

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
**PROPOSED
HIGHWAY PLANS**

F.A.I. ROUTE 155 (I-155)
SECTION 90(109,109-1,110)RS-2
PROJECT ACNHPP-0155 (101)
INTERSTATE
TAZEWELL COUNTY

C-94-036-12

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|-----------------------|----------|--------------------|-----------|
| 155 | 90(109,109-1,110)RS-2 | TAZEWELL | 45 | 1 |
| ILLINOIS | | | CONTRACT NO. 68A77 | |

INDEX OF SHEETS

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- 2.-3. COMMITMENTS/GENERAL NOTES/JOB SPECIFIC NOTES
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- 16.-26. SCHEDULES OF QUANTITIES
27. LINE DIAGRAM
- 28.-29. PAVEMENT MARKING DETAIL
30. I-155/IL 122 N JCT INTERCHANGE DETAIL
- 31.-32. PROTECTIVE SHIELD DETAIL
33. RECOVERABLE DELINEATORS DETAIL
34. BRIDGE APPROACH MILLING DETAIL AND MEDIAN CROSS-OVER DETAIL
- 35.-45. DISTRICT STANDARDS
HIGHWAY STANDARDS

HIGHWAY STANDARDS

| | | |
|-----------|-----------|-----------|
| 280001-07 | 635001-01 | 701401-09 |
| 442101-07 | 642001-02 | 701411-09 |
| 606301-04 | 643001-02 | 701421-07 |
| 630001-10 | 701101-04 | 701426-07 |
| 630301-06 | 701106-02 | 701456-03 |
| 631011-09 | 701306-03 | 701901-04 |
| 631026-06 | 701316-09 | 780001-05 |
| 631031-13 | 701400-08 | 781001-03 |
| 631033-06 | 701406-09 | 782001 |

DISTRICT STANDARDS - INCLUDED AS SHEETS 35-45.

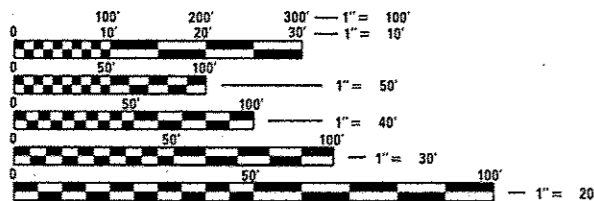
- 281001-D4
- 406001-D4
- 406101-D4
- 440001-D4
- 630101-D4
- 650004-D4
- 780001-D4

DESIGN DESIGNATION

INTERSTATE
ADT 11,600 (2013)
SU 6.67% (2013)
MU 15.27% (2013)

PROJECT DESCRIPTION

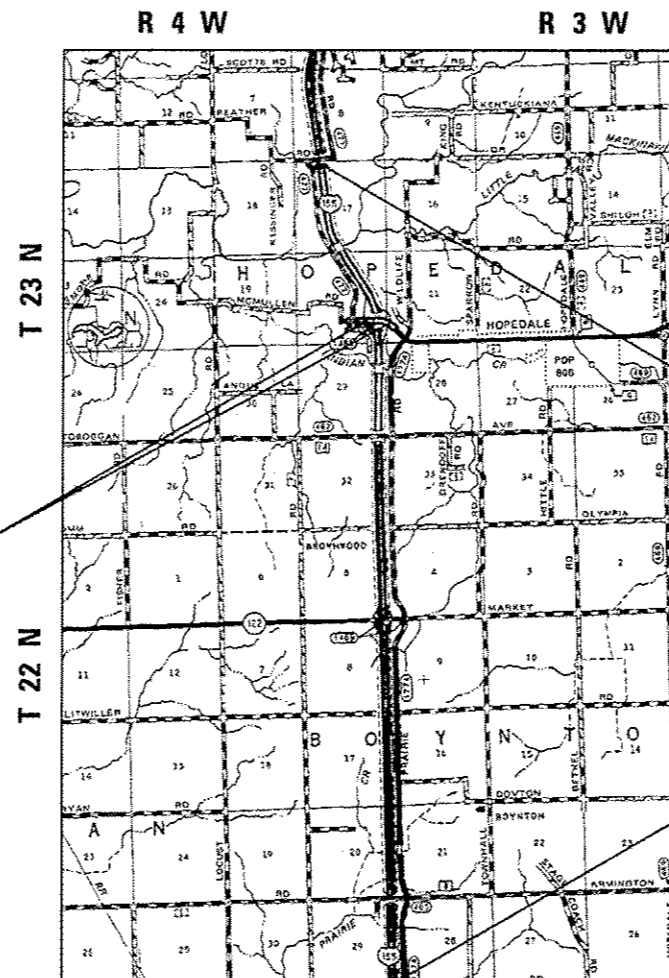
WORK ON THIS PROJECT CONSISTS OF MILLING, RESURFACING AND PATCHING ON I-155; ISOLATED GRADING AND SHAPING OF DITCHES; BRIDGE DECK WEARING SURFACE REPLACEMENT ON S.N. 090-0095 CARRYING IL 122 OVER I-155.



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER CHRISTOPHER MAUSHARD (309)671-3453
PROJECT MANAGER MIKE MOHAMED (309)671-3462
CONTRACT NO. 68A77
CATALOG NO. 034751-00D



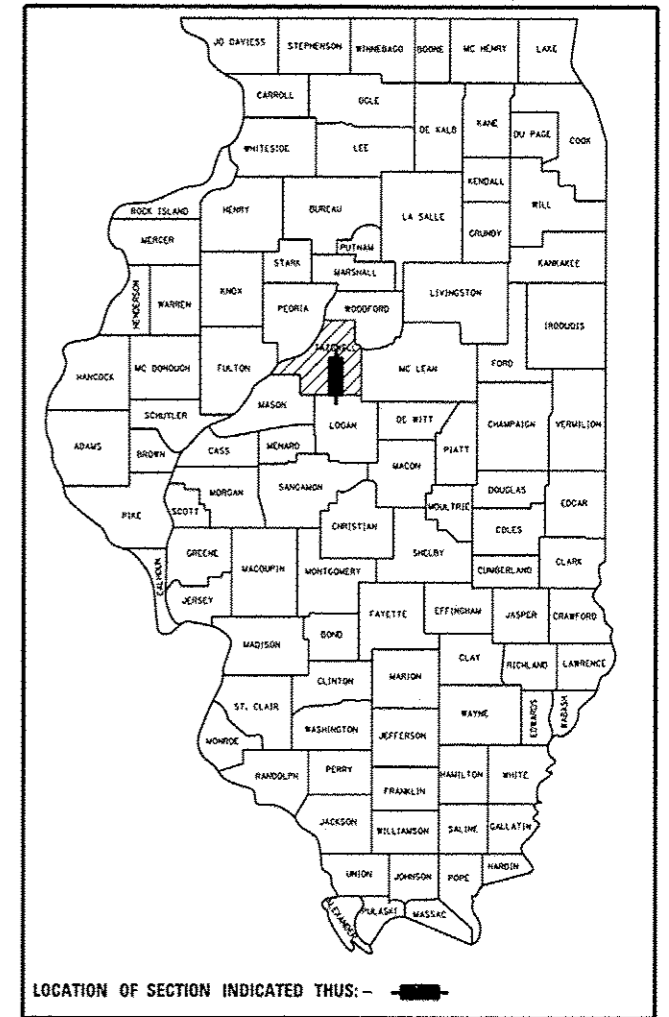
IL 122 N. JCT
STA. 20 + 80.31 TO
STA. 38 + 39.73

PROJECT BEGINS
NB STA. 616 + 26.95
SB STA. 616 + 36.21

PROJECT ENDS
STA. 1102 + 30.32 (BK)
STA. 546 + 16.17 (AH)

NOTE: SEE LINE DIAGRAM FOR STATION EQUATIONS AND STRUCTURE OMISSIONS

GROSS LENGTH = 48,582.88 FT. = 9.20 MILE
NET LENGTH = 47,846.04 FT. = 9.06 MILE



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED Dec 9 2014
Kenneth A. Skerrett
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

Jan 30 2015
John D. Baranzelli, PE
ENGINEER OF DESIGN AND ENVIRONMENT

Jan 30 2015
Omer Osman, PE
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

COMMITMENTS

NO COMMITMENTS HAVE BEEN MADE ON THIS PROJECT

GENERAL NOTES

TREE REMOVAL

RESIDENT ENGINEER SHOULD BE CONTACTED AND PRIOR APPROVAL OBTAINED FOR ANY TREE REMOVAL BEYOND THE LIMITS/LOCATIONS INCLUDED IN THE PLANS.

ENVIRONMENTAL REVIEWS

PRIOR TO THE USE OF ANY PROPOSED BORROW AREAS, USE AREAS (TEMPORARY ACCESS ROADS, DETOURS, RUN-AROUNDS, ETC.) AND/OR WASTE AREAS, THE CONTRACTOR SHALL FILE THE REQUIRED ENVIRONMENTAL RESOURCE REQUEST SURVEYS ACCORDING TO SECTION 107.22 OF THE STANDARD SPECIFICATIONS. THESE SURVEYS ARE REQUIRED IN ORDER FOR THE DEPARTMENT TO CONDUCT CULTURAL AND BIOLOGICAL RESOURCE SURVEYS FOR THE PROPOSED SITE.

PRIOR TO ANY WASTE MATERIALS BEING REMOVED FROM THE CONSTRUCTION SITE THE REQUIRED ENVIRONMENTAL RESOURCE SURVEYS WILL NEED TO BE OBTAINED AND FILED BY THE CONTRACTOR. EXCESS WASTE PRODUCTS REMOVED FROM THE CONSTRUCTION SITE SHALL BE DISPOSED OF AS REQUIRED IN SECTION 202.03 OF THE STANDARD SPECIFICATIONS.

ANY PROTRUDING METAL BARS SHALL BE REMOVED PRIOR TO THE DISPOSAL OF BROKEN CONCRETE AT APPROVED DISPOSAL SITES.

THE REQUIRED ENVIRONMENTAL RESOURCE DOCUMENTATION SHALL INCLUDE THE FOLLOWING:

- * BDE FORM 2289 (ENVIRONMENTAL SURVEY REQUEST)
- * A LOCATION MAP SHOWING THE SIZE LIMITS AND LOCATION OF THE USE AREA
- * SIGNED PROPERTY OWNER AGREEMENT FORM - D4 PI0100
- * COLOR PHOTOGRAPHS DEPICTING THE USE AREA
- * BORROW AREA ENTRY AGREEMENT FORM - D4 PI0101

PLEASE NOTE THAT A MINIMUM OF TWO WEEKS SHALL BE ALLOWED FOR THE DISTRICT TO OBTAIN THE REQUIRED ENVIRONMENTAL CLEARANCES.

BRIDGE OVERLAY NOTIFICATION

AFTER PLACEMENT OF THE BRIDGE DECK OVERLAY, THE RESIDENT ENGINEER SHALL NOTIFY THE DISTRICT BRIDGE MAINTENANCE ENGINEER OF THE "AS CONSTRUCTED" MILLING DEPTH AND OVERLAY THICKNESS FOR UPDATING THE ILLINOIS HIGHWAY INFORMATION SYSTEM.

MINIMUM VERTICAL CLEARANCE

THE CONTRACTOR SHALL VERIFY THE EXISTING STRUCTURE VERTICAL CLEARANCE PRIOR TO PLACING BITUMINOUS OVERLAY UNDER ANY STRUCTURE. A MINIMUM 16 FT VERTICAL CLEARANCE SHALL BE MAINTAINED UNDER ALL STRUCTURES.

BUTT JOINT CUTTING TIME RESTRICTION

BUTT JOINTS SHALL NOT BE MILLED MORE THAN THREE (3) DAYS PRIOR TO PLACEMENT OF THE BITUMINOUS SURFACE COURSE.

PAVING SURFACE COURSE

CONTINUOUS PAVING OPERATIONS ON THE MAIN ROADWAY SHALL BE MAINTAINED AT ALL TIMES DURING THE CONSTRUCTION OF THE HOT-MIX ASPHALT SURFACE. NO INTERRUPTIONS FOR SIDE ROADS, ENTRANCES, TURN LANES, ETC. WILL BE ALLOWED.

PAVEMENT STATION NUMBERS & PLACEMENT

THE CONTRACTOR SHALL PROVIDE LABOR AND MATERIALS REQUIRED TO IMPRINT PAVEMENT STATION NUMBERS IN THE FINISHED SURFACE OF THE PAVEMENT AND/OR OVERLAY. THEY NUMBERS SHALL BE APPROXIMATELY 3/4 INCH (20 MM) WIDE, 5 INCHES (125 MM) HIGH AND 5/8 INCH (15 MM) DEEP.

THE PAVEMENT STATION NUMBERS SHALL BE INSTALLED AS SPECIFIED HEREIN:

INTERVAL - 200 FEET (ENGLISH STATIONING) OR 100 METERS (METRIC STATIONING)

BOTTOM OF NUMBERS - 6 INCHES (150 MM) FROM THE INSIDE EDGE OF THE PAVEMENT MARKING

LOCATION:

- 2, 3 & 5 LANE PAVEMENTS - RIGHT EDGE OF PAVEMENT IN DIRECTION OF INCREASING STATIONS
- MULTI-LANE DIVIDED ROADWAYS - OUTSIDE EDGE OF PAVEMENT IN BOTH DIRECTIONS
- RAMPS - ALONG BASELINE EDGE OF PAVEMENT

POSITION - STATIONS SHALL BE PLACED SO THEY CAN BE READ FROM THE ADJACENT SHOULDER

FORMAT - ENGLISH (METRIC) PAVEMENT STATIONS SHALL USE THIS FORMAT "XXX (XX+X00)", WHERE X REPRESENTS THE PAVEMENT STATION

THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT WILL BE INCLUDED IN THE COST OF THE ASSOCIATED PAVEMENT AND/OR OVERLAY PAY ITEMS.

MEDIAN AND ISLAND NOSES

WHEN CONSTRUCTING MEDIAN AND ISLAND NOSES THE FOLLOWING CRITERIA SHOULD BE FOLLOWED:

- BARRIER CURB SHALL BE USED TO CONSTRUCT NOSES WHEN THE MEDIAN OR ISLAND SURROUNDS A MAST ARM OR OTHER NON-BREAKAWAY FOUNDATION
- RAMPED NOSES SHALL BE USED ON MEDIANS OR ISLANDS WITH BREAKAWAY POSTS.

SIGN POST HOLES

VERTICAL HOLES SHALL BE CONSTRUCTED IN THE ISLAND PAVEMENT AND/OR CONCRETE MEDIAN OF THE TYPE SPECIFIED OR CONCRETE MEDIAN SURFACE 4 INCHES (100 MM). THE HOLES SHALL BE 24 INCHES (600 MM) IN DIAMETER OR 24 INCHES (600 MM) SQUARE AND THEY SHALL BE FREE OF ANY OBSTRUCTION, EXCEPT EARTH, FOR A DEPTH OF 5 FEET (1.5 M) AT THE LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. ANY HOLES NOT USED FOR THE PLACEMENT OF SIGNS SHALL BE FILLED AND COMPACTED FLUSH WITH THE TOP OF THE ISLAND PAVEMENT, CONCRETE MEDIAN OF THE TYPES SPECIFIED, OR CONCRETE MEDIAN SURFACE 4 INCHES (100 MM). THE TOP 3 INCHES (75 MM) OF SAID COMPACTED FILL SHALL CONSIST OF A HOT-MIX ASPHALT MIXTURE. ALL HOLES IN WHICH THE SIGN POSTS ARE INSTALLED AT THE TIME OF THIS CONTRACT SHALL BE SIMILARLY FILLED.

THIS WORK, INCLUDING ANY REQUIRED PAVEMENT REMOVAL NECESSARY TO CONSTRUCT THE SIGN POST HOLES, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER SQUARE FOOT (SQUARE METER) FOR ISLAND PAVEMENT AND/OR CONCRETE MEDIAN OF THE TYPE SPECIFIED, OR CONCRETE MEDIAN SURFACE, 4 INCHES (100 MM).

| | | | | | | | | | | | |
|---|---------------------------------|------------|-----------|---|--------------------------------------|--------------------|-----------------------|---------------------------|--------------|-----------|--|
| FILE NAME : 68A77 1-155 Plans.dgn Default | USER NAME : kashbr | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | COMMITMENTS GENERAL NOTES | F.A.I. RTE.: | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| | PLOT SCALE : 100,0000 1" = 100' | DRAWN - | REVISED - | | | 155 | 901109,109-1,1101RS-2 | TAZEWELL | 45 | 2 | |
| | PLOT DATE : 12/11/2014 | CHECKED - | REVISED - | | | CONTRACT NO. 68A77 | | ILLINOIS FED. AID PROJECT | | | |
| | | DATE - | REVISED - | | | SCALE: | SHEET 1 | OF 2 SHEETS | STA. | TO STA. | |

GENERAL NOTES (CONTINUED)

HOT -MIX ASPHALT MIXTURE REQUIREMENTS

| MIXTURE USE(S): | POLYMERIZED SURFACE CSE I-155 (MAINLINE, RAMPS & CROSS-OVERS) | POLYMERIZED BINDER CSE I-155 (MAINLINE) | POLYMERIZED LEVELING BINDER (RAMPS & CROSS-OVERS) | POLYMERIZED SURFACE CSE IL 122 (MAINLINE & S.N. 090-0095) | HMA SHOULDER (MAINLINE, RAMPS & IL 122) |
|--|---|---|---|---|---|
| AC/PG: | SBS or SBR 76-28 | SBS or SBR 76-28 | SBS or SBR 76-28 | SBS or SBR 76-22 | PG 64-22 |
| DESIGN AIR VOIDS: | 4.0% @ N=70 | 4.0% @ N=70 | 4.0% @ N=50 | 4.0% @ N=50 | 4.0% @ N=50 |
| MIXTURE COMPOSITION: (MIXTURE GRADATION) | IL 9.5 | IL 9.5 | IL 4.75 | IL 9.5 | IL 9.5 |
| FRICTION AGGREGATE: | MIXTURE E | N.A. | N.A. | MIXTURE E | MIXTURE C |
| QUALITY MANAGEMENT PROGRAM: | PFP (does not include cross-overs) | PFP | QC/QA | QC/QA | QC/QA |

**** IF THE RAP OPTION IS SELECTED, THE ASPHALT CEMENT GRADE MAY NEED TO BE ADJUSTED; THIS WILL BE DETERMINED BY THE ENGINEER.**

ENGINEER'S FIELD OFFICE

ADD THE FOLLOWING SENTENCE TO THE END OF PARAGRAPH 670.02 (i) AND 670.04 (e):

ALL OF THE TELEPHONE LINES PROVIDED SHALL HAVE UNPUBLISHED NUMBERS.

JOB SPECIFIC NOTES

CROSSING EXISTING STRUCTURES WITH A MATERIAL TRANSFER DEVICE

AN EMPTIED MTD WILL BE ALLOWED OVER THE FOLLOWING STRUCTURES:

- S.N. 090-0092 S.N. 090-0100
- S.N. 090-0093 S.N. 090-0101
- S.N. 090-0094 S.N. 090-0139
- S.N. 090-0095

RAMP CLOSURES

NO RAMP CLOSURES WILL BE ALLOWED.

PAVEMENT MARKINGS

RESIDENT ENGINEER SHALL CONTACT BUREAU OF OPERATIONS PRIOR TO PLACEMENT OF PAVEMENT MARKINGS ON INTERCHANGES.

VEGETATION REMOVAL ON HMA SHOULDER

CONTRACTOR SHALL REMOVE ALL EXISTING VEGETATION ON HMA SHOULDER PRIOR TO STARTING PAVING OPERATIONS. ALL WORK WILL BE PAID FOR IN ACCORDANCE WITH THE ARTICLE 109.04 OF STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.

| | | | | | | | | | | | |
|----------------------------------|-----------------------------|------------|-----------|---|----------------------|--------------------|---------------------|----------|--------------|-----------|---------|
| FILE NAME: SBA77 1-155 Plans.dgn | USER NAME: kashtr | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | GENERAL NOTES | F.A.I. RTE: | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| Default | PLOT SCALE: 100,0000' / in. | DRAWN - | REVISED - | | | 155 | 90109,109-1,110RS-2 | TAZEWELL | 45 | 3 | |
| | PLOT DATE: 12/11/2014 | CHECKED - | REVISED - | | | CONTRACT NO. 68A77 | | | | | |
| | | DATE - | REVISED - | | | SCALE: | SHEET 2 | OF 2 | SHEETS | STA. | TO STA. |

SUMMARY OF QUANTITIES

| CODE NO. | PAY ITEM | UNIT | TOTAL QUANTITY | 90/10 FED / STATE | |
|----------|--|-------|----------------|--------------------------------|--------------------------------|
| | | | | 0004 | 0014 |
| | | | | TAZEWELL CO. RURAL 90/10 | TAZEWELL CO. RURAL 90/10 |
| 20100110 | TREE REMOVAL (6 TO 15 UNITS DIAMETER) | UNIT | 18 | 18 | |
| 21400100 | GRADING AND SHAPING DITCHES | FOOT | 1,065 | 1,065 | |
| 25000210 | SEEDING, CLASS 2A | ACRE | 0.5 | 0.5 | |
| 25000400 | NITROGEN FERTILIZER NUTRIENT | POUND | 44 | 44 | |
| 25000500 | PHOSPHORUS FERTILIZER NUTRIENT | POUND | 44 | 44 | |
| 25000600 | POTASSIUM FERTILIZER NUTRIENT | POUND | 44 | 44 | |
| 25100115 | MULCH, METHOD 2 | ACRE | 0.5 | 0.5 | |
| 28000250 | TEMPORARY EROSION CONTROL SEEDING | POUND | 49 | 49 | |
| 28000305 | TEMPORARY DITCH CHECKS | FOOT | 210 | 210 | |
| 28100807 | STONE DUMPED RIPRAP, CLASS A4 | TON | 50 | 50 | |
| 28200200 | FILTER FABRIC | SQ YD | 75 | 75 | |
| 40600285 | POLYMERIZED BITUMINOUS MATERIALS (PRIME COAT) | POUND | 325,529 | 324,864 | 665 |
| 40600827 | POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 | TON | 1,593 | 1,593 | |
| 40600982 | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT | SQ YD | 2,490 | 2,490 | |
| 40600990 | TEMPORARY RAMP | SQ YD | 872 | 872 | |

SUMMARY OF QUANTITIES

| CODE NO. | PAY ITEM | UNIT | TOTAL QUANTITY | 90/10 FED / STATE | |
|----------|--|-------|----------------|-----------------------------|-----------------------------|
| | | | | 0004 | 0014 |
| | | | | TAZEWELL CO. RURAL 90/10 | TAZEWELL CO. RURAL 90/10 |
| 40603560 | POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N50 | TON | 633 | 509 | 124 |
| 40603565 | POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70 | TON | 24,302 | 24,302 | |
| 44000155 | HOT-MIX ASPHALT SURFACE REMOVAL, 1 1/2" | SQ YD | 306,782 | 306,782 | |
| 44000164 | HOT-MIX ASPHALT SURFACE REMOVAL, 3 3/4" | SQ YD | 583 | 583 | |
| 44000500 | COMBINATION CURB AND GUTTER REMOVAL | FOOT | 4,442 | 4,442 | |
| 44201007 | CLASS B PATCHES, TYPE II, 13 INCH | SQ YD | 118 | 118 | |
| 44201025 | CLASS B PATCHES, TYPE IV, 14 INCH | SQ YD | 32 | 32 | |
| 44201043 | CLASS B PATCHES, TYPE II, 16 INCH | SQ YD | 16 | 16 | |
| 44201047 | CLASS B PATCHES, TYPE III, 16 INCH | SQ YD | 32 | 32 | |
| 44201299 | DOWEL BARS 1 1/2" | EACH | 386 | 386 | |
| 44213200 | SAW CUTS | FOOT | 1,020 | 1,020 | |
| 48102100 | AGGREGATE WEDGE SHOULDER, TYPE B | TON | 6,324 | 6,324 | |
| 48203100 | HOT-MIX ASPHALT SHOULDERS | TON | 23,753 | 23,753 | |
| X5017305 | PROTECTIVE SHIELD (PERMANENT) | SQ YD | 765 | | 765 |
| 60608600 | COMBINATION CONCRETE CURB AND GUTTER, TYPE M-6.06 | FOOT | 4,523 | 4,523 | |

15

| SUMMARY OF QUANTITIES | | | | | | |
|-----------------------|--|--------|----------------|--|--|--|
| CODE NO. | PAY ITEM | UNIT | TOTAL QUANTITY | 90/10 FED/STATE | | |
| | | | | 0004 TAZEWELL CO. RURAL 90/10 | 0014 TAZEWELL CO. RURAL 90/10 | |
| 60618300 | CONCRETE MEDIAN SURFACE, 4 INCH | SQ FT | 26,527 | 26,527 | | |
| * 63000001 | STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS | FOOT | 4,670 | 4,670 | | |
| * 63100045 | TRAFFIC BARRIER TERMINAL, TYPE 2 | EACH | 7 | 7 | | |
| * 63100070 | TRAFFIC BARRIER TERMINAL, TYPE 5 | EACH | 3 | 3 | | |
| * 63100085 | TRAFFIC BARRIER TERMINAL, TYPE 6 | EACH | 7 | 7 | | |
| * 63100089 | TRAFFIC BARRIER TERMINAL, TYPE 6B | EACH | 1 | 1 | | |
| * 63100167 | TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT | EACH | 12 | 12 | | |
| 63200310 | GUARDRAIL REMOVAL | FOOT | 5,815 | 5,815 | | |
| 64200116 | SHOULDER RUMBLE STRIPS, 16 INCH | FOOT | 191,263 | 191,263 | | |
| 64300450 | IMPACT ATTENUATORS (NON-REDIRECTIVE), TEST LEVEL 3 | EACH | 32 | 32 | | |
| 67000400 | ENGINEER'S FIELD OFFICE, TYPE A | CAL MO | 14 | 14 | | |
| 67100100 | MOBILIZATION | L SUM | 1 | 1 | | |
| 70100420 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701411 | EACH | 20 | 20 | | |
| 70100700 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701406 | L SUM | 1 | 1 | | |
| 70100800 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701401 | L SUM | 1 | 1 | | |
| 70100825 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701456 | L SUM | 1 | 1 | | |

* SPECIALTY ITEM

| | | | | | | | | | | | | | |
|--------------------------------------|-----------------------|------------------|-----------------|---|------------------------------|---------------------|------|---------|--------------------|---------------------|----------|-----------------|--------------|
| FILE NAME : 68A77 1-155 Plans.dgn | USER NAME : kathbr | DESIGNED : -- | REVISED : -- | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | SUMMARY OF QUANTITIES | | | | F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | | DRAWN : -- | REVISED : -- | | SCALE: | SHEET 3 OF 6 SHEETS | STA. | TO STA. | 155 | 90109.109-1.110RS-2 | TAZEWELL | 45 | 6 |
| | | CHECKED : -- | REVISED : -- | | | | | | CONTRACT NO. 68A77 | | | | |
| | | DATE : -- | REVISED : -- | | ILLINOIS FED. AID PROJECT | | | | | | | | |

SUMMARY OF QUANTITIES

| CODE NO. | PAY ITEM | UNIT | TOTAL QUANTITY | 90/10 FED / STATE | |
|------------|---|--------|----------------|--------------------------------|--------------------------------|
| | | | | 0004 | 0014 |
| | | | | TAZEWELL CO. RURAL 90/10 | TAZEWELL CO. RURAL 90/10 |
| 70103815 | TRAFFIC CONTROL SURVEILLANCE | CAL DA | 10 | 10 | |
| 70107008 | PAVEMENT MARKING BLACKOUT TAPE, 8" | FOOT | 1,000 | 1,000 | |
| 70300100 | SHORT-TERM PAVEMENT MARKING | FOOT | 56,390 | 56,390 | |
| 70300210 | TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS | SQ FT | 62 | 62 | |
| 70300220 | TEMPORARY PAVEMENT MARKING - LINE 4" | FOOT | 215,424 | 214,278 | 1,146 |
| 70300240 | TEMPORARY PAVEMENT MARKING - LINE 6" | FOOT | 24,289 | 24,289 | |
| 70300250 | TEMPORARY PAVEMENT MARKING - LINE 8" | FOOT | 8,965 | 8,965 | |
| 70300260 | TEMPORARY PAVEMENT MARKING - LINE 12" | FOOT | 846 | 846 | |
| 70300275 | TEMPORARY PAVEMENT MARKING - LINE 18" | FOOT | 48 | 48 | |
| 70300280 | TEMPORARY PAVEMENT MARKING - LINE 24" | FOOT | 144 | 144 | |
| 70301000 | WORK ZONE PAVEMENT MARKING REMOVAL | SQ FT | 6,266 | 6,266 | |
| * 78004230 | PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 6" | FOOT | 24,289 | 24,289 | |
| * 78009000 | MODIFIED URETHANE PAVEMENT MARKING - LETTERS AND SYMBOLS | SQ FT | 62 | 62 | |
| * 78009004 | MODIFIED URETHANE PAVEMENT MARKING - LINE 4" | FOOT | 215,424 | 214,278 | 1,146 |
| * 78009008 | MODIFIED URETHANE PAVEMENT MARKING - LINE 8" | FOOT | 8,965 | 8,965 | |

* SPECIALTY ITEM

| | | | | | | | | | | |
|--------------------------------------|--------------------|------------|-----------|---|------------------------------|-----------------------|--------------------------------|---|--------------------|----------------|
| FILE NAME = 68A77 1-155 Plans.dgn | USER NAME = kashbr | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | SUMMARY OF QUANTITIES | F.A.I. RTE. 155 | SECTION 90109,109-1,110RS-2 | COUNTY TAZEWELL | TOTAL SHEETS 45 | SHEET NO. 7 |
| PLOT SCALE = 100.0000' / 1" | CHECKED - | DATE - | REVISED - | SCALE: | SHEET 4 OF 6 SHEETS | STA. | TO STA. | ILLINOIS FED. AID PROJECT CONTRACT NO. 68A77 | | |

SUMMARY OF QUANTITIES

| CODE NO. | PAY ITEM | UNIT | TOTAL QUANTITY | 90/10 FED / STATE | |
|---------------------|---|-----------------|----------------|--|--|
| | | | | 0004 TAZEWELL CO. RURAL 90/10 | 0014 TAZEWELL CO. RURAL 90/10 |
| * 78009012 | MODIFIED URETHANE PAVEMENT MARKING - LINE 12" | FOOT | 846 | 846 | |
| * 78009018 | MODIFIED URETHANE PAVEMENT MARKING - LINE 18" | FOOT | 48 | 48 | |
| * 78009024 | MODIFIED URETHANE PAVEMENT MARKING - LINE 24" | FOOT | 144 | 144 | |
| * 78100100 | RAISED REFLECTIVE PAVEMENT MARKER | EACH | 1,343 | 1,343 | |
| * 78200300 | PRISMATIC CURB REFLECTOR | EACH | 148 | 138 | 10 |
| * 78201000 | TERMINAL MARKER - DIRECT APPLIED | EACH | 12 | 12 | |
| 78300200 | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL | EACH | 1,345 | 1,345 | |
| X0322584 | REVTMENT MAT REMOVAL | SQ YD | 75 | 75 | |
| X0325893 | CLEAN EXISTING END SECTION | EACH | 6 | 6 | |
| Δ X2503100 | MOWING | UNIT | 1,549 | 1,549 | |
| X4401198 | HOT-MIX ASPHALT SURFACE REMOVAL, VARIABLE DEPTH | SQ YD | 5,600 | 5,600 | |
| X4402805 | ISLAND REMOVAL | SQ FT | 205 | 205 | |
| X4403800 | MEDIAN SURFACE REMOVAL | SQ FT | 26,322 | 26,322 | |
| X6431120 | REMOVE IMPACT ATTENUATOR SAND MODULE | EACH | 32 | 32 | |
| X6432110 | REPLACE IMPACT ATTENUATORS (NON-REDIRECTIVE), TEST LEVEL 3 | EACH | 32 | 32 | |
| 15 X7015005 | CHANGEABLE MESSAGE SIGN | CAL DA | 28 | 14 | 14 |

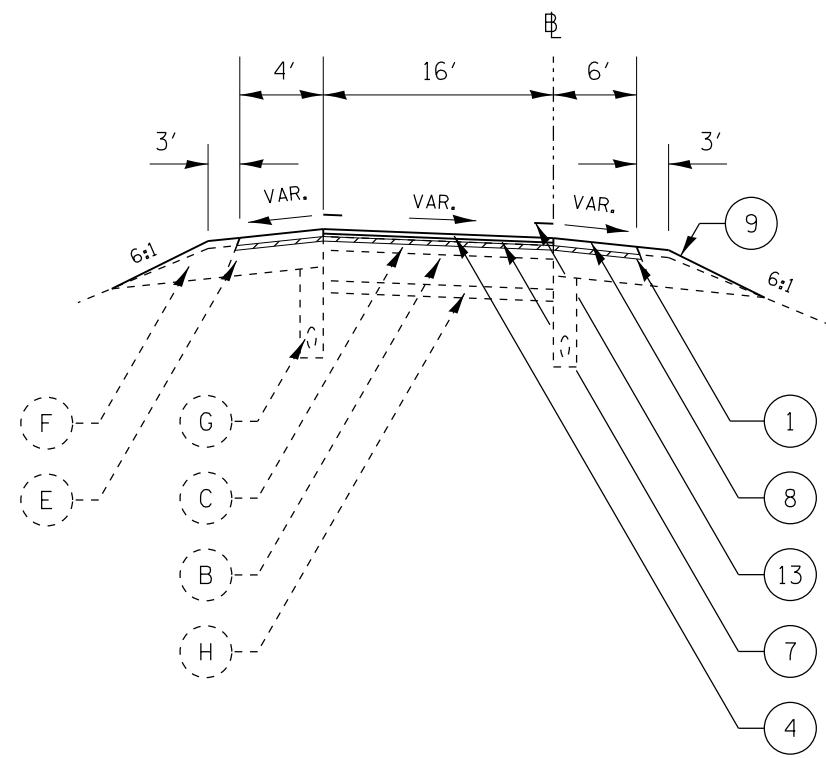
* SPECIALTY ITEM

Δ 100% STATE

SUMMARY OF QUANTITIES

| CODE NO. | PAY ITEM | UNIT | TOTAL QUANTITY | 90/10 FED / STATE | |
|----------|---|-------|----------------|-----------------------------|-----------------------------|
| | | | | 0004 | 0014 |
| | | | | TAZEWELL CO. RURAL 90/10 | TAZEWELL CO. RURAL 90/10 |
| X7830060 | GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS AND SYMBOLS | SQ FT | 62 | 62 | |
| X7830070 | GROOVING FOR RECESSED PAVEMENT MARKING 5" | FOOT | 214,278 | 214,278 | |
| X7830076 | GROOVING FOR RECESSED PAVEMENT MARKING 9" | FOOT | 8,965 | 8,965 | |
| X7830078 | GROOVING FOR RECESSED PAVEMENT MARKING 13" | FOOT | 846 | 846 | |
| X7830084 | GROOVING FOR RECESSED PAVEMENT MARKING 19" | FOOT | 48 | 48 | |
| X7830090 | GROOVING FOR RECESSED PAVEMENT MARKING 25" | FOOT | 144 | 144 | |
| XZ013798 | CONSTRUCTION STATION LAYOUT | L SUM | 1 | 1 | |
| Z0001002 | GUARDRAIL AGGREGATE EROSION CONTROL | TON | 678 | 678 | |
| Z0004556 | HOT-MIX ASPHALT SURFACE REMOVAL (DECK) | SQ YD | 1,480 | | 1,480 |
| Z0020805 | EROSION CONTROL CURB REMOVAL | FOOT | 1,999 | 1,999 | |
| Z0034105 | MATERIAL TRANSFER DEVICE | TON | 58,604 | 58,604 | |
| X2600002 | RECOVERABLE DELINEATORS | EACH | 438 | 438 | |
| 40603208 | POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-9.5, N70 | TON | 32,075 | 32,075 | |

12



TYPICAL SECTION 2
 I-155 AND IL 122 N. JCT. INTERCHANGE
 RAMPS A, B, C, & D

NOTES:

- ** EXIST. HMA OVERLAY 5 1/2"
- **** EXIST. HMA SHOULDER 15"

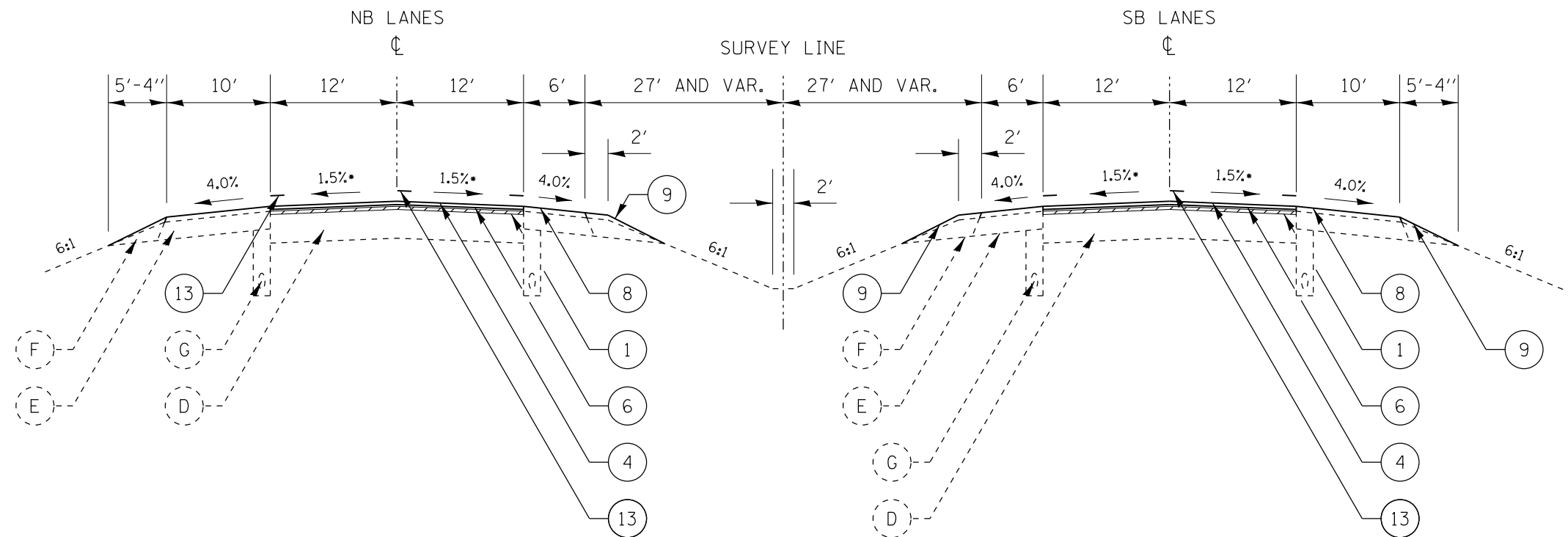
LEGEND - EXISTING:

- (A) CONTINUOUSLY REINFORCED PCC PAVEMENT, 8"
- (B) PCC PAVEMENT, 10"
- (C) HMA OVERLAY (SEE NOTE **)
- (D) FULL-DEPTH HMA PVMT, 15" - MAINLINE (FOR RAMPS SEE NOTE ***)
- (E) HMA SHOULDER (SEE NOTE ****)
- (F) AGGREGATE SHOULDERS
- (G) PIPE UNDERDRAIN, 4"
- (H) STABILIZED SUB-BASE, 4"
- (I) BRIDGE WEARING SURFACE
- (J) PCC MEDIAN SURFACE (4 IN) AND CCC&G, TY M-6.06

LEGEND - PROPOSED:

- (1) HMA SURF. REMOVAL, 1 1/2"
- (2) HMA SURF. REMOVAL (DECK), 1 1/2"
- (3) MEDIAN SURFACE REMOVAL/ CONC. CC&G REMOVAL
- (4) POLY. HMA SURF. CSE, MIX "E", N70, 1 1/2" (I-155)
- (5) POLY. HMA SURF.CSE, MIX "E", N50, 1 1/2" (IL 122)
- (6) POLY. HMA BINDER CSE, IL-9.5, N70, 2 1/4"
- (7) POLY. LEVEL. BIND. (MM), IL-4.75, N50, 3/4" (RAMPS)
- (8) HMA SHOULDER
- (9) AGGREGATE WEDGE SHOULDER, TYPE B
- (10) PROTECTIVE SHIELD
- (11) CONCRETE MEDIAN SURFACE, 4 INCH
- (12) CCC&G, TY M-6.06
- (13) PVMT MARKING:
 EDGELINE - GROOVED IN
 ☐ - PREFORMED PLASTIC, TY B - INLAID

| | | | | | | | | | | | | |
|-----------------------------------|---------------------|------------------------------|-----------|---|-------------------------------|-----------------------|-------------------------|-------------|---------------------------|--------|--------------|-----------|
| FILE NAME = 68A77 I-155 Plans.dgn | USER NAME = keithbr | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | I-155 TYPICAL SECTIONS | | | F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | | DRAWN - | REVISED - | | 155 | 90(109,109-1,110)R5-2 | TAZEWELL | 45 | 11 | | | |
| | | PLOT SCALE = 100.0000' / 1". | CHECKED - | | REVISED - | CONTRACT NO. 68A77 | | | ILLINOIS FED. AID PROJECT | | | |
| | | PLOT DATE = 12/11/2014 | DATE - | | REVISED - | NOT TO SCALE | SHEET NO. 2 OF 6 SHEETS | STA. | TO STA. | | | |



TYPICAL SECTION 3
I-155 MAINLINE

NB & SB STA. 785+38.00 TO STA. 744+80.17
 STRUCTURE OMISSION: STA. 744+80.17 TO STA. 746+58.00
 STA. 746+58.00 TO STA. 1102+30.32 BK (STA. 546+16.17 AH)

LEGEND - EXISTING:

- (A) CONTINUOUSLY REINFORCED PCC PAVEMENT, 8"
- (B) PCC PAVEMENT, 10"
- (C) HMA OVERLAY (SEE NOTE **)
- (D) FULL-DEPTH HMA PVMT, 15" - MAINLINE (FOR RAMPS SEE NOTE ***)
- (E) HMA SHOULDER (SEE NOTE ****)
- (F) AGGREGATE SHOULDERS
- (G) PIPE UNDERDRAIN, 4"
- (H) STABILIZED SUB-BASE, 4"
- (I) BRIDGE WEARING SURFACE
- (J) PCC MEDIAN SURFACE (4 IN) AND CCC&G, TY M-6.06

NOTES:

**** EXIST. HMA SHOULDER 8"

LEGEND - PROPOSED:

- (1) HMA SURF. REMOVAL, 1 1/2"
- (2) HMA SURF. REMOVAL (DECK), 1 1/2"
- (3) MEDIAN SURFACE REMOVAL/ CONC. CC&G REMOVAL
- (4) POLY. HMA SURF. CSE, MIX "E", N70, 1 1/2" (I-155)
- (5) POLY. HMA SURF. CSE, MIX "E", N50, 1 1/2" (IL 122)
- (6) POLY. HMA BINDER CSE, IL-9.5, N70, 2 1/4"
- (7) POLY. LEVEL. BIND. (MM), IL-4.75, N50, 3/4" (RAMPS)
- (8) HMA SHOULDER
- (9) AGGREGATE WEDGE SHOULDER, TYPE B
- (10) PROTECTIVE SHIELD
- (11) CONCRETE MEDIAN SURFACE, 4 INCH
- (12) CCC&G, TY M-6.06
- (13) PVMT MARKING:
 EDGELINE - GROOVED IN
 ☉ - PREFORMED PLASTIC, TY B - INLAID

FILE NAME = 68A77 I-155 Plans.dgn

USER NAME = keithbr

DESIGNED -

REVISED -

DRAWN -

REVISED -

PLOT SCALE = 100.0000' / 1"

CHECKED -

REVISED -

PLOT DATE = 12/11/2014

DATE -

REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-155 TYPICAL SECTIONS

NOT TO SCALE

SHEET NO. 3 OF 6 SHEETS

STA.

TO STA.

F.A.I. RTE.

SECTION

COUNTY

TOTAL SHEETS

SHEET NO.

155

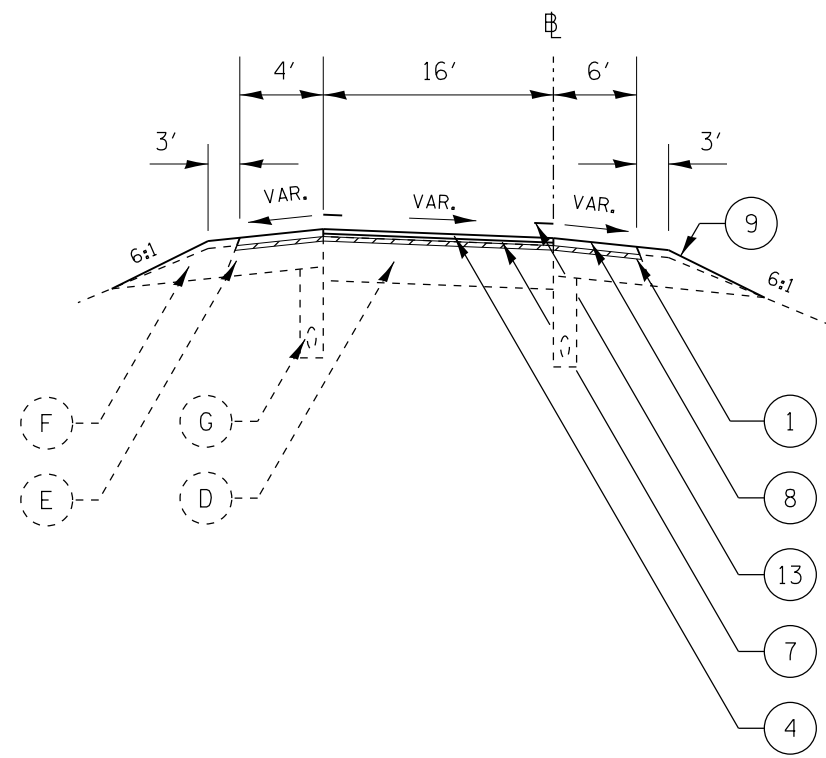
90(109,109-1,110)R5-2

TAZEWELL

45 12

CONTRACT NO. 68A77

ILLINOIS FED. AID PROJECT



TYPICAL SECTION 4
 I-155 AND IL 122 S. JCT. INTERCHANGE
 RAMPS A, B, C, & D

NOTES:

- *** EXIST. HMA PAVEMENT 13"
- **** EXIST. HMA SHOULDER 8"

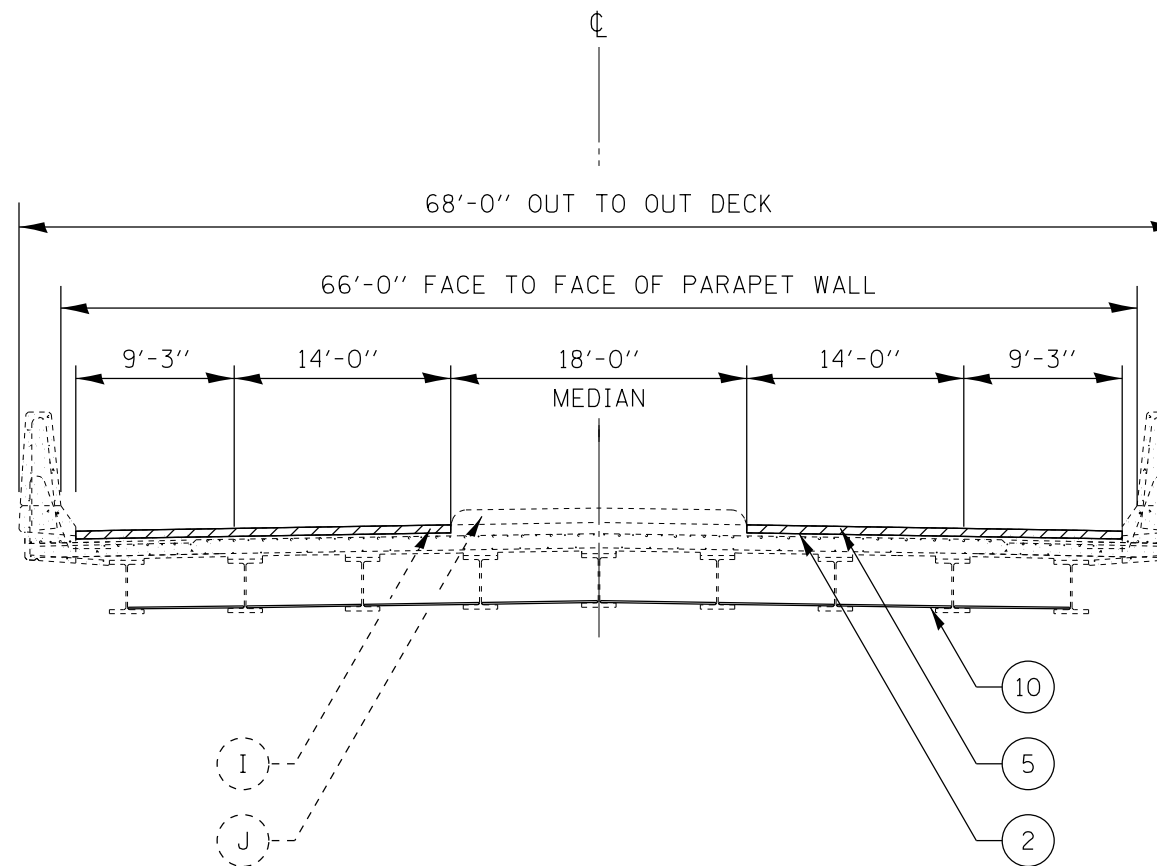
LEGEND - EXISTING:

- (A) CONTINUOUSLY REINFORCED PCC PAVEMENT, 8"
- (B) PCC PAVEMENT, 10"
- (C) HMA OVERLAY (SEE NOTE **)
- (D) FULL-DEPTH HMA PVMT, 15" - MAINLINE (FOR RAMPS SEE NOTE ***)
- (E) HMA SHOULDER (SEE NOTE ****)
- (F) AGGREGATE SHOULDERS
- (G) PIPE UNDERDRAIN, 4"
- (H) STABILIZED SUB-BASE, 4"
- (I) BRIDGE WEARING SURFACE
- (J) PCC MEDIAN SURFACE (4 IN) AND CCC&G, TY M-6.06

LEGEND - PROPOSED:

- (1) HMA SURF. REMOVAL, 1 1/2"
 - (2) HMA SURF. REMOVAL (DECK), 1 1/2"
 - (3) MEDIAN SURFACE REMOVAL/ CONC. CC&G REMOVAL
 - (4) POLY. HMA SURF. CSE, MIX "E", N70, 1 1/2" (I-155)
 - (5) POLY. HMA SURF. CSE, MIX "E", N50, 1 1/2" (IL 122)
 - (6) POLY. HMA BINDER CSE, IL-9.5, N70, 2 1/4"
 - (7) POLY. LEVEL. BIND. (MM), IL-4.75, N50, 3/4" (RAMPS)
 - (8) HMA SHOULDER
 - (9) AGGREGATE WEDGE SHOULDER, TYPE B
 - (10) PROTECTIVE SHIELD
 - (11) CONCRETE MEDIAN SURFACE, 4 INCH
 - (12) CCC&G, TY M-6.06
 - (13) PVMT MARKING:
- EDGE LINE - GROOVED IN
 ☐ - PREFORMED PLASTIC, TY B - INLAID

| | | | | | | | | | | | | | |
|-----------------------------------|---------------------|------------------------------|-----------|---|-------------------------------|-------------------------|------|-------------|---------------------------|-----------------------|--------------|-----------|----|
| FILE NAME = 68A77 I-155 Plans.dgn | USER NAME = keithbr | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | I-155 TYPICAL SECTIONS | | | F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| | | DRAWN - | REVISED - | | NOT TO SCALE | SHEET NO. 4 OF 6 SHEETS | STA. | TO STA. | 155 | 90(109,109-1,110)RS-2 | TAZEWELL | 45 | 13 |
| | | PLOT SCALE = 100.0000' / 1". | CHECKED - | | REVISED - | | | | CONTRACT NO. 68A77 | | | | |
| | | PLOT DATE = 12/11/2014 | DATE - | | REVISED - | | | | ILLINOIS FED. AID PROJECT | | | | |



TYPICAL SECTION 5
 S.N. 090-0095 IL 122 N.JCT. OVER I-155
 I-155 STA. 720+18.05
 IL 122 STA. 28+18.38 TO STA. 31+04.76

LEGEND - EXISTING:

- (A) CONTINUOUSLY REINFORCED PCC PAVEMENT, 8"
- (B) PCC PAVEMENT, 10"
- (C) HMA OVERLAY (SEE NOTE **)
- (D) FULL-DEPTH HMA PVMT, 15" - MAINLINE (FOR RAMPS SEE NOTE ***)
- (E) HMA SHOULDER (SEE NOTE ****)
- (F) AGGREGATE SHOULDERS
- (G) PIPE UNDERDRAIN, 4"
- (H) STABILIZED SUB-BASE, 4"
- (I) BRIDGE WEARING SURFACE
- (J) PCC MEDIAN SURFACE (4 IN) AND CCC&G, TY M-6.06

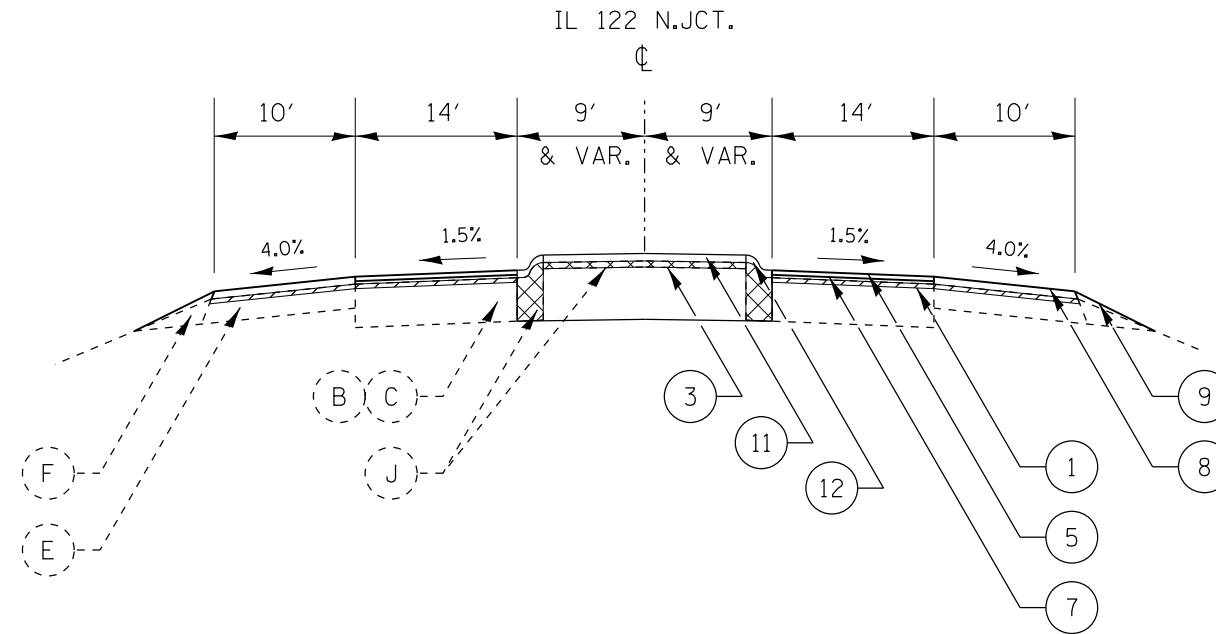
LEGEND - PROPOSED:

- (1) HMA SURF. REMOVAL, 1 1/2"
- (2) HMA SURF. REMOVAL (DECK), 1 1/2"
- (3) MEDIAN SURFACE REMOVAL/ CONC. CC&G REMOVAL
- (4) POLY. HMA SURF. CSE, MIX "E", N70, 1 1/2" (I-155)
- (5) POLY. HMA SURF. CSE, MIX "E", N50, 1 1/2" (IL 122)
- (6) POLY. HMA BINDER CSE, IL-9.5, N70, 2 1/4"
- (7) POLY. LEVEL. BIND. (MM), IL-4.75, N50, 3/4" (RAMPS)
- (8) HMA SHOULDER
- (9) AGGREGATE WEDGE SHOULDER, TYPE B
- (10) PROTECTIVE SHIELD
- (11) CONCRETE MEDIAN SURFACE, 4 INCH
- (12) CCC&G, TY M-6.06
- (13) PVMT MARKING:

EDGE LINE - GROOVED IN

☐ - PREFORMED PLASTIC, TY B - INLAID

| | | | | | | | | | | |
|-----------------------------------|-------------------------------|------------|-----------|---|---------------------------------------|-------------------------|-------------------------------|-----------------|--|--------------|
| FILE NAME = 68A77 1-155 Plans.dgn | USER NAME = keithbr | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | S.N. 090-0095 TYPICAL SECTIONS | F.A.I. RTE. 155 | SECTION 90(109,109-1,110)R5-2 | COUNTY TAZEWELL | TOTAL SHEETS 45 | SHEET NO. 14 |
| | PLOT SCALE = 100.0000' / 1" = | CHECKED - | REVISED - | | | | | | | |
| | PLOT DATE = 12/11/2014 | DATE - | REVISED - | | NOT TO SCALE | SHEET NO. 5 OF 6 SHEETS | STA. | TO STA. | ILLINOIS FED. AID PROJECT CONTRACT NO. 68A77 | |



TYPICAL SECTION 6
IL 122 N. JCT.

STA. 20+80.31 TO STA. 28+18.38
STA. 31+04.76 TO STA. 38+39.73

MEDIAN REMOVAL AND RECONSTRUCTION:
STA. 16+52.95 TO STA. 43+09.13

LEGEND - EXISTING:

- (A) CONTINUOUSLY REINFORCED PCC PAVEMENT, 8"
- (B) PCC PAVEMENT, 10"
- (C) HMA OVERLAY (SEE NOTE **)
- (D) FULL-DEPTH HMA PVMT, 15" - MAINLINE (FOR RAMPS SEE NOTE ***)
- (E) HMA SHOULDER (SEE NOTE ****)
- (F) AGGREGATE SHOULDERS
- (G) PIPE UNDERDRAIN, 4"
- (H) STABILIZED SUB-BASE, 4"
- (I) BRIDGE WEARING SURFACE
- (J) PCC MEDIAN SURFACE (4 IN) AND CCC&G, TY M-6.06

NOTES:

- ** EXIST. PCC PAVEMENT (8") WITH HMA OVERLAY 1 1/2"
- **** EXIST. HMA SHOULDER 8"

LEGEND - PROPOSED:

- (1) HMA SURF. REMOVAL, 1 1/2"
- (2) HMA SURF. REMOVAL (DECK), 1 1/2"
- (3) MEDIAN SURFACE REMOVAL/ CONC. CC&G REMOVAL
- (4) POLY. HMA SURF. CSE, MIX "E", N70, 1 1/2" (I-155)
- (5) POLY. HMA SURF. CSE, MIX "E", N50, 1 1/2" (IL 122)
- (6) POLY. HMA BINDER CSE, IL-9.5, N70, 2 1/4"
- (7) POLY. LEVEL. BIND. (MM), IL-4.75, N50, 3/4" (RAMPS)
- (8) HMA SHOULDER
- (9) AGGREGATE WEDGE SHOULDER, TYPE B
- (10) PROTECTIVE SHIELD
- (11) CONCRETE MEDIAN SURFACE, 4 INCH
- (12) CCC&G, TY M-6.06
- (13) PVMT MARKING:
EDGE LINE - GROOVED IN
☒ - PREFORMED PLASTIC, TY B - INLAID

| | | | | | | | | | | | | |
|--------------------------------------|-------------------------------|------------|--------------|---|---------------------------------------|------|--------------------|---------------------------|-----------------------|----------|--------------|-----------|
| FILE NAME = 68A77 1-155 Plans.dgn | USER NAME = keithbr | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | IL 122 N. JCT TYPICAL SECTIONS | | | F.A.I. RTÉ. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | PLOT SCALE = 100.0000' / 1in. | CHECKED - | REVISED - | | | | | 155 | 90(109,109-1,110)RS-2 | TAZEWELL | 45 | 15 |
| PLOT DATE = 12/11/2014 | DATE - | REVISED - | NOT TO SCALE | | SHEET NO. 6 OF 6 SHEETS | STA. | TO STA. | ILLINOIS FED. AID PROJECT | | | | |
| | | | | | | | CONTRACT NO. 68A77 | | | | | |

TABULATION OF RESURFACING QUANTITIES

| LOCATION | NOTES | HMA SURFACE REMOVAL - BUTT JOINT | TEMP. RAMP | HMA SURFACE REMOVAL (DECK) | HMA SURFACE REMOVAL, 1 1/2 IN | HMA SURFACE REMOVAL, 3 3/4 IN | HMA SURFACE REMOVAL, VAR. DEPTH | POLYMERIZED BITUMINOUS MATERIALS (PRIME COAT) | | | POLY HMA SURFACE CSE, MIX "E", N70 | POLY HMA SURFACE CSE, MIX "E", N50 | POLY HMA BINDER CSE, IL-9.5, N70 | POLY LEVELING BINDER (MM) IL-4.75, N50 | HMA SHOULDERS | SHOULDER RUMBLE STRIPS, 16 INCH | AGG. WEDGE SHOULDER, TY B |
|---------------------------------------|---------------------------------|----------------------------------|------------|----------------------------|-------------------------------|-------------------------------|---------------------------------|---|---------------|----------------|------------------------------------|------------------------------------|----------------------------------|--|---------------|---------------------------------|---------------------------|
| | | | | | | | | NON-MILLED | COLD-MILLED | FOG COAT | | | | | | | |
| | | | | | | | | 0.05 Lb/sq ft | 0.05 Lb/sq ft | 0.025 Lb/sq ft | | | | | | | |
| | | SQ YD | SQ YD | SQ YD | SQ YD | SQ YD | SQ YD | POUND | | | TON | TON | TON | TON | TON | FOOT | TON |
| I-155 SOUTHBOUND LANES | | | | | | | | | | | | | | | | | |
| STA. 616+36.21 TO STA. 616+66.21 | BUTT JOINT | 133.33 | 26.67 | | | | | | 60.00 | | 6.72 | | | 4.48 | 60.00 | 1.71 | |
| STA. 616+66.21 TO STA. 665+07.93 | | | | | 12,911.25 | | | 3,873.38 | 5,810.06 | 4,841.72 | 1,084.55 | 1,626.82 | 1,084.55 | 9,683.44 | 275.98 | | |
| STA. 665+07.93 TO STA. 665+19.37 BK | | | | | 30.51 | | | 9.15 | 13.73 | 11.44 | 2.56 | 3.84 | 2.56 | 22.88 | 0.65 | | |
| STA. 665+39.86 AH TO STA. 665+48.42 | | | | | 22.83 | | | 6.85 | 10.27 | 8.56 | 1.92 | 2.88 | 1.92 | 17.12 | 0.49 | | |
| STA. 665+48.42 TO STA. 665+58.42 | BUTT JOINT | 44.44 | 13.33 | | | | | | 20.00 | | 2.24 | | 1.49 | 20.00 | 0.57 | | |
| STA. 665+58.42 TO STA. 671+08.17 | S.N. 090-0092 | OMISSION | | | | | | | | | | | | | | | |
| STA. 671+08.17 TO STA. 671+18.17 | BUTT JOINT | 44.44 | 13.33 | | | | | | 20.00 | | 2.24 | | 1.49 | 20.00 | 0.57 | | |
| STA. 671+18.17 TO STA. 717+75.05 | | | | | 12,418.35 | | | 3,725.50 | 5,588.26 | 4,656.88 | 1,043.14 | 1,564.71 | 1,043.14 | 9,313.76 | 265.44 | | |
| STA. 717+75.05 TO STA. 722+61.05 | STD. 406001-D4 S.N. 090-0095 | | | | | 293.33 | 1,866.67 | 388.80 | 583.20 | 486.00 | 108.86 | 163.30 | 181.44 | 972.00 | 27.70 | | |
| STA. 722+61.05 TO STA. 744+70.17 | | | | | 5,890.99 | | | 1,767.30 | 2,650.94 | 2,209.12 | 494.84 | 742.26 | 494.84 | 4,418.24 | 125.92 | | |
| STA. 744+70.17 TO STA. 744+80.17 | BUTT JOINT | 44.44 | 13.33 | | | | | | 20.00 | | 2.24 | | 1.49 | 20.00 | 0.57 | | |
| STA. 744+80.17 TO STA. 746+58.00 | S.N. 090-0093 | OMISSION | | | | | | | | | | | | | | | |
| STA. 746+58.00 TO STA. 746+68.00 | BUTT JOINT | 44.44 | 13.33 | | | | | | 20.00 | | 2.24 | | 1.49 | 20.00 | 0.57 | | |
| STA. 746+68.00 TO STA. 1046+87.38 | | | | | 80,051.68 | | | 24,015.50 | 36,023.26 | 30,019.38 | 6,724.34 | 10,086.51 | 6,724.34 | 60,038.76 | 1711.10 | | |
| STA. 1046+87.38 TO STA. 1051+40.54 | STD. 406001-D4 S.N. 090-0139 | | | | | 147.41 | 1,866.64 | 362.53 | 543.79 | 453.16 | 101.51 | 152.26 | 169.18 | 906.32 | 25.83 | | |
| STA. 1051+40.54 TO STA. 1102+00.32 | | | | | 13,492.75 | | | 4,047.82 | 6,071.74 | 5,059.78 | 1,133.39 | 1,700.09 | 1,133.39 | 10,119.56 | 288.41 | | |
| STA. 1102+00.32 TO STA. 1102+30.32 BK | BUTT JOINT | 133.33 | 26.67 | | | | | | 60.00 | | 6.72 | | 4.48 | | 1.71 | | |
| SOUTHBOUND LANES SUB-TOTAL: | | 444 | 107 | | 124,818 | 441 | 3,733 | 38,197 | 57,495 | 47,746 | 10,718 | 16,043 | 10,850 | 95,632 | 2,727 | | |
| I-155 NORTHBOUND LANES | | | | | | | | | | | | | | | | | |
| STA. 616+26.95 TO STA. 616+56.95 | BUTT JOINT | 133.33 | 26.67 | | | | | | 60.00 | | 6.72 | | 4.48 | 60.00 | 1.71 | | |
| STA. 616+56.95 TO STA. 665+19.37 BK | | | | | 12,966.45 | | | 3,889.94 | 5,834.90 | 4,862.42 | 1,089.18 | 1,633.77 | 1,089.18 | 9,724.84 | 277.16 | | |
| STA. 665+39.86 AH TO STA. 665+60.45 | | | | | 54.91 | | | 16.47 | 24.71 | 20.59 | 4.61 | 6.92 | 4.61 | 41.18 | 1.17 | | |
| STA. 665+80.45 TO STA. 665+90.45 | BUTT JOINT | 44.44 | 13.33 | | | | | | 20.00 | | 2.24 | | 2.24 | 20.00 | 0.57 | | |
| STA. 665+90.45 TO STA. 671+40.20 | S.N. 090-0100 | OMISSION | | | | | | | | | | | | | | | |
| STA. 671+40.20 TO STA. 671+50.20 | BUTT JOINT | 44.44 | 13.33 | | | | | | 20.00 | | 2.24 | | 2.24 | 20.00 | 0.57 | | |
| STA. 671+50.20 TO STA. 744+50.17 | | | | | 19,466.59 | | | 5,839.98 | 8,759.96 | 7,299.97 | 1,635.19 | 2,452.79 | 1,635.19 | 14,599.94 | 416.10 | | |
| STA. 744+70.17 TO STA. 744+80.17 | BUTT JOINT | 44.44 | 13.33 | | | | | | 20.00 | | 2.24 | | 2.24 | 20.00 | 0.57 | | |
| STA. 744+80.17 TO STA. 746+58.00 | S.N. 090-0094 | OMISSION | | | | | | | | | | | | | | | |
| STA. 746+58.00 TO STA. 746+68.00 | BUTT JOINT | 44.44 | 13.33 | | | | | | 20.00 | | 2.24 | | 2.24 | 20.00 | 0.57 | | |
| STA. 746+68.00 TO STA. 782+76.10 | | | | | 9,621.60 | | | 2,886.48 | 4,329.72 | 3,608.10 | 808.21 | 1,212.32 | 808.21 | 7,216.20 | 205.66 | | |
| STA. 782+76.10 TO STA. 787+28.10 | STD. 406001-D4 S.N. 090-0101 | | | | | 142.22 | 1,866.67 | 361.60 | 542.40 | 452.00 | 101.25 | 151.87 | 168.75 | 904.00 | 25.76 | | |
| STA. 787+28.10 TO STA. 1102+00.32 | | | | | 83,925.92 | | | 25,177.78 | 37,766.66 | 31,472.22 | 7,049.78 | 10,574.67 | 7,049.78 | 62,944.44 | 1793.92 | | |
| STA. 1102+00.32 TO STA. 1102+30.32 BK | BUTT JOINT | 133.33 | 26.67 | | | | | | 60.00 | | 6.72 | | 6.72 | 60.00 | 1.71 | | |
| NORTHBOUND LANES SUB-TOTAL: | | 444 | 107 | | 126,035 | 142 | 1,867 | 38,172 | 57,458 | 47,715 | 10,711 | 16,032 | 10,776 | 95,631 | 2,725 | | |

TABULATION OF RESURFACING QUANTITIES (CONTINUED)

| LOCATION | NOTES | HMA SURFACE REMOVAL - BUTT JOINT | TEMP. RAMP | HMA SURFACE REMOVAL (DECK) | HMA SURFACE REMOVAL, 1 1/2 IN | HMA SURFACE REMOVAL, 3 3/4 IN | HMA SURFACE REMOVAL, VAR. DEPTH | POLYMERIZED BITUMINOUS MATERIALS (PRIME COAT) | | | POLY HMA SURFACE CSE, MIX "E", N70 | POLY HMA SURFACE CSE, MIX "E", N50 | POLY HMA BINDER CSE, IL-9.5, N70 | POLY LEVELING BINDER (MM) IL-4.75, N50 | HMA SHOULDERS | SHOULDER RUMBLE STRIPS, 16 INCH | AGG. WEDGE SHOULDER, TY B |
|--------------------------|------------------|----------------------------------|------------|----------------------------|-------------------------------|-------------------------------|---------------------------------|---|---------------|----------------|------------------------------------|------------------------------------|----------------------------------|--|---------------|---------------------------------|---------------------------|
| | | | | | | | | NON-MILLED | COLD-MILLED | FOG COAT | | | | | | | |
| | | | | | | | | 0.05 Lb/sq ft | 0.05 Lb/sq ft | 0.025 Lb/sq ft | | | | | | | |
| | | SQ YD | SQ YD | SQ YD | SQ YD | SQ YD | SQ YD | POUND | | | TON | TON | TON | TON | TON | FOOT | TON |
| IL 122 N. JCT. | | | | | | | | | | | | | | | | | |
| RAMP A | | | | | | | | | | | | | | | | | |
| STA. 0+23.20 | TO STA. 0+33.20 | BUTT JOINT | 166.11 | 77.50 | | | | | 74.75 | | | 13.02 | | | | 0.93 | 0.51 |
| STA. 0+33.20 | TO STA. 1+23.66 | | | | 703.58 | | | | 316.61 | 158.31 | 50.66 | | 25.33 | 12.66 | | | 4.58 |
| STA. 1+23.66 | TO STA. 15+43.30 | | | | 4,101.18 | | | | 1,845.53 | 922.77 | 212.00 | | 106.00 | 198.75 | | | 71.93 |
| STA. 15+43.30 | TO STA. 28+23.30 | ENTRANCE TERMINAL | | | 2,773.33 | | | | 1,248.00 | 624.00 | 161.28 | | 80.64 | 107.52 | | | 64.85 |
| RAMP A SUB-TOTAL: | | | 166 | 78 | 7,578 | | | | 3,485 | 1,705 | 437 | | 212 | 320 | | | 142 |
| RAMP B | | | | | | | | | | | | | | | | | |
| STA. 00+00 | TO STA. 6+60.00 | EXIT TERMINAL | | | 1,805.10 | | | | 812.30 | 406.15 | 114.67 | | 57.33 | 55.44 | | | 33.44 |
| STA. 6+60.00 | TO STA. 15+75.63 | | | | 2,645.15 | | | | 1,190.32 | 595.16 | 136.73 | | 68.37 | 128.19 | | | 46.39 |
| STA. 15+75.63 | TO STA. 16+97.13 | | | | 870.75 | | | | 391.84 | 195.92 | 61.80 | | 30.90 | 17.01 | | | 6.16 |
| STA. 16+97.13 | TO STA. 17+07.13 | BUTT JOINT | 175.83 | 82.36 | | | | | 79.13 | | 13.84 | | | 0.93 | | | 0.51 |
| RAMP B SUB-TOTAL: | | | 176 | 82 | 5,321 | | | | 2,474 | 1,197 | 327 | | 157 | 202 | | | 86 |
| RAMP C | | | | | | | | | | | | | | | | | |
| STA. 0+24.62 | TO STA. 0+34.62 | BUTT JOINT | 172.22 | 80.56 | | | | | 77.50 | | 13.53 | | | 0.93 | | | 0.51 |
| STA. 0+34.62 | TO STA. 1+27.51 | | | | 727.64 | | | | 327.44 | 163.72 | 52.45 | | 26.23 | 13.00 | | | 4.71 |
| STA. 1+27.51 | TO STA. 12+96.69 | | | | 3,377.63 | | | | 1,519.93 | 759.97 | 174.60 | | 87.30 | 163.69 | | | 59.24 |
| STA. 12+96.69 | TO STA. 25+76.69 | ENTRANCE TERMINAL | | | 2,773.33 | | | | 1,248.00 | 624.00 | 161.28 | | 80.64 | 107.52 | | | 64.85 |
| RAMP C SUB-TOTAL: | | | 172 | 81 | 6,879 | | | | 3,173 | 1,548 | 402 | | 194 | 285 | | | 129 |
| RAMP D | | | | | | | | | | | | | | | | | |
| STA. 00+00 | TO STA. 6+60.00 | EXIT TERMINAL | | | 1,805.10 | | | | 812.30 | 406.15 | 114.67 | | 57.33 | 55.44 | | | 33.44 |
| STA. 6+60.00 | TO STA. 17+45.34 | | | | 3,135.43 | | | | 1,410.94 | 705.47 | 162.08 | | 81.04 | 151.95 | | | 54.99 |
| STA. 17+45.34 | TO STA. 18+48.44 | | | | 756.07 | | | | 340.23 | 170.12 | 53.89 | | 26.94 | 14.43 | | | 5.22 |
| STA. 18+48.44 | TO STA. 18+58.44 | BUTT JOINT | 161.67 | 75.28 | | | | | 72.75 | | 12.65 | | | 0.93 | | | 0.51 |
| RAMP D SUB-TOTAL: | | | 162 | 75 | 5,697 | | | | 2,636 | 1,282 | 343 | | 165 | 223 | | | 94 |

FILE NAME = 68A77 1-155 Plans.dgn
 USER NAME = keithbr
 PLOT SCALE = 100.0000' / 1" = 10000
 PLOT DATE = 12/11/2014

| | |
|------------|-----------|
| DESIGNED - | REVISED - |
| DRAWN - | REVISED - |
| CHECKED - | REVISED - |
| DATE - | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SCHEDULES OF QUANTITIES

SCALE: SHEET 2 OF 11 SHEETS STA. TO STA.

| | | | | |
|---------------------------|-----------------------|----------|--------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 155 | 90(109,109-1,110)R5-2 | TAZEWELL | 45 | 17 |
| CONTRACT NO. 68A77 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

TABULATION OF RESURFACING QUANTITIES (CONTINUED)

| LOCATION | NOTES | HMA SURFACE REMOVAL - BUTT JOINT | TEMP. RAMP | HMA SURFACE REMOVAL (DECK) | HMA SURFACE REMOVAL, 1 1/2 IN | HMA SURFACE REMOVAL, 3 3/4 IN | HMA SURFACE REMOVAL, VAR. DEPTH | POLYMERIZED BITUMINOUS MATERIALS (PRIME COAT) | | | POLY HMA SURFACE CSE, MIX "E", N70 | POLY HMA SURFACE CSE, MIX "E", N50 | POLY HMA BINDER CSE, IL-9.5, N70 | POLY LEVELING BINDER (MM) IL-4.75, N50 | HMA SHOULDERS | SHOULDER RUMBLE STRIPS, 16 INCH | AGG. WEDGE SHOULDER, TY B |
|--------------------------|-------------------|----------------------------------|------------|----------------------------|-------------------------------|-------------------------------|---------------------------------|---|---------------|----------------|------------------------------------|------------------------------------|----------------------------------|--|---------------|---------------------------------|---------------------------|
| | | | | | | | | NON-MILLED | COLD-MILLED | FOG COAT | | | | | | | |
| | | | | | | | | 0.05 Lb/sq ft | 0.05 Lb/sq ft | 0.025 Lb/sq ft | | | | | | | |
| | | SQ YD | SQ YD | SQ YD | SQ YD | SQ YD | SQ YD | POUND | | | TON | TON | TON | TON | TON | FOOT | TON |
| IL 122 S. JCT. | | | | | | | | | | | | | | | | | |
| RAMP A | | | | | | | | | | | | | | | | | |
| STA. 200+25.75 | TO STA. 200+35.75 | BUTT JOINT | 160.00 | 74.44 | | | | | 72.00 | | | 12.51 | | | | 0.93 | 0.51 |
| STA. 200+35.75 | TO STA. 201+48.35 | | | | 875.78 | | | | 394.10 | 197.05 | 63.06 | | 31.53 | 15.76 | | | 5.71 |
| STA. 201+48.35 | TO STA. 208+07.00 | | | | 1,902.77 | | | | 856.25 | 428.12 | 98.36 | | 49.18 | 92.21 | | | 33.37 |
| STA. 208+07.00 | TO STA. 220+87.00 | ENTRANCE TERMINAL | | | 2,773.33 | | | | 1,248.00 | 624.00 | 161.28 | | 80.64 | 107.52 | | | 64.85 |
| RAMP A SUB-TOTAL: | | | 160 | 74 | 5,552 | | | | 2,570 | 1,249 | 335 | | 161 | 216 | | | 104 |
| RAMP B | | | | | | | | | | | | | | | | | |
| STA. 100+00.00 | TO STA. 106+60.00 | EXIT TERMINAL | | | 1,805.10 | | | | 812.30 | 406.15 | 114.67 | | 57.33 | 55.44 | | | 33.44 |
| STA. 106+60.00 | TO STA. 115+23.45 | | | | 2,494.41 | | | | 1,122.49 | 561.24 | 128.94 | | 64.47 | 120.88 | | | 43.75 |
| STA. 115+23.45 | TO STA. 116+22.25 | | | | 686.11 | | | | 308.75 | 154.37 | 48.41 | | 24.21 | 13.83 | | | 5.01 |
| STA. 116+22.25 | TO STA. 116+32.25 | BUTT JOINT | 141.67 | 65.28 | | | | | 63.75 | | 10.97 | | | 0.93 | | | 0.51 |
| RAMP B SUB-TOTAL: | | | 142 | 65 | 4,986 | | | | 2,307 | 1,122 | 303 | | 146 | 191 | | | 83 |
| RAMP C | | | | | | | | | | | | | | | | | |
| STA. 400+26.73 | TO STA. 400+36.73 | BUTT JOINT | 161.11 | 75.00 | | | | | 72.50 | | 12.60 | | | 0.93 | | | 0.51 |
| STA. 400+36.73 | TO STA. 401+50.23 | | | | 882.78 | | | | 397.25 | 198.63 | 63.56 | | 31.78 | 15.89 | | | 5.75 |
| STA. 401+50.23 | TO STA. 408+06.28 | | | | 1,895.26 | | | | 852.86 | 426.43 | 97.97 | | 48.99 | 91.85 | | | 33.24 |
| STA. 408+06.28 | TO STA. 420+86.28 | ENTRANCE TERMINAL | | | 2,773.33 | | | | 1,248.00 | 624.00 | 161.28 | | 80.64 | 107.52 | | | 64.85 |
| RAMP C SUB-TOTAL: | | | 161 | 75 | 5,551 | | | | 2,571 | 1,249 | 335 | | 161 | 216 | | | 104 |
| RAMP D | | | | | | | | | | | | | | | | | |
| STA. 300+00 | TO STA. 306+60.00 | EXIT TERMINAL | | | 1,805.10 | | | | 812.30 | 406.15 | 114.67 | | 57.33 | 55.44 | | | 33.44 |
| STA. 306+60 | TO STA. 315+76.77 | | | | 2,648.45 | | | | 1,191.80 | 595.90 | 136.90 | | 68.45 | 128.35 | | | 46.45 |
| STA. 315+76.77 | TO STA. 316+75.37 | | | | 690.20 | | | | 310.59 | 155.29 | 48.77 | | 24.39 | 13.80 | | | 5.00 |
| STA. 316+75.37 | TO STA. 316+85.37 | BUTT JOINT | 142.78 | 65.83 | | | | | 64.25 | | 11.06 | | | 0.93 | | | 0.51 |
| RAMP D SUB-TOTAL: | | | 143 | 66 | 5,144 | | | | 2,379 | 1,157 | 311 | | 150 | 199 | | | 85 |

FILE NAME = 68A77 1-155 Plans.dgn
 USER NAME = keithbr
 PLOT SCALE = 100.0000' / 1" = 12/11/2014

| | |
|------------|-----------|
| DESIGNED - | REVISED - |
| DRAWN - | REVISED - |
| CHECKED - | REVISED - |
| DATE - | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SCHEDULES OF QUANTITIES

SCALE: SHEET 3 OF 11 SHEETS STA. TO STA.

| | | | | |
|---------------------------|-----------------------|----------|--------------|-----------|
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 155 | 90(109,109-1,110)R5-2 | TAZEWELL | 45 | 18 |
| CONTRACT NO. 68A77 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

TABULATION OF RESURFACING QUANTITIES (CONTINUED)

| LOCATION | NOTES | HMA SURFACE REMOVAL - BUTT JOINT | TEMP. RAMP | HMA SURFACE REMOVAL (DECK) | HMA SURFACE REMOVAL, 1 1/2 IN | HMA SURFACE REMOVAL, 3 3/4 IN | HMA SURFACE REMOVAL, VAR. DEPTH | POLYMERIZED BITUMINOUS MATERIALS (PRIME COAT) | | | POLY HMA SURFACE CSE, MIX "E", N70 | POLY HMA SURFACE CSE, MIX "E", N50 | POLY HMA BINDER CSE, IL-9.5, N70 | POLY LEVELING BINDER (MM) IL-4.75, N50 | HMA SHOULDERS | SHOULDER RUMBLE STRIPS, 16 INCH | AGG. WEDGE SHOULDER, TY B | |
|--------------------------------------|--------------|----------------------------------|------------|----------------------------|-------------------------------|-------------------------------|---------------------------------|---|---------------|----------------|------------------------------------|------------------------------------|----------------------------------|--|---------------|---------------------------------|---------------------------|--------------|
| | | | | | | | | NON-MILLED | COLD-MILLED | FOG COAT | | | | | | | | |
| | | | | | | | | 0.05 Lb/sq ft | 0.05 Lb/sq ft | 0.025 Lb/sq ft | | | | | | | | |
| | | SQ YD | SQ YD | SQ YD | SQ YD | SQ YD | SQ YD | POUND | | | TON | TON | TON | TON | TON | FOOT | TON | |
| IL 122 (MCMULLEN RD) | | | | | | | | | | | | | | | | | | |
| STA. 20+80.31 TO STA. 20+90.31 | BUTT JOINT | 53.33 | 15.56 | | | | | | | 24.00 | | | 2.61 | | | | 1.87 | |
| STA. 20+90.31 TO STA. 22+14.65 | | | | | | 663.15 | | | | 298.42 | 87.04 | | 32.49 | | | 16.25 | 23.21 | |
| STA. 22+14.65 TO STA. 22+65.86 | | | | | | 375.54 | | | | 168.99 | 58.89 | | 21.99 | | | 10.99 | 9.56 | |
| STA. 22+65.86 TO STA. 26+00.00 | | | | | | 2,227.60 | | | | 1,002.42 | 334.14 | | 124.75 | | | 62.37 | 62.37 | |
| STA. 26+00.00 TO STA. 27+98.38 | | | | | | 1,058.03 | | | | 476.11 | 138.87 | | 51.84 | | | 25.92 | 37.03 | |
| STA. 27+98.38 TO STA. 28+18.38 | BUTT JOINT | 106.67 | 15.56 | | | 106.67 | | | | 48.00 | 14.00 | | 5.23 | | | | 3.73 | |
| STA. 28+18.38 TO STA. 31+04.76 | S.N.090-0095 | | | | | 1,479.63 | | | | 665.83 | | | 124.29 | | | | | |
| STA. 31+04.76 TO STA. 31+24.76 | BUTT JOINT | 106.67 | 15.56 | | | 106.67 | | | | 48.00 | 14.00 | | 5.23 | | | | 3.73 | |
| STA. 31+24.76 TO STA. 32+00.00 | | | | | | 401.28 | | | | 180.58 | 52.67 | | 19.66 | | | 9.83 | 14.04 | |
| STA. 32+00.00 TO STA. 36+52.00 | | | | | | 3,013.33 | | | | 1,356.00 | 452.00 | | 168.75 | | | 84.37 | 84.37 | |
| STA. 36+52.00 TO STA. 37+03.29 | | | | | | 341.93 | | | | 153.87 | 51.29 | | 19.15 | | | 9.57 | 9.57 | |
| STA. 37+03.29 TO STA. 38+29.73 | | | | | | 927.23 | | | | 417.25 | 145.41 | | 54.28 | | | 27.14 | 23.60 | |
| STA. 38+29.73 TO STA. 38+39.73 | BUTT JOINT | 53.33 | 15.56 | | | | | | | 24.00 | | | 2.61 | | | | 1.87 | |
| IL 122 SUB-TOTAL: | | 320 | 62 | 1,480 | 9,221 | | | | | 4,863 | 1,348 | | 633 | | | 246 | 275 | 42 |
| MEDIAN CROSS-OVERS | | | | | | | | | | | | | | | | | | |
| STA. 650+50.00 | | | | | | | | | | 80.60 | | 40.30 | 22.57 | | | | | |
| STA. 737+80.00 | | | | | | | | | | 80.60 | | 40.30 | 22.57 | | | | | |
| STA. 805+15.00 | | | | | | | | | | 62.40 | | 31.20 | 17.47 | | | | | |
| STA. 1102+30.32 | | | | | | | | | | 62.40 | | 31.20 | 17.47 | | | | | |
| MEDIAN CROSS OVERS SUB-TOTAL: | | | | | | | | | | 286 | | 143 | 80.08 | | | | | |
| TOTAL: | | 2,490 | 872 | 1,480 | 306,782 | 583 | 5,600 | | | 325,529 | | 24,302 | 633 | 32,075 | 1,593 | 23,753 | 191,263 | 6,324 |

| MATERIAL TRANSFER DEVICE | |
|-----------------------------|---------------|
| LOCATION | TON |
| POLYMERIZED HMA SURFACE CSE | 24,935 |
| POLYMERIZED HMA BINDER CSE | 32,075 |
| POLYMERIZED LEVELING BINDER | 1,593 |
| TOTAL | 58,604 |

PAVEMENT MARKING SCHEDULE

| LOCATION | | PREFORMED PLASTIC PVMT MARKING, TY B - INLAID | MODIFIED URETHANE PAVEMENT MARKING, | | | | | | | RAISED REFLECTIVE PAVEMENT MARKER | | PRISMATIC CURB REFLECTORS | | |
|-------------------------------|-----------------------|---|-------------------------------------|-------------------|---------------|--------------|-----------------|-------------|-------------|-----------------------------------|--------------------------|---------------------------|--------------------------|--|
| | | | LINE 6" | LETTERS & SYMBOLS | LINE 4" | | LINE 8" | LINE 12" | LINE 18" | LINE 24" | ONE-WAY CRYSTAL 80' O.C. | | ONE-WAY CRYSTAL 40' O.C. | |
| | | | SKIP DASH WHITE | TURN ARROWS WHITE | SOLID WHITE | SOLID YELLOW | SKIP DASH WHITE | SOLID WHITE | SOLID WHITE | WHITE | SOLID WHITE | | | |
| | | | FT | SQ FT | FT | | FT | FT | FT | FT | FT | | EACH | |
| I-155 SOUTHBOUND LANES | | | | | | | | | | | | | | |
| STA. 616+36.21 | TO STA. 665+19.37 BK | 1,221 | | 4,883 | 4,883 | | | | | | 62 | | | |
| STA. 665+39.86 AH | TO STA. 697+73.49 | 808 | | 3,234 | 3,234 | | | | | | 40 | | | |
| STA. 697+73.49 | TO STA. 700+73.49 | 75 | | | 300 | 75 | | | | | 4 | | | |
| STA. 700+73.49 | TO STA. 704+33.49 | 90 | | | 360 | | 720 | | | | 5 | 10 | | |
| STA. 704+33.49 | TO STA. 728+80.86 | 612 | | 2,447 | 2,447 | | | | | | 31 | | | |
| STA. 728+80.86 | TO STA. 733+10.86 | 108 | | | 430 | | 860 | | | | 5 | 12 | | |
| STA. 733+10.86 | TO STA. 741+60.86 | 213 | | | 850 | 213 | | | | | 11 | | | |
| STA. 741+60.86 | TO STA. 874+41.09 | 3,320 | | 13,280 | 13,280 | | | | | | 166 | | | |
| STA. 874+41.09 | TO STA. 877+41.09 | 75 | | | 300 | 75 | | | | | 4 | | | |
| STA. 877+41.09 | TO STA. 881+01.09 | 90 | | | 360 | | 720 | 88 | | | 5 | 10 | | |
| STA. 881+01.09 | TO STA. 897+24.48 | 406 | | 1,623 | 1,623 | | | | | | 20 | | | |
| STA. 897+24.48 | TO STA. 901+54.48 | 108 | | | 430 | | 860 | | | | 5 | 12 | | |
| STA. 901+54.48 | TO STA. 910+04.48 | 213 | | | 850 | 213 | | | | | 11 | | | |
| STA. 910+04.48 | TO STA. 1102+30.32 BK | 4,806 | | 19,226 | 19,226 | | | | 24 | | 240 | | | |
| SB LANES SUB-TOTAL: | | 12,143 | | 44,694 | 48,574 | 575 | 3,160 | 88 | 24 | | 608 | 44 | | |
| I-155 NORTHBOUND LANES | | | | | | | | | | | | | | |
| STA. 616+26.95 | TO STA. 665+19.37 BK | 1,223 | | 4,892 | 4,892 | | | | | | 62 | | | |
| STA. 665+39.86 AH | TO STA. 700+86.70 | 887 | | 3,547 | 3,547 | | | | | | 44 | | | |
| STA. 700+86.70 | TO STA. 709+36.70 | 213 | | | 850 | 213 | | | | | 11 | | | |
| STA. 709+36.70 | TO STA. 713+66.70 | 108 | | | 430 | | 860 | | | | 5 | 12 | | |
| STA. 713+66.70 | TO STA. 732+97.16 | 483 | | 1,930 | 1,930 | | | | | | 24 | | | |
| STA. 732+97.16 | TO STA. 736+57.16 | 90 | | | 360 | | 720 | | | | 5 | 10 | | |
| STA. 736+57.16 | TO STA. 739+57.16 | 75 | | | 300 | 75 | | | | | 4 | | | |
| STA. 739+57.16 | TO STA. 870+10.31 | 3,263 | | 13,053 | 13,053 | | | | | | 163 | | | |
| STA. 870+10.31 | TO STA. 878+60.31 | 213 | | | 850 | 213 | | | | | 11 | | | |
| STA. 878+60.31 | TO STA. 882+90.31 | 108 | | | 430 | | 860 | | | | 5 | 12 | | |
| STA. 882+90.31 | TO STA. 899+15.52 | 406 | | 1,625 | 1,625 | | | | | | 20 | | | |
| STA. 899+15.52 | TO STA. 902+75.52 | 90 | | | 360 | | 720 | 87 | | | 5 | 10 | | |
| STA. 902+75.52 | TO STA. 905+75.52 | 75 | | | 300 | 75 | | | | | 4 | | | |
| STA. 905+75.52 | TO STA. 1102+30.32 BK | 4,914 | | 19,655 | 19,655 | | | | 24 | | 246 | | | |
| NB LANES SUB-TOTAL: | | 12,146 | | 44,703 | 48,583 | 575 | 3,160 | 87 | 24 | | 608 | 44 | | |

FILE NAME = 68A77 I-155 Plans.dgn
 USER NAME = keithbr
 PLOT SCALE = 100.0000' / 1" / 1" / 1"
 PLOT DATE = 12/11/2014

| | |
|------------|-----------|
| DESIGNED - | REVISED - |
| DRAWN - | REVISED - |
| CHECKED - | REVISED - |
| DATE - | REVISED - |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SCHEDULES OF QUANTITIES

SCALE: SHEET 5 OF 11 SHEETS STA. TO STA.

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------------------|----------|--------------|-----------|
| 155 | 90(109,109-1,110)R5-2 | TAZEWELL | 45 | 20 |
| CONTRACT NO. 68A77 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

PAVEMENT MARKING SCHEDULE (CONTINUED)

| LOCATION | PREFORMED PLASTIC PVMT MARKING, TY B - INLAID | MODIFIED URETHANE PAVEMENT MARKING, | | | | | | | | RAISED REFLECTIVE PAVEMENT MARKER | | PRISMATIC CURB REFLECTORS | |
|--------------------------|---|-------------------------------------|-------------------|--------------|--------------|-----------------|-------------|-------------|-----------|-----------------------------------|--------------------------|---------------------------|--------------------------|
| | | LINE 6" | LETTERS & SYMBOLS | LINE 4" | | LINE 8" | | LINE 12" | LINE 18" | LINE 24" | ONE-WAY CRYSTAL 80' O.C. | | ONE-WAY CRYSTAL 40' O.C. |
| | | SKIP DASH WHITE | TURN ARROWS WHITE | SOLID WHITE | SOLID YELLOW | SKIP DASH WHITE | SOLID WHITE | SOLID WHITE | WHITE | SOLID WHITE | | | |
| | | FT | SQ FT | FT | | FT | FT | FT | FT | FT | EACH | | |
| IL 122 N. JCT. | | | | | | | | | | | | | |
| RAMP A | | | | | | | | | | | | | |
| STA. 0+23.20 | TO STA. 1+23.66 | | | 100 | 100 | | | | | | | | |
| STA. 1+23.66 | TO STA. 15+43.30 | | | 1,420 | 1,420 | | | | | | | | |
| STA. 15+43.30 | TO STA. 28+23.30 | | | 850 | | | | | | | | | |
| RAMP A SUB-TOTAL: | | | | 2,370 | 1,520 | | | | | | | | |
| RAMP B | | | | | | | | | | | | | |
| STA. 00+00 | TO STA. 6+60.00 | | | 360 | | | | | | | 10 | | |
| STA. 6+60.00 | TO STA. 15+85.63 | | | 926 | 926 | | | | | | | | |
| STA. 15+85.63 | TO STA. 17+07.13 | | | 122 | 122 | 160 | 65 | | 36 | | | 11 | |
| RAMP B SUB-TOTAL: | | | | 1,407 | 1,047 | 160 | 65 | | 36 | | 10 | 11 | |
| RAMP C | | | | | | | | | | | | | |
| STA. 0+24.62 | TO STA. 1+27.51 | | | 103 | 103 | | | | | | | | |
| STA. 1+27.51 | TO STA. 12+96.69 | | | 1,169 | 1,169 | | | | | | | | |
| STA. 12+96.69 | TO STA. 25+76.69 | | | 850 | | | | | | | | | |
| RAMP C SUB-TOTAL: | | | | 2,122 | 1,272 | | | | | | | | |
| RAMP D | | | | | | | | | | | | | |
| STA. 00+00 | TO STA. 6+60.00 | | | 360 | | | | | | | 10 | | |
| STA. 6+60.00 | TO STA. 17+55.34 | | | 1,095 | 1,095 | | | | | | | | |
| STA. 17+55.34 | TO STA. 18+58.44 | | | 103 | 103 | 160 | 65 | | 36 | | | 11 | |
| RAMP D SUB-TOTAL: | | | | 1,558 | 1,198 | 160 | 65 | | 36 | | 10 | 11 | |
| IL 122 S. JCT. | | | | | | | | | | | | | |
| RAMP A | | | | | | | | | | | | | |
| STA. 200+25.75 | TO STA. 201+48.35 | | | 123 | 123 | 190 | 169 | | | | | | |
| STA. 201+48.35 | TO STA. 208+07.00 | | | 659 | 659 | | | | | | | | |
| STA. 208+07.00 | TO STA. 220+87.00 | | | 850 | | | | | | | | | |
| RAMP A SUB-TOTAL: | | | | 1,631 | 781 | 190 | 169 | | | | | | |
| RAMP B | | | | | | | | | | | | | |
| STA. 100+00.00 | TO STA. 106+60.00 | | | 360 | | | | | | | 10 | | |
| STA. 106+60.00 | TO STA. 115+33.45 | | | 873 | 873 | | | | | | | | |
| STA. 115+33.45 | TO STA. 116+32.25 | | | 99 | 99 | 155 | 90 | | 36 | | | | |
| RAMP B SUB-TOTAL: | | | | 1,332 | 972 | 155 | 90 | | 36 | | 10 | | |
| RAMP C | | | | | | | | | | | | | |
| STA. 400+26.73 | TO STA. 401+50.23 | | | 124 | 124 | 190 | 199 | | | | | | |
| STA. 401+50.23 | TO STA. 408+06.28 | | | 656 | 656 | | | | | | | | |
| STA. 408+06.28 | TO STA. 420+86.28 | | | 850 | | | | | | | | | |
| RAMP C SUB-TOTAL: | | | | 1,630 | 780 | 190 | 199 | | | | | | |
| RAMP D | | | | | | | | | | | | | |
| STA. 300+00 | TO STA. 306+60.00 | | | 360 | | | | | | | 10 | | |
| STA. 306+60 | TO STA. 315+86.77 | | | 927 | 927 | | | | | | | | |
| STA. 315+86.77 | TO STA. 316+85.37 | | | 99 | 99 | 155 | 83 | | 36 | | | | |
| RAMP D SUB-TOTAL: | | | | 1,385 | 1,025 | 155 | 83 | | 36 | | 10 | | |

FILE NAME = 68A77 1-155 Plans.dgn

USER NAME = keithbr

DESIGNED -

REVISED -

DRAWN -

REVISED -

PLOT SCALE = 1/8" = 100.0000' / 1" / 1"

CHECKED -

REVISED -

PLOT DATE = 12/11/2014

DATE -

REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULES OF QUANTITIES

SCALE:

SHEET 6 OF 11 SHEETS

STA.

TO STA.

F.A.I. RTE.

SECTION

COUNTY

TOTAL SHEETS

SHEET NO.

155

90(109,109-1,110)R5-2

TAZEWELL

45

21

CONTRACT NO. 68A77

ILLINOIS FED. AID PROJECT

PAVEMENT MARKING SCHEDULE (CONTINUED)

| LOCATION | PREFORMED PLASTIC PVMT MARKING, TY B - INLAID | MODIFIED URETHANE PAVEMENT MARKING, | | | | | | | | RAISED REFLECTIVE PAVEMENT MARKER | | PRISMATIC CURB REFLECTORS | |
|-----------------------------------|---|-------------------------------------|-------------------|----------------|--------------|-----------------|-------------|-------------|------------|-----------------------------------|--------------------------|---------------------------|--------------------------|
| | | LINE 6" | LETTERS & SYMBOLS | LINE 4" | | LINE 8" | | LINE 12" | LINE 18" | LINE 24" | ONE-WAY CRYSTAL 80' O.C. | | ONE-WAY CRYSTAL 40' O.C. |
| | | SKIP DASH WHITE | TURN ARROWS WHITE | SOLID WHITE | SOLID YELLOW | SKIP DASH WHITE | SOLID WHITE | SOLID WHITE | WHITE | SOLID WHITE | | | |
| | | FT | SQ FT | FT | | FT | FT | FT | FT | FT | EACH | | EACH |
| IL 122 N JCT (MCMULLEN RD) | | | | | | | | | | | | | |
| STA. 20+80.31 TO STA. 22+14.65 | | | 268.68 | 268.68 | | | | | | | | 15 | |
| STA. 22+14.65 TO STA. 22+65.86 | | | | 102.42 | | | | | | | | | |
| STA. 22+65.86 TO STA. 26+00.00 | | 31.20 | 668.28 | 668.28 | | 260 | | | | | | 38 | |
| STA. 26+00.00 TO STA. 32+00.00 | | | 1,200.00 | 1,200.00 | | | | | | | | 10 | |
| STA. 32+00.00 TO STA. 36+55.00 | | 31.20 | 910.00 | 910.00 | | | | | | | | 48 | |
| STA. 36+55.00 TO STA. 37+03.29 | | | | 96.58 | | 225 | | | | | | | |
| STA. 37+03.29 TO STA. 38+39.73 | | | 272.88 | 272.88 | | | | | | | | 15 | |
| IL 122 SUB-TOTAL: | | 62 | 3,320 | 3,519 | | 485 | | | | | | 126 | |
| TOTAL: | | 24,289 | 62 | 215,424 | | 8,965 | 846 | 48 | 144 | | 1,343 | 148 | |

NOTE: BUREAU OF OPERATIONS MUST BE CONTACTED TWO WEEKS PRIOR TO PLACING AERIAL SPEED ZONE PAVEMENT MARKINGS.

| WORK ZONE PAVEMENT MARKING REMOVAL | |
|------------------------------------|--------------|
| LOCATION | SQ FT |
| I-155 MAINLINE | 5,829 |
| RAMPS | 436 |
| TOTAL | 6,266 |

| TREE REMOVAL (6 TO 15 UNITS DIAMETER) | |
|--|-----------|
| LOCATION | UNIT |
| I-155 MAINLINE | |
| STA. 729+50.00 NB | 9 |
| STA. 737+50.00 NB | 9 |
| TOTAL: | 18 |

| MOBILIZATION | |
|--------------|----------|
| LOCATION | L SUM |
| I-155 | 1 |
| TOTAL | 1 |

| RAISED REFLECTIVE PAVEMENT MARKER REMOVAL | |
|---|--------------|
| LOCATION | EACH |
| I-155 | 1,345 |
| TOTAL | 1,345 |

| PAVEMENT MARKING BLACKOUT TAPE, 8" | |
|------------------------------------|--------------|
| LOCATION | FOOT |
| I-155 MAINLINE | 1,000 |
| TOTAL | 1,000 |

| ENGINEER'S FIELD OFFICE, TYPE A | |
|---------------------------------|-----------|
| LOCATION | CAL MO |
| I-155 | 14 |
| TOTAL | 14 |

| SHORT-TERM PAVEMENT MARKING | | | | | | |
|---|------------------|----------|-------|------------------------|---------------|--|
| LOCATION | CENTERLINE FT | EDGELINE | | NO. OF APPLICATIONS | TOTAL FT | |
| | | LT | RT | | | |
| | | FT | FT | | | |
| I-155 SOUTHBOUND LANES | | | | | | |
| STA. 616+36.21 TO STA. 665+19.37 BK | 488 | 195 | 195 | 3 | 2,637 | |
| STA. 665+39.86 AH TO STA. 1102+30.32 BK | 4,369 | 1,748 | 1,748 | 3 | 23,593 | |
| I-155 NORTHBOUND LANES | | | | | | |
| STA. 616+26.95 TO STA. 665+19.37 BK | 489 | 196 | 196 | 3 | 2,642 | |
| STA. 665+39.86 AH TO STA. 1102+30.32 BK | 4,369 | 1,748 | 1,748 | 3 | 23,593 | |
| IL 122 N. JCT | | | | | | |
| RAMP A | | | | | | |
| STA. 0+23.20 TO STA. 28+23.30 | | 112 | 112 | 3 | 672 | |
| RAMP B | | | | | | |
| STA. 00+00 TO STA. 17+07.13 | | 68 | 68 | 3 | 410 | |
| RAMP C | | | | | | |
| STA. 0+24.62 TO STA. 25+76.69 | | 102 | 102 | 3 | 612 | |
| RAMP D | | | | | | |
| STA. 00+00 TO STA. 18+58.44 | | 74 | 74 | 3 | 446 | |
| IL 122 S. JCT | | | | | | |
| RAMP A | | | | | | |
| STA. 200+25.75 TO STA. 220+87.00 | | 82 | 82 | 3 | 495 | |
| RAMP B | | | | | | |
| STA. 100+00 TO STA. 116+32.25 | | 65 | 65 | 3 | 392 | |
| RAMP C | | | | | | |
| STA. 400+26.73 TO STA. 420+86.28 | | 82 | 82 | 3 | 494 | |
| RAMP D | | | | | | |
| STA. 300+00 TO STA. 316+85.37 | | 67 | 67 | 3 | 404 | |
| TOTAL | | | | | 56,390 | |

| SCHEDULE OF ISLAND MEDIAN QUANTITIES | | | | | |
|--------------------------------------|-----------------|-------------------|------------------------------|---------------------|--|
| LOCATION | CC&G REMOVAL | ISLAND REMOVAL | MEDIAN SURFACE REMOVAL | CCC&G, TY M-6.06 | CONCRETE MEDIAN SURFACE, 4 INCH |
| | FT | SQ FT | SQ FT | FT | SQ FT |
| IL 122 N JCT INTERCHANGE | | | | | |
| RAMP B | | 127.07 | | 46.17 | 127.07 |
| RAMP D | | 77.64 | | 34.72 | 77.64 |
| IL 122 N JCT | | | | | |
| STA. 16+52.95 TO STA. 22+14.65 | 1,135.56 | | 7,978.50 | 1,135.56 | 7,978.50 |
| STA. 22+65.86 TO STA. 28+18.38 | 1,085.11 | | 5,051.12 | 1,085.11 | 5,051.12 |
| STA. 31+04.76 TO STA. 36+55.00 | 1,083.93 | | 5,241.96 | 1,083.93 | 5,241.96 |
| STA. 37+03.29 TO STA. 43+09.13 | 1,137.25 | | 8,050.45 | 1,137.25 | 8,050.45 |
| TOTAL: | | | | | 26,527 |

| GROOVING FOR RECESSED PAVEMENT MARKING | | | | | |
|--|---------|-------|------|------|------------|
| LOCATION | 5" | 9" | 13" | 19" | 25" |
| | FOOT | FOOT | FOOT | FOOT | FOOT |
| I-155 | 214,278 | 8,965 | 846 | 48 | 144 |
| TOTAL | | | | | 144 |

| TRAFFIC CONTROL AND PROTECTION | | | | |
|--------------------------------|-------------|-------------|-------------|-------------|
| LOCATION | STD. 701401 | STD. 701406 | STD. 701411 | STD. 701456 |
| | L SUM | L SUM | EACH | L SUM |
| I-155 MAIN LINE AND RAMPS | 1 | 1 | 20 | 1 |
| IL 122 | | | | |
| TOTAL | | | | |

| MOWING | |
|--------------|-------|
| LOCATION | UNIT |
| I-155 | 1,549 |
| TOTAL | |

| PROTECTIVE SHIELD | |
|--------------------------------|--------|
| LOCATION | SQ YD |
| STA. 28+18.38 TO STA. 31+04.76 | 765.00 |
| TOTAL | |

| CONSTRUCTION STATION LAYOUT | |
|-----------------------------|-------|
| LOCATION | L SUM |
| I-155 | 1 |
| TOTAL | |

| EROSION CONTROL SCHEDULE | | | | | | | | | | | | |
|------------------------------|----------------------|----------------------------|---------------|-----------------------------|------------------------|-----------------------------------|-------------------|-----------------|------------------------------|--------------------------------|-------------------------------|-----------|
| LOCATION | REVTMENT MAT REMOVAL | STONE DUMPED RIPRAP, CL A4 | FILTER FABRIC | GRADING AND SHAPING DITCHES | TEMPORARY DITCH CHECKS | TEMPORARY EROSION CONTROL SEEDING | SEEDING, CLASS 2A | MULCH, METHOD 2 | NITROGEN FERTILIZER NUTRIENT | PHOSPHORUS FERTILIZER NUTRIENT | POTASSIUM FERTILIZER NUTRIENT | |
| | SQ YD | TON | SQ YD | FT | FT | POUND | ACRE | ACRE | POUND | POUND | POUND | |
| | | | | | | 100 LB/ACRE | | | 90 LB/ACRE | 90 LB/ACRE | 90 LB/ACRE | |
| I-155 MAINLINE | | | | | | | | | | | | |
| STA. 1039+31 TO STA. 1049+96 | CENTER MEDIAN | 75 | 50 | 75 | 1,065.05 | 210 | 48.90 | 0.49 | 0.49 | 44.01 | 44.01 | 44.01 |
| TOTAL | | 75 | 50 | 75 | 1,065 | 210 | 49 | 0.5 | 0.5 | 44 | 44 | 44 |

| SCHEDULE OF GUARDRAIL QUANTITIES | | | | | | | | | | | |
|----------------------------------|---------------|-------------------|------------------------------|----------------------------------|------------|------------|------------|-------------|---------------------------|-------------------------------------|------------------------------|
| LOCATION | NOTES | GUARDRAIL REMOVAL | TBT TYPE 1 (SPECIAL TANGENT) | TERMINAL MARKER - DIRECT APPLIED | TBT TYPE 2 | TBT TYPE 5 | TBT TYPE 6 | TBT TYPE 6B | SPBGR, TYPE A, 6 FT POSTS | GUARDRAIL AGGREGATE EROSION CONTROL | EROSION CONTROL CURB REMOVAL |
| | | FOOT | EACH | EACH | EACH | EACH | EACH | EACH | FOOT | TON | FOOT |
| I-155 SOUTHBOUND LANES | | | | | | | | | | | |
| RT. STA. 662+40 TO STA. 665+58 | S.N. 090-0092 | 319 | 1 | 1 | | | 1 | | 225 | | |
| LT. STA. 661+90 TO STA. 665+58 | | 369 | 1 | 1 | | | 1 | | 275 | 28.03 | |
| RT. STA. 671+08 TO STA. 672+09 | | 101 | | | | 1 | 1 | | 75 | 7.66 | |
| RT. STA. 742+36 TO STA. 744+80 | S.N. 090-0093 | 244 | 1 | 1 | | | | 1 | 150 | 49.40 | 200 |
| LT. STA. 740+99 TO STA. 744+80 | | 381 | 1 | 1 | | | 1 | | 288 | 28.98 | |
| RT. STA. 746+58 TO STA. 749+21 | | 263 | | | | 1 | 1 | | 170 | 53.36 | 263 |
| LT. STA. 885+94 TO STA. 890+07 | S.N. 090-0138 | 413 | 1 | 1 | 1 | | | | 350 | 31.35 | |
| LT. STA. 1045+26 TO STA. 1049+14 | S.N. 090-0139 | 388 | 1 | 1 | 1 | | | | 325 | 29.45 | |
| SB LANES SUB-TOTAL: | | 2,477 | 6 | 6 | 4 | 2 | 3 | 1 | 1,857 | 228 | 463 |
| I-155 NORTHBOUND LANES | | | | | | | | | | | |
| RT. STA. 662+09 TO STA. 665+90 | S.N. 090-0100 | 381 | 1 | 1 | | | 1 | | 288 | 28.98 | |
| LT. STA. 662+72 TO STA. 665+90 | | 319 | 1 | 1 | | | 1 | | 225 | 24.23 | |
| RT. STA. 740+99 TO STA. 744+80 | S.N. 090-0094 | 381 | 1 | 1 | | | 1 | | 288 | 28.98 | |
| LT. STA. 738+74 TO STA. 744+80 | | 606 | 1 | 1 | | | 1 | | 513 | 122.87 | 606 |
| LT. STA. 746+58 TO STA. 755+96 | | 938 | | | | 1 | 1 | | 913 | 190.16 | 930 |
| RT. STA. 886+32 TO STA. 890+07 | S.N. 090-0138 | 375 | 1 | 1 | 1 | | | | 313 | 28.50 | |
| RT. STA. 1045+76 TO STA. 1049+14 | S.N. 090-0139 | 338 | 1 | 1 | 1 | | | | 275 | 25.65 | |
| NB LANES SUB-TOTAL: | | 3,338 | 6 | 6 | 3 | 1 | 4 | | 2,813 | 449 | 1,536 |
| TOTAL | | 5,815 | 12 | 12 | 7 | 3 | 7 | 1 | 4,670 | 678 | 1,999 |

| CLASS B PATCHES | | | | | | | | |
|--------------------------------------|---------|-----|------------|-----------|-----------|-----------|--------------|----------------------|
| LOCATION | SIZE | NO. | 13 INCH | 14 INCH | 16 INCH | | SAW CUTS | DOWEL BARS 1 1/2" |
| | | | TYPE II | TYPE IV | TYPE II | TYPE III | | |
| | | | SQ YD | SQ YD | SQ YD | SQ YD | | |
| I-155 SB LANES | | | | | | | | |
| STA. 683+00 | 6 X 12 | 1 | | | 8.00 | | 48 | 20 |
| I-155 NB LANES | | | | | | | | |
| STA. 619+00 | 6 X 12 | 1 | | | 8.00 | | 48 | 20 |
| STA. 664+00 (SOUTH OF S.N. 090-0100) | 6 X 24 | 1 | | | | 16.00 | 84 | 20 |
| STA. 672+00 (NORTH OF S.N. 090-0100) | 6 X 24 | 1 | | | | 16.00 | 84 | 20 |
| STA. 785+00 (TOBOGGAN RD) | 12 X 24 | 1 | | 32.00 | | | 96 | 20 |
| IL 122 N JCT INTERCHANGE | | | | | | | | |
| RAMP A | 6 X 16 | 2 | 21.33 | | | | 120 | 52 |
| RAMP B | 6 X 16 | 2 | 21.33 | | | | 120 | 52 |
| RAMP C | 6 X 16 | 4 | 42.67 | | | | 240 | 104 |
| RAMP D | 6 X 16 | 3 | 32.00 | | | | 180 | 78 |
| TOTAL: | | | 118 | 32 | 16 | 32 | 1,020 | 386 |

| REPLACE IMPACT ATTENUATORS (NON-REDIRECTIVE), TEST LEVEL 3 | | |
|---|---------------|-----------|
| LOCATION | | EACH |
| STA. 785+02.10 | S.N. 090-0101 | 32 |
| TOTAL | | 32 |

| CLEAN EXISTING END SECTION | | |
|----------------------------|----|----------|
| LOCATION | | EACH |
| I-155 MAINLINE | | |
| LT. STA. 690+75 | NB | 1 |
| RT. STA. 690+75 | NB | 1 |
| RT. STA. 812+00 | SB | 1 |
| RT. STA. 980+20 | SB | 1 |
| RT. STA. 1049+95 | SB | 1 |
| SUB-TOTAL: | | 5 |
| IL 122 A JCT RAMP A | | |
| LT. STA. 201+00 | SB | 1 |
| SUB-TOTAL: | | 1 |
| TOTAL: | | 6 |

| CHANGEABLE MESSAGE SIGN | | |
|-------------------------|-----------|-----------|
| LOCATION | NO. SIGNS | CAL DA |
| I-155 MAINLINE | 2 | 7 |
| IL 122 N JCT | 2 | 7 |
| TOTAL | | 28 |

| TRAFFIC CONTROL SURVEILLANCE | |
|------------------------------|-----------|
| LOCATION | CAL DA |
| I-155 MAINLINE AND RAMPS | 10 |
| TOTAL | 10 |

| | | | |
|-----------------------------------|-------------------------------|------------|-----------|
| FILE NAME = 68A77 I-155 Plans.dgn | USER NAME = keithbr | DESIGNED - | REVISED - |
| | | DRAWN - | REVISED - |
| | PLOT SCALE = 100.0000' / 1" = | CHECKED - | REVISED - |
| | PLOT DATE = 12/11/2014 | DATE - | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULES OF QUANTITIES

SCALE: SHEET 10 OF 11 SHEETS STA. TO STA.

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------------------|----------|--------------|-----------|
| 155 | 90(109,109-1,110)R5-2 | TAZEWELL | 45 | 25 |
| CONTRACT NO. 68A77 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

| RECOVERABLE DELINEATORS | | | | | | |
|--------------------------------|--------------|---------|--------------|-------|------------|----------|
| LOCATION | | | SPACING | WHITE | YELLOW | |
| | | | FT | EACH | | |
| I-155 MAINLINE SB LANES | | | | | | |
| STA. | 616+36.21 | TO STA. | 651+80.92 | 400 | 9 | |
| STA. | 651+80.92 | TO STA. | 662+33.42 | 200 | 5 | |
| STA. | 662+33.42 | TO STA. | 665+19.37 BK | 0 | STRUCTURE | |
| STA. | 665+39.86 AH | TO STA. | 672+20.67 | 0 | OMISSION | |
| STA. | 672+20.67 | TO STA. | 697+73.49 | 400 | 6 | |
| STA. | 697+73.49 | TO STA. | 704+34.97 | 0 | RAMP D | |
| STA. | 710+57.22 | TO STA. | 726+66.38 | 200 | 8 | |
| STA. | 726+66.38 | TO STA. | 728+80.86 | 400 | 1 | |
| STA. | 728+80.86 | TO STA. | 741+60.86 | 0 | RAMP C | |
| STA. | 741+60.86 | TO STA. | 742+30.17 | 0 | STRUCTURE | |
| STA. | 742+30.17 | TO STA. | 749+20.50 | 0 | OMISSION | |
| STA. | 749+20.50 | TO STA. | 874+41.09 | 400 | 31 | |
| STA. | 874+41.09 | TO STA. | 881+01.09 | 0 | RAMP B | |
| STA. | 881+01.09 | TO STA. | 897+24.48 | 400 | 4 | |
| STA. | 897+24.48 | TO STA. | 910+04.21 | 0 | RAMP A | |
| STA. | 910+04.21 | TO STA. | 1102+30.32 | 400 | 48 | |
| SB LANES SUB-TOTAL: | | | | | 113 | 0 |
| I-155 MAINLINE NB LANES | | | | | | |
| STA. | 616+26.95 | TO STA. | 651+80.92 | 400 | 9 | |
| STA. | 651+80.92 | TO STA. | 665+19.37 BK | 200 | 7 | |
| STA. | 665+39.86 AH | TO STA. | 665+90.45 | 400 | 0 | |
| STA. | 665+90.45 | TO STA. | 674+65.20 | 0 | STRUCTURE | |
| STA. | 674+65.20 | TO STA. | 700+86.70 | 400 | 7 | |
| STA. | 700+86.70 | TO STA. | 713+66.70 | 0 | RAMP A | |
| STA. | 713+66.70 | TO STA. | 726+66.38 | 200 | 6 | |
| STA. | 726+66.38 | TO STA. | 732+97.16 | 400 | 2 | |
| STA. | 732+97.16 | TO STA. | 739+57.16 | 0 | RAMP B | |
| STA. | 739+57.16 | TO STA. | 752+70.50 | 0 | STRUCTURE | |
| STA. | 752+70.50 | TO STA. | 870+10.31 | 400 | 29 | |
| STA. | 870+10.31 | TO STA. | 882+91.31 | 0 | RAMP A | |
| STA. | 882+91.31 | TO STA. | 899+15.52 | 400 | 4 | |
| STA. | 899+15.52 | TO STA. | 905+72.52 | 0 | RAMP D | |
| STA. | 905+72.52 | TO STA. | 1102+30.32 | 400 | 49 | |
| NB LANES SUB-TOTAL: | | | | | 113 | 0 |

| RECOVERABLE DELINEATORS (CONTINUED) | | | | | | |
|-------------------------------------|-----------|---------|-----------|-------|------------|-----------|
| LOCATION | | | SPACING | WHITE | YELLOW | |
| | | | FT | EACH | | |
| IL 122 N. JCT | | | | | | |
| RAMP A | | | | | | |
| STA. | 0+23.20 | TO STA. | 2+20.00 | 100 | 2 | |
| STA. | 2+20.00 | TO STA. | 7+45.08 | 55 | 10 | |
| STA. | 7+45.08 | TO STA. | 13+95.08 | 100 | 7 | |
| STA. | 13+95.08 | TO STA. | 17+73.30 | 80 | 5 | 5 |
| STA. | 17+73.30 | TO STA. | 28+23.30 | 100 | 11 | |
| RAMP A SUB-TOTAL: | | | | | 33 | 5 |
| RAMP B | | | | | | |
| STA. | 00+00 | TO STA. | 6+62.40 | 100 | 7 | |
| STA. | 6+62.40 | TO STA. | 10+86.97 | 80 | 5 | 5 |
| STA. | 10+86.97 | TO STA. | 17+07.13 | 100 | 6 | |
| RAMP B SUB-TOTAL: | | | | | 18 | 5 |
| RAMP C | | | | | | |
| STA. | 0+24.62 | TO STA. | 2+20.00 | 100 | 2 | |
| STA. | 2+20.00 | TO STA. | 4+79.88 | 55 | 5 | |
| STA. | 4+79.88 | TO STA. | 8+59.88 | 100 | 4 | |
| STA. | 8+59.88 | TO STA. | 15+26.69 | 80 | 8 | |
| STA. | 15+26.69 | TO STA. | 25+76.69 | 100 | 11 | |
| RAMP C SUB-TOTAL: | | | | | 29 | 0 |
| RAMP D | | | | | | |
| STA. | 00+00 | TO STA. | 6+62.40 | 100 | 7 | |
| STA. | 6+62.40 | TO STA. | 11+09.96 | 80 | 6 | 6 |
| STA. | 11+09.96 | TO STA. | 18+58.44 | 100 | 7 | |
| RAMP D SUB-TOTAL: | | | | | 20 | 6 |
| IL 122 S. JCT. | | | | | | |
| RAMP A | | | | | | |
| STA. | 200+25.75 | TO STA. | 205+41.37 | 100 | 5 | |
| STA. | 205+41.37 | TO STA. | 209+37.00 | 80 | 5 | 5 |
| STA. | 209+37.00 | TO STA. | 220+87.00 | 100 | 12 | |
| RAMP A SUB-TOTAL: | | | | | 22 | 5 |
| RAMP B | | | | | | |
| STA. | 100+00.00 | TO STA. | 107+66.35 | 100 | 8 | |
| STA. | 107+66.35 | TO STA. | 111+15.33 | 80 | 4 | 4 |
| STA. | 111+15.33 | TO STA. | 116+32.25 | 100 | 5 | |
| RAMP B SUB-TOTAL: | | | | | 17 | 4 |
| RAMP C | | | | | | |
| STA. | 400+26.73 | TO STA. | 405+41.14 | 100 | 5 | |
| STA. | 405+41.14 | TO STA. | 409+36.27 | 80 | 5 | 5 |
| STA. | 409+36.27 | TO STA. | 420+86.28 | 100 | 12 | |
| RAMP C SUB-TOTAL: | | | | | 22 | 5 |
| RAMP D | | | | | | |
| STA. | 300+00 | TO STA. | 307+66.35 | 100 | 8 | |
| STA. | 307+66 | TO STA. | 311+15.33 | 80 | 4 | 4 |
| STA. | 311+15 | TO STA. | 316+85.37 | 100 | 6 | |
| RAMP D SUB-TOTAL: | | | | | 18 | 4 |
| SUB-TOTAL: | | | | | 404 | 34 |
| TOTAL: | | | | | 438 | |

FILE NAME = 68A77 I-155 Plans.dgn

USER NAME = keithbr

DESIGNED -

REVISED -

DRAWN -

REVISED -

PLOT SCALE = 100.0000' / 1" =

CHECKED -

REVISED -

PLOT DATE = 12/11/2014

DATE -

REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULES OF QUANTITIES

SCALE:

SHEET 11 OF 11 SHEETS

STA.

TO STA.

F.A.I.
RTE.

SECTION

COUNTY

TOTAL SHEETS

SHEET NO.

155

90(109,109-1,110)R5-2

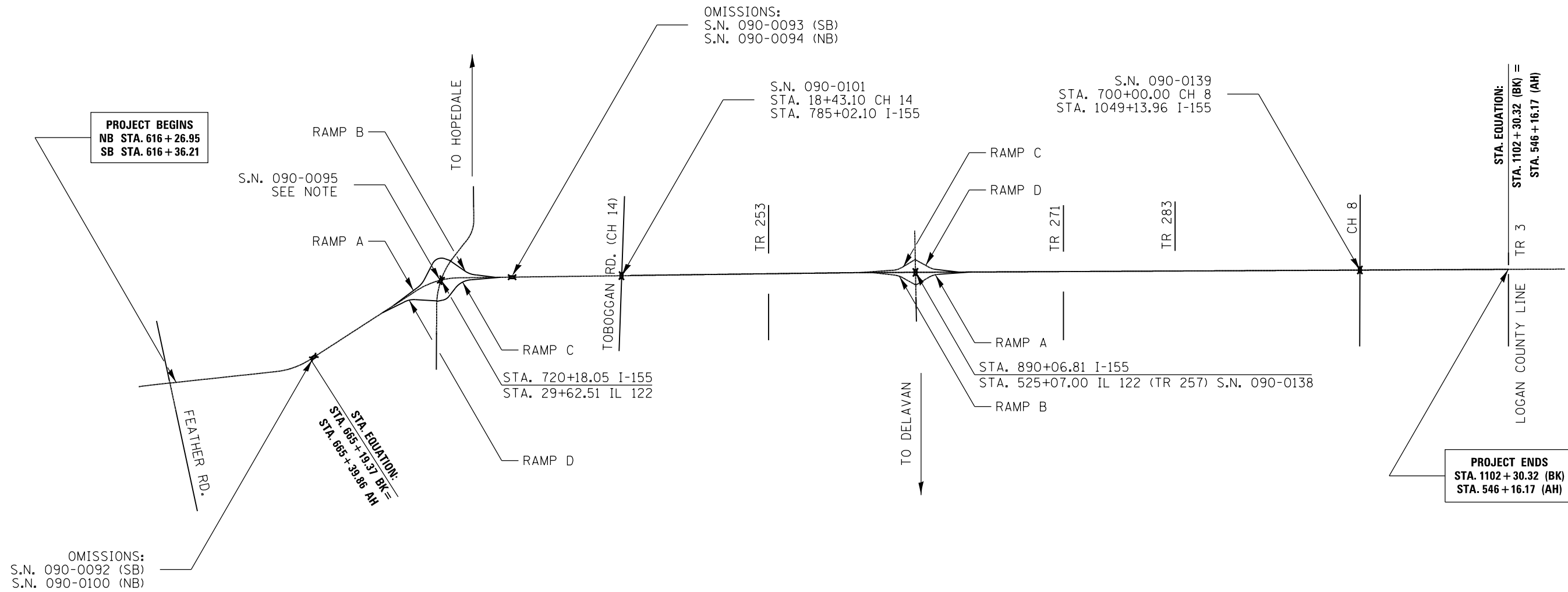
TAZEWELL

45

26

CONTRACT NO. 68A77

ILLINOIS FED. AID PROJECT



STRUCTURE OMISSIONS:

S.N. 090-0100 (NB) OVER MACKINAW RIVER
STA. 665+90.45 TO STA. 671+40.20

S.N. 090-0092 (SB) OVER MACKINAW RIVER
STA. 665+58.42 TO STA. 671+08.17

S.N. 090-0093 (SB) AND S.N. 090-0094 (NB) OVER INDIAN CREEK
STA. 744+80.17 TO STA. 746+58.00

NOTE:

S.N. 090-0095 IMPROVEMENT
STA. 28+18.38 TO STA. 31+04.76

IL 122 IMPROVEMENT
STA. 16+52.95 TO STA. 43+09.13

| | |
|-------------|-----------------------|
| FILE NAME = | 68A77 I-155 Plans.dgn |
|-------------|-----------------------|

| | |
|--------------|-----------------|
| USER NAME = | keithbr |
| PLOT SCALE = | 100.0000' / in. |
| PLOT DATE = | 12/11/2014 |

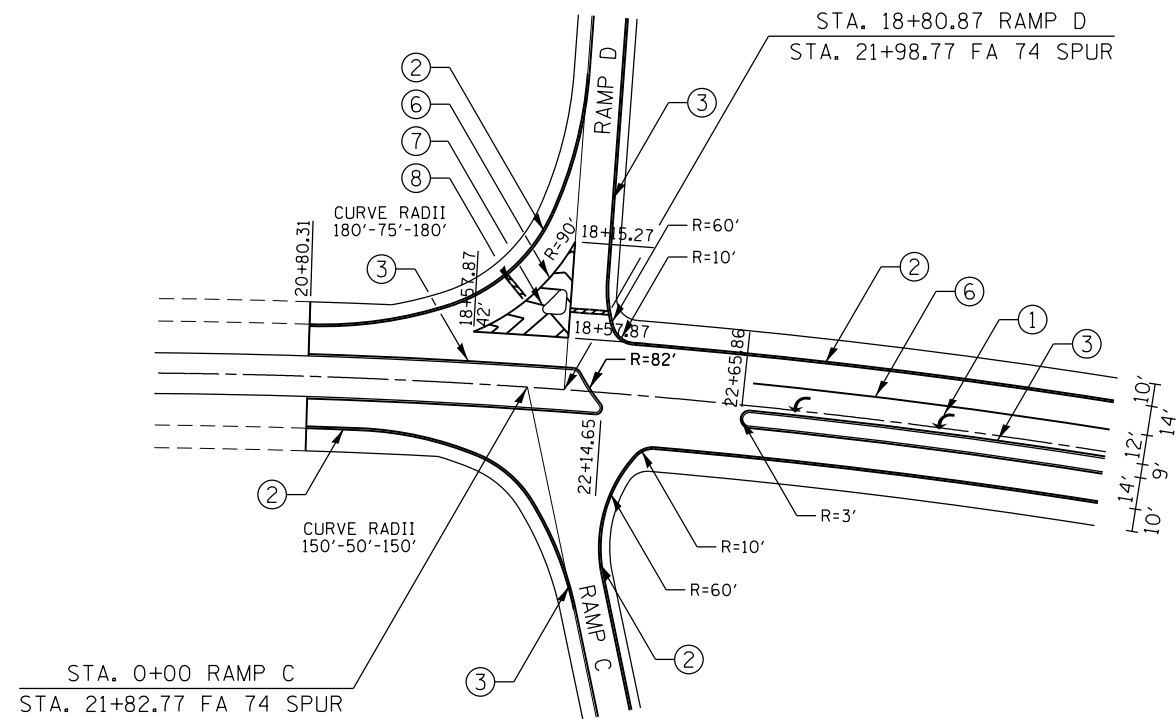
| | |
|------------|-----------|
| DESIGNED - | REVISOR - |
| DRAWN - | REVISOR - |
| CHECKED - | REVISOR - |
| DATE - | REVISOR - |

| | |
|------------|-----------|
| DESIGNED - | REVISOR - |
| DRAWN - | REVISOR - |
| CHECKED - | REVISOR - |
| DATE - | REVISOR - |

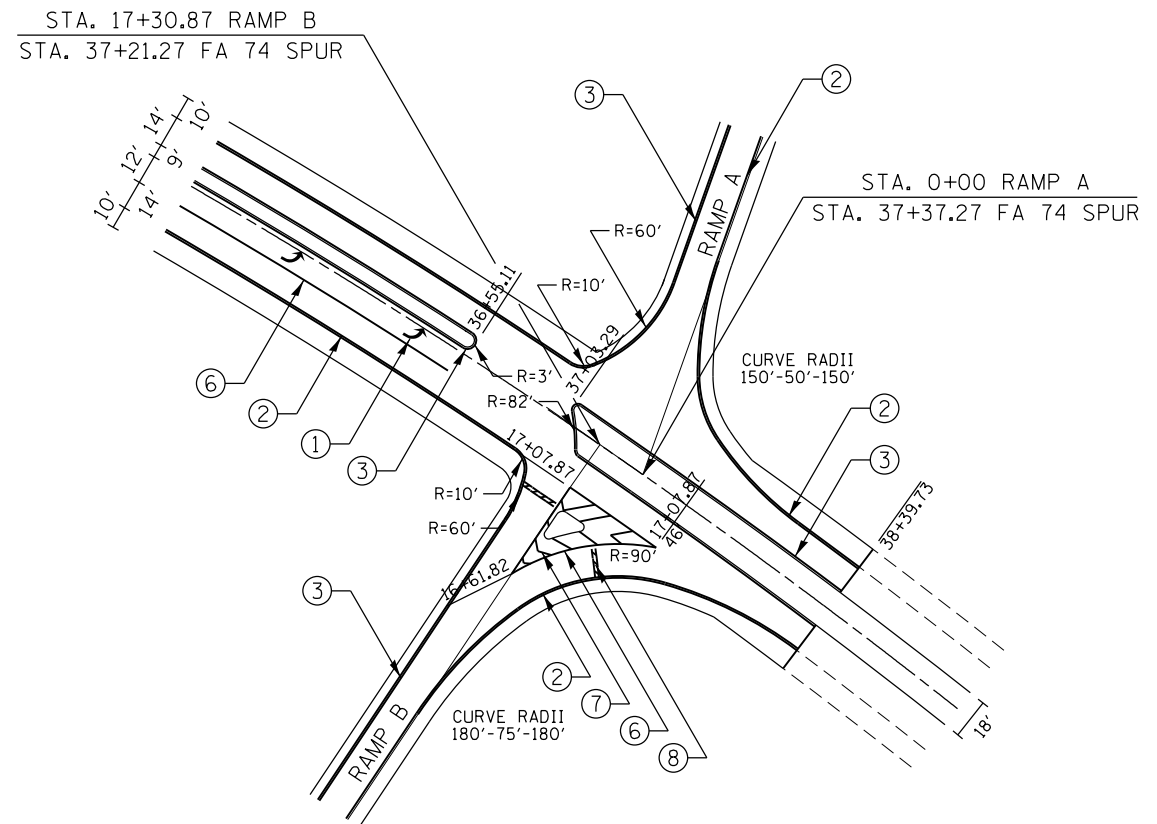
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

| | | |
|---------------------------|-------------------------|--------------|
| I-155 LINE DIAGRAM | | |
| NOT TO SCALE | SHEET NO. 1 OF 1 SHEETS | STA. TO STA. |

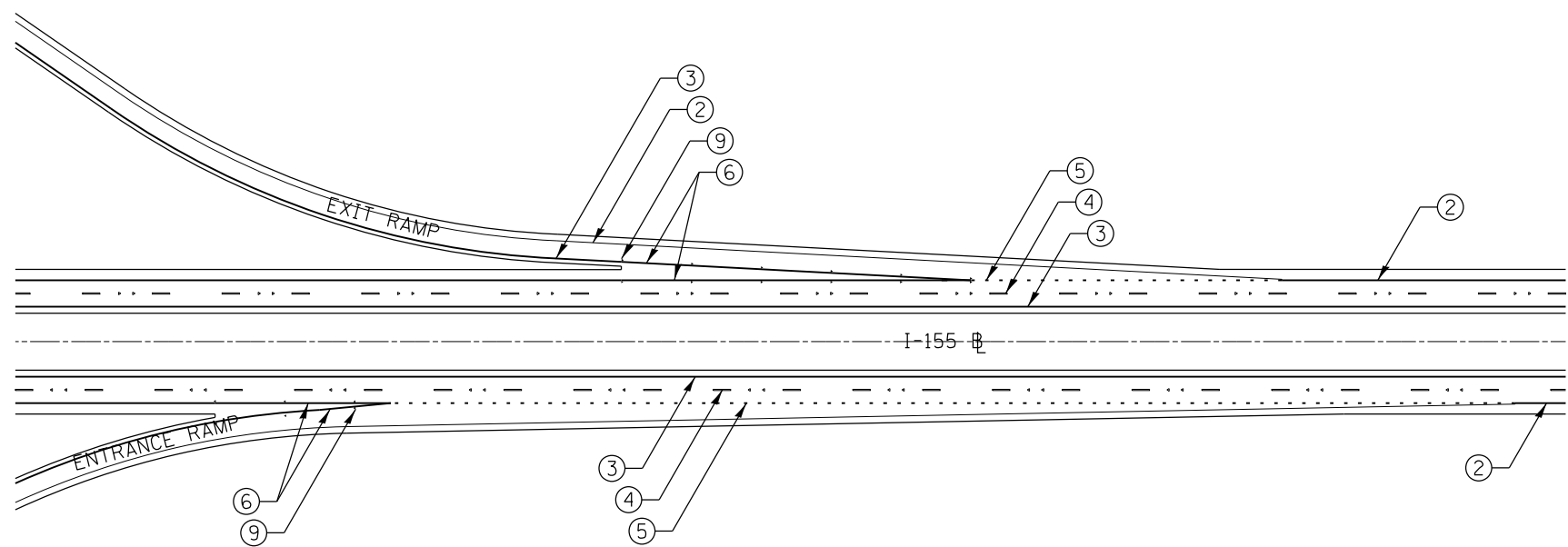
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|---------------------------|----------|--------------|-----------|
| 155 | 90(108,109,109-1,110)RS-2 | TAZEWELL | 45 | 27 |
| CONTRACT NO. 68A77 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



I-155 AND IL 122 N JCT INTERCHANGE
RAMPS C AND D



I-155 AND IL 122 N JCT INTERCHANGE
RAMPS A AND B

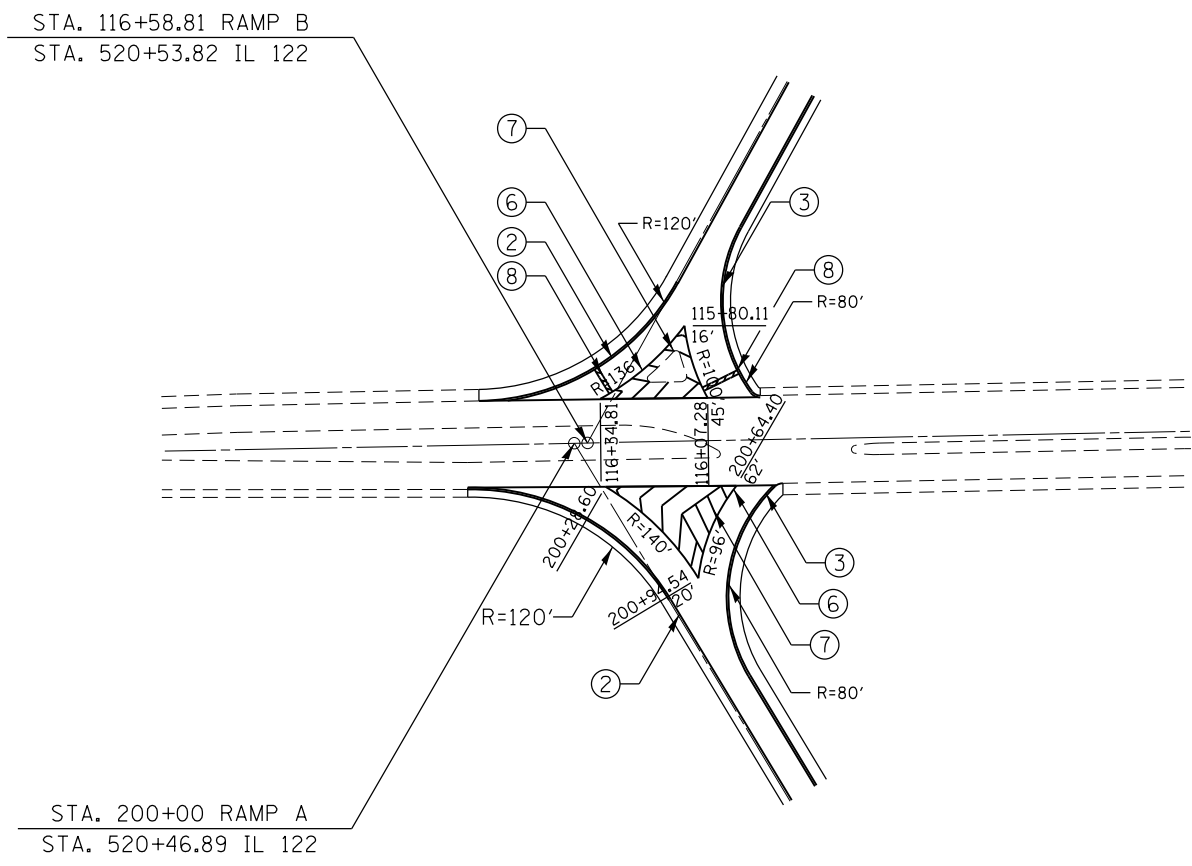


I-155 AND IL 122 N JCT INTERCHANGE
EXIT AND ENTRANCE RAMPS PAVEMENT MARKINGS

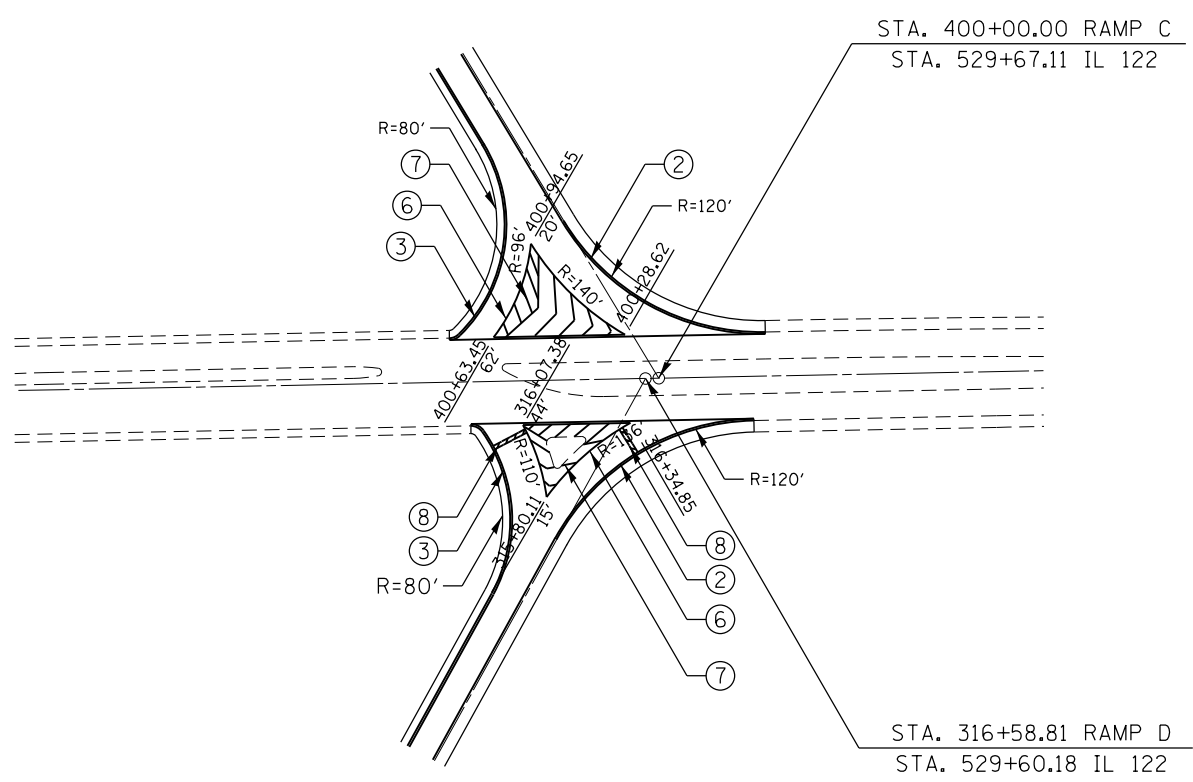
| LEGEND | |
|--------|------------------------|
| ① | LETTERS AND SYMBOLS |
| ② | 4" SOLID (WHITE) |
| ③ | 4" SOLID (YELLOW) |
| ④ | 6" SKIP DASH (WHITE) |
| ⑤ | 8" SKIP DASH (WHITE) |
| ⑥ | 8" SOLID (WHITE) |
| ⑦ | 12" DIAGONAL (WHITE) |
| ⑧ | 24" STOP BAR |
| ⑨ | ONE-WAY CRYSTAL MARKER |

NOTE:
ALL STATIONS AND OFFSETS ARE APPROXIMATE. RESIDENT ENGINEER SHALL CONTACT BUREAU OF OPERATIONS PRIOR TO PLACEMENT OF PAVEMENT MARKINGS ON INTERCHANGES.

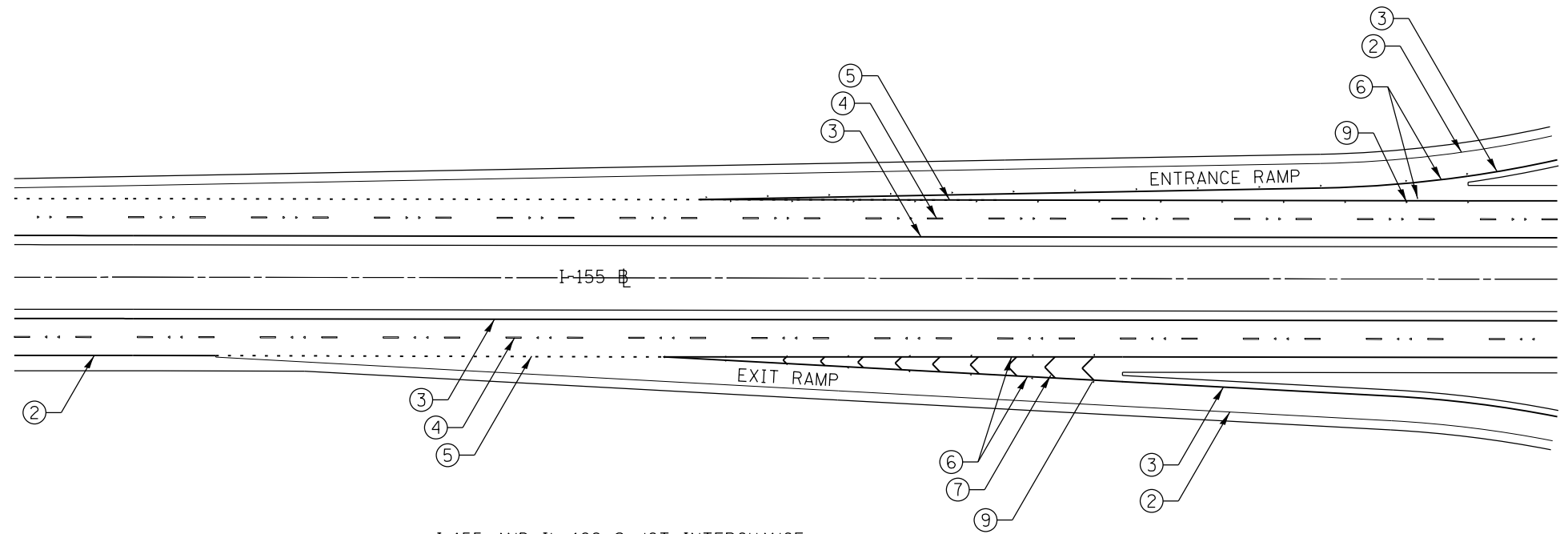
| LEGEND | |
|--------|------------------------|
| ① | LETTERS AND SYMBOLS |
| ② | 4" SOLID (WHITE) |
| ③ | 4" SOLID (YELLOW) |
| ④ | 6" SKIP DASH (WHITE) |
| ⑤ | 8" SKIP DASH (WHITE) |
| ⑥ | 8" SOLID (WHITE) |
| ⑦ | 12" DIAGONAL (WHITE) |
| ⑧ | 24" STOP BAR |
| ⑨ | ONE-WAY CRYSTAL MARKER |



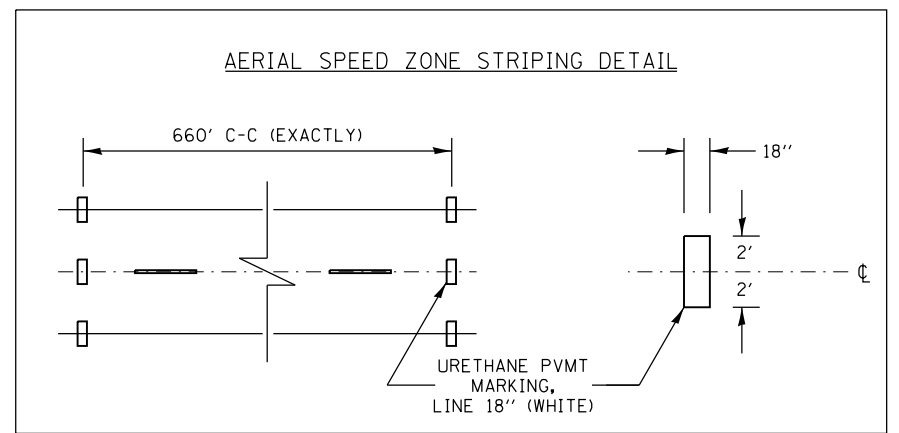
I-155 AND IL 122 S JCT INTERCHANGE
RAMPS A AND B



I-155 AND IL 122 S JCT INTERCHANGE
RAMPS C AND D

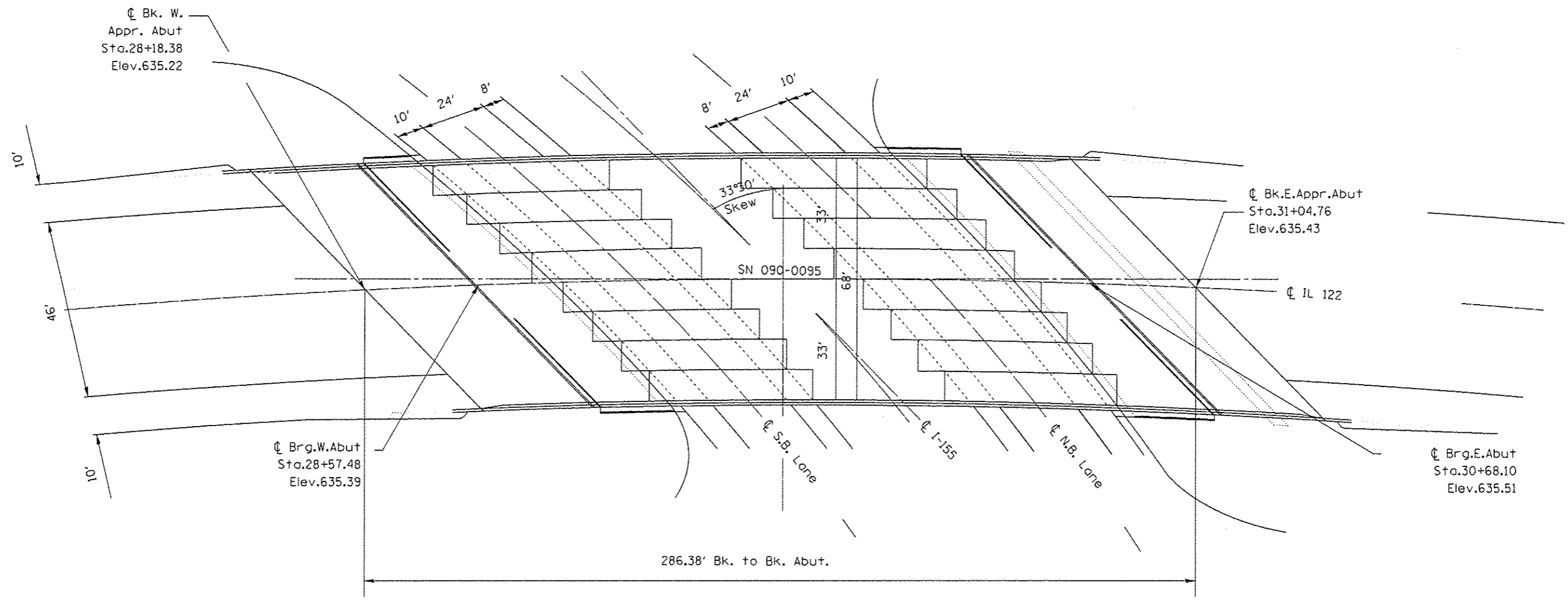


I-155 AND IL 122 S JCT INTERCHANGE
EXIT AND ENTRANCE RAMPS PAVEMENT MARKINGS



NOTE:
ALL STATIONS AND OFFSETS ARE APPROXIMATE. RESIDENT ENGINEER SHALL CONTACT BUREAU OF OPERATIONS PRIOR TO PLACEMENT OF PAVEMENT MARKINGS ON INTERCHANGES.

| | | | | | | | | | | | | |
|-----------------------------------|-------------------------------|------------|-----------|---|--------------------------------|-------------------------|------|-----------------|-------------------------------|-----------------|-----------------|--------------|
| FILE NAME = 68A77 I-155 Plans.dgn | USER NAME = keithbr | DESIGNED - | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | PAVEMENT MARKING DETAIL | | | F.A.I. RTE. 155 | SECTION 90(109,109-1,110)RS-2 | COUNTY TAZEWELL | TOTAL SHEETS 45 | SHEET NO. 29 |
| | PLOT SCALE = 100.0000' / 1" = | DRAWN - | REVISED - | | NOT TO SCALE | SHEET NO. 6 OF 6 SHEETS | STA. | TO STA. | CONTRACT NO. 68A77 | | | |
| | PLOT DATE = 12/11/2014 | CHECKED - | REVISED - | | ILLINOIS FED. AID PROJECT | | | | | | | |
| | | DATE - | REVISED - | | | | | | | | | |



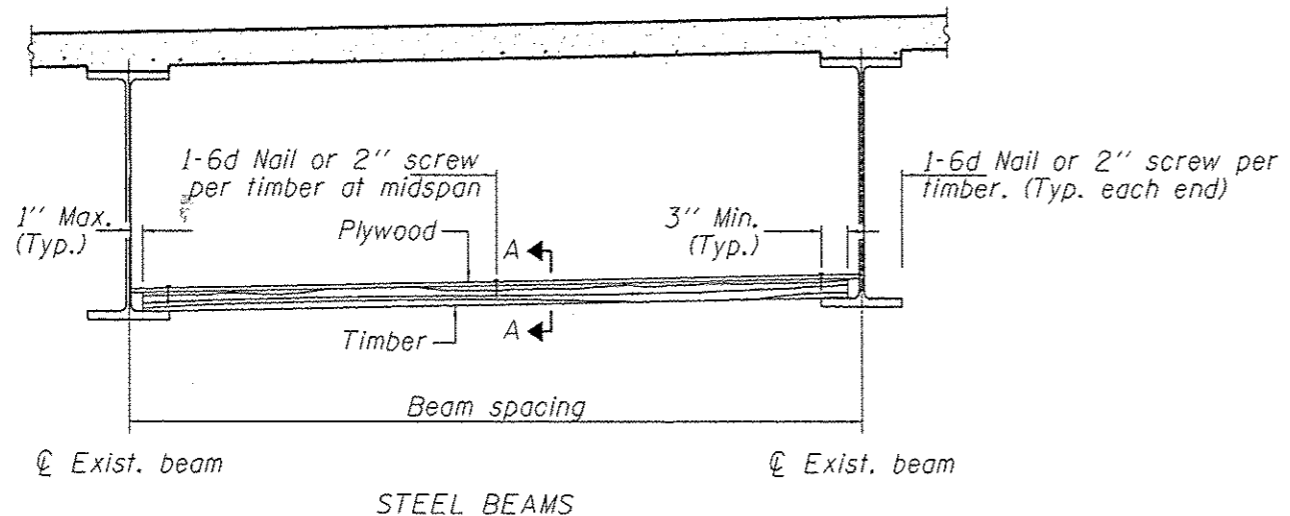
PLAN VIEW



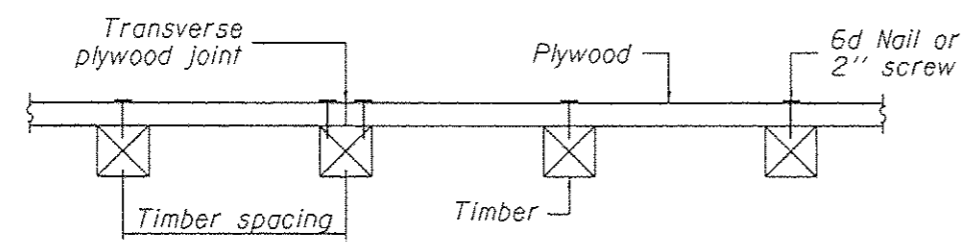
David Carl Puzey 1/21/15
Expires 1/30/16

NOTE:
PROTECTIVE SHIELD SHALL EXTEND OVER NB AND SB LANES, AND HMA SHOULDERS (INSIDE AND OUTSIDE)

| | | | | | | | | | | | | |
|------------------------------------|----------------------------|----------------|---------|---|---|-------------------------|------|-----------------------|--------------------------------|--------------------|---------------------------|-----------------|
| FILE NAME 68A77 1-155 Plans.dgn | USER NAME kcolibr | DESIGNED MM | REVISED | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | PROTECTIVE SHIELD DETAIL S.N. 090-0095 | | | F.A.I. RTE. 155 | SECTION 90109.109-1.110RS-2 | COUNTY TAZEWELL | TOTAL SHEETS 45 | SHEET NO. 31 |
| | PLOT SCALE 1/8" = 1'-0" | CHECKED VHV | REVISED | | NOT TO SCALE | SHEET NO. 1 OF 2 SHEETS | STA. | TO STA. | CONTRACT NO. 68A77 | | ILLINOIS FED. AID PROJECT | |



STEEL BEAMS



SECTION A-A

BILL OF MATERIALS

| Item | Unit | Total |
|-------------------------------|---------|-------|
| Protective Shield (Permanent) | Sq. Yd. | 765 |

TIMBER SPACING

| Beam Spacing (ft.) | Timber Sizes (in.) | | |
|--------------------|---|---|---|
| | 4" x 4" with min. Fb=775 psi Fv=135 psi | 4" x 6" with min. Fb=775 psi Fv=135 psi | 6" x 6" with min. Fb=575 psi Fv=125 psi |
| | Maximum Timber Spacing (in.) | | |
| 4.5 | 16 | 16 | 16 |
| 4.75 | 16 | 16 | 16 |
| 5.0 | 16 | 16 | 16 |
| 5.25 | 16 | 16 | 16 |
| 5.5 | 16 | 16 | 16 |
| 5.75 | 16 | 16 | 16 |
| 6.0 | 16 | 16 | 16 |
| 6.25 | 12 | 16 | 16 |
| 6.5 | 12 | 16 | 16 |
| 6.75 | 12 | 16 | 16 |
| 7.0 | 8 | 16 | 16 |
| 7.25 | 8 | 16 | 16 |
| 7.5 | 8 | 16 | 16 |
| 7.75 | 8 | 16 | 16 |
| 8.0 | 8 | 12 | 16 |
| 8.25 | 8 | 12 | 16 |
| 8.5 | 6 | 12 | 12 |
| 8.75 | 6 | 12 | 12 |
| 9.0 | 6 | 8 | 12 |

Notes:

Timber sizes shown are nominal sizes. Rough sawn timber of the dimensions shown will also be considered acceptable.

The minimum Fb and Fv values shown are the tabulated design values given in the National Design Specification for Wood Construction for No. 2 Spruce-Pine-Fir without adjustment factors applied. Better grades or other species with equal or higher allowable stresses will also be considered acceptable.

The timber spacings shown have been determined using allowable stresses with all adjustment factors necessary for the anticipated service conditions.

All timber shall be treated.

Plywood shall be 5/8" Exterior type plywood. **by APA.**

Plywood shall be placed such that the face grain is perpendicular to the timber supports. When less than a full sheet (4' width) of plywood is used, the width of the strip used shall not be less than 2'.

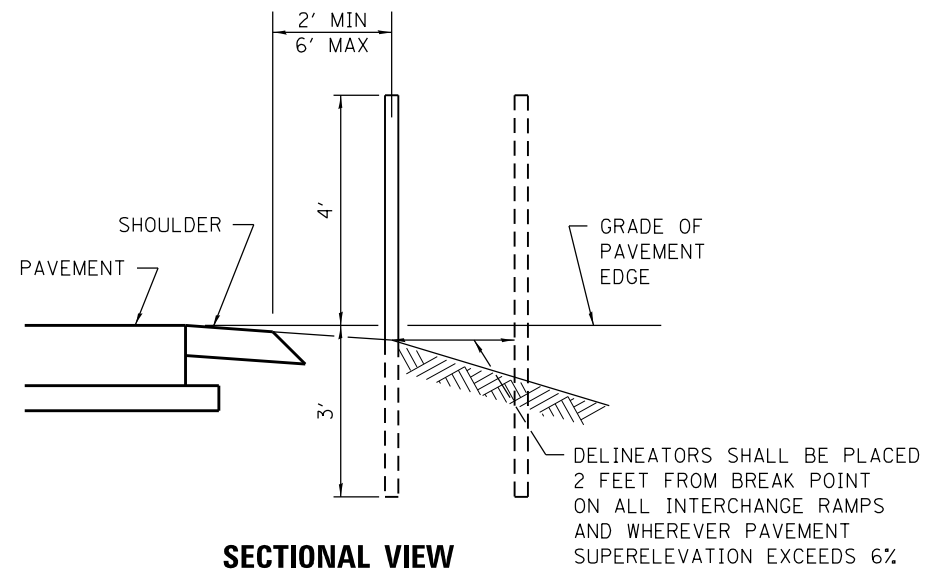
Transverse plywood joints shall be supported by timbers.

When 4" x 6" timbers are used, they shall be placed such that the wide face is horizontal and the narrow face is vertical.

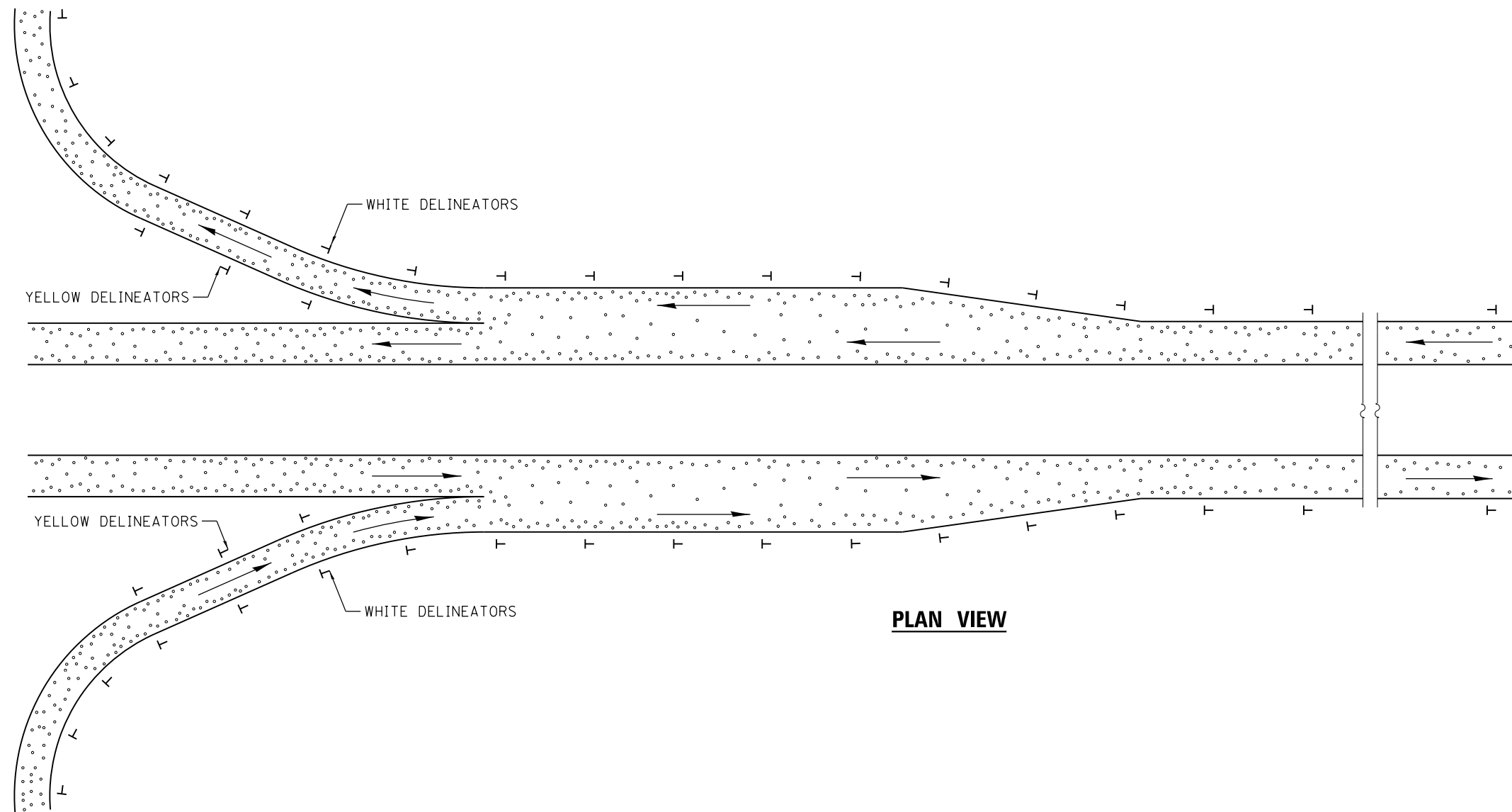
Design load = 200 psf.

See special provision for Permanent Protective Shield System.

**SPACING FOR
RECOVERABLE DELINEATORS
ON HORIZONTAL CURVES**



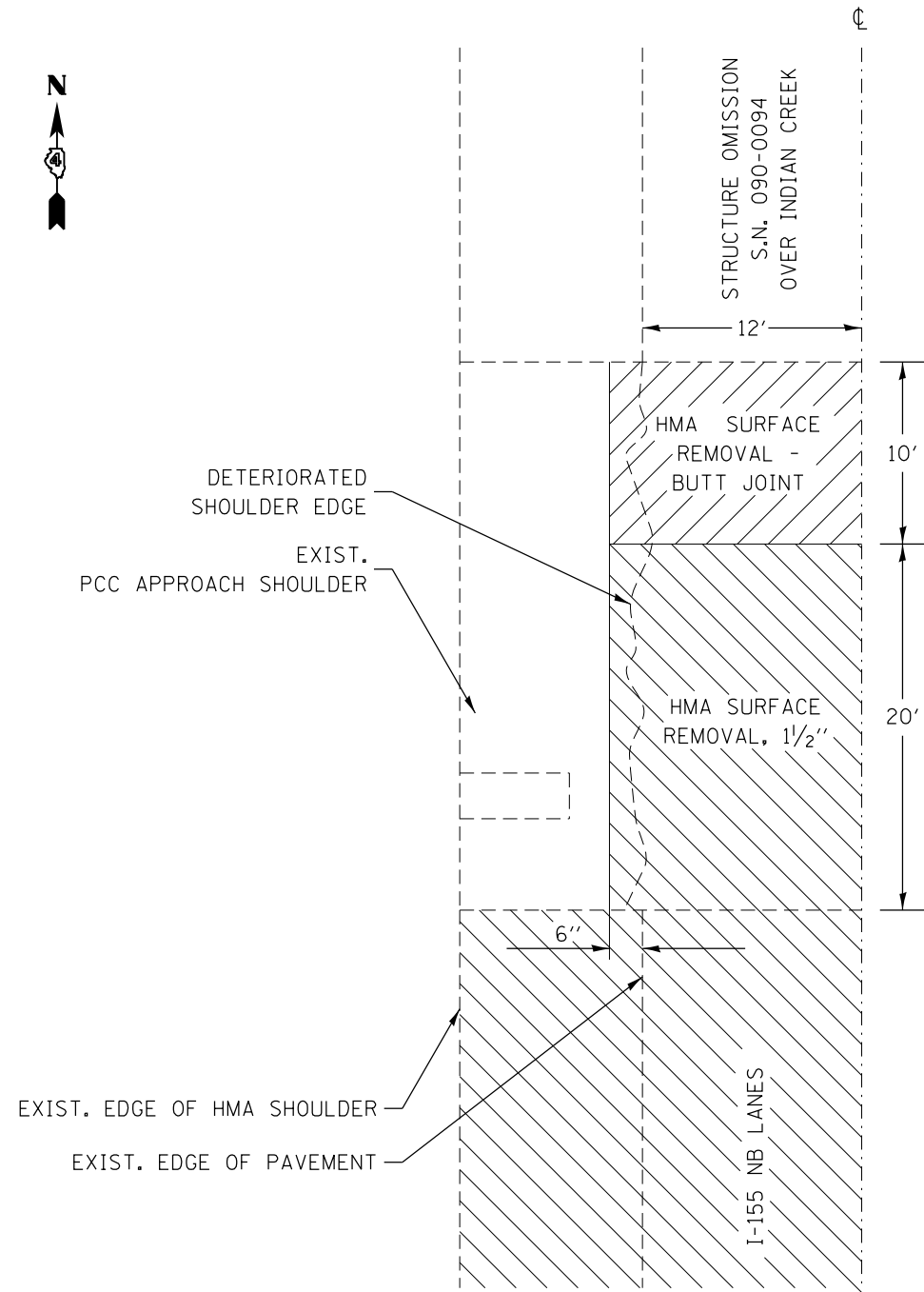
| INTERCHANGE | RADIUS OF CURVE (FEET) | SPACING ON CURVE (FEET) |
|---------------|------------------------|-------------------------|
| IL 122 N. JCT | RAMP A | 366 |
| | RAMP B | 760 |
| | RAMP C | 760 |
| | RAMP D | 366 |
| | RAMP D | 760 |
| IL 122 S. JCT | RAMP A | 763.94 |
| | RAMP B | 763.94 |
| | RAMP C | 763.94 |
| | RAMP D | 763.94 |
| | RAMP D | 763.94 |
| MAINLINE | CURVE 1 | 2654.79 |
| | CURVE 2 | 2864.79 |



GENERAL NOTES

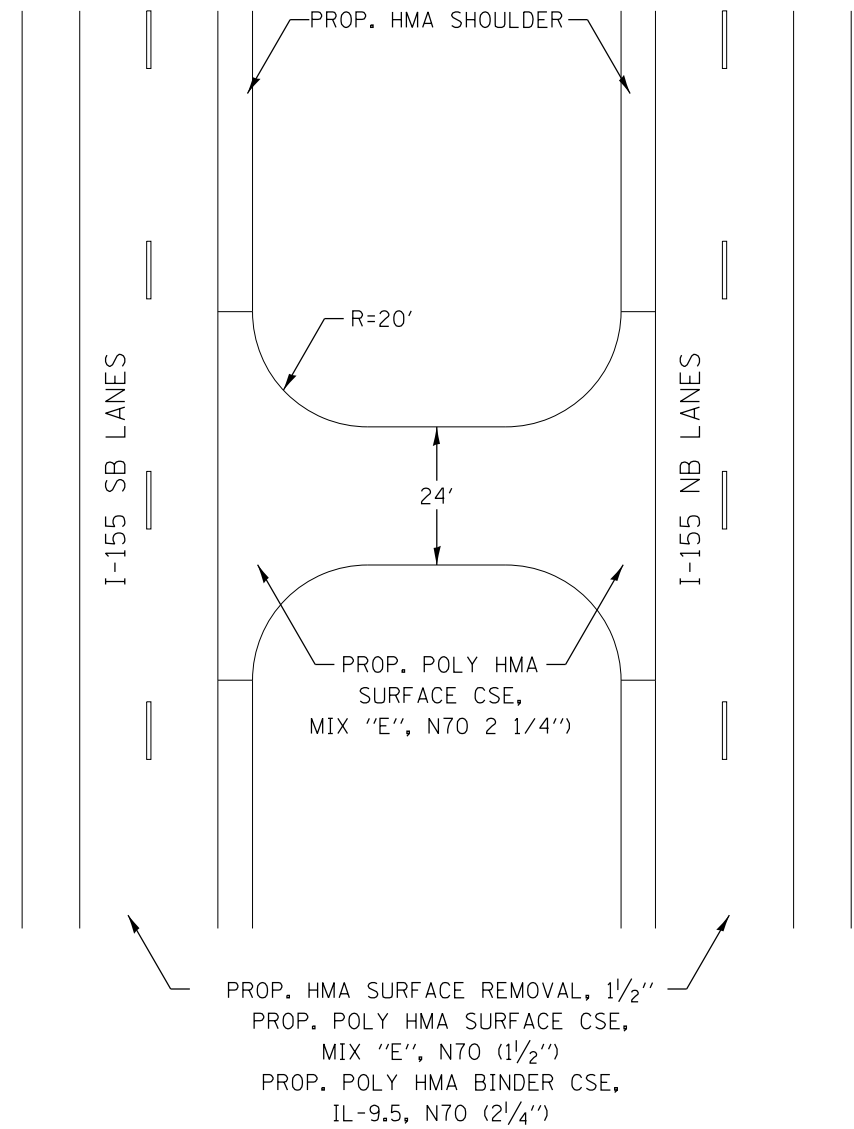
DELINEATORS ON MAINLINE SHALL BE PLACED AT 400' SPACING ON TANGENT SECTIONS, AND 200' SPACING ON CURVED SECTIONS.

DELINEATORS ON RAMP AND ACCELERATION AND DECELERATION LANES SHALL BE PLACED AT A MAXIMUM SPACING OF 100'. IN ADDITION, DELINEATORS SHALL BE PLACED ON OUTSIDE OF ALL CURVED SECTIONS OF RAMP (SEE TABLE ABOVE).



NOTE:
 TO PRODUCE A CLEAN EDGE ON THE DETERIORATED PCC SHOULDER, MILLING SHALL EXTEND 6 INCHES BEYOND THE EDGE OF PAVEMENT AS SHOWN IN THE DETAIL. THIS WORK WILL BE INCLUDED IN THE COST OF HMA SURFACE REMOVAL - BUTT JOINT OR HMA SURFACE REMOVAL, 1 1/2".

MILLING DETAIL BRIDGE APPROACH S.N. 090-0094 OVER INDIAN CREEK



MEDIAN CROSS-OVER DETAIL

FILE NAME = 68A77 1-155 Plans.dgn

USER NAME = keithbr

DESIGNED -

REVISED -

DRAWN -

REVISED -

PLOT SCALE = 100.0000' / 1in.

CHECKED -

REVISED -

PLOT DATE = 12/11/2014

DATE -

REVISED -

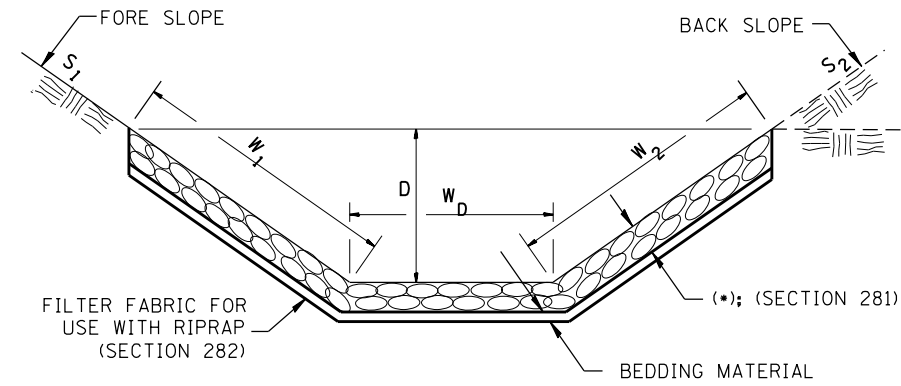
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

BRIDGE APPROACH MILLING DETAIL (S.N. 090-0094 OVER INDIAN CREEK)
 MEDIAN CROSS-OVER DETAIL

NOT TO SCALE SHEET 1 OF 1 SHEETS STA. TO STA.

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-----------------------|----------|--------------|-----------|
| 155 | 90(109,109-1,110)RS-2 | TAZEWELL | 45 | 34 |
| CONTRACT NO. 68A77 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

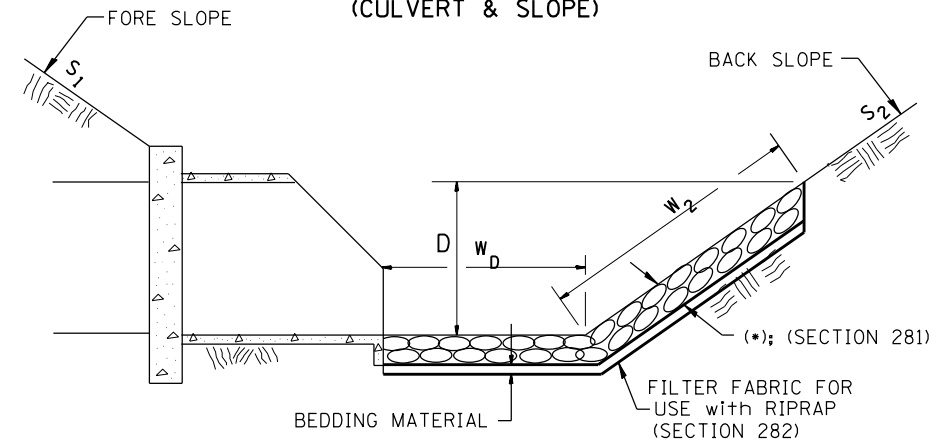
**CASE 1
(DITCH)**



| (*) | | | |
|--------------------|---------------|---------------|------------------|
| LOCATION | WIDTH (1) | LENGTH | RIPRAP |
| STA TO STA | lin ft (m) | lin ft (m) | tons (m tons) |
| 1039+31 TO 1049+96 | 14 | 24 | 50 |
| TOTAL | | | 50 |

(1) WIDTH = $W_1 + W_2 + W_D$

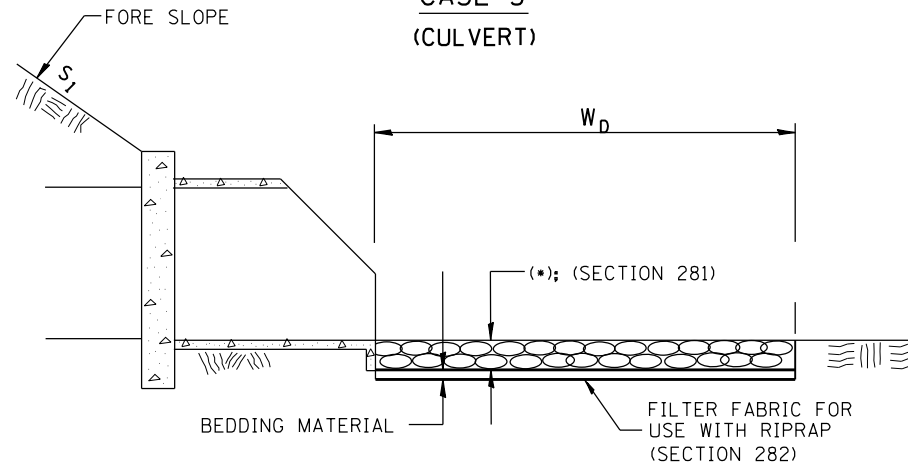
**CASE 2
(CULVERT & SLOPE)**



| (*) | | | | |
|------------|---------------|---------------|------------------|-----------------------------|
| LOCATION | WIDTH (1) | LENGTH | RIPRAP | FABRIC |
| STA TO STA | lin ft (m) | lin ft (m) | tons (m tons) | sq yds (m ²) |
| | | | | |
| | | | | |
| | | | | |
| TOTAL | | | | |

(1) WIDTH = $W_2 + W_D$

**CASE 3
(CULVERT)**



| (*) | | | | |
|------------|---------------|---------------|------------------|-----------------------------|
| LOCATION | WIDTH (1) | LENGTH | RIPRAP | FABRIC |
| STA TO STA | lin ft (m) | lin ft (m) | tons (m tons) | sq yds (m ²) |
| | | | | |
| | | | | |
| | | | | |
| TOTAL | | | | |

(1) WIDTH = W_D

All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).

All dimensions are in inches (millimeters) unless otherwise noted.

| | | |
|----------|--|------|
| 1-1-97 | RENUM. A-12.02, NEW REVISION BOX | T.P. |
| 12-1-97 | CORRECT FILTER FABRIC LEADER ARROW | J.A. |
| 10-16-06 | REVISED TO 2007 SPEC. | M.A. |
| 9-6-12 | REMOVED A DESIGNER NOTE AND MADE MINOR CHANGES | R.D. |

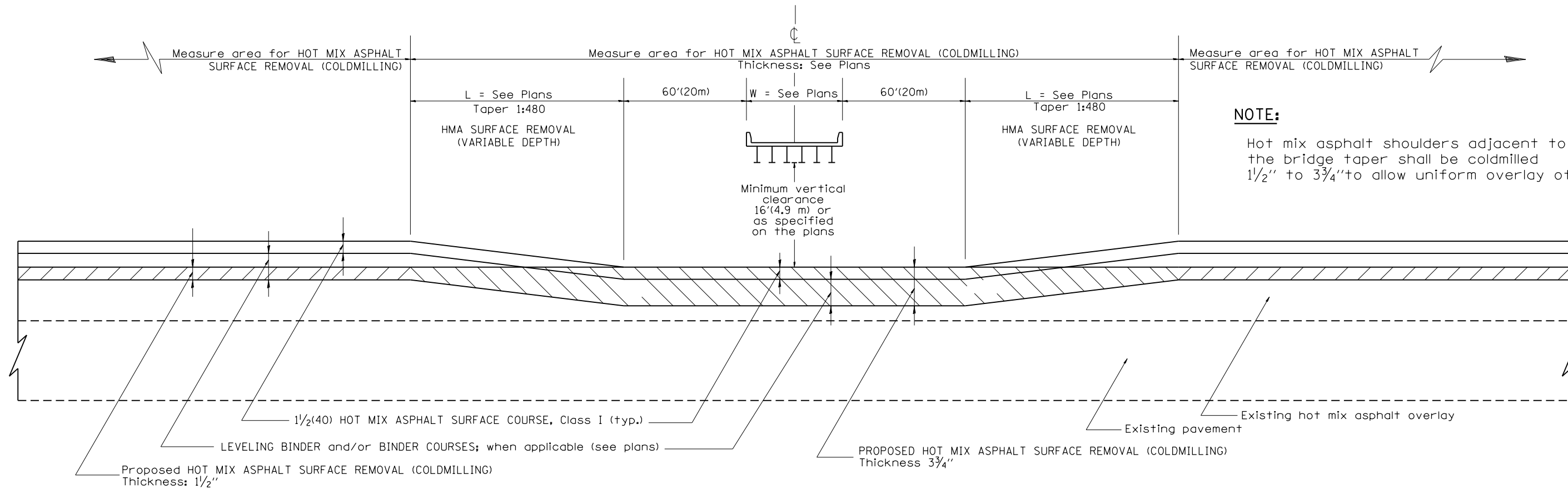
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

RIPRAP DITCH FOR EROSION PROTECTION

NOT TO SCALE

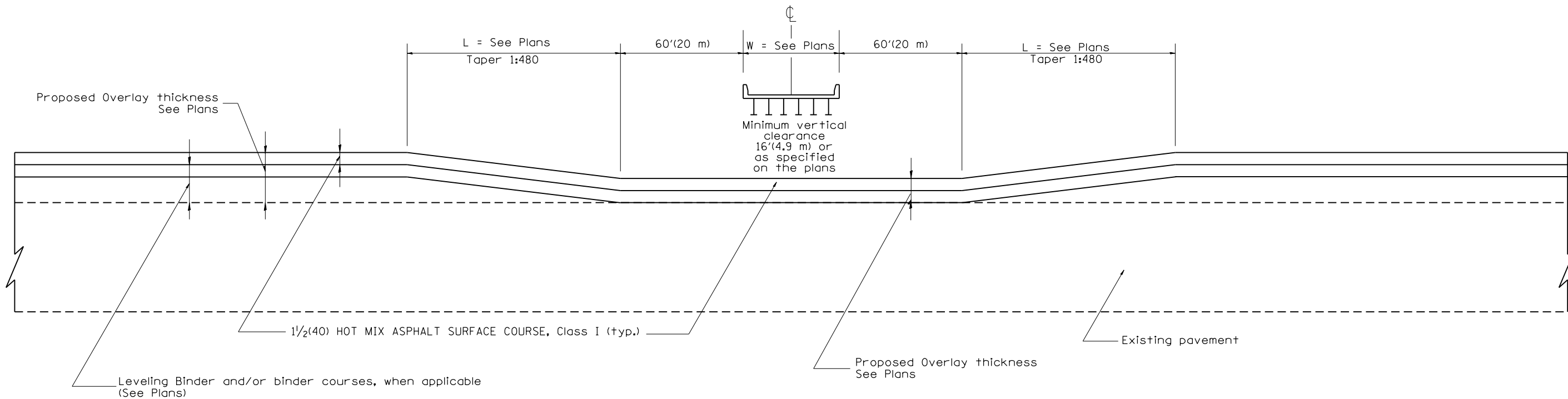
CADD STD. 281001-D4

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|-----------------------|----------|--------------|-----------|
| 155 | 90(109,109-1,110)R5-2 | TAZEWELL | 45 | 35 |
| CONTRACT NO. 68A77 | | | | |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | |



NOTE:
Hot mix asphalt shoulders adjacent to the bridge taper shall be coldmilled 1/2" to 3/4" to allow uniform overlay of 3/4".

CASE 1: WITH HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)



CASE 2: NO HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)

All dimensions are in inches (millimeters) unless otherwise noted.

| | | |
|----------|----------------------------------|------|
| 01-01-97 | RENUM. C-23.03, NEW REVISION BOX | T.P. |
| 10-16-06 | REVISED TO 2007 SPEC. | M.A. |

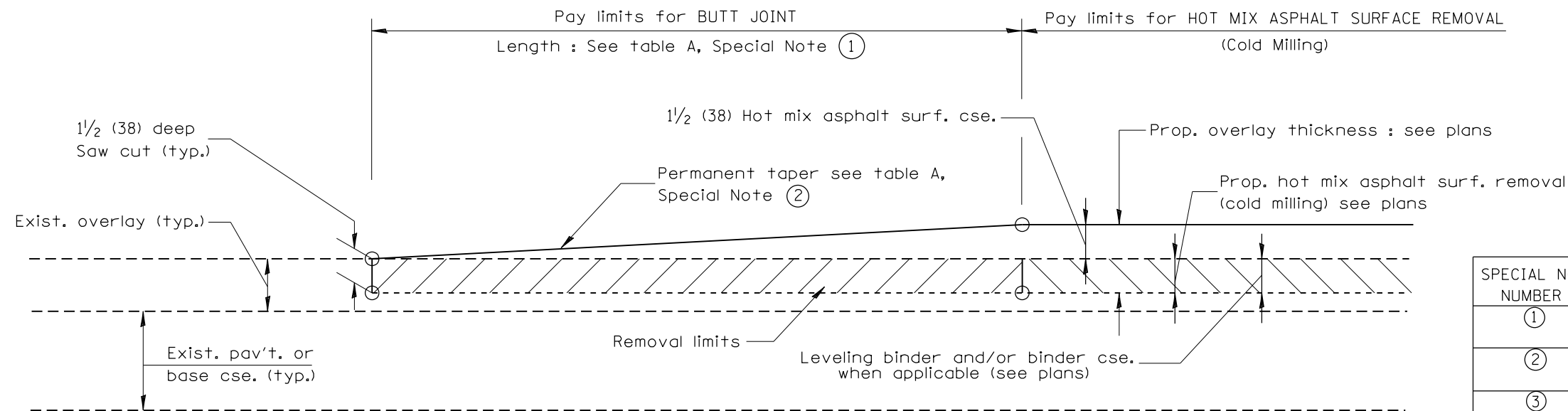
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT OVERLAY UNDER STRUCTURES
SPECIAL TREATMENT**

NOT TO SCALE

CADD STD. 406001-D4

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|-----------------------|----------|--------------|-----------|
| 155 | 90(109,109-1,110)R5-2 | TAZEWELL | 45 | 36 |
| CONTRACT NO. 68A77 | | | | |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | |



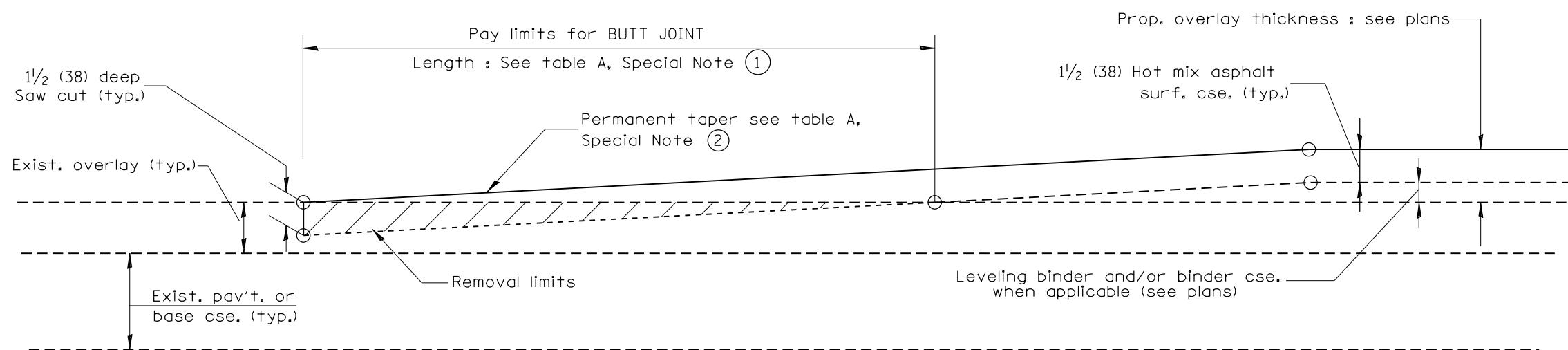
CASE 1 : WITH HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)

TABLE A
(LENGTHS AND TAPER RATES)

| SPECIAL NOTE NUMBER | ELEMENT | MAINLINE INTERSTATES & 4-LANE EXPRESSWAYS | ALL OTHERS |
|---------------------|---------------------------|---|------------|
| ① | LENGTH OF BUTT JOINT | 60'(18.0 m) | 30'(9.0 m) |
| ② | PERMANENT TAPER RATE | 1:480 | 1:240 |
| ③ | TEMPORARY RAMP TAPER RATE | 1:80 | 1:40 |
| ④ | TEMPORARY RAMP LENGTH | 10'(3.0 m) | 5'(1.5 m) |
| ⑤ | LENGTH OF BUTT JOINT | 10'(3.0 m) | 10'(3.0 m) |

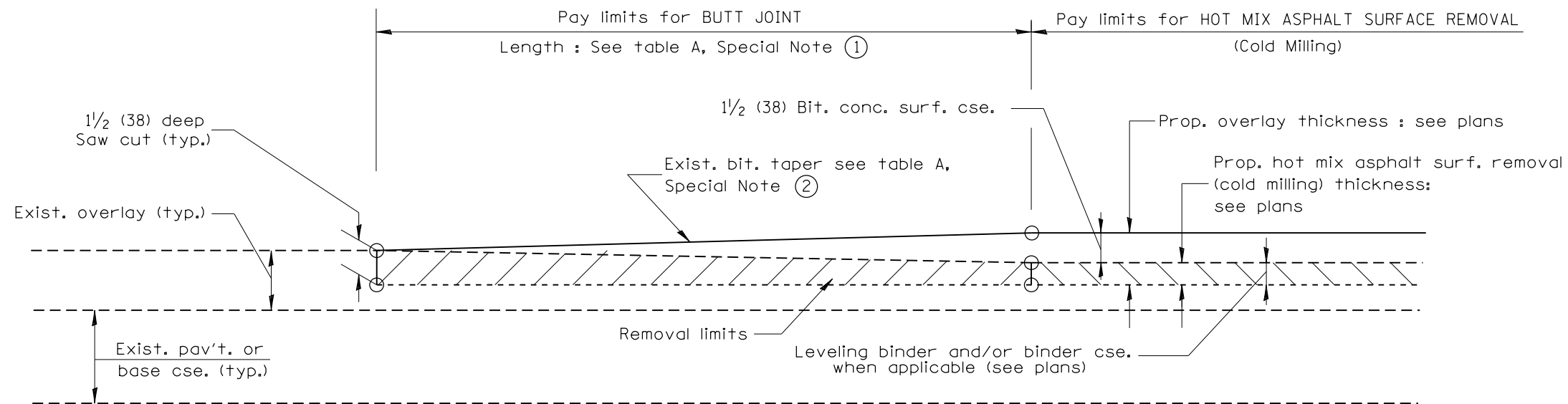
GENERAL NOTES

1. The work shall be done in accordance with Article 406.08 and the Special Provision for Butt Joints.
2. The pavement surface to be removed may be either bituminous or P.C. concrete. The work shall be performed in accordance with Article 440.04 and the Special Provisions for Butt Joints.
3. The saw cut joints shall be primed just prior to the placing of bituminous material. The work will be in accordance with the applicable portions of Article 406.05.

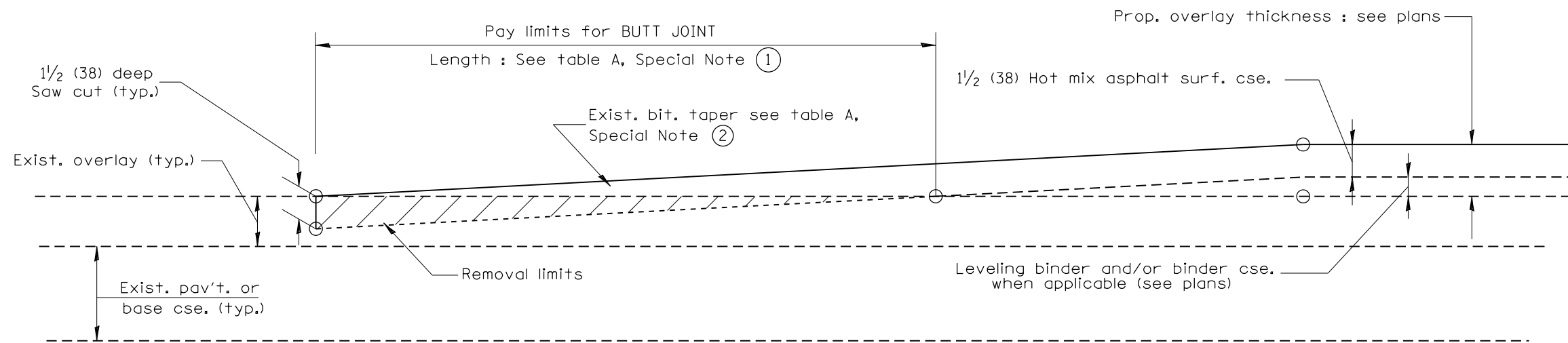


CASE 2 : NO HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)

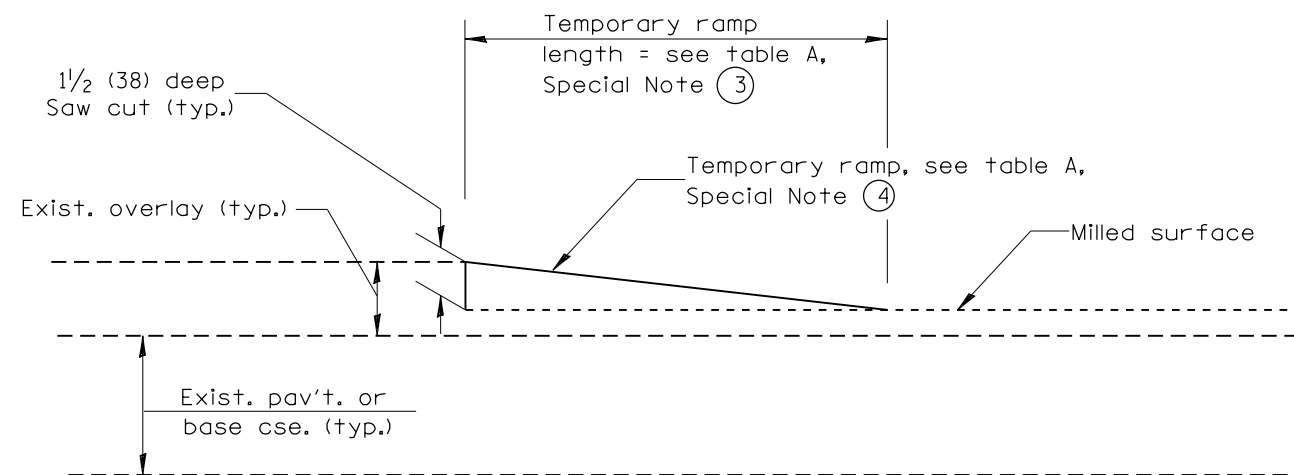
All dimensions are in inches (millimeters) unless otherwise noted.



CASE 3 : WITH HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)
TIE-IN TO EXISTING BITUMINOUS TAPER



CASE 4 : NO HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)
TIE-IN TO EXISTING BITUMINOUS TAPER



DETAIL TEMPORARY RAMP

All dimensions are in inches (millimeters) unless otherwise noted.

| | | | | | | | | | |
|--|--|--|--|--|--|--|--|--|--|
| | | | | | | | | | |
|--|--|--|--|--|--|--|--|--|--|

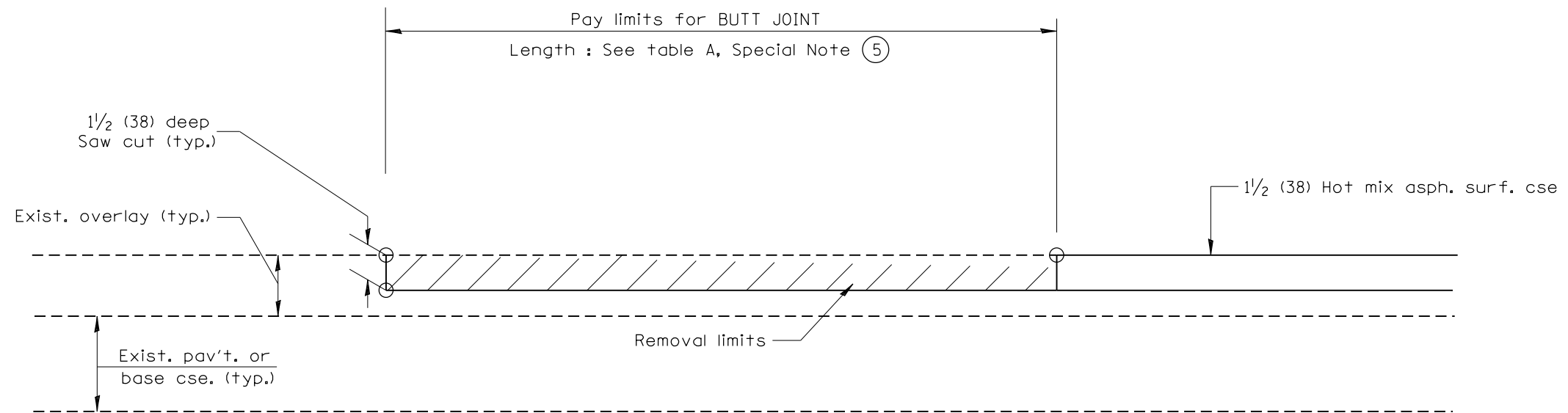
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINTS

NOT TO SCALE

SHT. 2 OF 3
 CADD STD. 406101-D4

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|-----------------------|----------|--------------------|-----------|
| 155 | 90(109,109-1,110)R5-2 | TAZEWELL | 45 | 38 |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 68A77 | |



CASE 5 : WITH HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)
TIE-IN TO EXISTING BITUMINOUS TAPER

All dimensions are in inches (millimeters) unless otherwise noted.

| | | | | | |
|--|--|--|--|--|--|
| | | | | | |
| | | | | | |
| | | | | | |
| | | | | | |

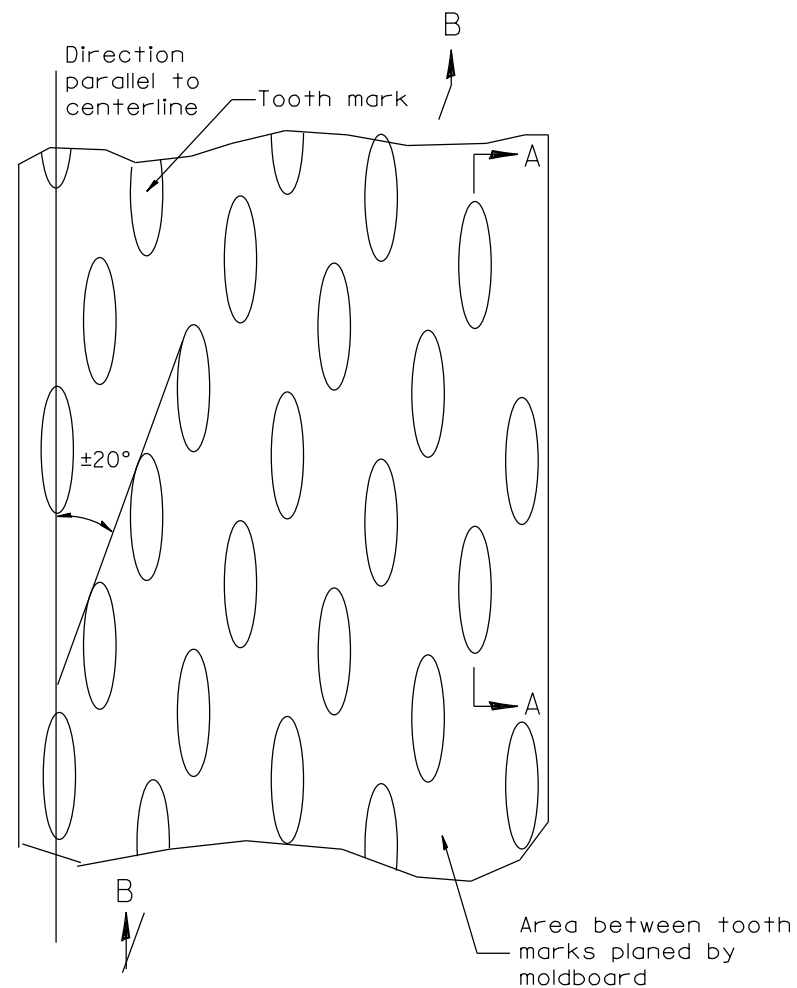
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINTS

NOT TO SCALE

SHT. 3 OF 3
CADD STD. 406101-D4

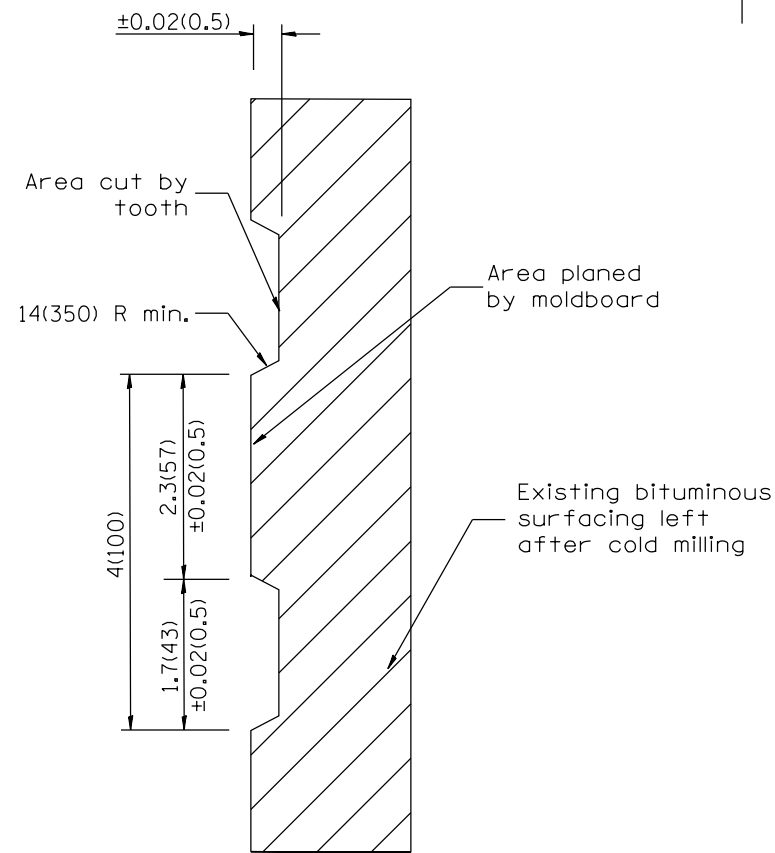
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|-----------------------|----------|--------------------|-----------|
| 155 | 90(109,109-1,110)R5-2 | TAZEWELL | 45 | 39 |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 68A77 | |



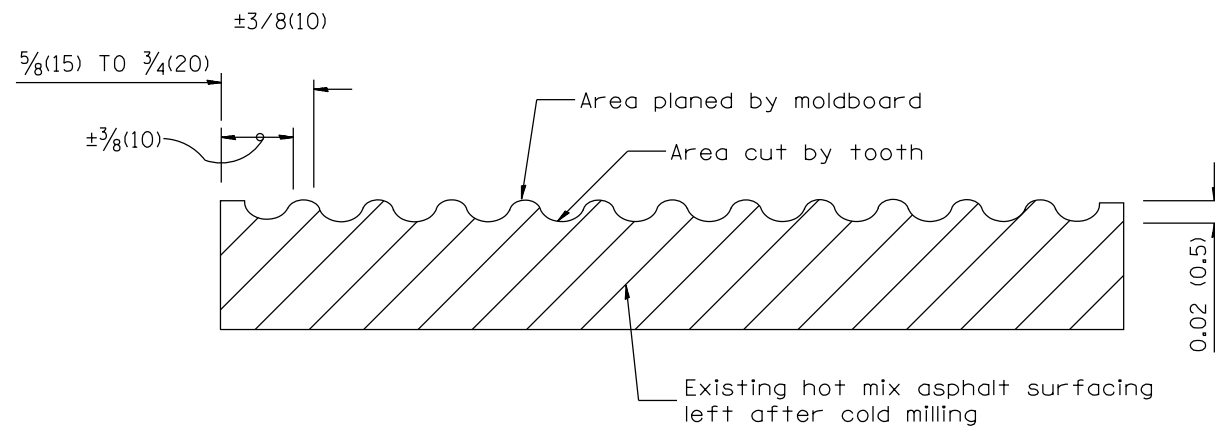
PLAN

General notes:

1. Coldmilling shall consist of two processes: Cutting with carbide teeth mounted on a rotating drum, and planing with a moldboard mounted immediately behind the cutting drum.
2. Other similar patterns will be acceptable if they consist of a smooth, flat, planed surface interspersed with a pattern of discontinuous longitudinal striations.



SECTION A-A



SECTION B-B PROJECTED PERPENDICULAR TO CENTERLINE

All dimensions are in inches (millimeters) unless otherwise noted.

| | | |
|----------|--------------------------------------|------|
| 01-01-97 | RENUM. C-104.01, NEW REVISION BOX | T.P. |
| 04-20-98 | REMOVED MILLING DETAIL FROM STANDARD | J.A. |
| 09-08-98 | CORRECT NOTE LEADER PLACEMENT | R.W. |
| 10-16-06 | REVISED TO 2007 SPEC. | M.A. |

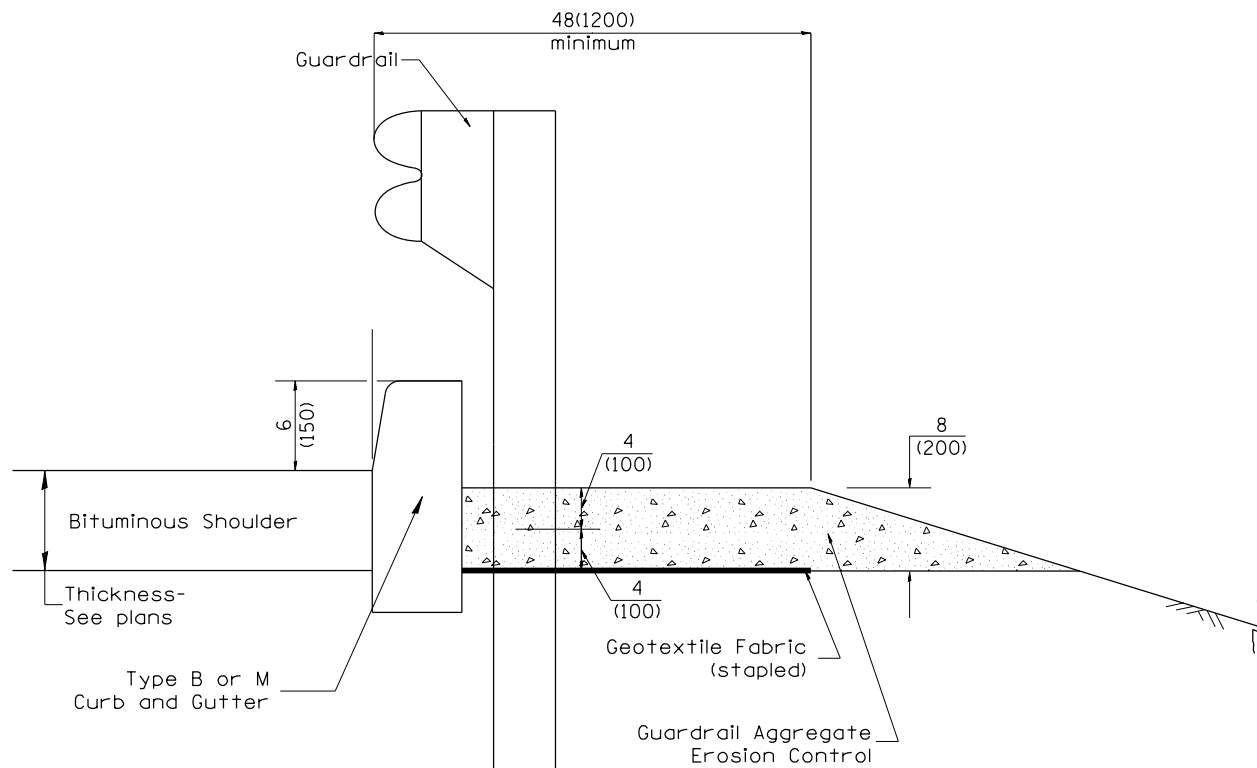
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

HOT MIX ASPHALT SURFACE REMOVAL (COLD MILLING)

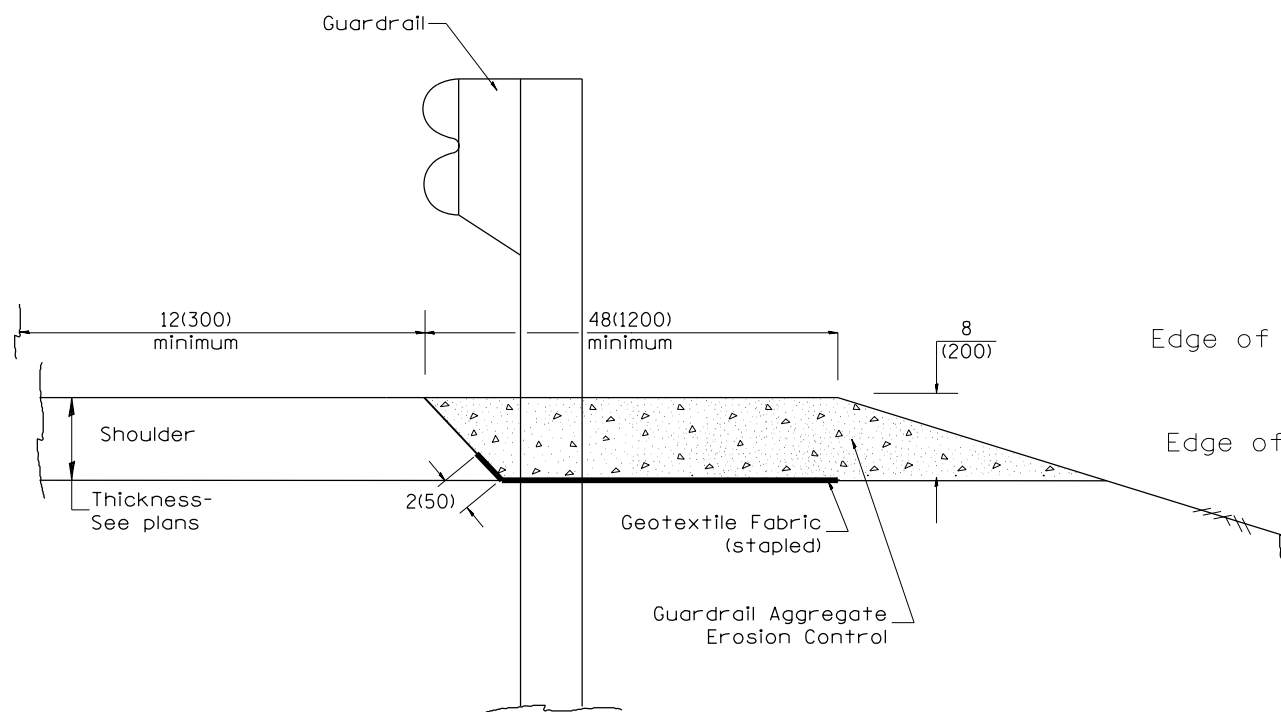
NOT TO SCALE

CADD STD. 440001-D4

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|-----------------------|----------|--------------------|-----------|
| 155 | 90(109,109-1,110)R5-2 | TAZEWELL | 45 | 40 |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 68A77 | |



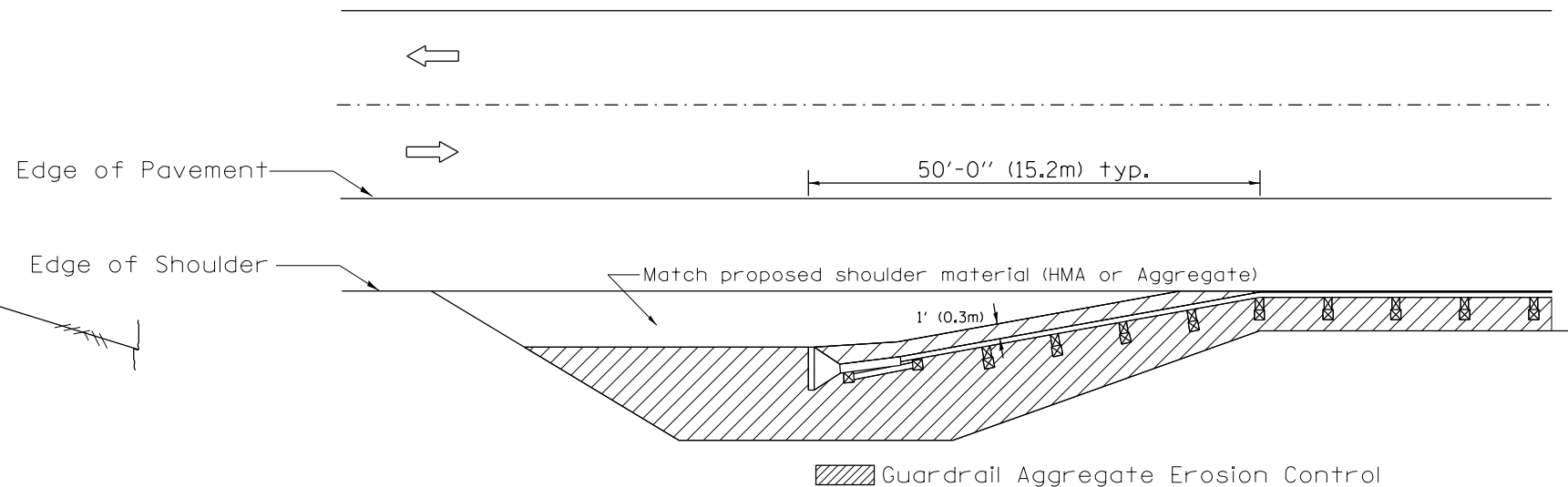
TYPICAL SECTION WITH EROSION CONTROL CURB



TYPICAL SECTION WITHOUT EROSION CONTROL CURB

GENERAL NOTES: GUARDRAIL AGGREGATE EROSION CONTROL

1. This work shall consist of grading as needed, furnishing and installing geotextile fabric and staples, and furnishing, placing and shaping crushed aggregate around and behind Steel Plate Beam Guardrail posts in accordance with Plan Details.
2. Before placing the aggregate and the Geotextile Fabric, weeds and grass shall be removed from the area to be covered.
3. After the area has been prepared, and in a dry condition, the Geotextile fabric shall be placed with a 12(300) minimum overlap. A knife cut for guardrail post installation is necessary.
4. The aggregate shall be deposited, compacted and shaped by either mechanical or hand methods, in a manner reasonably true to line and grade.
5. The Contractor shall have the option of placing the guardrail before or after the Geotextile Fabric and Aggregate are in place. If the guardrail is placed after the Geotextile Fabric and Aggregate, then any voids must be filled and the aggregate returned to line and grade.
6. Materials shall meet the following requirements:
 - A. The crushed aggregate shall be CA1 gradation in accordance with Article 1004.01(c) of the Standard Specifications.
 - B. The Geotextile Fabric shall be nonwoven fabric in accordance with Article 1080.02 of the Standard Specifications.



All dimensions are in inches (millimeters) unless otherwise noted.

| | | | | | |
|----------|-------------------------------------|------|---------|--------------------------------|------|
| 01-01-97 | RENUM. C-22.01, NEW REVISION BOX | T.P. | 3-7-11 | Added Detail showing plan view | R.D. |
| 03-01-97 | CORRECT STD. NUMBERS IN NOTES PG. 2 | J.A. | 8-10-12 | Revised curb "B" and aggregate | R.D. |
| 11-03-00 | CORRECTION TO NOTES | M.A. | | | |
| 10-16-06 | REVISED TO 2007 SPEC. | M.A. | | | |

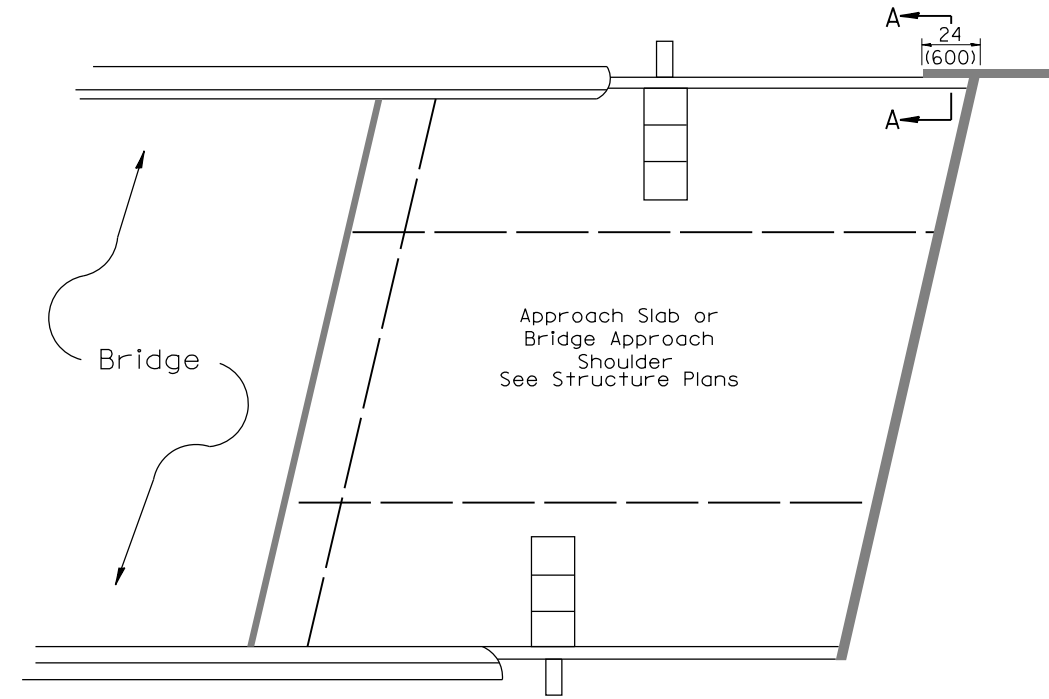
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

GUARDRAIL EROSION CONTROL TREATMENTS

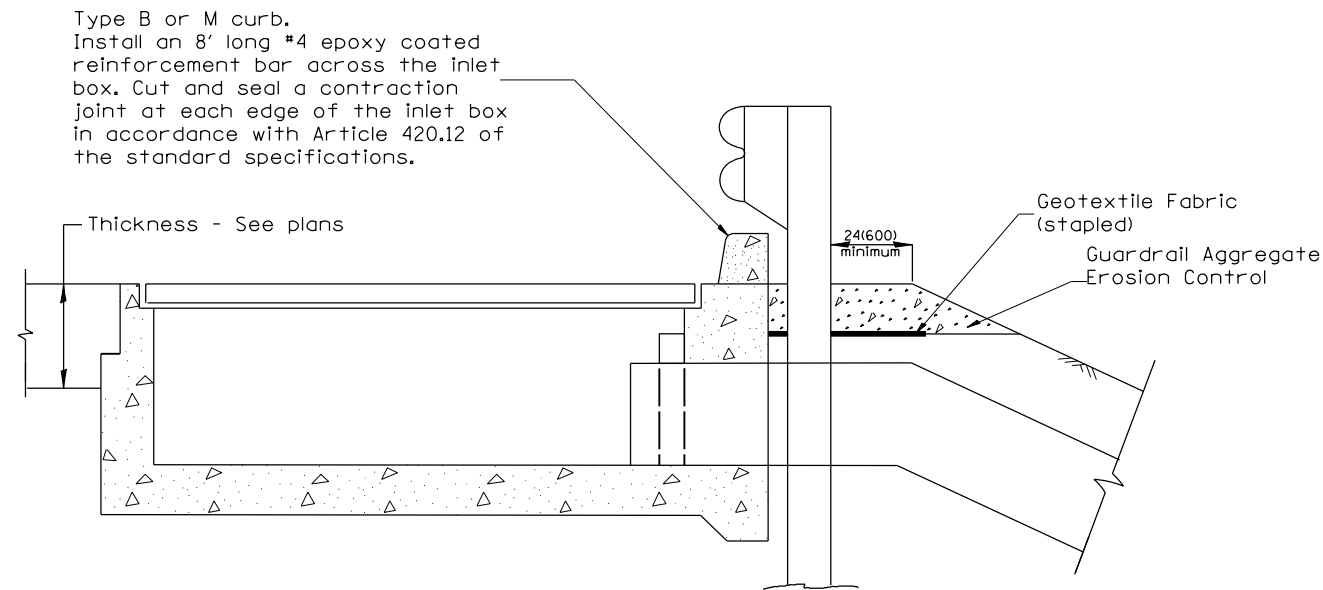
NOT TO SCALE

SHT. 1 OF 2
CADD STD. 630101-D4

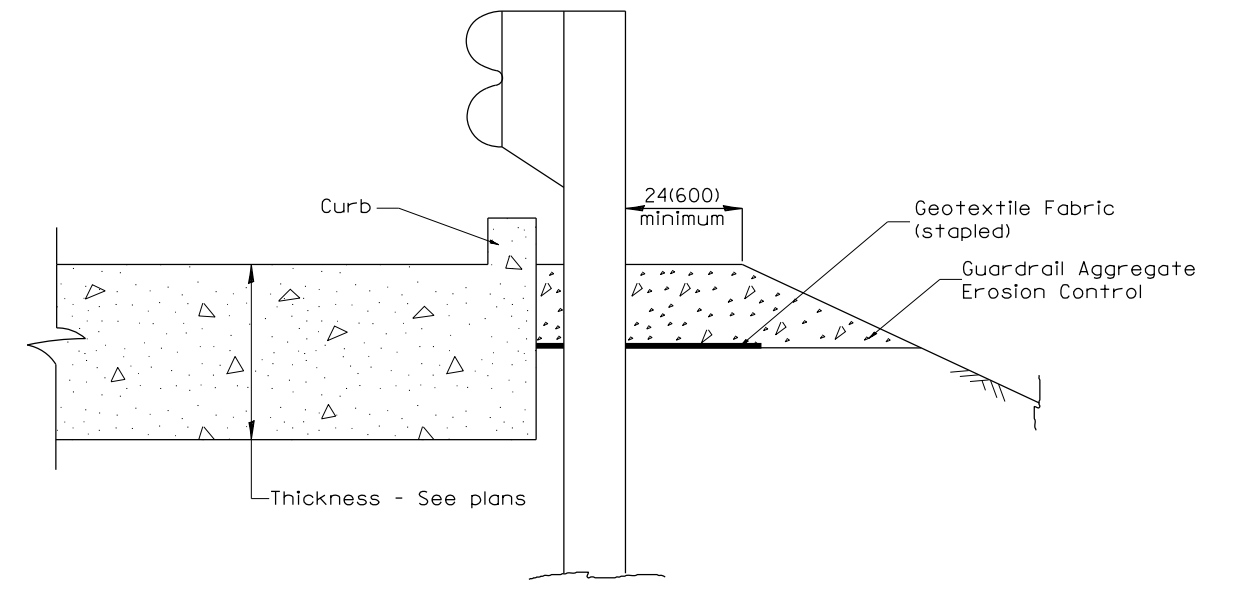
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|-----------------------|----------|--------------------|-----------|
| 155 | 90(109,109-1,110)R5-2 | TAZEWELL | 45 | 41 |
| FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 68A77 | |



PLAN VIEW
APPROACH SLAB OR BRIDGE APPROACH SHOULDER
 (STANDARD 609001 or 609006)



TYPICAL SECTION AT INLETS
TYPE E & F (STANDARD 610001)

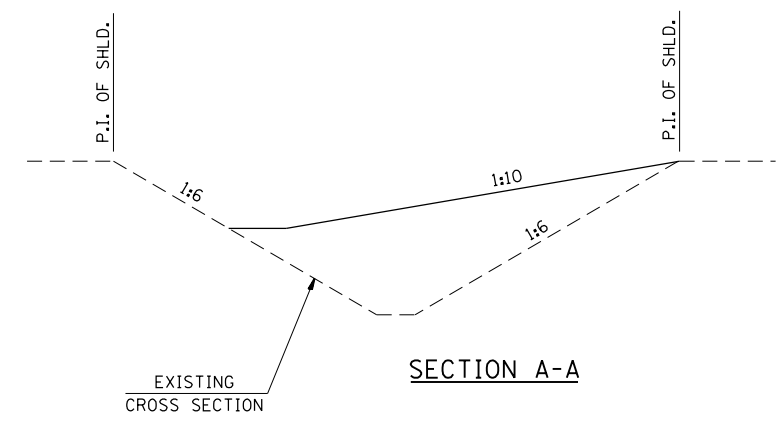
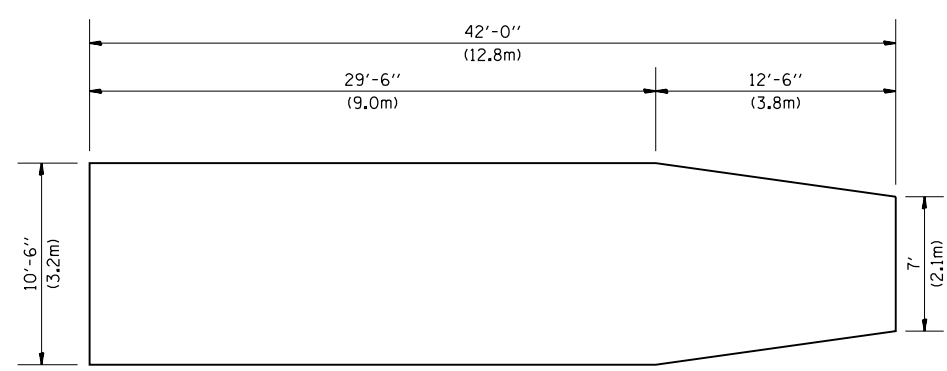
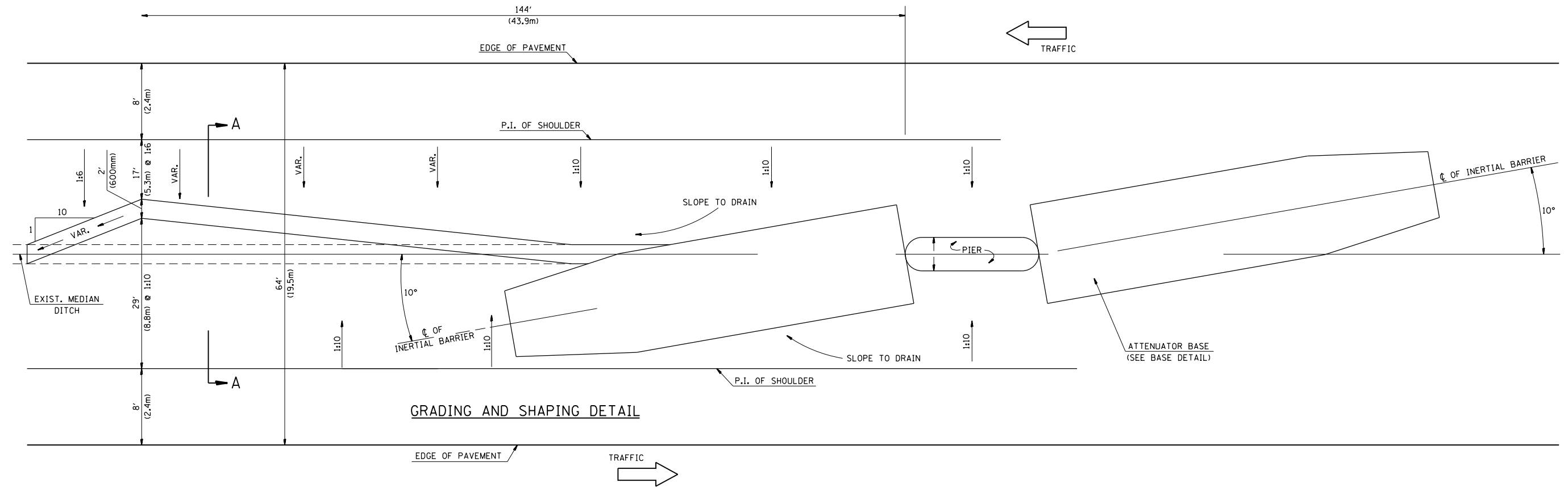


SECTION A-A
TYPICAL SECTION WITH BRIDGE APPROACH CURB

All dimensions are in inches (millimeters) unless otherwise noted.

| | | | | | | | | |
|---|-----------------------|---------------------------|--------------|---|--------------------|--|--|---------------------|
| STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | | | | GUARDRAIL EROSION CONTROL TREATMENTS | | | | SHT. 2 OF 2 |
| | | | | NOT TO SCALE | | | | CADD STD. 630101-D4 |
| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | | | | |
| 155 | 90(109,109-1,110)RS-2 | TAZEWELL | 45 | 42 | CONTRACT NO. 68A77 | | | |
| FED. ROAD DIST. NO. | | ILLINOIS FED. AID PROJECT | | | | | | |

DETAIL OF INERTIAL BARRIERS (70 MPH DESIGN – 64' MEDIAN)



GENERAL NOTES

1. ALL 1:10 SLOPES SHOWN ON THIS DETAIL SHALL BE CONSTRUCTED 1:10 OR FLATTER.
2. THE SLOPES AS SHOWN ON THIS DETAIL SHALL APPLY TO BOTH ENDS OF THE BRIDGE PIERS.
3. ATTENUATOR BASE AND BARREL ARRAY SHALL BE INSTALLED IN ACCORDANCE WITH STATE STANDARD 643001 AND MANUFACTURER'S SPECIFICATIONS.
4. IN AREAS OF 1:10 SLOPES PRECEDING THE ATTENUATOR IN THE MEDIAN INSTALLATION, FOUR OR MORE WOOD POSTS SHALL BE PLACED AT 5' (1.5m) INTERVALS IN THE MEDIAN ϕ . SEE SPECIAL PROVISIONS AND SCHEDULES.

All slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H)

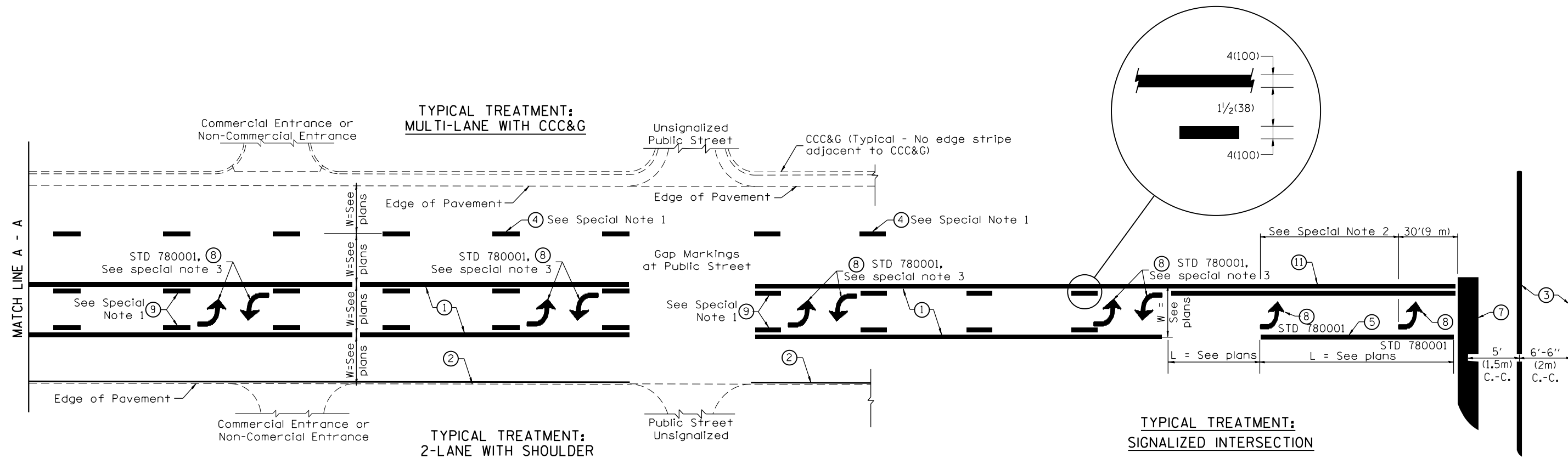
All dimensions are in inches (millimeters) unless otherwise noted.

| | | | | | | | | | | | | | |
|----------|---|------|----------|-----------------|------|---|--|---------------------|-----------------------|----------|--------------|-----------|--|
| 01-01-97 | RENUM. F-3.02a, NEW REVISION BOX, REVISED | T.P. | 03-15-12 | REMOVED BARRELS | R.D. | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | MEDIAN SAND MODULE IMPACT ATTENUATOR 64' MEDIAN | F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | |
| | TITLE BOX, REVISED NOTES | | | | | | | 155 | 90/109,109-1,110/RS-2 | TAZEWELL | 45 | 43 | |
| 08-02 | REVISED | M.A. | | | | | | CONTRACT NO. 68A77 | | | | | |
| 10-16-06 | REVISED TO 2007 SPEC. | M.A. | | | | | | CADD STD. 650004-D4 | | | | | |

NOT TO SCALE

CADD STD. 650004-D4

FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT



FLUSH PAVED MEDIAN: TWO-WAY LEFT TURN LANE WITH ONE-WAY LEFT TURN LANE AT SIGNALIZED INTERSECTION

TYPICAL PAVEMENT MARKING LEGEND

(Note: This is a District Standard Legend. Some elements may not apply to specific project.)

- ① 4(100) Solid (Yellow)
- ② 4(100) Solid (White)
- ③ 2-6(150) Crosswalk @ 6'-6" (2m)min C.-C. (White)
2-8(200) Crosswalk @ 6'-6" (2m)min C.-C. (White) (When traffic signals are present.)
- ④ 6(150) Skip-Dash (White) (See Special Note 1)
- ⑤ 8(200) Solid (White)
- ⑥ 12(300) Diagonal (White) (Item ⑥ is shown on Std. 780001)
- ⑦ 24(600) Stop Bar (White)
- ⑧ Letters & Arrows (See Std. 780001 and Special Notes 2 & 3)
- ⑨ 4(100) Skip-Dash (Yellow) (See Special Note 1)
- ⑩ 12(300) Diagonal (Yellow) (See Table A) (See Table A)
- ⑪ 4(100) Double Solid (Yellow) (See Table A)

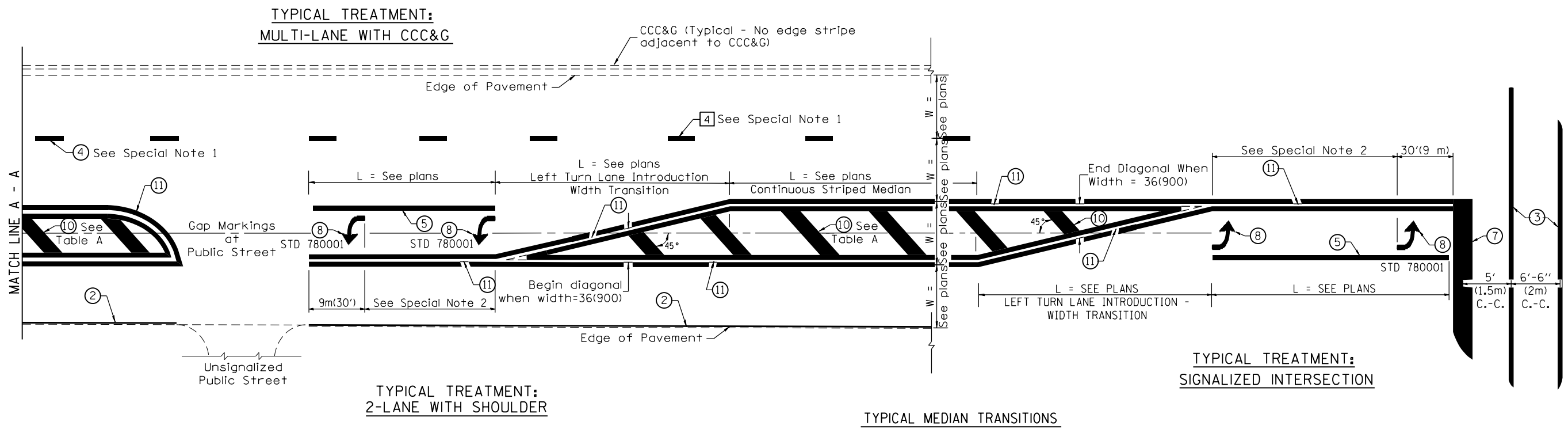
SPECIAL NOTES

1. Skip-Dash markings will be centered between both ends of city blocks and shall be placed in alignment transversely across the pavement.
2. The following shall apply to arrows located in one-way left turn lanes:
 - A. A minimum of two (2) arrows is required.
 - B. The maximum spacing between arrows is 80' (24 m).
 - C. Arrows shall be evenly spaced if three (3) or more are required.
3. The following shall apply to arrow pairs located in two-way left turn lanes:
 - A. A minimum of two (2) arrow pairs is required.
 - B. The maximum spacing between arrow pairs is 200' (61 m).
 - C. Arrow pairs shall be evenly spaced if three (3) or more are required.
 - D. The spacing between Bi Directional Left Turn Arrows is 33' (10 m).

GENERAL NOTES

1. Refer to State Standard 780001 for additional Pavement Markings including letters & arrows.
2. See Plans for Pavement Markings adjacent to curbed islands and medians, and through lane reductions.

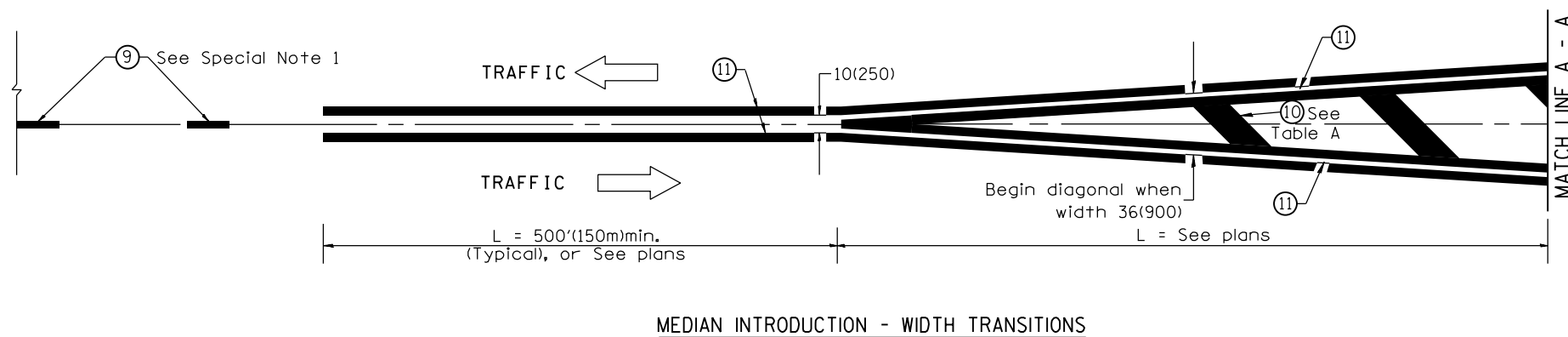
| | | | | | | | | | | | | |
|----------|----------------------------------|------|----------|-----------------------|---|----------------------------------|--------------|---|-----------------------|----------|--------------------|-----------|
| 01-01-97 | RENUM. F-8.03, NEW REVISION BOX | T.P. | 10-16-06 | REVISED TO 2007 SPEC. | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TYPICAL PAVEMENT MARKINGS | NOT TO SCALE | F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 02-07-97 | ADD BI DIRECTIONAL DIMENSION | J.A. | | | | | | 155 | 90(109,109-1,110)R5-2 | TAZEWELL | 45 | 44 |
| 10-97 | CORRECT BI DIRECTIONAL DIMENSION | J.A. | | | | | | SHT. 1 OF 2 CADD STD. 780001-D4 | | | CONTRACT NO. 68A77 | |
| 08-02 | ADD CROSSWALK DMNS. WITH T.S. | M.A. | | | | | | FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT | | | | |



FLUSH PAVED MEDIAN: RESTRICTED LEFT TURN LANE

TABLE A
RECOMMENDED SPACING BETWEEN DIAGONAL LINES

| SPEED LIMIT RANGE | CONTINUOUS | INTERSECTION CHANNELIZATION (Includes Width Transitions for Median and Left Turn Lane Introductions) |
|----------------------------|------------|---|
| | | |
| Less Than 30 mph (50 km/h) | 50' (15m) | 15' (5m) |
| 30 - 45 mph (50 - 70 km/h) | 75' (23m) | 20' (6m) |
| Over 45 mph (70 km/h) | 150' (46m) | 30' (9m) |



All dimensions are in inches (millimeters) unless otherwise noted.