

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	2020-130-RS	COOK	41	1
FED. ROAD DIST. NO. 1 ILLINOIS		CONTRACT NO. 62M08		

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

**PROPOSED HIGHWAY PLANS**  
F.A.P. 353 /US 30 (LINCOLN HWY.)  
IL 43 (HARLEM AVE.) TO IL 50 (CICERO AVE.)  
SMART OVERLAY, ADA PEDESTRIAN RAMPS  
AND MILLED RUMBLE STRIP  
SECTION NO.: 2020-130-RS  
PROJECT: NHPP-9GTI(190)  
COOK COUNTY  
C-91-328-20

FOR INDEX OF SHEETS, SEE SHEET NO. 2

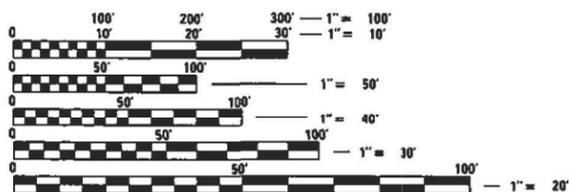
PROJECT IS LOCATED IN THE VILLAGE OF MATTESON

**TRAFFIC DATA:**

2019 ADT - 18700 TO 36500  
SPEED LIMIT - 35 TO 40 MPH

**OMISSIONS:**

BUTTERFIELD CREEK  
STA. 53+66 TO 53+93  
BRIDGE OVER I-57  
STA. 144+36 TO 147+42

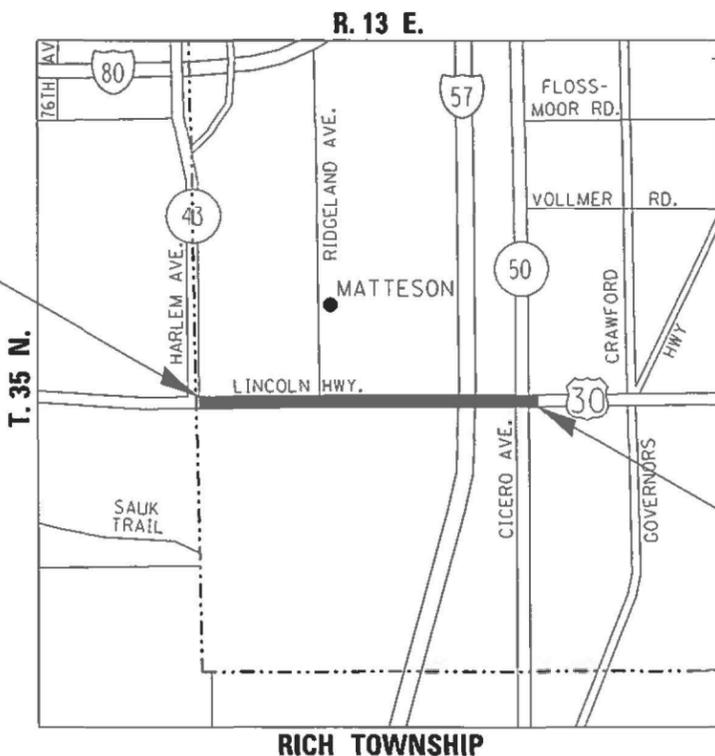


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811

PROJECT ENGINEER: VESELIN VELICHKOV (847) 705-4432  
PROJECT MANAGER: FAWAD AQUEEL (847) 705-4247

CONTRACT NO. 62M08



PROJECT BEGINS  
STA. 25 + 97

PROJECT ENDS  
STA. 175 + 71

GROSS LENGTH OF PROJECT - 14974 FEET - 2.84 MILES  
NET LENGTH OF PROJECT - 14641 FEET - 2.77 MILES

D-91-530-20



LOCATION OF SECTION INDICATED THUS: - [Symbol]

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED December 9, 2020  
*Anthony J. Ramirez* REGIONAL ENGINEER  
January 29, 2021  
*Scott A. Elk*  
ENGINEER OF DESIGN AND ENVIRONMENT  
January 29, 2021  
*James J. Guerin*  
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

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OF THE STATE OF ILLINOIS

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1	COVER SHEET
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37-40	DETECTOR LOOP LOCATION DETAILS
41	ARTERIAL ROAD INFORMATION SIGNING (TC-22)

STATE STANDARDS	
000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-11	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424006-05	DIAGONAL CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
604001-05	FRAME AND LIDS, TYPE 1
604006-05	FRAME AND GRATE, TYPE 3
606001-07	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701101-05	OFF-ROAD OPERATIONS, MULTILANE, 15' (4.5M) TO 24" (600MM) FROM PAVEMENT EDGE
701411-09	LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP GREATER THAN OR EQUAL TO 45 MPH
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	LANE CLOSURE, MULTILANE, 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS
781001-04	TYPICAL APPLICATIONS FOR RAISED REFLECTIVE PAVEMENT MARKERS
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUTS FOR DETECTION LOOPS

**GENERAL NOTES:**

BEFORE STARTING ANY EXCAVATION THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR "CUAN" (CHICAGO UTILITY ALERT NETWORK) AT 312-744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES, (48 HOURS NOTIFICATION IS REQUIRED).

10 FEET (3 METERS) TRANSITION SHALL BE USED TO MATCH PROPOSED CURB AND GUTTER AND MEDIAN ITEMS OF WORK TO EXISTING CURBS AND GUTTERS AND MEDIANS IN THE FIELD, UNLESS OTHERWISE SHOWN. THE TRANSITION SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEMS OF WORK SPECIFIED.

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH THE UTILITY COMPANIES AND THE VILLAGE OF MATTESON.

DUE TO THE PRESENCE OF RED LIGHT RUNNING (RLR) CAMERAS AT THE LOCATIONS LISTED BELOW, THE CONTRACTOR SHALL NOTIFY THE LOCAL MUNICIPALITY AND RLR CAMERA COMPANY PRIOR TO THE START OF CONSTRUCTION. THE LOCAL MUNICIPALITY AND/ OR THE RLR CAMERA COMPANY SHALL MAKE THE CAMERA INOPERATIVE FOR THE TIME OF CONSTRUCTION. ANY RLR EQUIPMENT THAT IS IN CONFLICT WITH THE PROPOSED CONSTRUCTION SHALL BE REMOVED BY ITS RESPECTIVE OWNER PRIOR TO THE START OF CONSTRUCTION.

RLR CAMERA LOCATION: US 30 (LINCOLN HWY.) @ IL 50 (CICERO AVE.)

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.

ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT NO ADDITIONAL COST TO THE DEPARTMENT.

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (OR COMBINATION CURB AND GUTTER (THE TYPE SPECIFIED ON THE PLANS)), WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND EXISTING CONDITIONS IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENT SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT IN PART OF THIS CONTRACT.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

WHEN THE MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1½ INCHES (40 mm) WHERE THE SPEED LIMIT IS 40 MPH (80 km/ h) OR LESS AND 1 INCH (25 mm) WHERE THE SPEED LIMIT IS GREATER THAN 40 MPH (80 km/ h). WITH WRITTEN APPROVAL OF THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 mm) MAY BE ALLOWED IF THE EDGE OF THE MILLING MACHINE IS SLOPED A MINIMUM (1:3).

THE CONTRACTOR SHALL REQUEST AND GAIN APPROVAL FROM THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S EXPRESSWAY TRAFFIC OPERATIONS ENGINEER AT WWW.IDOTLCS.COM TWENTY-FOUR (24) HOURS IN ADVANCE OF DAILY LANE, RAMP AND SHOULDER CLOSURES AND SEVEN (7) DAYS IN ADVANCE OF ALL PAVEMENT AND WEEKEND CLOSURES ON ALL FREEWAYS AND/OR EXPRESSWAYS IN DISTRICT ONE. THIS ADVANCE NOTIFICATION IS CALCULATED BASED ON MONDAY THROUGH FRIDAY AND SHALL NOT INCLUDE WEEKENDS OR HOLIDAYS.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL CENTER SUPERVISOR FOR ARTERIALS AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

PERMANENT PAVEMENT MARKINGS SHALL BE THERMOPLASTIC (OF THE EXTRUDED TYPE) AND SHOULD BE PLACED IN ACCORDANCE WITH THE "DISTRICT ONE TYPICAL PAVEMENT MARKINGS" DETAIL. (TC-13)

RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH THE "TYPICAL APPLICATIONS FOR RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" DETAIL. (TC-11)

THE RESIDENT ENGINEER SHALL CONTACT PATRICE HARRIS, AREA TRAFFIC FIELD ENGINEER, AT PATRICE.HARRIS@ILLINOIS.GOV, A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN ACCESS AT ALL TIMES DURING CONSTRUCTION.

THE CONTRACTOR SHALL CONTACT THE ROADSIDE DEVELOPMENT UNIT AT (847) 705-4171 AT LEAST TWO WEEKS PRIOR TO BEGINNING LANDSCAPE AND FORESTRY WORK FOR LAYOUT.

PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT-TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		80% FED 20% STATE 0005	100% STATE 0005			
20100110	TREE REMOVAL ( 6 TO 15 UNITS DIAMETER)	UNIT	100	100				
20100210	TREE REMOVAL ( OVER 15 UNITS DIAMETER)	UNIT	100	100				
20200100	EARTH EXCAVATION	CU YD	7	7				
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	148	148				
25200110	SODDING, SALT TOLERANT	SO YD	148	148				
25200200	SUPPLEMENTAL WATERING	UNIT	3	3				
40600290	BITUMINOUS MATERIALS ( TACK COAT)	POUND	59222	59222				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	198	198				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	856	856				
40604062	HOT-MIX ASPHALT SURFACE COURSE, 1L-9.5, MIX "D", N70	TON	2161	2161				
40605026	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80	TON	10737	10737				
42001300	PROTECTIVE COAT	SO YD	193	193				
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	414	414				

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		80% FED 20% STATE 0005	100% STATE 0005			
42400800	DETECTABLE WARNINGS	SO FT	40	40				
44000156	HOT-MIX ASPHALT SURFACE REMOVAL, 1 3/4"	SO YD	131604	131604				
44000600	SIDEWALK REMOVAL	SO FT	394	394				
44201803	CLASS D PATCHES, TYPE II, 13 INCH	SO YD	659	659				
44201807	CLASS D PATCHES, TYPE III, 13 INCH	SO YD	396	396				
44201809	CLASS D PATCHES, TYPE IV, 13 INCH	SO YD	964	964				
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	976	976				
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	11	11				
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	2	2				
64200108	SHOULDER RUMBLE STRIPS, 8 INCH	FOOT	25252	25252				
*66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	7	7				
*66900530	SOIL DISPOSAL ANALYSIS	EACH	2	2				
*66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	LSUM	1	1				
*66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	LSUM	1	1				
*66901006	REGULATED SUBSTANCES MONITORING	CAL DA	15	15				

REV-SEP

\* SPECIALTY ITEMS  
 NON PARTICIPATING ITEMS

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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES  
 FAP 353 /US 30—IL 43 (HARLEM AVE.) TO IL 50 (CICERO AVE.)**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	2020-130-RS	COOK	41	3
CONTRACT NO. 62M08				
SCALE: NONE		SHEET OF SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		80% FED 20% STATE 0005	100% STATE 0005			
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	12	12				
67100100	MOBILIZATION	L SUM	1	1				
70100420	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	EACH	1	1				
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	1				
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1				
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1				
70107025	CHANGEABLE MESSAGE SIGN	CAL DA	30	30				
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	841	841				
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	71160	71160				
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	2812	2812				
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	2353	2353				
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	670	670				
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	702	702				

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		80% FED 20% STATE 0005	100% STATE 0005			
*78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	841	841				
*78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	71160	71160				
*78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	2812	2812				
*78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	2353	2353				
*78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	670	670				
*78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	702	702				
*78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	1874	1874				
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	1687	1687				
*88600600	DETECTOR LOOP REPLACEMENT	FOOT	1896	1896				
*K0026700	TREE CARE	EACH	50	50				
*K0029624	WEED CONTROL, TEASEL	GALLON	2.5	2.5				
*X0100018	TREE REMOVAL (UNDER 6 UNITS DIAMETER)	UNIT	50	50				

REV-SEP

\* SPECIALTY ITEMS  
 NON PARTICIPATING ITEMS

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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>SUMMARY OF QUANTITIES</b>			
<b>FAP 353 /US 30—IL 43 (HARLEM AVE.) TO IL 50 (CICERO AVE.)</b>			
SCALE: NONE	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	2020-130-RS	COOK	41	4
CONTRACT NO. 62M08			ILLINOIS FED. AID PROJECT	

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		80% FED 20% STATE	100% STATE				
X0320050	CONSTRUCTION LAYOUT (SPECIAL)	L SUM	1	1					
X2020110	GRADING AND SHAPING SHOULDERS	UNIT	244	244					
X4400501	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT EQUAL TO OR LESS THAN 10 FEET	FOOT	100	100					
X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	1500		1500				
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	6	6					
X7011015	TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS)	L SUM	1	1					
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	885	885					
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	11		11				
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	205.6	205.6					
Z0033700	LONGITUDINAL JOINT SEALANT	FOOT	40219	40219					
Z0076600	TRAINEES	HOURS	500	500					
Z0076604	TRAINEES - TRAINING PROGRAM GRADUATE	HOURS	500	500					

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		80% FED 20% STATE	100% STATE				
60250200	CATCH BASIN TO BE ADJUSTED	EACH	8	8					
60618300	CONCRETE MEDIAN SURFACE, 4 IN.	SQ FT	300	300					
63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6-FOOT POST	FOOT	225	225					
63100167	TRAFFIC BARRIER TERMINAL, TYPE 1, (SPECIAL), TANGENT	EACH	2	2					
63200310	GUARDRAIL REMOVAL	FOOT	225	225					
X4402020	CONCRETE MEDIAN SURFACE REMOVAL	SQ FT	300	300					

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REV-SEP

\* SPECIALTY ITEMS  
 NON PARTICIPATING ITEMS

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUMMARY OF QUANTITIES**  
**FAP 353 /US 30 (LINCOLN HWY.) TO IL 43 (HARLEM AVE.)**  
 SCALE: NONE SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	2020-130-R5	COOK	41	5
CONTRACT NO. 62M08			ILLINOIS FED. AID PROJECT	

**LEGEND:**

- ① EXISTING P.C.C. PAVEMENT ±9"
  - ② EXISTING HOT-MIX ASPHALT SURFACE ±4" (AFTER MILLING)
  - ③ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
  - ④ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.12
  - ⑤ EXISTING AGGREGATE SHOULDER
  - ⑥ EXISTING CONCRETE BARRIER MEDIAN
  - ⑦ EXISTING LANDSCAPE MEDIAN
  - ⑧ EXISTING STABILIZED SUB-BASE
  - ⑨ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 1¾"
  - ⑩ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80, 1¾"
  - ⑪ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", 1¾"
  - ⑫ PROPOSED GRADING AND SHAPING SHOULDERS
  - ⑬ PROPOSED AGGREGATE WEDGE SHOULDERS, TYPE B
  - ⑭ PROPOSED SHOULDER RUMBLE STRIPS, (8" WIDE)
- R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)

MIXTURE REQUIREMENTS		
MIXTURE TYPE	AIR VOIDS @ Ndes	QMP
PATCHING		
CLASS "D" PATCHES (HMA BINDER, IL-19MM)	4% @ 70 GYR.	QC / QA
PAVEMENT RESURFACING		
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80, 1¾"	3.5% @ 80 GYR.	PPF
SHOULDER RESURFACING		
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 1¾"	4% @ 70 GYR.	QCP
QMP DESIGNATION: QUALITY CONTROL FOR PERFORMANCE (QCP); QUALITY CONTROL/ QUALITY ASSURANCE (QC/ QA); PAY FOR PERFORMANCE (PPF)		

**NOTES:**

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE QUANTITIES IS 112 LBS./SQ. YD./ IN.

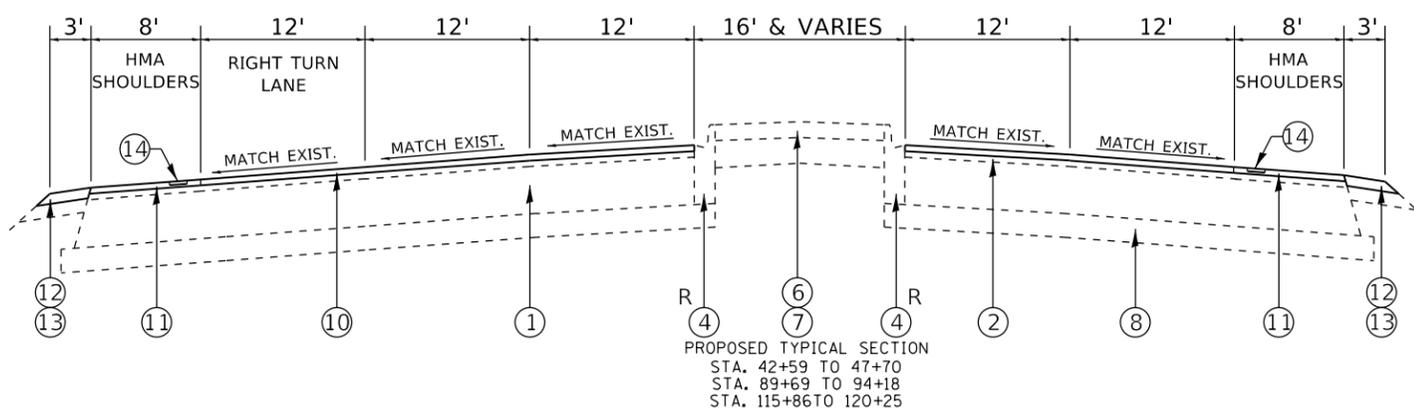
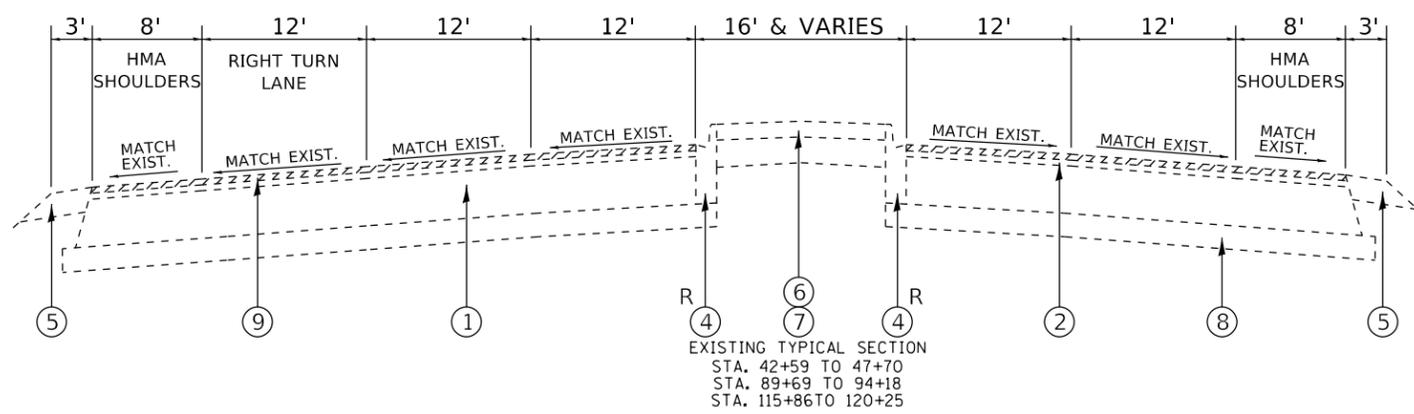
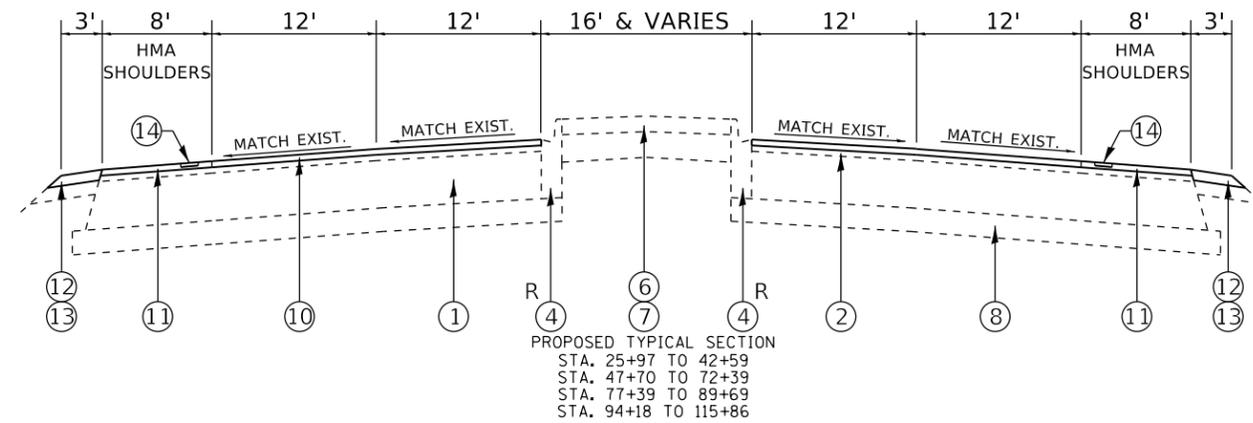
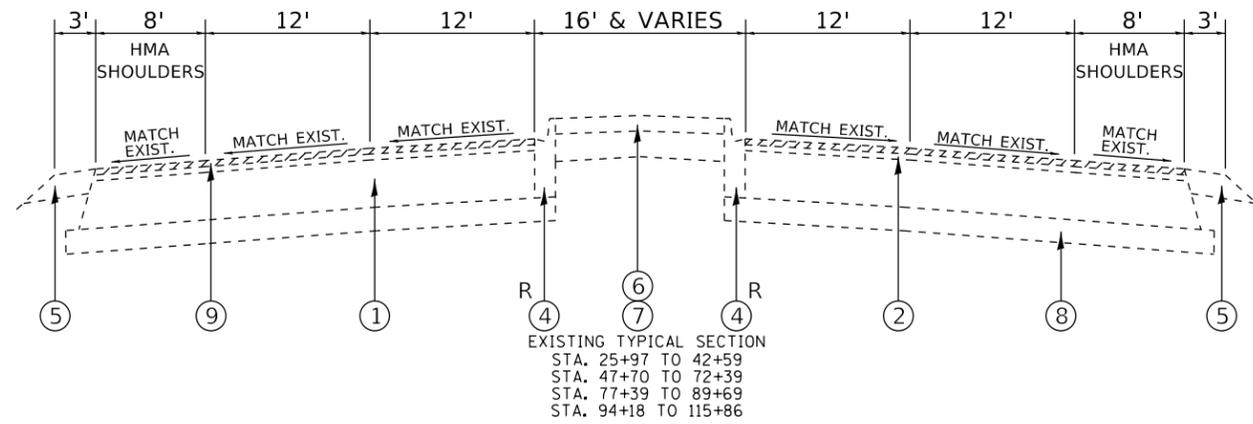
THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY SPECIAL PROVISIONS.

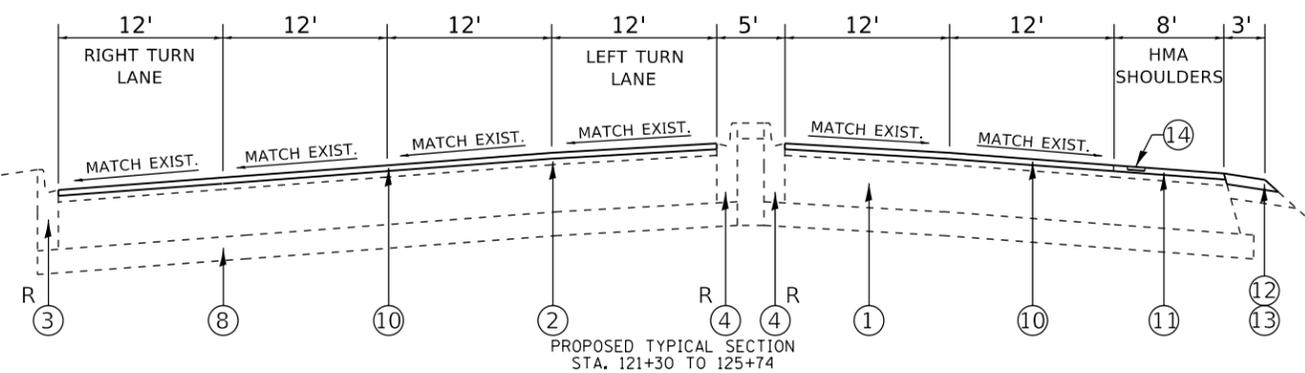
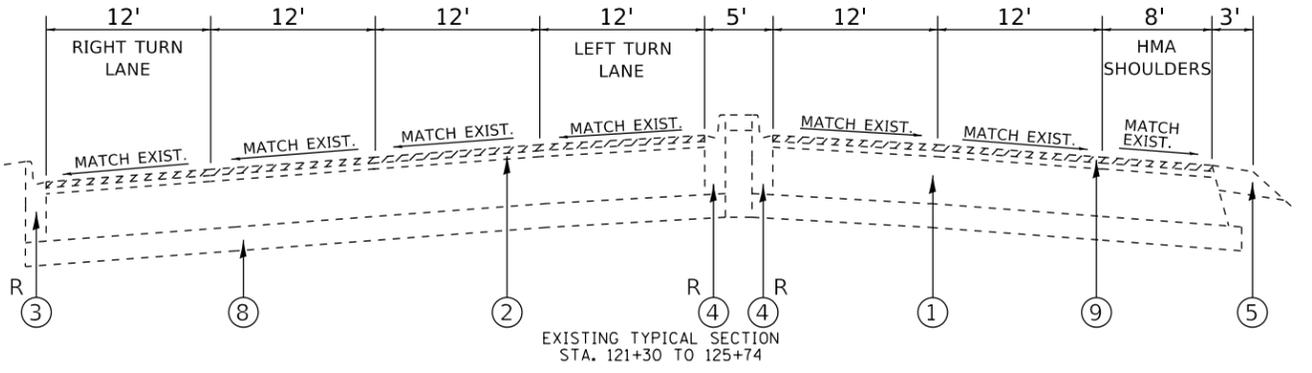
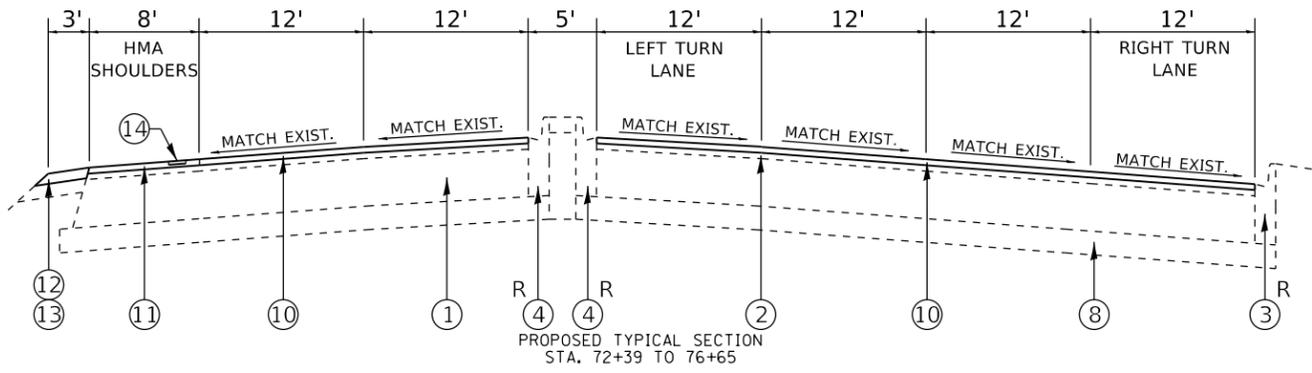
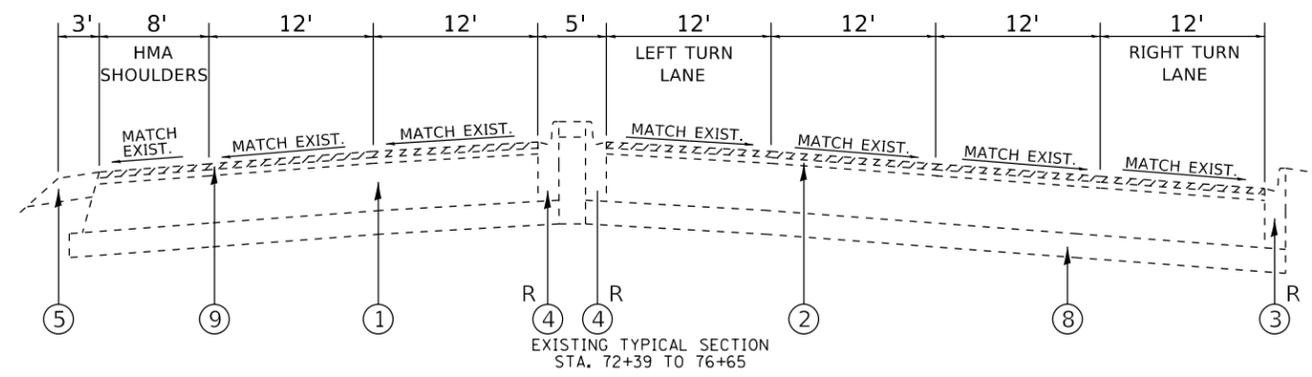
THE MILLING SHALL BE DONE PRIOR TO PATCHING

QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE

THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE MILLED SURFACE

FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS





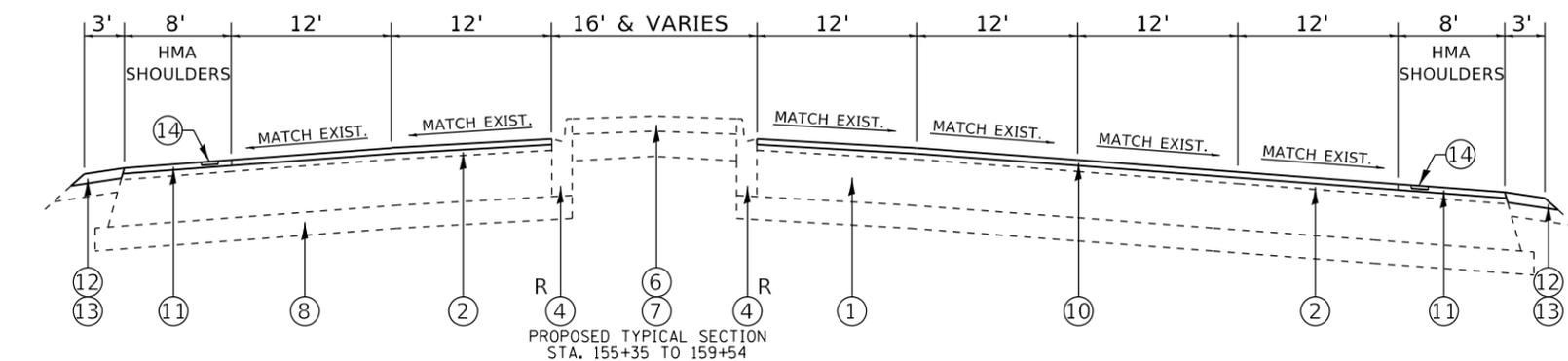
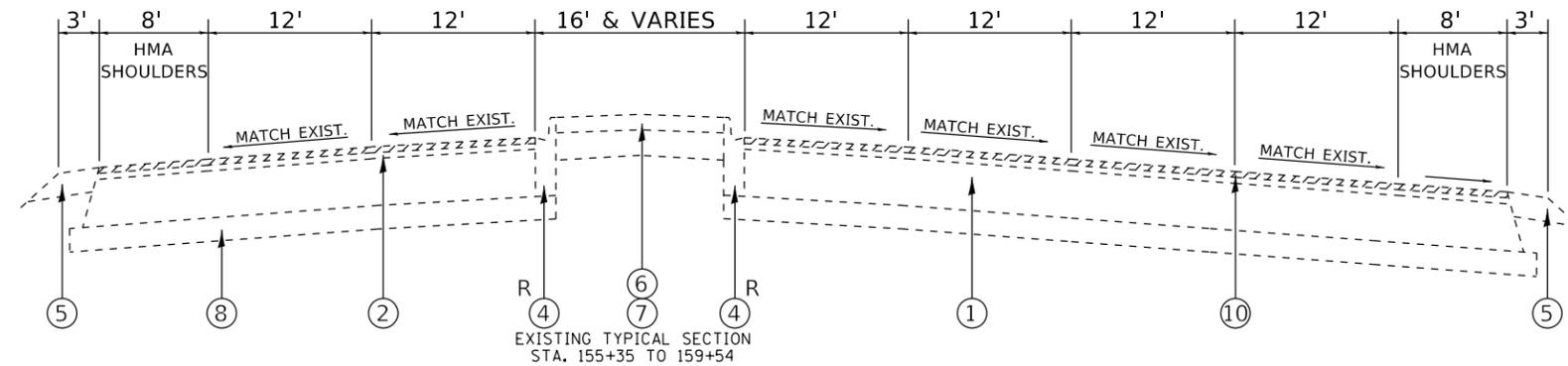
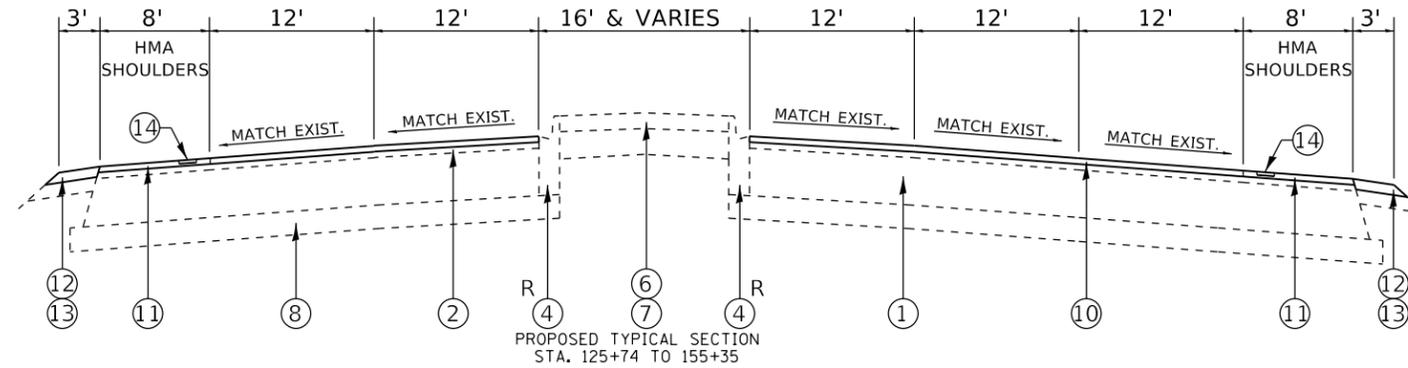
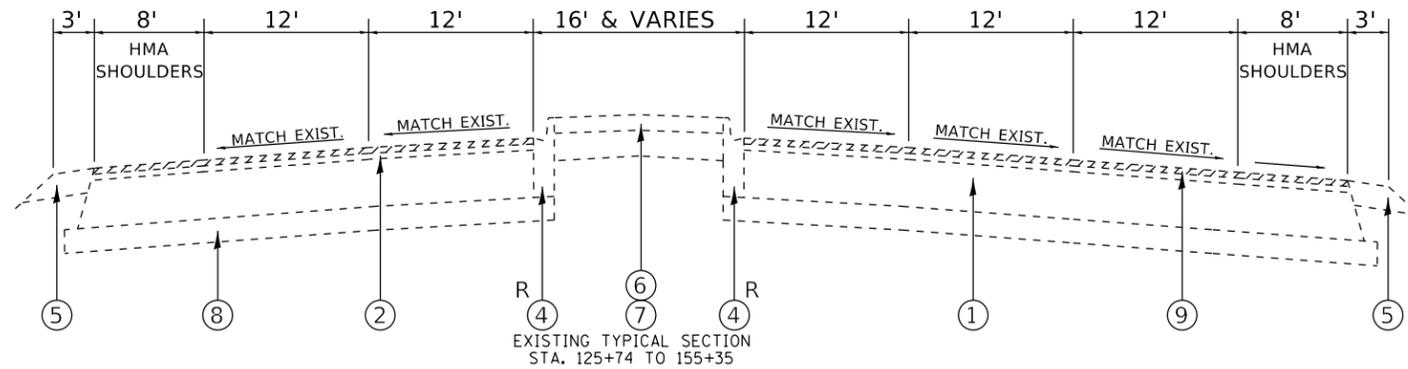
**LEGEND:**

- ① EXISTING P.C.C. PAVEMENT ±9"
  - ② EXISTING HOT-MIX ASPHALT SURFACE ±4" (AFTER MILLING)
  - ③ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
  - ④ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.12
  - ⑤ EXISTING AGGREGATE SHOULDER
  - ⑥ EXISTING CONCRETE BARRIER MEDIAN
  - ⑦ EXISTING LANDSCAPE MEDIAN
  - ⑧ EXISTING STABILIZED SUB-BASE
  - ⑨ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 1¾"
  - ⑩ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80, 1¾"
  - ⑪ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", 1¾"
  - ⑫ PROPOSED GRADING AND SHAPING SHOULDERS
  - ⑬ PROPOSED AGGREGATE WEDGE SHOULDERS, TYPE B
  - ⑭ PROPOSED SHOULDER RUMBLE STRIPS, (8" WIDE)
- R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)

FILE NAME =	USER NAME = steepp	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>EXISTING AND PROPOSED TYPICAL SECTIONS FAP 353 /US 30—IL 43 (HARLEM AVE.) TO IL 50 (CICERO AVE.)</b>			F.A.P. R.E. =	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\0153020\DRAWING\Design\0153020-sht-plan.dgn		CHECKED -	REVISED -		353	2020-130-RS	COOK	41	7			
PLOT SCALE = 100.0000' / in.		DATE -	REVISED -		CONTRACT NO. 62M08			ILLINOIS FED. AID PROJECT				
PLOT DATE = 12/11/2020					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.					

**LEGEND:**

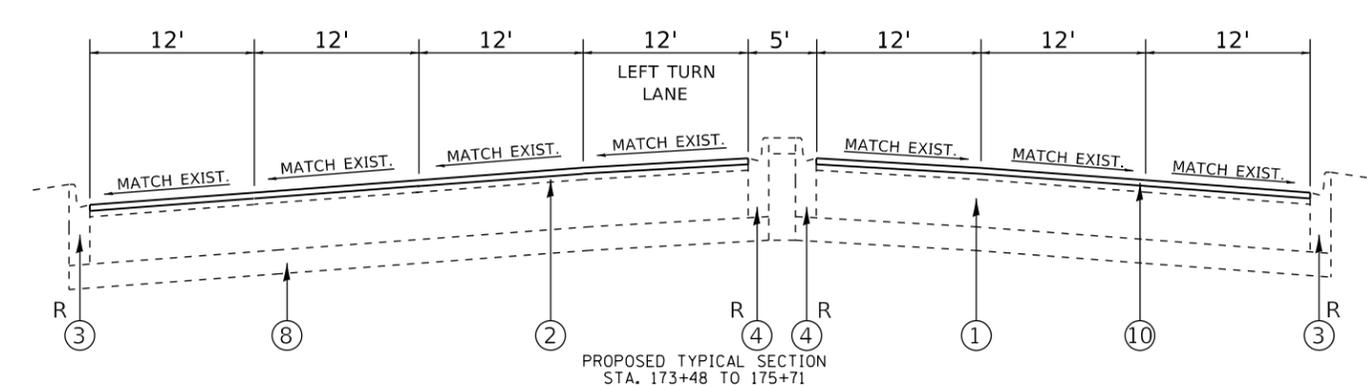
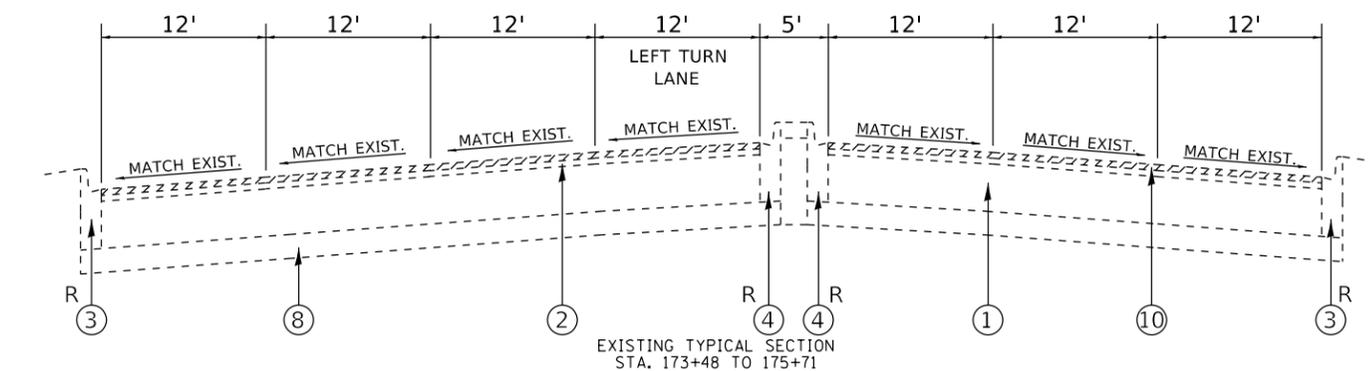
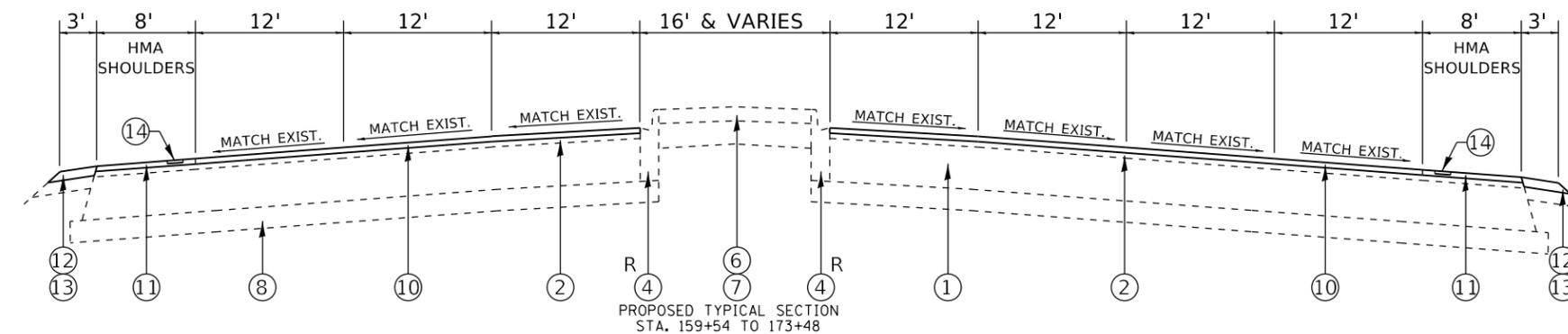
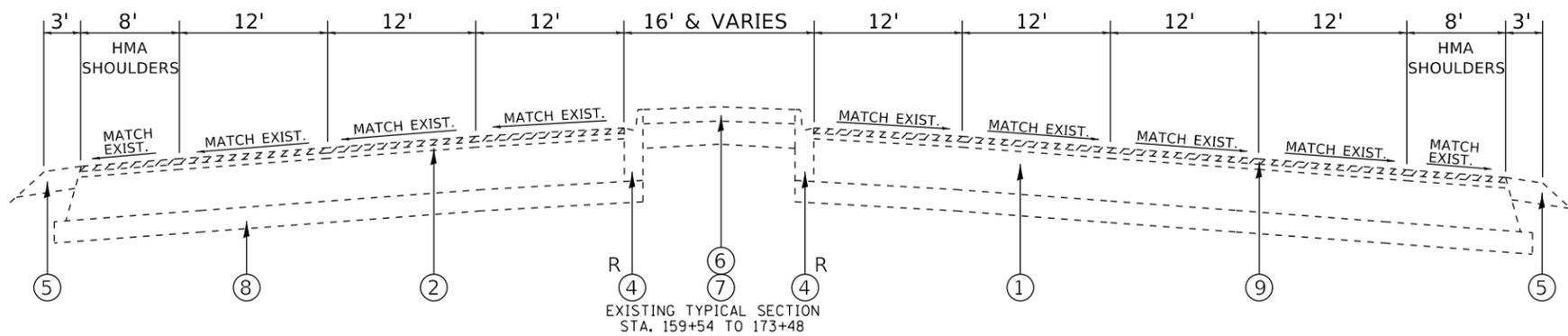
- ① EXISTING P.C.C. PAVEMENT ±9"
  - ② EXISTING HOT-MIX ASPHALT SURFACE ±4" (AFTER MILLING)
  - ③ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
  - ④ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.12
  - ⑤ EXISTING AGGREGATE SHOULDER
  - ⑥ EXISTING CONCRETE BARRIER MEDIAN
  - ⑦ EXISTING LANDSCAPE MEDIAN
  - ⑧ EXISTING STABILIZED SUB-BASE
  - ⑨ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 1¾"
  - ⑩ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80, 1¾"
  - ⑪ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", 1¾"
  - ⑫ PROPOSED GRADING AND SHAPING SHOULDERS
  - ⑬ PROPOSED AGGREGATE WEDGE SHOULDERS, TYPE B
  - ⑭ PROPOSED SHOULDER RUMBLE STRIPS, (8" WIDE)
- R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)



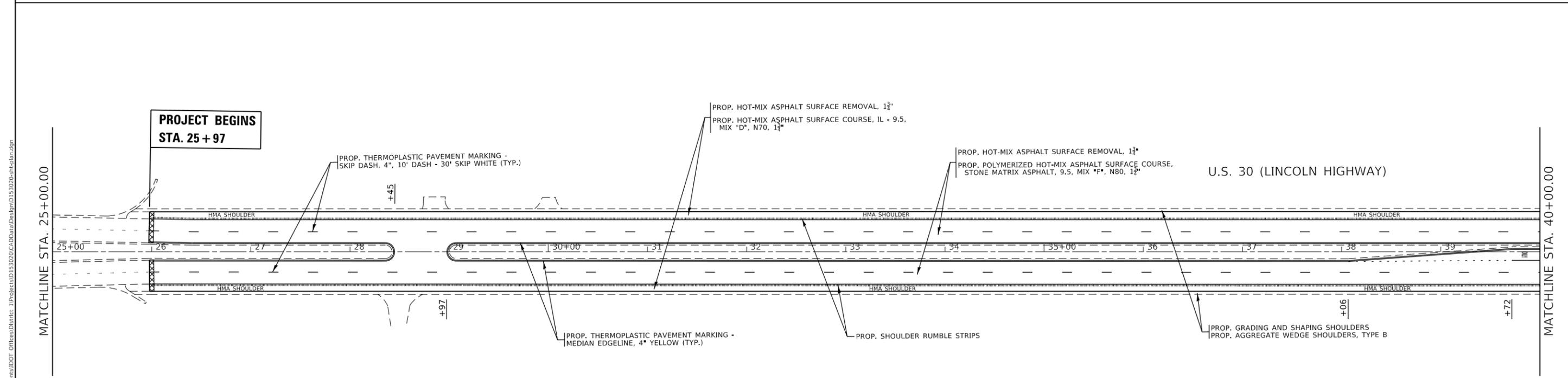
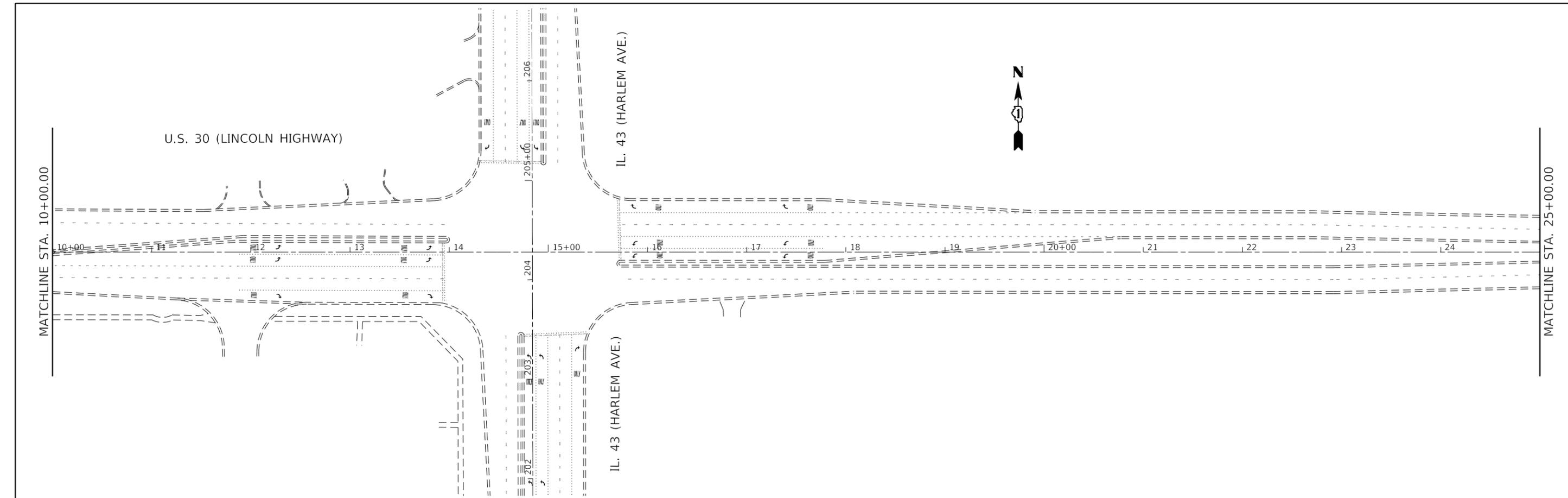
FILE NAME =	USER NAME = steedpo	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>EXISTING AND PROPOSED TYPICAL SECTIONS FAP 353 /US 30—IL 43 (HARLEM AVE.) TO IL 50 (CICERO AVE.)</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
						353	2020-130-RS	COOK	41	8	
						CONTRACT NO. 62M08					
						ILLINOIS FED. AID PROJECT					
PLOT SCALE = 100.0000' / 1" /		CHECKED -		SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA. TO STA.			
PLOT DATE = 12/11/2020		DATE -									

**LEGEND:**

- ① EXISTING P.C.C. PAVEMENT ±9"
- ② EXISTING HOT-MIX ASPHALT SURFACE ±4" (AFTER MILLING)
- ③ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ④ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE M-2.12
- ⑤ EXISTING AGGREGATE SHOULDER
- ⑥ EXISTING CONCRETE BARRIER MEDIAN
- ⑦ EXISTING LANDSCAPE MEDIAN
- ⑧ EXISTING STABILIZED SUB-BASE
- ⑨ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 1¾"
- ⑩ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, MIX "F", N80, 1¾"
- ⑪ PROPOSED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", 1¾"
- ⑫ PROPOSED GRADING AND SHAPING SHOULDERS
- ⑬ PROPOSED AGGREGATE WEDGE SHOULDERS, TYPE B
- ⑭ PROPOSED SHOULDER RUMBLE STRIPS, (8" WIDE)
- R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)



FILE NAME =	USER NAME = steedp	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>EXISTING AND PROPOSED TYPICAL SECTIONS FAP 353 /US 30—IL 43 (HARLEM AVE.) TO IL 50 (CICERO AVE.)</b>			F.A.P. R.T.E. =	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\planroom.dot.illinois.gov\PIDOT\Documents\IDOT Offices\District 1\Projects\0153020\DRAWING\Design\0153020-sht-plan.dgn	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -					353	2020-130-RS	COOK	41	9
PLOT DATE = 12/11/2020	DATE -	REVISED -	REVISED -		CONTRACT NO. 62M08			ILLINOIS FED. AID PROJECT				
					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.				



**LEGEND:**  
 HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

MODEL: Default  
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 USER: steedpa  
 DATE: 12/11/2020

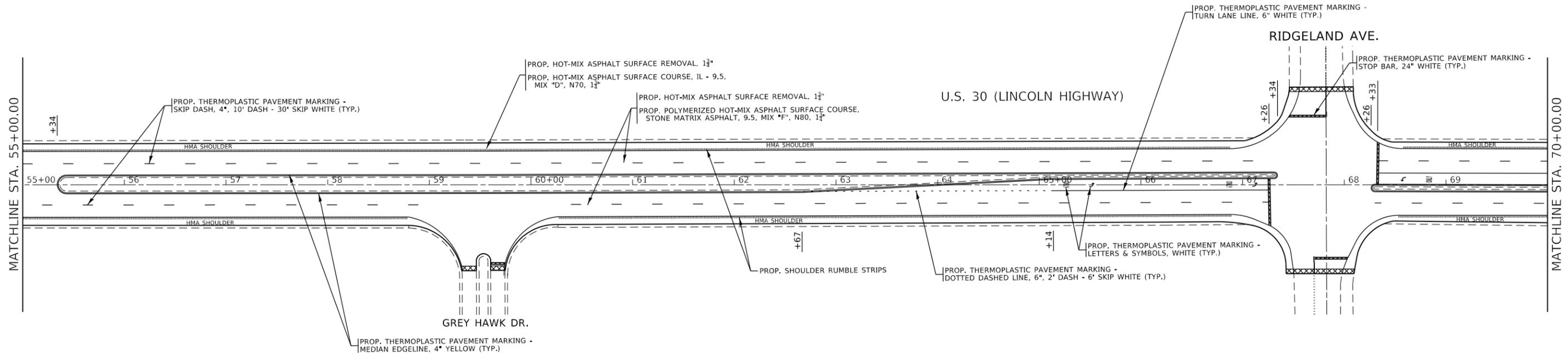
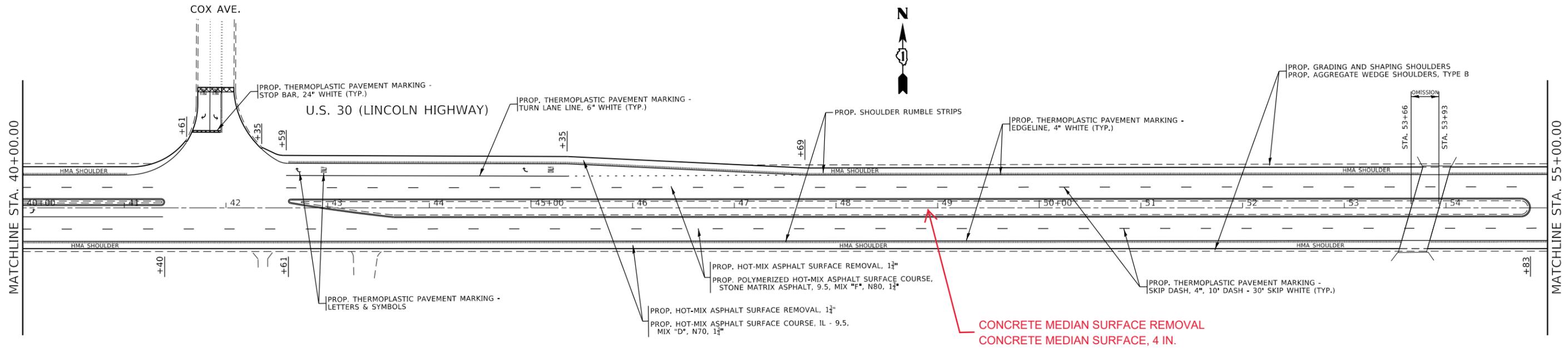
USER NAME = steedpa	DESIGNED -	REVISED -
PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/11/2020	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED ROADWAY PLAN  
 FAP 353 /US 30—IL 43 (HARLEM AVE.) TO IL 50 (CICERO AVE.)**

SCALE: 1" = 50'    SHEET    OF    SHEETS    STA.    TO    STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	2020-130-R5	COOK	41	10
CONTRACT NO. 62M08				
ILLINOIS FED. AID PROJECT				



**LEGEND:**  
 [Hatched Box] HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

MODEL: Default  
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PLOT DATE = 12/11/2020	CHECKED -	REVISED -
	DATE -	REVISED -

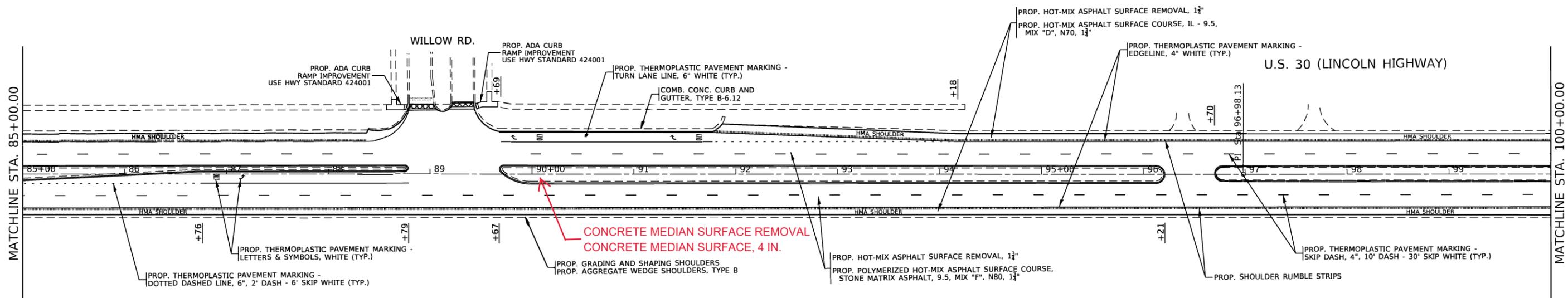
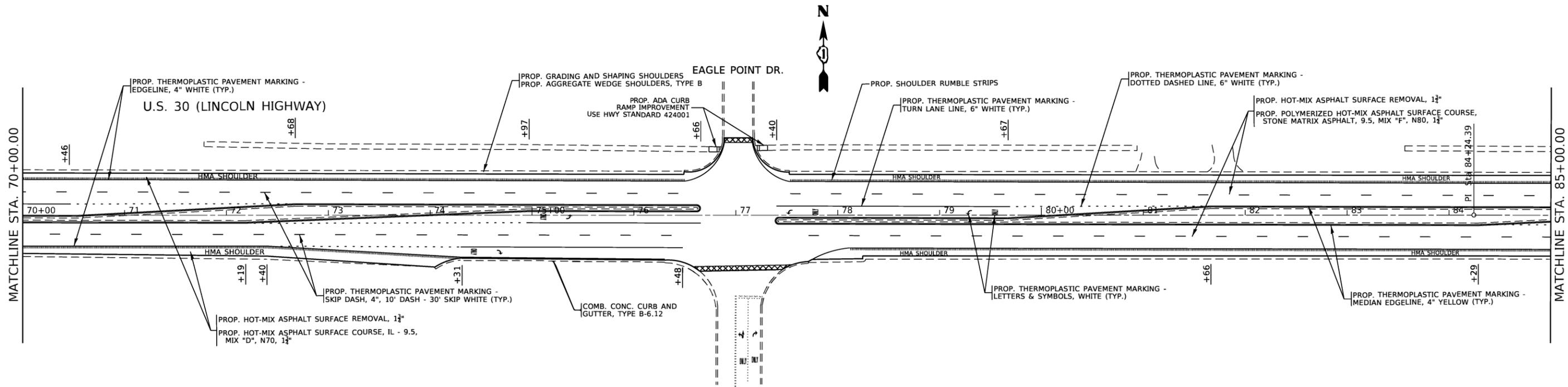
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED ROADWAY PLAN**  
**FAP 353 /US 30—IL 43 (HARLEM AVE.) TO IL 50 (CICERO AVE.)**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	2020-130-R5	COOK	41	11
CONTRACT NO. 62M08				
ILLINOIS FED. AID PROJECT				

SCALE: 1" = 50'    SHEET    OF    SHEETS    STA.    TO    STA.

MODEL: Default  
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**LEGEND:**

☒ HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

USER NAME = steedpa	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/11/2020	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED ROADWAY PLAN  
 FAP 353 /US 30—IL 43 (HARLEM AVE.) TO IL 50 (CICERO AVE.)**

SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 353	SECTION 2020-130-R5	COUNTY COOK	TOTAL SHEETS 41	SHEET NO. 12
CONTRACT NO. 62M08			ILLINOIS FED. AID PROJECT	



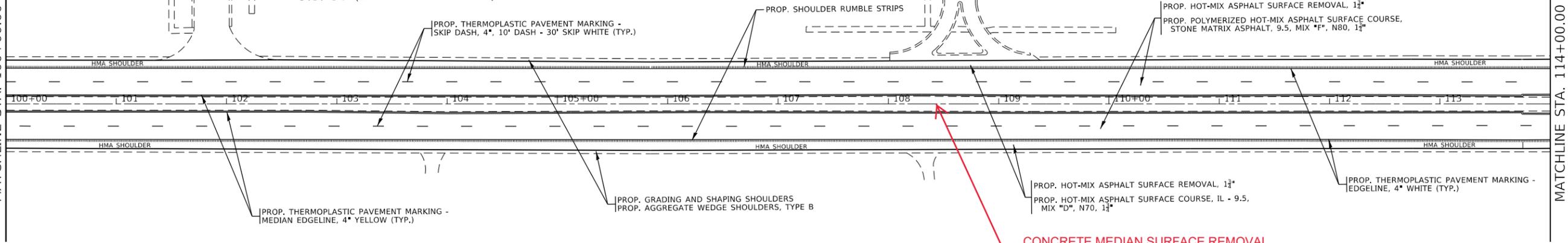
MATCHLINE STA. 100+00.00

MATCHLINE STA. 114+00.00

HOGER DR.

REGENT DR.

U.S. 30 (LINCOLN HIGHWAY)



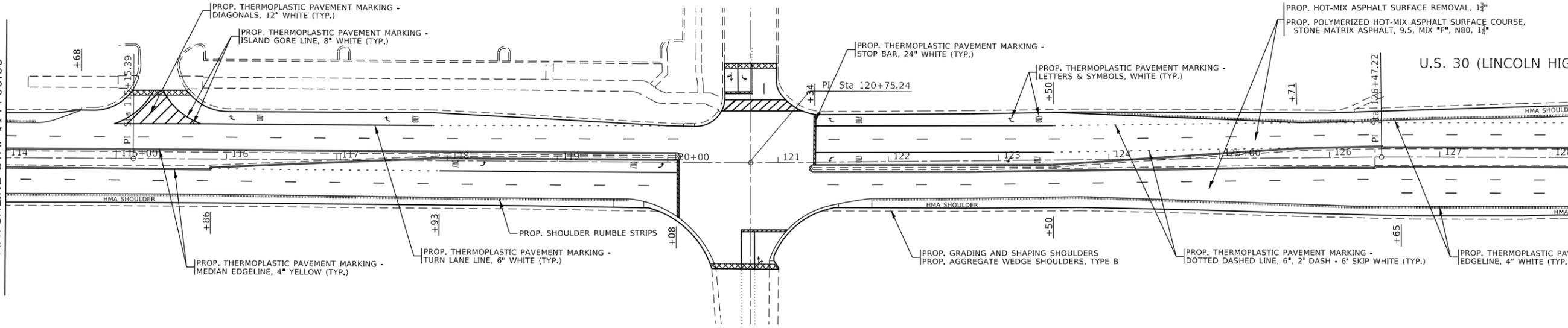
CONCRETE MEDIAN SURFACE REMOVAL  
CONCRETE MEDIAN SURFACE, 4 IN.

MATCHLINE STA. 114+00.00

MATCHLINE STA. 129+00.00

CENTRAL AVE.

U.S. 30 (LINCOLN HIGHWAY)



**LEGEND:**

☒ HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

MODEL: Default  
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PROJECT: 153020\CADD\Drawings\DOT - Office\Drawings\153020-sh-4-bar.dwg

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PLOT DATE = 12/11/2020	CHECKED -	REVISED -
	DATE -	REVISED -

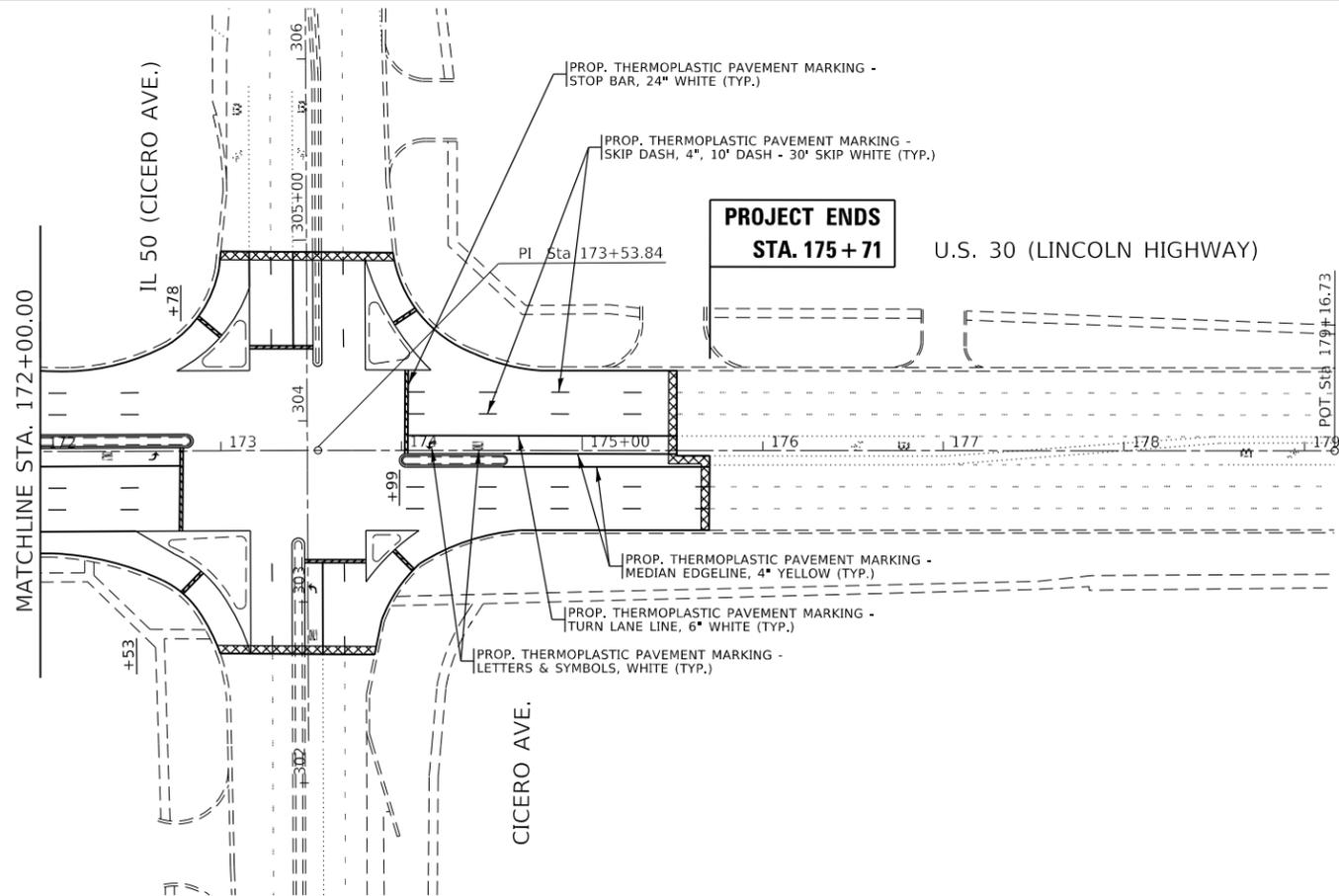
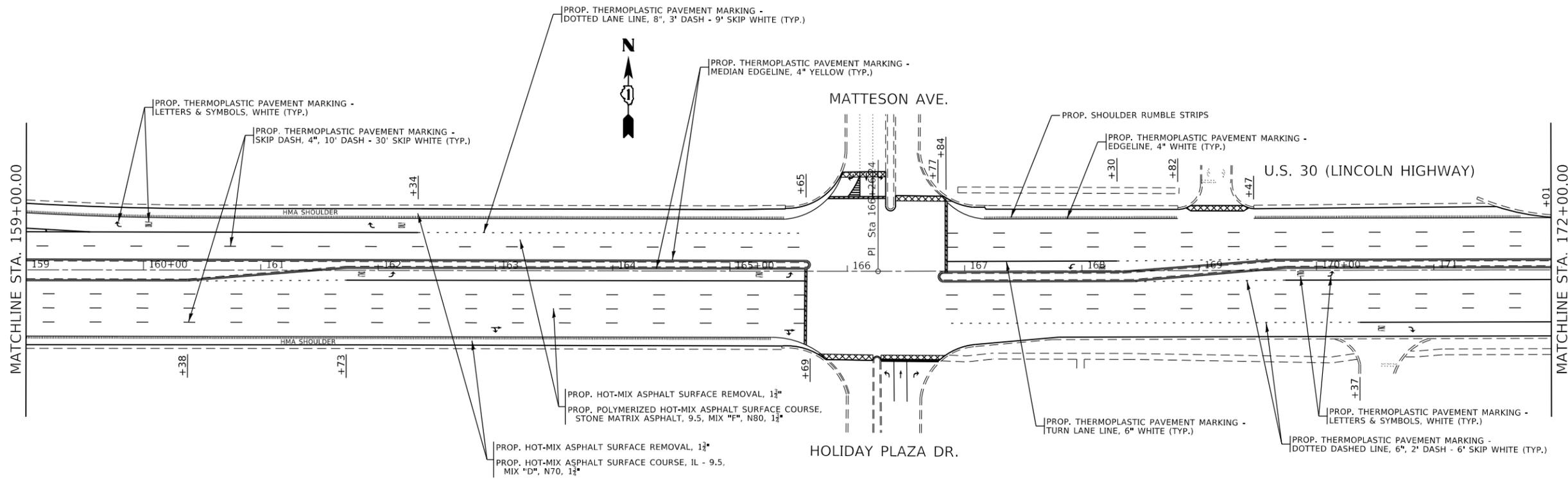
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED ROADWAY PLAN  
FAP 353 /US 30—IL 43 (HARLEM AVE.) TO IL 50 (CICERO AVE.)**

SCALE: 1" = 50'    SHEET    OF    SHEETS    STA.    TO    STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	2020-130-R5	COOK	41	13
CONTRACT NO. 62M08				
ILLINOIS FED. AID PROJECT				





**LEGEND:**

☒ HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT

MODEL: Default  
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PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 12/11/2020	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

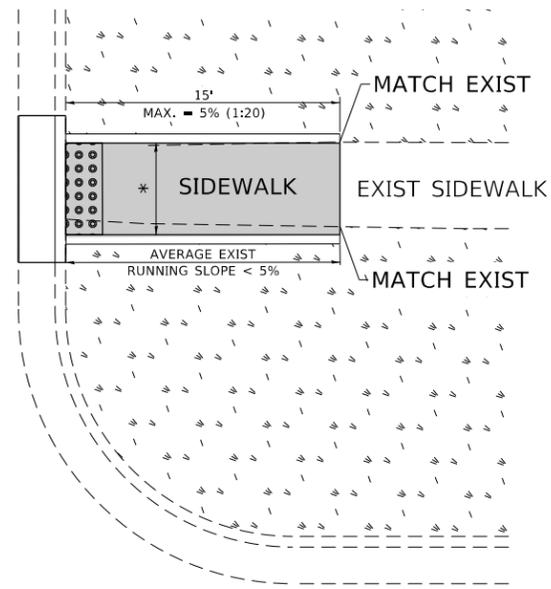
**EXISTING AND PROPOSED ROADWAY PLAN  
FAP 353 /US 30—IL 43 (HARLEM AVE.) TO IL 50 (CICERO AVE.)**

SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

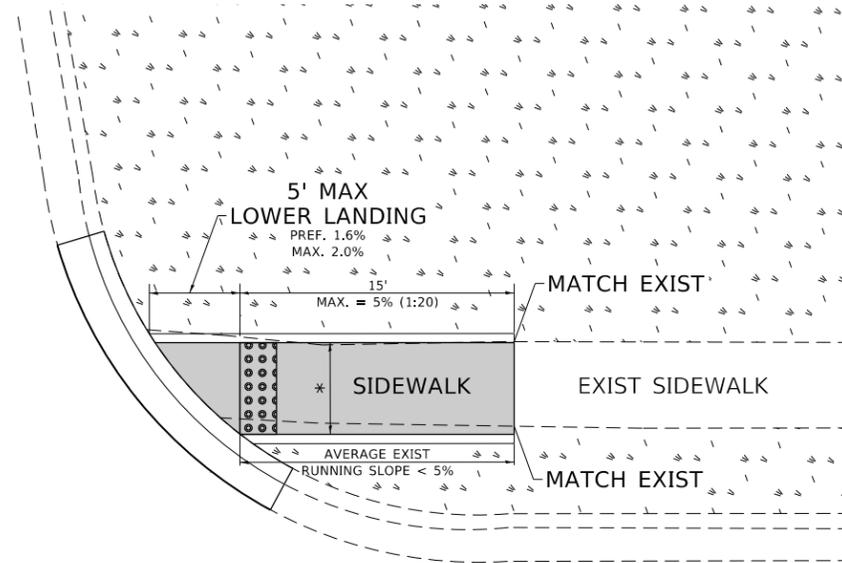
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353	2020-130-R5	COOK	41	15
CONTRACT NO. 62M08				
ILLINOIS FED. AID PROJECT				

# ADA DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS W/ EXIST. 5% OR LESS RUN. SLOPE

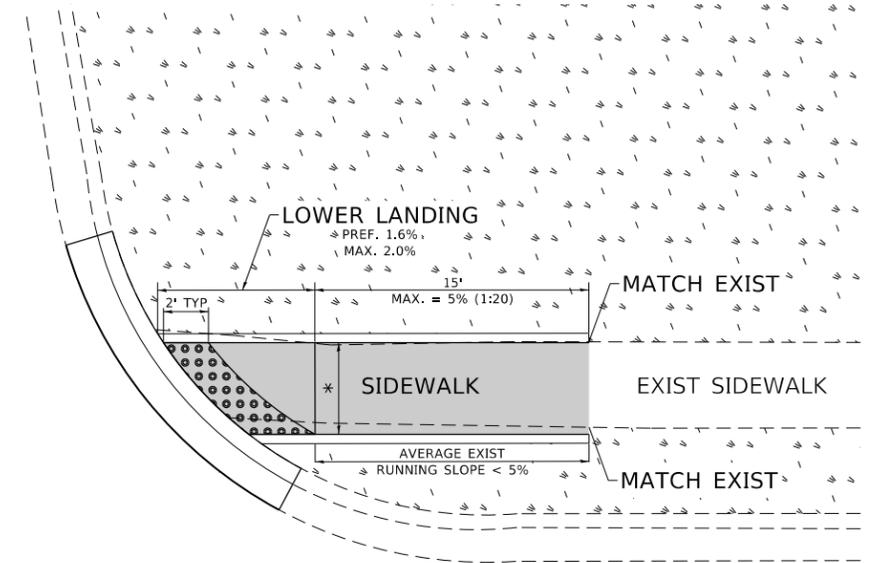
**PD-01A**



**PD-01B**



**PD-01C**



**LEGEND**

- PROPOSED SIDE CURB
- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS

**CONSTRUCTION NOTES:**

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- \* MATCH EXISTING SIDEWALK WIDTH

MODEL: Default  
FILE: \\nrcs\pub\baronm\dat\illinois.gov\RW\DOT\Documents\DOT\_Offices\Dir\dtc\_1\Project\ID153020\Cadd\Drawings\Design\DESIGN.dgn

USER NAME = steedpa	DESIGNED -	REVISED -
	DRAWN - R. LEDEZMA	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/4/2020	DATE - 10/02/2019	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PROJECT DETAIL FOR SINGLE PERPENDICULAR CURB RAMPS  
(PD-01)**

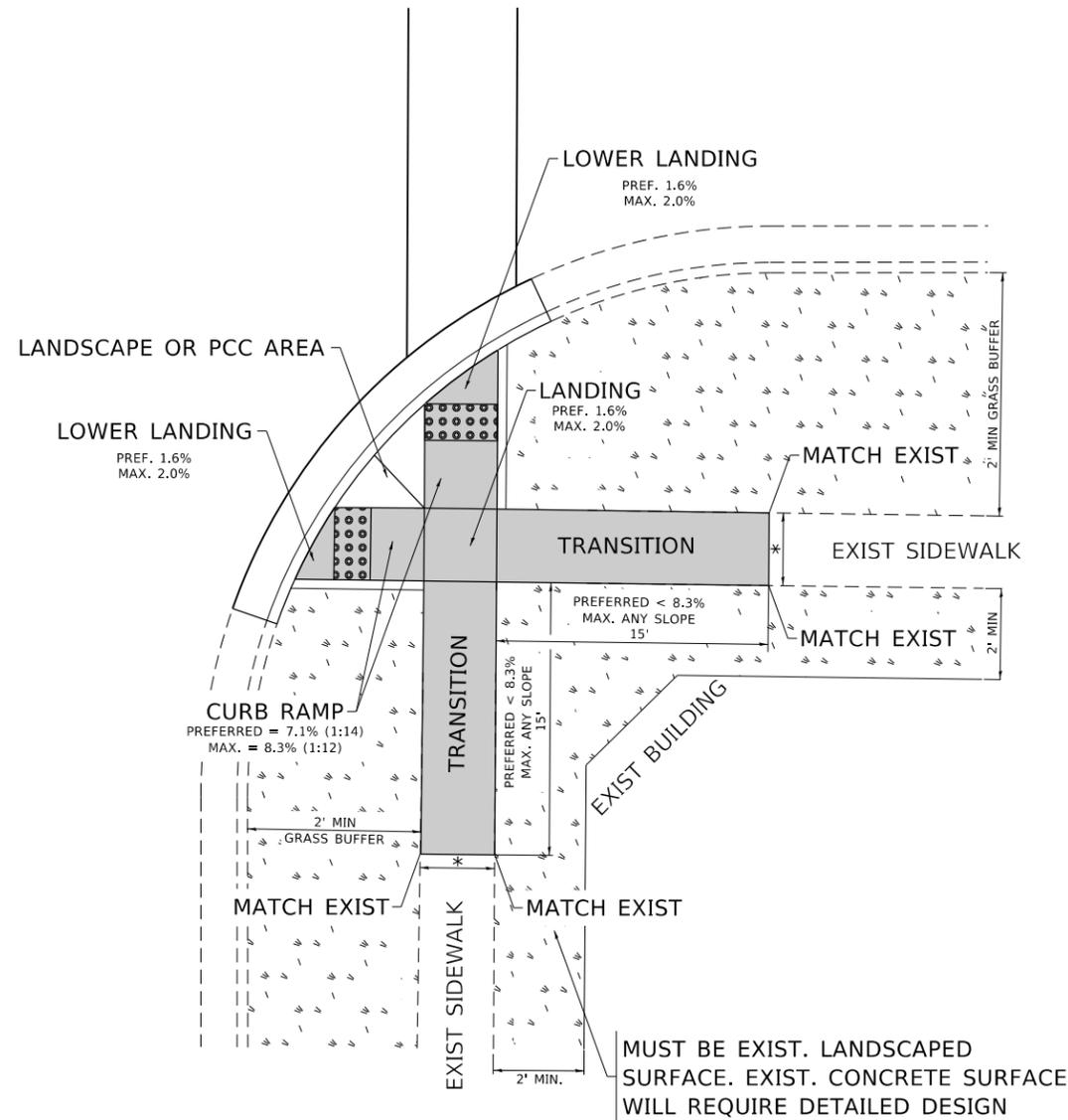
SCALE: NONE    SHEET    OF    SHEETS    STA.    TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	2020-130-RS	COOK	41	16
PD-01		CONTRACT NO. 62M08		
		ILLINOIS FED. AID PROJECT		

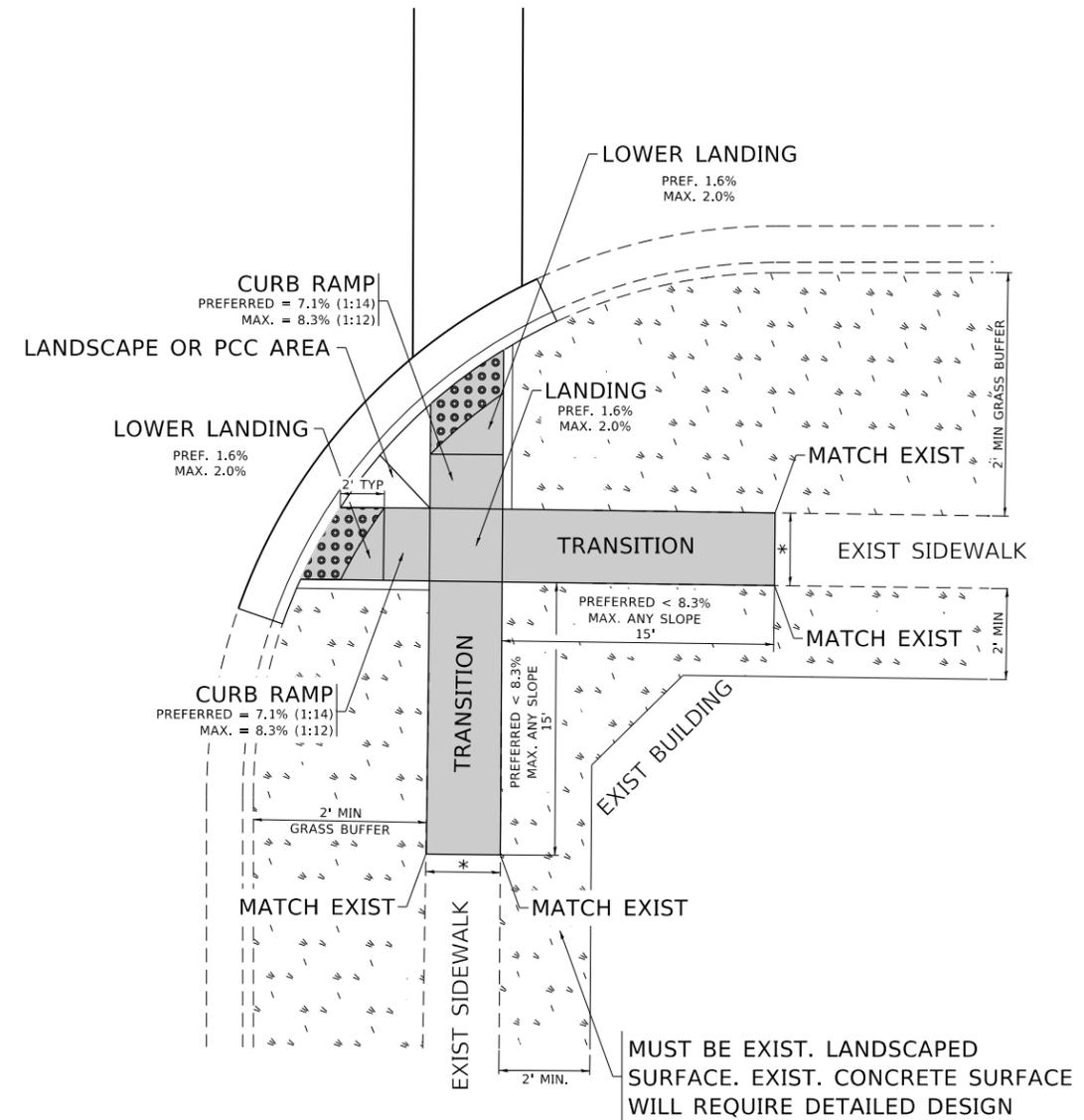


# ADA DETAIL FOR DOUBLE PERPENDICULAR CURB RAMPS

**PD-03A**



**PD-03B**



**LEGEND**

- PROPOSED SIDE CURB
- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS

**CONSTRUCTION NOTES:**

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- \* MATCH EXISTING SIDEWALK WIDTH

MODEL: Default  
FILE: h:\mhc\p\pub\harcam\dat\illinois.gov\p\WIDOT\Documents\DOT\_Offices\Illinet\1\Project\ID153020\CaddData\Design\DES03A.dgn

USER NAME = steedpa	DESIGNED -	REVISED -
	DRAWN - R. LEDEZMA	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/4/2020	DATE - 10/02/2019	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PROJECT DETAIL FOR DOUBLE PERPENDICULAR CURB RAMPS  
(PD-03)**

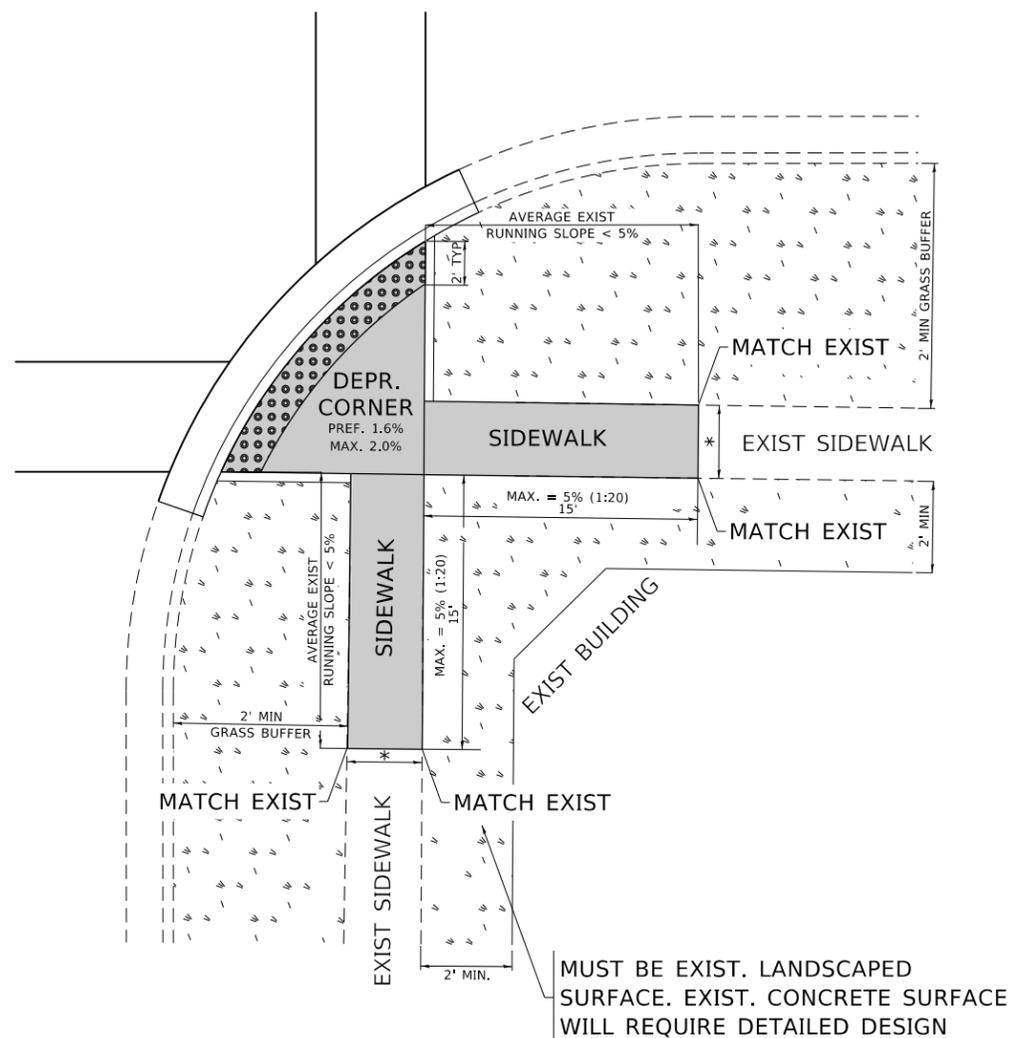
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F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	2020-130-RS	COOK	41	18
PD-03		CONTRACT NO. 62M08		
ILLINOIS FED. AID PROJECT				

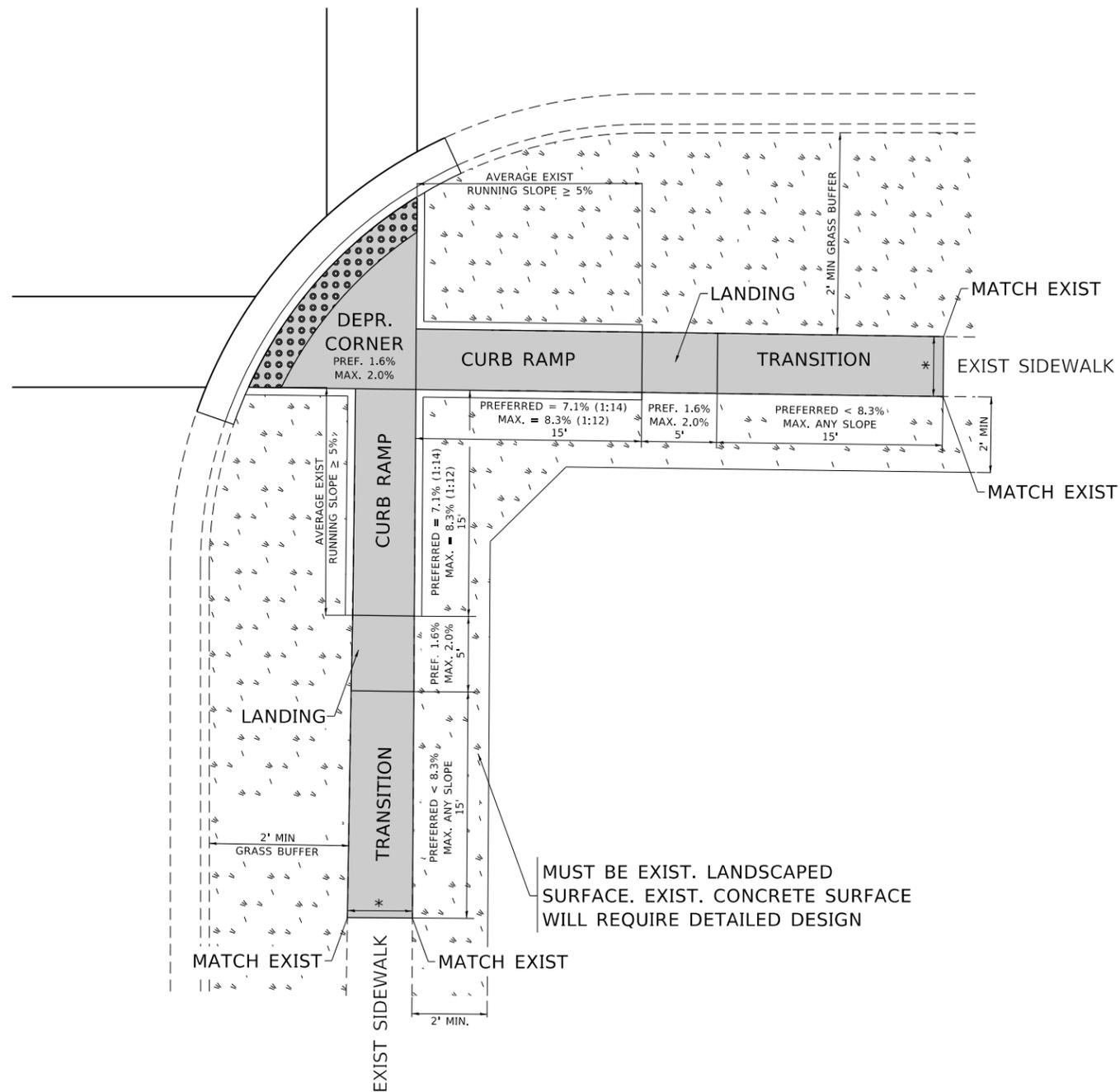


# ADA DETAIL FOR DEPRESSED CORNER CURB RAMPS

**PD-05A**



**PD-05B**



**LEGEND**

- PROPOSED SIDE CURB
- EXIST. GRASS
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS

**CONSTRUCTION NOTES:**

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- \* MATCH EXISTING SIDEWALK WIDTH

MODEL: Default  
FILE: h:\m1111\p101\planroom\ada\illinois\p05\PD05A\Documents\DOT\_Offices\Drawings\1\Project\137020\Cadd\Drawings\Design\DES05A.dgn

USER NAME = steedpa	DESIGNED -	REVISED -
	DRAWN - R. LEDEZMA	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/4/2020	DATE - 10/02/2019	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

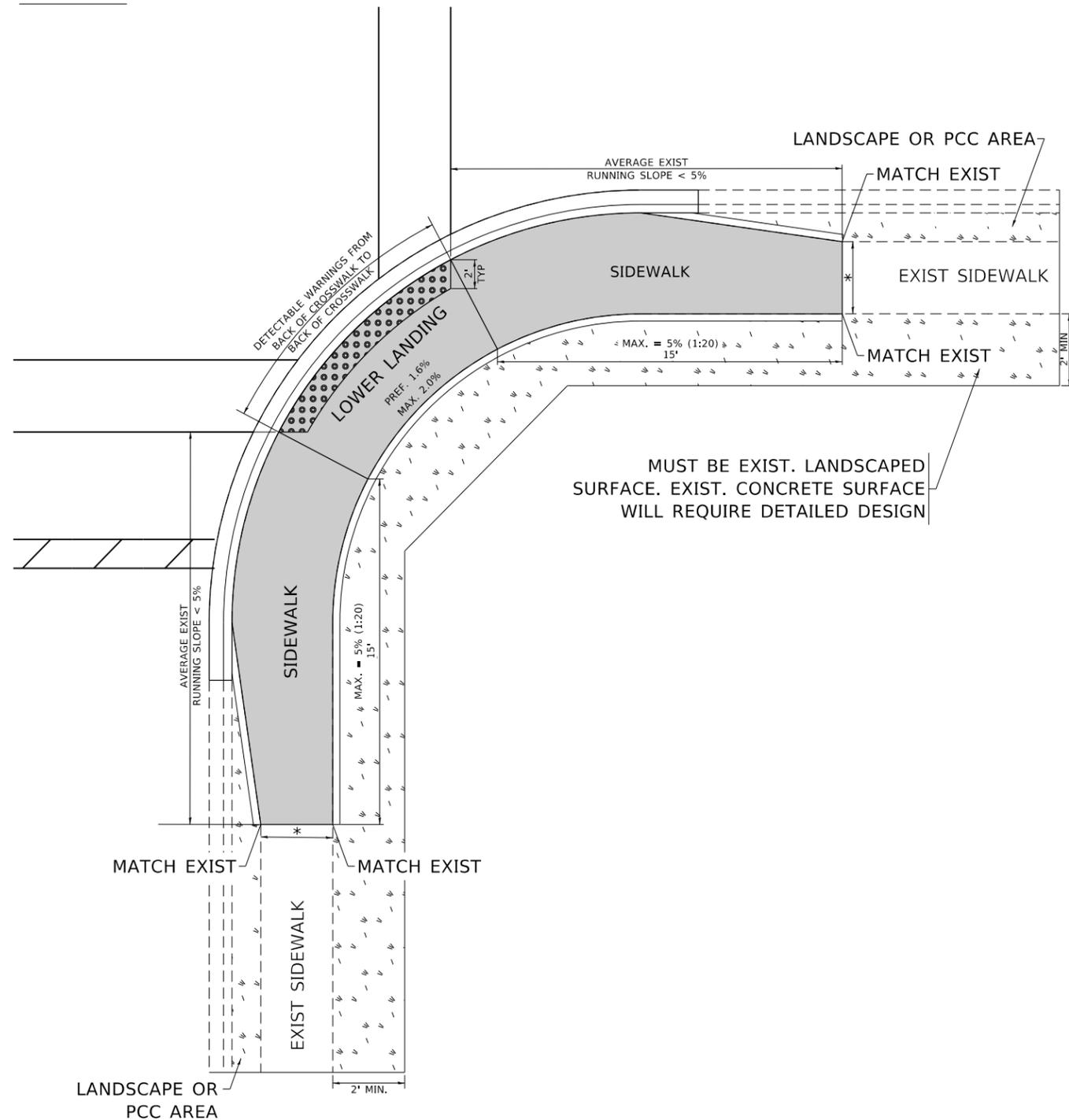
**PROJECT DETAIL FOR DEPRESSED CORNER CURB RAMPS  
(PD-05)**

SCALE: NONE    SHEET    OF    SHEETS    STA.    TO STA.

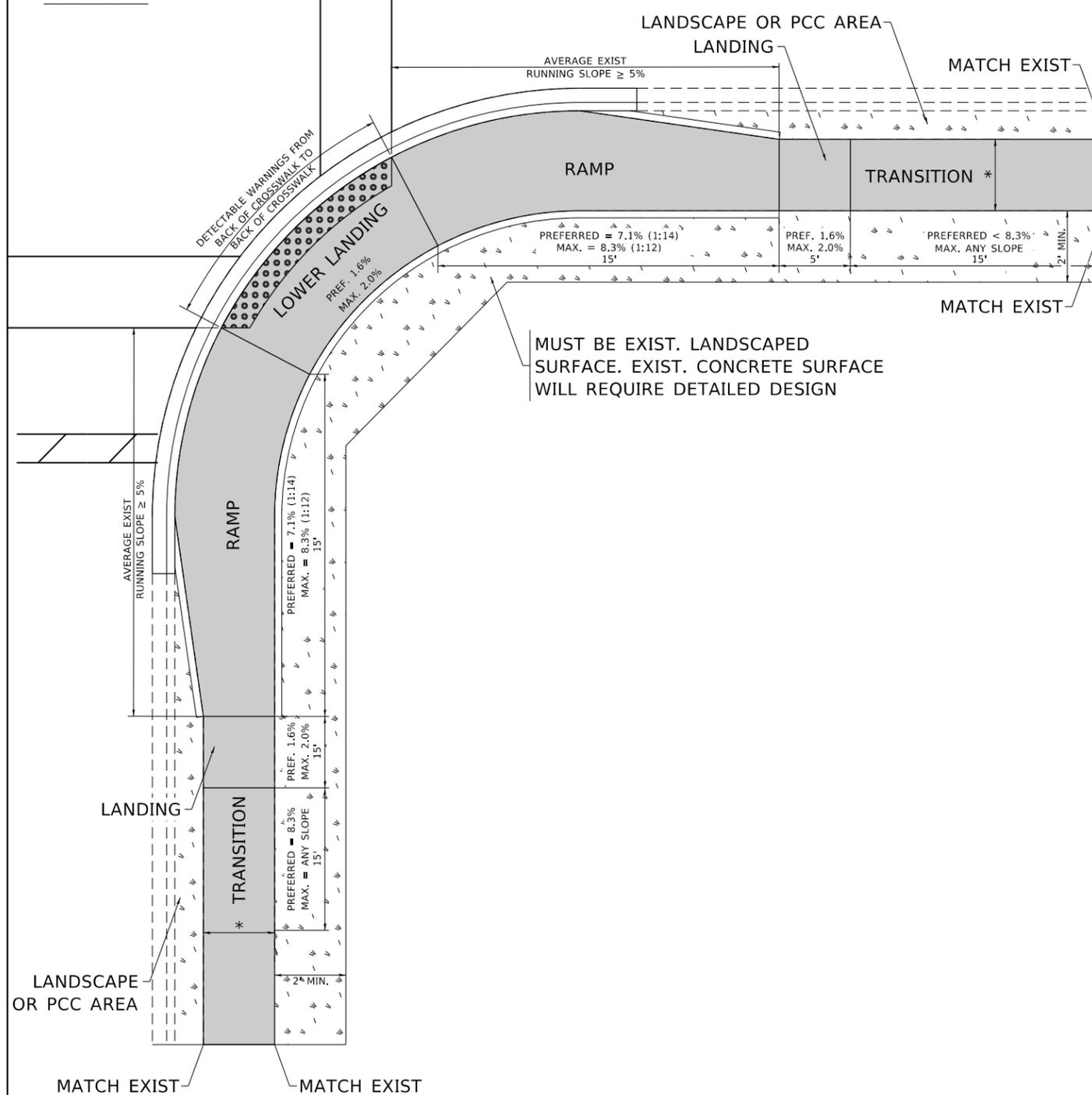
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	2020-130-R5	COOK	41	20
PD-05		CONTRACT NO. 62M08		
		ILLINOIS FED. AID PROJECT		

# ADA DETAIL FOR PARALLEL CURB RAMPS ADJACENT TO LANDSCAPING

**PD-06A**



**PD-06B**



**LEGEND**

- EXIST. GRASS
- PROPOSED SIDE CURB
- PROPOSED SIDEWALK
- DETECTABLE WARNINGS

**CONSTRUCTION NOTES:**

- 1) ALL CROSS SLOPES ARE PREFERRED 1.6% (1:64), MAXIMUM 2% (1:50) EXCEPT WHEN TRANSITIONING TO EXISTING SIDEWALK
- \* MATCH EXISTING SIDEWALK WIDTH

MODEL: Default  
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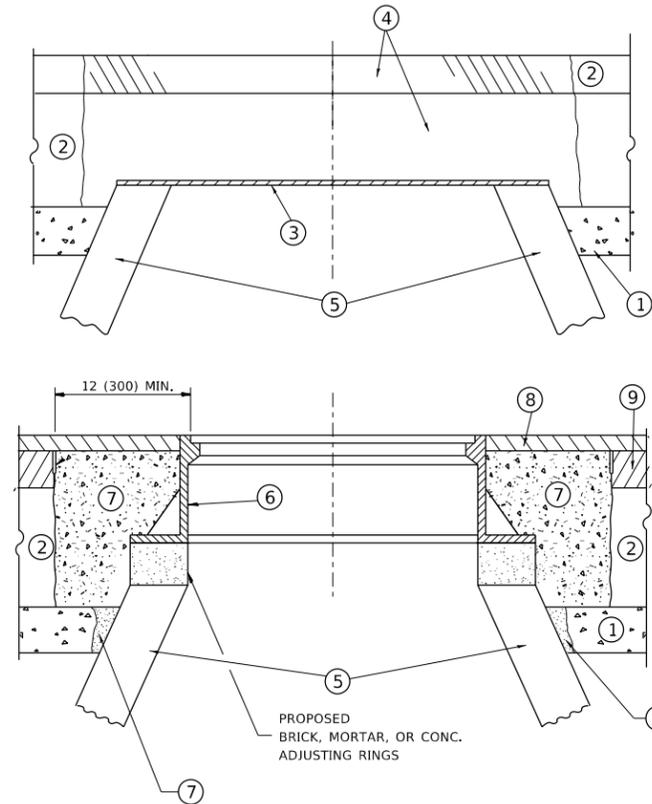
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	DRAWN - R. LEDEZMA	REVISED -
PLOT SCALE = 100,0000' / in.	CHECKED -	REVISED -
PLOT DATE = 12/4/2020	DATE - 10/02/2019	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PROJECT DETAIL FOR PARALLEL CURB RAMPS  
(PD-06)**

SCALE: NONE    SHEET    OF    SHEETS    STA.    TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	2020-130-RS	COOK	41	21
PD-06		CONTRACT NO. 62M08		
ILLINOIS		FED. AID PROJECT		



**NOTES**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1 \* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

\* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT \*THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1 \*CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR \*FRAMES AND LIDS TO BE ADJUSTED (SPECIAL).\*

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

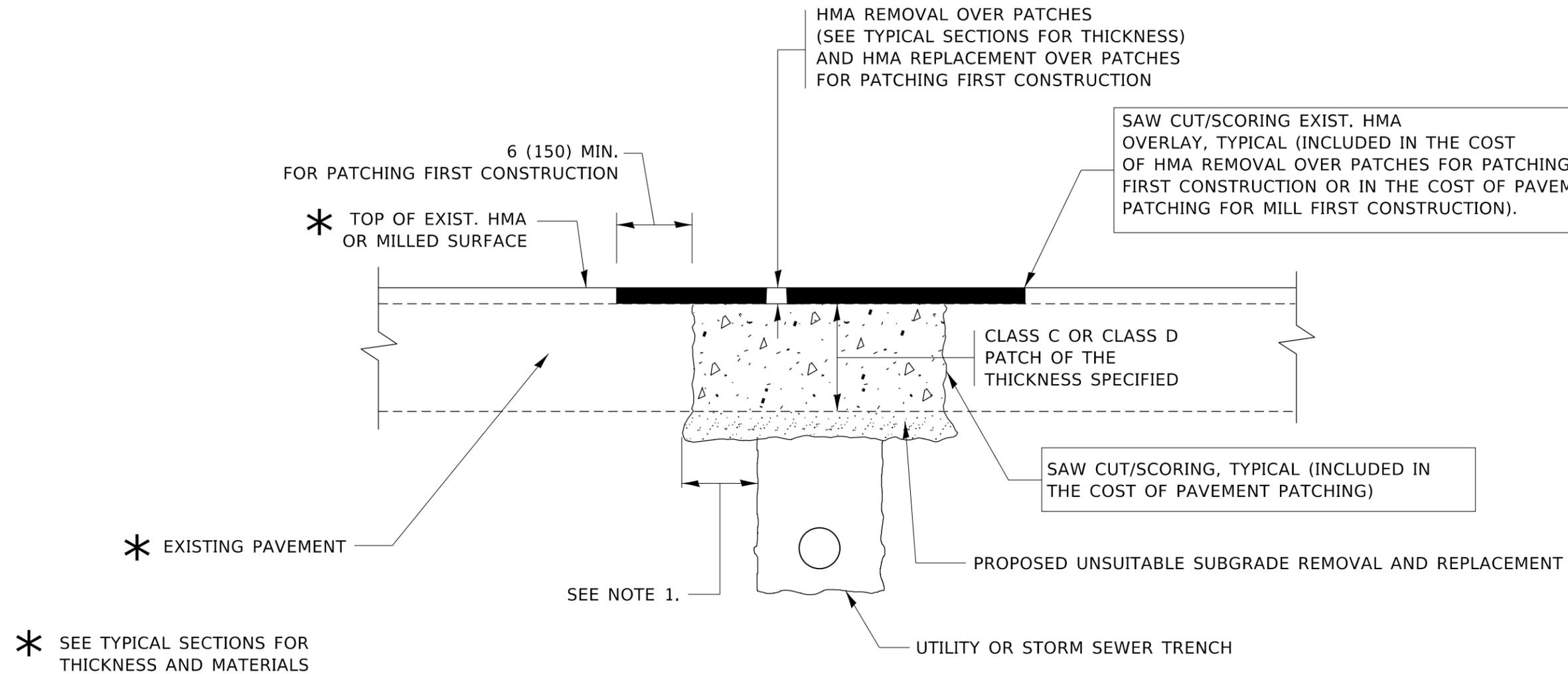
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DETAILS FOR  
FRAMES AND LIDS ADJUSTMENT WITH MILLING

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	2020-130-RS	COOK	41	22
BD600-03 (BD-8)		CONTRACT NO. 62M08		
ILLINOIS		FED. AID PROJECT		

USER NAME = steedpa	DESIGNED - R. SHAH	REVISED - R. WEDEMAN 05-14-04
	DRAWN -	REVISED - R. BORO 01-01-07
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - R. BORO 03-09-11
PLOT DATE = 12/4/2020	DATE - 10-25-94	REVISED - R. BORO 12-06-11



**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

**SEQUENCE OF CONSTRUCTION (PATCHING FIRST)**

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

**SEQUENCE OF CONSTRUCTION (MILLING FIRST)**

1. MILL HMA FIRST IF THERE IS AT LEAST 4½ INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: Default  
FILE: hma12c\_patchpavement.dwg  
PROJECT: I:\Projects\133020\CADD\Design\Design\133020.dwg

USER NAME = steedpa	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98
	DRAWN -	REVISED - R. BORO 01-01-07
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - R. BORO 09-04-07
PLOT DATE = 12/4/2020	DATE - 10-25-94	REVISED - K. ENG 10-27-08

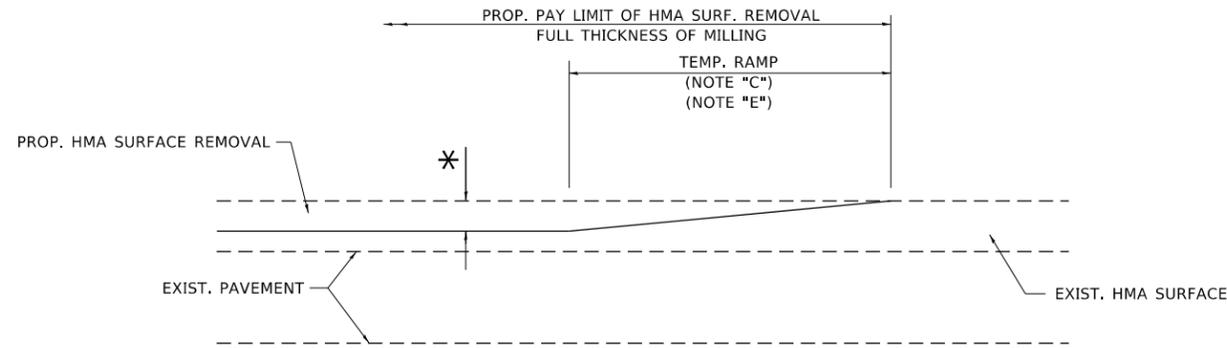
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT PATCHING FOR  
HMA SURFACED PAVEMENT**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	2020-130-R5	COOK	41	23
<b>BD400-04 (BD-22)</b>		CONTRACT NO. 62M08		
ILLINOIS FED. AID PROJECT				

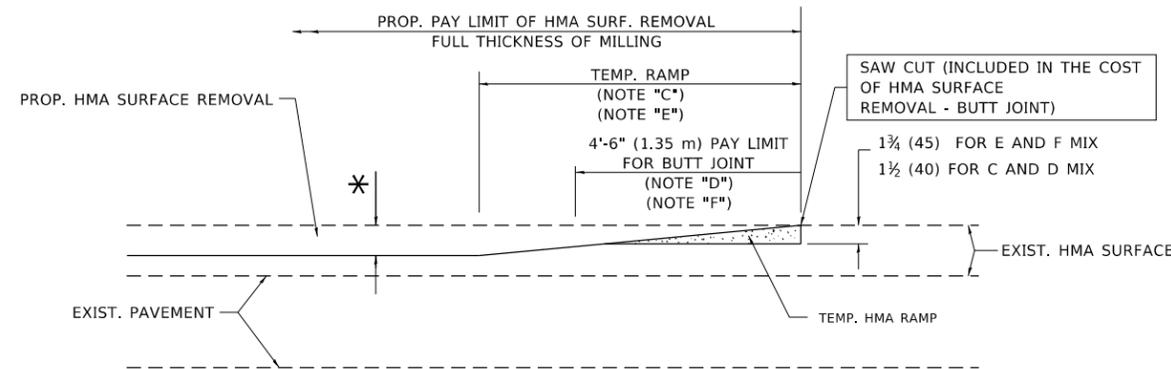




**MILLED TEMPORARY RAMP**

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

**OPTION 1**

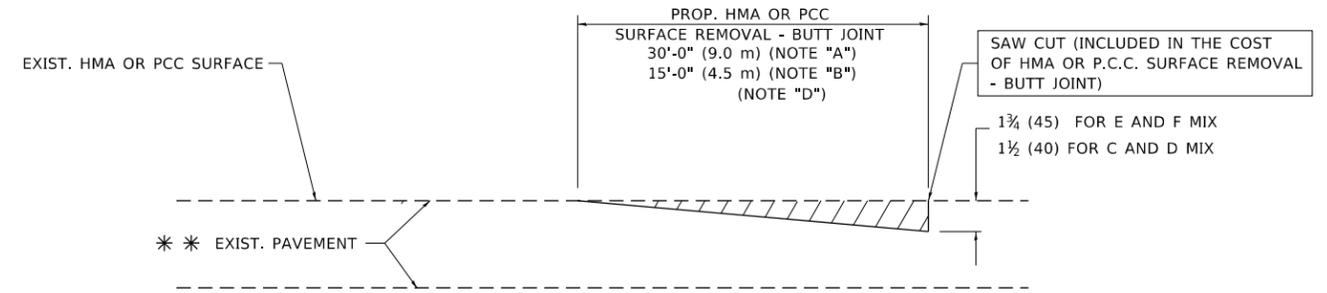


**HMA CONSTRUCTED TEMPORARY RAMP**

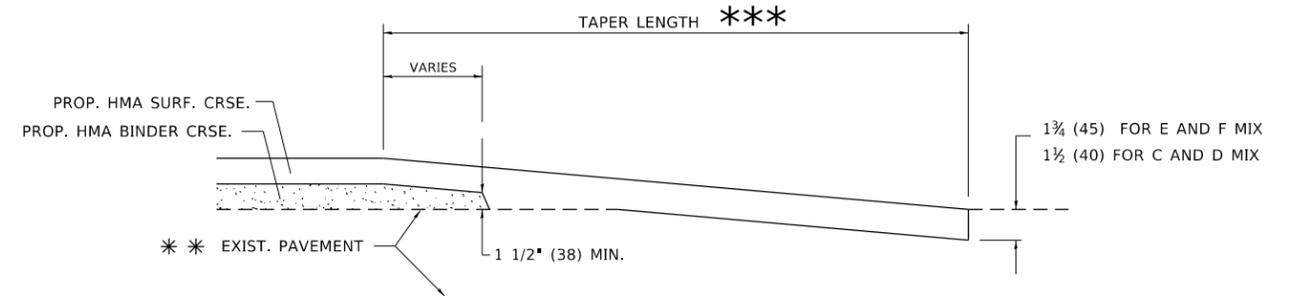
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

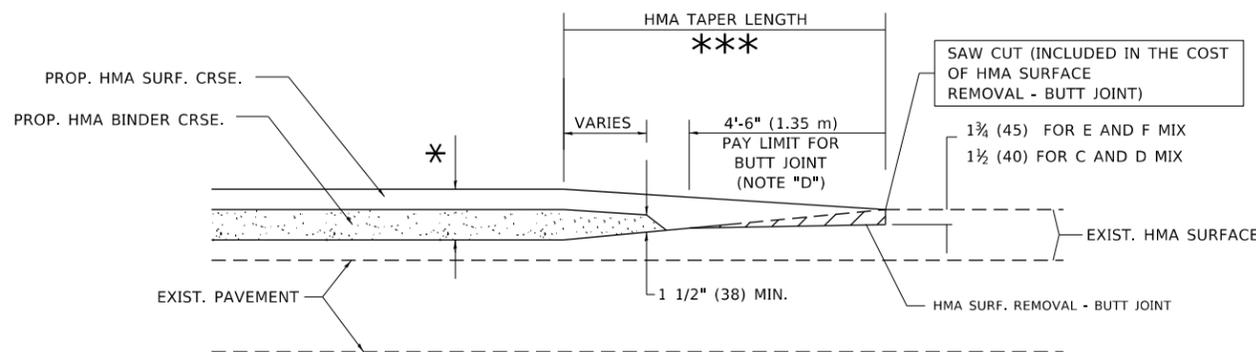
**NOTES**

- A. MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B. MINOR SIDE ROADS.
- C. THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D. THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E. TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F. INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT.  
\* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- G. SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".  
\*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



**BUTT JOINT AND HMA TAPER**

**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

MODEL: Default  
FILE: \\hpc\p\pub\hpc\room\dat\illinois\pcc\p\WIDOT\Documents\IBDOT\_Offices\Bartlett\_1\Projects\ID 153020\CADD\Drawings\Design\DES54.dwg

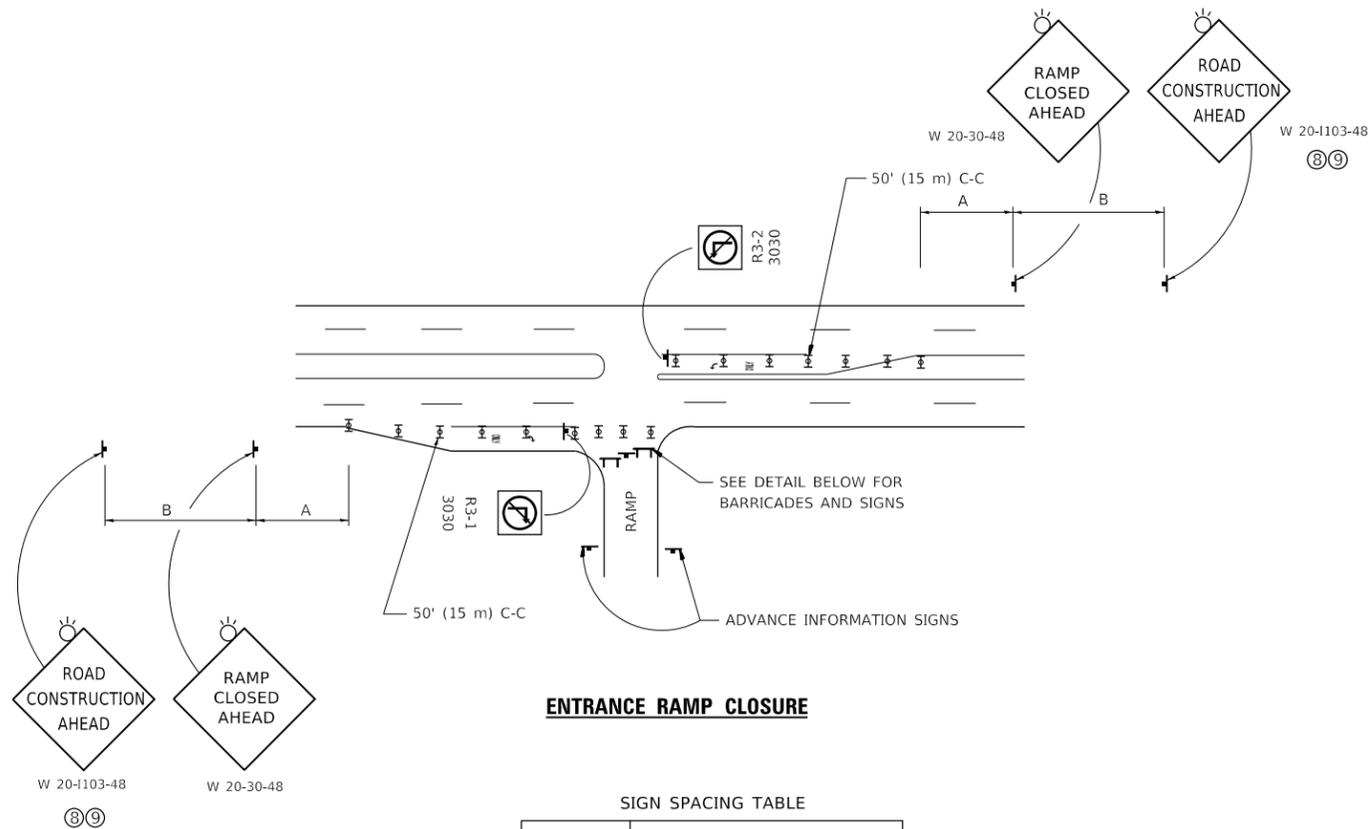
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	DRAWN -	REVISED - A. ABBAS 03-21-97
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - M. GOMEZ 04-06-01
PLOT DATE = 12/4/2020	DATE - 06-13-90	REVISED - R.BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND  
HMA TAPER DETAILS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	2020-130-RS	COOK	41	25
BD400-05 BD32		CONTRACT NO. 62M08		
ILLINOIS FED. AID PROJECT				

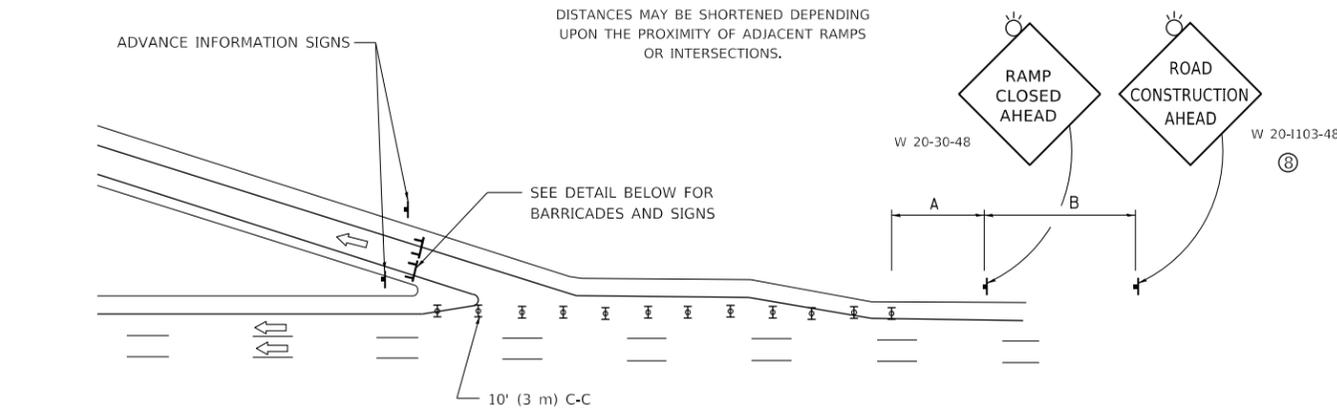


**ENTRANCE RAMP CLOSURE**

SIGN SPACING TABLE

FACILITY	DISTANCE BETWEEN SIGNS	
	A	B
EXPRESSWAY >24 HOURS	1000' (300 m)	1500' (450 m)
EXPRESSWAY ≤24 HOURS	500' (150 m)	500' (150 m)
ARTERIAL 55 MPH	500' (150 m)	500' (150 m)
ARTERIAL 50-45 MPH	350' (100 m)	350' (100 m)
ARTERIAL <45 MPH	200' (60 m)	200' (60 m)

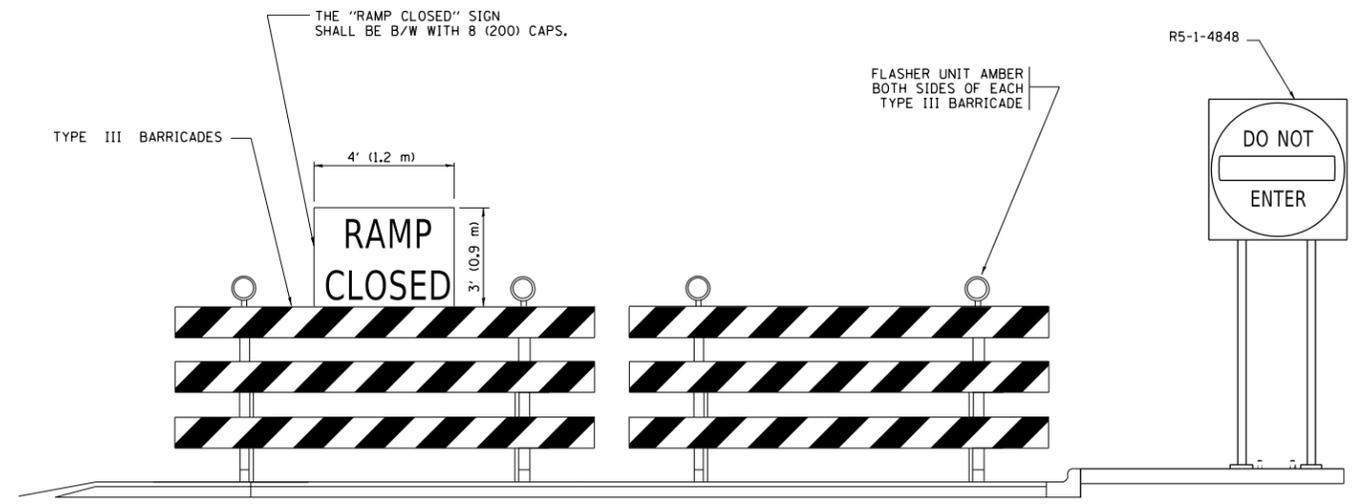
DISTANCES MAY BE SHORTENED DEPENDING UPON THE PROXIMITY OF ADJACENT RAMPS OR INTERSECTIONS.



**EXIT RAMP CLOSURE**

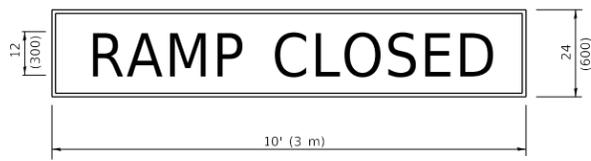
**SYMBOLS**

- ▬ TYPE II BARRICADE OR DRUM
- ▬ TYPE III BARRICADE WITH 2 FLASHING LIGHTS



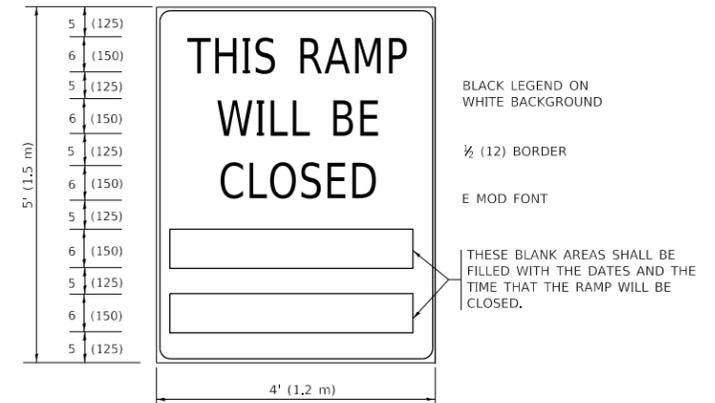
**DETAIL FOR REQUIRED BARRICADES & SIGNS**

**RAMP CLOSURE ADVANCE WARNING SIGN**



BLACK LEGEND ON ORANGE BACKGROUND MOUNTED DIAGONALLY  
E MOD FONT  
1 (25) BORDER  
THESE SIGNS ARE REQUIRED ON ALL THE EXIT GUIDE SIGNS FOR EXIT RAMPS THAT WILL BE CLOSED FOR MORE THAN FOUR (4) CONSECUTIVE DAYS.

**RAMP CLOSURE ADVANCE INFORMATION SIGN**



THESE SIGNS ARE REQUIRED ON BOTH SIDES OF THE RAMP, MINIMUM OF 1 WEEK IN ADVANCE OF THE CLOSURE.  
THESE SIGNS SHALL BE FABRICATED AND PAID FOR ACCORDING TO THE TEMPORARY INFORMATION SIGNING SPECIAL PROVISION

**GENERAL NOTES:**

- ① CONES MAY BE SUBSTITUTED FOR DRUMS OR TYPE II BARRICADES DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (700) HIGH.
- ② VERTICAL BARRICADES SHALL NOT BE USED FOR RAMP CLOSURES.
- ③ A FLAGGER SHALL BE POSITIONED AT EACH CLOSED RAMP THAT IS OPEN TO CONSTRUCTION VEHICLES, PRECEDED BY A W20-7 FLAGGER WARNING SIGN.
- ④ ALL ROUTE MARKERS AND TRAILBLAZER ASSEMBLIES WHICH DIRECT MOTORISTS TO A CLOSED ENTRANCE RAMP SHALL BE COVERED WHEN THE RAMP IS CLOSED FOR MORE THAN FOUR (4) DAYS.
- ⑤ THE SIGNING AND BARRICADING WHICH IS REQUIRED BY THIS DETAIL SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS).
- ⑥ AUTHORIZATION FROM THE DISTRICT'S BUREAU OF TRAFFIC IS REQUIRED FOR ALL RAMP CLOSURES.
- ⑦ THE RAMP CLOSURE ADVANCE INFORMATION SIGNS SHALL BE ERECTED IF THE CLOSURE TIME EXCEEDS TWENTY-FOUR (24) HOURS. ADDITIONAL ADVANCE WARNING SIGNS ON EXIT GUIDE SIGNING WILL BE REQUIRED FOR EXIT RAMP CLOSURES THAT EXCEED FOUR (4) DAYS IN LENGTH.
- ⑧ ROAD CONSTRUCTION AHEAD SIGNS MAY BE OMITTED WHEN THIS DETAIL IS USED IN CONJUNCTION WITH OTHER TRAFFIC CONTROL THAT ALREADY INCLUDES A ROAD CONSTRUCTION AHEAD SIGN.
- ⑨ ARTERIAL ROAD CONSTRUCTION AHEAD SIGNS SHALL BE INSTALLED ON THE LEFT SIDE OF TRAFFIC IF THE MEDIAN IS MORE THAN 10 FT WIDE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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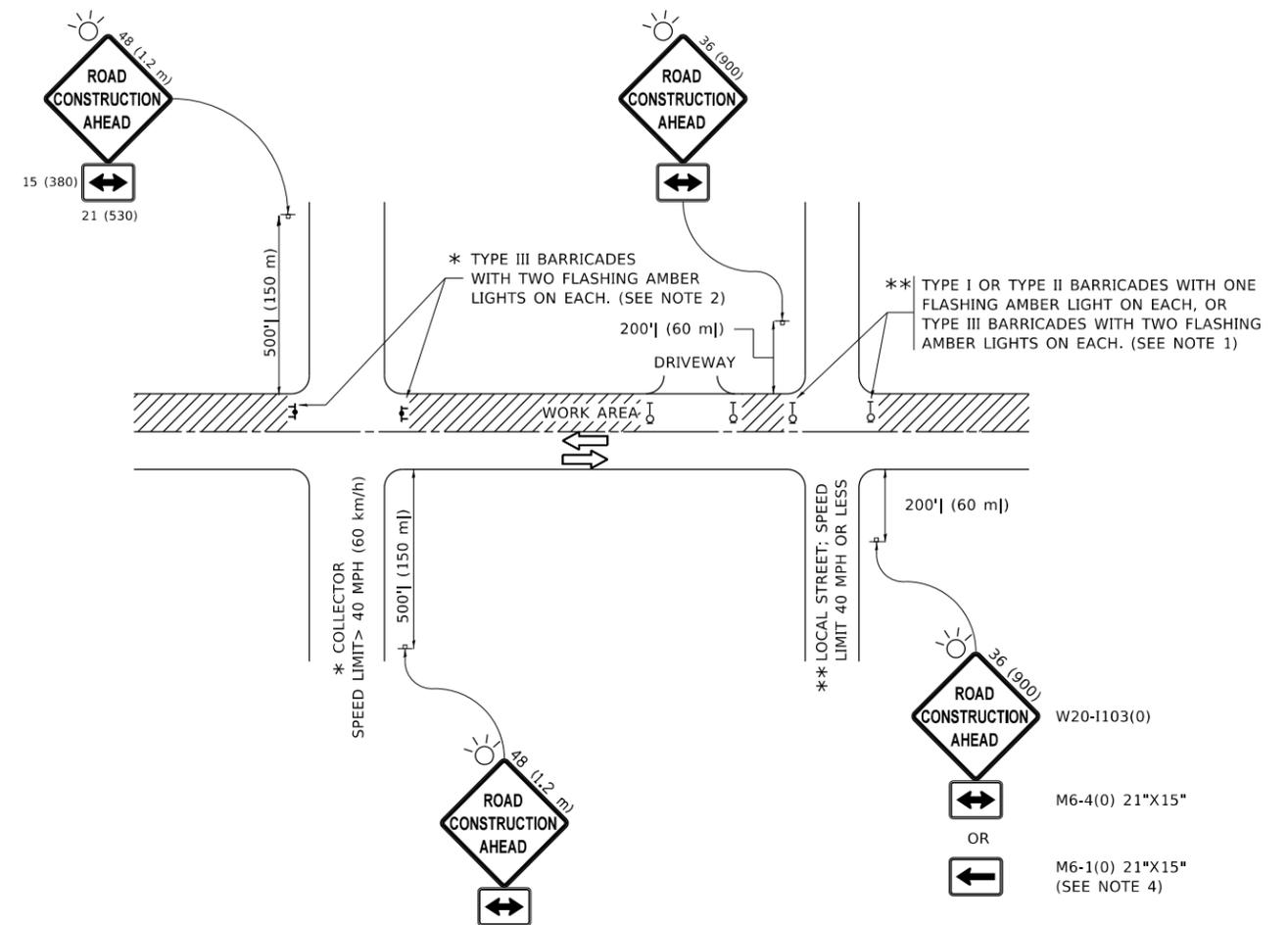
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PLOT DATE = 12/4/2020	CHECKED -	REVISED - M.D._06-13
	DATE - 02-83	REVISED - M.D._01-18

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ENTRANCE\_AND\_EXIT\_RAMP  
CLOSURE\_DETAILS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE. 353	SECTION 2020-130-R5	COUNTY COOK	TOTAL SHEETS 41	SHEET NO. 26
TC-08		CONTRACT NO. 62M08		
ILLINOIS		FED. AID PROJECT		



**NOTES:**

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: Default  
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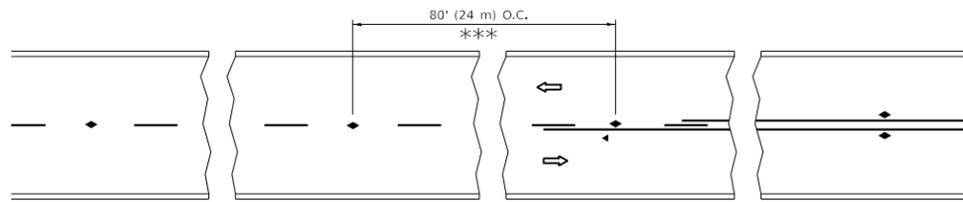
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PLOT DATE = 12/4/2020	DATE - 06-89	REVISED - A. SCHUETZE 07-01-13
		REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

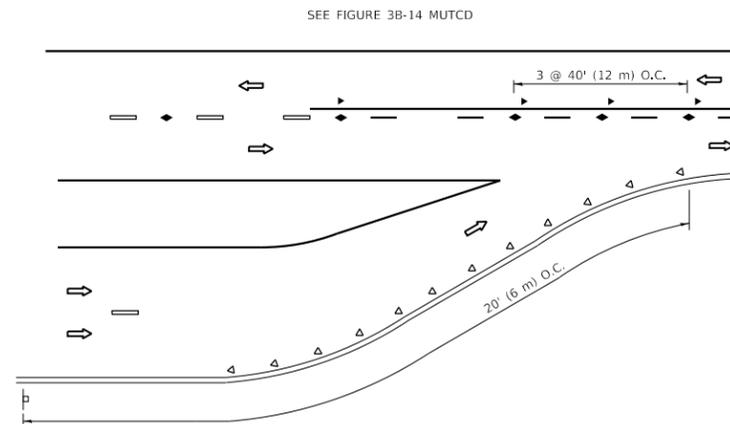
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	2020-130-RS	COOK	41	27
<b>TC-10</b>		CONTRACT NO. 62M08		
ILLINOIS FED. AID PROJECT				

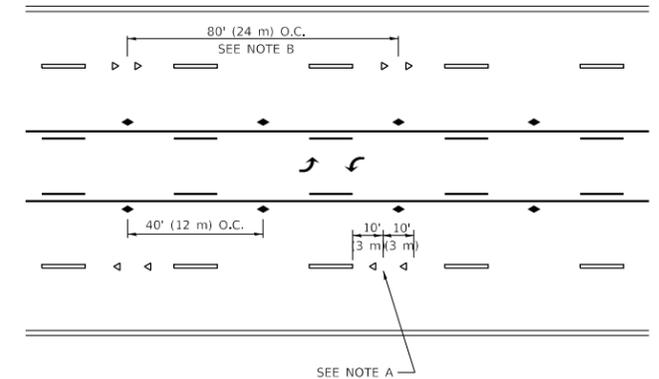


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

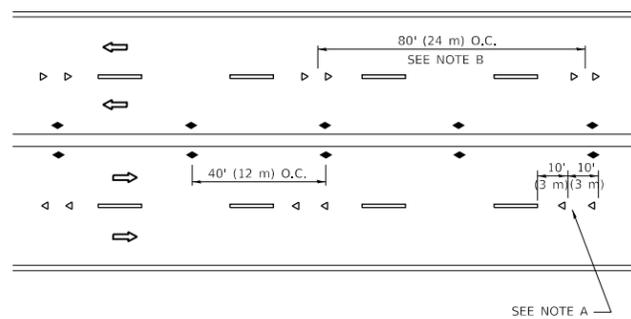
**TWO-LANE/TWO-WAY**



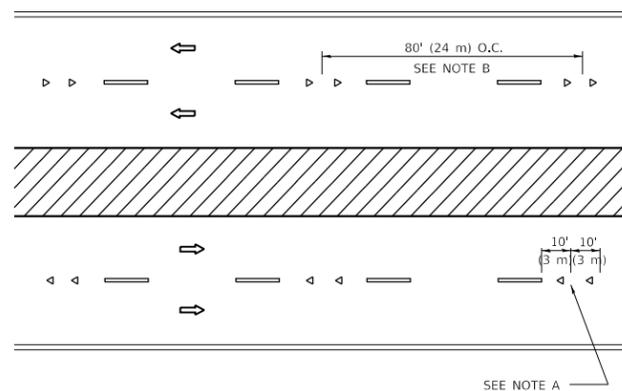
**LANE REDUCTION TRANSITION**



**TWO-WAY LEFT TURN**



**MULTI-LANE/UNDIVIDED**



**MULTI-LANE/DIVIDED**

**GENERAL NOTES**

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

**SYMBOLS**

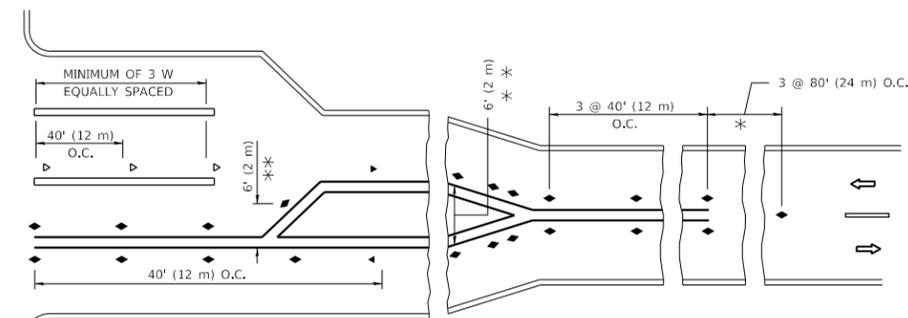
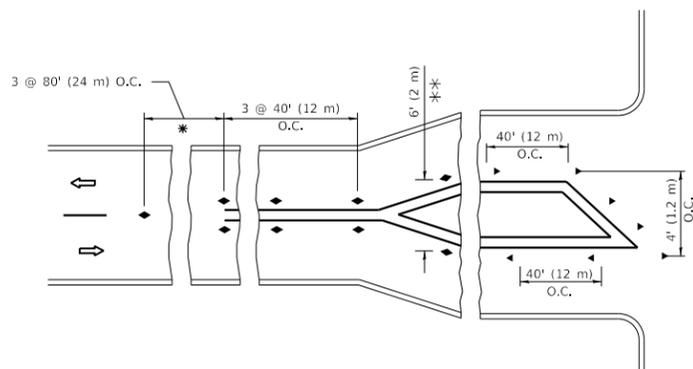
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

**LANE MARKER NOTES**

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

**DESIGN NOTES**

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

**TURN LANES**

All dimensions are in inches (millimeters) unless otherwise shown.

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	DRAWN -	REVISED - T. RAMMACHER 01-06-00
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - C. JUCIUS 09-09-09
PLOT DATE = 12/4/2020	DATE -	REVISED - C. JUCIUS 07-01-13

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**TYPICAL APPLICATIONS  
 RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

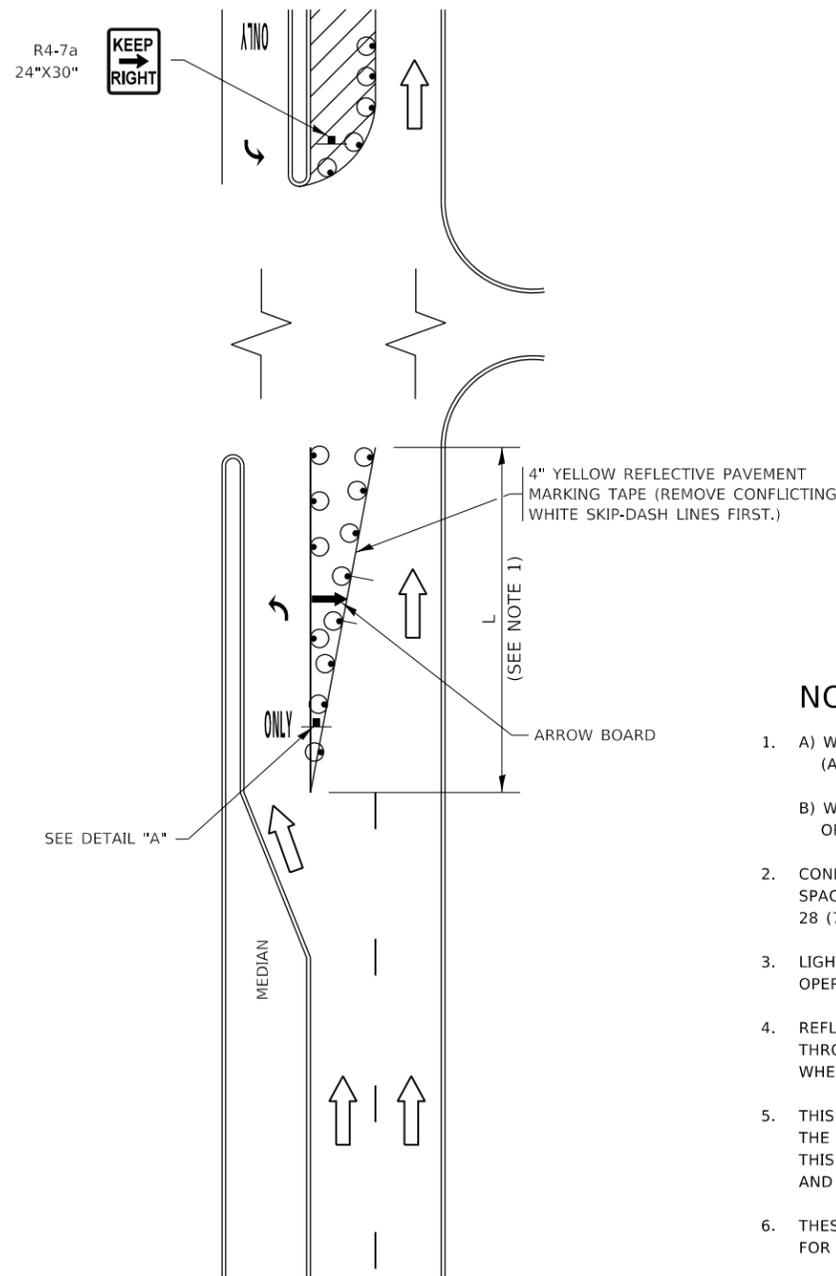
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-11		CONTRACT NO. 62M08		
ILLINOIS		FED. AID PROJECT		





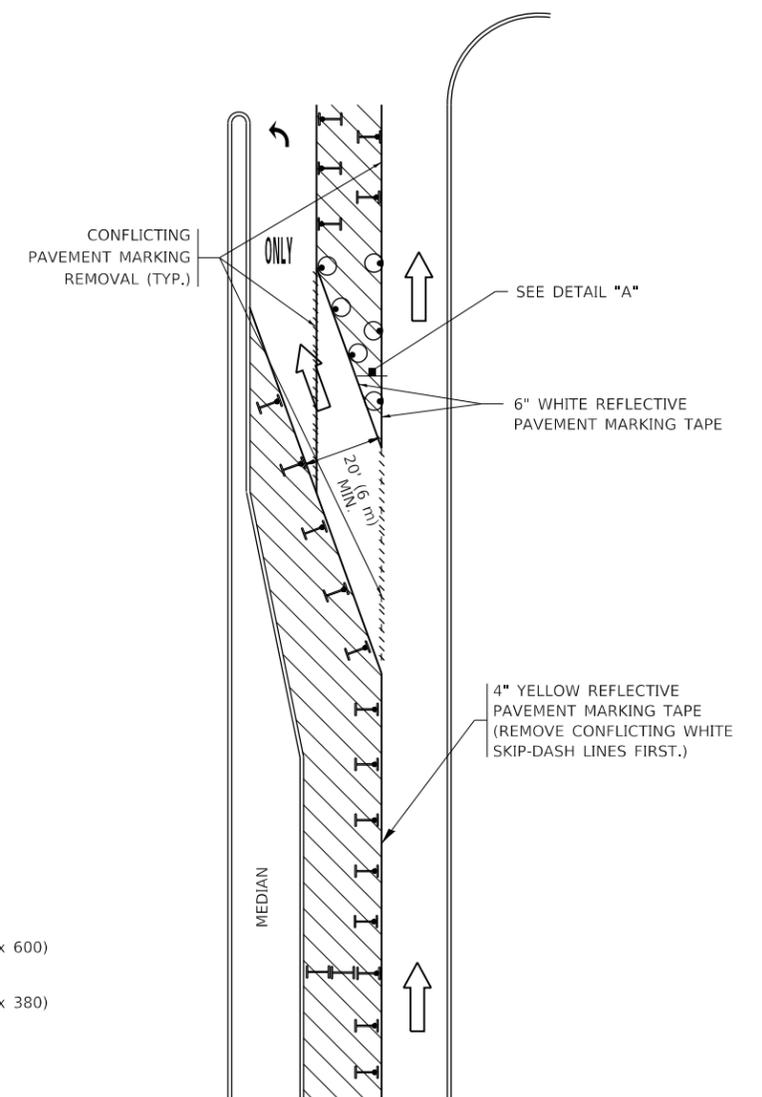


# TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER



**FIGURE 1**

# TURN BAY ENTRANCE WITHIN A LANE CLOSURE



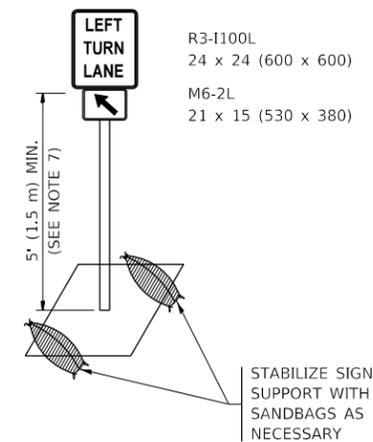
**FIGURE 2**

### LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

### NOTES:

1. A) WHEN "L" IS  $\leq$  THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.  
B) WHEN "L" IS  $>$  THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



**DETAIL A**

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: Default  
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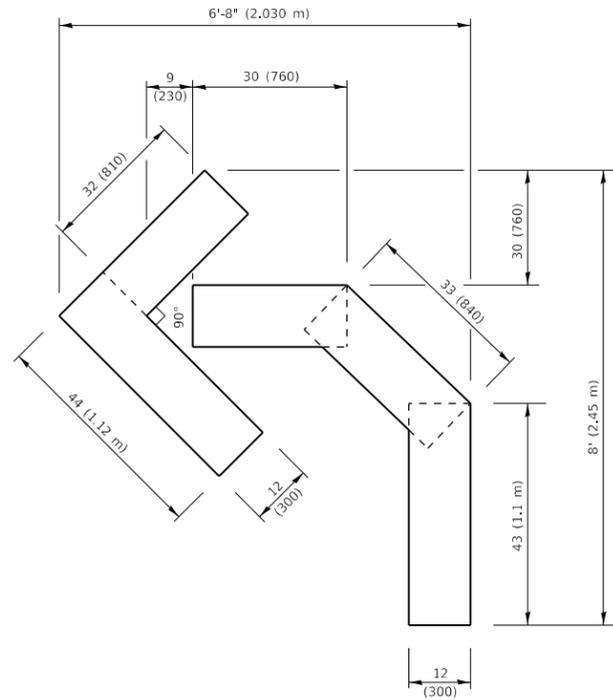
USER NAME = steedpa	DESIGNED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09
	DRAWN - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13
PLOT SCALE = 100,0000' / In.	CHECKED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16
PLOT DATE = 12/4/2020	DATE - T. RAMMACHER 01-06-00	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION AT TURN BAYS  
(TO REMAIN OPEN TO TRAFFIC)

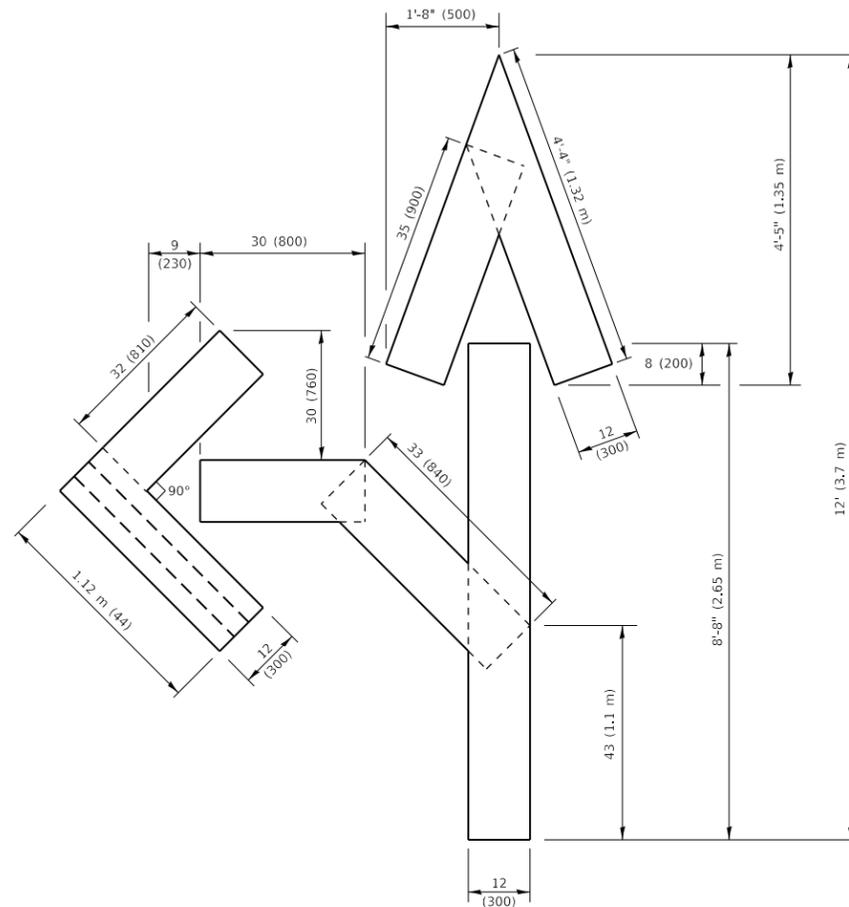
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	2020-130-R5	COOK	41	32
TC-14		CONTRACT NO. 62M08		
ILLINOIS FED. AID PROJECT				



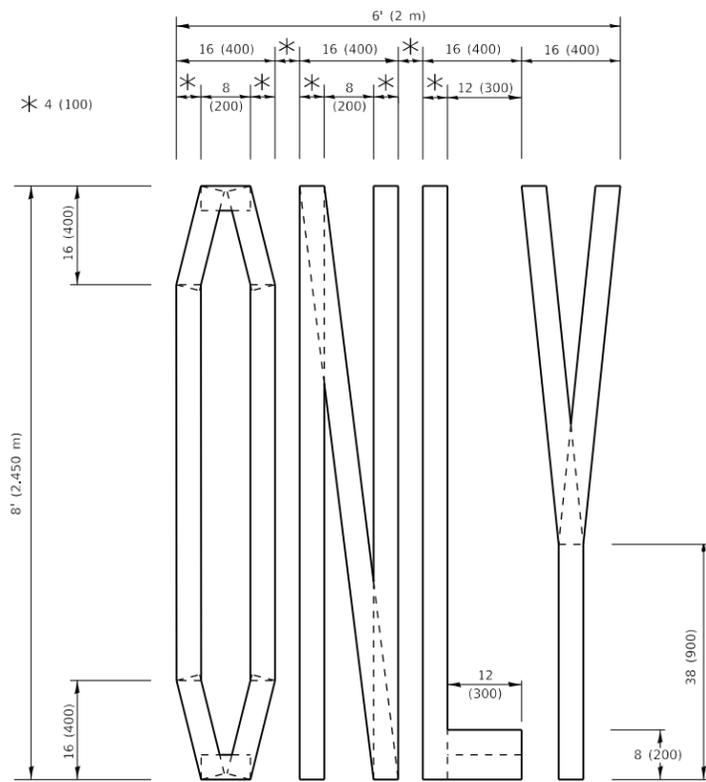
**QUANTITY**

4 (100) LINE = 45.5 ft. (13.9 m)  
15.2 sq. ft. (1.41 sq. m)



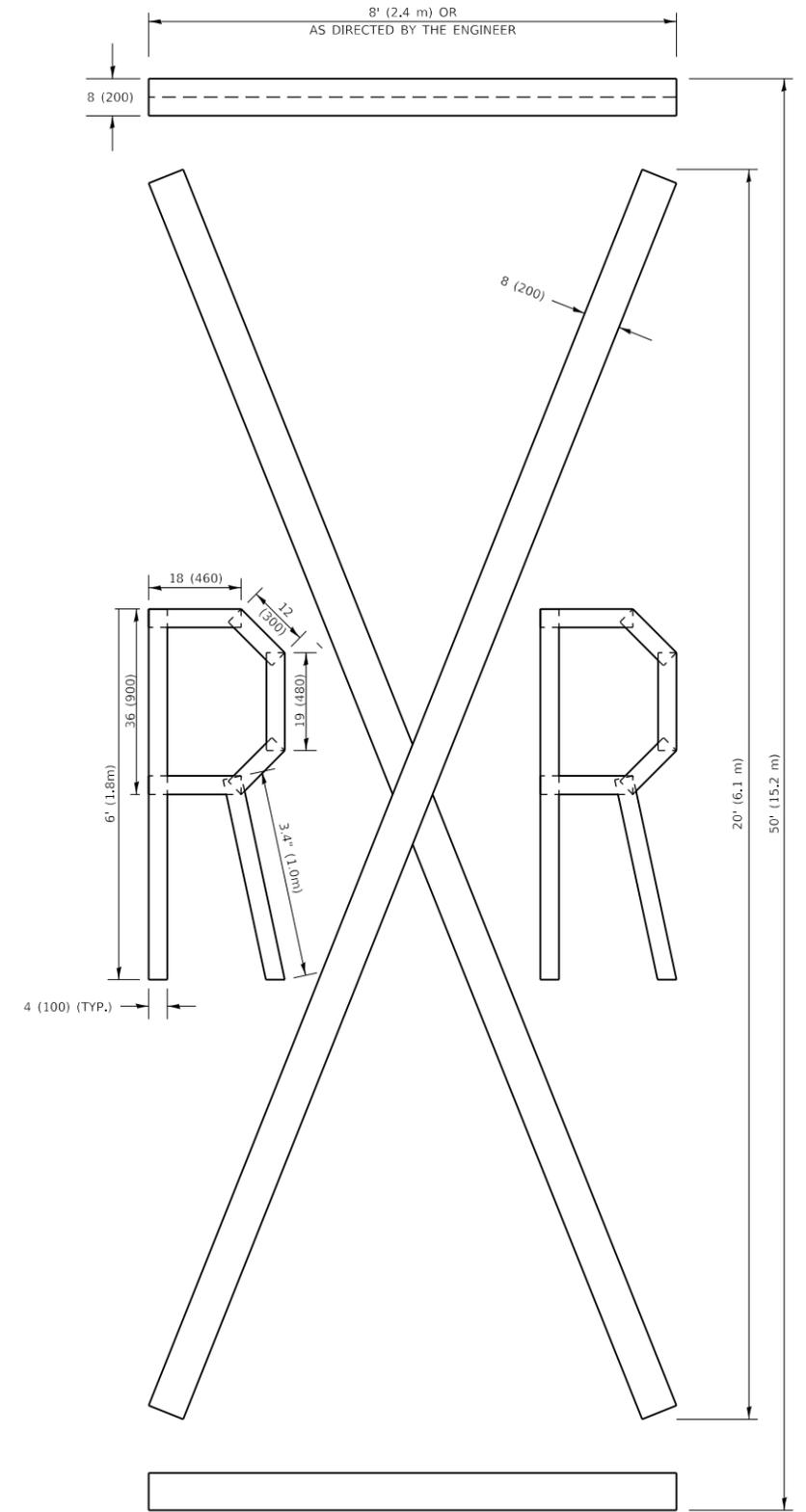
**QUANTITY**

4 (100) LINE = 82.5 ft. (25.1 m)  
27.5 sq. ft. (2.53 sq. m)



**QUANTITY**

4 (100) LINE = 64.1 ft. (19.5 m)  
21.4 sq. ft. (1.99 sq. m)



**QUANTITY**

4 (100) LINE = 225.9 ft. (68.9 m)  
75.3 sq. ft. (6.99 sq. m)

**NOTE:**

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.

All dimensions are in inches (millimeters) unless otherwise shown.

MODEL: Default  
FILE NAME: P:\publan\com.ecs.illinois\poc\p\WIDOT\Documental\DOT\_Offices\Drawt\1\Projects\15702\CAD\data\Design\BIS\Std.dgn

USER NAME = steedpa	DESIGNED -	REVISED - T. RAMMACHER 03-02-98
	DRAWN -	REVISED - E. GOMEZ 08-28-00
PLOT SCALE = 100,0010 ' / In.	CHECKED -	REVISED - E. GOMEZ 08-28-00
PLOT DATE = 12/4/2020	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16

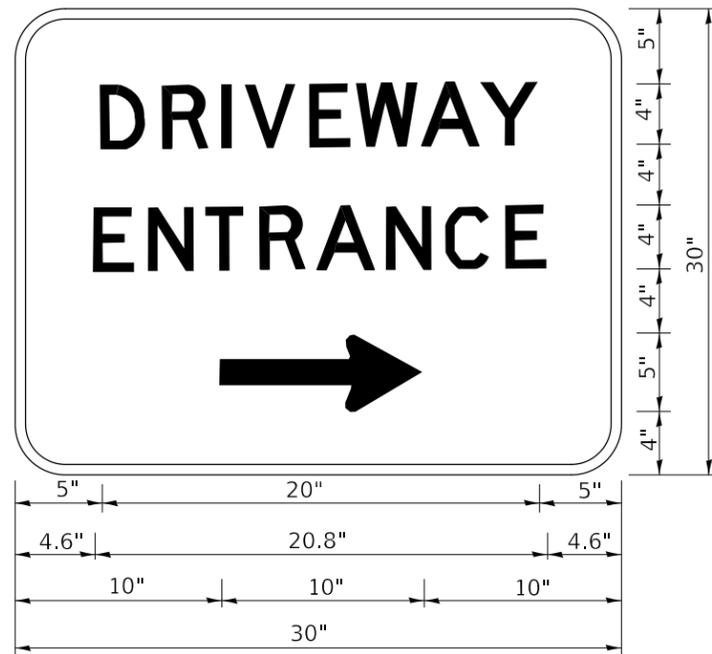
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	2020-130-R5	COOK	41	33
<b>TC-16</b>		CONTRACT NO. 62M08		
ILLINOIS FED. AID PROJECT				





3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED  
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

MODEL: Default  
 FILE: Model: p:\pub\planroom.dwg  
 I:\projects\133020\CADD\data\Design\DRIVE.dwg

USER NAME = steedpa	DESIGNED -	REVISED - C. JUCIUS 02-15-07
	DRAWN -	REVISED -
PLOT SCALE = 100.0010' / in.	CHECKED -	REVISED -
PLOT DATE = 12/4/2020	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**DRIVEWAY ENTRANCE SIGNING**

SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

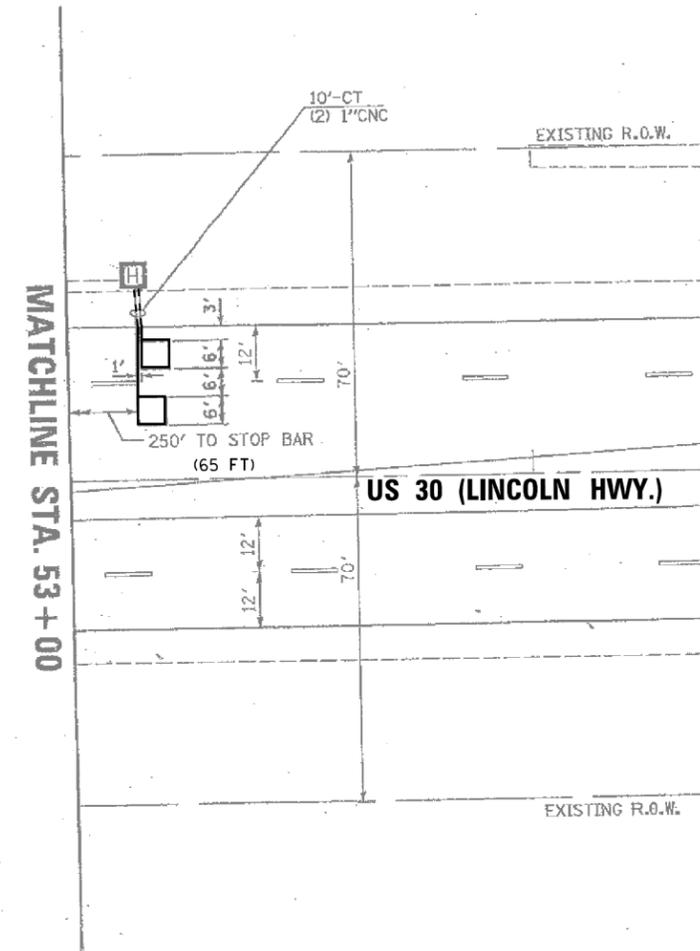
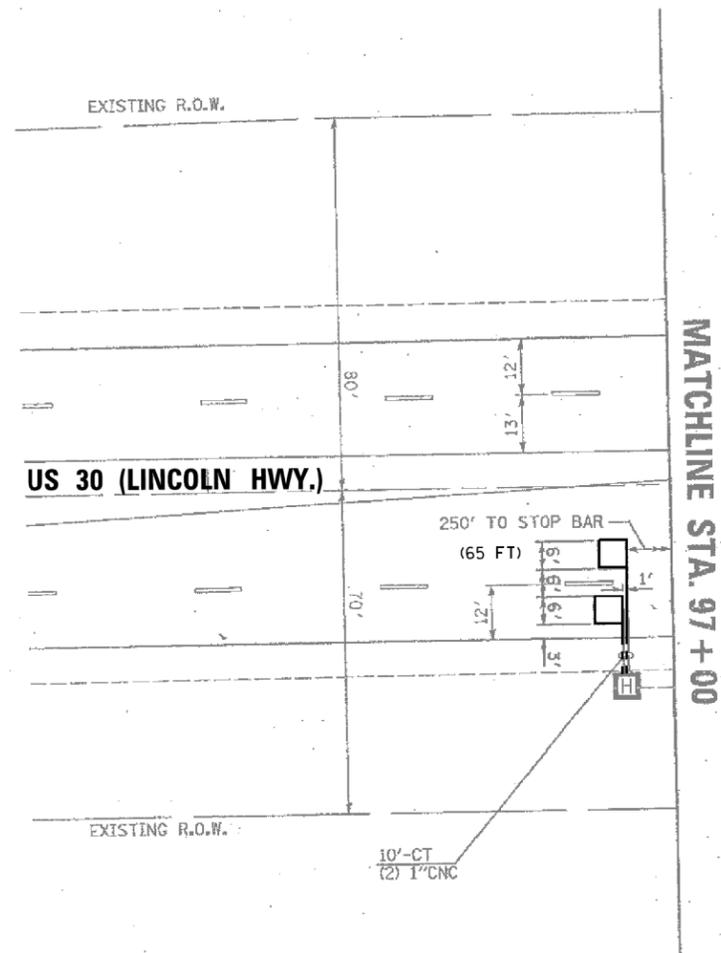
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	2020-130-RS	COOK	41	35
<b>TC-26</b>		CONTRACT NO. 62M08		
		ILLINOIS FED. AID PROJECT		





**NOTES:**

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



**REPLACE ALL DETECTOR LOOPS AS SHOWN  
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	130	FOOT

FILE NAME = 62M08 - US 30 @ Ridgeland Rd.dgn	USER NAME = vargosa	DESIGNED - AV	REVISED -
		DRAWN - AV	REVISED -
		CHECKED - LP	REVISED -
		DATE - 11/19/2020	REVISED -

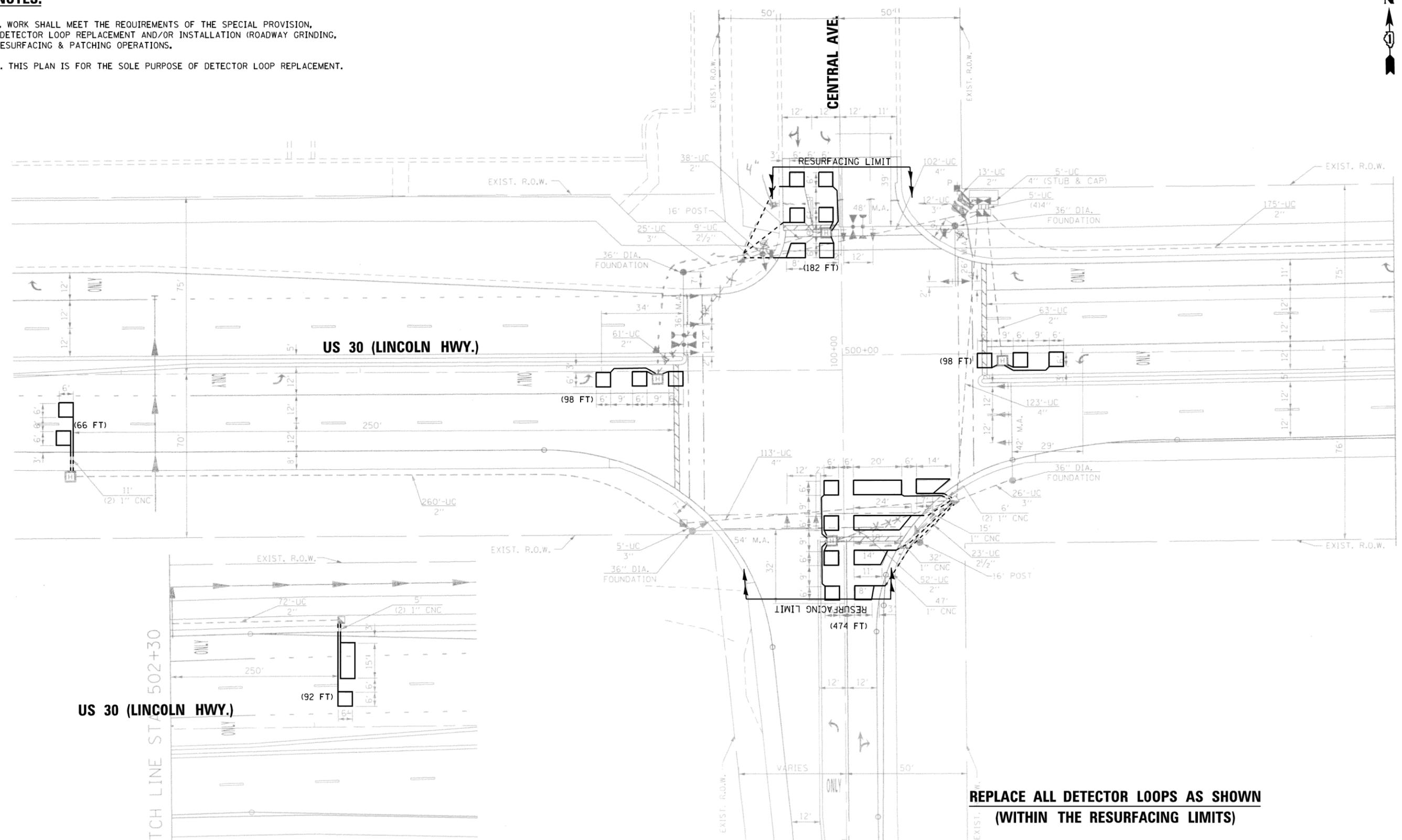
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DETECTOR LOOP REPLACEMENT PLAN US 30 (LINCOLN HWY.) AT RIDGELAND RD.</b>		F.A.P. RTE. 353	SECTION 2020-130-R5	COUNTY COOK	TOTAL SHEETS 41	SHEET NO. 38
SCALE:	SHEET OF SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT			

CONTRACT NO. 62M08

**NOTES:**

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



**REPLACE ALL DETECTOR LOOPS AS SHOWN  
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	1010	FOOT

TS 10620

FILE NAME = 62M08 - US 30 @ Central Ave.dgn	USER NAME = vargesso	DESIGNED - AV	REVISED -
		DRAWN - AV	REVISED -
		CHECKED - LP	REVISED -
		DATE - 11/19/2020	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

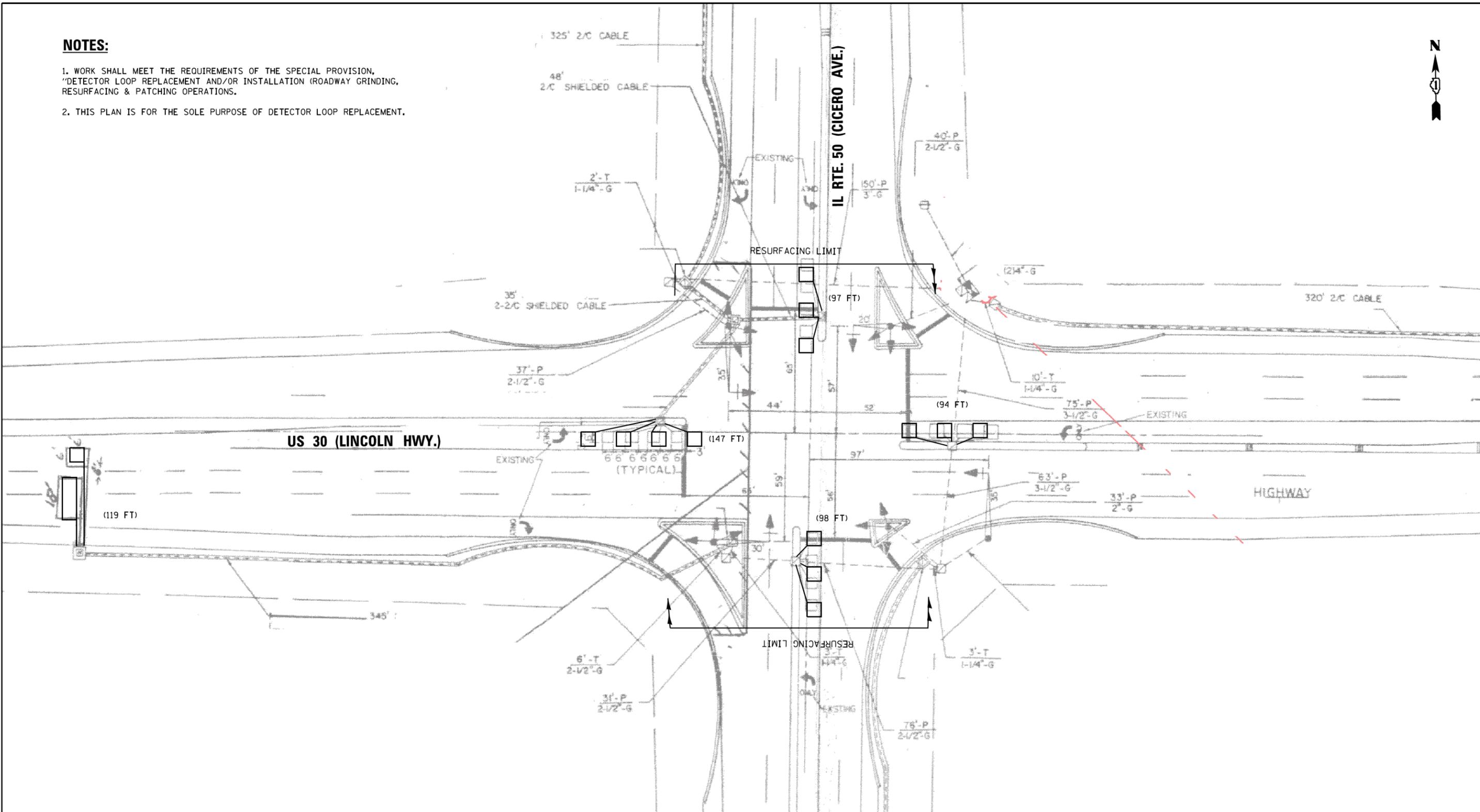
**DETECTOR LOOP REPLACEMENT PLAN  
US 30 (LINCOLN HWY.) AT CENTRAL AVE.**

F.A.P. RTE. 353	SECTION 2020-130-RS	COUNTY COOK	TOTAL SHEETS 41	SHEET NO. 39
CONTRACT NO. 62M08				
ILLINOIS FED. AID PROJECT				

SCALE: SHEET OF SHEETS STA. TO STA.

**NOTES:**

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



**REPLACE ALL DETECTOR LOOPS AS SHOWN  
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	555	FOOT

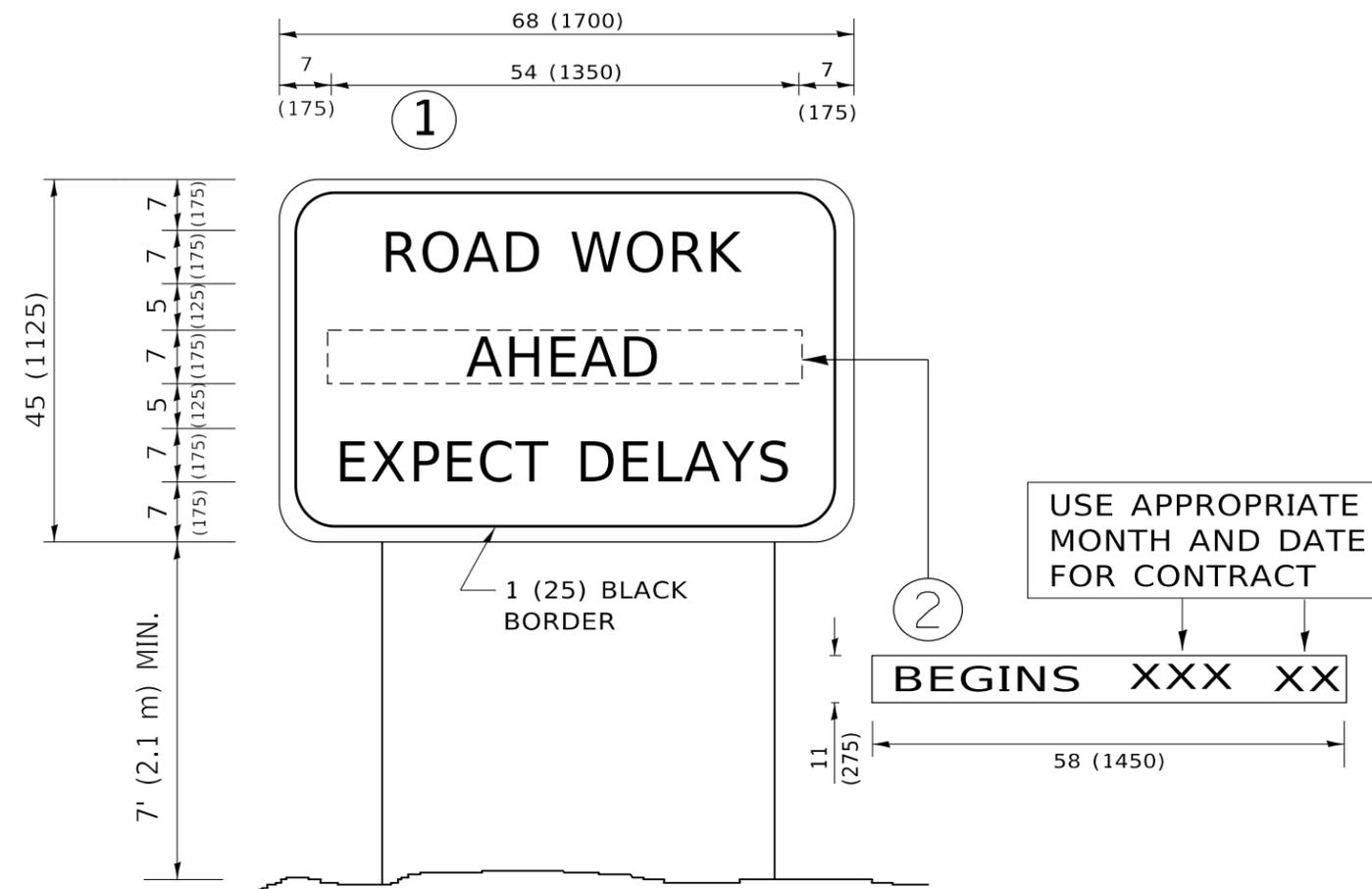
FILE NAME = 62M08 - US 30 @ IL Rte.50 Cicero Ave.dwg	USER NAME = vargaso	DESIGNED - AV	REVISED -
		DRAWN - AV	REVISED -
		CHECKED - LP	REVISED -
		DATE - 11/19/2020	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETECTOR LOOP REPLACEMENT PLAN  
US 30 (LINCOLN HWY.) AT IL RTE. 50 (CICERO AVE.)**

F.A.P. RTE. 353	SECTION 2020-130-RS	COUNTY COOK	TOTAL SHEETS 41	SHEET NO. 40
CONTRACT NO. 62M08				
ILLINOIS FED. AID PROJECT				

SCALE: SHEET OF SHEETS STA. TO STA.



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

MODEL: Default  
 FILE: \\nls-px1\pub\anrcom\data\illinois.gov\PWIDOT\Documents\DOT\_Offices\Dir\etec\_1\Project\ID153020\CADD\ata\Design\DESIGN.dgn

USER NAME = steedpa	DESIGNED -	REVISED - R. MIRS 09-15-97
	DRAWN -	REVISED - R. MIRS 12-11-97
PLOT SCALE = 100.0010 ' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
PLOT DATE = 12/4/2020	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD  
INFORMATION SIGN**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
353	2020-130-RS	COOK	41	41
<b>TC-22</b>			CONTRACT NO. 62M08	
ILLINOIS FED. AID PROJECT				