
FILE NAME =	USER NAME = Velichkovv	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING AND PROPOSED TYPICAL SECTIONS			F.A.U. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\0124720\0124720-1\Design\0124720-sht-plan.dgn		CHECKED -	REVISED -		FAU 2710 /DES PLAINES RIVER RD.—FOSTER AVE. TO IL 171 (1ST AVE.)			2710	2019-098-RS-SW&SR	COOK	54	8
PLOT SCALE = 100.0000' / in.		DATE -	REVISED -		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.			CONTRACT NO. 62J88				
PLOT DATE = 12/13/2019					ILLINOIS FED. AID PROJECT							



EXISTING TYPICAL SECTION
STA. 35+83 TO 38+85

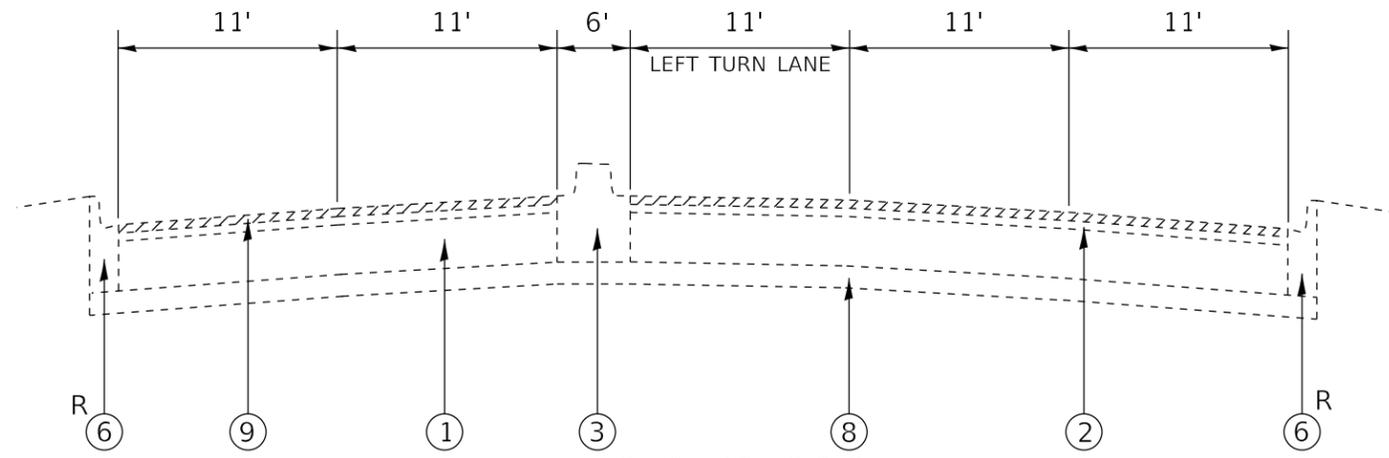
LEGEND:

- ① EXISTING P.C.C. PAVEMENT ±8"
- ② EXISTING HOT-MIX ASPHALT SURFACE ±3" (AFTER MILLING)
- ③ EXISTING P.C.C. BARRIER MEDIAN
- ④ EXISTING HOT-MIX ASPHALT BARRIER MEDIAN
- ⑤ EXISTING AGGREGATE SHOULDER
- ⑥ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ⑦ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.12
- ⑧ EXISTING STABILIZED SUB-BASE
- ⑨ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 3¾"
- ⑩ PROPOSED AGGREGATE SHOULDER REMOVAL
- ⑪ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 2"
- ⑫ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 6¼"
- ⑬ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, MIX "F", 9.5, N80, 1¾"
- ⑭ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1¾"
- ⑮ PROPOSED GRADING AND SHAPING SHOULDERS
- ⑯ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)

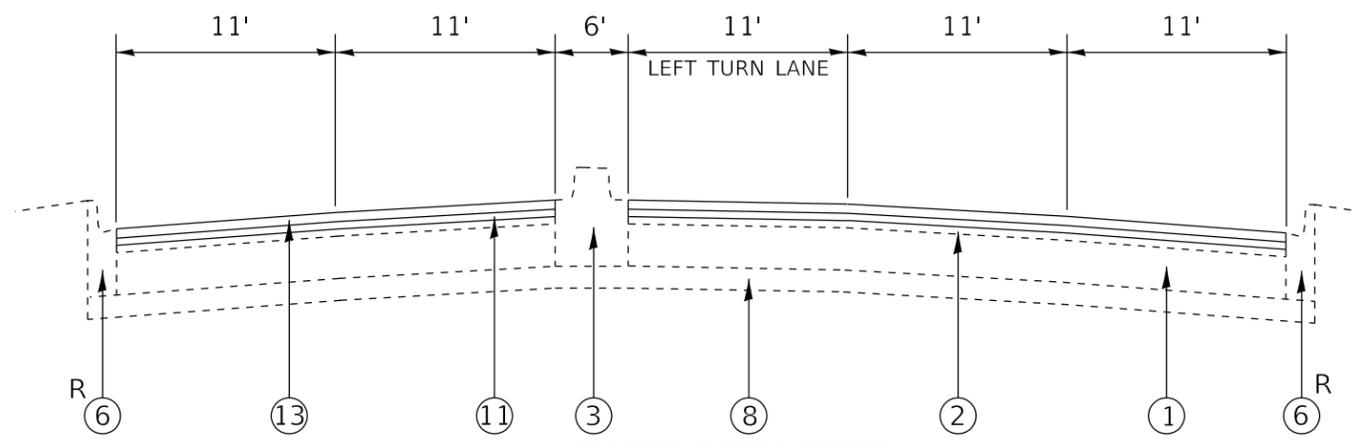


PROPOSED TYPICAL SECTION
STA. 35+83 TO 38+85

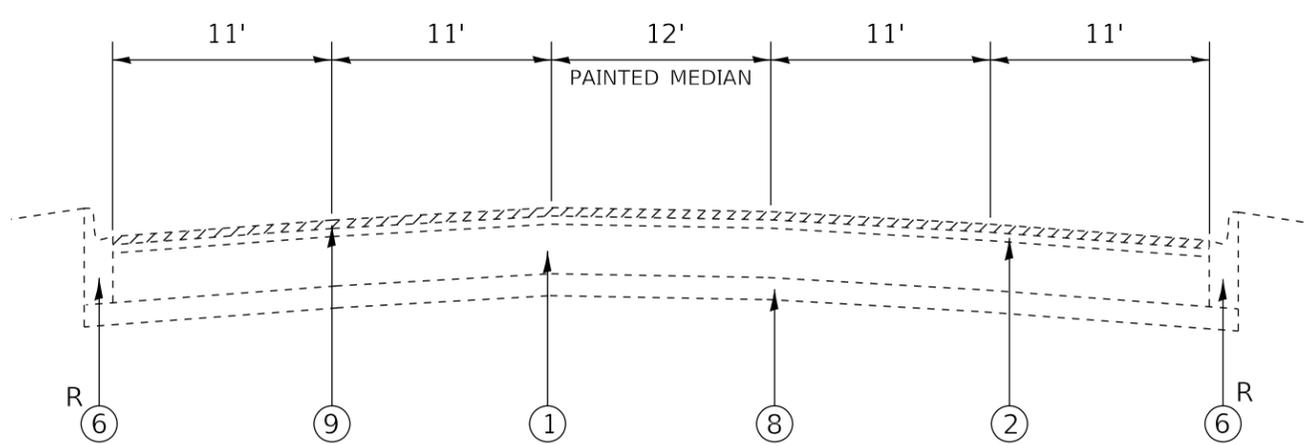
FILE NAME =	USER NAME = Velichkovv	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING AND PROPOSED TYPICAL SECTIONS			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\0124720\Drawings\Design\0124720-sht-plan.dgn		CHECKED -	REVISED -		FAU 2710 /DES PLAINES RIVER RD.—FOSTER AVE. TO IL 171 (1ST AVE.)			2710	2019-098-RS-SW&SR	COOK	54	9
PLOT SCALE = 100.0000' / in.		DATE -	REVISED -		SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.			CONTRACT NO. 62J88				
PLOT DATE = 12/13/2019								ILLINOIS FED. AID PROJECT				



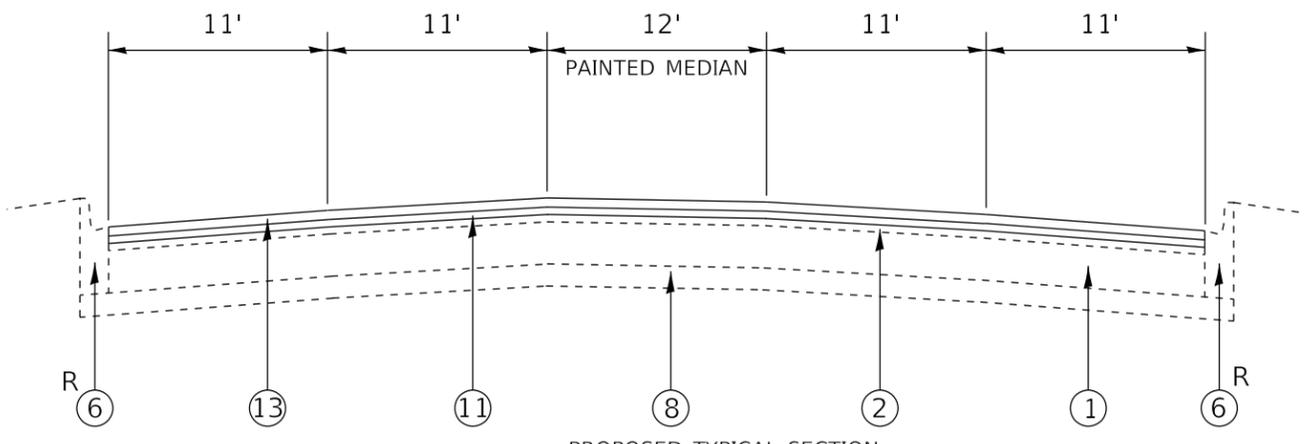
EXISTING TYPICAL SECTION
STA. 38+85 TO 40+61



PROPOSED TYPICAL SECTION
STA. 38+85 TO 40+61



EXISTING TYPICAL SECTION
STA. 62+73 TO 64+96
STA. 69+56 TO 72+72
STA. 128+27 TO 134+30
STA. 137+29 TO 143+01
STA. 152+27 TO 157+80
STA. 203+38 TO 207+76
STA. 229+55 TO 235+03



PROPOSED TYPICAL SECTION
STA. 62+73 TO 64+96
STA. 69+56 TO 72+72
STA. 128+27 TO 134+30
STA. 137+29 TO 143+01
STA. 152+27 TO 157+80
STA. 203+38 TO 207+76
STA. 229+55 TO 235+03

LEGEND:

- ① EXISTING P.C.C. PAVEMENT ±8"
- ② EXISTING HOT-MIX ASPHALT SURFACE ±3" (AFTER MILLING)
- ③ EXISTING P.C.C. BARRIER MEDIAN
- ④ EXISTING HOT-MIX ASPHALT BARRIER MEDIAN
- ⑤ EXISTING AGGREGATE SHOULDER
- ⑥ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ⑦ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.12
- ⑧ EXISTING STABILIZED SUB-BASE
- ⑨ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 3¾"
- ⑩ PROPOSED AGGREGATE SHOULDER REMOVAL
- ⑪ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 2"
- ⑫ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 6¼"
- ⑬ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, MIX "F", 9.5, N80, 1¾"
- ⑭ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1¾"
- ⑮ PROPOSED GRADING AND SHAPING SHOULDERS
- ⑯ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)

FILE NAME =	USER NAME = Velichkovv	DESIGNED -	REVISED -
pw\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\0124720\Drawings\Design\0124720-sht-plan.dgn		CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

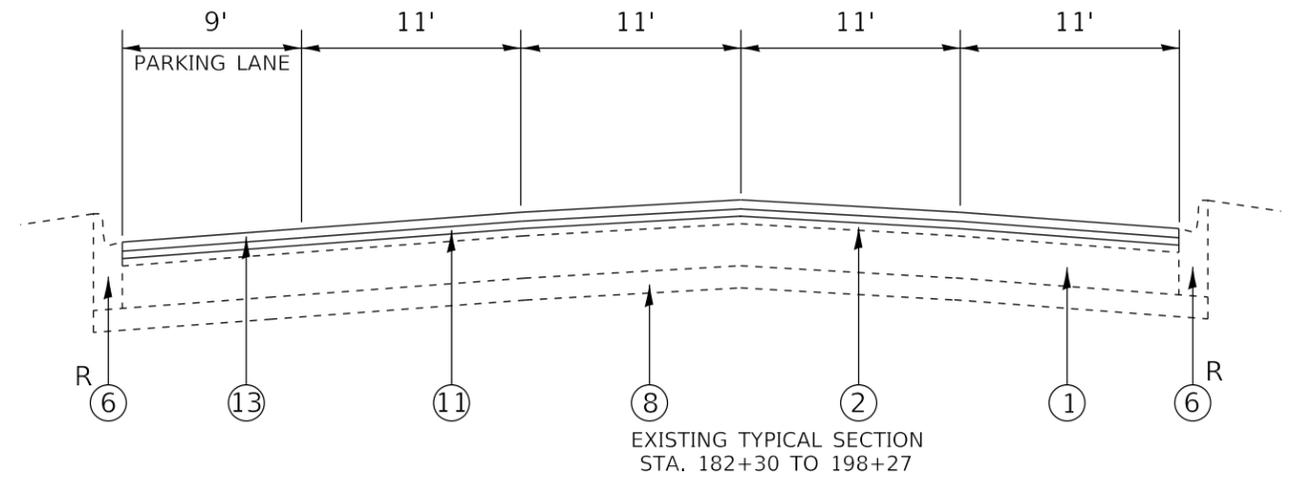
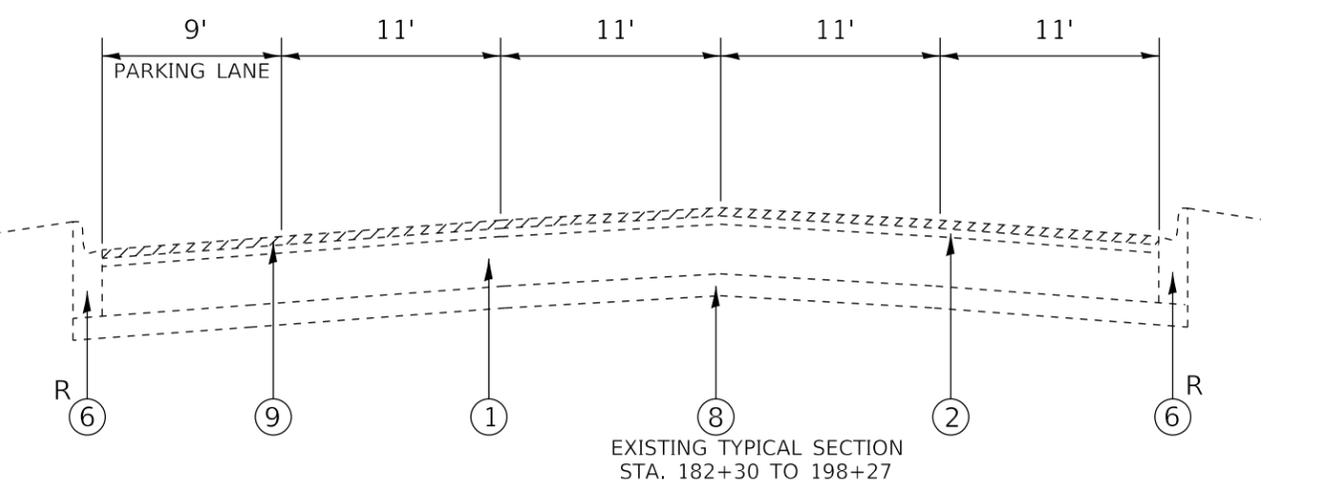
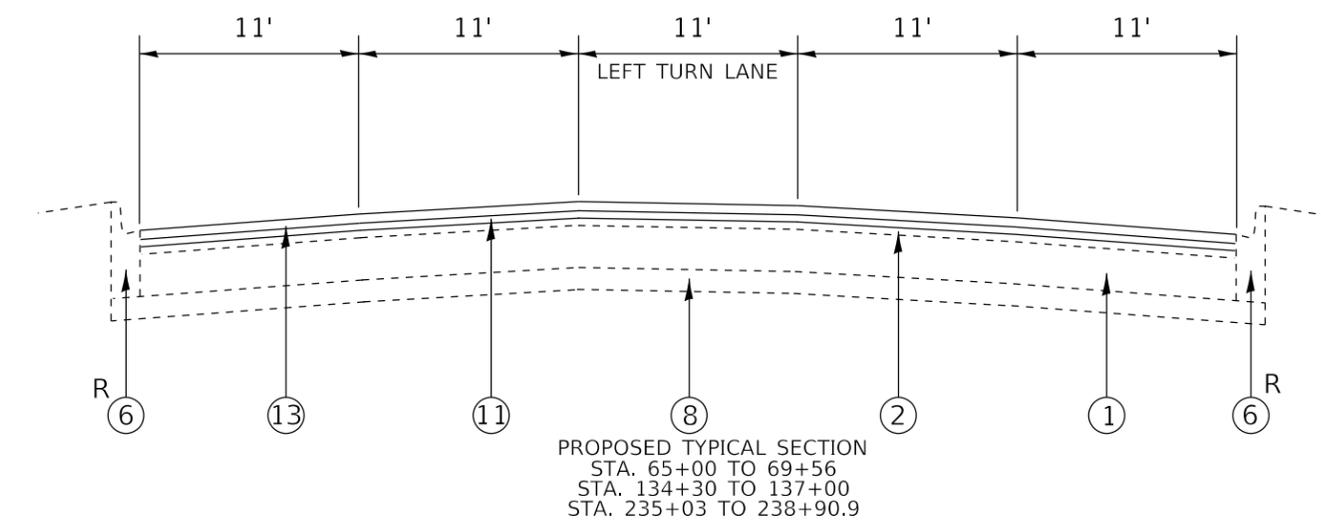
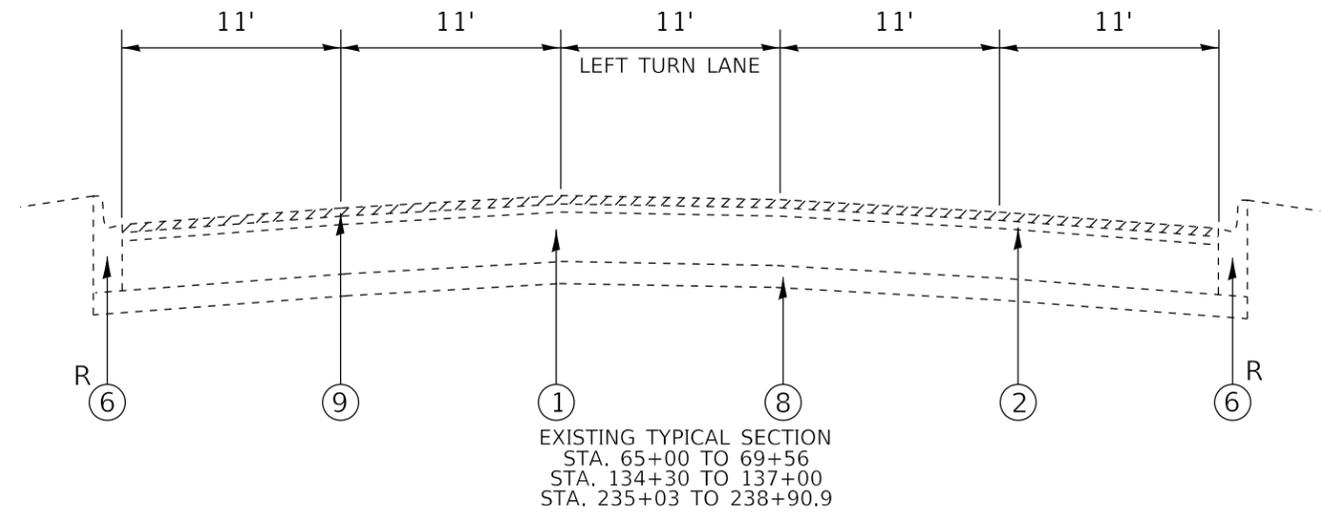
**EXISTING AND PROPOSED TYPICAL SECTIONS
FAU 2710 /DES PLAINES RIVER RD.—FOSTER AVE. TO IL 171 (1ST AVE.)**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	2019-098-RS-SW&SR	COOK	54	10
CONTRACT NO. 62J88				
ILLINOIS FED. AID PROJECT				

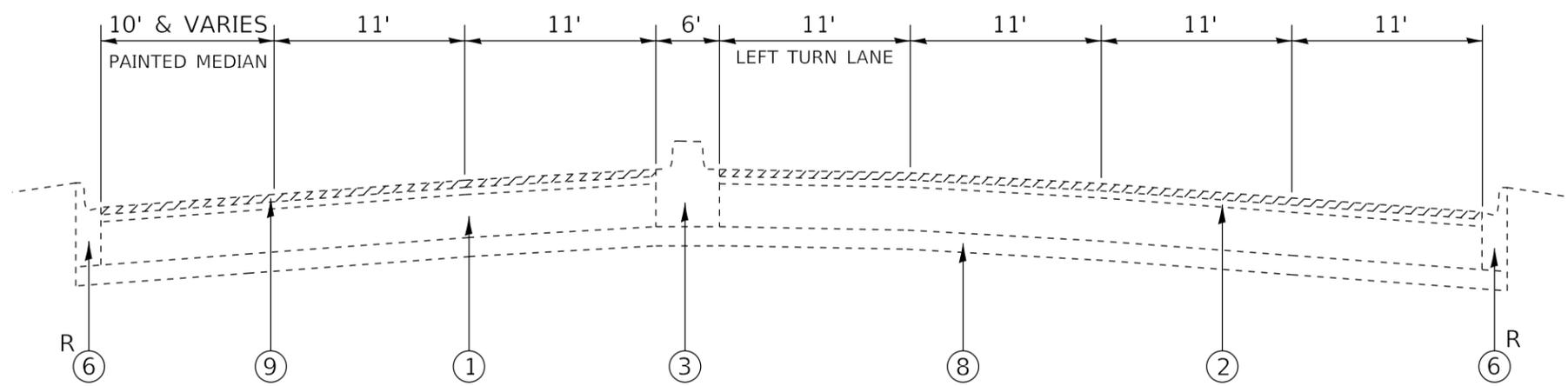
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

LEGEND:

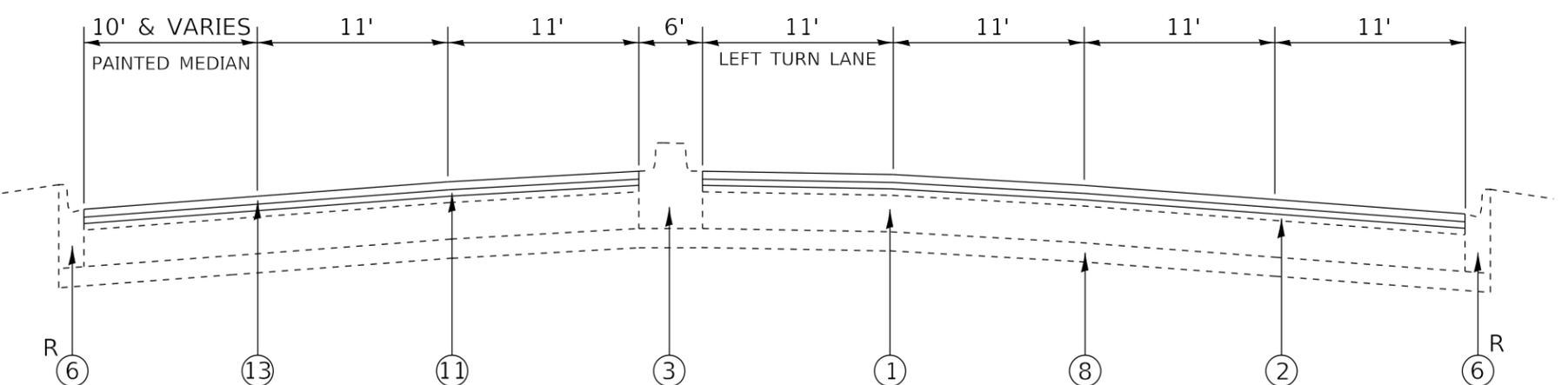
- ① EXISTING P.C.C. PAVEMENT ±8"
- ② EXISTING HOT-MIX ASPHALT SURFACE ±3" (AFTER MILLING)
- ③ EXISTING P.C.C. BARRIER MEDIAN
- ④ EXISTING HOT-MIX ASPHALT BARRIER MEDIAN
- ⑤ EXISTING AGGREGATE SHOULDER
- ⑥ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ⑦ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.12
- ⑧ EXISTING STABILIZED SUB-BASE
- ⑨ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 3¾"
- ⑩ PROPOSED AGGREGATE SHOULDER REMOVAL
- ⑪ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 2"
- ⑫ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 6¼"
- ⑬ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, MIX "F", 9.5, N80, 1¾"
- ⑭ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1¾"
- ⑮ PROPOSED GRADING AND SHAPING SHOULDERS
- ⑯ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)



FILE NAME =	USER NAME = Velichkovv	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING AND PROPOSED TYPICAL SECTIONS			F.A.U. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
pw:\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\DI24720\Drawings\Design\DI24720-sht-plan.dgn					REVISED -	FAU 2710 /DES PLAINES RIVER RD.—FOSTER AVE. TO IL 171 (1ST AVE.)			2710	2019-098-RS-SW&SR	COOK	54	11	
PLOT SCALE = 100.0000' / in.					CHECKED -	SCALE: NONE			SHEET NO. 1 OF 1 SHEETS		STA. TO STA.		CONTRACT NO. 62J88	
PLOT DATE = 12/13/2019					DATE -	ILLINOIS FED. AID PROJECT								



EXISTING TYPICAL SECTION
STA. 207+09 to 210+18

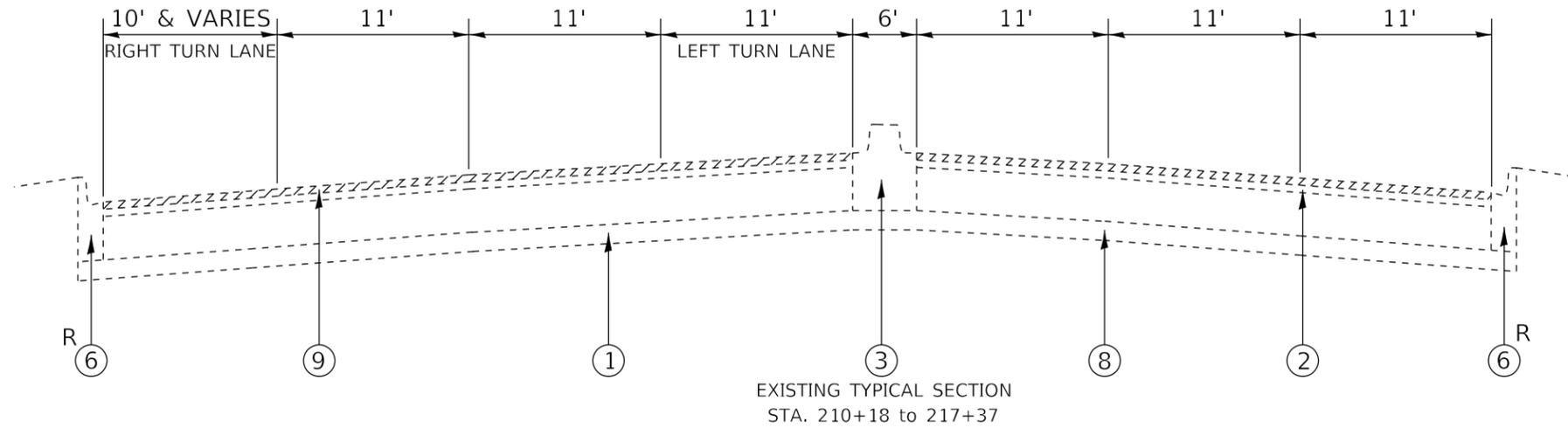


PROPOSED TYPICAL SECTION
STA. 207+09 to 210+18

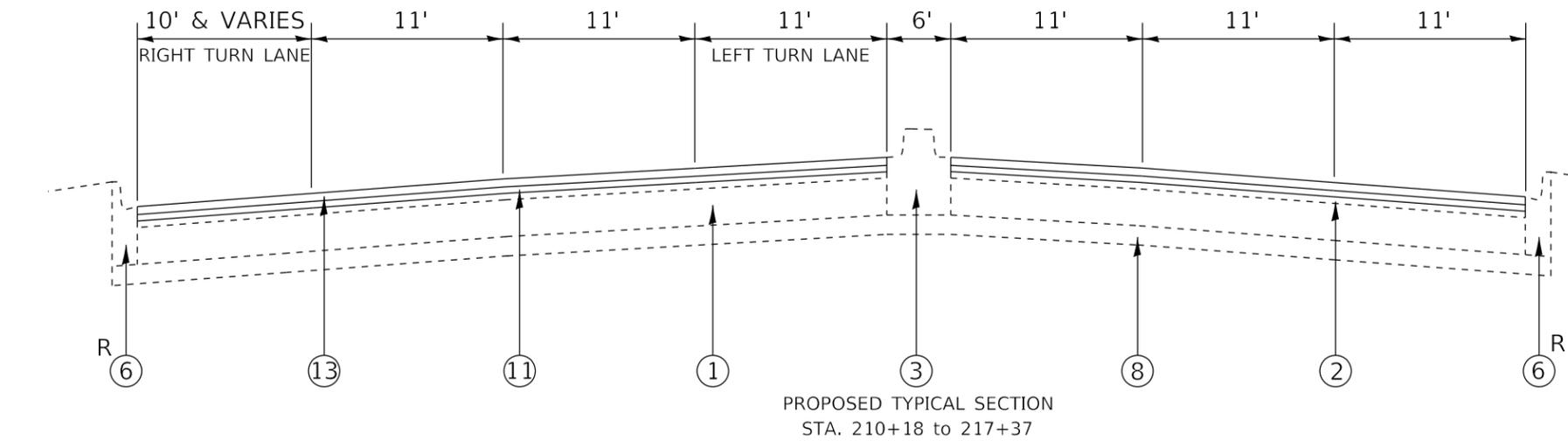
LEGEND:

- ① EXISTING P.C.C. PAVEMENT ±8"
- ② EXISTING HOT-MIX ASPHALT SURFACE ±3" (AFTER MILLING)
- ③ EXISTING P.C.C. BARRIER MEDIAN
- ④ EXISTING HOT-MIX ASPHALT BARRIER MEDIAN
- ⑤ EXISTING AGGREGATE SHOULDER
- ⑥ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ⑦ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.12
- ⑧ EXISTING STABILIZED SUB-BASE
- ⑨ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 3¾"
- ⑩ PROPOSED AGGREGATE SHOULDER REMOVAL
- ⑪ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 2"
- ⑫ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 6¼"
- ⑬ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, MIX "F", 9.5, N80, 1¾"
- ⑭ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1¾"
- ⑮ PROPOSED GRADING AND SHAPING SHOULDERS
- ⑯ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
- R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)

FILE NAME =	USER NAME = Velichkovv	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING AND PROPOSED TYPICAL SECTIONS			F.A.U. RTÉ.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\DI24720\Drawings\Design\DI24720-sht-plan.dgn		CHECKED -	REVISED -		FAU 2710 /DES PLAINES RIVER RD.—FOSTER AVE. TO IL 171 (1ST AVE.)			2710	2019-098-RS-SW&SR	COOK	54	12
PLOT SCALE = 100.0000' / in.		DATE -	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	CONTRACT NO. 62J88				
PLOT DATE = 12/13/2019					ILLINOIS FED. AID PROJECT							

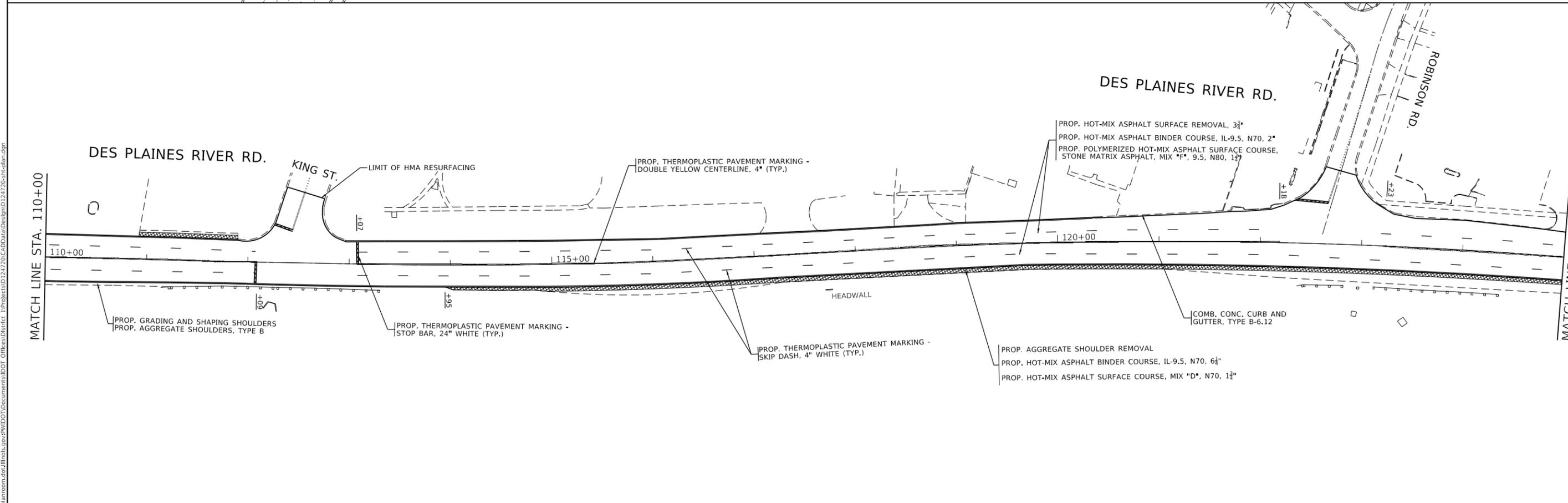
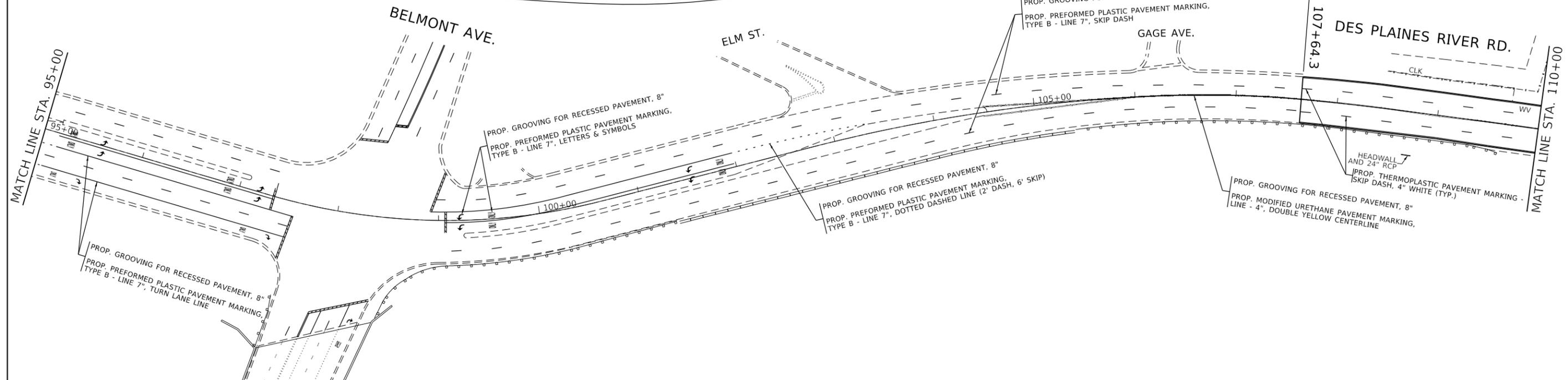


- LEGEND:**
- ① EXISTING P.C.C. PAVEMENT ±8"
 - ② EXISTING HOT-MIX ASPHALT SURFACE ±3" (AFTER MILLING)
 - ③ EXISTING P.C.C. BARRIER MEDIAN
 - ④ EXISTING HOT-MIX ASPHALT BARRIER MEDIAN
 - ⑤ EXISTING AGGREGATE SHOULDER
 - ⑥ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
 - ⑦ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.12
 - ⑧ EXISTING STABILIZED SUB-BASE
 - ⑨ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 3¾"
 - ⑩ PROPOSED AGGREGATE SHOULDER REMOVAL
 - ⑪ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 2"
 - ⑫ PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70, 6¼"
 - ⑬ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, MIX "F", 9.5, N80, 1¾"
 - ⑭ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1¾"
 - ⑮ PROPOSED GRADING AND SHAPING SHOULDERS
 - ⑯ PROPOSED AGGREGATE WEDGE SHOULDER, TYPE B
 - R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)



FILE NAME =	USER NAME = Velichkovv	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING AND PROPOSED TYPICAL SECTIONS			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\DI24720\Drawings\Design\DI24720-sht-plan.dgn		CHECKED -	REVISED -		FAU 2710 /DES PLAINES RIVER RD.—FOSTER AVE. TO IL 171 (1ST AVE.)			2710	2019-098-RS-SW&SR	COOK	54	13
PLOT SCALE = 100.0000' / in.		DATE -	REVISED -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	CONTRACT NO. 62J88				
PLOT DATE = 12/13/2019					ILLINOIS FED. AID PROJECT							

OMISSION FOR HMA RESURFACING



MODEL: Default
 FILE: Model: p:\pub\barroom.dwg
 PROJECT: 2019-098-R5-SW&SR
 OFFICE: 2710
 DESIGNED: 1/22/2020
 DRAWN: 1/22/2020
 CHECKED: 1/22/2020
 DATE: 1/22/2020

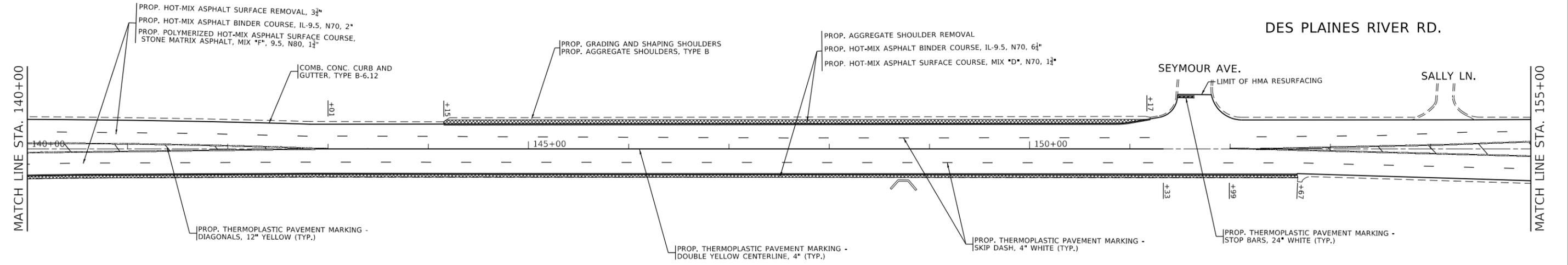
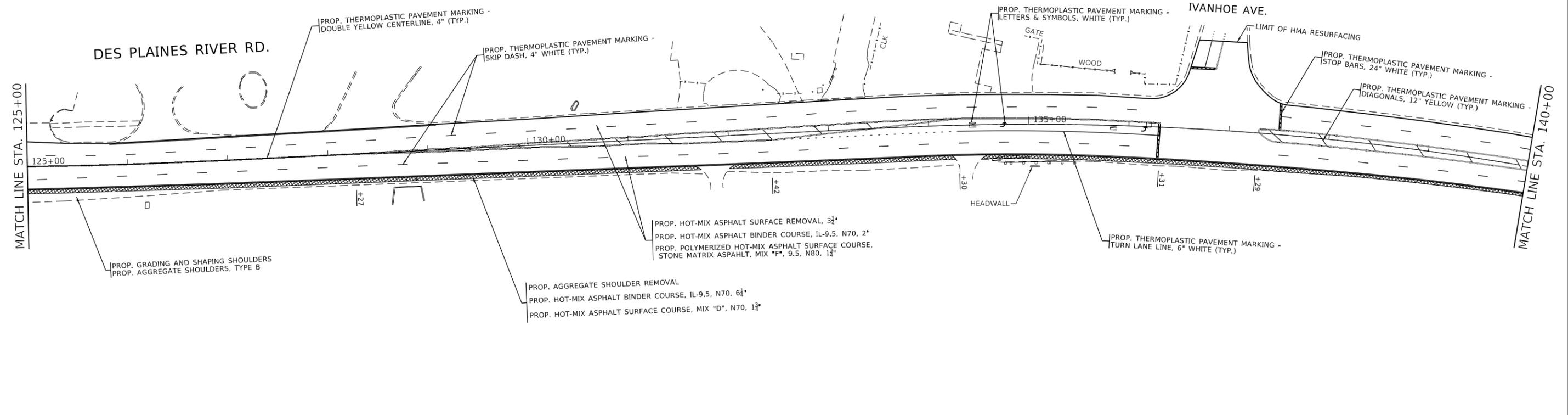
USER NAME = steedpa	DESIGNED -	REVISED -
PLOT SCALE = 100,0000' / in.	DRAWN -	REVISED -
PLOT DATE = 1/22/2020	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED ROADWAY PLAN
FAU 2710 (DES PLAINES RIVER RD.)—FOSTER AVE. TO IL. 171 (1ST AVE.)

SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	2019-098-R5-SW&SR	COOK	54	17
CONTRACT NO. 62J88				
ILLINOIS FED. AID PROJECT				



MODEL: Default
 FILE: Model - 20200122.ctb
 PROJECT: 2710-098-R5-SW&SR
 OFFICE: District 1
 DESIGNED: 1/22/2020
 DRAWN: 1/22/2020
 CHECKED: 1/22/2020
 DATE: 1/22/2020

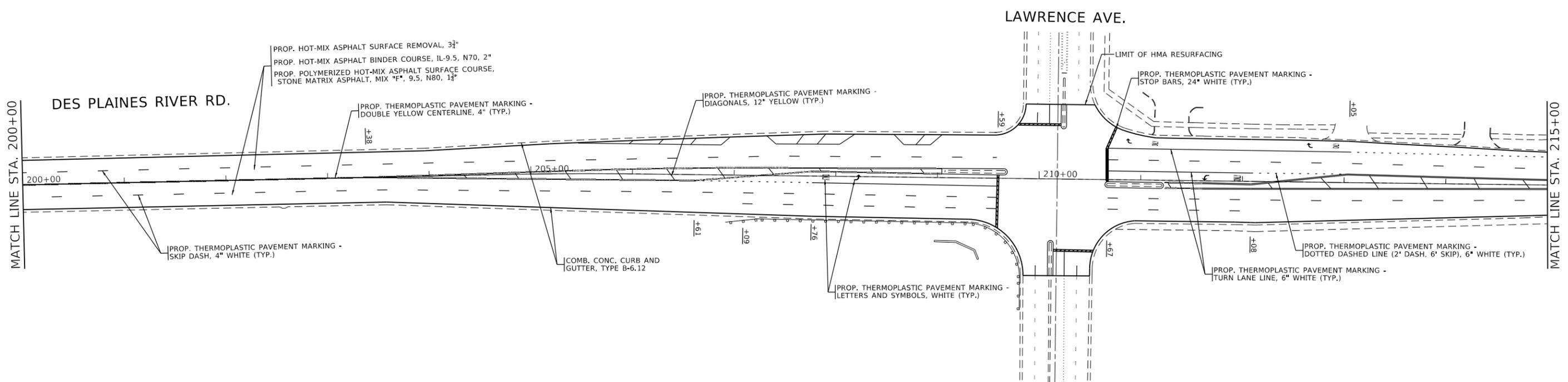
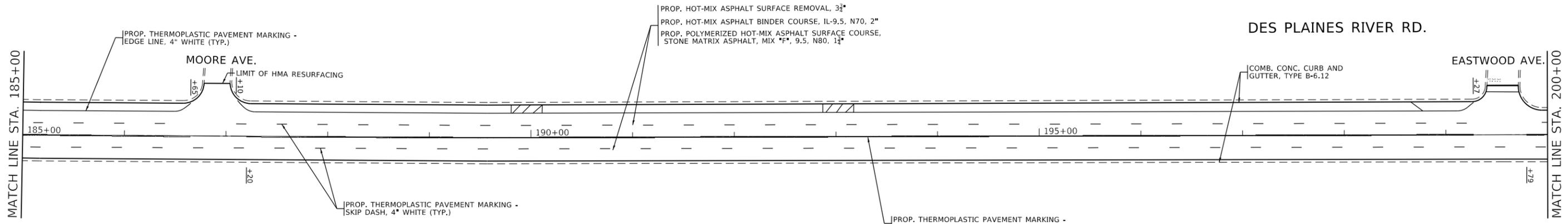
USER NAME = steedpa	DESIGNED -	REVISED -
PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 1/22/2020	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED ROADWAY PLAN
FAU 2710 (DES PLAINES RIVER RD.)—FOSTER AVE. TO IL. 171 (1ST AVE.)**

SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	2019-098-R5-SW&SR	COOK	54	18
CONTRACT NO. 62J88				
ILLINOIS FED. AID PROJECT				



MODEL: Default
 FILE: h:\m\c_p\pub\harcam.dwg
 PROJECT: 2019-098-R5-SW&SR
 OFFICE: IDOT
 DESIGNED: steedpa
 DRAWN: steedpa
 CHECKED: steedpa
 DATE: 1/22/2020

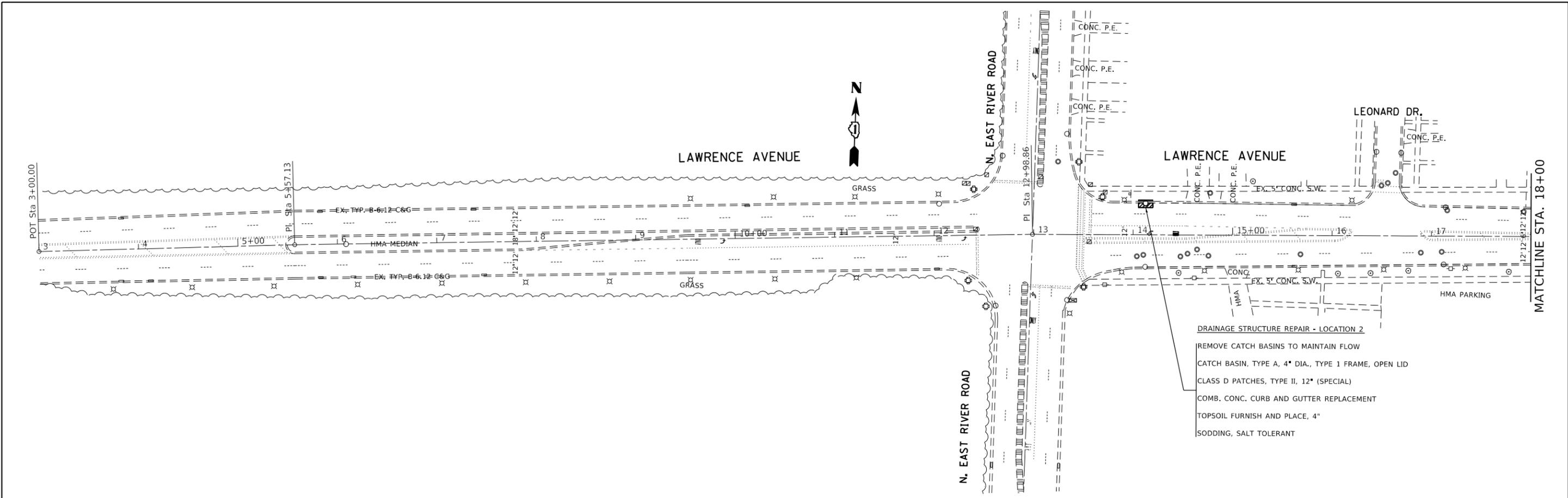
USER NAME = steedpa	DESIGNED -	REVISED -
PLOT SCALE = 100,0000' / in.	DRAWN -	REVISED -
PLOT DATE = 1/22/2020	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING AND PROPOSED ROADWAY PLAN
FAU 2710 (DES PLAINES RIVER RD.)—FOSTER AVE. TO IL. 171 (1ST AVE.)

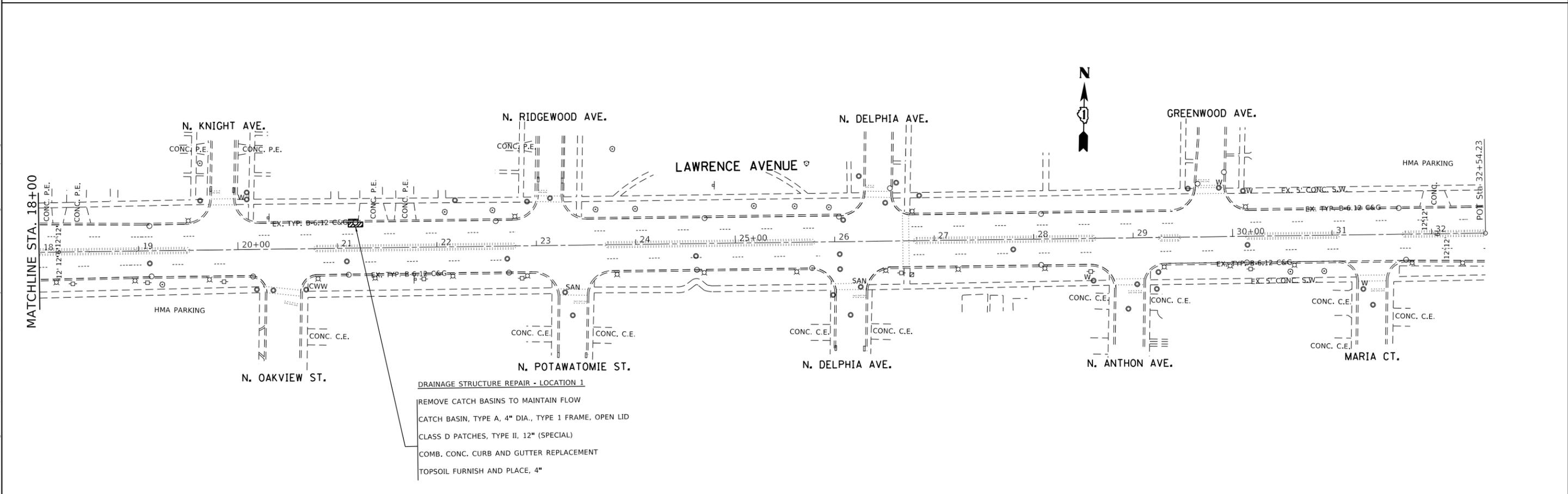
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	2019-098-R5-SW&SR	COOK	54	20
CONTRACT NO. 62J88				
ILLINOIS FED. AID PROJECT				

SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.



DRAINAGE STRUCTURE REPAIR - LOCATION 2

- REMOVE CATCH BASINS TO MAINTAIN FLOW
- CATCH BASIN, TYPE A, 4" DIA., TYPE 1 FRAME, OPEN LID
- CLASS D PATCHES, TYPE II, 12" (SPECIAL)
- COMB. CONC. CURB AND GUTTER REPLACEMENT
- TOPSOIL FURNISH AND PLACE, 4"
- SODDING, SALT TOLERANT



DRAINAGE STRUCTURE REPAIR - LOCATION 1

- REMOVE CATCH BASINS TO MAINTAIN FLOW
- CATCH BASIN, TYPE A, 4" DIA., TYPE 1 FRAME, OPEN LID
- CLASS D PATCHES, TYPE II, 12" (SPECIAL)
- COMB. CONC. CURB AND GUTTER REPLACEMENT
- TOPSOIL FURNISH AND PLACE, 4"

MODEL: Default
 FILE: \\mhc-pw\pub\arcom\dat\illinois\gov\PW\DOT\Documents\DOT_Offices\Dir\et 1\Projects\124720\CADD\Drawings\124720-sh-c-lar.dgn

USER NAME = steedpa	DESIGNED -	REVISED -
PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 1/22/2020	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRAINAGE STRUCTURE REPAIR PLAN LAWRENCE AVE. (EAST RIVER RD. TO KNIGHT AVE.)			
SCALE: 1" = 50'	SHEET	OF SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	2019-098-R5-SW&SR	COOK	54	22
CONTRACT NO. 62J88				
ILLINOIS FED. AID PROJECT				

RIVER ROAD SIDEWALK DETAIL SCHEDULE

SHEET NO.	LOCATION	EARTH EXCAVATION	TOPSOIL FURNISH AND PLACE, 4"	SODDING, SALT TOLERANT	HMA SC "D" N50	PROTECTIVE COAT	PC CONC SIDEWALK 5	DETECTABLE WARNINGS	DRIVEWAY PVMT REM.	SIDEWALK REM	FR & GRATE ADJUST	FR & LIDS ADJUST	MAIN EX TR SIG INSTAL	RELOC EX PED PUSH-BUT	REBUILD EX HANDHOLE	COMB CC&G REM & REPL
		CU YD	SQ YD	SQ YD	TON	SQ YD	SQ FT	SQ FT	SQ YD	SQ FT	EACH	EACH	EACH	EACH	EACH	FOOT
		20200100	21101615	25200110	40603335	42001300	42400200	42400800	44000200	44000600	60300105	60300305	85000200	89500400	89502376	Z0004562
ADA-01	Ridge St. SW	1.5	13.3	13.3		17	149	10		168						16.0
ADA-01	Ridge St. NW	2.5	19.4	19.4		27	245	10		245						35.0
ADA-02	Chestnut Ave. SW	1.4			8	16	144	10	36	144	1					10.0
ADA-02	Chestnut Ave. NW	2.3	14.0	14.0		25	228	10		267						8.0
ADA-03	Franklin Ave. SW	0.9				10	89	10		89		1				15.0
ADA-03	Robinson Rd. NW	2.4	14.0	14.0		27	239	17		290			1		1	41.0
ADA-04	Cullom St. SW	0.7	6.3	6.3		8	74	10		74						15.0
ADA-04	Montrose Ave. SW	1.3	8.6	8.6		14	125	10		125						14.0
ADA-05	Eastwood Ave. SW	1.2	10.5	10.5		13	118	10		147						14.7
ADA-05	Eastwood Ave. NW	1.2	6.0	6.0		14	124	10		129						12.0
ADA-06	Foster Ave. SW	2.2	27.5	27.5		24	217	10		283						22.5
ADA-06	Foster Ave. NW	2.7	19.0	19.0		29	265	10		307			1	1	1	22.5
ADA-07	Fullerton Ave. NE	1.6				18	163	22		163			1		1	37.0
ADA-08	Moore LN. NW															
ADA-08	Lawrence Ave. NW															
TOTAL		21.8	138.6	138.6	8	242	2180	149	36	2431	1	1	3	1	3	262.7

FILE NAME = P:\2014\ME14027_Ver-Var-PhI\W022_62J88_Des Plaines River Rd_ADA\01-CADD\02-Shts\DI62J88_ADA_SHT00.dgn
 PLOT SCALE = 10.0000' / 1" = 10000.0000
 USER NAME = Millennium Professional Services



USER NAME = jpham	DESIGNED - JP	REVISED -
	DRAWN - JP	REVISED -
PLOT SCALE = 10.0000' / 1"	CHECKED - TN	REVISED -
PLOT DATE = 12/12/2019	DATE - 12/12/2019	REVISED -

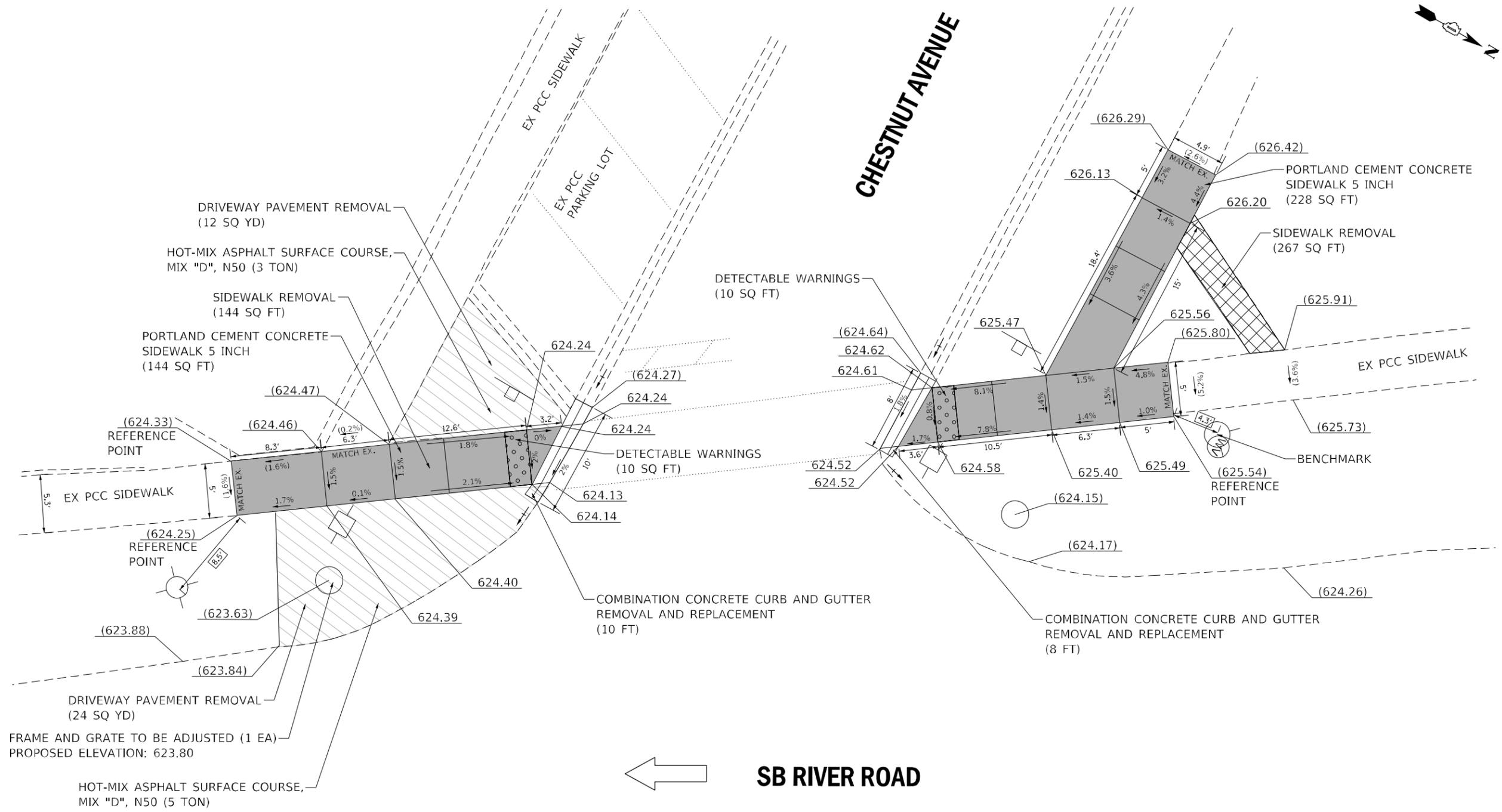
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SIDEWALK DETAIL PLAN
SCHEDULE OF QUANTITIES**

SCALE: N/A SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	2019-098-RS-SW&SR	COOK	54	23
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62J88	

P:\2014\ME14027_Ver-Var-PhI\W022_62J88_Des Plaines River Rd_ADA\01-CADD\02-Shts\DI62J88_ADA_SHT00.dgn



SB RIVER ROAD

NOTE:
 PLACE TOPSOIL 4" AND SODDING SALT TOLERANT 18"
 MAX BEYOND BACK OF CURB AND GUTTER AND SIDEWALK
 REPLACEMENT ACCORDING TO DISTRICT STANDARD BD-24

ADA-02

REFERENCE BENCHMARK ELEV: 626.389
 BENCHMARK : X-CUT ON SOUTHWEST BOLT ON FIRE HYDRANT
 LOCATION : NORTHEAST CORNER OF RIVER RD. & CHESTNUT AVE

LEGEND	
xx.xx'	EXISTING LENGTH
=====	PROPOSED SIDE CURB
()	EXISTING ELEVATION/SLOPE
	PROPOSED SIDEWALK
	DETECTABLE WARNINGS
	SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV: 626.389
 BENCHMARK : X-CUT ON SOUTHWEST BOLT ON FIRE HYDRANT
 LOCATION : NORTHEAST CORNER OF RIVER RD. & CHESTNUT AVE

FILE NAME = P:\2014\ME14027_Ver-Var-Phi\11\W022-62J88 Des Plaines River Rd_ADA-01-CADD\02-Shts\DI62J88_ADA_SHT02.dgn
 PLOT SCALE = 10.0000' / in.
 USER NAME = Millennium Professional Services

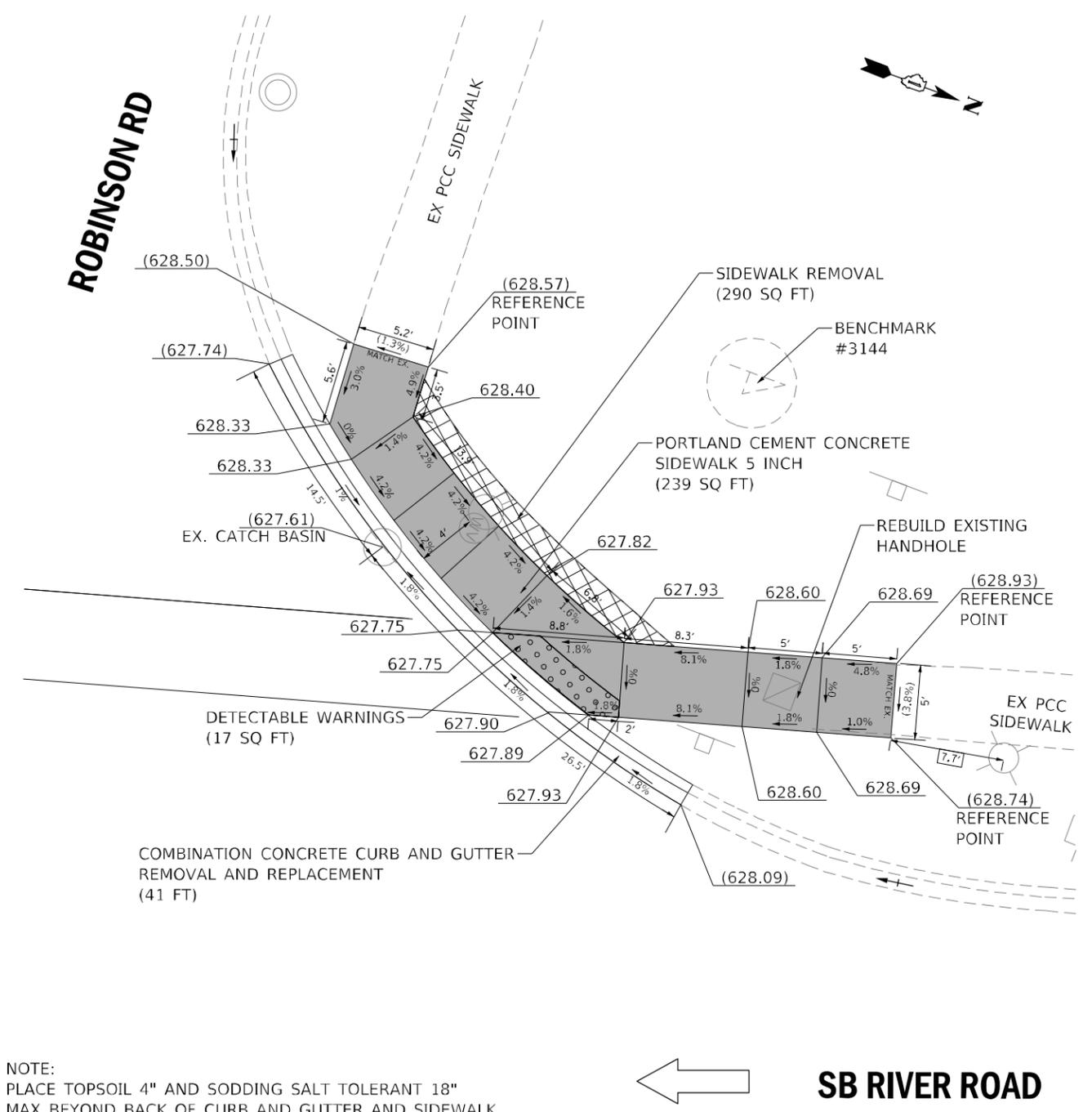
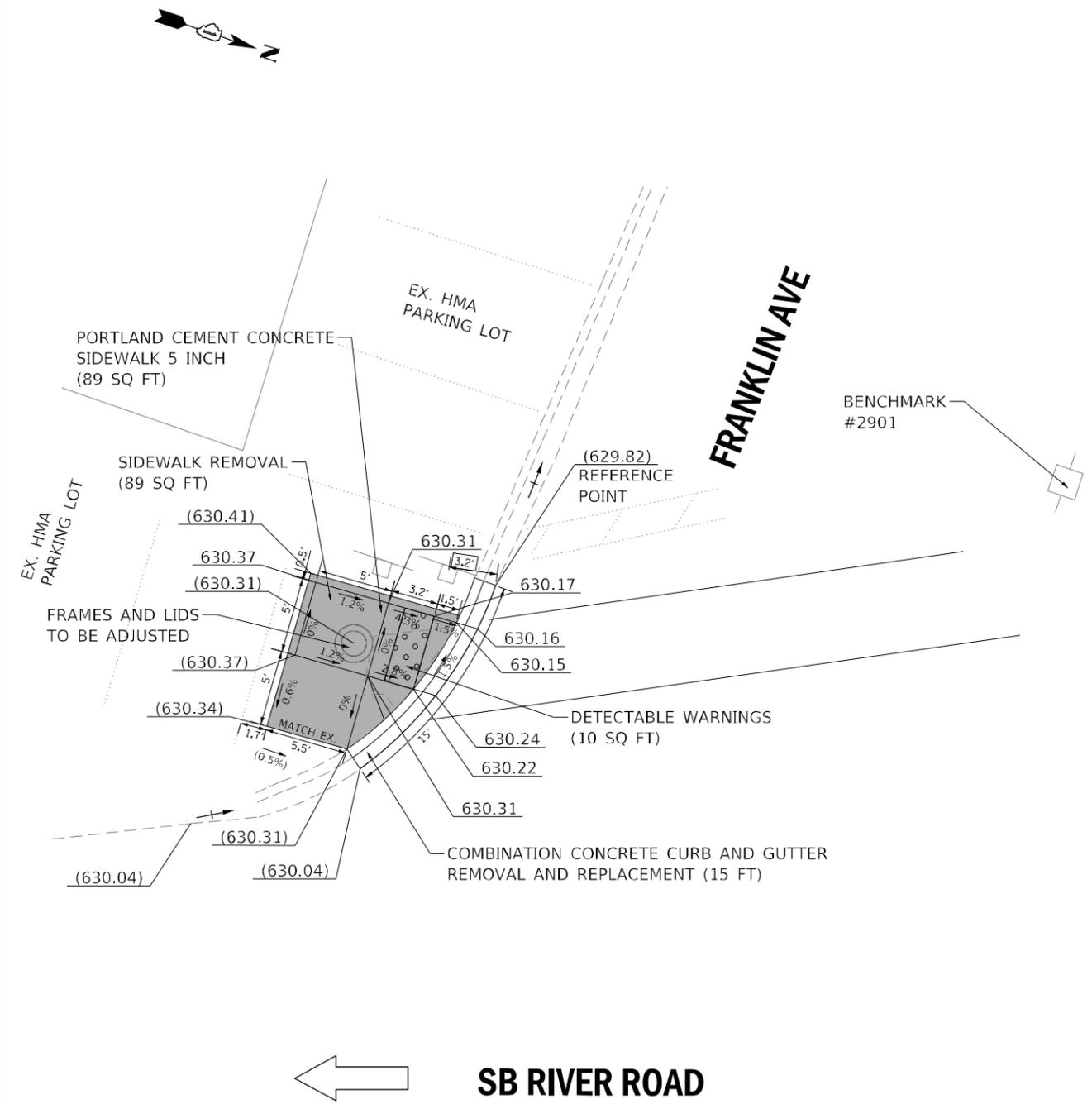


USER NAME = jpham	DESIGNED - MDK	REVISED -
	DRAWN - MDK	REVISED -
PLOT SCALE = 10.0000' / in.	CHECKED - TN	REVISED -
PLOT DATE = 12/12/2019	DATE - 12/12/2019	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**SIDEWALK DETAIL PLAN
 RIVER ROAD AT CHESTNUT AVE.**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	2019-098-RS-SW&SR	COOK	54	25
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62J88	



NOTE:
 PLACE TOPSOIL 4" AND SODDING SALT TOLERANT 18"
 MAX BEYOND BACK OF CURB AND GUTTER AND SIDEWALK
 REPLACEMENT ACCORDING TO DISTRICT STANDARD BD-24

ADA-03

REFERENCE BENCHMARK ELEV: 632.11
 BENCHMARK : BM #2901 (R/R SPIKE IN SOUTH FACE OF PP)
 LOCATION : NW CORNER OF RIVER RD AND FRANKLIN AVE

REFERENCE BENCHMARK ELEV: 629.63
 BENCHMARK : BM #3144 (X CUT ON SW BOLT OF FLAG POLE)
 LOCATION : NW CORNER OF ROBINSON RD AND RIVER RD

LEGEND	
xx.xx'	EXISTING LENGTH
=====	PROPOSED SIDE CURB
()	EXISTING ELEVATION/SLOPE
■	PROPOSED SIDEWALK
●●●●	DETECTABLE WARNINGS
▣	SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SIDEWALK DETAIL PLAN
 RIVER RD AT FRANKLIN AVE AND AT ROBINSON RD

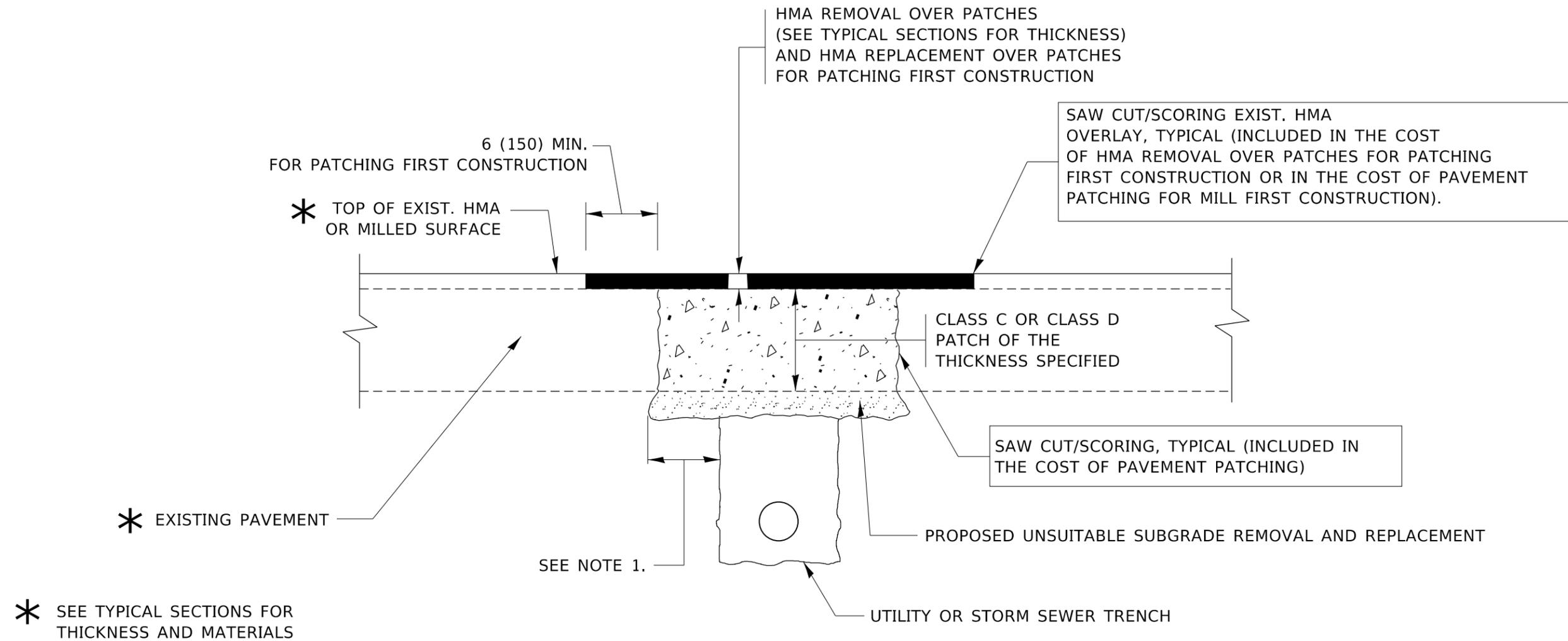
USER NAME = jpuham	DESIGNED - JP	REVISED -
	DRAWN - JP	REVISED -
PLOT SCALE = 10,0004' / in.	CHECKED - TN	REVISED -
PLOT DATE = 12/12/2019	DATE - 12/12/2019	REVISED -

SCALE: 1"=5'	SHEET NO. OF SHEETS STA.	TO STA.
--------------	--------------------------	---------

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	2019-098-RS-SW&SR	COOK	54	26
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62J88	

FILE NAME = P:\2014\ME14027_Ver-Var-Phi\W022-62J88 Des Plaines River Rd_ADA\01-CADD\02-Shts\02J88_ADA_SHT03.dgn
 PLOT SCALE = 10,0004' / in.
 USER NAME = Millennium Professional Services





NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: Default
FILE: \\hpc\p\pub\hpcroom\dat\illinois\p\w\DOT\Documents\DOT_Offices\Bartlett_1\Projects\124720\CADD\Design\Design\DESIGN.dgn

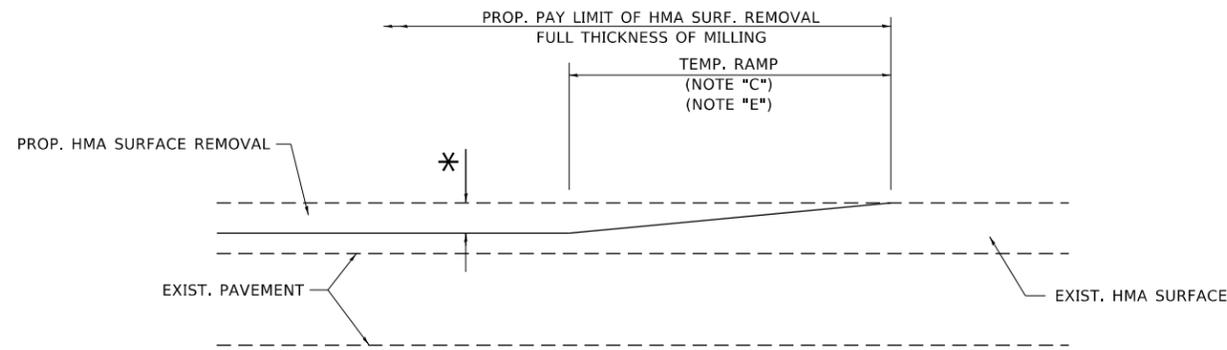
USER NAME = Velichkovv	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98
	DRAWN -	REVISED - R. BORO 01-01-07
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - R. BORO 09-04-07
PLOT DATE = 12/13/2019	DATE - 10-25-94	REVISED - K. ENG 10-27-08

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PAVEMENT PATCHING FOR
HMA SURFACED PAVEMENT

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

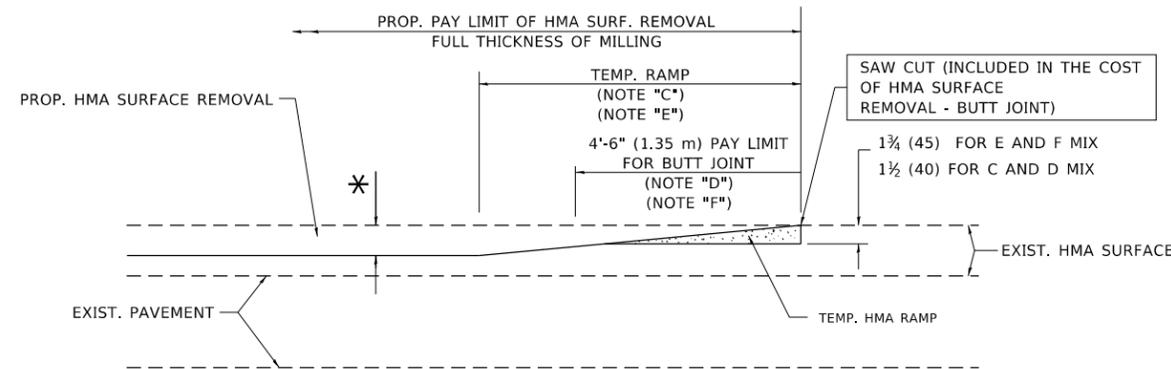
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	2019-098-R5-SW&SR	COOK	54	33
BD400-04 (BD-22)			CONTRACT NO. 62J88	
ILLINOIS FED. AID PROJECT				



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

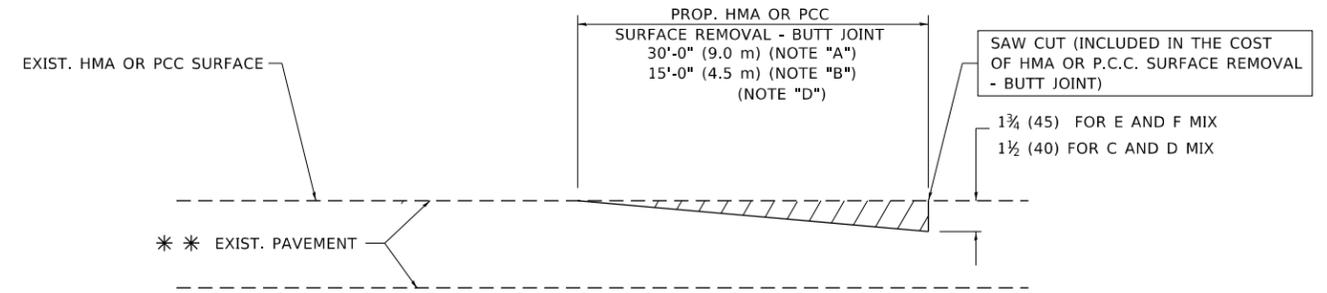


HMA CONSTRUCTED TEMPORARY RAMP

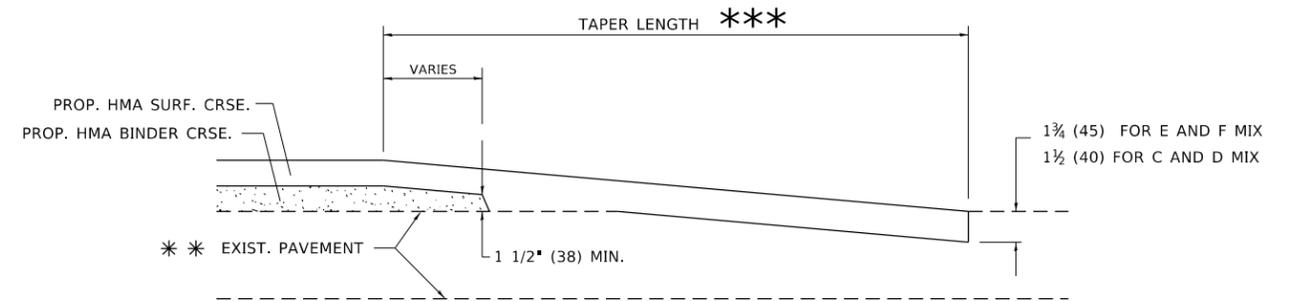
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

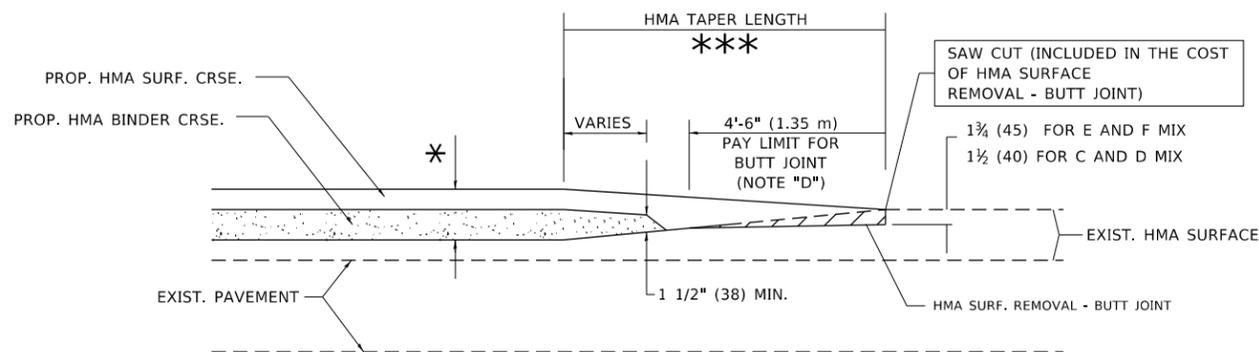
NOTES

- A. MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F. INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT.
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- G. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

MODEL: Default
FILE: \\hpc\p\pub\hpc\room\dat\illinois\poc\p\WIDOT\Documents\DOT_Offices\Bldgctc_1\Projects\124720\CaddData\Design\DES54.dgn

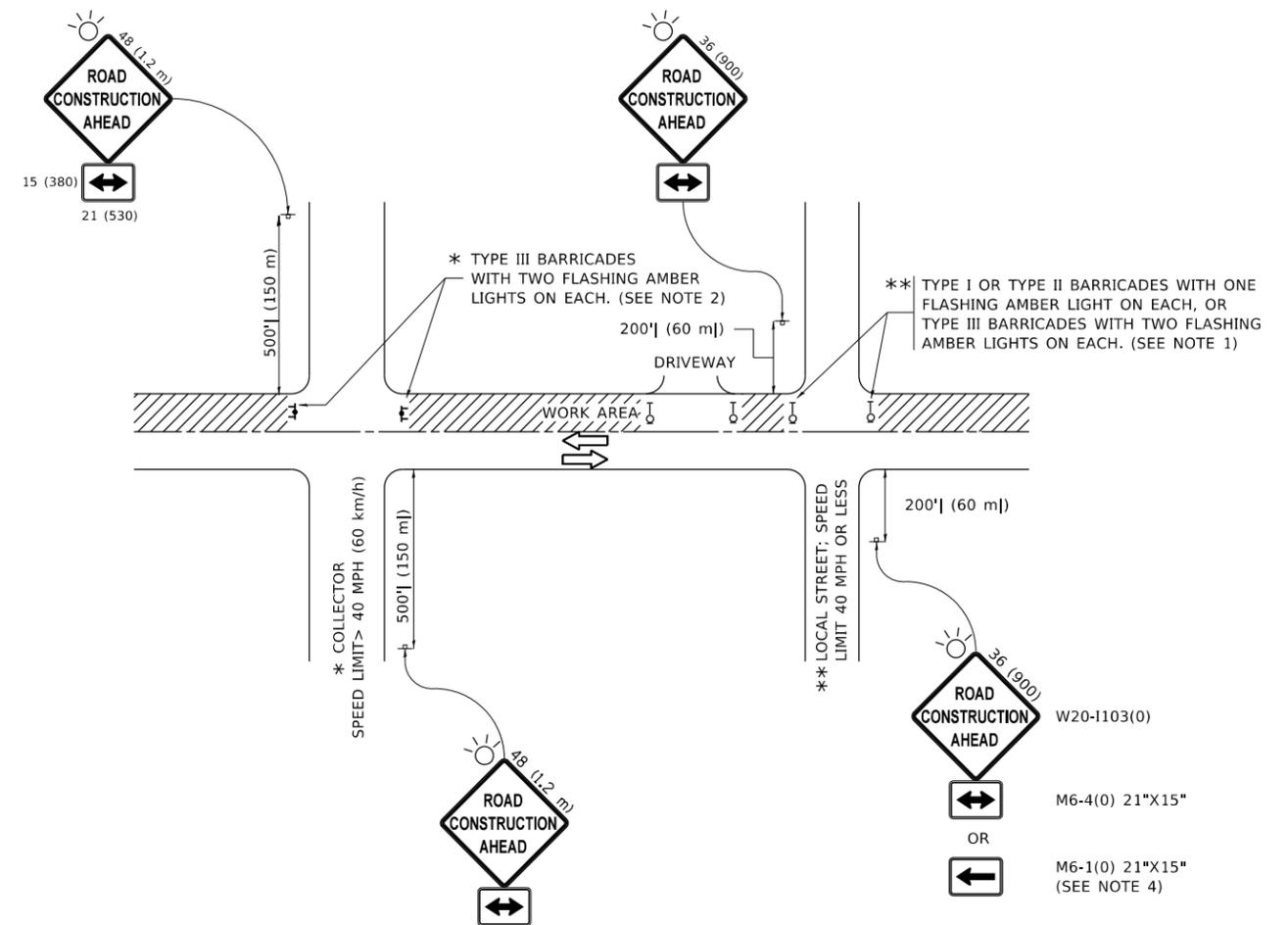
USER NAME = Velichkovv	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
	DRAWN -	REVISED - A. ABBAS 03-21-97
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - M. GOMEZ 04-06-01
PLOT DATE = 12/13/2019	DATE - 06-13-90	REVISED - R.BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	2019-098-R5-SW&SR	COOK	54	35
BD400-05 BD32		CONTRACT NO. 62J88		
ILLINOIS FED. AID PROJECT				



NOTES:

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: Default
FILE: h:\mhc\p\pub\haronm.d\at\illinois.gov\PI\DOT\Documents\DOT_Offices\Dir\det_1\Projects\124720\CADD\Drawings\Design\DESIGN.dgn

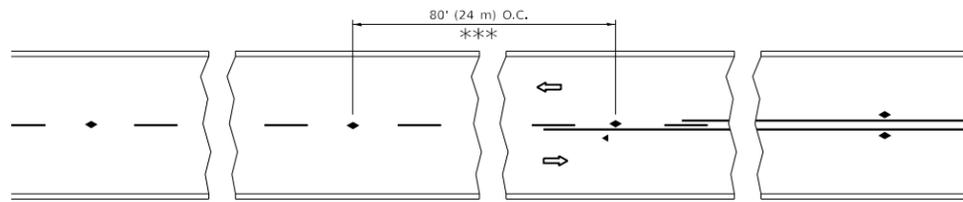
USER NAME = Velichkovv	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - T. RAMMACHER 01-06-00
PLOT DATE = 12/13/2019	DATE - 06-89	REVISED - A. SCHUETZE 07-01-13
		REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

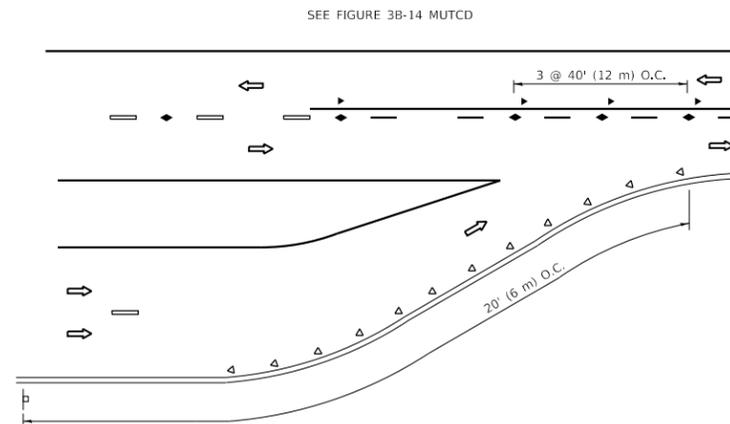
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	2019-098-R5-SW&SR	COOK	54	36
TC-10			CONTRACT NO. 62J88	
ILLINOIS FED. AID PROJECT				

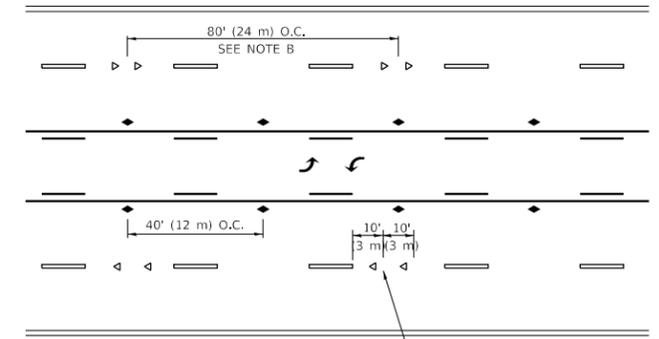


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

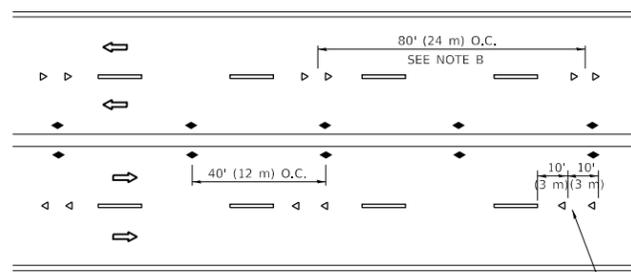
TWO-LANE/TWO-WAY



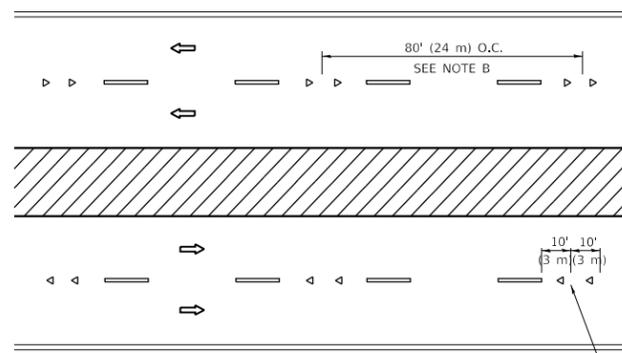
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

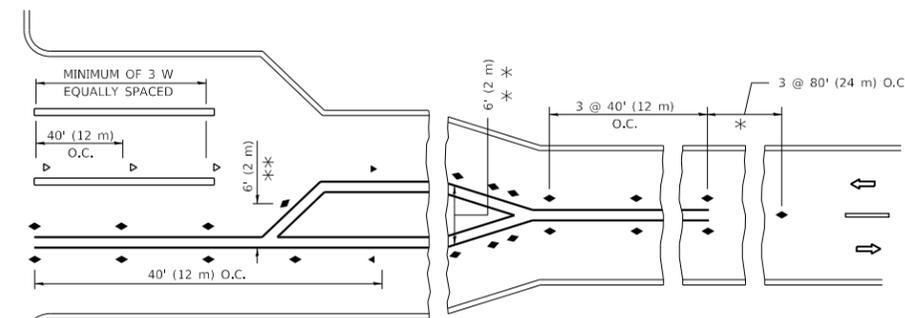
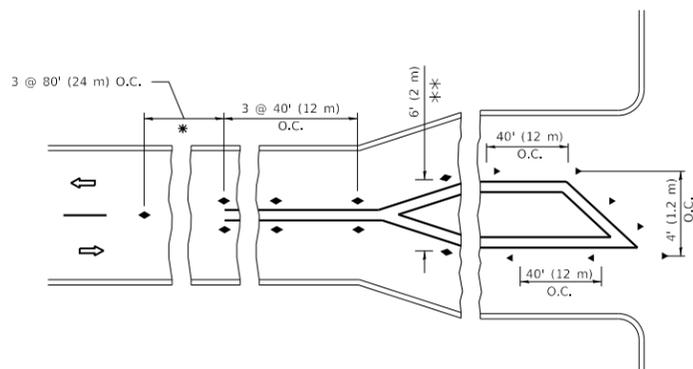
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

TURN LANES

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: Default
 FILE: \\blm01c-pub\pubroom\dat\illinois.gov\PI\DOT\Documents\DOT_Offices\I\trfct_1\Projects\124720\CADD\Data\Design\DES\54.dgn

USER NAME = Velichkovv	DESIGNED -	REVISED - T. RAMMACHER 03-12-99
PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED - T. RAMMACHER 01-06-00
PLOT DATE = 12/13/2019	CHECKED -	REVISED - C. JUCIUS 09-09-09
	DATE -	REVISED - C. JUCIUS 07-01-13

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**TYPICAL APPLICATIONS
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	2019-098-R5-SW&SR	COOK	54	37
TC-11			CONTRACT NO. 62J88	
ILLINOIS		FED. AID PROJECT		

TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

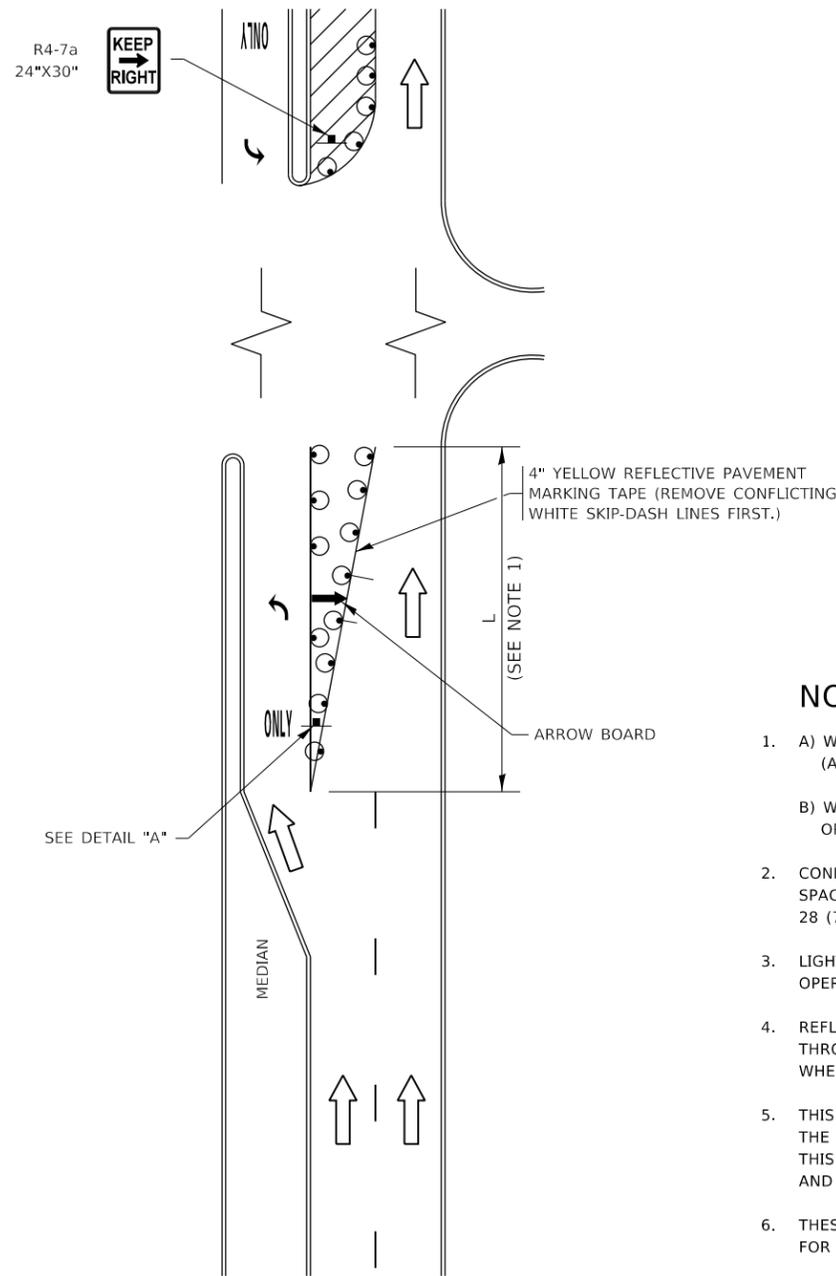


FIGURE 1

TURN BAY ENTRANCE WITHIN A LANE CLOSURE

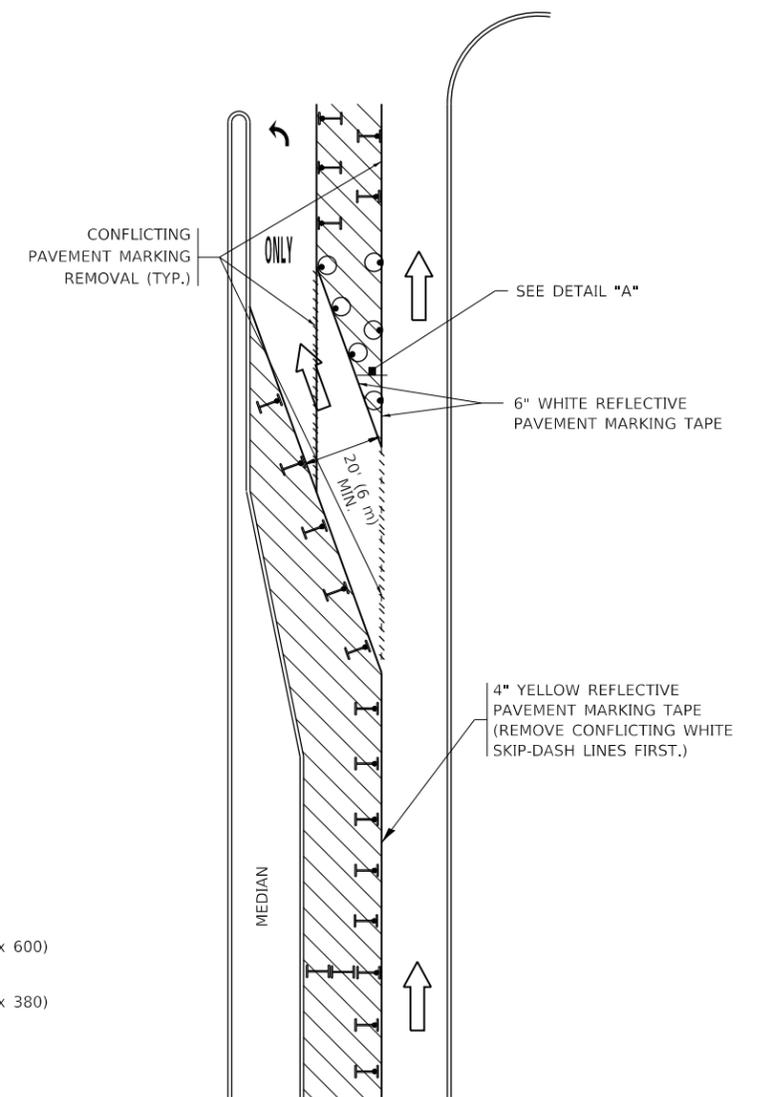
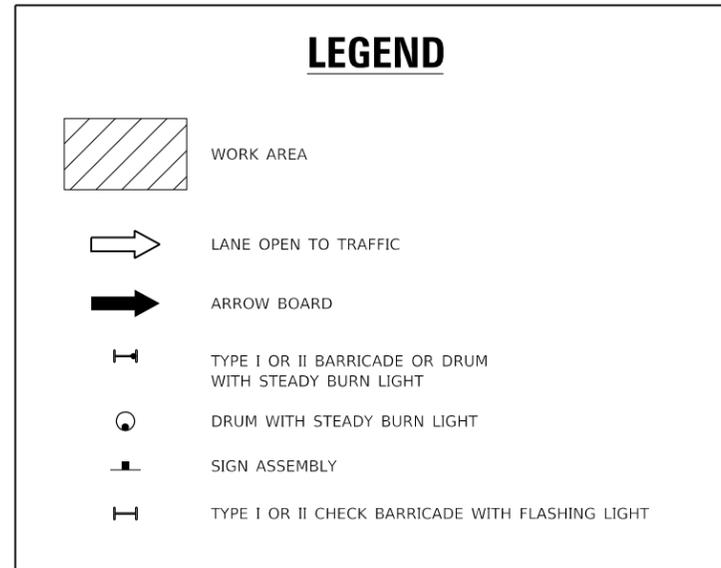
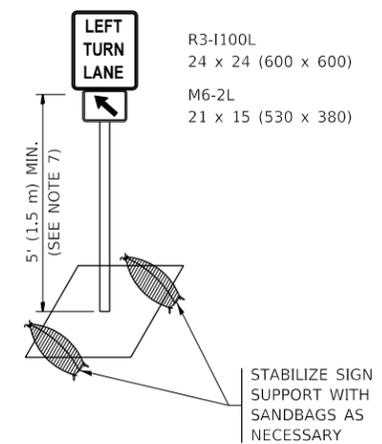


FIGURE 2



NOTES:

1. A) WHEN "L" IS \leq THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.
B) WHEN "L" IS $>$ THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



DETAIL A

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: Default
FILE: Model: ProjectRoom.dwg
PROJECT: 2019-098-R5-SW&SR
OFFICE: Cook County
DATE: 12/13/2019

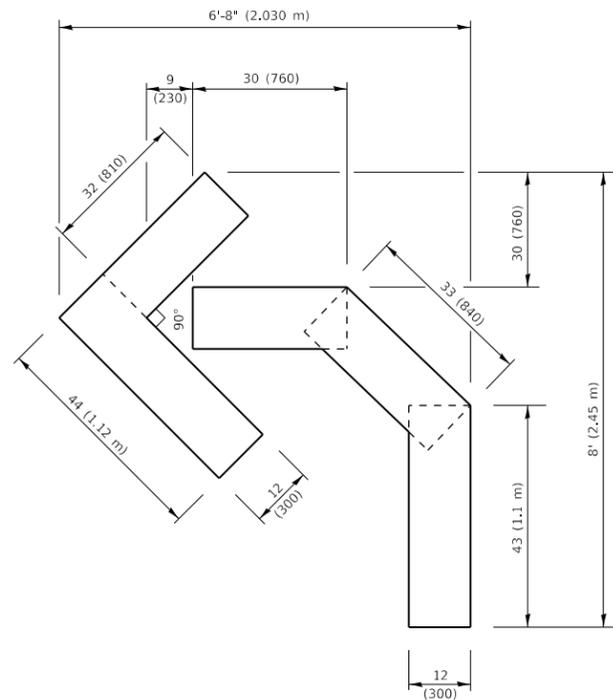
USER NAME = Velichkovvv	DESIGNED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09
	DRAWN - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13
PLOT SCALE = 100,0000' / in.	CHECKED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16
PLOT DATE = 12/13/2019	DATE - T. RAMMACHER 01-06-00	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

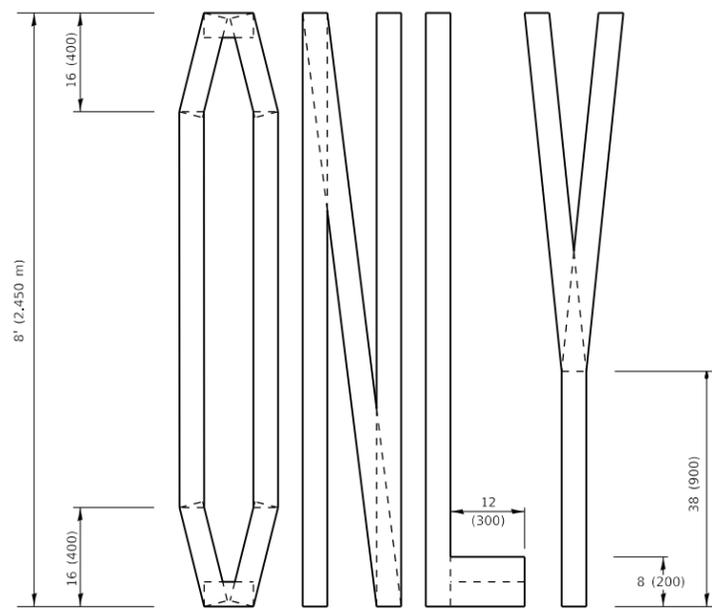
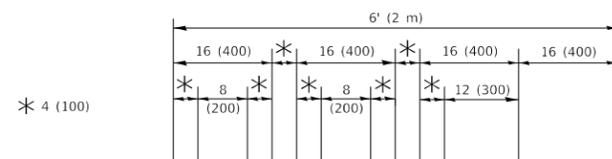
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	2019-098-R5-SW&SR	COOK	54	39
TC-14		CONTRACT NO. 62J88		
ILLINOIS FED. AID PROJECT				



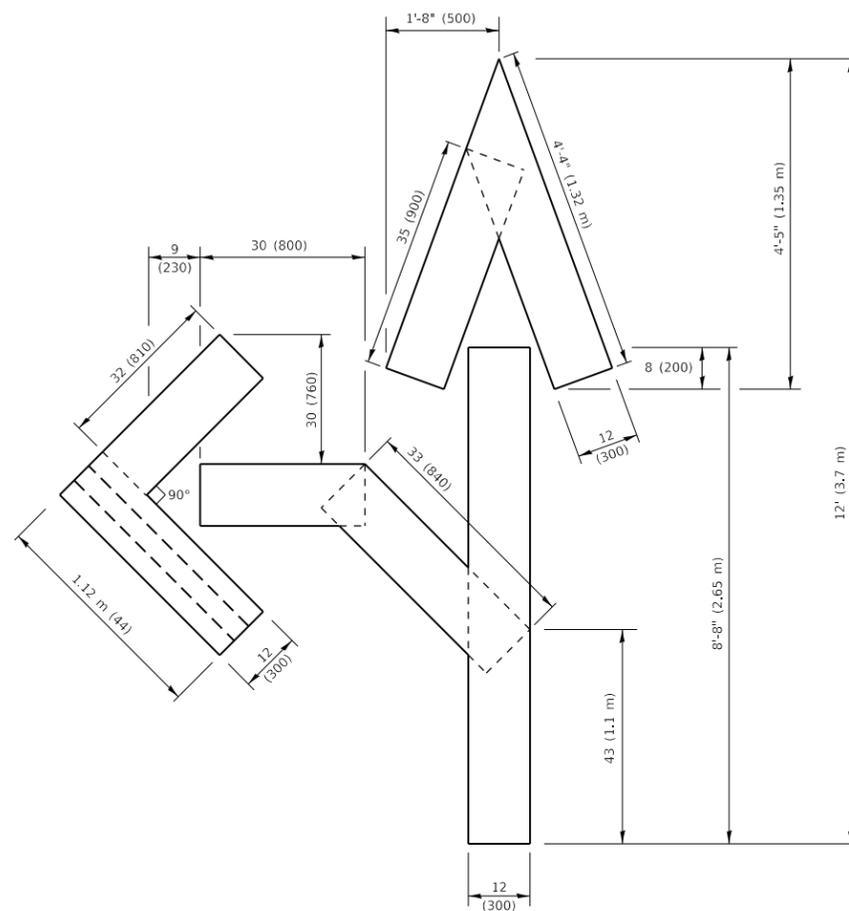
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

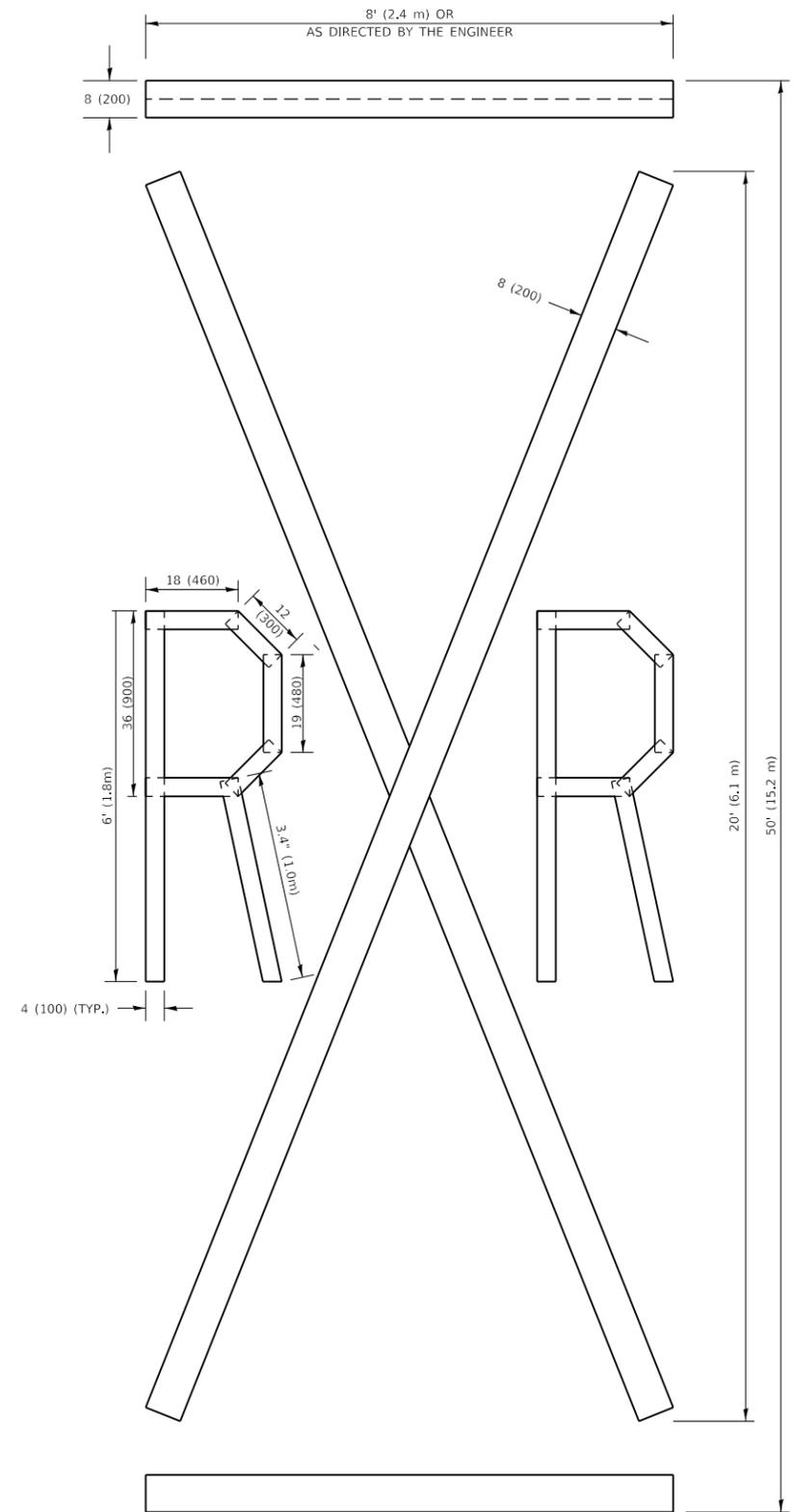


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: Default
FILE NAME: P:\pub\harcam.com.edu\illinois\poc\p\WIDOT\Documental\DOT_Offices\Dir\trt 1\Projects\124720\CADD\data\Design\BIS\Std.dgn

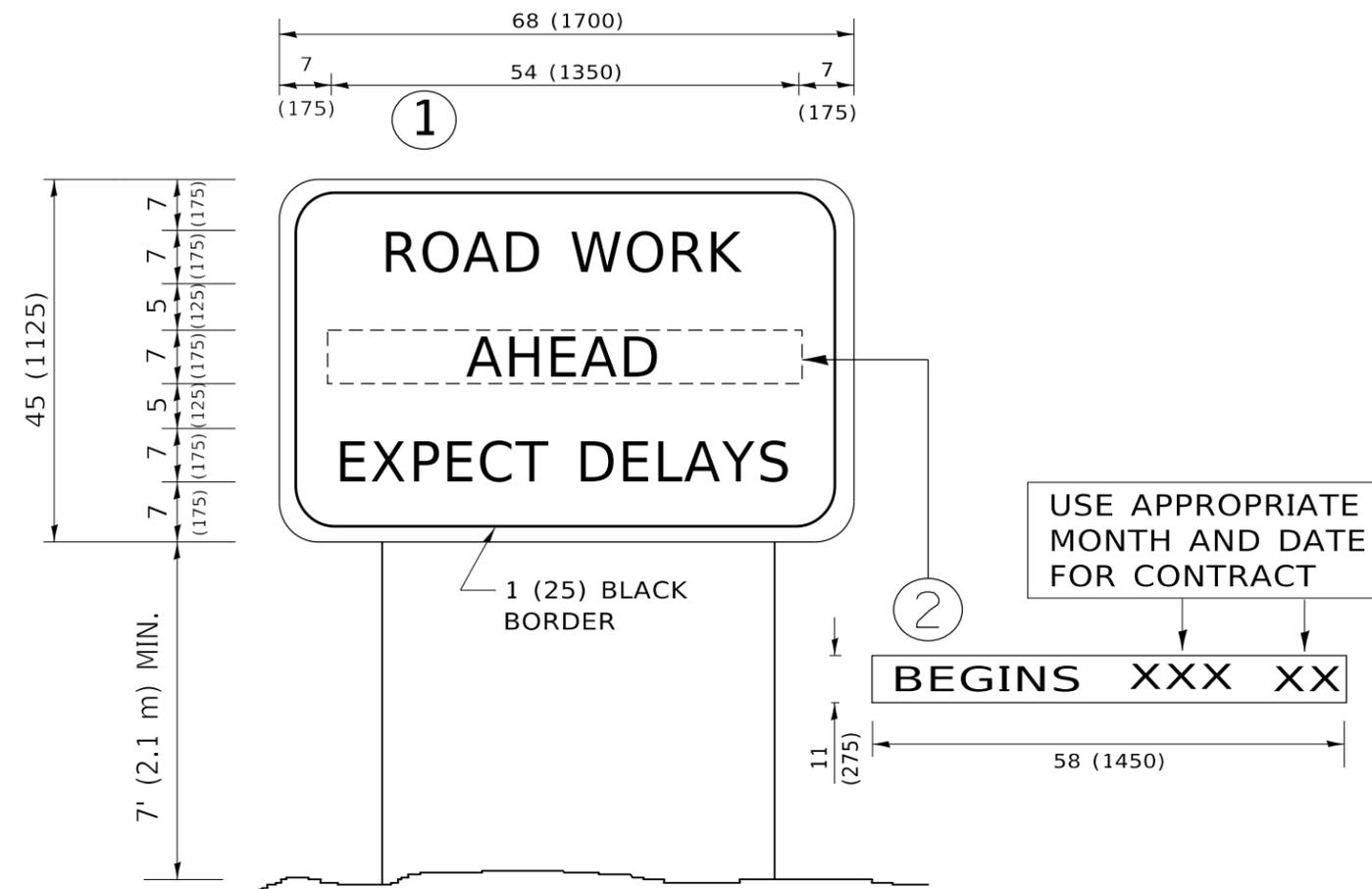
USER NAME = Vellchikovv	DESIGNED -	REVISED - T. RAMMACH 03-02-98
	DRAWN -	REVISED - E. GOMEZ 08-28-00
PLOT SCALE = 100,0010 ' / In.	CHECKED -	REVISED - E. GOMEZ 08-28-00
PLOT DATE = 12/13/2019	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	2019-098-R5-SW&SR	COOK	54	40
TC-16			CONTRACT NO. 62188	
ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: Default
 FILE: \\nas01.cpe.ppub.com\data\illinois.gov\PWIDOT\Documents\DOT_Offices\District_1\Projects\124720\CADD\Design\Design\DES.dgn

USER NAME = Velichkovv	DESIGNED -	REVISED - R. MIRS 09-15-97
	DRAWN -	REVISED - R. MIRS 12-11-97
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
PLOT DATE = 12/13/2019	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

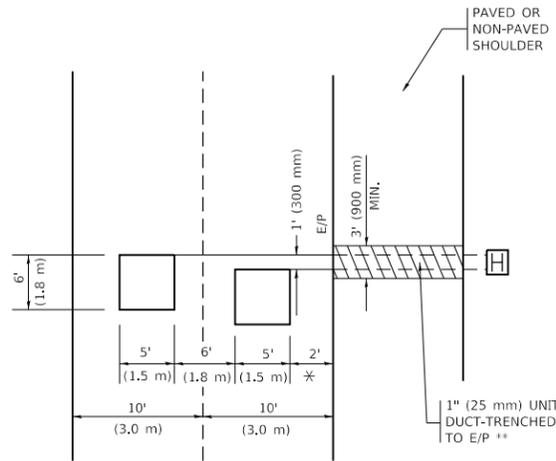
**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	2019-098-R5-SW&SR	COOK	54	41
TC-22			CONTRACT NO. 62J88	
ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

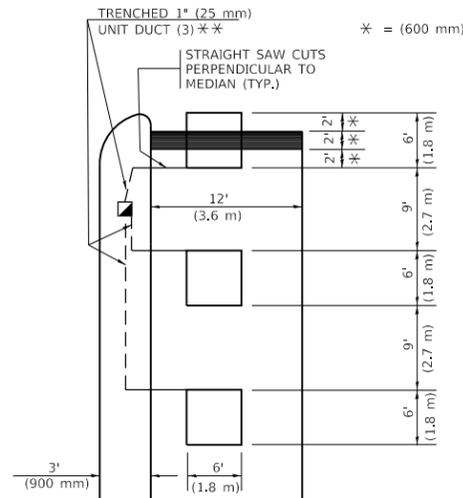


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

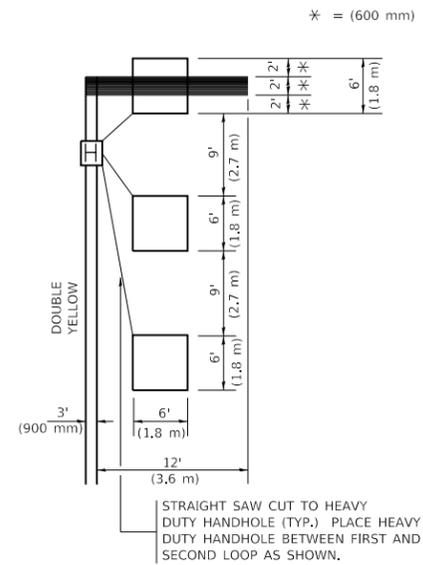


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)



* = (600 mm)

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

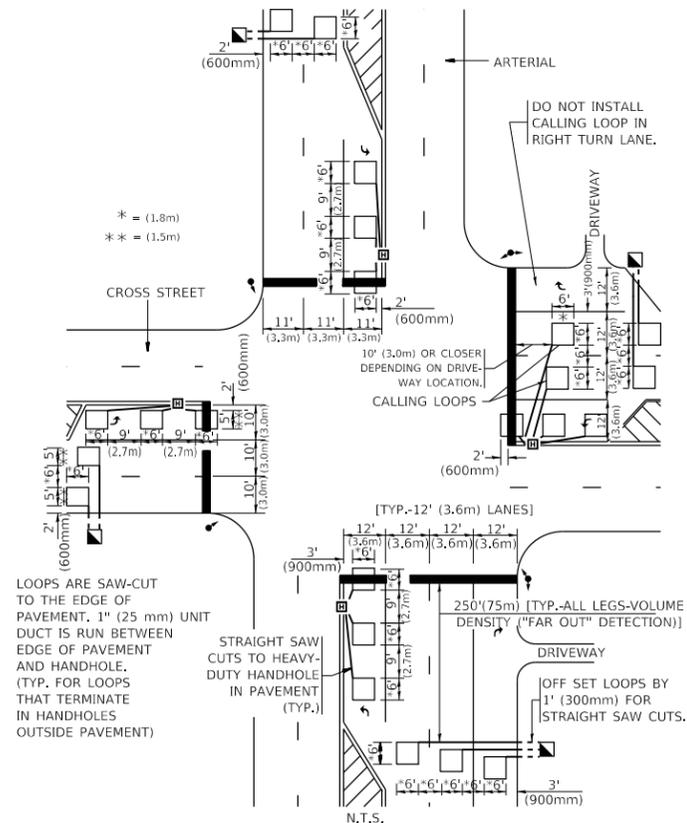
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("FAR OUT" DETECTION)



* = (1.8m)
 ** = (1.5m)

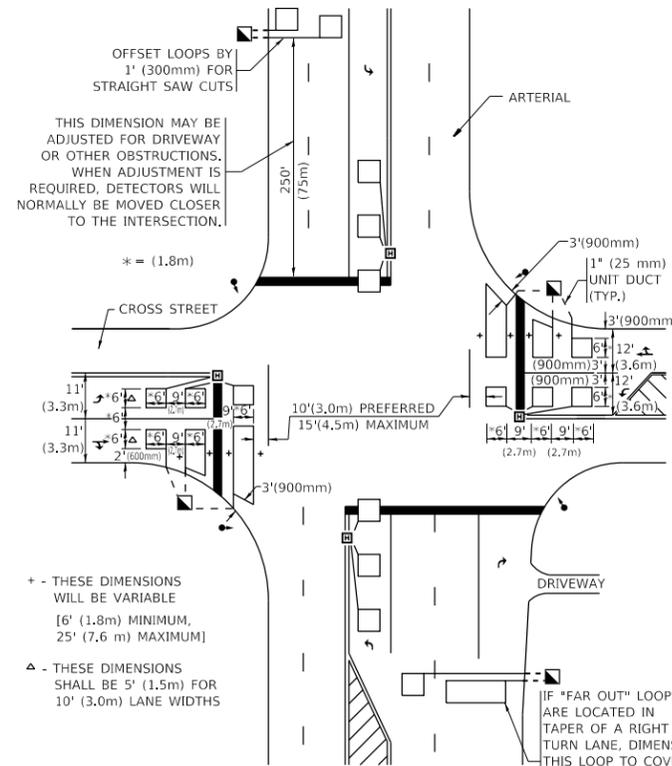
LOOPS ARE SAW-CUT TO THE EDGE OF PAVEMENT. 1" (25 mm) UNIT DUCT IS RUN BETWEEN EDGE OF PAVEMENT AND HANDHOLE. (TYP. FOR LOOPS THAT TERMINATE IN HANDHOLES OUTSIDE PAVEMENT)

STRAIGHT SAW CUTS TO HEAVY-DUTY HANDHOLE IN PAVEMENT (TYP.)

[TYP.-12' (3.6m) LANES]
 250' (75m) [TYP.-ALL LEGS-VOLUME DENSITY ("FAR OUT" DETECTION)]
 DRIVEWAY
 OFF SET LOOPS BY 1' (300mm) FOR STRAIGHT SAW CUTS.

DETAIL 1
 N.T.S.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



THIS DIMENSION MAY BE ADJUSTED FOR DRIVEWAY OR OTHER OBSTRUCTIONS. WHEN ADJUSTMENT IS REQUIRED, DETECTORS WILL NORMALLY BE MOVED CLOSER TO THE INTERSECTION.

* = (1.8m)

+ - THESE DIMENSIONS WILL BE VARIABLE [6' (1.8m) MINIMUM, 25' (7.6 m) MAXIMUM]

△ - THESE DIMENSIONS SHALL BE 5' (1.5m) FOR 10' (3.0m) LANE WIDTHS

IF "FAR OUT" LOOPS ARE LOCATED IN TAPER OF A RIGHT TURN LANE, DIMENSION THIS LOOP TO COVER TAPER AREA. DO NOT COVER THE LEFT TURN LANE OR LEFT TURN LANE TAPER.

DETAIL 2
 N.T.S.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING

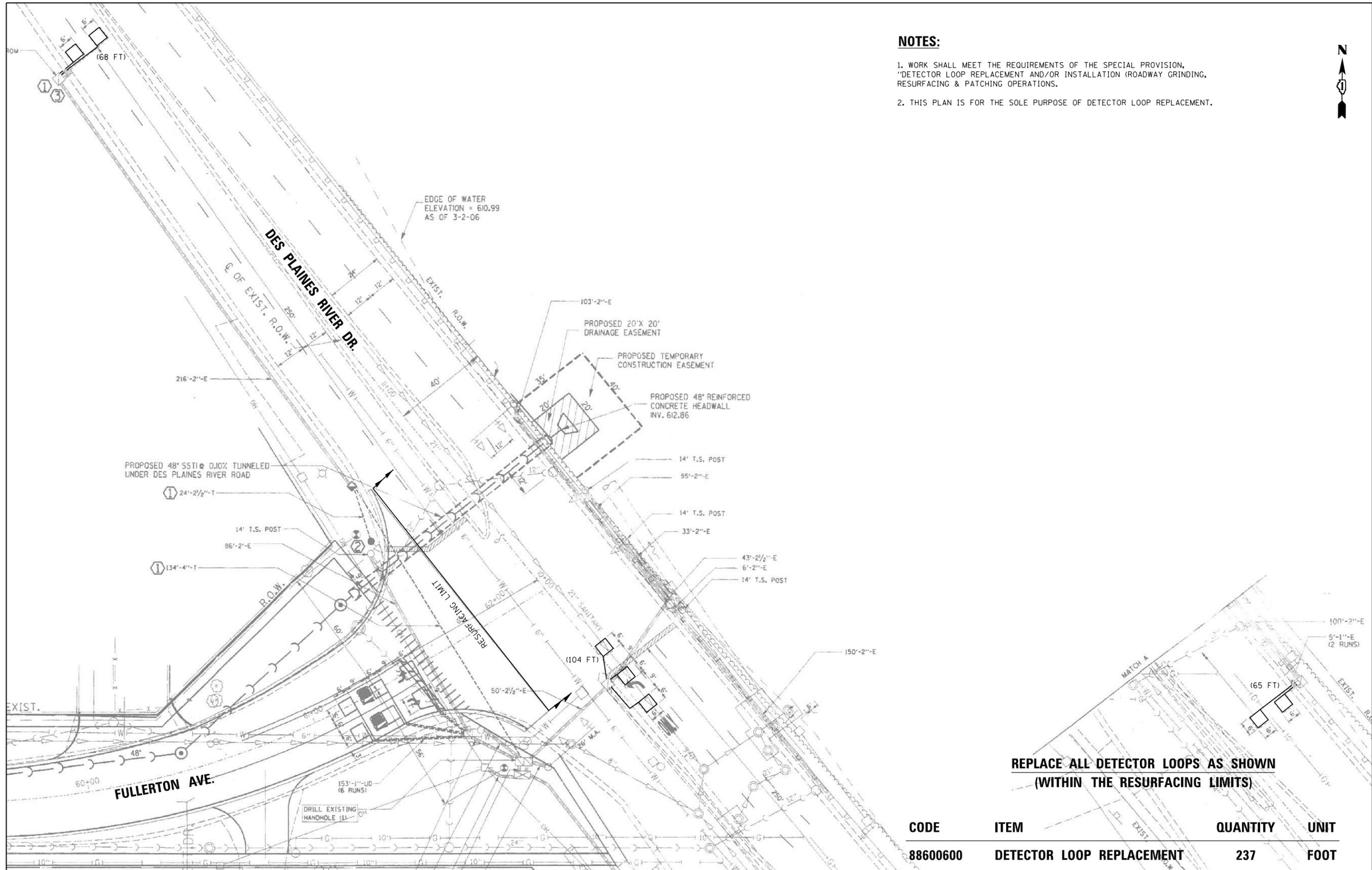
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	2019-098-R5-SW&SR	COOK	54	43
TS-07		CONTRACT NO. 62J88		
ILLINOIS FED. AID PROJECT				



NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	237	FOOT

FILE NAME = 62J88 - DesPlaines River Rd. @ Fullerton Ave.dgn	USER NAME = vargoso	DESIGNED - AV	REVISED -
		DRAWN - AV	REVISED -
		CHECKED - LP	REVISED -
		DATE - 11/13/2019	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

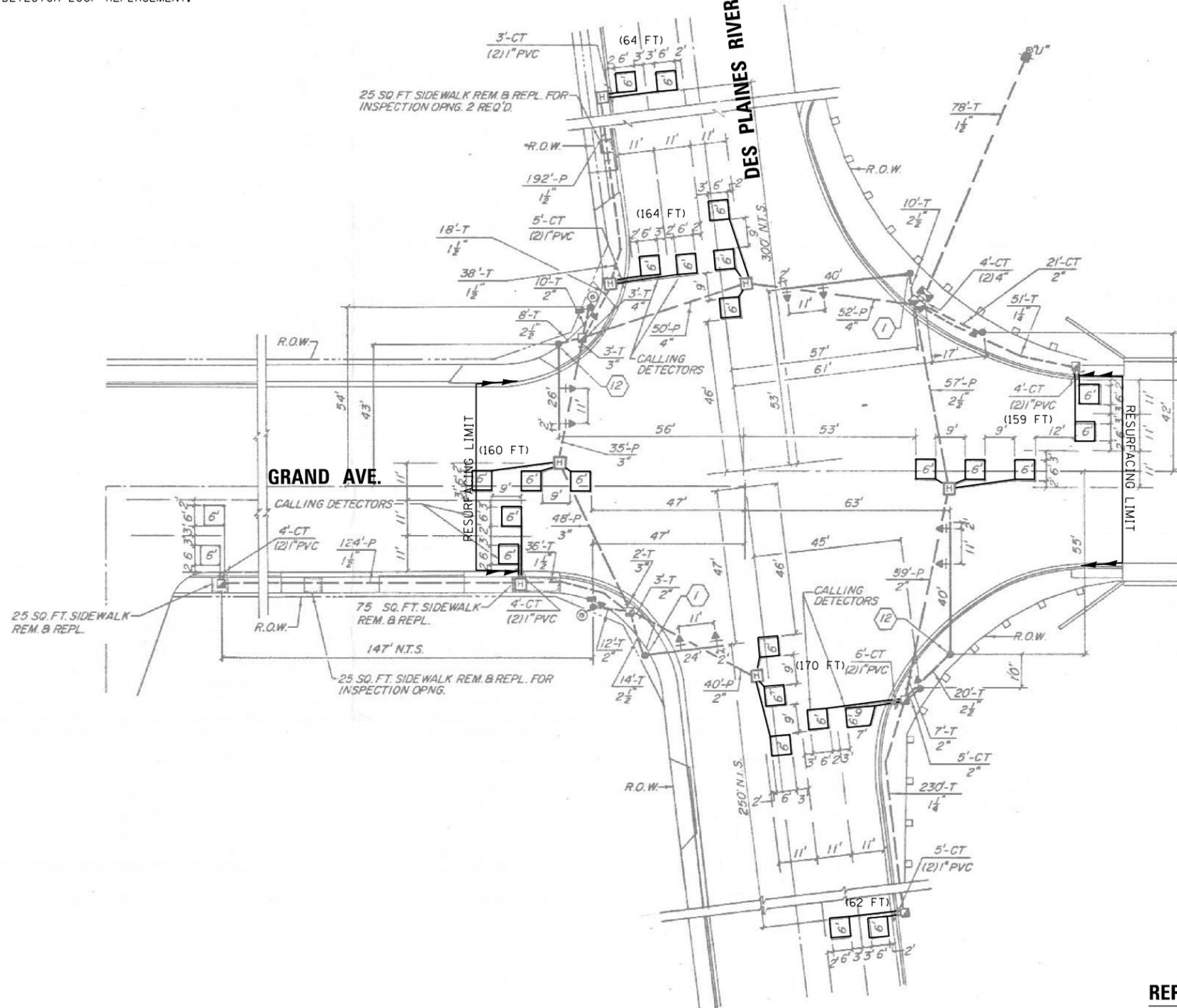
**DETECTOR LOOP REPLACEMENT PLAN
DES PLAINES RIVER RD. AT FULLERTON AVE.**

F.A.P. RTE. 2710	SECTION 2019-098-RS-SW&SR	COUNTY COOK	TOTAL SHEETS 54	SHEET NO. 46
CONTRACT NO. 62J88				
ILLINOIS FED. AID PROJECT				

SCALE: SHEET OF SHEETS STA. TO STA.

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	779	FOOT

FILE NAME = 62J88 - DesPlaines River Rd. @ Grand Ave	USER NAME = vargoso	DESIGNED - AV	REVISED -
		DRAWN - AV	REVISED -
		CHECKED - LP	REVISED -
		DATE - 11/13/2019	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

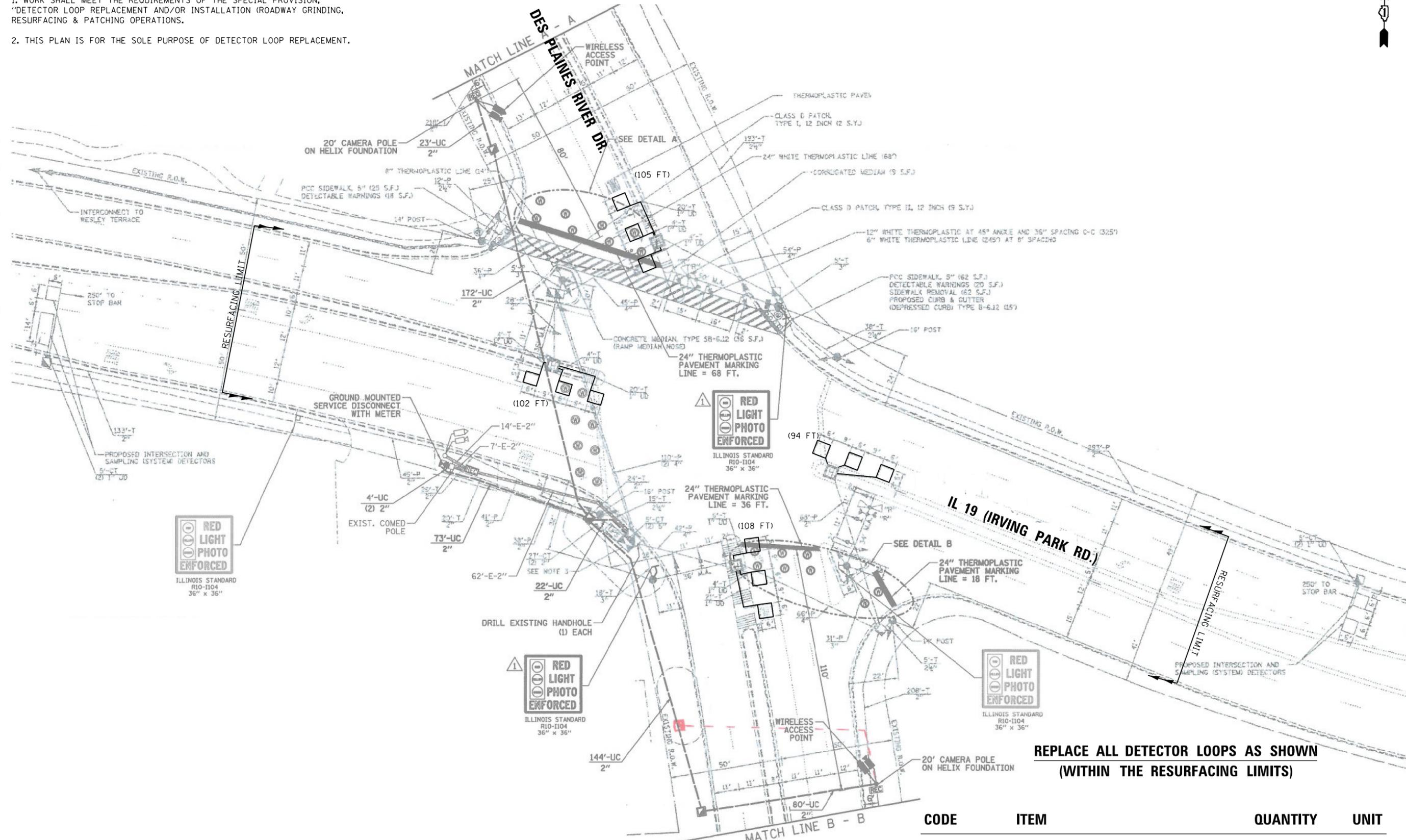
**DETECTOR LOOP REPLACEMENT PLAN
DES PLAINES RIVER RD. AT GRAND AVE.**

F.A.P. RTE. 2710	SECTION 2019-098-RS-SW&SR	COUNTY COOK	TOTAL SHEETS 54	SHEET NO. 47
CONTRACT NO. 62J88				
ILLINOIS FED. AID PROJECT				

SCALE: SHEET OF SHEETS STA. TO STA.

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	409	FOOT

FILE NAME = 62J88 - DesPlaines River Rd. @ Irving Park Rd.dgn	USER NAME = vargosa	DESIGNED - AV	REVISED -
PLOT SCALE = 40.0073' / in.	CHECKED - LP	DATE - 11/13/2019	REVISED -
Default	PLOT DATE = 11/14/2019		

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

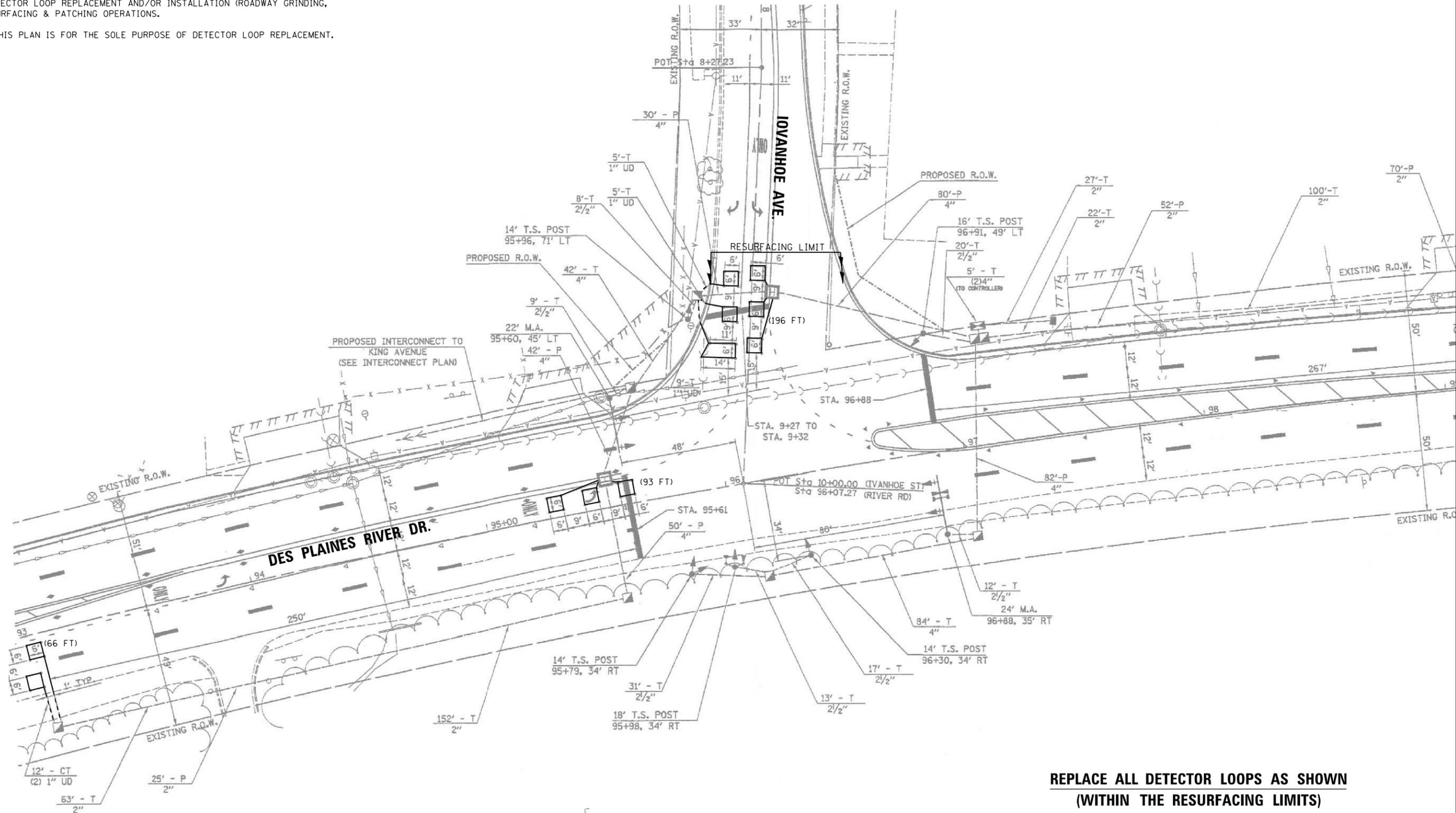
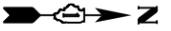
**DETECTOR LOOP REPLACEMENT PLAN
DES PLAINES RIVER RD. AT IL 19 (IRVING PARK RD.)**

F.A.P. RTE. 2710	SECTION 2019-098-RS-SW&SR	COUNTY COOK	TOTAL SHEETS 54	SHEET NO. 49
CONTRACT NO. 62J88				
ILLINOIS FED. AID PROJECT				

SCALE: SHEET OF SHEETS STA. TO STA.

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



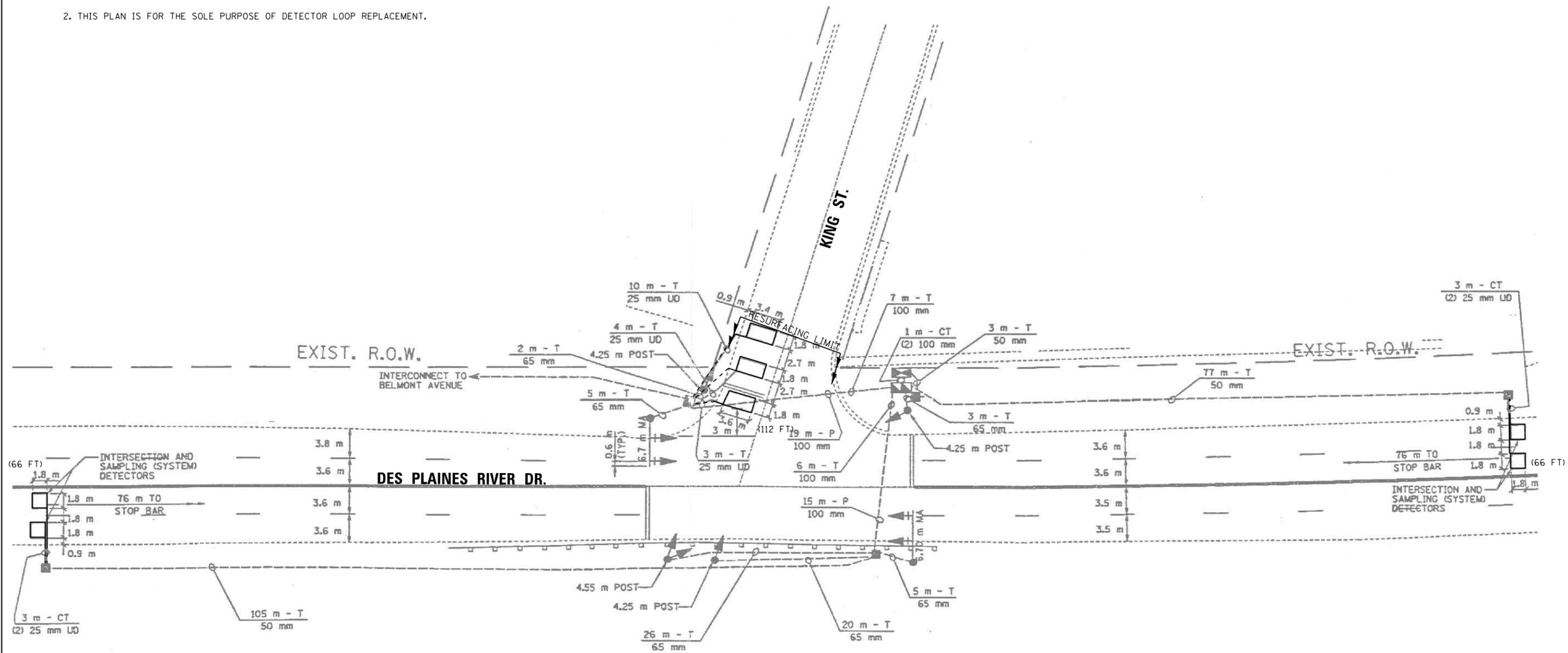
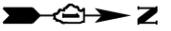
**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	355	FOOT

FILE NAME = 62J88 - DesPlaines River Rd. @ Ivanhoe Ave.dgn	USER NAME = vargoso	DESIGNED - AV	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETECTOR LOOP REPLACEMENT PLAN DES PLAINES RIVER RD. AT IVANHOE AVE.	F.A.P. RTE. 2710	SECTION 2019-098-RS-SW&SR	COUNTY COOK	TOTAL SHEETS 54	SHEET NO. 51		
Default	PLOT SCALE = 40.0070' / in.	CHECKED - LP	REVISED -			SCALE:	SHEET OF SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT			
	PLOT DATE = 11/14/2019	DATE - 11/13/2019	REVISED -									
CONTRACT NO. 62J88												

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



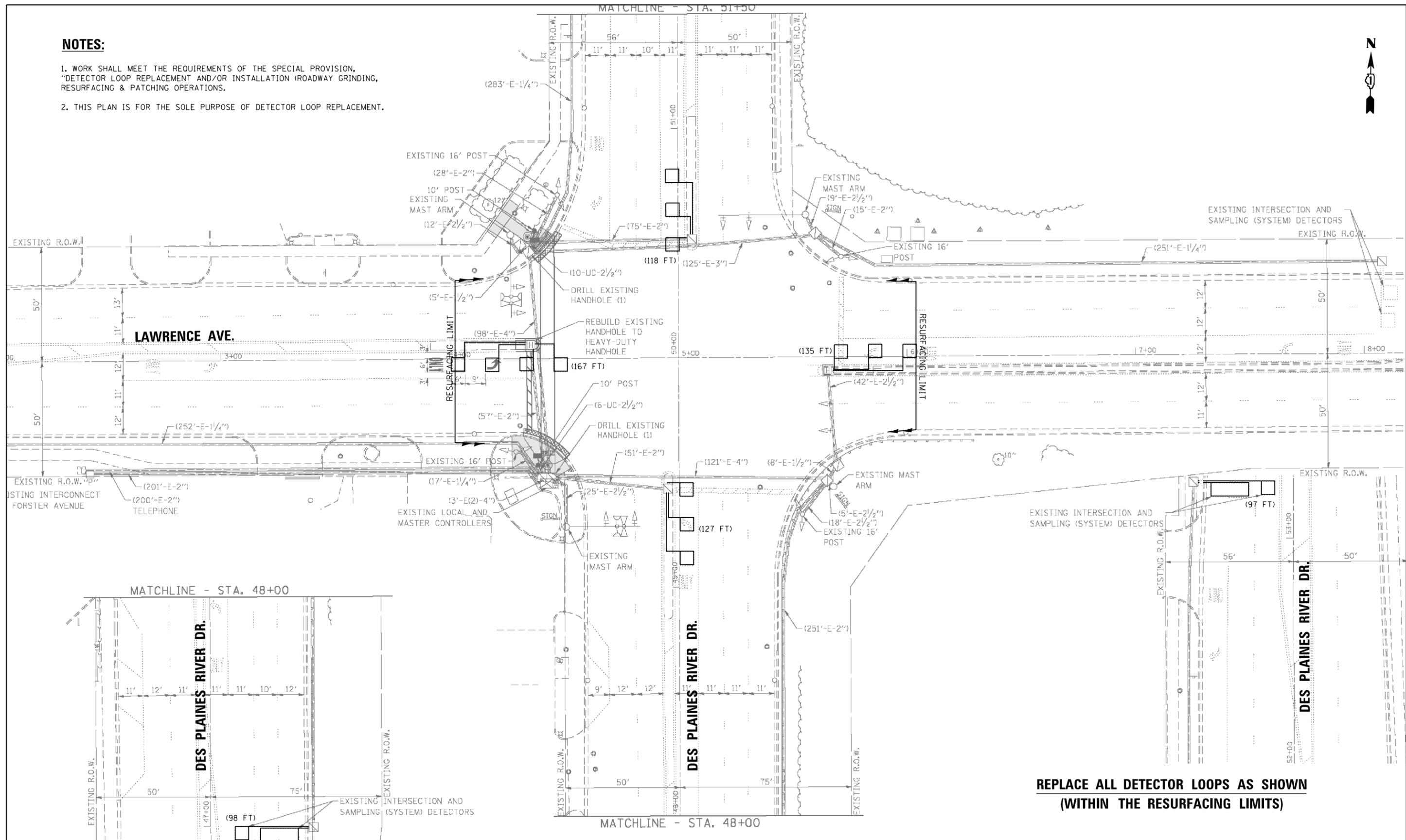
**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	244	FOOT

FILE NAME = 62J88 - DesPlaines River Rd. @ King St.dgn	USER NAME = vargasa	DESIGNED - AV	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETECTOR LOOP REPLACEMENT PLAN DES PLAINES RIVER RD. AT KING ST.	F.A.P. RTE. = 2710	SECTION = 2019-098-RS-SW&SR	COUNTY = COOK	TOTAL SHEETS = 54	SHEET NO. = 52		
Default	PLOT SCALE = 40.0004' / in.	CHECKED - LP	REVISED -			SCALE:	SHEET OF SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT			
	PLOT DATE = 11/14/2019	DATE = 11/13/2019	REVISED -									

NOTES:

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



**REPLACE ALL DETECTOR LOOPS AS SHOWN
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	742	FOOT

FILE NAME = 62J88 - DesPlaines River Rd. @ Lawrence Ave.dgn	USER NAME = vargaso	DESIGNED - AV	REVISED -
		DRAWN - AV	REVISED -
		CHECKED - LP	REVISED -
		DATE - 11/13/2019	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

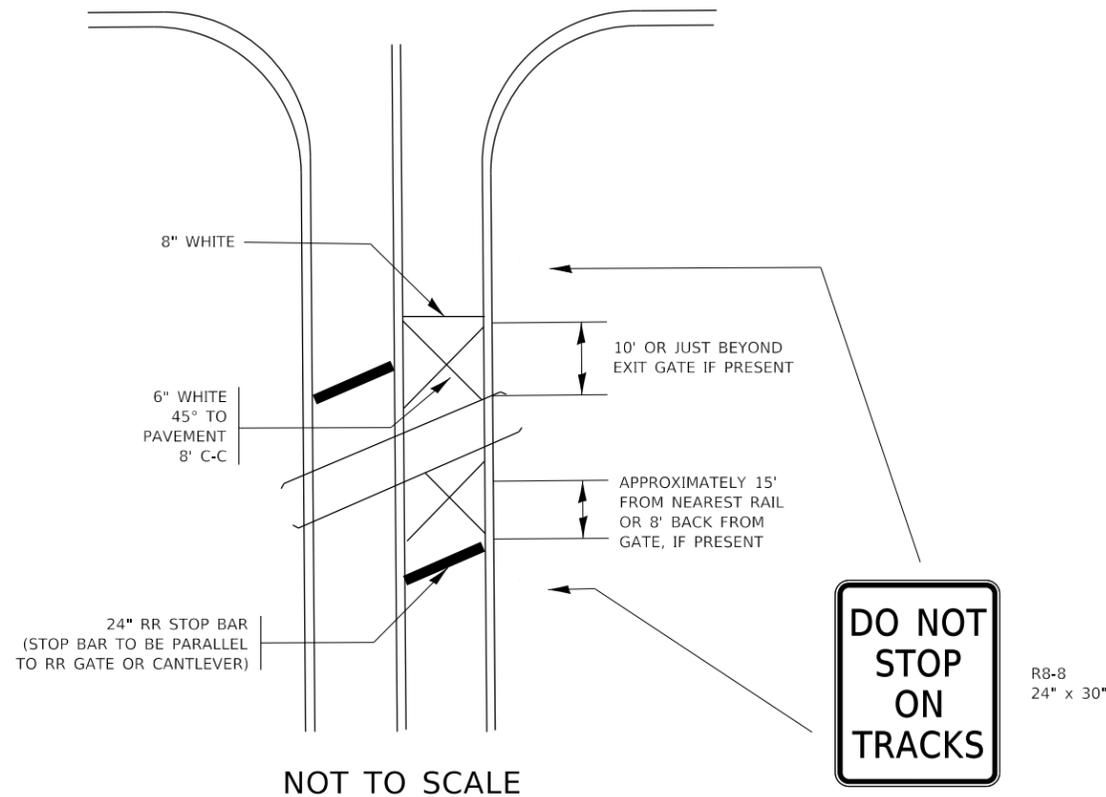
**DETECTOR LOOP REPLACEMENT PLAN
DES PLAINES RIVER RD. AT LAWRENCE AVE.**

F.A.P. RTE. 2710	SECTION 2019-098-RS-SW&SR	COUNTY COOK	TOTAL SHEETS 54	SHEET NO. 53
CONTRACT NO. 62J88				
ILLINOIS FED. AID PROJECT				

SCALE: SHEET OF SHEETS STA. TO STA.

TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING TREATMENT FOR RAILROAD CROSSINGS

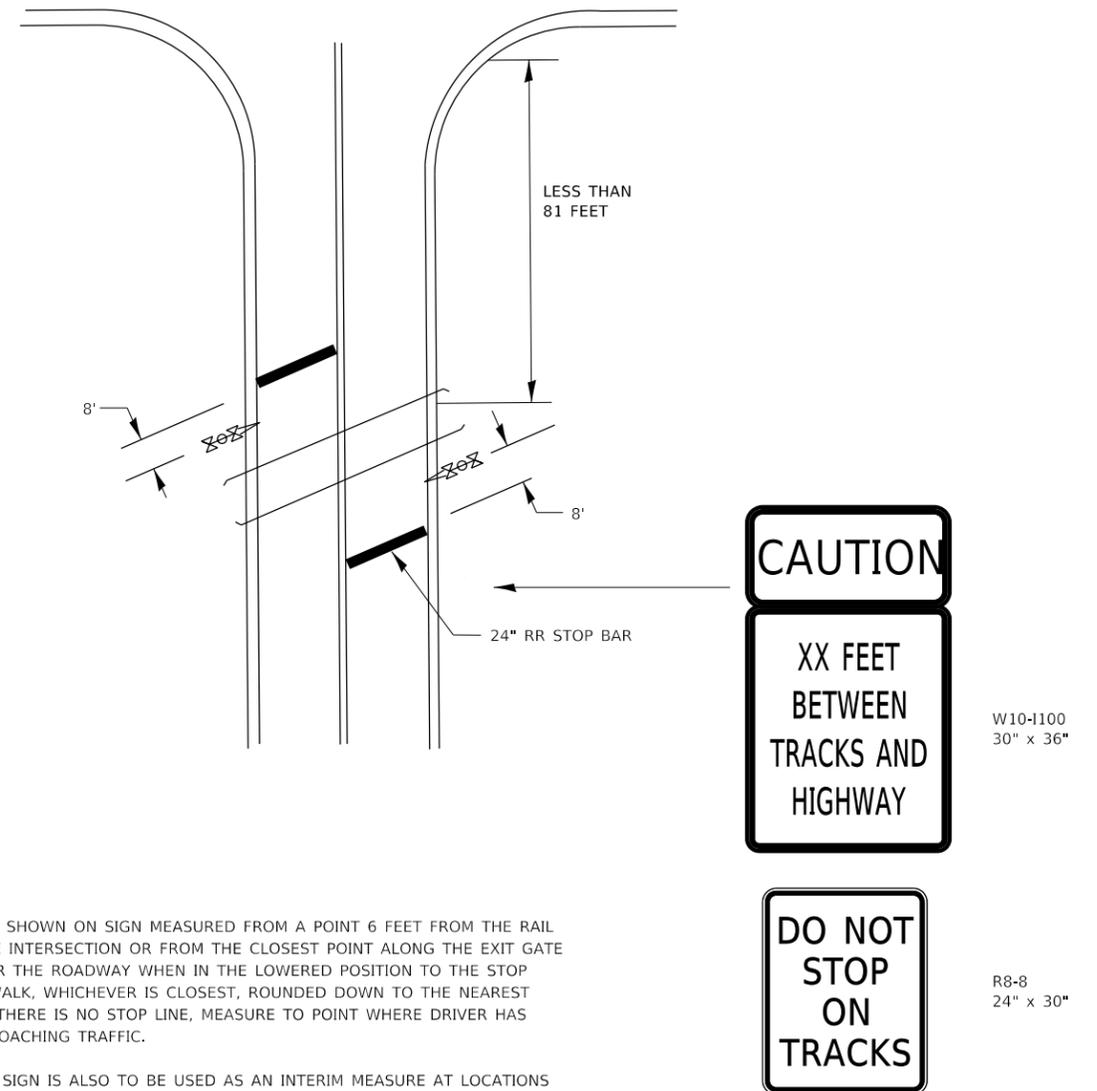
WITH SIGNALIZED INTERSECTION



NOTE:

- PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
- WHERE NEAR-SIDE TRAFFIC SIGNALS ARE USED THE PAVEMENT MARKINGS EXTEND TO THE INTERSECTION. (SEE DETAIL FOR PRE-SIGNALS).

WITH NON-SIGNALIZED INTERSECTION 81' OR LESS TO CLOSEST RAIL



NOTE:

- DISTANCE TO BE SHOWN ON SIGN MEASURED FROM A POINT 6 FEET FROM THE RAIL CLOSEST TO THE INTERSECTION OR FROM THE CLOSEST POINT ALONG THE EXIT GATE IF PRESENT OVER THE ROADWAY WHEN IN THE LOWERED POSITION TO THE STOP BAR OR CROSSWALK, WHICHEVER IS CLOSEST, ROUNDED DOWN TO THE NEAREST 5 FEET. WHERE THERE IS NO STOP LINE, MEASURE TO POINT WHERE DRIVER HAS A VIEW OF APPROACHING TRAFFIC.
- THE CLEARANCE SIGN IS ALSO TO BE USED AS AN INTERIM MEASURE AT LOCATIONS WITH INTERCONNECTED INTERSECTION TRAFFIC SIGNALS WHERE IT IS PLANNED TO CHANGE THEM TO NEAR-SIDE SIGNALS AT A FUTURE TIME. IN THIS CASE, THE DISTANCE TO BE SHOWN ON THE SIGN IS MEASURED FROM THE EDGE OF THE STRIPED-OUT AREA INSTEAD OF 6 FEET FROM THE RAIL. THE SIGN IS TO BE REMOVED WHEN THE NEAR-SIDE SIGNALS ARE INSTALLED AND THE PAVEMENT MARKING EXTEND TO THE INTERSECTION.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

MODEL: Default
FILE: Model: P:\pub\baronm.dwg
PROJECT: 121720\CaddData\Design\DESIGN.dwg

USER NAME = Velichkovv	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0001' / in.	CHECKED -	REVISED -
PLOT DATE = 12/13/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SUPPLEMENTAL SIGNING AND PAVEMENT MARKING
TREATMENT FOR RAILROAD CROSSINGS

SCALE: NONE SHEET 2 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2710	2019-098-R5-SW&SR	COOK	54	54
TC-23			CONTRACT NO. 62J88	
ILLINOIS FED. AID PROJECT				