

THE CITY OF MORRIS, ILLINOIS

MORRIS MUNICIPAL AIRPORT

JAMES R. WASHBURN FIELD

CONSTRUCTION PLANS FOR APPLY EMULSIFIED ASPHALT SEAL COAT AND REMARK RUNWAY 18/36

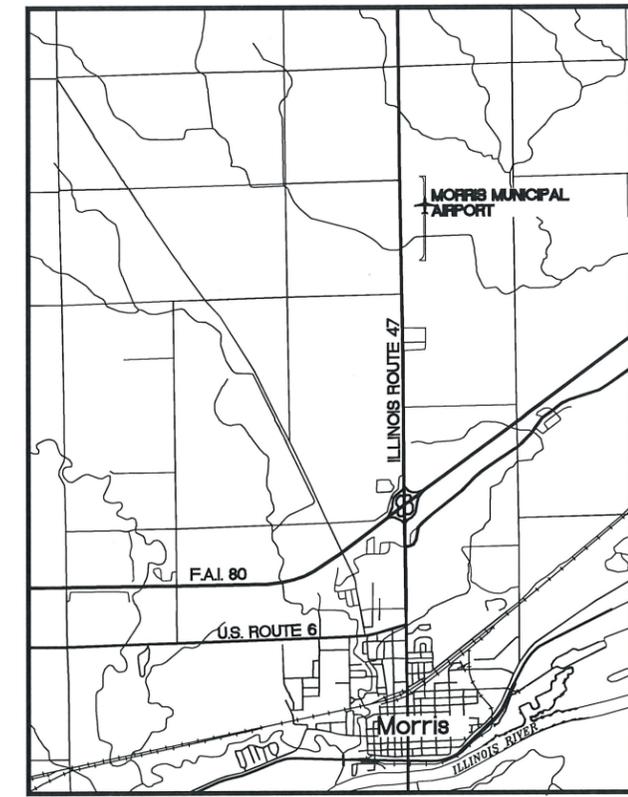
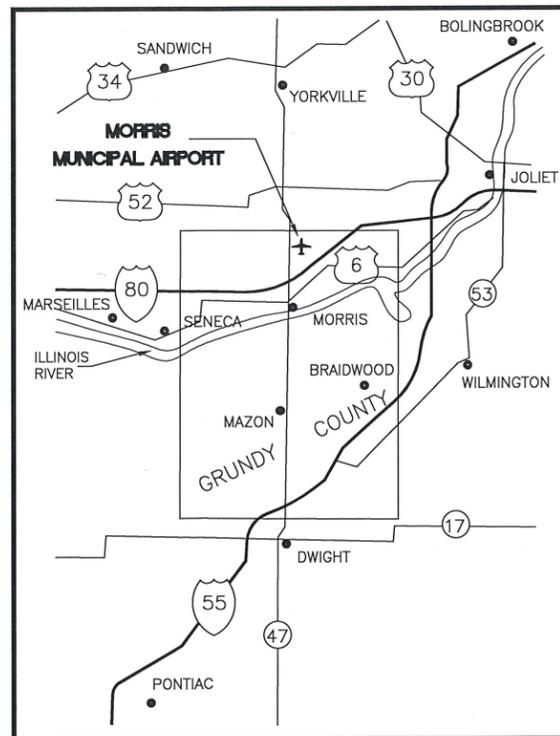
ILLINOIS PROJECT NO. C09-4763
 SBG PROJECT NO. 3-17-SBGP-133/139/144

LATITUDE 41° - 25' - 31.8" LONGITUDE 88° - 25' - 7.2"
 ELEVATION 585.01
 JANUARY 29, 2021
 RUNWAY CATEGORY B, GROUP II

INDEX OF SHEETS	
SHEET #	DESCRIPTION
1	COVER SHEET
2	SITE MAP/ GENERAL NOTES
3	SAFETY PLAN
4	PLAN VIEW
5	PAVEMENT MARKING DETAIL 1
6	PAVEMENT MARKING DETAIL 2

BENCHMARK DATA	
DESCRIPTION	ELEVATION ENGLISH
S.E. CORNER OF CONCRETE BASE FOR OLD WIND SOCK AT OFFICE BLDG	(589.10)
BRASS PLUG IN WEST WALL @ GRACE LUTH. CHURCH	(589.84)
DESCRIPTION	
MORRIS MUNICIPAL AIRPORT	
SECTIONS 10 & 15, TOWNSHIP 34N., RANGE 7E. OF 3RD P.M. GRUNDY COUNTY, SARATOGA TOWNSHIP	

SUMMARY OF QUANTITIES			
ITEM #	DESCRIPTION	UNIT	QUANTITY
AR 150520	MOBILIZATION	L.S.	1
AR 201661	CLEAN AND SEAL BITUMINOUS CRACKS	L.F.	18,500
AR 608510	EXISTING ASPHALT SURFACE TREATMENT	S.Y.	47,000
AR 620520	PAVEMENT MARKING - WATERBORNE	S.F.	29,500



1/29/21
 date

CASEY J. MCCOLLOM
 LICENSED PROFESSIONAL ENGINEER
 NO. 062-059173
 STATE OF ILLINOIS
 expires 11-30-2021

 signature
 PROFESSIONAL DESIGN FIRM
 LICENSE NO. 184-001717

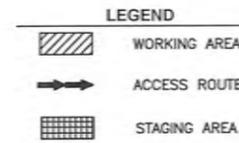
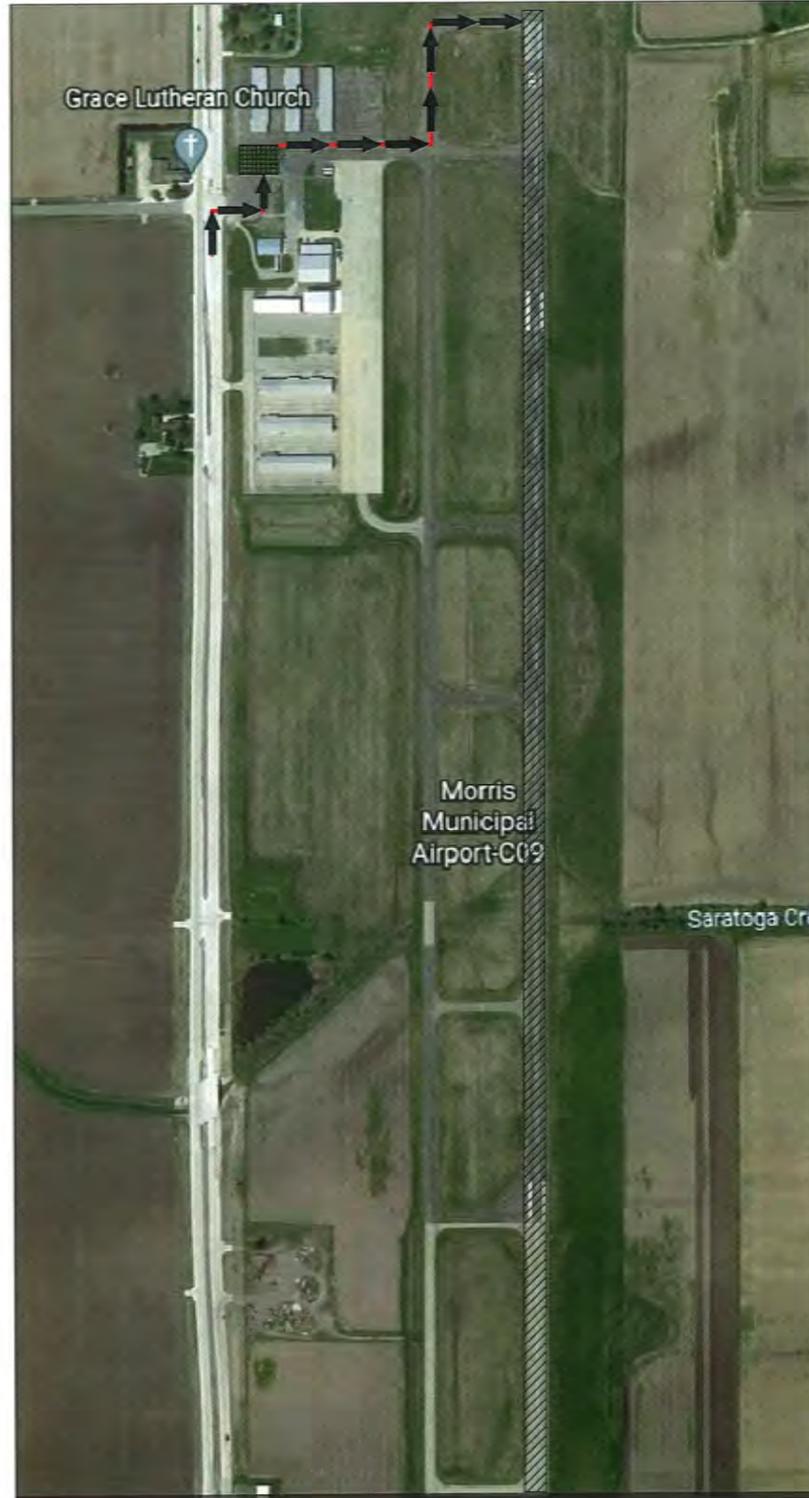
CHAMLIN & ASSOCIATES, INC. © 2020
 Drawing Name: H:\A\024\11217-00 MORRIS AIR RUNWAY 18-36 REHAB\CAD\01-COVER.dwg Last Modified: Jan 28, 2021 - 2:43pm Plotted on: Feb 04, 2021 - 1:54pm by caseym

<p>CITY OF MORRIS</p> <p>APPROVED MAYOR DATE 1-29-2021 2021</p> <p>APPROVED CITY CLERK DATE January 29, 2021</p>	<p>Illinois Department of Transportation Division of Aeronautics</p> <p>APPROVED _____ CHIEF ENGINEER DATE _____ 2020</p>
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DRAWN BY: ECW	LEVEL	BY	DATE
CHECKED BY: CHECKED			
DATE: 10/01/2020			

CONSTRUCTION PLANS	CURRENT AS OF: 01/27/21	
	SCALE: AS-NOTED	SHEET 1
	FILE NO.: 1217-00	OF 6



SAFETY PLAN:

1. All proposed work will be inside of the Airfield Operations Area (AOA). Runway 18/36 and surrounding taxiways will be closed for the duration of the project. Contractor must advise the Airport Manager no less than 72 hours before any runway/taxiway closures can take place.
2. The Contractor shall be responsible for familiarizing himself and comply with the requirements of FAA Advisory Circular 150/5370-2G. As part of this compliance, the Contractor shall prepare and submit a Safety Plan Compliance Document in accordance with AC 150/5370-2G. This document should be submitted to the Airport Manager for review. Once approved, the Airport Manager will submit the document to the Illinois Division of Aeronautics for final approval. The Contractor's Notice to Proceed will not occur until the Safety Plan Compliance Document has been approved.
3. All material storage, equipment parking, and employee parking shall occur in the area designated on the plans or as defined by the Resident Engineer as the Parking and Staging area. Stockpiled materials shall not violate FAA part 77 Airport Surfaces. The material storage, equipment and employee parking shall be clearly delineated in the field.
4. The Contractor shall limit the height of all equipment and material storage stockpiles to a maximum of 30 feet above the ground.
5. The Contractor shall identify all vehicles and equipment with 3' square checkered flags (international orange and white) while on airport property.
6. The Contractor shall limit the number of vehicles traversing the designated work area. Under no circumstances shall the contractor's employees be allowed to operate personal vehicles within the AOA.
7. The Contractor shall limit the use of construction equipment on pavements to remain or completed proposed pavements.
8. Only rubber tired vehicles may be operated on airport pavement that is to remain or has been completed.
9. All barricades located in the AOA shall be low profile as provided in the plan details and shall be kept outside of the denoted Runway Safety Area (RSA) and Taxiway Safety Area (TSA).
10. The Contractor shall provide a Safety Plan Compliance Document (SPCD) that details the contractor's methods for compliance with the project safety plan and the requirements of the FAA Advisory Circular 150/5370-2G. The SPCD shall be provided with the proposed project schedule, both of which shall be provided a minimum of 5 business days prior to the pre-construction meeting. The SPCD shall include the following information:
 - A. Proposed access points and storage areas
 - B. Temporary marking to be used
 - C. Location and type of traffic control devices
 - D. Areas of construction operations and measures to prevent equipment and employees from entering restricted areas
 - E. Any requested alternatives to the requirements of the plans and specifications.
11. All Notice to Airmen (NOTAM) will be issued by the Airport Manager. A minimum of 72 hours' notice to the Airport Manager will be required for the issuance of a NOTAM. No runway or taxiway shall be closed without authorization by the Airport Manager.

GENERAL NOTES:

1. The Contractor shall notify the Resident Engineer a minimum of 7 days prior to the start of construction.
2. The Contractor shall provide a Construction Superintendent. The Superintendent shall be present on the site at any time work is being performed. Additionally, the Superintendent shall be available by phone 24 hours a day, 7 days a week until the project has been deemed complete.
3. The Contractor shall be responsible for the protection, preservation, and repair to any existing facilities that are to remain in place. The cost of this protection, preservation, and repair will not be paid for separately but should be included in the adjacent pay items.
4. The Contractor shall be responsible to verify the existing conditions of the work area prior to beginning construction. Should discrepancies between the condition in the field and the information provided on the plan be discovered, the Contractor shall immediately notify the Resident Engineer.
5. The Contractor shall maintain consistent coordination with the Resident Engineer, Airport Manager, and his subcontractors. Proposed work schedules shall be provided to and approved by the Resident Engineer. Updated schedules shall be provided to the Resident Engineer as the project progresses. The sequence of work shall closely follow the approved schedule unless written permission is given from the Resident Engineer to vary from the proposed schedule.
6. The Contractor shall not enter the Airfield Operations Area (AOA) without expressed authorization from the Airport Manager. This authorization shall be obtained each day entry into the AOA is required.
7. Prior to the request for final inspection, the Contractor shall have all waste, surplus materials, and non-critical equipment removed from the Airport Property. All areas disturbed by the Contractor shall be re-graded, top-soiled, and seeded.
8. The Contractor shall provide all necessary traffic control and maintenance as specified in the plans and as directed by the Resident Engineer. Traffic Control and maintenance may have to be adjusted from time to time to accommodate operations of the airport. These adjustments must be made as quickly as practicable. All work and equipment associated with traffic control and maintenance and adjustments thereof, shall not be paid for separately but shall be included in the cost of the contract.
9. The Contractor shall be responsible for the protection of all pavements that have been inspected and approved. Any damage to occur to a previously inspected and approved pavement shall be repaired by the Contractor at his expense.
10. The Calendar days for this contract were determined based on an approximated number of working days required to complete the proposed improvement. The number of available working days per month has been assumed as follows.

WORKING DAYS			
JANUARY	0	JULY	13
FEBRUARY	0	AUGUST	0
MARCH	0	SEPTEMBER	0
APRIL	0	OCTOBER	0
MAY	9	NOVEMBER	0
JUNE	4	DECEMBER	0

SITE MAP
NOT TO SCALE

CHAMLIN & ASSOCIATES, INC. © 2020 DRAWING NUMBER: HA-0001-1217-00 MORRIS AIR RUNWAY 18-36 REPAIRS AND SURFACING SITE PLAN & NOTES.dwg Last Modified: Jan 27, 2021 - 11:25am Plotted on: Jan 28, 2021 - 2:39pm by ecw

DRAWN BY: ECW	REVISIONS		
	LEVEL	BY	DATE
CHECKED BY: CHECKED			
CREATED: 10/19/2020			

PERU OTTAWA MORRIS
ILLINOIS
Chamlin & Associates

**MORRIS MUNICIPAL AIRPORT
SEAL COAT AND PAVEMENT MARKING
MORRIS, ILLINOIS**

SITE PLAN & GENERAL NOTES

FINAL

CURRENT AS OF: 01/27/21	
SCALE: AS NOTED	SHEET 2
FILE NO.: 1217-00	OF 6

BARRICADE NOTES:

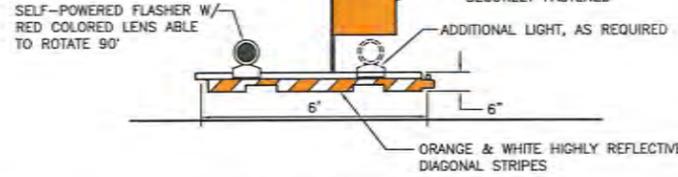
BARRICADES TO BE HIGH IMPACT UV-RESISTANT POLYETHYLENE, LIQUID OR SAND BALLASTED TO WITHSTAND DISPLACEMENT BY WEATHER, JET OR PROP BLAST.

PLACE AT 12' INTERVALS (CENTER TO CENTER) UNLESS SPECIFIED OTHERWISE ON PLANS OR BY ENGINEER.

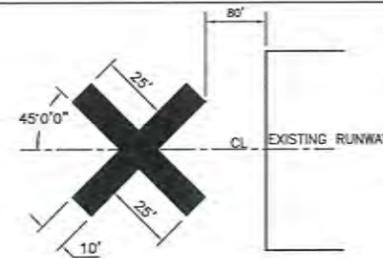
STEADY BURNING, RED OMNI-DIRECTIONAL LIGHTS MAY BE SUBSTITUTED.

LEGEND

- RSA--- RUNWAY SAFETY AREA (RSA)
- OFA--- OBJECT FREE AREA (OFA)
- BRL--- BUILDING RESTRICTION LINE (BRL)
- >--- CONTRACTORS ACCESS ROUTE
- ▨ PHASE I WORK AREA
- BARRICADE



LOW PROFILE BARRICADE
NOT TO SCALE

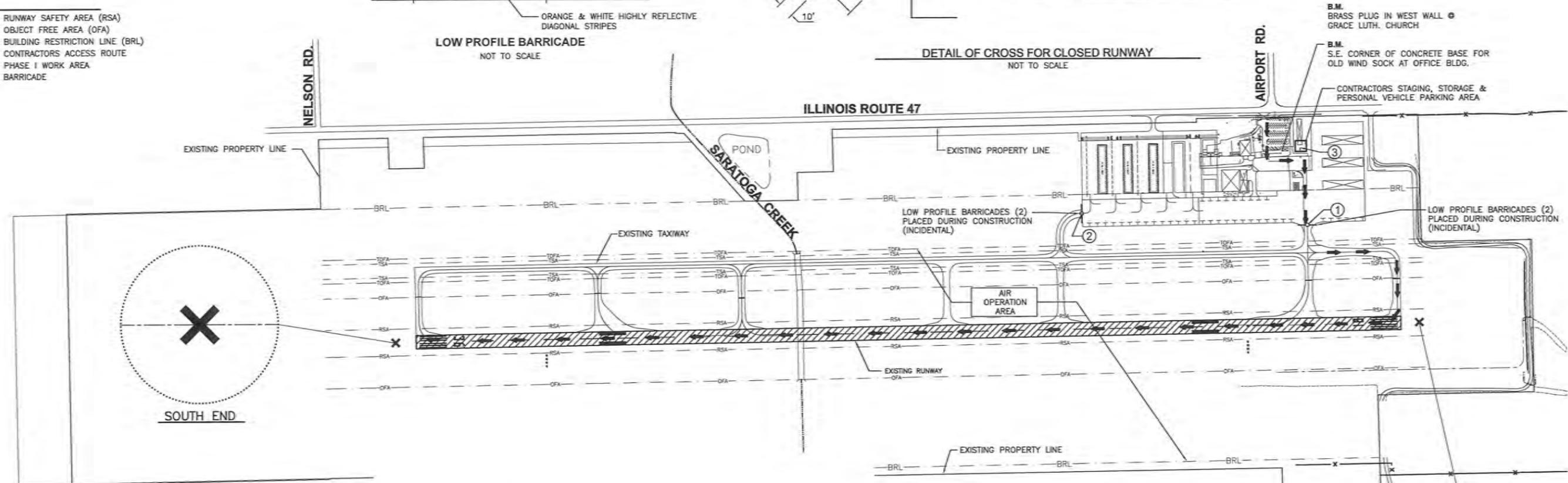
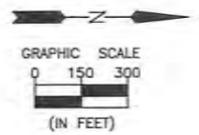


DETAIL OF CROSS FOR CLOSED RUNWAY
NOT TO SCALE

NOTE:

- 1.) CLOSED RUNWAY MARKERS SHALL BE YELLOW.
- 2.) MARKERS SHALL BE DOUBLE LAYERED PAINTED SNOW FENCE, COLORED PLASTIC, PAINTED PLYWOOD OR OTHER MATERIAL APPROVED BY THE ENGINEER.
- 3.) CONTRACTOR SHALL MAINTAIN MARKERS AS DIRECTED BY THE ENGINEER.
- 4.) COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS AND BARRICADES SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.

CONTRACT NO. = MR024



GENERAL NOTES AND SCOPE OF WORK

- 1.) SCOPE OF WORK: THE PROJECT WILL CONSIST OF INSTALLING A SEAL COAT AND PAVEMENT MARKINGS TO EXISTING PAVEMENT ON RUNWAY 18/36.
- 2.) THE MAXIMUM ANTICIPATED CONSTRUCTION EQUIPMENT HEIGHT IS 13'-0".
- 3.) THE CONTRACTOR SHALL USE THE DESIGNATED ACCESS ROUTE AS SHOWN ON THIS SHEET. THE CONTRACTOR SHALL MAINTAIN THE ROUTE AND REMOVE ANY FOREIGN OBJECT DEBRIS FROM THE WORK AREA AND ACCESS ROUTE AT THE CLOSE OF EACH WORK DAY. THE CONTRACTOR IS RESPONSIBLE TO SWEEP THE ACCESS ROUTE PRIOR TO THE CLOSE OF WORK EACH DAY. THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- 5.) DUE TO THE PROXIMITY TO AIRCRAFT OPERATIONS, THE CONTRACTOR IS REQUIRED TO STRICTLY ADHERE TO THE GUIDELINES REGARDING CONSTRUCTION SAFETY AS SET FORTH IN FAA ADVISORY CIRCULAR 150/5370-26.
- 6.) BARRICADES USED ON AIRPORT SURFACES SHALL BE LOW PROFILE WITH REFLECTIVE STRIPES & RED LIGHTS AS PER AC 150/5370-26 "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION".
- 7.) THE AIRPORT WILL BE CLOSED FOR THE DURATION OF THE PROJECT.

SEQUENCE OF CONSTRUCTION & SAFETY

PHASE I: (RUNWAY 18/36 CLOSED FOR DURATION)

- A. INSTALL TRAFFIC CONTROL TO ALL PHASE I LOCATIONS.
- B. SEAL COAT RUNWAY 18/36 IN ENTIRETY, INCLUDING AREA PREP, TACK COAT, SEAL COAT.
- C. INSTALL PAVEMENT MARKINGS.
- D. REMOVE ALL TRAFFIC CONTROL MEASURES FROM PROJECT AREA.

SAFETY PLAN NOTES

- 1.) THE SEQUENCE OF CONSTRUCTION SHOWN ON THIS SHEET IS INTENDED TO ALLOW ORDERLY AND SAFE CONSTRUCTION.
- 2.) BARRICADES SHALL BE PLACED AND MAINTAINED AS SHOWN HEREIN, AS INDICATED IN THE SPECIAL PROVISIONS AND AS DIRECTED BY THE ENGINEER. PLACEMENT AND MAINTENANCE OF BARRICADES ARE INCIDENTAL TO CONTRACT.
- 3.) THE CONTRACTOR SHALL BE RESPONSIBLE FOR FURNISHING AND INSTALLING CLOSED RUNWAY MARKERS AS SHOWN ON THIS SHEET AND AS DIRECTED BY THE AIRPORT MANAGER AND THE ENGINEER. MARKERS SHALL BE PLACED AND REMOVED WHEN SO DIRECTED BY THE OWNER THROUGH THE ENGINEER. THE OWNER SHALL BE RESPONSIBLE FOR NOTIFYING THE FLIGHT SERVICE STATION REGARDING RUNWAY CLOSURE.
- 4.) ALL BARRICADES, MARKINGS, LATHE, FLAGGING, AND TRAFFIC CONTROL ITEMS ARE INCIDENTAL TO THE CONTRACT.
- 5.) WHEN THE CONTRACTORS VEHICLES AND EQUIPMENT ARE ON THE AIRPORT PROPERTY THEY SHALL CARRY A HAZARD IDENTIFICATION FLAG CONSISTING OF A 3 FOOT SQUARE INTERNATIONAL ORANGE AND WHITE CHECKERED FLAG.
- 6.) WHEN THE CONTRACTOR IS OPERATING IN AOA, HE/SHE SHALL CARRY A TWO WAY RADIO TUNED TO THE FREQUENCY SPECIFIED BY THE AIRPORT MANAGER IN ORDER TO HAVE CONSISTENT AND IMMEDIATE CONTACT WITH AIRPORT OPERATIONS STAFF.
- 7.) THE CONTRACTOR SHALL NOT UTILIZE EQUIPMENT WITH A HEIGHT GREATER THAN 30 FOOT WITHOUT PRIOR APPROVAL FROM THE RESIDENT ENGINEER.
- 8.) ALL COSTS INCURRED BY THE CONTRACTOR TO IMPLEMENT AND MAINTAIN THE SAFETY PLAN SHALL BE INCLUDED IN THE COST OF THE CONTRACT.
- 9.) RESTORATION OF HAUL ROUTES AND STAGING AREAS SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR. NO MEASUREMENT FOR PAYMENT WILL BE MADE FOR THIS WORK AND SHALL BE INCLUDED IN THE COST OF THE CONTRACT.

CRITICAL POINT DATA

PNT. LOCATION	LATITUDE	LONGITUDE	GRND. ELEV.	HEIGHT (AMSL)	PHASE
(N) APRON BARRICADE ①	N. 41° 25' 51.03"	W. 88° 25' 15.02"	585.50	588.00	I
(S) APRON BARRICADE ②	N. 41° 25' 38.60"	W. 88° 25' 15.88"	574.50	577.00	I
CONSTRUCTION STAGING AREA ③	N. 41° 25' 50.67"	W. 88° 25' 20.78"	587.71	612.71	I

POINT OF CONTACT

AIRPORT MANAGER:
JEFF VOGEN
MORRIS MUNICIPAL AIRPORT
9980 N. RTE. 47
MORRIS, IL 60450
(815) 942-1600

REVISIONS	LEVEL	BY	DATE	DESCRIPTION

CA PERU OTTAWA MORRIS ILLINOIS
Chamlin & Associates

MORRIS MUNICIPAL AIRPORT
SEAL COAT AND PAVEMENT MARKING
MORRIS, ILLINOIS

CONSTRUCTION SAFETY PLAN

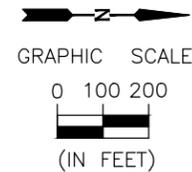
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CURRENT AS OF: 01/27/21

SCALE: AS NOTED SHEET 3

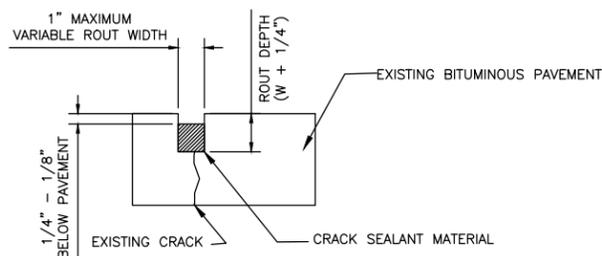
FILE NO.: 1217-00 OF 6

CHAMLIN & ASSOCIATES, INC. © 2020 Drawing Number: 1217-00 MORRIS AIR RUNWAY 18-36 ROADWAY/CONSTRUCTION SAFETY PLAN Last Modified: Jun 27, 2021 - 11:33am. Plotted on: Jun 28, 2021 - 2:39pm by eric



CLEAN AND SEAL BITUMINOUS CRACKS

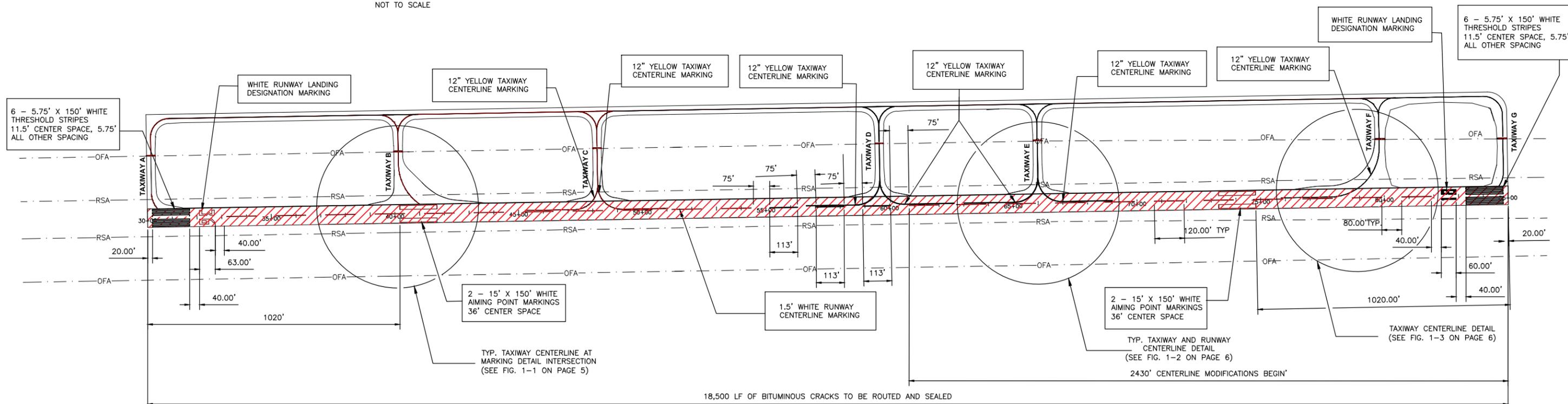
1. REMOVE ALL VEGETATION AND DEBRIS FROM CRACKS TO A MINIMUM DEPTH OF 1 INCH.
2. THE PAVEMENT TEMPERATURE SHOULD BE 50°F AND RISING OR MEET THE MANUFACTURER'S RECOMMENDATIONS AT THE TIME OF THE APPLICATION OF THE CRACK SEALING MATERIAL.
3. SEALANT IS NOT TO BE INSTALLED IF MOISTURE IS OBSERVED IN THE CRACK.
4. IF EXTENSIVE VEGETATION EXISTS, TREAT THE SPECIFIC AREA WITH A CONCENTRATED WATER BASED HERBICIDE, APPROVED BY THE RESIDENT ENGINEER.
5. USE AN AIR COMPRESSOR WITH AN OPERABLE OIL AND WATER TRAP TO CLEAN ALL CRACKS, WITH COMPRESSED HOT AIR.
6. IF NECESSARY, SAW OR ROUT THE CRACKS TO THE REQUIRED WIDTH AND DEPTH.
7. THE CONTRACTOR SHALL APPLY THE SEALANT UNIFORMLY FROM THE BOTTOM TO THE TOP OF THE CRACK AVOIDING VOIDS OR ENTRAPPING AIR.
8. FILL ALL CRACKS GREATER THEN 1/4 INCH WIDE WITH CRACK SEALANT (PER ASTM D6690).
9. UNDER-FILL THE CRACK SEALANT A MINIMUM OF 1/8 INCH, NOT TO EXCEED 1/4 INCH, TO AVOID CONTAMINATION OF SEAL COAT.
10. ANY EXCESS JOINT OR CRACK SEALER SHALL BE REMOVED FROM THE PAVEMENT SURFACE.
11. CONTRACTOR SHALL OPERATE IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-10H (CHAPTER 3) IN REGARDS TO THE CLEANING AND SEALING OF BITUMINOUS CRACKS ON RUNWAY 18/36.



HMA CRACK SEALING DETAIL
NOT TO SCALE

EMULSIFIED ASPHALT SEAL COAT W/ AGGREGATE

1. AGGREGATE MATERIAL SHALL BE DRY, CLEAN, DUST AND DIRT FREE, SOUND, DURABLE, ANGULAR SHAPED MANUFACTURED SPECIALTY SAND, SUCH AS THAT USED AS AN ABRASIVE, WITH MOHS HARDNESS OF 6 TO 8.
2. THE CONTRACTOR SHALL SUBMIT THE SPECIALTY SAND MANUFACTURER'S TECHNICAL DATA AND A MANUFACTURE'S CERTIFICATION OF ANALYSIS INDICATING THAT THE SPECIALTY SAND MEETS THE REQUIREMENTS OF THE SPECIFICATION PRIOR TO THE START OF CONSTRUCTION.
3. THE CONTRACTOR SHALL PROVIDE A CERTIFICATION SHOWING PARTICLE SIZE ANALYSIS AND PROPERTIES OF THE MATERIAL DELIVERED FOR USE ON THE PROJECT.
4. THE CONTRACTOR'S CERTIFICATION MAY BE SUBJECT TO VERIFICATION BY TESTING THE MATERIAL DELIVERED FOR USE ON THE PROJECT.
5. DILUTION RATE FOR THE PROJECT SHALL BE DILUTED AT A ONE-TO-ONE RATIO, ONE PART CONCENTRATE TO ONE PART WATER.
6. THE CONTRACTOR SHALL PROVIDE A COPY OF THE MANUFACTURER'S COA FOR THE EMULSIFIED ASPHALT DELIVERED TO THE PROJECT.
7. THE COA SHALL BE SUBMITTED TO AND APPROVED BY THE RESIDENT ENGINEER PRIOR TO CONSTRUCTION OF THE PROJECT.
8. THE ASPHALT MATERIAL STORAGE AND HANDLING TEMPERATURE SHALL BE BETWEEN 50°F - 160°F.
9. PRIOR TO FULL APPLICATION, THE CONTRACTOR SHALL PLACE A SERIES OF CONTROL STRIPS A MINIMUM OF 300 FEET LONG BY 12 FEET WIDE, AT VARYING APPLICATION RATES AS RECOMMENDED BY THE MANUFACTURER'S REPRESENTATIVE AND ACCEPTABLE TO THE RESIDENT ENGINEER TO DETERMINE APPROPRIATE APPLICATION RATE.
10. THE CONTRACTOR SHALL FURNISH ALL EQUIPMENT, TOOLS, AND MACHINERY NECESSARY FOR THE PERFORMANCE OF THE WORK.
11. CONTRACTOR SHALL OPERATE IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-10H (CHAPTER 8) IN REGARDS TO EMULSIFIED SEAL COATING OF RUNWAY 18/36.



18,500 LF OF BITUMINOUS CRACKS TO BE ROUTED AND SEALED

LEGEND

	EMULSIFIED ASPHALT SEAL COAT W/ AGGREGATE
	RUNWAY SAFETY AREA (RSA)
	OBJECT FREE AREA (OFA)

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Drawing Name: H:\A\08\1\1217-00 MORRIS AIR RUNWAY 18-36 REHAB\04-PLAN VIEW.dwg Last Modified: Jan 28, 2021 - 4:30pm Plotted on: Feb 01, 2021 - 8:48am by ccaseym

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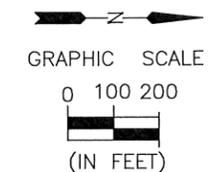


MORRIS MUNICIPAL AIRPORT
SEAL COAT AND PAVEMENT MARKING
MORRIS, ILLINOIS

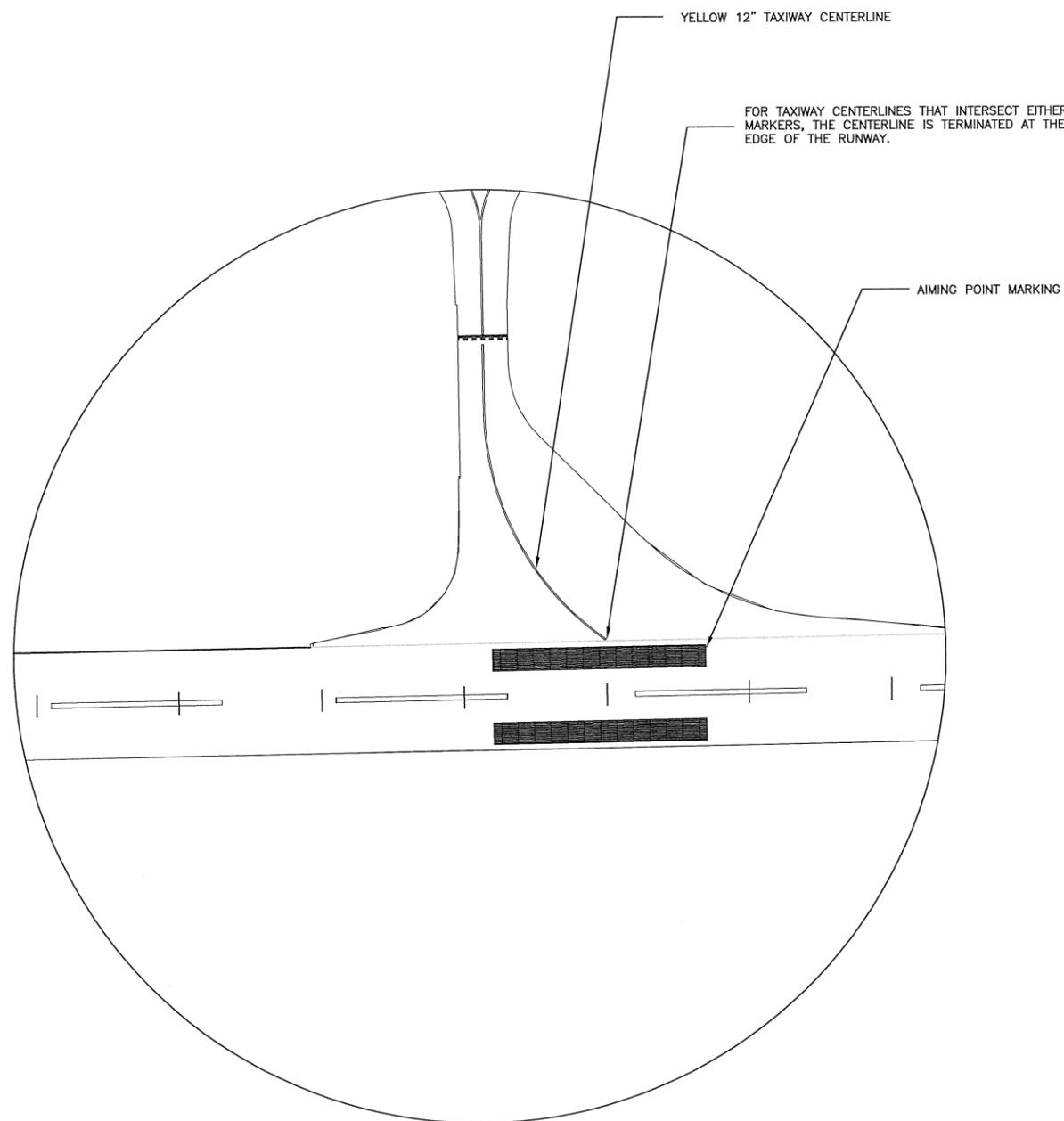
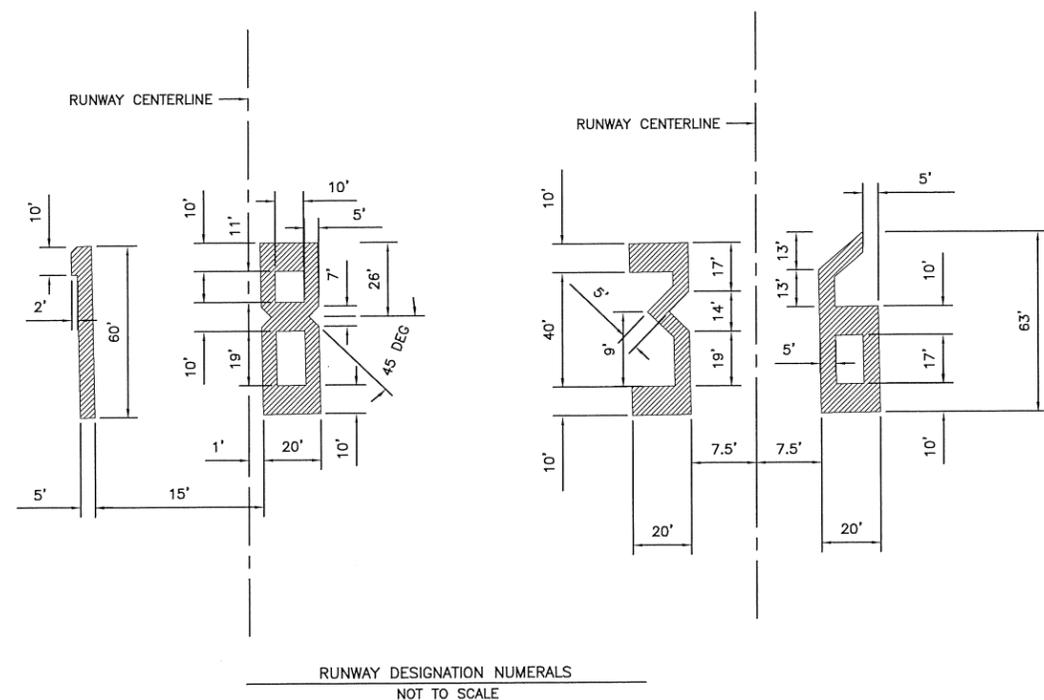
PLAN VIEW

FINAL

CURRENT AS OF: 01/29/21	SHEET 4
SCALE: AS NOTED	OF 6
FILE NO.: 1217-00	



PAVEMENT MARKING - WATERBORNE SCHEDULE			
DESCRIPTION	UNIT AREA	NO. REQ'D	TOTAL
NUMERAL 1	318 SF	1	318 SF
NUMERAL 8	876 SF	1	876 SF
NUMERAL 3	633 SF	1	633 SF
NUMERAL 6	712 SF	1	712 SF
CENTERLINE STRIPE	180 SF	25	4,500 SF
THRESHOLD MARKING	863 SF	12	10,350 SF
AIMING POINT MARKING	2,250 SF	4	9,000 SF
		TOTAL WHITE	26,389 SF
TAXIWAY CENTERLINE	301 SF	7	2,107 SF
		TOTAL YELLOW	2,107 SF
	TOTAL WATERBORNE		28,496 SF



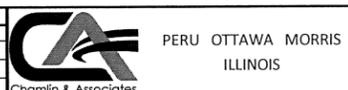
(FIGURE 1-1) TAXIWAY CENTERLINE AT INTERSECTION OF RUNWAY MARKERS (TYP. TAXIWAY A,B & G)
NOT TO SCALE

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Drawing Number: 1217-00 MORRIS AIR RUNWAY 18-36 REHAB/CAD/05-PAVEMENT MARKING DETAIL 1.dwg Last Modified: Jan. 27, 2021 - 1:27pm Plotted on: Jan. 28, 2021 - 2:40pm by: ericw

PAVEMENT MARKING NOTES:

1. ALL RUNWAY MARKINGS ARE WHITE INCLUDING: CENTERLINES, RUNWAY DESIGNATION NUMERALS, THRESHOLD, AND AIMING POINT MARKERS
2. ALL TAXIWAY CENTERLINE MARKINGS ARE YELLOW
3. PERMANENT MARKINGS SHALL BE "WATERBORNE" PAINT AT AN APPLICATION RATE APPROVED BY THE RESIDENT ENGINEER
4. APPLY REFLECTIVE MEDIA ON ALL PAINTED SURFACES

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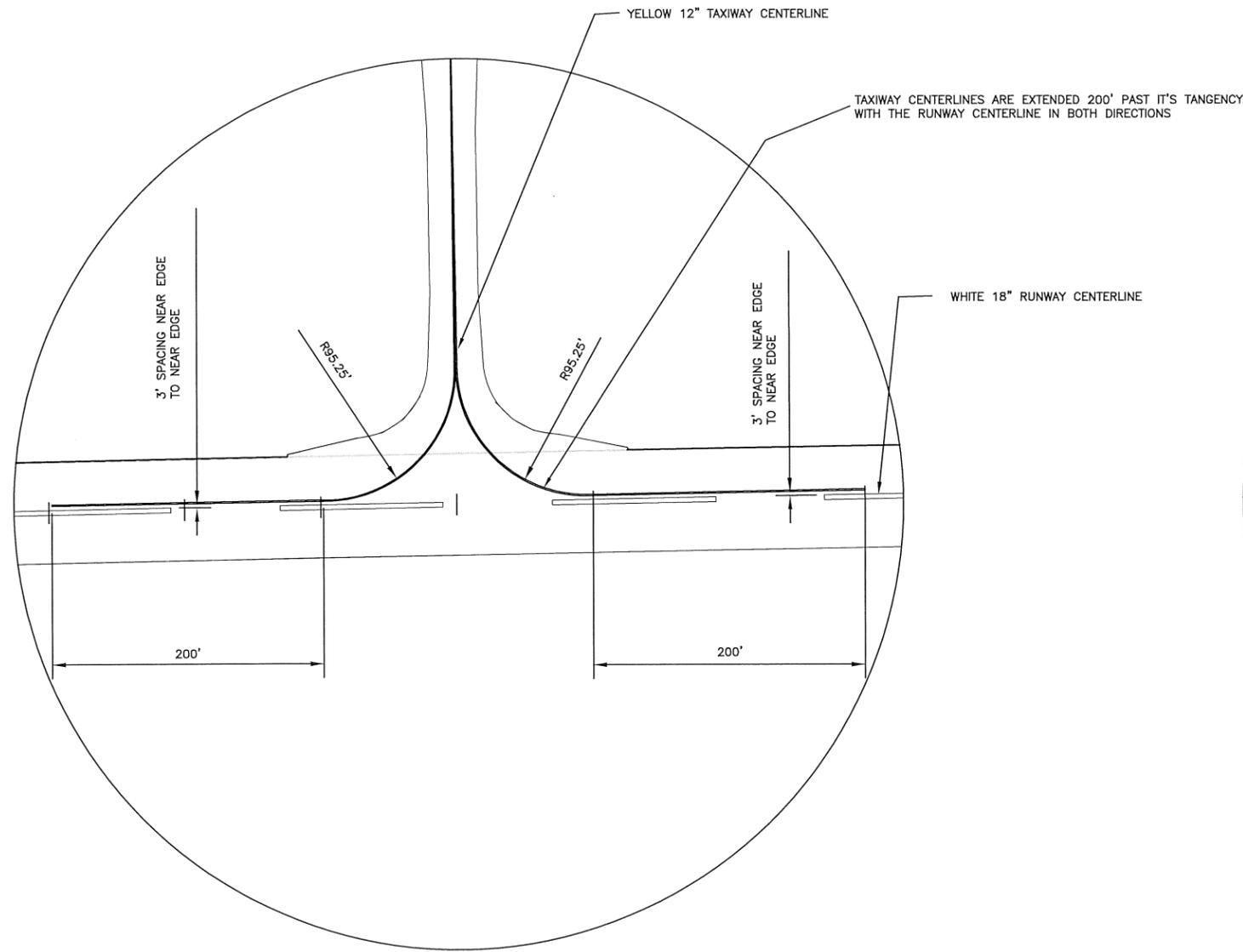
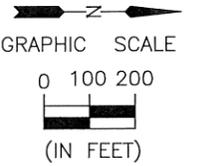


MORRIS MUNICIPAL AIRPORT
SEAL COAT AND PAVEMENT MARKING
MORRIS, ILLINOIS

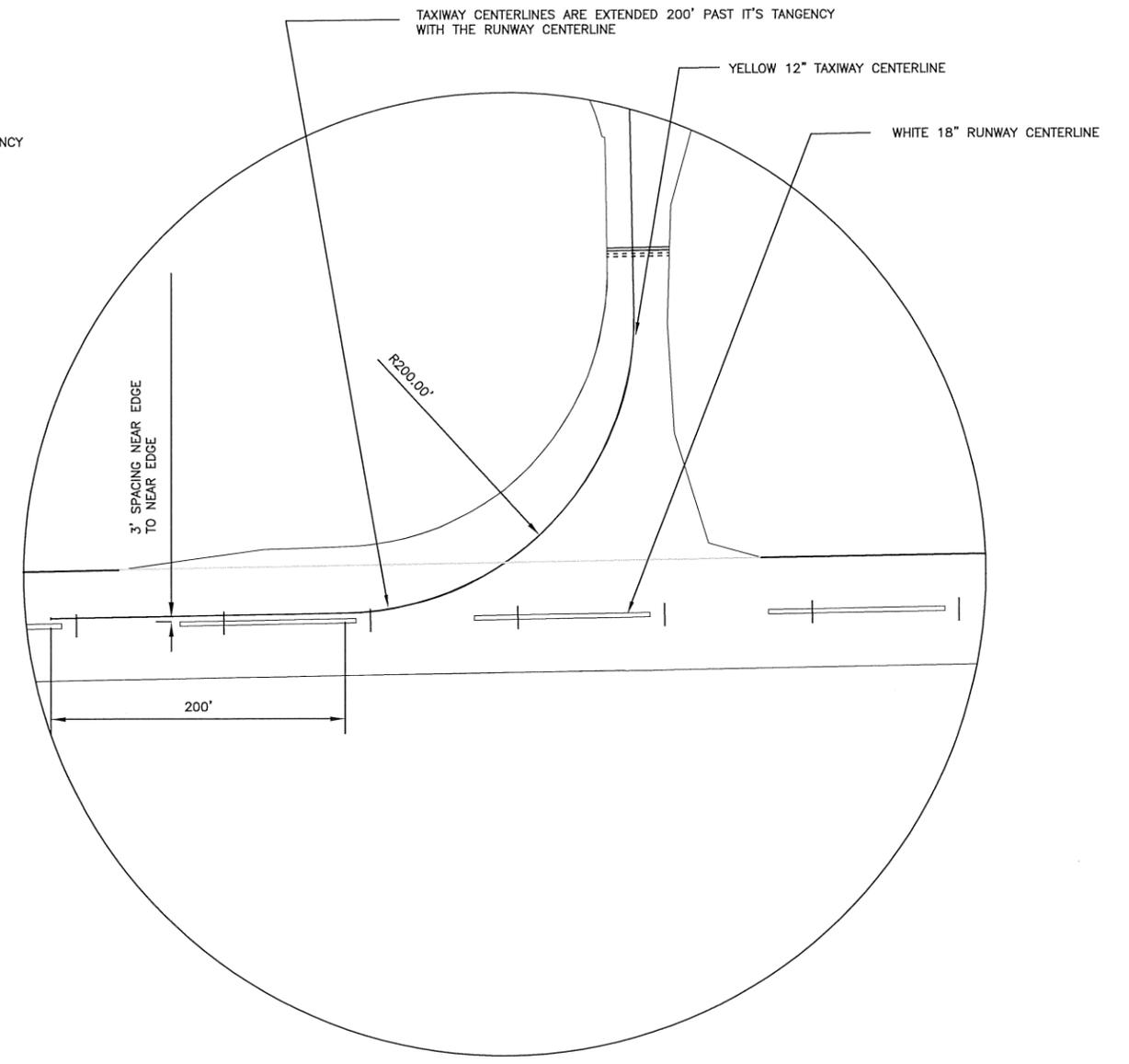
PAVEMENT MARKING DETAIL

FINAL

CURRENT AS OF: 01/27/21	
SCALE: AS NOTED	SHEET 5
FILE NO.: 1217-00	OF 6



(FIGURE 1-2) TAXIWAY CENTERLINE AND RUNWAY CENTERLINE INTERSECTION (TYP. TAXIWAY C,D & E)
NOT TO SCALE



(FIGURE 1-3) TAXIWAY CENTERLINE AND RUNWAY CENTERLINE INTERSECTION AT TAXIWAY F
NOT TO SCALE

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Drawing Name: H:\AUG1\1717-00 MORRIS AIR RUNWAY 18-36 REHAUS\06-PAVEMENT MARKING DETAIL 2.dwg Last Modified: Jan 27, 2021 - 1:32pm Plotted on: Jan 28, 2021 - 2:41 pm by: ecw

- PAVEMENT MARKING NOTES:**
1. ALL RUNWAY MARKINGS ARE WHITE INCLUDING: CENTERLINES, RUNWAY DESIGNATION NUMERALS, THRESHOLD, AND AIMING POINT MARKERS
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PERU OTTAWA MORRIS
ILLINOIS

**MORRIS MUNICIPAL AIRPORT
SEAL COAT AND PAVEMENT MARKING
MORRIS, ILLINOIS**

PAVEMENT MARKING DETAIL 2

FINAL

CURRENT AS OF: 01/27/21	
SCALE: AS NOTED	SHEET 6
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