

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	1
		ILLINOIS	CONTRACT NO. 62N22	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED HIGHWAY PLANS

F.A.I. ROUTE 55 (INTERSTATE 55)
SOUTH OF I-80 TO SOUTH OF WEBER RD.

SECTION: 2020-253-BR&PP
PROJECT: NHPP-DHUM(590)
BRIDGE DECK OVERLAY, BRIDGE JOINT REPAIRS,
SHOULDER RECONSTRUCTION AND PAVEMENT PATCHING
WILL COUNTY

C-91-068-21

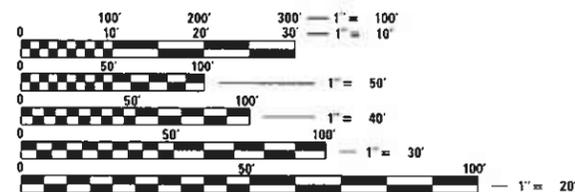
FOR INDEX OF SHEETS, SEE SHEET NO. 4

THE PROJECT IS LOCATED IN THE CITY OF JOLIET AND THE VILLAGES OF ROMEOVILLE, PLAINFIELD, SHOREWOOD, BOLINGBROOK AND CRYSTAL LAWNS (CDP)

BRIDGE LOCATIONS (NB)
SN 099-0017 (STA 418 + 18 to 420 + 06)
SN 099-0018 (STA 439 + 81 to 441 + 42)
SN 099-4615 (STA 460 + 99 to 461 + 77)
SN 099-0022 (STA 541 + 07 to 542 + 25)

BRIDGE LOCATIONS (SB)
SN 099-0016 (STA 418 + 19 to 420 + 05)
SN 099-0019 (STA 439 + 82 to 441 + 43)
SN 099-4616 (STA 460 + 99 to 461 + 77)
SN 099-0023 (STA 541 + 07 to 542 + 25)

TRAFFIC DATA:
POSTED SPEED LIMIT = 65 MPH
2019 ADT 113,400



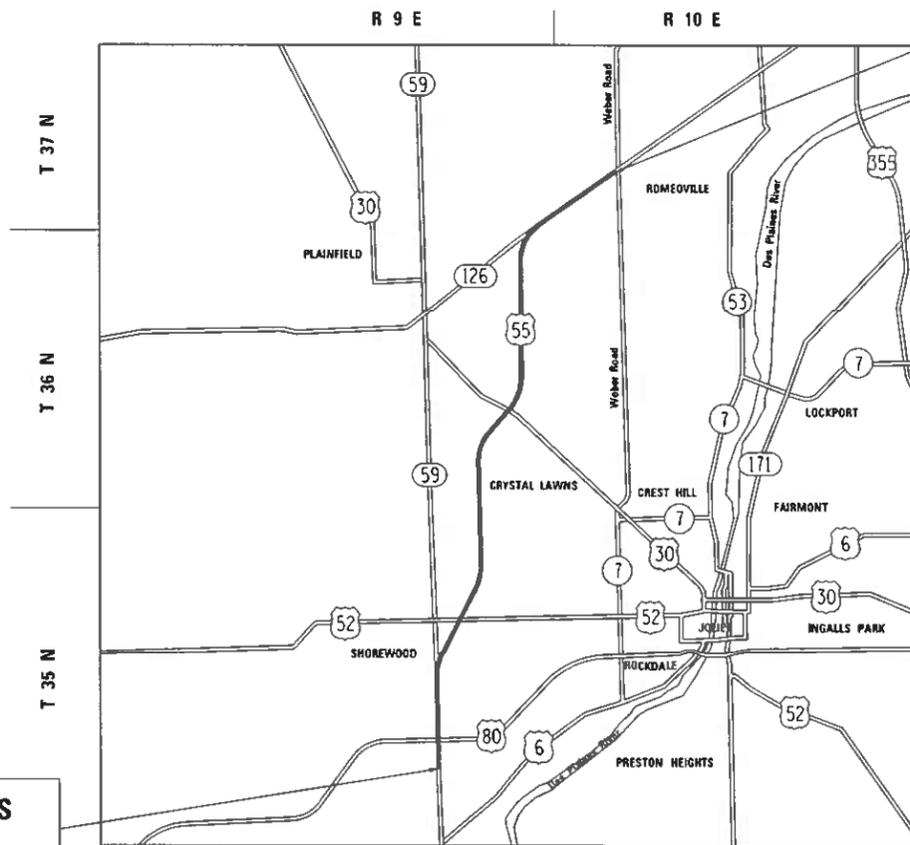
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS
1-800-892-0123 OR 811
MEADE ELECTRIC CO.
DISTRICT ONE ELECTRICAL MAINTENANCE CONTRACTOR
LOCATES IDOT ELECTRICAL EQUIPMENT AND UNDERGROUND CABLES: 773-287-7672

PROJECT ENGINEER: Veselin Velichkov, PE phone (847-705-4432)
PROJECT MANAGER: Fawad Aqueel, PE, PTOE phone (847-705-4247)
CONTRACT NO. 62N22



LOCATION MAP (NOT TO SCALE)



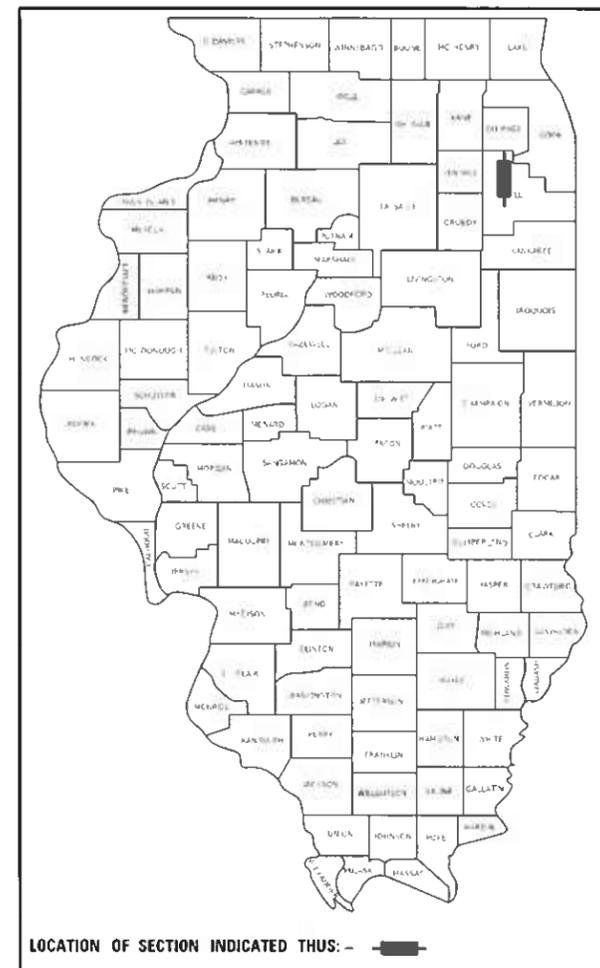
PROJECT BEGINS
STA. 16 + 68

PROJECT ENDS
STA. 735 + 49

TROY, PLAINFIELD, WHEATLAND, DuPAGE TOWNSHIPS

GROSS LENGTH = 71,881 FT = 13.6 MI
NET LENGTH = 71,881 FT = 13.6 MI

D-91-063-21



LOCATION OF SECTION INDICATED THIS: - [black bar]



Peter Johnston
PETER M. JOHNSTON, P.E.
DATE SIGNED: 12-1-2020
EXPIRATION DATE: 11-30-2021

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

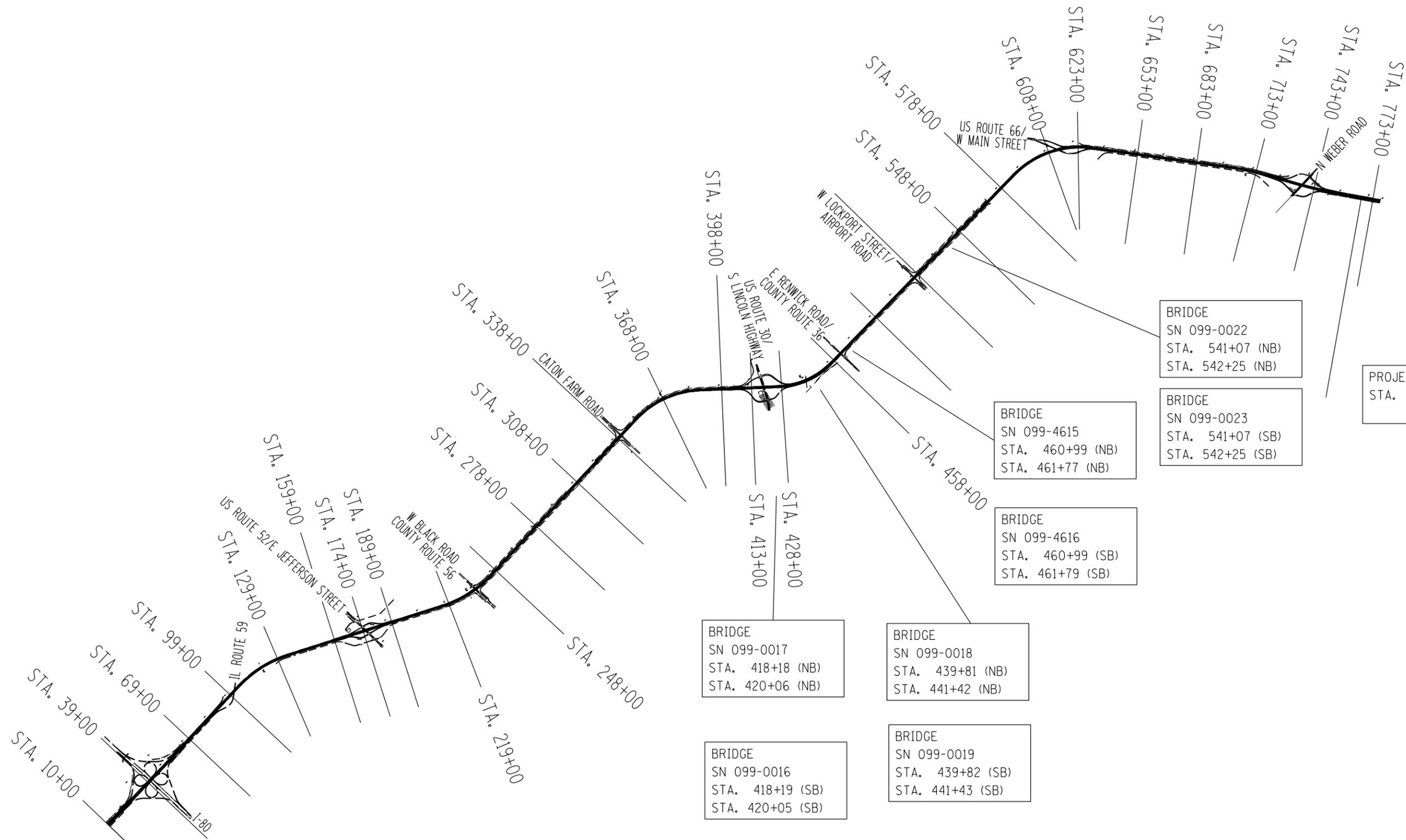
SUBMITTED December 9, 2020

Audrey A. Bandyk REGIONAL ENGINEER
January 29, 2021

Scott A. Elk
ENGINEER OF DESIGN AND ENVIRONMENT
January 29, 2021

James J. [Signature]
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS



PROJECT START
STA. 17+93 (NB)

BRIDGE
SN 099-0022
STA. 541+07 (NB)
STA. 542+25 (NB)

BRIDGE
SN 099-0023
STA. 541+07 (SB)
STA. 542+25 (SB)

PROJECT END
STA. 735+49 (NB)

BRIDGE
SN 099-4615
STA. 460+99 (NB)
STA. 461+77 (NB)

BRIDGE
SN 099-4616
STA. 460+99 (SB)
STA. 461+79 (SB)

BRIDGE
SN 099-0017
STA. 418+18 (NB)
STA. 420+06 (NB)

BRIDGE
SN 099-0016
STA. 418+19 (SB)
STA. 420+05 (SB)

BRIDGE
SN 099-0018
STA. 439+81 (NB)
STA. 441+42 (NB)

BRIDGE
SN 099-0019
STA. 439+82 (SB)
STA. 441+43 (SB)

MODEL: MODELNAM1ES
FILE: MODEL: ST1E1S



USER NAME = sUSERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = sSCALEs	CHECKED -	REVISED -
PLOT DATE = sDATEs	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

KEY MAP			
I-55 (SOUTH OF I-80 TO WEBER ROAD)			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	2
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

SHEET INDEX

Sheet No.	Sheet Description
1	Cover Sheet
2	Key Map
3	General Notes and Index of Sheets
4 - 8	Summary of Quantities
9 - 13	Core Sheets
14 - 15	Typical Sections
16	Mix Table
17 - 24	Suggested NB Maintenance of Traffic
25 - 33	Suggested SB Maintenance of Traffic
34 - 41	NB Roadway Plans
42 - 48	SB Roadway Plans
49	Roadway Details
50 - 58	Traffic Systems Plans
59 - 73	S.N. 099-0017 I-55 NB Over US 30
74 - 87	S.N. 099-0018 I-55 NB Over WCL RR
88 - 95	S.N. 099-4615 I-55 NB Over Mink Creek (0.6 Miles N of US 30)
96 - 113	S.N. 099-0022 I-55 NB Over Material Service RR (Abandoned)
114 - 128	S.N. 099-0016 I-55 SB Over US 30
129 - 143	S.N. 099-0019 I-55 SB Over WCL RR
144 - 150	S.N. 099-4616 I-55 SB Over Mink Creek (0.8 Miles N of US 30)
151 - 168	S.N. 099-0023 I-55 SB Over Material Service RR (Abandoned)
169	Pavement Patching for HMA Surfaced Pavement (BD-22)
170	Butt Joint HMA Taper Details (BD-32)
171	Entrance and Exit Ramp Closure Details (TC-08)
172	Traffic Control Details for Freeway Single & Mulit-Lane Weave (TC-09)
173	Typical Applications Raised Reflective Pavement Markers (Snow-Plow Resistant) (TC-11)
174	Multi-Lane Freeway Pavement Marking Details (TC-12A)
175	Multi-Lane Freeway Pavement Marking Details (TC-12B)
176	District One Typical Pavement Markings (TC-13)
177	Traffic Control Details for Freeway Shoulder Closures and Partial Ramp Closures (TC-17)
178	Freeway/Expressway Signing for Flagging Operations at Work Zone Openings on Freeways/Expressways (TC-18)

STATE STANDARDS

000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001001-02	AREAS OF REINFORCEMENT BARS
001006	DECIMAL OF AN INCH AND OF A FOOT
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
420001-09	PAVEMENT JOINTS
442201-03	CLASS C AND D PATCHES
482011-03	HMA SHOULDER STRIPS/ SHOULDER WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS
642001-02	SHOULDER RUMBLE STRIPS, 16 IN.
701001-02	OFF-RD OPERATIONS, 2L, 2W, MORE THAN 15' (4.5 m) AWAY
701106-02	OFF-RD OPERATIONS, MULTILANE, MORE THAN 15' (4.5 m) AWAY
701201-05	LANE CLOSURE, 2L, 2W, DAY ONLY, FOR SPEEDS > 45 MPH
701400-10	APPROACH TO LANE CLOSURE, FREEWAY/EXPRESSWAY
701401-12	LANE CLOSURE, FREEWAY/EXPRESSWAY
701411-09	LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS ≥ 45 MPH
701428-01	TRAFFIC CONTROL SETUP AND REMOVAL FREEWAY/EXPRESSWAY
701446-11	TWO LANE CLOSURE FREEWAY/EXPRESSWAY
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701901-08	TRAFFIC CONTROL DEVICES
704001-08	TEMPORARY CONCRETE BARRIER
780001-05	TYPICAL PAVEMENT MARKINGS (*ONLY USE FOR LANE DROP ARROW*)
782006-01	GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS

GENERAL NOTES

- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "I.U.L.I.E." AT 1-800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED).
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGES OF ROMEVILLE, PLAINFIELD, CRYSTAL LAWN, WILL COUNTY, AND SHOREWOOD.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE DISTRICT ONE "BUTT JOINT AND HMA TAPER DETAILS" (BD-32) AND DETAIL HEREIN.
- ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE PLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- THE RESIDENT ENGINEER SHALL VERIFY THE LOCATIONS OF ALL EXISTING PAVEMENT MARKINGS, PAVEMENT PATCHING, OR DRAINAGE ADJUSTMENT PRIOR TO MILLING OR RESURFACING.
- THE RESIDENT ENGINEER SHALL CONTACT THE EXPRESSWAY FIELD ENGINEER AT (847) 705-4153 TWO WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- THE RESIDENT ENGINEER SHALL CONTACT THE EXPRESSWAY TRAFFIC CONTROL SUPERVISOR AT (847) 705-4155 A MINIMUM OF 72 HOURS PRIOR TO THE INSTALLATION OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.
- ALL EXTRA EXCAVATED SOIL SHALL BE PLACED WITHIN IDOT RIGHT-OF-WAY, WITHIN PROJECT LIMITS.
- MISCELLANEOUS PAY ITEMS HAVE BEEN INCLUDED FOR THE RESIDENT ENGINEER TO USE AT THEIR DISCRETION. ITEMS INCLUDE AGGREGATE WEDGE SHOULDER AND GRADING AND SHAPING OF SHOULDERS
- IDOT FACILITIES ARE NOT LOCATED BY JULIE OR DIGGER. IDOT ELECTRICAL FACILITIES INCLUDING ROADWAY LIGHTING, FIBER OPTIC, ITS EQUIPMENT, TRAFFIC SIGNAL AND PUMP STATION FACILITIES ARE LOCATED BY THE DEPARTMENT'S ELECTRICAL MAINTENANCE CONTRACTOR. AS OF THE LETTING DATE, CONTACT THE MEADE ELECTRIC COMPANY AT 773-287-7672.
- ALL STAGE CHANGES REQUIRING THE STOPPING AND/OR PACING OF TRAFFIC SHALL TAKE PLACE DURING THE ALLOWABLE HOURS FOR FULL EXPRESSWAY CLOSURES AND SHALL BE APPROVED BY THE DEPARTMENT. THE CONTRACTOR SHALL NOTIFY THE DISTRICT ONE EXPRESSWAY TRAFFIC CONTROL SUPERVISOR AT LEAST 3 WORKING DAYS (WEEKENDS AND HOLIDAYS DO NOT COUNT IN THIS 72 HOUR NOTIFICATION) IN ADVANCE OF ANY PROPOSED STAGE CHANGE.
- A MAINTENANCE OF TRAFFIC CONTROL PLAN SHALL BE SUBMITTED TO THE DISTRICT ONE EXPRESSWAY TRAFFIC CONTROL SUPERVISOR, 14 DAYS IN ADVANCE OF ANY STAGE CHANGES OR FULL EXPRESSWAY CLOSURES. THE MAINTENANCE OF TRAFFIC PLAN SHALL INCLUDE, BUT NOT LIMITED TO: LANE AND RAMP CLOSURES, EXISTING GEOMETRICS, AND EQUIPMENT AND MATERIAL LOCATION.
- THE CONTRACTOR SHALL REQUEST AND GAIN APPROVAL FROM THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S EXPRESSWAY TRAFFIC OPERATIONS ENGINEER AT WWW.IDOTLCS.COM TWENTY -FOUR (24) HOURS IN ADVANCE OF ALL DAILY LANE, RAMP, AND SHOULDER CLOSURES AND 7 DAYS IN ADVANCE OF ALL PERMANENT AND WEEKEND CLOSURES ON ALL FREEWAYS AND/OR EXPRESSWAYS IN DISTRICT ONE. THIS ADVANCE NOTIFICATION IS CALCULATED BASED ON A WORK-WEEK OF MONDAY THROUGH FRIDAY AND SHALL NOT INCLUDE WEEKENDS OR HOLIDAYS.
- LONGITUDINAL PARTIAL DEPTH REMOVAL, 2 INCH (X4405020) AND LONGITUDINAL PARTIAL DEPTH PATCHING (X4420900) SHALL BE 3 FOOT IN WIDTH. THIS SUPERCEDES THE RECURRING SPECIAL PROVISION'S STATED WIDTH.
- PATCHING WORK TO TAKE PLACE OVERNIGHT AND LANE CLOSURES SHALL BE OPEN FOR DAY TIME TRAFFIC.
- PAVING UP TO BRIDGES SN 099-0017, SN 099-0018, SN 099-4615, SN 099-0022, SN 099-0016, SN 099-0019, SN 099-4616, AND SN 099-0023 SHOULD FOLLOW THE APPROACH PAVEMENT HMA OVERLAY DETAIL FOR TAPERS AT THE BRIDGE APPROACH SLABS. ALL OTHER BRIDGE SEGMENTS SHALL BE OMITTED.
- CLASS D, TYPE II AND TYPE III, 16 INCH PATCHING SHALL BE FULL DEPTH. VARIOUS AREAS HAVE EXISTING PCC BASE. CONTRACTOR TO REMOVE PCC OR HMA BASE AND INSTALL CLASS D PATCH AS DIRECTED THE RESIDENT ENGINEER.

THE FIRST OPERATION WILL BE THE FULL DEPTH CLASS D, BINDER COURSE PATCH. FOLLOWING THE BINDER INSTALLATION, THE CONTRACTOR SHALL MILL OFF 2" OF THE BINDER COURSE PATCH (PAID AS HMA SURFACE REMOVAL 2 INCH) AND INSTALL 2" OF SURFACE COURSE (PAID AS HMA SURFACE COURSE, MIX "D" IL-9.5, N70).
- HOT-MIX ASPHALT SHALL NOT COVER ANY BRIDGE DRAIN.
- THE PAY ITEM DRAINAGE STRUCTURES TO BE ADJUSTED HAS BEEN PROVIDED TO ADJUST THE DRAINAGE STRUCTURES ALONG THE MEDIAN BARRIER WALL IN THE AREA OF THE HMA BUTT JOINT ADJACENT TO BRIDGE APPROACH SLAB. DRAINAGE STRUCTURES ARE TO BE ADJUSTED AFTER MILLING TO MILLED GRADE TO PREVENT PONDING WATER. ADDITIONAL QUANTITY HAS BEEN PROVIDED TO ADJUST THE DRAINAGE STRUCTURE TO THE PROPOSED BUTT JOINT GRADE.
- INSTALL INLET FILTERS IN DRAINAGE STRUCTURES IN BUTT JOINT AREA ALONG MEDIAN BARRIER WALL ADJACENT TO BRIDGE APPROACH SLABS. REMOVE INLET FILTERS AFTER OVERLAY AND BUTT JOINT WORK IS COMPLETED.
- MONODIRECTIONAL REFLECTORS, TYPE C, SHALL BE INSTALLED ON TEMPORARY BARRIER WALLS AND BRIDGE PARAPET WALLS AT EACH BRIDGE IN EACH STAGE. REFLECTORS SHALL BE INSTALLED PER STANDARDS 704001 AND 782006. REFLECTORS SHALL BE YELLOW ON THE LEFT SIDE OF TRAFFIC FLOW AND WHITE ON THE RIGHT. NORTHBOUND AND SOUTHBOUND.
- ALL TEMPORARY PAVEMENT MARKINGS SHALL BE PAVEMENT MARKING TAPE, TYPE IV.

MODEL: \\MODELS\BAMES
FILE: \\MODELS\FILES



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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GENERAL NOTES AND INDEX OF SHEETS
I-55 (SOUTH OF I-80 TO WEBER ROAD)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	3
CONTRACT NO. 62N22				
ILLINOIS		FED. AID PROJECT		

CODE NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	ROADWAY	BRIDGE	BRIDGE	ROADWAY							
				0005	0047	0047	0047	0047	0047	0047	0047	0047	0047	0047
				90% FED 10% STATE	SN 099-0016 90% FED 10% STATE	SN 099-0019 90% FED 10% STATE	SN 099-4616 90% FED 10% STATE	SN 099-0023 90% FED 10% STATE	SN 099-0017 90% FED 10% STATE	SN 099-0018 90% FED 10% STATE	SN 099-4615 90% FED 10% STATE	SN 099-0022 90% FED 10% STATE		100% STATE
20200100	EARTH EXCAVATION	CU YD	2,803	2,803										
28000510	INLET FILTERS	EACH	12	12										
31101200	SUBBASE GRANULAR MATERIAL, TYPE B 4"	SQ YD	14,417	14,417										
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	32,407	21,157										11,250
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	50	50										
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	8,991	8,991										
40604062	HOT-MIX ASPHALT, SURFACE COURSE, MIX "D", IL-9.5, N70	TON	3,437	595										2,842
40605026	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE STONE MATRIX ASPHALT, 9.5, MIX "F", N80	TON	1,450	1,450										
44000157	HOT-MIX ASPHALT SURFACE REMOVAL, 2"	SQ YD	30,236	5,236										25,000
44004250	PAVED SHOULDER REMOVAL	SQ YD	14,417	14,417										
44201839	CLASS D PATCHES, TYPE II, 16 INCH	SQ YD	800											800
44201843	CLASS D PATCHES, TYPE III, 16 INCH	SQ YD	200											200
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	334	334										
48203037	HOT-MIX ASPHALT SHOULDERS, 10"	SQ YD	14,417	14,417										
50102400	CONCRETE REMOVAL	CU YD	102.7		16.2	19.3		14.8	16.1	20.3			16.0	
50157300	PROTECTIVE SHIELD	SQ YD	2,203		656	468			656	423				
50300255	CONCRETE SUPERSTRUCTURE	CU YD	111.4		17.9	20.8		16.8	17.4	20.3			18.2	

MODEL NUMBER: 62N22
FILE NAME: 62N22



USER NAME = \$USERS	DESIGNED -	REVISED -
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PLOT DATE = \$DATES	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES
I-55 (SOUTH OF I-80 TO WEBER ROAD)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	4
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62N22	

CODE NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	ROADWAY	BRIDGE	BRIDGE	ROADWAY							
				0005	0047	0047	0047	0047	0047	0047	0047	0047	0047	0047
				90% FED 10% STATE	SN 099-0016 90% FED 10% STATE	SN 099-0019 90% FED 10% STATE	SN 099-4616 90% FED 10% STATE	SN 099-0023 90% FED 10% STATE	SN 099-0017 90% FED 10% STATE	SN 099-0018 90% FED 10% STATE	SN 099-4615 90% FED 10% STATE	SN 099-0022 90% FED 10% STATE	100% STATE	
50300260	BRIDGE DECK GROOVING	SQ YD	6,760		1,092	1,205	448	665	1,092	1,140	454	664		
50300300	PROTECTIVE COAT	SQ YD	8,639		1,457	1,497	539	811	1,457	1,525	539	814		
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	17,050		2,220	3,780		2,740	2,220	3,490		2,600		
50800515	BAR SPLICERS	EACH	156		24	28		26	24	28		26		
52000030	PREFORMED JOINT SEAL 2 1/2"	FOOT	733		249	221	78	185						
52000110	PREFORMED JOINT STRIP SEAL	FOOT	728		115	141		110	115	137		110		
58700300	CONCRETE SEALER	SQ FT	1,988		333	407		254	333	407		254		
59000200	EPOXY CRACK INJECTION	FOOT	28			20				8				
64200116	SHOULDER RUMBLE STRIPS, 16 INCH	FOOT	26,282	26,282										
67100100	MOBILIZATION	L SUM	1	1										
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	1,440	1,440										
70200100	NIGHTTIME WORK ZONE LIGHTING	L SUM	1	1										
70300904	PAVEMENT MARKING TAPE, TYPE IV, 4"	FOOT	124,442	107,202									17,240	
70300905	PAVEMENT MARKING TAPE, TYPE IV, 5"	FOOT	91,571	19,691									71,880	
70300908	PAVEMENT MARKING TAPE, TYPE IV, 8"	FOOT	6,000	5,000									1,000	
70400100	TEMPORARY CONCRETE BARRIER	FOOT	3,889	3,889										
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	3,723	3,723										

MODEL NUMBER
FILE NAME: 62N22



8501 W. Higgins Road, Suite 280
Chicago, Illinois 60631
(773) 398-0112

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES
I-55 (SOUTH OF I-80 TO WEBER ROAD)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	5
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

CODE NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	ROADWAY	BRIDGE	BRIDGE	ROADWAY							
				0005	0047	0047	0047	0047	0047	0047	0047	0047	0047	0005
				90% FED 10% STATE	SN 099-0016 90% FED 10% STATE	SN 099-0019 90% FED 10% STATE	SN 099-4616 90% FED 10% STATE	SN 099-0023 90% FED 10% STATE	SN 099-0017 90% FED 10% STATE	SN 099-0018 90% FED 10% STATE	SN 099-4615 90% FED 10% STATE	SN 099-0022 90% FED 10% STATE		100% STATE
70600270	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, WIDE), TEST LEVEL 3	EACH	8	8										
70600330	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE), TEST LEVEL 3	EACH	8	8										
* 78004220	PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - INLAID - LINE 5"	FOOT	1,019	1,019										
* 78009004	MODIFIED URETHANE PAVEMENT MARKING - LINE 4"	FOOT	96,002	78,762										17,240
* 78009005	MODIFIED URETHANE PAVEMENT MARKING - LINE 5"	FOOT	91,571	19,691										71,880
* 78009008	MODIFIED URETHANE PAVEMENT MARKING - LINE 8"	FOOT	6,000	5,000										1,000
* 78009012	MODIFIED URETHANE PAVEMENT MARKING - LINE 12"	FOOT	3,379	3,379										
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	424	424										
* 78200011	BARRIER WALL REFLECTORS, TYPE C	EACH	608	608										
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	360	360										
78300202	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	41,171	41,171										
X0326766	CLEAN & RESEAL RELIEF JOINT	FOOT	959	959										
X1400337	WIRELESS IN PAVEMENT DETECTOR	EACH	5	5										
X2020110	GRADING AND SHAPING SHOULDERS	UNIT	11	11										
* X2700027	PREFORMED PLASTIC PAVEMENT MARKING, TYPE D - LINE 8", CONTRAST	FOOT	1,019	1,019										
X4405020	LONGITUDINAL PARTIAL DEPTH REMOVAL, 2"	FOOT	43,100											43,100
X4420900	LONGITUDINAL PARTIAL DEPTH PATCHING	TON	1,609											1,609

* SPECIALTY ITEM

MODEL NUMBER
FILE NAME: ST113



8501 W. Higgins Road, Suite 280
Chicago, Illinois 60631
(773) 398-0112

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
I-55 (SOUTH OF I-80 TO WEBER ROAD)

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	6
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

REV-SEP

CODE NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	ROADWAY	BRIDGE	BRIDGE	ROADWAY							
				0005	0047	0047	0047	0047	0047	0047	0047	0047	0047	0047
				90% FED 10% STATE	SN 099-0016 90% FED 10% STATE	SN 099-0019 90% FED 10% STATE	SN 099-4616 90% FED 10% STATE	SN 099-0023 90% FED 10% STATE	SN 099-0017 90% FED 10% STATE	SN 099-0018 90% FED 10% STATE	SN 099-4615 90% FED 10% STATE	SN 099-0022 90% FED 10% STATE		100% STATE
X6700410	ENGINEER'S FIELD OFFICE, TYPE A (SPECIAL)	CAL MO	12	12										
X7011015	TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)	L SUM	1	1										
X7013820	TRAFFIC CONTROL SURVEILLANCE, EXPRESSWAYS	CAL DA	180	180										
X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SQ FT	83,485	83,485										
X7830076	GROOVING FOR RECESSED PAVEMENT MARKING 9"	FOOT	1,019	1,019										
X8570001	SMART TRAFFIC MONITORING SYSTEM	CAL DA	180	180										
* X8730312	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 18 4/C. TWISTED, SHIELDED	FOOT	100	100										
* X8850102	INDUCTION LOOP	FOOT	950	950										
Z0001800	APPROACH SLAB REPAIR (PARTIAL DEPTH)	SQ YD	18		1	6	1	1	4	2	2	1		
Z0006016	BRIDGE DECK LATEX CONCRETE OVERLAY, 2 3/4 INCHES	SQ YD	6,833		1,108	1,208	464	669	1,108	1,144	464	668		
Z0012130	BRIDGE DECK SCARIFICATION 3/4"	SQ YD	6,833		1,108	1,208	464	669	1,108	1,144	464	668		
Z0012754	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	237		58.0	80.0		30.0	6.0	39.8	5.0	18.0		
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1										
Z0018400	DRAINAGE STRUCTURES TO BE ADJUSTED	EACH	24	24										
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	206	206										
Z0041895	POLYMER CONCRETE	CU FT	18				9				9			

* SPECIALTY ITEM

MODEL: 4400E14MMES
FILE NAME: STELE2



USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATE\$	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES			
I-55 (SOUTH OF I-80 TO WEBER ROAD)			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	7
			CONTRACT NO. 62N22	
		ILLINOIS FED. AID PROJECT		

REV-SEP

CODE NO.	DESCRIPTION	UNIT	TOTAL QUANTITY	ROADWAY 0005 QUANTITY	BRIDGE 0047							
				90% FED 10% STATE	SN 099-0016 90% FED 10% STATE	SN 099-0019 90% FED 10% STATE	SN 099-4616 90% FED 10% STATE	SN 099-0023 90% FED 10% STATE	SN 099-0017 90% FED 10% STATE	SN 099-0018 90% FED 10% STATE	SN 099-4615 90% FED 10% STATE	SN 099-0022 90% FED 10% STATE
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	1								
X1300003	CLEAN APPROACH SLAB DRAIN	EACH	1						1			
X1700115	NOISE ABATEMENT WALL PANEL REMOVAL AND RE-ERECTION	EACH	2									2
Ø Z0076600	TRAINEES	HOURS	500	500								
Ø Z0076604	TRAINEES - TRAINING PROGRAM GRADUATE	HOURS	500	500								

Ø 0042

REV-SEP

MODEL: MODELNAME\$
FILE: NAME\$.PLOT\$



USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATE\$	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES			
I-55 (SOUTH OF I-80 TO WEBER ROAD)			
SCALE:	SHEET	OF	SHEETS
	STA.		TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	8
CONTRACT NO. 62N22			ILLINOIS FED. AID PROJECT	

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Geo Services, Inc.
Geotechnical, Environmental & Civil Engineering
805 Amherst Court, Suite 204
Naperville, Illinois 60565
(630) 355-2838

PAVEMENT CORE SUMMARY

Project: IDOT PTB 193: Work Order #3: GSI Job No.: 19120-B
 Location: I-55 NB inside Shoulder (South of I-80 to Weber Road) Date: 3/23/2020
 County: Cook Cored By: RT
 Client: GRAEF Checked By: AJP

CORE NO.	THICKNESS (in.)	MATERIAL DESCRIPTION
C1-1	1.75 1.75 3.0 3.5 4.0	Station: 92+78 Offset: 4.7' Right Total Thickness: 14.0"
		Location: I-55 NB inside Shoulder
		ASPHALT-slightly porous, fine to medium coarse aggregate. (weathered @ surface, high AC% slag mix)
		ASPHALT-well consolidated, medium coarse aggregate.
		ASPHALT-slightly porous, poorly consolidated, medium coarse aggregate. (poorly bonding @ base)
C3-1	2.0 2.0 3.0 3.5 4.5	Station: 295+05 Offset: 4.7' Right Total Thickness: 15.0"
		Location: I-55 NB inside Shoulder
		ASPHALT-porous, poorly consolidated, fine to medium coarse aggregate. (weathered @ surface, high AC% slag mix)
		ASPHALT-porous, very poorly consolidated @ base, medium coarse aggregate. (poorly bonding @ base)
		ASPHALT-porous, very poorly consolidated, medium coarse aggregate. (poorly bonding @ base)
C6-1	2.0 1.5 2.5 4.0 4.5	Station: 497+26 Offset: 4.7' Right Total Thickness: 14.5"
		Location: I-55 NB inside Shoulder
		ASPHALT-slightly porous, fine to medium coarse aggregate. (weathered @ surface, high AC% slag mix)
		ASPHALT-porous, very poorly consolidated, medium coarse aggregate.
		ASPHALT-slightly porous, medium coarse aggregate. (poorly bonding @ base)
C9-1	2.0 2.0 2.5 4.0 3.0 5.0	Station: 624+92 Offset: 4.8' Right Total Thickness: 18.5"
		Location: I-55 NB inside Shoulder
		ASPHALT-slightly porous, fine to medium coarse aggregate. (weathered @ surface, high AC% slag mix)
		ASPHALT-slightly porous, medium coarse aggregate.
		ASPHALT-porous, very poorly consolidated, medium coarse aggregate.

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805 Amherst Court, Suite 204
Naperville, Illinois 60565
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PAVEMENT CORE SUMMARY

Project: IDOT PTB 193: Work Order #3: GSI Job No.: 19120-B
 Location: I-55 NB Lane 1 (South of I-80 to Weber Road) Date: 3/23/2020
 County: Cook Cored By: RT
 Client: GRAEF Checked By: AJP

CORE NO.	THICKNESS (in.)	MATERIAL DESCRIPTION
C1-2	2.0 2.0 1.0 2.5 3.0 4.0	Station: 92+78 Offset: 14.0' Right Total Thickness: 14.5"
		Location: I-55 NB Lane 1
		ASPHALT-slightly porous, fine to medium coarse aggregate. (weathered @ surface, high AC% slag mix)
		ASPHALT-well consolidated, medium coarse aggregate.
		ASPHALT-slightly porous, poorly consolidated, medium coarse aggregate. (levelling course)
C3-2	1.75 1.25 2.50 2.50 3.0 3.0	Station: 295+05 Offset: 14.0' Right Total Thickness: 14.0"
		Location: I-55 NB Lane 1
		ASPHALT-slightly porous, fine to medium coarse aggregate. (weathered @ surface, high AC% slag mix)
		ASPHALT-slightly porous, poorly consolidated @ base, medium coarse aggregate.
		ASPHALT-slightly porous, medium coarse aggregate.
C6-2	2.0 1.0 3.0 4.0 2.0 2.0	Station: 497+26 Offset: 14.0' Right Total Thickness: 14.0"
		Location: I-55 NB Lane 1
		ASPHALT-slightly porous, fine to medium coarse aggregate. (weathered @ surface, high AC% slag mix)
		ASPHALT-slightly porous, fine to medium coarse aggregate.
		ASPHALT-slightly porous, medium coarse aggregate.
C9-2	2.0 1.5 3.0 4.5 4.0 4.0	Station: 624+92 Offset: 14.0' Right Total Thickness: 19.0"
		Location: I-55 NB Lane 1
		ASPHALT-slightly porous, fine to medium coarse aggregate. (weathered @ surface, high AC% slag mix)
		ASPHALT-well consolidated, medium coarse aggregate.
		ASPHALT-well consolidated, medium coarse aggregate.

 MODEL: SMOBELNAMES
 FILE: SMOBELNAMES_SHEETS


USER NAME = SUSERS	DESIGNED -	REVISED -
PLOT SCALE = SCALES	DRAWN -	REVISED -
PLOT DATE = SDATES	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY CORES	
I-55 (SOUTH OF I-80 TO WEBER ROAD)	
SCALE:	SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	9
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

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Geo Services, Inc.
Geotechnical, Environmental & Civil Engineering
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PAVEMENT CORE SUMMARY

Project: IDOT PTB 193: Work Order #3: GSI Job No.: 19120-B
 Location: I-55 NB Lane 3 (South of I-80 to Weber Road) Date: 3/16/2020
 County: Cook Cored By: RT
 Client: GRAEF Checked By: AJP

CORE NO.	THICKNESS (in.)	MATERIAL DESCRIPTION
C2-1	1.75 2.0 1.0 2.0 10.5	Station: 198+06 Offset: 38.0' Right Total Thickness: 17.25"
		Location: I-55 NB Lane 3
		ASPHALT-slightly porous, fine to medium coarse aggregate. (weathered @ surface, high AC% slag mix)
		ASPHALT-well consolidated, fine to medium coarse aggregate.
		ASPHALT-well consolidated, medium coarse aggregate. (levelling course)
C4-1	1.75 1.75 2.5 2.0 10.0	Station: 383+28 Offset: 38.0' Right Total Thickness: 18.0"
		Location: I-55 NB Lane 3
		ASPHALT-slightly porous, fine to medium coarse aggregate. (weathered @ surface, high AC% slag mix)
		ASPHALT-well consolidated, fine to medium coarse aggregate.
		ASPHALT-well consolidated, medium coarse aggregate.
C5-1	1.75 1.75 1.50 1.50 11.25	Station: 456+88 Offset: 38.0' Right Total Thickness: 17.75"
		Location: I-55 NB Lane 3
		ASPHALT-slightly porous, fine to medium coarse aggregate. (weathered @ surface, high AC% slag mix)
		ASPHALT-well consolidated, medium coarse aggregate.
		ASPHALT-well consolidated, medium coarse aggregate.
C7-1	1.75 1.75 2.5 10.5	Station: 537+17 Offset: 38.4' Right Total Thickness: 16.5"
		Location: I-55 NB Lane 3
		ASPHALT-slightly porous, fine to medium coarse aggregate. (weathered @ surface, high AC% slag mix)
		ASPHALT-well consolidated, fine to medium coarse aggregate.
		ASPHALT-well consolidated, medium coarse aggregate.
C8-1	2.0 1.0 1.0 3.0 10.5	Station: 586+65 Offset: 38.0' Right Total Thickness: 17.5"
		Location: I-55 NB Lane 3
		ASPHALT-slightly porous, fine to medium coarse aggregate. (weathered @ surface, high AC% slag mix)
		ASPHALT-well consolidated, fine to medium coarse aggregate.
		ASPHALT-well consolidated, fine to medium coarse aggregate. (levelling course)
C10-1	2.0 1.75 1.75 10.0	Station: 656+10 Offset: 38.4' Right Total Thickness: 15.5"
		Location: I-55 NB Lane 3
		ASPHALT-slightly porous, fine to medium coarse aggregate. (weathered @ surface, high AC% slag mix)
		ASPHALT-well consolidated, medium coarse aggregate.
		ASPHALT-well consolidated, medium coarse aggregate.

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PAVEMENT CORE SUMMARY

Project: IDOT PTB 193: Work Order #3: GSI Job No.: 19120-B
 Location: I-55 NB outside Shoulder (South of I-80 to Weber Road) Date: 3/17/2020
 County: Cook Cored By: RT
 Client: GRAEF Checked By: AJP

CORE NO.	THICKNESS (in.)	MATERIAL DESCRIPTION
C2-2	1.75 1.25 2.5 4.0 6.0 15.5+	Station: 198+06 Offset: 45.0' Right Total Thickness: 9.5"
		Location: I-55 NB outside Shoulder @ 3.0' from inside EOP
		ASPHALT-slightly porous, fine to medium coarse aggregate. (weathered @ surface)
		ASPHALT-slightly porous, fine to medium coarse aggregate.
		ASPHALT-slightly porous, medium coarse aggregate.
C2-3	1.5 2.0 3.0 3.5 6.0 16.0+	Station: 198+06 Offset: 53.0' Right Total Thickness: 10.0"
		Location: I-55 NB outside Shoulder @ 1.0' from outside EOP
		ASPHALT-porous, very poorly consolidated, fine to medium coarse aggregate. (weathered @ surface) (vertical fracture poorly bonding @ base)
		ASPHALT-porous, very poorly consolidated @ top, medium coarse aggregate.
		ASPHALT-porous, very poorly consolidated, medium coarse aggregate. (deteriorated-poorly bonding @ base)
C4-2	2.0 1.5 2.5 3.5 9.5+	Station: 383+28 Offset: 47.0' Right Total Thickness: 9.5"
		Location: I-55 NB outside Shoulder @ 3.0' from inside EOP
		ASPHALT-slightly porous, fine to medium coarse aggregate. (weathered @ surface)
		ASPHALT-slightly porous, fine to medium coarse aggregate. (poorly bonding @ base)
		ASPHALT-well consolidated, medium coarse aggregate.
C4-3	2.5 2.5 2.0 7.0+	Station: 383+28 Offset: 53.3' Right Total Thickness: 7.0"
		Location: I-55 NB outside Shoulder @ 1.0' from outside EOP
		ASPHALT-porous, very poorly consolidated, fine to medium coarse aggregate. (weathered @ surface) (vertical fracture poorly bonding @ base)
		ASPHALT-porous, very poorly consolidated, medium coarse aggregate. (deteriorated-poorly bonding @ top)
		ASPHALT-porous, very poorly consolidated @ base, medium coarse aggregate. (deteriorated)
C5-2	2.0 4.5 8.0 14.5+	Station: 456+88 Offset: 47.0' Right Total Thickness: 6.5"
		Location: I-55 NB outside Shoulder @ 3.0' from inside EOP
		ASPHALT-porous, very poorly consolidated, fine to medium coarse aggregate. (weathered @ surface)
		ASPHALT-porous, very poorly consolidated, medium coarse aggregate.
		CRUSHED LIME STONE-Subbase (Apparent CA-06)
C5-3	2.0 3.0 3.5 8.0 16.5+	Station: 456+88 Offset: 63.2' Right Total Thickness: 8.5"
		Location: I-55 NB outside Shoulder @ 1.0' from outside EOP
		ASPHALT-slightly porous poorly consolidated, fine to medium coarse aggregate. (weathered @ surface)
		ASPHALT-slightly porous poorly consolidated, fine to medium coarse aggregate.
		ASPHALT-slightly porous poorly consolidated, medium coarse aggregate.

MODEL, MODIFICATIONS, FILE NUMBER, SHEETS



USER NAME = SUSERS	DESIGNED -	REVISED -
PLOT SCALE = SCALES	DRAWN -	REVISED -
PLOT DATE = SDATES	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY CORES	
I-55 (SOUTH OF I-80 TO WEBER ROAD)	
SCALE:	SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	10
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

Page: 2 of 2



Geo Services, Inc.
Geotechnical, Environmental & Civil Engineering
805 Amherst Court, Suite 204
Naperville, Illinois 60565
(630) 355-2838

PAVEMENT CORE SUMMARY

Project: IDOT PTB 193: Work Order #3: GSI Job No.: 19120-B
 Location: I-55 NB outside Shoulder (South of I-80 to Weber Road) Date: 3/17/2020
 County: Cook Cored By: RT
 Client: GRAEF Checked By: AJP

CORE NO.	THICKNESS (in.)	MATERIAL DESCRIPTION
C10-2		Station: 656+10 Offset: 47.7' Right Total Thickness: 7.0'
		Location: I-55 NB outside Shoulder @ 3.0' from inside EOP
	2.0	ASPHALT-slightly porous poorly consolidated, fine to medium coarse aggregate. (weathered @ surface)
	2.0	ASPHALT-porous very poorly consolidated, medium coarse aggregate. (diagonal crack)
	3.0	ASPHALT-slightly porous poorly consolidated, medium coarse aggregate. (diagonal crack)
	6.0	SAND & GRAVEL-Subbase
	13.0+	CLAY-Subgrade
C10-3		Station: 656+10 Offset: 54.2' Right Total Thickness: 6.0'
		Location: I-55 NB outside Shoulder @ 1.0' from outside EOP
	2.0	ASPHALT-slightly porous poorly consolidated, fine to medium coarse aggregate. (weathered @ surface)
	1.5	ASPHALT-well consolidated, medium coarse aggregate.
	2.5	ASPHALT-porous very poorly consolidated, medium coarse aggregate.(deteriorated)
	6.0	SAND & GRAVEL-Subbase
	12.0+	CLAY-Subgrade

 MODEL: MODELNAME
 FILE NUMBER: FILE#



8501 W. Higgins Road, Suite 280
Chicago, Illinois 60631
(773) 398-0112

USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATE\$	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY CORES
I-55 (SOUTH OF I-80 TO WEBER ROAD)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	11
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

Page: 1 of 1



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Geotechnical, Environmental & Civil Engineering
805 Amherst Court, Suite 204
Naperville, Illinois 60565
(630) 355-2838

PAVEMENT CORE SUMMARY

Project: IDOT PTB 193: Work Order #3: GSI Job No.: 19120-B
 Location: I-55 SB Lane 1 from South of I-80 to Weber Road Date: 3/23/2020
 County: Cook Cored By: RT
 Client: GRAEF Checked By: AJP

CORE NO.	THICKNESS (in.)	MATERIAL DESCRIPTION
C11-1		Station: 34+96 Offset: 14.3' Left Total Thickness: 23.0"
		Location: I-55 SB Lane 1
	2.0	ASPHALT-slightly porous, fine to medium coarse aggregate. (weathered @ surface) high AC% slag mix)
	1.5	ASPHALT-slightly porous, medium coarse aggregate.
	3.5	ASPHALT-slightly porous, medium coarse aggregate.
	4.0	ASPHALT-slightly porous, medium coarse aggregate.
C13-1		Station: 244+14 Offset: 13.6' Left Total Thickness: 14.0"
		Location: I-55 SB Lane 1
	2.0	ASPHALT-slightly porous, fine to medium coarse aggregate. (weathered @ surface) high AC% slag mix)
	1.75	ASPHALT-well consolidated, fine to medium coarse aggregate.
	3.75	ASPHALT-porous, very poorly consolidated, medium coarse aggregate.
	2.5	ASPHALT-slightly porous, poorly consolidated, medium coarse aggregate.
C15-1		Station: 437+41 Offset: 14.2' Left Total Thickness: 14.0"
		Location: I-55 SB Lane 1
	1.75	ASPHALT-slightly porous, fine to medium coarse aggregate. (weathered @ surface) high AC% slag mix)
	1.25	ASPHALT-slightly porous, medium coarse aggregate.
	1.0	ASPHALT-well consolidated, fine to medium coarse aggregate.
	2.50	ASPHALT-slightly porous, medium coarse aggregate.
C19-1		Station: 667+04 Offset: 14.0' Left Total Thickness: 18.5"
		Location: I-55 SB Lane 1
	1.5	ASPHALT-slightly porous, fine to medium coarse aggregate. (weathered @ surface) high AC% slag mix)
	2.0	ASPHALT-slightly porous, medium coarse aggregate.
	2.5	ASPHALT-slightly porous, medium coarse aggregate.
	4.0	ASPHALT-porous, poorly consolidated, fine to medium coarse aggregate.

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PAVEMENT CORE SUMMARY

Project: IDOT PTB 193: Work Order #3: GSI Job No.: 19120-B
 Location: I-55 SB Lane 3 (South of I-80 to Weber Road) Date: 3/16/2020
 County: Cook Cored By: RT
 Client: GRAEF Checked By: AJP

CORE NO.	THICKNESS (in.)	MATERIAL DESCRIPTION
C12-1		Station: 138+72 Offset: 38.0' Left Total Thickness: 19.0"
		Location: I-55 SB Lane 3
	1.75	ASPHALT-slightly porous, fine to medium coarse aggregate. (weathered @ surface) high AC% slag mix)
	1.50	ASPHALT-well consolidated, medium coarse aggregate.
	1.25	ASPHALT-well consolidated, fine to medium coarse aggregate. (levelling course)
	1.50	ASPHALT-slightly porous, poorly consolidated @ base, medium coarse aggregate. (poorly bonding)
C14-1		Station: 344+98 Offset: 38.0' Left Total Thickness: 18.0"
		Location: I-55 SB Lane 3
	2.0	ASPHALT-slightly porous, fine to medium coarse aggregate. (weathered @ surface, high AC% slag mix)
	3.0	ASPHALT-slightly porous, poorly consolidated, fine to medium coarse aggregate.
	1.0	ASPHALT-well consolidated, fine to medium coarse aggregate. (levelling course)
	2.25	ASPHALT-well consolidated, medium coarse aggregate.
C16-1		Station: 479+91 Offset: 38.0' Left Total Thickness: 18.0"
		Location: I-55 SB Lane 3
	1.50	ASPHALT-slightly porous, fine to medium coarse aggregate. (weathered @ surface) high AC% slag mix)
	2.50	ASPHALT-slightly porous, poorly consolidated, medium coarse aggregate.
	3.50	ASPHALT-slightly porous, poorly consolidated, medium coarse aggregate.
	10.50	CONCRETE-well consolidated-air entrained, 1/4" wire mesh @ 3.75"
C17-1		Station: 551+17 Offset: 38.4' Left Total Thickness: 17.0"
		Location: I-55 SB Lane 3
	2.0	ASPHALT-slightly porous, fine to medium coarse aggregate. (weathered @ surface, high AC% slag mix)
	1.5	ASPHALT-well consolidated, medium coarse aggregate.
	1.5	ASPHALT-well consolidated, medium coarse aggregate.
	2.0	ASPHALT-well consolidated, medium coarse aggregate.
C18-1		Station: 610+61 Offset: 42.1' Left Total Thickness: 18.75"
		Location: I-55 SB Lane 3
	1.75	ASPHALT-slightly porous, fine to medium coarse aggregate. (weathered @ surface, high AC% slag mix)
	1.75	ASPHALT-well consolidated, fine to medium coarse aggregate.
	1.0	ASPHALT-well consolidated, fine to medium coarse aggregate. (levelling course)
	4.0	ASPHALT-well consolidated, medium coarse aggregate. (horizontal fracture @ -3.0")
C20-1		Station: 686+06 Offset: 36.6' Left Total Thickness: 13.0"
		Location: I-55 SB Lane 3
	2.0	ASPHALT-slightly porous, fine to medium coarse aggregate. (weathered @ surface) high AC% slag mix)
	1.75	ASPHALT-well consolidated, fine to medium coarse aggregate.
	1.5	ASPHALT-well consolidated, medium coarse aggregate.
	3.0	ASPHALT-slightly porous, poorly consolidated, medium coarse aggregate. (horizontal fracture @ -1.0")

MODEL: SMOBELNAMES
FILE: SMOBELNAMES



USER NAME = SUSERS	DESIGNED -	REVISED -
PLOT SCALE = SCALES	DRAWN -	REVISED -
PLOT DATE = SDATES	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT CORES	
I-55 (SOUTH OF I-80 TO WEBER ROAD)	
SCALE:	SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	12
CONTRACT NO. 62N22				
		ILLINOIS	FED. AID PROJECT	

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805 Amherst Court, Suite 204
Naperville, Illinois 60565
(630) 355-2838

PAVEMENT CORE SUMMARY

Project: IDOT PTB 193: Work Order #3: GSI Job No.: 19120-B
 Location: I-55 SB Outside Shoulder from South of I-80 to Weber Road Date: 3/16/2020
 County: Cook Cored By: RT
 Client: GRAEF Checked By: AJP

CORE NO.	THICKNESS (in.)	MATERIAL DESCRIPTION
C12-2	1.75 1.0 3.0 2.5 1.75 6.0 16.0+	Station: 138+72 Offset: 47.0' Left Total Thickness: 10.0"
		Location: I-55 SB Outside Shoulder @ 3.0' from Inside E.O.P.
		ASPHALT-slightly porous, fine to medium coarse aggregate. (weathered @ surface)
		ASPHALT-slightly porous, poorly consolidated, medium coarse aggregate. (diagonal crack)
		ASPHALT-very poorly consolidated @ base, medium coarse aggregate. (vertical fracture-poorly bonding-deteriorated)
C12-3	1.75 1.0 3.75 6.0 12.5+	Station: 138+72 Offset: 52.9' Left Total Thickness: 6.5"
		Location: I-55 SB Outside Shoulder @ 1.0' from outside E.O.P.
		ASPHALT-slightly porous, fine to medium coarse aggregate. (weathered @ surface)
		ASPHALT-very poorly consolidated @ base, medium coarse aggregate. (horizontal fracture)
		ASPHALT-very poorly consolidated, medium coarse aggregate. (diagonal crack-poorly bonding-deteriorated)
C14-2	2.0 2.0 1.5 2.5 3.0 6.0 17.0+	Station: 344+98 Offset: 47.0' Left Total Thickness: 11.0"
		Location: I-55 SB Outside Shoulder @ 3.0' from Inside E.O.P.
		ASPHALT-slightly porous, fine to medium coarse aggregate. (weathered @ surface)
		ASPHALT-slightly porous, poorly consolidated @ base, medium coarse aggregate.
		ASPHALT-very poorly consolidated @ base, medium coarse aggregate. (poorly bonding)
		ASPHALT-very poorly consolidated @ top & base, medium coarse aggregate. (poorly bonding)
C14-3	1.5 1.0 3.5 6.0 12.0+	Station: 344+98 Offset: 53.4' Left Total Thickness: 6.0"
		Location: I-55 SB Outside Shoulder @ 1.0' from outside E.O.P.
		ASPHALT-slightly porous, poorly consolidated, fine to medium coarse aggregate. (weathered @ surface, vertical fracture)
		ASPHALT-very poorly consolidated, medium coarse aggregate. (deteriorated, poorly bonding)
		ASPHALT-very poorly consolidated, medium coarse aggregate. (deteriorated, poorly bonding)
C16-2	1.5 1.5 1.5 2.0 3.5 6.0 16.0+	Station: 479+91 Offset: 48.7' Left Total Thickness: 10.0"
		Location: I-55 SB Outside Shoulder @ 3.0' from Inside E.O.P.
		ASPHALT-very poorly consolidated, fine to medium coarse aggregate. (vertical fracture-poorly bonding-deteriorated)
		ASPHALT-very poorly consolidated, fine to medium coarse aggregate.
		ASPHALT-slightly porous, poorly consolidated @ base, medium coarse aggregate.
		ASPHALT-very poorly consolidated, medium coarse aggregate. (deteriorated, poorly bonding)

Page: 2 of 2



Geo Services, Inc.
Geotechnical, Environmental & Civil Engineering
805 Amherst Court, Suite 204
Naperville, Illinois 60565
(630) 355-2838

PAVEMENT CORE SUMMARY

Project: IDOT PTB 193: Work Order #3: GSI Job No.: 19120-B
 Location: I-55 SB Outside Shoulder from South of I-80 to Weber Road Date: 3/16/2020
 County: Cook Cored By: RT
 Client: GRAEF Checked By: AJP

CORE NO.	THICKNESS (in.)	MATERIAL DESCRIPTION
C16-3	2.0 1.5 4.0 6.0 13.5+	Station: 479+91 Offset: 53.3' Left Total Thickness: 7.5"
		Location: I-55 SB Outside Shoulder @ 1.0' from outside E.O.P.
		ASPHALT-slightly porous, poorly consolidated, fine to medium coarse aggregate. (weathered @ surface)
C20-2	1.5 1.5 3.0 2.5 2.75 6.0 17.25+	Station: 689+06 Offset: 48.1' Left Total Thickness: 11.25"
		Location: I-55 SB Outside Shoulder @ 3.0' from Inside E.O.P.
		ASPHALT-slightly porous, poorly consolidated, fine to medium coarse aggregate. (weathered @ surface)
		ASPHALT-well consolidated, fine to medium coarse aggregate.
		ASPHALT-very poorly consolidated @ base, medium coarse aggregate. (deteriorated, poorly bonding)
		ASPHALT-slightly porous, poorly consolidated @ base, medium coarse aggregate. (deteriorated, poorly bonding)
C20-3	2.5 2.5 3.5 3.5 6.0 18.0+	Station: 689+06 Offset: 54.0' Left Total Thickness: 12.0"
		Location: I-55 SB Outside Shoulder @ 1.0' from outside E.O.P.
		ASPHALT-slightly porous, poorly consolidated, fine to medium coarse aggregate. (weathered @ surface)
		ASPHALT-very poorly consolidated, medium coarse aggregate. (deteriorated, poorly bonding)
		ASPHALT-very poorly consolidated, medium coarse aggregate. (deteriorated, poorly bonding)

MODEL NUMBER: 11/15/11
FILE NAME: 3/16/20



USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATE\$	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT CORES			
I-55 (SOUTH OF I-80 TO WEBER ROAD)			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	13
JOB NO.		CONTRACT NO. 62N22		
ILLINOIS		FED. AID PROJECT		

LEGEND

- ① EXISTING HMA SURFACE COURSE
- ② EXISTING P.C.C. BASE COURSE
- ③ EXISTING AGGREGATE SUBGRADE
- ④ EXISTING HMA BASE COURSE
- ⑤ EXISTING HMA SHOULDER, DEPTH VARIES (14" TO 18.5")
- ⑥ EXISTING PIPE UNDERDRAIN
- ⑦ EXISTING AGGREGATE SHOULDER
- ⑧ EXISTING CONC. BARRIER WALL DOUBLE FACE (2'-3' WIDE)
- ⑨ EXISTING HMA SHOULDER, DEPTH VARIES (6" TO 9.5" ON 6" & VAR. SUBBASE)
- ⑩ LONGITUDINAL PARTIAL DEPTH REMOVAL, 2"
- ⑪ PAVED SHOULDER REMOVAL
- ⑫ HMA SURFACE REMOVAL, 2"

NOTES:

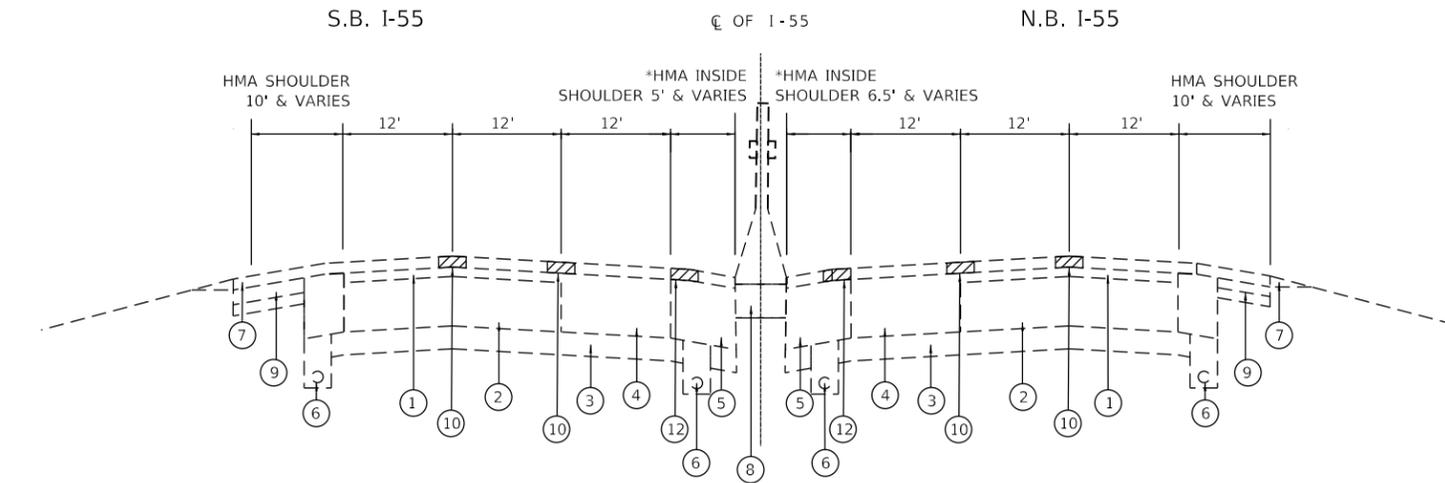
1. LOCATIONS OF REMOVAL FOR PATCHING WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER. THIS IS NOT FOR THE ENTIRE LENGTH OF THE PROJECT.
2. THE LONGITUDINAL PARTIAL DEPTH REMOVAL AND LONGITUDINAL PARTIAL DEPTH PATCHING WIDTH SHALL BE 3 FT. THE LENGTH SHALL BE A MINIMUM OF 10 FT, AND THE DEPTH SHALL BE 2 INCHES.
3. LEGEND ITEM 12 ABOVE (HMA SURFACE REMOVAL, 2") IS FOR RUMBLE STRIP REMOVAL DURING PRE-STAGE MOT. THE WIDTH SHALL BE 3 FT.

NB INSIDE SHOULDER RUMBLE STRIP REMOVAL LOCATIONS

STA 412+73 TO STA 417+83
 STA 420+46 TO STA 439+43
 STA 441+76 TO STA 460+66
 STA 462+16 TO STA 474+51
 STA 535+55 TO STA 540+69
 STA 542+63 TO STA 553+53

SB INSIDE SHOULDER RUMBLE STRIP REMOVAL LOCATIONS

STA 404+63 TO STA 417+79
 STA 420+41 TO STA 439+47
 STA 441+81 TO STA 460+61
 STA 462+10 TO STA 474+15
 STA 529+26 TO STA 540+71
 STA 542+62 TO STA 553+81



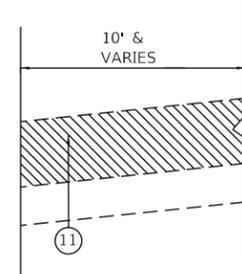
I-55
 EXISTING TYPICAL CROSS SECTION APPROXIMATE LOCATION(S)

STA. 17+93 TO STA. 735+49 (NORTHBOUND)
 STA. 16+68 TO STA. 735+49 (SOUTHBOUND)

LOOKING NORTH

(INSIDE SHOULDER WIDTH NARROWS TO 4.5'
 NB STA 351+14 TO 388+34 AND NB STA 612+00 TO 621+87)

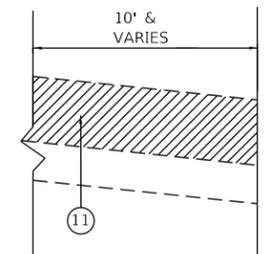
OUTSIDE SHOULDER RECONSTRUCTION



SB OUTSIDE SHOULDER RECONSTRUCTION LOCATIONS

STA 410+00 TO STA 417+68
 STA 420+27 TO STA 435+60 (GORE AREA)
 STA 454+03 TO STA 460+47
 STA 461+82 TO STA 469+97
 STA 533+55 TO STA 540+71
 STA 542+62 TO STA 549+31

OUTSIDE SHOULDER RECONSTRUCTION



NB OUTSIDE SHOULDER RECONSTRUCTION LOCATIONS

STA 411+50 TO STA 417+77
 STA 420+44 TO STA 434+50
 STA 437+50 TO STA 439+46
 STA 452+30 TO STA 460+60
 STA 462+12 TO STA 470+40
 STA 534+43 TO STA 540+69
 STA 542+63 TO STA 549+15

MODEL: MODELNAME
FILE NUMBER: FILE#



8501 W. Higgins Road, Suite 280
 Chicago, Illinois 60631
 (773) 398-0112

USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATE\$	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

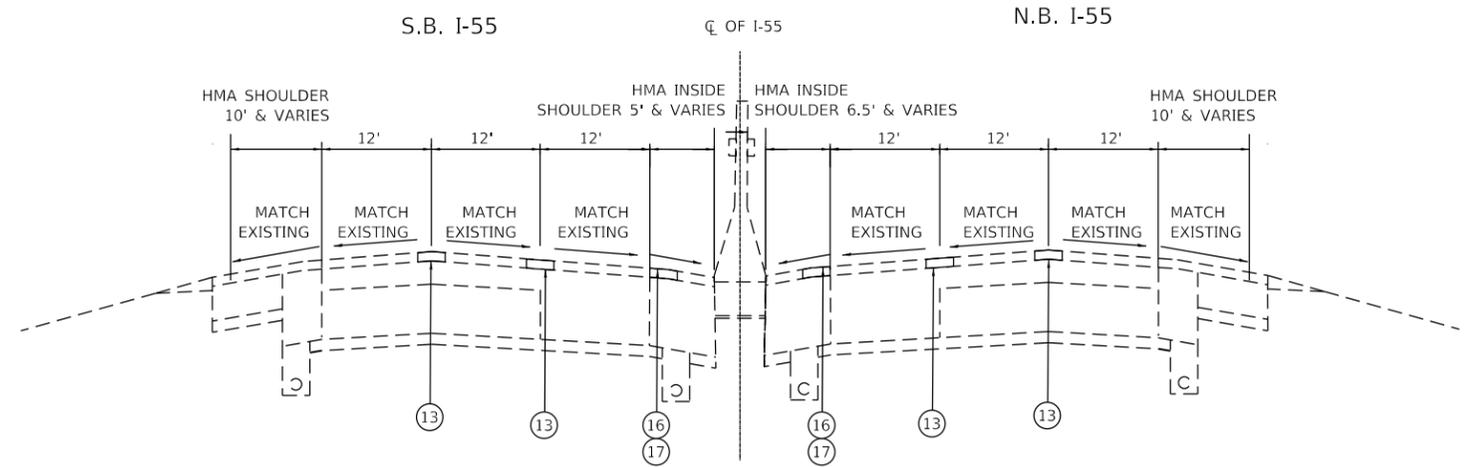
TYPICAL SECTION
I-55 (SOUTH OF I-80 TO WEBER ROAD)

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	14
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

LEGEND

- ① EXISTING HMA SURFACE COURSE
- ② EXISTING P.C.C. BASE COURSE
- ③ EXISTING AGGREGATE SUBGRADE
- ④ EXISTING HMA BASE COURSE
- ⑤ EXISTING HMA SHOULDER, DEPTH VARIES (14" TO 18.5")
- ⑥ EXISTING PIPE UNDERDRAIN
- ⑦ EXISTING AGGREGATE SHOULDER
- ⑧ EXISTING CONC. BARRIER WALL DOUBLE FACE (2'-3" WIDE)
- ⑨ EXISTING HMA SHOULDER, DEPTH VARIES (6" TO 9.5" ON 6" & VAR. SUBBASE)
- ⑩ LONGITUDINAL PARTIAL DEPTH REMOVAL, 2"
- ⑪ PAVED SHOULDER REMOVAL
- ⑫ HMA SURFACE REMOVAL, 2"
- ⑬ LONGITUDINAL JOINT PATCHING, 2"
- ⑭ PROPOSED SUBBASE GRANULAR MATERIAL TYPE B, 4"
- ⑮ PROPOSED HMA SHOULDERS, 10"
- ⑯ HMA SURFACE COURSE, MIX "D", IL-9.5, N70 (2")
- ⑰ SHOULDER RUMBLE STRIPS, 16"



I-55
PROPOSED TYPICAL CROSS SECTION
 STA. 17+93 TO STA. 735+49 (NORTHBOUND)
 STA. 16+68 TO STA. 735+49 (SOUTHBOUND)

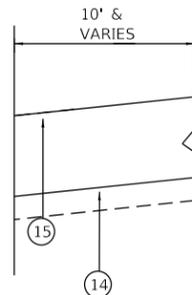
LOOKING NORTH

(INSIDE SHOULDER WIDTH NARROWS TO 4.5'
 NB STA 351+14 TO 388+34 AND NB STA 612+00 TO 621+87)

NOTES:

1. LOCATIONS OF REMOVAL FOR LONGITUDINAL JOINT PATCHING WILL BE DETERMINED IN THE FIELD BY THE RESIDENT ENGINEER. THIS IS NOT FOR THE ENTIRE LENGTH OF THE PROJECT.
2. THE LONGITUDINAL PARTIAL DEPTH REMOVAL AND LONGITUDINAL PARTIAL DEPTH PATCHING WIDTH SHALL BE 3 FT. THE LENGTH SHALL BE A MINIMUM OF 10 FT, AND THE DEPTH SHALL BE 2 INCHES.
3. EXISTING PAVEMENT MARKING REMOVED DUE TO PATCHING, SHALL BE REPLACED WITH MODIFIED URETHANE PAVEMENT MARKING, TO MATCH EXISTING.
4. THE PROPOSED INSIDE SHOULDER SURFACE COURSE IS TO FILL IN THE REMOVED INSIDE SHOULDER RUMBLE STRIP IN A PRE-STAGE PRIOR TO SHIFTING TRAFFIC. TRAFFIC SHALL NOT TRAVEL OVER THE EXISTING RUMBLE STRIPS.
5. THE PROPOSED RUMBLE STRIPS SHALL NOT BE CONSTRUCTED UNTIL TRAFFIC IS SHIFTED OFF THE SHOULDER. TRAFFIC SHALL NOT TRAVEL OVER THE PROPOSED RUMBLE STRIPS.

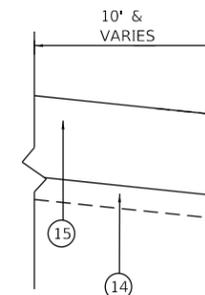
PROPOSED TYPICAL SECTION -
 OUTSIDE SHOULDER RECONSTRUCTION



SB OUTSIDE SHOULDER RECONSTRUCTION LOCATIONS

- STA 410+00 TO STA 417+68
- STA 420+27 TO STA 435+60 (GORE AREA)
- STA 454+03 TO STA 460+47
- STA 461+82 TO STA 469+97
- STA 533+55 TO STA 540+71
- STA 542+62 TO STA 549+31

PROPOSED TYPICAL SECTION -
 OUTSIDE SHOULDER RECONSTRUCTION



NB OUTSIDE SHOULDER RECONSTRUCTION LOCATIONS

- STA 411+50 TO STA 417+77
- STA 420+44 TO STA 434+50
- STA 437+50 TO STA 439+46
- STA 452+30 TO STA 460+60
- STA 462+12 TO STA 470+40
- STA 534+43 TO STA 540+69
- STA 542+63 TO STA 549+15

NB INSIDE HMA AND SHOULDER RUMBLE STRIP LOCATIONS

- STA 412+73 TO STA 417+83
- STA 420+46 TO STA 439+43
- STA 441+76 TO STA 460+66
- STA 462+16 TO STA 474+51
- STA 535+55 TO STA 540+69
- STA 542+63 TO STA 553+53

SB INSIDE HMA AND SHOULDER RUMBLE STRIP LOCATIONS

- STA 404+63 TO STA 417+79
- STA 420+41 TO STA 439+47
- STA 441+81 TO STA 460+61
- STA 462+10 TO STA 474+15
- STA 529+26 TO STA 540+71
- STA 542+62 TO STA 553+81

MODEL: SHOBELNAMES
FILE: 2020-253-PP-FILES



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USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALES	CHECKED -	REVISED -
PLOT DATE = \$DATES	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTION
I-55 (SOUTH OF I-80 TO WEBER ROAD)

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	15
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		
MIXTURE TYPE	AIR VOIDS @ Ndes	QUALITY MANAGEMENT PROGRAM (QMP)
HMA SHOULDER 10" (OUTSIDE SHOULDER)		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N70 (2")	4.0% @ 70 Gyr.	QC/QA
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 (8")	4.0% @ 70 Gyr.	QC/QA
CLASS D PATCHING		
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70 (16")	4.0% @ 70 Gyr.	QC/QA
INTERMITTENT RESURFACING (LANE PATCHING)		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N70 (2")	4.0% @ 70 Gyr.	QC/QA
INSIDE SHOULDER (RUMBLE STRIP REMOVAL AND REPLACEMENT)		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N70 (2")	4.0% @ 70 Gyr.	QC/QA
LONGITUDINAL PARTIAL DEPTH PATCHES		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", IL-9.5, N70 (2")	4.0% @ 70 Gyr.	QC/QA
BRIDGE TAPER		
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80 (2")	3.5% @ 80 Gyr.	QC/QA
QMP DESIGNATION: QUALITY CONTROL / QUALITY ASSURANCE (QC/QA); QUALITY CONTROL FOR PERFORMANCE (QCP); PAY FOR PERFORMANCE (PFP)		

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURES IS 112 LBS/SQ YD/IN.
 THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS. QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

MODEL: \$MODELNAME\$
FILE: \$NAME\$. \$FILE\$

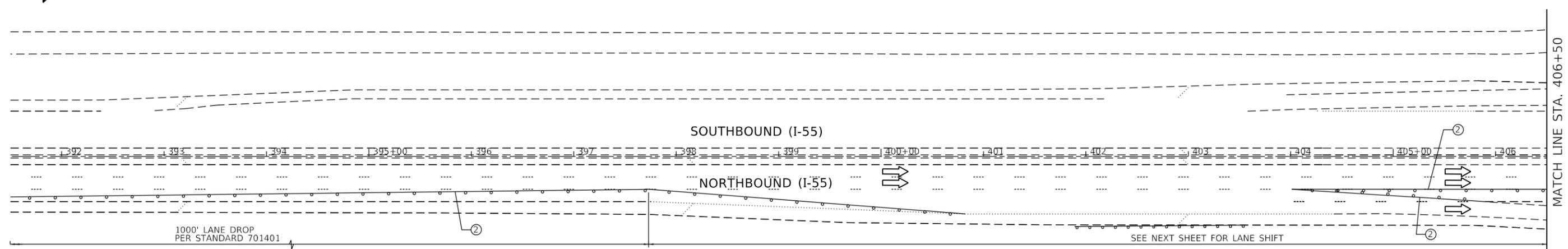


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	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATE\$	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

MIX TABLE			
I-55 (SOUTH OF I-80 TO WEBER ROAD)			
SCALE:	SHEET	OF	SHEETS
	STA.	TO	STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	16
ILLINOIS			CONTRACT NO. 62N22	
FED. AID PROJECT				



SEE NOTE 5 FOR
ADVANCED SIGNING

LEGEND

- WORK ZONE
- TEMPORARY CONCRETE BARRIER
- DRUM

EXIT RAMP CONFIGURATION AND
SIGNING AS PER STANDARD 701411

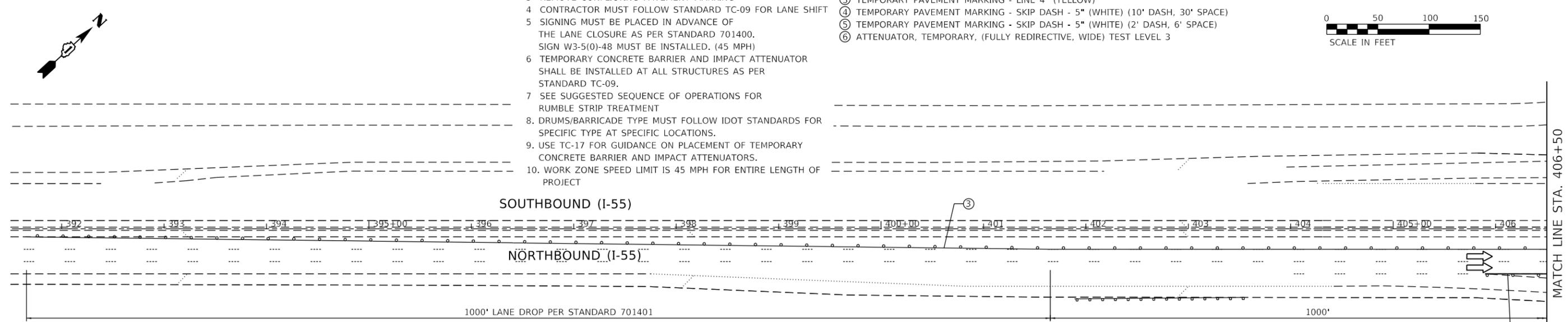
STAGE 1

NOTES

1. POSTED 65MPH SPEED LIMIT.
2. SEE BRIDGE STAGING TYPICAL SECTIONS FOR TEMPORARY BARRIER AND PAVEMENT MARKING OFFSETS
3. REMOVE CONFLICTING PAVEMENT MARKING
4. CONTRACTOR MUST FOLLOW STANDARD TC-09 FOR LANE SHIFT
5. SIGNING MUST BE PLACED IN ADVANCE OF THE LANE CLOSURE AS PER STANDARD 701400. SIGN W3-5(0)-48 MUST BE INSTALLED. (45 MPH)
6. TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATOR SHALL BE INSTALLED AT ALL STRUCTURES AS PER STANDARD TC-09.
7. SEE SUGGESTED SEQUENCE OF OPERATIONS FOR RUMBLE STRIP TREATMENT
8. DRUMS/BARRICADE TYPE MUST FOLLOW IDOT STANDARDS FOR SPECIFIC TYPE AT SPECIFIC LOCATIONS.
9. USE TC-17 FOR GUIDANCE ON PLACEMENT OF TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATORS.
10. WORK ZONE SPEED LIMIT IS 45 MPH FOR ENTIRE LENGTH OF PROJECT

MOT LEGEND

- ① TEMPORARY CONCRETE BARRIER
- ② TEMPORARY PAVEMENT MARKING - LINE 4" (WHITE)
- ③ TEMPORARY PAVEMENT MARKING - LINE 4" (YELLOW)
- ④ TEMPORARY PAVEMENT MARKING - SKIP DASH - 5" (WHITE) (10' DASH, 30' SPACE)
- ⑤ TEMPORARY PAVEMENT MARKING - SKIP DASH - 5" (WHITE) (2' DASH, 6' SPACE)
- ⑥ ATTENUATOR, TEMPORARY, (FULLY REDIRECTIVE, WIDE) TEST LEVEL 3



SEE NOTE 5 FOR
ADVANCED SIGNING

LEGEND

- WORK ZONE
- TEMPORARY CONCRETE BARRIER
- DRUM

STAGE 2

MODEL: SMOBELNAMES
FILE: 2020-253-PP-FILES



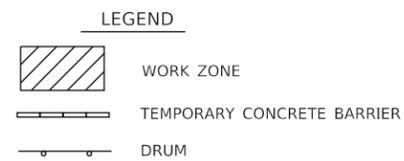
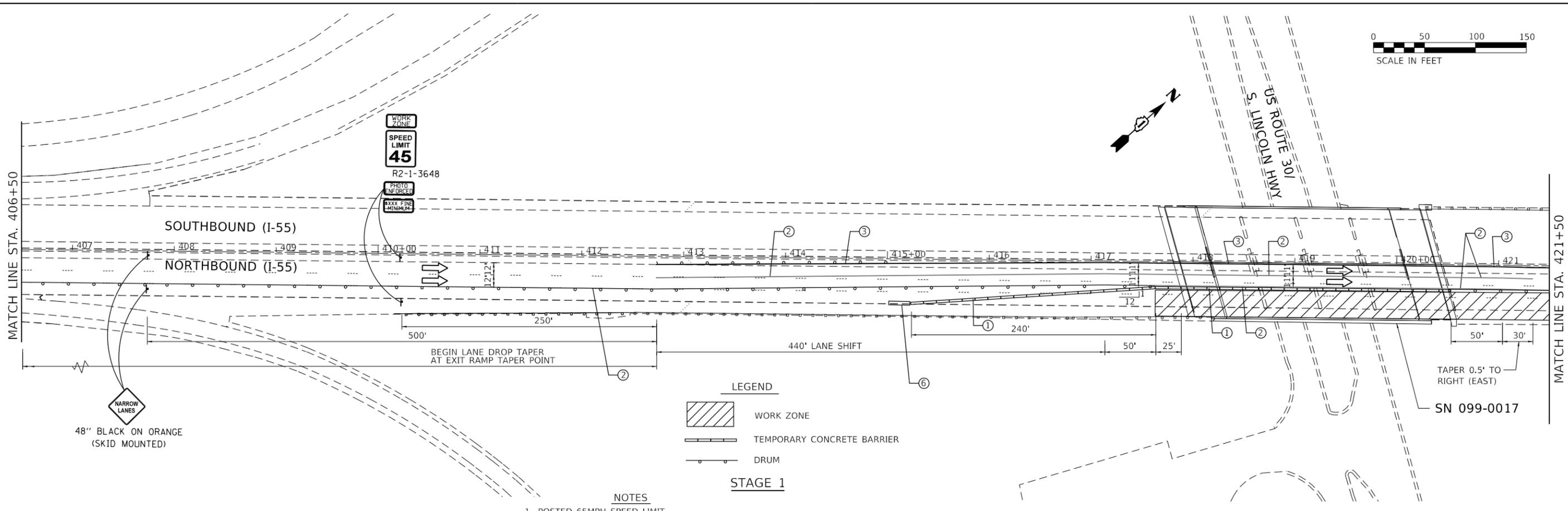
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PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATES	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED MAINTENANCE OF TRAFFIC
I-55 (SOUTH OF I-80 TO WEBER ROAD)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	17
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				



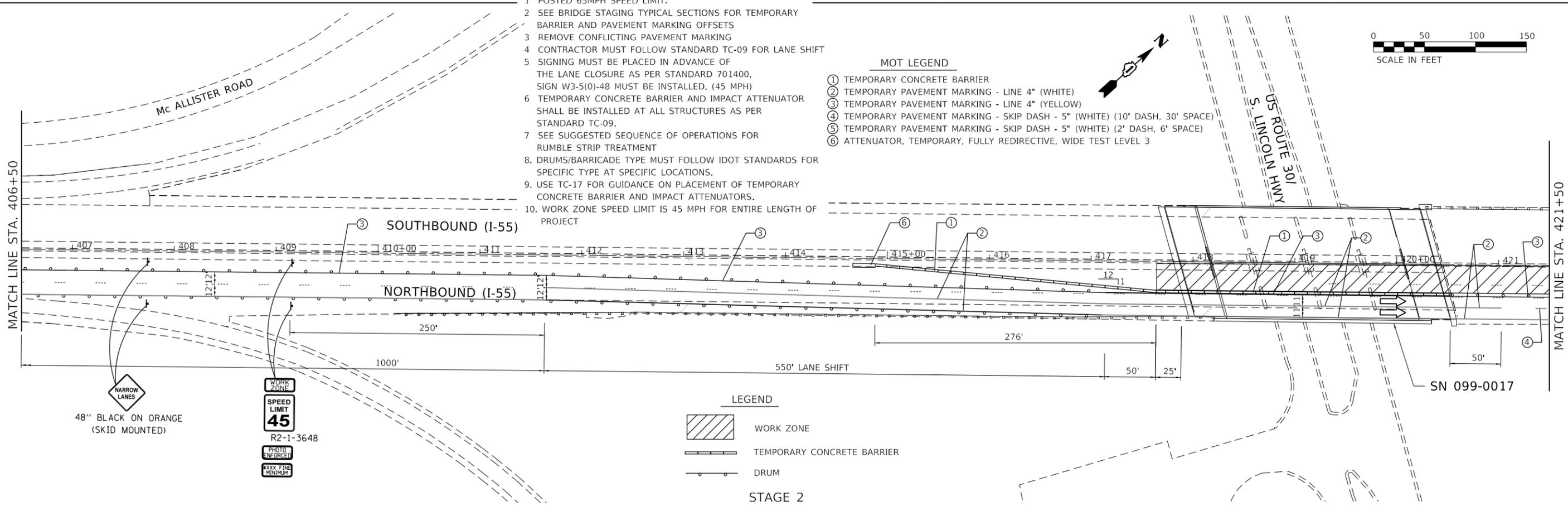
STAGE 1

NOTES

- 1 POSTED 65MPH SPEED LIMIT.
- 2 SEE BRIDGE STAGING TYPICAL SECTIONS FOR TEMPORARY BARRIER AND PAVEMENT MARKING OFFSETS
- 3 REMOVE CONFLICTING PAVEMENT MARKING
- 4 CONTRACTOR MUST FOLLOW STANDARD TC-09 FOR LANE SHIFT
- 5 SIGNING MUST BE PLACED IN ADVANCE OF THE LANE CLOSURE AS PER STANDARD 701400. SIGN W3-5(0)-48 MUST BE INSTALLED. (45 MPH)
- 6 TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATOR SHALL BE INSTALLED AT ALL STRUCTURES AS PER STANDARD TC-09.
- 7 SEE SUGGESTED SEQUENCE OF OPERATIONS FOR RUMBLE STRIP TREATMENT
- 8 DRUMS/BARRICADE TYPE MUST FOLLOW IDOT STANDARDS FOR SPECIFIC TYPE AT SPECIFIC LOCATIONS.
9. USE TC-17 FOR GUIDANCE ON PLACEMENT OF TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATORS.
10. WORK ZONE SPEED LIMIT IS 45 MPH FOR ENTIRE LENGTH OF PROJECT

MOT LEGEND

- ① TEMPORARY CONCRETE BARRIER
- ② TEMPORARY PAVEMENT MARKING - LINE 4" (WHITE)
- ③ TEMPORARY PAVEMENT MARKING - LINE 4" (YELLOW)
- ④ TEMPORARY PAVEMENT MARKING - SKIP DASH - 5" (WHITE) (10' DASH, 30' SPACE)
- ⑤ TEMPORARY PAVEMENT MARKING - SKIP DASH - 5" (WHITE) (2' DASH, 6' SPACE)
- ⑥ ATTENUATOR, TEMPORARY, FULLY REDIRECTIVE, WIDE TEST LEVEL 3



STAGE 2

MODEL: S:\MODELS\MAMES FILE: S:\MAMES: STILES



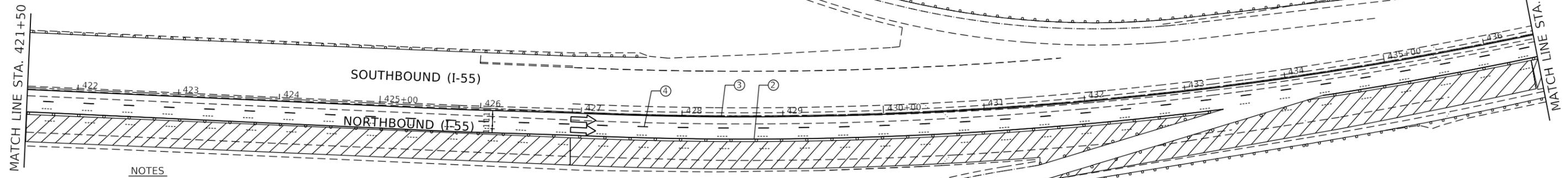
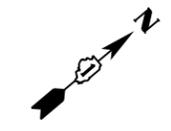
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PLOT SCALE = \$SCALE\$	DRAWN -	REVISED -
PLOT DATE = \$DATE\$	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED MAINTENANCE OF TRAFFIC
I-55 (SOUTH OF I-80 TO WEBER ROAD)**

SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	18
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				



NOTES

- 1 POSTED 65MPH SPEED LIMIT.
- 2 SEE BRIDGE STAGING TYPICAL SECTIONS FOR TEMPORARY BARRIER AND PAVEMENT MARKING OFFSETS
- 3 REMOVE CONFLICTING PAVEMENT MARKING
- 4 CONTRACTOR MUST FOLLOW STANDARD TC-09 FOR LANE SHIFT
- 5 SIGNING MUST BE PLACED IN ADVANCE OF THE LANE CLOSURE AS PER STANDARD 701400. SIGN W3-5(0)-48 MUST BE INSTALLED. (45 MPH)
- 6 TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATOR SHALL BE INSTALLED AT ALL STRUCTURES AS PER STANDARD TC-09.
- 7 SEE SUGGESTED SEQUENCE OF OPERATIONS FOR RUMBLE STRIP TREATMENT
8. DRUMS/BARRICADE TYPE MUST FOLLOW IDOT STANDARDS FOR SPECIFIC TYPE AT SPECIFIC LOCATIONS.
9. USE TC-17 FOR GUIDANCE ON PLACEMENT OF TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATORS.
10. WORK ZONE SPEED LIMIT IS 45 MPH FOR ENTIRE LENGTH OF PROJECT.

MOT LEGEND

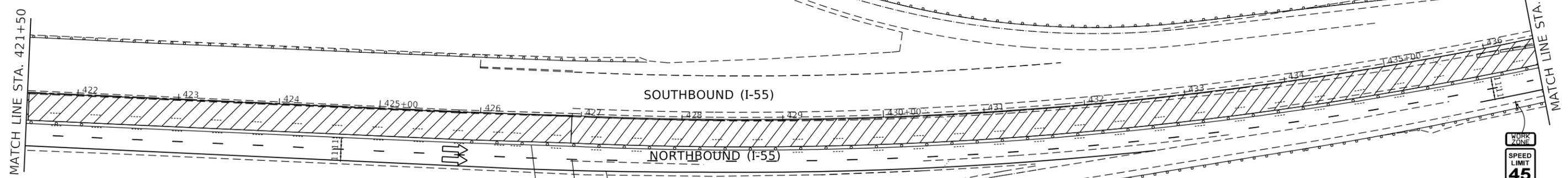
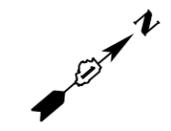
- ① TEMPORARY CONCRETE BARRIER
- ② TEMPORARY PAVEMENT MARKING - LINE 4" (WHITE)
- ③ TEMPORARY PAVEMENT MARKING - LINE 4" (YELLOW)
- ④ TEMPORARY PAVEMENT MARKING - SKIP DASH - 5" (WHITE) (10' DASH, 30' SPACE)
- ⑤ TEMPORARY PAVEMENT MARKING - SKIP DASH - 5" (WHITE) (2' DASH, 6' SPACE)
- ⑥ ATTENUATOR, TEMPORARY, FULLY REDIRECTIVE, WIDE TEST LEVEL 3

LEGEND

- WORK ZONE
- TEMPORARY CONCRETE BARRIER
- DRUM

STAGE 1

ENTRANCE MUST REMAIN OPEN
USE STANDARD 701411
FOR ENTRANCE RAMP CONFIGURATION
AND SIGNING THROUGH WORK ZONE



LEGEND

- WORK ZONE
- TEMPORARY CONCRETE BARRIER
- DRUM

STAGE 2

ENTRANCE MUST REMAIN OPEN
USE STANDARD 701411
FOR ENTRANCE RAMP CONFIGURATION
AND SIGNING THROUGH WORK ZONE

WORK ZONE
SPEED LIMIT
45
R2-1-3648
PHOTO ENFORCED
XXX FINE MINIMUM

MODEL: SMOBELNAMES
FILE NUMBER: 111215



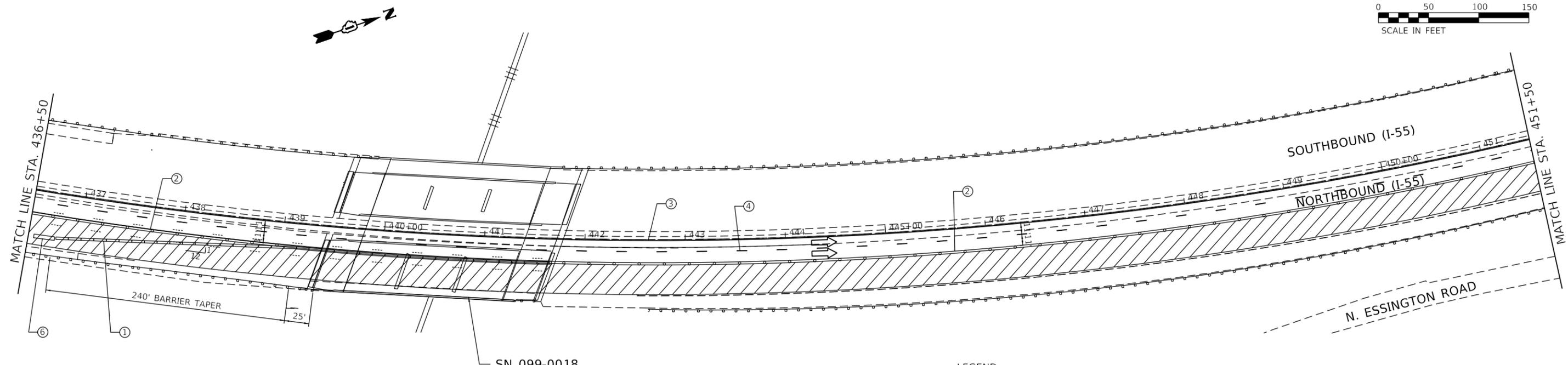
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED MAINTENANCE OF TRAFFIC
I-55 (SOUTH OF I-80 TO WEBER ROAD)**

SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	19
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62N22	



SN 099-0018

STAGE 1

LEGEND

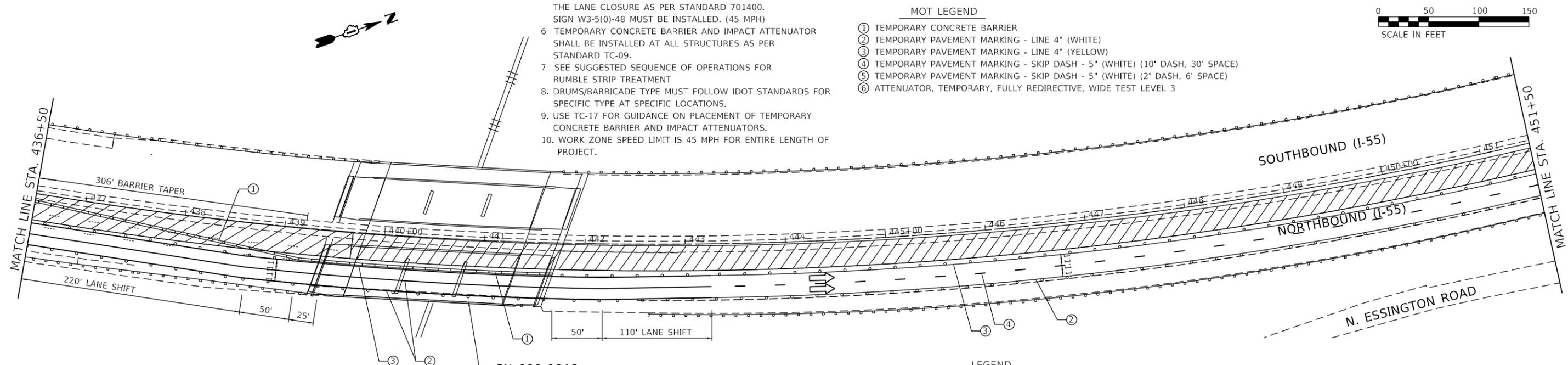
- WORK ZONE
- TEMPORARY CONCRETE BARRIER
- DRUM

NOTES

- 1 POSTED 65MPH SPEED LIMIT.
- 2 SEE BRIDGE STAGING TYPICAL SECTIONS FOR TEMPORARY BARRIER AND PAVEMENT MARKING OFFSETS
- 3 REMOVE CONFLICTING PAVEMENT MARKING
- 4 CONTRACTOR MUST FOLLOW STANDARD TC-09 FOR LANE SHIFT
- 5 SIGNING MUST BE PLACED IN ADVANCE OF THE LANE CLOSURE AS PER STANDARD 701400. SIGN W3-5(0)-48 MUST BE INSTALLED. (45 MPH)
- 6 TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATOR SHALL BE INSTALLED AT ALL STRUCTURES AS PER STANDARD TC-09.
- 7 SEE SUGGESTED SEQUENCE OF OPERATIONS FOR RUMBLE STRIP TREATMENT
- 8 DRUMS/BARRICADE TYPE MUST FOLLOW IDOT STANDARDS FOR SPECIFIC TYPE AT SPECIFIC LOCATIONS.
9. USE TC-17 FOR GUIDANCE ON PLACEMENT OF TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATORS.
10. WORK ZONE SPEED LIMIT IS 45 MPH FOR ENTIRE LENGTH OF PROJECT.

MOT LEGEND

- ① TEMPORARY CONCRETE BARRIER
- ② TEMPORARY PAVEMENT MARKING - LINE 4" (WHITE)
- ③ TEMPORARY PAVEMENT MARKING - LINE 4" (YELLOW)
- ④ TEMPORARY PAVEMENT MARKING - SKIP DASH - 5" (WHITE) (10' DASH, 30' SPACE)
- ⑤ TEMPORARY PAVEMENT MARKING - SKIP DASH - 5" (WHITE) (2' DASH, 6' SPACE)
- ⑥ ATTENUATOR, TEMPORARY, FULLY REDIRECTIVE, WIDE TEST LEVEL 3



SN 099-0018

STAGE 2

LEGEND

- WORK ZONE
- TEMPORARY CONCRETE BARRIER
- DRUM

MODEL: SMOBELNAMES
FILE NUMBER: 51125



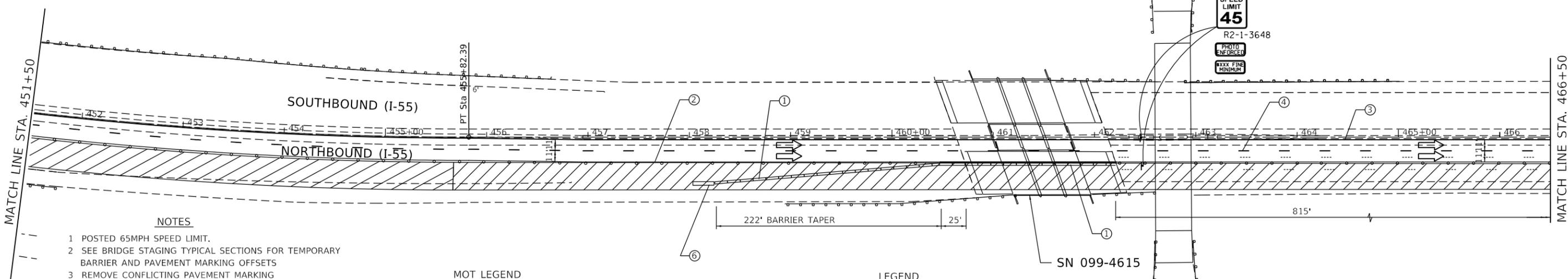
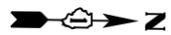
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED MAINTENANCE OF TRAFFIC
I-55 (SOUTH OF I-80 TO WEBER ROAD)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	20
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

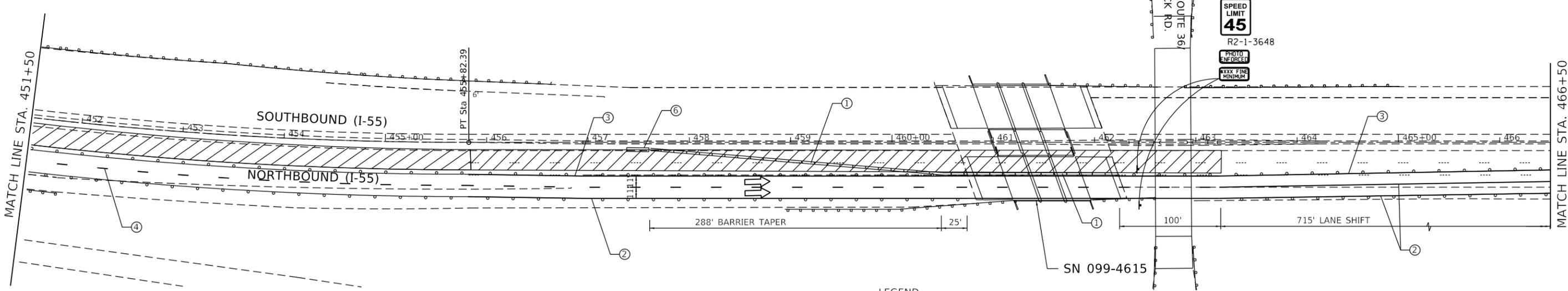
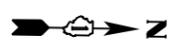


- NOTES**
- 1 POSTED 65MPH SPEED LIMIT.
 - 2 SEE BRIDGE STAGING TYPICAL SECTIONS FOR TEMPORARY BARRIER AND PAVEMENT MARKING OFFSETS
 - 3 REMOVE CONFLICTING PAVEMENT MARKING
 - 4 CONTRACTOR MUST FOLLOW STANDARD TC-09 FOR LANE SHIFT
 - 5 SIGNING MUST BE PLACED IN ADVANCE OF THE LANE CLOSURE AS PER STANDARD 701400. SIGN W3-5(0)-48 MUST BE INSTALLED. (45 MPH)
 - 6 TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATOR SHALL BE INSTALLED AT ALL STRUCTURES AS PER STANDARD TC-09.
 - 7 SEE SUGGESTED SEQUENCE OF OPERATIONS FOR RUMBLE STRIP TREATMENT
 - 8 DRUMS/BARRICADE TYPE MUST FOLLOW IDOT STANDARDS FOR SPECIFIC TYPE AT SPECIFIC LOCATIONS.
 9. USE TC-17 FOR GUIDANCE ON PLACEMENT OF TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATORS.
 10. WORK ZONE SPEED LIMIT IS 45 MPH FOR ENTIRE LENGTH OF PROJECT.

- MOT LEGEND**
- ① TEMPORARY CONCRETE BARRIER
 - ② TEMPORARY PAVEMENT MARKING - LINE 4" (WHITE)
 - ③ TEMPORARY PAVEMENT MARKING - LINE 4" (YELLOW)
 - ④ TEMPORARY PAVEMENT MARKING - SKIP DASH - 5" (WHITE) (10' DASH, 30' SPACE)
 - ⑤ TEMPORARY PAVEMENT MARKING - SKIP DASH - 5" (WHITE) (2' DASH, 6' SPACE)
 - ⑥ ATTENUATOR, TEMPORARY, FULLY REDIRECTIVE, WIDE TEST LEVEL 3

- LEGEND**
- WORK ZONE
 - TEMPORARY CONCRETE BARRIER
 - DRUM

STAGE 1



- LEGEND**
- WORK ZONE
 - TEMPORARY CONCRETE BARRIER
 - DRUM

STAGE 2

MODEL: SMOBELNAMES
FILE: W000001.PLT



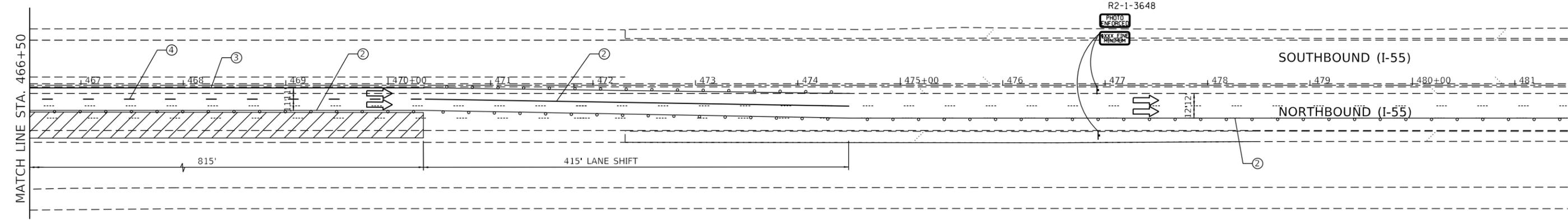
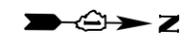
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PLOT DATE = \$DATE\$	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED MAINTENANCE OF TRAFFIC
I-55 (SOUTH OF I-80 TO WEBER ROAD)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	21
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				



NOTES

- 1 POSTED 65MPH SPEED LIMIT.
- 2 SEE BRIDGE STAGING TYPICAL SECTIONS FOR TEMPORARY BARRIER AND PAVEMENT MARKING OFFSETS
- 3 REMOVE CONFLICTING PAVEMENT MARKING
- 4 CONTRACTOR MUST FOLLOW STANDARD TC-09 FOR LANE SHIFT
- 5 SIGNING MUST BE PLACED IN ADVANCE OF THE LANE CLOSURE AS PER STANDARD 701400. SIGN W3-5(0)-48 MUST BE INSTALLED. (45 MPH)
- 6 TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATOR SHALL BE INSTALLED AT ALL STRUCTURES AS PER STANDARD TC-09.
- 7 SEE SUGGESTED SEQUENCE OF OPERATIONS FOR RUMBLE STRIP TREATMENT
- 8 DRUMS/BARRICADE TYPE MUST FOLLOW IDOT STANDARDS FOR SPECIFIC TYPE AT SPECIFIC LOCATIONS.
- 9 USE TC-17 FOR GUIDANCE ON PLACEMENT OF TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATORS.
10. WORK ZONE SPEED LIMIT IS 45 MPH FOR ENTIRE LENGTH OF PROJECT.

MOT LEGEND

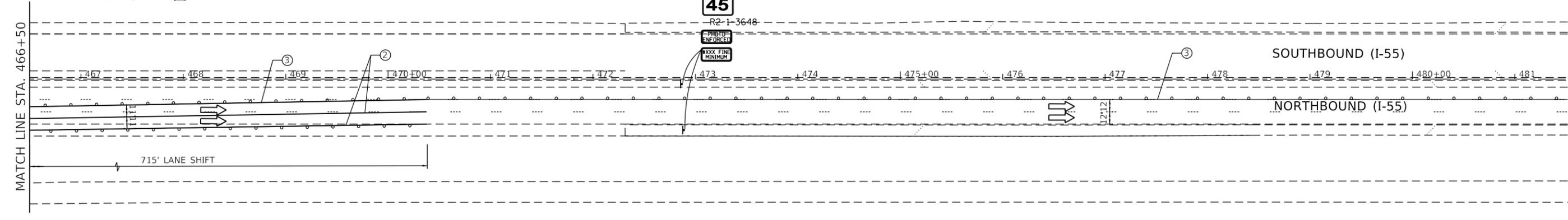
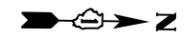
- ① TEMPORARY CONCRETE BARRIER
- ② TEMPORARY PAVEMENT MARKING - LINE 4" (WHITE)
- ③ TEMPORARY PAVEMENT MARKING - LINE 4" (YELLOW)
- ④ TEMPORARY PAVEMENT MARKING - SKIP DASH - 5" (WHITE) (10' DASH, 30' SPACE)
- ⑤ TEMPORARY PAVEMENT MARKING - SKIP DASH - 5" (WHITE) (2' DASH, 6' SPACE)
- ⑥ ATTENUATOR, TEMPORARY, FULLY REDIRECTIVE, WIDE TEST LEVEL 3

LEGEND

- WORK ZONE
- TEMPORARY CONCRETE BARRIER
- DRUM

STAGE 1

EXTEND TEMPORARY PAVEMENT MARKING AND DRUMS AS SHOWN TO STATION 524+00



LEGEND

- WORK ZONE
- TEMPORARY CONCRETE BARRIER
- DRUM

STAGE 2

EXTEND TEMPORARY PAVEMENT MARKING AND DRUMS AS SHOWN TO STATION 524+00

MODEL: SMOBELNAMES
FILE: IWBDRG_P11212

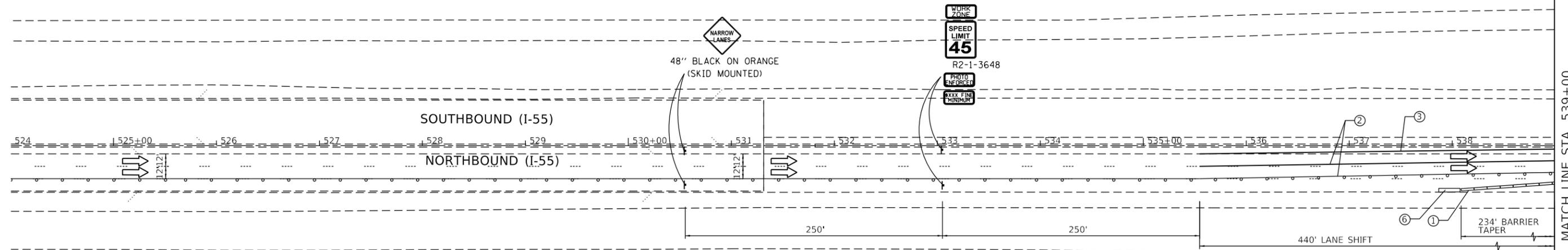
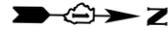


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PLOT DATE = \$DATE\$	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUGGESTED MAINTENANCE OF TRAFFIC I-55 (SOUTH OF I-80 TO WEBER ROAD)			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	22
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

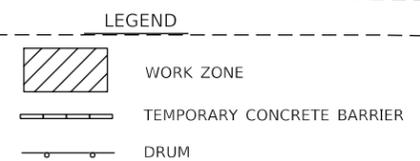


EXTEND TEMPORARY PAVEMENT MARKING AND DRUMS AS SHOWN TO STATION 481+00

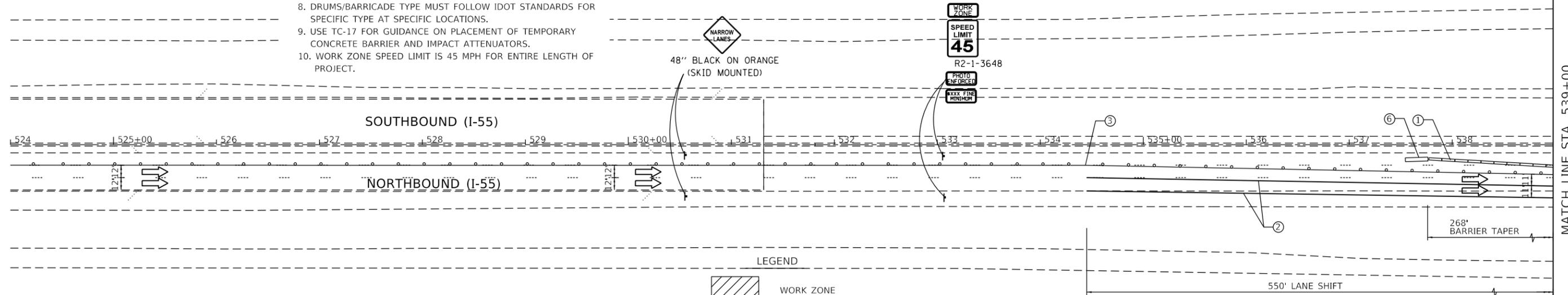
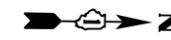
NOTES

- 1 POSTED 65MPH SPEED LIMIT.
- 2 SEE BRIDGE STAGING TYPICAL SECTIONS FOR TEMPORARY BARRIER AND PAVEMENT MARKING OFFSETS
- 3 REMOVE CONFLICTING PAVEMENT MARKING
- 4 CONTRACTOR MUST FOLLOW STANDARD TC-09 FOR LANE SHIFT
- 5 SIGNING MUST BE PLACED IN ADVANCE OF THE LANE CLOSURE AS PER STANDARD 701400. SIGN W3-5(0)-48 MUST BE INSTALLED. (45 MPH)
- 6 TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATOR SHALL BE INSTALLED AT ALL STRUCTURES AS PER STANDARD TC-09.
- 7 SEE SUGGESTED SEQUENCE OF OPERATIONS FOR RUMBLE STRIP TREATMENT
- 8 DRUMS/BARRICADE TYPE MUST FOLLOW IDOT STANDARDS FOR SPECIFIC TYPE AT SPECIFIC LOCATIONS.
9. USE TC-17 FOR GUIDANCE ON PLACEMENT OF TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATORS.
10. WORK ZONE SPEED LIMIT IS 45 MPH FOR ENTIRE LENGTH OF PROJECT.

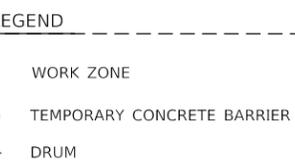
STAGE 1



- MOT LEGEND**
- ① TEMPORARY CONCRETE BARRIER
 - ② TEMPORARY PAVEMENT MARKING - LINE 4" (WHITE)
 - ③ TEMPORARY PAVEMENT MARKING - LINE 4" (YELLOW)
 - ④ TEMPORARY PAVEMENT MARKING - SKIP DASH - 5" (WHITE) (10' DASH, 30' SPACE)
 - ⑤ TEMPORARY PAVEMENT MARKING - SKIP DASH - 5" (WHITE) (2' DASH, 6' SPACE)
 - ⑥ ATTENUATOR, TEMPORARY, FULLY REDIRECTIVE, WIDE TEST LEVEL 3



EXTEND TEMPORARY PAVEMENT MARKING AND DRUMS AS SHOWN TO STATION 481+00



STAGE 2

MODEL: SMOBELNAMES
FILE: 2020-253-PP-FILES



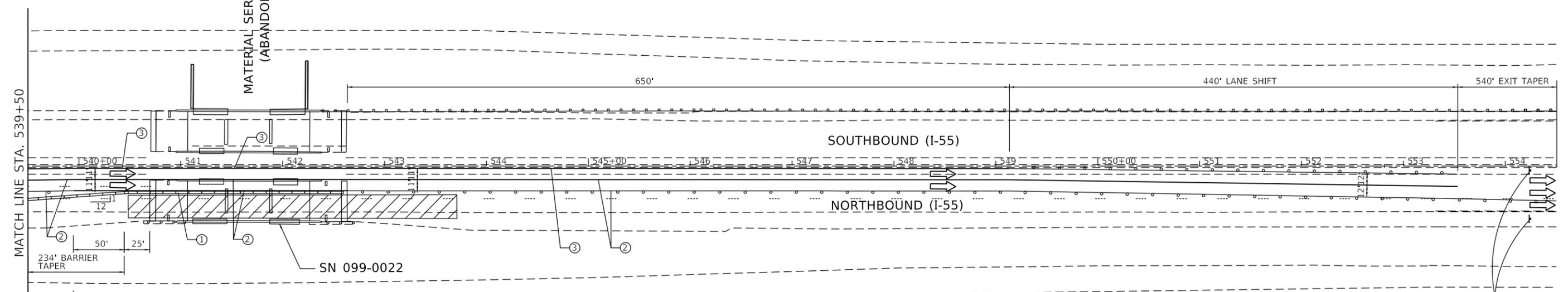
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	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED MAINTENANCE OF TRAFFIC
I-55 (SOUTH OF I-80 TO WEBER ROAD)**

SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	23
CONTRACT NO. 62N22				
ILLINOIS		FED. AID PROJECT		



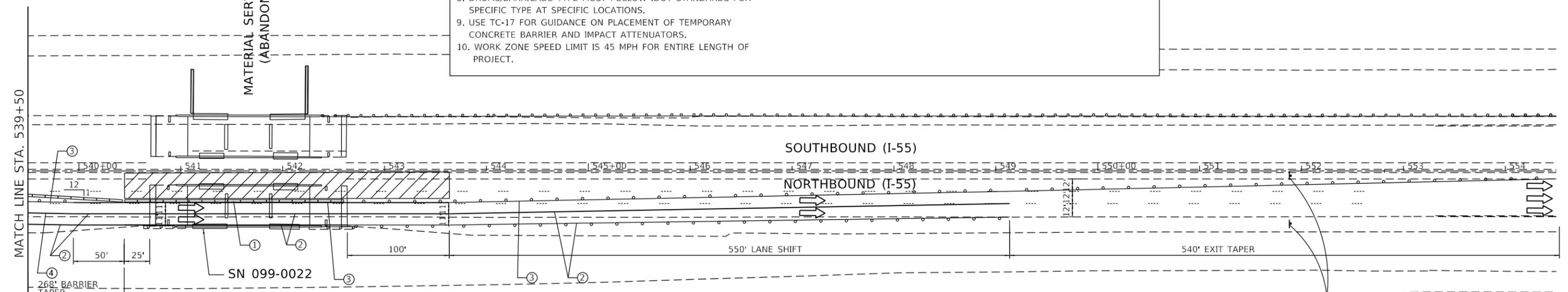
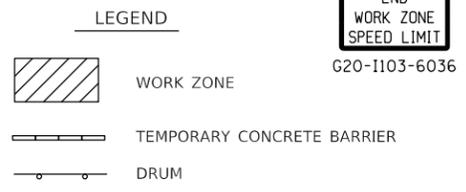
STAGE 1

NOTES

- 1 POSTED 65MPH SPEED LIMIT.
- 2 SEE BRIDGE STAGING TYPICAL SECTIONS FOR TEMPORARY BARRIER AND PAVEMENT MARKING OFFSETS
- 3 REMOVE CONFLICTING PAVEMENT MARKING
- 4 CONTRACTOR MUST FOLLOW STANDARD TC-09 FOR LANE SHIFT
- 5 SIGNING MUST BE PLACED IN ADVANCE OF THE LANE CLOSURE AS PER STANDARD 701400. SIGN W3-5(0)-48 MUST BE INSTALLED. (45 MPH)
- 6 TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATOR SHALL BE INSTALLED AT ALL STRUCTURES AS PER STANDARD TC-09.
- 7 SEE SUGGESTED SEQUENCE OF OPERATIONS FOR RUMBLE STRIP TREATMENT
8. DRUMS/BARRICADE TYPE MUST FOLLOW IDOT STANDARDS FOR SPECIFIC TYPE AT SPECIFIC LOCATIONS.
9. USE TC-17 FOR GUIDANCE ON PLACEMENT OF TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATORS.
10. WORK ZONE SPEED LIMIT IS 45 MPH FOR ENTIRE LENGTH OF PROJECT.

MOT LEGEND

- ① TEMPORARY CONCRETE BARRIER
- ② TEMPORARY PAVEMENT MARKING - LINE 4" (WHITE)
- ③ TEMPORARY PAVEMENT MARKING - LINE 4" (YELLOW)
- ④ TEMPORARY PAVEMENT MARKING - SKIP DASH - 5" (WHITE) (10' DASH, 30' SPACE)
- ⑤ TEMPORARY PAVEMENT MARKING - SKIP DASH - 5" (WHITE) (2' DASH, 6' SPACE)
- ⑥ ATTENUATOR, TEMPORARY, FULLY REDIRECTIVE, WIDE TEST LEVEL 3



STAGE 2



MODEL: SMOBELNAMES
FILE: 2018-01-12



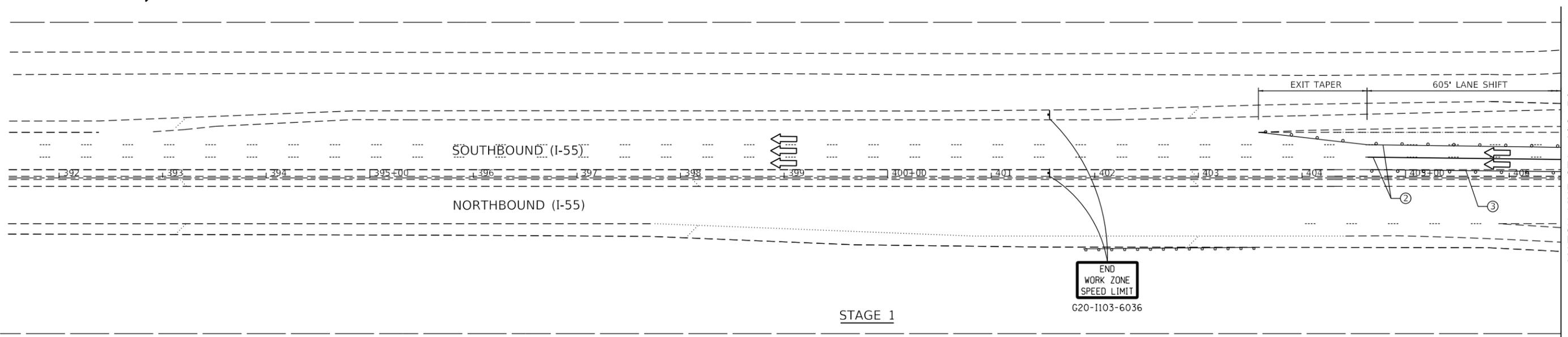
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED MAINTENANCE OF TRAFFIC
I-55 (SOUTH OF I-80 TO WEBER ROAD)**

SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	24
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				



STAGE 1

END
WORK ZONE
SPEED LIMIT
G20-I103-6036

MATCH LINE STA. 406+50

NOTES

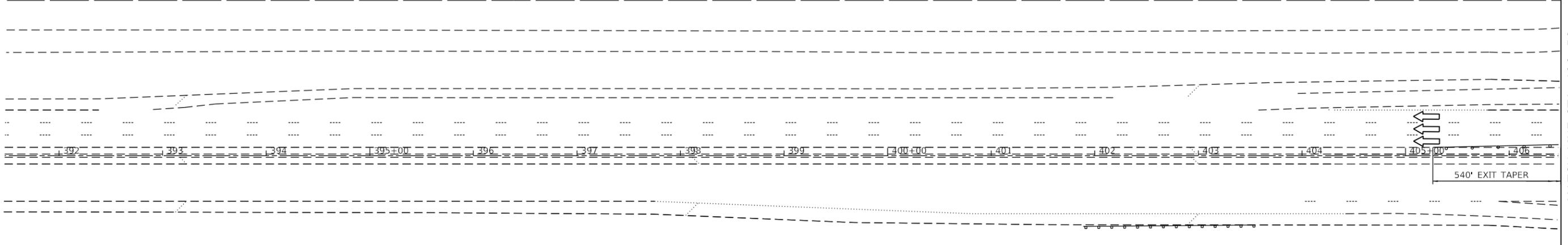
- 1 POSTED 65MPH SPEED LIMIT.
- 2 SEE BRIDGE STAGING TYPICAL SECTIONS FOR TEMPORARY BARRIER AND PAVEMENT MARKING OFFSETS
- 3 REMOVE CONFLICTING PAVEMENT MARKING
- 4 CONTRACTOR MUST FOLLOW STANDARD TC-09 FOR LANE SHIFT
- 5 SIGNING MUST BE PLACED IN ADVANCE OF THE LANE CLOSURE AS PER STANDARD 701400. SIGN W3-5(0)-48 MUST BE INSTALLED. (45 MPH)
- 6 TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATOR SHALL BE INSTALLED AT ALL STRUCTURES AS PER STANDARD TC-09.
- 7 SEE SUGGESTED SEQUENCE OF OPERATIONS FOR RUMBLE STRIP TREATMENT
8. DRUMS/BARRICADE TYPE MUST FOLLOW IDOT STANDARDS FOR SPECIFIC TYPE AT SPECIFIC LOCATIONS.
9. USE TC-17 FOR GUIDANCE ON PLACEMENT OF TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATORS.
10. WORK ZONE SPEED LIMIT IS 45 MPH FOR ENTIRE LENGTH OF PROJECT.

MOT LEGEND

- ① TEMPORARY CONCRETE BARRIER
- ② TEMPORARY PAVEMENT MARKING - LINE 4" (WHITE)
- ③ TEMPORARY PAVEMENT MARKING - LINE 4" (YELLOW)
- ④ TEMPORARY PAVEMENT MARKING - SKIP DASH - 5" (WHITE) (10' DASH, 30' SPACE)
- ⑤ TEMPORARY PAVEMENT MARKING - SKIP DASH - 5" (WHITE) (2' DASH, 6' SPACE)
- ⑥ ATTENUATOR, TEMPORARY, FULLY REDIRECTIVE, WIDE TEST LEVEL 3

LEGEND

- WORK ZONE
- TEMPORARY CONCRETE BARRIER
- DRUM



MATCH LINE STA. 406+50

MODEL: PROBLEMANAMES
FILE: I103-6036_PP1.DWG



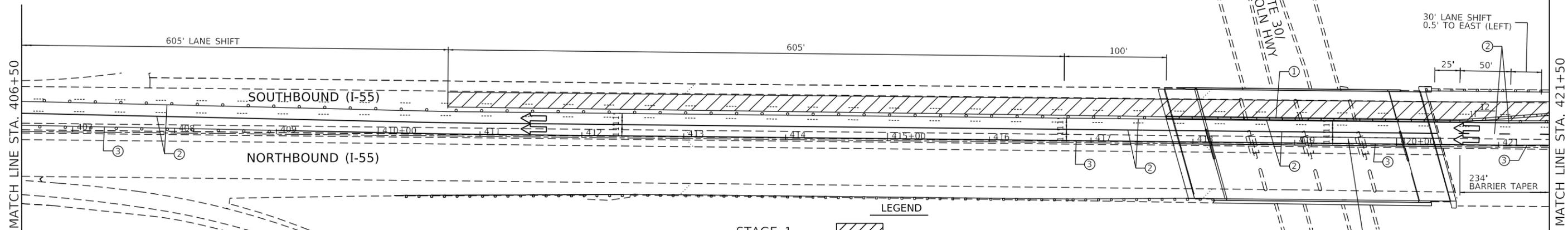
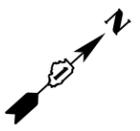
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED MAINTENANCE OF TRAFFIC AT BRIDGES
I-55 (SOUTH OF I-80 TO WEBER ROAD)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	25
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				



LEGEND

- WORK ZONE
- TEMPORARY CONCRETE BARRIER
- DRUM

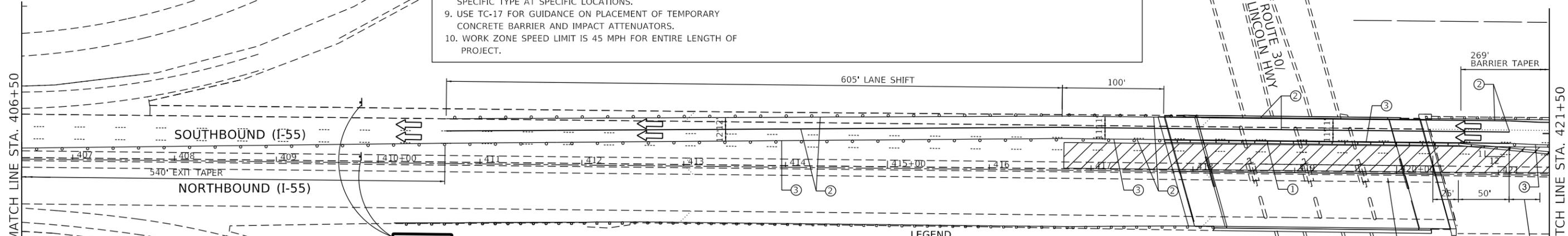
STAGE 1

NOTES

- 1 POSTED 65MPH SPEED LIMIT.
- 2 SEE BRIDGE STAGING TYPICAL SECTIONS FOR TEMPORARY BARRIER AND PAVEMENT MARKING OFFSETS
- 3 REMOVE CONFLICTING PAVEMENT MARKING
- 4 CONTRACTOR MUST FOLLOW STANDARD TC-09 FOR LANE SHIFT
- 5 SIGNING MUST BE PLACED IN ADVANCE OF THE LANE CLOSURE AS PER STANDARD 701400. SIGN W3-5(0)-48 MUST BE INSTALLED. (45 MPH)
- 6 TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATOR SHALL BE INSTALLED AT ALL STRUCTURES AS PER STANDARD TC-09.
- 7 SEE SUGGESTED SEQUENCE OF OPERATIONS FOR RUMBLE STRIP TREATMENT
8. DRUMS/BARRICADE TYPE MUST FOLLOW IDOT STANDARDS FOR SPECIFIC TYPE AT SPECIFIC LOCATIONS.
9. USE TC-17 FOR GUIDANCE ON PLACEMENT OF TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATORS.
10. WORK ZONE SPEED LIMIT IS 45 MPH FOR ENTIRE LENGTH OF PROJECT.

MOT LEGEND

- ① TEMPORARY CONCRETE BARRIER
- ② TEMPORARY PAVEMENT MARKING - LINE 4" (WHITE)
- ③ TEMPORARY PAVEMENT MARKING - LINE 4" (YELLOW)
- ④ TEMPORARY PAVEMENT MARKING - SKIP DASH - 5" (WHITE) (10' DASH, 30' SPACE)
- ⑤ TEMPORARY PAVEMENT MARKING - SKIP DASH - 5" (WHITE) (2' DASH, 6' SPACE)
- ⑥ ATTENUATOR, TEMPORARY, FULLY REDIRECTIVE, WIDE TEST LEVEL 3



LEGEND

- WORK ZONE
- TEMPORARY CONCRETE BARRIER
- DRUM

STAGE 2

END WORK ZONE SPEED LIMIT
G20-1103-6036

MODEL: SMOBELNAMES
FILE: W09E1_01E1E2



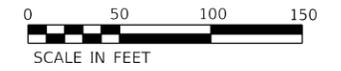
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

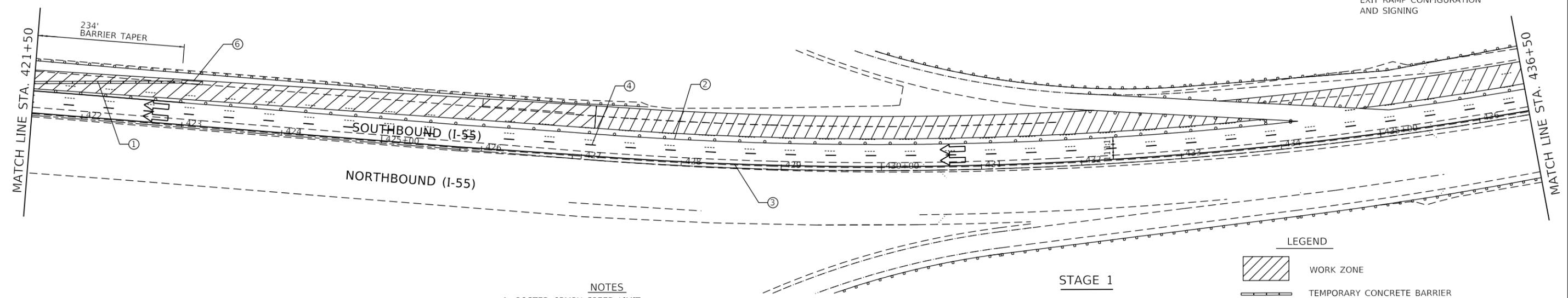
SUGGESTED MAINTENANCE OF TRAFFIC AT BRIDGES
I-55 (SOUTH OF I-80 TO WEBER ROAD)

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	26
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				



EXIT RAMP MUST REMAIN OPEN
USE STANDARD 701411 FOR
EXIT RAMP CONFIGURATION
AND SIGNING



LEGEND

- WORK ZONE
- TEMPORARY CONCRETE BARRIER
- DRUM

STAGE 1

NOTES

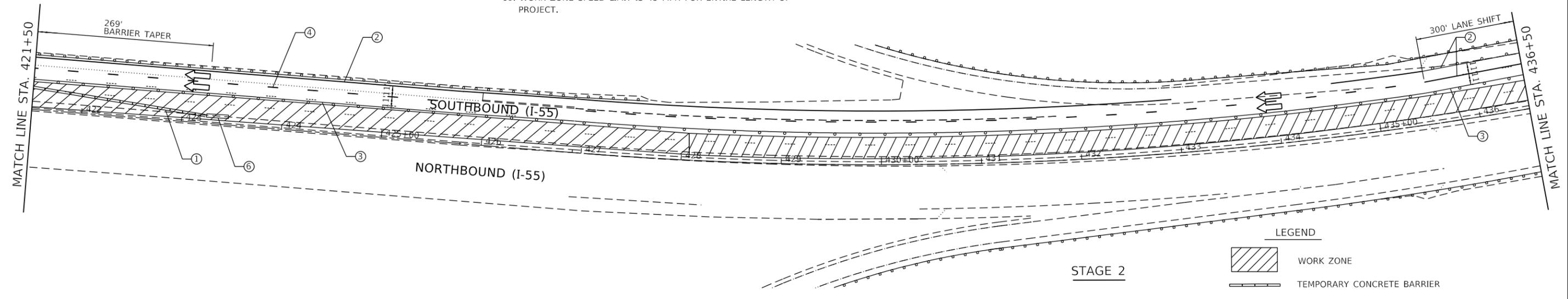
- 1 POSTED 65MPH SPEED LIMIT.
- 2 SEE BRIDGE STAGING TYPICAL SECTIONS FOR TEMPORARY BARRIER AND PAVEMENT MARKING OFFSETS
- 3 REMOVE CONFLICTING PAVEMENT MARKING
- 4 CONTRACTOR MUST FOLLOW STANDARD TC-09 FOR LANE SHIFT
- 5 SIGNING MUST BE PLACED IN ADVANCE OF THE LANE CLOSURE AS PER STANDARD 701400. SIGN W3-5(0)-48 MUST BE INSTALLED. (45 MPH)
- 6 TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATOR SHALL BE INSTALLED AT ALL STRUCTURES AS PER STANDARD TC-09.
- 7 SEE SUGGESTED SEQUENCE OF OPERATIONS FOR RUMBLE STRIP TREATMENT
- 8 DRUMS/BARRICADE TYPE MUST FOLLOW IDOT STANDARDS FOR SPECIFIC TYPE AT SPECIFIC LOCATIONS.
- 9 USE TC-17 FOR GUIDANCE ON PLACEMENT OF TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATORS.
10. WORK ZONE SPEED LIMIT IS 45 MPH FOR ENTIRE LENGTH OF PROJECT.

MOT LEGEND

- ① TEMPORARY CONCRETE BARRIER
- ② TEMPORARY PAVEMENT MARKING - LINE 4" (WHITE)
- ③ TEMPORARY PAVEMENT MARKING - LINE 4" (YELLOW)
- ④ TEMPORARY PAVEMENT MARKING - SKIP DASH - 5" (WHITE) (10' DASH, 30' SPACE)
- ⑤ TEMPORARY PAVEMENT MARKING - SKIP DASH - 5" (WHITE) (2' DASH, 6' SPACE)
- ⑥ ATTENUATOR, TEMPORARY, FULLY REDIRECTIVE, WIDE TEST LEVEL 3



EXIT RAMP MUST REMAIN OPEN
USE STANDARD 701411 FOR
EXIT RAMP CONFIGURATION
AND SIGNING



LEGEND

- WORK ZONE
- TEMPORARY CONCRETE BARRIER
- DRUM

STAGE 2

MODEL: SMOBELNAMES
FILE NUMBER: 171215



USER NAME = sUSERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = sSCALEs	CHECKED -	REVISED -
PLOT DATE = sDATES	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

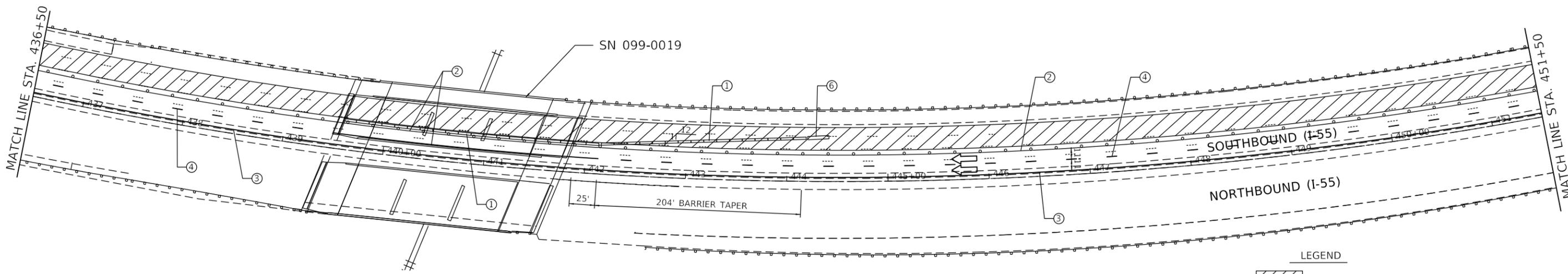
**SUGGESTED MAINTENANCE OF TRAFFIC AT BRIDGES
I-55 (SOUTH OF I-80 TO WEBER ROAD)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	27
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				



EXIT MUST REMAIN OPEN
USE STANDARD 701411 FOR
EXIT RAMP CONFIGURATION
AND SIGNING



LEGEND

- WORK ZONE
 - TEMPORARY CONCRETE BARRIER
 - DRUM
- STAGE 1**

NOTES

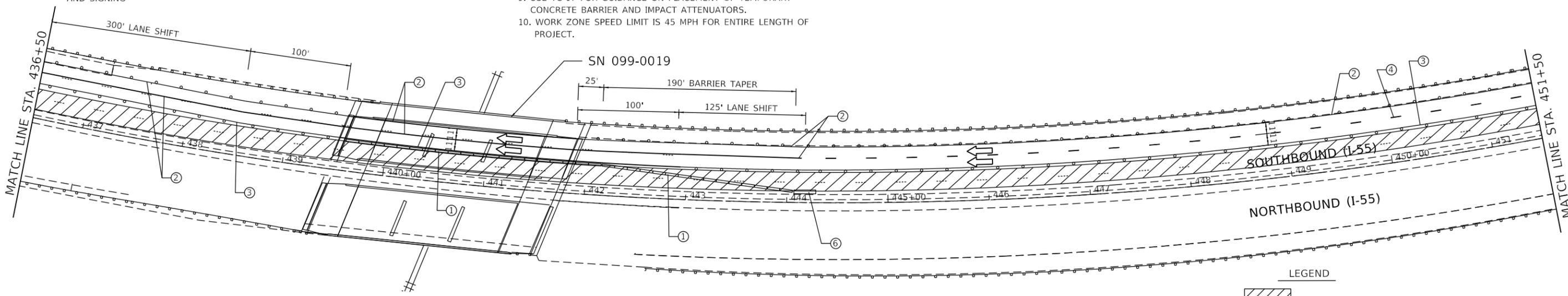
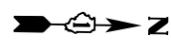
- 1 POSTED 65MPH SPEED LIMIT.
- 2 SEE BRIDGE STAGING TYPICAL SECTIONS FOR TEMPORARY BARRIER AND PAVEMENT MARKING OFFSETS
- 3 REMOVE CONFLICTING PAVEMENT MARKING
- 4 CONTRACTOR MUST FOLLOW STANDARD TC-09 FOR LANE SHIFT
- 5 SIGNING MUST BE PLACED IN ADVANCE OF THE LANE CLOSURE AS PER STANDARD 701400. SIGN W3-5(0)-48 MUST BE INSTALLED. (45 MPH)
- 6 TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATOR SHALL BE INSTALLED AT ALL STRUCTURES AS PER STANDARD TC-09.
- 7 SEE SUGGESTED SEQUENCE OF OPERATIONS FOR RUMBLE STRIP TREATMENT
8. DRUMS/BARRICADE TYPE MUST FOLLOW IDOT STANDARDS FOR SPECIFIC TYPE AT SPECIFIC LOCATIONS.
9. USE TC-17 FOR GUIDANCE ON PLACEMENT OF TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATORS.
10. WORK ZONE SPEED LIMIT IS 45 MPH FOR ENTIRE LENGTH OF PROJECT.

MOT LEGEND

- ① TEMPORARY CONCRETE BARRIER
- ② TEMPORARY PAVEMENT MARKING - LINE 4" (WHITE)
- ③ TEMPORARY PAVEMENT MARKING - LINE 4" (YELLOW)
- ④ TEMPORARY PAVEMENT MARKING - SKIP DASH - 5" (WHITE) (10' DASH, 30' SPACE)
- ⑤ TEMPORARY PAVEMENT MARKING - SKIP DASH - 5" (WHITE) (2' DASH, 6' SPACE)
- ⑥ ATTENUATOR, TEMPORARY, FULLY REDIRECTIVE, WIDE TEST LEVEL 3



EXIT MUST REMAIN OPEN
USE STANDARD 701411 FOR
EXIT RAMP CONFIGURATION
AND SIGNING



LEGEND

- WORK ZONE
 - TEMPORARY CONCRETE BARRIER
 - DRUM
- STAGE 2**

MODEL: SMOBELMARTS
FILE NUMBER: 611215

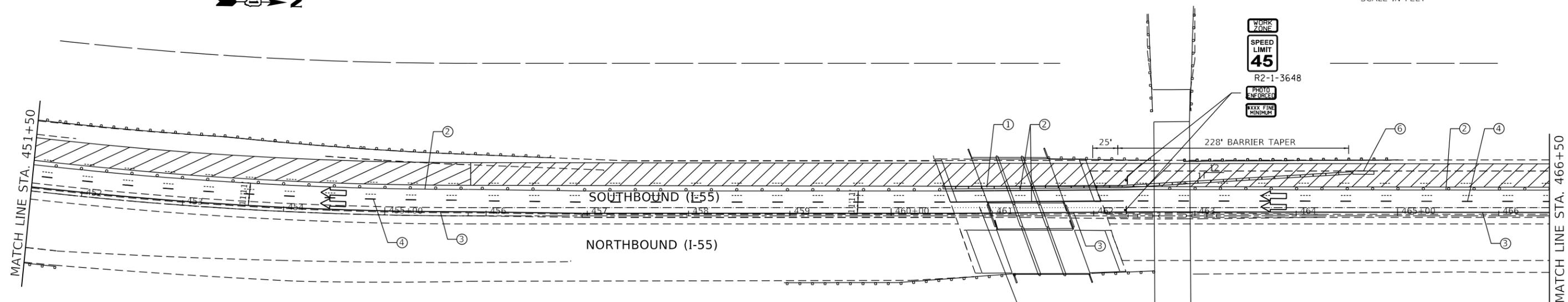
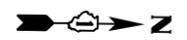


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PLOT DATE = \$DATES	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

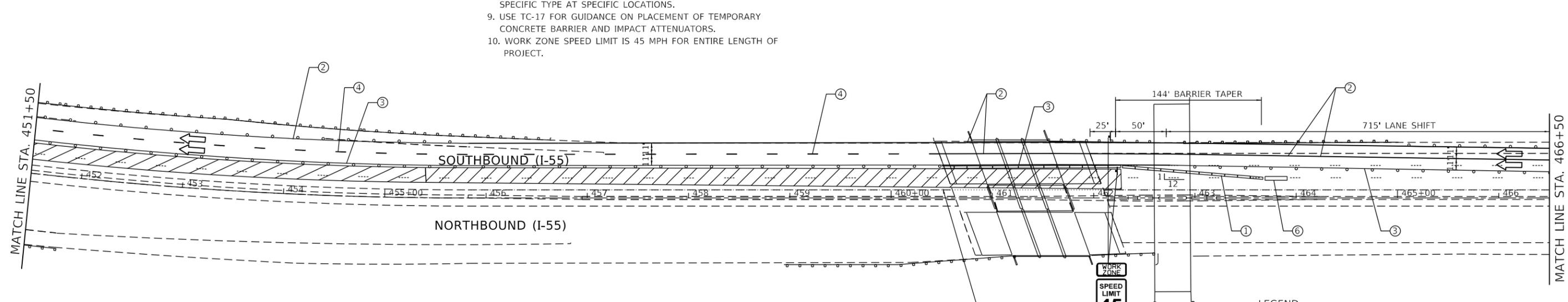
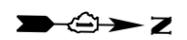
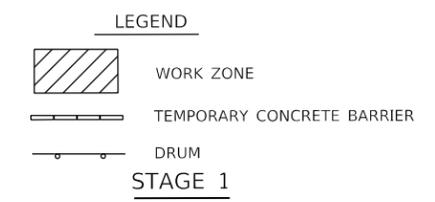
SUGGESTED MAINTENANCE OF TRAFFIC AT BRIDGES I-55 (SOUTH OF I-80 TO WEBER ROAD)			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	28
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				



- NOTES**
- 1 POSTED 65MPH SPEED LIMIT.
 - 2 SEE BRIDGE STAGING TYPICAL SECTIONS FOR TEMPORARY BARRIER AND PAVEMENT MARKING OFFSETS
 - 3 REMOVE CONFLICTING PAVEMENT MARKING
 - 4 CONTRACTOR MUST FOLLOW STANDARD TC-09 FOR LANE SHIFT
 - 5 SIGNING MUST BE PLACED IN ADVANCE OF THE LANE CLOSURE AS PER STANDARD 701400. SIGN W3-5(0)-48 MUST BE INSTALLED. (45 MPH)
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 9. USE TC-17 FOR GUIDANCE ON PLACEMENT OF TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATORS.
 10. WORK ZONE SPEED LIMIT IS 45 MPH FOR ENTIRE LENGTH OF PROJECT.

- MOT LEGEND**
- ① TEMPORARY CONCRETE BARRIER
 - ② TEMPORARY PAVEMENT MARKING - LINE 4" (WHITE)
 - ③ TEMPORARY PAVEMENT MARKING - LINE 4" (YELLOW)
 - ④ TEMPORARY PAVEMENT MARKING - SKIP DASH - 5" (WHITE) (10' DASH, 30' SPACE)
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 - ⑥ ATTENUATOR, TEMPORARY, FULLY REDIRECTIVE, WIDE TEST LEVEL 3



MODEL: SMOBLENAMES
FILE: SMOBLENAMES.PLT



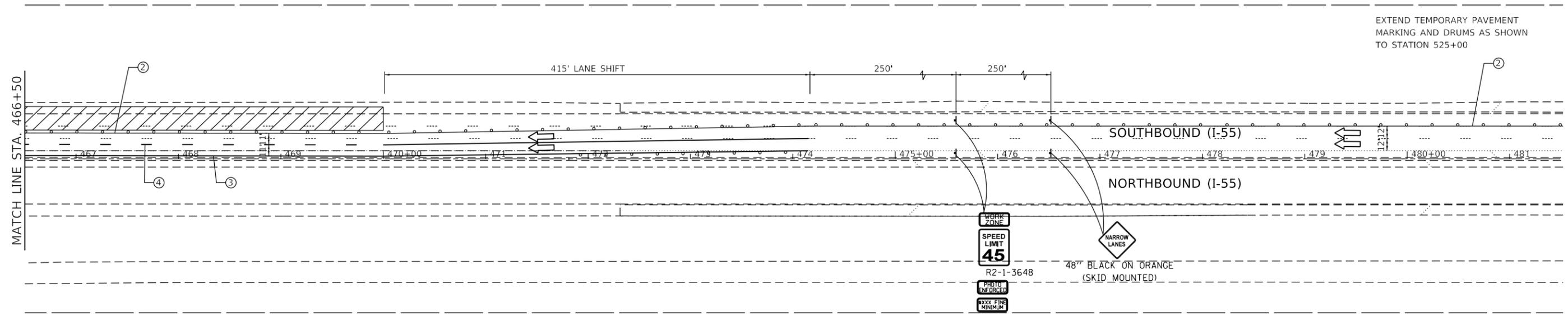
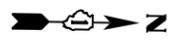
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PLOT DATE = \$DATES	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED MAINTENANCE OF TRAFFIC AT BRIDGES
I-55 (SOUTH OF I-80 TO WEBER ROAD)**

SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	29
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				



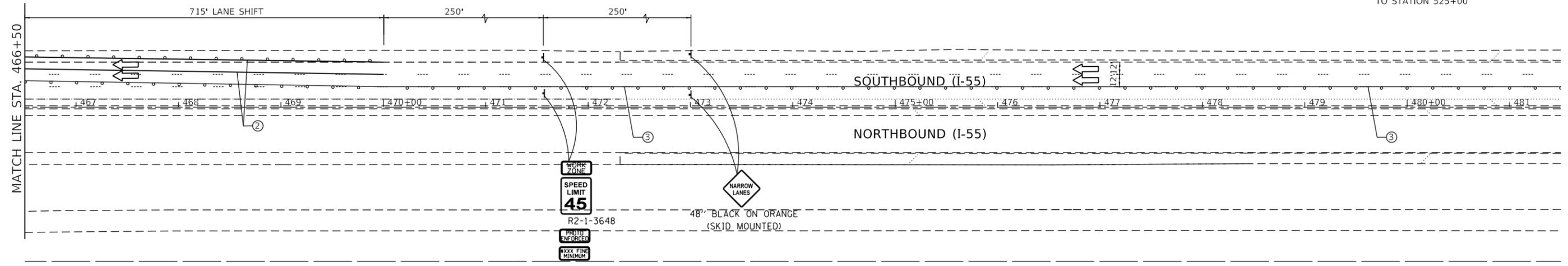
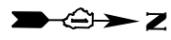
EXTEND TEMPORARY PAVEMENT MARKING AND DRUMS AS SHOWN TO STATION 525+00

NOTES

- 1 POSTED 65MPH SPEED LIMIT.
- 2 SEE BRIDGE STAGING TYPICAL SECTIONS FOR TEMPORARY BARRIER AND PAVEMENT MARKING OFFSETS
- 3 REMOVE CONFLICTING PAVEMENT MARKING
- 4 CONTRACTOR MUST FOLLOW STANDARD TC-09 FOR LANE SHIFT
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9. USE TC-17 FOR GUIDANCE ON PLACEMENT OF TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATORS.
10. WORK ZONE SPEED LIMIT IS 45 MPH FOR ENTIRE LENGTH OF PROJECT.

MOT LEGEND

- ① TEMPORARY CONCRETE BARRIER
- ② TEMPORARY PAVEMENT MARKING - LINE 4" (WHITE)
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- ⑤ TEMPORARY PAVEMENT MARKING - SKIP DASH - 5" (WHITE) (2' DASH, 6' SPACE)
- ⑥ ATTENUATOR, TEMPORARY, FULLY REDIRECTIVE, WIDE TEST LEVEL 3



EXTEND TEMPORARY PAVEMENT MARKING AND DRUMS AS SHOWN TO STATION 525+00

MODEL: SMOBELMAMES
FILE: 2020-253-PP-112



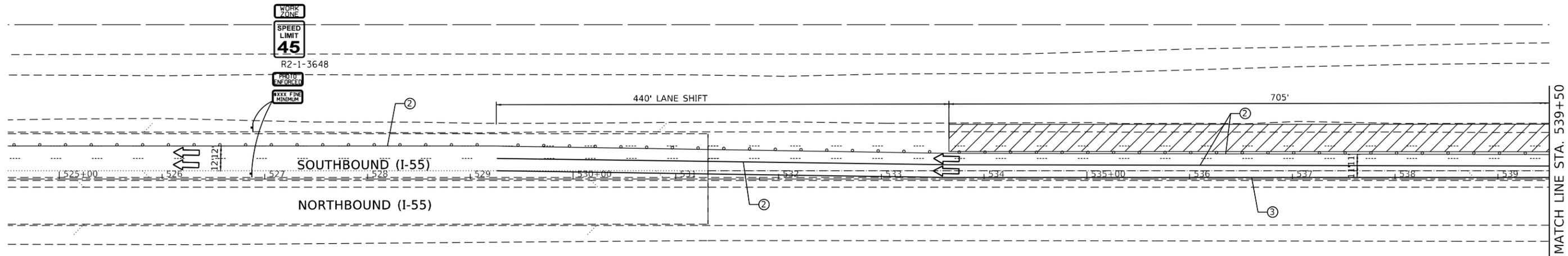
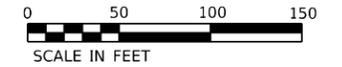
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	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED MAINTENANCE OF TRAFFIC AT BRIDGES
I-55 (SOUTH OF I-80 TO WEBER ROAD)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	30
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				



EXTEND TEMPORARY PAVEMENT MARKING AND DRUMS AS SHOWN TO STATION 481+00

NOTES

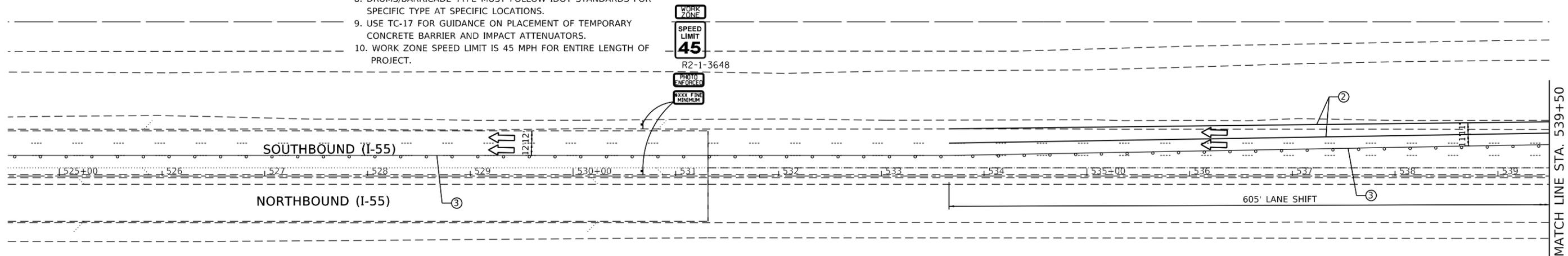
- 1 POSTED 65MPH SPEED LIMIT.
- 2 SEE BRIDGE STAGING TYPICAL SECTIONS FOR TEMPORARY BARRIER AND PAVEMENT MARKING OFFSETS
- 3 REMOVE CONFLICTING PAVEMENT MARKING
- 4 CONTRACTOR MUST FOLLOW STANDARD TC-09 FOR LANE SHIFT
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- 7 SEE SUGGESTED SEQUENCE OF OPERATIONS FOR RUMBLE STRIP TREATMENT
8. DRUMS/BARRICADE TYPE MUST FOLLOW IDOT STANDARDS FOR SPECIFIC TYPE AT SPECIFIC LOCATIONS.
9. USE TC-17 FOR GUIDANCE ON PLACEMENT OF TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATORS.
10. WORK ZONE SPEED LIMIT IS 45 MPH FOR ENTIRE LENGTH OF PROJECT.

MOT LEGEND

- ① TEMPORARY CONCRETE BARRIER
- ② TEMPORARY PAVEMENT MARKING - LINE 4" (WHITE)
- ③ TEMPORARY PAVEMENT MARKING - LINE 4" (YELLOW)
- ④ TEMPORARY PAVEMENT MARKING - SKIP DASH - 5" (WHITE) (10' DASH, 30' SPACE)
- ⑤ TEMPORARY PAVEMENT MARKING - SKIP DASH - 5" (WHITE) (2' DASH, 6' SPACE)
- ⑥ ATTENUATOR, TEMPORARY, FULLY REDIRECTIVE, WIDE TEST LEVEL 3

LEGEND

- WORK ZONE
 - TEMPORARY CONCRETE BARRIER
 - DRUM
- STAGE 1**



EXTEND TEMPORARY PAVEMENT MARKING AND DRUMS AS SHOWN TO STATION 481+00

LEGEND

- WORK ZONE
 - TEMPORARY CONCRETE BARRIER
 - DRUM
- STAGE 2**

MODEL: SMOBELNAMES
FILE NUMBER: 311215



USER NAME = \$USERS	DESIGNED -	REVISED -
PLOT SCALE = \$SCALE\$	DRAWN -	REVISED -
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	DATE -	REVISED -

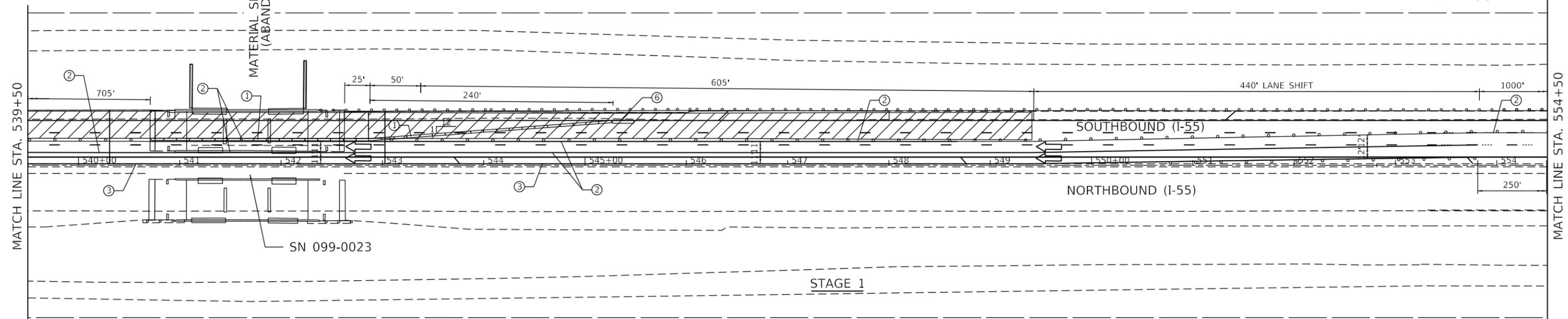
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED MAINTENANCE OF TRAFFIC
I-55 (SOUTH OF I-80 TO WEBER ROAD)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	31
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

MOT PAVEMENT MARKING AND WORK ZONE CONTINUES SOUTH TO TO SN 099-0016



NOTES

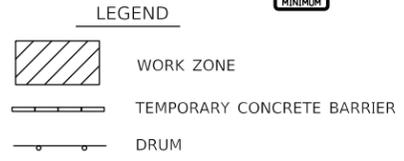
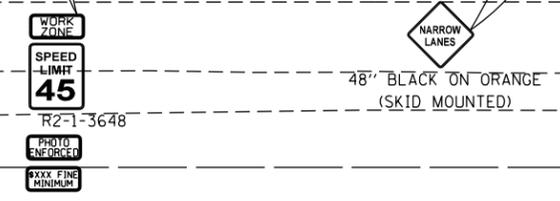
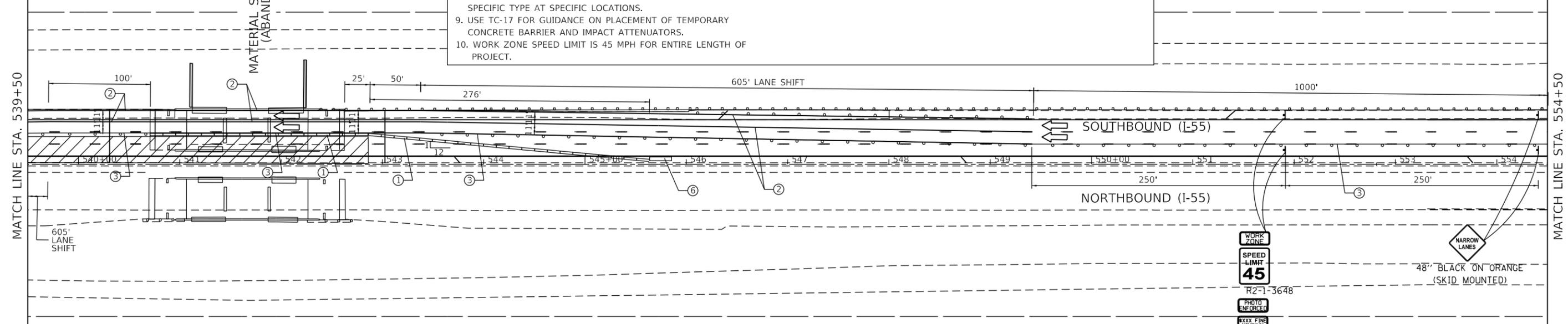
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9. USE TC-17 FOR GUIDANCE ON PLACEMENT OF TEMPORARY CONCRETE BARRIER AND IMPACT ATTENUATORS.
10. WORK ZONE SPEED LIMIT IS 45 MPH FOR ENTIRE LENGTH OF PROJECT.

MOT LEGEND

- ① TEMPORARY CONCRETE BARRIER
- ② TEMPORARY PAVEMENT MARKING - LINE 4" (WHITE)
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- ⑥ ATTENUATOR, TEMPORARY, FULLY REDIRECTIVE, WIDE TEST LEVEL 3



MOT PAVEMENT MARKING AND WORK ZONE CONTINUES SOUTH TO TO SN 099-0016



MODEL: SMOBELNAMES
FILE: 2020-253-PP-0112



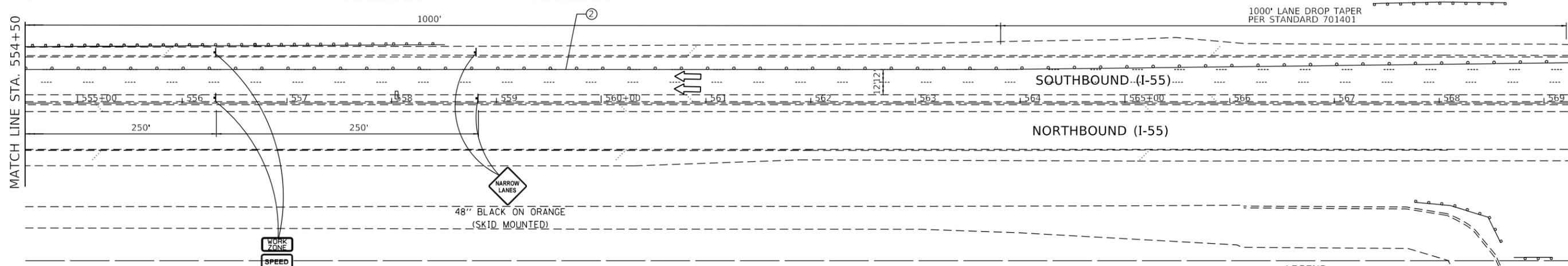
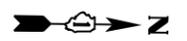
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

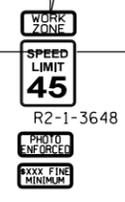
**SUGGESTED MAINTENANCE OF TRAFFIC AT BRIDGES
I-55 (SOUTH OF I-80 TO WEBER ROAD)**

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	32
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

SCALE: SHEET OF SHEETS STA. TO STA.



SEE NOTE 5
FOR ADVANCED
SIGNING



NOTES

- 1 POSTED 65MPH SPEED LIMIT.
- 2 SEE BRIDGE STAGING TYPICAL SECTIONS FOR TEMPORARY BARRIER AND PAVEMENT MARKING OFFSETS
- 3 REMOVE CONFLICTING PAVEMENT MARKING
- 4 CONTRACTOR MUST FOLLOW STANDARD TC-09 FOR LANE SHIFT
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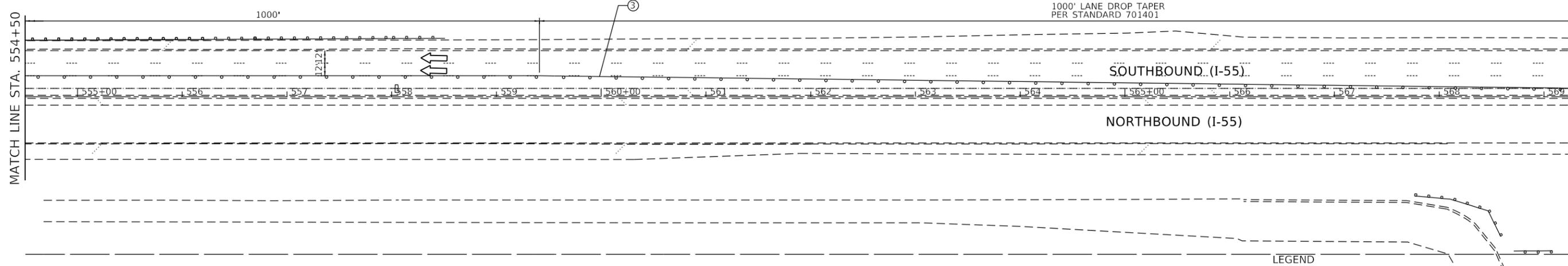
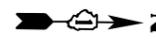
MOT LEGEND

- ① TEMPORARY CONCRETE BARRIER
- ② TEMPORARY PAVEMENT MARKING - LINE 4" (WHITE)
- ③ TEMPORARY PAVEMENT MARKING - LINE 4" (YELLOW)
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- ⑥ ATTENUATOR, TEMPORARY, FULLY REDIRECTIVE, WIDE TEST LEVEL 3

LEGEND

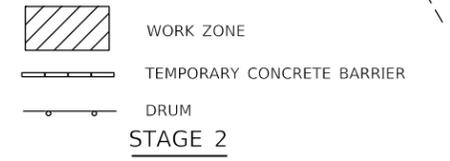


STAGE 1



SEE NOTE 5
FOR ADVANCED
SIGNING

LEGEND



STAGE 2

MODEL: SMOBELNAMES
FILE: M001P: STILES



USER NAME = \$USERS	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED -	REVISED -
PLOT DATE = \$DATES	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED MAINTENANCE OF TRAFFIC
I-55 (SOUTH OF I-80 TO WEBER ROAD)**

SCALE:	SHEET	OF	SHEETS	STA.	TO	STA.
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	33
CONTRACT NO. 62N22				
		ILLINOIS	FED. AID PROJECT	

886+83.31

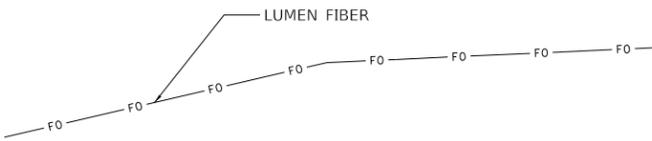
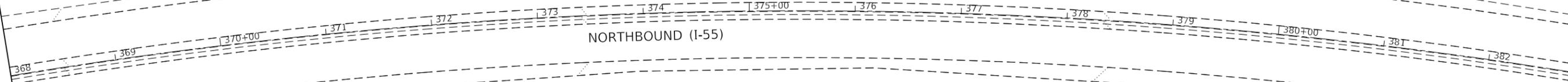


FRONTAGE ROAD

SOUTHBOUND (I-55)

NORTHBOUND (I-55)

MATCH LINE STA. 383+00



ROADWAY WORK LEGEND		PAVEMENT MARKING LEGEND	
(A)	PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 2" OVER BRIDGE APPROACH SLAB	①	PROP. MODIFIED URETHANE PVT MRK - LINE 4", WHITE (TYP.)
(B)	PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 2" BUTT JOINT (PROPOSED HMA TAPER, REFER TO DETAIL)	②	PROP. MODIFIED URETHANE PVT MRK - LINE 4", YELLOW (TYP.)
(C)	PROP. HOT-MIX ASPHALT SHOULDERS, 10" (SEE TYPICAL SECTIONS FOR SHOULDER STATION LIMITS)	③	PROP. MODIFIED URETHANE PVT MRK, 5", 10'-DASH, 30'-SKIP, WHITE (TYP.)
		④	PROP. MODIFIED URETHANE PVT MRK - LINE 8", WHITE (TYP.)
		⑤	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 500' SPACING, WHITE (TYP.)
		⑥	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 500' SPACING, YELLOW (TYP.)
		⑦	PROP. PREFORMED PLASTIC PVT MRK, TYPE B-INLAID - LINE 5", 10' DASH, 30' SKIP, WHITE, WITH PREFORMED PLASTIC PVT MRK, TYPE D - LINE 8", CONTRAST GROOVING FOR RECESSED PVT MRK - 9" (FOR PCC BRIDGE SURFACE ONLY)
		⑧	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 30' SPACING, CHEVRON, WHITE, (TYP.)

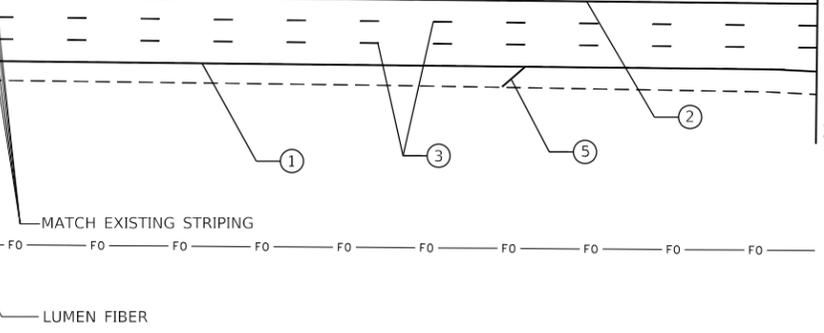
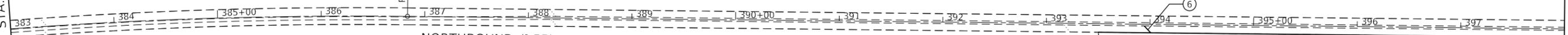
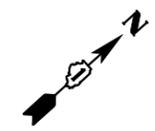
FRONTAGE ROAD

SOUTHBOUND (I-55)

NORTHBOUND (I-55)

MATCH LINE STA. 383+00

MATCH LINE STA. 398+00



MODEL: 140DELENAME
FILE: 140DELENAME.DWG

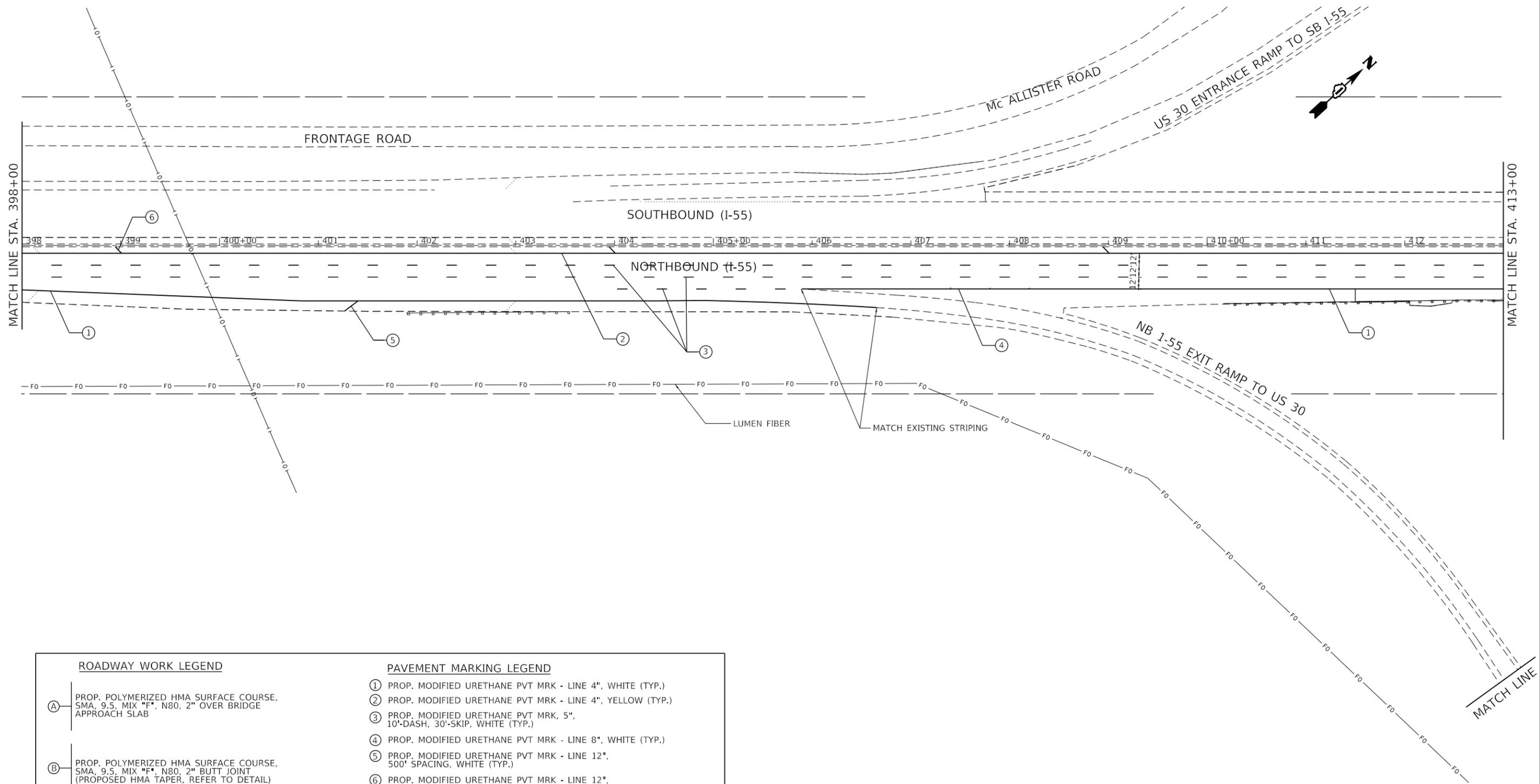


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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN			
I-55 (SOUTH OF I-80 TO WEBER ROAD)			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	34
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				



ROADWAY WORK LEGEND

- (A) PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 2" OVER BRIDGE APPROACH SLAB
- (B) PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 2" BUTT JOINT (PROPOSED HMA TAPER, REFER TO DETAIL)
- (C) PROP. HOT-MIX ASPHALT SHOULDERS, 10" (SEE TYPICAL SECTIONS FOR SHOULDER STATION LIMITS)

PAVEMENT MARKING LEGEND

- (1) PROP. MODIFIED URETHANE PVT MRK - LINE 4", WHITE (TYP.)
- (2) PROP. MODIFIED URETHANE PVT MRK - LINE 4", YELLOW (TYP.)
- (3) PROP. MODIFIED URETHANE PVT MRK, 5", 10'-DASH, 30'-SKIP, WHITE (TYP.)
- (4) PROP. MODIFIED URETHANE PVT MRK - LINE 8", WHITE (TYP.)
- (5) PROP. MODIFIED URETHANE PVT MRK - LINE 12", 500' SPACING, WHITE (TYP.)
- (6) PROP. MODIFIED URETHANE PVT MRK - LINE 12", 500' SPACING, YELLOW (TYP.)
- (7) PROP. PREFORMED PLASTIC PVT MRK, TYPE B-INLAID - LINE 5", 10' DASH, 30' SKIP, WHITE, WITH PREFORMED PLASTIC PVT MRK, TYPE D - LINE 8", CONTRAST GROOVING FOR RECESSED PVT MRK - 9" (FOR PCC BRIDGE SURFACE ONLY)
- (8) PROP. MODIFIED URETHANE PVT MRK - LINE 12", 30' SPACING, CHEVRON, WHITE, (TYP.)

MODEL: 140DELENAME
FILE: 140DELENAME.PLT

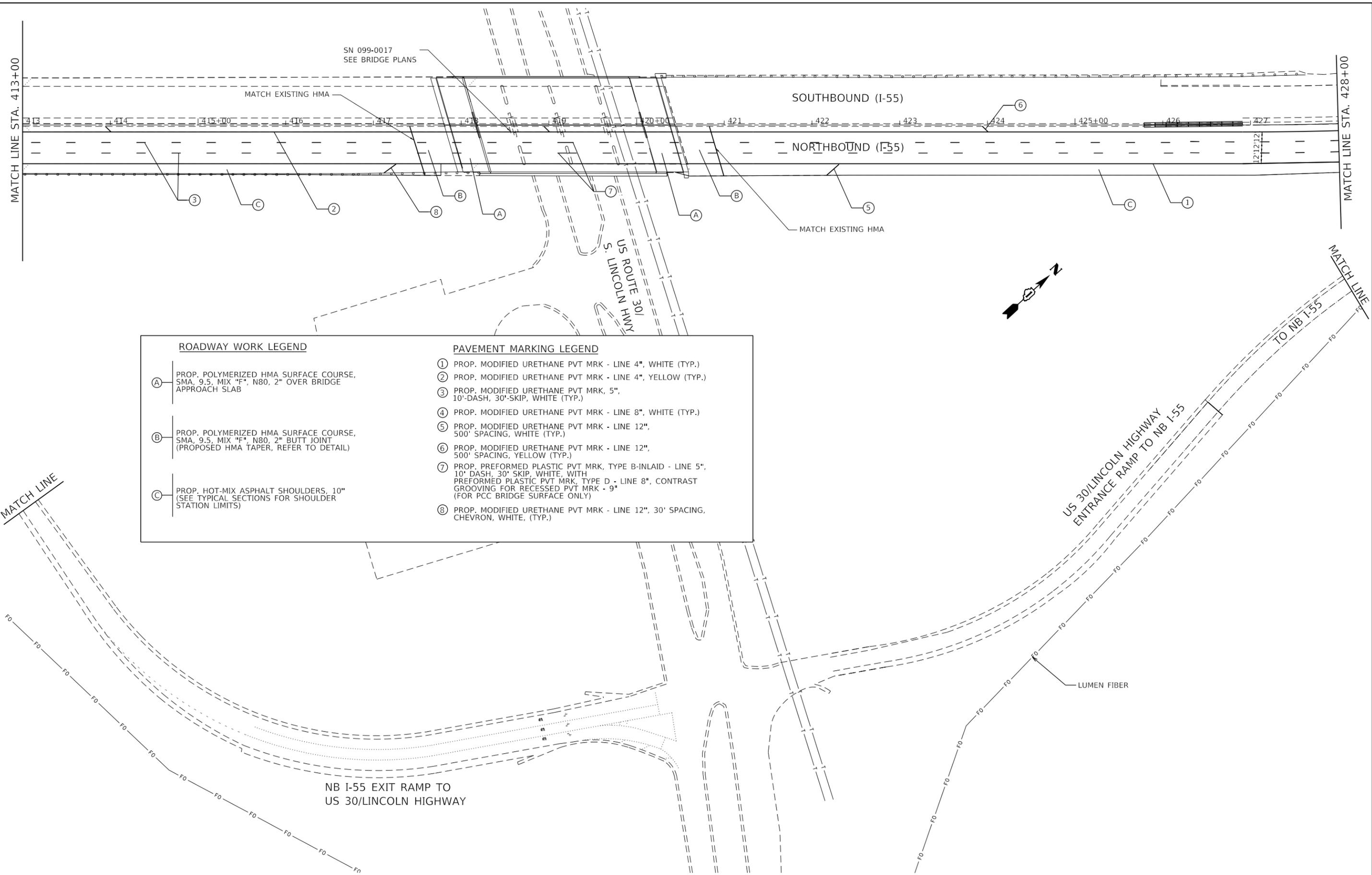


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	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY PLAN			
I-55 (SOUTH OF I-80 TO WEBER ROAD)			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	35
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				



ROADWAY WORK LEGEND		PAVEMENT MARKING LEGEND	
(A)	PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 2" OVER BRIDGE APPROACH SLAB	(1)	PROP. MODIFIED URETHANE PVT MRK - LINE 4", WHITE (TYP.)
(B)	PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 2" BUTT JOINT (PROPOSED HMA TAPER, REFER TO DETAIL)	(2)	PROP. MODIFIED URETHANE PVT MRK - LINE 4", YELLOW (TYP.)
(C)	PROP. HOT-MIX ASPHALT SHOULDERS, 10" (SEE TYPICAL SECTIONS FOR SHOULDER STATION LIMITS)	(3)	PROP. MODIFIED URETHANE PVT MRK, 5", 10'-DASH, 30'-SKIP, WHITE (TYP.)
		(4)	PROP. MODIFIED URETHANE PVT MRK - LINE 8", WHITE (TYP.)
		(5)	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 500' SPACING, WHITE (TYP.)
		(6)	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 500' SPACING, YELLOW (TYP.)
		(7)	PROP. PREFORMED PLASTIC PVT MRK, TYPE B-INLAID - LINE 5", 10' DASH, 30' SKIP, WHITE, WITH PREFORMED PLASTIC PVT MRK, TYPE D - LINE 8", CONTRAST GROOVING FOR RECESSED PVT MRK - 9" (FOR PCC BRIDGE SURFACE ONLY)
		(8)	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 30' SPACING, CHEVRON, WHITE, (TYP.)

MODEL: SMOBELNAMES
FILE: SMOBELNAMES_STYLES

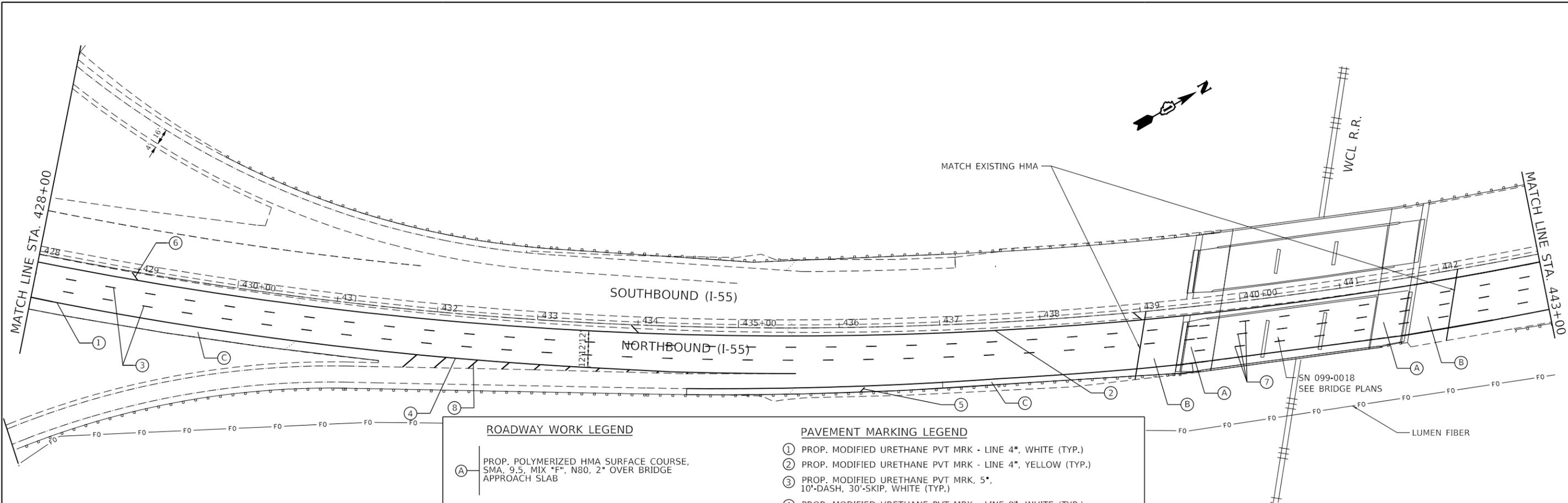


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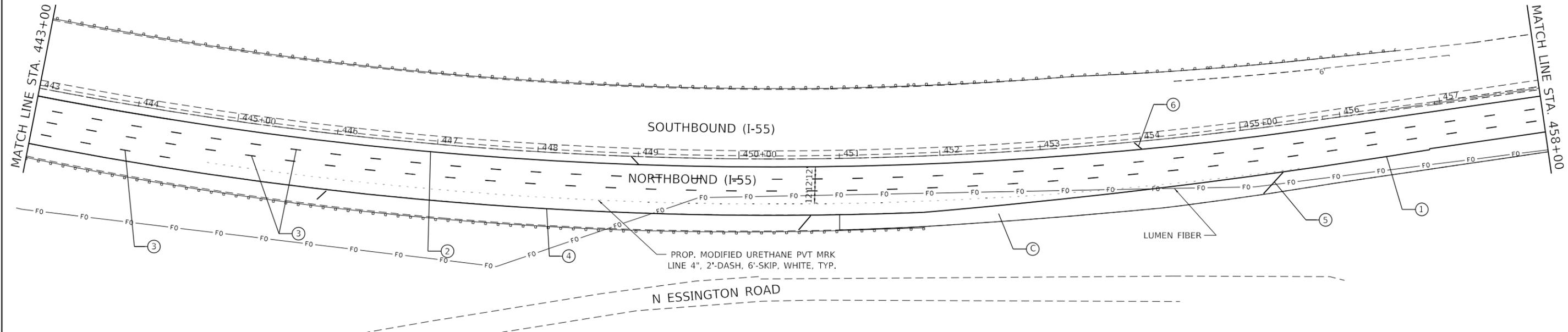
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY PLAN			
I-55 (SOUTH OF I-80 TO WEBER ROAD)			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	36
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				



ROADWAY WORK LEGEND		PAVEMENT MARKING LEGEND	
(A)	PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 2" OVER BRIDGE APPROACH SLAB	①	PROP. MODIFIED URETHANE PVT MRK - LINE 4", WHITE (TYP.)
(B)	PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 2" BUTT JOINT (PROPOSED HMA TAPER, REFER TO DETAIL)	②	PROP. MODIFIED URETHANE PVT MRK - LINE 4", YELLOW (TYP.)
(C)	PROP. HOT-MIX ASPHALT SHOULDERS, 10" (SEE TYPICAL SECTIONS FOR SHOULDER STATION LIMITS)	③	PROP. MODIFIED URETHANE PVT MRK, 5", 10'-DASH, 30'-SKIP, WHITE (TYP.)
		④	PROP. MODIFIED URETHANE PVT MRK - LINE 8", WHITE (TYP.)
		⑤	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 500' SPACING, WHITE (TYP.)
		⑥	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 500' SPACING, YELLOW (TYP.)
		⑦	PROP. PREFORMED PLASTIC PVT MRK, TYPE B-INLAID - LINE 5", 10' DASH, 30' SKIP, WHITE, WITH PREFORMED PLASTIC PVT MRK, TYPE D - LINE 8", CONTRAST GROOVING FOR RECESSED PVT MRK - 9" (FOR PCC BRIDGE SURFACE ONLY)
		⑧	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 30' SPACING, CHEVRON, WHITE, (TYP.)



MODEL: 14082618.MXD
FILE: 14082618.DWG

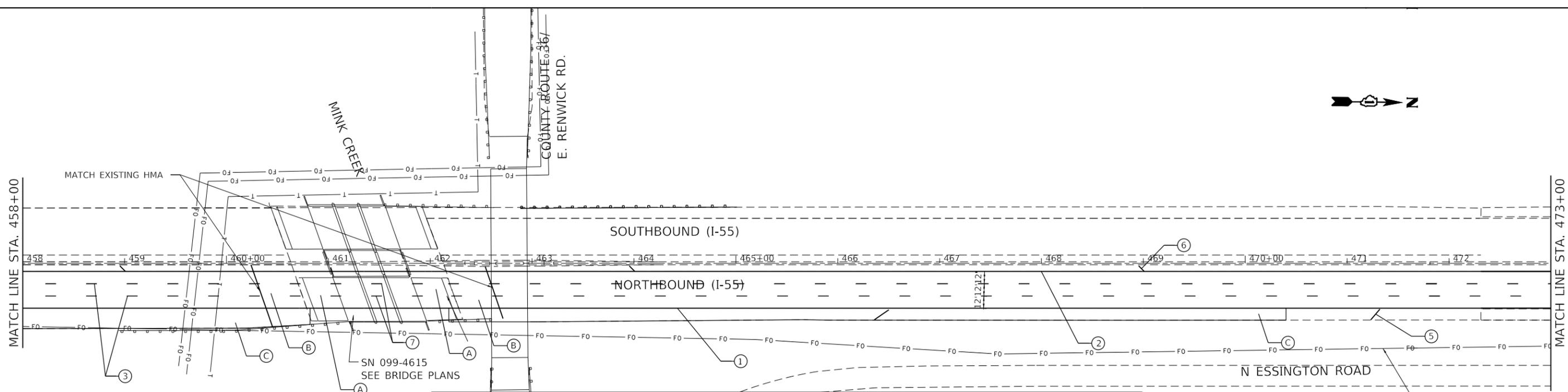


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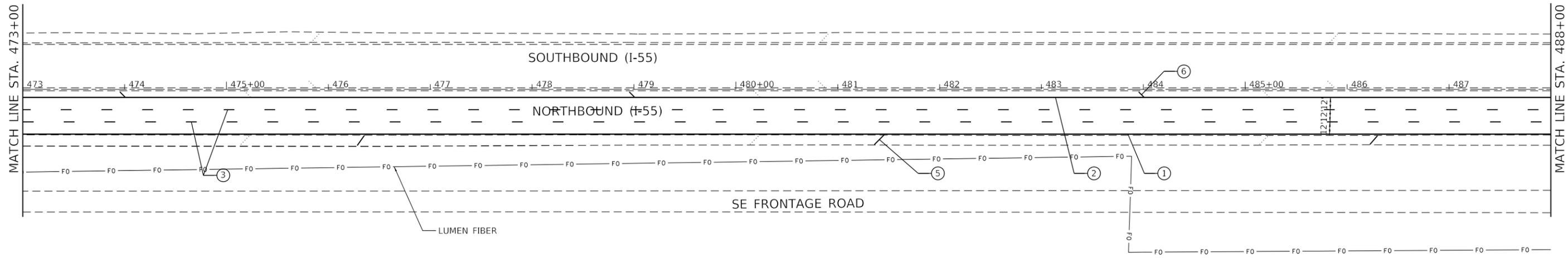
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY PLAN			
I-55 (SOUTH OF I-80 TO WEBER ROAD)			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	37
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				



ROADWAY WORK LEGEND		PAVEMENT MARKING LEGEND	
(A)	PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 2" OVER BRIDGE APPROACH SLAB	(1)	PROP. MODIFIED URETHANE PVT MRK - LINE 4", WHITE (TYP.)
(B)	PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 2" BUTT JOINT (PROPOSED HMA TAPER, REFER TO DETAIL)	(2)	PROP. MODIFIED URETHANE PVT MRK - LINE 4", YELLOW (TYP.)
(C)	PROP. HOT-MIX ASPHALT SHOULDERS, 10" (SEE TYPICAL SECTIONS FOR SHOULDER STATION LIMITS)	(3)	PROP. MODIFIED URETHANE PVT MRK, 5", 10'-DASH, 30'-SKIP, WHITE (TYP.)
		(4)	PROP. MODIFIED URETHANE PVT MRK - LINE 8", WHITE (TYP.)
		(5)	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 500' SPACING, WHITE (TYP.)
		(6)	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 500' SPACING, YELLOW (TYP.)
		(7)	PROP. PREFORMED PLASTIC PVT MRK, TYPE B-INLAID - LINE 5", 10' DASH, 30' SKIP, WHITE, WITH PREFORMED PLASTIC PVT MRK, TYPE D - LINE 8", CONTRAST GROOVING FOR RECESSED PVT MRK - 9" (FOR PCC BRIDGE SURFACE ONLY)
		(8)	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 30' SPACING, CHEVRON, WHITE, (TYP.)



MODEL: 140DELENAME
FILE: 140DELENAME.PLT

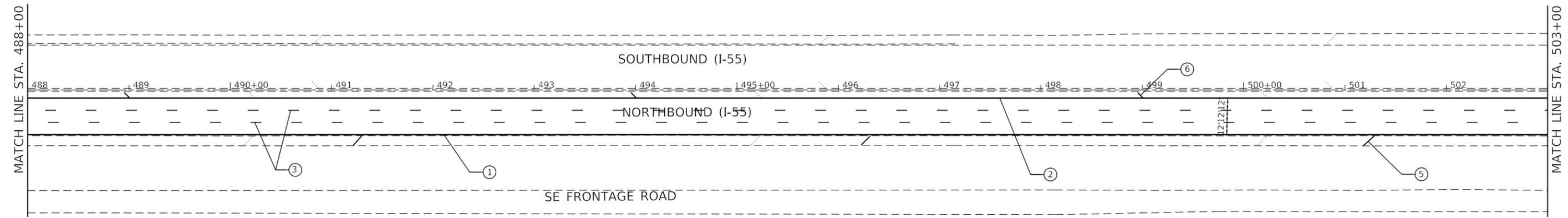
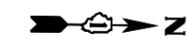


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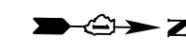
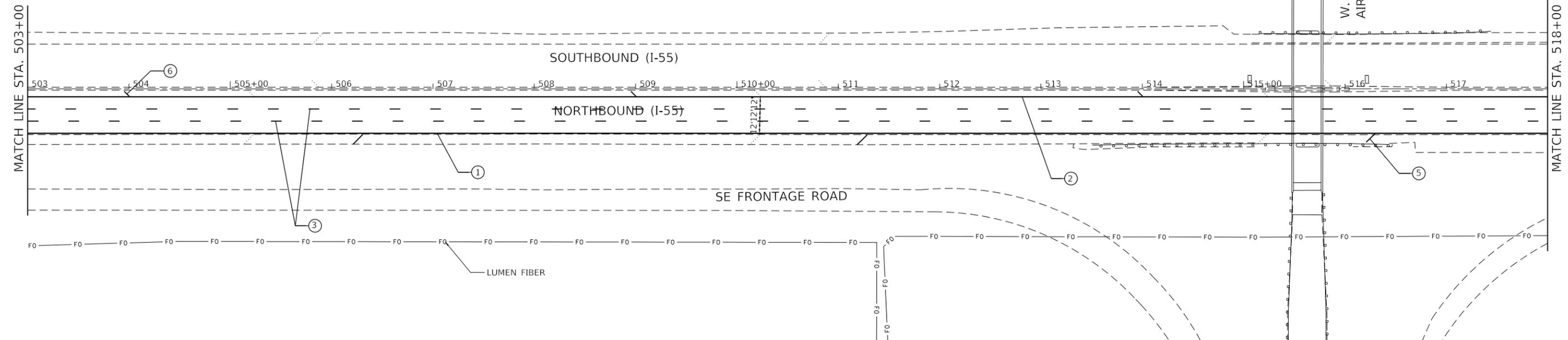
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY PLAN			
I-55 (SOUTH OF I-80 TO WEBER ROAD)			
SCALE:	SHEET	OF	SHEETS
	STA.	TO	STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	38
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				



ROADWAY WORK LEGEND		PAVEMENT MARKING LEGEND	
(A)	PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 2" OVER BRIDGE APPROACH SLAB	(1)	PROP. MODIFIED URETHANE PVT MRK - LINE 4", WHITE (TYP.)
(B)	PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 2" BUTT JOINT (PROPOSED HMA TAPER, REFER TO DETAIL)	(2)	PROP. MODIFIED URETHANE PVT MRK - LINE 4", YELLOW (TYP.)
(C)	PROP. HOT-MIX ASPHALT SHOULDERS, 10" (SEE TYPICAL SECTIONS FOR SHOULDER STATION LIMITS)	(3)	PROP. MODIFIED URETHANE PVT MRK, 5", 10'-DASH, 30'-SKIP, WHITE (TYP.)
		(4)	PROP. MODIFIED URETHANE PVT MRK - LINE 8", WHITE (TYP.)
		(5)	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 500' SPACING, WHITE (TYP.)
		(6)	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 500' SPACING, YELLOW (TYP.)
		(7)	PROP. PREFORMED PLASTIC PVT MRK, TYPE B-INLAID - LINE 5", 10' DASH, 30' SKIP, WHITE, WITH PREFORMED PLASTIC PVT MRK, TYPE D - LINE 8", CONTRAST GROOVING FOR RECESSED PVT MRK - 9" (FOR PCC BRIDGE SURFACE ONLY)
		(8)	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 30' SPACING, CHEVRON, WHITE, (TYP.)



MODEL: 140DELMARIES
FILE: 140MAR_21123

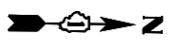
GR2EF
8501 W. Higgins Road, Suite 280
Chicago, Illinois 60631
(773) 398-0112

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PLOT DATE = \$DATE\$	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY PLAN			
I-55 (SOUTH OF I-80 TO WEBER ROAD)			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	39
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				



MATCH LINE STA. 518+00

MATCH LINE STA. 533+00

FRONTAGE ROAD

SOUTHBOUND (I-55)

NORTHBOUND (I-55)

SE FRONTAGE ROAD

LUMEN FIBER

ROADWAY WORK LEGEND

- (A) PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 2" OVER BRIDGE APPROACH SLAB
- (B) PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 2" BUTT JOINT (PROPOSED HMA TAPER, REFER TO DETAIL)
- (C) PROP. HOT-MIX ASPHALT SHOULDERS, 10" (SEE TYPICAL SECTIONS FOR SHOULDER STATION LIMITS)

PAVEMENT MARKING LEGEND

- (1) PROP. MODIFIED URETHANE PVT MRK - LINE 4", WHITE (TYP.)
- (2) PROP. MODIFIED URETHANE PVT MRK - LINE 4", YELLOW (TYP.)
- (3) PROP. MODIFIED URETHANE PVT MRK, 5", 10'-DASH, 30'-SKIP, WHITE (TYP.)
- (4) PROP. MODIFIED URETHANE PVT MRK - LINE 8", WHITE (TYP.)
- (5) PROP. MODIFIED URETHANE PVT MRK - LINE 12", 500' SPACING, WHITE (TYP.)
- (6) PROP. MODIFIED URETHANE PVT MRK - LINE 12", 500' SPACING, YELLOW (TYP.)
- (7) PROP. PREFORMED PLASTIC PVT MRK, TYPE B-INLAID - LINE 5", 10' DASH, 30' SKIP, WHITE, WITH PREFORMED PLASTIC PVT MRK, TYPE D - LINE 8", CONTRAST GROOVING FOR RECESSED PVT MRK - 9" (FOR PCC BRIDGE SURFACE ONLY)
- (8) PROP. MODIFIED URETHANE PVT MRK - LINE 12", 30' SPACING, CHEVRON, WHITE, (TYP.)

MATCH LINE STA. 533+00

MATCH LINE STA. 548+00

FRONTAGE ROAD

MATCH EXISTING HMA

SN 099-0022
SEE BRIDGE PLANS

MATCH EXISTING HMA

SOUTHBOUND (I-55)

NORTHBOUND (I-55)

LUMEN FIBER

SE FRONTAGE ROAD

MATERIAL
SERVICE
RAILROAD
(ABANDONED)

MODEL: \$MODELNAME\$
FILE: \$NAME\$. \$FILE\$

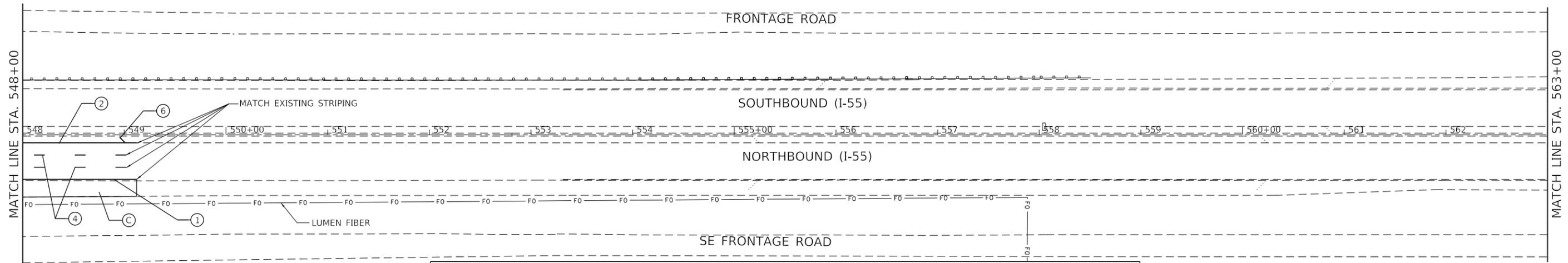


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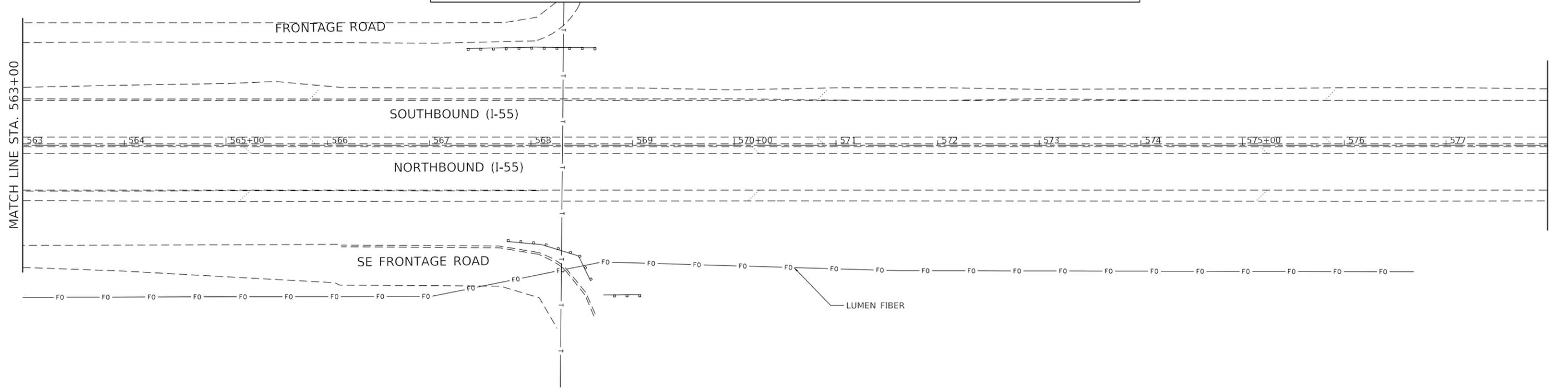
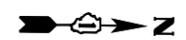
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY PLAN			
I-55 (SOUTH OF I-80 TO WEBER ROAD)			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	40
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				



ROADWAY WORK LEGEND		PAVEMENT MARKING LEGEND	
(A)	PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 2" OVER BRIDGE APPROACH SLAB	①	PROP. MODIFIED URETHANE PVT MRK - LINE 4", WHITE (TYP.)
(B)	PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 2" BUTT JOINT (PROPOSED HMA TAPER, REFER TO DETAIL)	②	PROP. MODIFIED URETHANE PVT MRK - LINE 4", YELLOW (TYP.)
(C)	PROP. HOT-MIX ASPHALT SHOULDERS, 10" (SEE TYPICAL SECTIONS FOR SHOULDER STATION LIMITS)	③	PROP. MODIFIED URETHANE PVT MRK, 5", 10'-DASH, 30'-SKIP, WHITE (TYP.)
		④	PROP. MODIFIED URETHANE PVT MRK - LINE 8", WHITE (TYP.)
		⑤	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 500' SPACING, WHITE (TYP.)
		⑥	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 500' SPACING, YELLOW (TYP.)
		⑦	PROP. PREFORMED PLASTIC PVT MRK, TYPE B-INLAID - LINE 5", 10' DASH, 30' SKIP, WHITE, WITH PREFORMED PLASTIC PVT MRK, TYPE D - LINE 8", CONTRAST GROOVING FOR RECESSED PVT MRK - 9" (FOR PCC BRIDGE SURFACE ONLY)
		⑧	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 30' SPACING, CHEVRON, WHITE, (TYP.)



MODEL: 140DELMARIES
FILE: 140MAR_21123



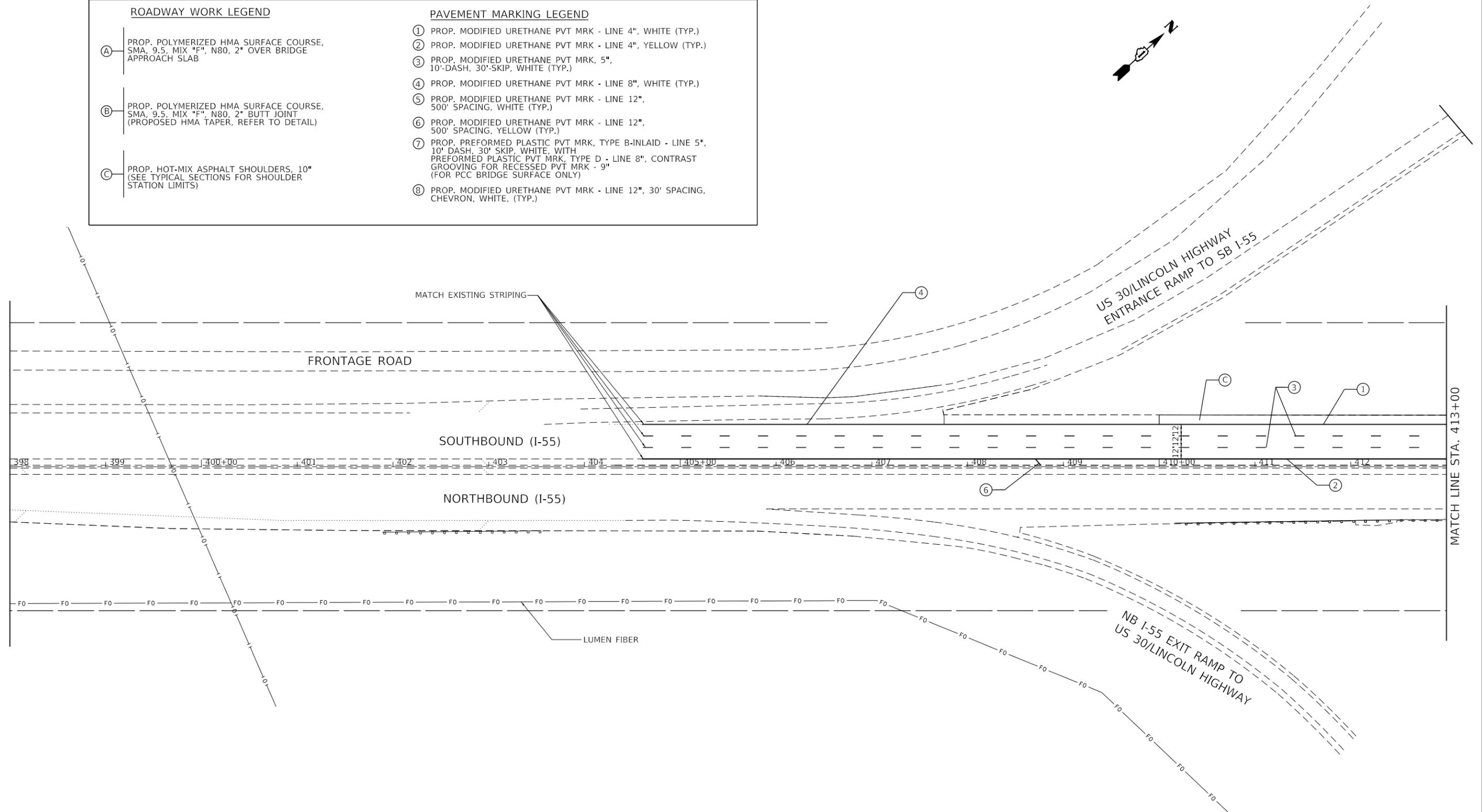
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY PLAN			
I-55 (SOUTH OF I-80 TO WEBER ROAD)			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	41
CONTRACT NO. 62N22				
		ILLINOIS	FED. AID PROJECT	

ROADWAY WORK LEGEND		PAVEMENT MARKING LEGEND	
(A)	PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 2" OVER BRIDGE APPROACH SLAB	(1)	PROP. MODIFIED URETHANE PVT MRK - LINE 4", WHITE (TYP.)
(B)	PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 2" BUTT JOINT (PROPOSED HMA TAPER, REFER TO DETAIL)	(2)	PROP. MODIFIED URETHANE PVT MRK - LINE 4", YELLOW (TYP.)
(C)	PROP. HOT-MIX ASPHALT SHOULDERS, 10" (SEE TYPICAL SECTIONS FOR SHOULDER STATION LIMITS)	(3)	PROP. MODIFIED URETHANE PVT MRK, 5", 10'-DASH, 30'-SKIP, WHITE (TYP.)
		(4)	PROP. MODIFIED URETHANE PVT MRK - LINE 8", WHITE (TYP.)
		(5)	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 500' SPACING, WHITE (TYP.)
		(6)	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 500' SPACING, YELLOW (TYP.)
		(7)	PROP. PREFORMED PLASTIC PVT MRK, TYPE B-INLAID - LINE 5", 10' DASH, 30' SKIP, WHITE, WITH PREFORMED PLASTIC PVT MRK, TYPE D - LINE 8", CONTRAST GROOVING FOR RECESSED PVT MRK - 9" (FOR PCC BRIDGE SURFACE ONLY)
		(8)	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 30' SPACING, CHEVRON, WHITE, (TYP.)



MODEL: 140DELENAME
FILE: 140DELENAME.PLT



8501 W. Higgins Road, Suite 280
Chicago, Illinois 60631
(773) 398-0112

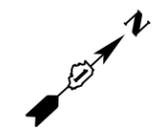
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

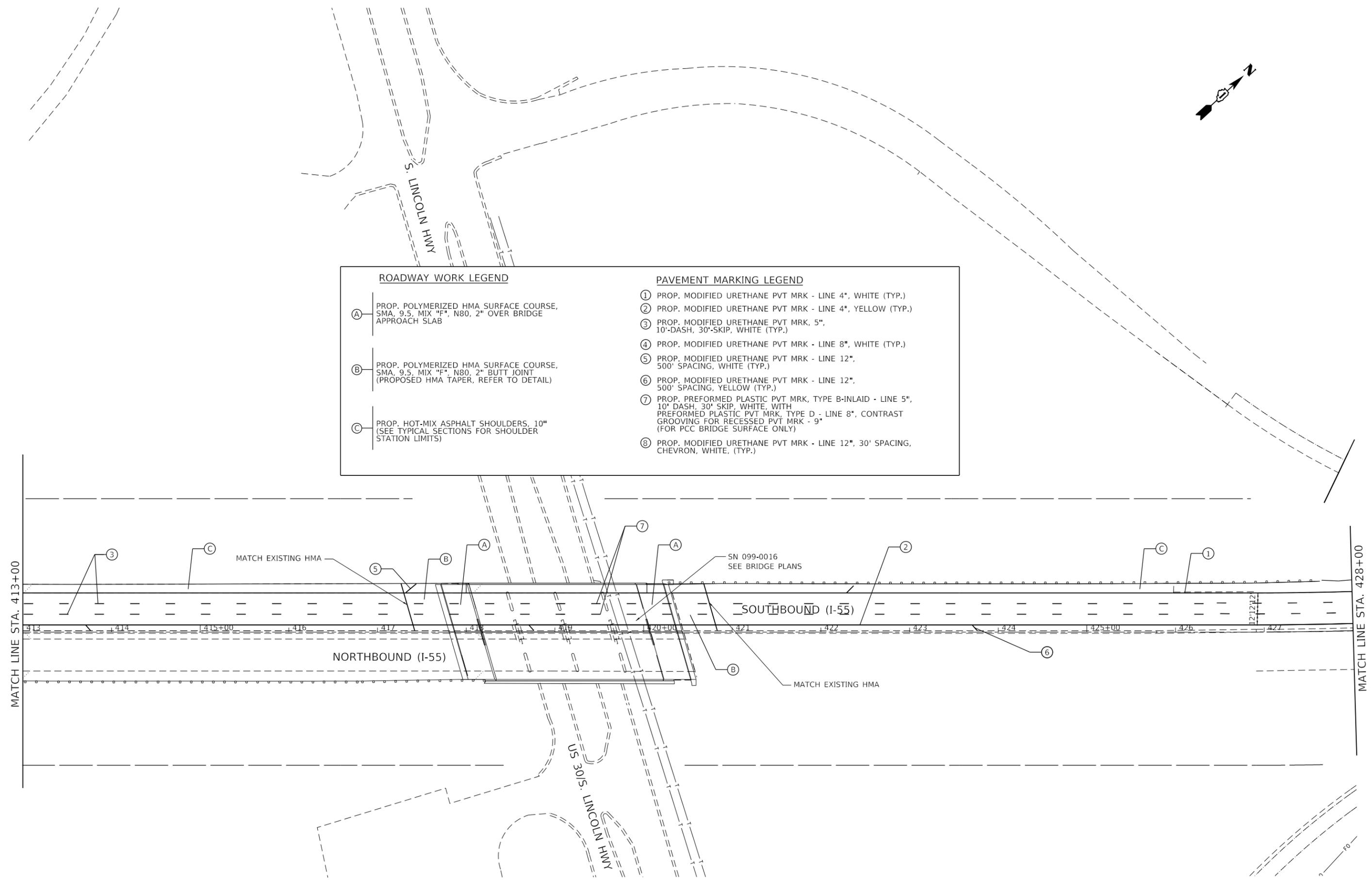
ROADWAY PLAN			
I-55 (SOUTH OF I-80 TO WEBER ROAD)			
SCALE:	SHEET	OF	SHEETS
	STA.	TO	STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	42
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

MATCH LINE STA. 413+00



ROADWAY WORK LEGEND		PAVEMENT MARKING LEGEND	
(A)	PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 2" OVER BRIDGE APPROACH SLAB	①	PROP. MODIFIED URETHANE PVT MRK - LINE 4", WHITE (TYP.)
(B)	PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 2" BUTT JOINT (PROPOSED HMA TAPER, REFER TO DETAIL)	②	PROP. MODIFIED URETHANE PVT MRK - LINE 4", YELLOW (TYP.)
(C)	PROP. HOT-MIX ASPHALT SHOULDERS, 10" (SEE TYPICAL SECTIONS FOR SHOULDER STATION LIMITS)	③	PROP. MODIFIED URETHANE PVT MRK, 5", 10'-DASH, 30'-SKIP, WHITE (TYP.)
		④	PROP. MODIFIED URETHANE PVT MRK - LINE 8", WHITE (TYP.)
		⑤	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 500' SPACING, WHITE (TYP.)
		⑥	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 500' SPACING, YELLOW (TYP.)
		⑦	PROP. PREFORMED PLASTIC PVT MRK, TYPE B-INLAID - LINE 5", 10' DASH, 30' SKIP, WHITE, WITH PREFORMED PLASTIC PVT MRK, TYPE D - LINE 8", CONTRAST GROOVING FOR RECESSED PVT MRK - 9" (FOR PCC BRIDGE SURFACE ONLY)
		⑧	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 30' SPACING, CHEVRON, WHITE, (TYP.)



MODEL: \$MODELNAME\$
FILE: \$NAME\$. \$FILE\$

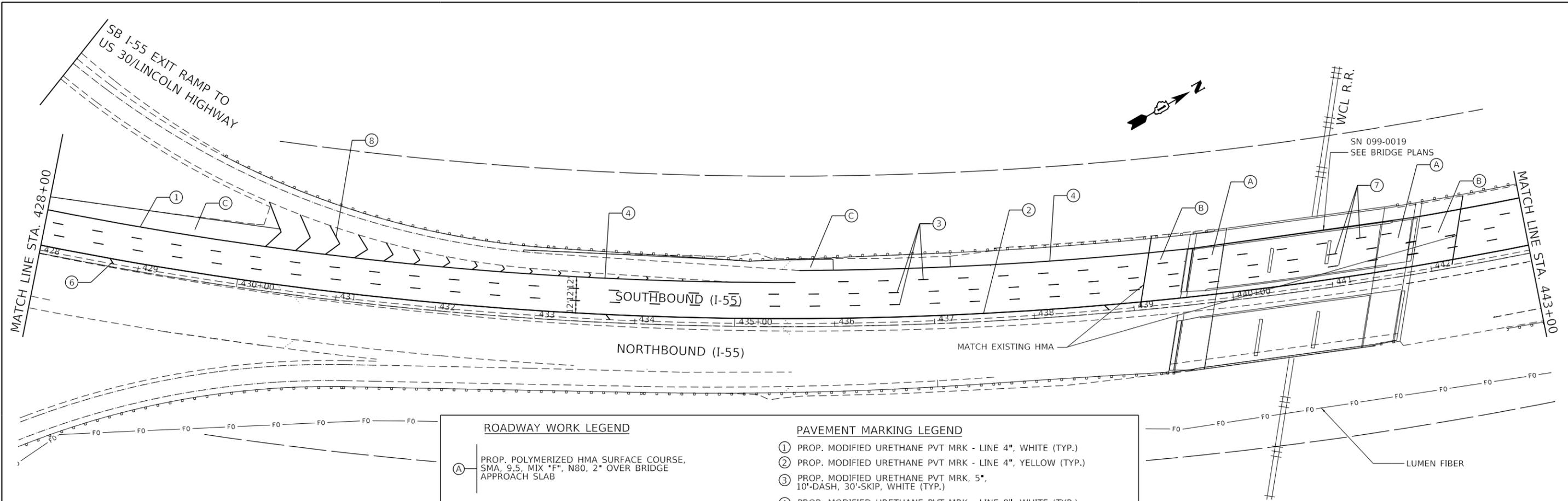


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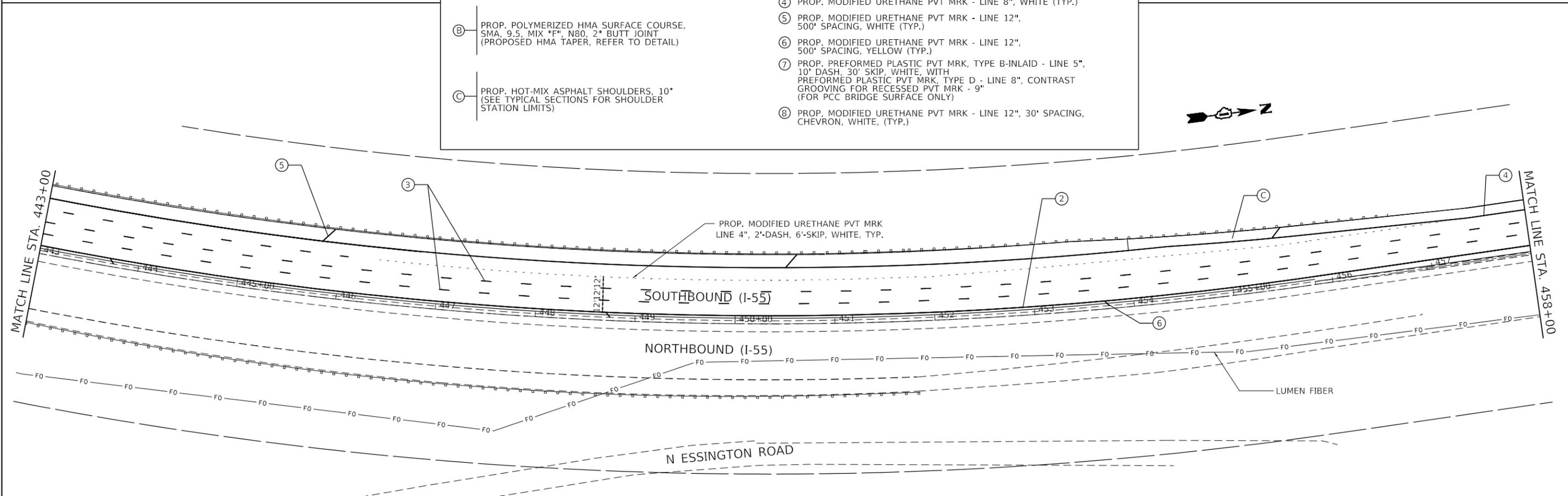
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY PLAN			
I-55 (SOUTH OF I-80 TO WEBER ROAD)			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	43
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				



ROADWAY WORK LEGEND		PAVEMENT MARKING LEGEND	
(A)	PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 2" OVER BRIDGE APPROACH SLAB	①	PROP. MODIFIED URETHANE PVT MRK - LINE 4", WHITE (TYP.)
(B)	PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 2" BUTT JOINT (PROPOSED HMA TAPER, REFER TO DETAIL)	②	PROP. MODIFIED URETHANE PVT MRK - LINE 4", YELLOW (TYP.)
(C)	PROP. HOT-MIX ASPHALT SHOULDERS, 10" (SEE TYPICAL SECTIONS FOR SHOULDER STATION LIMITS)	③	PROP. MODIFIED URETHANE PVT MRK, 5", 10'-DASH, 30'-SKIP, WHITE (TYP.)
		④	PROP. MODIFIED URETHANE PVT MRK - LINE 8", WHITE (TYP.)
		⑤	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 500' SPACING, WHITE (TYP.)
		⑥	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 500' SPACING, YELLOW (TYP.)
		⑦	PROP. PREFORMED PLASTIC PVT MRK, TYPE B-INLAID - LINE 5", 10'-DASH, 30'-SKIP, WHITE, WITH PREFORMED PLASTIC PVT MRK, TYPE D - LINE 8", CONTRAST GROOVING FOR RECESSED PVT MRK - 9" (FOR PCC BRIDGE SURFACE ONLY)
		⑧	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 30' SPACING, CHEVRON, WHITE, (TYP.)



MODEL: 1400BENAMES
FILE: 1400BENAMES_STYLES

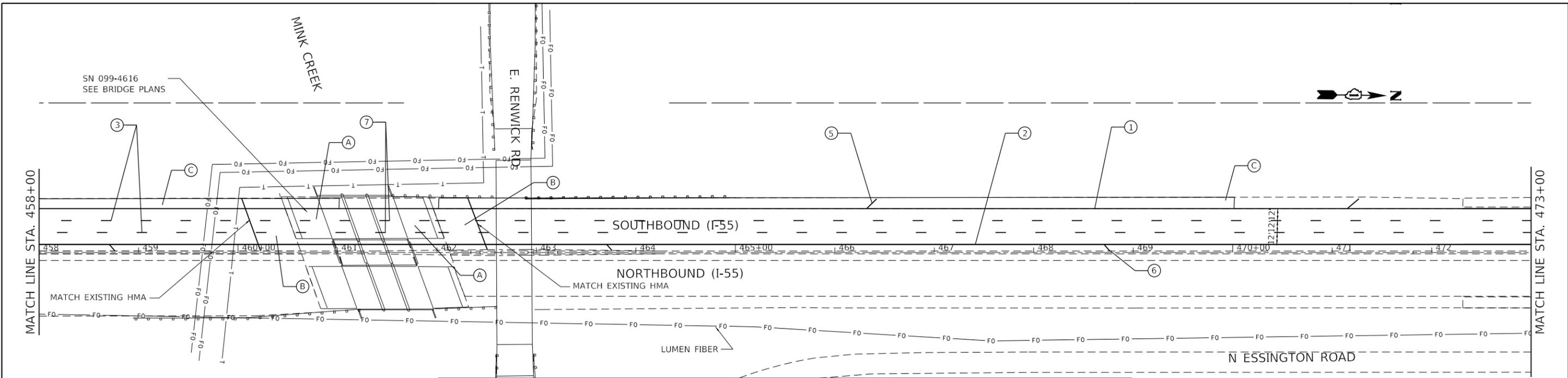


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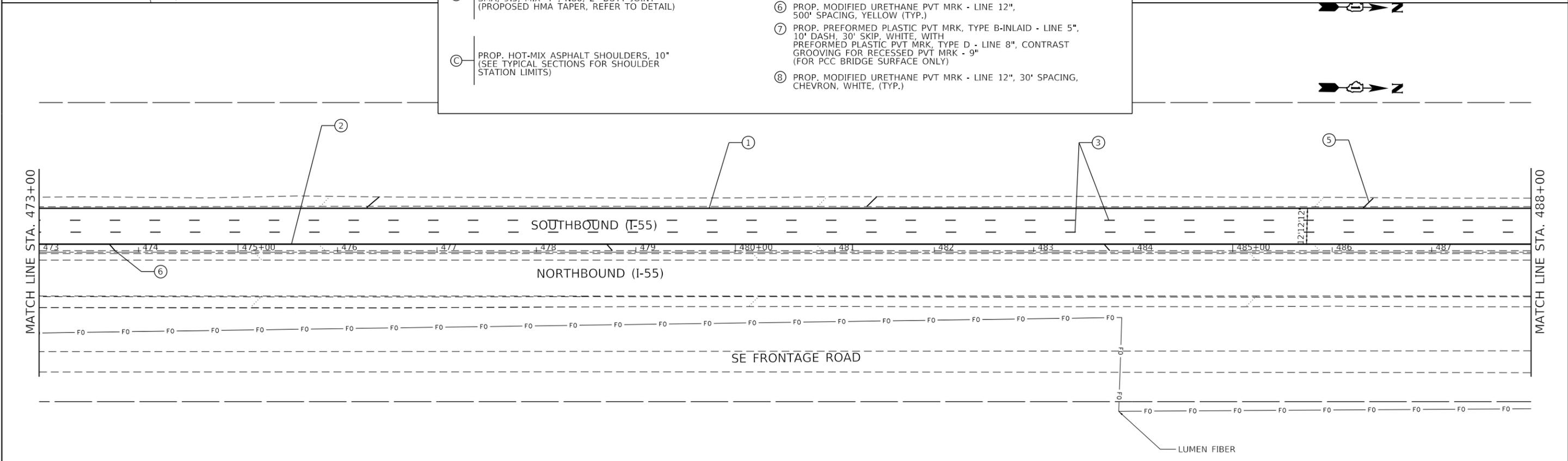
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY PLAN			
I-55 (SOUTH OF I-80 TO WEBER ROAD)			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	44
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				



ROADWAY WORK LEGEND		PAVEMENT MARKING LEGEND	
(A)	PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 2" OVER BRIDGE APPROACH SLAB	①	PROP. MODIFIED URETHANE PVT MRK - LINE 4", WHITE (TYP.)
(B)	PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 2" BUTT JOINT (PROPOSED HMA TAPER, REFER TO DETAIL)	②	PROP. MODIFIED URETHANE PVT MRK - LINE 4", YELLOW (TYP.)
(C)	PROP. HOT-MIX ASPHALT SHOULDERS, 10" (SEE TYPICAL SECTIONS FOR SHOULDER STATION LIMITS)	③	PROP. MODIFIED URETHANE PVT MRK, 5", 10'-DASH, 30'-SKIP, WHITE (TYP.)
		④	PROP. MODIFIED URETHANE PVT MRK - LINE 8", WHITE (TYP.)
		⑤	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 500' SPACING, WHITE (TYP.)
		⑥	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 500' SPACING, YELLOW (TYP.)
		⑦	PROP. PREFORMED PLASTIC PVT MRK, TYPE B-INLAID - LINE 5", 10' DASH, 30' SKIP, WHITE, WITH PREFORMED PLASTIC PVT MRK, TYPE D - LINE 8", CONTRAST GROOVING FOR RECESSED PVT MRK - 9" (FOR PCC BRIDGE SURFACE ONLY)
		⑧	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 30' SPACING, CHEVRON, WHITE, (TYP.)



MODEL: 140DELENAME
FILE: 140DELENAME.DWG

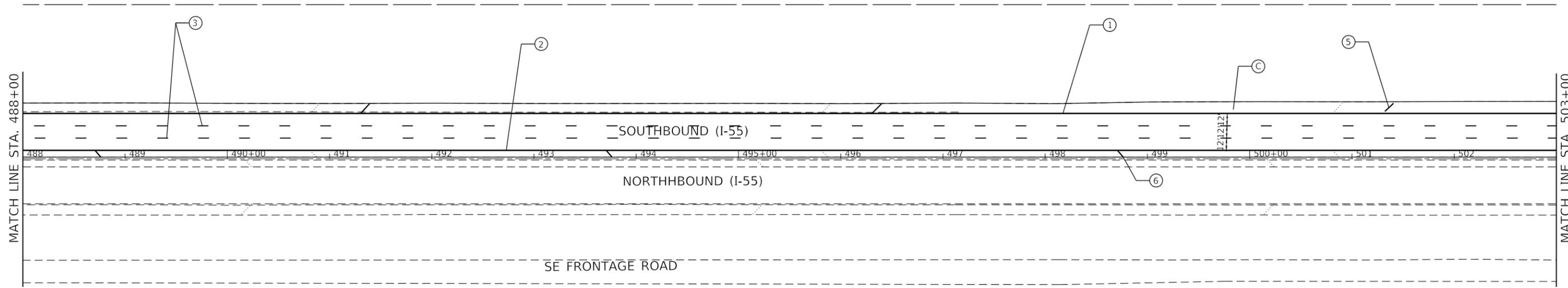


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	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY PLAN			
I-55 (SOUTH OF I-80 TO WEBER ROAD)			
SCALE:	SHEET	OF	SHEETS
	STA.	TO	STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	45
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				



ROADWAY WORK LEGEND		PAVEMENT MARKING LEGEND	
(A)	PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 2" OVER BRIDGE APPROACH SLAB	①	PROP. MODIFIED URETHANE PVT MRK - LINE 4", WHITE (TYP.)
(B)	PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 2" BUTT JOINT (PROPOSED HMA TAPER, REFER TO DETAIL)	②	PROP. MODIFIED URETHANE PVT MRK - LINE 4", YELLOW (TYP.)
(C)	PROP. HOT-MIX ASPHALT SHOULDERS, 10" (SEE TYPICAL SECTIONS FOR SHOULDER STATION LIMITS)	③	PROP. MODIFIED URETHANE PVT MRK, 5", 10'-DASH, 30'-SKIP, WHITE (TYP.)
		④	PROP. MODIFIED URETHANE PVT MRK - LINE 8", WHITE (TYP.)
		⑤	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 500' SPACING, WHITE (TYP.)
		⑥	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 500' SPACING, YELLOW (TYP.)
		⑦	PROP. PREFORMED PLASTIC PVT MRK, TYPE B-INLAID - LINE 5", 10' DASH, 30' SKIP, WHITE, WITH PREFORMED PLASTIC PVT MRK, TYPE D - LINE 8", CONTRAST GROOVING FOR RECESSED PVT MRK - 9" (FOR PCC BRIDGE SURFACE ONLY)
		⑧	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 30' SPACING, CHEVRON, WHITE, (TYP.)

W. LOCKPORT RD./ AIRPORT ROAD

LUMEN FIBER

MODEL: 140DELMARIES
FILE: 140MAR_21113

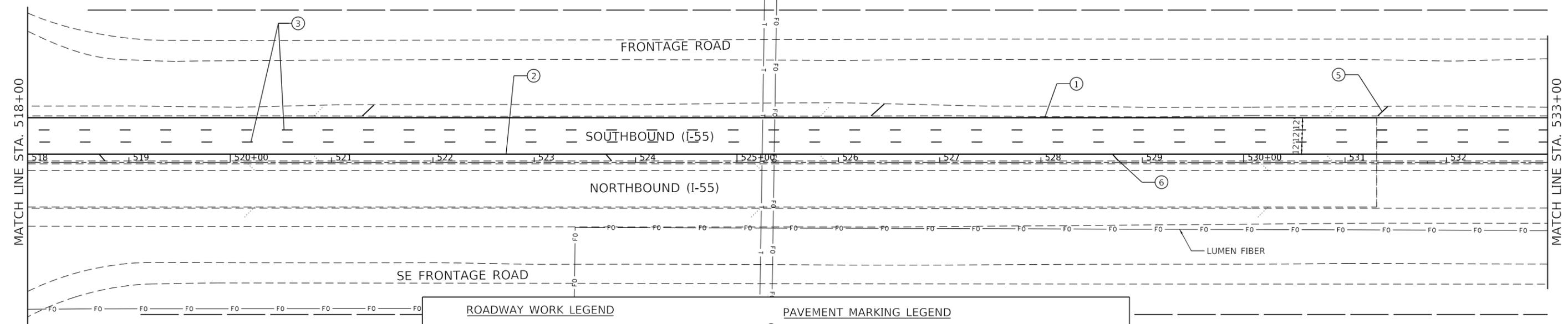


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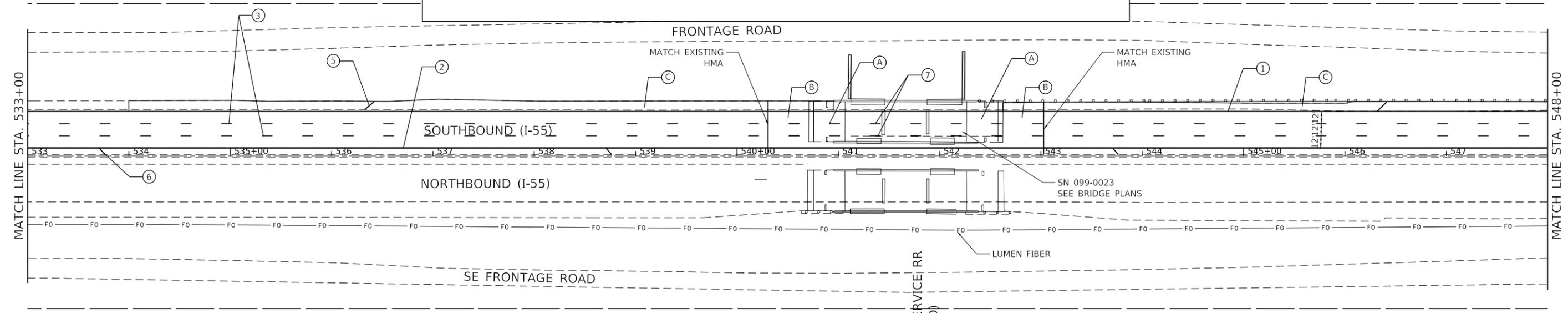
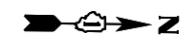
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN			
I-55 (SOUTH OF I-80 TO WEBER ROAD)			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	46
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				



ROADWAY WORK LEGEND		PAVEMENT MARKING LEGEND	
(A)	PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 2" OVER BRIDGE APPROACH SLAB	(1)	PROP. MODIFIED URETHANE PVT MRK - LINE 4", WHITE (TYP.)
(B)	PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 2" BUTT JOINT (PROPOSED HMA TAPER, REFER TO DETAIL)	(2)	PROP. MODIFIED URETHANE PVT MRK - LINE 4", YELLOW (TYP.)
(C)	PROP. HOT-MIX ASPHALT SHOULDERS, 10" (SEE TYPICAL SECTIONS FOR SHOULDER STATION LIMITS)	(3)	PROP. MODIFIED URETHANE PVT MRK, 5", 10'-DASH, 30'-SKIP, WHITE (TYP.)
		(4)	PROP. MODIFIED URETHANE PVT MRK - LINE 8", WHITE (TYP.)
		(5)	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 500' SPACING, WHITE (TYP.)
		(6)	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 500' SPACING, YELLOW (TYP.)
		(7)	PROP. PREFORMED PLASTIC PVT MRK, TYPE B-INLAID - LINE 5", 10' DASH, 30' SKIP, WHITE, WITH PREFORMED PLASTIC PVT MRK, TYPE D - LINE 8", CONTRAST GROOVING FOR RECESSED PVT MRK - 9" (FOR PCC BRIDGE SURFACE ONLY)
		(8)	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 30' SPACING, CHEVRON, WHITE, (TYP.)



MODEL: 140DELEMARIES
FILE: 140MAR_21123

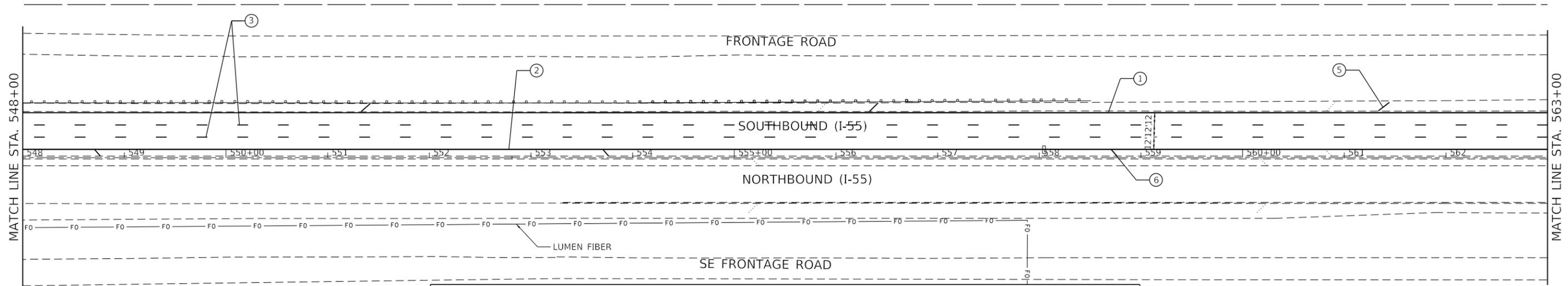


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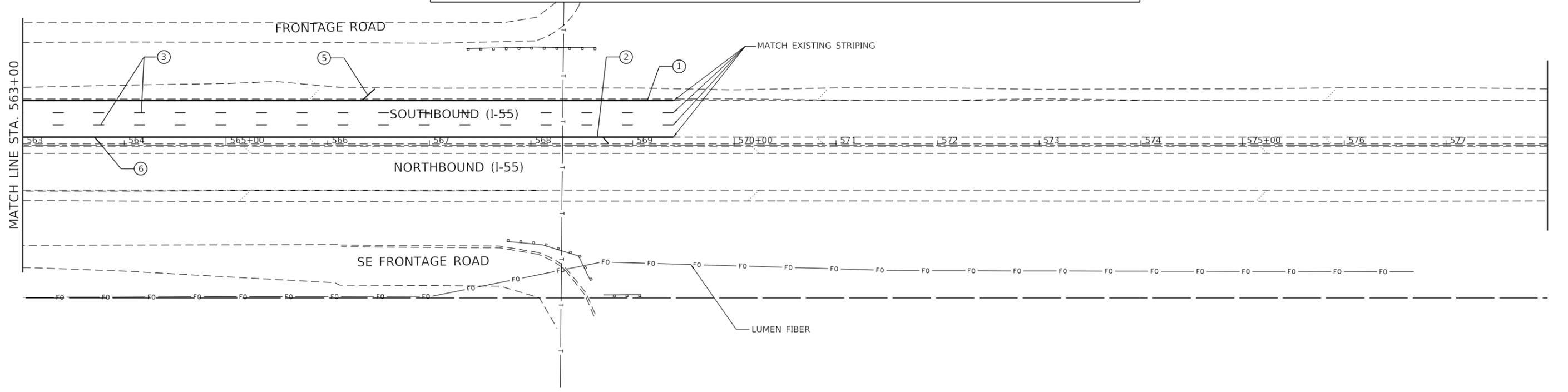
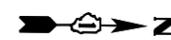
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY PLAN			
I-55 (SOUTH OF I-80 TO WEBER ROAD)			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	47
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				



ROADWAY WORK LEGEND		PAVEMENT MARKING LEGEND	
(A)	PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 2" OVER BRIDGE APPROACH SLAB	(1)	PROP. MODIFIED URETHANE PVT MRK - LINE 4", WHITE (TYP.)
(B)	PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, MIX "F", N80, 2" BUTT JOINT (PROPOSED HMA TAPER, REFER TO DETAIL)	(2)	PROP. MODIFIED URETHANE PVT MRK - LINE 4", YELLOW (TYP.)
(C)	PROP. HOT-MIX ASPHALT SHOULDERS, 10" (SEE TYPICAL SECTIONS FOR SHOULDER STATION LIMITS)	(3)	PROP. MODIFIED URETHANE PVT MRK, 5", 10'-DASH, 30'-SKIP, WHITE (TYP.)
		(4)	PROP. MODIFIED URETHANE PVT MRK - LINE 8", WHITE (TYP.)
		(5)	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 500' SPACING, WHITE (TYP.)
		(6)	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 500' SPACING, YELLOW (TYP.)
		(7)	PROP. PREFORMED PLASTIC PVT MRK, TYPE B-INLAID - LINE 5", 10' DASH, 30' SKIP, WHITE, WITH PREFORMED PLASTIC PVT MRK, TYPE D - LINE 8", CONTRAST GROOVING FOR RECESSED PVT MRK - 9" (FOR PCC BRIDGE SURFACE ONLY)
		(8)	PROP. MODIFIED URETHANE PVT MRK - LINE 12", 30' SPACING, CHEVRON, WHITE, (TYP.)



MODEL: 140DELEMARIES
FILE: 140DELEMARIES_P11E13

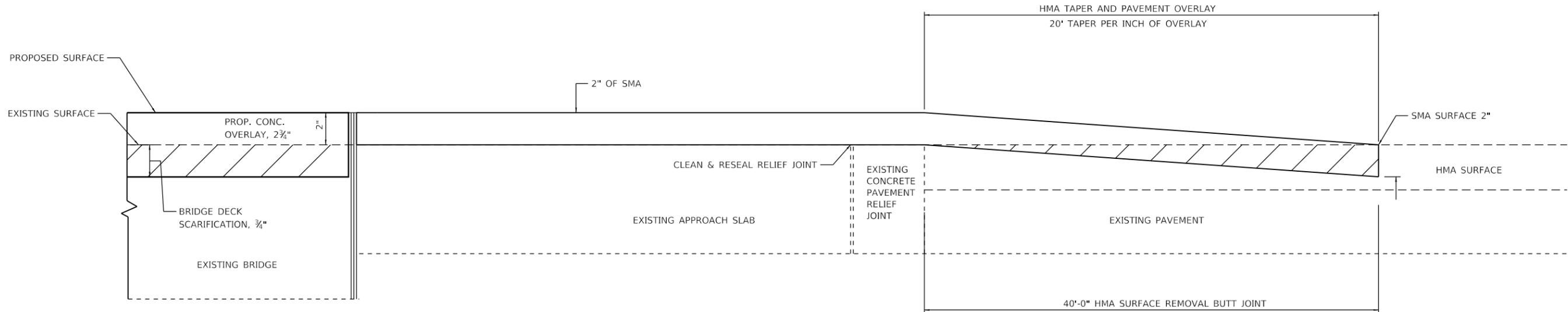


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	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY PLAN			
I-55 (SOUTH OF I-80 TO WEBER ROAD)			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	48
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				



NOTES:

1. SEE BUTT JOINT AND HMA TAPER DETAILS (BD32) FOR ADDITIONAL NOTES AND DETAILS.

APPROACH PAVEMENT HMA OVERLAY DETAIL

MODEL: SMODELNAM1ES
FILE: SMODEL: SMODELNAM1ES

GRAF
8501 W. Higgins Road, Suite 280
Chicago, Illinois 60631
(773) 398-0112

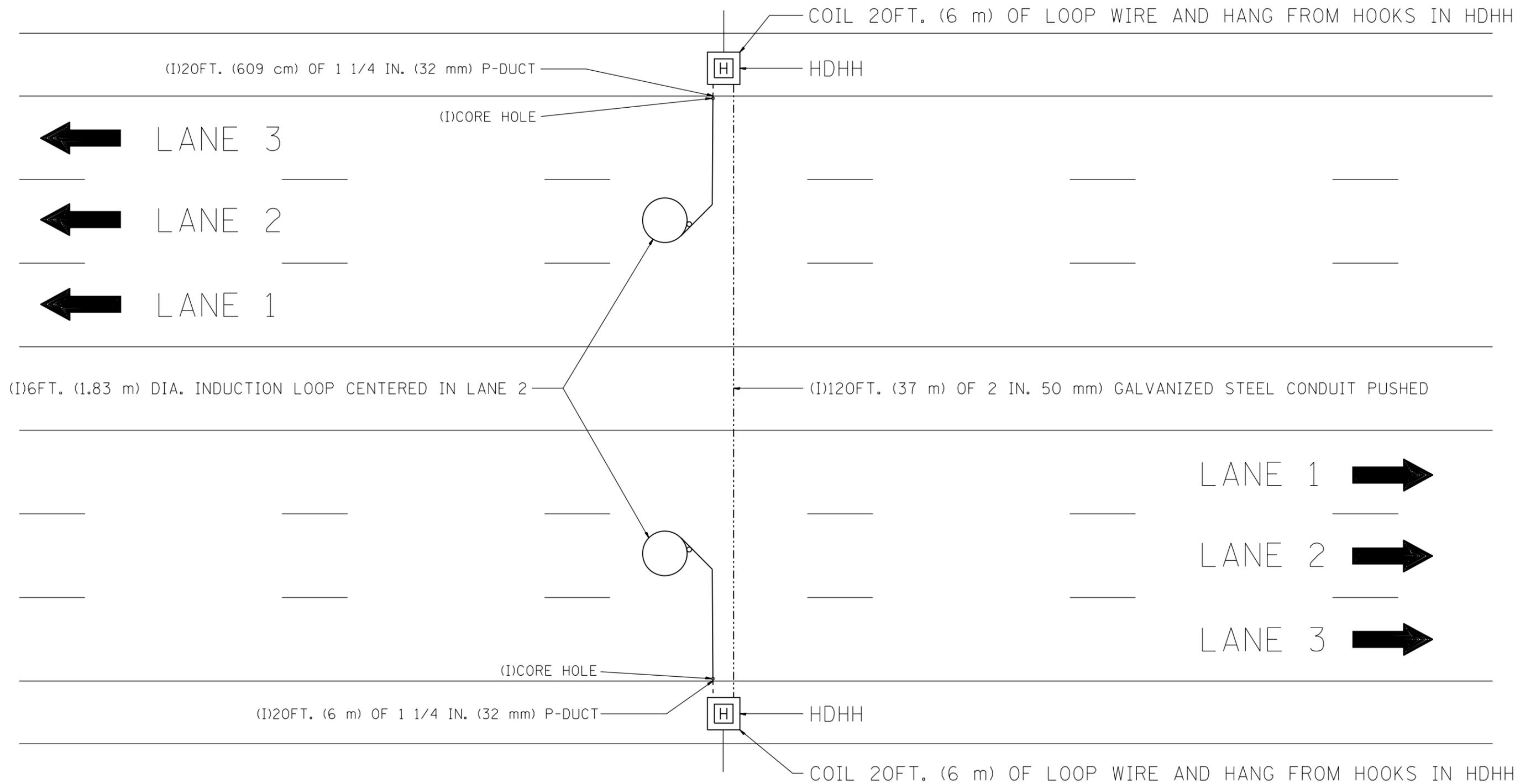
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PLOT DATE = \$DATES	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY DETAILS
I-55 (SOUTH OF I-80 TO WEBER ROAD)**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	49
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				



NOTE:

THE COST OF LOOP WIRE IN HDHH IS INCIDENTAL TO THE INDUCTION LOOP.
IT SHALL NOT BE MEASURE FOR PAYMENT.

MODEL: MODELNAMES
FILE: MODELNAMES.PLT

GRAF
8501 W. Higgins Road, Suite 280
Chicago, Illinois 60631
(773) 398-0112

USER NAME = \$USERS	DESIGNED - J.G.	REVISED - 12-02-2010
	DRAWN - G.M.	REVISED -
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PLOT DATE = \$DATE\$	DATE - 01-31-07	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
TRAFFIC SYSTEMS CENTER

ONE LANE COUNT STATION

SCALE: NONE SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	50
CONTRACT NO. 62N22				
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

TRAFFIC SYSTEMS CENTER (TY-1TSC-400#31)

COIL 20FT. (6 m) OF LOOP WIRE AND HANG FROM HOOKS IN HDHH

HDHH WITH STUB OUT 1FT. (30.48 cm) PAST EDGE OF SHOULDER

(I)10FT. (3 m) OF 1 1/4 IN. (32 mm) (P-DUCT(TRENCHED))



LANE 3



LANE 2



LANE 1

(I)120FT. (37 m) OF 2 IN. (50 mm) GALVANIZED STEEL CONDUIT PUSHED

(I)6FT. DIA. (1.83 m) INDUCTION LOOP CENTERED IN EACH LANE

LANE 1



LANE 2



LANE 3



HDHH WITH STUB OUT 1FT. (30.48 cm) PAST EDGE OF SHOULDER

(I)CORE HOLE

(I)10FT. (3 m) OF 1 1/4 IN. (30.0 mm) P-DUCT(TRENCHED)

COIL 20FT. (6 m) OF LOOP WIRE AND HANG FROM HOOKS IN HDHH

NOTE:

THE COST OF LOOP WIRE IN HDHH IS INCIDENTAL TO THE INDUCTION LOOP.
IT SHALL NOT BE MEASURE FOR PAYMENT.

MODEL: MODELNAMIES
FILE: MODEL: FILES



8501 W. Higgins Road, Suite 280
Chicago, Illinois 60631
(773) 398-0112

USER NAME = \$USERS
PLOT SCALE = \$SCALE\$
PLOT DATE = \$DATES

DESIGNED - J.G.
DRAWN - G.M.
CHECKED - J.G.
DATE - 01-31-07

REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
TRAFFIC SYSTEMS CENTER

3 LANE COUNT STATION

SCALE: NONE SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	51
CONTRACT NO. 62N22				
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

TRAFFIC SYSTEMS CENTER (TY-1TSC-400#32)

COIL 20FT. (6 m) OF LOOP WIRE AND HANG FROM HOOKS IN HDHH

HDHH WITH STUB OUT 1FT. (30.48 cm) PAST EDGE OF SHOULDER



LANE 3



LANE 2

2IN. (50 mm) GALVANIZED STEEL CONDUIT PUSHED



LANE 1

COIL 20FT. (6M) OF LOOP WIRE AND HANG FROM HOOKS IN HDHH

2IN. (50 mm) GALVANIZED STEEL CONDUIT PUSHED

COIL 20FT. (6M) OF LOOP WIRE AND HANG FROM HOOKS IN HDHH

2IN. (50 mm) GALVANIZED STEEL CONDUIT PUSHED

HDHH WITH STUB OUT 1FT. (30.48 cm) PAST EDGE OF SHOULDER

COIL 20FT. (6 m) OF LOOP WIRE AND HANG FROM HOOKS IN HDHH

10FT. (3 m) OF 1 1/4 IN. (32 mm) (P-DUCT(TRENCHED))

(1)6FT. DIA. (1.83 m) INDUCTION LOOP CENTERED IN EACH LANE

CORE HOLE
10FT. (3 m) OF 1 1/4 IN. (30.0 mm) P-DUCT(TRENCHED)

LANE 1



LANE 2



LANE 3



NOTE:

THE COST OF LOOP WIRE IN HDHH IS INCIDENTAL TO THE INDUCTION LOOP.
IT SHALL NOT BE MEASURED FOR PAYMENT.

MODEL: MODELNAME\$
FILE: MODEL\$.PLOT\$



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Chicago, Illinois 60631
(773) 398-0112

USER NAME = \$USERS	DESIGNED - J.G.	REVISED -
PLOT SCALE = \$SCALE\$	CHECKED - J.G.	REVISED -
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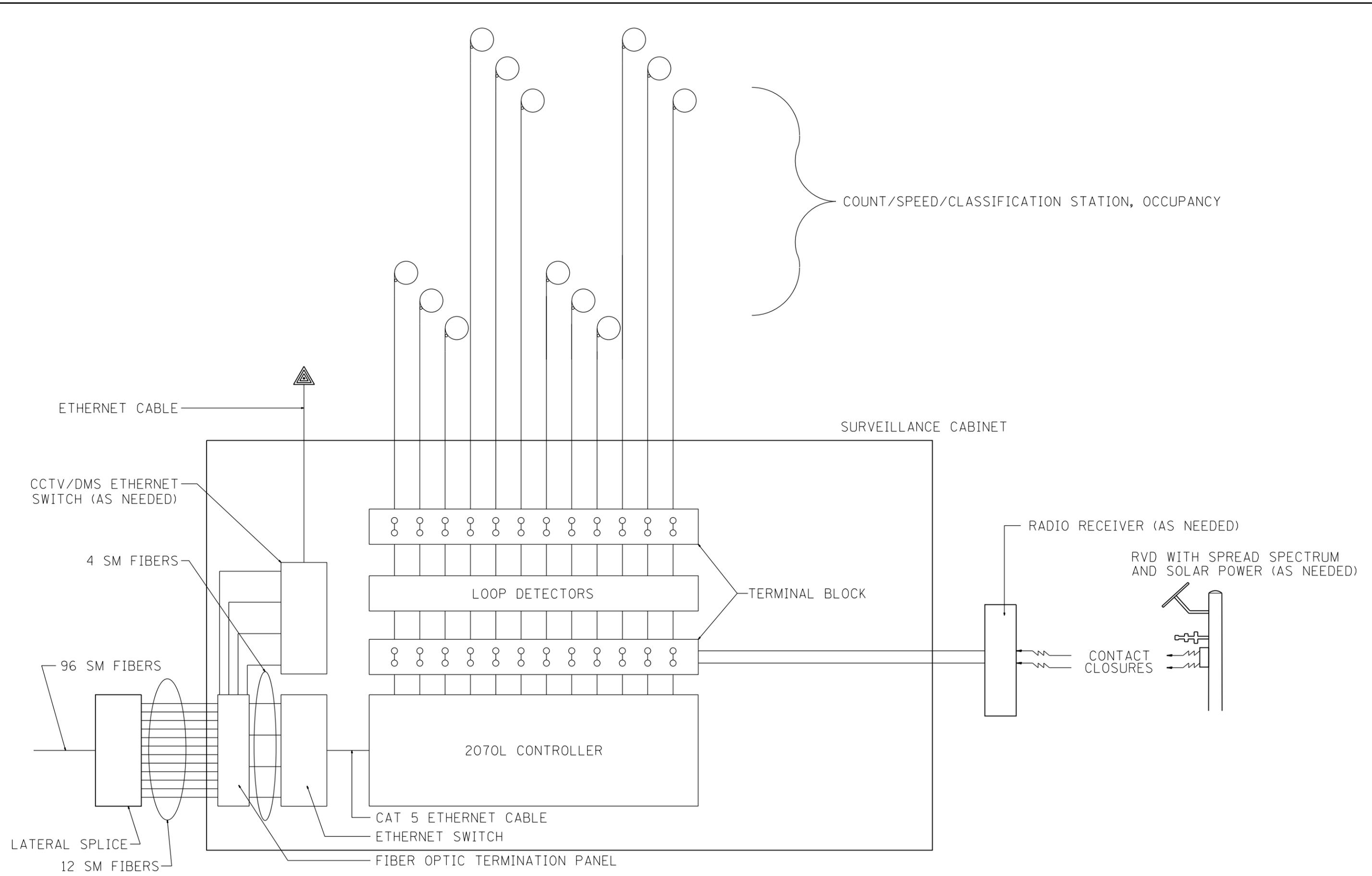
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
TRAFFIC SYSTEMS CENTER

3 LANE COUNT STATION
(DIFFERENT DIRECTIONS)

SCALE: NONE SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	52
CONTRACT NO. 62N22				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

TRAFFIC SYSTEMS CENTER (TY-1TSC-400#32A)



MODEL: MODELNAME
FILE: MODEL.PLT

GRAF
8501 W. Higgins Road, Suite 280
Chicago, Illinois 60631
(773) 398-0112

USER NAME = \$USERS	DESIGNED - J.G.	REVISED - 01/27/2010
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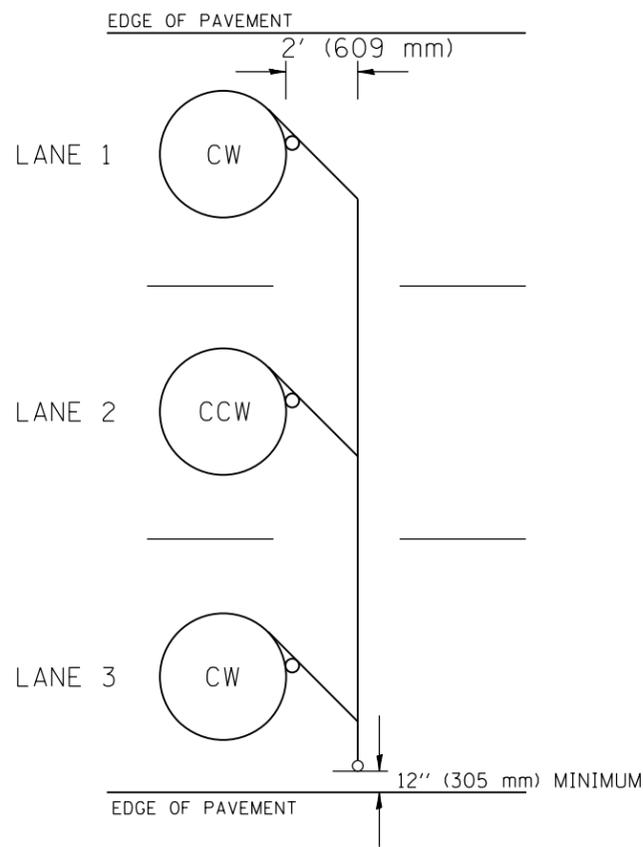
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
TRAFFIC SYSTEMS CENTER

LOOP DETECTOR/SPEED DETECTOR
CABINET LAYOUT

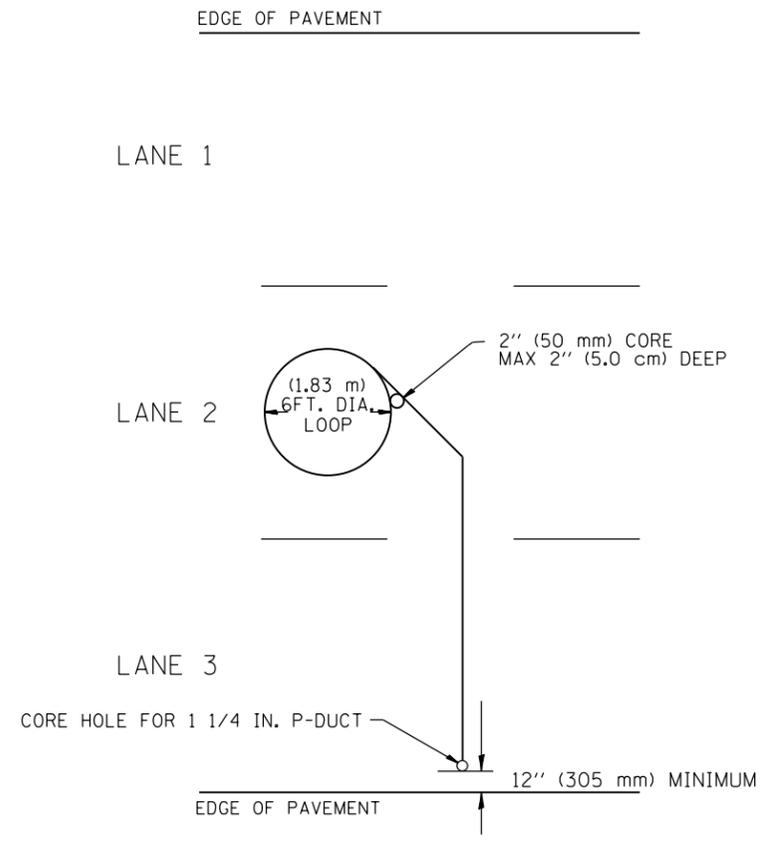
SCALE: NONE SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	53
CONTRACT NO. 62N22				
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

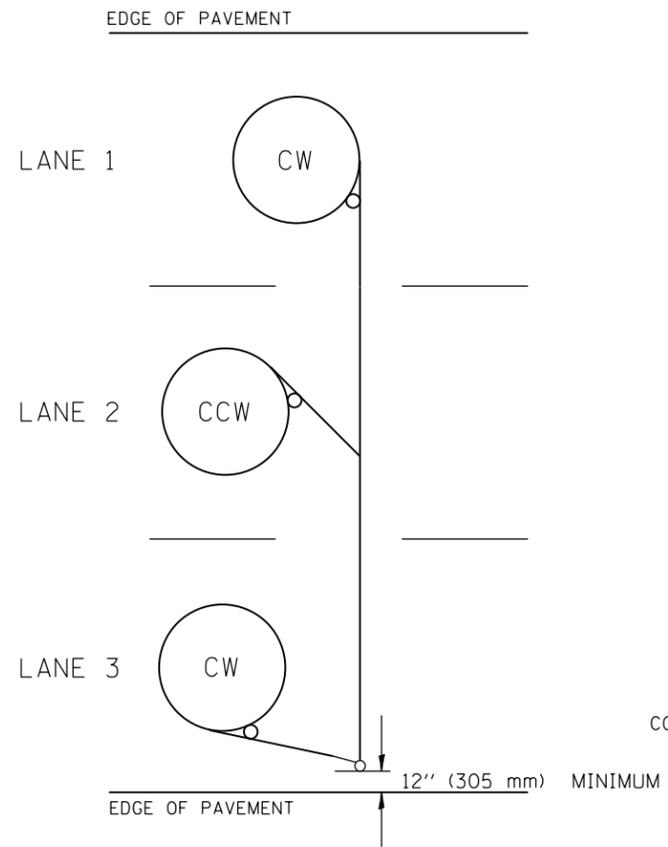
TRAFFIC SYSTEMS CENTER 4TY-1TSC-400#44)



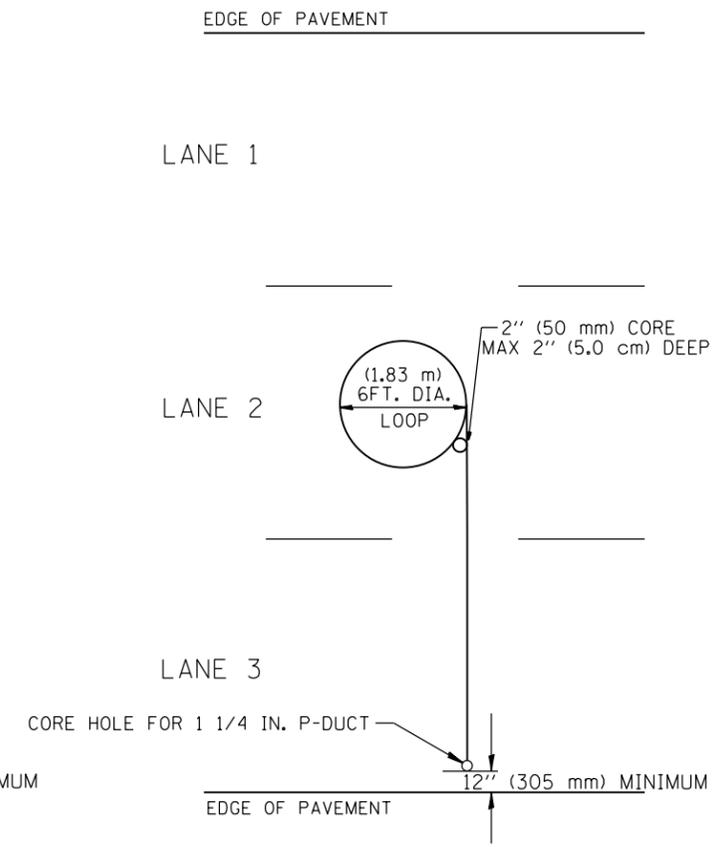
TYPICAL 6' (1.83 m) DIA. INDUCTION LOOP CORE DRILL
LAYOUT FOR MULTIPLE LANE ROADWAY



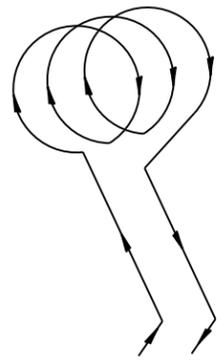
TYPICAL 6' (1.83 m) DIA. INDUCTION LOOP
LAYOUT FOR LANE 2



TYPICAL 6' (1.83 m) DIA. INDUCTION LOOP CORE DRILL
LAYOUT FOR MULTIPLE LANE ROADWAY



TYPICAL 6' (1.83 m) DIA. INDUCTION LOOP
LAYOUT FOR LANE 2



WIRING DETAILS

NOTES

1. EACH LOOP SHALL BE SPLICED TO A 4-C NO.18 TWISTED SHIELDED LEAD IN WHEN 150' (45 m) OR MORE FROM CABINET.
2. LOOPS SHALL BE SPLICED IN HANDHOLES ONLY, OTHERWISE WRITTEN PERMISSION SHALL BE OBTAINED FROM TSC ENGINEER.
3. LOOPS SHALL NOT BE SPLICED IN SERIES.
4. EACH LOOP LEAD IN SHALL BE IDENTIFIED AND PERMANENTLY COLOR CODED IN THE COREHOLE, HANDHOLE & CABINETS THRU WHICH THEY ENTER OR PASS AND TAGGED WITH THE CORRECT NOMENCLATURES.

MODEL: MODELNAMES
FILE: MODELNAMES.PRT



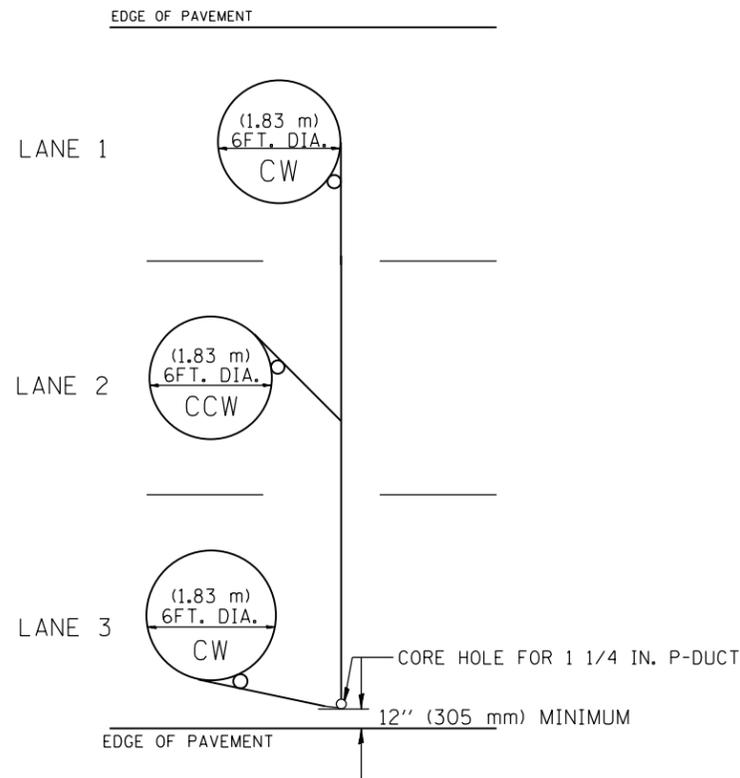
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STATE OF ILLINOIS
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TRAFFIC SYSTEMS CENTER

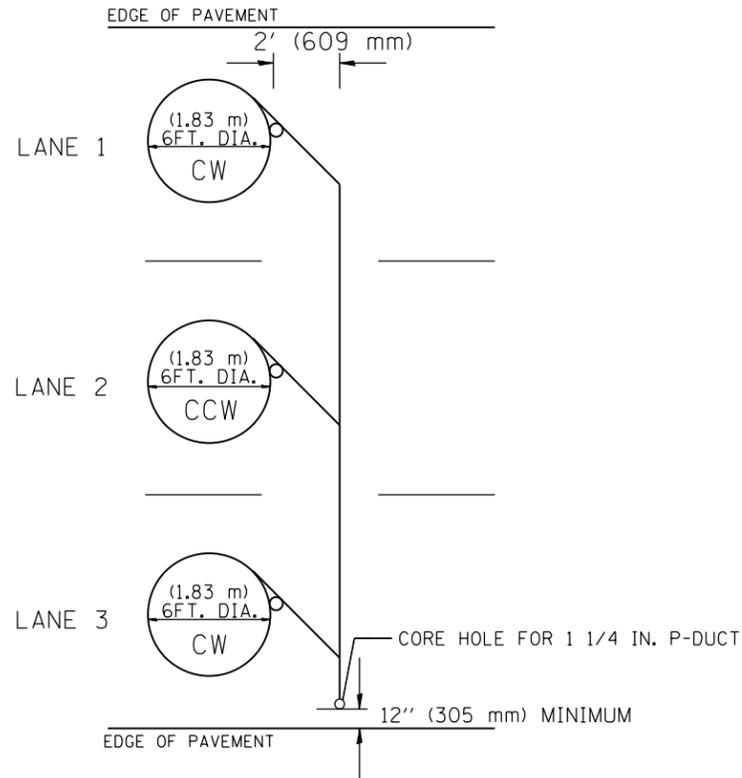
EXISTING ROUND LOOP
INSTALLATION

SCALE: NONE SHEET OF SHEETS STA. TO STA.

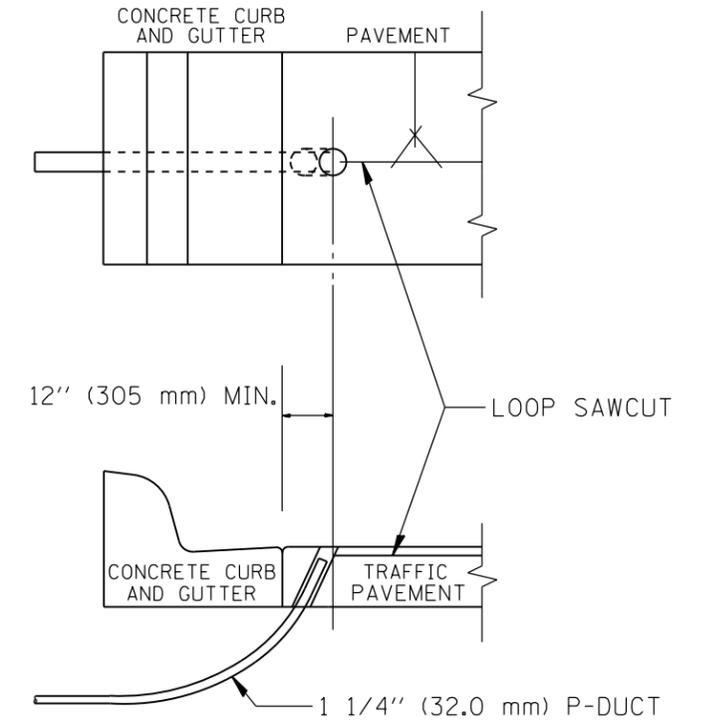
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55	2020-253-BR&PP	WILL	178	54
CONTRACT NO. 62N22				
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



TYPICAL 6FT. (1.83 m) DIA. INDUCTION LOOP CORE DRILL
LAYOUT FOR MULTIPLE LANE ROADWAY



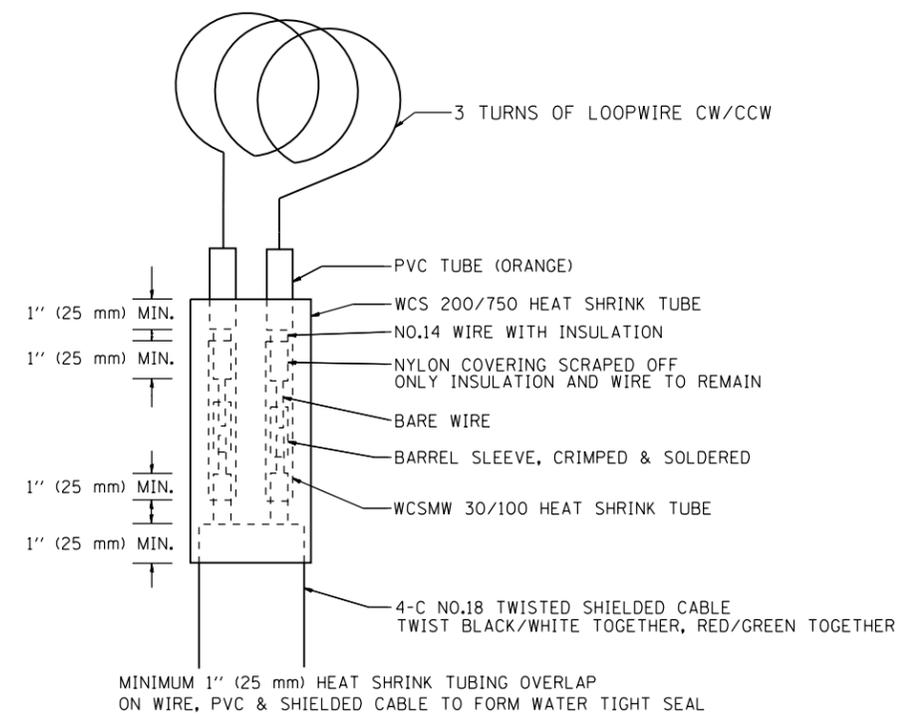
TYPICAL 6FT. (1.83 m) DIA. INDUCTION LOOP CORE DRILL
LAYOUT FOR MULTIPLE LANE ROADWAY



CURB AND GUTTER LOOP LEAD-IN
TRANSITION DETAIL

NOTES

1. EACH LOOP SHALL BE SPLICED TO A 4-C NO.18 TWISTED SHIELDED LEAD IN WHEN 150FT. (45 m) OR MORE FROM CABINET.
2. LOOPS SHALL BE SPLICED IN HANDHOLES ONLY, OTHERWISE WRITTEN PERMISSION SHALL BE OBTAINED FROM TSC ENGINEER.
3. LOOPS SHALL NOT BE SPLICED IN SERIES.
4. EACH LOOP LEAD IN SHALL BE IDENTIFIED AND PERMANENTLY COLOR CODED IN THE COREHOLE, HANDHOLE & CABINETS THRU WHICH THEY ENTER OR PASS AND TAGGED WITH THE CORRECT NOMENCLATURES.



LOOP SPLICING REQUIREMENTS

MODEL: MODELNAMES
FILE: MODEL FILES

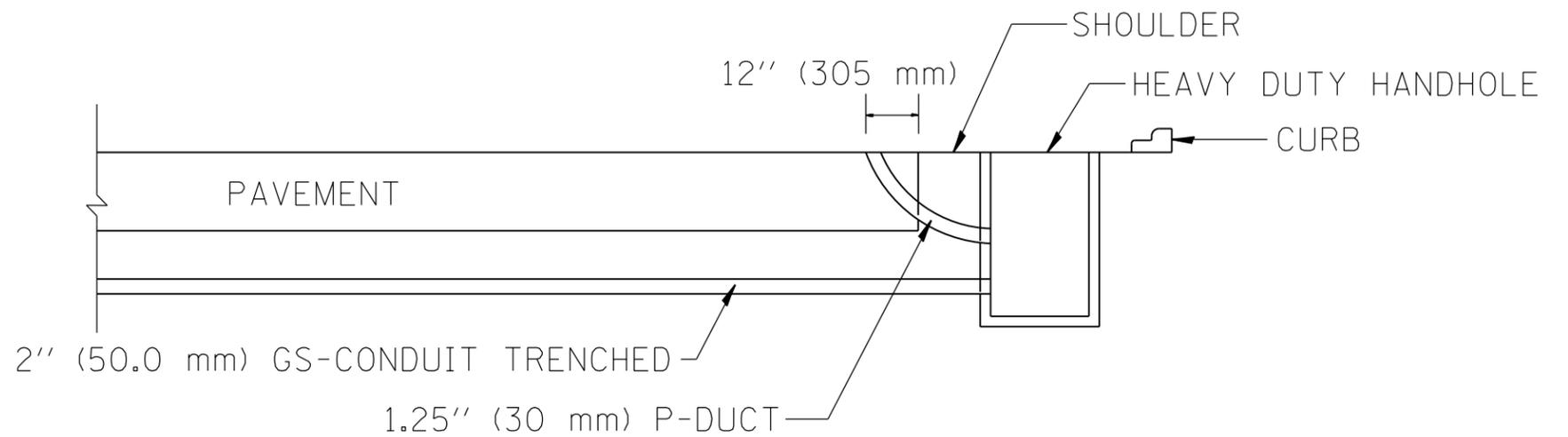
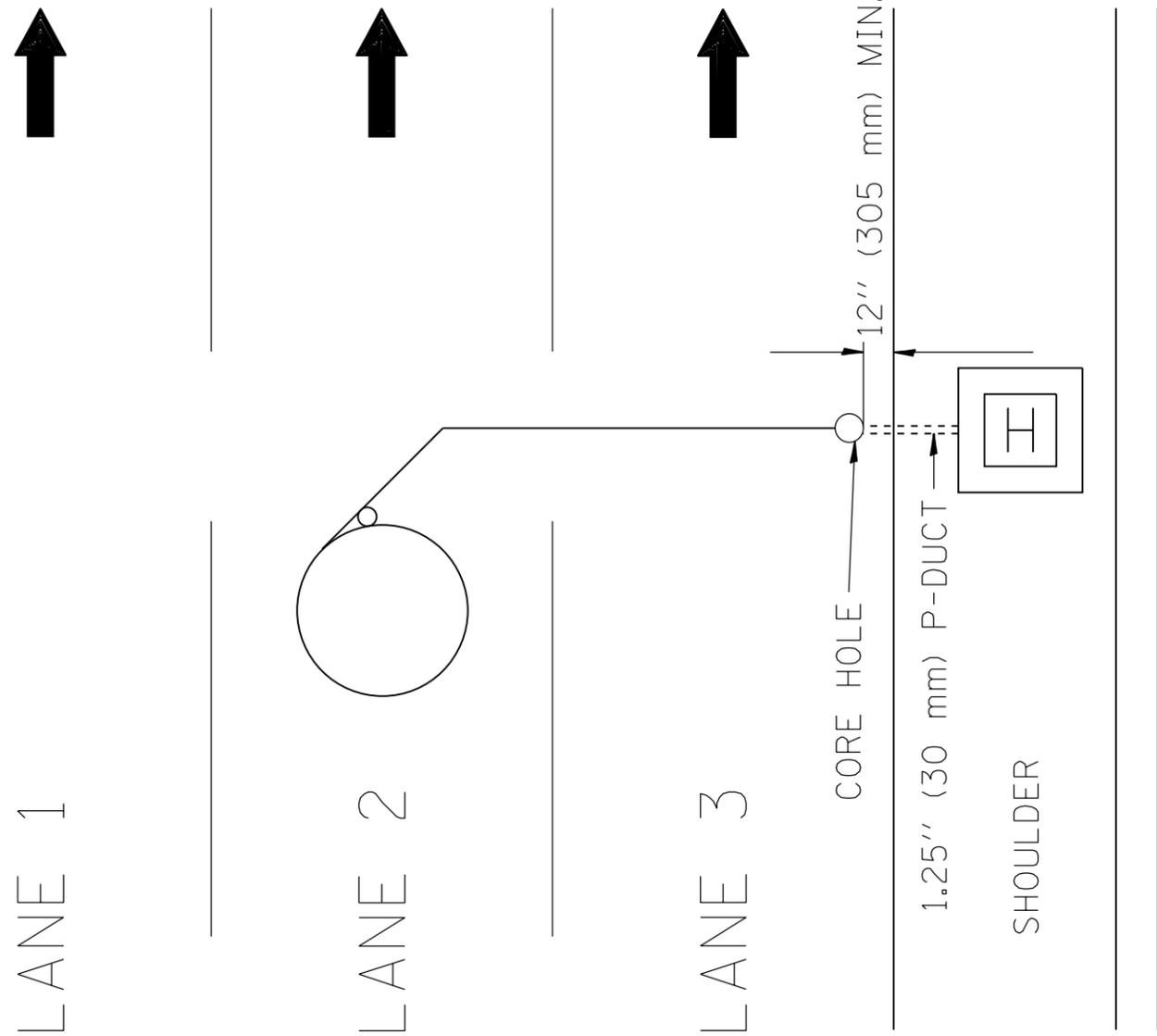
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8501 W. Higgins Road, Suite 280
Chicago, Illinois 60631
(773) 398-0112

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PLOT DATE = \$DATE\$	DATE - 6-22-94	REVISED -

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EXISTING ROUND INDUCTION LOOP TYPICALS		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: NONE	SHEET OF SHEETS	55	2020-253-BR&PP	WILL	178	55
STA. TO STA.		CONTRACT NO. 62N22				
		FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

TRAFFIC SYSTEMS CENTER (TY-1TSC-418#2)



MODEL: MODELNAME\$
FILE: MODEL\$.PLOT\$

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Chicago, Illinois 60631
(773) 398-0112

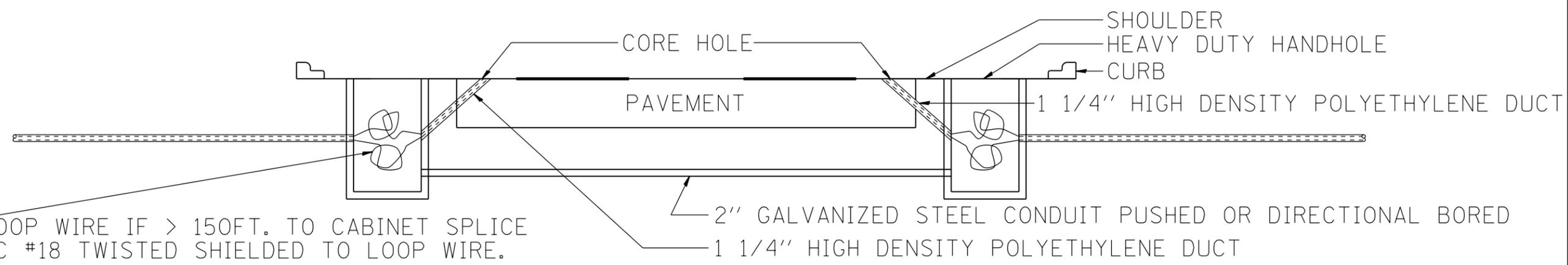
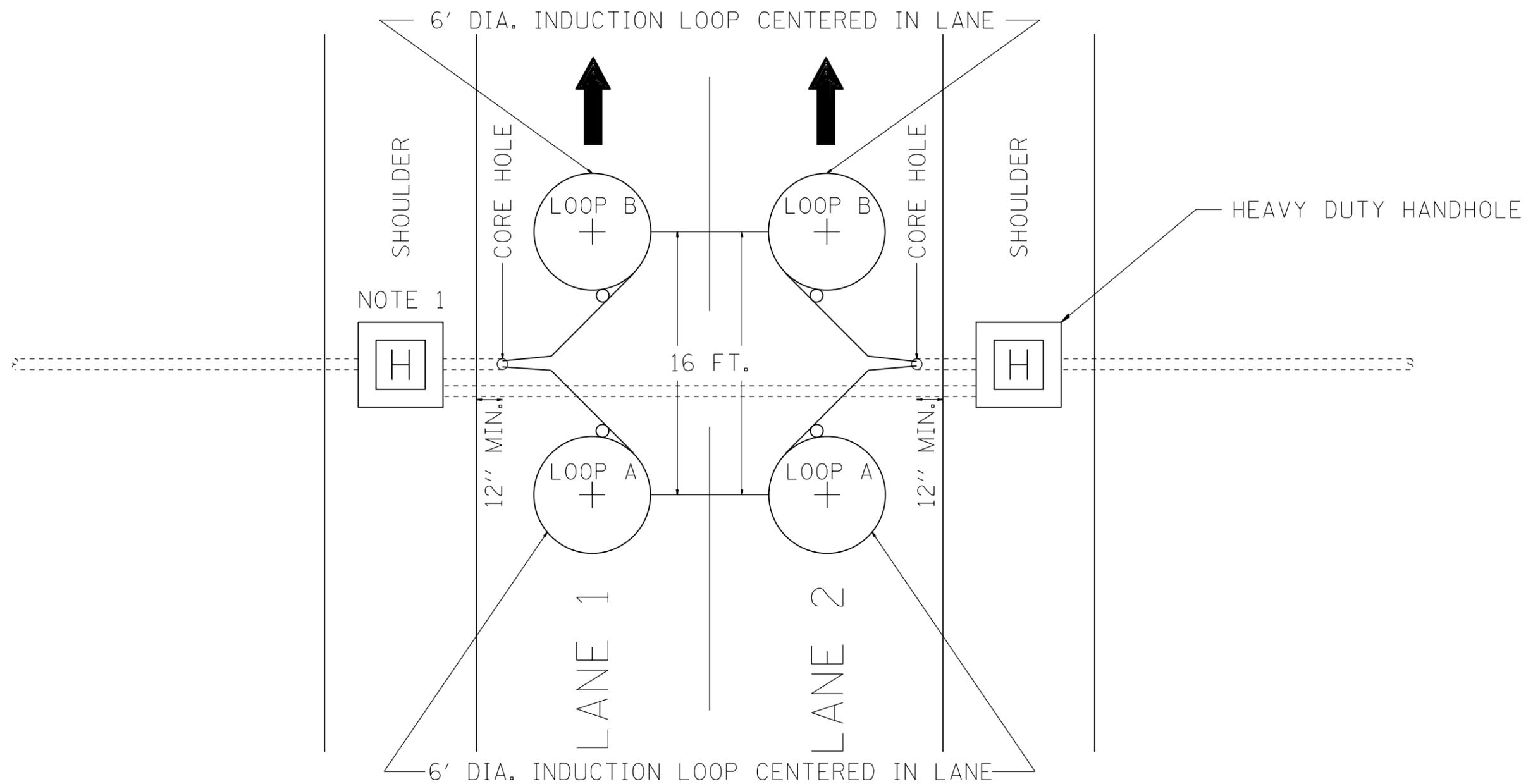
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PLOT DATE = \$DATE\$	DATE - 11/7/95	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
TRAFFIC SYSTEMS CENTER

DIVE HOLE DUCT SYSTEM

SCALE: NONE SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	57
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 62N22	



LOOP WIRE IF > 150FT. TO CABINET SPLICE
4C #18 TWISTED SHIELDED TO LOOP WIRE.

NOTE 1: IF HDHH NOT POSSIBLE THEN LANE 1 LOOPS USE SAME CORE HOLE AS LANE 2 LOOPS. IN THE OTHER DIRECTION, LANE 2 LOOPS WILL USE SAME CORE HOLE AS LANE 1 LOOPS.

MODEL: MODELNAME
FILE: MODEL.PLT



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DEPARTMENT OF TRANSPORTATION
TRAFFIC SYSTEMS CENTER

TRAFFIC SURVEILLANCE	
2 LANE SPEED, COUNT, CLASSIFICATION STATION	
SCALE: NONE	SHEET OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	58
CONTRACT NO. 62N22				
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

GENERAL NOTES

1. Reinforcement bars designated (E) shall be epoxy coated.
2. Prior to pouring the new concrete deck for Expansion Joints Reconstruction and Deck slab repairs, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.
3. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
4. Joint openings shall be adjusted according to Article 520.04 of the Standard Specification when the deck is poured at an ambient temperature other than 50° F.
5. Bars indicated thus, 3x2-#5, indicates 3 lines of #5 bars with 2 lengths of bar per line.
6. All exposed concrete edges shall have a 3/4" chamfer, except where shown otherwise.
7. During repair operations, the Contractor shall locate and protect all utilities in the vicinity of the work including, but not limited to, fiber optic and/or electrical conduits, conduits under the bridge deck, under-deck lighting, traffic signals or signs attached to the structure. This work shall be performed to the satisfaction of the Engineer and will not be paid for separately, but shall be included with the contract. It shall be the Contractor's responsibility to restore and replace any damaged utilities or facilities to the satisfaction of the Engineer at no cost to the Department.
8. Expansion joints shall be fabricated to conform to the existing cross slopes of the bridge.
9. Protective Coat shall be applied to the top and inside face of parapets, reconstructed transverse Expansion Joints and to the surface of the new overlay.
10. Existing reinforcement extended into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal operations shall be replaced using an approved bar splicer or anchorage system. Cost included with Concrete Removal.
11. Debris shall be removed from the tops of pier caps and abutment caps. Quantities are estimated and actual locations will be determined in the field, at the time of construction, by the Engineer.
12. The Contractor is responsible to protect the existing conduit embedded in the parapet during concrete removal and construction. Any damage to the existing conduit shall be repaired by the Contractor at no additional cost to the Department.

INDEX OF SHEETS

S1-01	General Plan and Elevation
S1-02	General Notes, Total Bill of Materials, & Index of Sheets
S1-03-S1-04	Construction Staging Details
S1-05	Temporary Concrete Barrier for Stage Construction
S1-06	Bridge Deck Repairs
S1-07	Parapet Repairs
S1-08	Expansion Joint Removal and Construction
S1-09	Expansion Joint Details
S1-10	Preformed Joint Strip Seal
S1-11	Pier 1 Repairs
S1-12	Pier 2 Repairs
S1-13	Pier 3 Repairs
S1-14	North and South Abutment Repairs
S1-15	Bar Splicer Assembly and Mechanical Splicer Details

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Concrete Removal	Cu Yd	16.1		16.1
Protective Shield	Sq Yd	656		656
Concrete Superstructure	Cu Yd	17.4		17.4
Bridge Deck Grooving	Sq Yd	1092		1092
Protective Coat	Sq Yd	1457		1457
Reinforcement Bars, Epoxy Coated	Pound	2220		2220
Bar Splicers	Each	24		24
Preformed Joint Strip Seal	Foot	115		115
Concrete Sealer	Sq Ft		333	333
Approach Slab Repair (Partial Depth)	Sq Yd	4		4
Bridge Deck Latex Concrete Overlay, 2 3/4"	Sq Yd	1108		1108
Bridge Deck Scarification 3/4"	Sq Yd	1108		1108
Structural Repair of Concrete (Depth Equal to or less than 5")	Sq Ft	1.5	4.5	6
Clean Approach Slab Drain	Each	1		1

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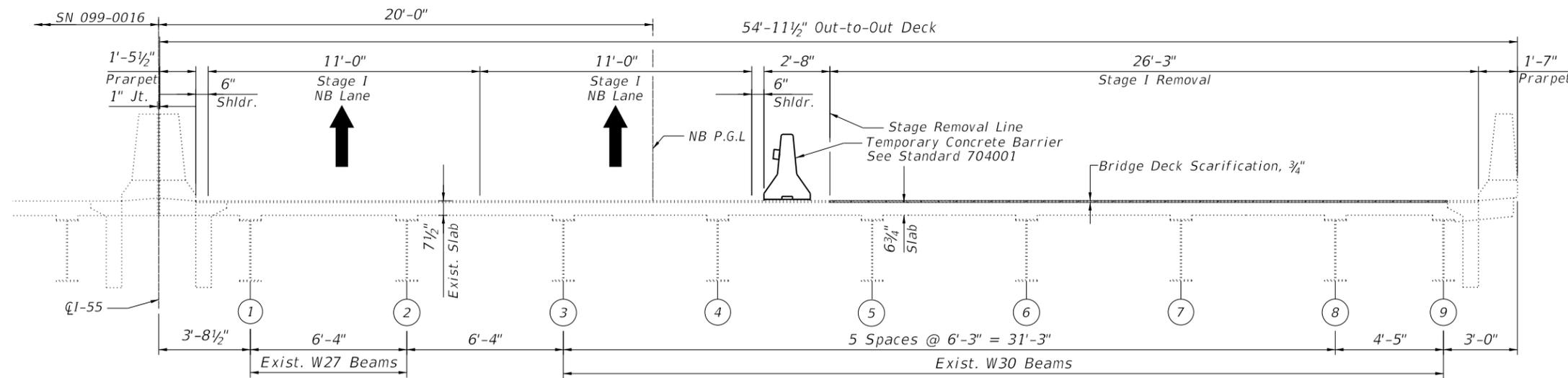
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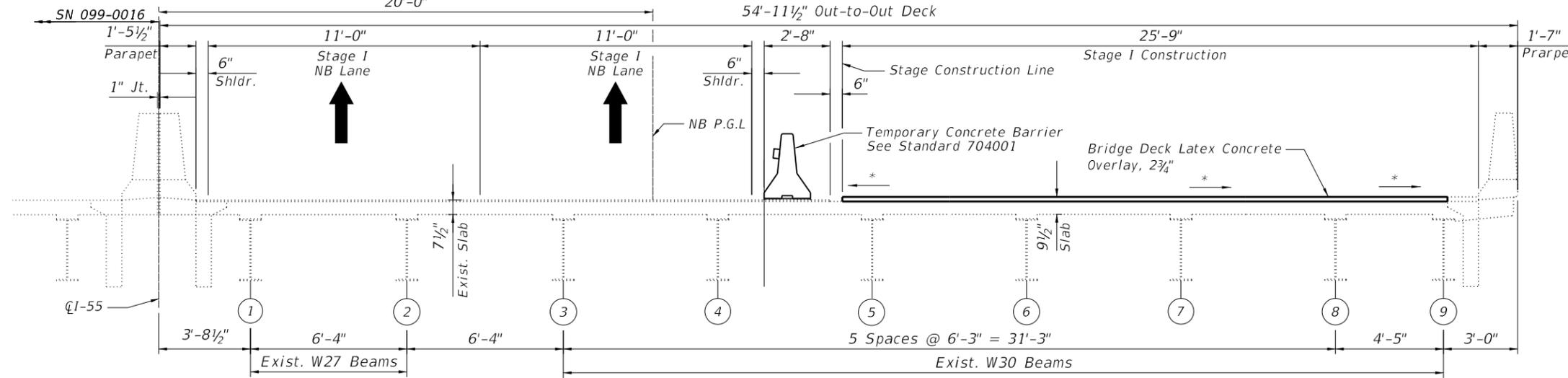
GENERAL NOTES, TOTAL BILL OF MATERIALS, & INDEX OF SHEETS
S.N. 099-0017 NB I-55 OVER US ROUTE 30

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62N22				
		ILLINOIS	FED. AID PROJECT	

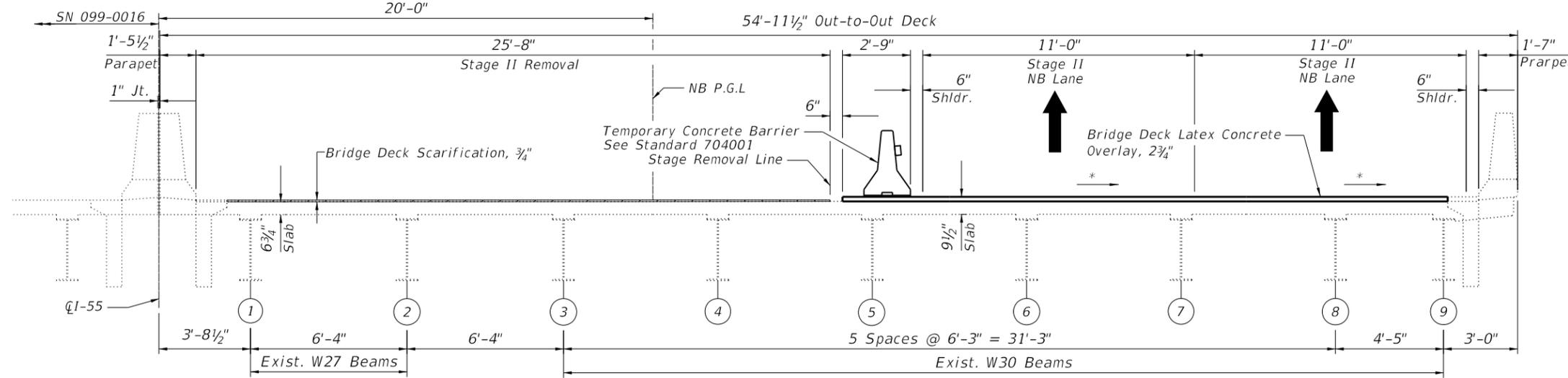
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STAGE I REMOVAL
 (Looking North)



STAGE I CONSTRUCTION
 (Looking North)



STAGE II REMOVAL
 (Looking North)

STAGE I REMOVAL

1. Install temporary concrete barrier as shown to locate traffic on the west side of the existing structure.
2. Scarify 3/4" from the top of deck slab.
3. Remove portions of bridge deck/approach slab adjacent to abutment joints, as shown in the plans.

STAGE I CONSTRUCTION

1. Perform full-depth deck slab repairs, and approach slab repairs, at locations as shown in the plans.
2. Install preformed joint strip seal at north and south abutments and replace associated reinforcement and concrete adjacent to the joint.
3. Perform structural repair of concrete for the abutments and piers.
4. Apply 2 3/4" bridge deck latex concrete overlay to bridge deck slab.
5. Perform bridge deck grooving for the 2 3/4" bridge deck latex concrete overlay.
6. Apply 2" Stone-Matrix Asphalt (SMA) Overlay to approach slabs.
7. Apply protective coat to the top of reconstructed transverse joint areas, the surface of the new overlay and the top and inside faces of parapets.

STAGE II REMOVAL

1. Install temporary concrete barrier as shown to locate traffic on the east side of the existing structure.
2. Scarify 3/4" from the top of deck slab.
3. Remove portions of bridge deck/approach slab adjacent to abutment joints, as shown in the plans.

*Match existing deck surface profile



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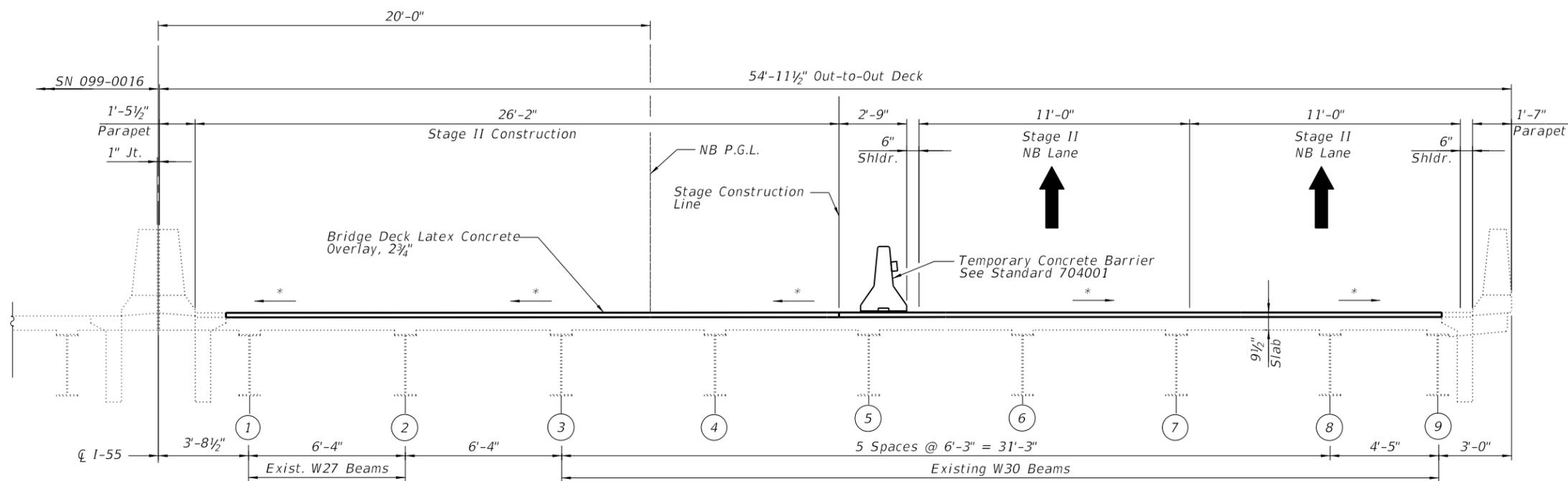
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CONSTRUCTION STAGING DETAILS - I
S.N. 099-0017 NB I-55 OVER US ROUTE 30

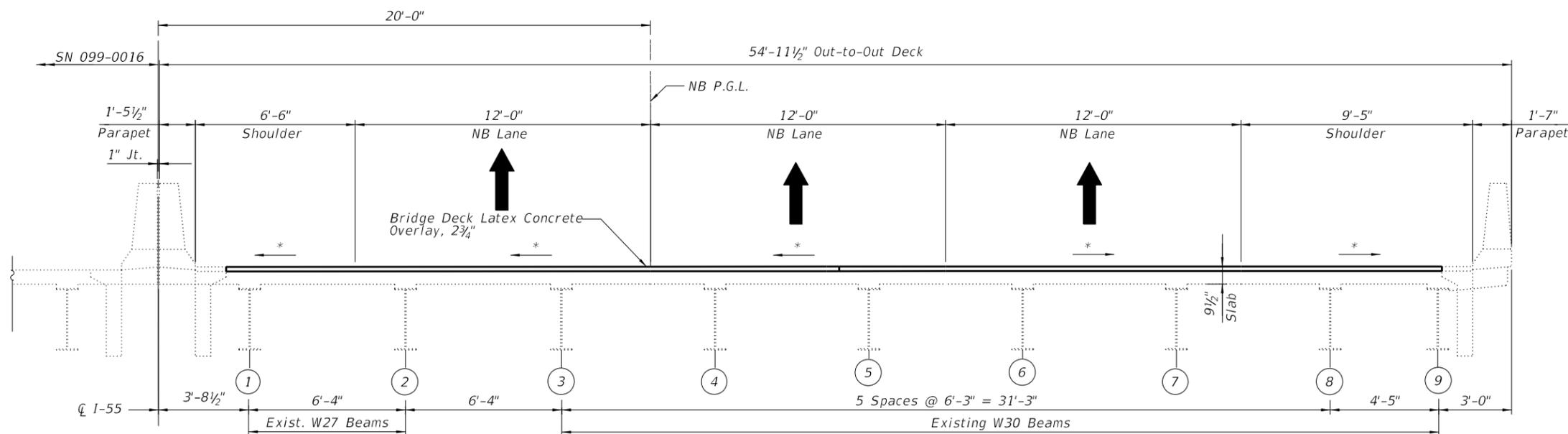
SHEET S1-03 OF S1-15 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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ILLINOIS FED. AID PROJECT				

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STAGE II CONSTRUCTION
 (Looking North)



FINAL CROSS SECTION
 (Looking North)

STAGE II CONSTRUCTION

1. Perform full- and partial-depth deck slab repairs, and approach slab repairs, at locations as shown in the plans.
2. Install preformed joint strip seal at north and south abutments and replace associated reinforcement and concrete adjacent to the joint.
3. Perform structural repair of concrete for the abutments and piers.
4. Apply 2 3/4" bridge deck latex concrete overlay to bridge deck slab.
5. Perform bridge deck grooving for the 2 3/4" bridge deck latex concrete overlay.
6. Apply 2" Stone-Matrix Asphalt (SMA) Overlay to approach slabs.
7. Apply protective coat to the top of reconstructed transverse joint areas, the surface of the new overlay and the top and inside faces of parapets.
8. Apply permanent pavement markings on top of deck and approach slabs.

*Match existing deck surface profile



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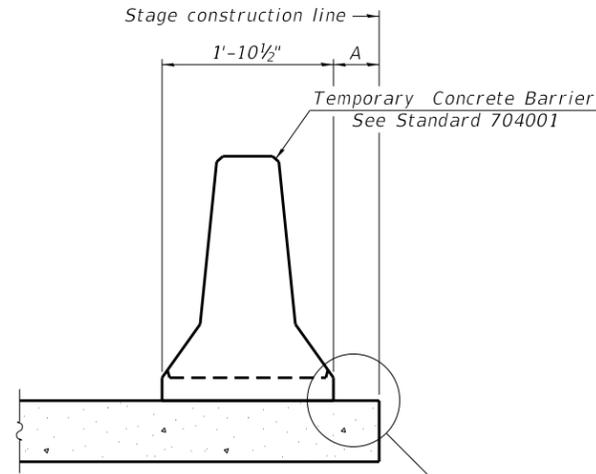
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CONSTRUCTION STAGING DETAILS - II
S.N. 099-0017 NB I-55 OVER US ROUTE 30

SHEET S1-04 OF S1-15 SHEETS

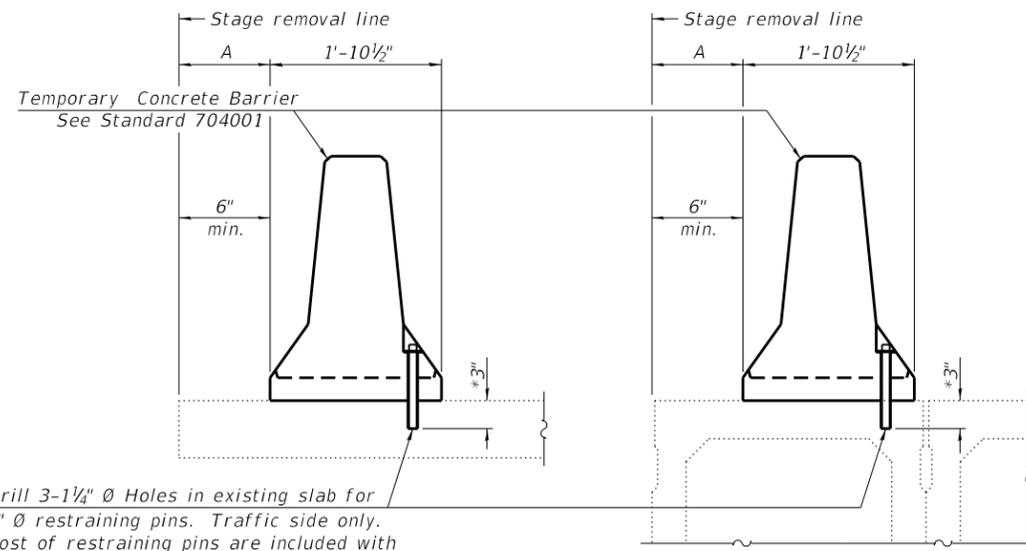
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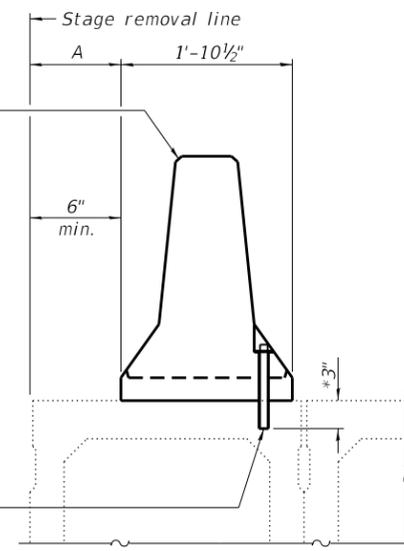
When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM



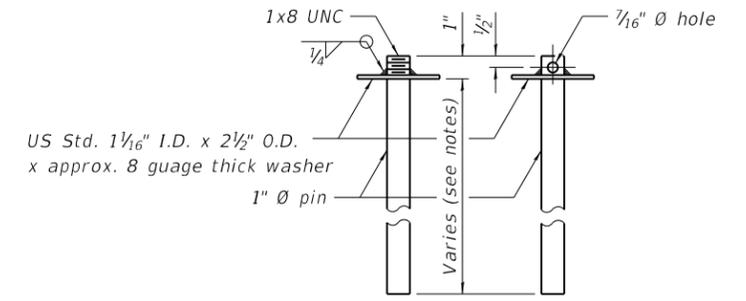
Drill 3-1/4" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

EXISTING SLAB



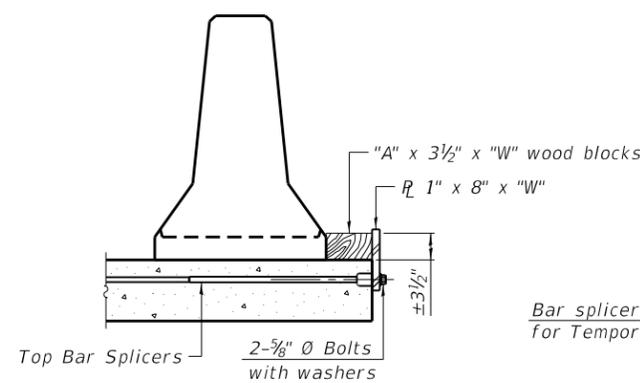
* When hot-mix asphalt wearing surface is present, embedment shall be 3" plus the wearing surface depth.

EXISTING DECK BEAM

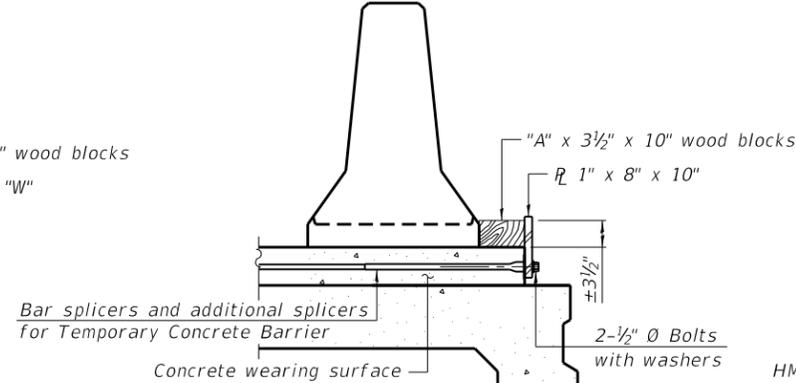


RESTRAINING PIN

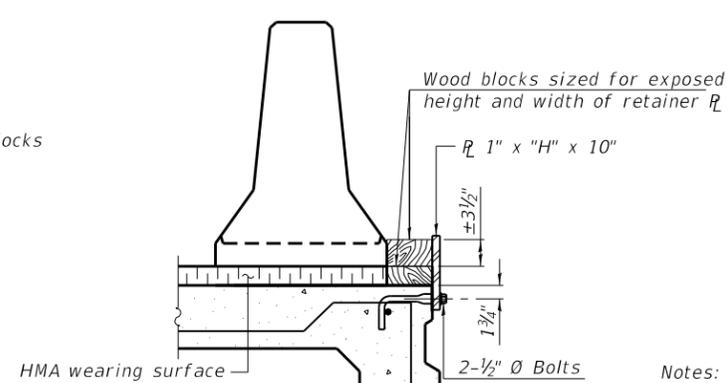
SECTIONS THRU SLAB OR DECK BEAM



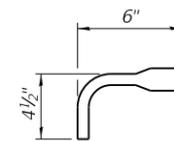
DETAIL I



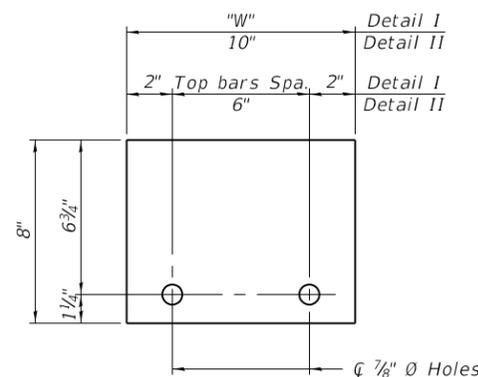
DETAIL II



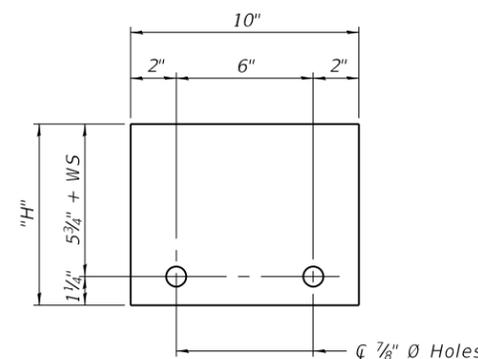
DETAIL III



BAR SPLICER FOR #4 BAR - DETAIL III



STEEL RETAINER R 1" x 8" x "W"
(Detail I and II)



STEEL RETAINER R 1" x "H" x 10"
(Detail III)

Notes:
 Cost of retainer assembly is included with Temporary Concrete Barrier.
 A retainer assembly shall be located at the approximate center of each temporary concrete barrier.
 The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.
 When the 'A' dimension is less than 1 1/2', the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6' to accommodate the shear key clamping device.

Detail I - Installation for a new bridge deck or bridge slab.
Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.
Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

R-27 8-11-2017



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DEPARTMENT OF TRANSPORTATION**

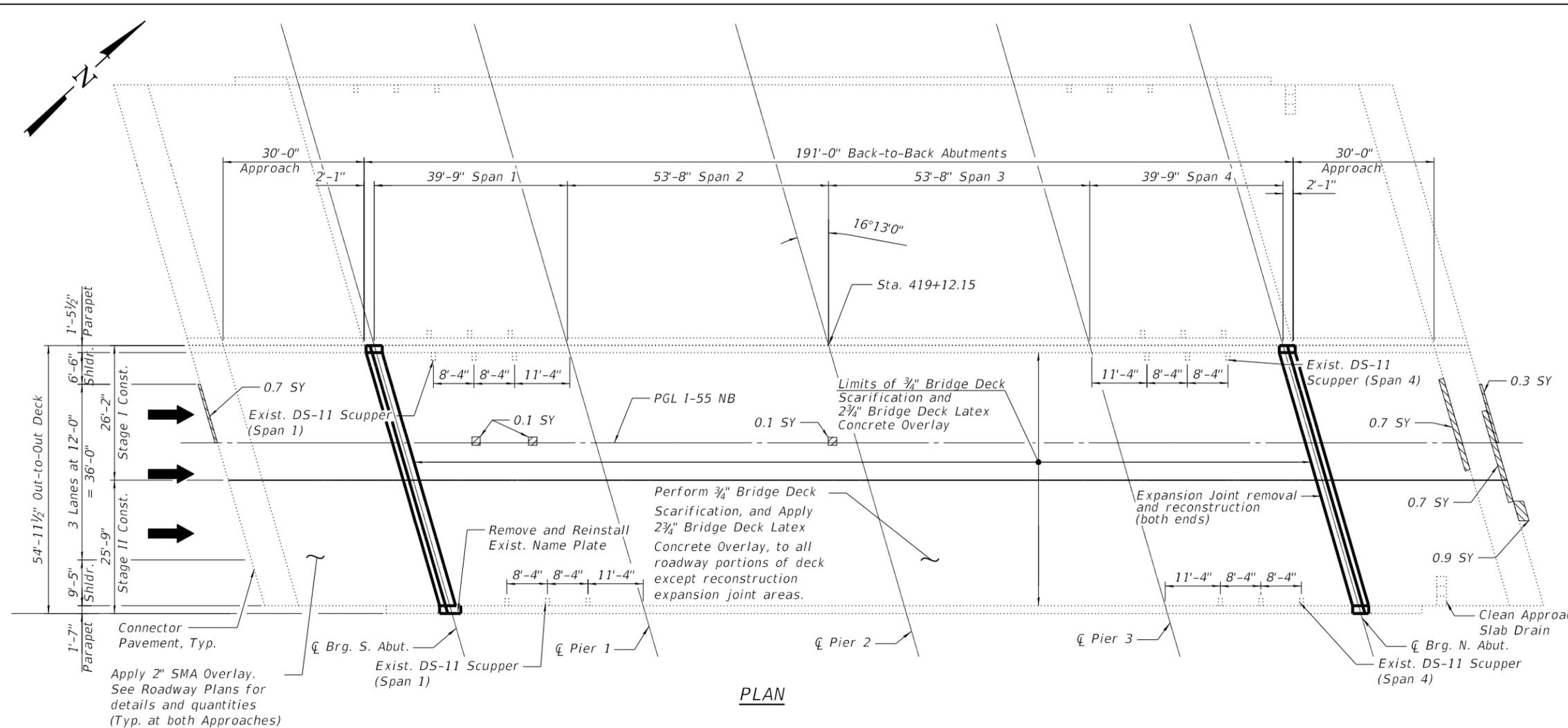
**TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION
S.N. 099-0017 NB I-55 OVER US ROUTE 30**

SHEET S1-05 OF S1-15 SHEETS

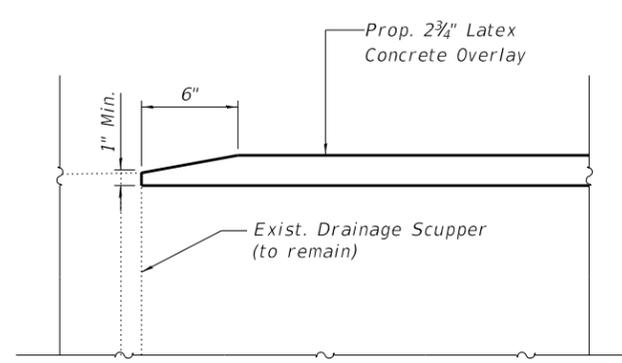
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55	2020-253-BR&PP	WILL	178	63
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

NOTES:

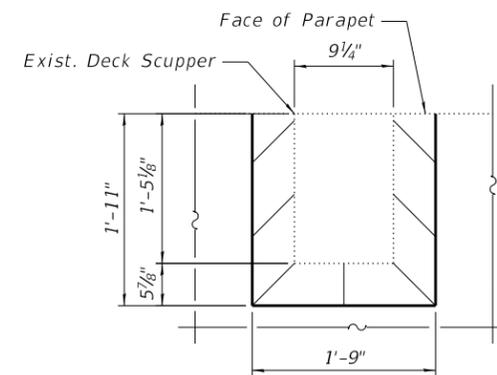
- Deck and approach slab repair areas are estimated based on visual inspection and will be paid for as specified in the Special Provision. Actual repair areas and locations shall be determined by the Engineer and shown on As-built plans. Engineer shall sound deck after deck scarification.
- Protective Coat shall be applied to the bridge overlay and front and top faces of the new and existing parapets.
- All dimensions are perpendicular to \bar{C} I-55 Bridge Deck.
- Protective Shield shall be placed over traffic lanes for US 30.
- For bridge deck final cross section, see Sheet S1-04.
- For North and South Transverse Joint Removal and Reconstruction, see Sheets S1-08 and S1-09.
- Perform Bridge Deck Grooving for the Bridge Deck Latex Concrete Overlay and the roadway portions of the Reconstructed Transverse Joints.
- Any reinforcement bars that are damaged during concrete removal operations shall be repaired or replaced using an approved bar splicer or anchorage system. Cost included with Concrete Removal.
- The Contractor shall exercise extreme caution during concrete removal to avoid damaging the steel beams and diaphragms to remain. Any damage to the existing steel beams and/or diaphragms to remain caused by the Contractor in the performance of his/her work shall be repaired by the Contractor, to the satisfaction of the Engineer at no cost to the Department.



PLAN



DRAINAGE SCUPPER DETAIL



TOP PLAN AT DRAINAGE SCUPPER

LEGEND

- Deck Slab Repair (Partial) *
- Approach Slab Repair (Full Depth)
- Structural Repair of Concrete (Depth Equal to or Less Than 5")
- SY Square Yards

*Areas of Deck Slab Repair (Partial) are provided for information only and shall be included in the cost of Bridge Deck Latex Concrete Overlay, 2 3/4"

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Approach Slab Repair (Partial Depth)	Sq. Yd.	4
Protective Coat	Sq. Yd.	1457
Bridge Deck Grooving	Sq. Yd.	1092
Bridge Deck Latex Concrete Overlay, 2 3/4"	Sq. Yd.	1108
Bridge Deck Scarification, 3/4"	Sq. Yd.	1108
Protective Shield	Sq. Yd.	656
Clean Approach Slab Drain	Each	1

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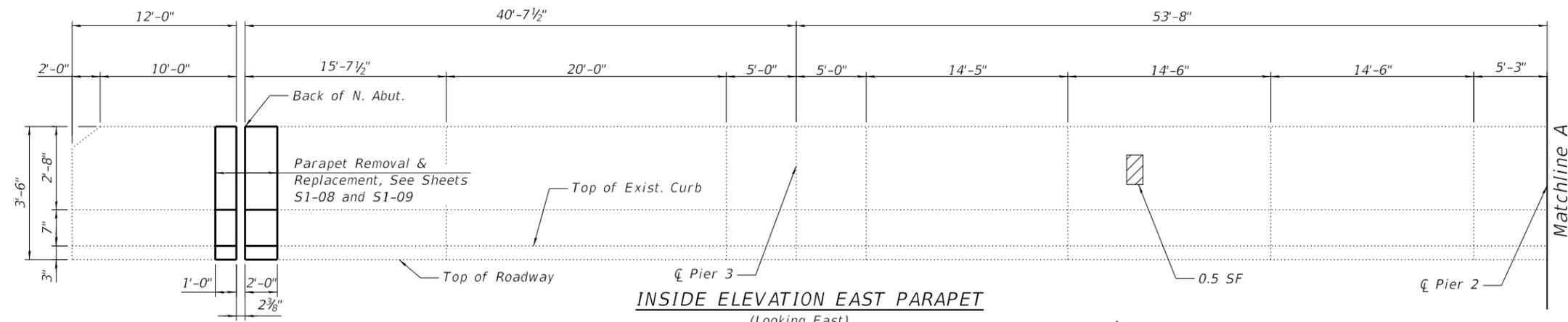
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BRIDGE DECK REPAIRS
S.N. 099-0017 NB I-55 OVER US ROUTE 30**

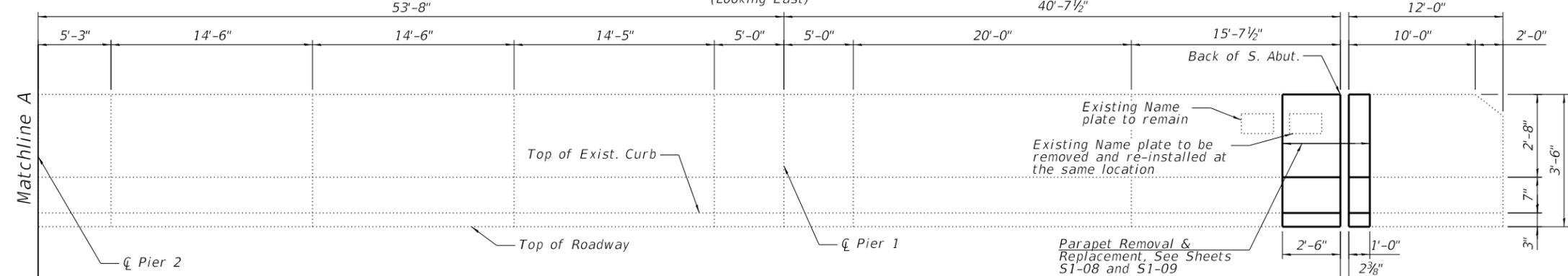
SHEET S1-06 OF S1-15 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	64
CONTRACT NO. 62N22				
ILLINOIS		FED. AID PROJECT		

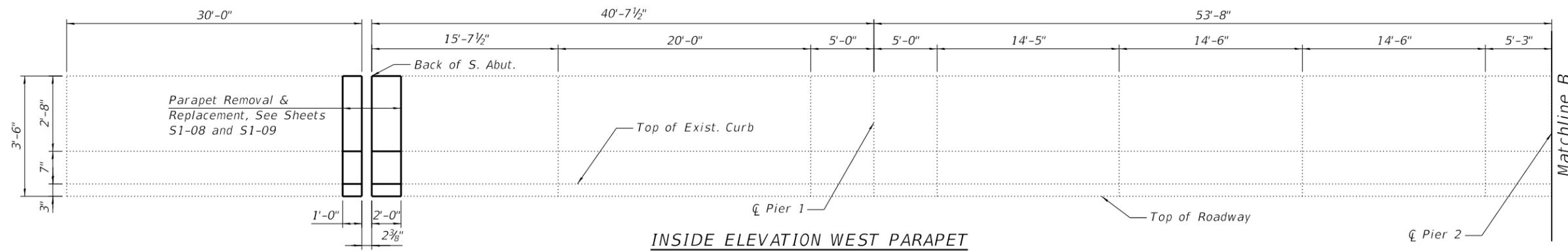
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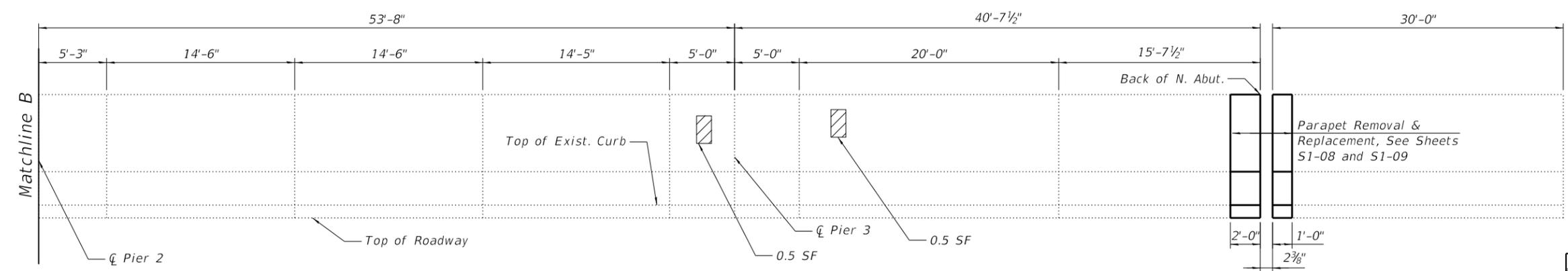
INSIDE ELEVATION EAST PARAPET
(Looking East)



INSIDE ELEVATION EAST PARAPET
(Looking East)



INSIDE ELEVATION WEST PARAPET
(Looking West)



INSIDE ELEVATION WEST PARAPET
(Looking West)

LEGEND

Structural Repair of Concrete (Depth Equal to or Less than 5 Inches)

SF Square Foot

BILL OF MATERIAL

Item	Unit	Total
Structural Repair of Concrete (Depth Equal to or Less than 5")	Sq Ft	1.5



USER NAME =	DESIGNED - JMT	REVISED -
	CHECKED - SPS	REVISED -
PLOT SCALE =	DRAWN - IH	REVISED -
PLOT DATE =	CHECKED - JMT	REVISED -

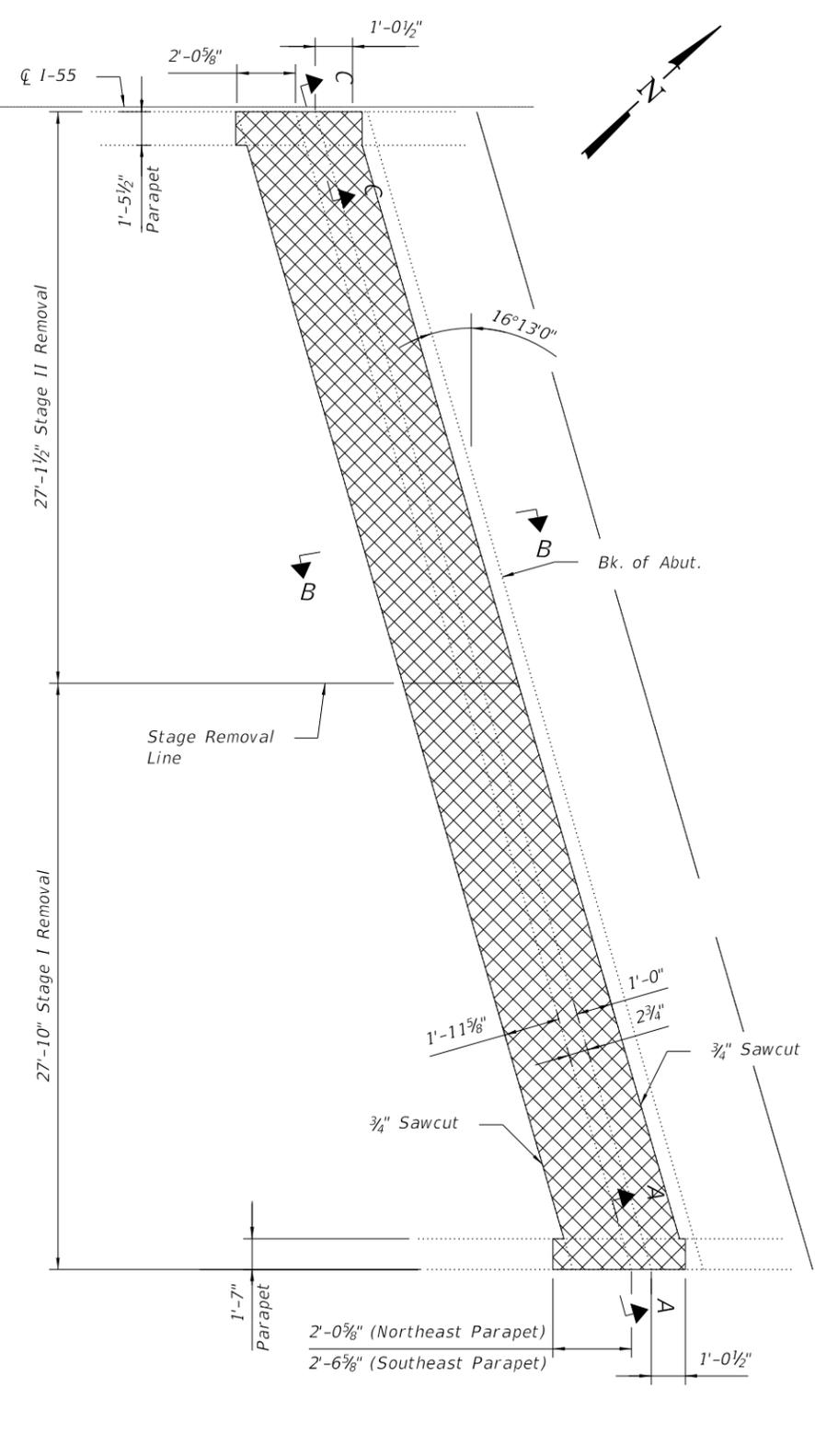
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PARAPET REPAIRS
S.N. 099-0017 NB I-55 OVER US ROUTE 30

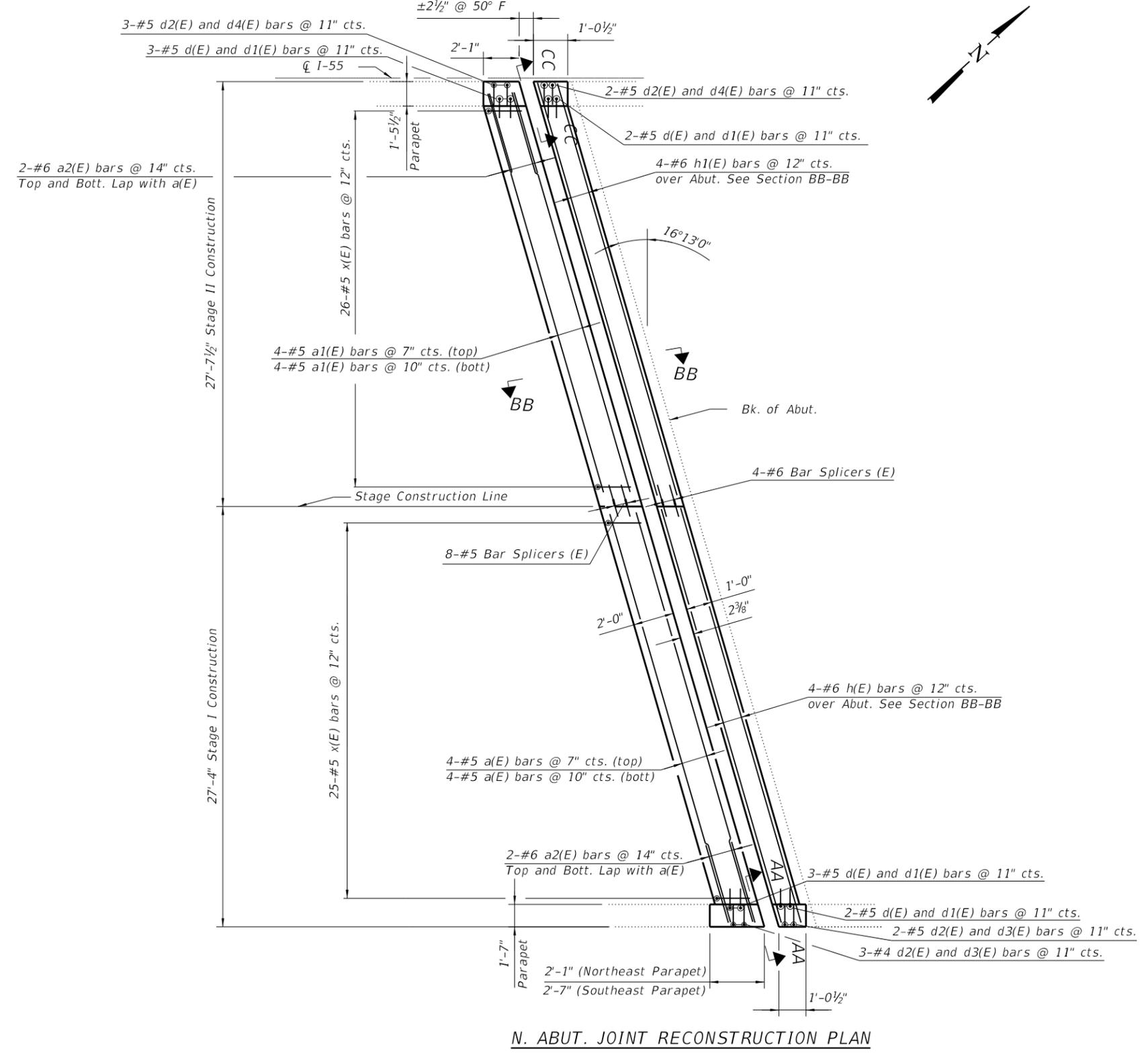
SHEET S1-07 OF S1-15 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

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N. ABUT. JOINT REMOVAL PLAN



N. ABUT. JOINT RECONSTRUCTION PLAN

- NOTES:**
1. North Abutment Shown, South Abutment similar.
 2. Horizontal bars in approach parapets shall be cleaned, straightened, and reused in new construction.
 3. Hatched areas indicate Concrete Removal.



USER NAME =	DESIGNED - JMT	REVISED -
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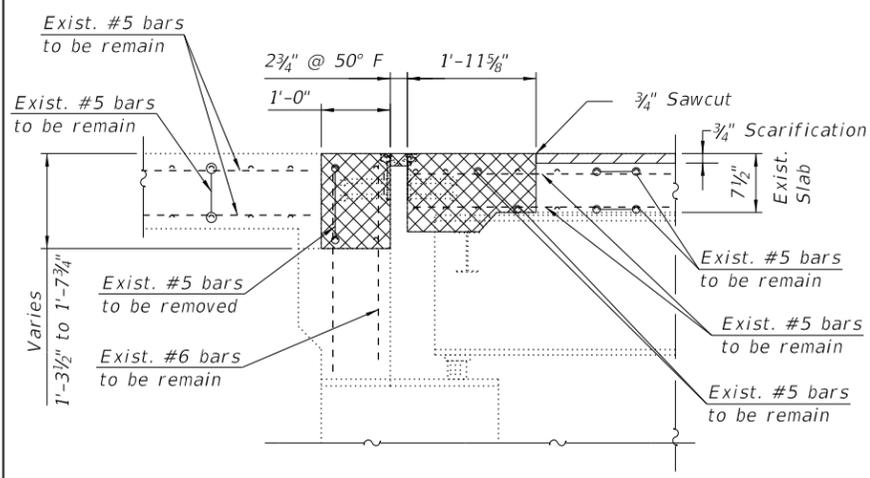
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**EXPANSION JOINT REMOVAL AND CONSTRUCTION
 S.N. 099-0017 NB I-55 OVER US ROUTE 30**

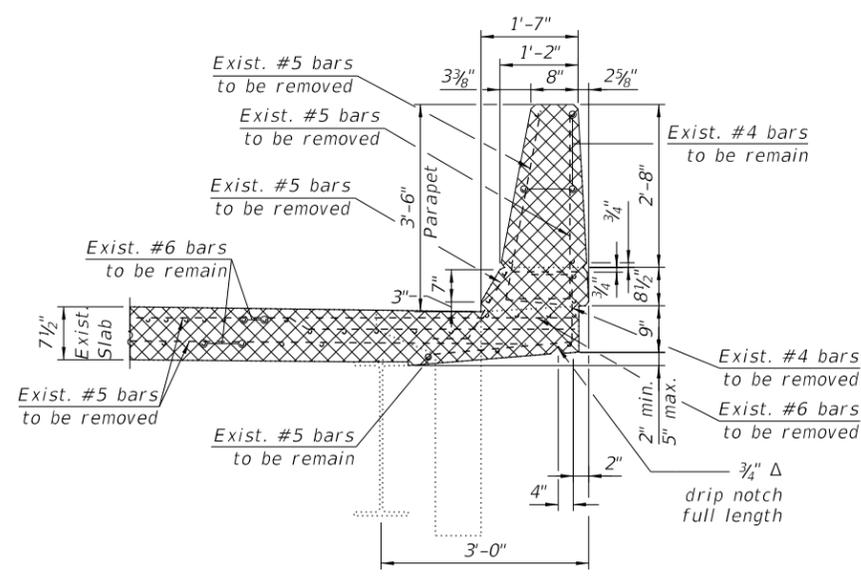
SHEET S1-08 OF S1-15 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

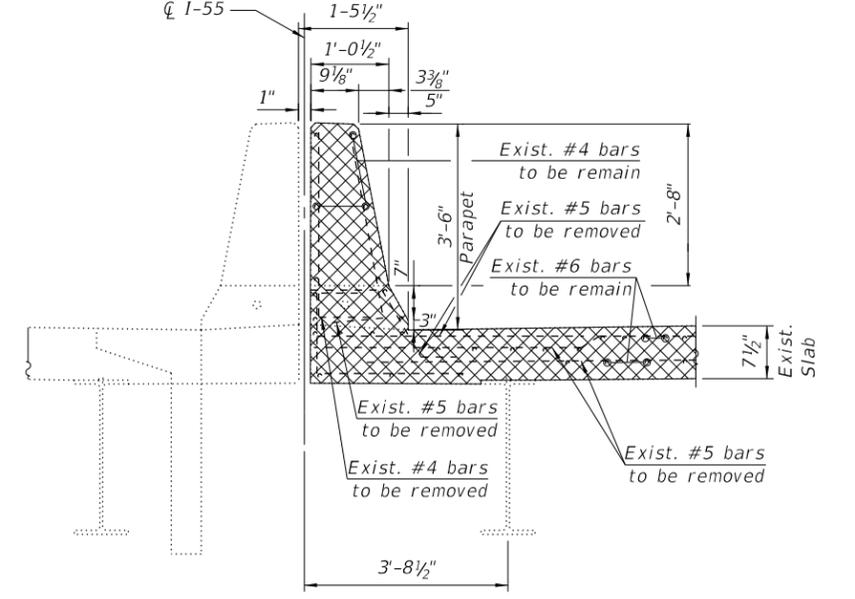
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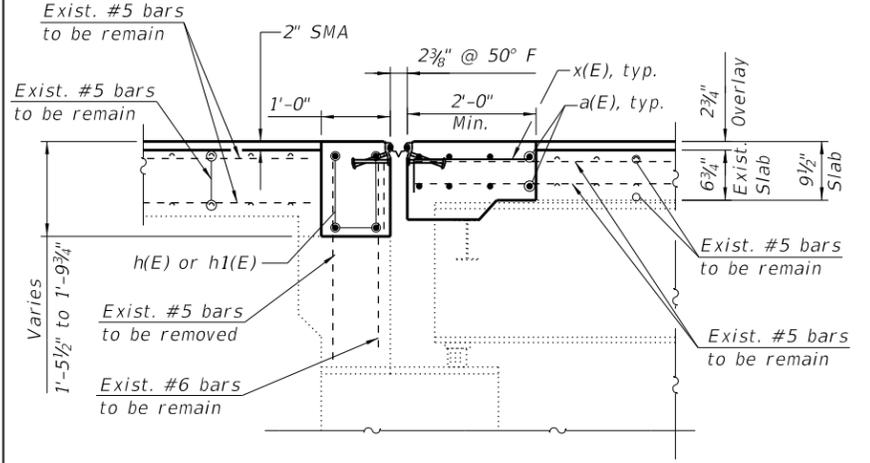
SECTION B-B



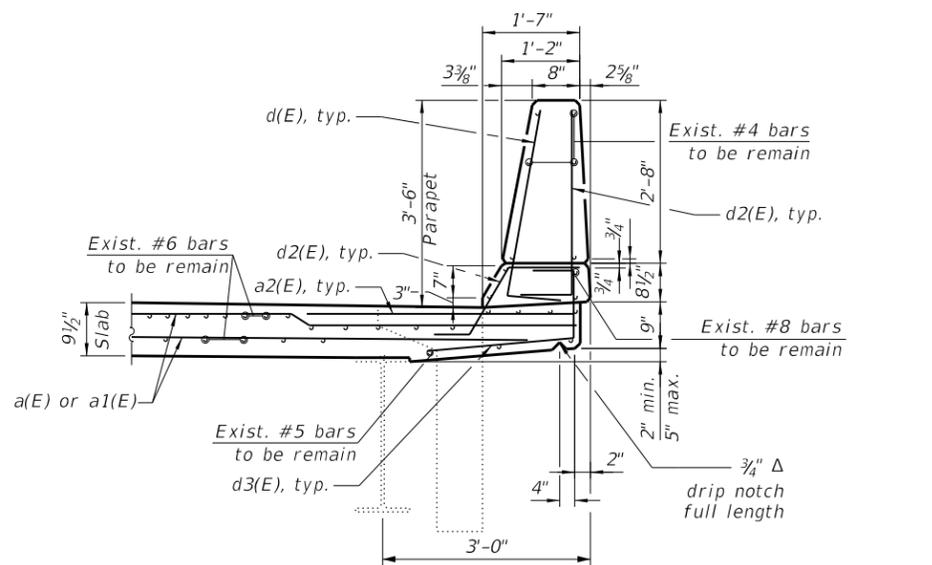
SECTION A-A



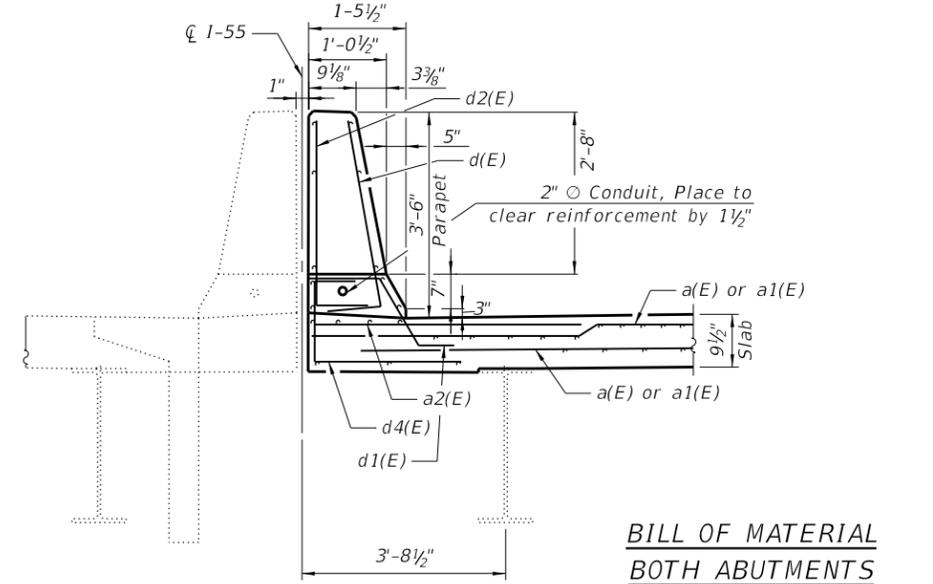
SECTION C-C



SECTION BB-BB



SECTION AA-AA



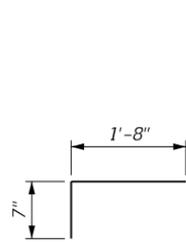
SECTION CC-CC

LEGEND:

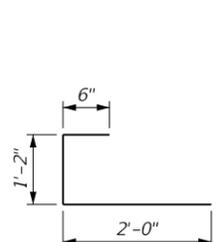
- Concrete Removal
- 3/4" Scarification

NOTES:

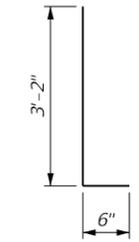
1. For preformed joint seal details, see sheets S1-10.
2. For Bar Splicer Assembly details, see sheet S1-15
3. Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.
4. Removal of Exist. Expansion Joint will not be paid for separately but will be included in the cost of Concrete Removal.



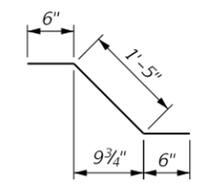
Bar x(E)



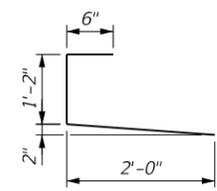
Bar d4(E)



Bar d(E) & d2(E)



Bar d1(E)



Bar d3(E)

BILL OF MATERIAL BOTH ABUTMENTS

Bar	No.	Size	Length	Shape
a(E)	16	#5	28'-1"	—
a1(E)	16	#5	28'-5"	—
a2(E)	16	#6	4'-6"	—
d(E)	20	#5	3'-8"	┌
d1(E)	20	#5	2'-5"	┌
d2(E)	20	#4	3'-8"	┌
d3(E)	10	#4	3'-8"	┌
d4(E)	10	#5	3'-8"	┌
h(E)	8	#6	28'-1"	—
h1(E)	8	#6	28'-5"	—
x(E)	106	#5	2'-3"	┌
Concrete Removal			Cu. Yd.	16.1
Reinforcement Bars, Epoxy Coated			Pound	2220
Concrete Superstructure			Cu. Yd.	17.4



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PLOT SCALE =	DRAWN - IH	REVISED -
PLOT DATE =	CHECKED - JMT	REVISED -

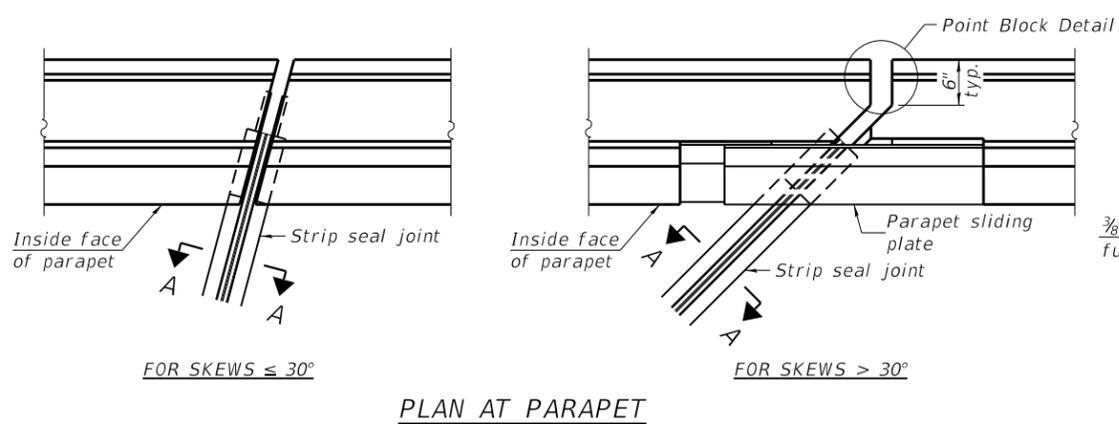
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

EXPANSION JOINT DETAILS
 S.N. 099-0017 NB I-55 OVER US ROUTE 30

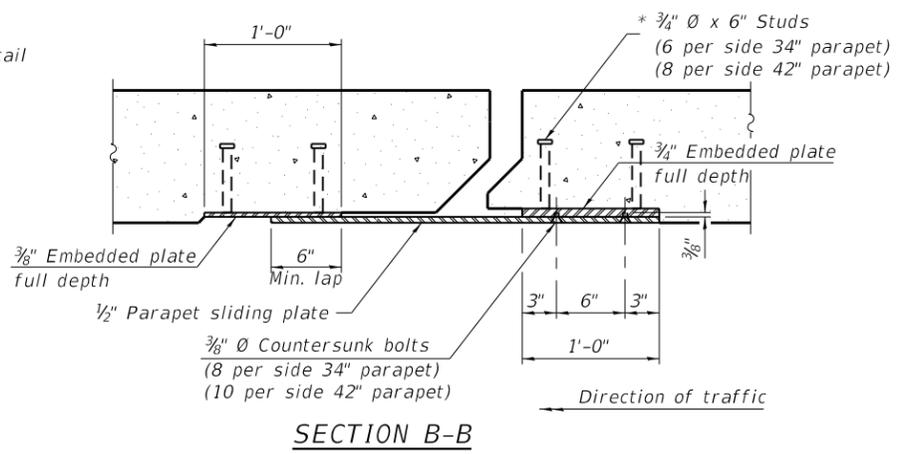
SHEET S1-09 OF S1-15 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	67
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

MODEL: Default
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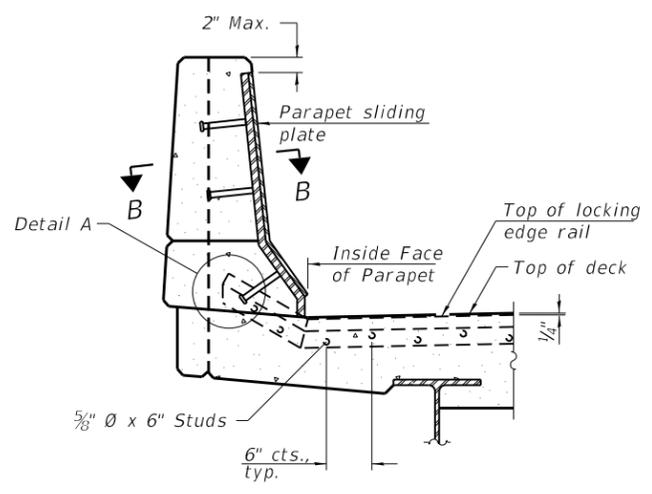


PLAN AT PARAPET



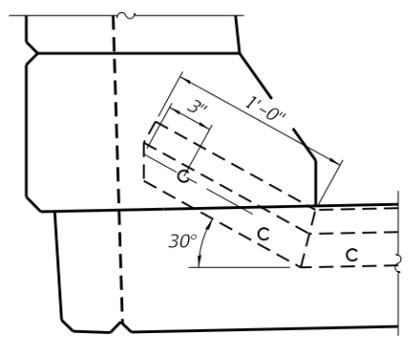
SECTION B-B

Notes:
 The strip seal shall be made continuous and shall have a minimum thickness of 1/4 inch. The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.
 The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the 4 1/2 inch maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.
 The manufacturer's recommended installation methods shall be followed.
 All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.
 The Maximum space between locking edge rail segments shall be 3/16 inch and sealed with a suitable sealant; however, any rail joint within 10 feet measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.
 Cost of parapet sliding plates, embedded plates, and anchorage studs included with Preformed Joint Strip Seal.
 34 inch F-shape barrier shown, 42 inch F-shape similar as noted.
 The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required. One exception to this would be the strip seal joint at the end of the precast bridge approach slab. For these cases the pavement connector length shall be adjusted, not the length of the bridge approach slab.

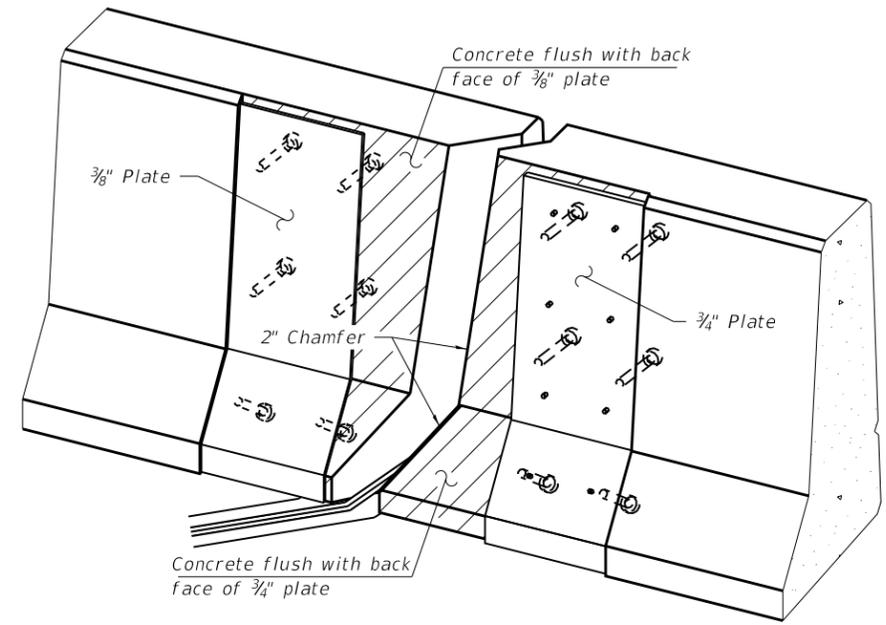


ELEVATION AT PARAPET

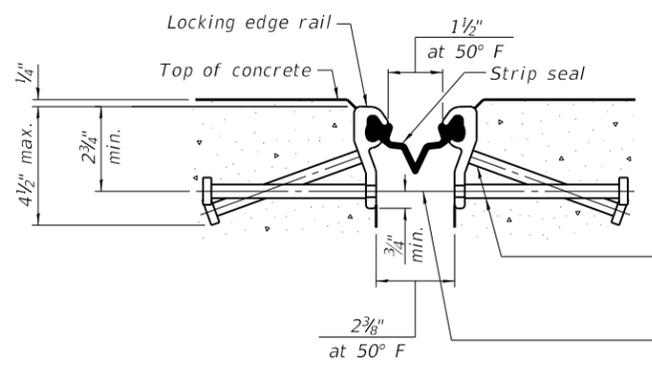
(Skews > 30° shown. Skews <= 30° similar except as shown in plan view.)



DETAIL A



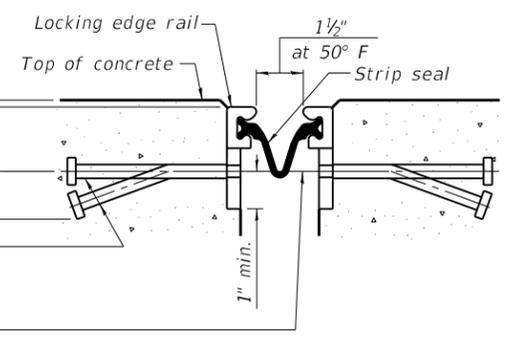
TRIMETRIC VIEW
(Showing embedded plates only)



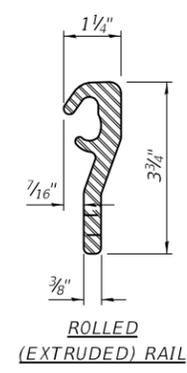
SHOWING ROLLED RAIL JOINT

* 5/8 inch diameter x 6 inch studs @ 6 inch cts. (alternate angled/bent studs with horizontal studs)

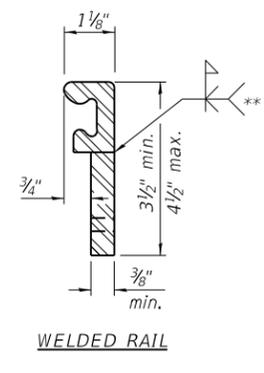
3/8 inch diameter threaded rods in 7/16 inch diameter holes at ±4'-0" cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.



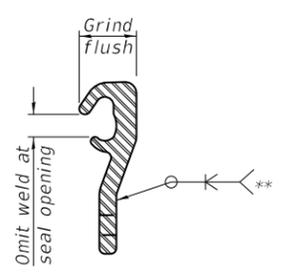
SHOWING WELDED RAIL JOINT



ROLLED (EXTRUDED) RAIL



WELDED RAIL



LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

LOCKING EDGE RAILS

** Back gouge not required if complete joint penetration is verified by mock-up.

SECTION A-A

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	115

EJ-SS

8-11-17



USER NAME =	DESIGNED - JMT	REVISED -
PLOT SCALE =	CHECKED - SPS	REVISED -
PLOT DATE =	DRAWN - IH	REVISED -
	CHECKED - JMT	REVISED -

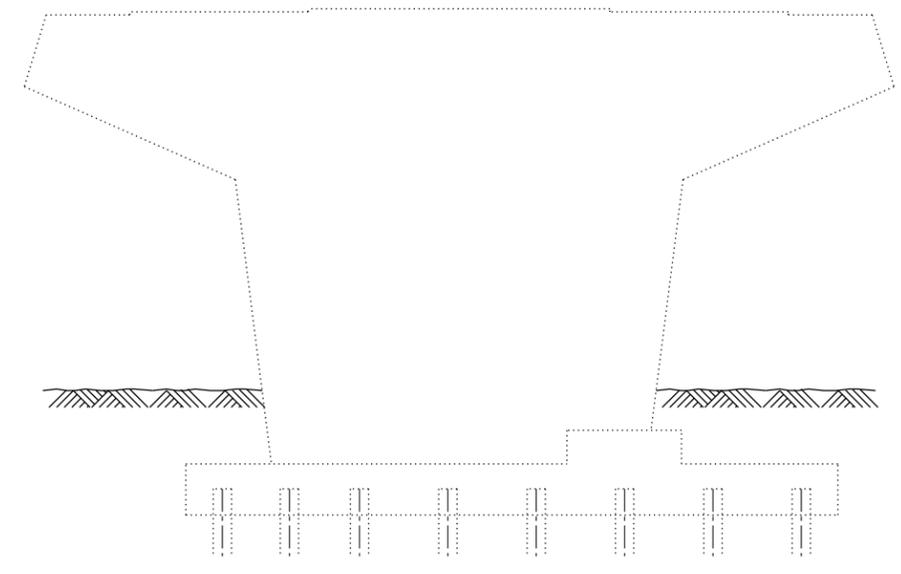
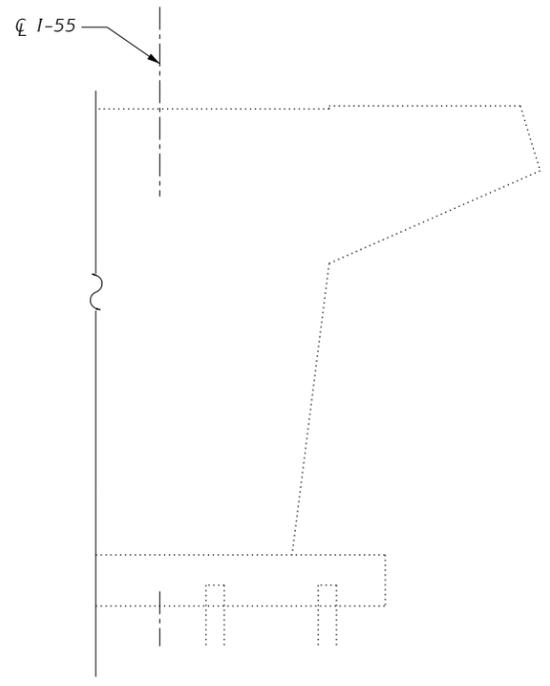
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PREFORMED JOINT STRIP SEAL
S.N. 099-0017 NB I-55 OVER US ROUTE 30**

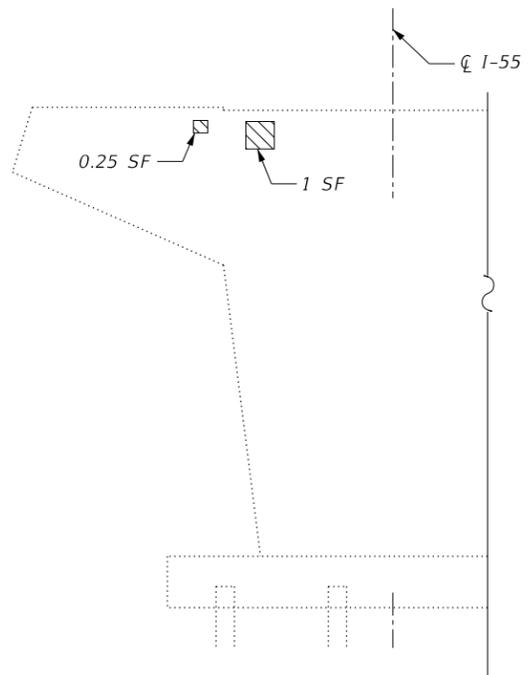
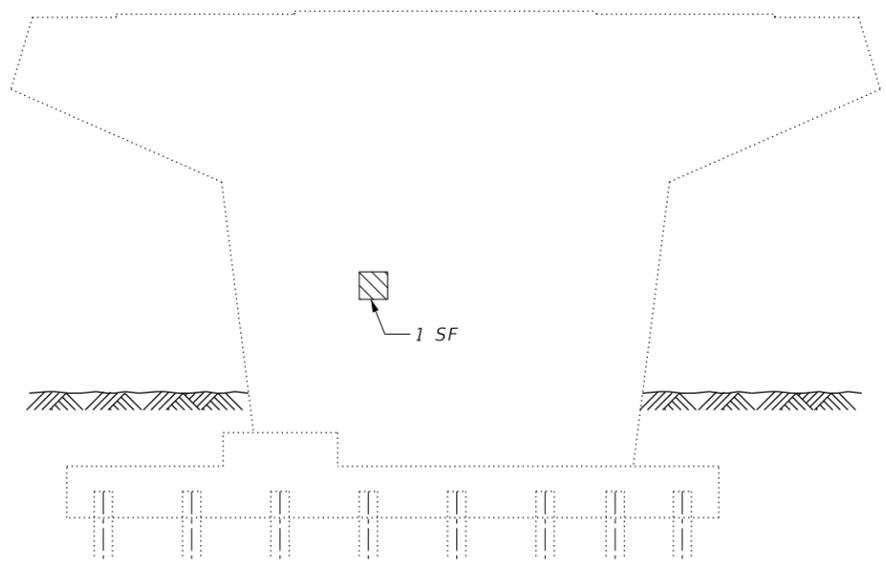
SHEET S1-10 OF S1-15 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

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PIER 1
(Looking North)



PIER 1
(Looking South)

LEGEND:

- Structural Repair of Concrete
(Depth Equal to or less than 5")
- Exposed Reinforcement
- Hairline Crack
- SF Square Foot

NOTES:

1. Quantities and limits shown are estimated for bidding purposes only. The actual areas to be repaired, and the type(s) of repairs to be used, will be determined by the Engineer in the field at the time of construction.

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth Equal to or Less Than 5")	Sq Ft	2.25



USER NAME =	DESIGNED - JMT	REVISED -
	CHECKED - SPS	REVISED -
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PLOT DATE =	CHECKED - JMT	REVISED -

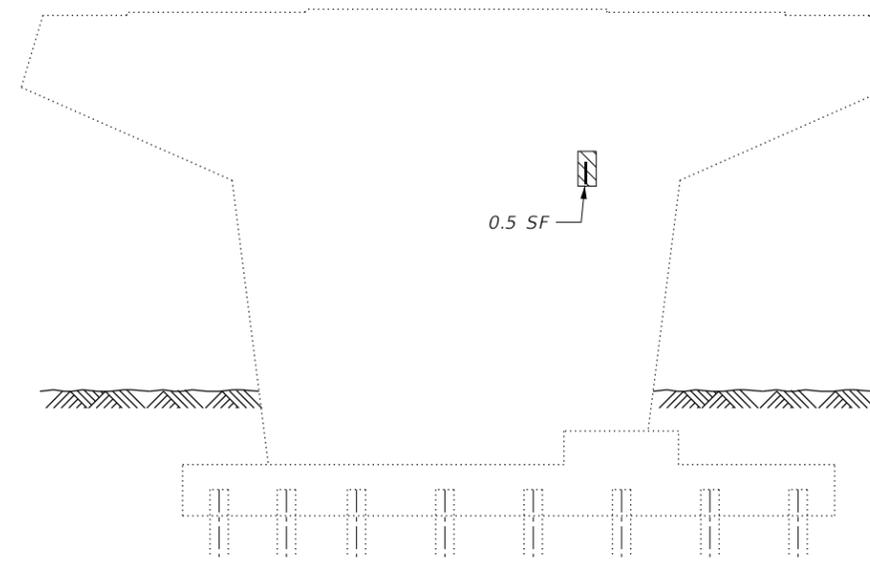
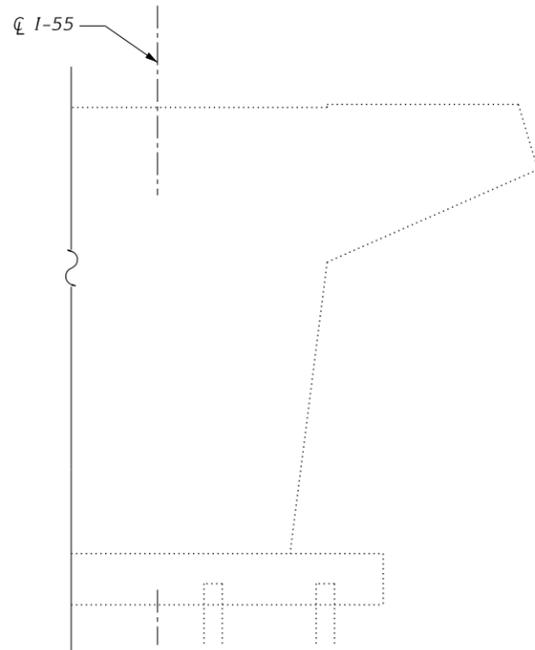
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER 1 REPAIRS
S.N. 099-0017 NB I-55 OVER US ROUTE 30

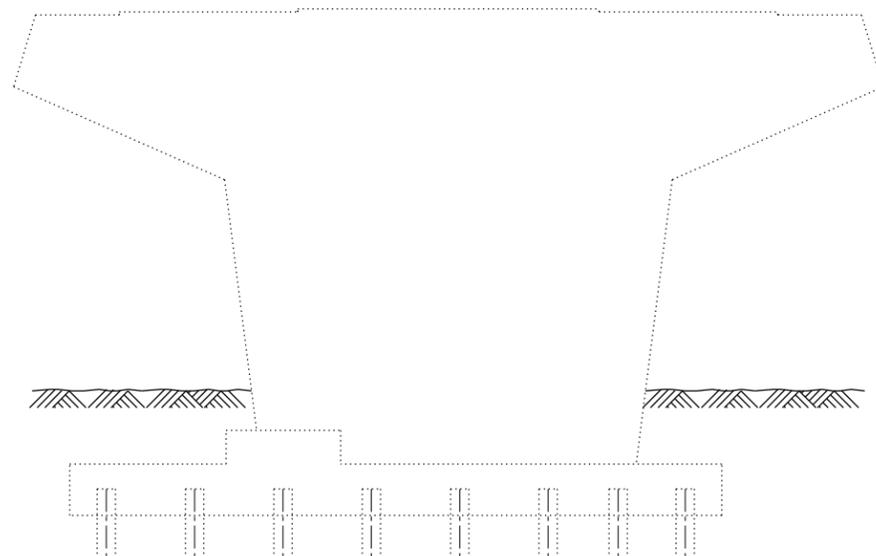
SHEET S1-11 OF S1-15 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	69
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62N22	

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PIER 2
 (Looking North)



PIER 2
 (Looking South)

LEGEND:



Structural Repair of Concrete
 (Depth Equal to or less than 5")



Exposed Reinforcement



Hairline Crack

SF Square Foot

NOTES:

- Quantities and limits shown are estimated for bidding purposes only. The actual areas to be repaired, and the type(s) of repairs to be used, will be determined by the Engineer in the field at the time of construction.

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth Equal to or Less Than 5")	Sq Ft	0.5



USER NAME =	DESIGNED - JMT	REVISED -
	CHECKED - SPS	REVISED -
PLOT SCALE =	DRAWN - IH	REVISED -
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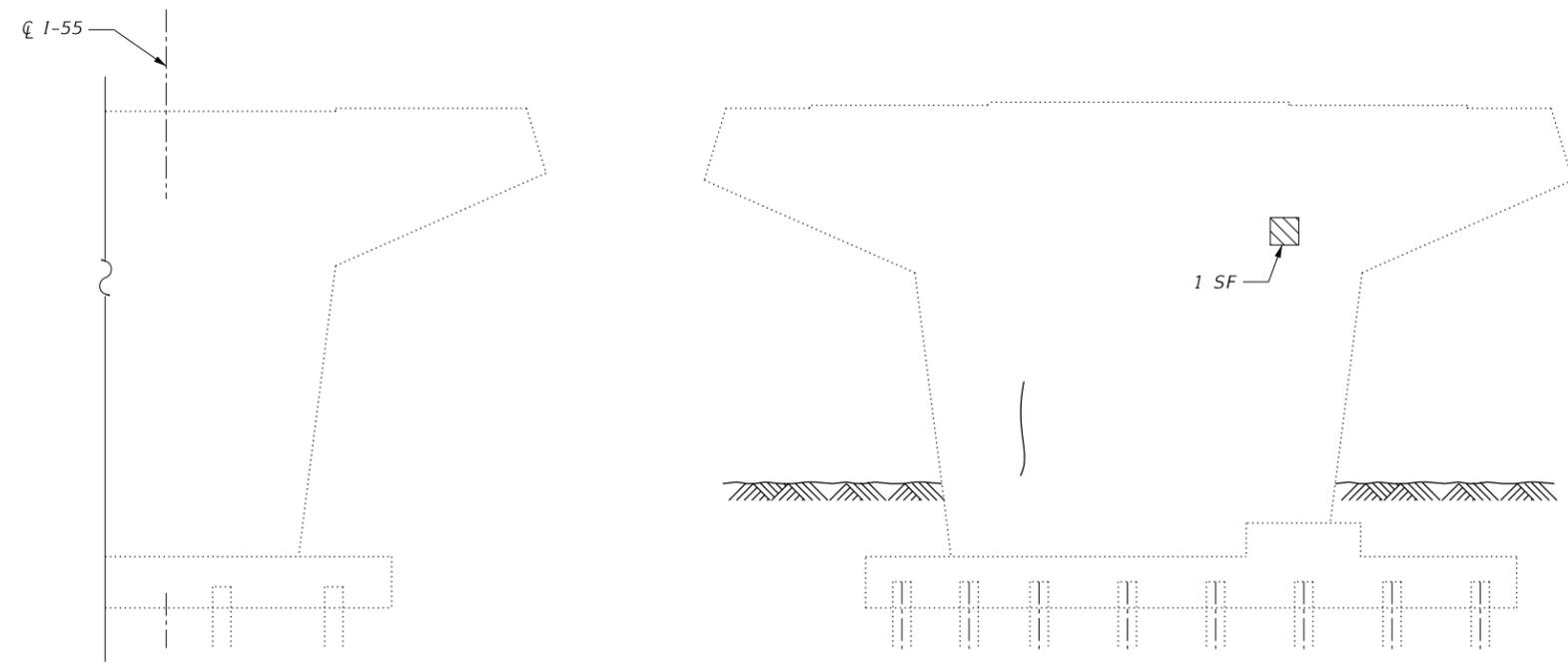
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER 2 REPAIRS
S.N. 099-0017 NB I-55 OVER US ROUTE 30

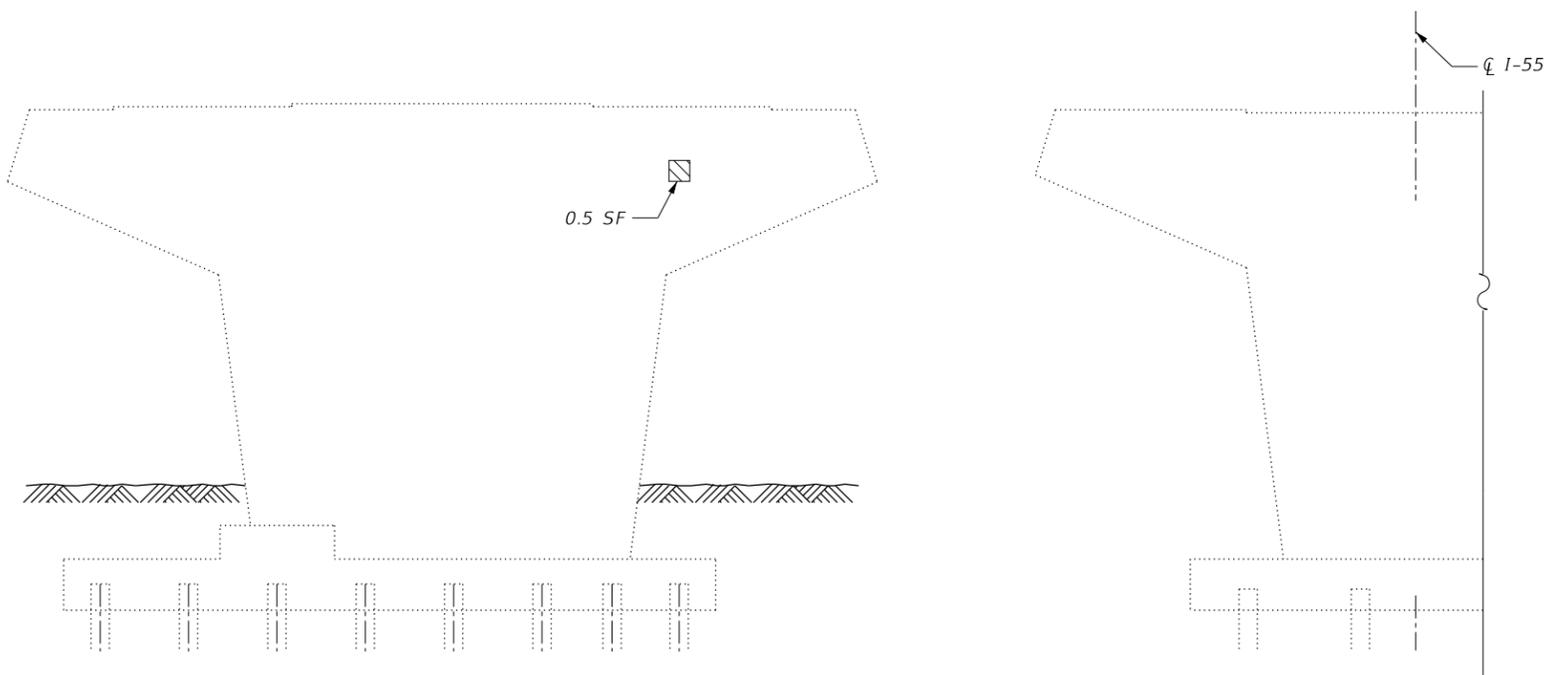
SHEET S1-12 OF S1-15 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	70
			CONTRACT NO. 62N22	
		ILLINOIS	FED. AID PROJECT	

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PIER 3
 (Looking North)



PIER 3
 (Looking South)

LEGEND:

-  Structural Repair of Concrete
 (Depth Equal to or less than 5")
-  Exposed Reinforcement
-  Hairline Crack
- SF Square Foot

NOTES:

1. Quantities and limits shown are estimated for bidding purposes only. The actual areas to be repaired, and the type(s) of repairs to be used, will be determined by the Engineer in the field at the time of construction.
2. Apply Concrete Sealer to new concrete surfaces of structurally repaired concrete.

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth Equal to or Less Than 5")	Sq Ft	1.5



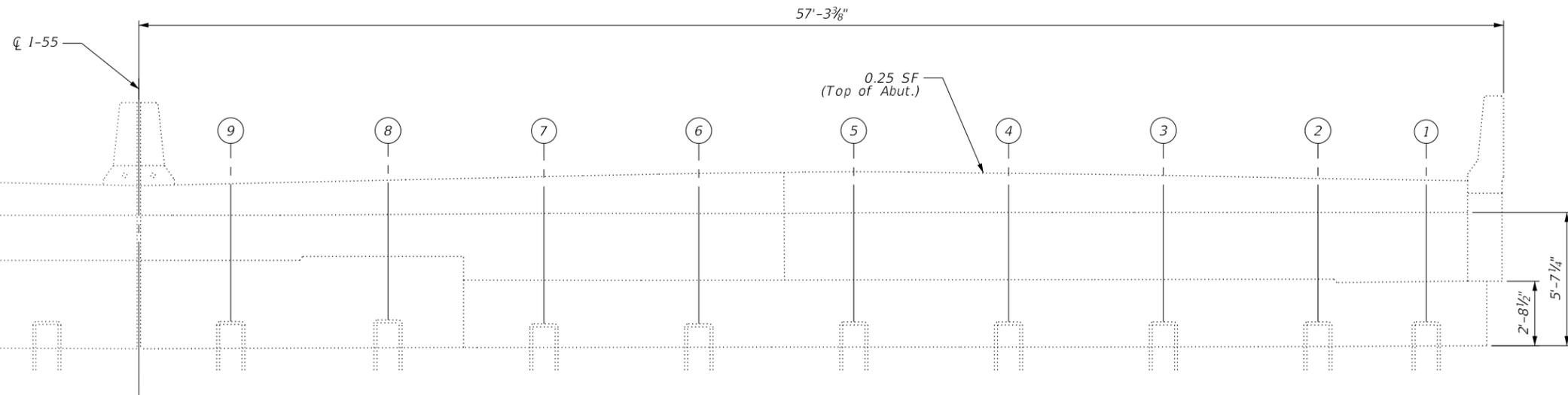
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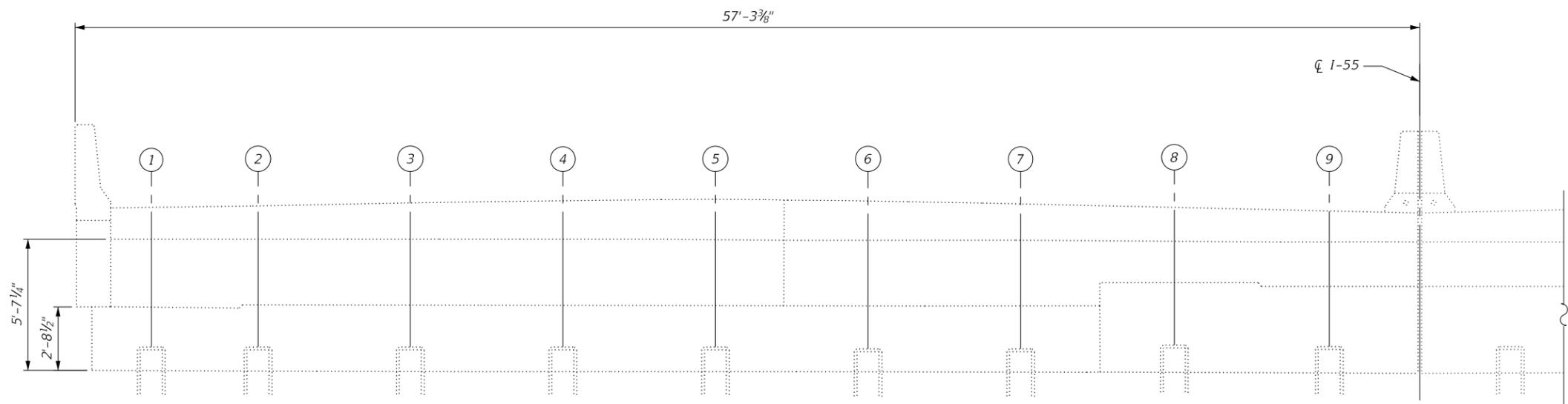
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S.N. 099-0017 NB I-55 OVER US ROUTE 30

SHEET S1-13 OF S1-15 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	71
CONTRACT NO. 62N22				
		ILLINOIS	FED. AID PROJECT	



ELEVATION - NORTH ABUTMENT
(Looking North)



ELEVATION - SOUTH ABUTMENT
(Looking South)

NOTE:

- Quantities and limits shown are estimated for bidding purposes only. The actual areas to be repaired, and the type(s) of repairs to be used, will be determined by the Engineer in the field at the time of construction.
- Apply Concrete Sealer to face of abutments.

LEGEND:

SF Square Foot

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth Equal to or Less Than 5")	Sq Ft	0.25
Concrete Sealer	Sq Yd	333

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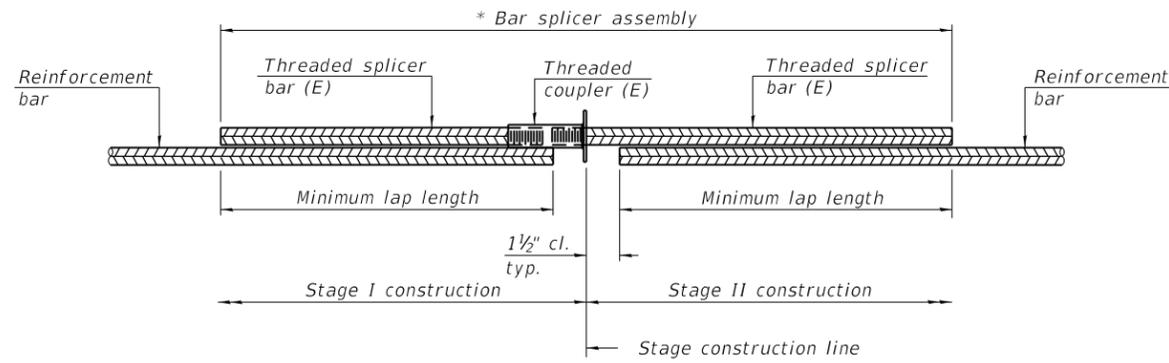
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**NORTH AND SOUTH ABUTMENT REPAIRS
S.N. 099-0017 NB I-55 OVER US ROUTE 30**

SHEET S1-14 OF S1-15 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	72
CONTRACT NO. 62N22				

ILLINOIS FED. AID PROJECT

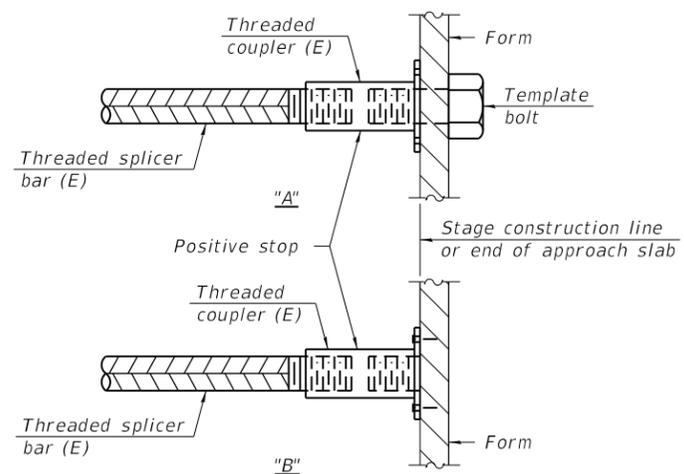


STANDARD BAR SPLICER ASSEMBLY PLAN
 (All components shall be provided from one supplier)

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

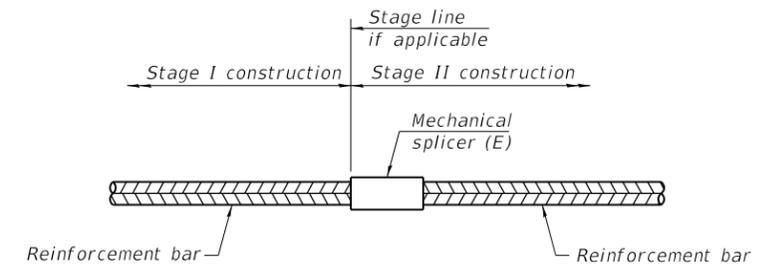
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
N. Abut.	#5	8	3'-6"
N. Abut.	#6	4	4'-0"
S. Abut.	#5	8	3'-6"
S. Abut.	#6	4	4'-0"



INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required

Notes:
 Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

MODEL: Default
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BSD-1

1-1-2020



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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
S.N. 099-0017 NB I-55 OVER US ROUTE 30

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	73
			CONTRACT NO. 62N22	

SHEET S1-15 OF S1-15 SHEETS

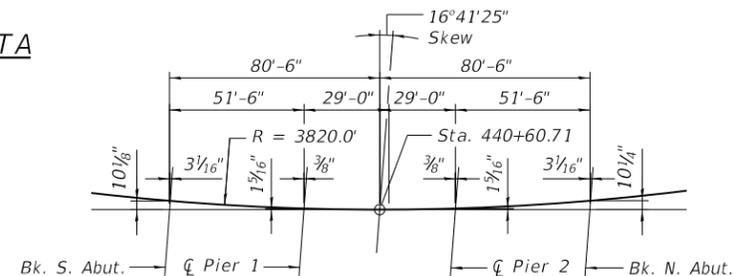
ILLINOIS FED. AID PROJECT

Existing Structure: F.A.I.-187(8), Will County. Both the superstructure and substructure were widened in 1977 as part of F.A.I. Route 55, Stevenson Expressway Rehabilitation, Section (99-1&2) R-5, Project I-IR-55-6 (125) 246, Will County. Also, additional rehabilitation work was performed in 1994 under contract for F.A.I. Route 55 (Stevenson Expressway) Section (26 & 27)RS, 27(B,VB)R; 27(B-1, B-3, HB, VB-1)BR (89), Station 1453+52.19, Project IM-NHI-55-6 (193) 252, Will County. The superstructures and substructures were widened to the inside in 2006 as part of F.A.I. Route 55 Sec. 2006-032 BY, Will County. Both structures consist of R.C. deck supported on three-span continuous steel beams. The abutments are supported on concrete piles and/or HP steel piles. The piers are supported on timber piles and/or HP steel piles. The bridge length is 161'-0" bk. to bk. abutments. The northbound width varies from 65'-0 $\frac{1}{2}$ " to 65'-10 $\frac{3}{8}$ " o. to o. of deck. The southbound deck width varies from 70'-1 $\frac{1}{2}$ " to 70'-1 $\frac{3}{8}$ " o. to o. of deck.

Traffic will be maintained utilizing staged construction.
No salvage.

CL I-55 CURVE DATA

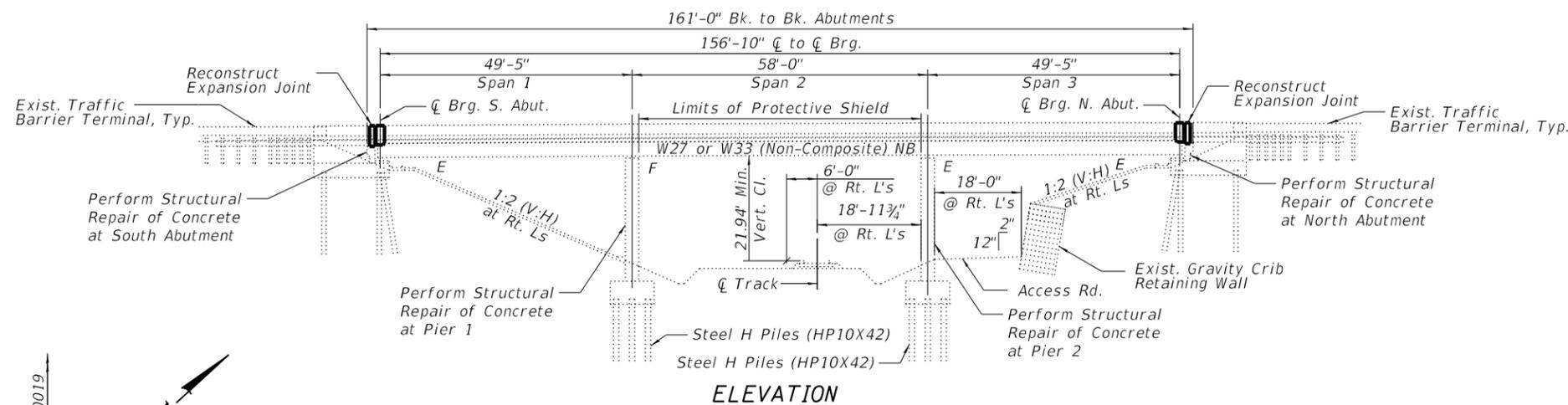
Exist. Curve I-55_EX_18
 $\Delta = 43^\circ 22' 01" (LT)$
 $T = 1,518.89'$
 $L = 2,891.35'$
 $E = 290.89'$
 $R = 3,820.00'$
 $SE = 2.0\%$
 $PC = 426+91.04$
 $PT = 455+82.39$
 $PI = 442+09.93$



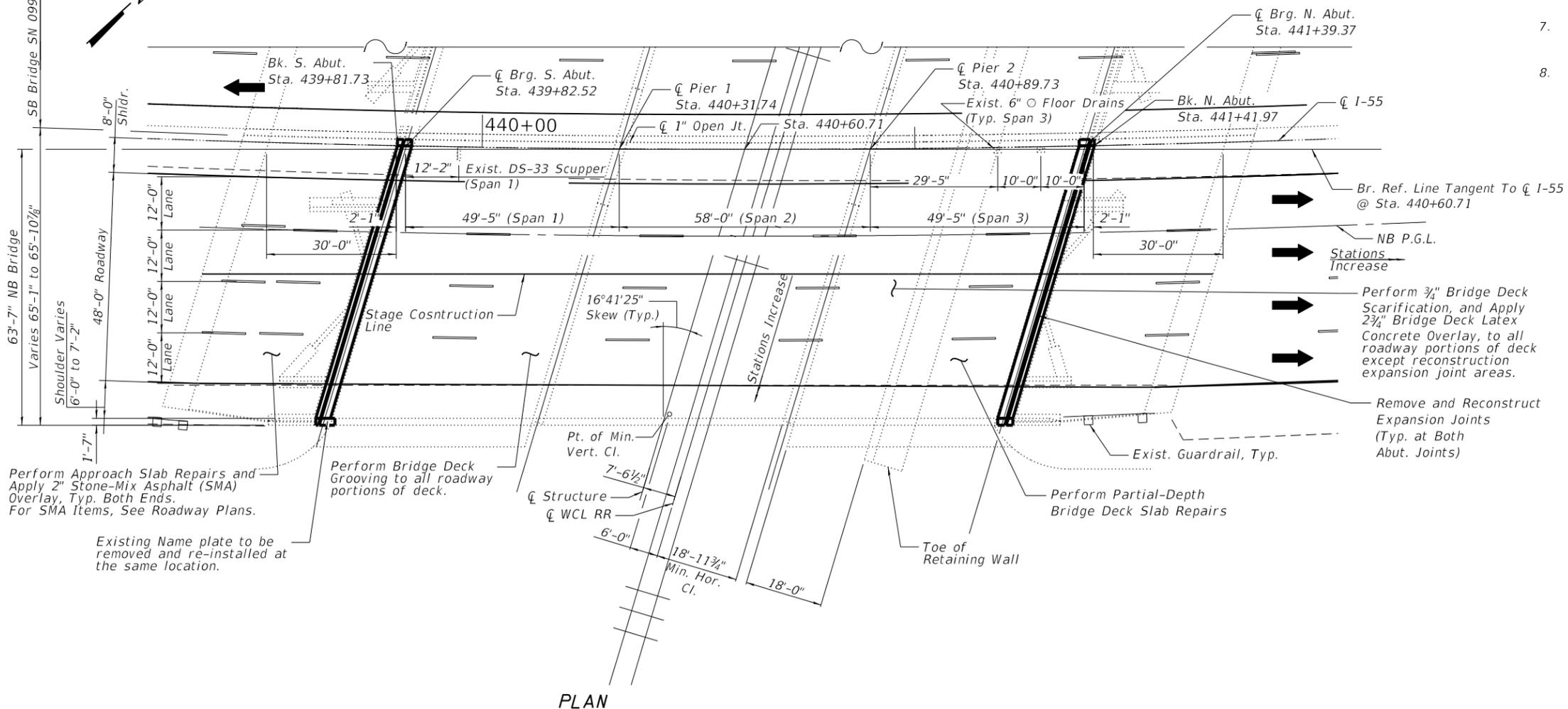
OFFSET SKETCH

SCOPE OF WORK

1. Scarify $\frac{3}{4}$ " from the bridge deck slab.
2. Perform deck slab and Approach slab repairs and adjust scuppers as required.
3. Remove and Reconstruct Expansion Joints at North and South Abutments and install new preformed joint strip seals.
4. Apply a 2 $\frac{3}{4}$ " Bridge Deck Latex Concrete Overlay on Bridge Deck and 2" Stone-Matrix Asphalt (SMA) Overlay on the Approach Slabs.
5. Perform Bridge Deck Grooving.
6. Apply Protective Coat to the top and inside faces of parapets, reconstructed transverse expansion joints and to the surface of the new overlay.
7. Perform Structural Concrete repairs to the Abutments and Piers as noted in the plans.
8. Existing name plate to be removed, cleaned and re-installed at the same location. Cost included with Concrete Removal.



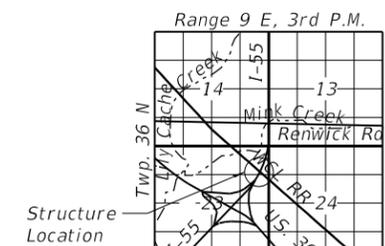
ELEVATION



PLAN



DATE SIGNED:
 EXP. DATE: 11/30/2022
 SHEETS S2-01 THRU S2-14



LOCATION SKETCH

GENERAL PLAN AND ELEVATION
NB I-55 OVER WCL RAILROAD
F.A.I. 55 SEC 2020-253-BR&PP
WILL COUNTY
STATION: 440+60.71
STRUCTURE NO. 099-0018

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

S.N. 099-0018 NB I-55 OVER WCL RAILROAD

SHEET S2-01 OF S2-14 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	74
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

GENERAL NOTES

1. Reinforcement bars designated (E) shall be epoxy coated.
2. Prior to pouring the new concrete deck for Expansion Joints Reconstruction and Deck slab repairs, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.
3. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
4. Joint openings shall be adjusted according to Article 520.04 of the Standard Specification when the deck is poured at an ambient temperature other than 50° F.
5. Bars noted thus, 3x2-#5, indicates 3 lines of #5 bars with 2 lengths of bar per line.
6. All exposed concrete edges shall have a 3/4" chamfer, except where shown otherwise.
7. The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.
8. During repair operations, the Contractor shall locate and protect all utilities in the vicinity of the work including, but not limited to, fiber optic and/or electrical conduits, conduits under the bridge deck, under-deck lighting, traffic signals or signs attached to the structure. This work shall be performed to the satisfaction of the Engineer and will not be paid for separately, but shall be included with the contract. It shall be the Contractor's responsibility to restore and replace any damaged utilities or facilities to the satisfaction of the Engineer at no cost to the Department.
9. Expansion joints shall be fabricated to conform to the existing cross slopes of the bridge.
10. Protective Coat shall be applied to the top and inside face of parapets, reconstructed transverse Expansion Joints and to the surface of the new overlay.
11. Existing reinforcement extended into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal operations shall be replaced using an approved bar splicer or anchorage system. Cost included with Concrete Removal.
12. The Contractor is responsible to protect the existing conduit embedded in the parapet during concrete removal and construction. Any damage to the existing conduit shall be repaired by the Contractor at no additional cost to the Department.

INDEX OF SHEETS

- S2-01. General Plan and Elevation
- S2-02. General Notes, Total Bill of Materials, & Index of Sheets
- S2-03-S2-04. Construction Staging Details
- S2-05. Temporary Concrete Barrier for Stage Construction
- S2-06. Bridge Deck Repairs
- S2-07. Drainage Scuppers and Floor Drains Details
- S2-08. Parapet Repairs
- S2-09. Abut. Expansion Joint Removal and Construction
- S2-10. Expansion Joint Details
- S2-11. Preformed Joint Strip Seal
- S2-12. Pier 1 and Pier 2 Repairs
- S2-13. North and South Abutment Repairs
- S2-14. Bar Splicer Assembly and Mechanical Splicer Details

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Concrete Removal	Cu Yd	20.3		20.3
Protective Shield	Sq Yd	423		423
Concrete Superstructure	Cu Yd	20.3		20.3
Bridge Deck Grooving	Sq Yd	1140		1140
Protective Coat	Sq Yd	1525		1525
Reinforcement Bars, Epoxy Coated	Pound	3490		3490
Bar Splicers	Each	28		28
Preformed Joint Strip Seal	Foot	137		137
Concrete Sealer	Sq Ft		407	407
Epoxy Crack Injection	Foot		8	8
Approach Slab Repair (Partial Depth)	Sq Yd	2		2
Bridge Deck Latex Concrete Overlay, 2 3/4"	Sq Yd	1144		1144
Bridge Deck Scarification 3/4"	Sq Yd	1144		1144
Structural Repair of Concrete (Depth Equal to or less than 5")	Sq Ft	9.3	30.5	39.8

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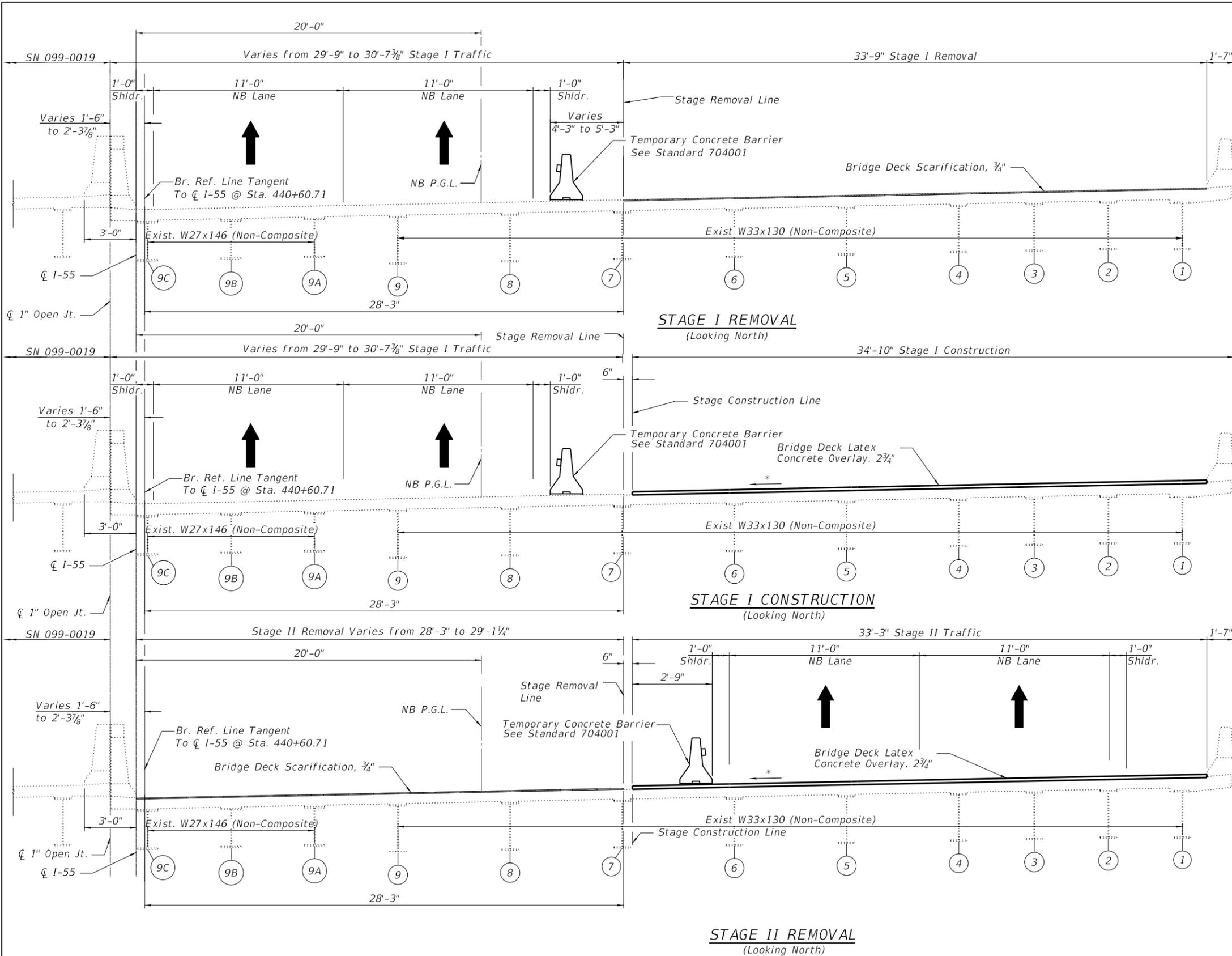
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES, TOTAL BILL OF MATERIALS, & INDEX OF SHEETS
S.N. 099-0018 NB I-55 OVER WCL RAILROAD

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	75
CONTRACT NO. 62N22				
ILLINOIS			FED. AID PROJECT	

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STAGE I REMOVAL

1. Install temporary concrete barrier as shown to locate traffic on the west side of the existing structure.
2. Scarify 3/4" from the top of deck slab.
3. Remove portions of bridge deck/approach slab adjacent to abutment joints, as shown in the plans.

STAGE I CONSTRUCTION

1. Perform full- and partial-depth deck slab repairs, and approach slab repairs, at locations as shown in the plans.
2. Install preformed joint strip seal at north and south abutments and replace associated reinforcement and concrete adjacent to the joint.
3. Perform structural repair of concrete for the abutments and piers.
4. Apply 2 3/4" bridge deck latex concrete overlay to bridge deck slab.
5. Perform bridge deck grooving for the 2 3/4" bridge deck latex concrete overlay.
6. Apply 2" Stone-Matrix Asphalt (SMA) Overlay to approach slabs.
7. Apply protective coat to the top of reconstructed transverse joint areas, the surface of the new overlay and the top and inside faces of parapets.

STAGE II REMOVAL

1. Install temporary concrete barrier as shown to locate traffic on the east side of the existing structure.
2. Scarify 3/4" from the top of deck slab.
3. Remove portions of bridge deck/approach slab adjacent to abutment joints, as shown in the plans.

*Match existing deck surface profile



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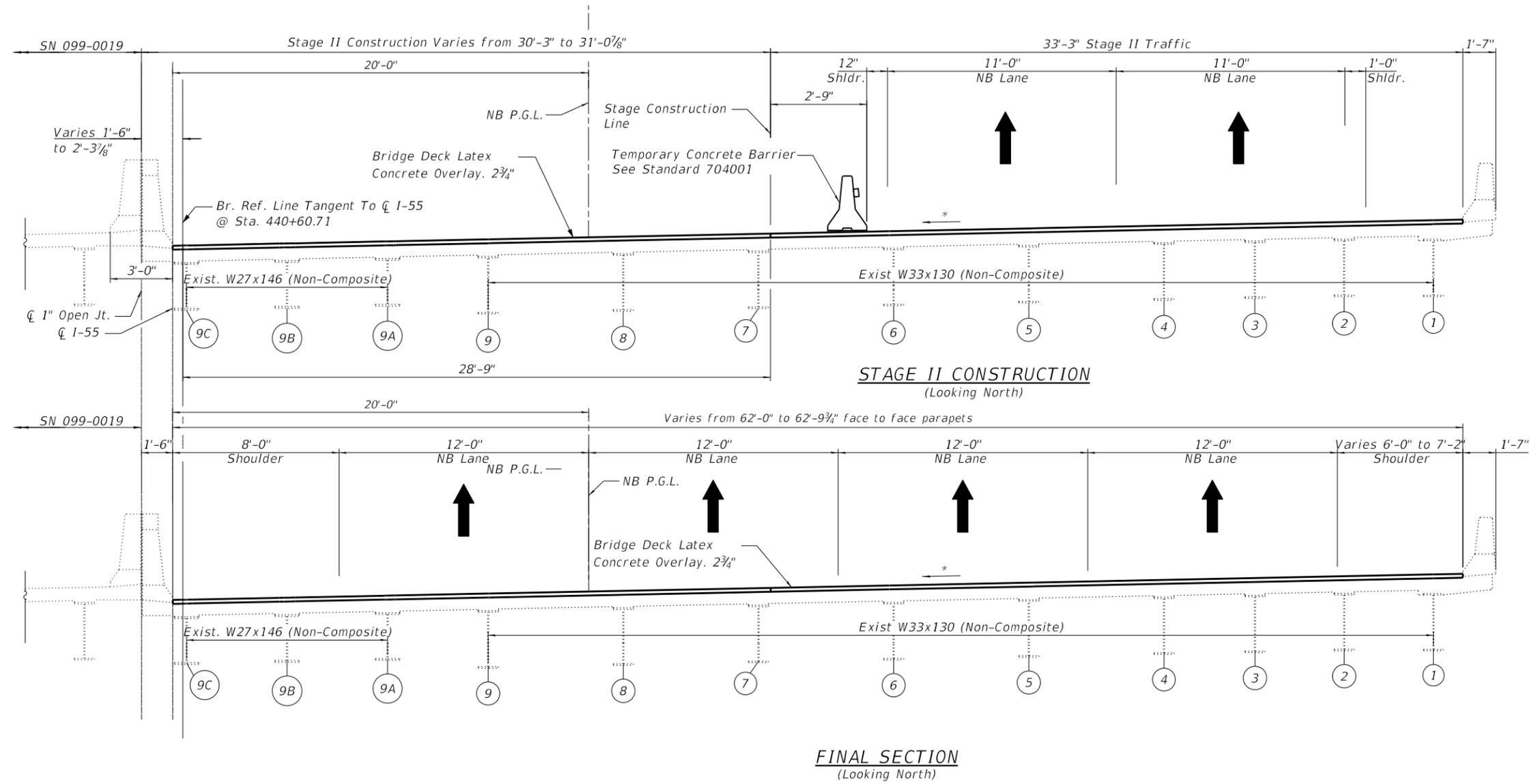
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CONSTRUCTION STAGING DETAILS - I
 S.N. 099-0018 NB I-55 OVER WCL RAILROAD**

SHEET S2-03 OF S2-14 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	76
			CONTRACT NO. 62N22	
		ILLINOIS	FED. AID PROJECT	

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STAGE II CONSTRUCTION

1. Perform full- and partial-depth deck slab repairs, and approach slab repairs, at locations as shown in the plans.
2. Install preformed joint strip seal at north and south abutments and replace associated reinforcement and concrete adjacent to the joint.
3. Perform structural repair of concrete for the abutments and piers.
4. Apply 2 3/4" bridge deck latex concrete overlay to bridge deck slab.
5. Perform bridge deck grooving for the 2 3/4" bridge deck latex concrete overlay.
6. Apply 2" Stone-Matrix Asphalt (SMA) Overlay to approach slabs.
7. Apply protective coat to the top of reconstructed transverse joint areas, the surface of the new overlay and the top and inside faces of parapets.
8. Apply permanent pavement markings on top of deck and approach slabs.

*Match existing deck surface profile



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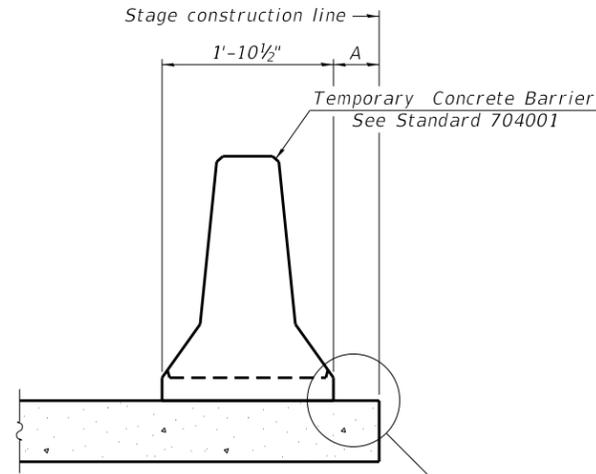
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CONSTRUCTION STAGING DETAILS - II
 S.N. 099-0018 NB I-55 OVER WCL RAILROAD**

SHEET S2-04 OF S2-14 SHEETS

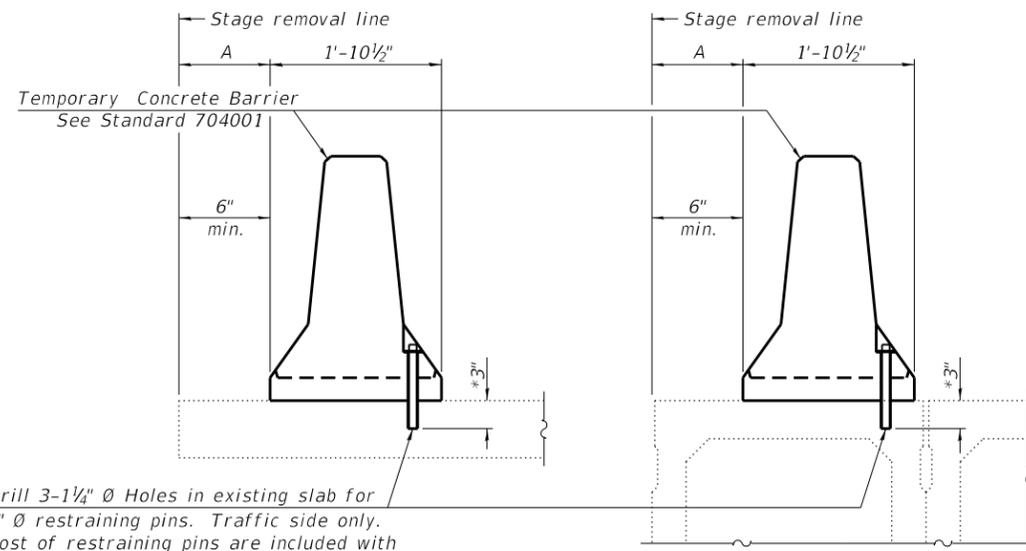
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	77
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

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When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM

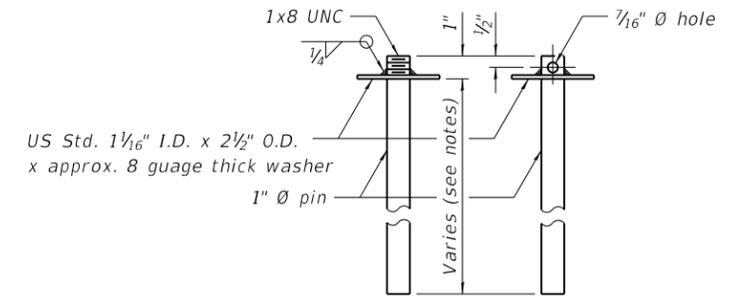


Drill 3-1/4" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

EXISTING SLAB

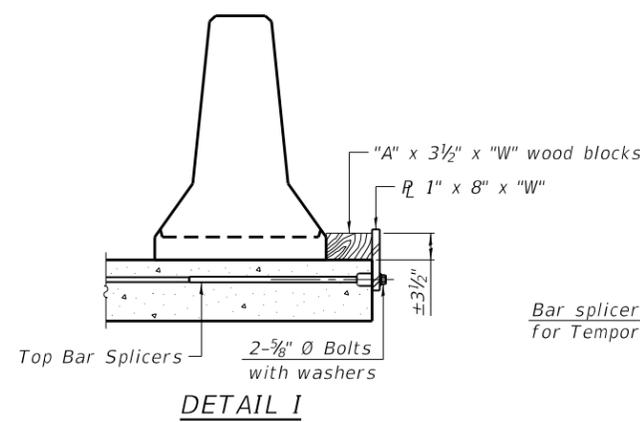
* When hot-mix asphalt wearing surface is present, embedment shall be 3" plus the wearing surface depth.

EXISTING DECK BEAM

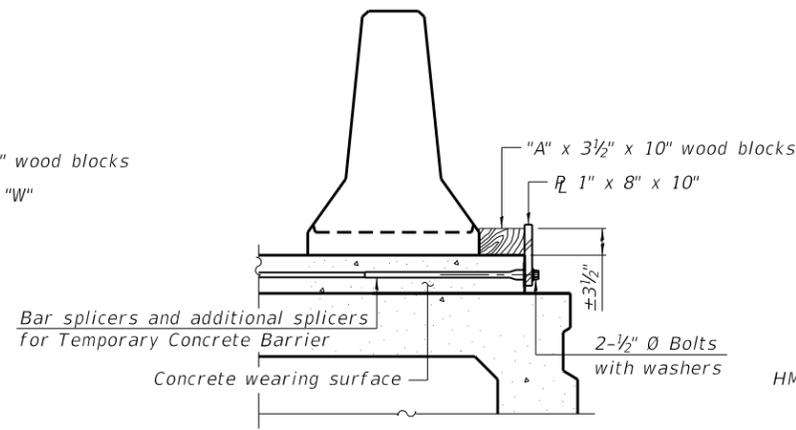


RESTRAINING PIN

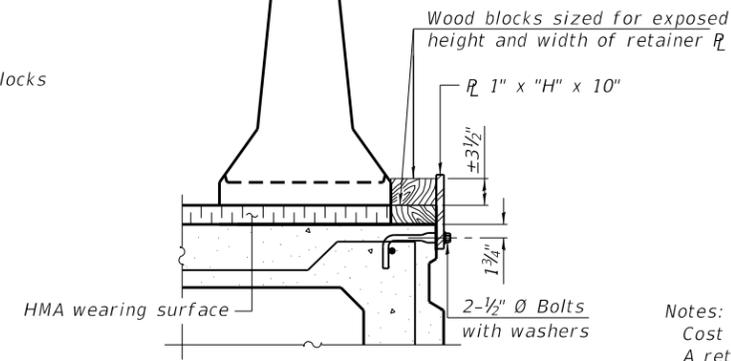
SECTIONS THRU SLAB OR DECK BEAM



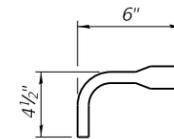
DETAIL I



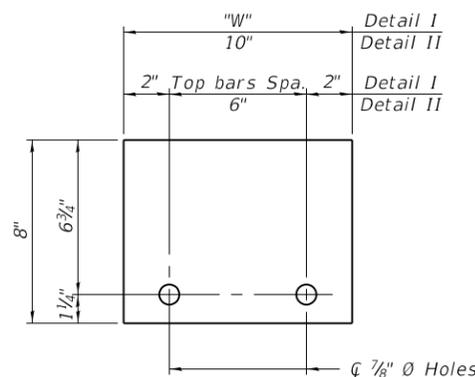
DETAIL II



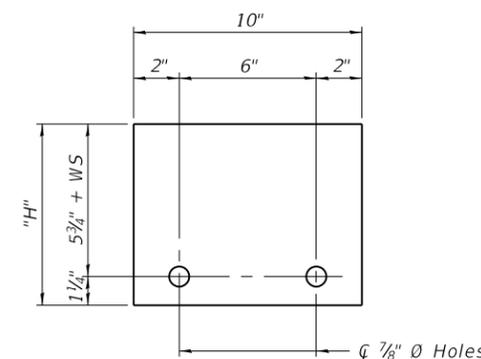
DETAIL III



BAR SPLICER FOR #4 BAR - DETAIL III



STEEL RETAINER 1" x 8" x "W"
(Detail I and II)



STEEL RETAINER 1" x "H" x 10"
(Detail III)

Notes:
 Cost of retainer assembly is included with Temporary Concrete Barrier.
 A retainer assembly shall be located at the approximate \bar{C} of each temporary concrete barrier.
 The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.
 When the 'A' dimension is less than 1 1/2', the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.

- Detail I** - Installation for a new bridge deck or bridge slab.
- Detail II** - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.
- Detail III** - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

R-27 8-11-2017



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DEPARTMENT OF TRANSPORTATION**

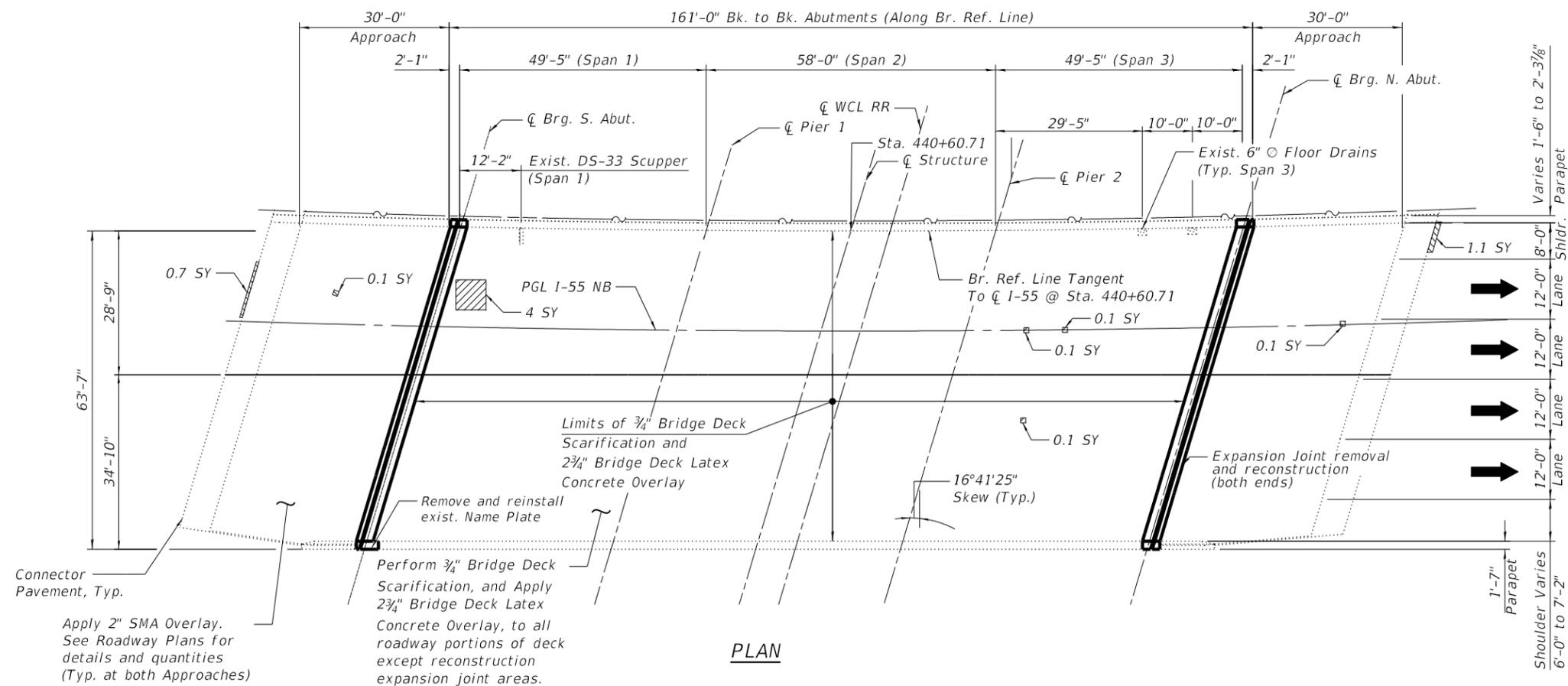
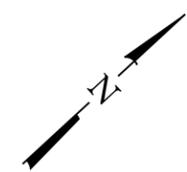
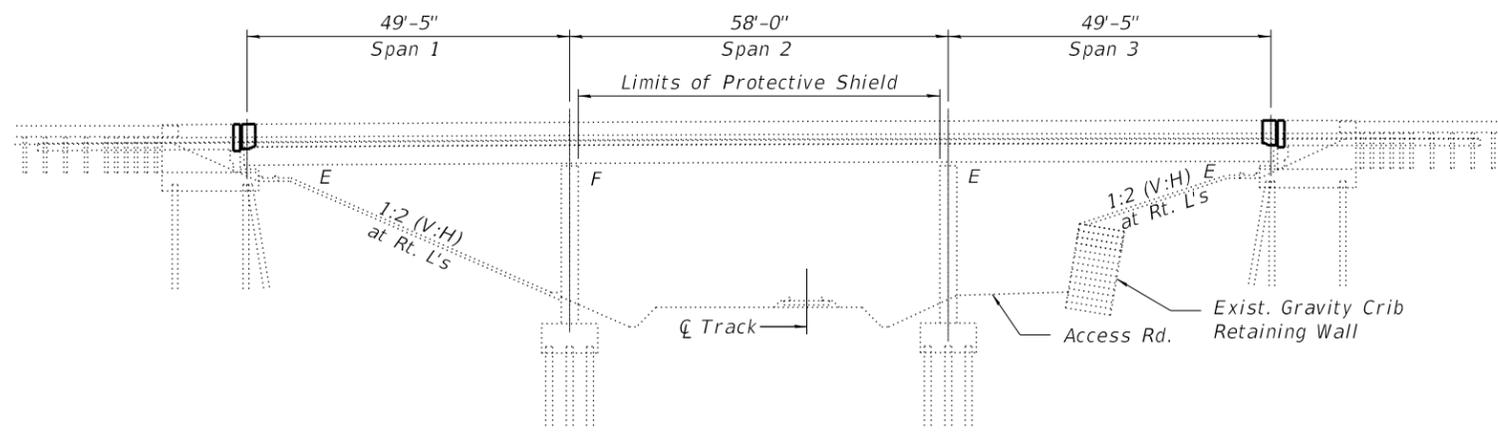
**TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION
SN 099-0018 NB I-55 OVER WCL RAILROAD**

SHEET S2-05 OF S2-14 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	78
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

NOTES:

- Deck and approach slab repair areas are estimated based on visual inspection and will be paid for as specified in the Special Provision. Actual repair areas and locations shall be determined by the Engineer and shown on As-built plans. Engineer shall sound deck after deck scarification.
- Protective Coat shall be applied to the bridge overlay and front and top faces of the new and existing parapets.
- All dimensions are perpendicular to CL I-55 Bridge Deck.
- Install Protective Shield over WCL Railroad.
- For bridge deck final cross section, see Sheet S1-04.
- For North and South Transverse Joint Removal and Reconstruction, see Sheets S1-09 and S1-10.
- Perform Bridge Deck Grooving for the Bridge Deck Latex Concrete Overlay and the roadway portions of the Reconstructed Transverse Joints.
- Any reinforcement bars that are damaged during concrete removal operations shall be repaired or replaced using an approved bar splicer or anchorage system. Cost included with Concrete Removal.
- The Contractor shall exercise extreme caution during concrete removal to avoid damaging the steel beams and diaphragms to remain. Any damage to the existing steel beams and/or diaphragms to remain caused by the Contractor in the performance of his/her work shall be repaired by the Contractor, to the satisfaction of the Engineer at no cost to the Department.
- See Sheet S2-07 of S2-14 for Drainage Scupper and Floor Drain Details.



LEGEND:

- Deck Slab Repair (Partial)*
- Approach Slab Repair (Full Depth)
- SY Square Yards

*Areas of Deck Slab Repair (Partial) are provided for information only and shall be included in the cost of Bridge Deck Latex Concrete Overlay, 2 3/4"

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Approach Slab Repair (Partial Depth)	Sq. Yd.	2
Protective Coat	Sq. Yd.	1525
Bridge Deck Grooving	Sq. Yd.	1140
Bridge Deck Latex Concrete Overlay, 2 3/4"	Sq. Yd.	1144
Bridge Deck Scarification, 3/4"	Sq. Yd.	1144
Protective Shield	Sq. Yd.	423

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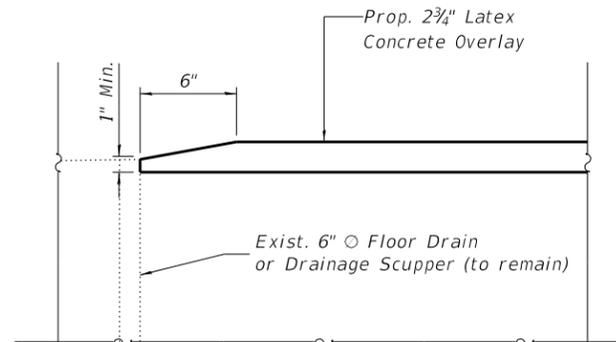
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BRIDGE DECK REPAIRS
S.N. 099-0018 NB I-55 OVER WCL RAILROAD**

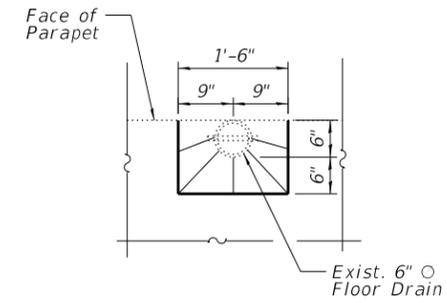
SHEET S2-06 OF S2-14 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62N22				
ILLINOIS		FED. AID PROJECT		

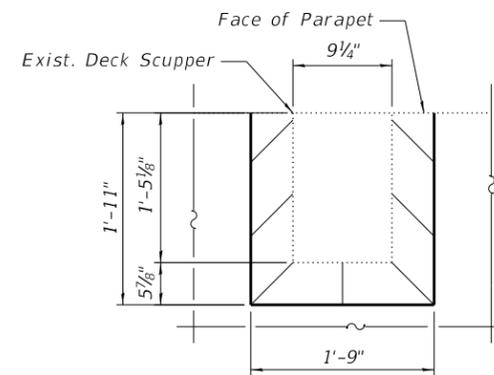
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FLOOR DRAIN/ DRAINAGE SCUPPER DETAIL



6" Ø FLOOR DRAIN TOP PLAN



TOP PLAN AT DRAINAGE SCUPPER



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PLOT DATE =	CHECKED - JMT	REVISED -

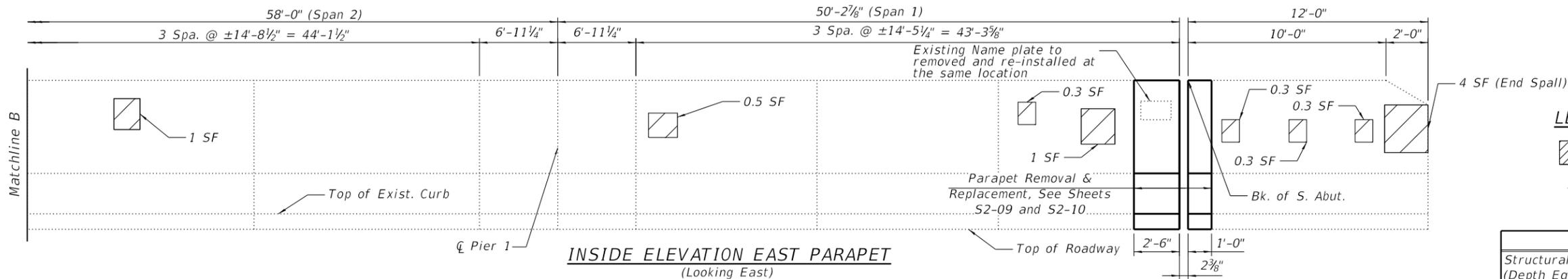
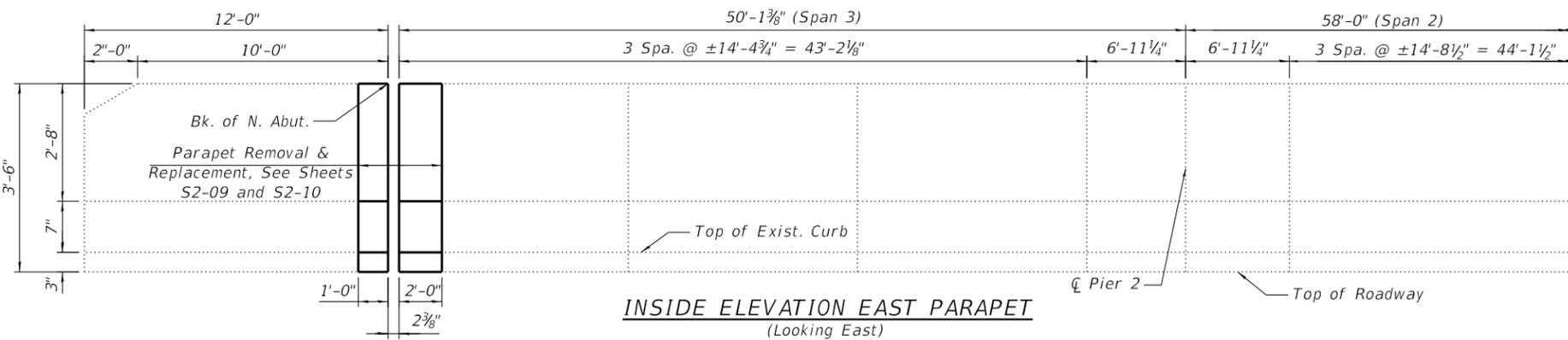
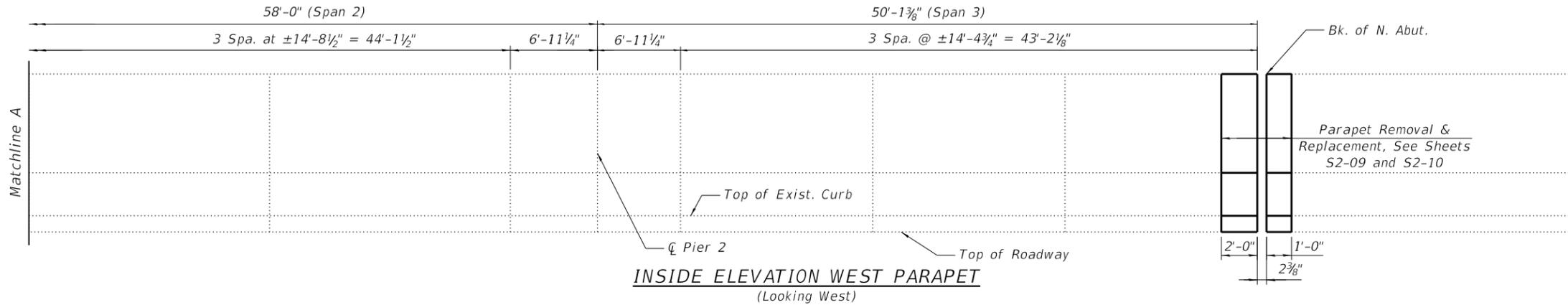
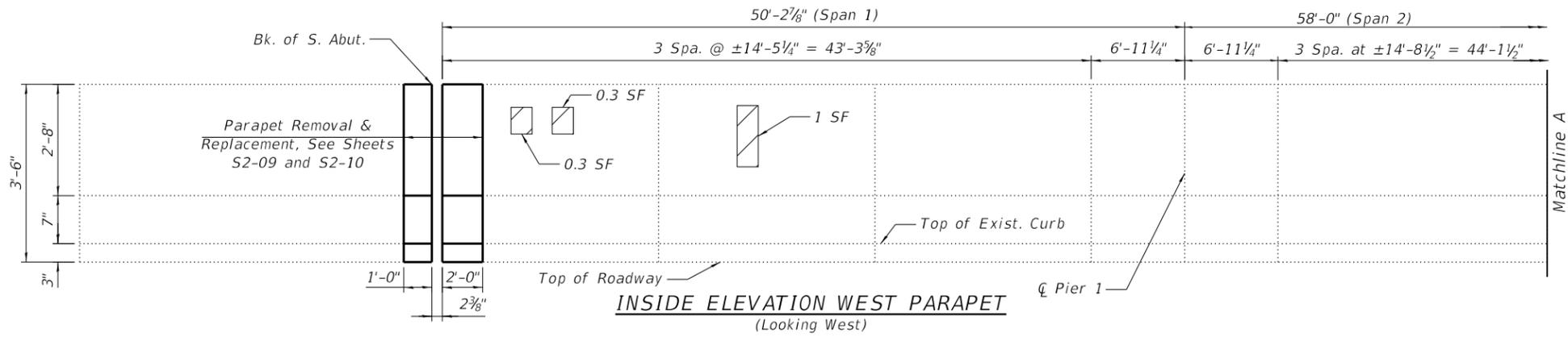
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**DRAINAGE SCUPPERS AND FLOOR DRAINS DETAILS
 S.N. 099-0018 NB I-55 OVER WCL RAILROAD**

SHEET S2-07 OF S2-14 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	80
				CONTRACT NO. 62N22
		ILLINOIS	FED. AID PROJECT	

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LEGEND

Structural Repair of Concrete (Depth Equal to or Less than 5 Inches)

SF Square Foot

BILL OF MATERIAL

Item	Unit	Total
Structural Repair of Concrete (Depth Equal to or less than 5 Inches)	Sq Ft	9.3



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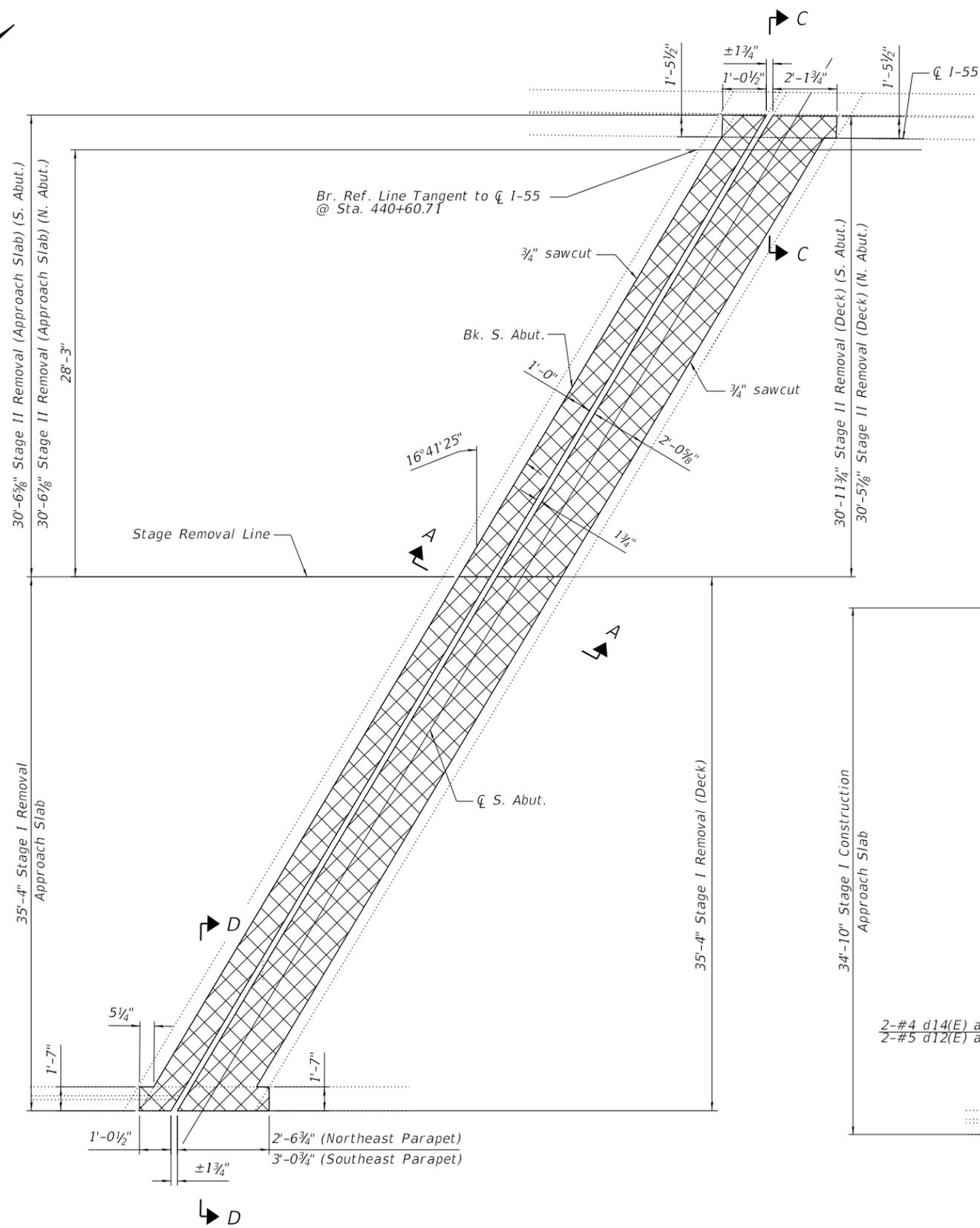
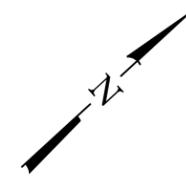
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**PARAPET REPAIRS
 S.N. 099-0018 NB I-55 OVER WCL RAILROAD**

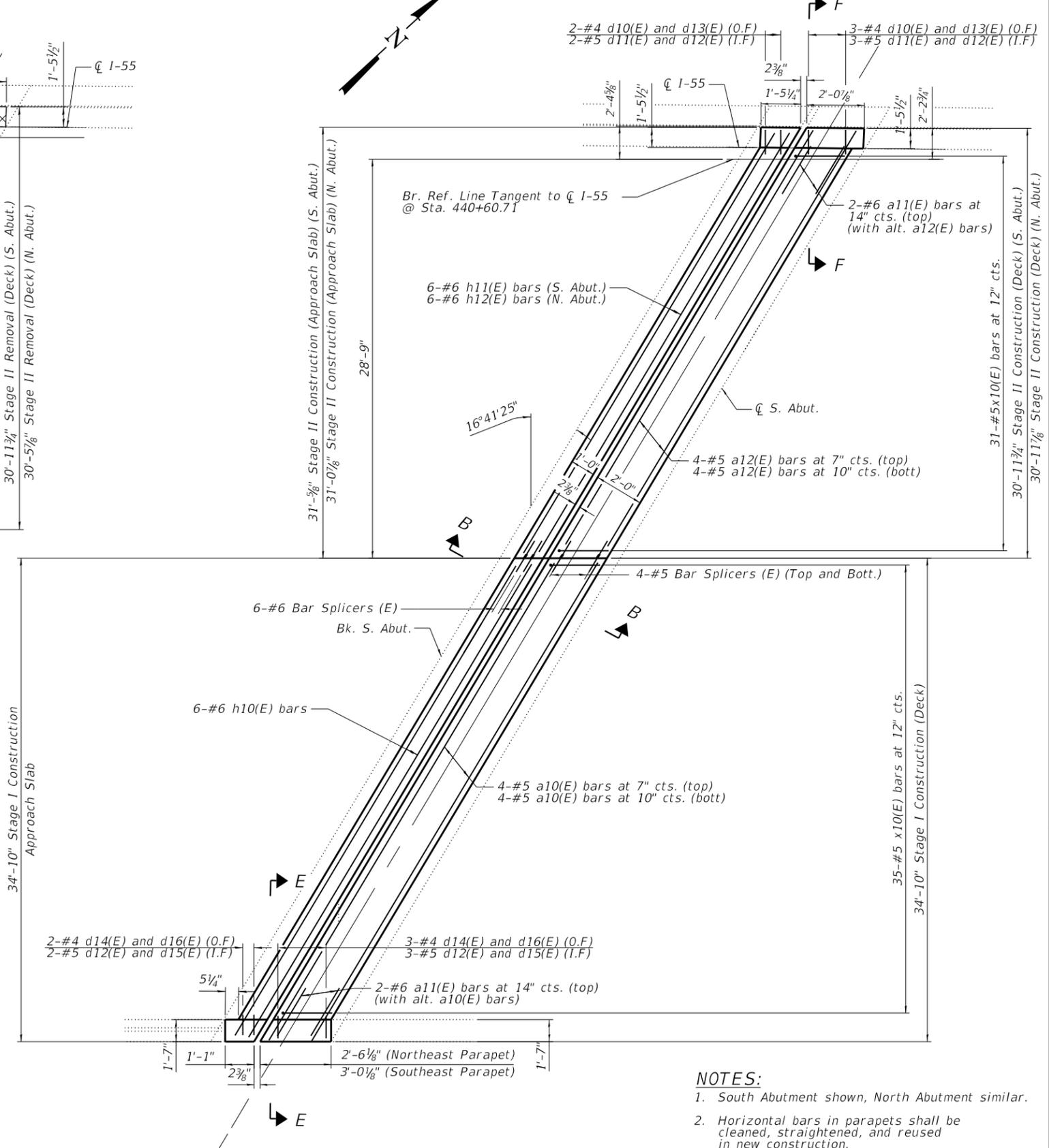
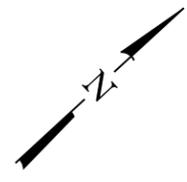
SHEET S2-08 OF S2-14 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	81
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

MODEL: Default
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S. ABUT. JOINT REMOVAL PLAN



S. ABUT. JOINT RECONSTRUCTION PLAN

NOTES:

1. South Abutment shown, North Abutment similar.
2. Horizontal bars in parapets shall be cleaned, straightened, and reused in new construction.
3. Hatched areas indicate Concrete Removal.



USER NAME =	DESIGNED - JMT	REVISED -
CHECKED - SPS	REVISED -	
PLOT SCALE =	DRAWN - IH	REVISED -
PLOT DATE =	CHECKED - JMT	REVISED -

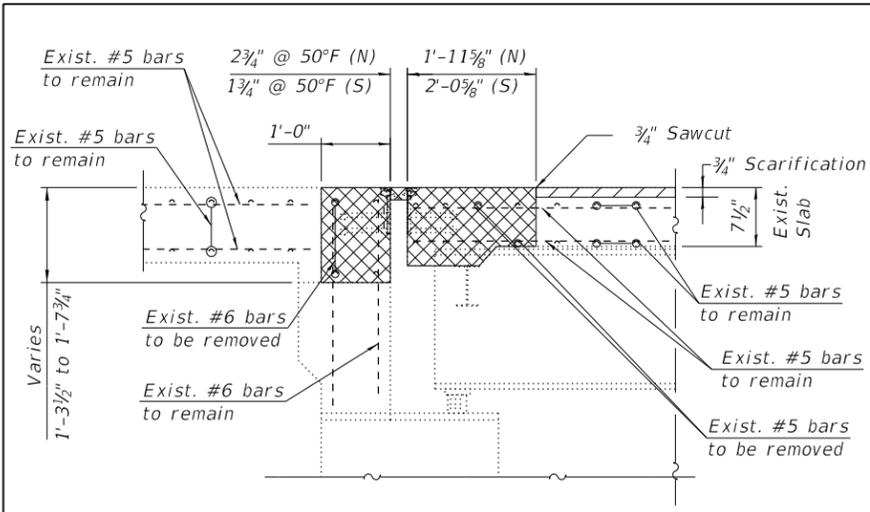
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ABUT. EXPANSION JOINT REMOVAL AND CONSTRUCTION
 S.N. 099-0018 NB I-55 OVER WCL RAILROAD**

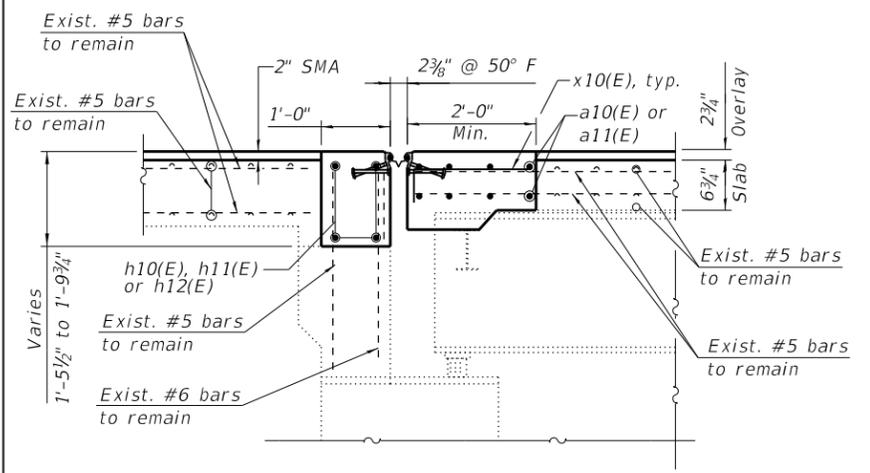
SHEET S2-09 OF S2-14 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

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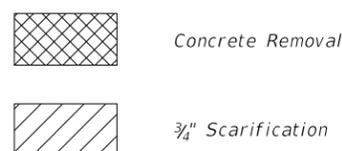


SECTION A-A



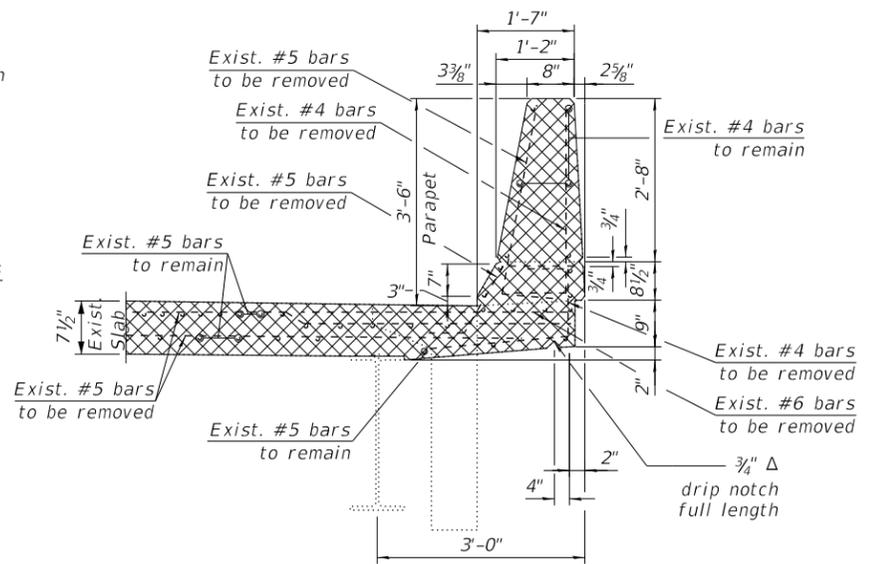
SECTION B-B

LEGEND:

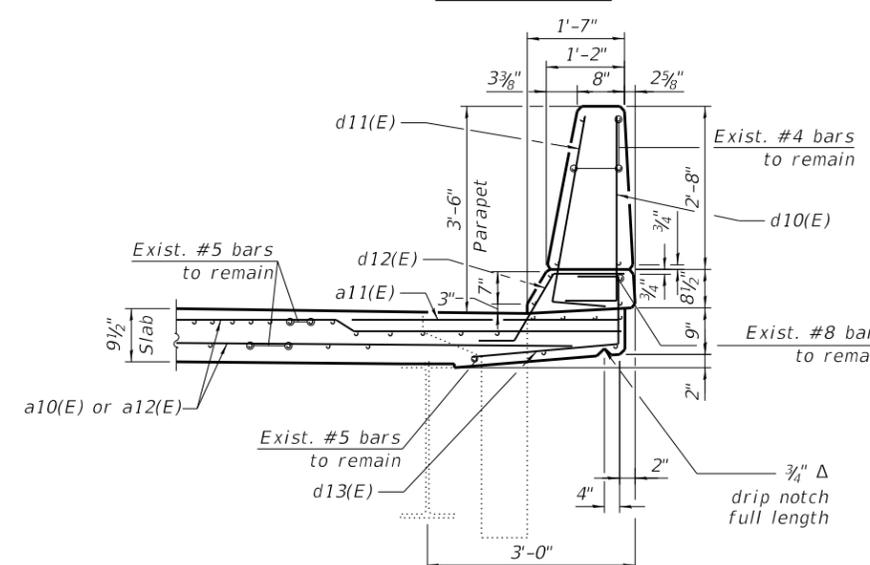


NOTES:

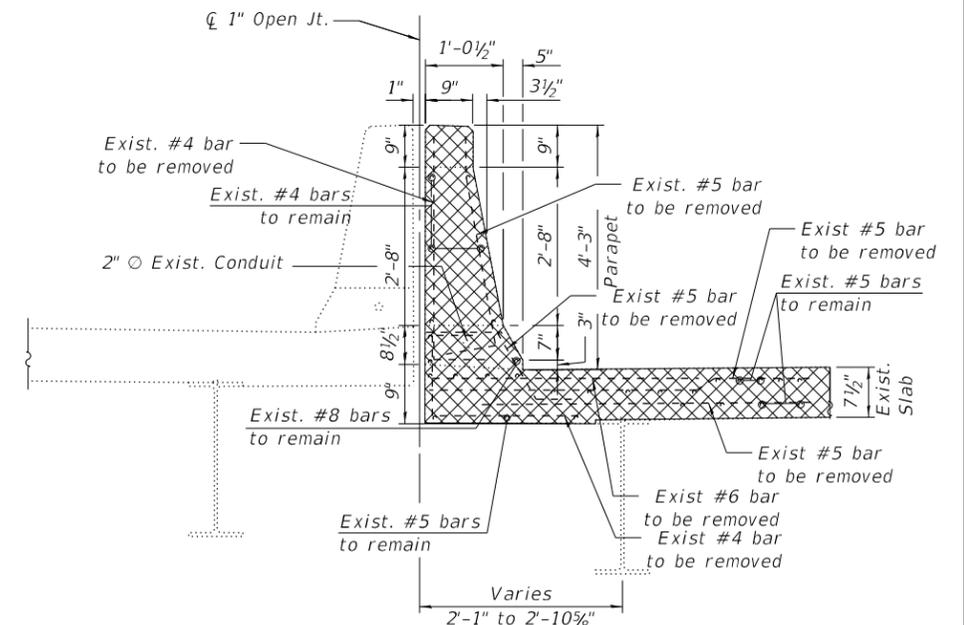
- For preformed joint seal details, see sheet S2-11.
- For Bar Splicer Assembly details, see sheet S2-14.
- Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.
- Removal of Exist. Expansion Joint will not be paid for separately but will be included in the cost of Concrete Removal.



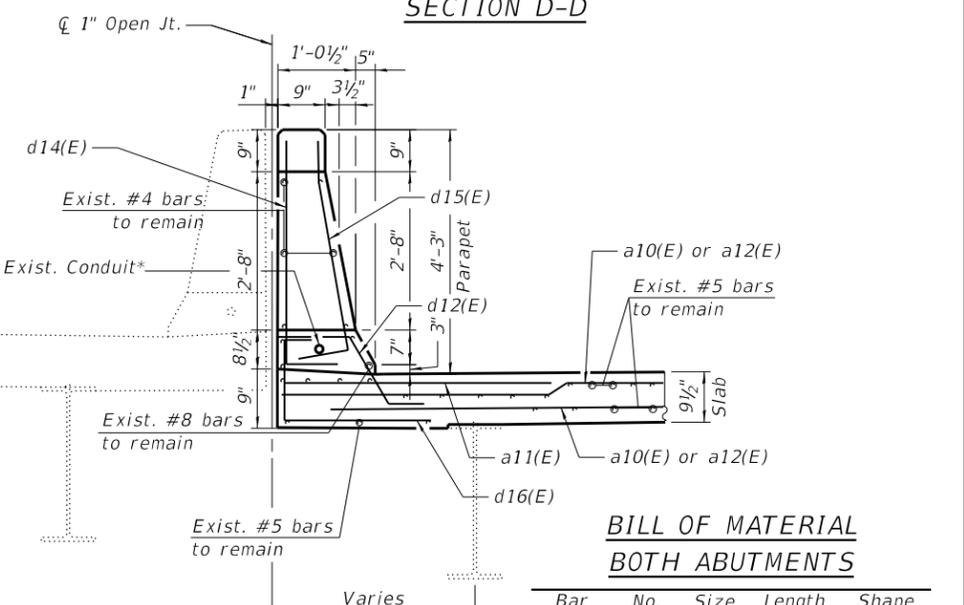
SECTION C-C



SECTION E-E



SECTION D-D

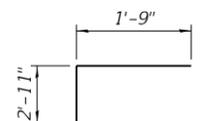


SECTION F-F

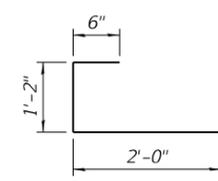
*Maintain a min. 1" cl. from all reinf. bars in parapet

BILL OF MATERIAL BOTH ABUTMENTS

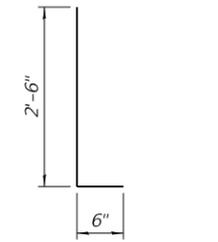
Bar	No.	Size	Length	Shape
a10(E)	16	#5	35'-10"	—
a11(E)	8	#5	5'-4"	—
a12(E)	16	#6	31'-10"	—
d10(E)	10	#4	3'-0"	┌┐
d11(E)	10	#5	3'-0"	┌┐
d12(E)	20	#5	2'-5"	┌┐
d13(E)	10	#4	3'-4"	┌┐
d14(E)	10	#4	3'-8"	┌┐
d15(E)	10	#5	3'-8"	┌┐
d16(E)	10	#4	3'-9"	┌┐
h10(E)	12	#6	36'-0"	—
h11(E)	6	#6	32'-0"	—
h12(E)	6	#6	32'-1"	—
x10(E)	132	#5	4'-8"	┌┐
Concrete Removal			Cu. Yd.	20.3
Reinforcement Bars, Epoxy Coated			Pound	3490
Concrete Superstructure			Cu. Yd.	20.3



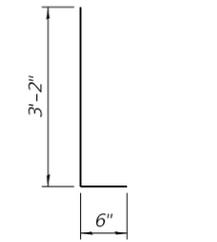
BAR x10(E)



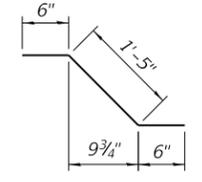
BAR d16(E)



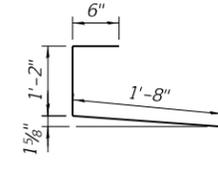
BAR d10(E) & d11(E)



BAR d14(E) & d15(E)



BAR d12(E)



BAR d13(E)

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

EXPANSION JOINT DETAILS
 S.N. 099-0018 NB I-55 OVER WCL RAILROAD

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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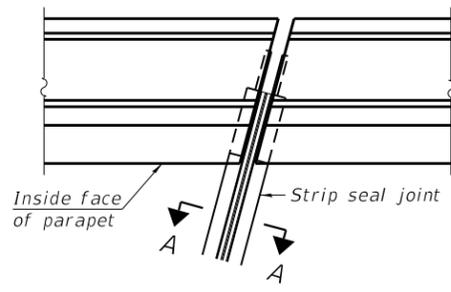
CONTRACT NO. 62N22

SHEET S2-10 OF S2-14 SHEETS

ILLINOIS FED. AID PROJECT

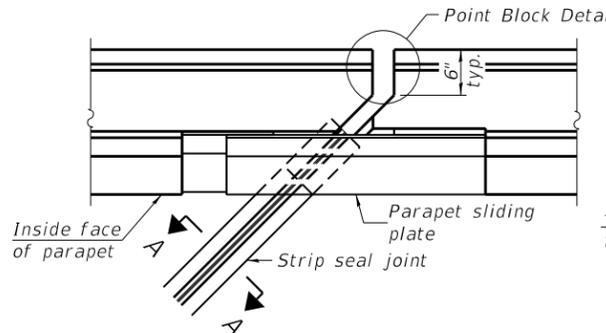


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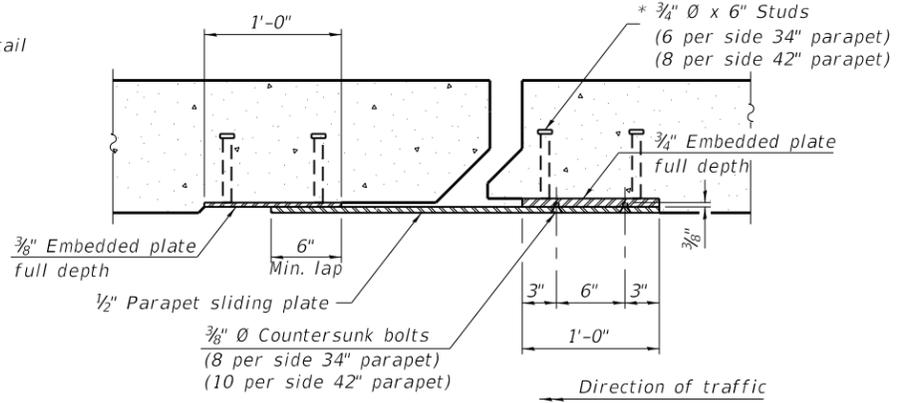


FOR SKEWS $\leq 30^\circ$

PLAN AT PARAPET

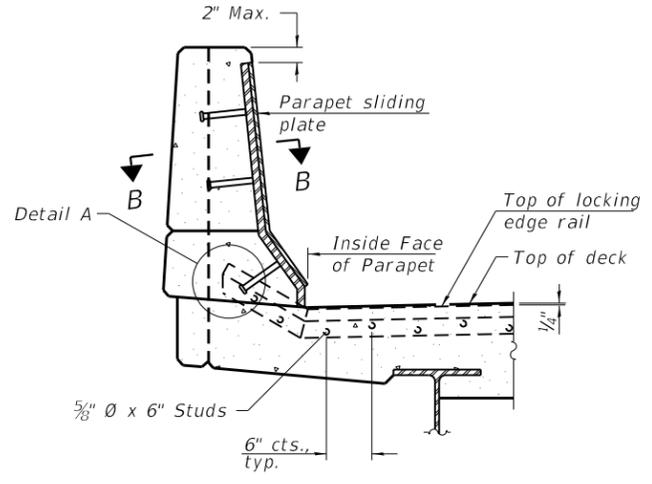


FOR SKEWS $> 30^\circ$



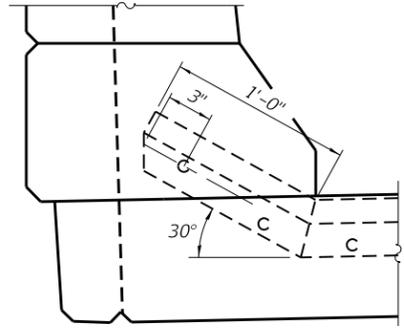
SECTION B-B

Notes:
 The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.
 The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the 4 1/2" maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.
 The manufacturer's recommended installation methods shall be followed.
 All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.
 The Maximum space between locking edge rail segments shall be 3/16" and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.
 Cost of parapet sliding plates, embedded plates, and anchorage studs included with Preformed Joint Strip Seal.
 34" F-shape barrier shown, 42" F-shape similar as noted.
 The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required. One exception to this would be the strip seal joint at the end of the precast bridge approach slab. For these cases the pavement connector length shall be adjusted, not the length of the bridge approach slab.

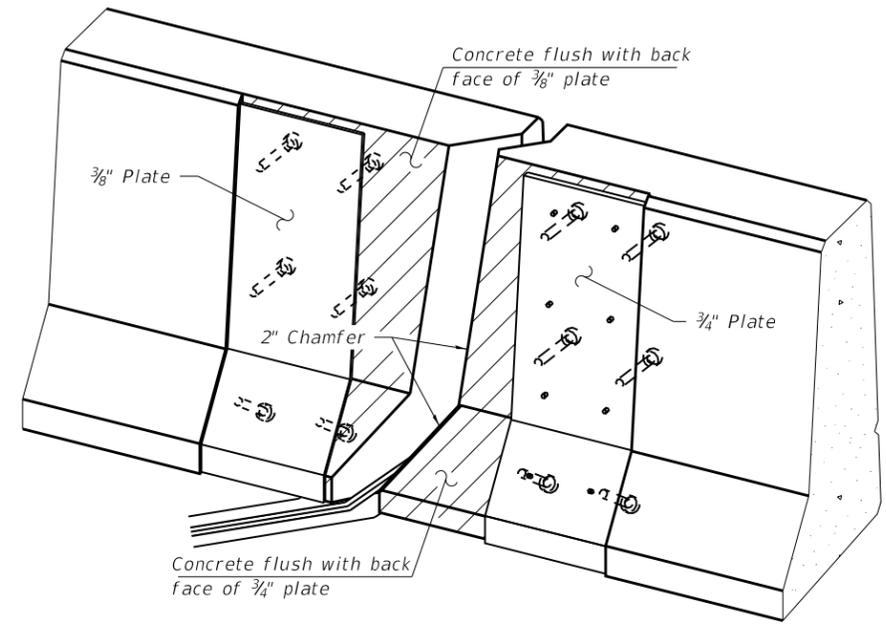


ELEVATION AT PARAPET

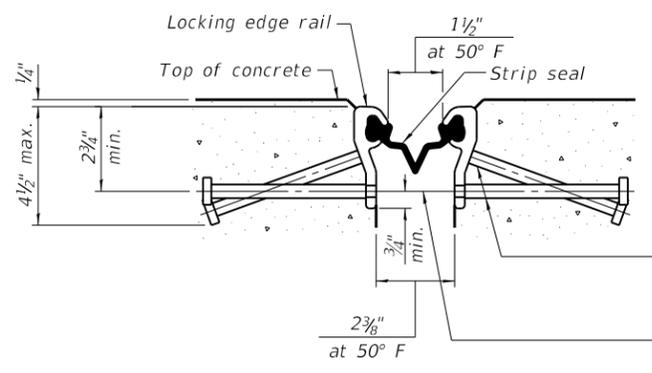
(Skews $> 30^\circ$ shown. Skews $\leq 30^\circ$ similar except as shown in plan view.)



DETAIL A



TRIMETRIC VIEW
 (Showing embedded plates only)

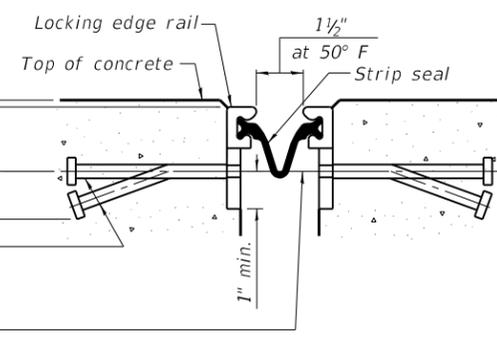


SHOWING ROLLED RAIL JOINT

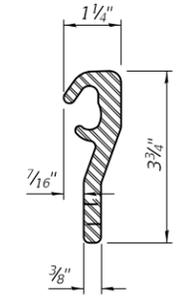
* 5/8" ϕ x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)
 3/8" ϕ threaded rods in 7/16" ϕ holes at ± 4 -0" cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

SECTION A-A

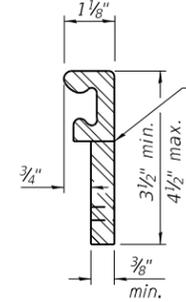
* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.



SHOWING WELDED RAIL JOINT



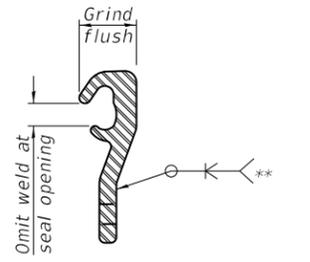
ROLLED (EXTRUDED) RAIL



WELDED RAIL

LOCKING EDGE RAILS

** Back gouge not required if complete joint penetration is verified by mock-up.



LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	137

EJ-SS

8-11-17



USER NAME =	DESIGNED - JMT	REVISED -
PLOT SCALE =	CHECKED - SPS	REVISED -
PLOT DATE =	DRAWN - IH	REVISED -
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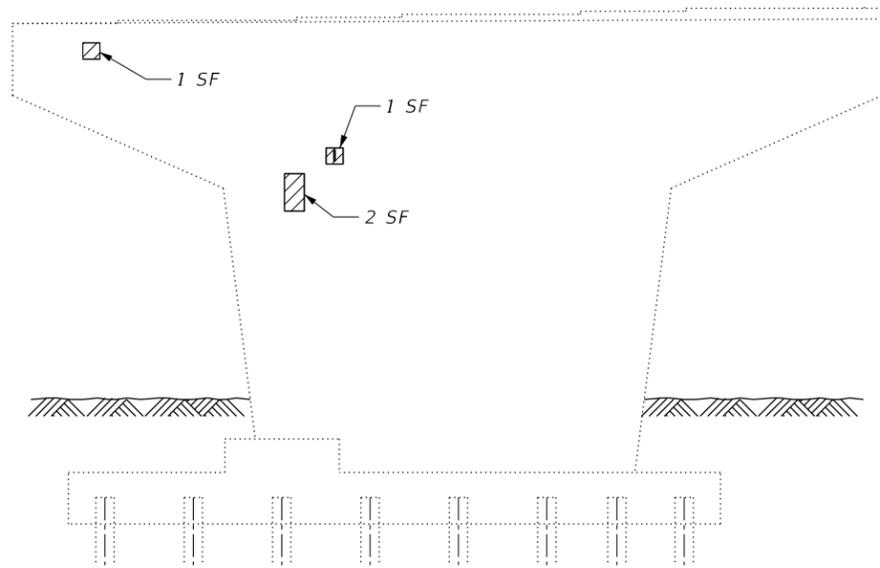
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PREFORMED JOINT STRIP SEAL
 SN 099-0018 NB I-55 OVER WCL RAILROAD

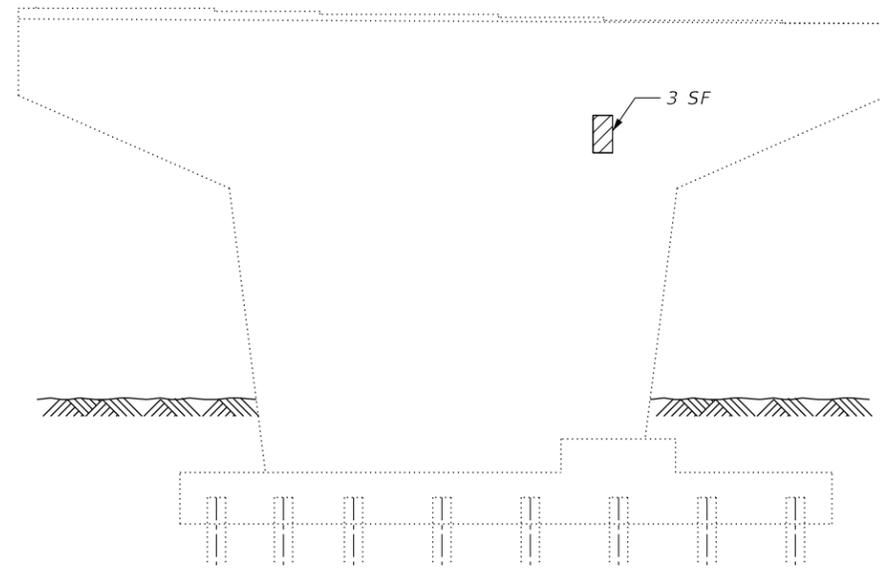
SHEET S2-11 OF S2-14 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	84
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

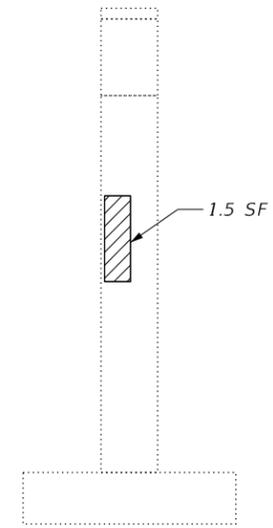
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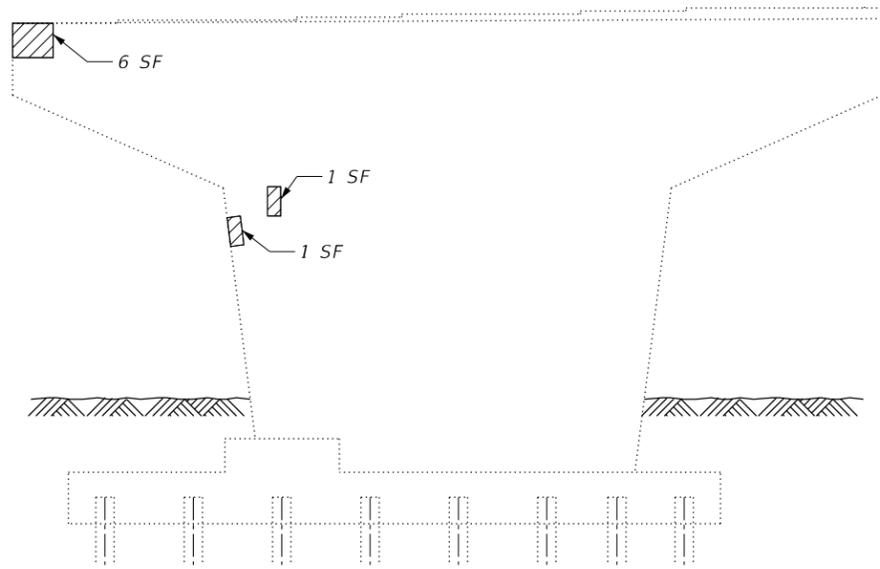
PIER 1
 (Looking North)



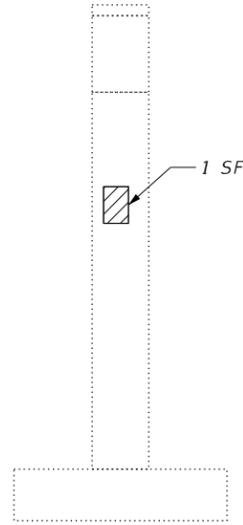
PIER 1
 (Looking South)



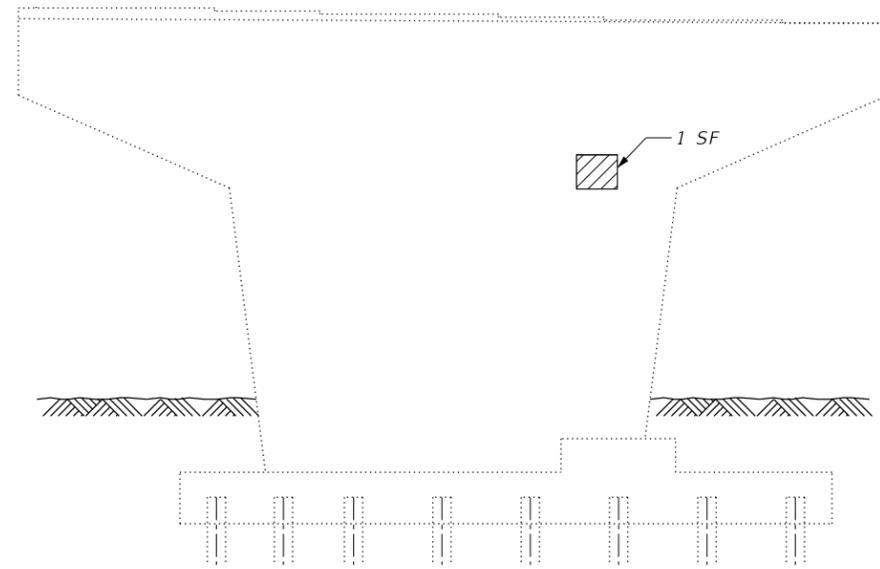
PIER 1 - END VIEW
 (Looking East)



PIER 2
 (Looking North)



PIER 2 - END VIEW
 (Looking West)



PIER 2
 (Looking South)

LEGEND:

 Structural Repair of Concrete
 (Depth Equal to or less than 5")

 Exposed Reinforcement

 Epoxy Crack Injection

SF Square Foot

NOTES:

- Quantities and limits shown are estimated for bidding purposes only. The actual areas to be repaired, and the type(s) of repairs to be used, will be determined by the Engineer in the field at the time of construction.
- Apply Concrete Sealer to new concrete surfaces of structurally repaired concrete.

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth Equal to or Less Than 5")	Sq Ft	18.5



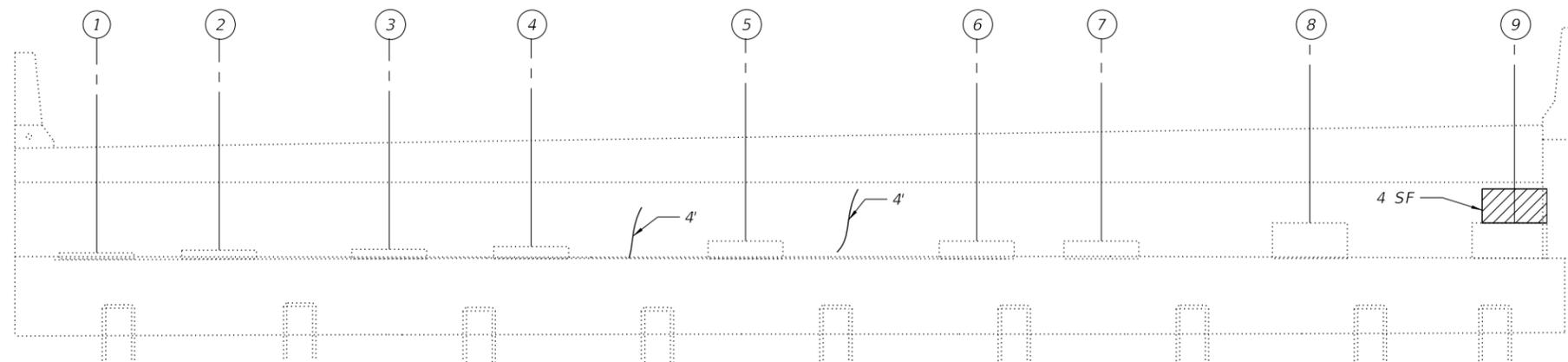
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

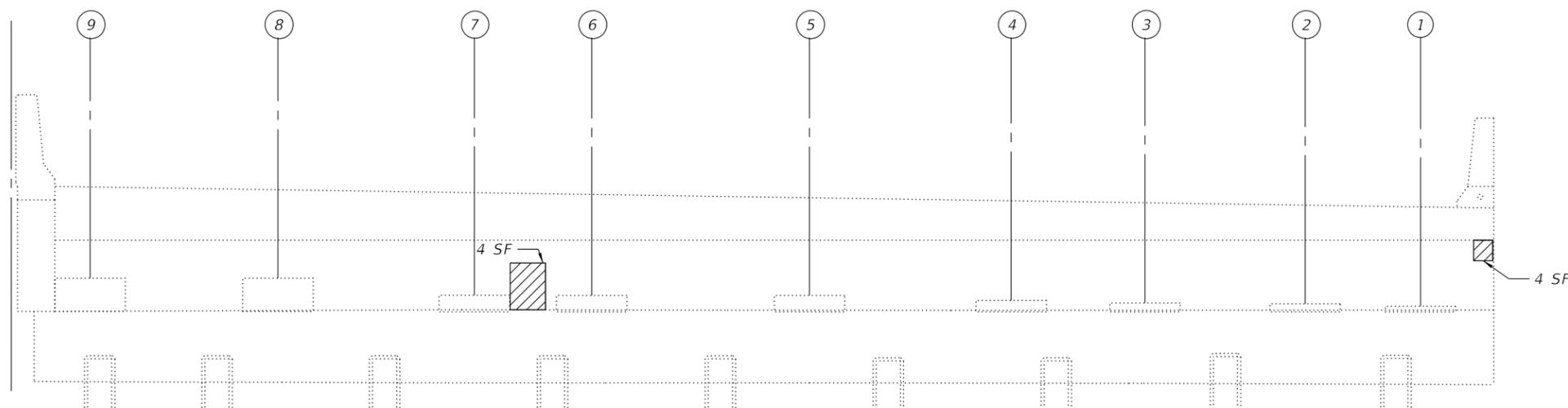
PIER 1 AND PIER 2 REPAIRS
S.N. 099-0018 NB I-55 OVER WCL RAILROAD

SHEET S2-12 OF S2-14 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	85
			CONTRACT NO. 62N22	
		ILLINOIS FED. AID PROJECT		



ELEVATION - NORTH ABUTMENT
(Looking North)



ELEVATION - SOUTH ABUTMENT
(Looking South)

LEGEND:

 Structural Repair of Concrete
(Depth Equal to or less than 5")

 Exposed Reinforcement

 Epoxy Crack Injection

SF Square Foot

NOTES:

- Quantities and limits shown are estimated for bidding purposes only. The actual areas to be repaired, and the type(s) of repairs to be used, will be determined by the Engineer in the field at the time of construction.
- Apply Concrete Sealer to new concrete surfaces of structurally repaired concrete.

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth Equal to or Less Than 5")	Sq Ft	12
Epoxy Crack Injection	Foot	8
Concrete Sealer	Sq Ft	407

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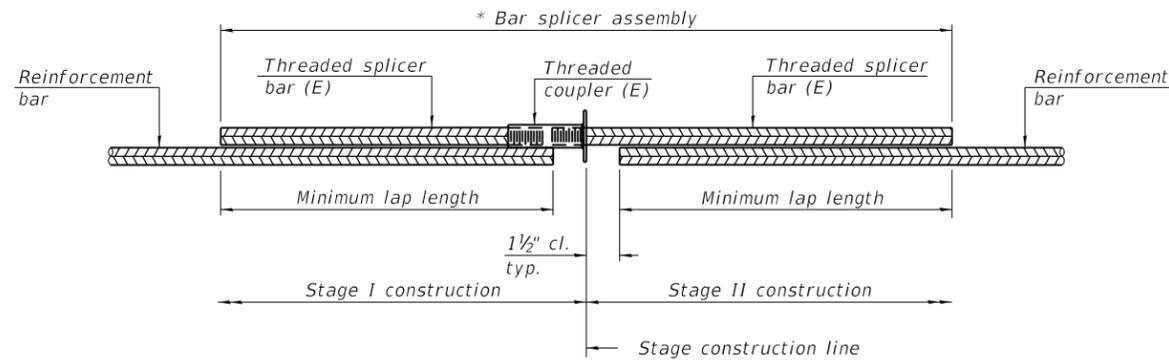
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PLOT DATE =	CHECKED - JMT	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**NORTH AND SOUTH ABUTMENT REPAIRS
S.N. 099-0018 NB I-55 OVER WCL RAILROAD**

SHEET S2-13 OF S2-14 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	86
CONTRACT NO. 62N22				
ILLINOIS		FED. AID PROJECT		

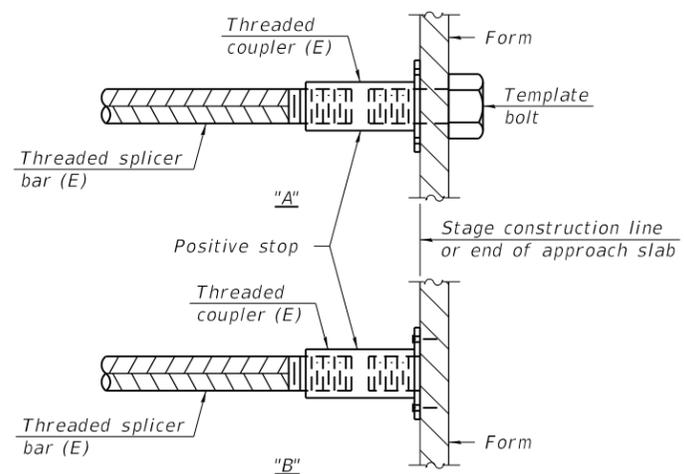


STANDARD BAR SPLICER ASSEMBLY PLAN
 (All components shall be provided from one supplier)

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

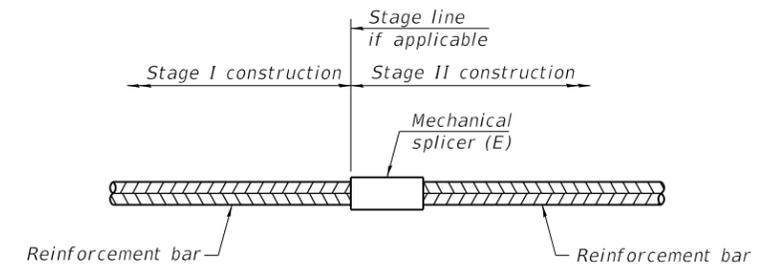
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
S. Abut. Deck	#5	8	3'-6"
S. Abut. Backwall	#6	6	4'-0"
N. Abut. Deck	#5	8	3'-6"
N. Abut. Backwall	#6	6	4'-0"



INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required

Notes:
 Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

MODEL: Default
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BSD-1

1-1-2020



USER NAME =	DESIGNED - JMT	REVISED -
	CHECKED - SPS	REVISED -
PLOT SCALE =	DRAWN - IH	REVISED -
PLOT DATE =	CHECKED - JMT	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
 SN 099-0018 NB I-55 OVER WCL RAILROAD

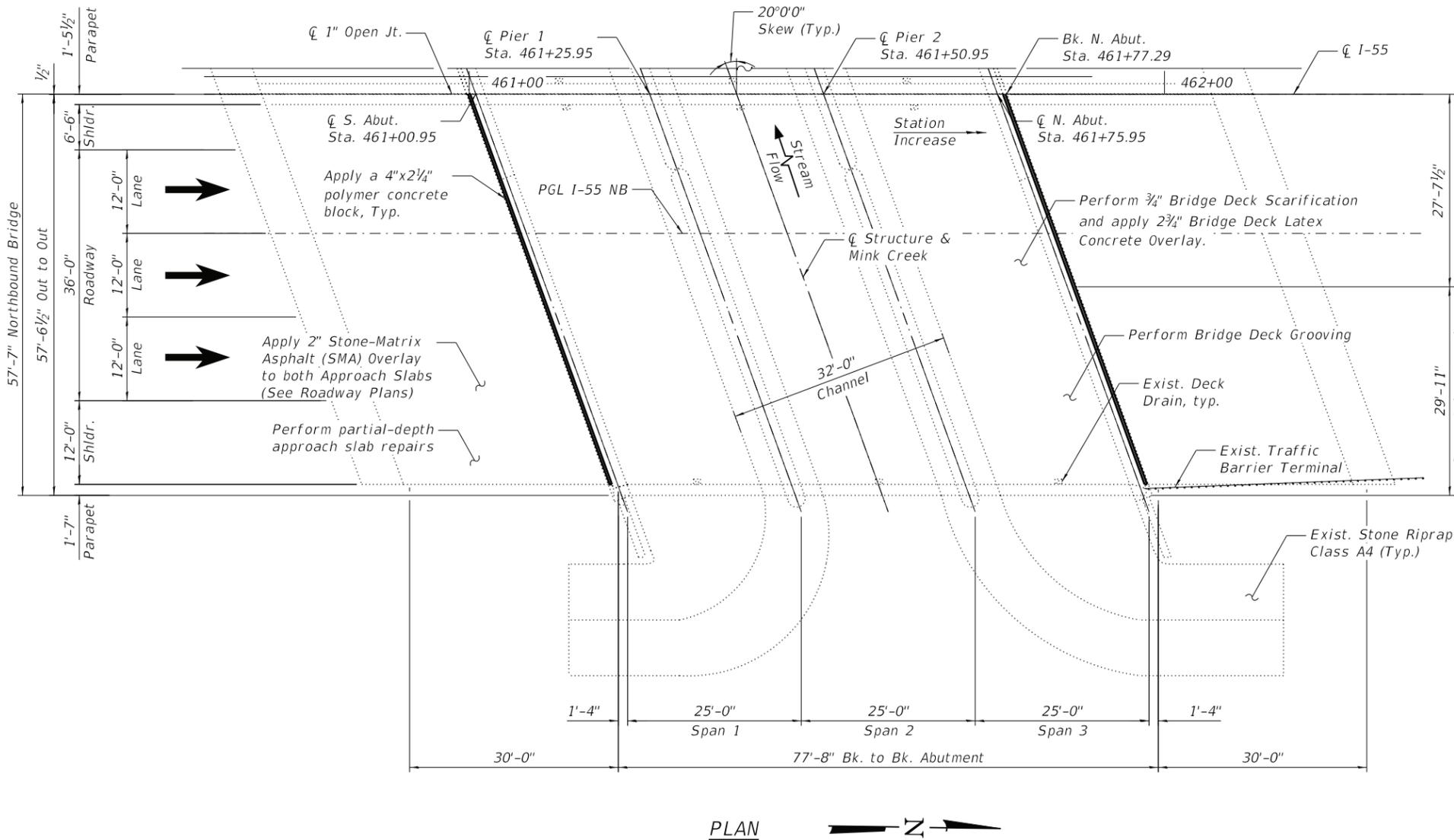
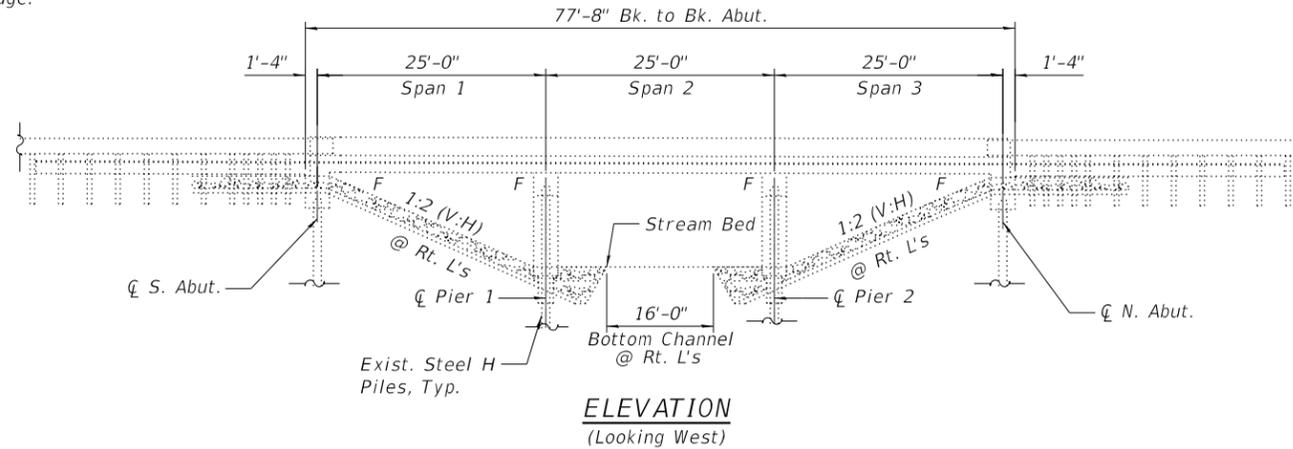
SHEET S2-14 OF S2-14 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
55	2020-253-BR&PP	WILL	178	87
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62N22	

Existing Structure: Structure No. 0994615 was constructed in 1994 as part of F.A.I. Route 55, Section 27(B-1,B-3,HB,VB-1)BR89 and project F.A. IM-NHI-55-6-(193)252 and reconstructed/widened in 2007. The Structure has a length of 77'-8" (back-to-back of abutments) and an out-to-out deck width of 57'-7". The continuous slab superstructure consists of three equal span lengths of 25'-0". The reinforced concrete deck slab is 12" thick. The substructure consists of reinforced concrete piers and abutments on steel piles.

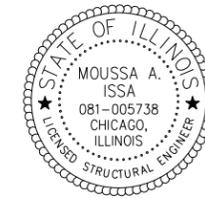
Traffic is to be maintained utilizing stage construction.

No Salvage.



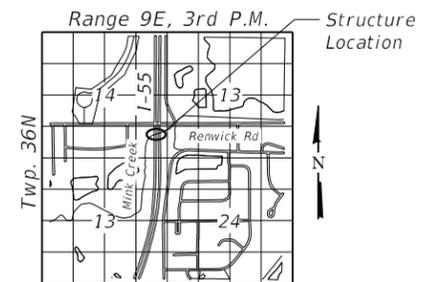
SCOPE OF WORK

1. Scarify $\frac{3}{4}$ " from the bridge deck slab.
2. Perform Deck Slab Repairs and Approach Slab Repairs as required.
3. Apply a 4"x2 $\frac{1}{4}$ " polymer concrete block, at both ends of deck
4. Apply a 2 $\frac{3}{4}$ " Bridge Deck Latex Concrete Overlay to the bridge deck.
5. Apply 2" Stone-Matrix Asphalt (SMA) Overlay on the Approach Slabs, see Roadway plans.
6. Apply Protective Coat to the top and inside face of reconstructed parapets and top of Latex Overlay.
7. Perform Bridge Deck Grooving.
8. Clean all floor drains.



Signed Moussa A. Issa
 Dr. Moussa A. Issa, S.E. Il. Lic. No. 081-005738
 Expires 11-30-2022

Date December 03, 2020 FOR SHEETS S3-01 THRU S3-08
 (Total of 8 Sheets)



LOCATION SKETCH

GENERAL PLAN AND ELEVATION
 I-55 OVER MINK CREEK
 F.A.I. ROUTE 55 - SEC. 2006-032
 WILL COUNTY
 STATION 630+07.11
 S.N. 099-4615

MODEL: Default
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CHECKED - MI,MAI	REVISOR -	
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PLOT DATE =	DATE - 12/3/2020	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

S.N. 099-4615 I-55 NB OVER MINK CREEK (0.8 MILES N OF US 30)

SHEET S3-01 OF S3-08 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	88
CONTRACT NO. 62N22				

ILLINOIS FED. AID PROJECT

GENERAL NOTES:

1. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
2. Protective coat shall be applied to top and inside face of parapets and Latex Overlay.
3. All exposed concrete edges shall have a 3/4"x45° chamfer, except where shown otherwise.
4. The Contractor is responsible to protect the existing conduit embedded in the parapet during concrete removal and construction. Any damage to the existing conduit shall be repaired by the Contractor at no additional cost to the Department.

INDEX OF SHEETS

- S3-01 General Plan & Elevation
- S3-02 Structure Notes, Index of Sheets & Total Bill of Material
- S3-03 Stage Construction (Sheet 1 of 2)
- S3-04 Stage Construction (Sheet 2 of 2)
- S3-05 Temporary Concrete Barrier For Stage Construction
- S3-06 Bridge Deck Repairs
- S3-07 Approach Slab Repairs
- S3-08 Parapet Repairs

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Bridge Deck Grooving	SQ YD	454	0	454
Protective Coat	SQ YD	539	0	539
Approach Slab Repair (Partial Depth)	SQ YD	2	0	2
Bridge Deck Latex Concrete Overlay, 2 3/4 Inches	SQ YD	464	0	464
Bridge Deck Scarification 3/4"	SQ YD	464	0	464
Structural Repair Of Concrete (Depth Equal To Or Less Than 5 Inches)	SQ FT	5	0	5
Polymer Concrete	CU FT	9	0	9

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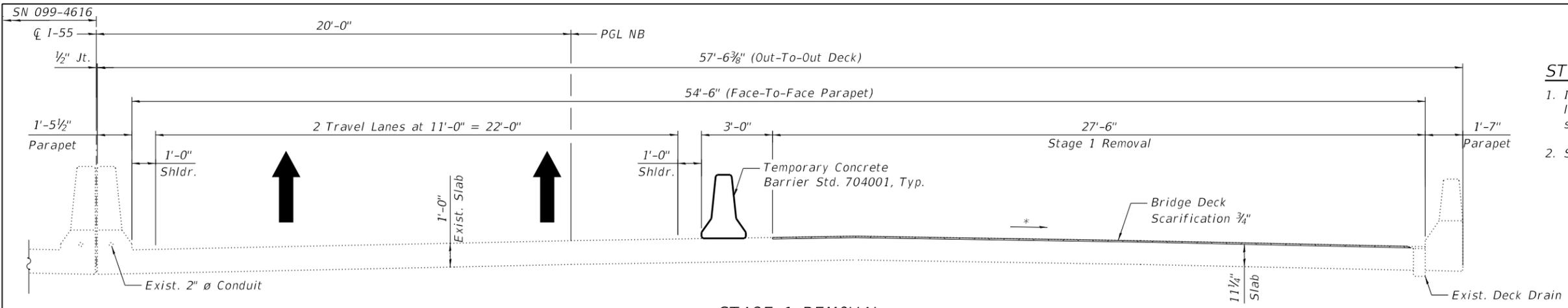
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STRUCTURE NOTES, INDEX OF SHEETS & TOTAL BILL OF MATERIAL
S.N. 099-4615 I-55 NB OVER MINK CREEK (0.8 MILES N OF US 30)

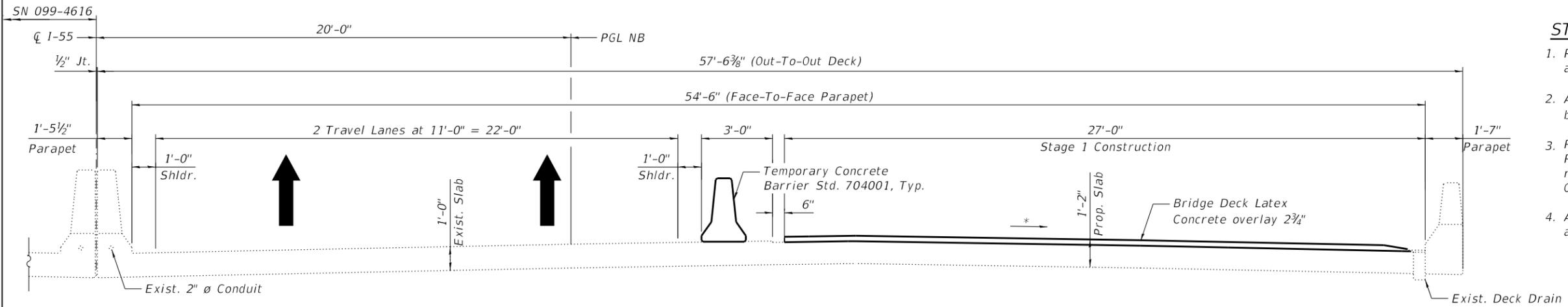
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I-55	2020-253-BR&PP	WILL	178	89
			CONTRACT NO. 62N22	
		ILLINOIS	FED. AID PROJECT	

SHEET S3-02 OF S3-08 SHEETS



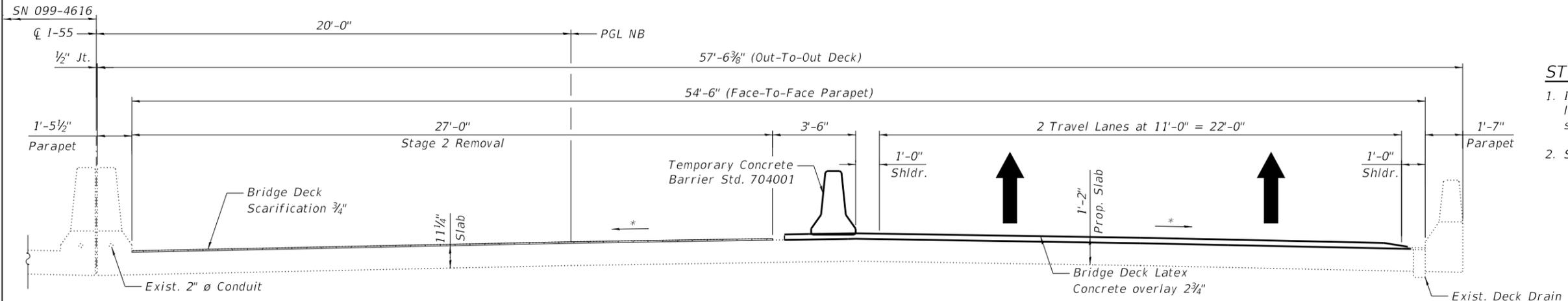
STAGE 1 REMOVAL
(Looking North)

- STAGE 1 REMOVAL**
1. Install temporary concrete barrier as shown to locate traffic on the west side of the existing structure.
 2. Scarify $\frac{3}{4}$ " from the top of deck slab.



STAGE 1 CONSTRUCTION
(Looking North)

- STAGE 1 CONSTRUCTION**
1. Perform partial-depth approach slab repairs, at locations as shown in the plans.
 2. Apply $2\frac{3}{4}$ " bridge deck latex concrete overlay to bridge deck slab.
 3. Perform bridge deck grooving and apply Protective Coat to the top and inside face of reconstructed parapets and top of Latex Overlay.
 4. Apply 2" Stone-Matrix Asphalt (SMA) Overlay to approach slabs, see Roadway plans.



STAGE 2 REMOVAL
(Looking North)

- STAGE 2 REMOVAL**
1. Install temporary concrete barrier as shown to locate traffic on east side of the existing structure.
 2. Scarify $\frac{3}{4}$ " from the top of deck slab.

* Match existing deck surface profile.

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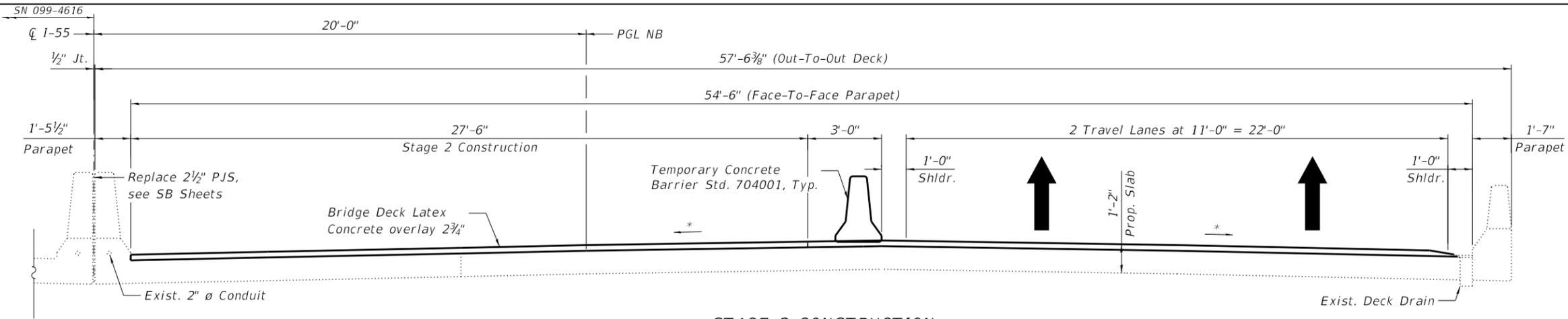
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PLOT DATE =	DATE - 12/2/2020	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGE CONSTRUCTION (SHEET 1 OF 2)
S.N. 099-4615 I-55 NB OVER MINK CREEK (0.8 MILES N OF US 30)

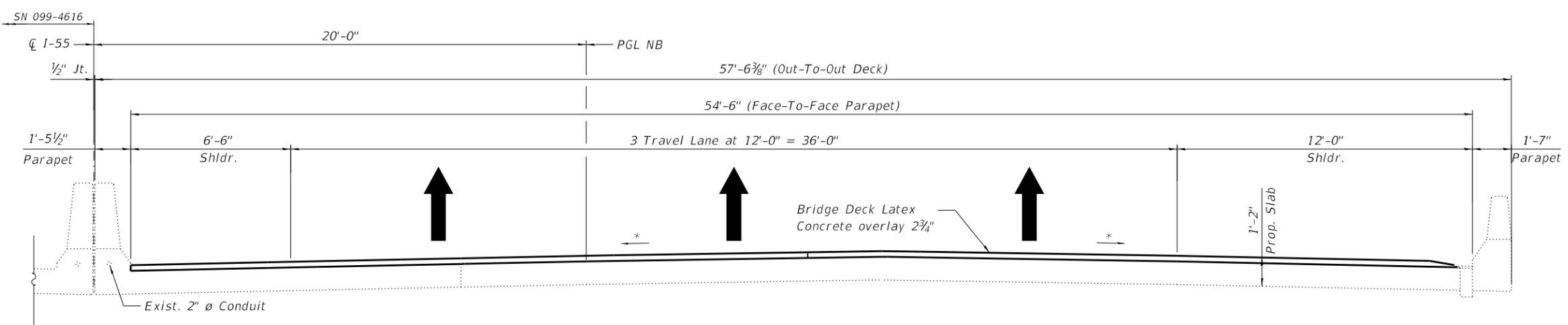
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I-55	2020-253-BR&PP	WILL	178	90
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

SHEET S3-03 OF S3-08 SHEETS



STAGE 2 CONSTRUCTION
(Looking North)

- STAGE 2 CONSTRUCTION**
1. Perform partial-depth approach slab repairs, at locations as shown in the plans.
 2. Apply 2 3/4" bridge deck latex concrete overlay to bridge deck slab.
 3. Perform bridge deck grooving and apply Protective Coat to the top and inside face of reconstructed parapets and top of Latex Overlay.
 4. Apply 2" Stone-Matrix Asphalt (SMA) Overlay to approach slabs, See Roadway plans.
 5. Apply Permanent pavement markings on top of deck and approach slabs. For Pavement Markings, see Roadway Plans.



FINAL CROSS SECTION
(Looking North)

* Match existing deck surface profile.

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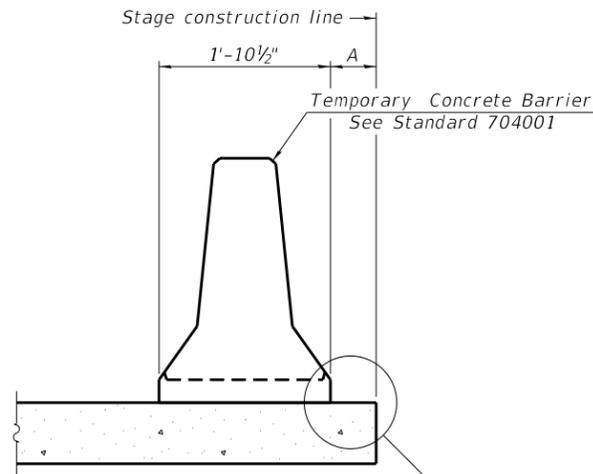
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PLOT DATE =	DATE - 12/2/2020	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGE CONSTRUCTION (SHEET 2 OF 2)
S.N. 099-4615 I-55 NB OVER MINK CREEK (0.8 MILES N OF US 30)

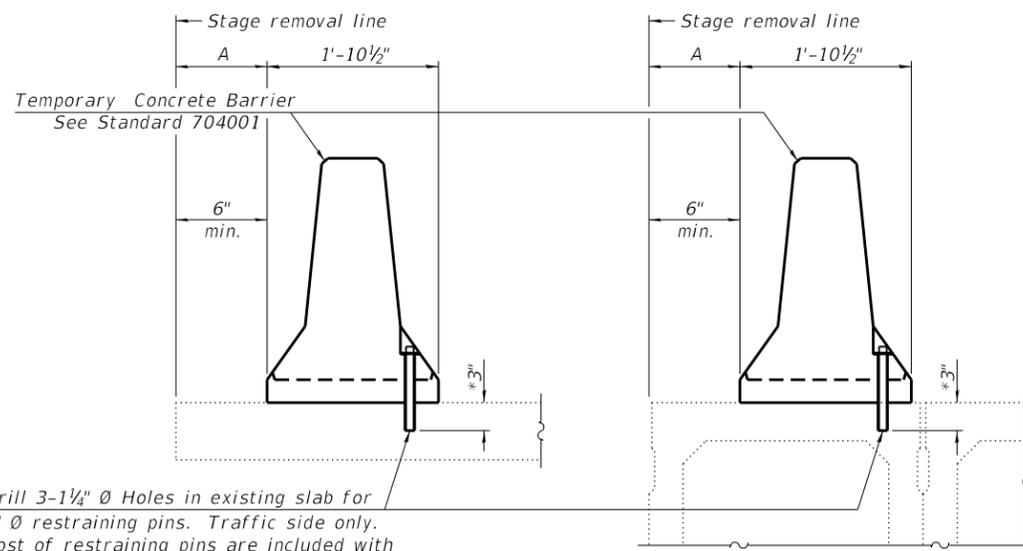
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	91
CONTRACT NO. 62N22				
ILLINOIS		FED. AID PROJECT		

SHEET S3-04 OF S3-08 SHEETS



When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM

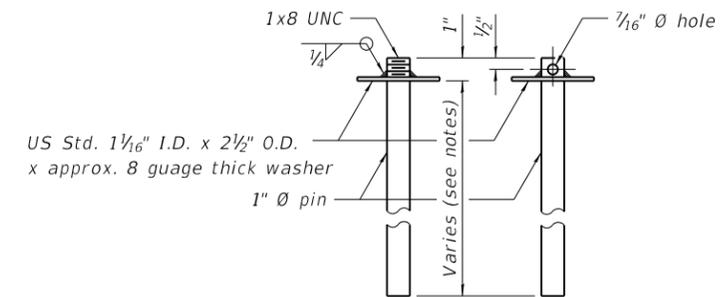


Drill 3-1/4" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

EXISTING SLAB

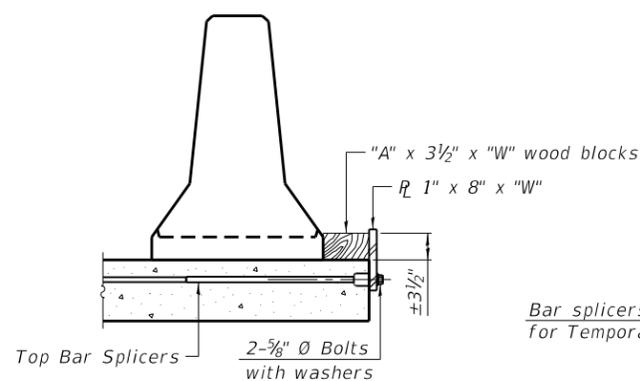
* When hot-mix asphalt wearing surface is present, embedment shall be 3" plus the wearing surface depth.

EXISTING DECK BEAM

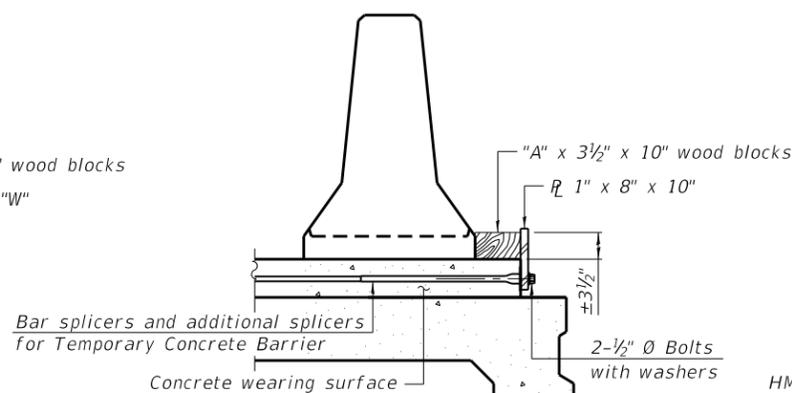


RESTRAINING PIN

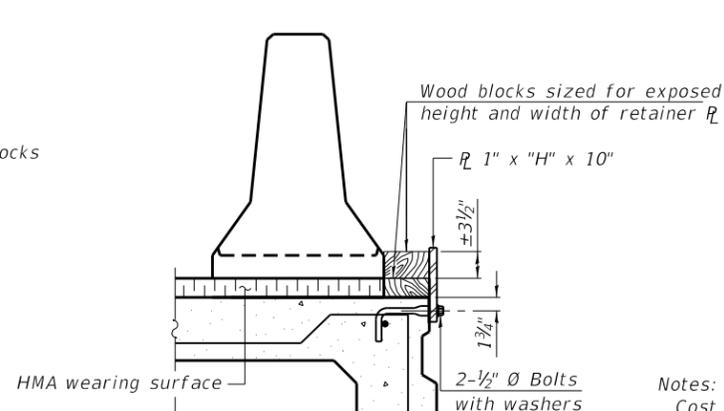
SECTIONS THRU SLAB OR DECK BEAM



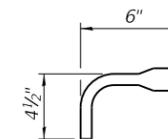
DETAIL I



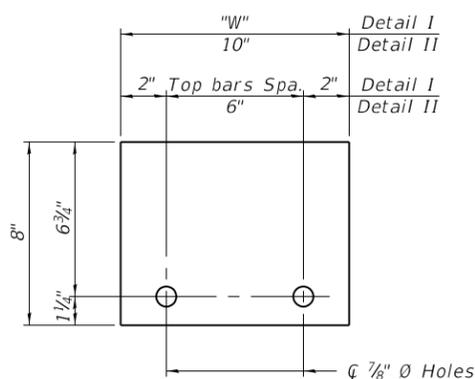
DETAIL II



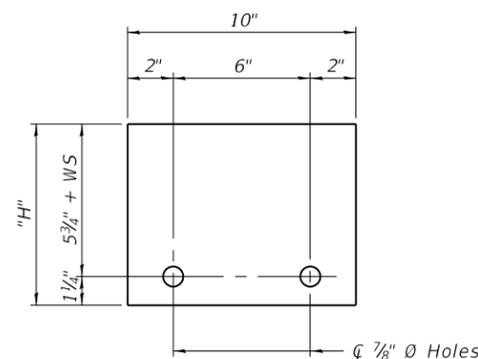
DETAIL III



BAR SPLICER FOR #4 BAR - DETAIL III



STEEL RETAINER R 1" x 8" x "W"
(Detail I and II)



STEEL RETAINER R 1" x "H" x 10"
(Detail III)

Notes:
 Cost of retainer assembly is included with Temporary Concrete Barrier.
 A retainer assembly shall be located at the approximate \bar{c} of each temporary concrete barrier.
 The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.
 When the 'A' dimension is less than 1 1/2', the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.

- Detail I - Installation for a new bridge deck or bridge slab.
- Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.
- Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

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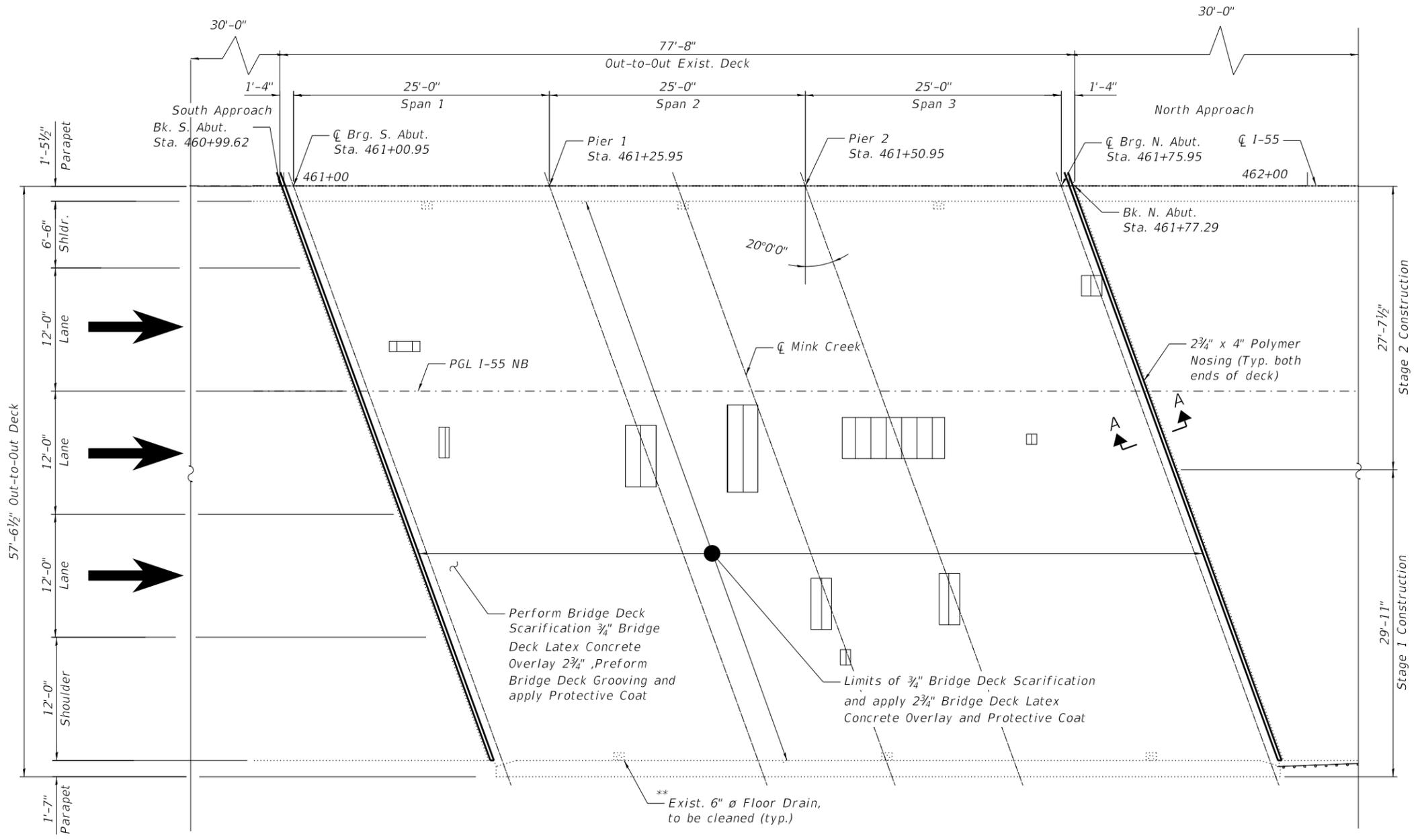
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION
 S.N. 099-4615 I-55 NB OVER MINK CREEK (0.8 MILES N OF US 30)

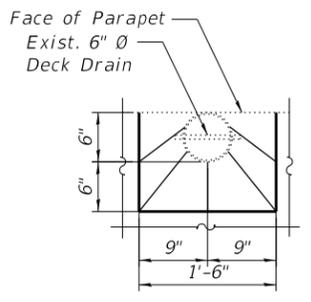
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I-55	2020-253-BR&PP	WILL	178	92
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

SHEET S3-05 OF S3-08 SHEETS

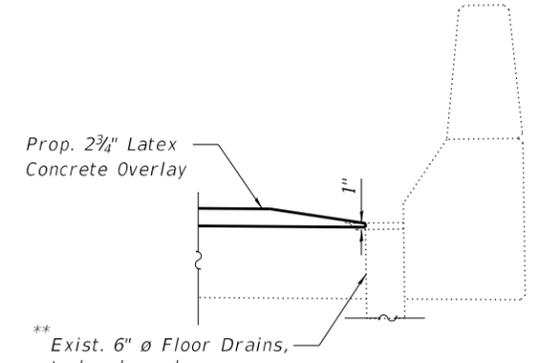
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DECK PLAN



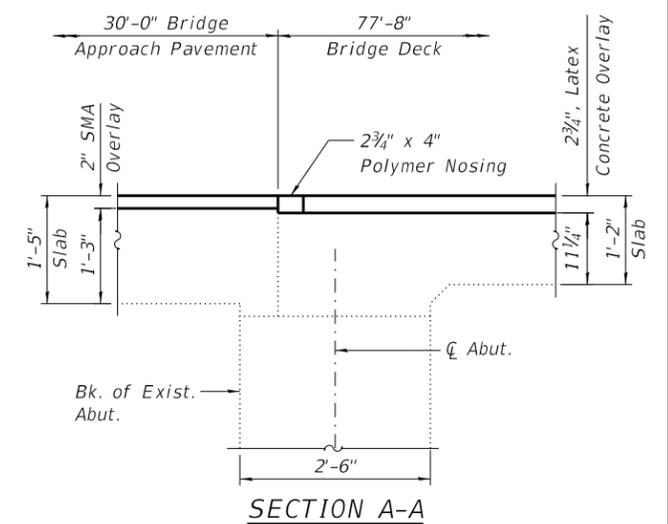
TOP PLAN



SECTION AT DECK DRAIN

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Bridge Deck Grooving	Sq. Yd.	454
Protective Coat	Sq. Yd.	539
Bridge Deck Latex Concrete Overlay, 2 3/4"	Sq. Yd.	464
Bridge Deck Scarification, 3/4"	Sq. Yd.	464
Polymer Concrete	Cu. Ft.	9



SECTION A-A

NOTES:

1. Areas of deck repair shown are estimated. The Engineer shall show actual locations of deck repairs at the time of construction.
2. For bridge deck final cross section, see Sheet #STG62
3. Perform bridge deck grooving.
4. Cost of all labour and materials necessary to clean all existing floor drains is included in the cost for Bridge Deck Latex Concrete Overlay 2 3/4".

* Areas of Deck Slab Repair (Partial) are provided for information only and shall be included in the cost of Bridge Deck Latex Concrete Overlay, 2 3/4"
 ** Cost included in the cost for Bridge Deck Latex Concrete Overlay 2 3/4".

LEGEND



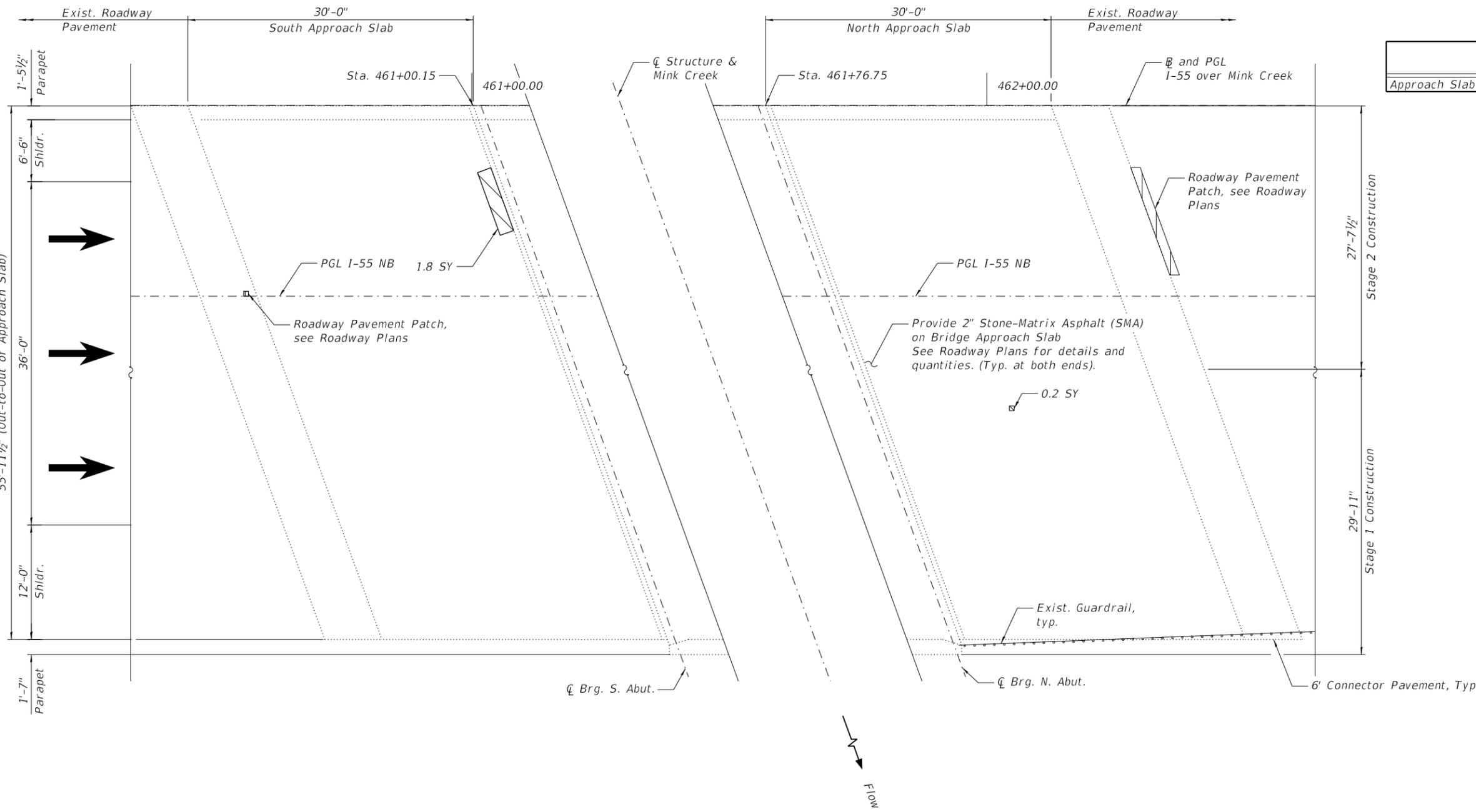
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	DATE - 12/2/2020	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

BRIDGE DECK REPAIRS
S.N. 099-4615 I-55 NB OVER MINK CREEK (0.8 MILES N OF US 30)
 SHEET S3-06 OF S3-08 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	93
CONTRACT NO. 62N22				
ILLINOIS FED. AID PROJECT				

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BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Approach Slab Repair (Partial Depth)	Sq. Yd.	2

PARTIAL PLAN
 (North and South Approaches)

NOTE:
 1. Areas of Approach Slab Repair (Partial Depth) shown are estimated. The Engineer shall show actual location of repairs at the time of construction.

LEGEND

	Approach Slab Repair (Partial Depth)
--	--------------------------------------



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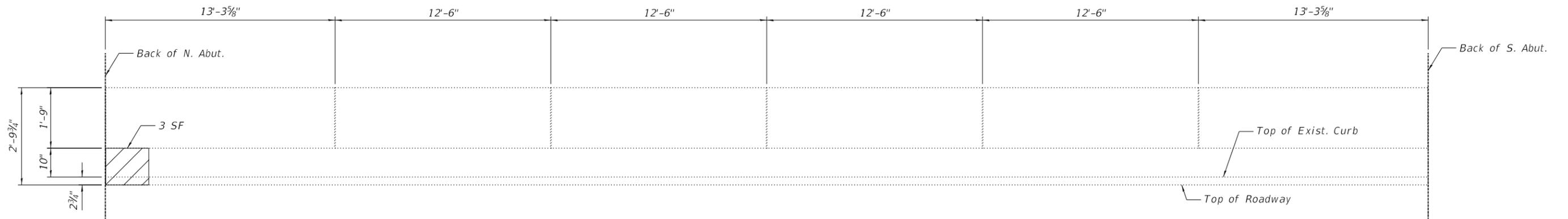
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

APPROACH SLAB REPAIRS
S.N. 099-4615 I-55 NB OVER MINK CREEK (0.8 MILES N OF US 30)

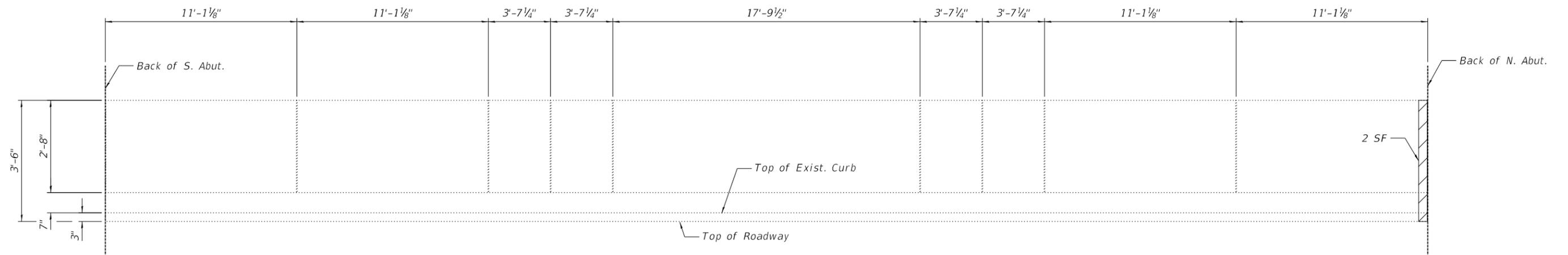
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I-55	2020-253-BR&PP	WILL	178	94
CONTRACT NO. 62N22				

SHEET S3-07 OF S3-08 SHEETS

ILLINOIS FED. AID PROJECT



INSIDE ELEVATION OF EAST PARAPET



INSIDE ELEVATION OF WEST PARAPET

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)	Sq. Ft.	5

LEGEND

- Structural Repair of Concrete (Depth Equal to or Less than 5 inches)
- SF Square Foot

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PARAPET REPAIRS
S.N. 099-4615 I-55 NB OVER MINK CREEK (0.8 MILES N OF US 30)**

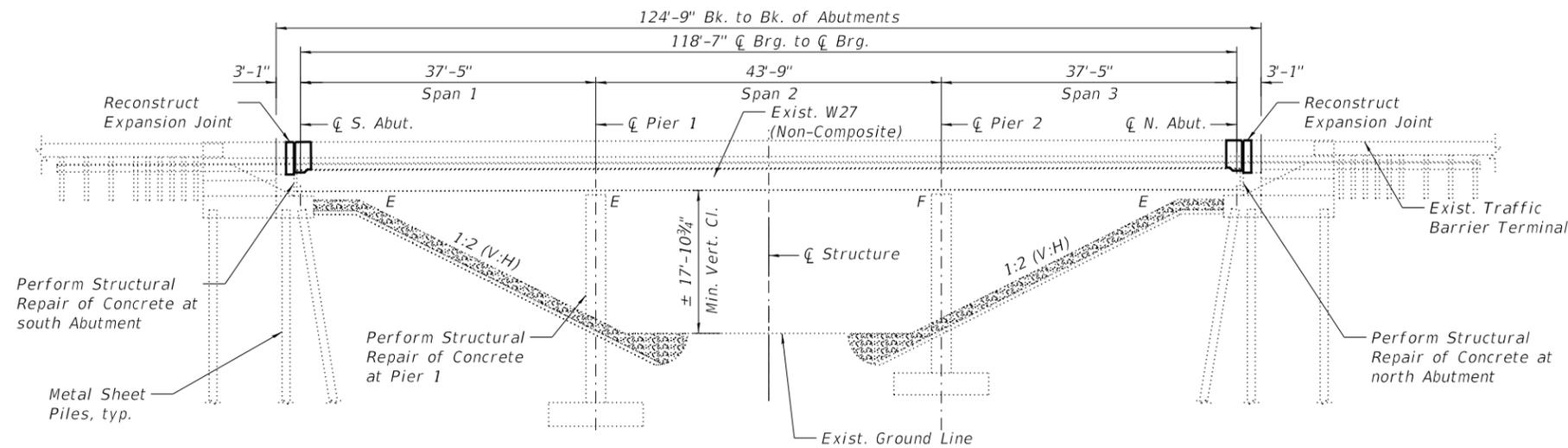
SHEET S3-08 OF S3-08 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62N22				
ILLINOIS		FED. AID PROJECT		

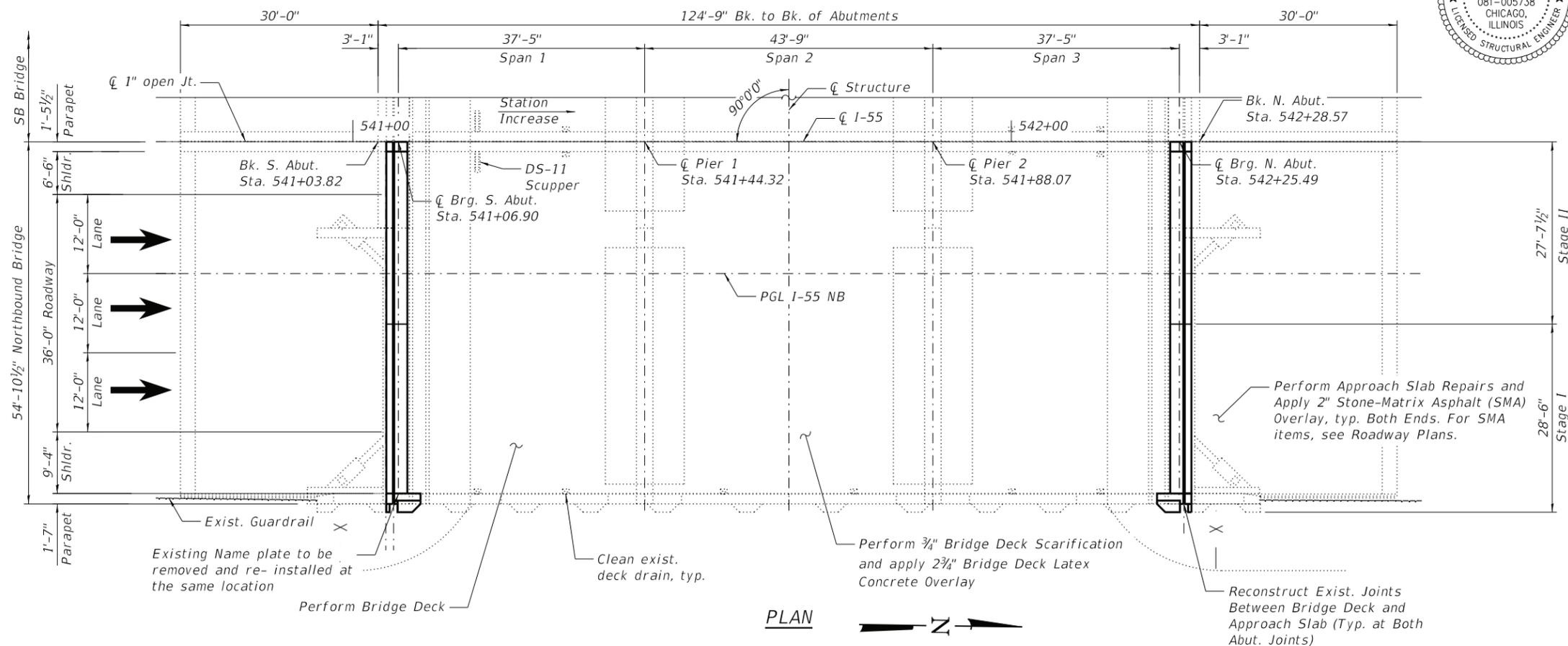
Existing Structure: Structure No. 099-0022 NB over Abandoned Railroad was originally constructed in 1956 as part of F.A. Route 34, Section 27-VB-1 and project FGI 187 (7) and reconstructed/widened in 2007. The structure has a length of 124'-9" (back-to-back of abutments) and an out-to-out deck width of 54'-11". The superstructure consists of three multi-beam spans of length 37'-5", 43'-9" and 37'-5" respectively. The reinforced concrete deck slab is 7½" thick. The substructure consists of reinforced concrete piers on footing and abutments on steel piles.

Traffic is to be maintained utilizing stage construction.

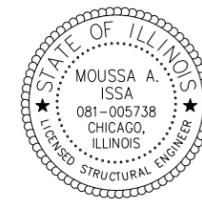
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ELEVATION
(Looking West)

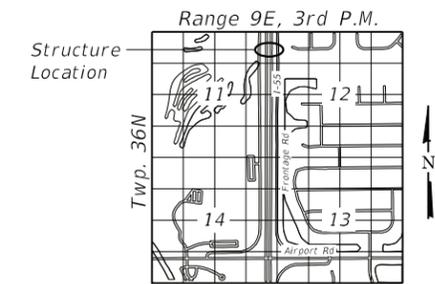


PLAN



Signed Moussa A. Issa
Dr. Moussa A. Issa, S.E. Il. Lic. No. 081-005738
Expires 11-30-2022

Date December 03, 2020 For Sheets S4-01 thru S4-18
(Total of 18 Sheets)



LOCATION SKETCH

GENERAL PLAN AND ELEVATION
I-55 NB OVER OVER MS RR (ABANDONED)
F.A.I. ROUTE 55 - SEC. 2006-032
WILL COUNTY
STATION 541+66.20
S.N. 099-0022

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PLOT DATE =	DATE - 12/3/2020	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

S.N. 099-0022 I-55 NB OVER MATERIAL SERVICE RR (ABANDONED)

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	96
CONTRACT NO. 62N22				

SHEET S4-01 OF S4-18 SHEETS

ILLINOIS FED. AID PROJECT

GENERAL NOTES

1. Reinforcement bars designated (E) shall be epoxy coated.
2. Prior to pouring the new concrete deck for expansion joints reconstruction and deck slab repairs, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.
3. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
4. Joint openings shall be adjusted according to Article 520.04 of the Standard Specifications when the deck is poured at an ambient temperature other than 50° F.
5. Bars noted thus, 3x2-#5, indicates 3 lines of #5 bars with 2 lengths of bars per line.
6. All exposed concrete edges shall have a 3/4"x45° chamfer, except where shown otherwise.
7. The existing structural steel coating contains lead. The Contractor shall take appropriate precautions to deal with the presence of lead on this project.
8. Concrete Sealer shall be applied to the designated areas of the abutments.
9. The Contractor is responsible to protect the existing conduit embedded in the parapet during concrete removal and construction. Any damage to the existing conduit shall be repaired by the Contractor at no additional cost to the Department.
10. Protective Coat shall be applied to the top and inside face of parapets, reconstructed transverse Expansion Joints and to the surface of the new overlay.

INDEX OF SHEETS

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- S4-02 Structure Notes, Index of Sheets & Total Bill of Material
- S4-03 Stage Construction (Sheet 1 of 2)
- S4-04 Stage Construction (Sheet 2 of 2)
- S4-05 Temporary Concrete Barrier for Stage Construction
- S4-06 Bridge Deck Repairs
- S4-07 Approach Slab Repairs
- S4-08 Parapet Repairs
- S4-09 S. Abut. Joint Removal and Reconstruction (Sheet 1 of 2)
- S4-10 S. Abut. Joint Removal and Reconstruction (Sheet 2 of 2)
- S4-11 N. Abut. Joint Removal and Reconstruction (Sheet 1 of 2)
- S4-12 N. Abut. Joint Removal and Reconstruction (Sheet 2 of 2)
- S4-13 Prefomed Joint Strip Seal
- S4-14 Noise Wall Details
- S4-15 South Abutment Repairs
- S4-16 Pier 1 Repairs
- S4-17 North Abutment Repairs
- S4-18 Bar Splicer Assembly Details

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Concrete Removal	CU YD	16	0	16
Concrete Superstructure	CU YD	18.2	0.0	18.2
Bridge Deck Grooving	SQ YD	664	0	664
Protective Coat	SQ YD	814	0	814
Reinforcement Bars, Epoxy Coated	POUND	2,600	0	2,600
Bar Splicers	EACH	26	0	26
Prefomed Joint Strip Seal	FOOT	110	0	110
Concrete Sealer	SQ FT	0	254	254
Approach Slab Repair (Partial Depth)	SQ YD	1	0	1
Bridge Deck Latex Concrete Overlay, 2 3/4 Inches	SQ YD	668	0	668
Bridge Deck Scarification 3/4"	SQ YD	668	0	668
Structural Repair Of Concrete (Depth Equal To Or Less Than 5 Inches)	SQ FT	4	14	18
Noise Abatement Wall Panel Removal And Re-Erection	EACH	2	0	2

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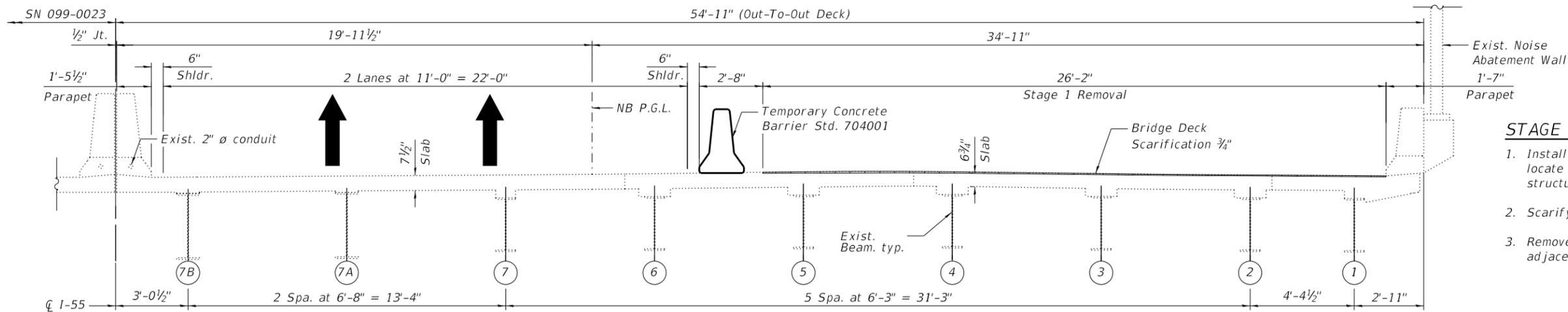


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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

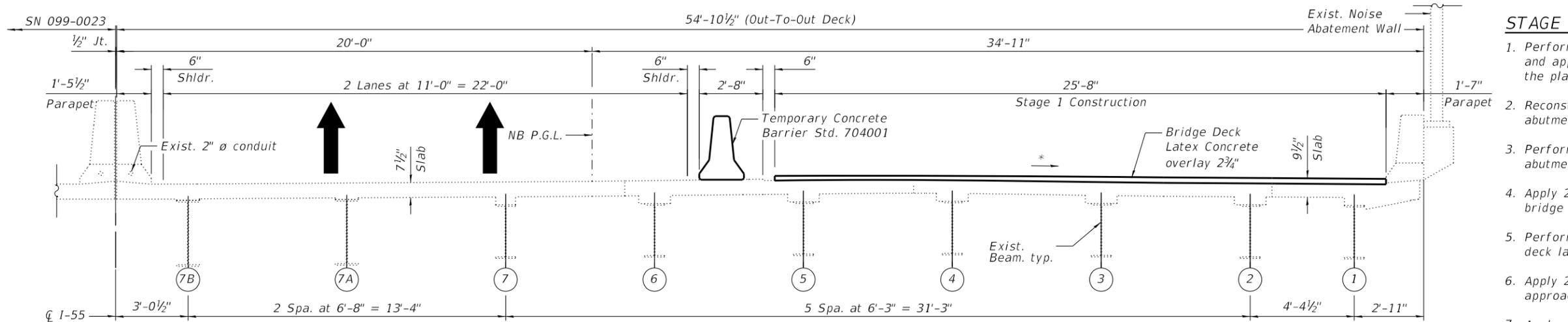
**STRUCTURE NOTES, INDEX OF SHEETS & TOTAL BILL OF MATERIAL
S.N. 099-0022 I-55 NB OVER MATERIAL SERVICE RR (ABANDONED)**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	97
			CONTRACT NO. 62N22	
SHEET S4-02 OF S4-18 SHEETS			ILLINOIS FED. AID PROJECT	



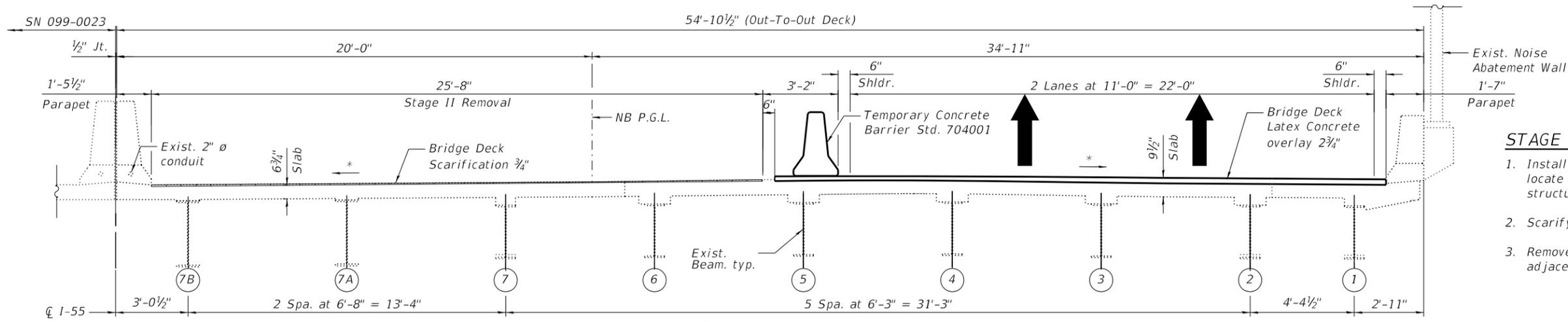
STAGE 1 REMOVAL
(Looking North)

- STAGE 1 REMOVAL**
1. Install temporary concrete barrier as shown to locate traffic on the west side of the existing structure.
 2. Scarify $\frac{3}{4}$ " from the top of deck slab.
 3. Remove portions of bridge deck/approach slab adjacent to abutment joints, as shown in the plans.



STAGE 1 CONSTRUCTION
(Looking North)

- STAGE 1 CONSTRUCTION**
1. Perform full- and partial-depth deck slab repairs, and approach slab repairs, at locations as shown in the plans.
 2. Reconstruct expansion joints at north and south abutments and install new strip seals
 3. Perform structural repair of concrete for the abutments and pier.
 4. Apply $2\frac{3}{4}$ " bridge deck latex concrete overlay to bridge deck slab.
 5. Perform bridge deck grooving for the $2\frac{3}{4}$ " bridge deck latex concrete overlay.
 6. Apply 2" Stone-Matrix Asphalt (SMA) Overlay to approach slabs, see Roadway plans.
 7. Apply protective coat to the top of reconstructed transverse joint areas and the top and inside faces of Parapets and top of Latex Overlay.



STAGE 2 REMOVAL
(Looking North)

- STAGE 2 REMOVAL**
1. Install temporary concrete barrier as shown to locate traffic on east and west side of the existing structure.
 2. Scarify $\frac{3}{4}$ " from the top of deck slab.
 3. Remove portions of bridge deck/approach slab adjacent to abutment joints, as shown in the plans.

* Match existing deck surface profile.

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PLOT DATE =	DATE - 12/2/2020	REVISED -

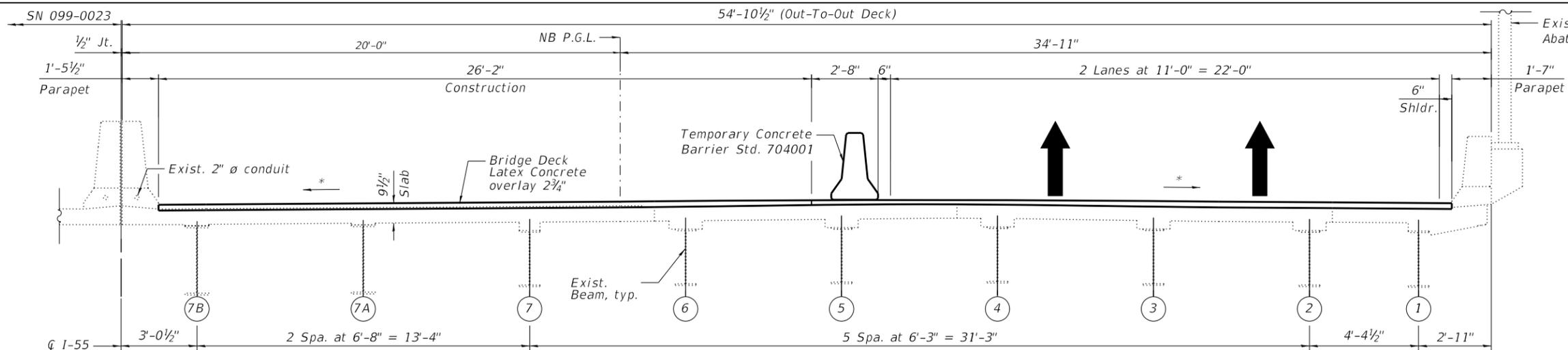
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGE CONSTRUCTION (SHEET 1 OF 2)
S.N. 099-0022 I-55 NB OVER MATERIAL SERVICE RR (ABANDONED)

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	98
CONTRACT NO. 62N22				

SHEET S4-03 OF S4-18 SHEETS

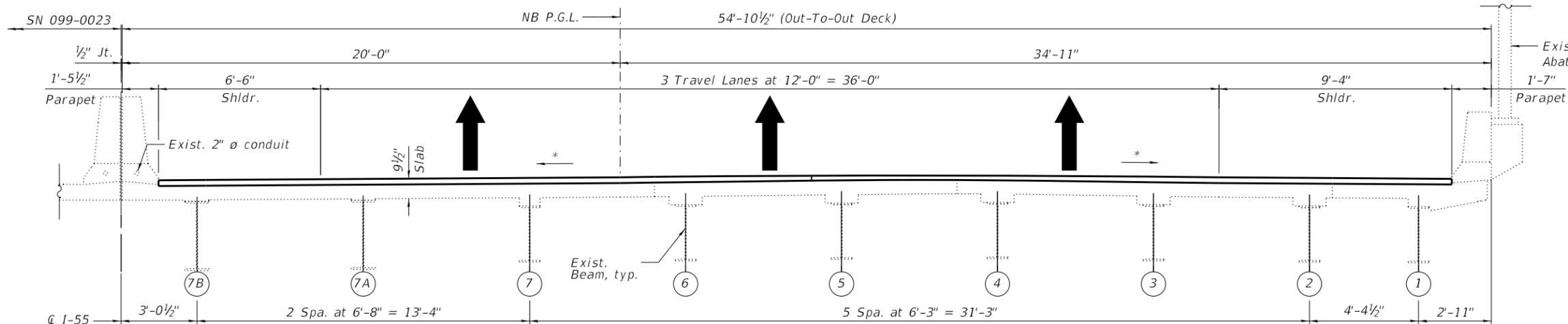
ILLINOIS FED. AID PROJECT



STAGE 2 CONSTRUCTION
(Looking North)

STAGE 2 CONSTRUCTION

1. Perform full- and partial-depth deck slab repairs, and approach slab repairs, at locations as shown in the plans.
2. Reconstruct expansion joints at north and south abutments and install new strip seals
3. Perform structural repair of concrete for the abutments and pier.
4. Apply 2 3/4" bridge deck latex concrete overlay to bridge deck slab.
5. Perform bridge deck grooving for the 2 3/4" bridge deck latex concrete overlay.
6. Apply 2" Stone-Matrix Asphalt (SMA) Overlay to approach slabs, see Roadway plans.
7. Apply protective coat to the top of reconstructed transverse joint areas and the top and inside faces of Parapets and top of Latex Overlay.
8. Apply permanent pavement markings on top of deck and approach slabs.



FINAL CROSS-SECTION
(Looking North)

* Match existing deck surface profile.

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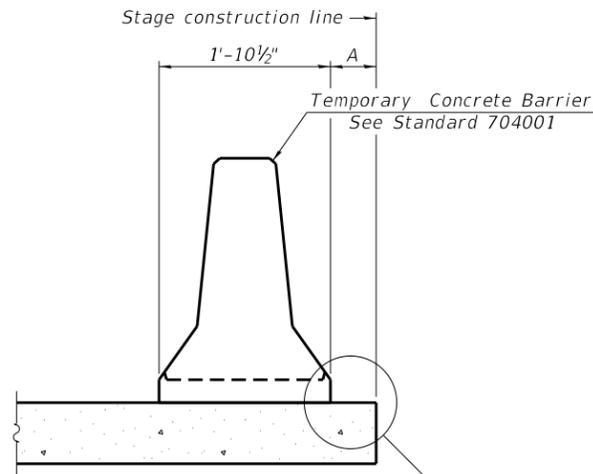


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PLOT DATE =	DATE - 12/2/2020	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

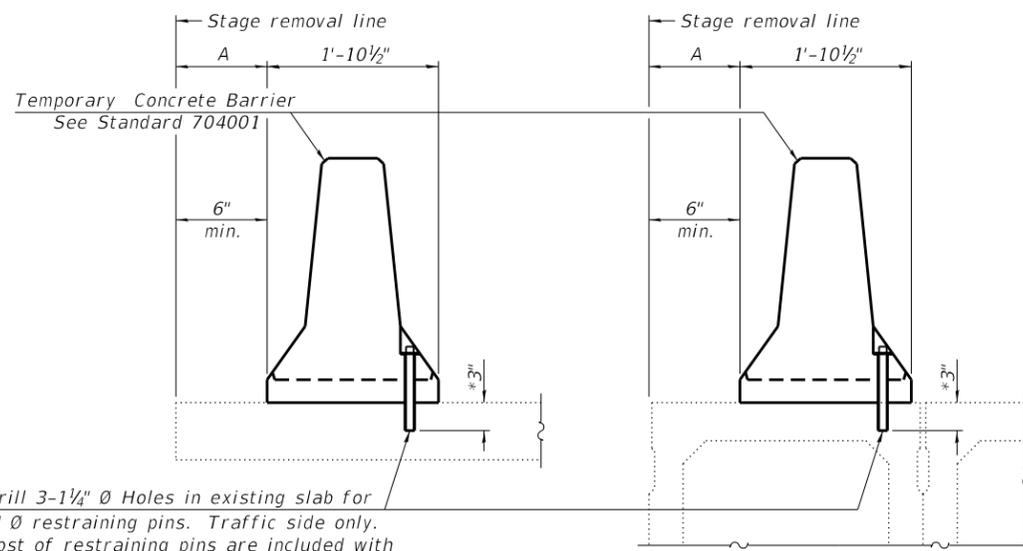
STAGE CONSTRUCTION (SHEET 2 OF 2)
S.N. 099-0022 I-55 NB OVER MATERIAL SERVICE RR (ABANDONED)

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	99
CONTRACT NO. 62N22				
SHEETS4-04 OF S4-18 SHEETS		ILLINOIS FED. AID PROJECT		



When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM



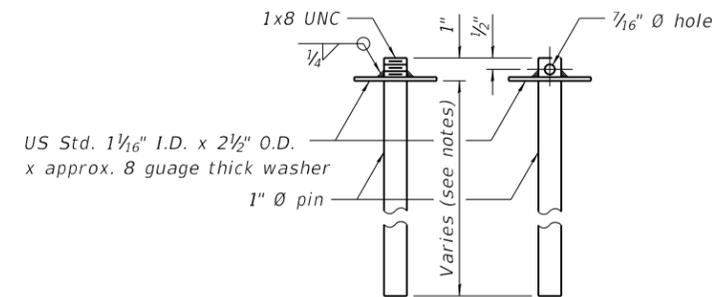
Drill 3-1/4" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

EXISTING SLAB

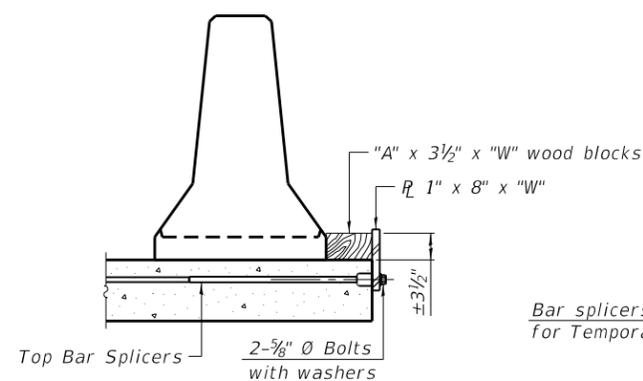
EXISTING DECK BEAM

* When hot-mix asphalt wearing surface is present, embedment shall be 3" plus the wearing surface depth.

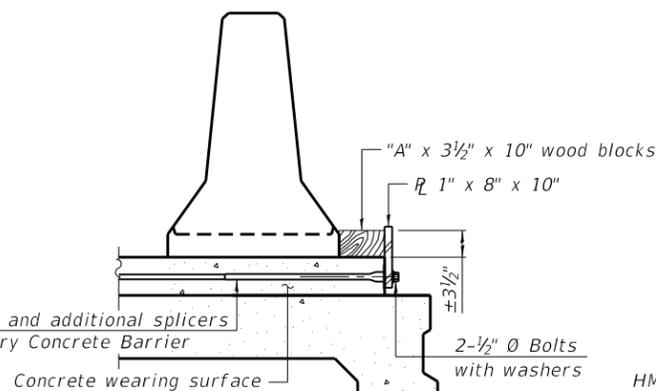
SECTIONS THRU SLAB OR DECK BEAM



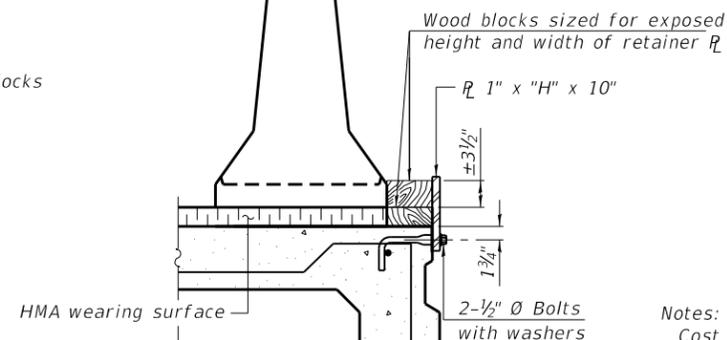
RESTRAINING PIN



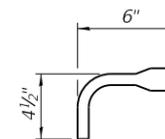
DETAIL I



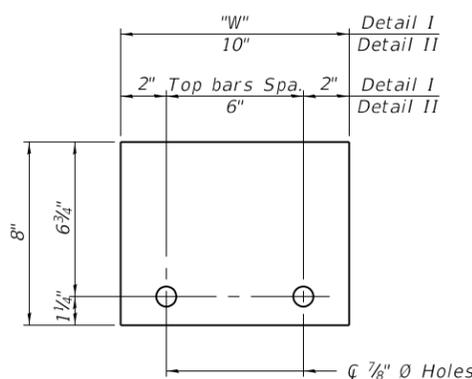
DETAIL II



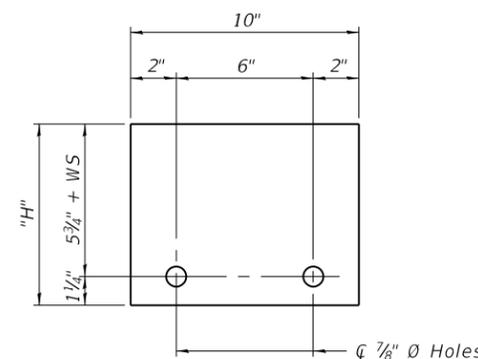
DETAIL III



BAR SPLICER FOR #4 BAR - DETAIL III



STEEL RETAINER R 1" x 8" x "W"
(Detail I and II)



STEEL RETAINER R 1" x "H" x 10"
(Detail III)

Notes:
 Cost of retainer assembly is included with Temporary Concrete Barrier.
 A retainer assembly shall be located at the approximate \bar{c} of each temporary concrete barrier.
 The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.
 When the 'A' dimension is less than 1 1/2', the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.

Detail I - Installation for a new bridge deck or bridge slab.
 Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.
 Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION
 S.N. 099-0022 I-55 NB OVER MATERIAL SERVICE RR (ABANDONED)

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
I-55	2020-253-BR&PP	WILL	178	100
ILLINOIS			CONTRACT NO. 62N22	
FED. AID PROJECT				

SHEET S4-05 OF S4-18 SHEETS