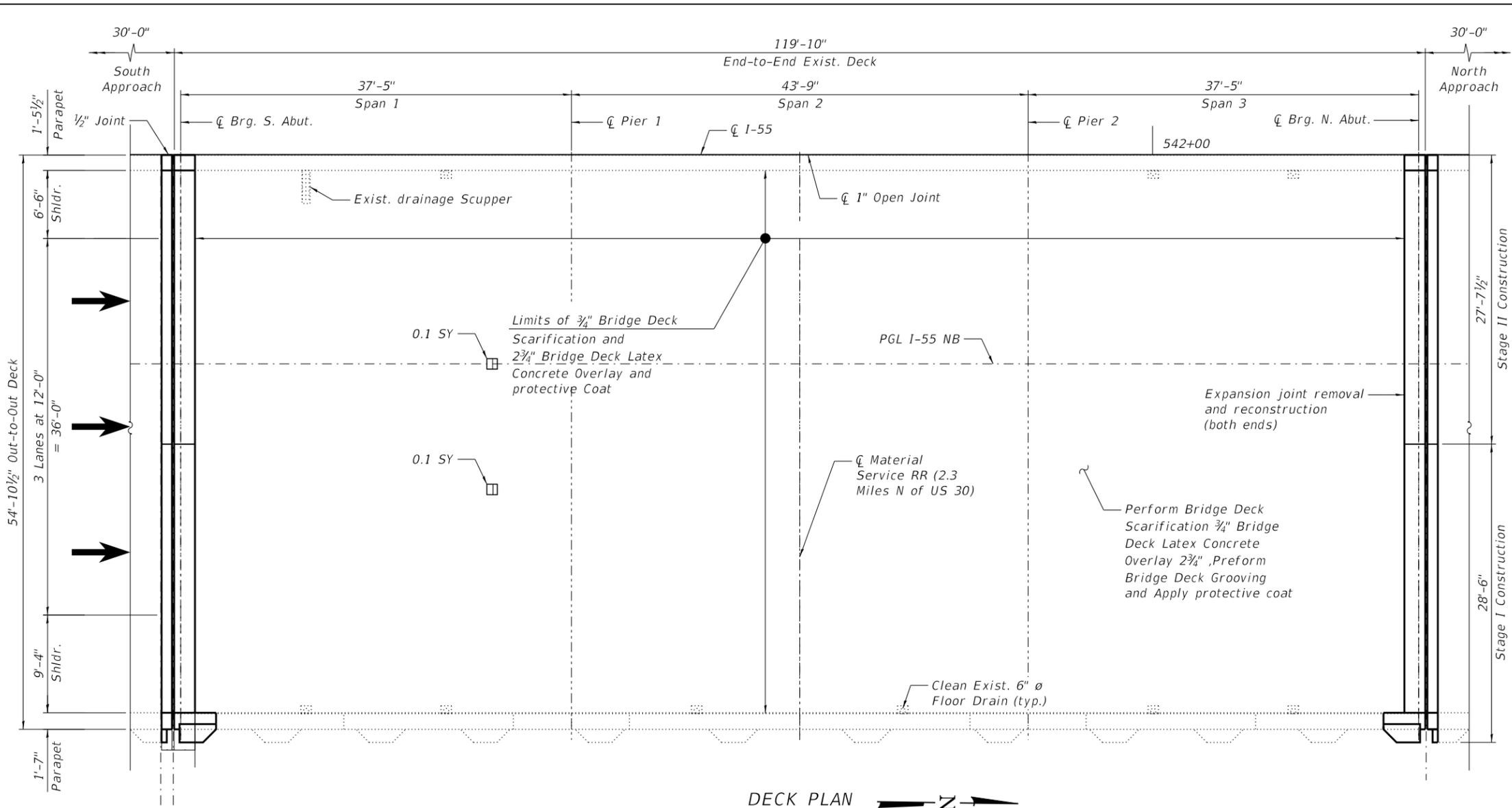


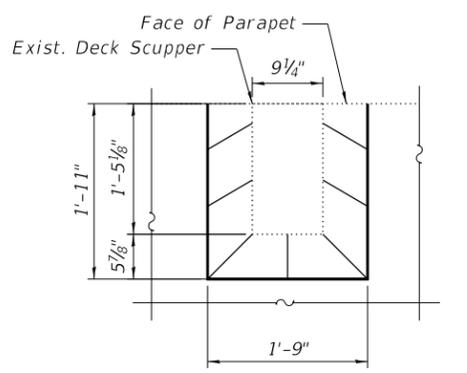
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DECK PLAN

BILL OF MATERIAL

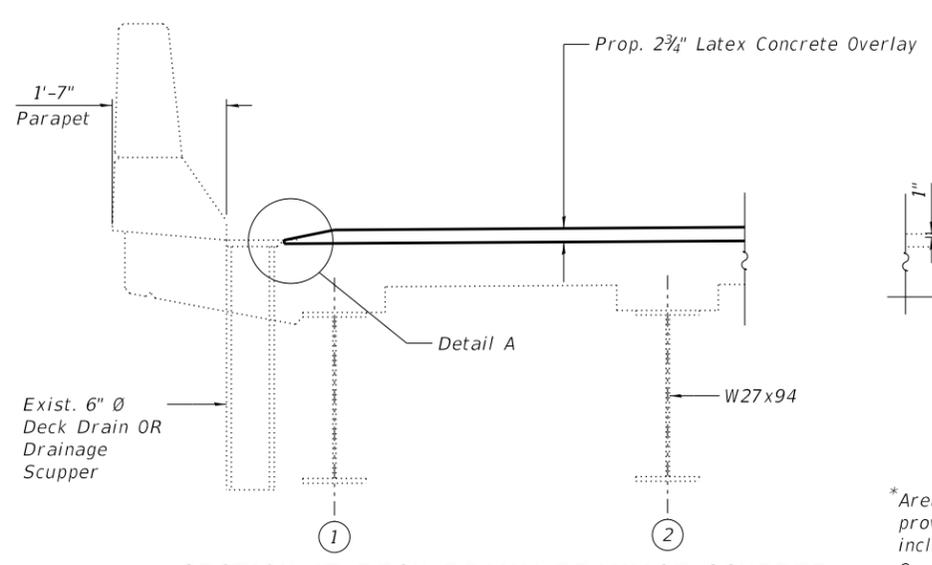
ITEM	UNIT	QUANTITY
Bridge Deck Grooving	SQ YD	664
Protective Coat	SQ YD	770
Bridge Deck Latex Concrete Overlay, 2 3/4 Inches	SQ YD	668
Bridge Deck Scarification 3/4"	SQ YD	668



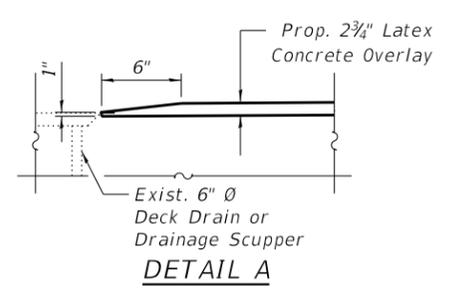
TOP PLAN AT DRAINAGE SCUPPER

NOTES:

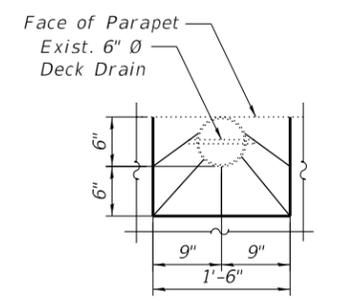
1. Areas of deck repair shown are estimated. The Engineer shall show actual locations of deck repairs at the time of construction.
2. For bridge deck final cross section, see Sheet S4-03.
3. For North and South transverse joint removal and reconstruction, see Sheets S4-09 thru S4-12 .
4. Perform bridge deck grooving for the bridge deck latex concrete overlay and the roadway portions of the reconstructed transverse joints.
5. Protective coat shall be applied to top and inside face of parapets, median and the reconstructed transverse expansion joint areas.
6. Any reinforcement bars that are damaged during concrete removal operations shall be repaired or replaced using an approved bar splicer or anchorage system. Cost incidental to Concrete Removal.
7. The Contractor shall exercise extreme caution during Concrete Removal to avoid damaging the steel beams and diaphragms to remain. Any damage to the existing steel beams and/or diaphragms to remain caused by the Contractor in the performance of his/her work shall be repaired by the Contractor, to the satisfaction of the Engineer, at no cost to the Department.



SECTION AT DECK DRAIN/ DRAINAGE SCUPPER
(West Parapet shown, East Parapet similar)



DETAIL A



TOP PLAN AT DECK DRAIN

*Areas of Deck Slab Repair (Partial) are provided for information only and shall be included in the cost of Bridge Deck Latex Concrete Overlay, 2 1/4"

LEGEND

- Deck Slab Repair (Partial Depth)
- Square Yard



USER NAME =	DESIGNED - JMI	REVISED -
	CHECKED - MI, MAI	REVISED -
PLOT SCALE =	DRAWN - JMI	REVISED -
PLOT DATE =	DATE - 12/2/2020	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

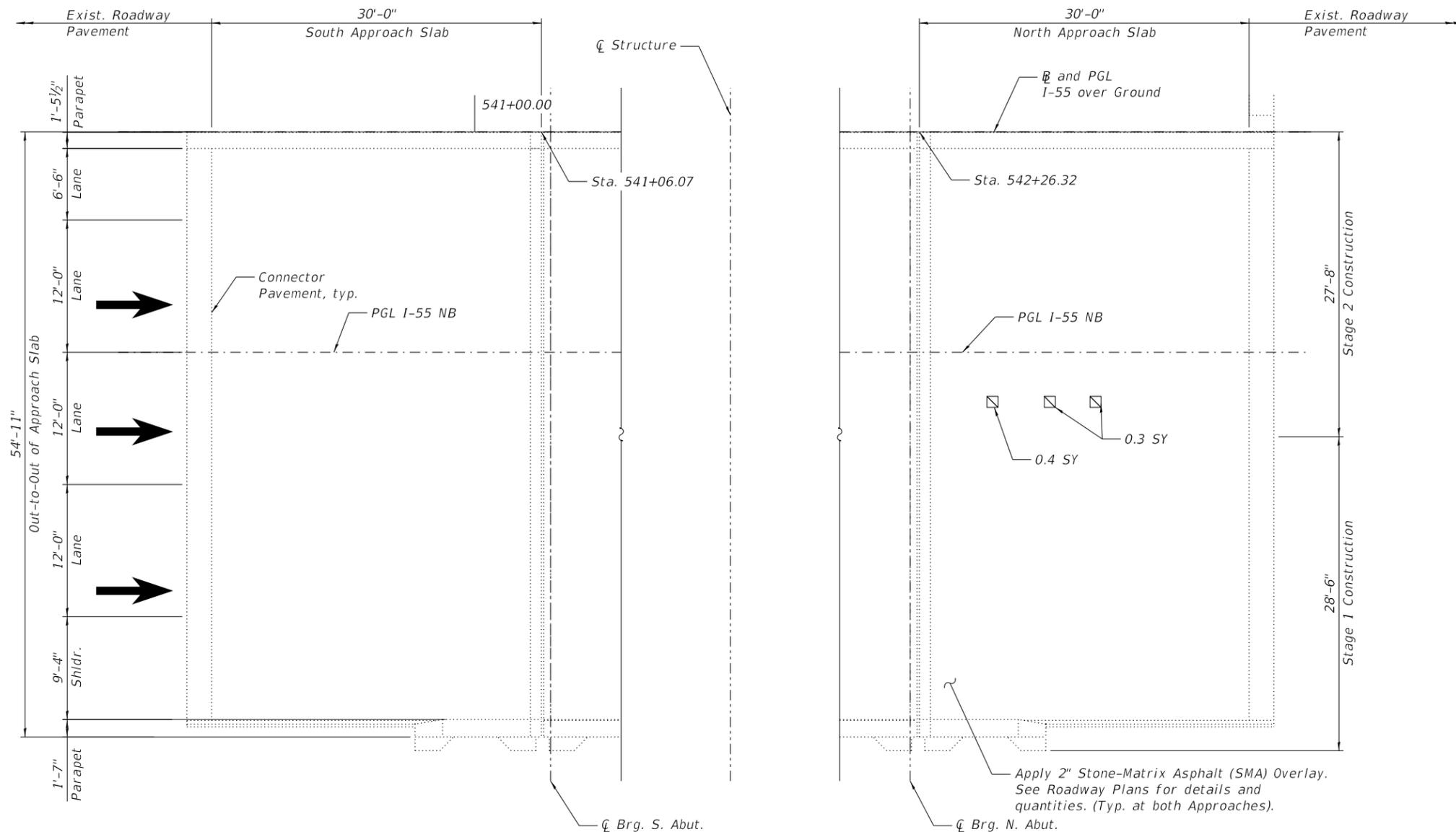
**BRIDGE DECK REPAIRS
S.N. 099-0022 I-55 NB OVER MATERIAL SERVICE RR (ABANDONED)**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	101
CONTRACT NO. 62N22				

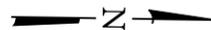
SHEET S4-06 OF S4-18 SHEETS

ILLINOIS FED. AID PROJECT

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PARTIAL PLAN
 (North and South Approaches)



BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Approach Slab Repair (Partial)	Sq. Yd.	1

NOTE:

1. Areas of Approach Slab Repair (Partial Depth) shown are estimated. The Engineer shall show actual location of repairs at the time of construction.

LEGEND



Approach Slab Repair (Partial Depth)

SY Square Yard



USER NAME =	DESIGNED - CP	REVISED -
	CHECKED - MI, MAI	REVISED -
PLOT SCALE =	DRAWN - CP	REVISED -
PLOT DATE =	DATE - 12/2/2020	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

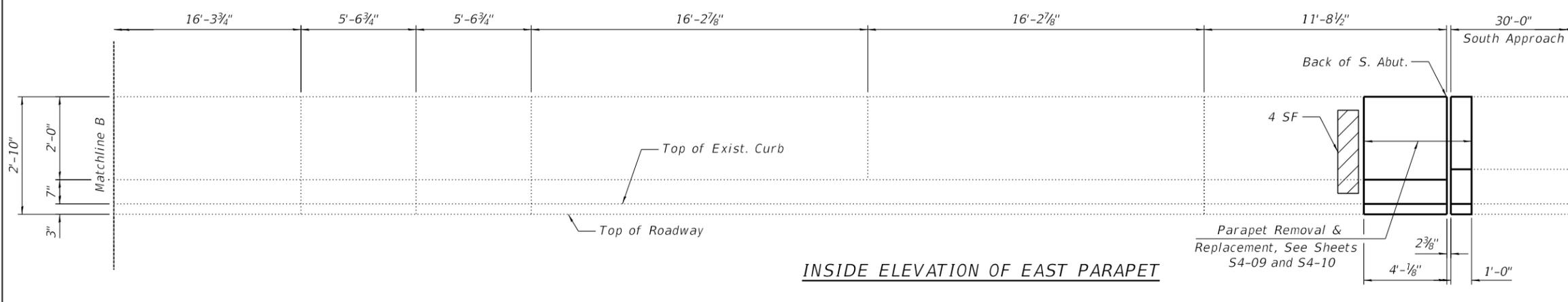
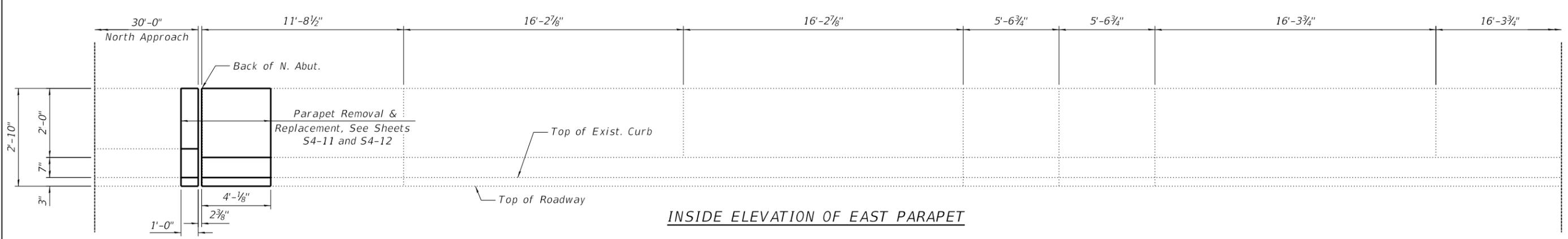
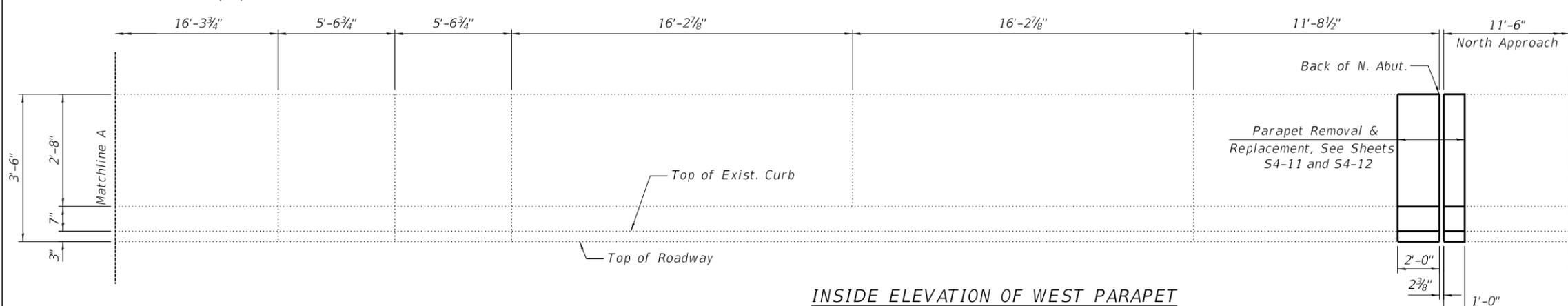
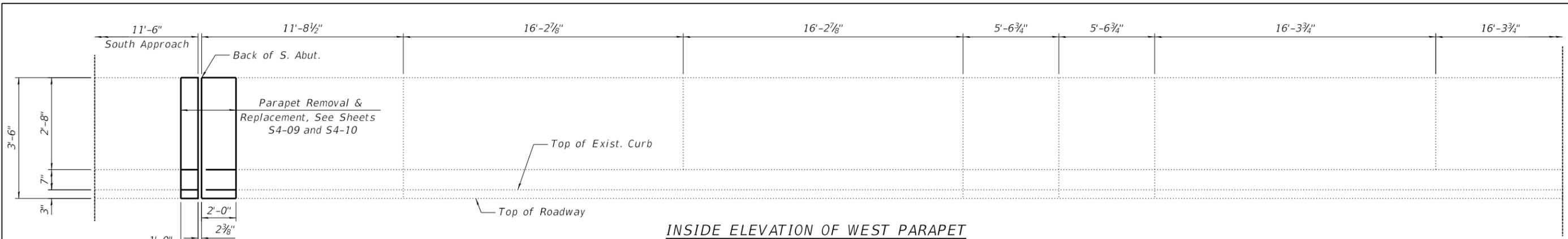
APPROACH SLAB REPAIRS
S.N. 099-0022 I-55 NB OVER MATERIAL SERVICE RR (ABANDONED)

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	102
CONTRACT NO. 62N22				

SHEET S4-07 OF S4-18 SHEETS

ILLINOIS FED. AID PROJECT

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BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth Equal to or Less than 5 inches)	Sq. Ft.	4

LEGEND

	Structural Repair of Concrete (Depth Equal to or Less than 5 inches)
SF	Square Foot

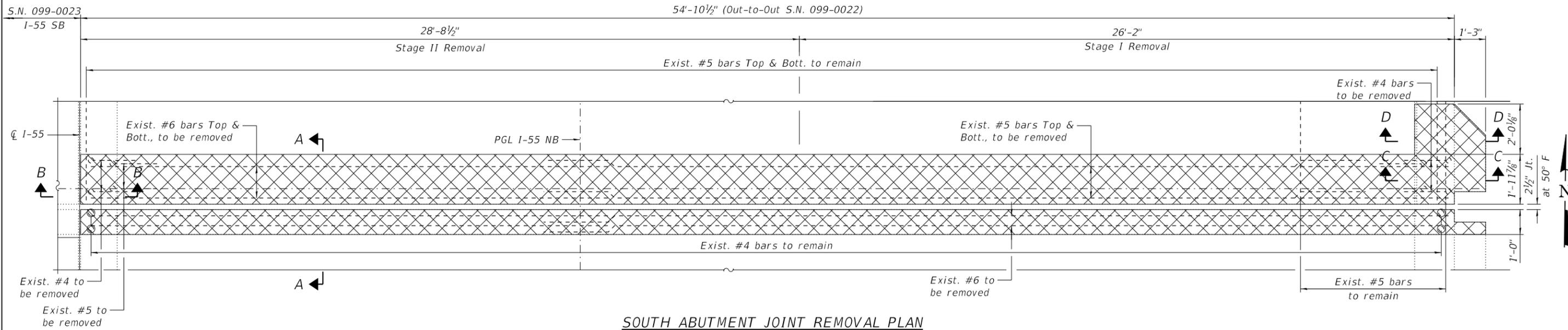


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CHECKED - MI, MAI	REVISED -	
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PLOT DATE =	DATE - 12/2/2020	REVISED -

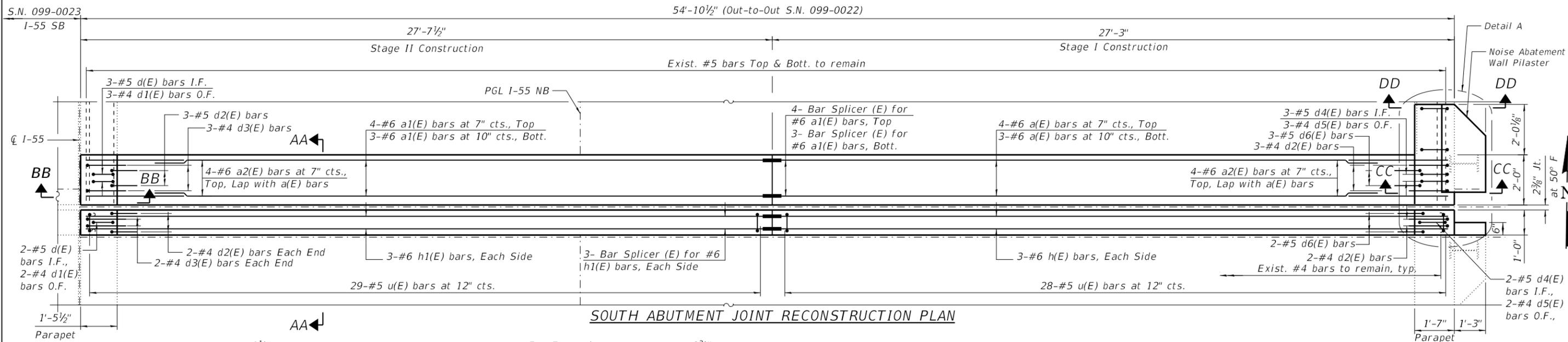
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

PARAPET REPAIRS
S.N. 099-0022 I-55 NB OVER MATERIAL SERVICE RR (ABANDONED)
 SHEET S4-08 OF S4-18 SHEETS

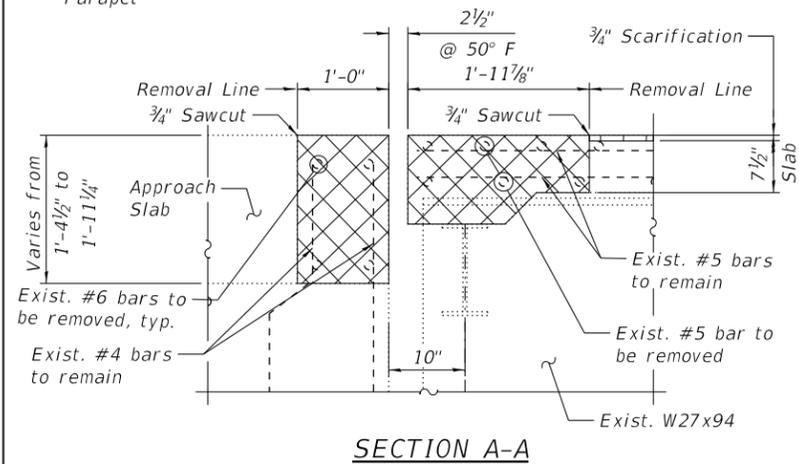
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	103
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				



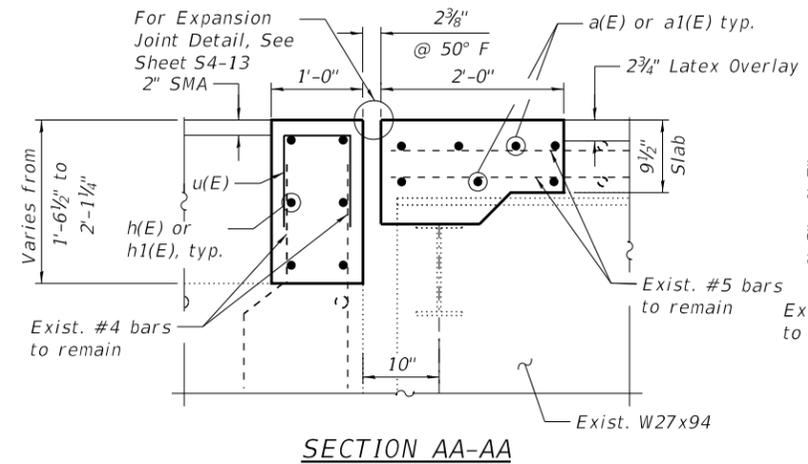
SOUTH ABUTMENT JOINT REMOVAL PLAN



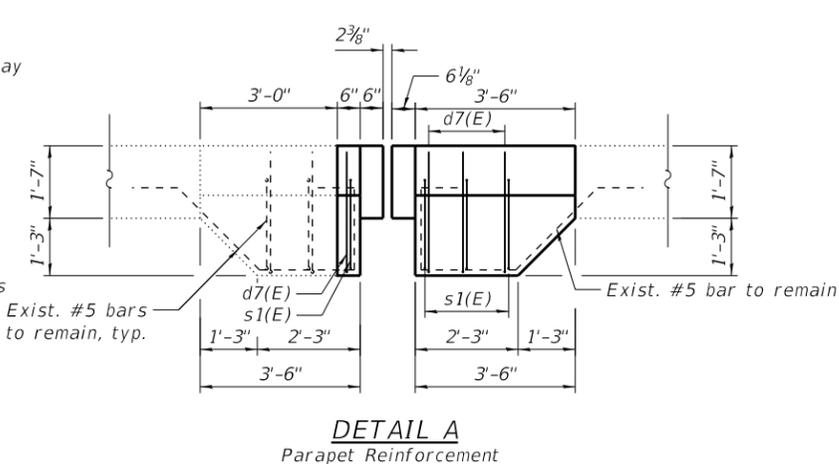
SOUTH ABUTMENT JOINT RECONSTRUCTION PLAN



SECTION A-A



SECTION AA-AA



DETAIL A
Parapet Reinforcement
not shown for clarity

NOTE:
1. For Notes, Sections, bar diagrams and Bill of Material, see Sheet S4-13

LEGEND:
Concrete Removal

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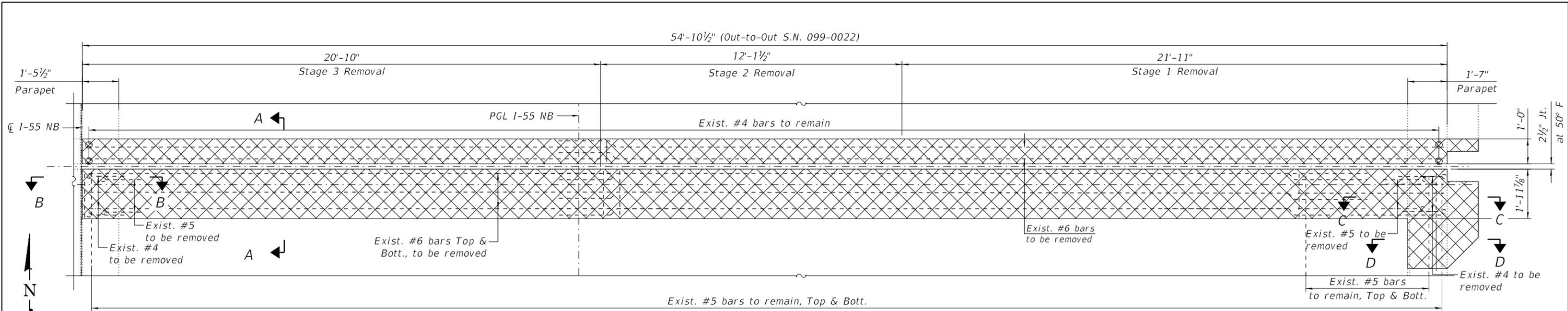
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**S. ABUT. JOINT REMOVAL & RECONSTRUCTION (SHEET 1 OF 2)
S.N. 099-0022 I-55 NB OVER MATERIAL SERVICE RR (ABANDONED)**

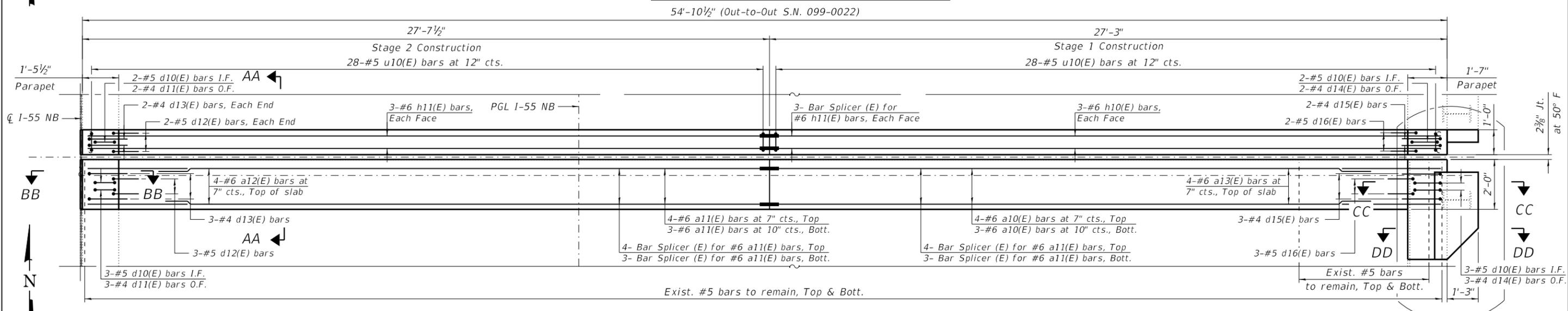
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	104
CONTRACT NO. 62N22				

SHEET S4-09 OF S4-18 SHEETS

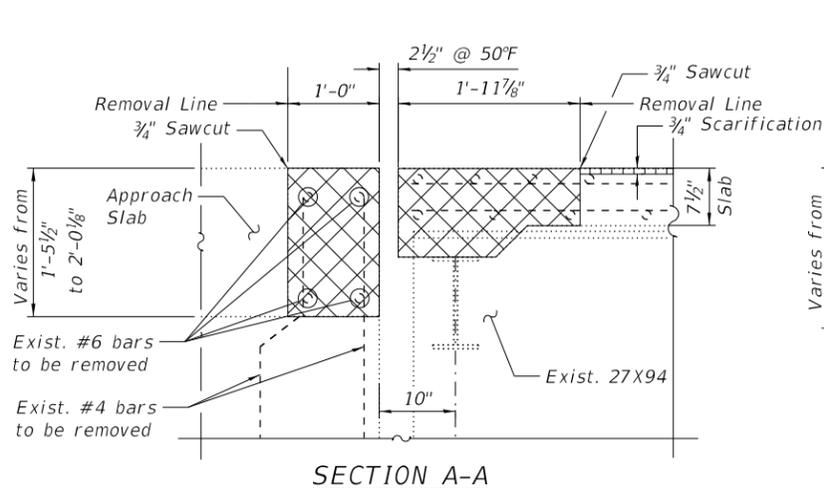
ILLINOIS FED. AID PROJECT



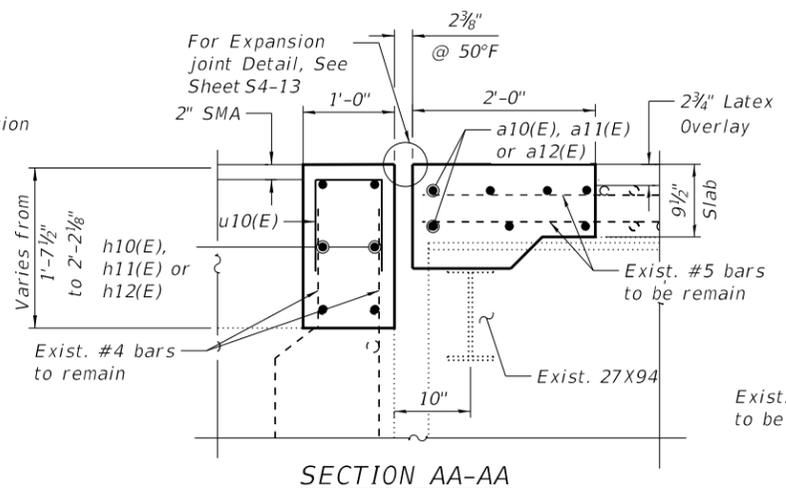
NORTH ABUTMENT JOINT REMOVAL PLAN



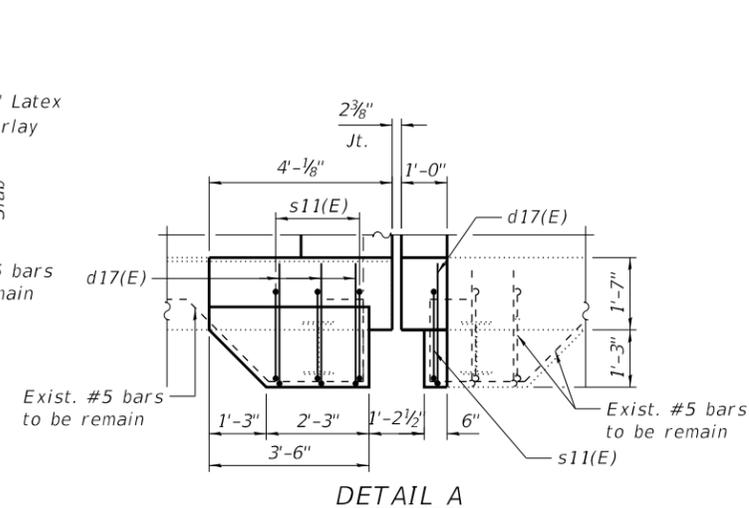
NORTH ABUTMENT JOINT RECONSTRUCTION PLAN



SECTION A-A



SECTION AA-AA



DETAIL A

NOTE:
1. For Notes, Sections, bar diagram and Bill of Material, see Sheet S4-12.

LEGEND
Concrete Removal

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

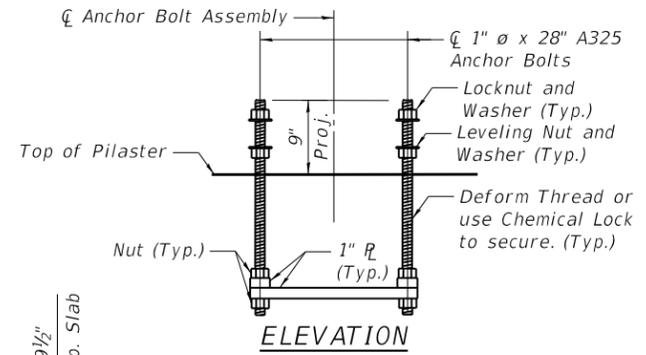
**N. ABUT. JOINT REMOVAL & RECONSTRUCTION (SHEET 1 OF 2)
S.N. 099-0022 I-55 NB OVER MATERIAL SERVICE RR (ABANDONED)**

SHEET S4-11 OF S4-18 SHEETS

F.A.I. RTE. I-55	SECTION 2020-253-BR&PP	COUNTY WILL	TOTAL SHEETS 178	SHEET NO. 106
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62N22	

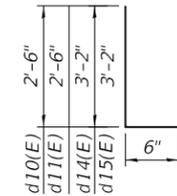
BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a10(E)	7	#6	27'-0"	—
a11(E)	7	#6	27'-5"	—
a12(E)	8	#6	6'-6"	—
d10(E)	5	#5	3'-8"	L
d11(E)	5	#4	3'-8"	L
d12(E)	10	#5	2'-5"	┘
d13(E)	5	#4	3'-11"	┘
d14(E)	5	#5	3'-0"	L
d15(E)	5	#4	3'-0"	L
d16(E)	5	#4	3'-9"	┘
d17(E)	4	#5	4'-0"	┘
h10(E)	6	#5	27'-0"	—
h11(E)	6	#5	27'-5"	—
s10(E)	1	#4	2'-7"	┘
s11(E)	4	#5	4'-4"	┘
u10(E)	57	#5	2'-8"	┘
Concrete Removal	CU YD		8.0	
Concrete Superstructure	CU YD		9.1	
Protective Coat	SQ YD		22	
Reinforcement Bars, Epoxy Coated	POUND		1,300	

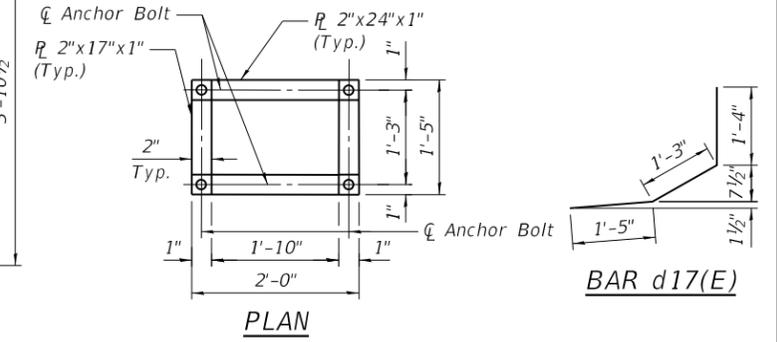


ANCHOR BOLT ASSEMBLY

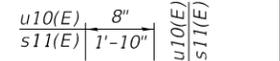
(All Anchor Bolts, Steel Plates, Nuts and Washers shall be Galvanized according to AASHTO M111 & M232 and cost included with Concrete Superstructure.)



BARS d10(E), d11(E), d14(E) & d15(E)



ANCHOR BOLT ASSEMBLY

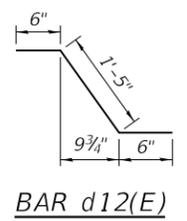


BARS s11(E) & u10(E)

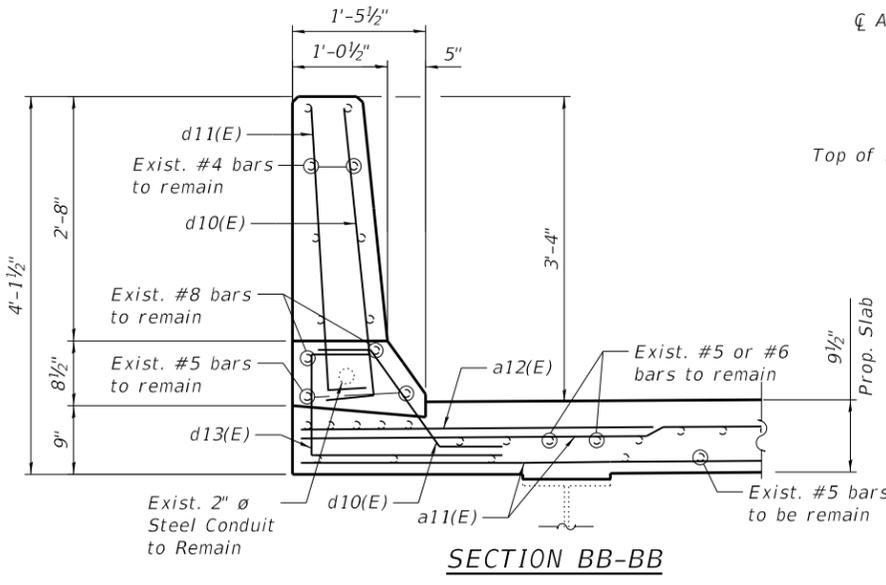


NOTES:

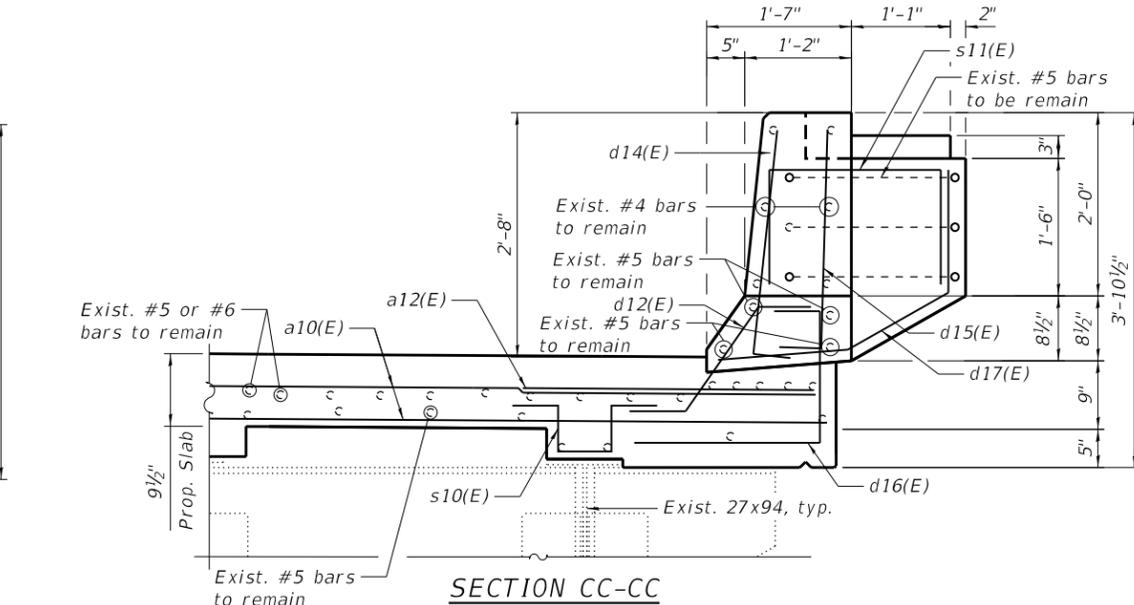
- For bar splicer assembly splicer details, see Sheet S4-18.
- Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.



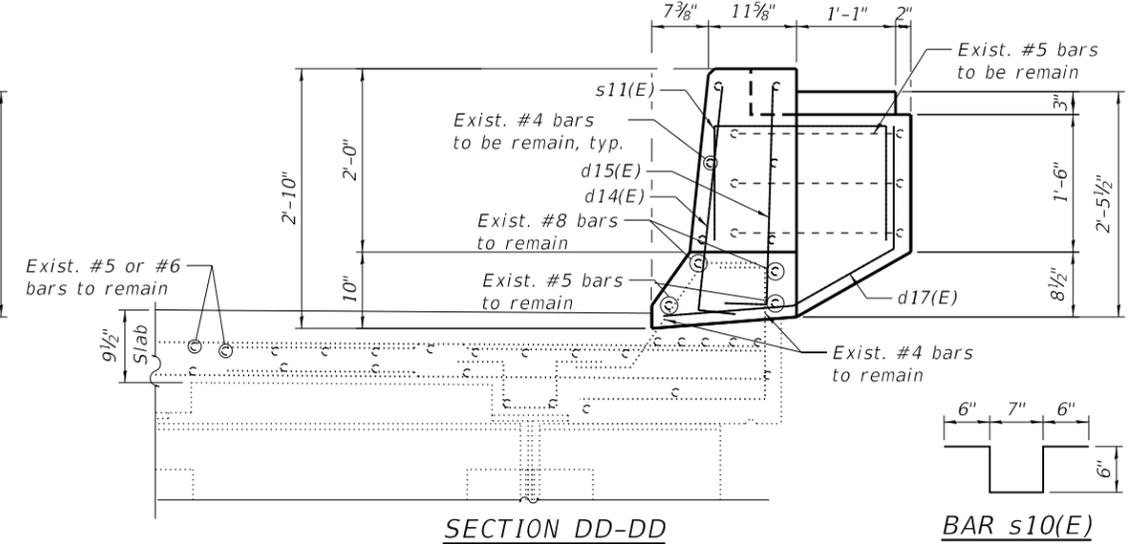
BARS d13(E) & d16(E)



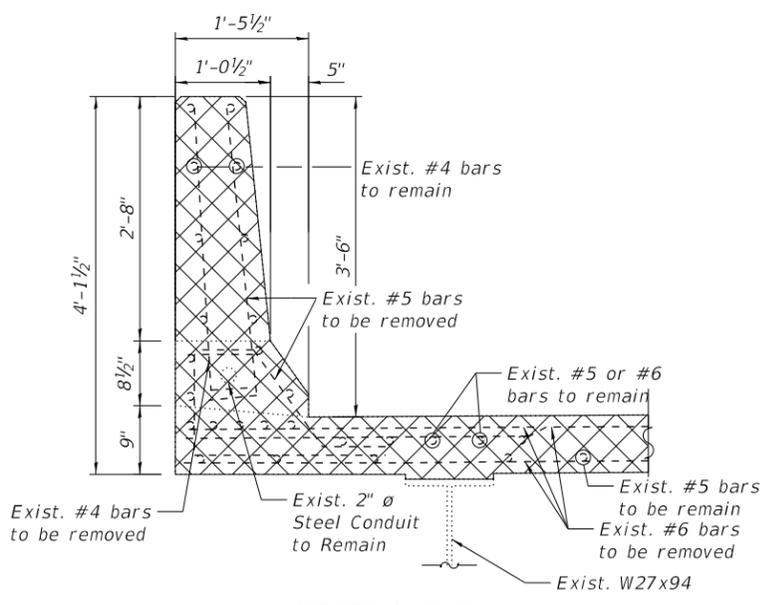
SECTION BB-BB



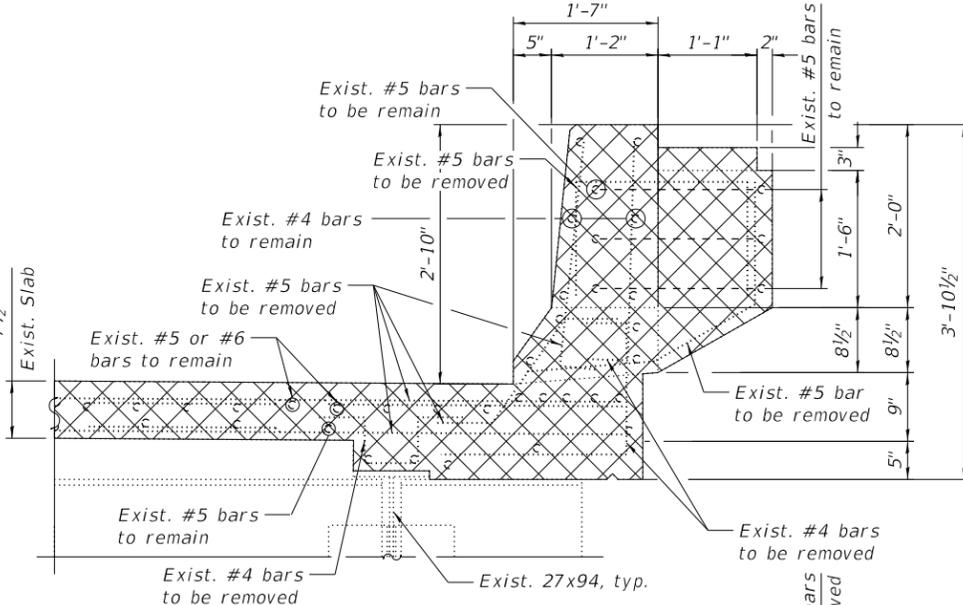
SECTION CC-CC



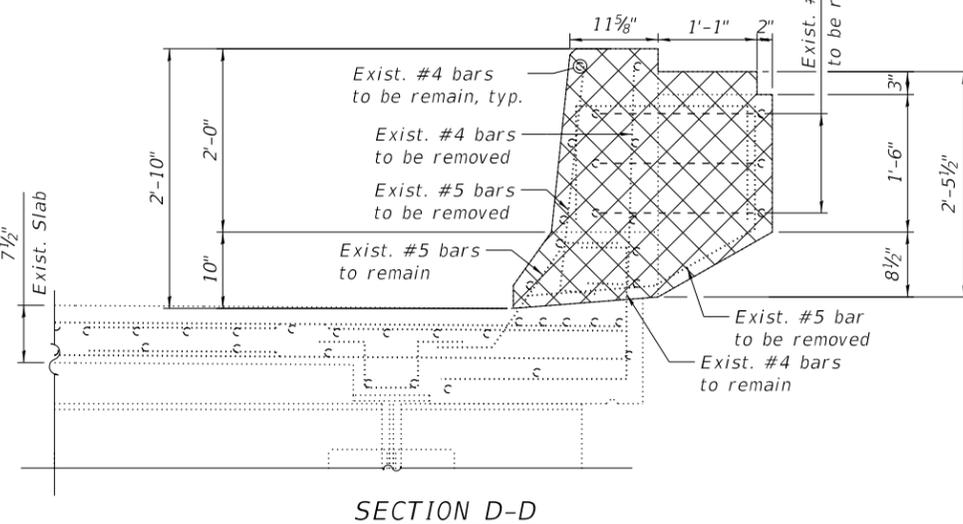
SECTION DD-DD



SECTION B-B



SECTION C-C



SECTION D-D

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	DATE - 12/2/2020	REVISED -

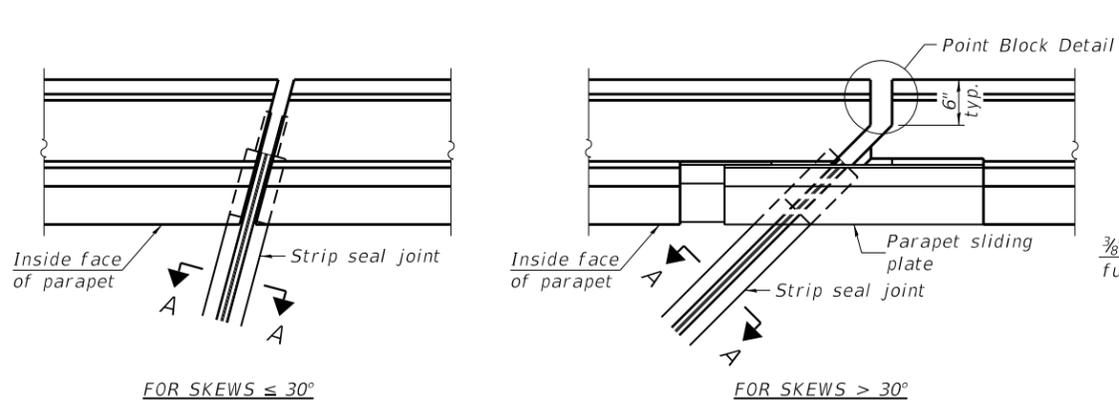
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

N. ABUT. JOINT REMOVAL & RECONSTRUCTION (SHEET 2 OF 2)
S.N. 099-0022 I-55 NB OVER MATERIAL SERVICE RR (ABANDONED)

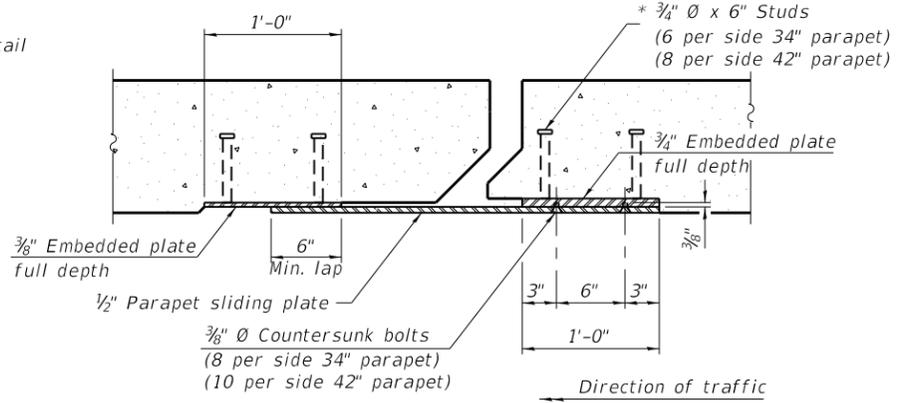
SHEET S4-12 OF S4-18 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	107
			CONTRACT NO. 62N22	
		ILLINOIS FED. AID PROJECT		

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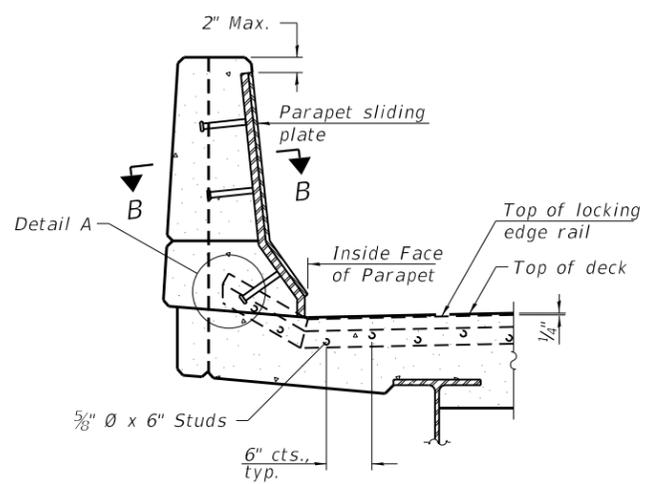


PLAN AT PARAPET



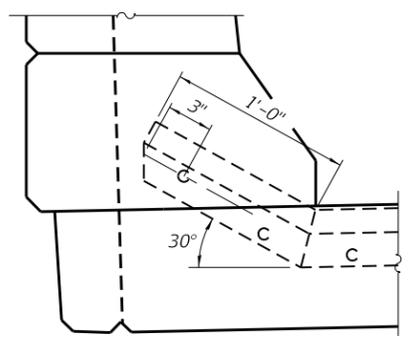
SECTION B-B

Notes:
 The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.
 The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the 4 1/2" maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.
 The manufacturer's recommended installation methods shall be followed.
 All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.
 The Maximum space between locking edge rail segments shall be 3/16" and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.

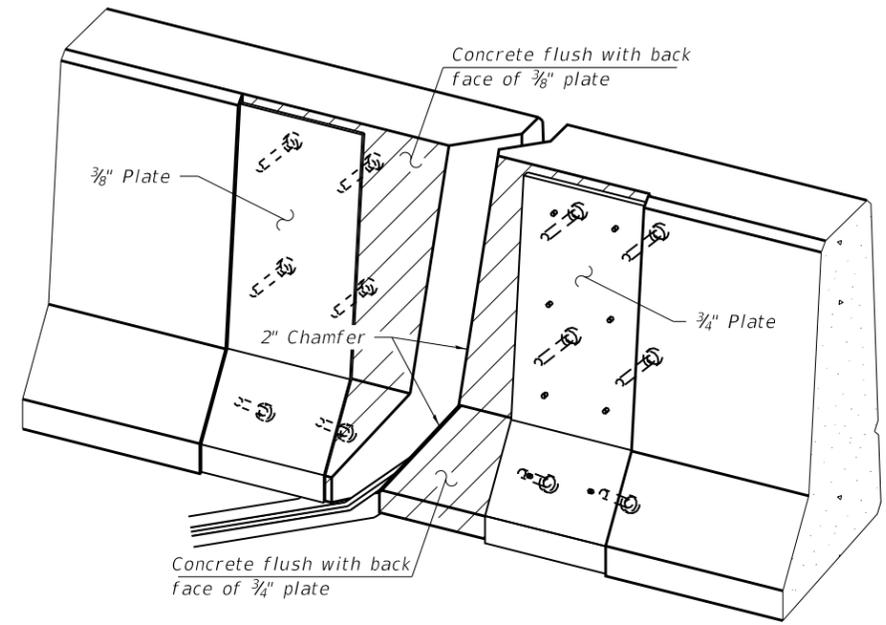


ELEVATION AT PARAPET

(Skews > 30° shown. Skews <= 30° similar except as shown in plan view.)

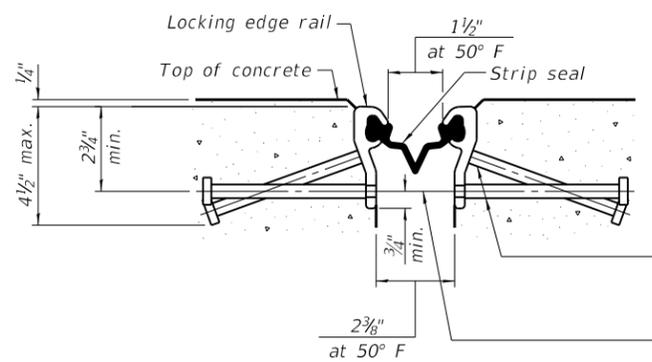


DETAIL A



TRIMETRIC VIEW
 (Showing embedded plates only)

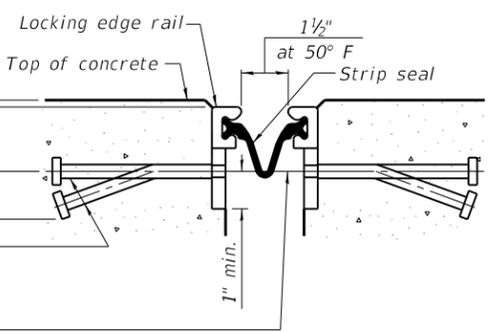
Cost of parapet sliding plates, embedded plates, and anchorage studs included with Preformed Joint Strip Seal.
 34" F-shape barrier shown, 42" F-shape similar as noted.
 The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required. One exception to this would be the strip seal joint at the end of the precast bridge approach slab. For these cases the pavement connector length shall be adjusted, not the length of the bridge approach slab.



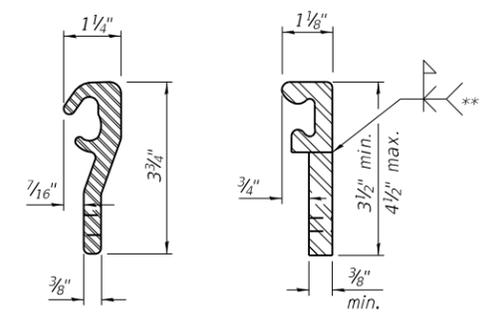
SHOWING ROLLED RAIL JOINT

* 3/8" diameter x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)

3/8" diameter threaded rods in 7/16" diameter holes at ±4'-0" cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

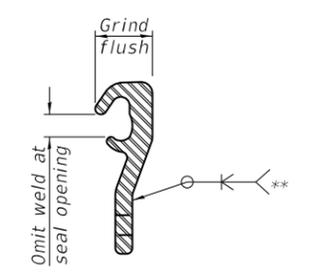


SHOWING WELDED RAIL JOINT



LOCKING EDGE RAILS

** Back gouge not required if complete joint penetration is verified by mock-up.



LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	110

EJ-SS

8-11-17



USER NAME =	DESIGNED - AMS	REVISED -
PLOT SCALE =	CHECKED - MI, MAI	REVISED -
PLOT DATE =	DRAWN - AMS	REVISED -
	DATE - 12/2/2020	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PREFORMED JOINT STRIP SEAL
 S.N. 099-0022 I-55 NB OVER MATERIAL SERVICE RR (ABANDONED)

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	108
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

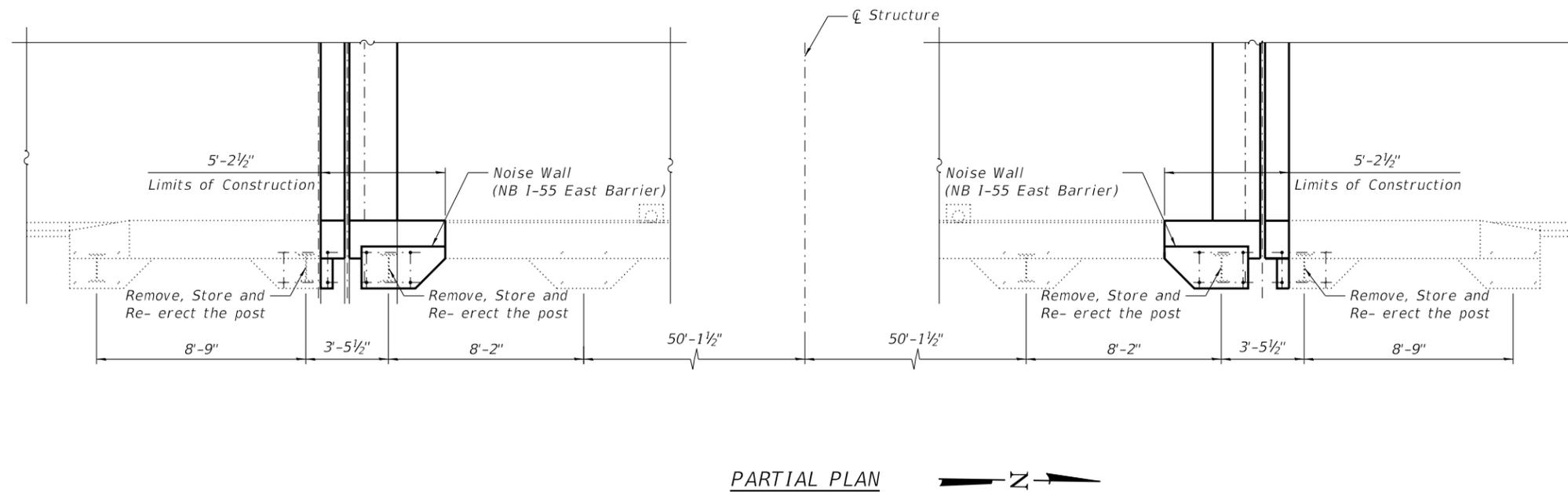
SHEET S4-13 OF S4-18 SHEETS

NOISE WALL REMOVAL & REERECTION SEQUENCE:

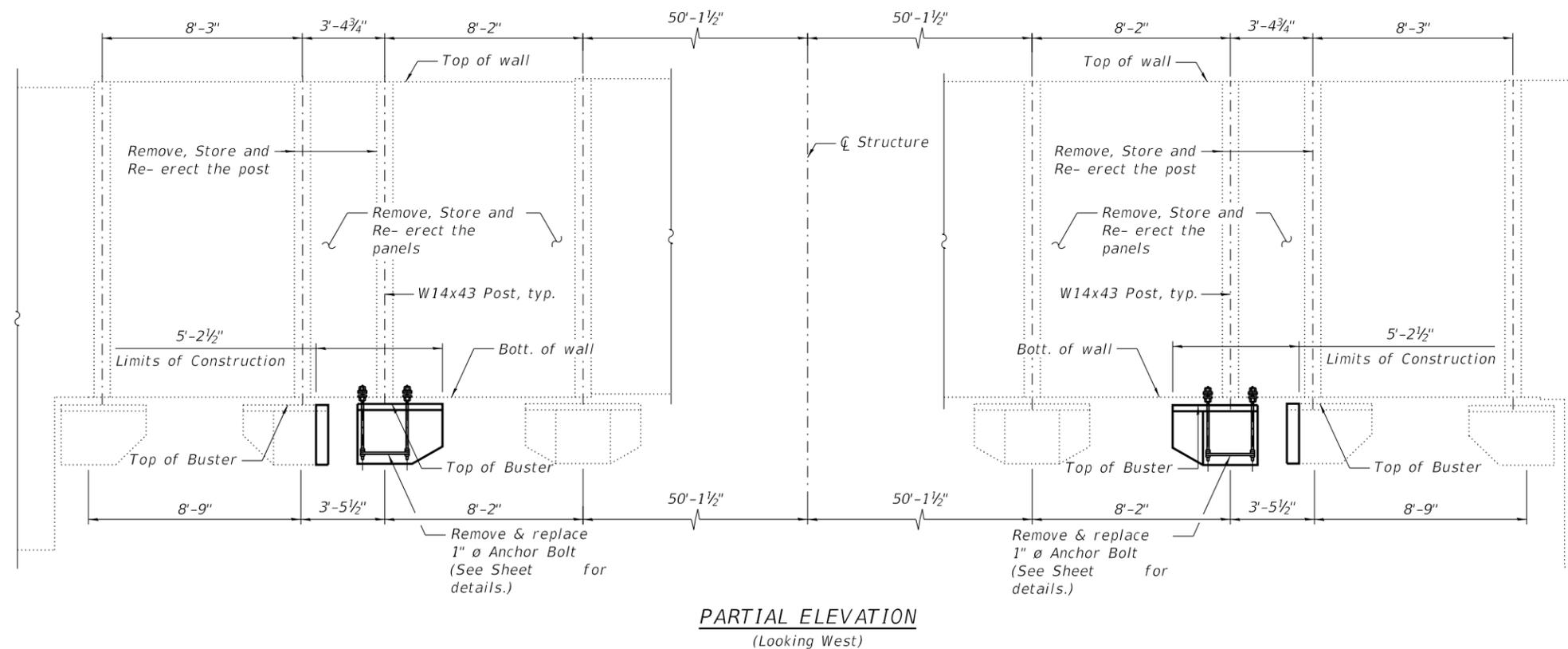
1. Remove existing 1" \varnothing anchor bolts.
2. Remove and store Noise Wall Panels, Noise Abatement Wall Post and Pilaster.
3. After reconstructing the barrier, re-erect Pilaster, and Noise Abatement Wall Post using new 1" \varnothing anchor bolts.
4. Install stored Noise Wall Panels as shown.

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Noise Abatement Wall Panel Removal and Re-erection	Each	2



PARTIAL PLAN



PARTIAL ELEVATION

(Looking West)

NOTE:

1. Verify all elevations in the field.

MODEL: Default
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12/2/2020 3:29:08 PM



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PLOT SCALE =	DRAWN - FL	REVISED -
PLOT DATE =	DATE - 12/2/2020	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

NOISE WALL DETAILS
S.N. 099-0022 I-55 NB OVER MATERIAL SERVICE RR (ABANDONED)

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	109
			CONTRACT NO. 62N22	
		ILLINOIS FED. AID PROJECT		

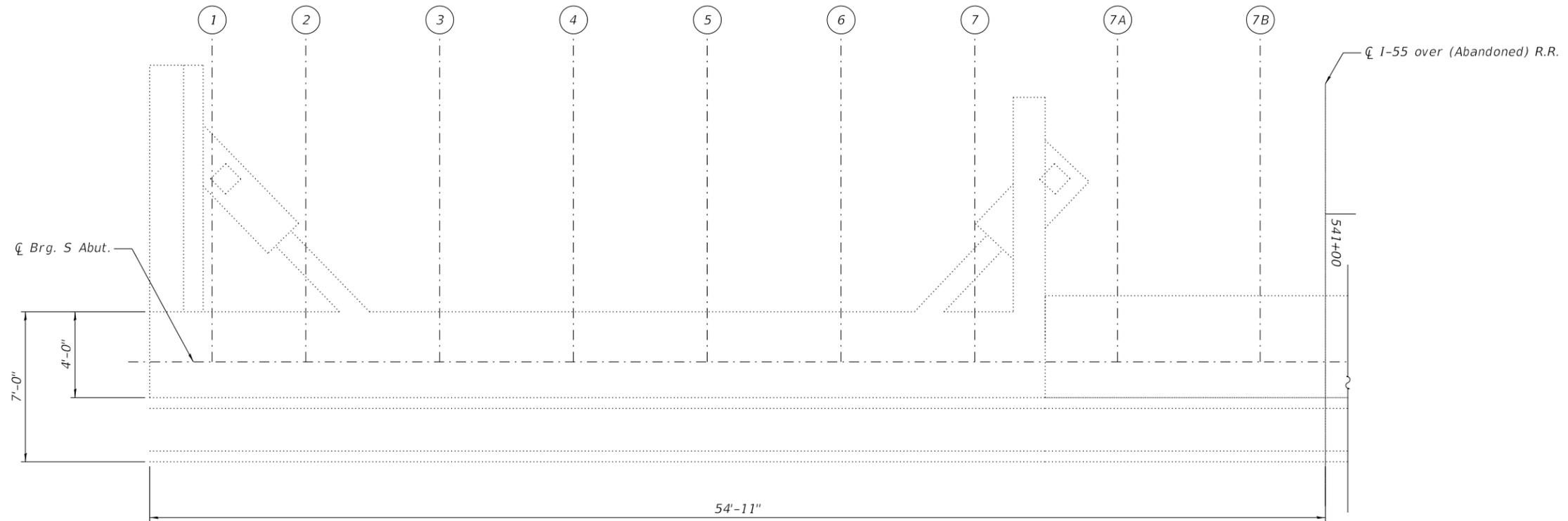
SHEET S4-14 OF S4-18 SHEETS

NOTE:

1. Quantities and limits shown are estimated for bidding purpose only. The actual areas to be repaired and the type(s) of repairs to be used will be determined by the engineer in the field at the time of construction.

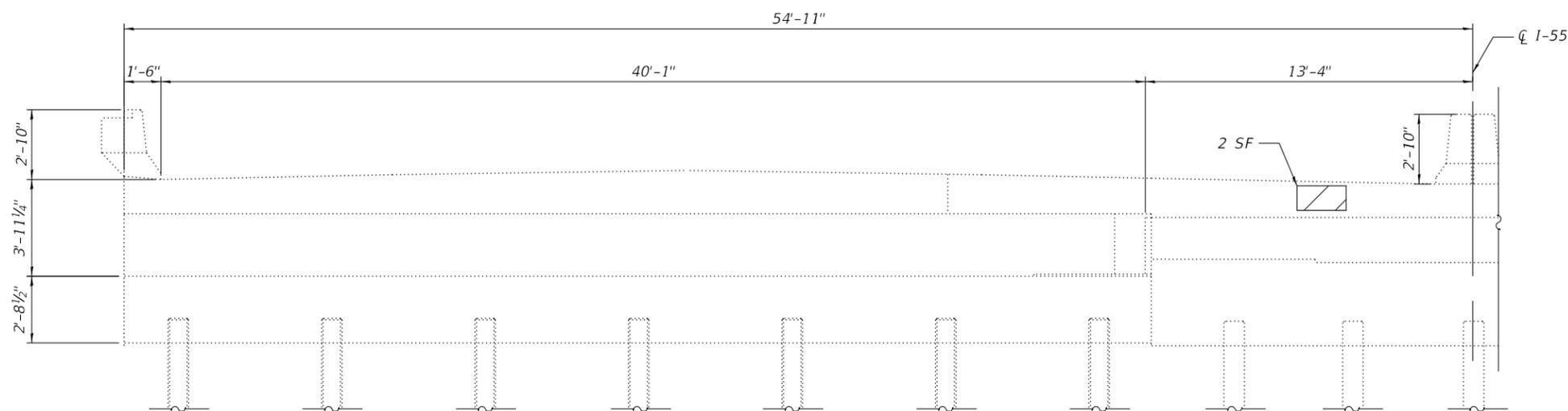
BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Concrete Sealer	Sq. Ft.	127
Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)	Sq. Ft.	2



PLAN

N



SOUTH ABUTMENT

(Looking South)

LEGEND



Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)

MODEL: Default
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PLOT SCALE =	DRAWN - AMS	REVISED -
PLOT DATE =	DATE - 12/2/2020	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SOUTH ABUTMENT REPAIRS
S.N. 099-0022 I-55 NB OVER MATERIAL SERVICE RR (ABANDONED)**

SHEET S4-15 OF S4-18 SHEETS

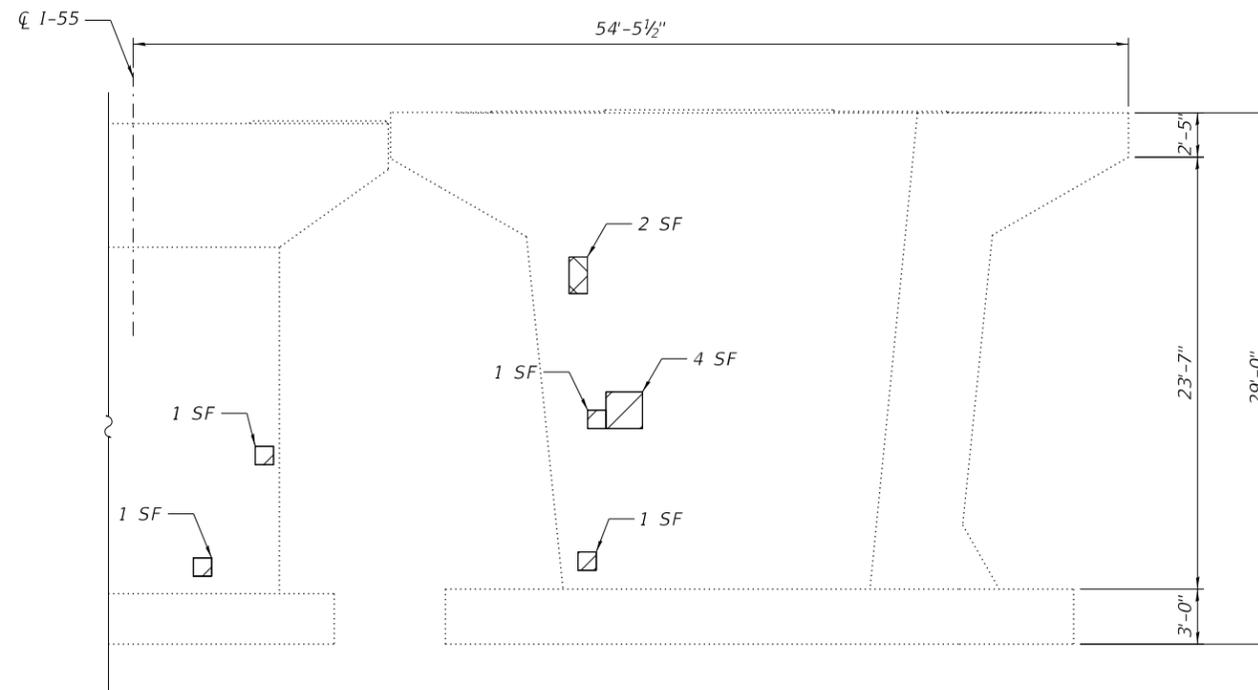
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	110
			CONTRACT NO. 62N22	
ILLINOIS		FED. AID PROJECT		

NOTE:

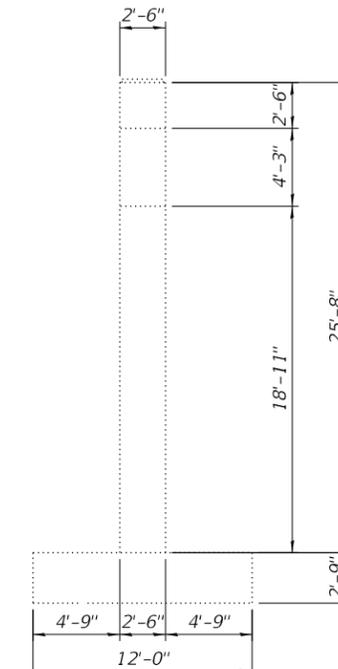
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BILL OF MATERIAL

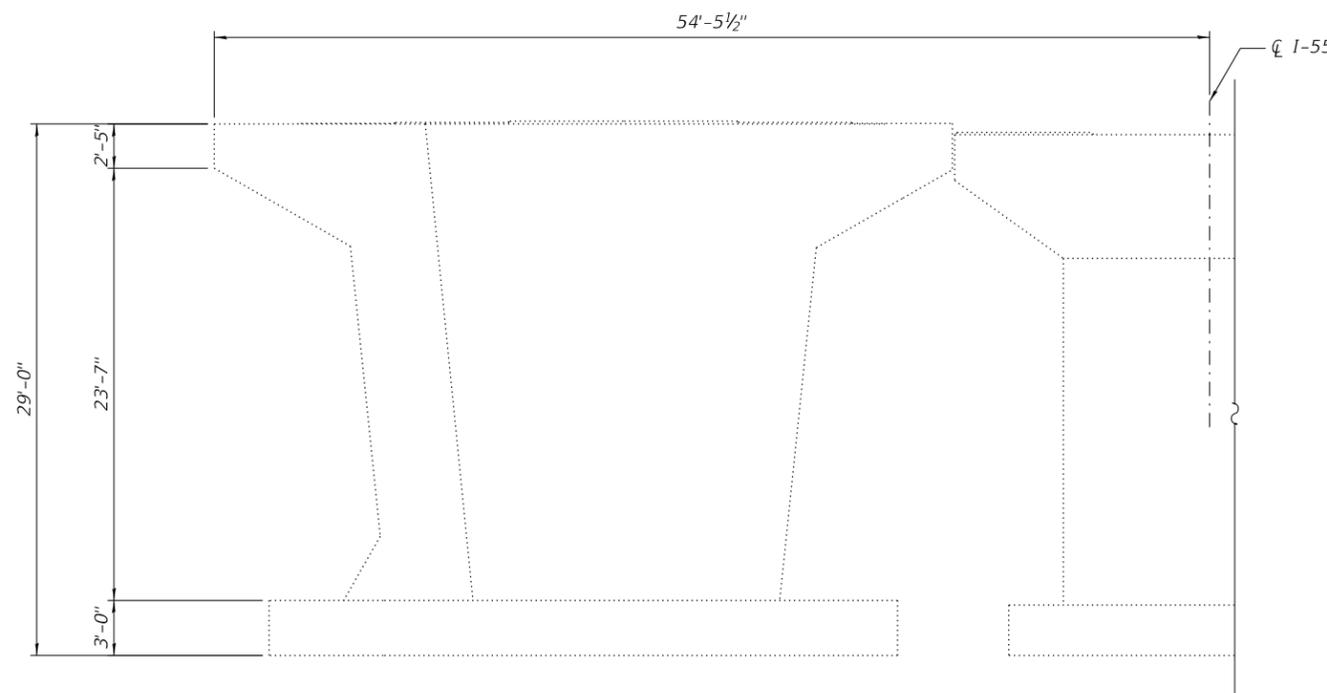
ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth Equal to or Less than 5 Inches)	Sq. Ft.	10



PIER 1 ELEVATION
(Looking North)



END ELEVATION
(Looking West)



PIER 1 ELEVATION
(Looking South)

LEGEND



Structural Repair of Concrete (Depth Equal to or Less than 5 Inches)

SF Square Feet

MODEL: Default
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PLOT DATE =	DATE - 12/2/2020	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

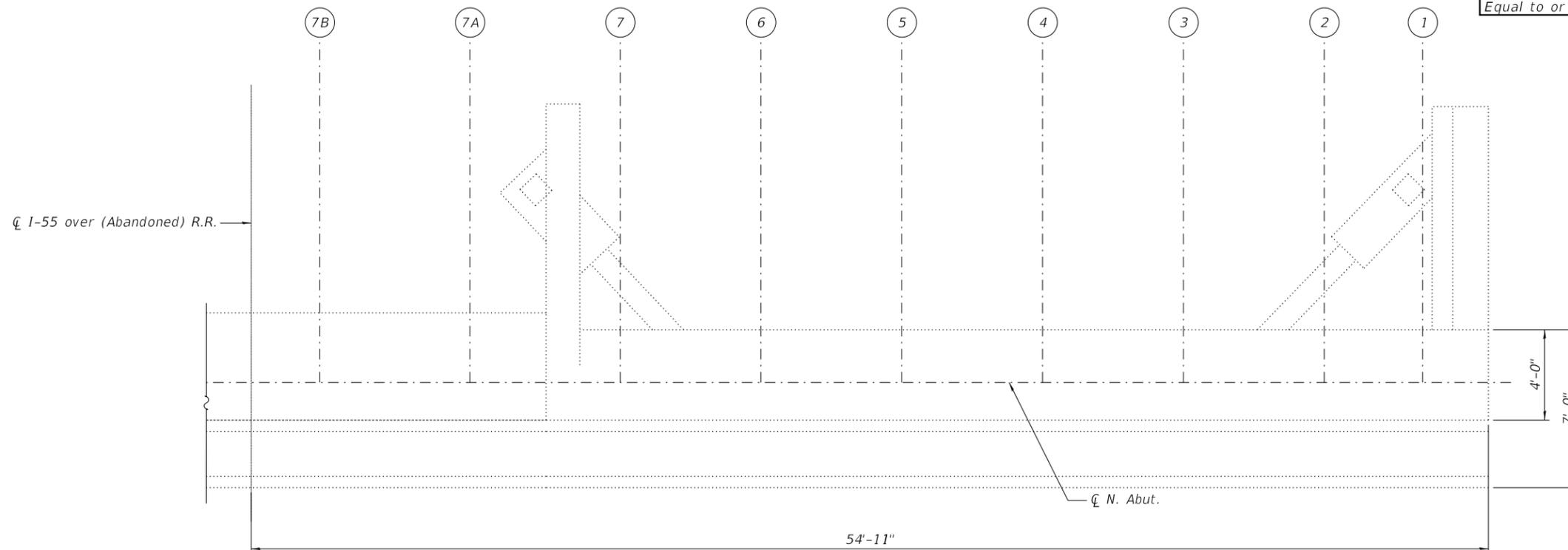
PIER 1 REPAIRS
S.N. 099-0022 I-55 NB OVER MATERIAL SERVICE RR (ABANDONED)

SHEET S4-16 OF S4-18 SHEETS

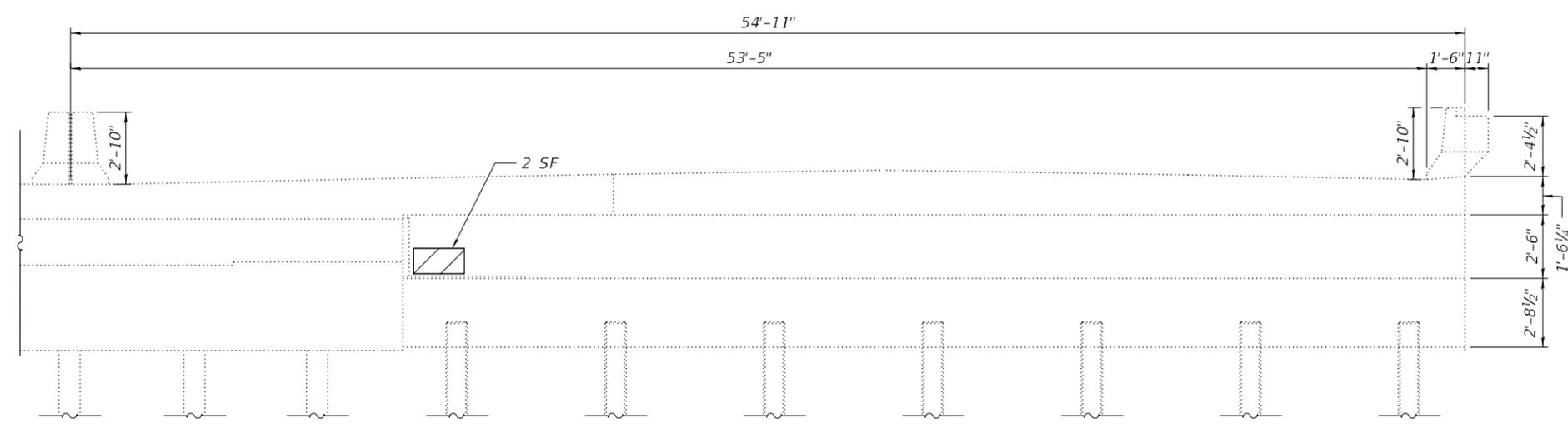
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	111
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62N22	

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Concrete Sealer	Sq. Ft.	127
Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)	Sq. Ft.	2



PLAN



NORTH ABUTMENT
(Looking North)

NOTE:

1. Quantities and limits shown are estimated for bidding purpose only. The actual areas to be repaired and the type(s) of repairs to be used will be determined by the engineer in the field at the time of construction.

LEGEND:

- Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)
- SF Square Feet

MODEL: Default
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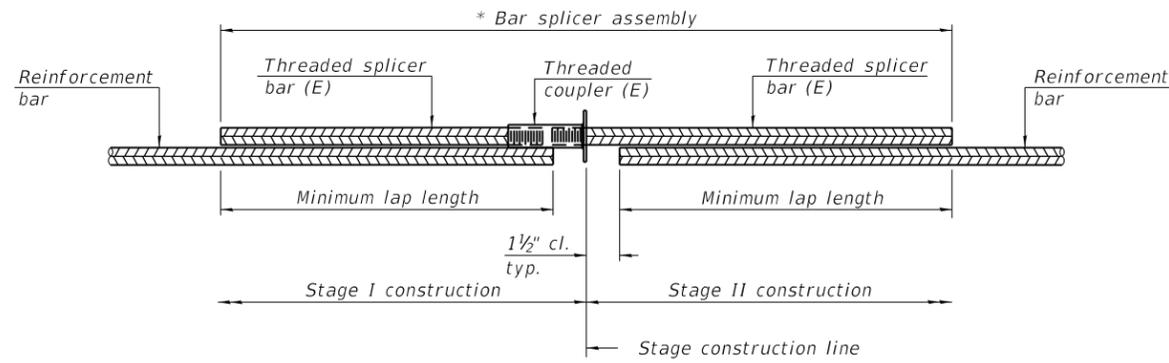
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PLOT DATE =	DATE - 12/2/2020	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

NORTH ABUTMENT REPAIRS
S.N. 099-0022 I-55 NB OVER MATERIAL SERVICE RR (ABANDONED)

SHEET S4-17 OF S4-18 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	112
CONTRACT NO. 62N22				
ILLINOIS		FED. AID PROJECT		

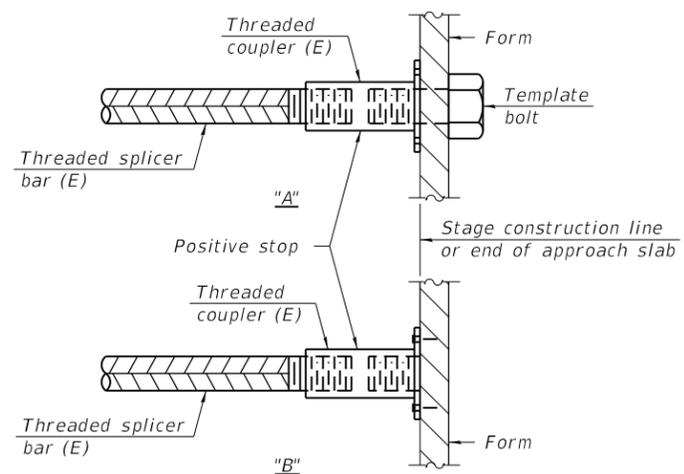


STANDARD BAR SPLICER ASSEMBLY PLAN
 (All components shall be provided from one supplier)

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

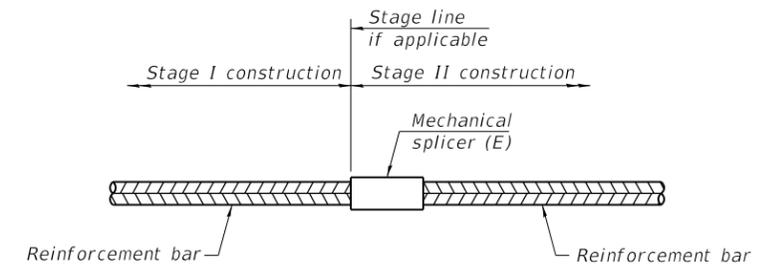
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
South Abutment	#6	13	4'-10"
North Abutment	#6	13	4'-10"



INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required

Notes:
 Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

MODEL: Default
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PLOT DATE =	DATE - 12/2/2020	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY DETAILS
 S.N. 099-0022 I-55 NB OVER MATERIAL SERVICE RR (ABANDONED)

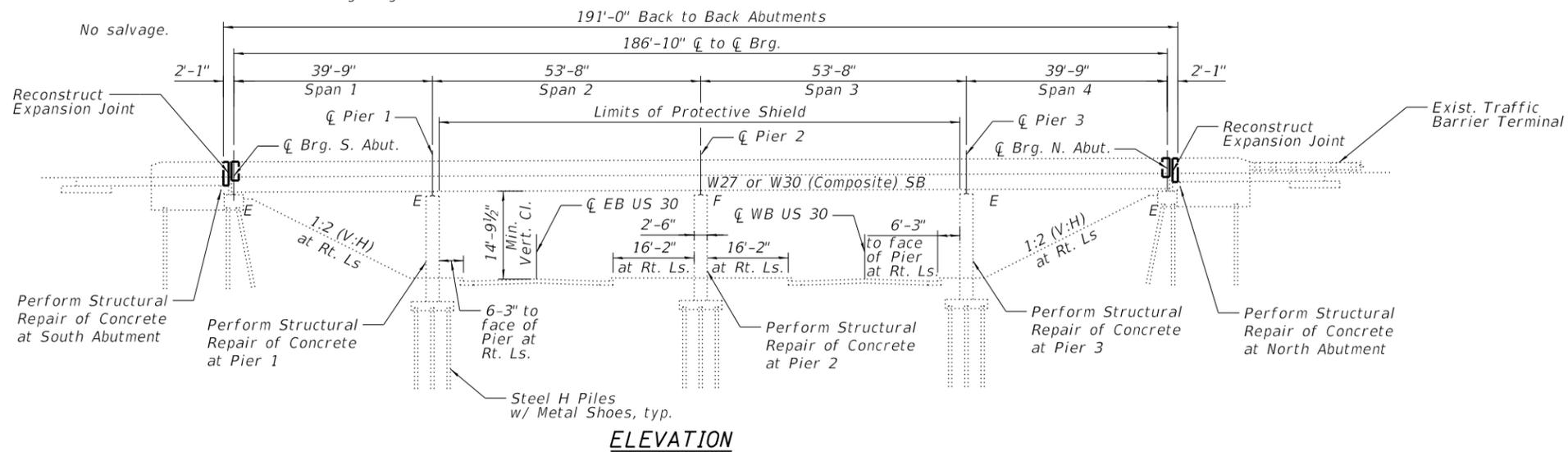
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	113
			CONTRACT NO. 62N22	
		ILLINOIS	FED. AID PROJECT	

SHEET S4-18 OF S4-18 SHEETS

Existing Structure: The existing Structure No. 099-0016 (SB bridge) and Structure No. 099-0017 (NB bridge) F.A.I.-55 were built in 1955 as F.A. Route 34 - Project IN-187(9), Section 27HB, widened in 1977 as U.S. 30 - Project I-55-6 I-55-6(123)246 Section (99-1&2)R-5. In 1994, the concrete deck was scarified 1/4" and resurfaced and the superstructure was rehabilitated as F.A.I. Route 55 (Stevenson Expressway) Section 27HB-BR(89). As part of project F.A.I. Route 55 Section 2006-032, the superstructures and substructures were widened to the inside in 2007 and a new 7 1/2" concrete deck was constructed. The structures are 4-span continuous non-composite wide flange beam bridges with a 7 1/2" concrete deck, 54'-11 1/2" out to out of structure and 191'-0" Back-to-Back Abutments.

Traffic will be maintained utilizing staged construction.

No salvage.



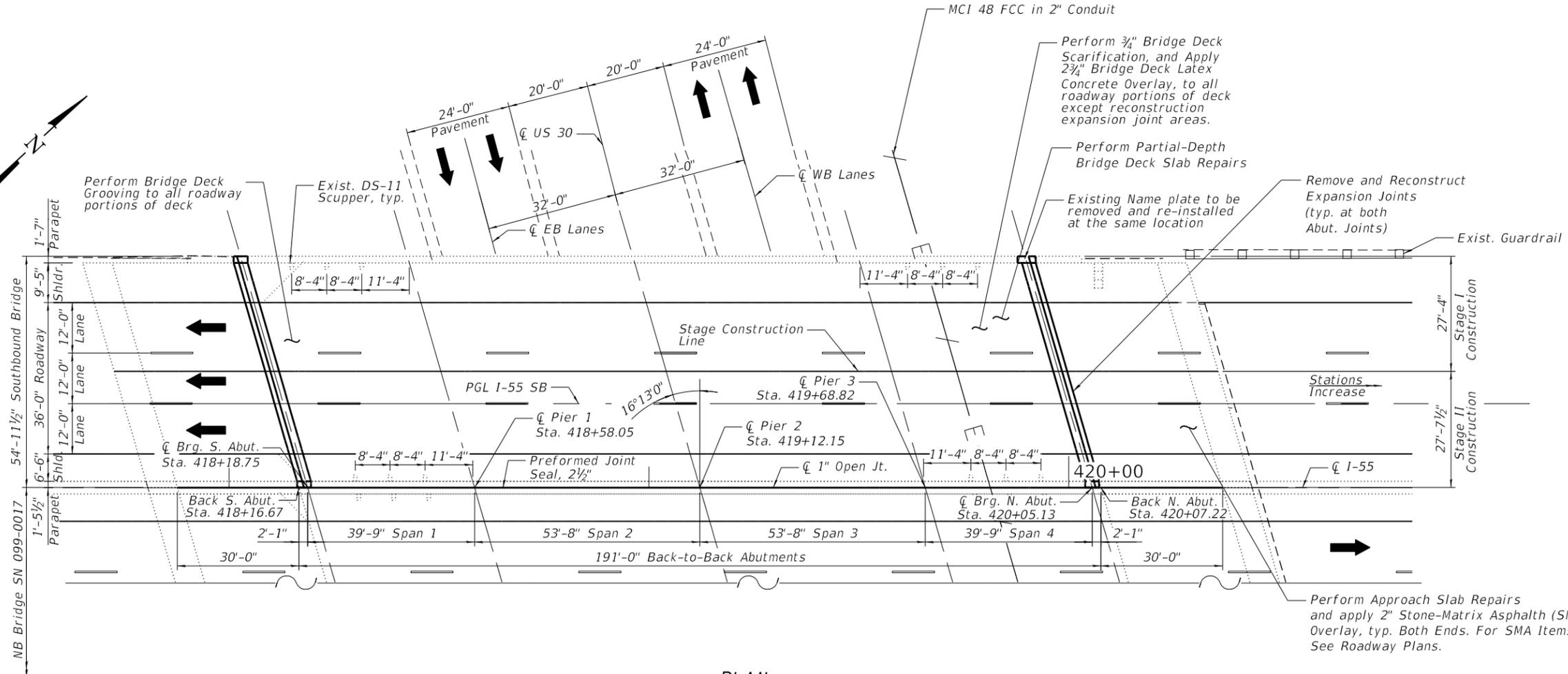
ELEVATION

SCOPE OF WORK

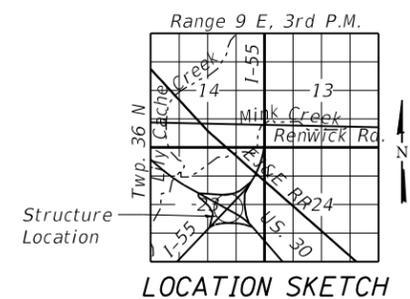
1. Scarify 3/4" from the bridge deck slab.
2. Perform Deck Slab and Approach Slab repairs as required.
3. Remove and Reconstruct Expansion Joints at North and South Abutments and install new preformed joint strip seals.
4. Apply a 2 3/4" Bridge Deck Latex Concrete Overlay and Protective Coat on Bridge Deck and 2" Stone-Matrix Asphalt (SMA) Overlay on the Approach Slabs.
5. Perform Bridge Deck Grooving.
6. Apply Protective Coat to the top and inside faces of parapets, reconstructed transverse expansion joints and to the surface of the new overlay.
7. Perform Structural Concrete repairs to the Abutments and Piers as noted in the plans.
8. Existing name plate to be removed, cleaned and re-installed at the same location. Cost included with Concrete Removal.



DATE SIGNED:
EXP. DATE: 11/30/2022
SHEETS S1-01 THRU S1-15



PLAN



GENERAL PLAN AND ELEVATION
SB I-55 OVER US ROUTE 30
F.A.I. 55 SEC 2020-253-BR&PP
WILL COUNTY
STATION: 419+12.15
STRUCTURE NO. 099-0016

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SN 099-0016 SB I-55 OVER US ROUTE 30

SHEET S5-01 OF S5-15 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	114
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

GENERAL NOTES

1. Reinforcement bars designated (E) shall be epoxy coated.
2. Prior to pouring the new concrete deck for Expansion Joints Reconstruction and Deck slab repairs, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.
3. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
4. Joint openings shall be adjusted according to Article 520.04 of the Standard Specification when the deck is poured at an ambient temperature other than 50° F.
5. Bars indicated thus, 3x2-#5, indicates 3 lines of #5 bars with 2 lengths of bar per line.
6. All exposed concrete edges shall have a 3/4" chamfer, except where shown otherwise.
7. During repair operations, the Contractor shall locate and protect all utilities in the vicinity of the work including, but not limited to, fiber optic and/or electrical conduits, conduits under the bridge deck, under-deck lighting, traffic signals or signs attached to the structure. This work shall be performed to the satisfaction of the Engineer and will not be paid for separately, but shall be included with the contract. It shall be the Contractor's responsibility to restore and replace any damaged utilities or facilities to the satisfaction of the Engineer at no cost to the Department.
8. Expansion joints shall be fabricated to conform to the existing cross slopes of the bridge.
9. Protective Coat shall be applied to the top and inside face of parapets, reconstructed transverse Expansion Joints and to the surface of the new overlay.
10. Existing reinforcement extended into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal operations shall be replaced using an approved bar splicer or anchorage system. Cost included with Concrete Removal.
11. The Contractor is responsible to protect the existing conduit embedded in the parapet during concrete removal and construction. Any damage to the existing conduit shall be repaired by the Contractor at no additional cost to the Department.

INDEX OF SHEETS

S5-01	General Plan and Elevation
S5-02	General Notes, Total Bill of Materials, & Index of Sheets
S5-03-55-04	Construction Staging Details
S5-05	Temporary Concrete Barrier for Stage Construction
S5-06	Bridge Deck Repairs
S5-07	Parapet Repairs
S5-08	Expansion Joint Removal and Construction
S5-09	Expansion Joint Details
S5-10	Preformed Joint Strip Seal
S5-11	Pier 1 Repairs
S5-12	Pier 2 Repairs
S5-13	Pier 3 Repairs
S5-14	North and South Abutment Repairs
S5-15	Bar Splicer Assembly and Mechanical Splicer Details

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Concrete Removal	Cu Yd	16.2		16.2
Protective Shield	Sq Yd	656		656
Concrete Superstructure	Cu Yd	17.9		17.9
Bridge Deck Grooving	Sq Yd	1092		1092
Protective Coat	Sq Yd	1457		1457
Reinforcement Bars, Epoxy Coated	Pound	2220		2220
Bar Splicers	Each	24		24
Preformed Joint Seal 2 1/2"	Foot	249		249
Preformed Joint Strip Seal	Foot	115		115
Concrete Sealer	Sq Ft		333	333
Approach Slab Repair (Partial Depth)	Sq Yd	1		1
Bridge Deck Latex Concrete Overlay, 2 3/4"	Sq Yd	1108		1108
Bridge Deck Scarification 3/4"	Sq Yd	1108		1108
Structural Repair of Concrete (Depth Equal to or less than 5")	Sq Ft	5.5	52.5	58

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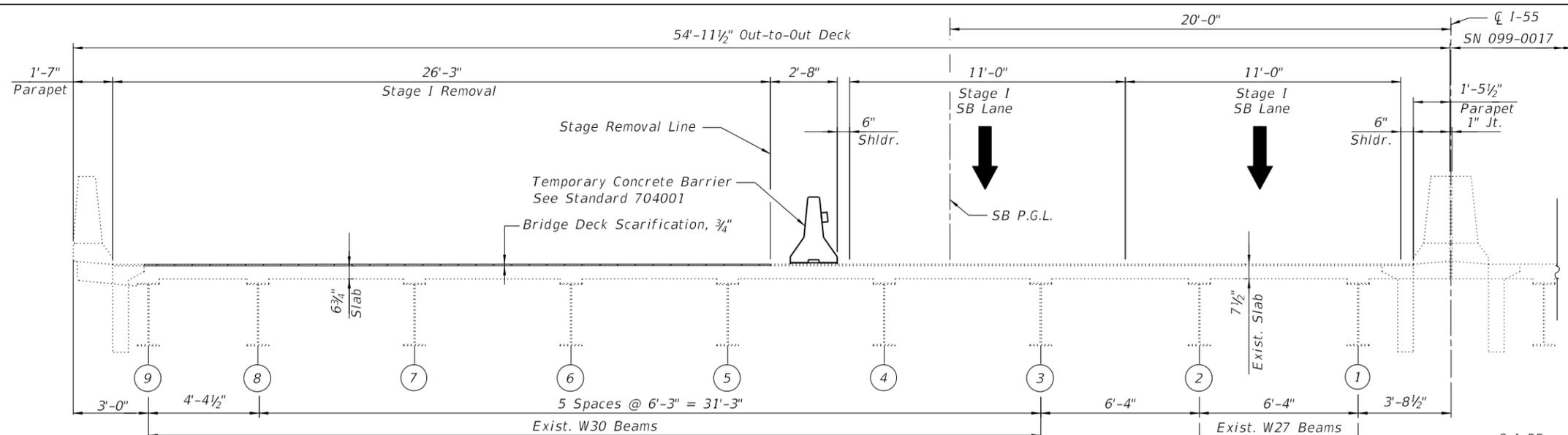
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

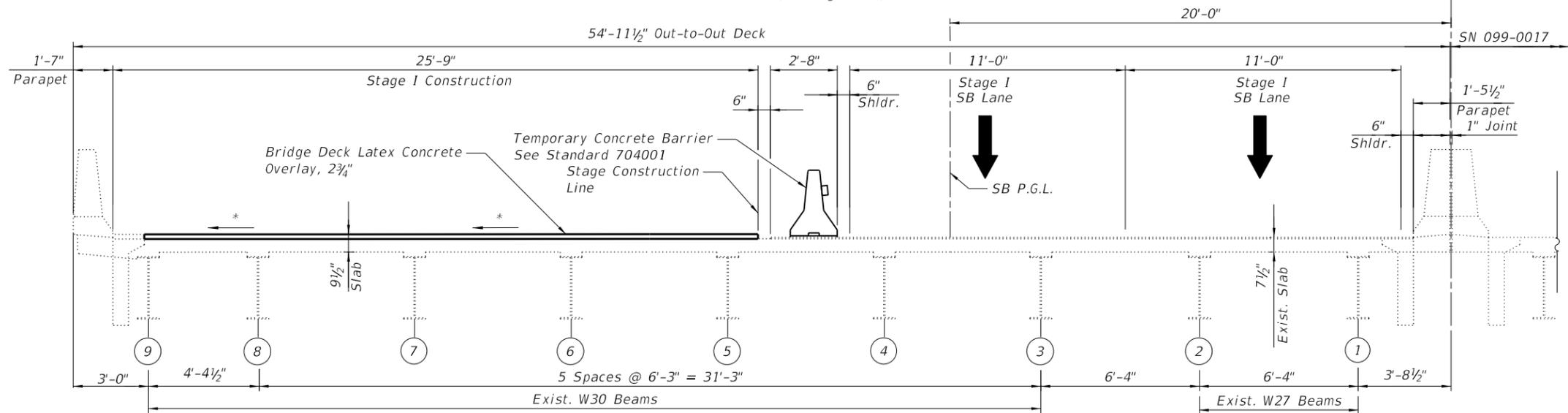
GENERAL NOTES, TOTAL BILL OF MATERIALS, & INDEX OF SHEETS
SN 099-0016 SB I-55 OVER US ROUTE 30

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	115
CONTRACT NO. 62N22				
SHEET S5-02 OF S5-15 SHEETS		ILLINOIS FED. AID PROJECT		

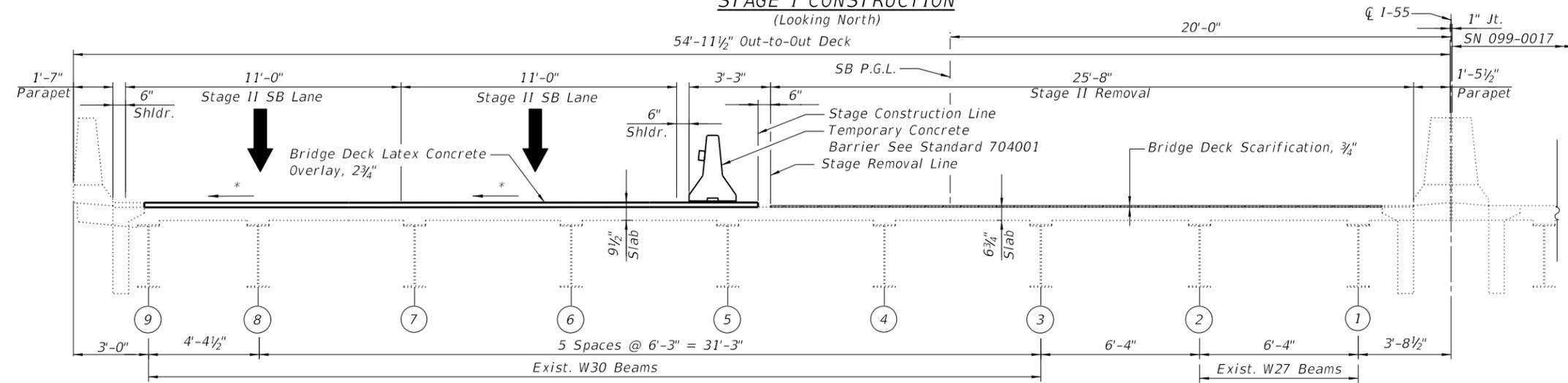
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STAGE I REMOVAL
(Looking North)



STAGE I CONSTRUCTION
(Looking North)



STAGE II REMOVAL
(Looking North)

STAGE I REMOVAL

1. Install temporary concrete barrier as shown to locate traffic on the west side of the existing structure.
2. Scarify 3/4" from the top of deck slab.
3. Remove portions of bridge deck/approach slab adjacent to abutment joints, as shown in the plans.

STAGE I CONSTRUCTION

1. Perform full-depth deck slab repairs, and approach slab repairs, at locations as shown in the plans.
2. Install preformed joint strip seal at north and south abutments and replace associated reinforcement and concrete adjacent to the joint.
3. Perform structural repair of concrete for the abutments and piers.
4. Apply 2 3/4" bridge deck latex concrete overlay to bridge deck slab.
5. Perform bridge deck grooving for the 2 3/4" bridge deck latex concrete overlay.
6. Apply 2" Stone-Matrix Asphalt (SMA) Overlay to approach slabs.
7. Apply protective coat to the top of reconstructed transverse joint areas, the surface of the new overlay and the top and inside faces of parapets.

STAGE II REMOVAL

1. Install temporary concrete barrier as shown to locate traffic on the east side of the existing structure.
2. Scarify 3/4" from the top of deck slab.
3. Remove portions of bridge deck/approach slab adjacent to abutment joints, as shown in the plans.

*Match existing deck surface profile



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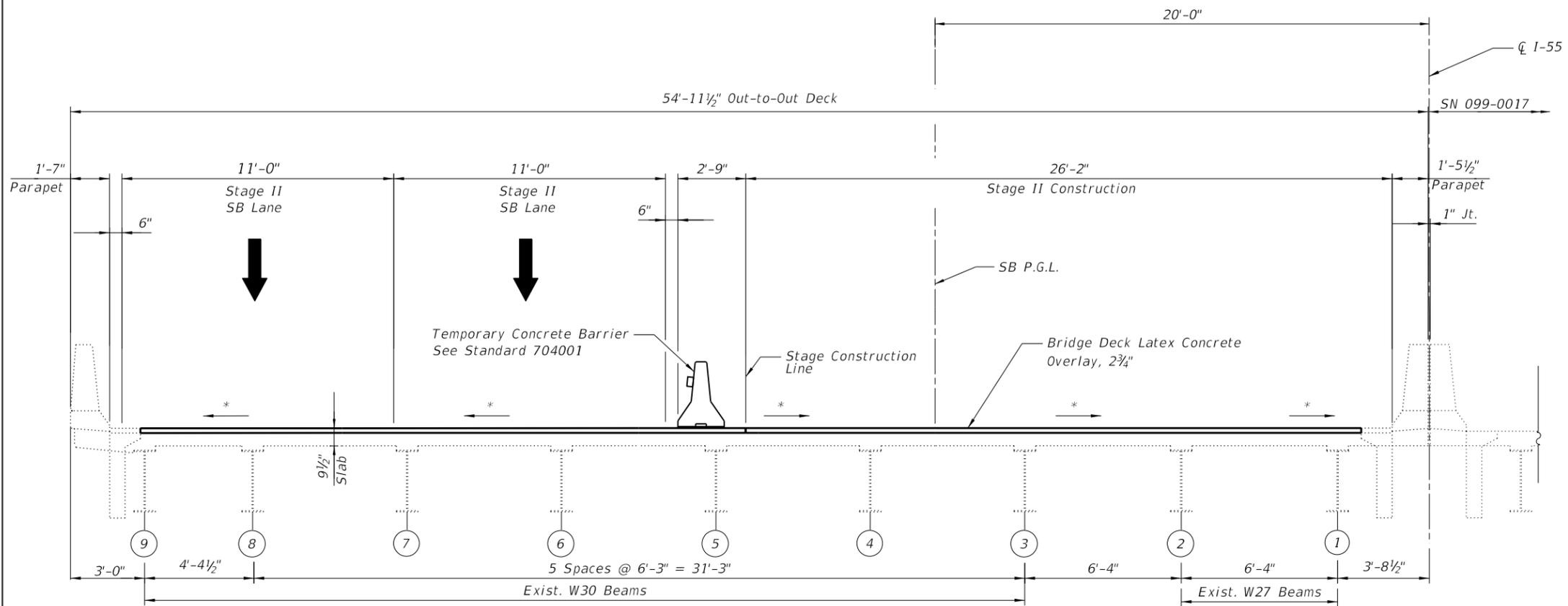
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CONSTRUCTION STAGING DETAILS - I
SN 099-0016 SB I-55 OVER US ROUTE 30

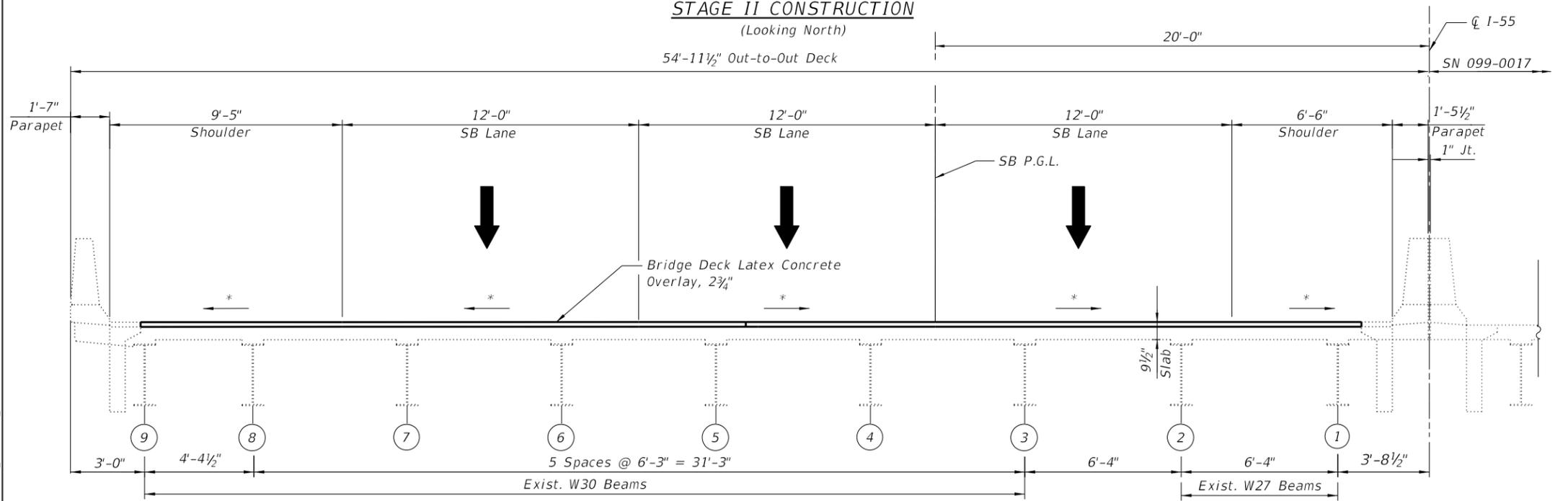
SHEET S5-03 OF S5-15 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	116
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62N22	

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STAGE II CONSTRUCTION
 (Looking North)



FINAL CROSS SECTION
 (Looking North)

STAGE II CONSTRUCTION

1. Perform full-depth deck slab repairs, and approach slab repairs, at locations as shown in the plans.
2. Install preformed joint strip seal at north and south abutments and replace associated reinforcement and concrete adjacent to the joint.
3. Perform structural repair of concrete for the abutments and piers.
4. Apply 2 3/4" bridge deck latex concrete overlay to bridge deck slab.
5. Perform bridge deck grooving for the 2 3/4" bridge deck latex concrete overlay.
6. Apply 2" Stone-Matrix Asphalt (SMA) Overlay to approach slabs.
7. Apply protective coat to the top of reconstructed transverse joint areas, the surface of the new overlay and the top and inside faces of parapets.
8. Apply permanent pavement markings on top of deck and approach slabs.

*Match existing deck surface profile



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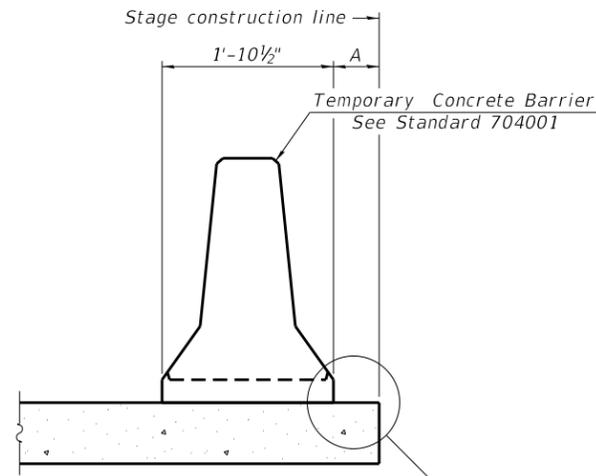
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CONSTRUCTION STAGING DETAILS - II
 SN 099-0016 SB I-55 OVER US ROUTE 30**

SHEET S5-04 OF S5-15 SHEETS

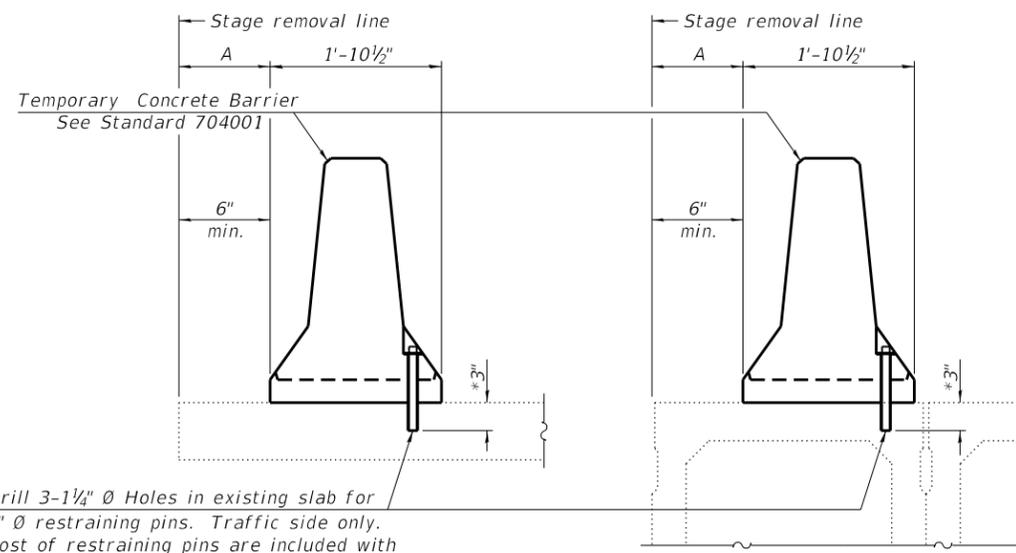
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	117
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

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When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM



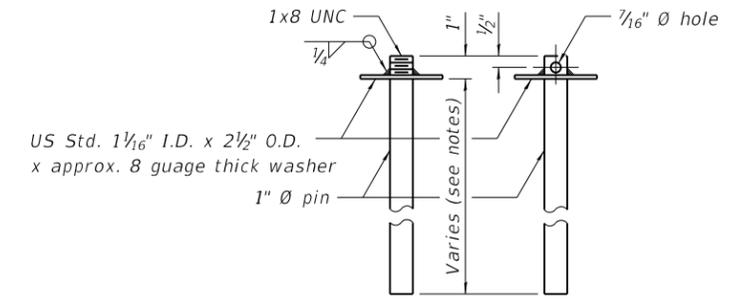
Drill 3-1 1/4" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

EXISTING SLAB

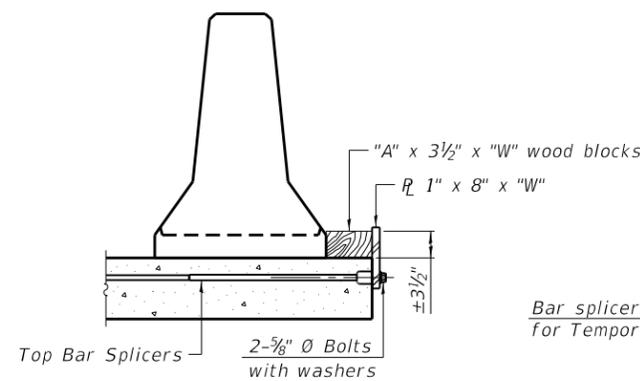
* When hot-mix asphalt wearing surface is present, embedment shall be 3" plus the wearing surface depth.

EXISTING DECK BEAM

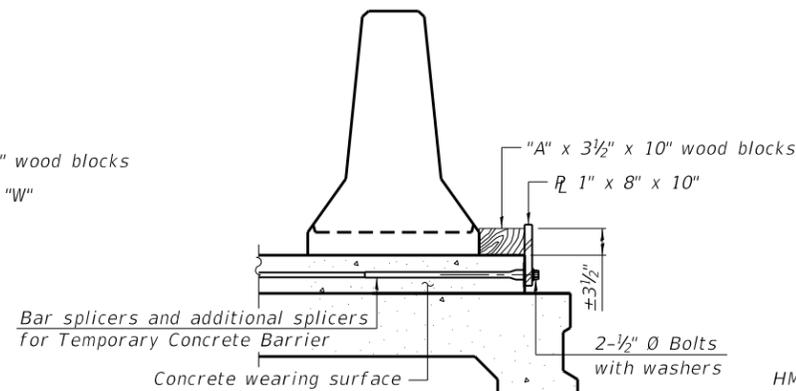
SECTIONS THRU SLAB OR DECK BEAM



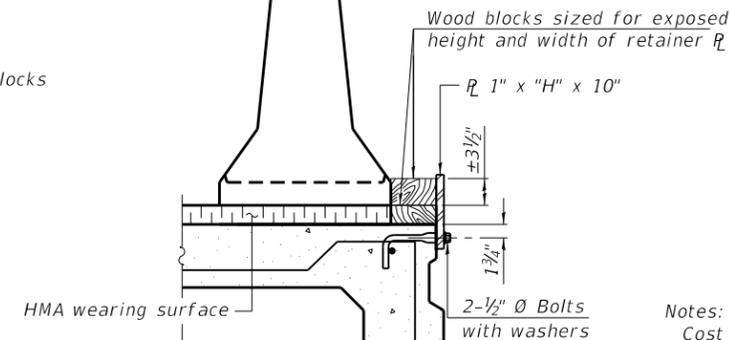
RESTRAINING PIN



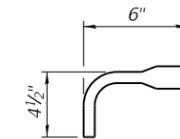
DETAIL I



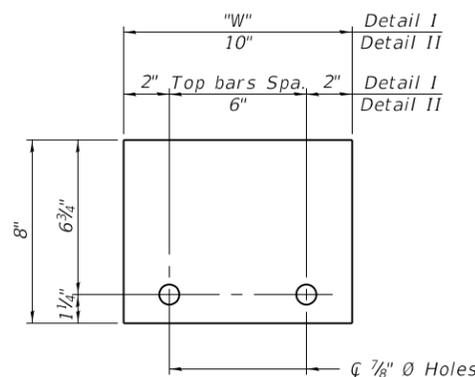
DETAIL II



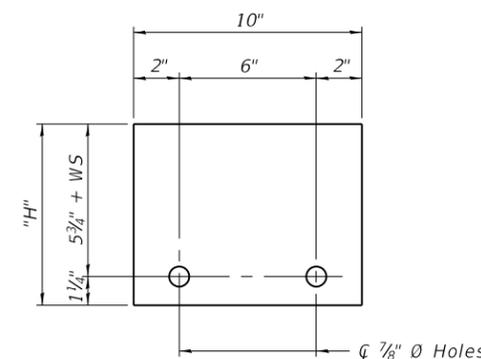
DETAIL III



BAR SPLICER FOR #4 BAR - DETAIL III



STEEL RETAINER 1" x 8" x "W"
 (Detail I and II)



STEEL RETAINER 1" x "H" x 10"
 (Detail III)

Notes:
 Cost of retainer assembly is included with Temporary Concrete Barrier.
 A retainer assembly shall be located at the approximate center of each temporary concrete barrier.
 The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.
 When the 'A' dimension is less than 1 1/2', the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate.
 For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.

- Detail I - Installation for a new bridge deck or bridge slab.
- Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.
- Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

R-27 8-11-2017



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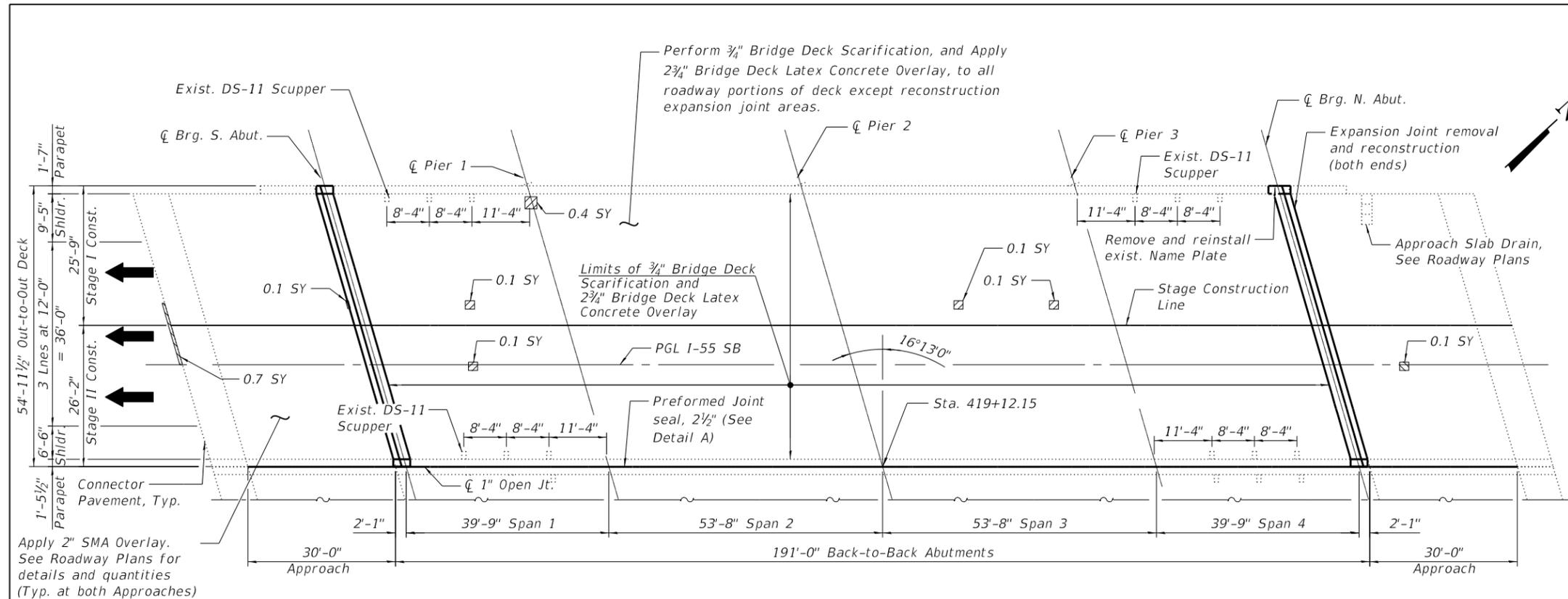
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION
 SN 099-0016 SB I-55 OVER US ROUTE 30

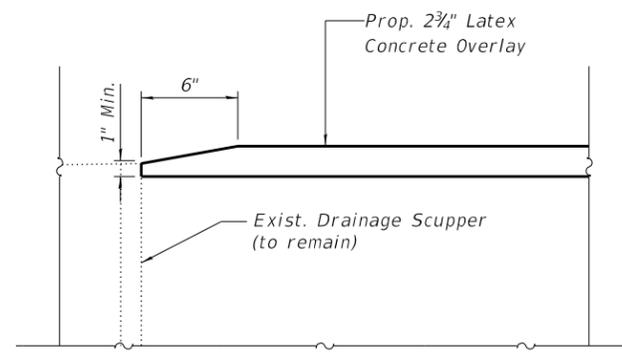
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55	2020-253-BR&PP	WILL	178	118
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

SHEET S5-05 OF S5-15 SHEETS

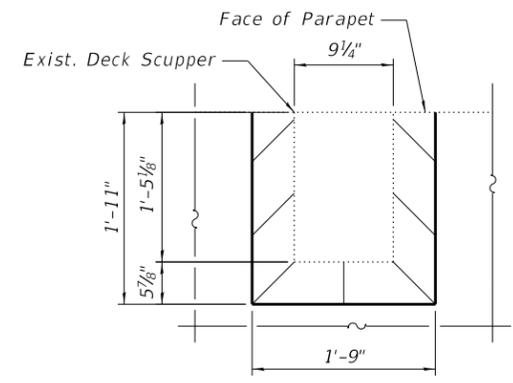
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PLAN



DRAINAGE SCUPPER DETAIL



TOP PLAN AT DRAINAGE SCUPPER

LEGEND

- Deck Slab Repair (Partial) *
- Approach Slab Repair (Partial Depth)
- Structural Repair of Concrete (Depth Equal to or Less Than 5")
- SY Square Yards

*Areas of Deck Slab Repair (Partial) are provided for information only and shall be included in the cost of Bridge Deck Latex Concrete Overlay, 2 3/4"

NOTES:

1. Deck and approach slab repair areas are estimated based on visual inspection and will be paid for as specified in the Special Provision. Actual repair areas and locations shall be determined by the Engineer and shown on As-built plans. Engineer shall sound deck after deck scarification.
2. Protective Coat shall be applied to the bridge overlay and front and top faces of the new and existing parapets.
3. All dimensions are perpendicular to \bar{C} I-55 Bridge Deck.
4. Protective Shield shall be placed over traffic lanes for US 30.
5. For bridge deck final cross section, see Sheet S5-04.
6. For North and South Transverse Joint Removal and Reconstruction, see Sheets S5-08 and S5-09.
7. Perform Bridge Deck Grooving for the Bridge Deck Latex Concrete Overlay and the roadway portions of the Reconstructed Transverse Joints.
8. Any reinforcement bars that are damaged during concrete removal shall be repaired or replaced using an approved bar splicer or anchorage system. Cost included with Concrete Removal.
9. The Contractor shall exercise extreme caution during concrete removal to avoid damaging the steel beams and diaphragms to remain. Any damage to the existing steel beams and/or diaphragms to remain caused by the Contractor in the performance of his/her work shall be repaired by the Contractor, to the satisfaction of the Engineer at no cost to the Department.
10. See Sheet S5-07 of S5-15 for Detail A.

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Approach Slab Repair (Partial Depth)	Sq. Yd.	1
Protective Coat	Sq. Yd.	1457
Bridge Deck Grooving	Sq. Yd.	1092
Bridge Deck Latex Concrete Overlay, 2 3/4"	Sq. Yd.	1108
Bridge Deck Scarification, 3/4"	Sq. Yd.	1108
Protective Shield	Sq. Yd.	656



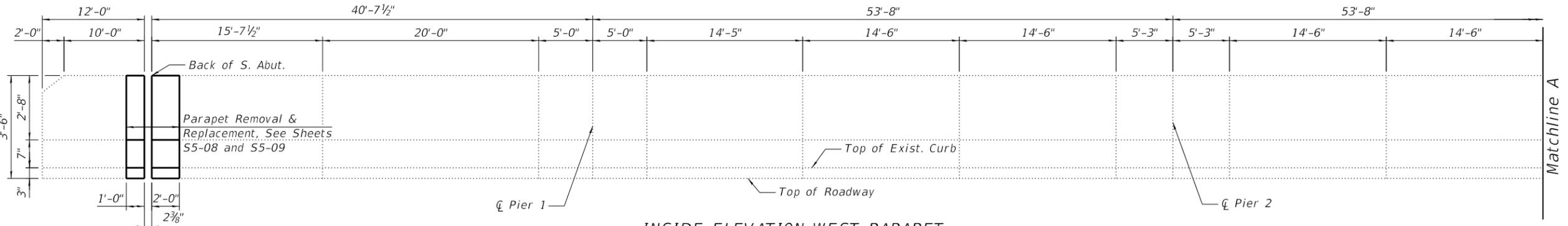
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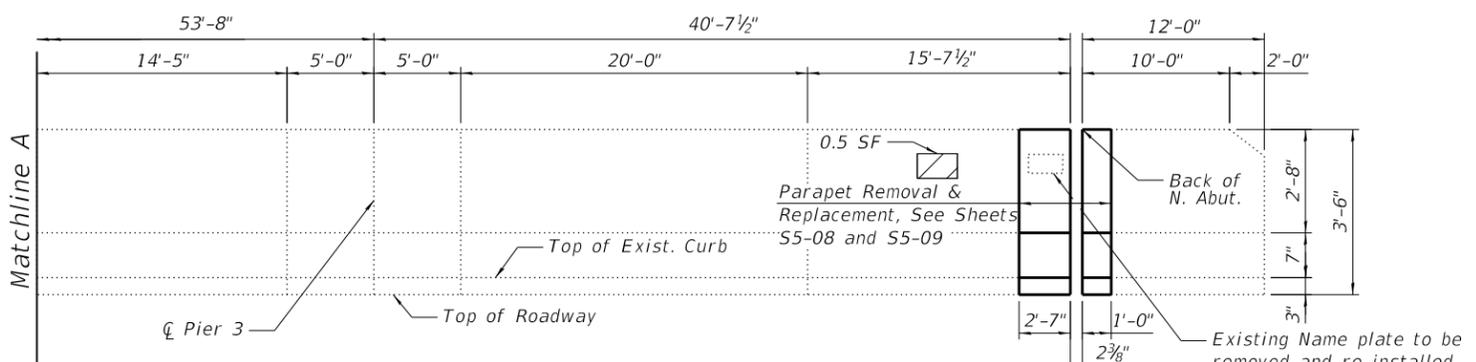
**BRIDGE DECK REPAIRS
 SN 099-0016 SB I-55 OVER US ROUTE 30**

SHEET S5-06 OF S5-15 SHEETS

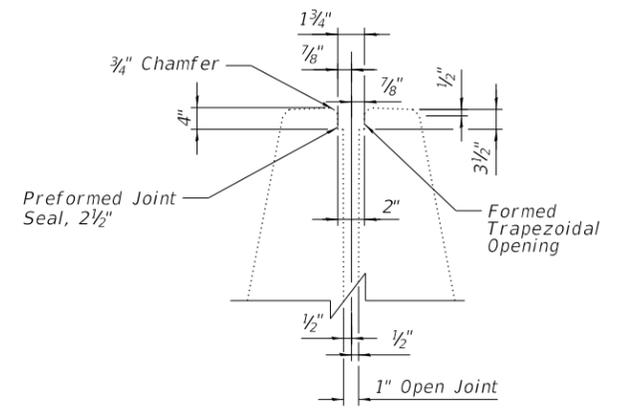
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55	2020-253-BR&PP	WILL	178	119
CONTRACT NO. 62N22				
ILLINOIS		FED. AID PROJECT		



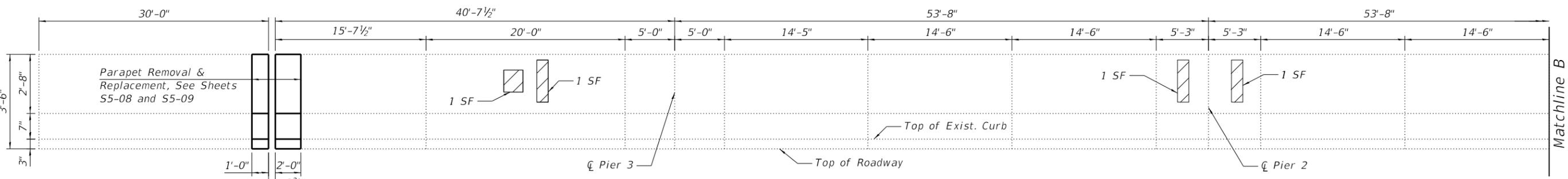
INSIDE ELEVATION WEST PARAPET
(Looking West)



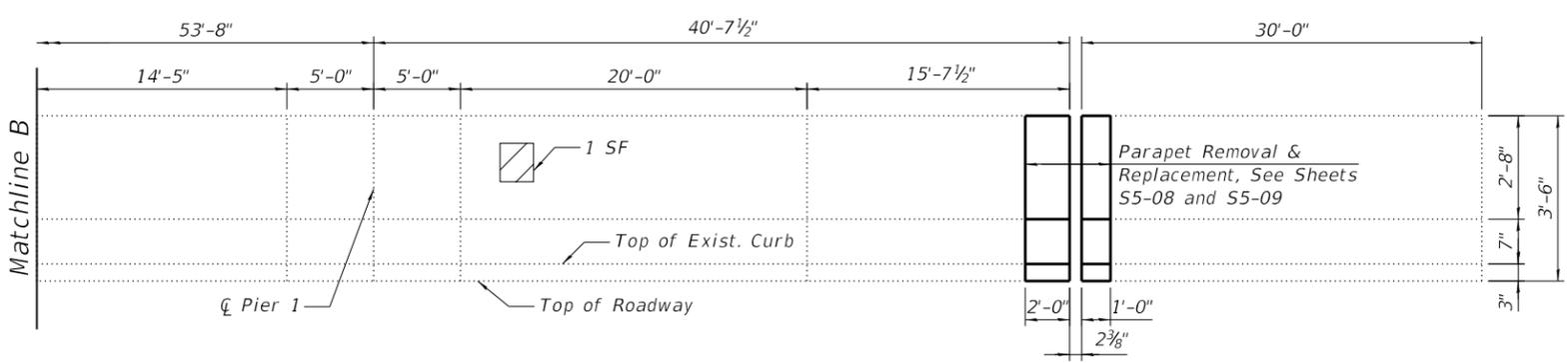
INSIDE ELEVATION WEST PARAPET
(Looking West)



DETAIL A



INSIDE ELEVATION EAST PARAPET
(Looking East)



INSIDE ELEVATION EAST PARAPET
(Looking East)

LEGEND:

Structural Repair of Concrete (Depth Equal to or Less than 5 Inches)

SF Square Foot

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Seal, 2 1/2"	Foot	249
Structural Repair of Concrete (Depth Equal to or Less Than 5")	Sq Ft	5.5

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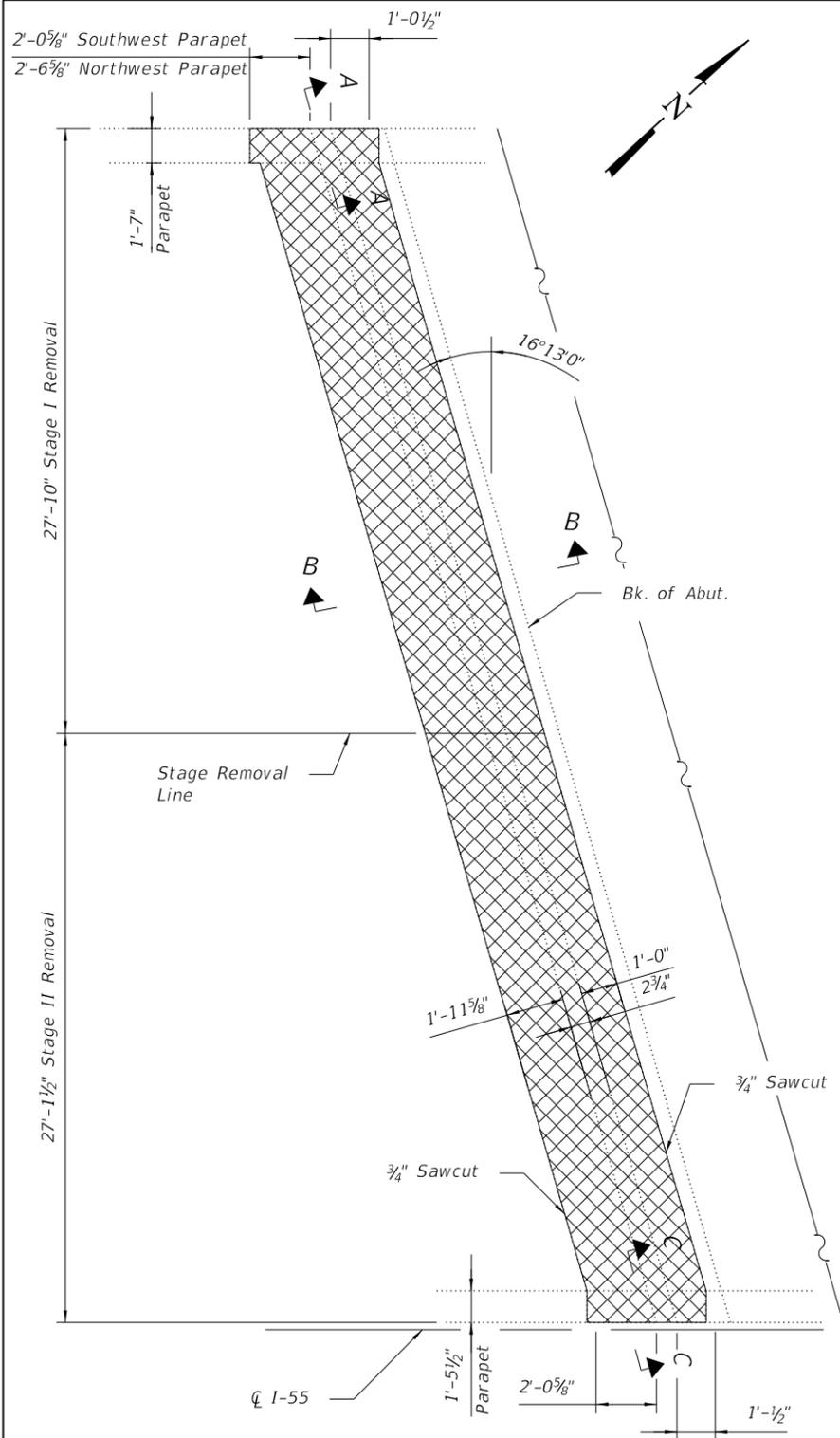
PARAPET REPAIRS
SN 099-0016 SB I-55 OVER US ROUTE 30

SHEET S3-07 OF S3-15 SHEETS

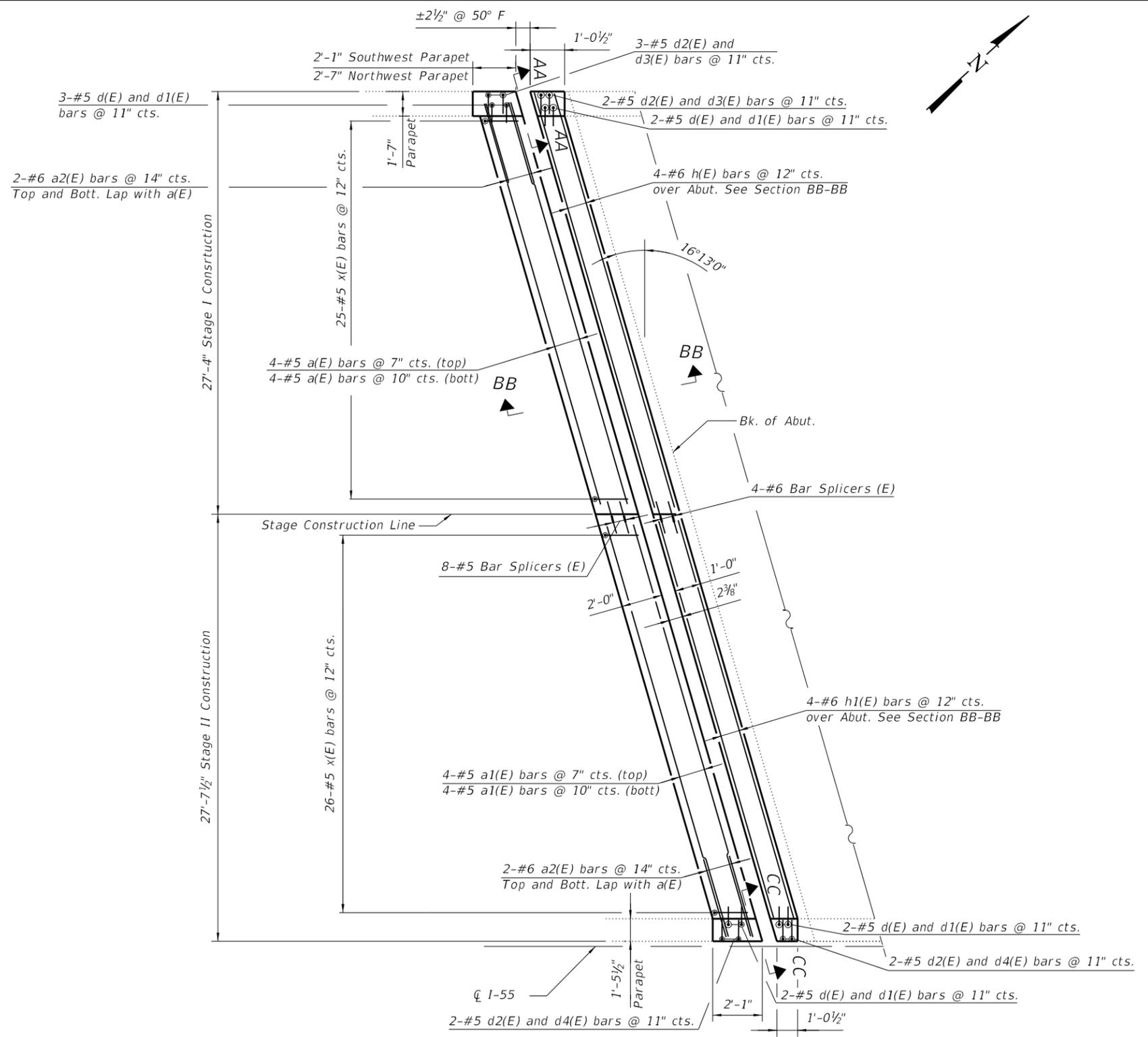
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55	2020-253-BR&PP	WILL	178	120
CONTRACT NO. 62N22				

ILLINOIS FED. AID PROJECT

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N. ABUT. JOINT REMOVAL PLAN



N. ABUT. JOINT RECONSTRUCTION PLAN

NOTES:

1. North Abutment Shown, South Abutment similar.
2. Horizontal bars in approach parapets shall be cleaned, straightened, and reused in new construction.
3. Hatched areas indicate Concrete Removal.



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PLOT DATE =	CHECKED - JMT	REVISED -

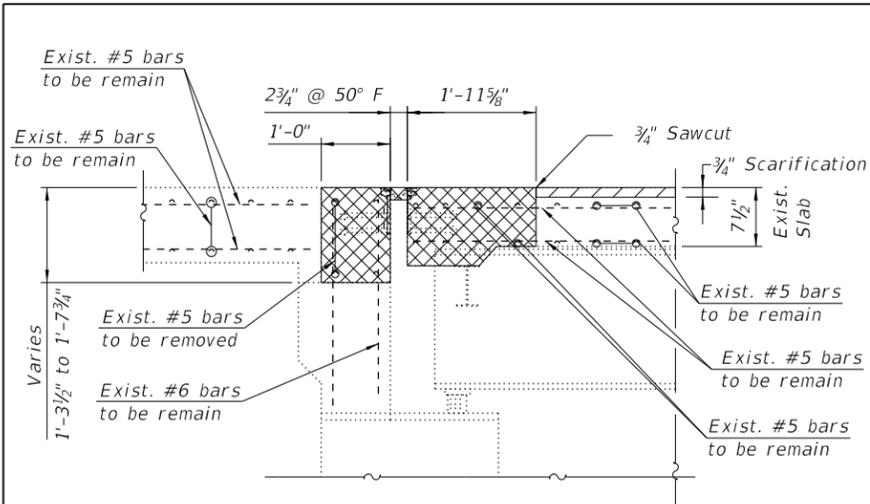
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**EXPANSION JOINT REMOVAL AND CONSTRUCTION
 SN 099-0016 SB I-55 OVER US ROUTE 30**

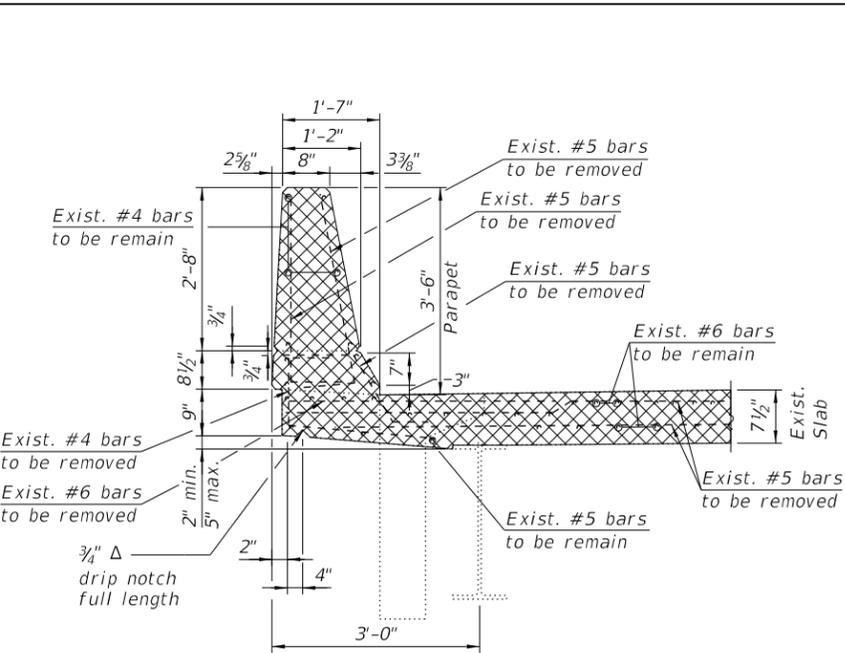
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CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

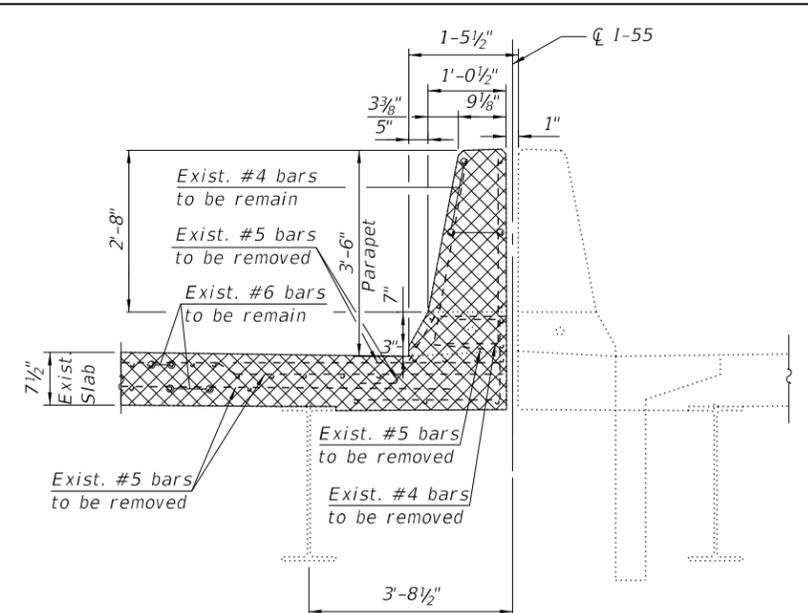
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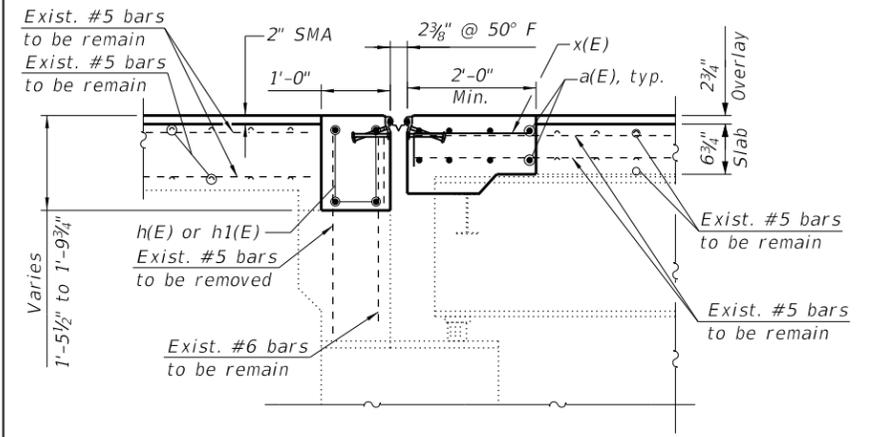
SECTION B-B



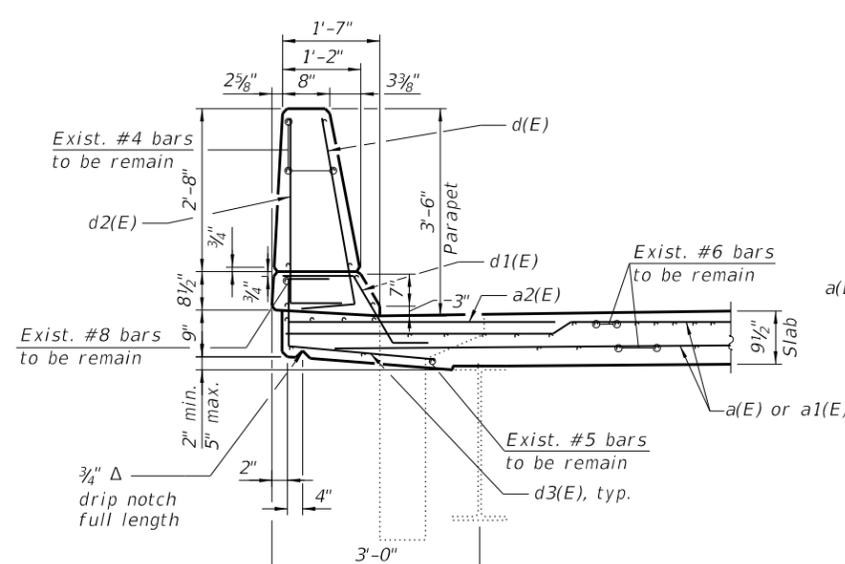
SECTION A-A



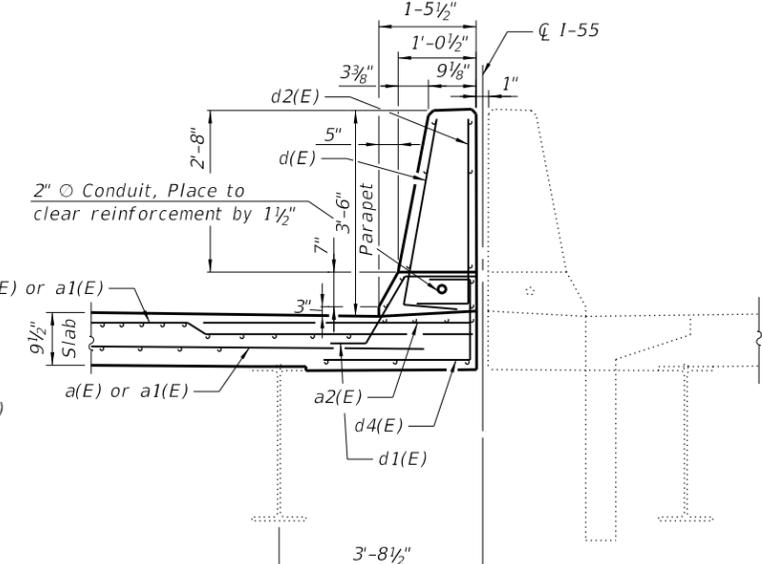
SECTION C-C



SECTION BB-BB

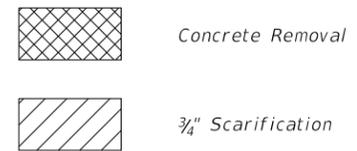


SECTION AA-AA



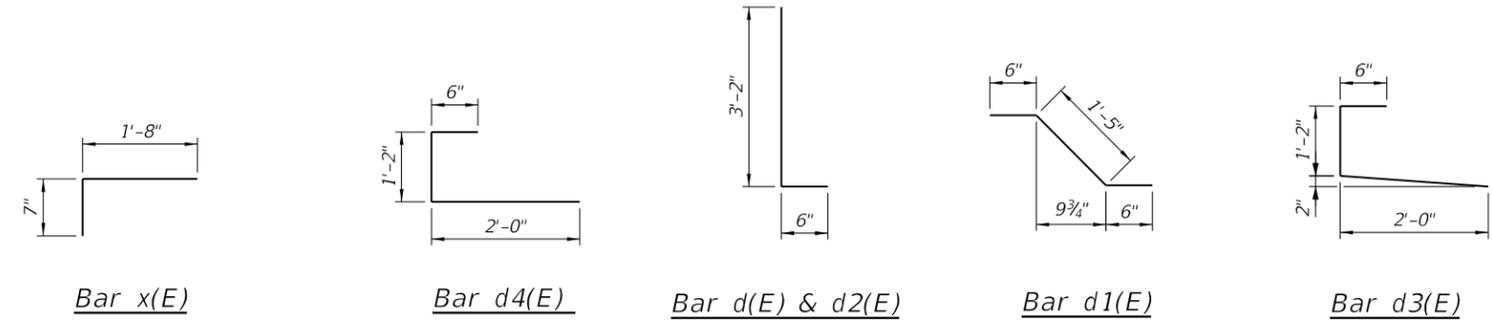
SECTION CC-CC

LEGEND:



NOTES:

- For preformed joint seal details, see sheet S5-10.
- For Bar Splicer Assembly details, see sheet S5-15
- Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.
- Removal of Exist. Expansion Joint will not be paid for separately but will be included in the cost of Concrete Removal.



BILL OF MATERIAL BOTH ABUTMENTS

Bar	No.	Size	Length	Shape
a(E)	16	#5	28'-1"	—
a1(E)	16	#5	28'-5"	—
a2(E)	16	#6	4'-6"	—
d(E)	20	#5	3'-8"	┌
d1(E)	20	#5	2'-5"	┌
d2(E)	20	#4	3'-8"	┌
d3(E)	10	#4	3'-8"	┌
d4(E)	10	#5	3'-8"	┌
h(E)	8	#6	28'-1"	—
h1(E)	8	#6	28'-5"	—
x(E)	106	#5	2'-3"	┌
Concrete Removal			Cu. Yd.	16.2
Reinforcement Bars, Epoxy Coated			Pound	2220
Concrete Superstructure			Cu. Yd.	17.2



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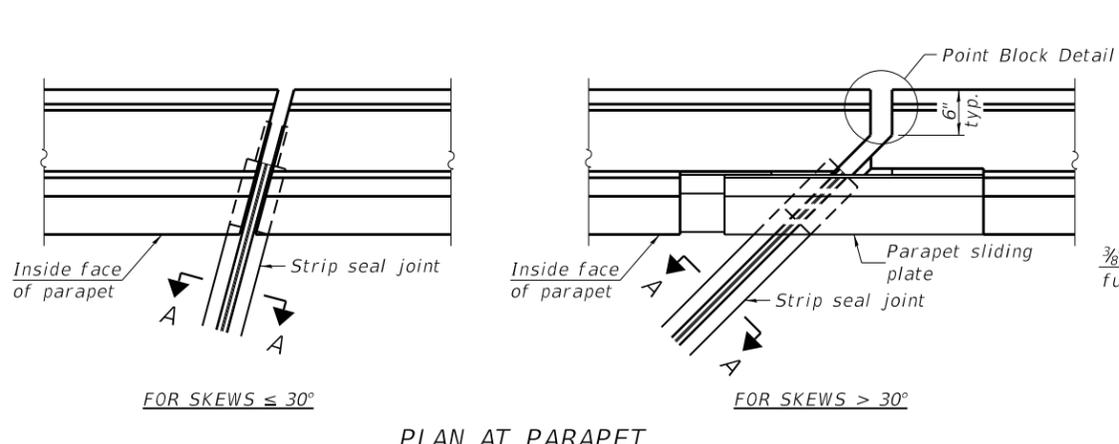
EXPANSION JOINT DETAILS
 SN 099-0016 SB I-55 OVER US ROUTE 30

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62N22				

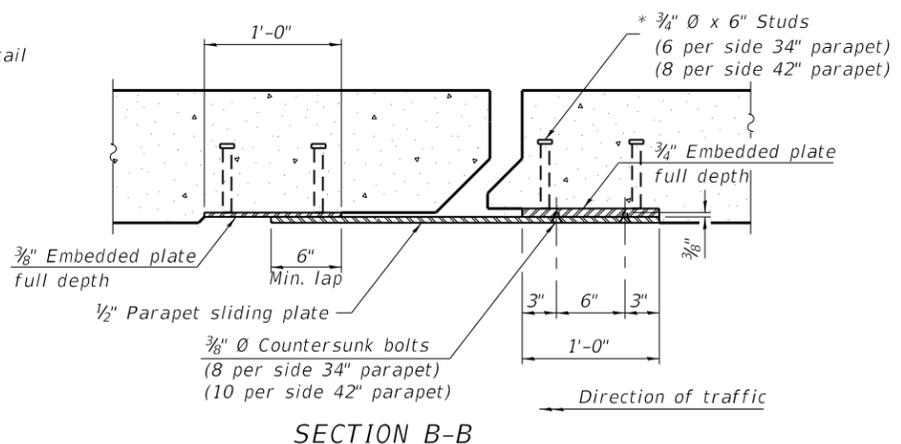
SHEET S5-09 OF S5-15 SHEETS

ILLINOIS FED. AID PROJECT

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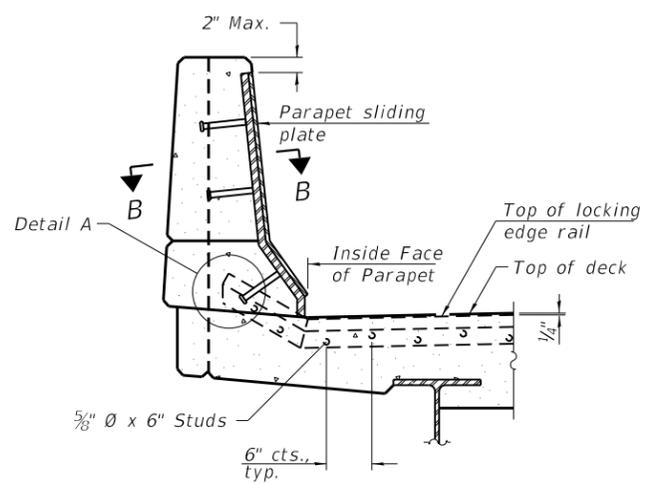


PLAN AT PARAPET



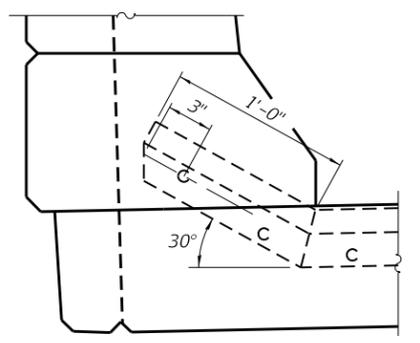
SECTION B-B

Notes:
 The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.
 The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the 4 1/2" maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.
 The manufacturer's recommended installation methods shall be followed.
 All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.
 The Maximum space between locking edge rail segments shall be 3/16" and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.

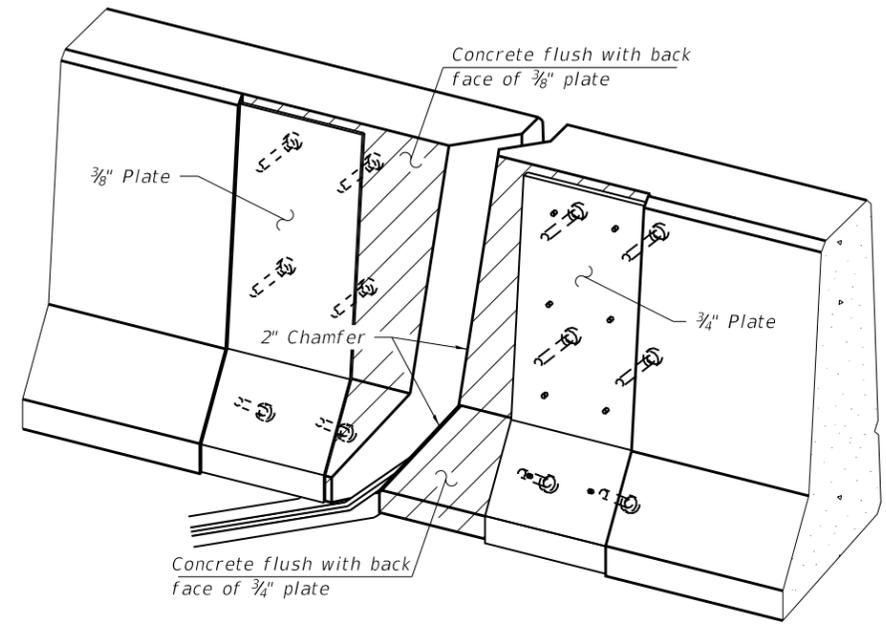


ELEVATION AT PARAPET

(Skews > 30° shown. Skews ≤ 30° similar except as shown in plan view.)

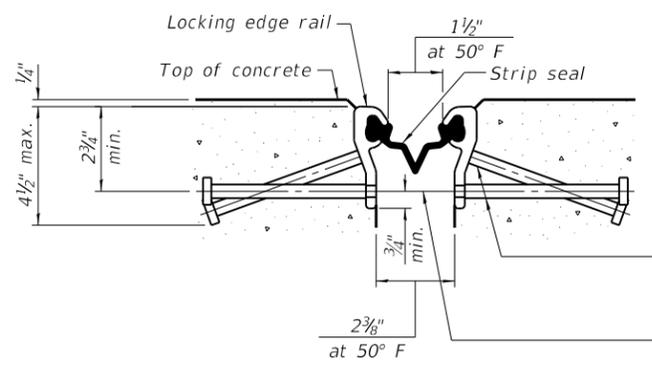


DETAIL A



TRIMETRIC VIEW
 (Showing embedded plates only)

Cost of parapet sliding plates, embedded plates, and anchorage studs included with Preformed Joint Strip Seal.
 34" F-shape barrier shown, 42" F-shape similar as noted.
 The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required. One exception to this would be the strip seal joint at the end of the precast bridge approach slab. For these cases the pavement connector length shall be adjusted, not the length of the bridge approach slab.



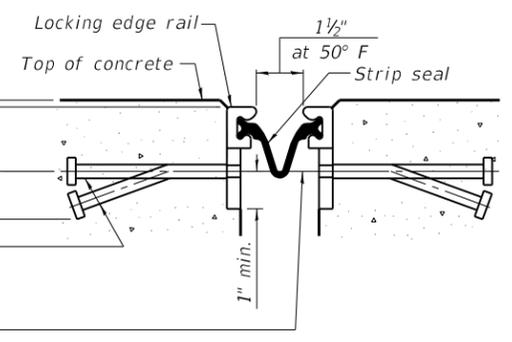
SHOWING ROLLED RAIL JOINT

* 3/8" Ø x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)

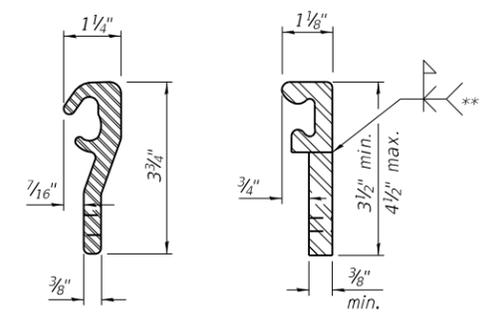
3/8" Ø threaded rods in 7/16" Ø holes at ±4'-0" cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

SECTION A-A

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

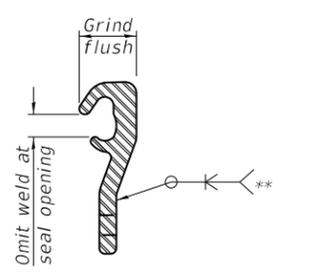


SHOWING WELDED RAIL JOINT



LOCKING EDGE RAILS

** Back gouge not required if complete joint penetration is verified by mock-up.



LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue.
 Rolled rail shown, welded rail similar.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	115

EJ-SS

8-11-17



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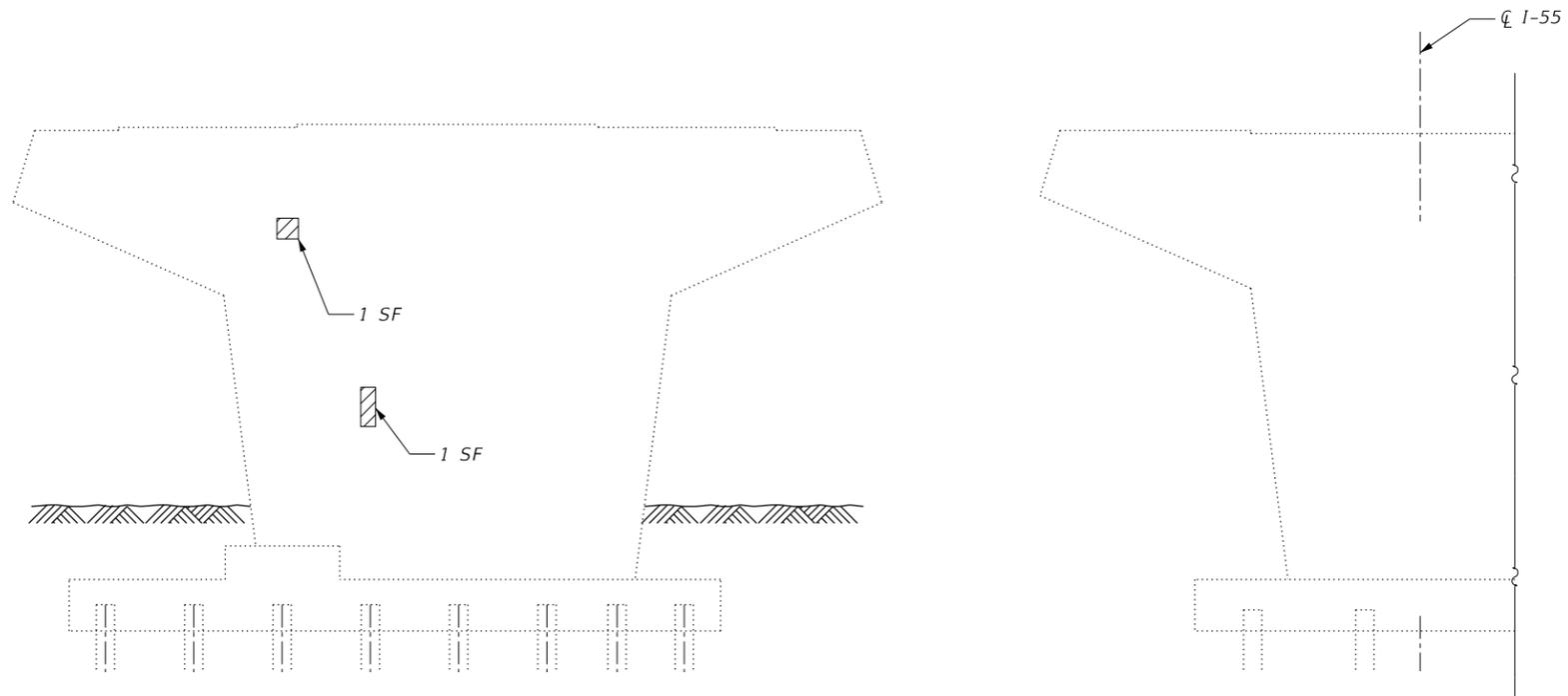
PREFORMED JOINT STRIP SEAL
 SN 099-0016 SB I-55 OVER US ROUTE 30

SHEET S5-10 OF S5-15 SHEETS

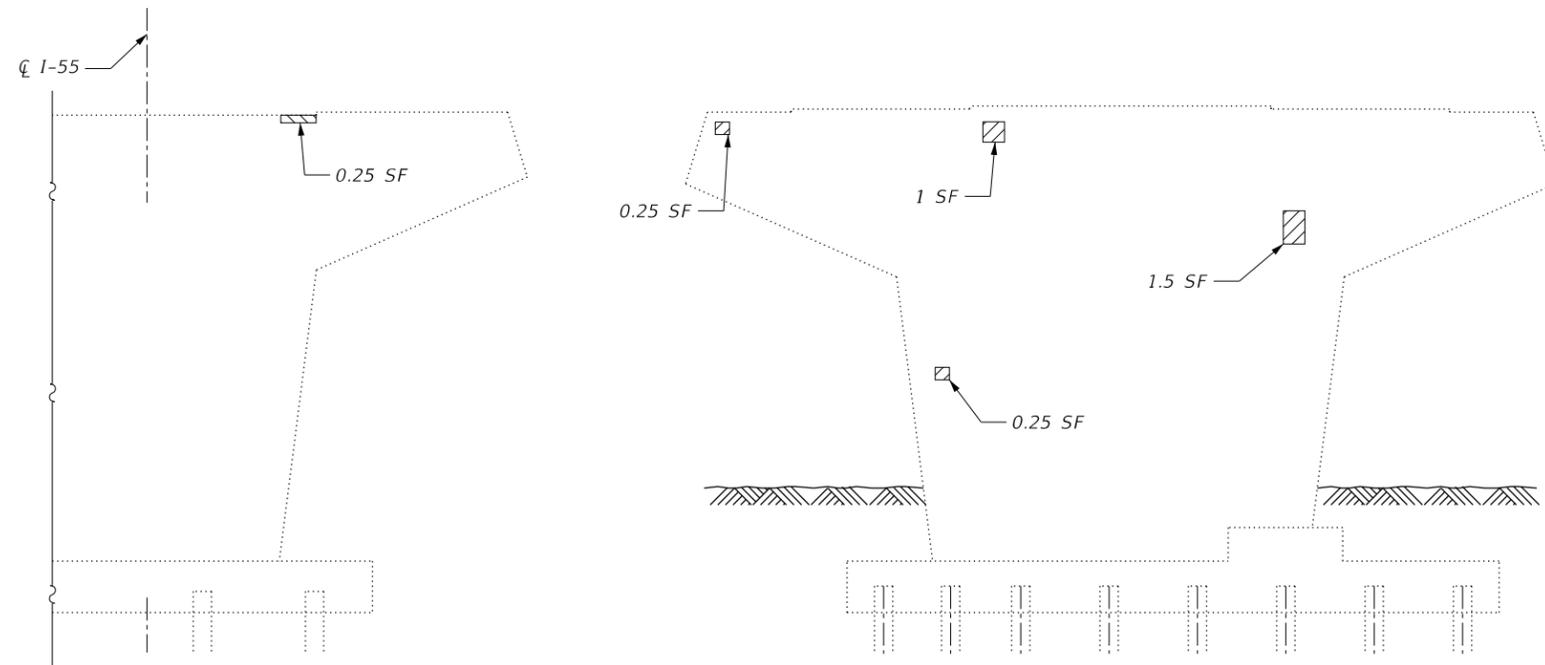
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	123
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

NOTES:

- Quantities and limits shown are estimated for bidding purposes only. The actual areas to be repaired, and the type(s) of repairs to be used, will be determined by the Engineer in the field at the time of construction.



PIER 1
(Looking North)



PIER 1
(Looking South)

LEGEND:

 Structural Repair of Concrete
(Depth Equal to or less than 5")

 Exposed Reinforcement

 Hairline Crack

SF Square Foot

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth Equal to or Less Than 5")	Sq Ft	5.25

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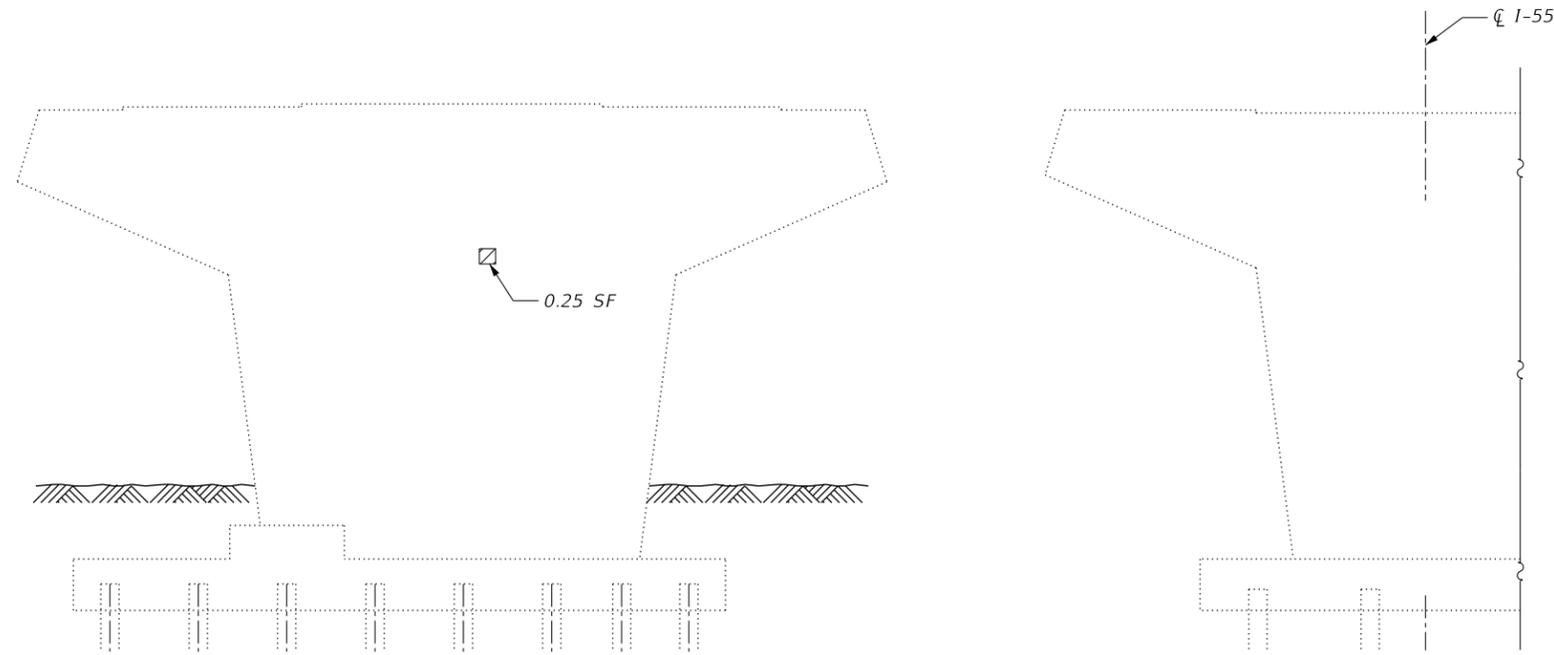
STATE OF ILLINOIS
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PIER 1 REPAIRS
SN 099-0016 SB I-55 OVER US ROUTE 30

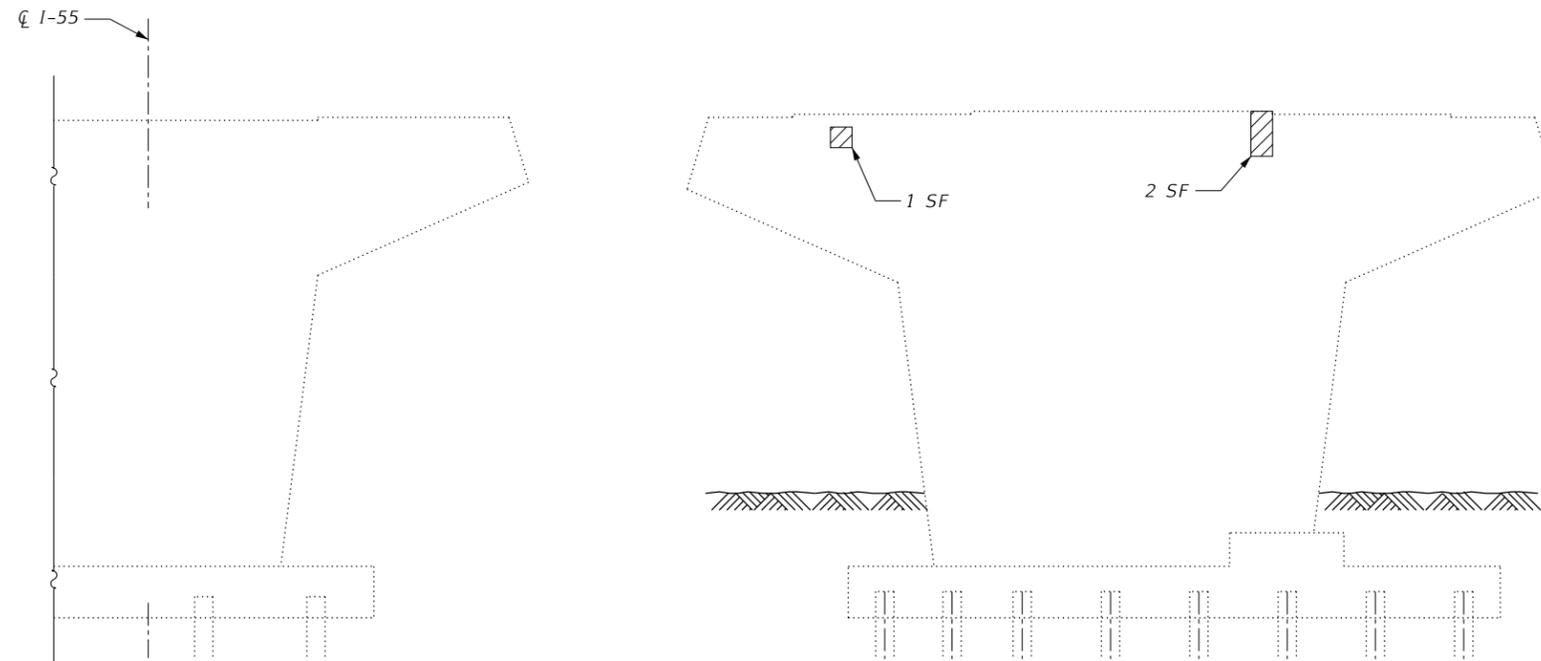
SHEET S5-11 OF S5-15 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62N22				
		ILLINOIS FED. AID PROJECT		

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PIER 2
 (Looking North)



PIER 2
 (Looking South)

LEGEND:

-  Structural Repair of Concrete
 (Depth Equal to or less than 5")
-  Exposed Reinforcement
-  Hairline Crack
- SF Square Foot

NOTES:

1. Quantities and limits shown are estimated for bidding purposes only. The actual areas to be repaired, and the type(s) of repairs to be used, will be determined by the Engineer in the field at the time of construction.

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth Equal to or Less Than 5")	Sq Ft	3.25



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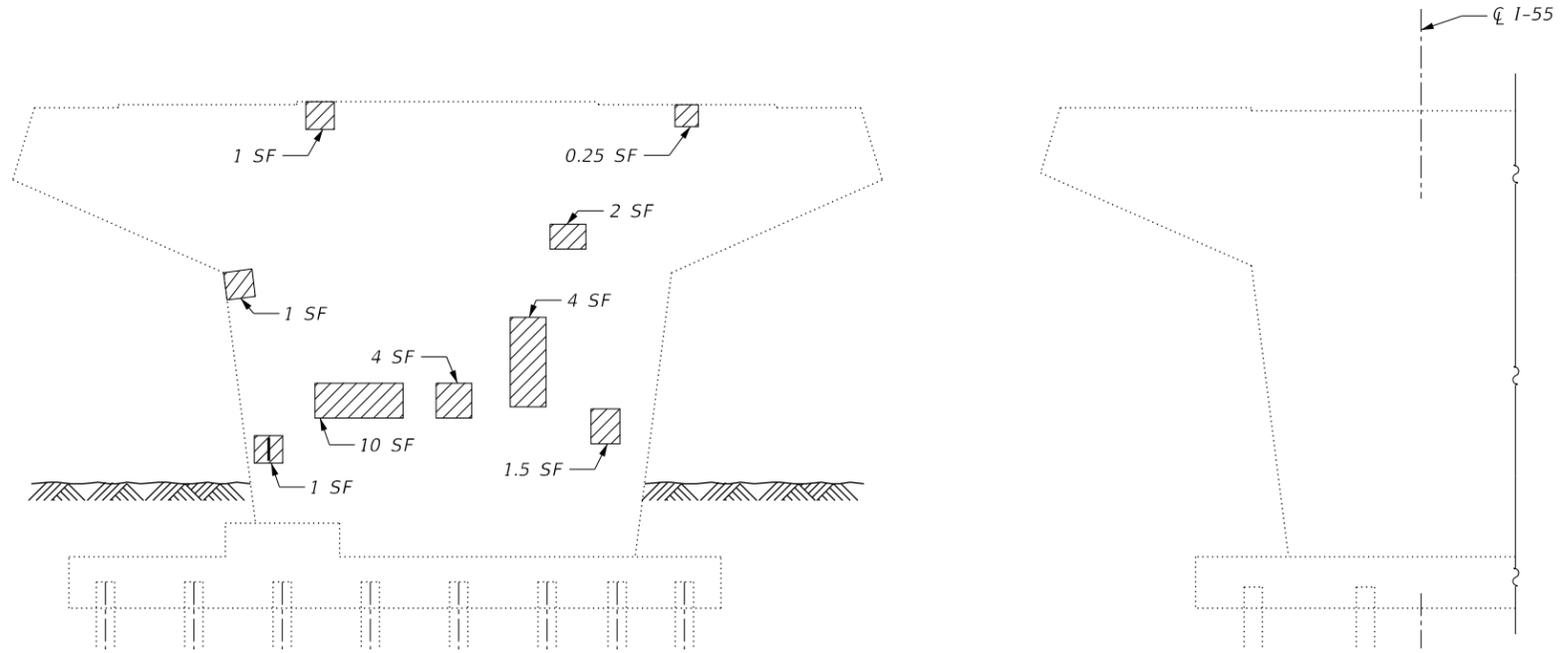
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER 2 REPAIRS
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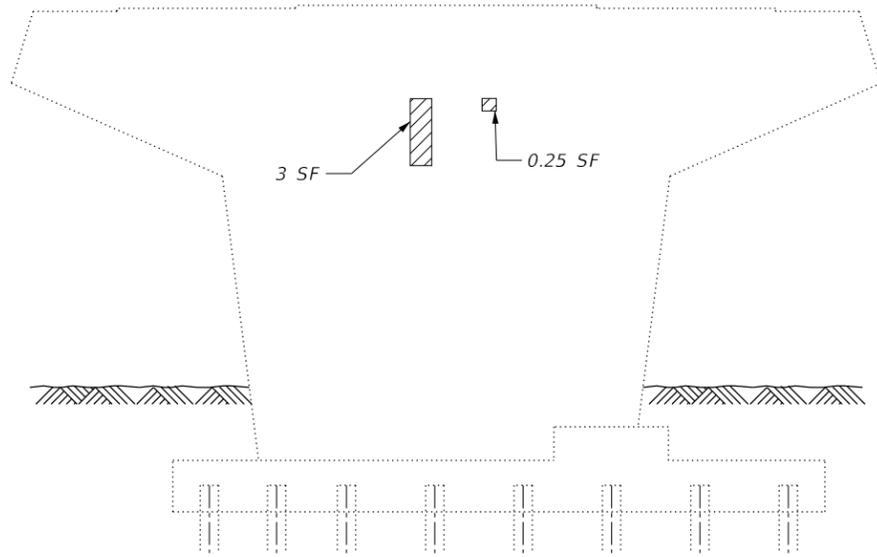
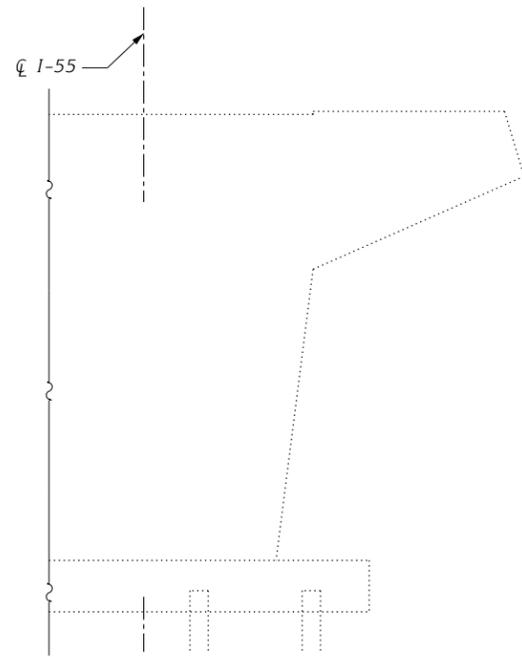
SHEET S5-12 OF S5-15 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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ILLINOIS FED. AID PROJECT			CONTRACT NO. 62N22	

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PIER 3
 (Looking North)



PIER 3
 (Looking South)

LEGEND:

- Structural Repair of Concrete
 (Depth Equal to or less than 5")
- Exposed Reinforcement
- Hairline Crack
- SF Square Foot

NOTES:

1. Quantities and limits shown are estimated for bidding purposes only. The actual areas to be repaired, and the type(s) of repairs to be used, will be determined by the Engineer in the field at the time of construction.

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth Equal to or Less Than 5")	Sq Ft	28



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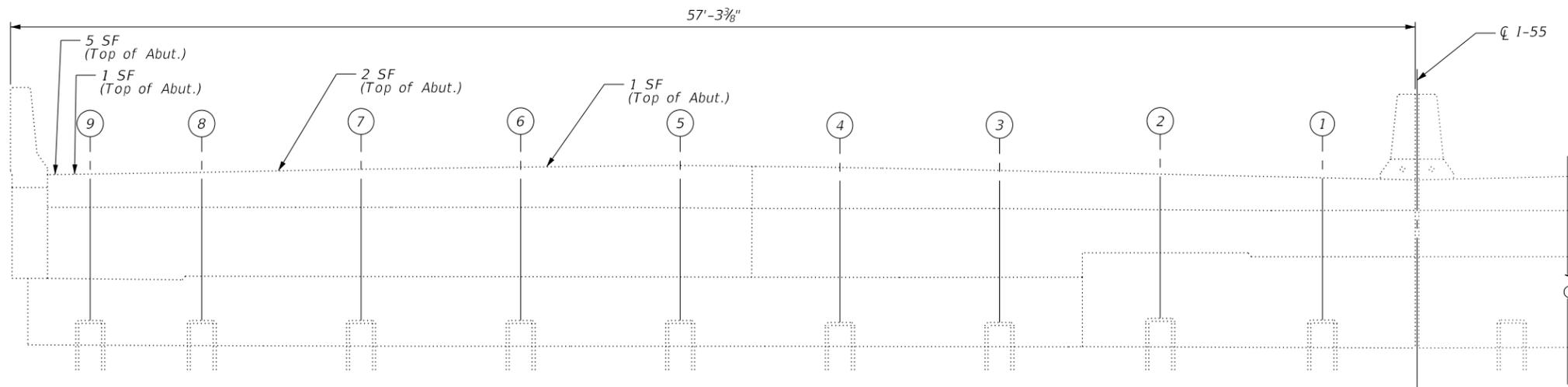
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER 3 REPAIRS
SN 099-0016 SB I-55 OVER US ROUTE 30

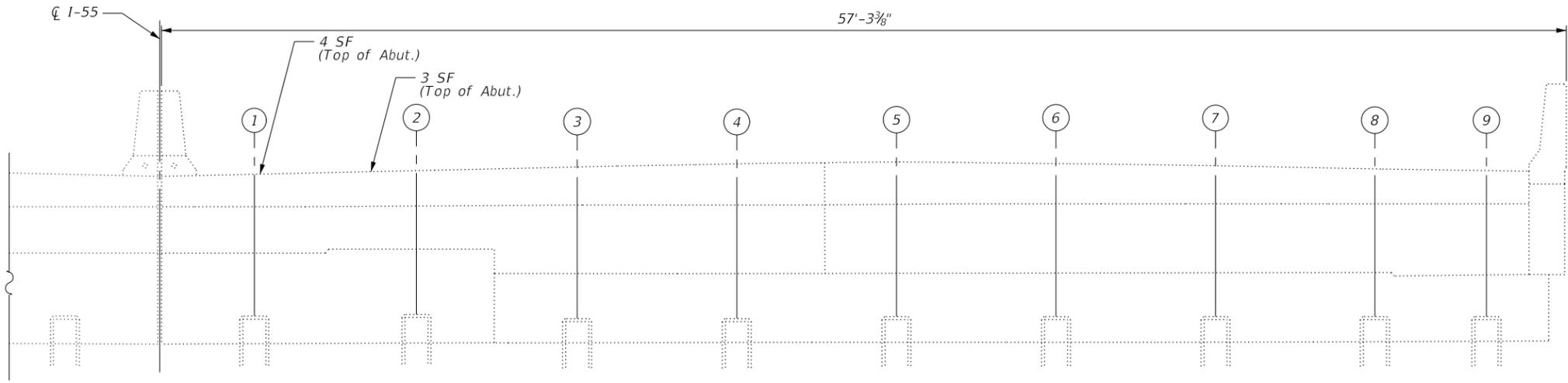
SHEET S5-13 OF S5-15 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62N22				
ILLINOIS		FED. AID PROJECT		

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ELEVATION - NORTH ABUTMENT
 (Looking North)



ELEVATION - SOUTH ABUTMENT
 (Looking South)

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth Equal to or Less Than 5")	Sq Ft	16
Concrete Sealer	Sq Ft	333

NOTE:

- Quantities and limits shown are estimated for bidding purposes only. The actual areas to be repaired, and the type(s) of repairs to be used, will be determined by the Engineer in the field at the time of construction.
- Apply Concrete Sealer to face of abutments.

LEGEND:

SF Square Foot



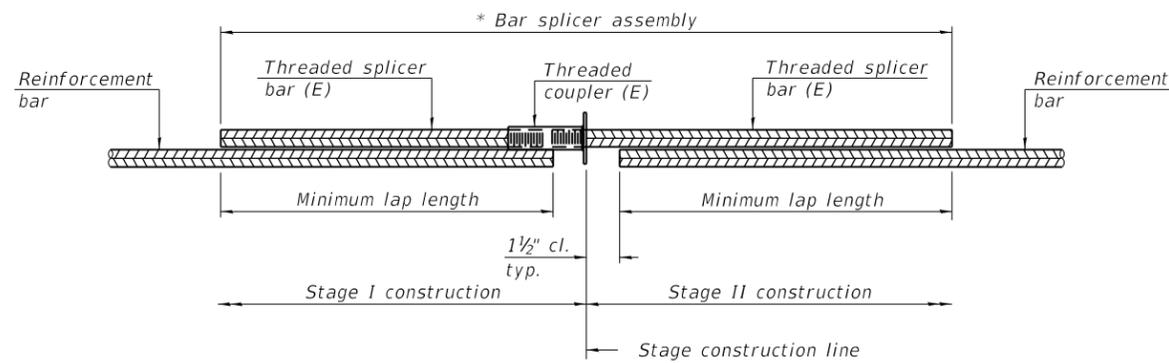
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**NORTH AND SOUTH ABUTMENT REPAIRS
 SN 099-0016 SB I-55 OVER US ROUTE 30**

SHEET S5-14 OF S5-15 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	127
CONTRACT NO. 62N22				
ILLINOIS		FED. AID PROJECT		

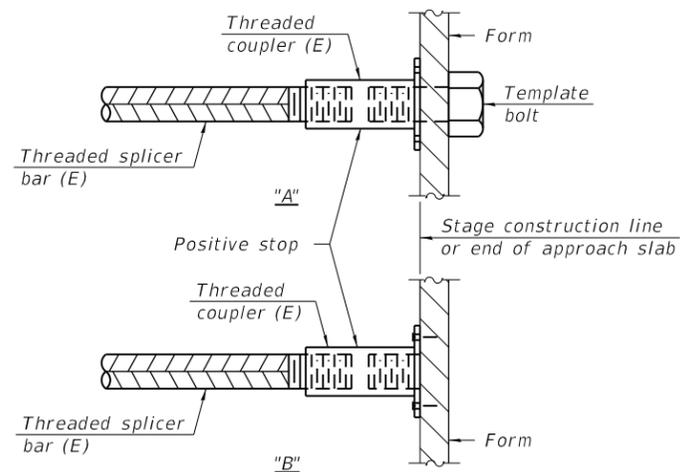


STANDARD BAR SPLICER ASSEMBLY PLAN
 (All components shall be provided from one supplier)

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

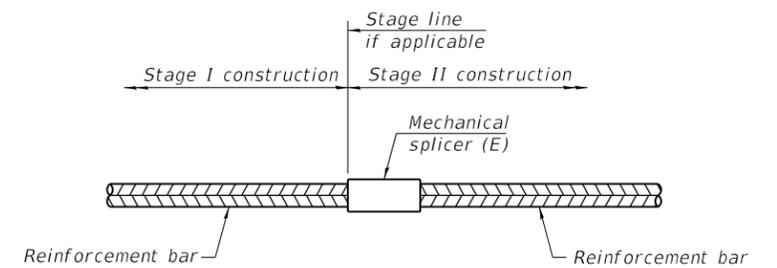
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
N. Abut.	#5	8	3'-6"
N. Abut. Bk. Wall	#6	4	4'-0"
S. Abut.	#5	8	3'-6"
S. Abut. Bk. Wall	#6	4	4'-0"



INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required

Notes:

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

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BSD-1

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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
SN 099-0016 SB I-55 OVER US ROUTE 30

SHEET S5-15 OF S5-15 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	128
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

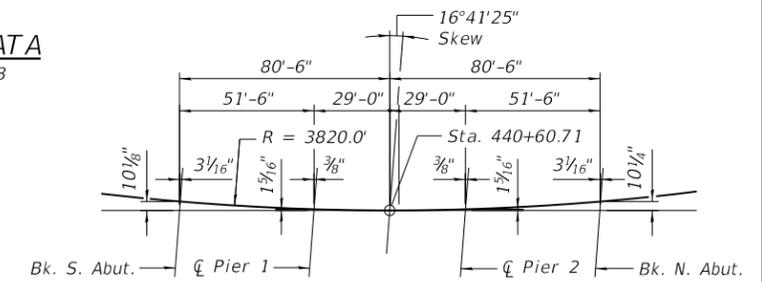
Existing Structure: F.A.I.-187(8), Will County. Both the superstructure and substructure were widened in 1977 as part of F.A.I. Route 55, Stevenson Expressway Rehabilitation, Section (99-1&2) R-5, Project I-IR-55-6 (125) 246, Will County. Also, additional rehabilitation work was performed in 1994 under contract for F.A.I. Route 55 (Stevenson Expressway) Section (26 & 27)RS, 27(B,VB)R; 27(B-1, B-3, HB, VB-1)BR (89), Station 1453+52.19, Project IM-NHI-55-6 (193) 252, Will County. The superstructures and substructures were widened to the inside in 2006 as part of F.A.I. Route 55 Sec. 2006-032 BY, Will County. Both structures consist of R.C. deck supported on three-span continuous steel beams. The abutments are supported on concrete piles and/or HP steel piles. The piers are supported on timber piles and/or HP steel piles. The bridge length is 161'-0" bk. to bk. abutments. The northbound width varies from 65'-0 $\frac{1}{2}$ " to 65'-10 $\frac{3}{8}$ " o. to o. of deck. The southbound deck width varies from 70'-11 $\frac{1}{2}$ " to 70'-1 $\frac{5}{8}$ " o. to o. of deck.

Traffic will be maintained utilizing staged construction.

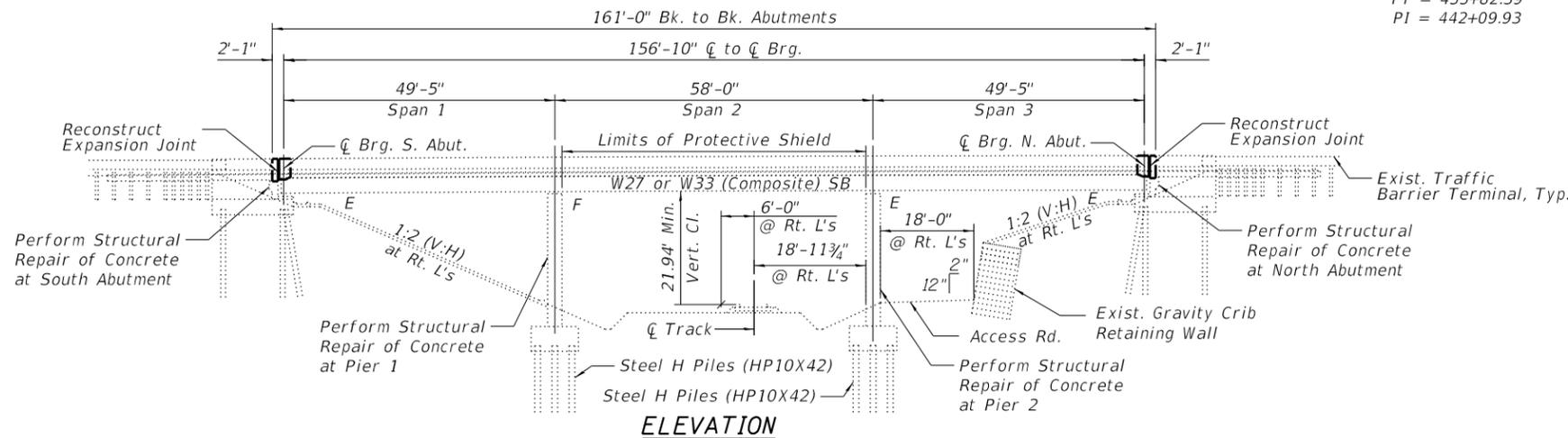
No salvage.

☉ I-55 CURVE DATA

Exist. Curve I-55_EX_18
 $\Delta = 43^\circ 22' 01"$ (LT)
 $T = 1,518.89'$
 $L = 2,891.35'$
 $E = 290.89'$
 $R = 3,820.00'$
 $SE = 2.0\%$
 $PC = 426+91.04$
 $PT = 455+82.39$
 $PI = 442+09.93$



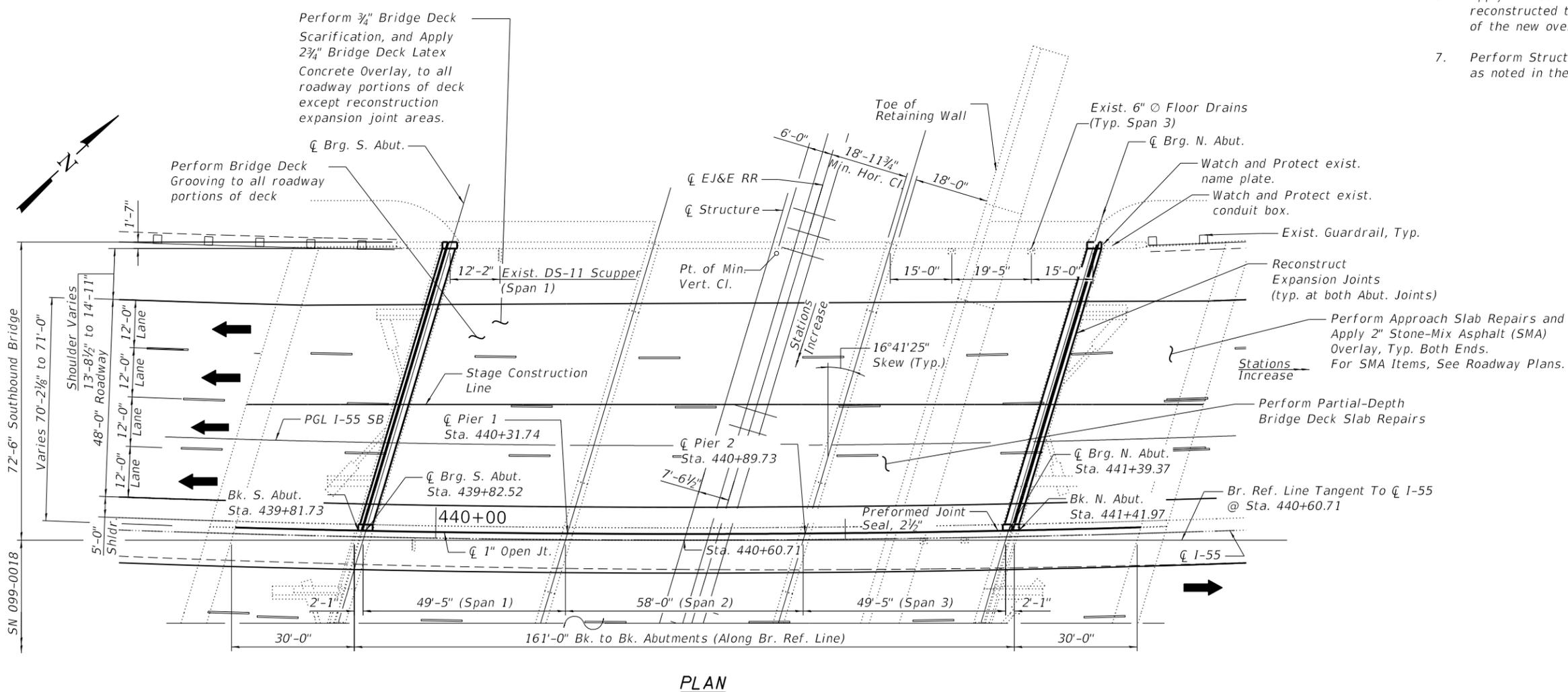
OFFSET SKETCH



ELEVATION

SCOPE OF WORK

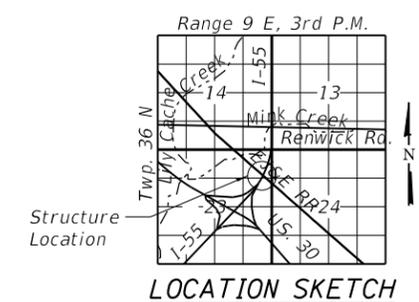
1. Scarify $\frac{3}{4}$ " from the bridge deck slab.
2. Perform deck slab and Approach slab repairs as required.
3. Remove and Reconstruct Expansion Joints at North and South Abutments and install new preformed joint strip seals.
4. Apply a 2 $\frac{3}{4}$ " Bridge Deck Latex Concrete Overlay and Protective Coat on Bridge Deck and 2" Stone-Matrix Asphalt (SMA) Overlay on the Approach Slabs.
5. Perform Bridge Deck Grooving.
6. Apply Protective Coat to the top and inside faces of parapets, reconstructed transverse expansion joints and to the surface of the new overlay.
7. Perform Structural Concrete repairs to the Abutments and Piers as noted in the plans.



PLAN



DATE SIGNED:
 EXP. DATE: 11/30/2022
 SHEETS S2-01 THRU S2-15



LOCATION SKETCH

**GENERAL PLAN AND ELEVATION
 SB I-55 OVER WCL RAILROAD
 F.A.I. 55 SEC 2020-253-BR&PP
 WILL COUNTY
 STATION: 440+60.71
 STRUCTURE NO. 099-0019**

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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SN 099-0019 SB I-55 OVER WCL RAILROAD

SHEET S6-01 OF S6-15 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	129
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

GENERAL NOTES

1. Reinforcement bars designated (E) shall be epoxy coated.
2. Prior to pouring the new concrete deck for Expansion Joints Reconstruction and Deck slab repairs, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.
3. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
4. Joint openings shall be adjusted according to Article 520.04 of the Standard Specification when the deck is poured at an ambient temperature other than 50° F.
5. Bars noted thus, 3x2-#5, indicates 3 lines of #5 bars with 2 lengths of bar per line.
6. All exposed concrete edges shall have a 3/4" chamfer, except where shown otherwise.
7. The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.
8. During repair operations, the Contractor shall locate and protect all utilities in the vicinity of the work including, but not limited to, fiber optic and/or electrical conduits, conduits under the bridge deck, under-deck lighting, traffic signals or signs attached to the structure. This work shall be performed to the satisfaction of the Engineer and will not be paid for separately, but shall be included with the contract. It shall be the Contractor's responsibility to restore and replace any damaged utilities or facilities to the satisfaction of the Engineer at no cost to the Department.
9. Expansion joints shall be fabricated to conform to the existing cross slopes of the bridge.
10. Protective Coat shall be applied to the top and inside face of parapets, reconstructed transverse Expansion Joints and to the surface of the new overlay.
11. Existing reinforcement extended into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal operations shall be replaced using an approved bar splicer or anchorage system. Cost included with Concrete Removal.
12. The Contractor is responsible to protect the existing conduit embedded in the parapet during concrete removal and construction. Any damage to the existing conduit shall be repaired by the Contractor at no additional cost to the Department.

INDEX OF SHEETS

S6-01	General Plan and Elevation
S6-02	General Notes, Total Bill of Materials, & Index of Sheets
S6-03-S6-04	Construction Staging Details
S6-05	Temporary Concrete Barrier for Stage Construction
S6-06	Bridge Deck Repairs
S6-07	Drainage Scuppers and Floor Drains Details
S6-08	Parapet Repairs
S6-09	S. Abut. Expansion Joint Removal and Construction
S6-10	N. Abut. Expansion Joint Removal and Construction
S6-11	Expansion Joint Details
S6-12	Preformed Joint Strip Seal
S6-13	Pier 1 and Pier 2 Repairs
S6-14	North and South Abutment Repairs
S6-15	Bar Splicer Assembly and Mechanical Splicer Details

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Concrete Removal	Cu Yd	19.3		19.3
Protective Shield	Sq Yd	468		468
Concrete Superstructure	Cu Yd	20.8		20.8
Bridge Deck Grooving	Sq Yd	1205		1205
Protective Coat	Sq Yd	1497		1497
Reinforcement Bars, Epoxy Coated	Pound	3780		3780
Bar Splicers	Each	28		28
Preformed Joint Seal 2 1/2"	Foot	221		221
Preformed Joint Strip Seal	Foot	141		141
Concrete Sealer	Sq Ft		407	407
Epoxy Crack Injection	Foot		20	20
Approach Slab Repair (Partial Depth)	Sq Yd	6		6
Bridge Deck Latex Concrete Overlay, 2 3/4"	Sq Yd	1208		1208
Bridge Deck Scarification 3/4"	Sq Yd	1208		1208
Structural Repair of Concrete (Depth Equal to or less than 5")	Sq Ft		80	80

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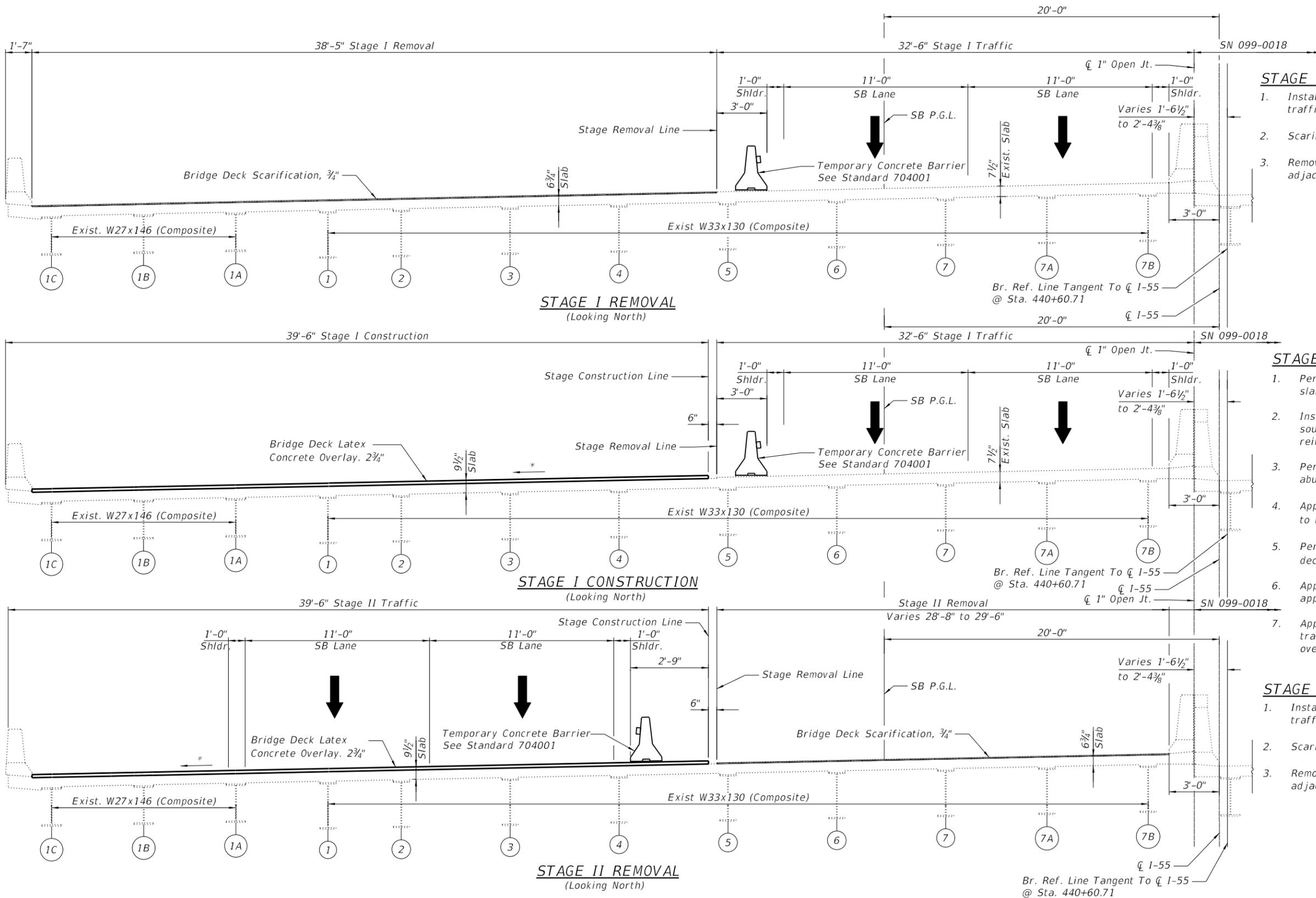
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES, TOTAL BILL OF MATERIALS, & INDEX OF SHEETS
SN 099-0019 SB I-55 OVER WCL RAILROAD

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	130
CONTRACT NO. 62N22				
ILLINOIS			FED. AID PROJECT	

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STAGE I REMOVAL

1. Install temporary concrete barrier as shown to locate traffic on the west side of the existing structure.
2. Scarify 3/4" from the top of deck slab.
3. Remove portions of bridge deck/approach slab adjacent to abutment joints, as shown in the plans.

STAGE I CONSTRUCTION

1. Perform full-depth deck slab repairs, and approach slab repairs, at locations as shown in the plans.
2. Install preformed joint strip seal at north and south abutments and replace associated reinforcement and concrete adjacent to the joint.
3. Perform structural repair of concrete for the abutments and piers.
4. Apply 2 3/4" bridge deck latex concrete overlay to bridge deck slab.
5. Perform bridge deck grooving for the 2 3/4" bridge deck latex concrete overlay.
6. Apply 2" Stone-Matrix Asphalt (SMA) Overlay to approach slabs.
7. Apply protective coat to the top of reconstructed transverse joint areas, the surface of the new overlay and the top and inside faces of parapets.

STAGE II REMOVAL

1. Install temporary concrete barrier as shown to locate traffic on the east side of the existing structure.
2. Scarify 3/4" from the top of deck slab.
3. Remove portions of bridge deck/approach slab adjacent to abutment joints, as shown in the plans.

*Match existing deck surface profile



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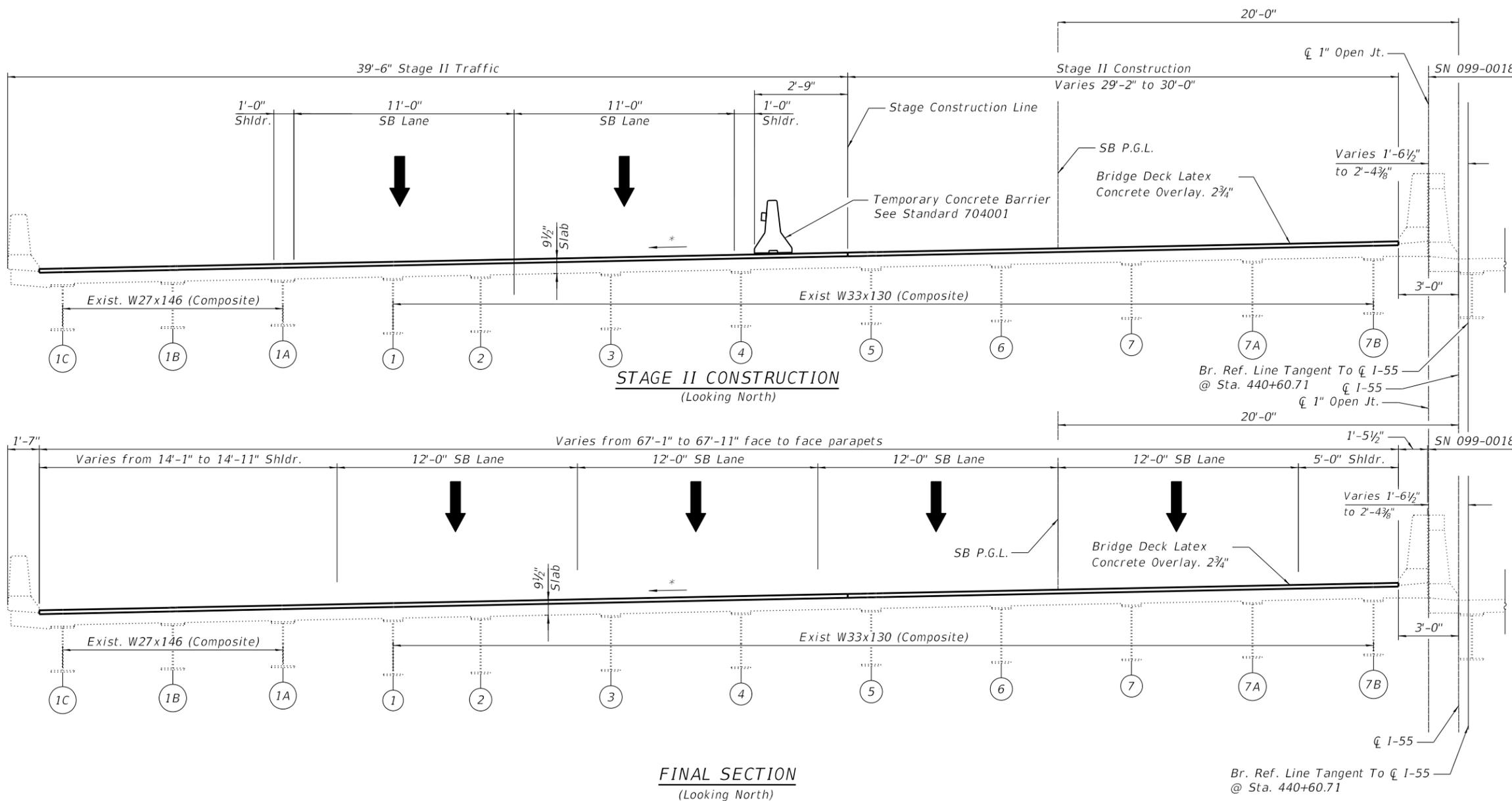
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CONSTRUCTION STAGING DETAILS - I
 SN 099-0019 SB I-55 OVER WCL RAILROAD**

SHEET S6-03 OF S6-15 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	131
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

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STAGE II CONSTRUCTION

1. Perform full-depth deck slab repairs, and approach slab repairs, at locations as shown in the plans.
2. Install preformed joint strip seal at north and south abutments and replace associated reinforcement and concrete adjacent to the joint.
3. Perform structural repair of concrete for the abutments and piers.
4. Apply 2 3/4" bridge deck latex concrete overlay to bridge deck slab.
5. Perform bridge deck grooving for the 2 3/4" bridge deck latex concrete overlay.
6. Apply 2" Stone-Matrix Asphalt (SMA) Overlay to approach slabs.
7. Apply protective coat to the top of reconstructed transverse joint areas, the surface of the new overlay and the top and inside faces of parapets.
8. Apply permanent pavement markings on top of deck and approach slabs.

*Match existing deck surface profile



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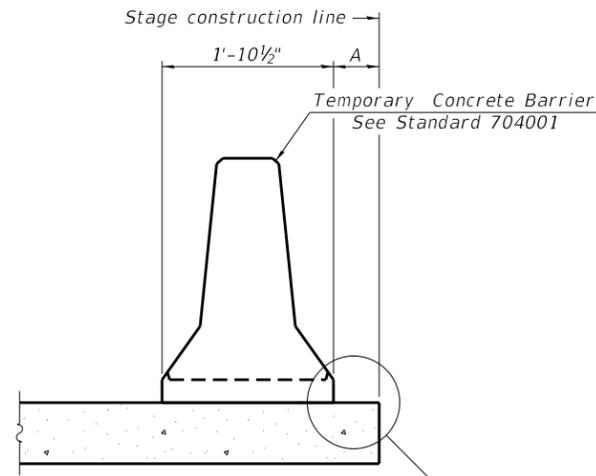
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CONSTRUCTION STAGING DETAILS - II
 SN 099-0019 SB I-55 OVER WCL RAILROAD**

SHEET S6-04 OF S6-15 SHEETS

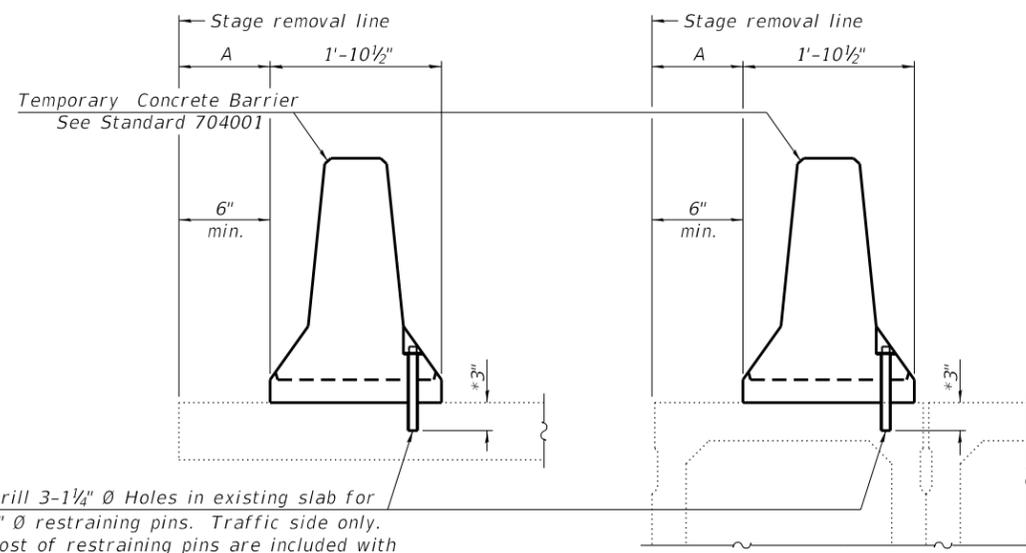
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	132
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

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When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM



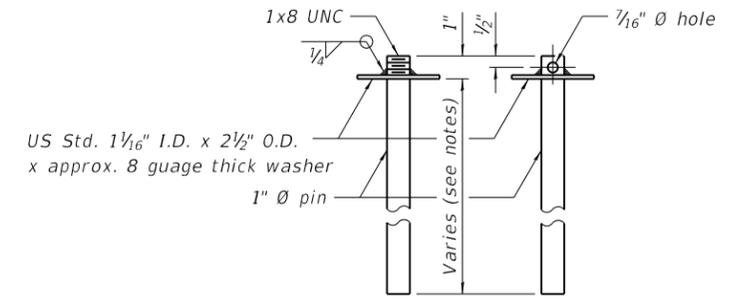
Drill 3-1/4" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

EXISTING SLAB

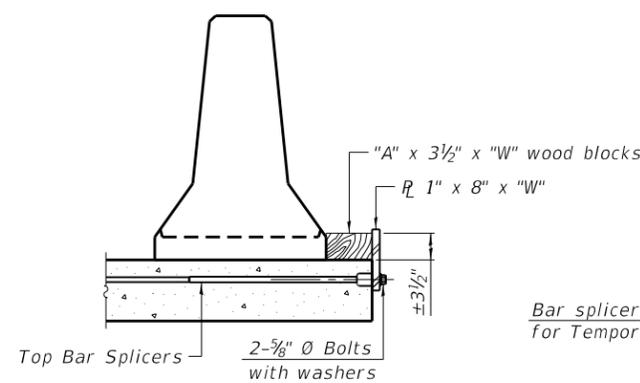
EXISTING DECK BEAM

* When hot-mix asphalt wearing surface is present, embedment shall be 3" plus the wearing surface depth.

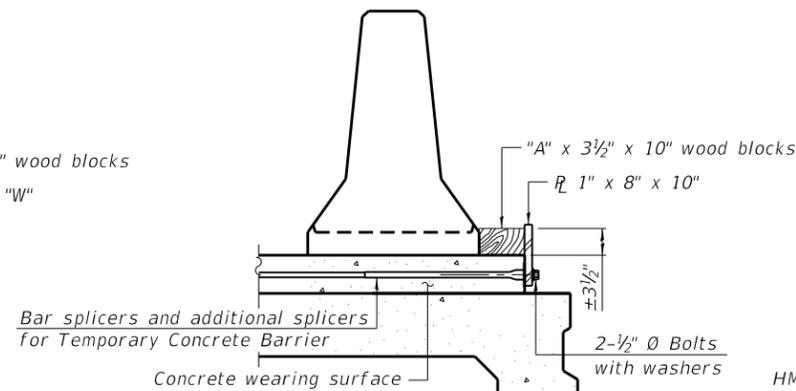
SECTIONS THRU SLAB OR DECK BEAM



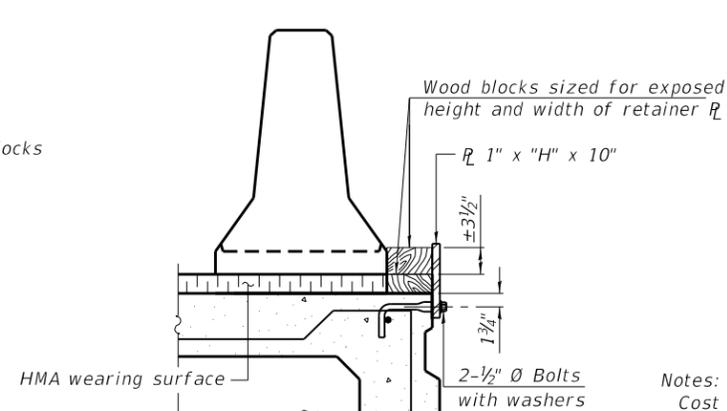
RESTRAINING PIN



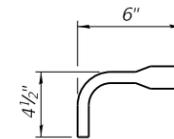
DETAIL I



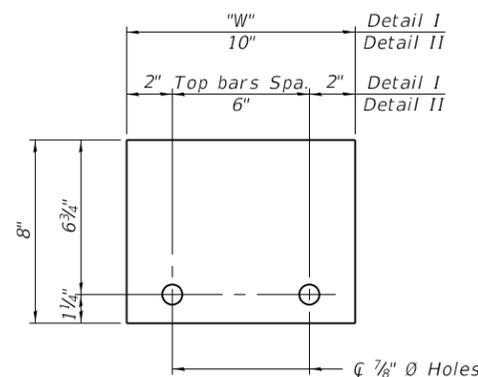
DETAIL II



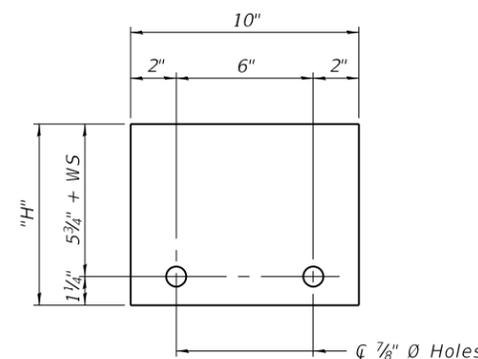
DETAIL III



BAR SPLICER FOR #4 BAR - DETAIL III



STEEL RETAINER 1" x 8" x "W"
(Detail I and II)



STEEL RETAINER 1" x "H" x 10"
(Detail III)

Notes:
 Cost of retainer assembly is included with Temporary Concrete Barrier.
 A retainer assembly shall be located at the approximate center of each temporary concrete barrier.
 The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.
 When the 'A' dimension is less than 1 1/2', the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.

Detail I - Installation for a new bridge deck or bridge slab.
Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.
Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

R-27 8-11-2017



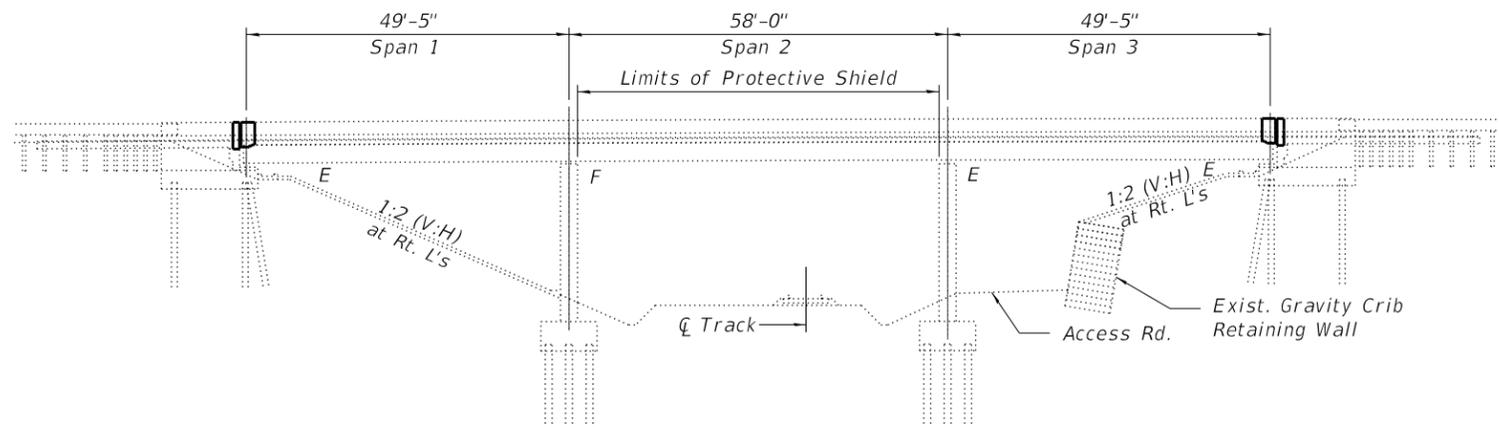
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

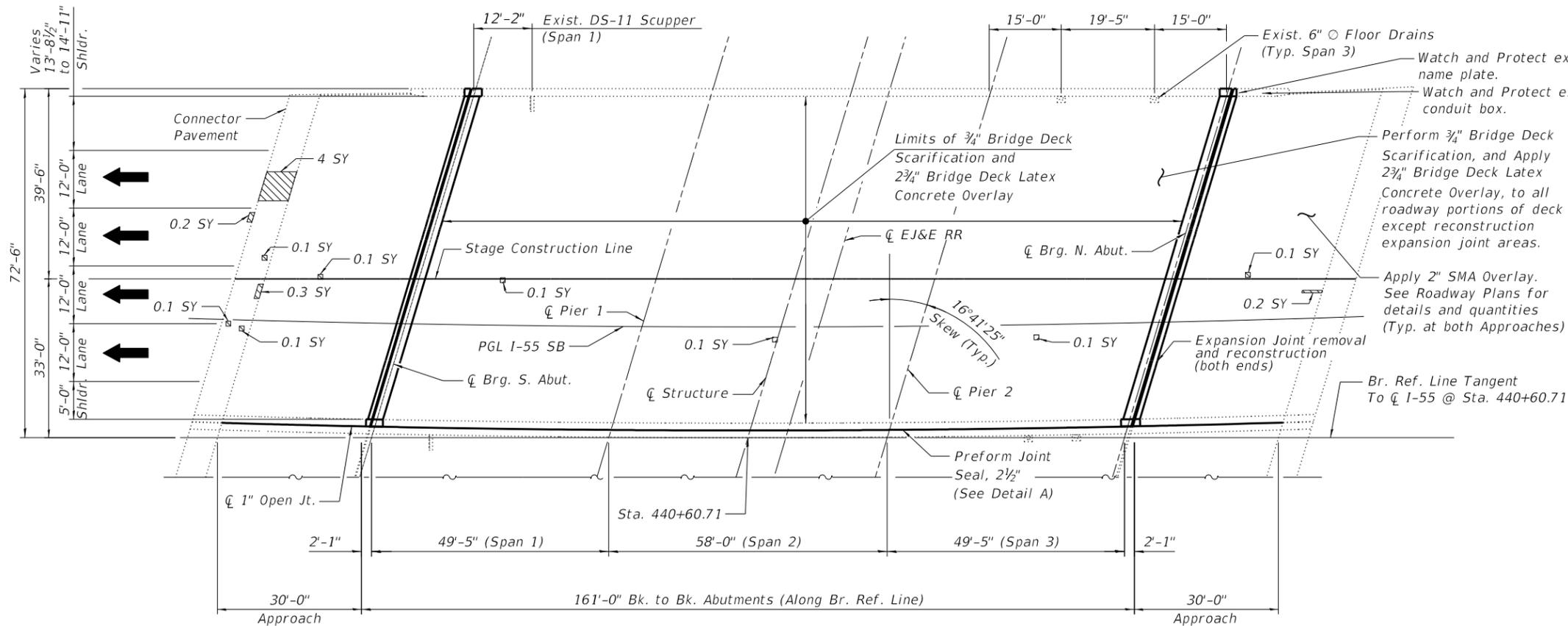
**TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION
SN 099-0019 SB I-55 OVER WCL RAILROAD**

SHEET S6-05 OF S5-15 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	133
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				



ELEVATION



PLAN

NOTES:

- Deck and approach slab repair areas are estimated based on visual inspection and will be paid for as specified in the Special Provision. Actual repair areas and locations shall be determined by the Engineer and shown on As-built plans. Engineer shall sound deck after deck scarification.
- Protective Coat shall be applied to the bridge overlay and front and top faces of the new and existing parapets.
- All dimensions are perpendicular to CL I-55 Bridge Deck.
- Install Protective Shield over EJ&E Railroad.
- For bridge deck final cross section, see Sheet S6-04.
- For North and South Transverse Joint Removal and Reconstruction, see Sheets S6-09 thru S6-10 of S6-15.
- Perform Bridge Deck Grooving for the Bridge Deck Latex Concrete Overlay and the roadway portions of the Reconstructed Transverse Joints.
- Any reinforcement bars that are damaged during concrete removal operations shall be repaired or replaced using an approved bar splicer or anchorage system. Cost included with Concrete Removal.
- The Contractor shall exercise extreme caution during concrete removal to avoid damaging the steel beams and diaphragms to remain. Any damage to the existing steel beams and/or diaphragms to remain caused by the Contractor in the performance of his/her work shall be repaired by the Contractor, to the satisfaction of the Engineer at no cost to the Department.
- See Sheet S6-07 of S6-15 for Drainage Scupper and Floor Drain Details.

LEGEND:

- Deck Slab Repair (Partial)*
- Approach Slab Repair (Full Depth)
- SY Square Yards

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Protective Coat	Sq. Yd.	1497
Bridge Deck Grooving	Sq. Yd.	1205
Bridge Deck Latex Concrete Overlay, 2 3/4"	Sq. Yd.	1208
Bridge Deck Scarification, 3/4"	Sq. Yd.	1208
Protective Shield	Sq. Yd.	468
Approach Slab Repair (Partial Depth)	Sq. Yd.	6

*Areas of Deck Slab Repair (Partial) are provided for information only and shall be included in the cost of Bridge Deck Latex Concrete Overlay, 2 3/4"

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BRIDGE DECK REPAIRS
SN 099-0019 SB I-55 OVER WCL RAILROAD**

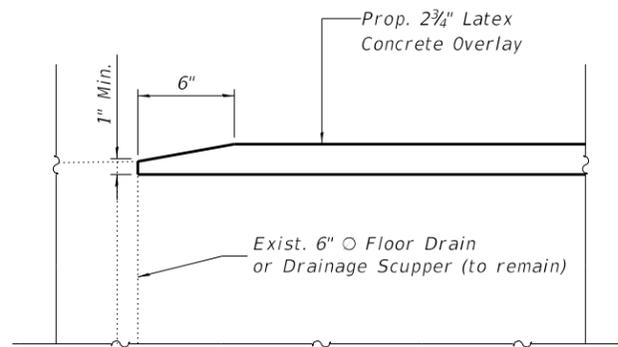
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55	2020-253-BR&PP	WILL	178	134
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

SHEET S6-06 OF S6-15 SHEETS

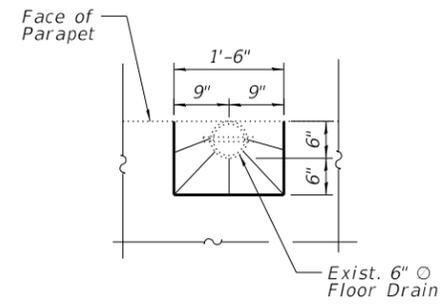
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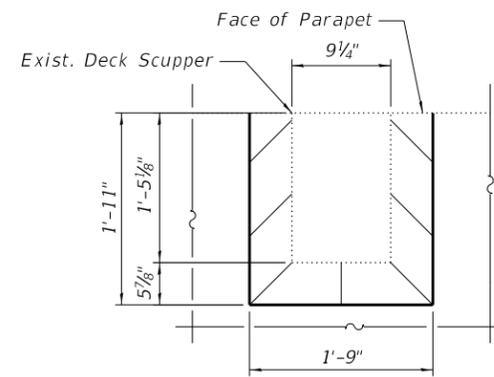
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FLOOR DRAIN/ DRAINAGE SCUPPER DETAIL



6" Ø FLOOR DRAIN TOP PLAN



TOP PLAN AT DRAINAGE SCUPPER

MODEL: Default
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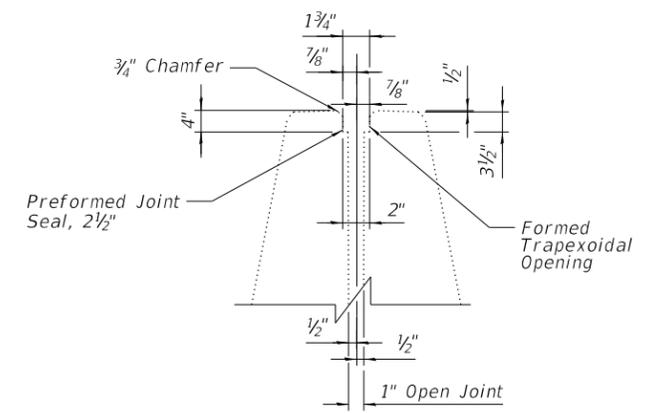
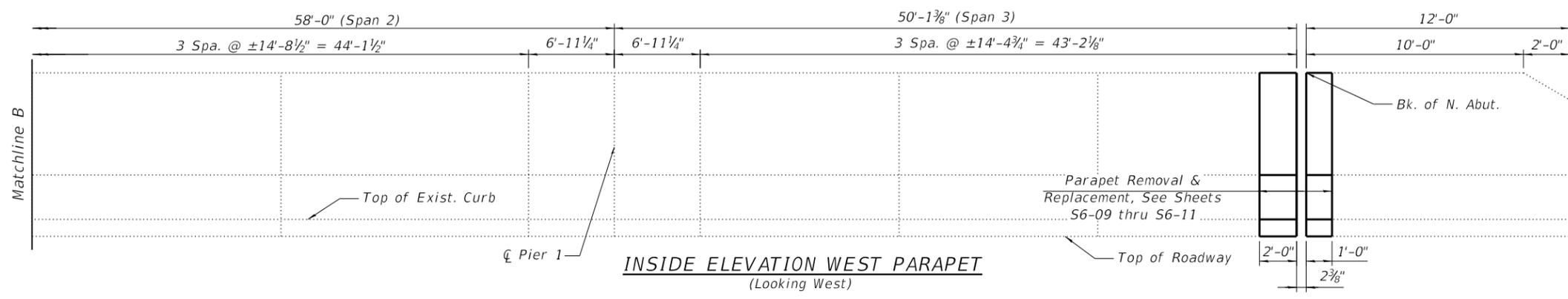
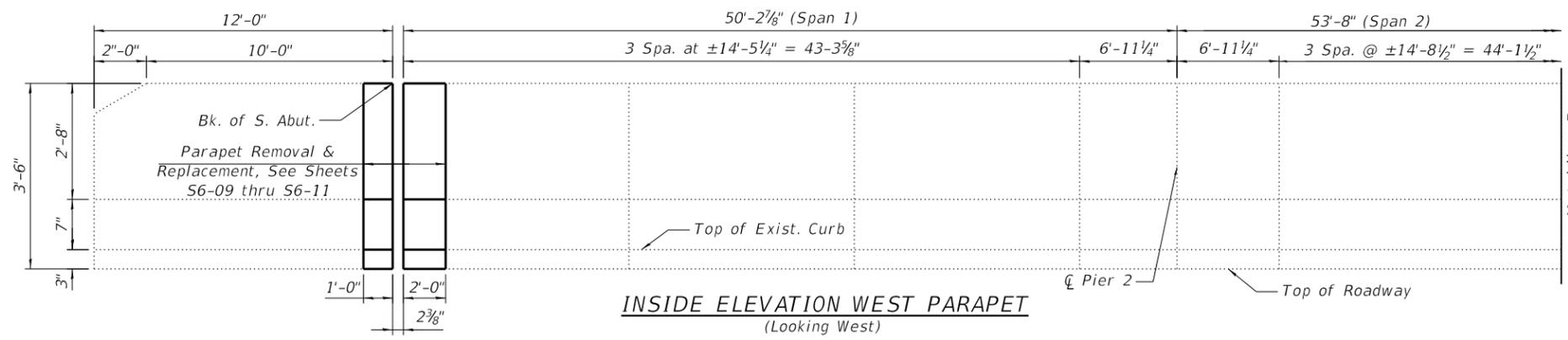
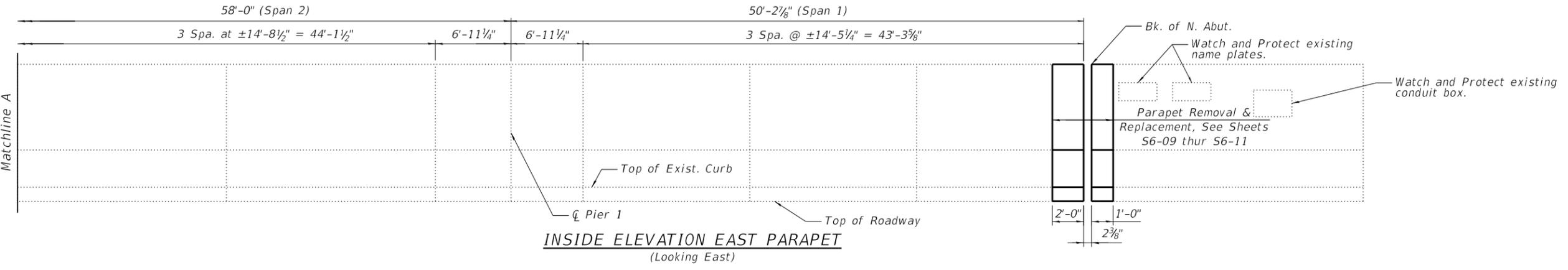
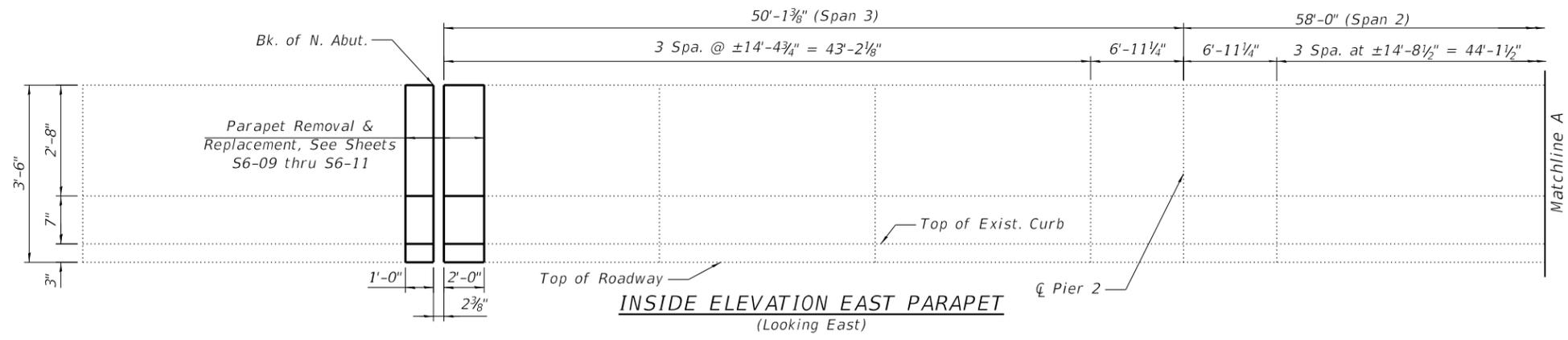
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**DRAINAGE SCUPPERS AND FLOOR DRAINS DETAILS
 SN 099-0019 SB I-55 OVER WCL RAILROAD**

SHEET S6-07 OF S6-15 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	135
CONTRACT NO. 62N22				
ILLINOIS		FED. AID PROJECT		

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BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Seal, 2 1/2"	Foot	221



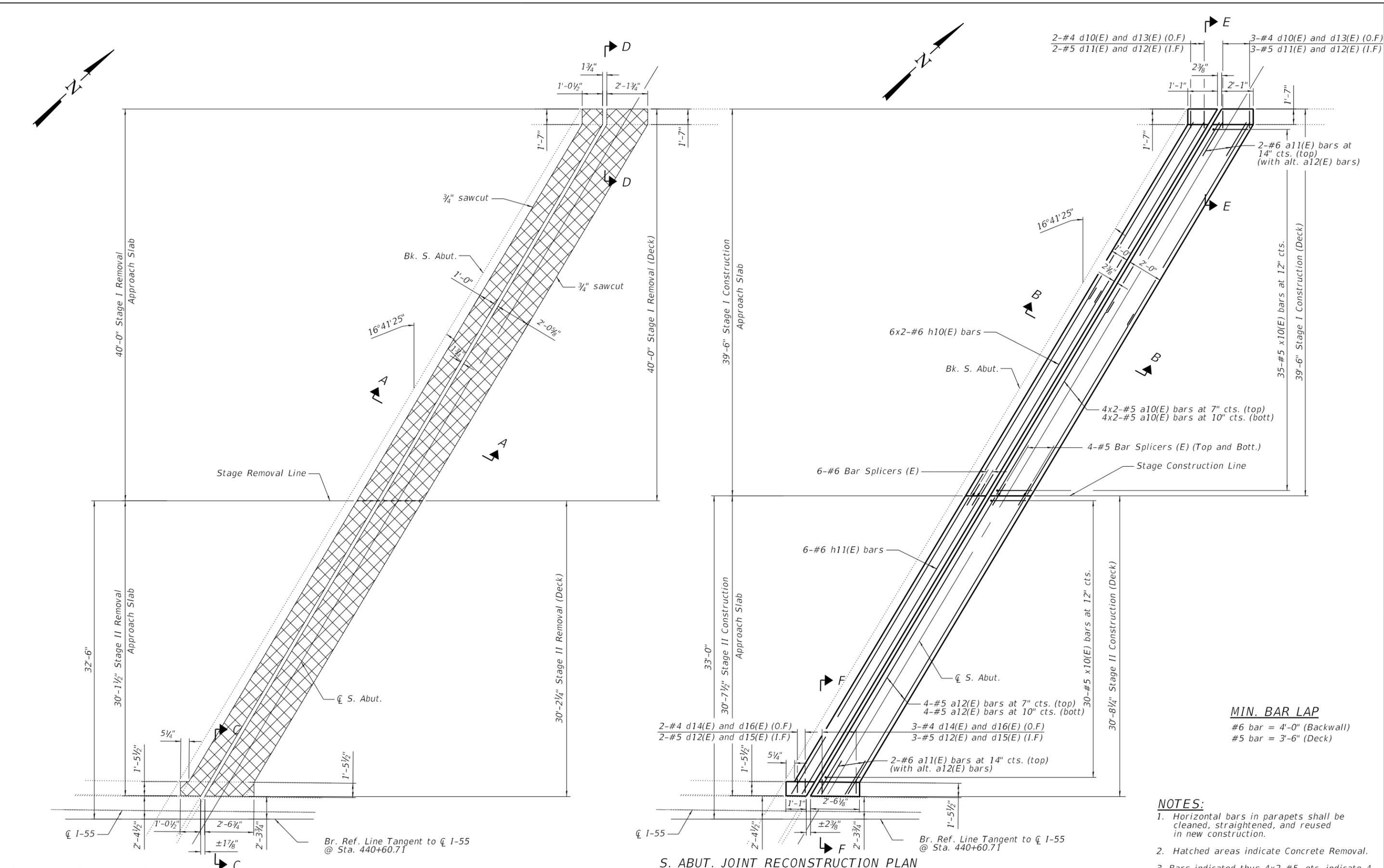
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PLOT SCALE =	DRAWN - IH	REVISED -
PLOT DATE =	CHECKED - JMT	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PARAPET REPAIRS
 SN 099-0019 SB I-55 OVER WCL RAILROAD**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	136
CONTRACT NO. 62N22				
SHEET S6-08 OF S6-15 SHEETS		ILLINOIS FED. AID PROJECT		

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MIN. BAR LAP
 #6 bar = 4'-0" (Backwall)
 #5 bar = 3'-6" (Deck)

- NOTES:**
- Horizontal bars in parapets shall be cleaned, straightened, and reused in new construction.
 - Hatched areas indicate Concrete Removal.
 - Bars indicated thus 4x2-#5, etc. indicate 4 lines of bars with 2 lengths per line.

S. ABUT. JOINT REMOVAL PLAN

S. ABUT. JOINT RECONSTRUCTION PLAN



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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

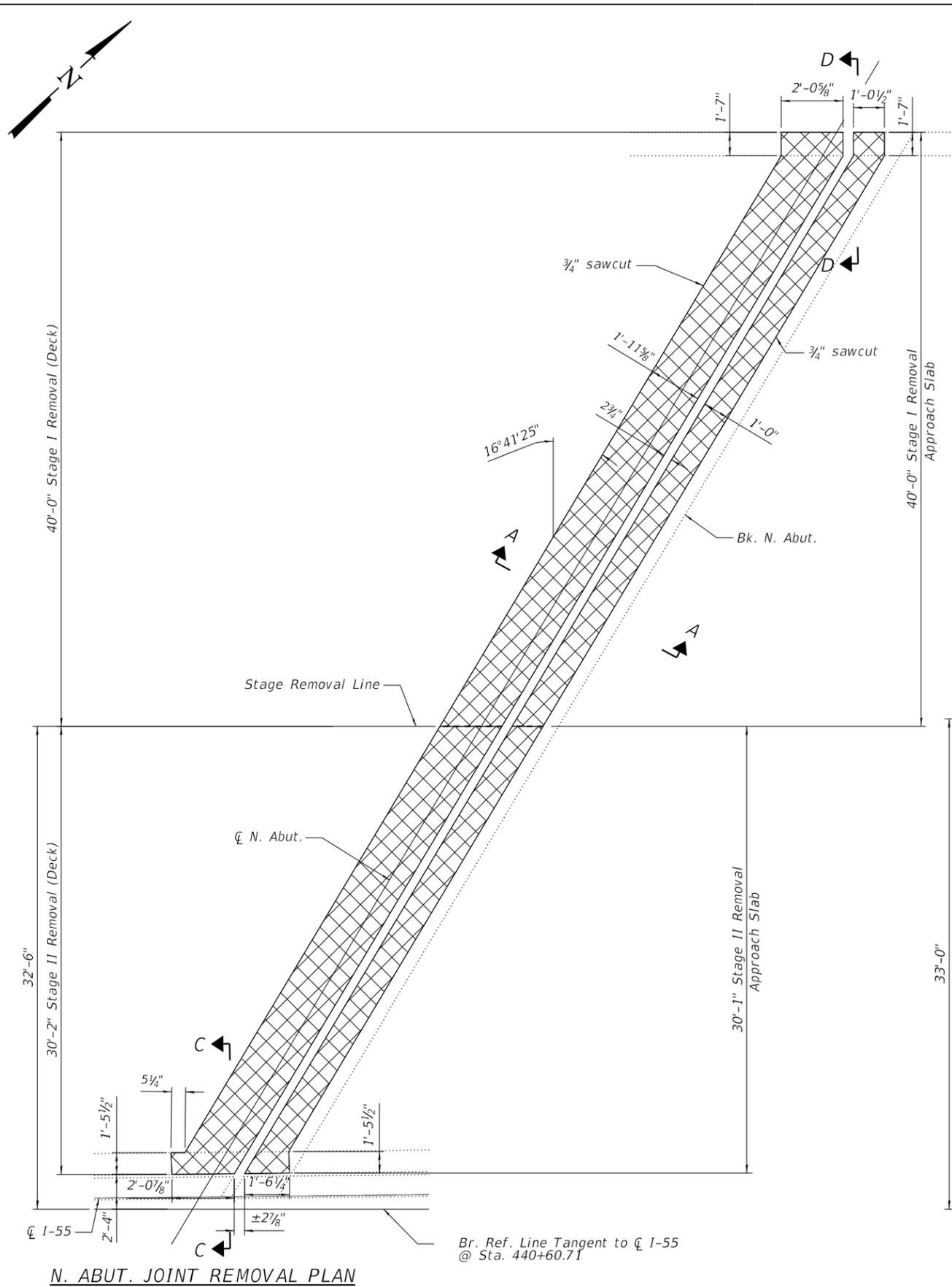
**S. ABUT. EXPANSION JOINT REMOVAL AND CONSTRUCTION
 SN 099-0019 SB I-55 OVER WCL RAILROAD**

SHEET S6-09 OF S6-15 SHEETS

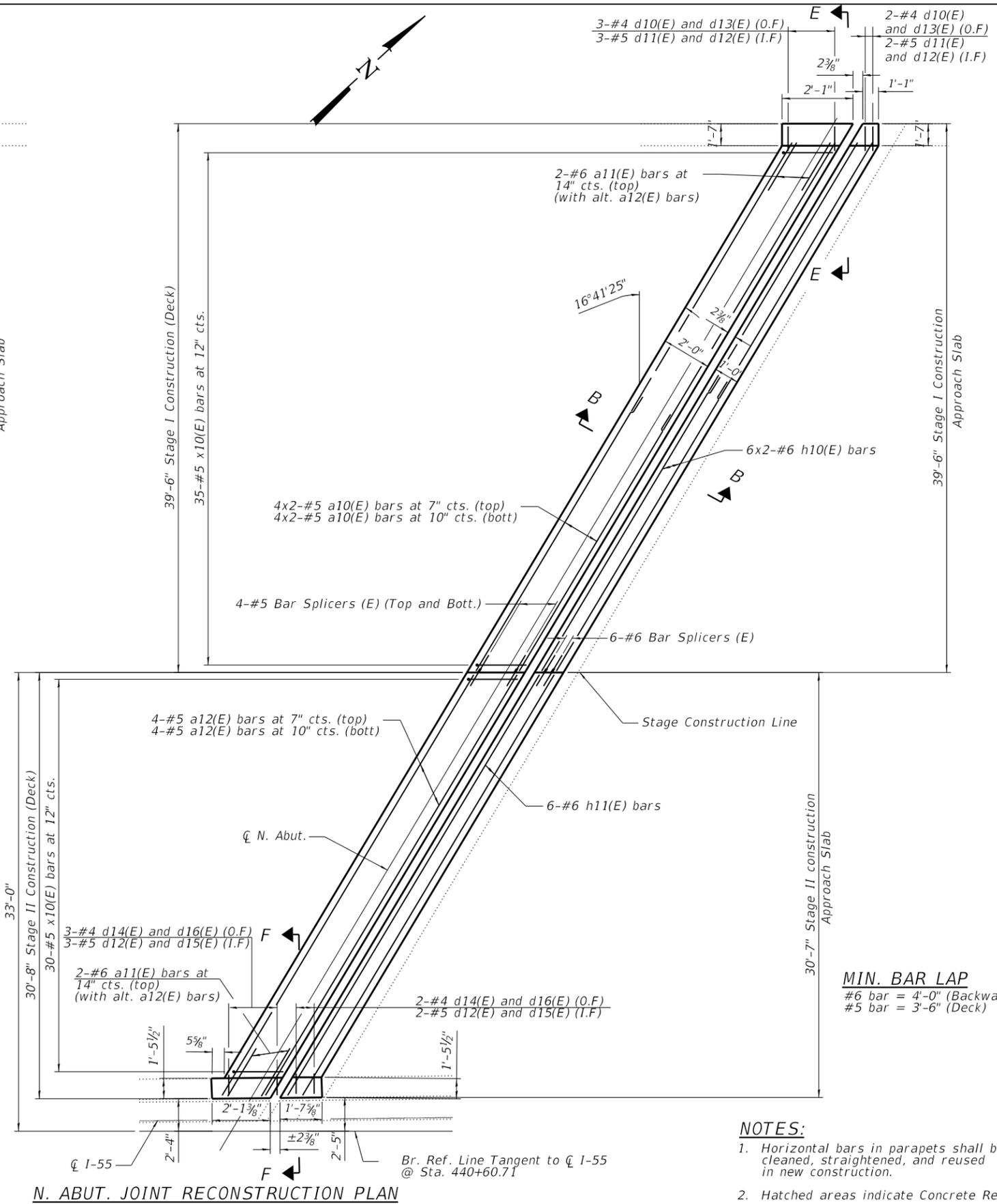
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55	2020-253-BR&PP	WILL	178	137
CONTRACT NO. 62N22				

ILLINOIS FED. AID PROJECT

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N. ABUT. JOINT REMOVAL PLAN



N. ABUT. JOINT RECONSTRUCTION PLAN

MIN. BAR LAP
 #6 bar = 4'-0" (Backwall)
 #5 bar = 3'-6" (Deck)

- NOTES:**
- Horizontal bars in parapets shall be cleaned, straightened, and reused in new construction.
 - Hatched areas indicate Concrete Removal.
 - Bars indicated thus 4x2-#5, etc. indicate 4 lines of bars with 2 lengths per line.



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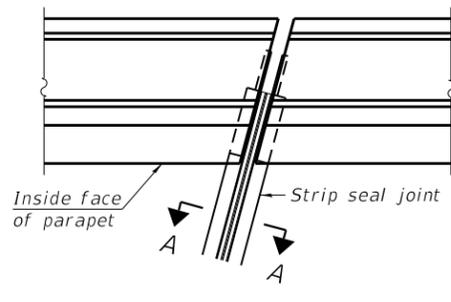
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**N. ABUT. EXPANSION JOINT REMOVAL AND CONSTRUCTION
 SN 099-0019 SB I-55 OVER WCL RAILROAD**

SHEET S6-10 OF S6-15 SHEETS

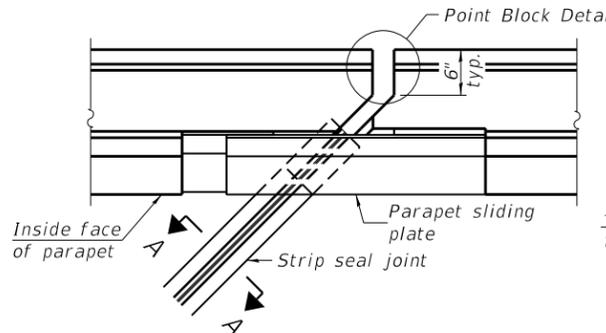
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	138
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

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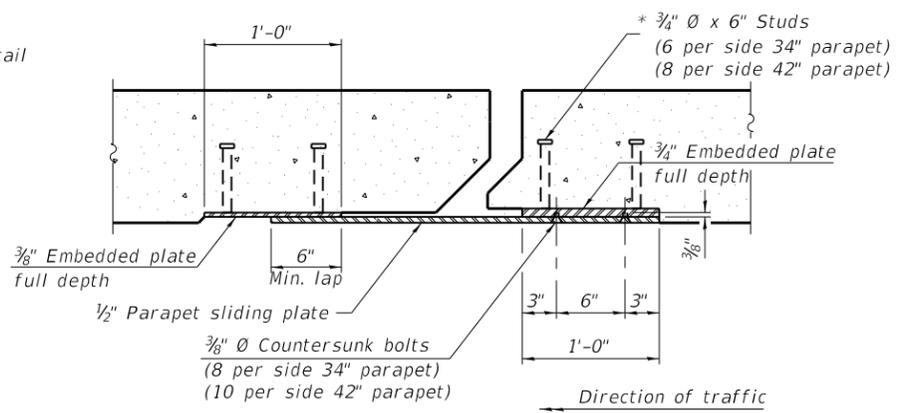


FOR SKEWS $\leq 30^\circ$

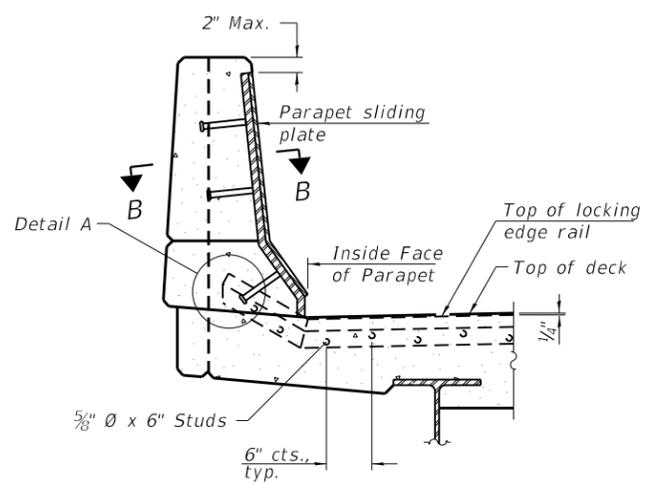
PLAN AT PARAPET



FOR SKEWS $> 30^\circ$

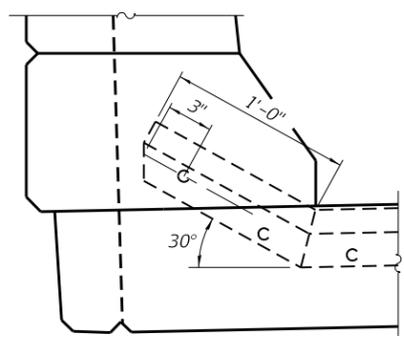


SECTION B-B

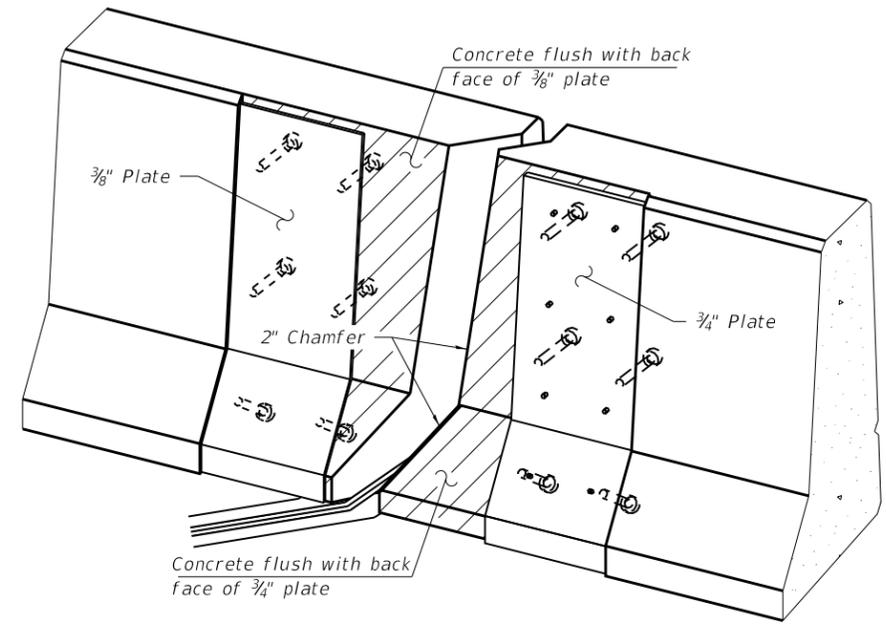


ELEVATION AT PARAPET

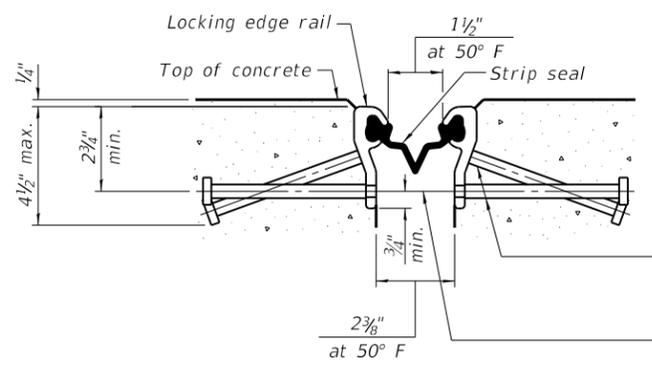
(Skews $> 30^\circ$ shown. Skews $\leq 30^\circ$ similar except as shown in plan view.)



DETAIL A

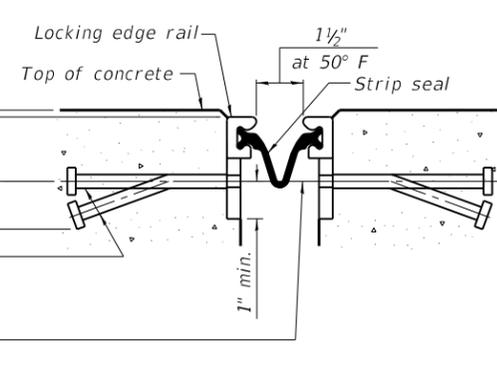


TRIMETRIC VIEW
(Showing embedded plates only)



SHOWING ROLLED RAIL JOINT

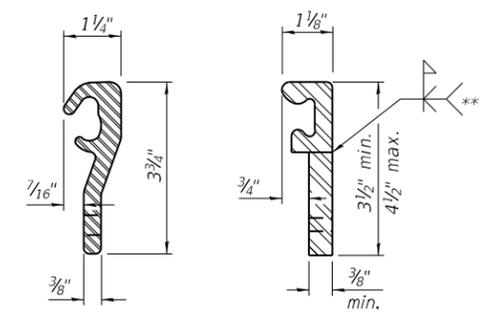
* 5/8" ϕ x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)
 3/8" ϕ threaded rods in 7/16" ϕ holes at ± 4 -0" cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.



SHOWING WELDED RAIL JOINT

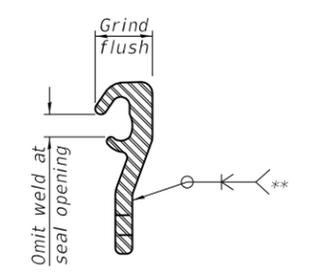
SECTION A-A

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.



LOCKING EDGE RAILS
ROLLED (EXTRUDED) RAIL WELDED RAIL

** Back gouge not required if complete joint penetration is verified by mock-up.



LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	141

EJ-SS

8-11-17



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PLOT SCALE =	CHECKED - SPS	REVISED -
PLOT DATE =	DRAWN - IH	REVISED -
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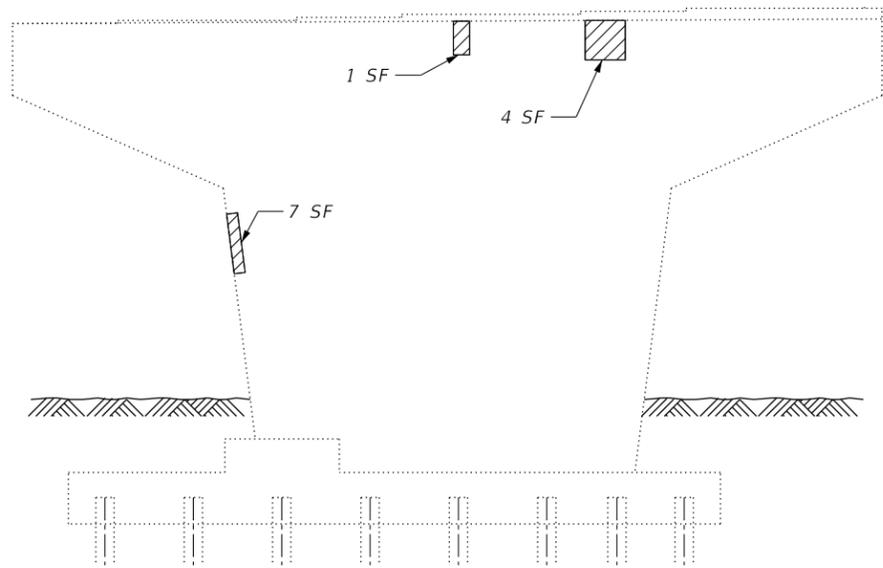
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PREFORMED JOINT STRIP SEAL
 SN 099-0019 SB I-55 OVER WCL RAILROAD

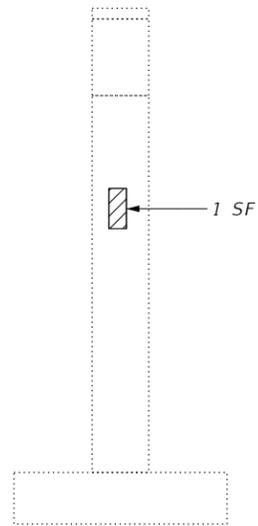
SHEET S6-12 OF S6-15 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

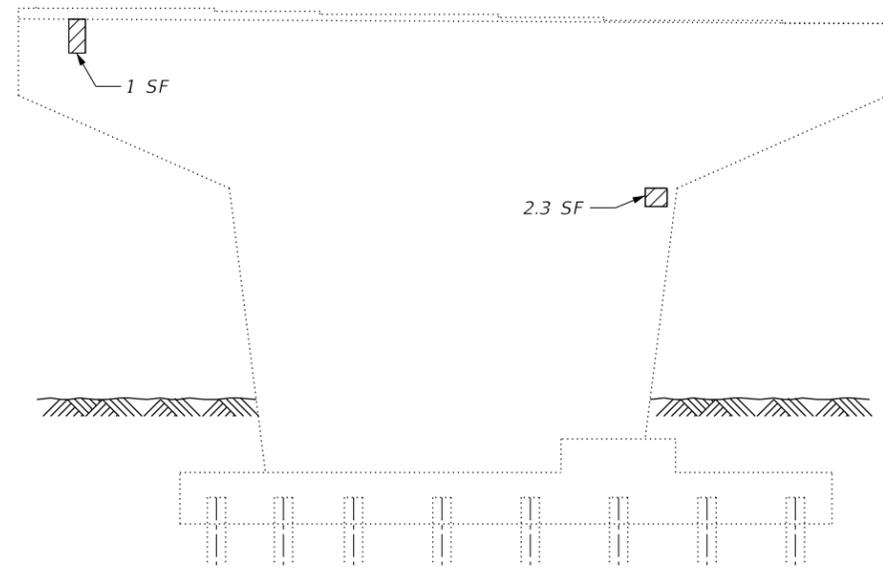
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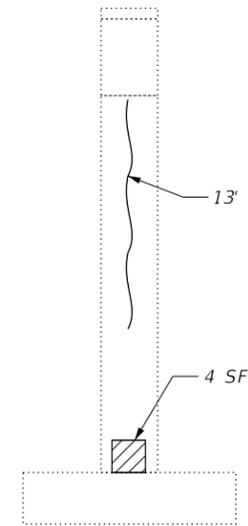
PIER 1
 (Looking North)



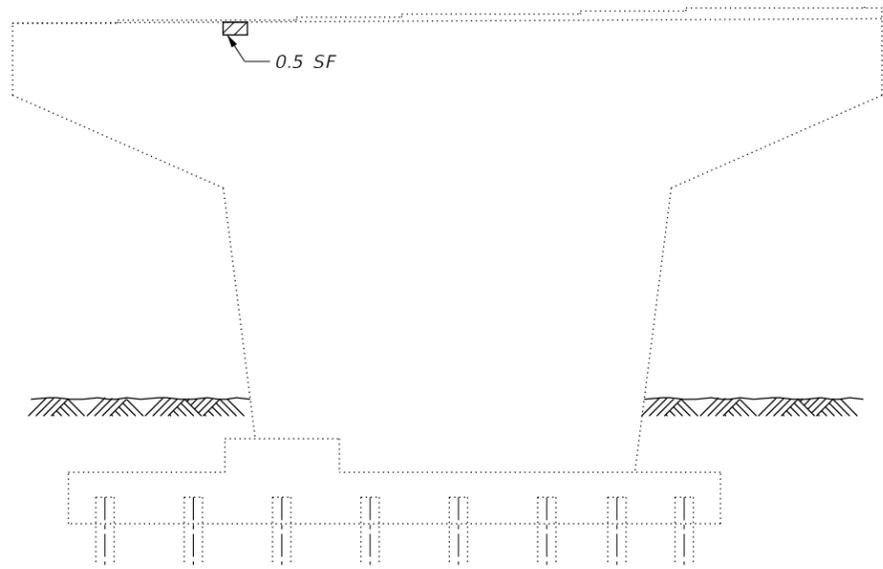
PIER 1 - END VIEW
 (Looking West)



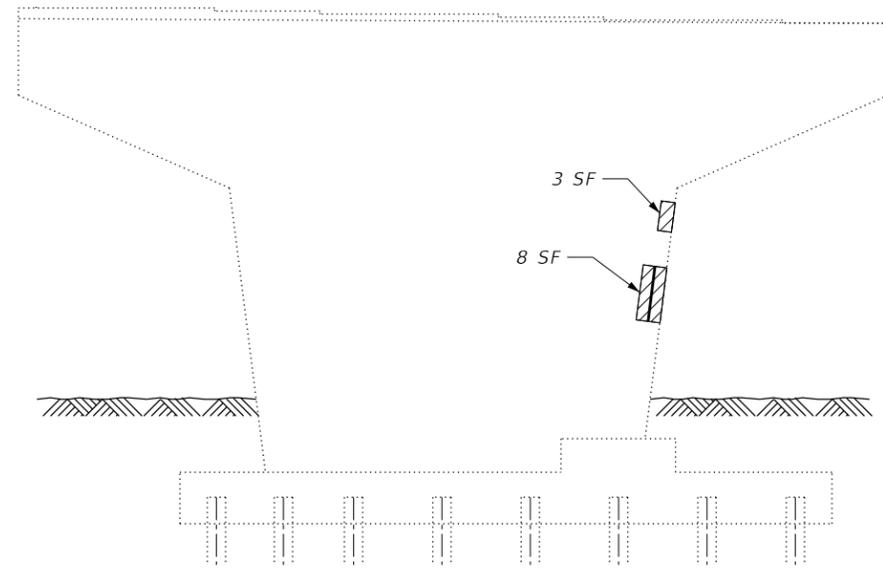
PIER 1
 (Looking South)



PIER 1 - END VIEW
 (Looking East)



PIER 2
 (Looking North)



PIER 2
 (Looking South)

LEGEND:

Structural Repair of Concrete
 (Depth Equal to or less than 5")

Exposed Reinforcement

Epoxy Crack Injection

SF Square Foot

NOTES:

- Quantities and limits shown are estimated for bidding purposes only. The actual areas to be repaired, and the type(s) of repairs to be used, will be determined by the Engineer in the field at the time of construction.

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth Equal to or Less Than 5")	Sq Ft	32
Epoxy Crack Injection	Foot	13



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

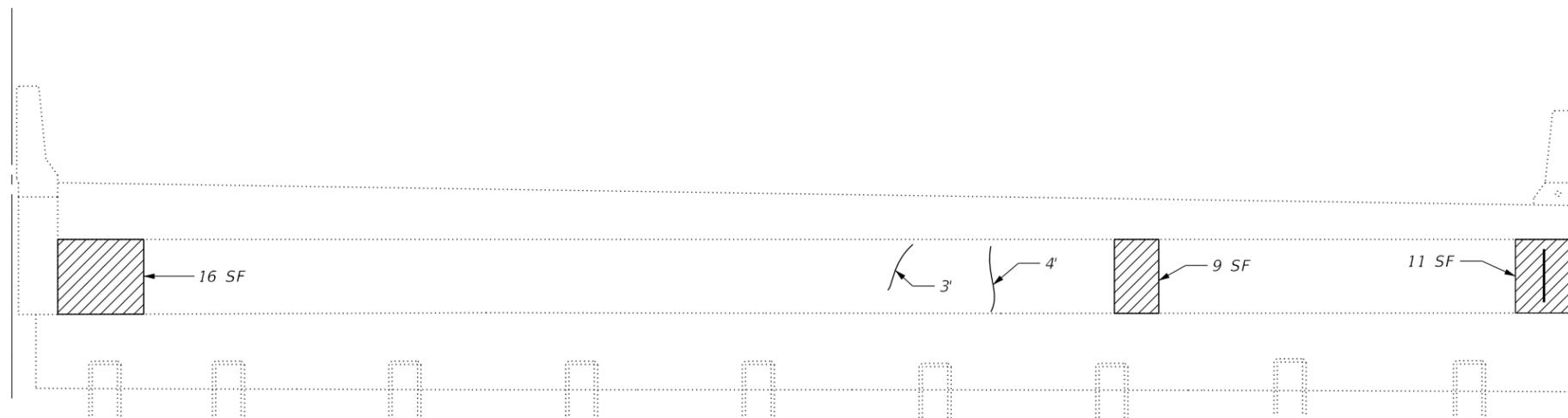
PIER 1 AND PIER 2 REPAIRS
SN 099-0019 SB I-55 OVER WCL RAILROAD

SHEET S6-13 OF S6-15 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	141
CONTRACT NO. 62N22				
		ILLINOIS	FED. AID PROJECT	



ELEVATION - NORTH ABUTMENT
(Looking North)



ELEVATION - SOUTH ABUTMENT
(Looking South)

LEGEND:

 Structural Repair of Concrete
(Depth Equal to or less than 5")

 Exposed Reinforcement

 Epoxy Crack Injection

SF Square Foot

NOTES:

- Quantities and limits shown are estimated for bidding purposes only. The actual areas to be repaired, and the type(s) of repairs to be used, will be determined by the Engineer in the field at the time of construction.
- Apply Concrete Sealer to face of Abutments.

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth Equal to or Less Than 5")	Sq Ft	48
Epoxy Crack Injection	Foot	7
Concrete Sealer	Sq Ft	407

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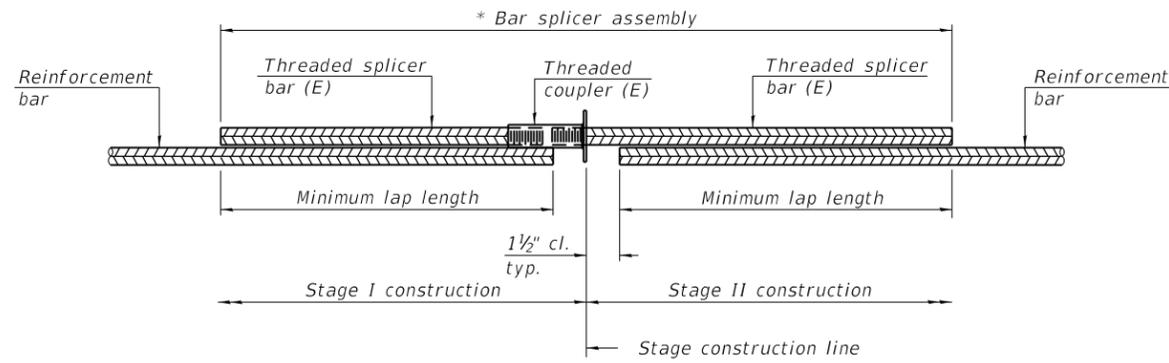
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PLOT DATE =	CHECKED - JMT	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

NORTH AND SOUTH ABUTMENT REPAIRS
SN 099-0019 SB I-55 OVER WCL RAILROAD

SHEET S6-14 OF S6-15 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	142
CONTRACT NO. 62N22				
		ILLINOIS	FED. AID PROJECT	

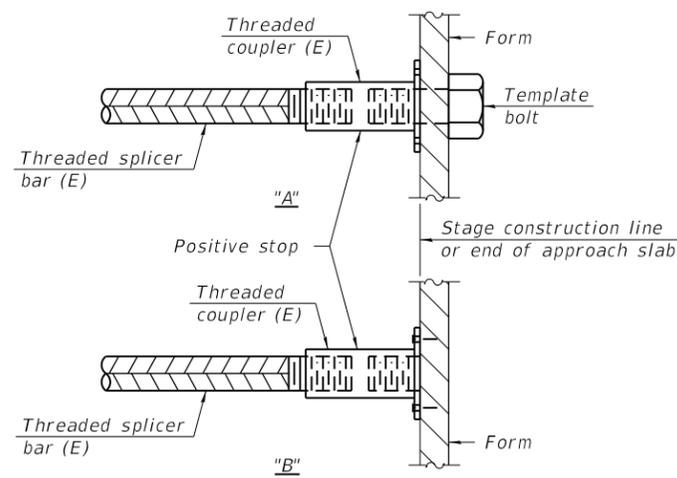


STANDARD BAR SPLICER ASSEMBLY PLAN
 (All components shall be provided from one supplier)

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

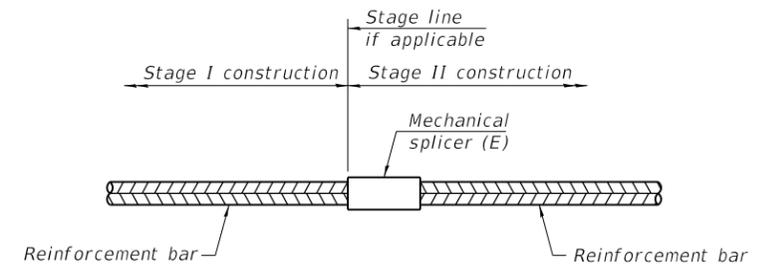
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
S. Abut. Deck	#5	8	3'-6"
S. Abut. Backwall	#6	6	4'-0"
N. Abut. Deck	#5	8	3'-6"
N. Abut. Backwall	#6	6	4'-0"



INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required

Notes:
 Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

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BSD-1

1-1-2020



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PLOT SCALE =	DRAWN - IH	REVISED -
PLOT DATE =	CHECKED - JMT	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
 SN 099-0019 SB I-55 OVER WCL RAILROAD

SHEET S6-15 OF S6-15 SHEETS

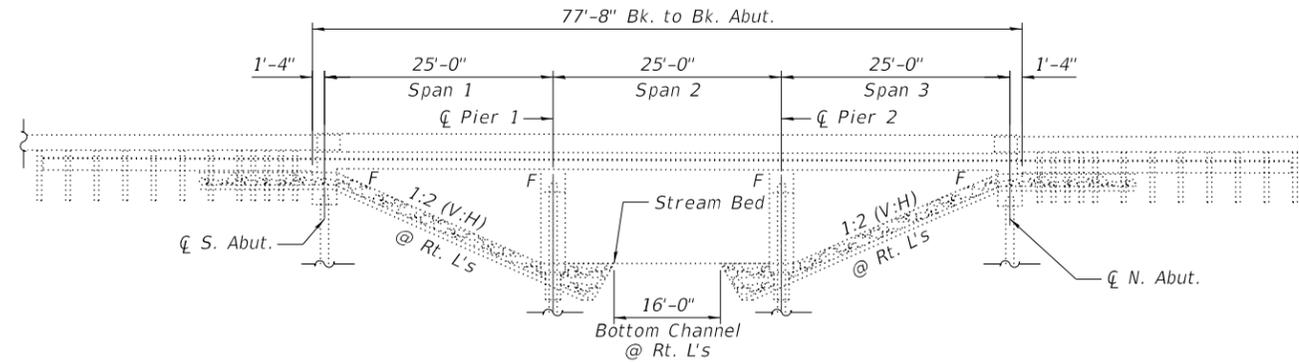
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	143
CONTRACT NO. 62N22				

ILLINOIS FED. AID PROJECT

Existing Structure: Structure No. 0994616 was constructed in 1994 as part of F.A.I. Route 55, Section 27(B-1,B-3,HB,VB-1)BR89 and project F.A. IM-NHI-55-6-(193)252 and reconstructed/widened in 2007. The structure has a length of 77'-8" (back-to-back of abutments) and an out-to-out deck width of 57'-7". The continuous slab superstructure consists of three equal span lengths of 25'-0". The reinforced concrete deck slab is 12" thick. The substructure consists of reinforced concrete piers and abutments on steel piles.

Traffic is to be maintained utilizing stage construction.

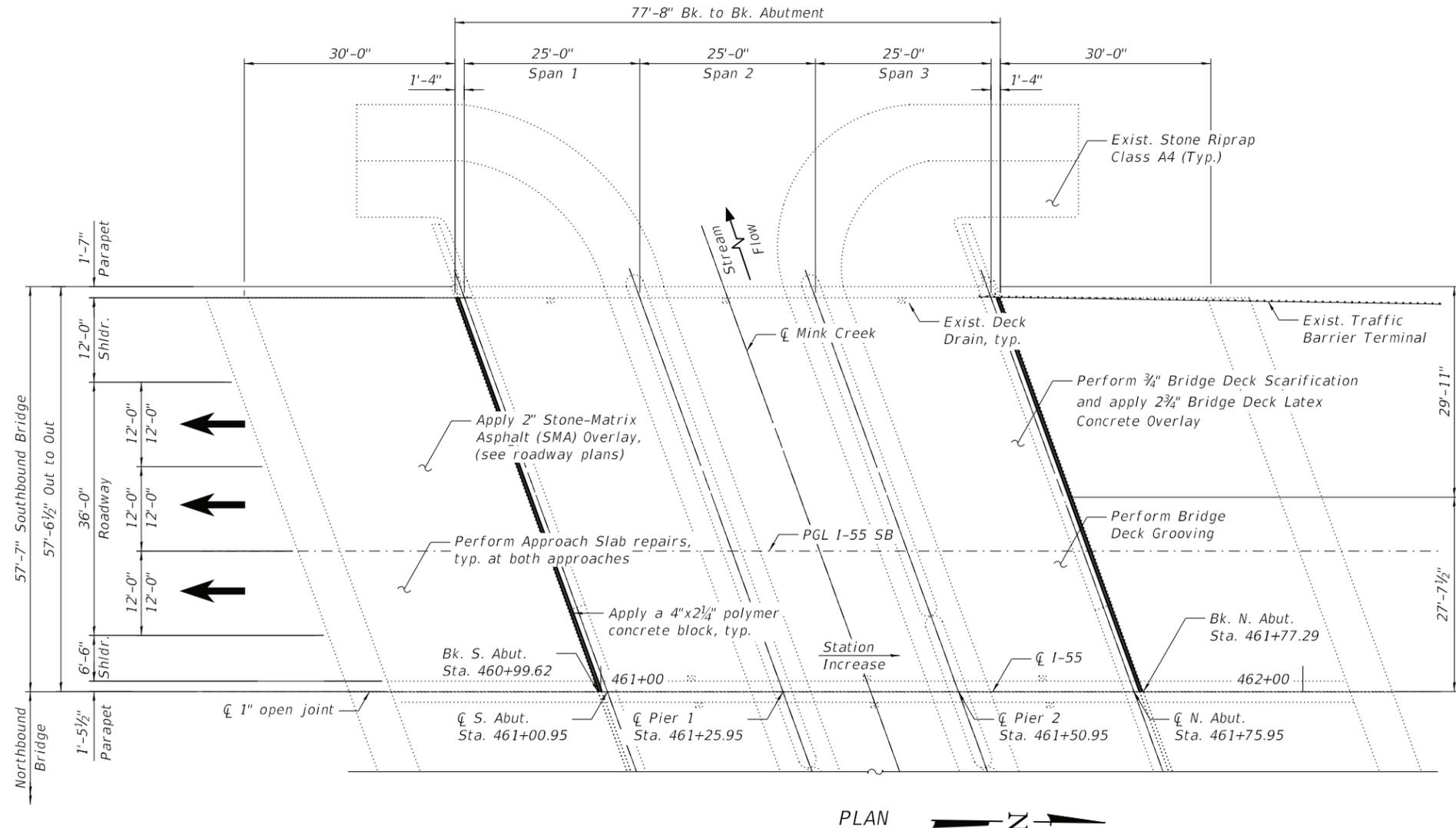
No Salvage.



ELEVATION

SCOPE OF WORK

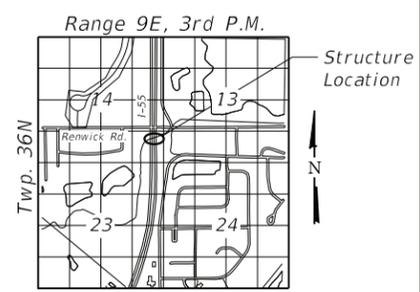
1. Scarify 3/4" from the bridge deck slab.
2. Perform Deck Slab Repairs and Approach Slab Repairs as required.
3. Apply a 4"x2 1/4" polymer concrete block, at both ends of deck.
4. Apply a 2 3/4" Bridge Deck Latex Concrete Overlay to the bridge deck and 2" Stone-Matrix Asphalt (SMA) Overlay on the Approach Slabs. See Roadway plans for SMA items.
5. Apply Protective Coat to the top and inside face of parapets and top of Latex Overlay.
6. Perform Bridge Deck Grooving.
7. Replace 2 1/2" PJS between parapets; coordinate with SN 099-4615.
8. Clean all deck drains.



PLAN



Signed Moussa A. Issa
 Dr. Moussa A. Issa, S.E. Il. Lic. No. 081-005738
 Expires 11-30-2022
 Date December 03, 2020 FOR SHEETS S7-01 THRU S7-07
 (Total of 7 Sheets)



LOCATION SKETCH

GENERAL PLAN AND ELEVATION
I-55 OVER MINK CREEK
F.A.I. ROUTE 55 - SEC. 2006-032
WILL COUNTY
STATION 630+07.11
S.N. 099-4616

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PLOT DATE =	DATE - 12/3/2020	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

S.N. 099-4616 I-55 SB OVER MINK CREEK (0.8 MILES N OF US 30)

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	144
			CONTRACT NO. 62N22	
		ILLINOIS	FED. AID PROJECT	

GENERAL NOTES:

1. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
2. Protective coat shall be applied to top and inside face of parapets and Latex Overlay.
3. All exposed concrete edges shall have a 3/4"x45° chamfer, except where shown otherwise.
4. The Contractor is responsible to protect the existing conduit embedded in the parapet during concrete removal and construction. Any damage to the existing conduit shall be repaired by the Contractor at no additional cost to the Department.
5. Coordinate with the Contractor for Contract No. 62K51 prior to installation of the preformed Joint Seal 2 1/2". All work on Parapets must be completed for Contract No. 62K51 and this Contract prior to installation of the Longitudinal Joint.

INDEX OF SHEETS

- S7-01 General Plan & Elevation
- S7-02 Structure Notes, Index of Sheets & Total Bill of Material
- S7-03 Stage Construction (Sheet 1 of 2)
- S7-04 Stage Construction (Sheet 2 of 2)
- S7-05 Temporary Concrete Barrier For Stage Construction
- S7-06 Bridge Deck Repairs
- S7-07 Approach Slab Repairs

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Bridge Deck Grooving	SQ YD	448	0	448
Protective Coat	SQ YD	539	0	539
Preformed Joint Seal 2 1/2"	FOOT	78	0	78
Approach Slab Repair (Partial Depth)	SQ YD	1	0	1
Bridge Deck Latex Concrete Overlay, 2 3/4 Inches	SQ YD	464	0	464
Bridge Deck Scarification 3/4"	SQ YD	464	0	464
Polymer Concrete	CU FT	9	0	9

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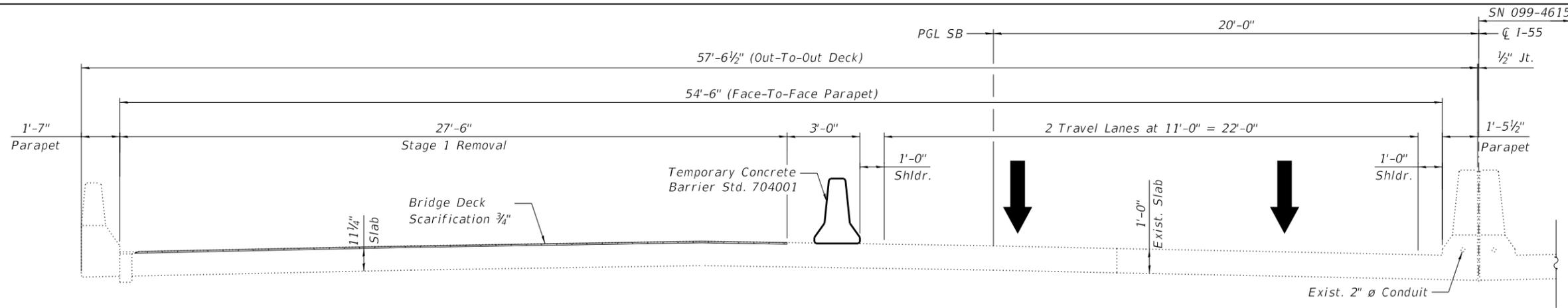
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PLOT DATE =	DATE - 12/2/2020	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**STRUCTURE NOTES, INDEX OF SHEETS & TOTAL BILL OF MATERIAL
S.N. 099-4616 I-55 SB OVER MINK CREEK (0.8 MILES N OF US 30)**

SHEET S7-02 OF S7-07 SHEETS

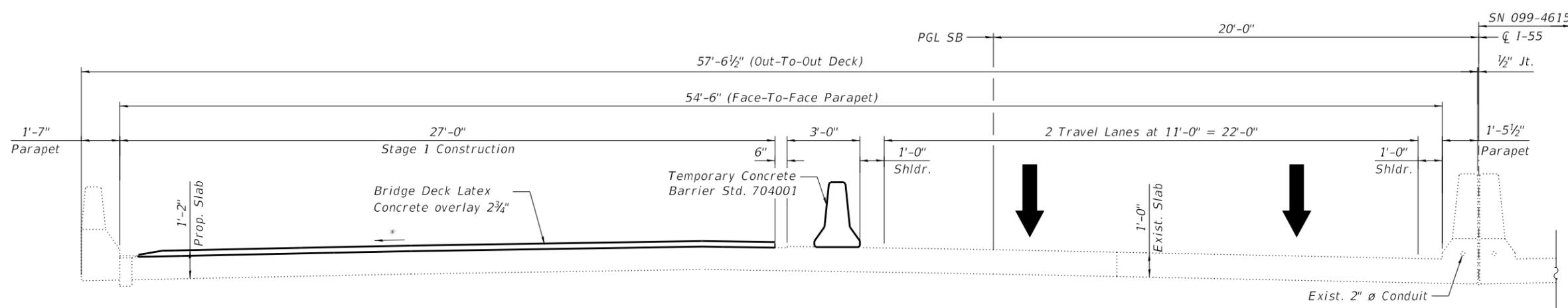
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	145
			CONTRACT NO. 62N22	
ILLINOIS		FED. AID PROJECT		



STAGE 1 REMOVAL
(Looking North)

STAGE 1 REMOVAL

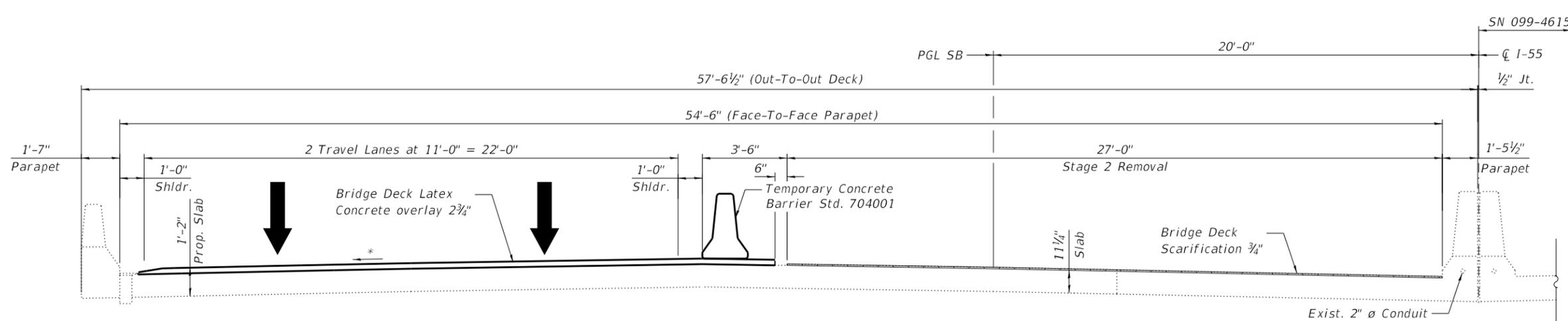
1. Install temporary concrete barrier as shown to locate traffic on the east side of the existing structure.
2. Scarify 3/4" from the top of deck slab.



STAGE 1 CONSTRUCTION
(Looking North)

STAGE 1 CONSTRUCTION

1. Perform partial-depth approach slab repairs, at locations as shown in the plans.
2. Apply 2 3/4" bridge deck latex concrete overlay to bridge deck slab.
3. Perform bridge deck grooving for the 2 3/4" bridge deck latex concrete overlay and apply Protective Coat to the top and inside face of parapets and top of Latex Overlay.
4. Apply 2" Stone-Matrix Asphalt (SMA) Overlay to approach slabs, see Roadway plans.



STAGE 2 REMOVAL
(Looking North)

STAGE 2 REMOVAL

1. Install temporary concrete barrier as shown to locate traffic on west side of the existing structure.
2. Scarify 3/4" from the top of deck slab.

* Match existing deck surface profile

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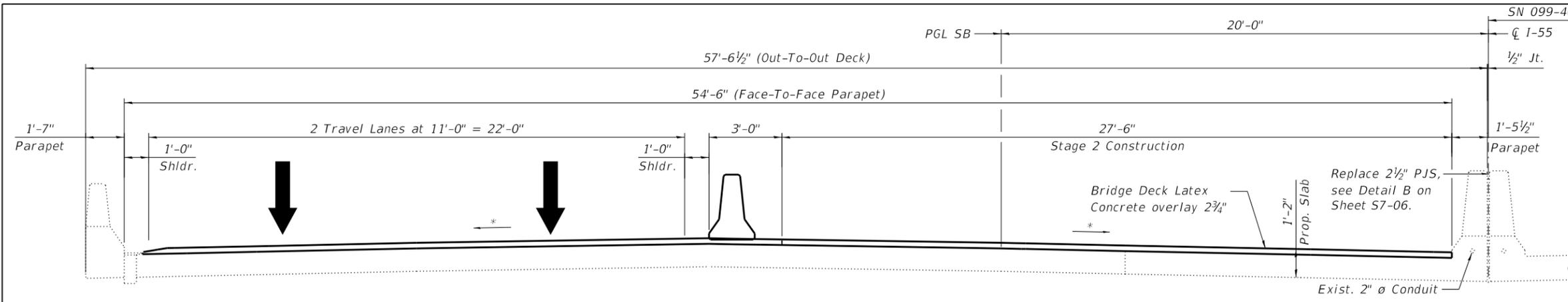
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PLOT DATE =	DATE - 12/4/2020	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGE CONSTRUCTION (SHEET 1 OF 2)
S.N. 099-4616 I-55 SB OVER MINK CREEK (0.8 MILES N OF US 30)

SHEET S7-03 OF S7-07 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	146
			CONTRACT NO. 62N22	
		ILLINOIS FED. AID PROJECT		

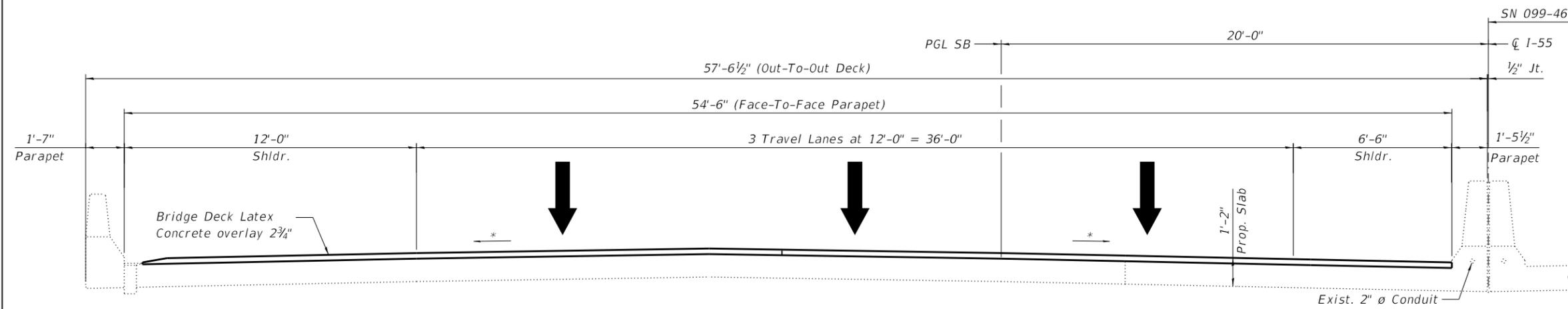


STAGE 2 CONSTRUCTION
(Looking North)

STAGE 2 CONSTRUCTION

1. Perform partial-depth deck slab repairs, and approach slab repairs, at locations as shown in the plans.
2. Apply 2 3/4" bridge deck latex concrete overlay to bridge deck slab.
3. Perform bridge deck grooving for the 2 3/4" bridge deck latex concrete overlay and apply Protective Coat to the top and inside face of parapets and top of Latex Overlay.
4. Apply 2" Stone-Matrix Asphalt (SMA) Overlay to approach slabs, see Roadway plans.
5. Replace 2 1/2" PJS in Parapet: coordinate with SN 099-4615.
6. Apply Permanent Pavement Markings on top of deck and approach slabs. For Pavement Markings, see Roadway Plans.

* Match existing deck surface profile



FINAL CROSS SECTION
(Looking North)

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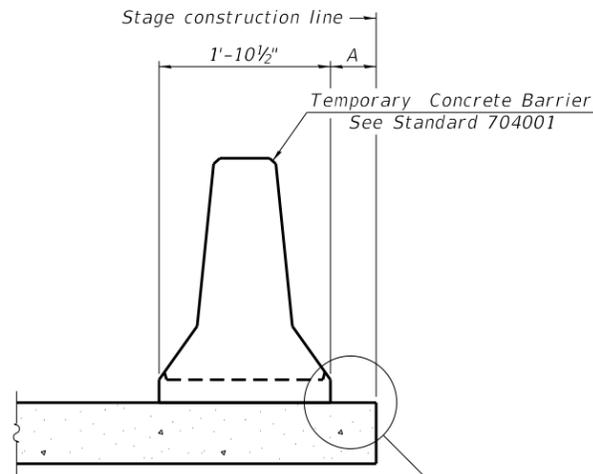
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PLOT DATE =	DATE - 12/4/2020	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGE CONSTRUCTION (SHEET 2 OF 2)
S.N. 099-4616 I-55 SB OVER MINK CREEK (0.8 MILES N OF US 30)

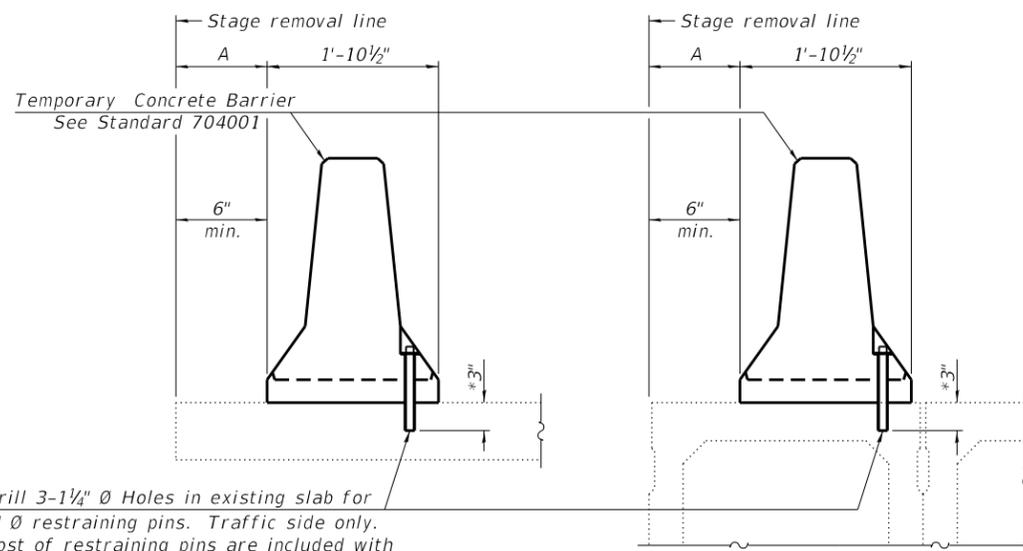
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	147
			CONTRACT NO. 62N22	
		ILLINOIS FED. AID PROJECT		

SHEET S7-04 OF S7-07 SHEETS



When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM

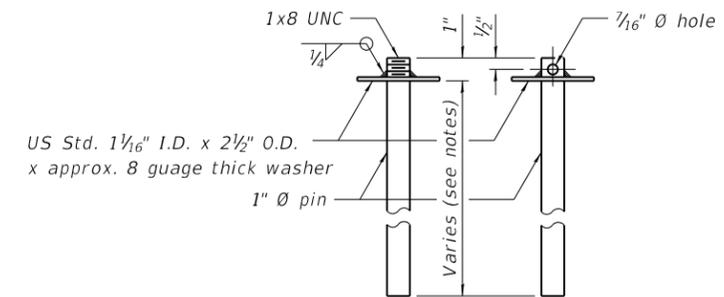


Drill 3-1/4" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

EXISTING SLAB

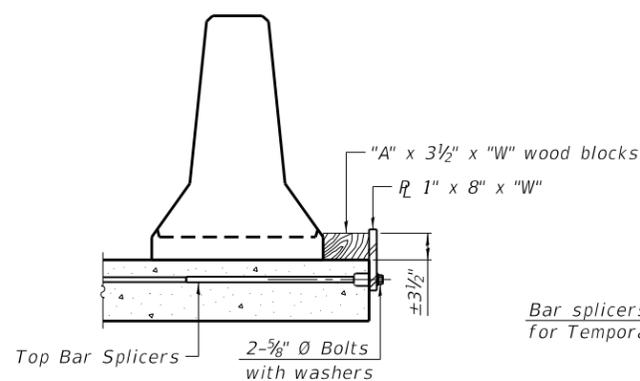
* When hot-mix asphalt wearing surface is present, embedment shall be 3" plus the wearing surface depth.

EXISTING DECK BEAM

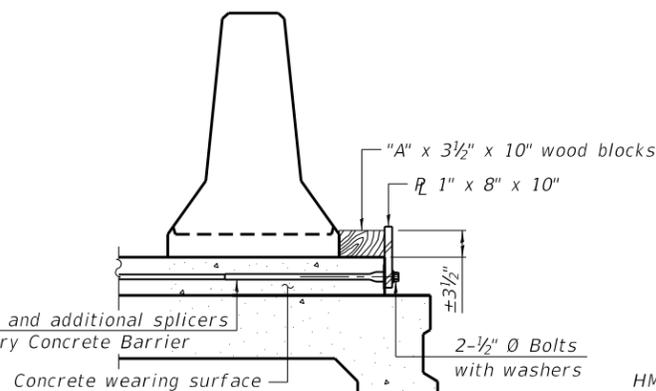


RESTRAINING PIN

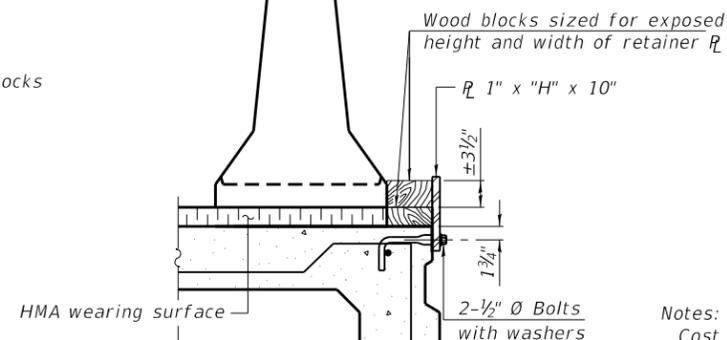
SECTIONS THRU SLAB OR DECK BEAM



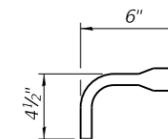
DETAIL I



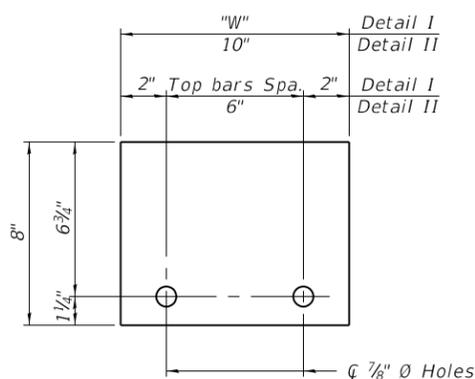
DETAIL II



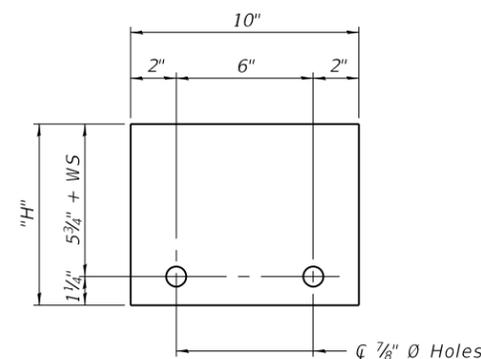
DETAIL III



BAR SPLICER FOR #4 BAR - DETAIL III



STEEL RETAINER R 1" x 8" x "W"
(Detail I and II)



STEEL RETAINER R 1" x "H" x 10"
(Detail III)

Notes:
 Cost of retainer assembly is included with Temporary Concrete Barrier.
 A retainer assembly shall be located at the approximate \bar{C} of each temporary concrete barrier.
 The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.
 When the 'A' dimension is less than 1 1/2', the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.

Detail I - Installation for a new bridge deck or bridge slab.
 Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.
 Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

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R-27 2-17-2017



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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

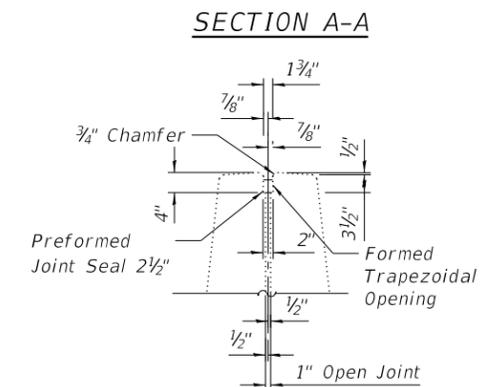
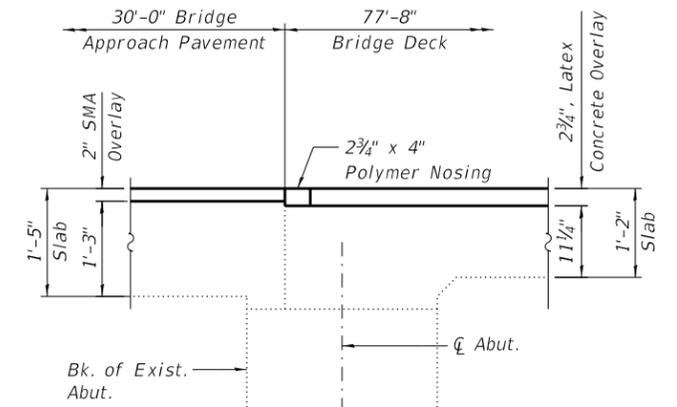
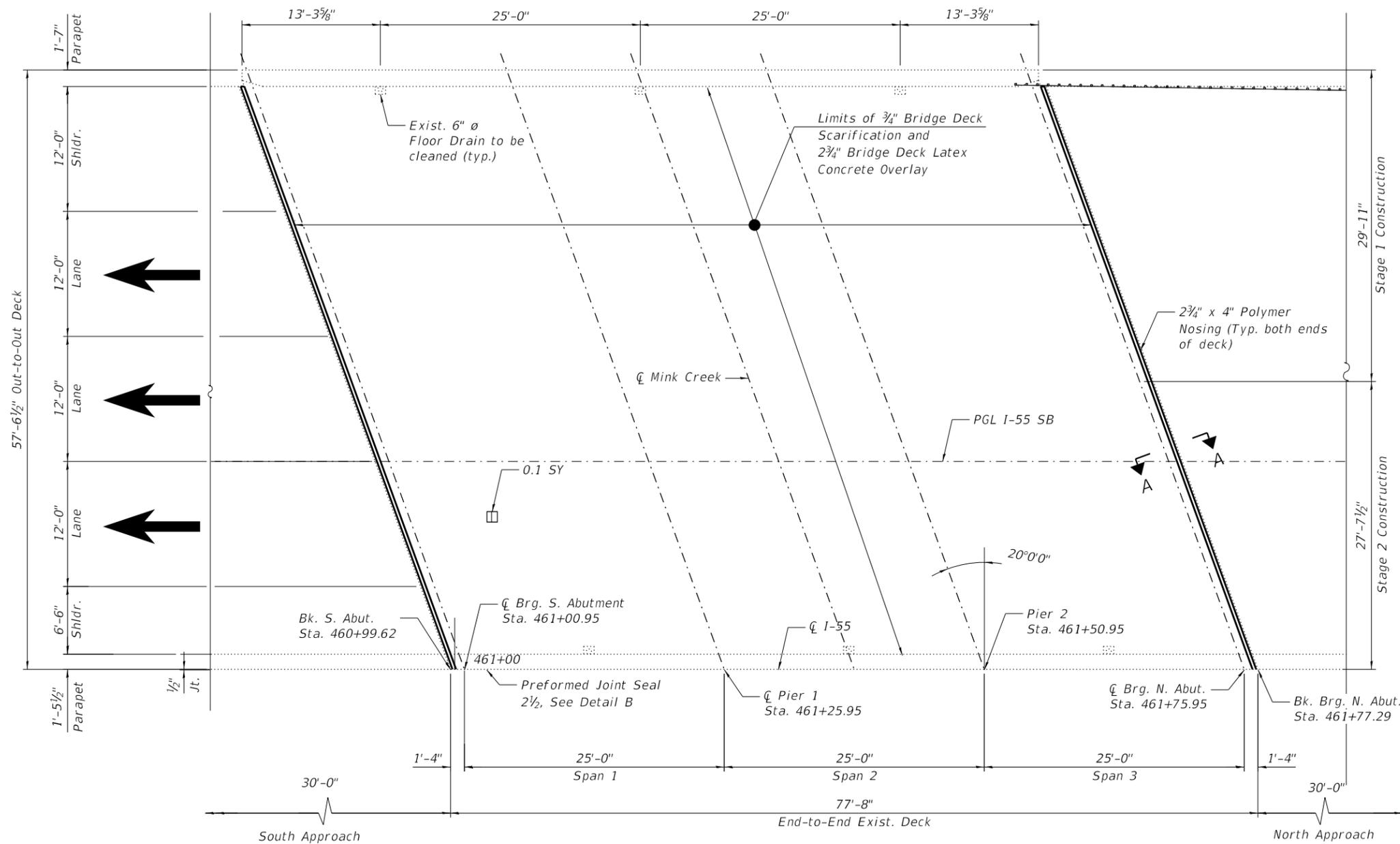
TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION
 S.N. 099-4616 I-55 SB OVER MINK CREEK (0.8 MILES N OF US 30)

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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			CONTRACT NO. 62N22	
ILLINOIS		FED. AID PROJECT		

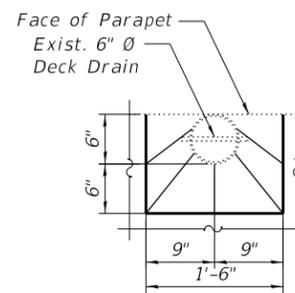
SHEET S7-05 OF S7-07 SHEETS

BILL OF MATERIAL

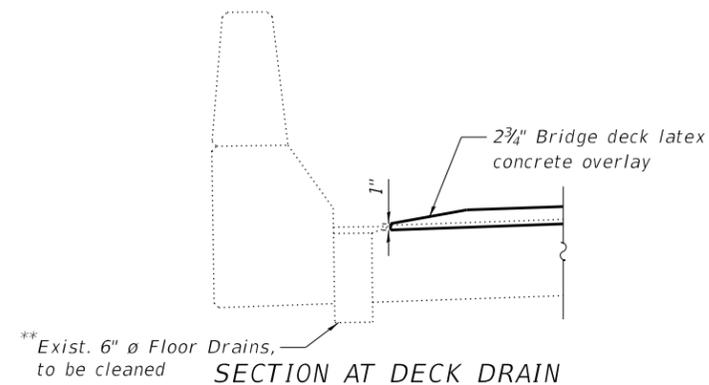
ITEM	UNIT	QUANTITY
Bridge Deck Grooving	SQ YD	448
Protective Coat	SQ YD	539
Preformed Joint Seal 2 1/2"	FOOT	78
Bridge Deck Latex Concrete Overlay, 2 3/4"	SQ YD	464
Bridge Deck Scarification, 3/4"	SQ YD	464
Polymer Concrete	CU FT	9



DECK PLAN



TOP PLAN



SECTION AT DECK DRAIN

NOTES:

1. Areas of deck repair shown are estimated. The Engineer shall show actual locations of deck repairs at the time of construction.
2. For bridge deck final cross section, see Sheet S7-04.
3. Perform bridge deck grooving for the bridge deck latex concrete overlay.
4. Clean Exist. Floor Drains, Cost Included with Bridge Deck Scarification, 3/4"

* Areas of Deck Slab Repair (Partial) are provided for information only and shall be included in the cost of Bridge Deck Latex Concrete Overlay, 2 1/4"

LEGEND

- * Deck Slab Repair (Partial)
- SY Square Yard

MODEL: Default
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

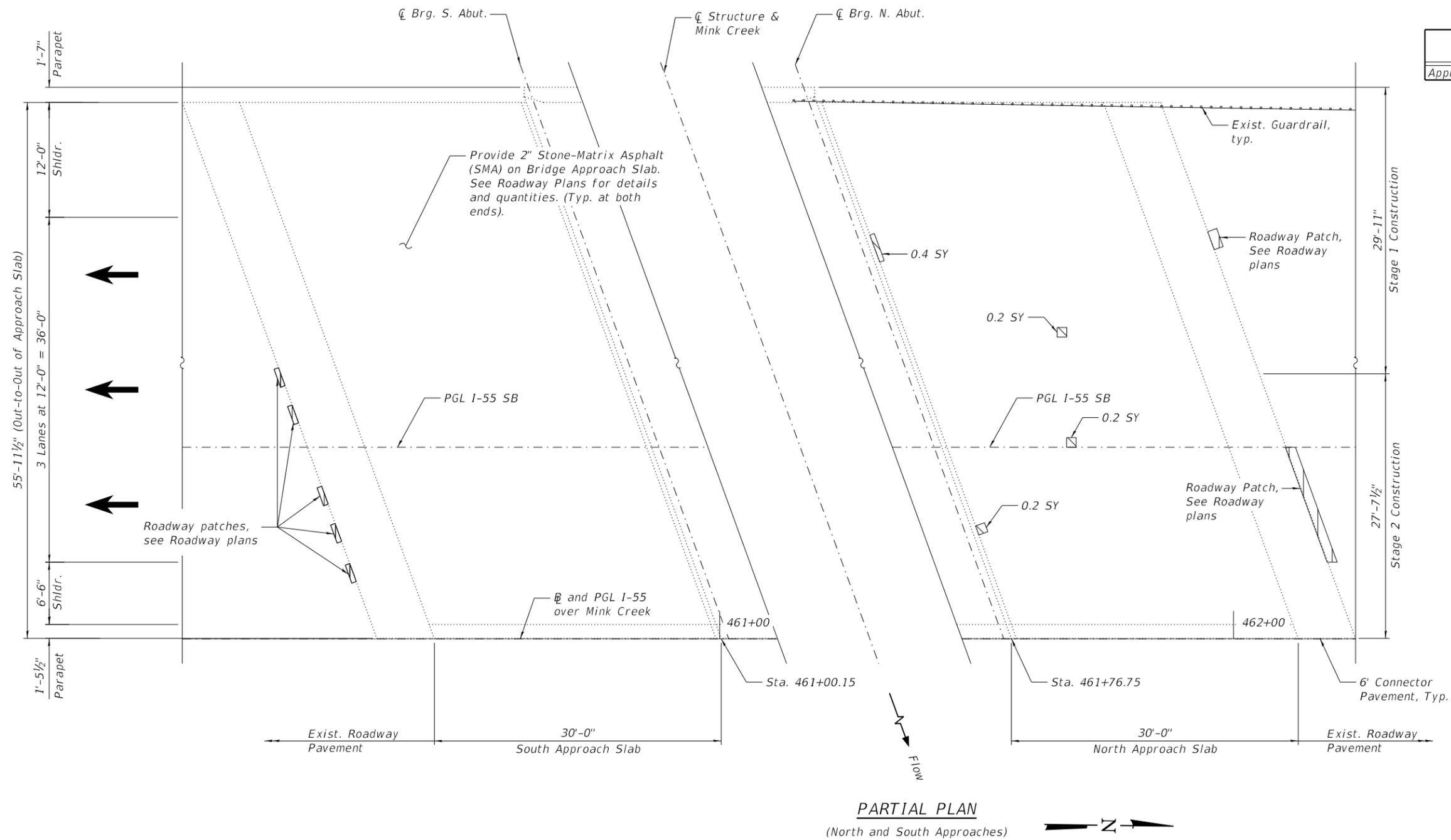
**BRIDGE DECK REPAIRS
S.N. 099-4616 I-55 SB OVER MINK CREEK (0.8 MILES N OF US 30)**

SHEET S7-06 OF S7-07 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	149
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Approach Slab Repair (Partial Depth)	Sq. Yd.	1



NOTE:

1. Areas of Approach Slab Repair (Partial Depth) shown are estimated. The Engineer shall show actual location of repairs at the time of construction.

LEGEND

 Approach Slab Repair (Partial Depth)

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PLOT DATE =	DATE - 12/2/2020	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

APPROACH SLAB REPAIRS
S.N. 099-4616 I-55 SB OVER MINK CREEK (0.8 MILES N OF US 30)

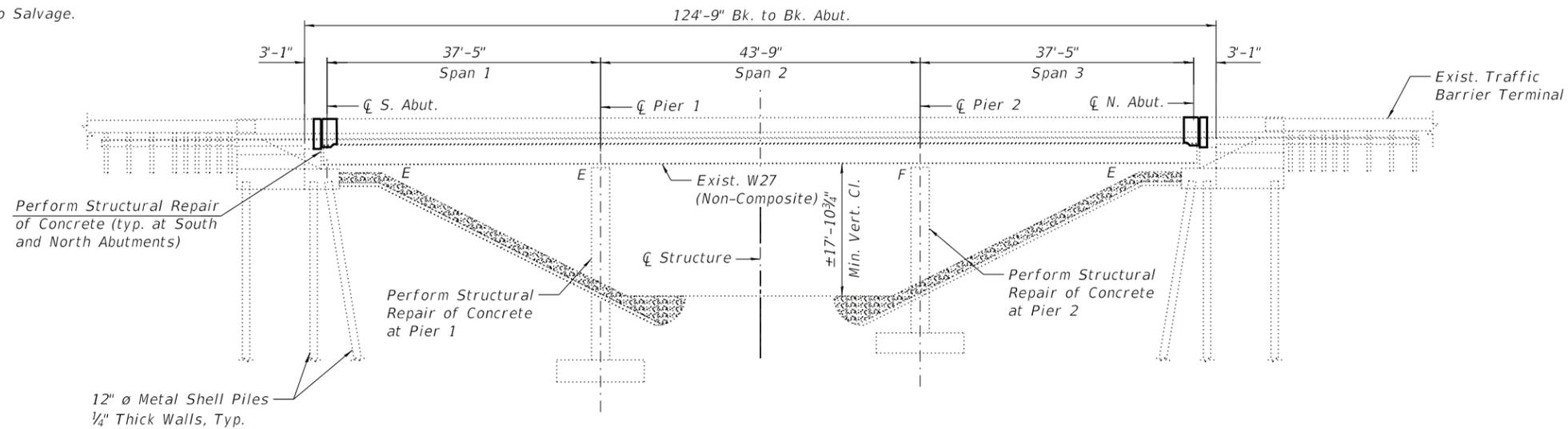
SHEET S7-07 OF S7-07 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	150
CONTRACT NO. 62N22				
ILLINOIS		FED. AID PROJECT		

Existing Structure: Structure No. 0990023 was originally constructed in 1956 as part of F.A. Route 34, Section 27-VB-1 and project FGI 187 (7) reconstructed/widened in 1977. Additional rehabilitation work was performed in 1994. The structure has a length of 122'-3" (back-to-back of abutments) and an out-to-out deck width of 54'-11". The continuous slab superstructure consists of three spans. The substructure consists of reinforced concrete piers and abutments on steel piles.

Traffic is to be maintained utilizing stage construction.

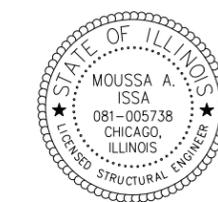
No Salvage.



ELEVATION

SCOPE OF WORK

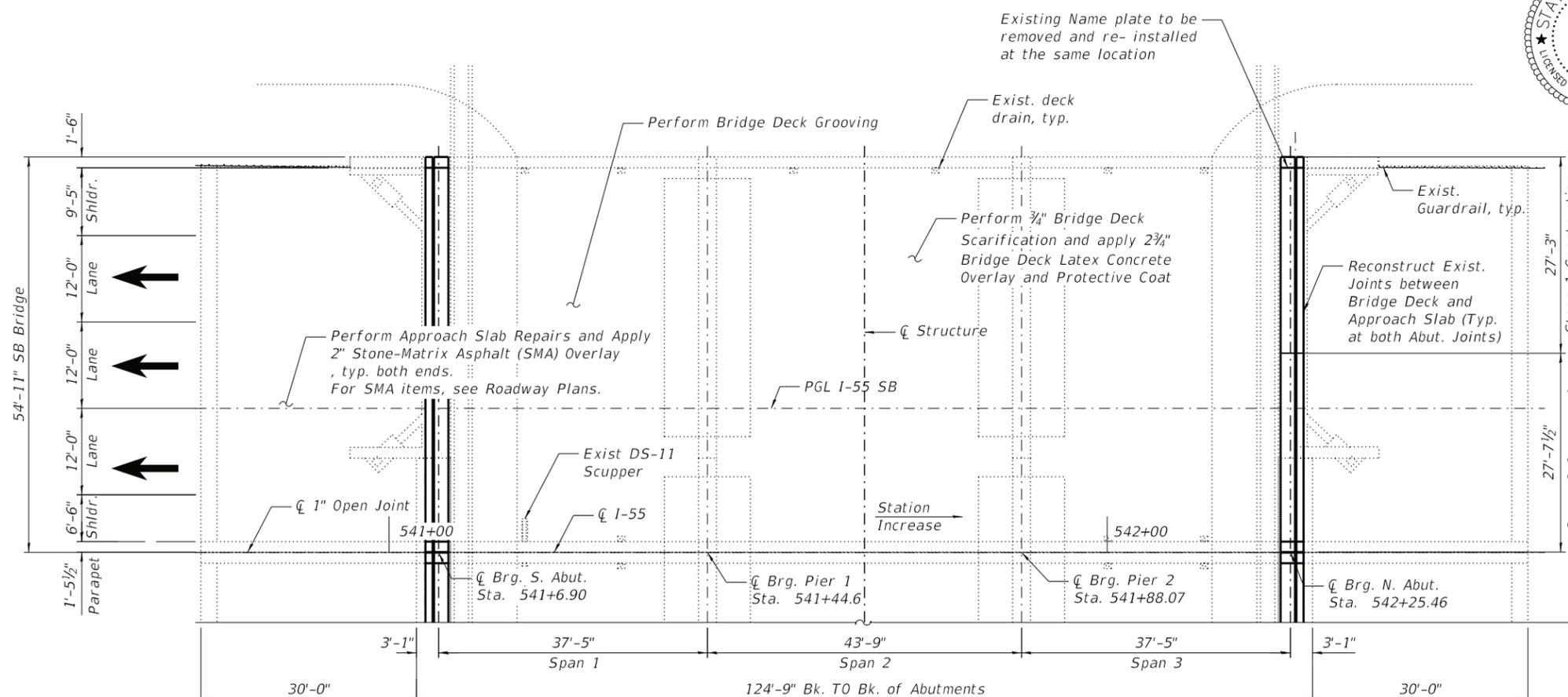
1. Scarify 3/4" from the bridge deck slab.
2. Perform Deck Slab Repairs and Approach Slab Repairs.
3. Reconstruct Expansion Joints at the North and South Abutments and install new preformed joint strip seals.
4. Apply a 2 3/4" Bridge Deck Latex Concrete Overlay on Bridge Deck and 2" Stone-Matrix Asphalt (SMA) Overlay on the Approach Slabs. For SMA items, see Roadway plans.
5. Perform Bridge Deck Grooving.
6. Replace 2 1/2" PJS between parapets; coordinate with SN 099-0022.
7. Apply protective coat to the top of reconstructed transverse joint areas and the top and inside faces of Parapets and top of Latex Overlay.
8. Perform structural concrete repairs for the Abutments, Pier 1 and Pier 2 as noted on the Plans.
9. Clean all deck drains.
10. Existing name plate to be removed, cleaned and re-installed at the same location. Cost included with Concrete Removal.



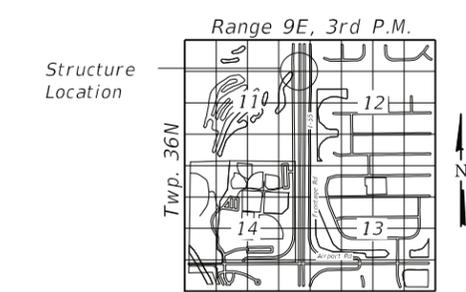
Signed Moussa A. Issa
 Dr. Moussa A. Issa, S.E. Il. Lic. No. 081-005738
 Expires 11-30-2022

Date December 03, 2020 For Sheets S8-01 Thru S8-18 (Total of 18 Sheets)

Existing Name plate to be removed and re-installed at the same location



PLAN



LOCATION SKETCH

GENERAL PLAN AND ELEVATION
 I-55 OVER MS RR (ABANDONED)
 F.A.I. ROUTE 55-SEC. 2006-032
 WILL COUNTY
 STAION 710+34.86
 S.N. 099-0023

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

S.N. 099-0023 I-55 SB OVER MATERIAL SERVICE RR (ABANDONED)

SHEET S8-01 OF S8-18 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	151
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

GENERAL NOTES

1. Reinforcement bars designated (E) shall be epoxy coated.
2. Prior to pouring the new concrete deck for expansion joints reconstruction and deck slab repairs, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.
3. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
4. Joint openings shall be adjusted according to Article 520.04 of the Standard Specifications when the deck is poured at an ambient temperature other than 50° F.
5. Bars noted thus, 3x2-#5, indicates 3 lines of #5 bars with 2 lengths of bars per line.
6. All exposed concrete edges shall have a 3/4"x45° chamfer, except where shown otherwise.
7. The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.
8. Concrete Sealer shall be applied to the designated areas of the abutments.
9. The Contractor is responsible to protect the existing conduit embedded in the parapet during concrete removal and construction. Any damage to the existing conduit shall be repaired by the Contractor at no additional cost to the Department.
10. Protective Coat shall be applied to the top and inside face of parapets, reconstructed transverse Expansion Joints and to the surface of the new overlay.
11. Coordinate with the Contractor for Contract No. 62K51 prior to installation of the preformed joint seal 2 1/2". All work on Parapets must be completed for Contract No. 62K51 and this Contract prior to installation of the longitudinal joint.

INDEX OF SHEETS

- S8-01 General Plan & Elevation
- S8-02 Structure Notes, Index of Sheets & Total Bill of Material
- S8-03 Stage Construction (Sheet 1 of 2)
- S8-04 Stage Construction (Sheet 2 of 2)
- S8-05 Temporary Concrete Barrier For Stage Construction
- S8-06 Bridge Deck Repairs
- S8-07 Approach Slab Repairs
- S8-08 Parapet Repairs
- S8-09 S. Abut. Joint Removal and Reconstruction (Sheet 1 of 2)
- S8-10 S. Abut. Joint Removal and Reconstruction (Sheet 2 of 2)
- S8-11 N. Abut. Joint Removal and Reconstruction (Sheet 1 of 2)
- S8-12 N. Abut. Joint Removal and Reconstruction (Sheet 2 of 2)
- S8-13 Preformed Joint Strip Seal
- S8-14 South Abutment Repairs
- S8-15 Pier 1 Repairs
- S8-16 Pier 2 Repairs
- S8-17 North Abutment Repairs
- S8-18 Bar Splicer Assembly Details

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Concrete Removal	CU YD	14.8	0	14.8
Concrete Superstructure	CU YD	16.8	0	16.8
Bridge Deck Grooving	SQ YD	665	0	665
Protective Coat	SQ YD	811	0	811
Reinforcement Bars, Epoxy Coated	POUND	2,740	0	2,740
Bar Splicers	EACH	26	0	26
Preformed Joint Seal 2 1/2"	FOOT	185	0	185
Preformed Joint Strip Seal	FOOT	110	0	110
Concrete Sealer	SQ FT	0	254	254
Approach Slab Repair (Partial Depth)	SQ YD	1	0	1
Bridge Deck Latex Concrete Overlay, 2 3/4 Inches	SQ YD	669	0	669
Bridge Deck Scarification 3/4"	SQ YD	669	0	669
Structural Repair Of Concrete (Depth Equal To Or Less Than 5 Inches)	SQ FT	7	23	30

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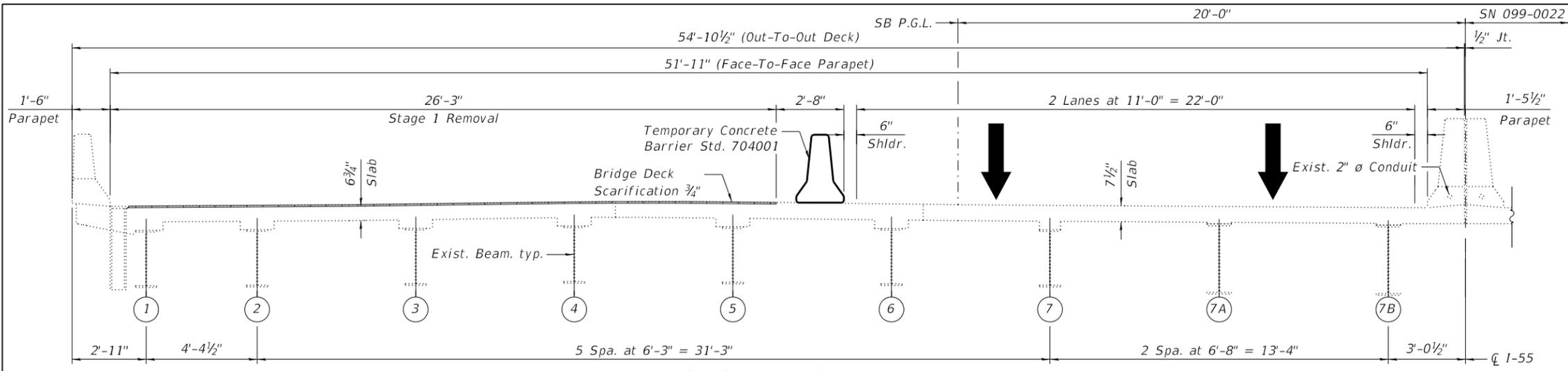
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PLOT DATE =	DATE - 12/2/2020	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**STRUCTURE NOTES, INDEX OF SHEETS & TOTAL BILL OF MATERIAL
S.N. 099-0023 I-55 SB OVER MATERIAL SERVICE RR (ABANDONED)**

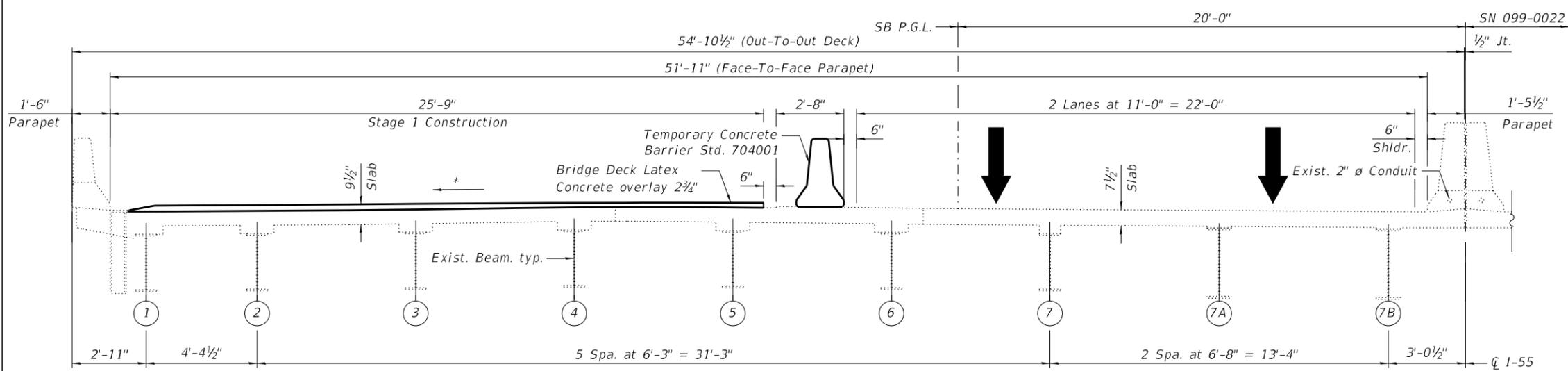
SHEET S8-02 OF S8-18 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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			CONTRACT NO. 62N22	
ILLINOIS		FED. AID PROJECT		



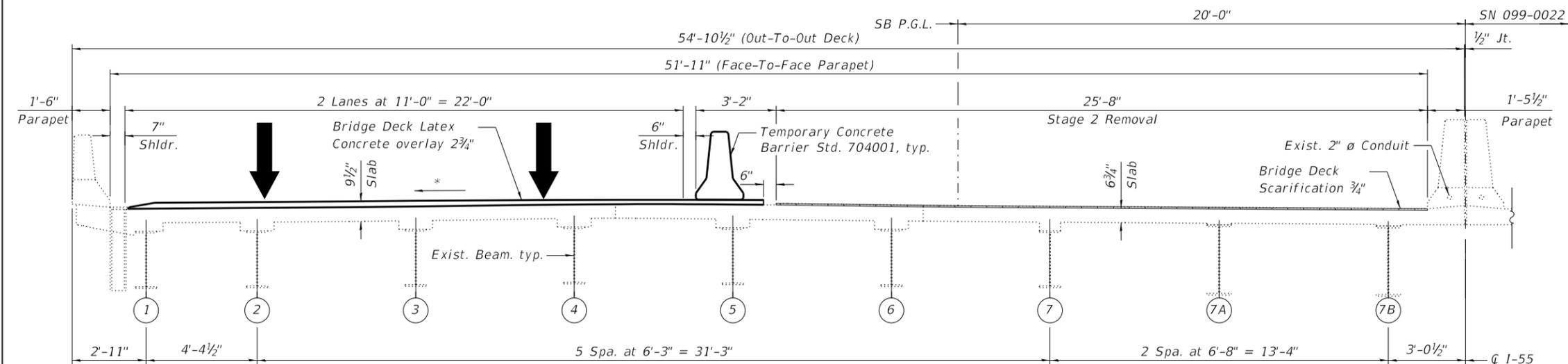
STAGE 1 REMOVAL
(Looking North)

- STAGE 1 REMOVAL**
1. Install temporary concrete barrier as shown to locate traffic on the east side of the existing structure.
 2. Scarify 3/4" from the top of deck slab.
 3. Remove portions of bridge deck/approach slab adjacent to abutment joints, as shown in the plans.



STAGE 1 CONSTRUCTION
(Looking North)

- STAGE 1 CONSTRUCTION**
1. Perform full- and partial-depth deck slab repairs, and approach slab repairs, at locations as shown in the plans.
 2. Reconstruct expansion joints at north and south abutments and install new strip seals.
 3. Perform structural repair of concrete for the abutments and pier.
 4. Apply 2 3/4" bridge deck latex concrete overlay to bridge deck slab.
 5. Perform bridge deck grooving for the 2 3/4" bridge deck latex concrete overlay.
 6. Apply 2" Stone-Matrix Asphalt (SMA) Overlay to approach slabs, see Roadway plans.
 7. Apply protective coat to the top of reconstructed transverse joint areas and the top and inside faces of Parapets and top of Latex Overlay.



STAGE 2 REMOVAL
(Looking North)

- STAGE 2 REMOVAL**
1. Install temporary concrete barrier as shown to locate traffic on east and west side of the existing structure.
 2. Scarify 3/4" from the top of deck slab.
 3. Remove portions of bridge deck/approach slab adjacent to abutment joints, as shown in the plans.

* Match existing deck profile

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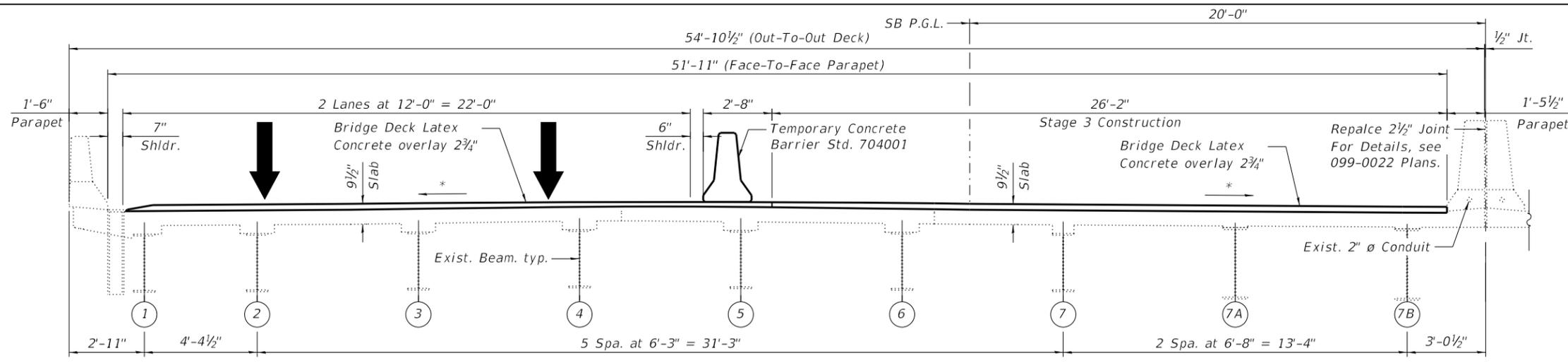


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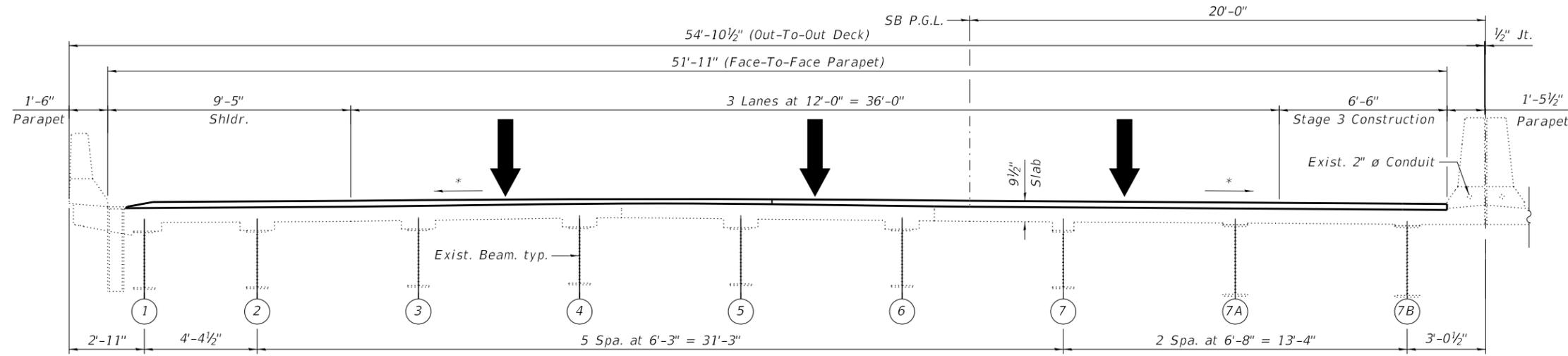
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGE CONSTRUCTION (SHEET 1 OF 2)
S.N. 099-0023 I-55 SB OVER MATERIAL SERVICE RR (ABANDONED)
SHEET S8-03 OF S8-18 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	153
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				



STAGE 2 CONSTRUCTION
(Looking North)



FINAL CROSS-SECTION
(Looking North)

STAGE 2 CONSTRUCTION

1. Perform full- and partial-depth deck slab repairs, and approach slab repairs, at locations as shown in the plans.
2. Reconstruct expansion joints at north and south abutments and install new strip seals.
3. Perform structural repair of concrete for the abutments and pier.
4. Apply 2 3/4" bridge deck latex concrete overlay to bridge deck slab.
5. Perform bridge deck grooving for the 2 3/4" bridge deck latex concrete overlay.
6. Apply 2" Stone-Matrix Asphalt (SMA) Overlay to approach slabs, see Roadway plans.
7. Replace 2 1/2" PJS between parapets; coordinate with SN 099-0022.
8. Apply protective coat to the top of reconstructed transverse joint areas and the top and inside faces of Parapets and top of Latex Overlay.
9. Apply permanent pavement markings on top of deck and approach slabs. For Pavement Markings, See Roadway Plans.

* Match existing deck surface profile

MODEL: Default
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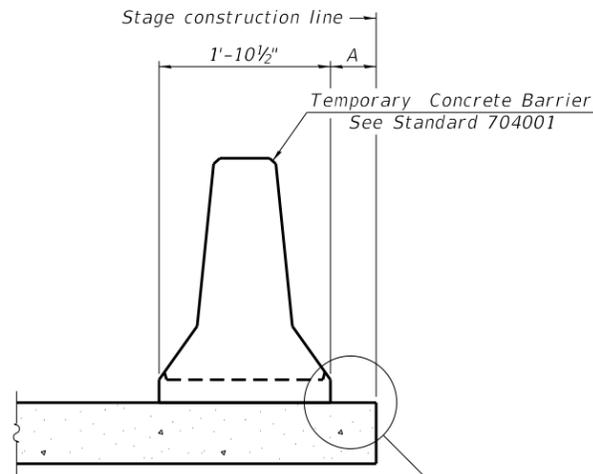
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PLOT DATE =	DATE - 12/4/2020	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGE CONSTRUCTION (SHEET 2 OF 2)
S.N. 099-0023 I-55 SB OVER MATERIAL SERVICE RR (ABANDONED)

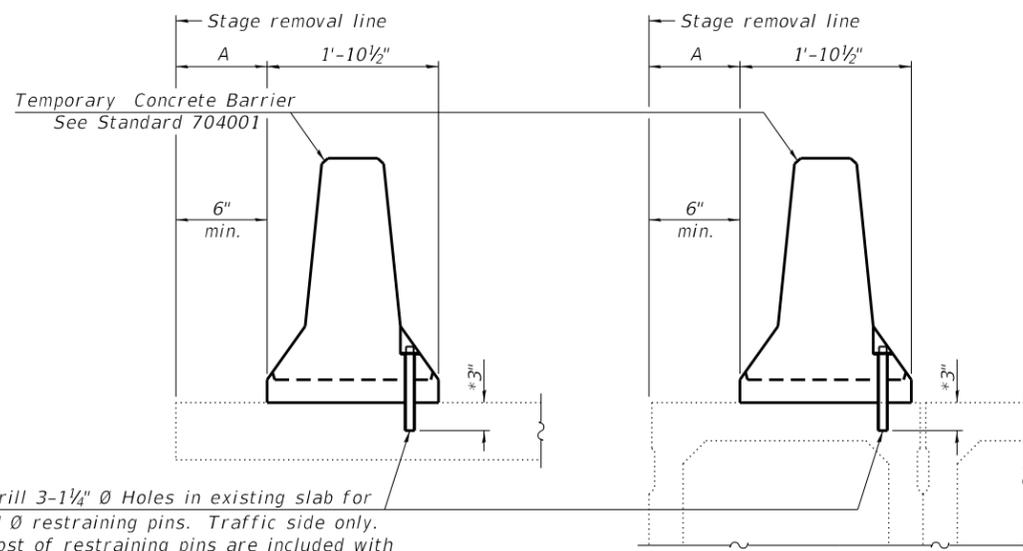
SHEET S8-04 OF S8-18 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	154
			CONTRACT NO. 62N22	
		ILLINOIS	FED. AID PROJECT	



When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM



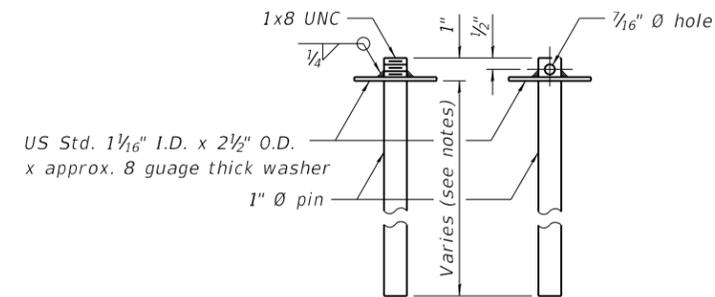
Drill 3-1/4" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

EXISTING SLAB

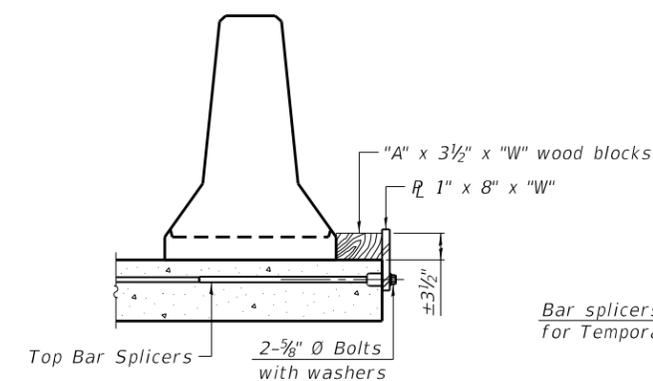
* When hot-mix asphalt wearing surface is present, embedment shall be 3" plus the wearing surface depth.

EXISTING DECK BEAM

SECTIONS THRU SLAB OR DECK BEAM

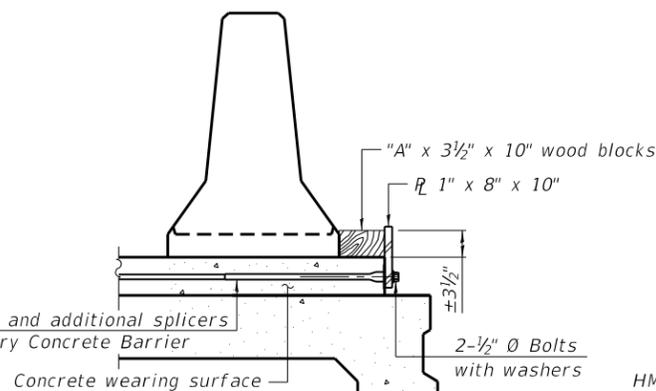


RESTRAINING PIN

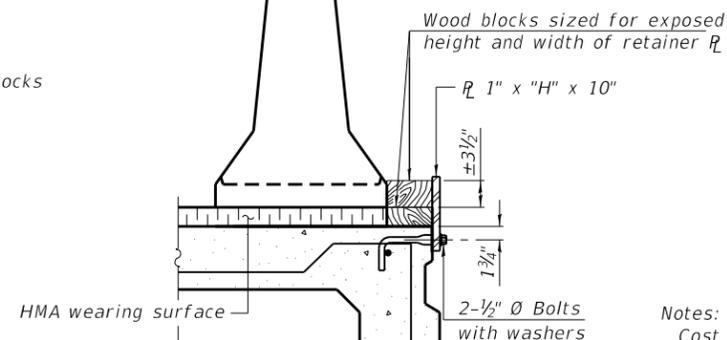


DETAIL I

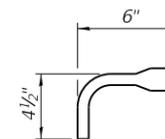
Bar splicers and additional splicers for Temporary Concrete Barrier



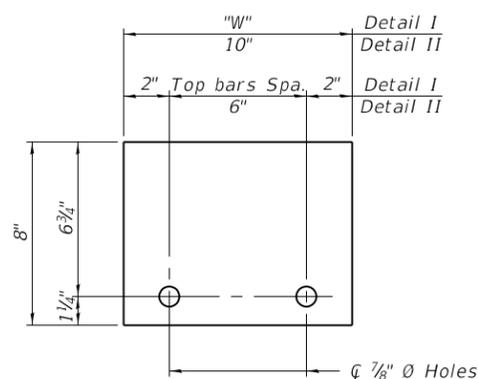
DETAIL II



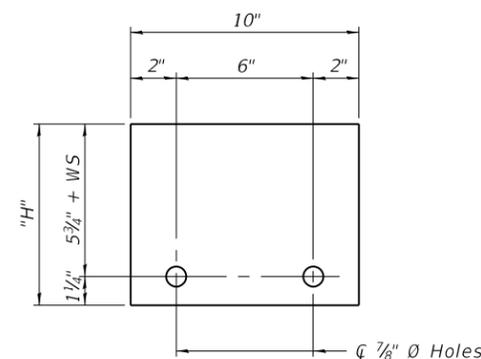
DETAIL III



BAR SPLICER FOR #4 BAR - DETAIL III



STEEL RETAINER 1" x 8" x "W"
(Detail I and II)



STEEL RETAINER 1" x "H" x 10"
(Detail III)

Notes:
 Cost of retainer assembly is included with Temporary Concrete Barrier.
 A retainer assembly shall be located at the approximate center of each temporary concrete barrier.
 The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.
 When the 'A' dimension is less than 1 1/2', the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6' to accommodate the shear key clamping device.

- Detail I - Installation for a new bridge deck or bridge slab.
- Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.
- Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

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R-27 2-17-2017



USER NAME =	DESIGNED - MAA,AMS	REVISED -
PLOT SCALE =	CHECKED - MI,RTB	REVISED -
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

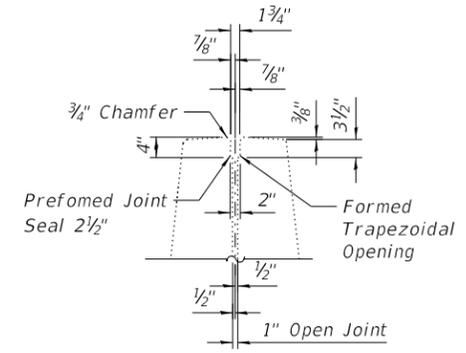
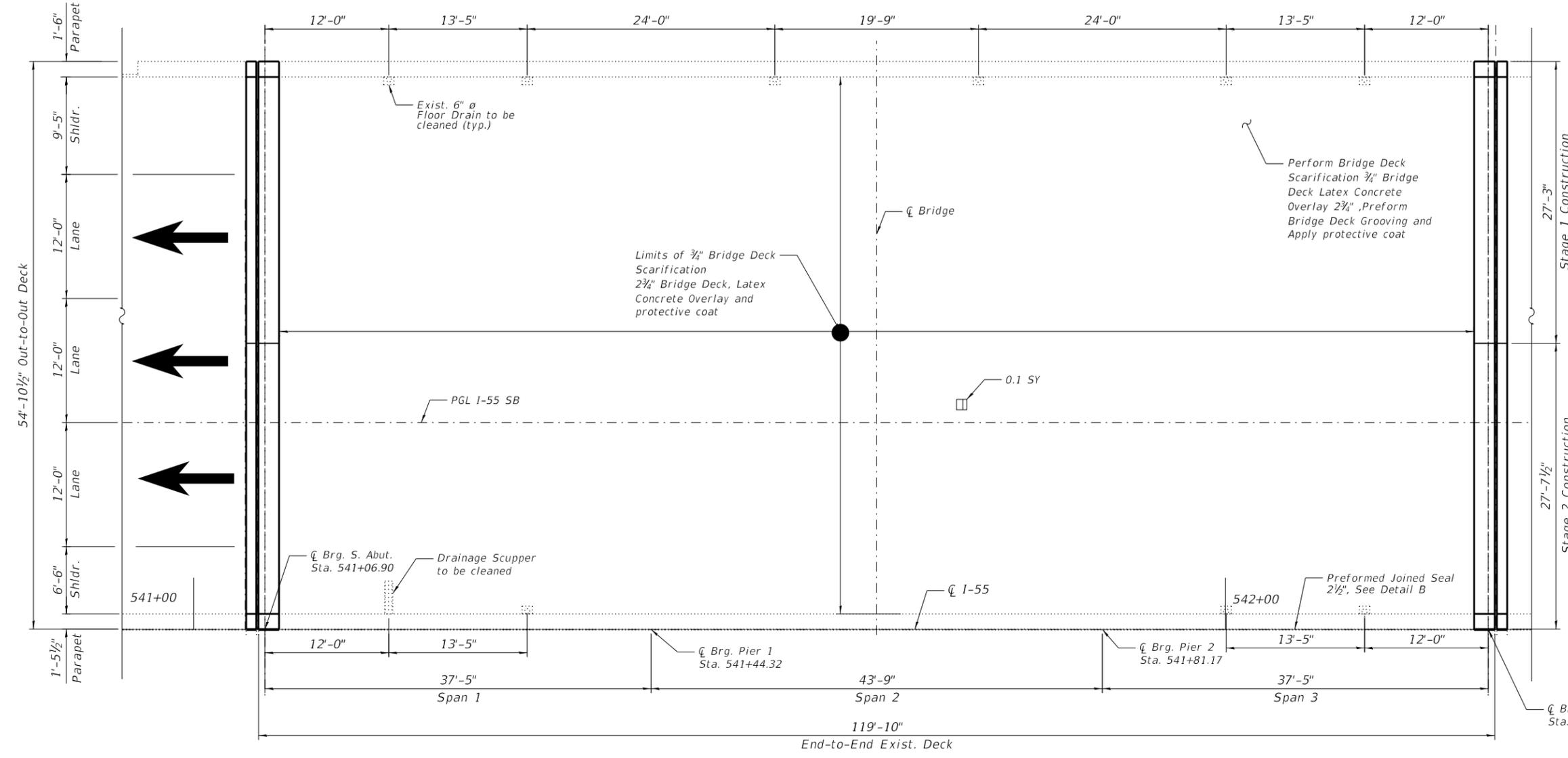
TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION
 S.N. 099-0023 I-55 SB OVER MATERIAL SERVICE RR (ABANDONED)

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	155
			CONTRACT NO. 62N22	
ILLINOIS		FED. AID PROJECT		

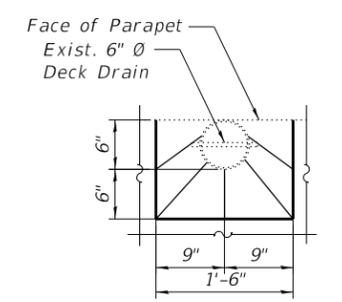
SHEET S8-05 OF S8-18 SHEETS

BILL OF MATERIAL

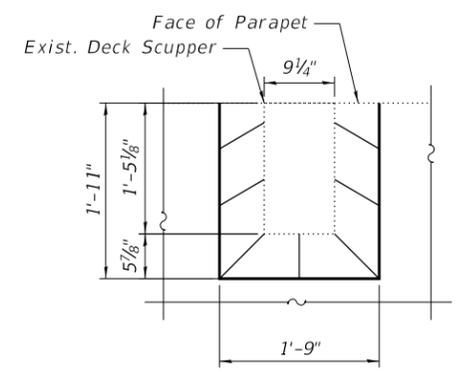
ITEM	UNIT	QUANTITY
Bridge Deck Grooving	SQ YD	665
Protective Coat	SQ YD	669
Preformed Joint Seal 2 1/2"	FOOT	185
Bridge Deck Latex Concrete Overlay, 2 3/4 Inches	SQ YD	669
Bridge Deck Scarification 3/4"	SQ YD	669



DETAIL B

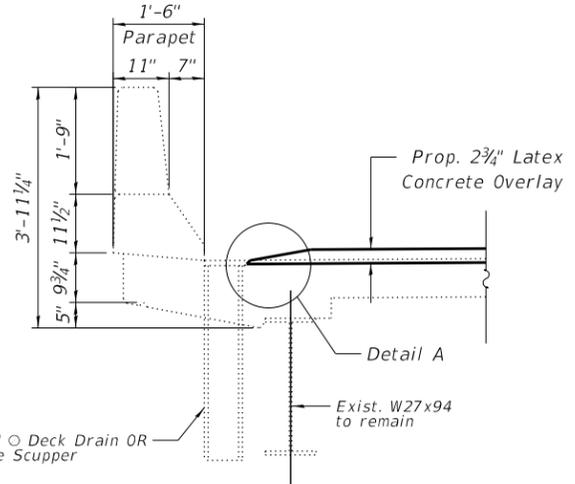


TOP PLAN AT FLOOR DRAIN

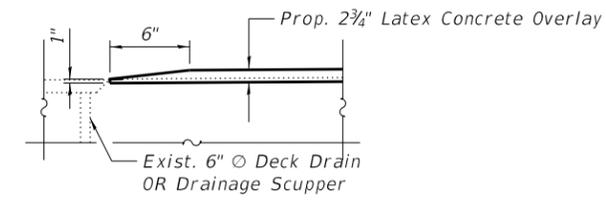


TOP PLAN AT DRAINAGE SCUPPER

DECK PLAN



SECTION AT DECK DRAIN/ DRAINAGE SCUPPER



DETAIL A

LEGEND

- * Deck Slab Repair (Partial)
- Square Yard

NOTES:

- Areas of deck repair shown are estimated. The Engineer shall show actual locations of deck repairs at the time of construction.
- For North and South transverse joint removal and reconstruction, see Sheets S8-09 thru S8-12.
- Perform bridge deck grooving for the bridge deck latex concrete overlay and the roadway portions of the reconstructed transverse joints.
- Any reinforcement bars that are damaged during concrete removal operations shall be repaired or replaced using an approved bar splicer or anchorage system. Cost incidental to Concrete Removal.
- The Contractor shall exercise extreme caution during Concrete Removal to avoid damaging the steel beams and diaphragms to remain. Any damage to the existing steel beams and/or diaphragms to remain caused by the Contractor in the performance of his/her work shall be repaired by the Contractor, to the satisfaction of the Engineer, at no cost to the Department.
- Clean Deck drains and Scupper, Cost included with Bridge Deck Scarification, 3/4"

* Areas of Deck Slab Repair (Partial) are provided for information only and shall be included in the cost of Bridge Deck Latex Concrete Overlay, 2 3/4"

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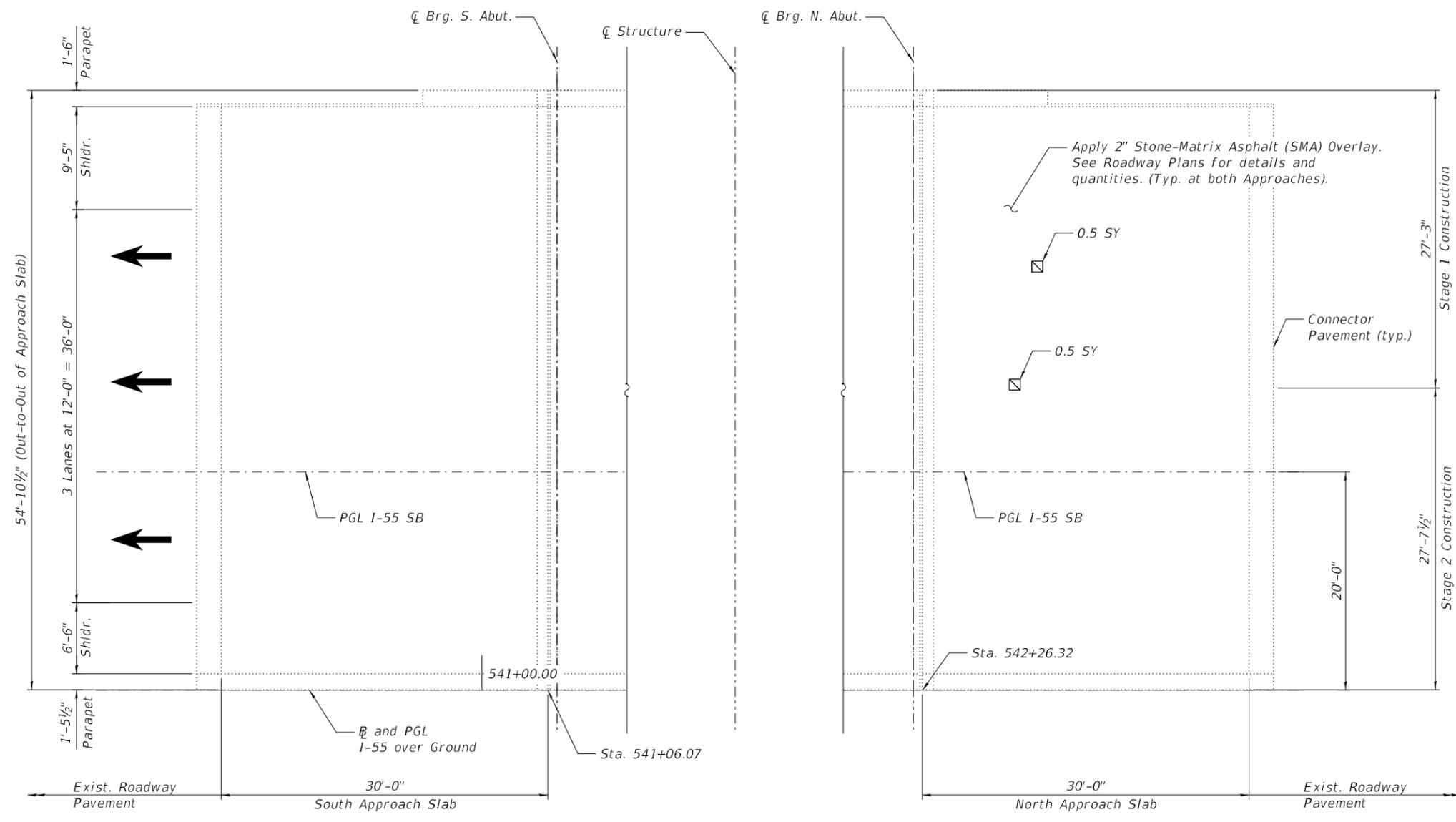
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BRIDGE DECK REPAIRS
S.N. 099-0023 I-55 SB OVER MATERIAL SERVICE RR (ABANDONED)**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	156
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

SHEET S8-06 OF S8-18 SHEETS

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PARTIAL PLAN
 (North and South Approaches)

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Approach Slab Repair (Partial)	Sq. Yd.	1

NOTE:

1. Areas of Approach Slab Repair (Partial Depth) shown are estimated. The Engineer shall show actual location of repairs at the time of construction.

LEGEND

- Approach Slab Repair (Partial Depth)
- Square Yard



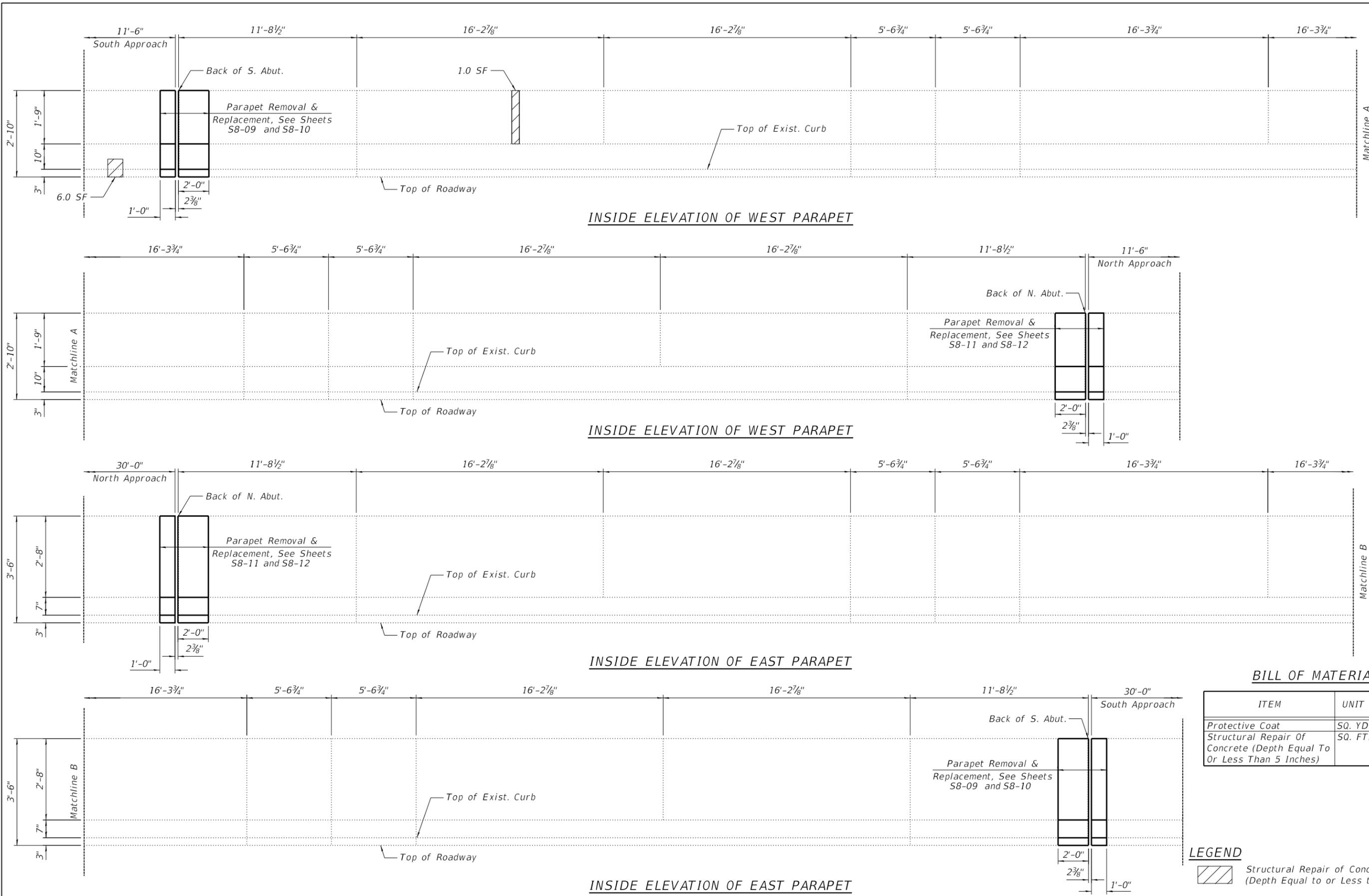
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PLOT DATE =	DATE - 12/4/2020	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

APPROACH SLAB REPAIRS
S.N. 099-0023 I-55 SB OVER MATERIAL SERVICE RR (ABANDONED)

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	157
			CONTRACT NO. 62N22	
		ILLINOIS	FED. AID PROJECT	

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BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Protective Coat	SQ. YD.	102
Structural Repair Of Concrete (Depth Equal To Or Less Than 5 Inches)	SQ. FT.	7

LEGEND
 Structural Repair of Concrete (Depth Equal to or Less than 5 inches)
 SF Square Foot



USER NAME =	DESIGNED - MAA, CP	REVISED -
PLOT SCALE =	CHECKED - MI, RTB	REVISED -
PLOT DATE =	DRAWN - CP	REVISED -
	DATE - 12/2/2020	REVISED -

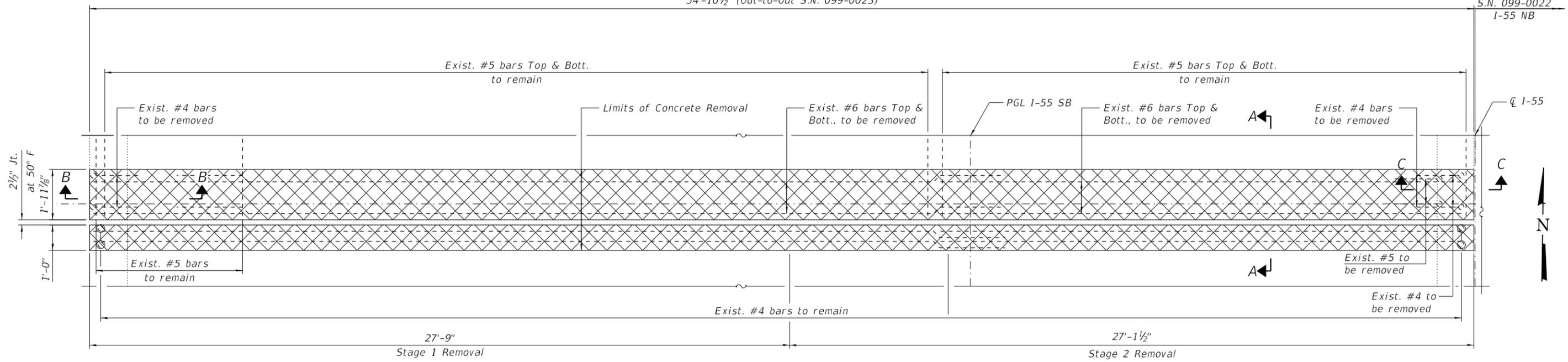
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

PARAPET REPAIRS
S.N. 099-0023 I-55 SB OVER MATERIAL SERVICE RR (ABANDONED)
 SHEET S8-08 OF S8-18 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	158
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

54'-10 1/2" (Out-to-Out S.N. 099-0023)

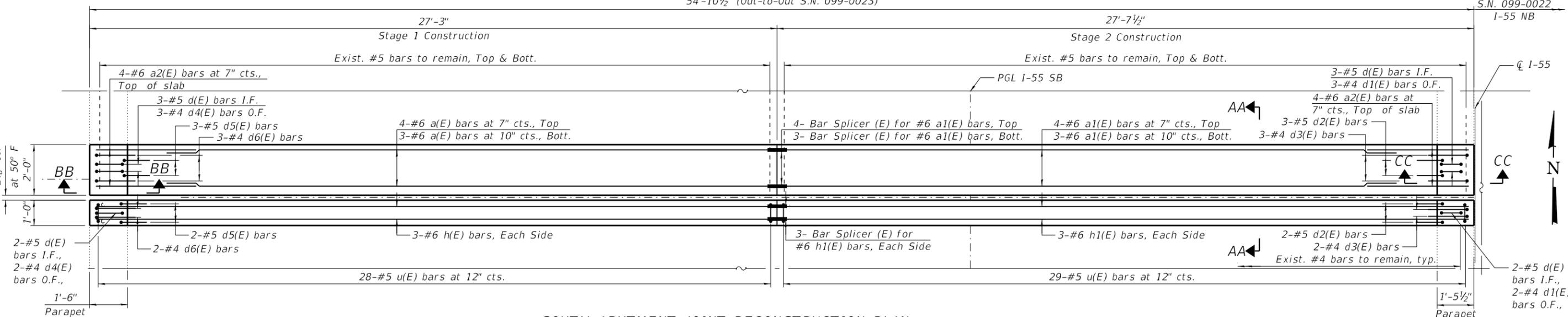
S.N. 099-0022
I-55 NB



SOUTH ABUTMENT JOINT REMOVAL PLAN

54'-10 1/2" (Out-to-Out S.N. 099-0023)

S.N. 099-0022
I-55 NB



SOUTH ABUTMENT JOINT RECONSTRUCTION PLAN

NOTE:

- For more notes and Sections A-A, B-B, C-C, AA-AA, BB-BB, CC-CC, bar diagrams and Bill of Material, see sheet S8-10

LEGEND:

- I.F. Inside Face
- O.F. Outside Face



MODEL: Default
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

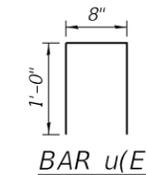
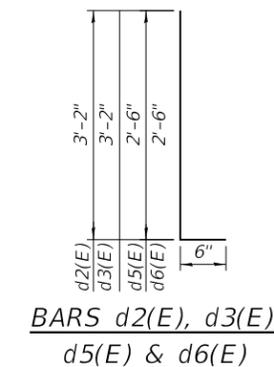
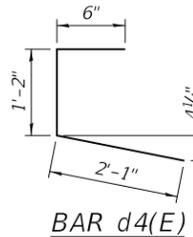
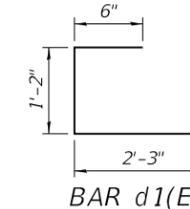
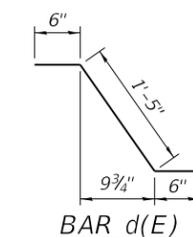
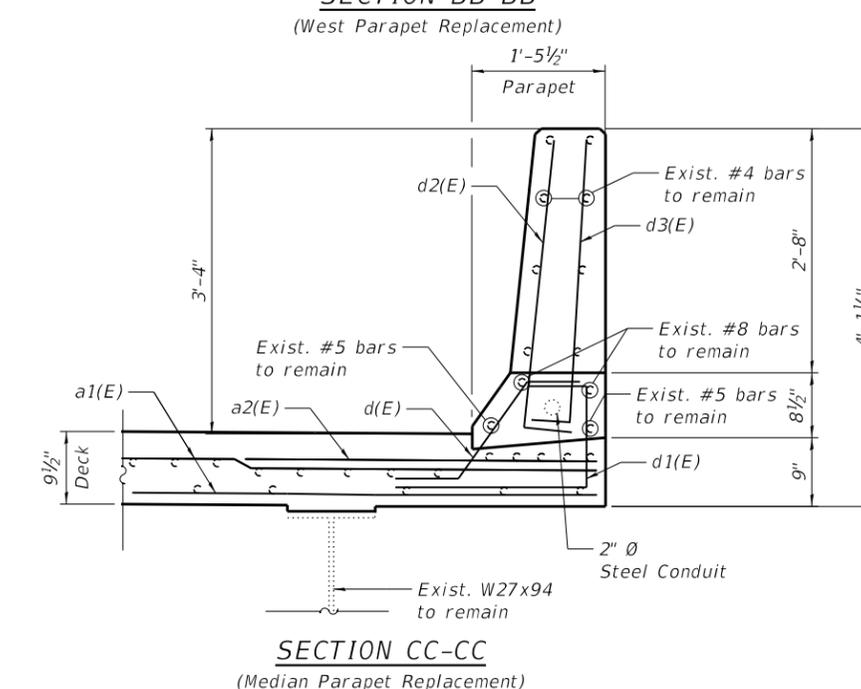
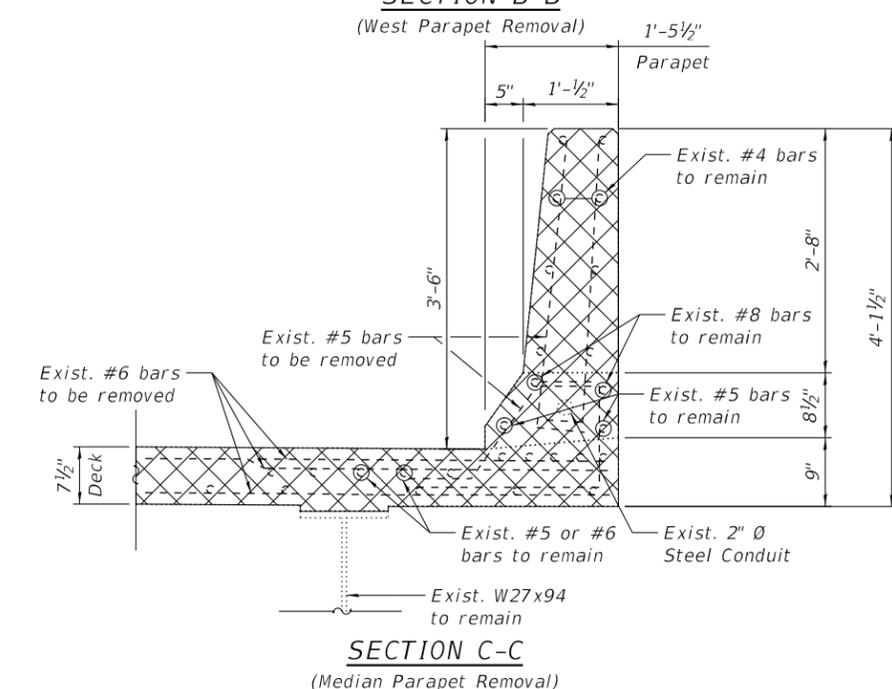
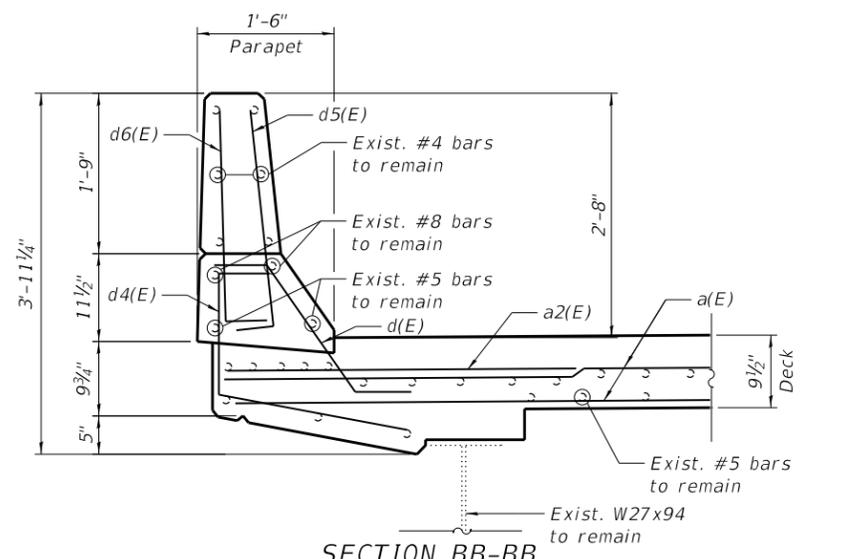
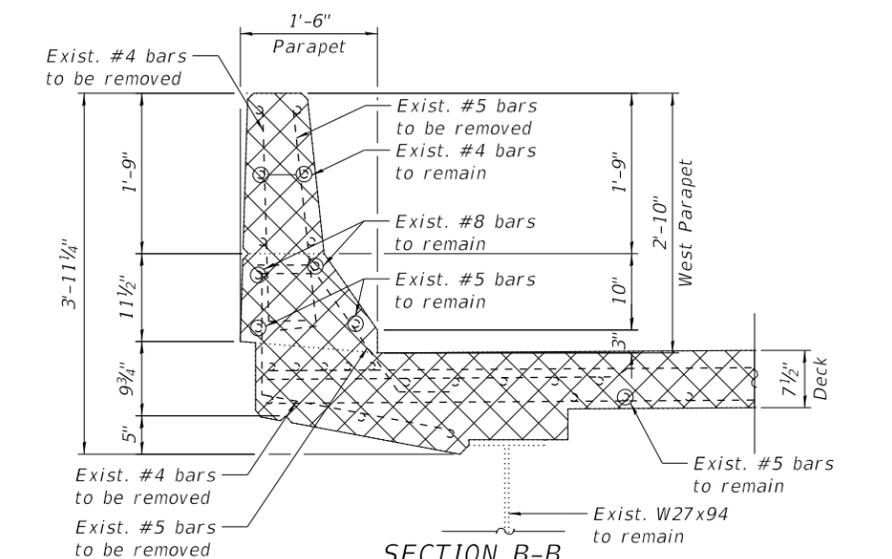
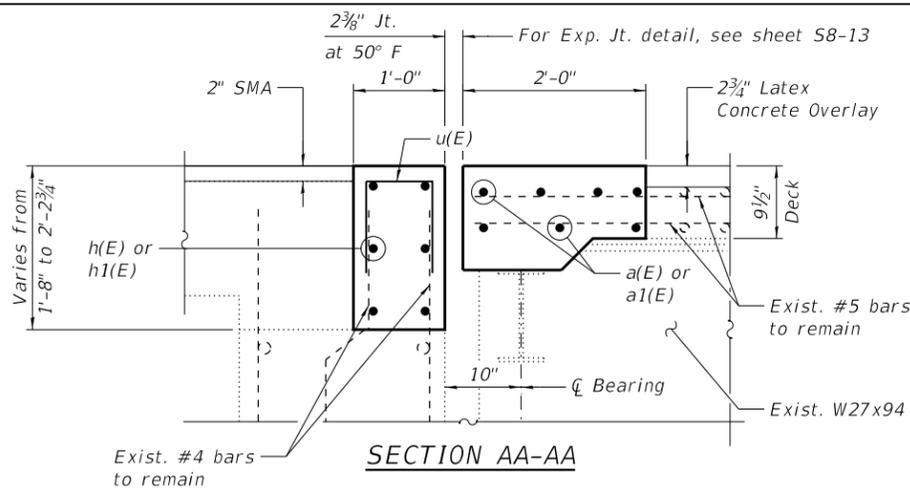
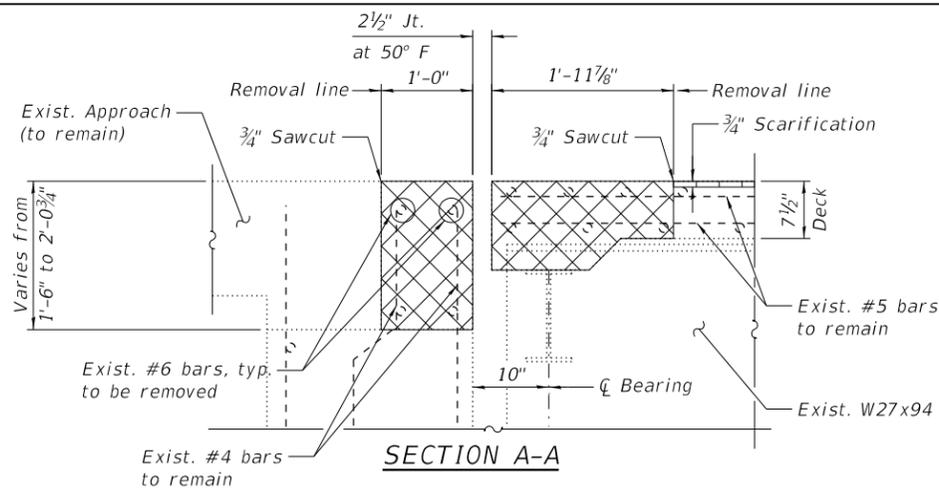
**S. ABUT. JOINT REMOVAL AND RECONSTRUCTION (SHEET 1 OF 2)
S.N. 099-0023 I-55 SB OVER MATERIAL SERVICE RR (ABANDONED)**

SHEET S8-09 OF S8-18 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	159
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	7	#6	26'-6"	—
a1(E)	7	#6	26'-11"	—
a2(E)	8	#6	4'-0"	—
d(E)	10	#5	2'-5"	┘
d1(E)	5	#4	3'-11"	┘
d2(E)	5	#5	3'-8"	┘
d3(E)	5	#4	3'-8"	┘
d4(E)	5	#4	3'-9"	┘
d5(E)	5	#5	3'-0"	┘
d6(E)	5	#4	3'-0"	┘
h(E)	6	#6	26'-11"	—
h1(E)	6	#6	27'-4"	—
u(E)	57	#5	2'-8"	┘
Concrete Removal			CU YD	7.4
Concrete Superstructure			CU YD	8.4
Protective Coat			SQ YD	20
Reinforcement Bars, Epoxy Coated			POUND	1,370



- NOTES:**
- For bar splicer assembly details, see Sheet S8-18.
 - Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

LEGEND

	Concrete Removal
--	------------------

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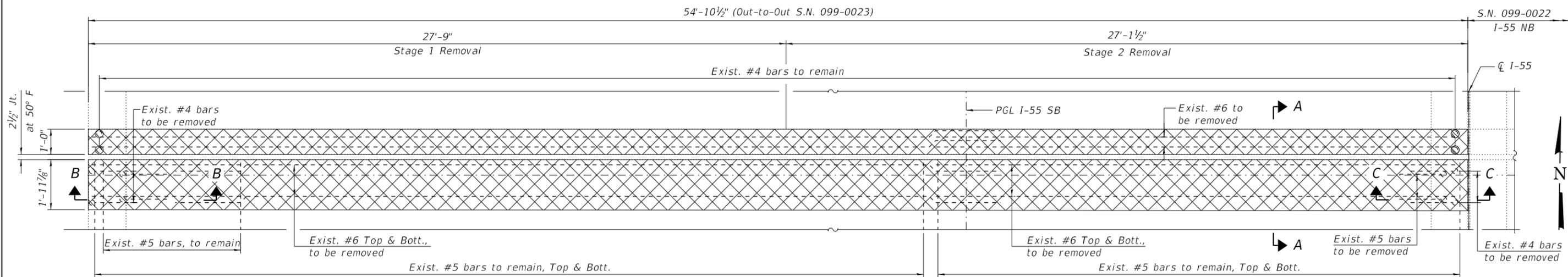


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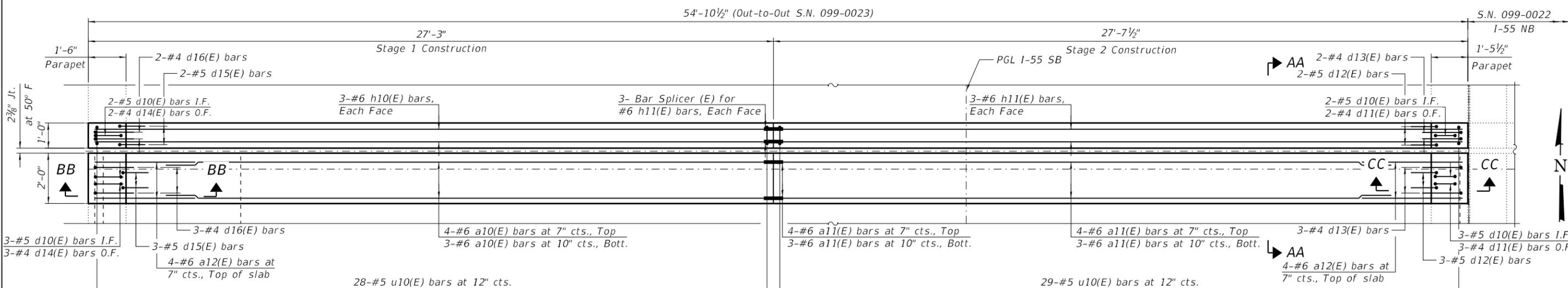
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

S. ABUT. JOINT REMOVAL AND RECONSTRUCTION (SHEET 2 OF 2)
S.N. 099-0023 I-55 SB OVER MATERIAL SERVICE RR (ABANDONED)

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	160
			CONTRACT NO. 62N22	
ILLINOIS		FED. AID PROJECT		



NORTH ABUTMENT JOINT REMOVAL PLAN



NORTH ABUTMENT JOINT RECONSTRUCTION PLAN

NOTES:

1. For bar splicer assembly details, see Sheet S8-18.
2. Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

LEGEND:

- I.F. Inside Face
- O.F. Outside Face
- Concrete Removal

MODEL: Default
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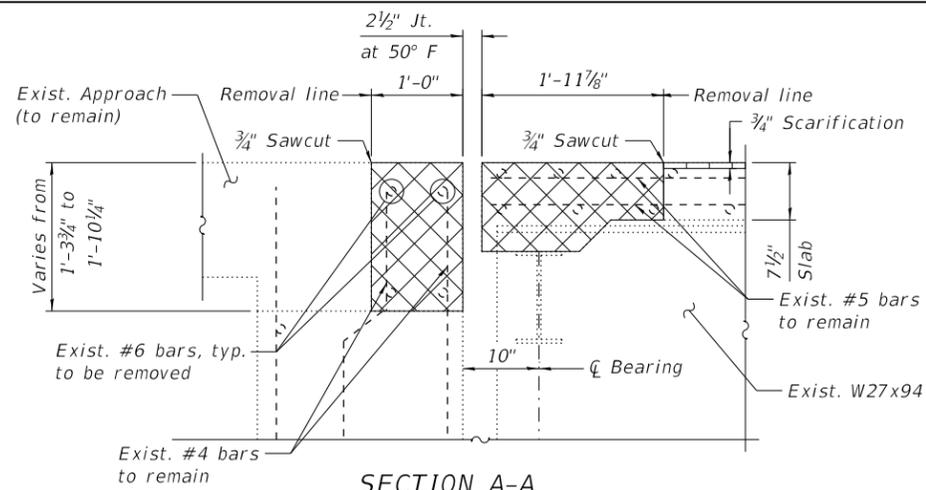
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**N. ABUT. JOINT REMOVAL AND RECONSTRUCTION (SHEET 1 OF 2)
S.N. 099-0023 I-55 SB OVER MATERIAL SERVICE RR (ABANDONED)**

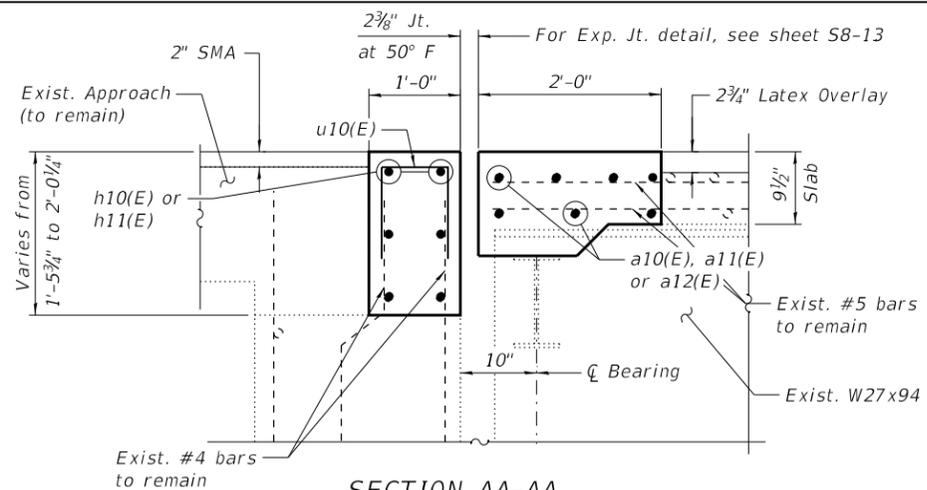
SHEET S8-11 OF S8-18 SHEETS

F.A.I. RTE. I-55	SECTION 2020-253-BR&PP	COUNTY WILL	TOTAL SHEETS 178	SHEET NO. 161
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

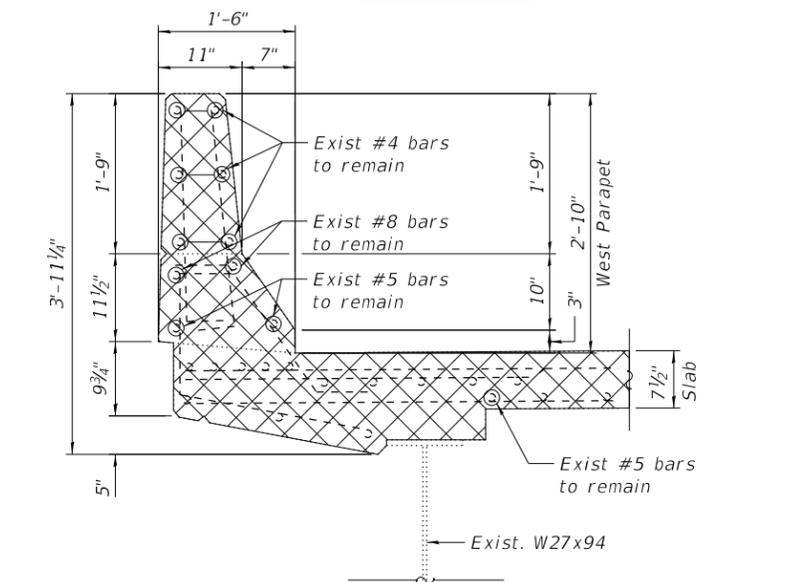
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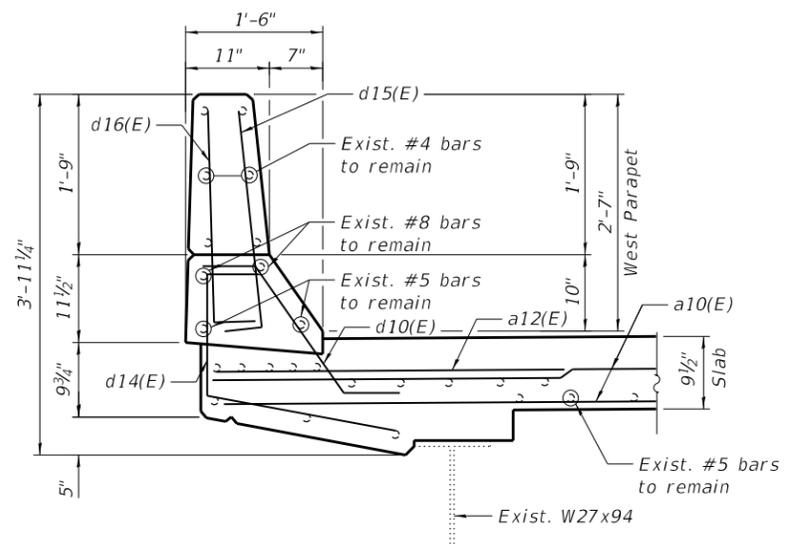
SECTION A-A



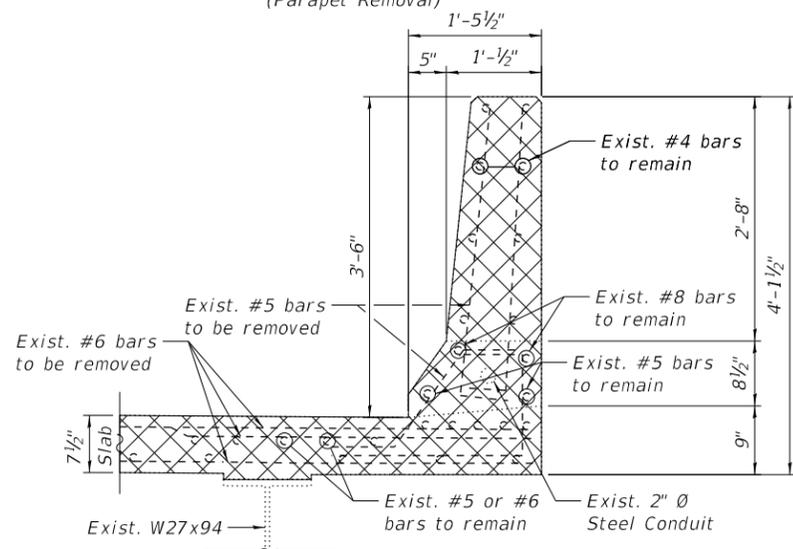
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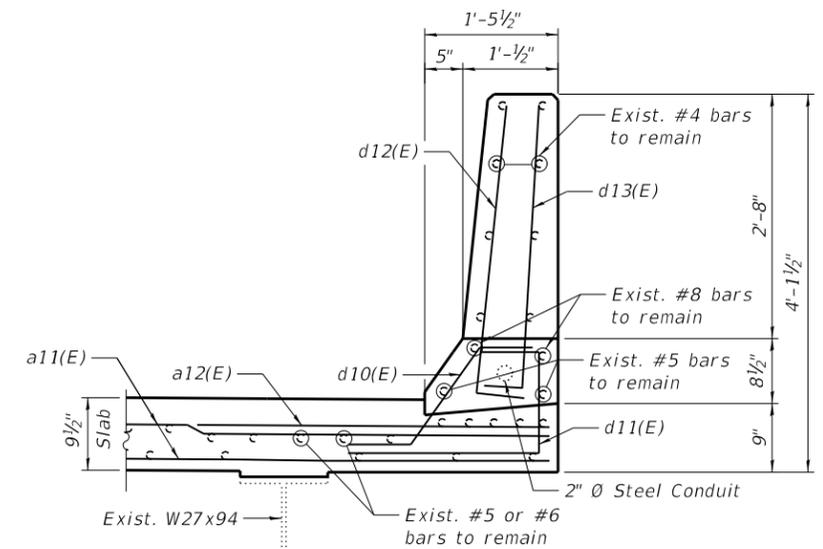
SECTION B-B
(Parapet Removal)



SECTION BB-BB
(Parapet Replacement)



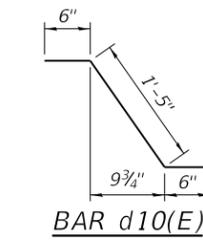
SECTION C-C
(Median Parapet Removal)



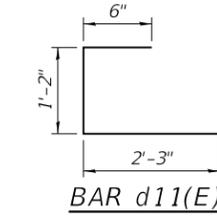
SECTION CC-CC
(Median Parapet Replacement)

BILL OF MATERIAL

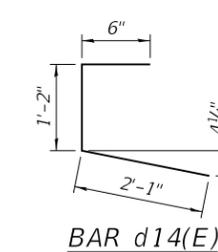
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a11(E)	7	#6	26'-11"	—
a12(E)	8	#6	4'-0"	—
d10(E)	10	#5	2'-5"	∩
d11(E)	5	#4	3'-11"	└
d12(E)	5	#5	3'-8"	└
d13(E)	5	#4	3'-8"	└
d14(E)	5	#4	3'-9"	└
d15(E)	5	#5	3'-0"	└
d16(E)	5	#4	3'-0"	└
h10(E)	6	#6	26'-11"	—
h11(E)	6	#6	27'-4"	—
u10(E)	57	#5	2'-8"	└
Concrete Removal			CU YD	7.4
Concrete Superstructure			CU YD	8.4
Protective Coat			SQ YD	20
Reinforcement Bars, Epoxy Coated			POUND	1,370



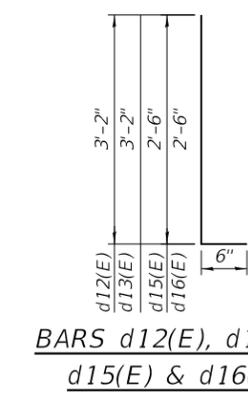
BAR d10(E)



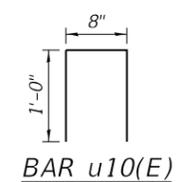
BAR d11(E)



BAR d14(E)



BARS d12(E), d13(E),
d15(E) & d16(E)



BAR u10(E)

NOTES:

- For bar splicer assembly and mechanical splicer details, see Sheet S8-18.
- Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

LEGEND



Concrete Removal



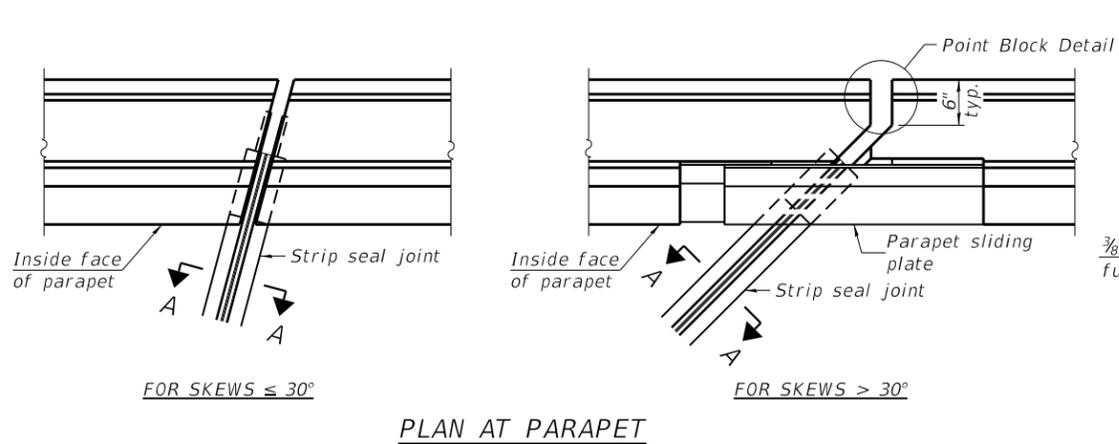
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CHECKED - MI,RTB	REVISED -	
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PLOT DATE =	DATE - 12/4/2020	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

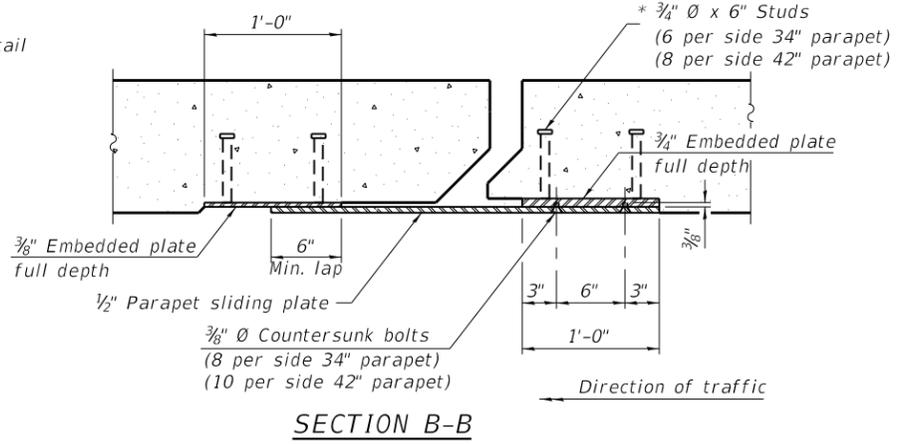
N. ABUT. JOINT REMOVAL AND RECONSTRUCTION (SHEET 2 OF 2)
 S.N. 099-0023 I-55 SB OVER MATERIAL SERVICE RR (ABANDONED)

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	162
CONTRACT NO. 62N22				

MODEL: Default
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12/2/2020 3:40:20 PM

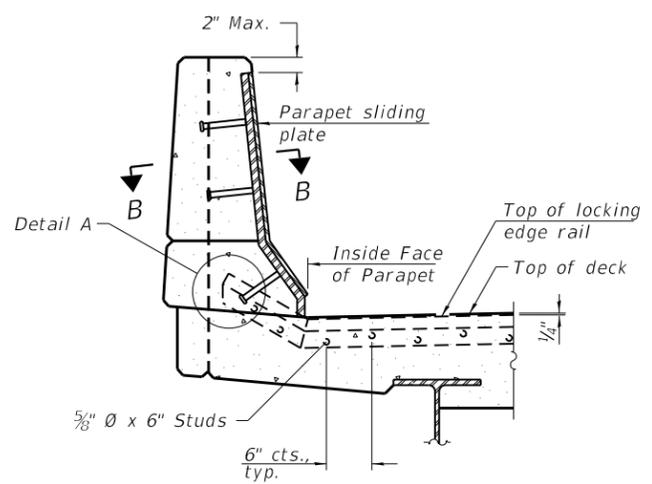


PLAN AT PARAPET



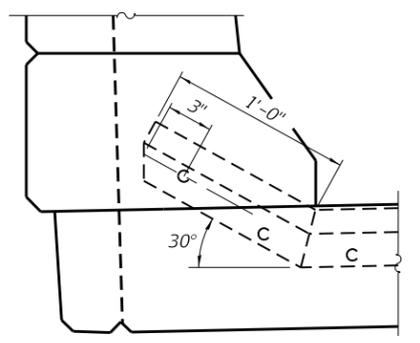
SECTION B-B

Notes:
The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.
The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the 4 1/2" maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.
The manufacturer's recommended installation methods shall be followed.
All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.
The Maximum space between locking edge rail segments shall be 3/16" and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.

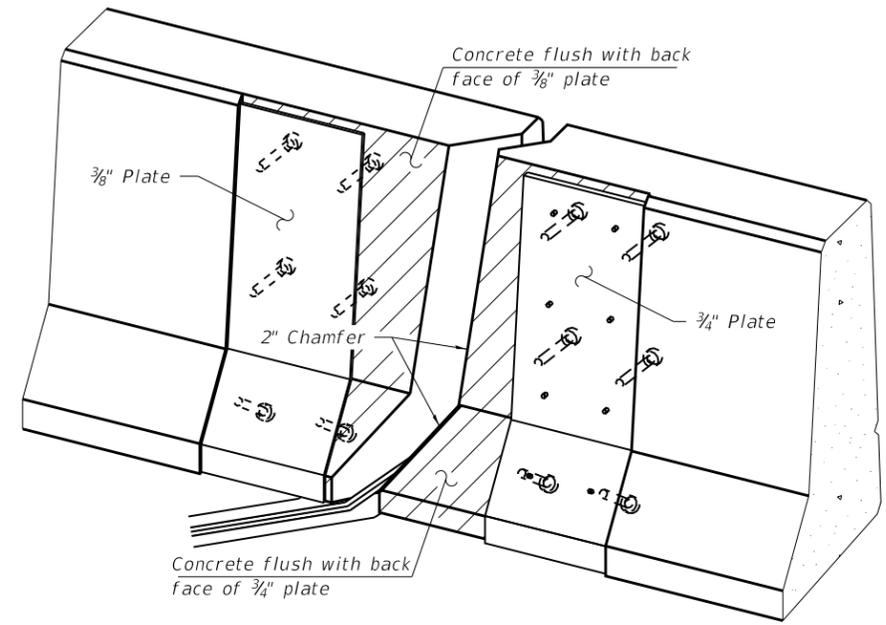


ELEVATION AT PARAPET

(Skews > 30° shown. Skews ≤ 30° similar except as shown in plan view.)

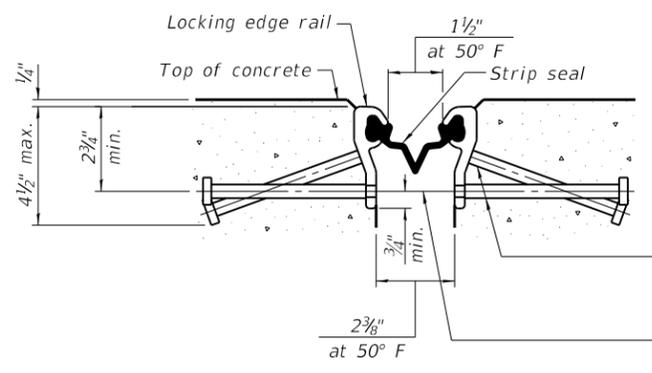


DETAIL A



TRIMETRIC VIEW
(Showing embedded plates only)

Cost of parapet sliding plates, embedded plates, and anchorage studs included with Preformed Joint Strip Seal.
34" F-shape barrier shown, 42" F-shape similar as noted.
The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required. One exception to this would be the strip seal joint at the end of the precast bridge approach slab. For these cases the pavement connector length shall be adjusted, not the length of the bridge approach slab.



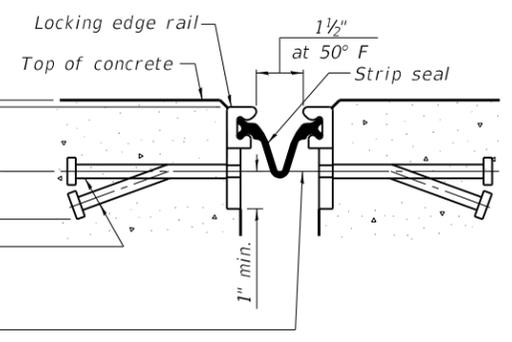
SHOWING ROLLED RAIL JOINT

* 3/8" Ø x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)

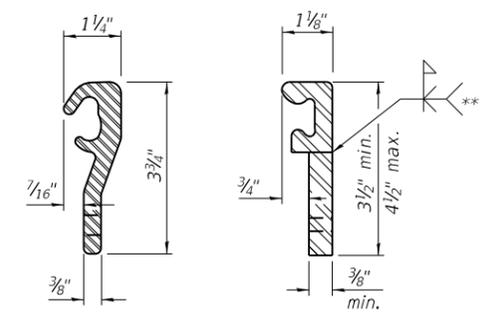
3/8" Ø threaded rods in 7/16" Ø holes at ±4'-0" cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

SECTION A-A

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

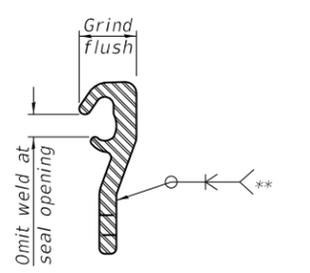


SHOWING WELDED RAIL JOINT



LOCKING EDGE RAILS

** Back gouge not required if complete joint penetration is verified by mock-up.



LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	110

EJ-SS

8-11-17



USER NAME =	DESIGNED - MAA,AMS	REVISED -
PLOT SCALE =	CHECKED - MI, RTB	REVISED -
PLOT DATE =	DRAWN - AMS	REVISED -
	DATE - 12/2/2020	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PREFORMED JOINT STRIP SEAL
S.N. 099-0023 I-55 SB OVER MATERIAL SERVICE RR (ABANDONED)**

SHEET S8-13 OF S8-18 SHEETS

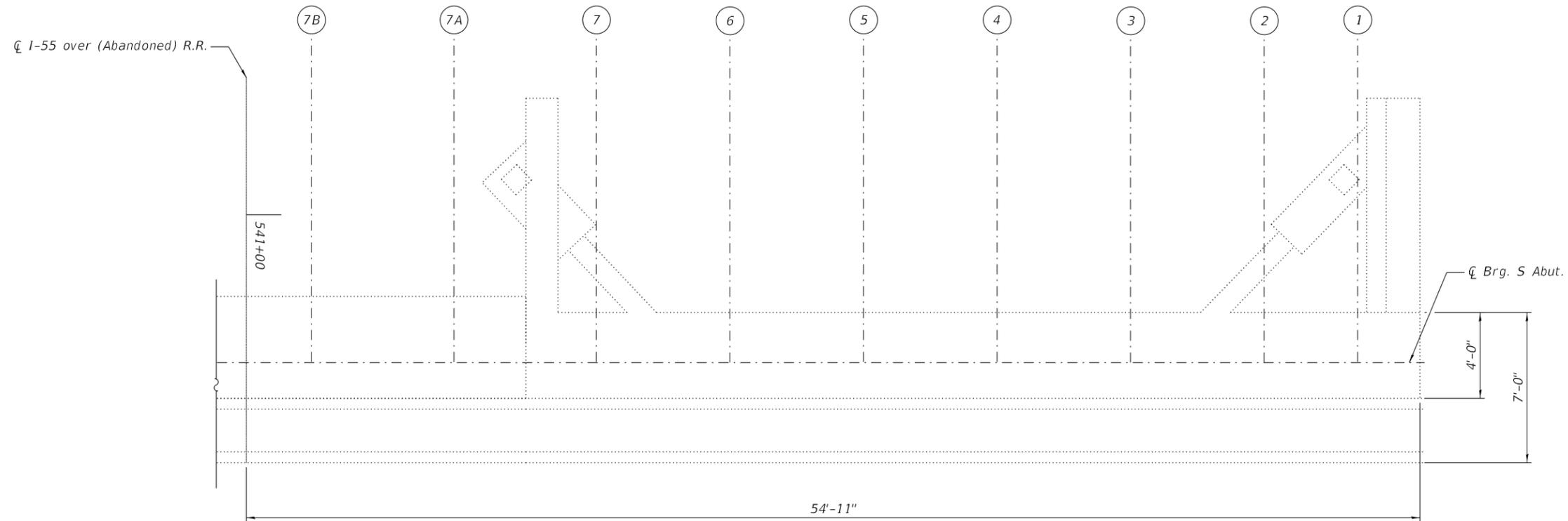
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	163
			CONTRACT NO. 62N22	
ILLINOIS FED. AID PROJECT				

NOTE:

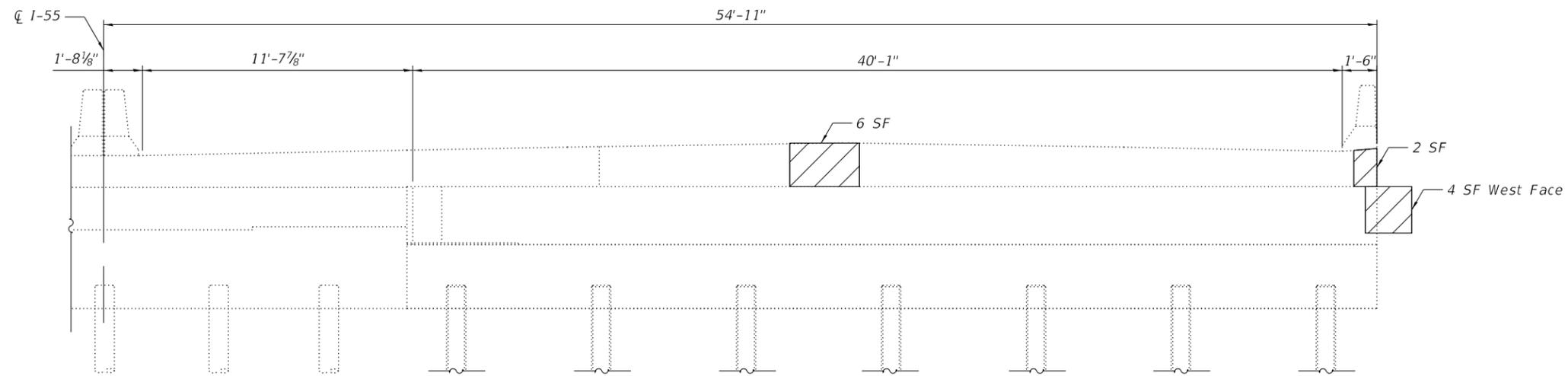
1. Quantities and limits shown are estimated for bidding purpose only. The actual areas to be repaired and the type(s) of repairs to be used will be determined by the engineer in the field at the time of construction.

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Concrete Sealer	Sq. Ft.	127
Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)	Sq. Ft.	12



PLAN



SOUTH ABUTMENT
(Looking South)

LEGEND

- Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)
- SF Square Feet

MODEL: Default
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PLOT DATE =	DATE - 12/2/2020	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SOUTH ABUTMENT REPAIRS
S.N. 099-0023 I-55 SB OVER MATERIAL SERVICE RR (ABANDONED)

SHEET S8-14 OF S8-18 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	164
CONTRACT NO. 62N22				

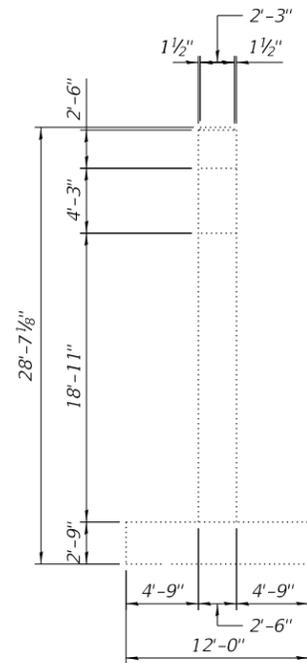
ILLINOIS FED. AID PROJECT

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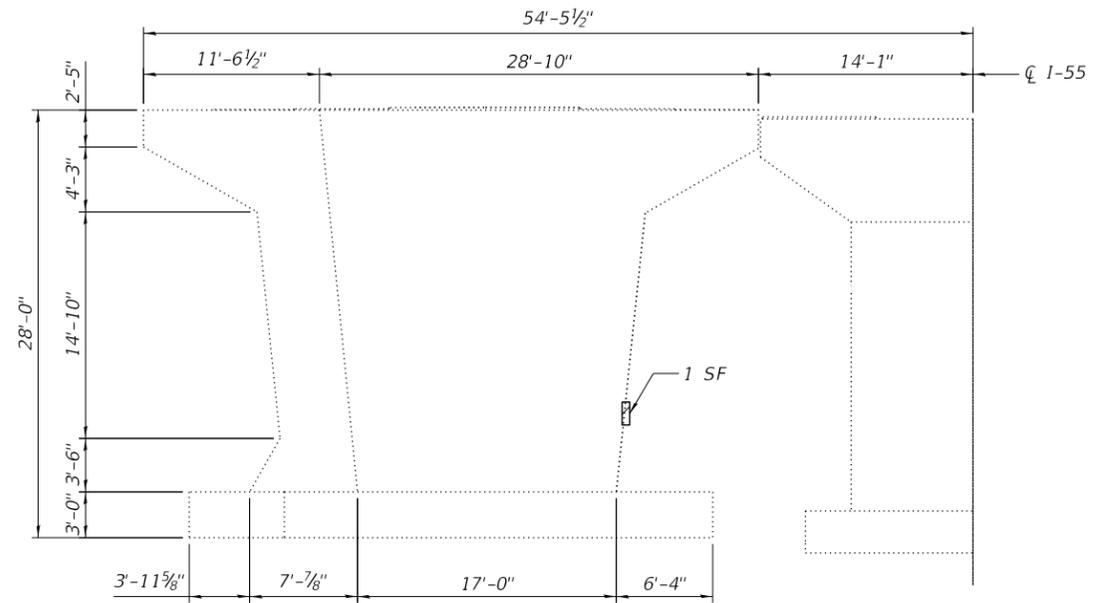
- Quantities and limits shown are estimated for bidding purpose only. The actual areas to be repaired and the type(s) of repairs to be used will be determined by the Engineer in the field at the time of construction.

BILL OF MATERIAL

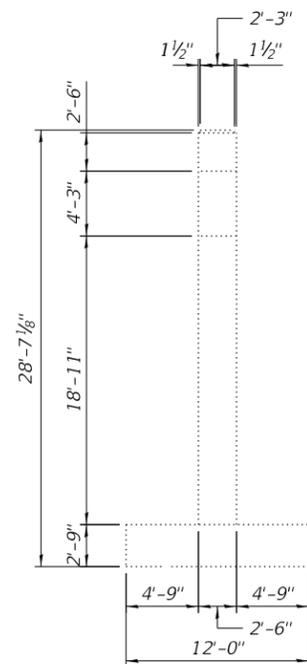
ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth Equal to or Less than 5 inches)	Sq Ft	1



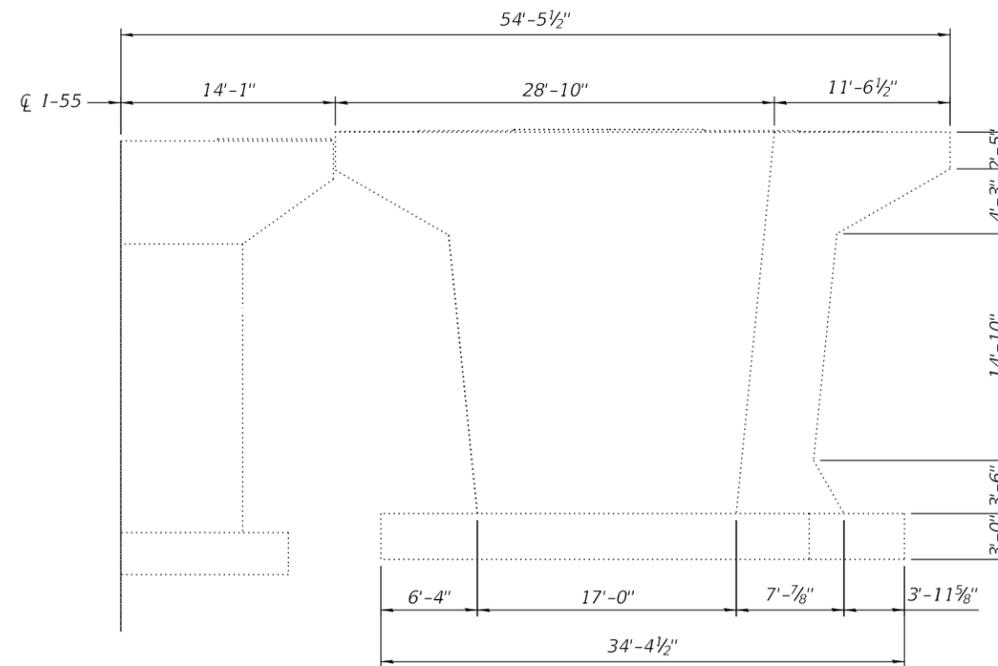
END ELEVATION
(Looking East)



PIER 1 ELEVATION
(Looking North)



END ELEVATION
(Looking West)



PIER 1 ELEVATION
(Looking South)

LEGEND



Structural Repair of Concrete
(Depth Equal to or Less than 5 inches)

SF

Square Feet

MODEL: Default
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PLOT DATE =	DATE - 12/2/2020	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PIER 1 REPAIRS
S.N. 099-0023 I-55 SB OVER MATERIAL SERVICE RR (ABANDONED)

SHEET S8-15 OF S8-18 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	165
CONTRACT NO. 62N22				

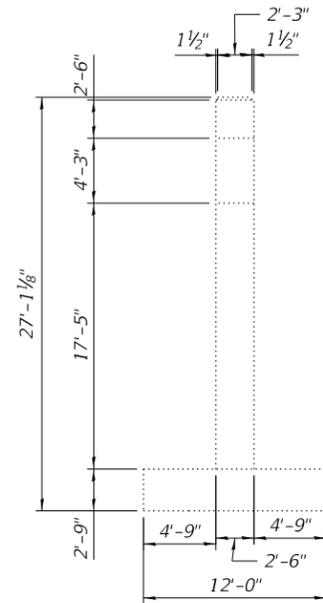
ILLINOIS FED. AID PROJECT

NOTES:

- Quantities and limits shown are estimated for bidding purpose only. The actual areas to be repaired and the type(s) of repairs to be used will be determined by the Engineer in the field at the time of construction.

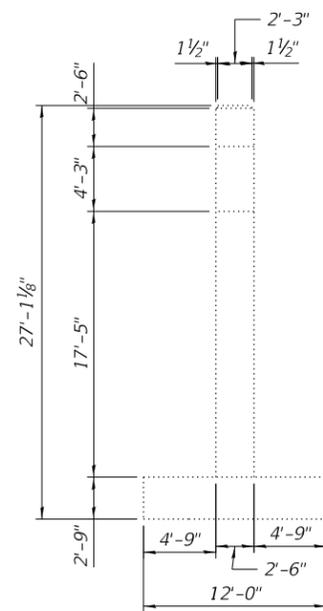
BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth Equal to or Less than 5 inches)	Sq Ft	8



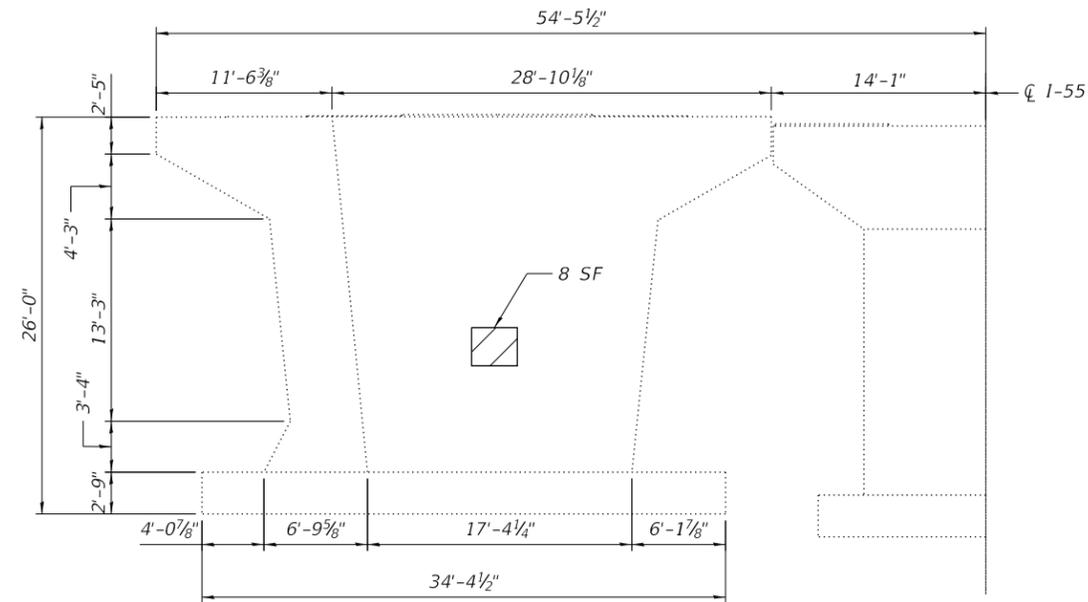
END ELEVATION

(Looking East)



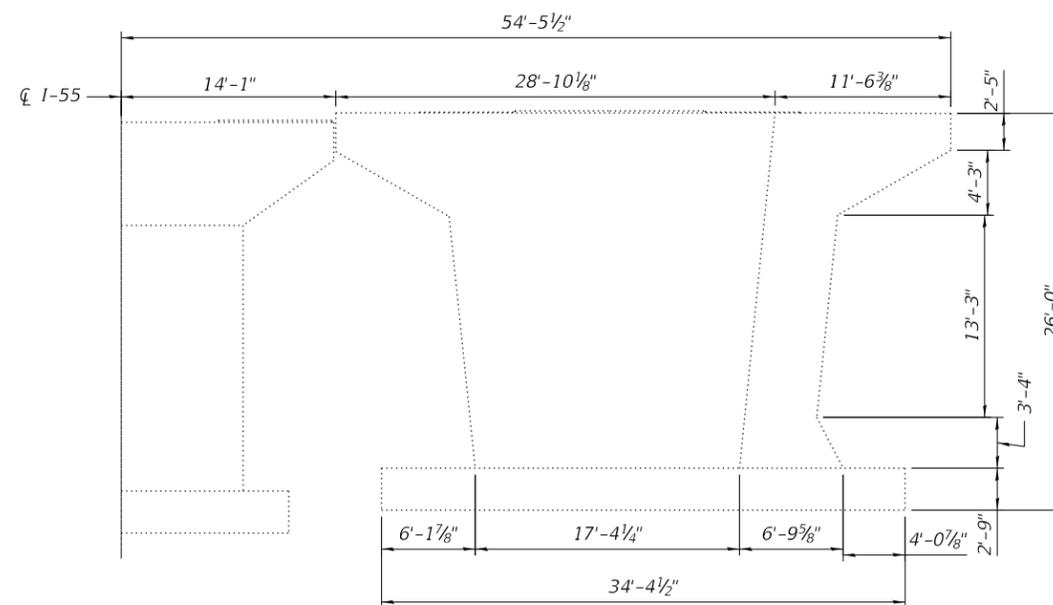
END ELEVATION

(Looking East)



PIER 2 ELEVATION

(Looking North)



PIER 2 ELEVATION

(Looking South)

LEGEND

Structural Repair of Concrete
(Depth Equal to or Less than 5 inches)

SF Square Feet

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

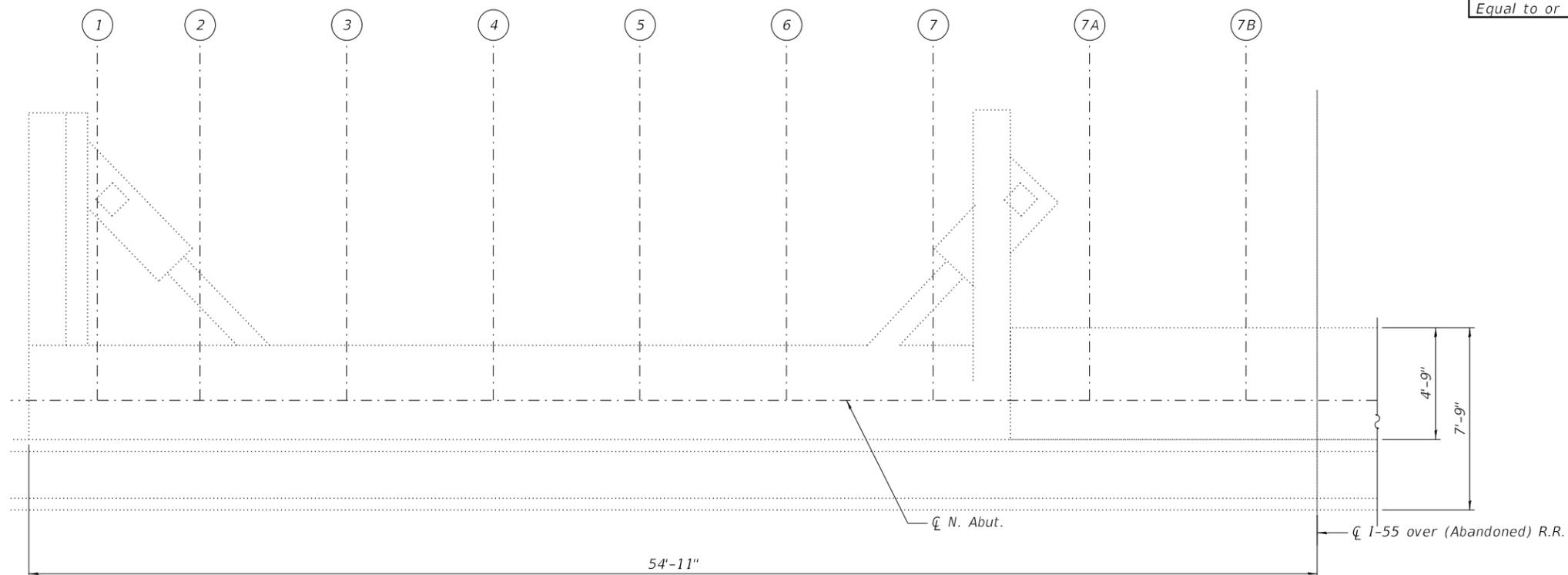
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S.N. 099-0023 I-55 SB OVER MATERIAL SERVICE RR (ABANDONED)

SHEET S8-16 OF S8-18 SHEETS

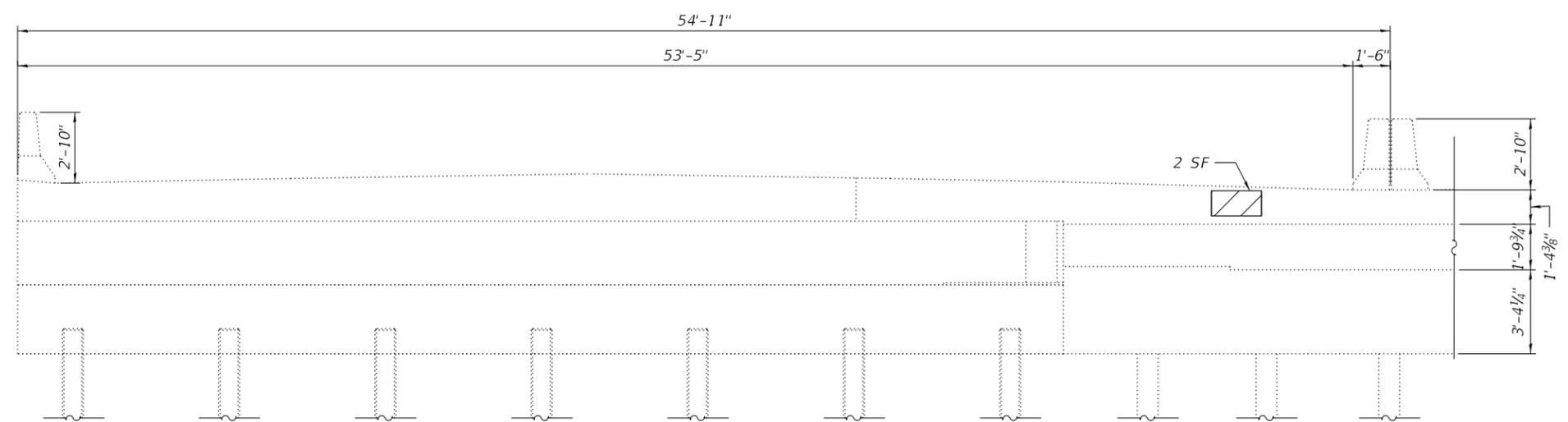
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I-55	2020-253-BR&PP	WILL	178	166
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62N22	

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Concrete Sealer	Sq. Ft.	127
Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)	Sq. Ft.	2



PLAN



NORTH ABUTMENT
(Looking North)

NOTE:

1. Quantities and limites shown are estimated for bidding purpose only. The actual areas to be repaired and the type(s) of repairs to be used will be determined by the engineer in the field at the time of construction.

LEGEND:

- Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)
- SF Square Feet

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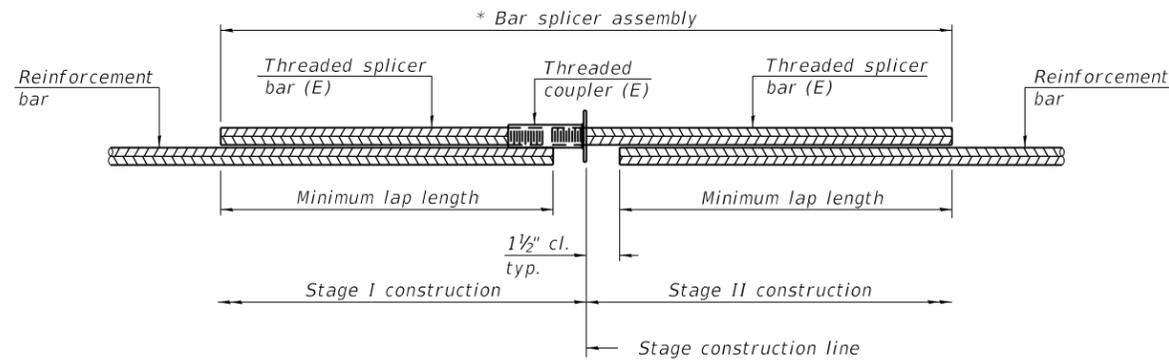
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PLOT DATE =	DATE - 12/2/2020	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

NORTH ABUTMENT REPAIRS
S.N. 099-0023 I-55 SB OVER MATERIAL SERVICE RR (ABANDONED)

SHEET S8-17 OF S8-18 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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			CONTRACT NO. 62N22	
ILLINOIS		FED. AID PROJECT		

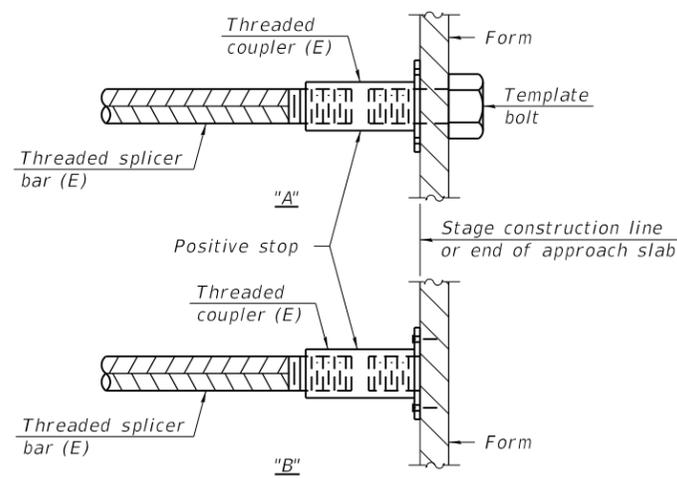


STANDARD BAR SPLICER ASSEMBLY PLAN
 (All components shall be provided from one supplier)

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
S Abutment	#6	13	4'-10"
N Abutment	#6	13	4'-10"

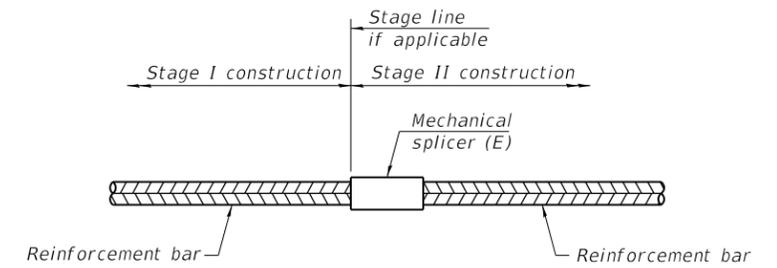


INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.

"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.

(E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required

Notes:

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.

All reinforcement shall be lapped and tied to the splicer bars.

Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.

See approved list of bar splicer assemblies and mechanical splicers for alternatives.

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BSD-1

1-1-2020



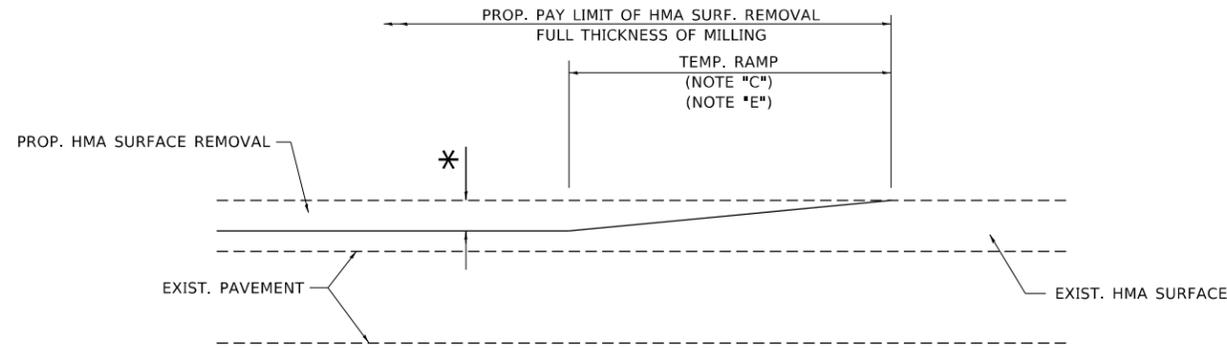
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PLOT DATE =	DATE - 12/2/2020	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

BAR SPLICER ASSEMBLY DETAILS
S.N. 099-0023 I-55 SB OVER MATERIAL SERVICE RR (ABANDONED)

SHEET S8-18 OF S8-18 SHEETS

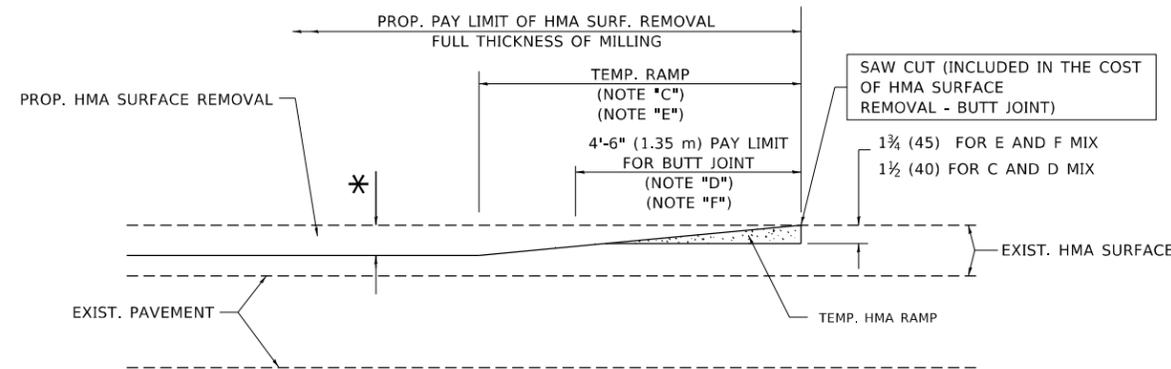
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	168
			CONTRACT NO. 62N22	
ILLINOIS		FED. AID PROJECT		



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

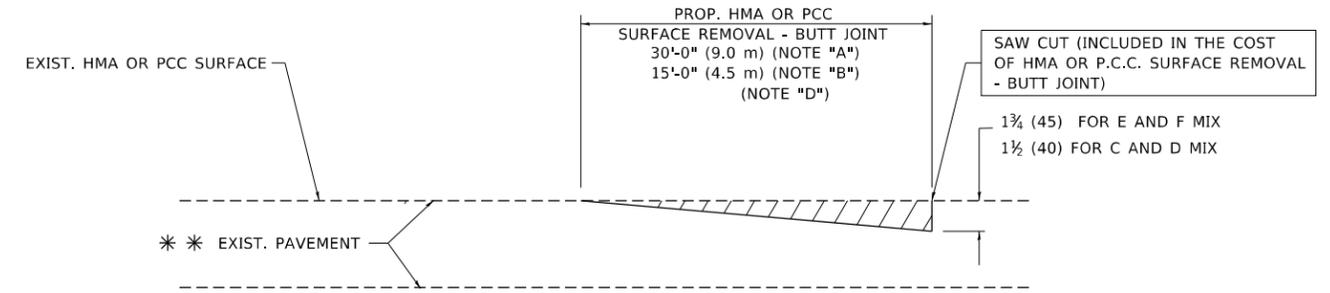


HMA CONSTRUCTED TEMPORARY RAMP

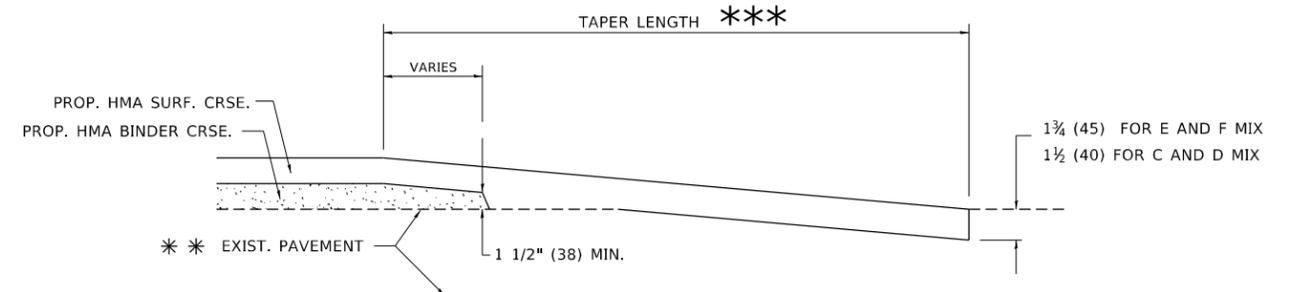
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

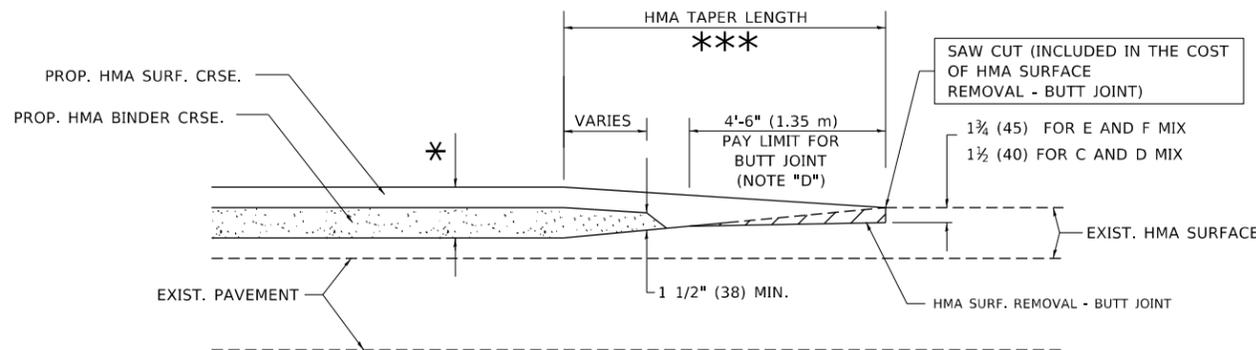
NOTES

- A. MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F. INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT.
* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- G. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
*** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

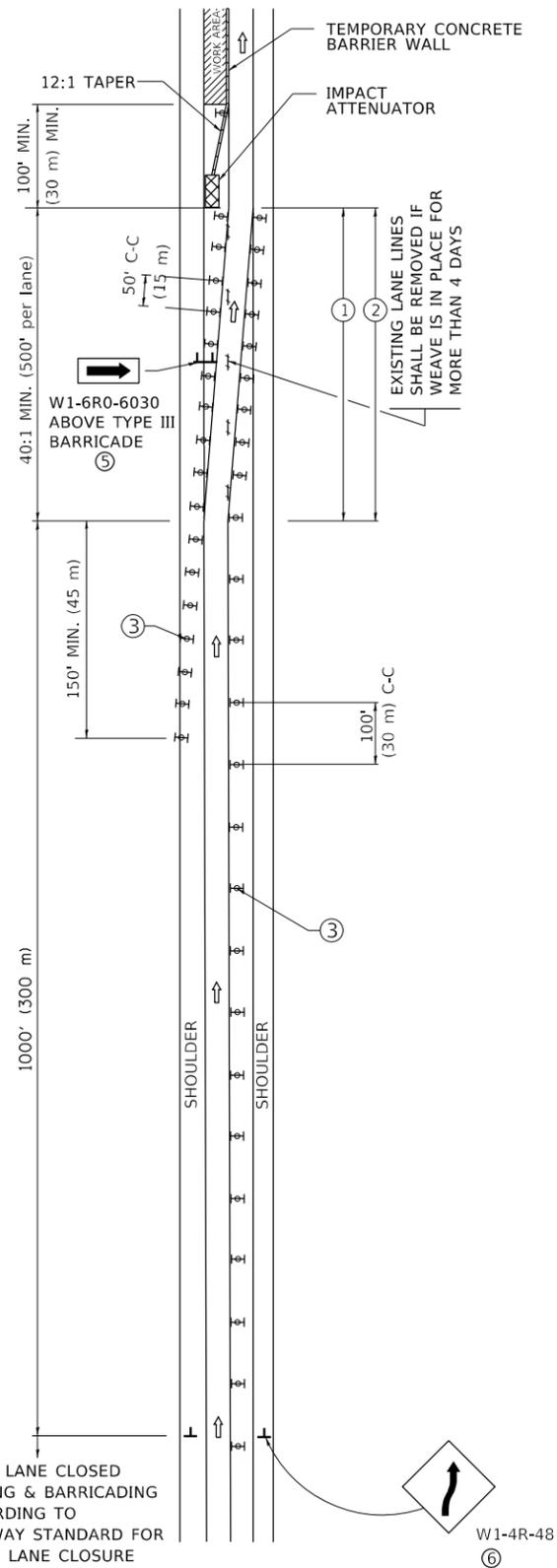
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			178	170
BD400-05 BD32		CONTRACT NO.		
ILLINOIS FED. AID PROJECT				

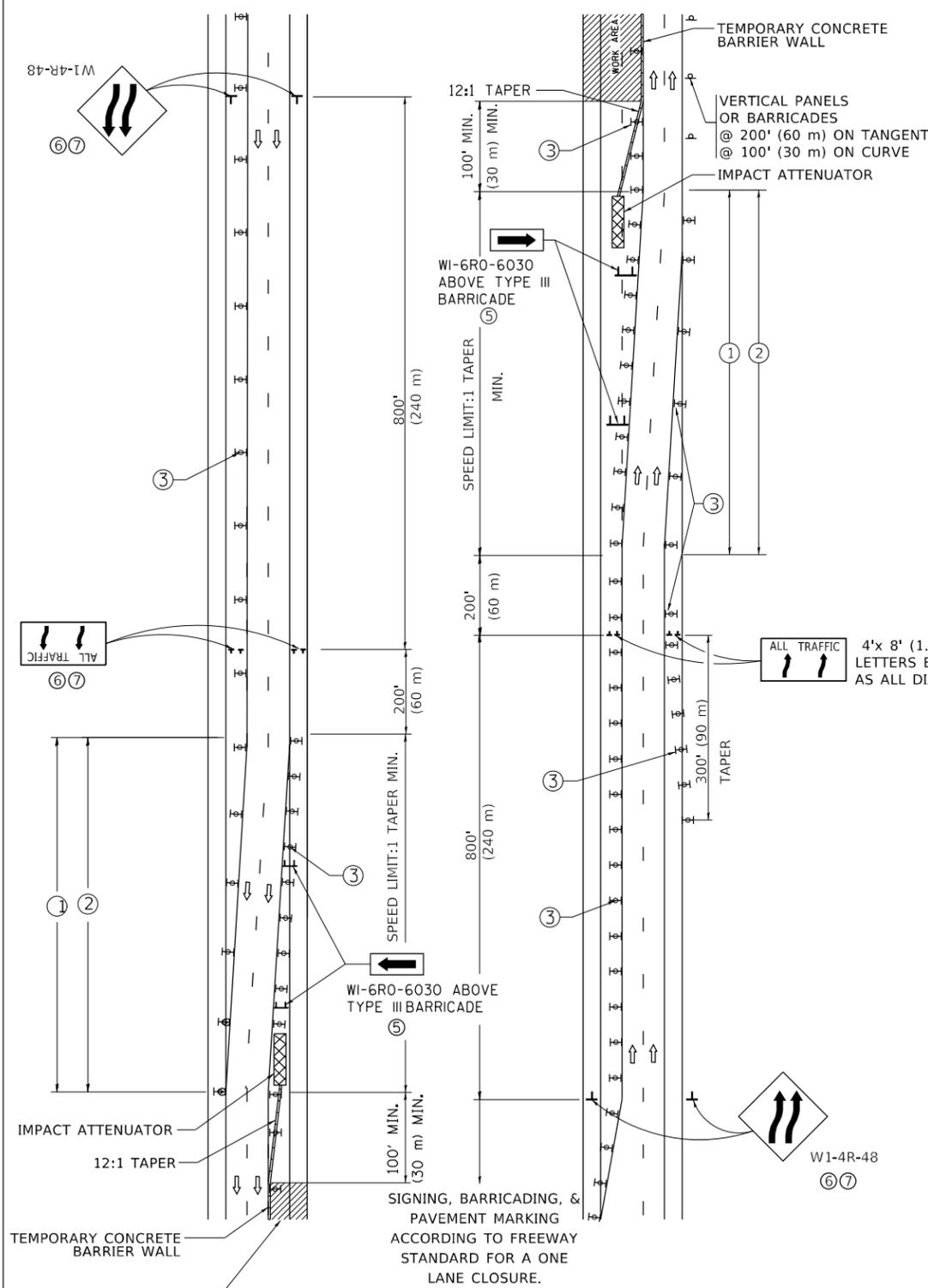
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PLOT DATE - 3/27/2019	CHECKED -	REVISED - M. GOMEZ 04-06-01
	DATE - 06-13-90	REVISED - R.BORO 01-01-07

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SINGLE LANE WEAVE



MULTI-LANE WEAVE



GENERAL NOTES:

- ① EXISTING CONFLICTING PAVEMENT MARKING LINES SHALL BE REMOVED. PAVEMENT MARKING REMOVAL SHALL NOT BE REQUIRED FOR SINGLE LANE WEAVES UNDER 4 DAYS IN DURATION.
- ② CONTINUOUS REFLECTIVE TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE TAPER AND FOR 300' (90 m) ALONG SIDE THE WORK AREA WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS. THE LEFT EDGE LINE SHALL BE YELLOW AND THE RIGHT EDGE LINE SHALL BE WHITE. FOR MULTI-LANE WEAVES LANE LINES SHALL BE 5 INCH, 10'-30' (3 m-9 m) SKIP DASH, WHITE.
- ③ PLASTIC DRUMS WITH STEADY BURN LIGHTS AT 50' (15 m) C-C SPACING IN TAPERS AND 100' (30 m) C-C SPACING IN TANGENTS.
- ④ ALL SIGNS SHALL BE POST MOUNTED IF THE CLOSURE TIME EXCEEDS FOUR DAYS.
- ⑤ TYPE III BARRICADES MAY BE OMITTED FOR SINGLE-LANE WEAVES UNDER 24-HOURS IN DURATION. W1-6 SIGNS WILL STILL BE REQUIRED. IF THE WIDTH OF OFFSET IS LESS THAN 6' THEN THE TYPE III BARRICADE WITH ATTACHED ARROW SIGN PANEL CAN BE ELIMINATED IN THE TAPER AREAS.
- ⑥ WHEN THE LENGTH OF THE SHIFTED SEGMENT (DISTANCE BETWEEN WEAVE POINTS) IS LESS THAN 1500', DOUBLE REVERSE CURVE SIGNS (W24-1) SHOULD BE USED INSTEAD OF THE REVERSE CURVE (W1-4) SIGNS. ARROWS ON THE 4'X8' "ALL TRAFFIC" SIGNS SHALL BE THE SAME SHAPE.
- ⑦ THE NUMBER OF ARROWS ON THESE SIGNS SHALL MATCH THE NUMBER OF LANES OPEN TO TRAFFIC.

4'x 8' (1.2 m x 2.4 m); 1 (25) BORDER; 10 (250) CAPITAL LETTERS BACKGROUND SHEETING SHALL BE THE SAME AS ALL DIAMOND SHAPED CONSTRUCTION SIGNS.

SYMBOLS

- DIRECTION OF TRAFFIC
- WORK AREA
- SIGN ON PORTABLE OR PERMANENT SUPPORT
- TYPE II BARRICADE OR DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT
- TEMPORARY CONCRETE BARRIER WALL
- IMPACT ATTENUATOR
- W1-4R-48
- W24-1-48

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

MODEL: D:\inf\... FILE NAME: P:\ILLINOIS\BARRIERS\TC-09\Drawings\TrafficControl\TrafficControl.dwg

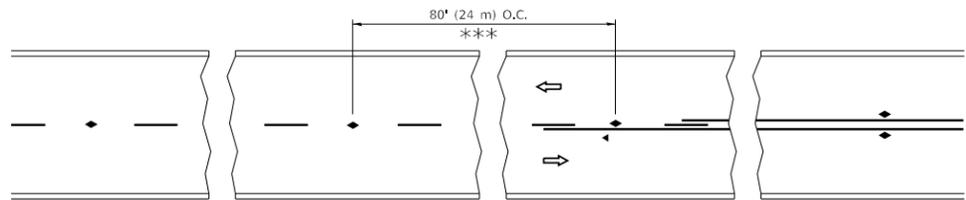
USER NAME	footemj	DESIGNED	D.W.S.	REVISED	J.A.F. 02-06
DRAWN		REVISION	S.P.B. 01-07	REVISION	S.P.B. 12-09
PLOT SCALE	1/8" = 1'-0"	CHECKED		REVISION	M.D. 06-13
PLOT DATE	3/4/2019	DATE	02-87		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL DETAILS FOR
FREEWAY SINGLE & MULTI-LANE WEAVE

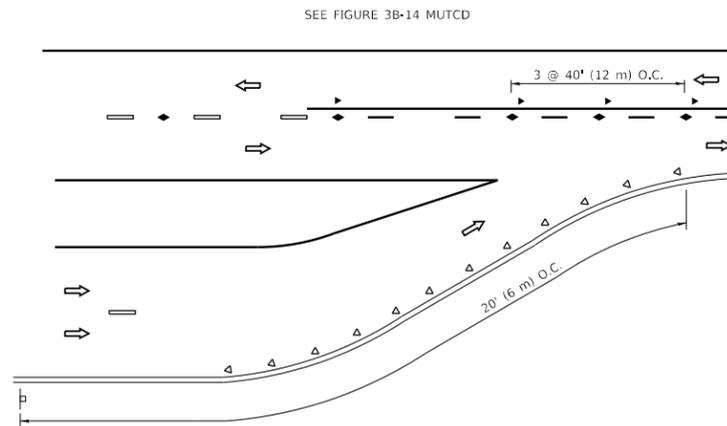
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	TC-09		178	172
CONTRACT NO.				
ILLINOIS FED. AID PROJECT				

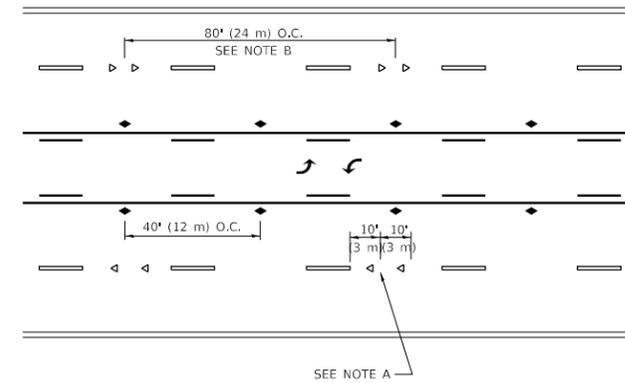


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

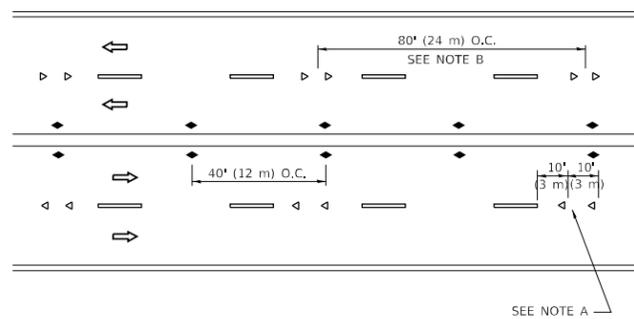
TWO-LANE/TWO-WAY



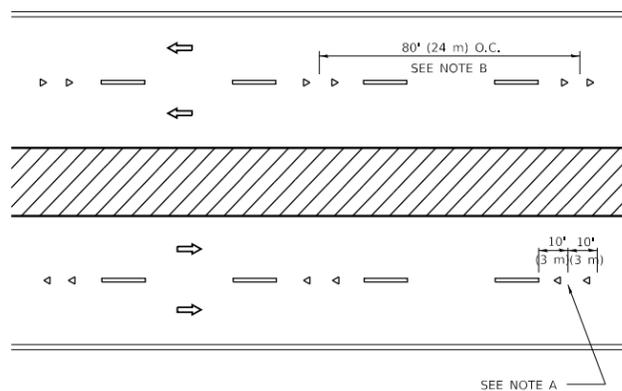
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

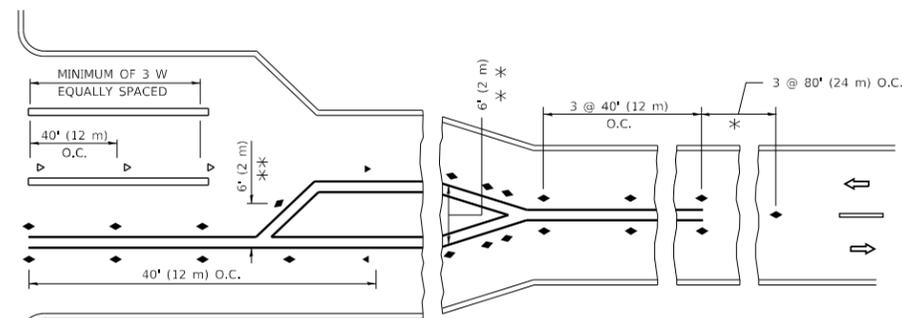
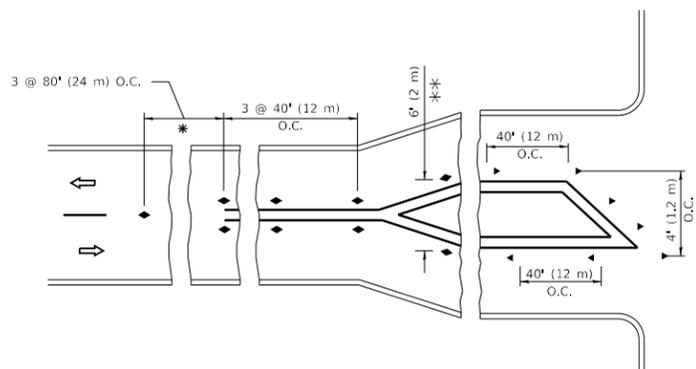
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

TURN LANES

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: D:\ref\... FILE: MAME.P... PROJECT: ...

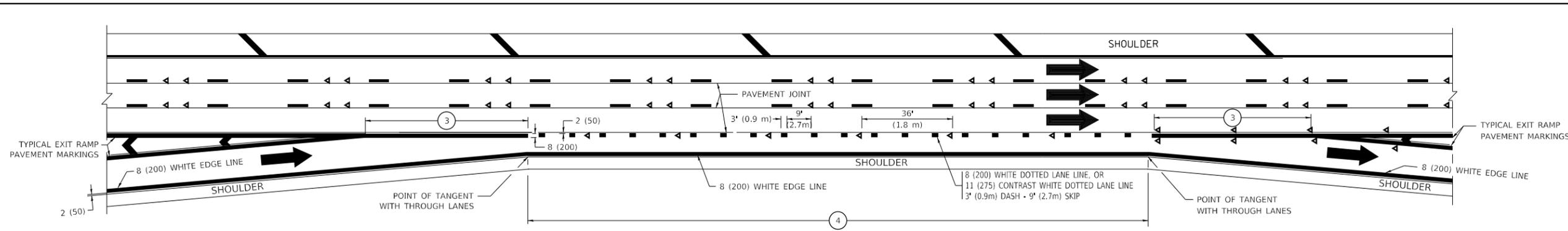
USER NAME	DESIGNED	REVISED
DRAWN	REVISOR	DATE
PLOT SCALE	CHECKED	DATE
PLOT DATE		

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

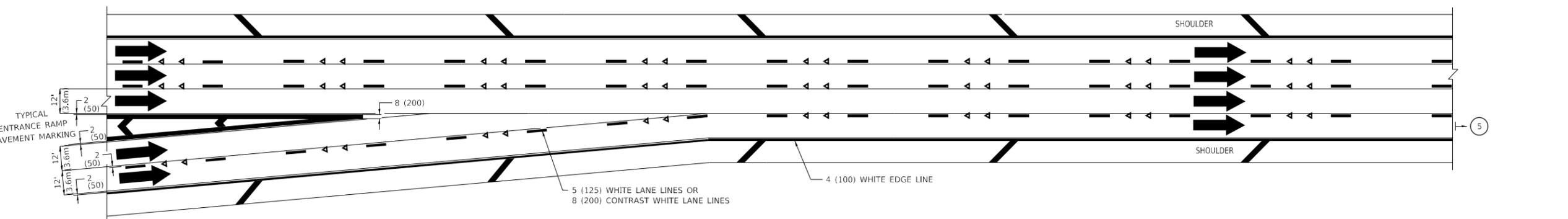
**TYPICAL APPLICATIONS
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

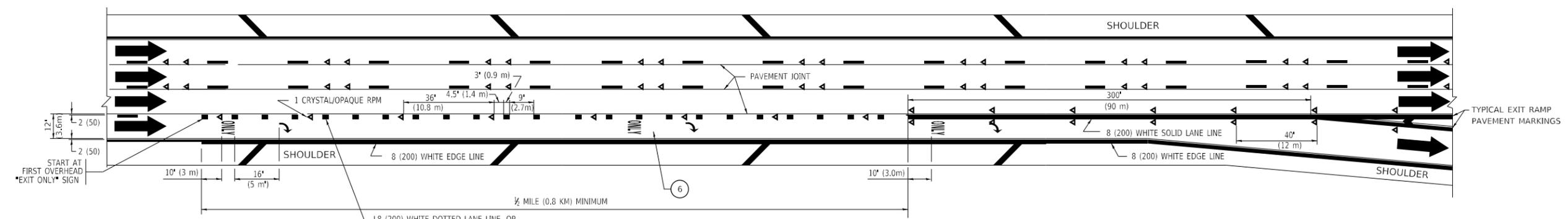
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			178	173
TC-11			CONTRACT NO.	
ILLINOIS FED. AID PROJECT				



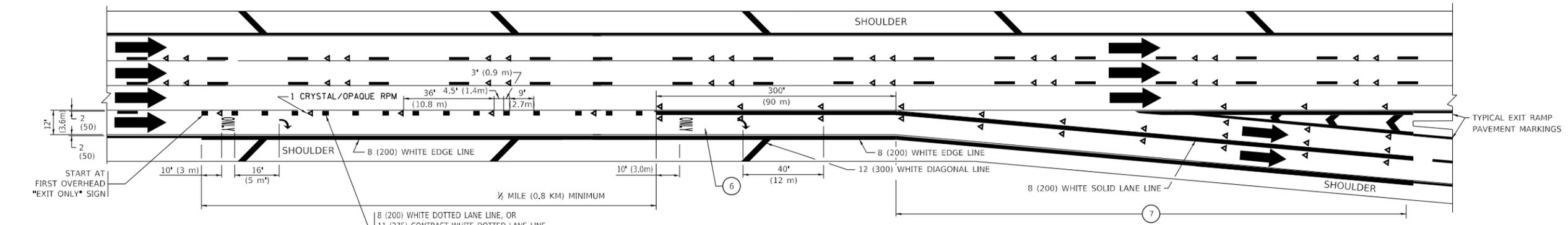
AUXILIARY LANE MARKINGS



TWO LANE ENTRANCE RAMP WITH MERGE MARKINGS



EXIT ONLY LANE MARKINGS



EXIT ONLY WITH OPTION LANE MARKINGS

- NOTES:**
- 3 OMIT WHEN LENGTH OF AUXILIARY LANE IS LESS THAN 500' (150 m).
 - 4 8-INCH WIDE DOTTED LANE LINE MARKINGS SHALL BE USED WHEN THE LENGTH OF THE AUXILIARY LANE IS 2 MILES OR LESS.
 - 5 FOR TWO-LANE ENTRANCE RAMP, IF RIGHT LANE ENDS, USE TYPICAL ENTRANCE RAMP PAVEMENT MARKINGS.
 - 6 ONLY AND ARROWS EQUALLY SPACED, 500' (150 m) MAXIMUM SPACING. FULL SIZE LETTERS AND ARROW SHALL BE USED.
 - 7 CONTINUE 8" SOLID LANE LINE THROUGH EXIT TO END OF PAVED GORE.

MODEL: D:\refurb\...
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 PROJECT: ...
 DATE: 3/4/2019

USER NAME	DESIGNED	REVISD
DRAWN	CHECKED	REVISD
PLOT SCALE	DATE	REVISD
PLOT DATE		

DESIGNED	REVISD	REVISD	REVISD
DRAWN	CHECKED	DATE	

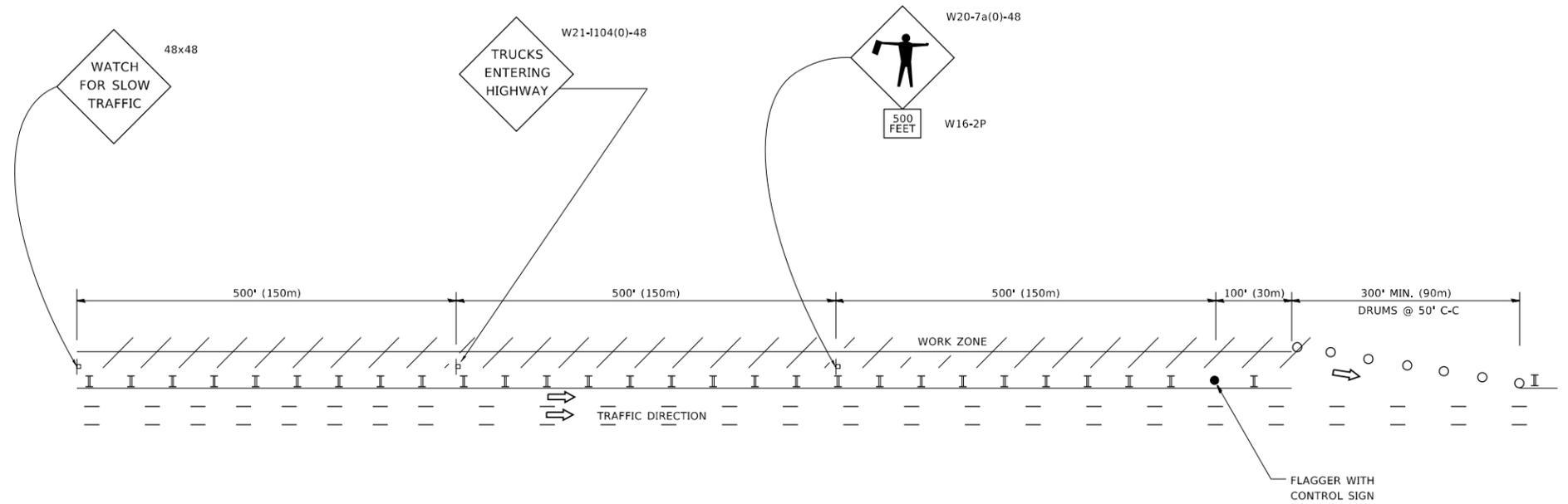
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

MULTI-LANE FREEWAY PAVEMENT MARKING DETAILS			
SCALE: NONE	SHEET 1	OF 2 SHEETS	STA. TO STA.

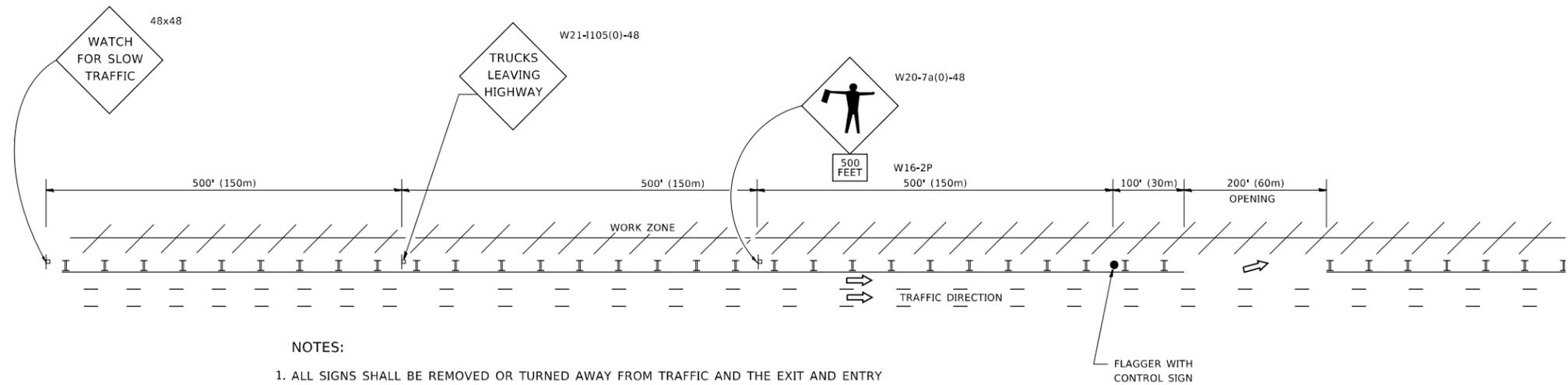
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			178	175
TC-12		CONTRACT NO.		
ILLINOIS		FED. AID PROJECT		

SIGNING FOR FLAGGING OPERATIONS AT WORK ZONE OPENINGS

WORK ZONE EXIT OPENING



WORK ZONE ENTRY OPENING



NOTES:

1. ALL SIGNS SHALL BE REMOVED OR TURNED AWAY FROM TRAFFIC AND THE EXIT AND ENTRY OPENINGS SHALL BE CLOSED WHEN THE FLAGGING OPERATION CEASES. NON OPERATING EQUIPMENT SHALL COMPLY WITH ARTICLE 701.11
2. WORK ZONE OPENINGS SHALL BE A MINIMUM OF ONE HALF MILE APART AND A MINIMUM OF ONE QUARTER MILE FROM ALL ENTRANCE AND EXIT RAMPS.
3. EXITING THE WORK ZONE AT ANY PLACE OTHER THAN AT A WORK ZONE EXIT OPENING WILL BE PROHIBITED.
4. ALL VEHICLES SHALL ENTER THE WORK ZONE AT ENTRY OPENINGS, USING THEIR TURN SIGNALS TO WARN MOTORISTS
5. FLAGGERS SHALL NOT STOP TRAFFIC OR DIRECT TRAFFIC INTO AN ADJACENT LANE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

MODEL: D:\inf\...
 FILE: NAME: P:\...
 PROJECT: ...
 DATE: 3/4/2019

USER NAME	DESIGNED	REVISED
DRAWN	CHECKED	REVISED
PLOT SCALE	DATE	REVISED
PLOT DATE		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FREEWAY /EXPRESSWAY SIGNING FOR FLAGGING OPERATIONS
AT WORK ZONE OPENINGS ON FREEWAYS /EXPRESSWAYS

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
			178	178
TC-18			CONTRACT NO.	
ILLINOIS FED. AID PROJECT				

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.