

INDEX OF SHEETS

1	COVER SHEET
2	INDEX, HIGHWAY STANDARDS, GENERAL NOTES AND COMMITMENTS
3-16	SUMMARY OF QUANTITIES
17-18	TYPICAL SECTIONS
19	SCHEDULE OF QUANTITIES
20	ALIGNMENT, TIES, AND BENCHMARKS
21	REMOVAL PLAN
22-24	PLAN AND PROFILE SHEETS
25	SUGGESTED MAINTENANCE OF TRAFFIC GENERAL NOTES
26	SUGGESTED MAINTENANCE OF TRAFFIC TYPICAL SECTIONS
27-32	SUGGESTED MAINTENANCE OF TRAFFIC PLAN SHEETS
33	EROSION AND SEDIMENT CONTROL GENERAL NOTES
34-37	EROSION AND SEDIMENT CONTROL PLAN SHEETS
38	GRADING PLAN
39	DRAINAGE AND UTILITIES PLAN
40-43	SUE PLANS
44	PAVEMENT MARKING AND SIGNING PLAN
45-46	LANDSCAPING PLAN
47-52	LIGHTING PLANS
53	ADA RAMP DETAILS
54	GUARDRAIL TRANSITION DETAILS
55	PACE BUS PAD DETAILS
56-114	STRUCTURAL PLANS - IHB RAILROAD / GARDNER ROAD
115-146	STRUCTURAL PLANS - 25 TH AVENUE
147-173	STRUCTURAL PLANS - ADDISON CREEK
174-179	DISTRICT DETAILS
180-193	CROSS SECTIONS - ROADWAY
194	CROSS SECTIONS - ADDISON CREEK

DISTRICT ONE DETAILS

TC-10	TRAFFIC CONTROL & PROTECTION FOR SIDEROADS, INTERSECTIONS & DRIVEWAYS
TC-11	RAISED REFLECTIVE PAVEMENT MARKERS (SNOW PLOW RESISTANT)
TC-13	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
TC-16	SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS
TC-22	ARTERIAL ROAD INFORMATION SIGN
TC-26	DRIVEWAY ENTRANCE SIGNING

HIGHWAY STANDARDS

000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001006	DECIMAL OF AN INCH AND OF A FOOT
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
420401-13	PAVEMENT CONNECTOR (PCC) FOR BRIDGE APPROACH SLAB
420406	PAVEMENT CONNECTOR (HMA) FOR BRIDGE APPROACH SLAB
424001-11	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
515001-04	NAME PLATE FOR BRIDGES
542301-03	PRECAST REINFORCED CONCRETE FLARED END SECTION
601001-05	PIPE UNDERDRAINS
601101-02	CONCRETE HEADWALL FOR PIPE UNDERDRAINS
602402-03	PRECAST MANHOLE TYPE A 5' (1.52 m) DIAMETER
602601-06	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
602701-02	MANHOLE STEPS
604001-05	FRAME AND LIDS TYPE 1
606001-07	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB & GUTTER
606301-04	PC CONCRETE ISLANDS AND MEDIANS
630001-12	STEEL PLATE BEAM GUARDRAIL
630116	BACK SIDE PROTECTION OF GUARDRAIL
630201-07	PCC/HMA STABILIZATION AT STEEL PLATE BEAM GUARDRAIL
630301-09	SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS
631011-10	TRAFFIC BARRIER TERMINAL, TYPE 2
631031-17	TRAFFIC BARRIER TERMINAL, TYPE 6
701101-05	OFF-ROAD OPERATIONS, MULTI-LN, 15 FT. (4.5 m) TO 24 IN. (600 mm) FROM PAVEMENT EDGE
701601-09	URBAN LN CLOSURE, MULTI-LN, 1W OR 2W W/ NON-TRAVERSABLE MEDIAN
701606-10	URBAN SINGLE LN CLOSURE, MULTI-LN, 2W W/ MOUNTABLE MEDIAN

HIGHWAY STANDARDS (CONT'D)

701611-01	URBAN HALF ROAD CLOSURE, MULTI-LN, 2W W/ MOUNTABLE MEDIAN
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES
704001-08	TEMPORARY CONCRETE BARRIER
725001-01	OBJECT AND TERMINAL MARKERS
780001-05	TYPICAL PAVEMENT MARKINGS
781001-04	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
782006-01	GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS

GENERAL NOTES

1. ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION ADOPTED APRIL 1, 2016 (HEREIN AFTER REFERRED TO AS THE STANDARD SPECIFICATIONS; THE SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS ADOPTED JANUARY 1, 2021; THE LATEST EDITION OF THE ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS; THE STANDARD SPECIFICATIONS FOR WATER & SEWER MAIN CONSTRUCTION IN ILLINOIS, LATEST EDITION; THE DETAILS IN THE PLANS; AND THE SPECIAL PROVISIONS INCLUDED IN THE CONTRACT DOCUMENTS.
2. ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED AS THE LATEST STANDARD OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL EXISTING AND PROPOSED UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE STANDARD SPECIFICATIONS, IF UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY.
4. THE LOCATIONS OF EXISTING UTILITIES SHOWN ON THE PLANS ARE FOR INFORMATIONAL PURPOSES ONLY AND ARE BASED ON FIELD INVESTIGATIONS AND THE BEST INFORMATION AVAILABLE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO ASCERTAIN THEIR EXACT LOCATIONS FROM THE UTILITY COMPANIES AND BY FIELD INSPECTION.
5. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "J.U.L.I.E." AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS UTILITIES. 48 HOURS NOTIFICATION IS REQUIRED.
6. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, THE VILLAGE, AND WITH LOCAL EMERGENCY SERVICES.
7. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
8. WHEN ARTIFICIAL LIGHTING IS UTILIZED IN NIGHT OPERATIONS THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.
9. THE RESIDENT ENGINEER SHALL CONTACT THE ARTERIAL TRAFFIC CONTROL SUPERVISOR, KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF TEMPORARY TRAFFIC CONTROL DEVICES.
10. THE RESIDENT ENGINEER SHALL CONTACT THE AREA TRAFFIC FIELD ENGINEER, EMAD.ALHUSSEINI@ILLINOIS.GOV TWO WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
11. THIS PROJECT REQUIRES A US ARMY CORPS OF ENGINEERS (USACE) 404 PERMIT THAT WILL BE SECURED BY THE DEPARTMENT. ALL CONDITIONS OF THE 404 PERMIT, FOUND IN THE SPECIAL PROVISIONS, MUST BE FOLLOWED. AS A CONDITION OF THIS PERMIT, THE CONTRACTOR WILL NEED TO SUBMIT AN IN-STREAM WORK PLAN (INCLUDING WORK WITHIN WETLANDS) TO THE DEPARTMENT FOR APPROVAL. GUIDELINES ON ACCEPTABLE IN-STREAM WORK TECHNIQUES (INCLUDING WORK WITHIN WETLANDS) CAN BE FOUND ON THE USACE WEBSITE. THE USACE DEFINES AND DETERMINES IN-STREAM WORK. THE COST OF ALL MATERIALS AND LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS TO PREPARE AND IMPLEMENT AN IN-STREAM WORK PLAN (INCLUDING WORK WITHIN WETLANDS) WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED WITH THE EXCEPTION OF COFFERDAMS WHICH WILL BE PAID FOR AS COFFERDAM (TYPE 1) (IN-STEAM/WETLAND WORK) WITH A BASIS OF PAYMENT OF EACH.
12. DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
13. THE DEPARTMENT HAS NOT OBTAINED ANY PERMITS FOR OFFSITE BORROW, WASTE, USE (BWU) AREAS. PRIOR TO WORKING IN BWU AREAS, IF THE CONTRACTOR CHOOSES TO USE ACTIVITIES REQUIRING PERMITS IT IS THE CONTRACTOR'S RESPONSIBILITY TO SECURE THE PROPER PERMITS. IN ADDITION TO THE BORROW REVIEW (BDE 2289) AND USE/WASTE REVIEW (BDE 2290) SUBMITTALS, THE CONTRACTOR WILL NEED TO SUBMIT AN EROSION AND SEDIMENT CONTROL (ESC) PLAN FOR EVERY BWU SITE TO THE DEPARTMENT FOR ACCEPTANCE. GUIDELINES FOR ACCEPTABLE BWU PRACTICES CAN BE FOUND IN SECTION IL.G.1 AND 2 OF THE SWPPP. THE COST OF ALL MATERIALS AND LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS TO PREPARE AND IMPLEMENT ESC PLANS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
14. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY MEASUREMENTS NEEDED BEFORE THE ORDERING OF MATERIALS. ANY VARIATIONS FROM THE PLANS ARE NOT THE RESPONSIBILITY OF THE DEPARTMENT AND NO ADDITIONAL COMPENSATION WILL BE AWARDED.
15. THE CONTRACTOR SHALL TAKE CARE IN REMOVING OR EXCAVATING NEAR ALL EXISTING ITEMS WHICH WILL REMAIN. ANY DAMAGE DONE TO EXISTING ITEMS BY THE CONTRACTOR SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE. ANYTHING DISTURBED OUTSIDE THE PROJECT LIMITS IS THE CONTRACTOR'S RESPONSIBILITY AND SHALL BE REPLACED IN KIND. NO ADDITIONAL COMPENSATION WILL BE AWARDED.
16. THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS, AS REQUIRED, PRIOR TO COMMENCING WITH CONSTRUCTION. ANY COST ASSOCIATED WITH OBTAINING THESE PERMITS SHALL BE CONSIDERED INCLUDED IN THE CONTRACT UNIT PRICE FOR THE ITEMS BEING INSTALLED.
17. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING TEMPORARY DRAINAGE THROUGHOUT THE CONSTRUCTION OF THIS PROJECT. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE PROPOSED DRAINAGE ITEMS.
18. NO WORK SHALL COMMENCE UNTIL TRAFFIC CONTROL REQUIREMENTS ARE MET.
19. THE COST OF SAW CUTTING PRIOR TO THE REMOVAL OF ANY ITEM NOTED ON THE PLAN OR AS DIRECTED BY THE ENGINEER, (FULL DEPTH) SHALL BE INCLUDED IN THE UNIT PRICES FOR THE VARIOUS REMOVAL PAY ITEMS.

GENERAL NOTES (CONT'D)

20. THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER A SATISFACTORY PROGRESS SCHEDULE AND CRITICAL PATH SCHEDULE WHICH SHALL SHOW THE PROPOSED SEQUENCE OF WORK AT THE TIME OF THE PRE-CONSTRUCTION CONFERENCE.
21. THE CONTRACTOR WILL BE REQUIRED TO RELOCATE OR REMOVE AND REPLACE ALL ROAD SIGNS WHICH INTERFERE WITH CONSTRUCTION OPERATIONS AND TO TEMPORARILY REST ALL SUCH SIGNS DURING ALL STAGES OF CONSTRUCTION. THIS COST SHALL BE INCLUDED IN THE COST OF THE CONTRACT. ALL WORK INVOLVING ROAD SIGNS SHALL BE GOVERNED BY THE FOLLOWING:

a. SIGNS SHALL NOT BE REMOVED UNTIL NECESSITATED BY THE PROGRESS OF WORK.

b. EVERY SIGN REMOVED MUST BE RE-ERECTED AT A TEMPORARY LOCATION IN A WORKMANLIKE MANNER AND VISIBLE TO THE TRAFFIC FOR WHICH IT IS INTENDED. ALL SIGNS MUST BE MAINTAINED STRAIGHT AND CLEAN FOR THE DURATION OF THE TEMPORARY SETTING.

c. ALL UNUSED SIGNS SHALL BE RETURNED TO THE APPROPRIATE GOVERNING BODY AS DIRECTED BY THE ENGINEER.

d. LONGER POSTS MAY BE REQUIRED AT SOME TEMPORARY OR PERMANENT SIGN LOCATIONS TO MAINTAIN PROPER SIGN ELEVATIONS AND SHALL BE INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT ACCORDING TO ARTICLE 107.25 OF THE STANDARD SPECIFICATIONS.
22. ALL ELEVATIONS ARE ON THE U.S.G.S. DATUM NAVD 88.
23. CONNECTION OF EXISTING SEWER PIPE INTO PROPOSED SEWER STRUCTURES SHALL BE INCLUDED IN THE COST OF THE PROPOSED SEWER STRUCTURE. ANY ADDITIONAL SEWER PIPE REQUIRED TO MAKE THE CONNECTION SHALL BE THE SAME SIZE AND MATERIAL TYPE AS THE EXISTING SEWER PIPE AND SHALL BE INCLUDED IN THE COST OF THE STRUCTURE.
24. ALL OFFSET LOCATIONS GIVEN ON THE DETAILED PLANS FOR STRUCTURES, BACKS OF CURB, ETC. ARE FROM THE CENTERLINE AS SHOWN ON THE PLANS.
25. ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES WHICH OBSTRUCTS THE NATURAL FLOW OF WATER SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF IMPROVEMENT. ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE PROPOSED DRAINAGE ITEMS.
26. FRAME ELEVATIONS GIVEN ON THE PLANS ARE ONLY TO ASSIST THE CONTRACTOR IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF THE STRUCTURE. FRAMES ON ALL STRUCTURES WILL BE ADJUSTED TO THE FINAL ELEVATION AND CROSS SLOPE OF THE AREA IN WHICH THEY ARE LOCATED.
27. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING FRESH CONCRETE FROM DAMAGE AND VANDALISM. ANY DAMAGED OR VANDALIZED CONCRETE SHALL BE REMOVED AND REPLACED AT THE CONTRACTOR'S EXPENSE.
28. ALL DISTURBED AREAS WITHIN THE PROJECT THAT ARE NOT OTHERWISE SURFACED SHALL BE CLEANED, LAYERED WITH TOPSOIL, AND SEEDED OR SODDED AS SHOWN ON THE PLANS.
29. THE COMPENSATORY STORAGE SHALL BE OPERATIONAL PRIOR TO PLACEMENT OF FILL, STRUCTURES, OR OTHER MATERIALS IN THE REGULATORY FLOODPLAIN. GRADING IN SPECIAL MANAGEMENT AREAS SHALL BE DONE IN SUCH A MANNER THAT THE EXISTING FLOODPLAIN STORAGE IS MAINTAINED AT ALL TIMES.
30. WHERE UNDERPASS LIGHTING IS PRESENT ON A STRUCTURE, THE CONTRACTOR SHALL ADJUST THE PROTECTIVE SHIELDING TO RIDE ABOVE THE EXISTING LIGHTING FIXTURES IN ORDER TO MAINTAIN THE EXISTING LEVEL OF LIGHTING ON THE ROADWAY UNDERNEATH. DETAILS SHALL BE APPROVED BY THE ENGINEER BEFORE INSTALLATION.

ANY ADJUSTMENT DONE TO THE PROTECTIVE SHIELD SYSTEM MUST NOT CHANGE THE LOAD-CARRYING CAPACITY OR CONTAINMENT SPECIFICATIONS AS INDICATED IN THE STANDARD SPECIFICATIONS. THE COST OF ADJUSTING SHIELDING IS INCLUDED IN THE COST OF PROTECTIVE SHIELD.

31. BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
32. THE PROPOSED CONCRETE BUS PAD SHALL BE OF 5 IN. THICKNESS AND PAID FOR AS PCC SIDEWALK 5". CONSTRUCTION OF THE BUS PAD SHALL BE COORDINATED WITH PACE.
33. THE LOCATION OF COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT [OR COMBINATION CURB AND GUTTER (OF THE TYPE SPECIFIED ON THE PLANS)] WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
34. THE SUBGRADE STABILITY SHALL BE VERIFIED BY PROOF ROLLING WITH A FULLY LOADED TANDEM-AXLE TRUCK.
35. ANY AGGREGATE SUBGRADE IMPROVEMENT CONTAMINATED AND/OR DAMAGED BY THE CONTRATOR'S VEHICLES AND/OR EQUIPMENTS IS TO BE REMOVED AND REPLACED AS DIRECTED BY THE ENGINEER AT CONTRACTOR'S EXPENSE.
36. PIPE UNDERDRAINS SHALL BE INSTALLED ACCORDING TO SECTION 601 OF THE SSRBC AND STANDARD 601001-05. TOP OF PIPE UNDERDRAINS SHALL BE PLACED MINIMUM 6" BELOW THE AGGREGATE SUBGRADE IMPROVEMENT LAYER. THE COST OF MAKING PIPE UNDERDRAIN CONNECTIONS TO DRAINAGE STRUCTURES SHALL BE INCLUDED IN THE COST OF THE PIPE UNDERDRAINS.
37. BACKFILLING STORM SEWER CONSTRUCTED UNDER THE ROADWAY SPECIFIED UNDER ART. 550.07(b,c) OF THE SSRBC WILL NOT BE ALLOWED.
38. THE CONTRACTOR SHALL CONTACT THE ROADSIDE DEVELOPMENT UNIT AT 847-705-4171 TO SCHEDULE A WALKTHROUGH TO DETERMINE TREES FOR PRUNING, ROOT PRUNING, REMOVAL, AND PROTECTION AT LEAST 7 DAYS PRIOR TO THE COMMENCEMENT OF CONSTRUCTION.

39. THERE ARE EXISTING 138 KV LINES WITHIN THE PROJECT LIMITS. THE CONTRACTOR NEEDS TO BE AWARE THAT NO OUTAGE OR PROTECTION CAN BE PROVIDED TO THESE FACILITIES. PER COMED AND OSHA REQUIREMENTS, ALL CONTRACT ACTIVITIES MUST STAY 15 FEET IN ANY DIRECTION FROM THE 138 KV LINES AND ANY COMED CONDUCTORS.

DUE TO THE OVERHEAD 138 KV ELECTRIC LINES, USING CRANES MAY NOT BE POSSIBLE. THE CONTRACTOR SHALL EXERCISE EXTREME CAUTION IN WORKING AROUND THESE FACILITIES AND ANY OTHER FACILITIES AND SHALL ENSURE CONFLICTS WITH CRANES AND OTHER EQUIPMENT ARE AVOIDED ENTIRELY. THE COST TO COMPLY WITH THESE REQUIREMENTS WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF HIGHWAY STRUCTURE PAY ITEMS.

MODEL Default
FILE NAME: W191168 IDOT_Cermak_Road/Cadd_Sheets/D62H51_SHT_GENERAL_NOTES.dgn



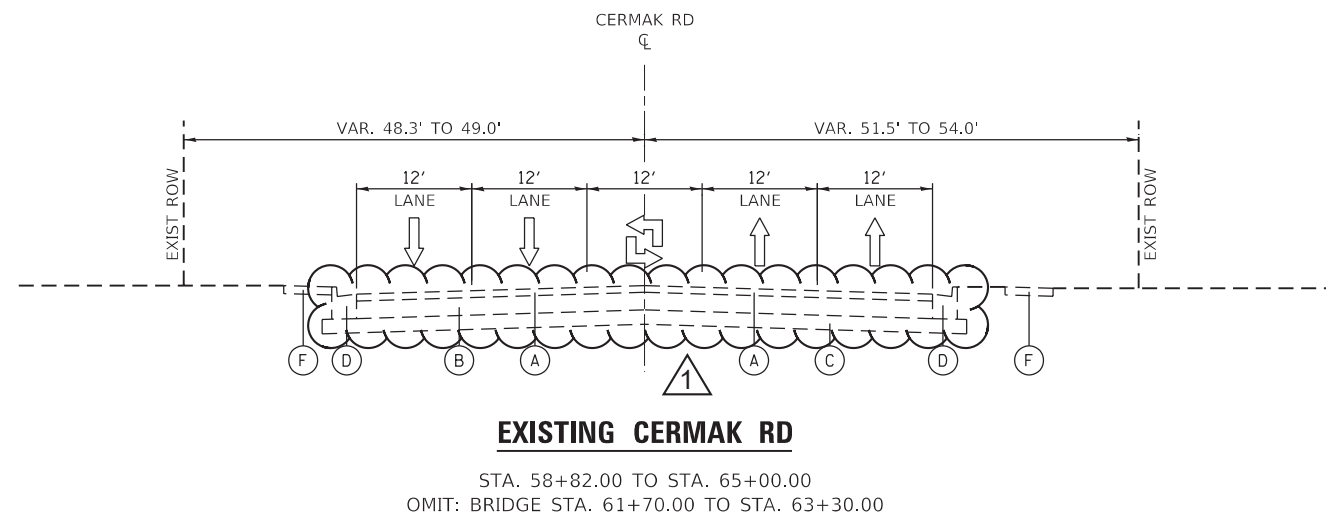
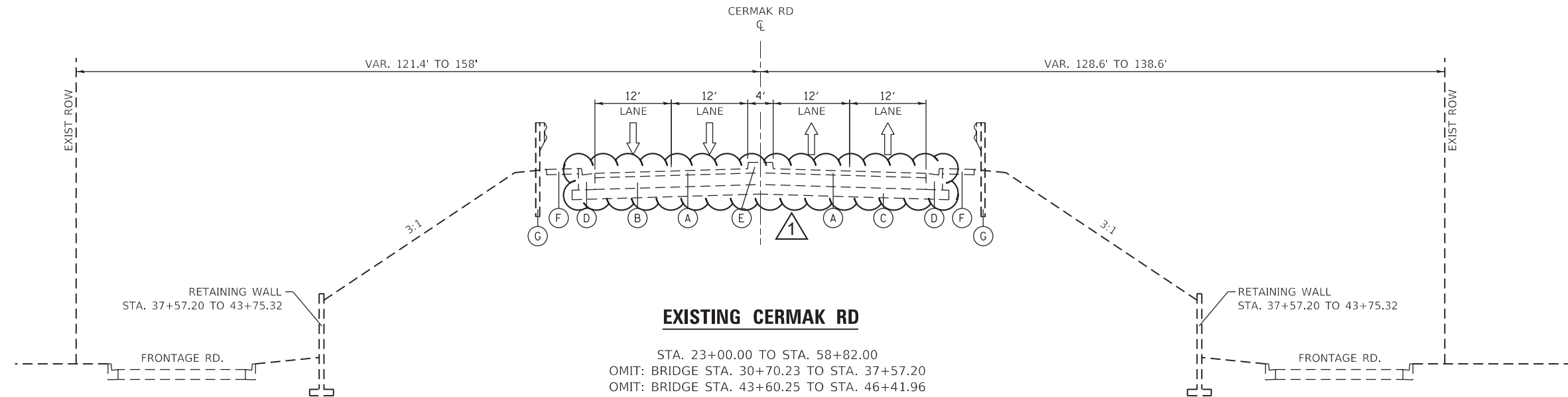
USER NAME = WInson	DESIGNED - WJT	REVISED - 02/23/2021
	DRAWN - WJT	REVISED -
PLOT SCALE = 20,0000 ' / in.	CHECKED - MTC	REVISED -
PLOT DATE = 2/23/2021	DATE - 11/05/20	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX, HIGHWAY STANDARDS, GENERAL NOTES, & COMMITMENTS
CERMAK RD OVER IHB RR, 25th AVE, & ADDISON CREEK

SCALE: N.T.S.	SHEET 1 OF 1 SHEETS	STA. N/A TO STA. N/A
---------------	---------------------	----------------------

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2018-126-BR	COOK	194	2
CONTRACT NO. 62H51				
		ILLINOIS	FED. AID PROJECT	



- EXISTING LEGEND**
- (A) EXIST. HMA PAVEMENT; 2.5" (R - PAVEMENT REMOVAL)
 - (B) EXIST. PCC PAVEMENT; 9" (R - PAVEMENT REMOVAL)
 - (C) EXIST. GRANULAR SUBBASE; 8" (R)
 - (D) EXIST. CONC. CURB AND GUTTER (R)
 - (E) EXIST. BARRIER MEDIAN (R)
 - (F) EXIST. PCC SIDEWALK (R)
 - (G) EXIST. GUARDRAIL (R)
 - (R) ITEM TO BE REMOVED PER LOCATIONS IN THE PLANS

MODEL Default
FILE Name: W191168 IDOT CermaK Road/CADD Sheets/D1621651_SHT_TYPICAL_SECTIONS.dgn



USER NAME = nwbpw	DESIGNED - WJT	REVISED - 02/22/2021
	DRAWN - WJT	REVISED -
PLOT SCALE = 20.0000 ' / in.	CHECKED - MTC	REVISED -
PLOT DATE = 2/22/2021	DATE - 11/05/20	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EXISTING TYPICAL SECTIONS
CERMAK RD OVER IHB RR, 25th AVE, & ADDISON CREEK

SCALE: N.T.S. SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2018-126-BR	COOK	194	17
CONTRACT NO. 62H51				
ILLINOIS FED. AID PROJECT				

REVISED SHEET 2/22/2021

1

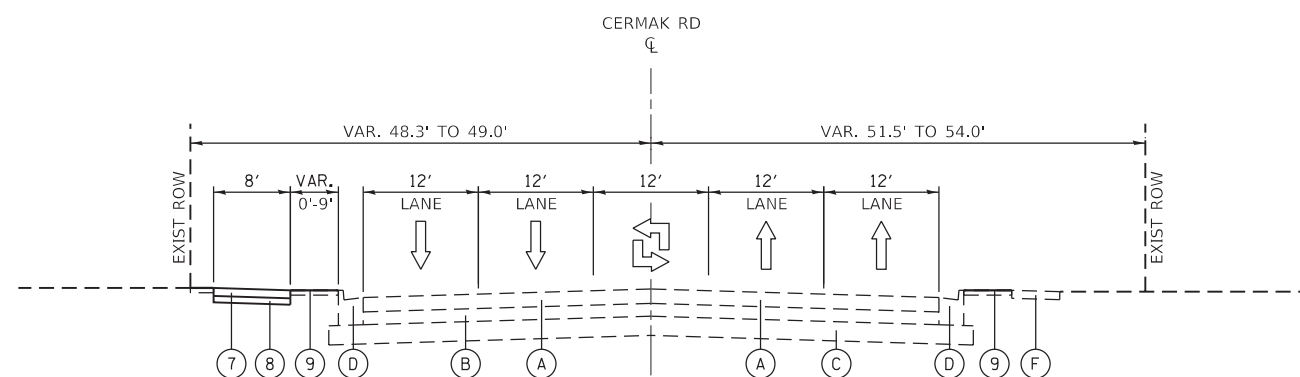
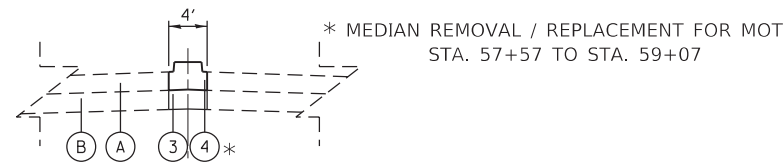
- (A) EXIST. HMA PAVEMENT; 2.5" (R - PAVEMENT REMOVAL)
- (B) EXIST. PCC PAVEMENT; 9" (R - PAVEMENT REMOVAL)
- (C) EXIST. GRANULAR SUBBASE; 8" (R)
- (D) EXIST. CONC. CURB AND GUTTER (R)

- ### PROPOSED LEGEND

- ## HOT-MIX ASPHALT MIXTURE REQUIREMENTS CHART

QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
2. THE AC TYPE FOR POLYMERIZED HMA MIXES SHALL BE SBS/SBR PG 76-22 AND FOR NON-POLYMERIZED HMA THE AC TYPE SHALL BE PG 64-22 UNLESS MODIFIED BY SPECIAL PROVISIONS.
3. PC CONCRETE TEMPORARY PAVEMENT (IF USED) SHALL BE 8.0 INCHES. AND CONSIST OF CLASS PV CONCRETE MEETING THE REQUIREMENTS OF ART. 1020 OF THE STANDARD SPECIFICATIONS. TEMPORARY PCC PAVEMENT DOES NOT REQUIRE DOWEL BARS.
4. TEMPORARY PAVEMENT SHALL BE PLACED ON SUBBASE GRANULAR MATERIAL, TYPE B 4 INCH
5. FOR THE USE OF RECYCLED MATERIALS SEE DISTRICT ONE SPECIAL PROVISIONS.




STA. 58+82.00 TO STA. 65+00.00
OMIT: BRIDGE STA. 61+70.00 TO STA. 63+30.00
PROJECT ENDS:
STA. 64+60



1 REVISED SHEET 2/22/2021



USER NAME = nwbpw	DESIGNED - WJT	REVISED -  02/22/2021
	DRAWN - WJT	REVISED -
PLOT SCALE = 20.0000" / in.	CHECKED - MTC	REVISED -
PLOT DATE = 2/22/2021	DATE - 11/05/20	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PROPOSED TYPICAL SECTIONS
CERMAK RD OVER IHB RR, 25th AVE, & ADDISON CREEK

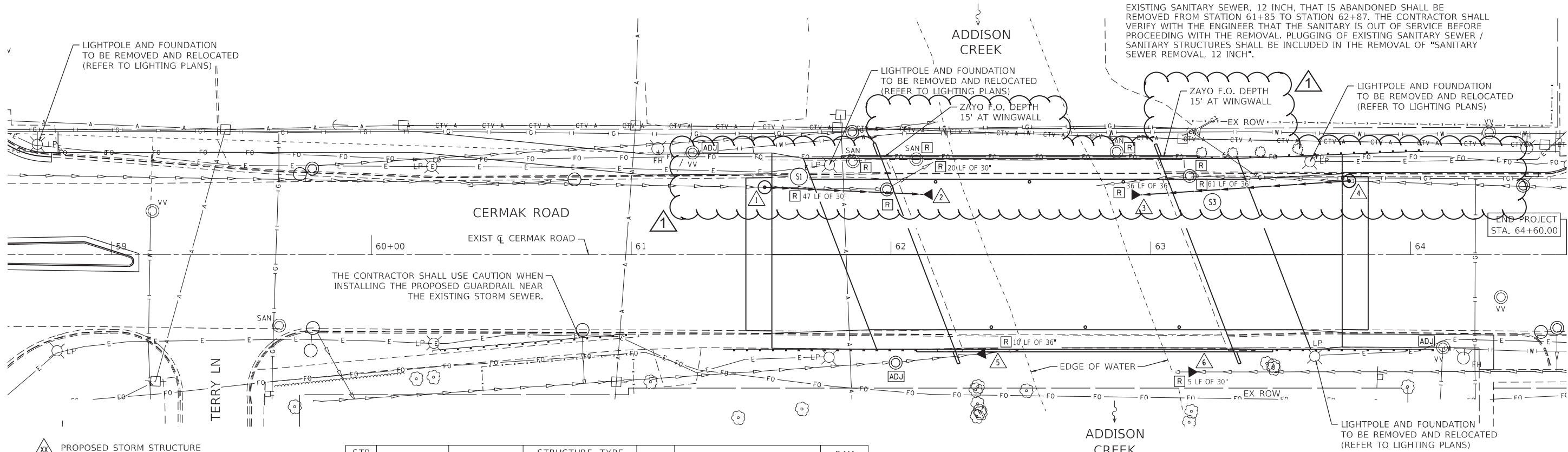
SCALE: N.T.S.				SHEET 2 OF 2 SHEETS		STA. TO STA.		ILLINOIS FED. AID PROJECT		CONTRACT NO. 62H51	
---------------	--	--	--	---------------------	--	--------------	--	---------------------------	--	--------------------	--

MODEL: Default
FILE NAME: W:\191-168 IDOT Cermak Road\CADD Sheets\162H51 SHT TYPICAL SECTIONS.dgn

PLAN	SURVEYED	DATE
NO.	BY	
NOTE BOOK		
NO.		
NO.		
NO.		

PROFILE	SURVEYED	DATE
NO.	BY	
NOTE BOOK		
NO.		
NO.		
NO.		

MODEL: Default
FILE NAME: W:\191-168 IDOT Cermak Road\CADD Sheets\02151_SHT_DRAINAGE.dgn

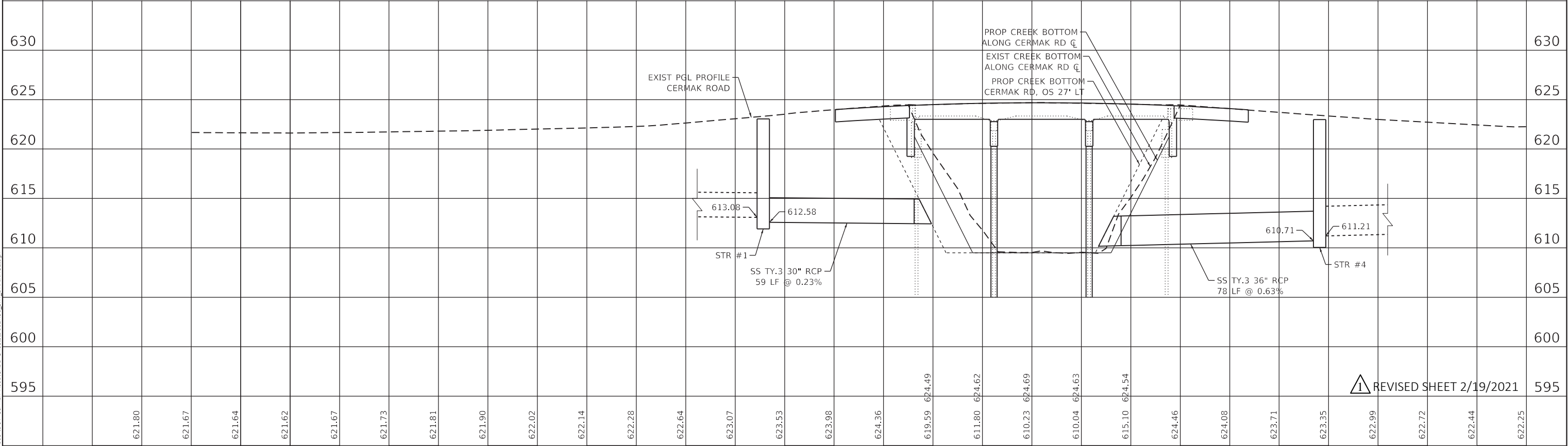


- XX PROPOSED STORM STRUCTURE
- SXX PROPOSED STORM SEWER
- R EXISTING STRUCTURE TO BE REMOVED
- ADJ EXISTING STRUCTURE TO BE ADJUSTED

STR NO	STA	O/S	STRUCTURE TYPE		F&G	INVERT ELEV		RIM ELEV
			MH	OTHER				
1	61+51.36	23.15' LT	5'A		1 CL	613.08 (W) 612.58 (E)	623.05	
2	62+12.34	24.90' LT		PRC FES 30"	-	612.44 (W)	-	
3	62+96.20	22.93' LT		PRC FES 36"	-	610.21 (E)	-	
4	63+37.95	29.49' LT	5'A		1 CL	610.71 (W) 611.21 (E)	622.98	
5	62+36.39	38.04' RT		PRC FES 36"	-	612.17 (W)	-	
6	63+14.47	45.21' RT		PRC FES 30"	-	611.30 (E)	-	

PIPE NO	FROM STR	TO STR	DESCRIPTION	DIA (IN)	LENGTH (FT)	SLOPE (%)	TBF (CY)
S1	1	2	STORM SEWERS, CLASS A, TYPE 3	30	59	0.23%	71
S3	4	3	STORM SEWERS, CLASS A, TYPE 3	36	78	0.63%	129

635 REFER TO PLAN AND PROFILE SHEETS FOR STRUCTURE ADJUSTMENTS PRIOR TO STATION 59+00



1 REVISED SHEET 2/19/2021



USER NAME = nwbpw	DESIGNED - WJT	REVISED - 02/19/2021
PLOT SCALE = 40,0000 ' / in.	DRAWN - WJT	REVISED -
PLOT DATE = 2/19/2021	CHECKED - MTC	REVISED -
	DATE - 11/05/20	REVISED -

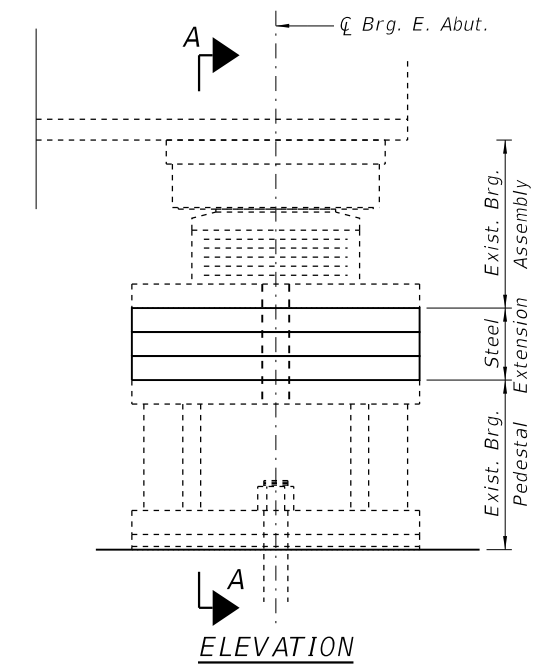
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRAINAGE AND UTILITIES
CERMAK RD OVER IHB RR, 25th AVE, & ADDISON CREEK

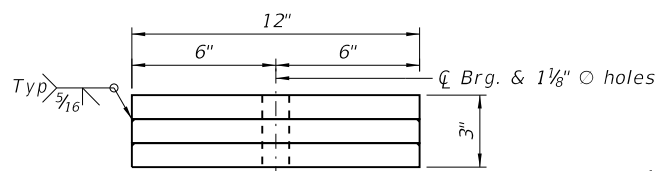
SCALE: 1" = 20' SHEET 1 OF 1 SHEETS STA. 58+60 TO STA. 64+60

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2018-126-BR	COOK	194	39
CONTRACT NO. 62H51				
ILLINOIS FED. AID PROJECT				

FILE NAME: \\10.10.10.8\\Data\\DOT Work Files\\191-168 IDOT Central Road\\CADD Sheets\\Structural\\FINAL PLANS\\HBR & Gardner\\SHT-46 unit 3 Bearing Extension Details.dgn

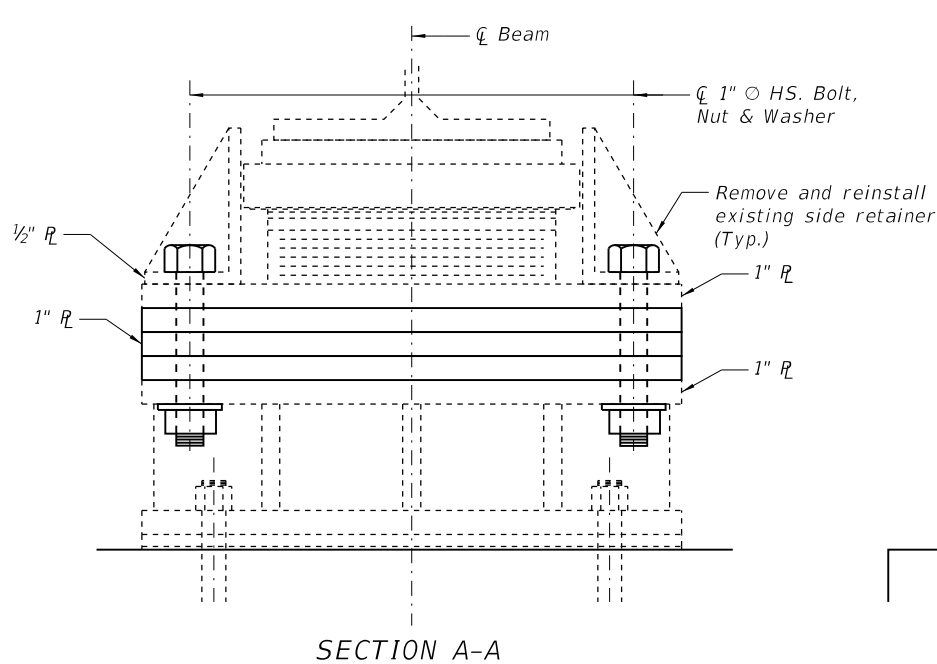


ELEVATION

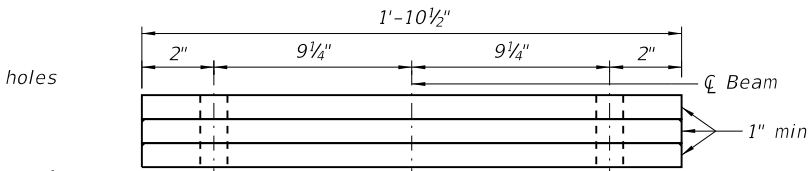


END VIEW STEEL EXTENSION

E. ABUT. EXTENSION DETAIL
(Existing Type II Bearing Assembly)



SECTION A-A



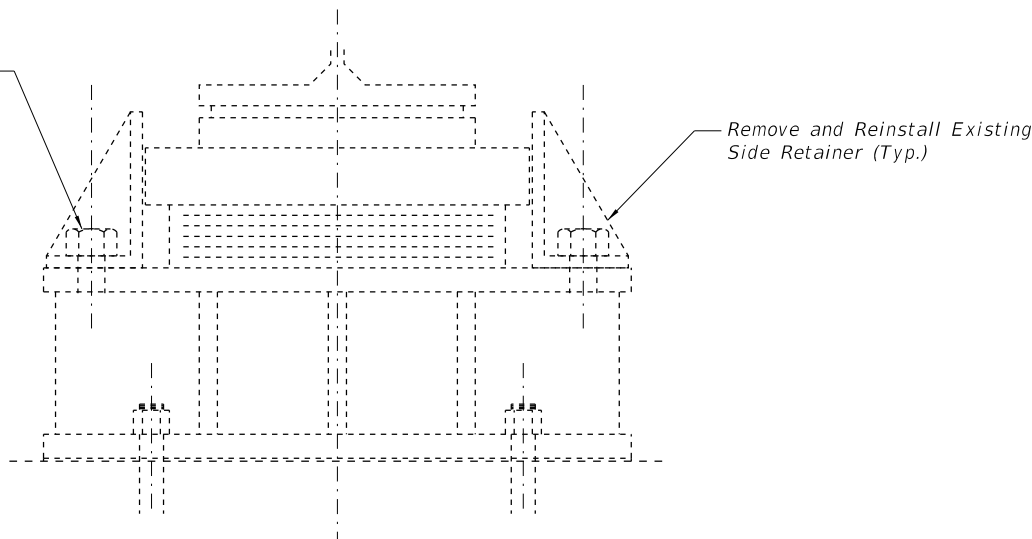
1/8" hole (typ.)

ELEVATION STEEL EXTENSION

(12 Req'd Thus)

1

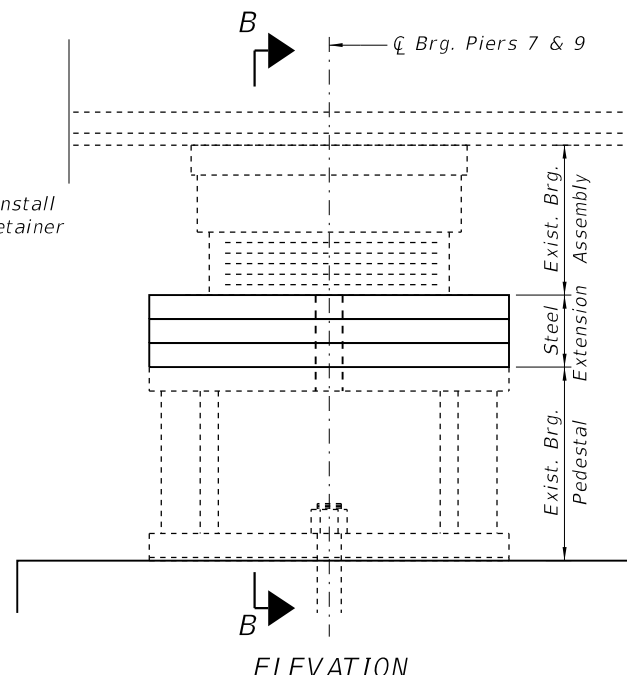
Existing 1" \varnothing Bolt installed in drilled and tapped hole in existing extension. Remove bolt and ream hole in existing extension to 1 1/8" \varnothing . Replace with 1" \varnothing H.S. Bolt after extension is installed. (typ.)



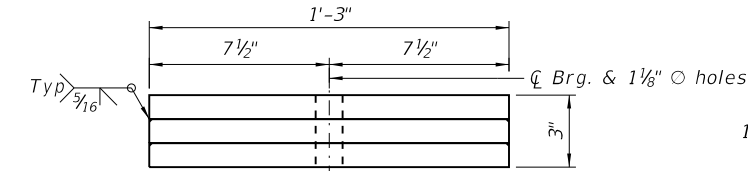
TYPICAL EXISTING SIDE
RETAINER DETAIL

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Furnishing and Erecting Structural Steel	Pound	11,780

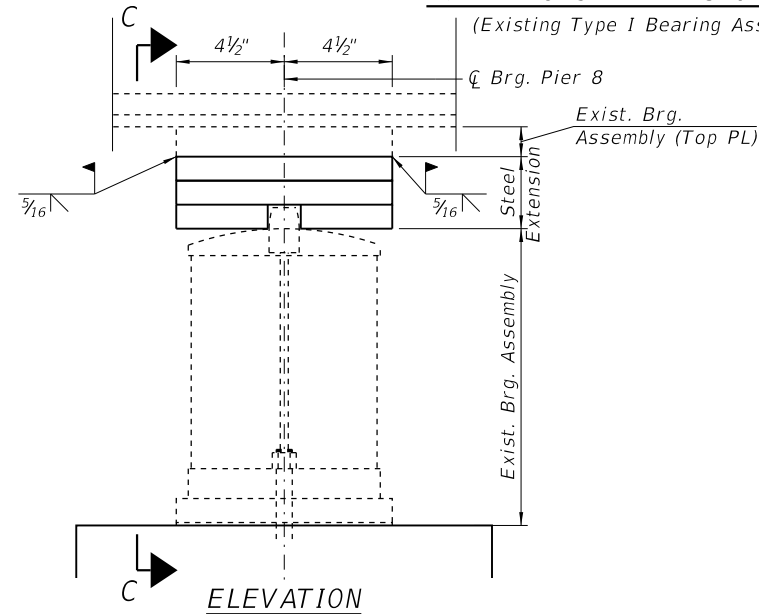


ELEVATION

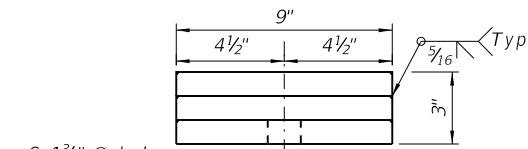


END VIEW STEEL EXTENSION

PIER 7 & 9 EXTENSION DETAIL
(Existing Type I Bearing Assembly)



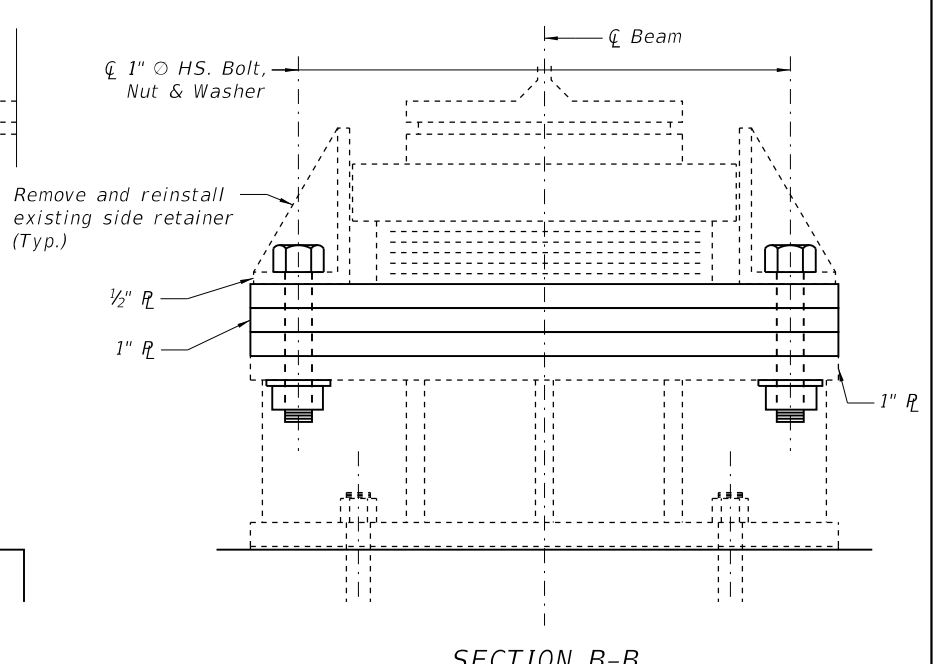
ELEVATION



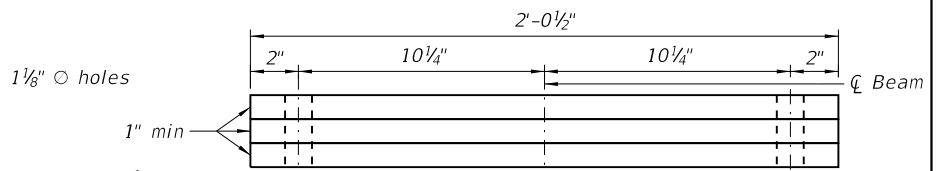
3/16" holes

END VIEW STEEL EXTENSION

PIER 8 EXTENSION DETAIL
(Existing Steel Fixed Bearing)

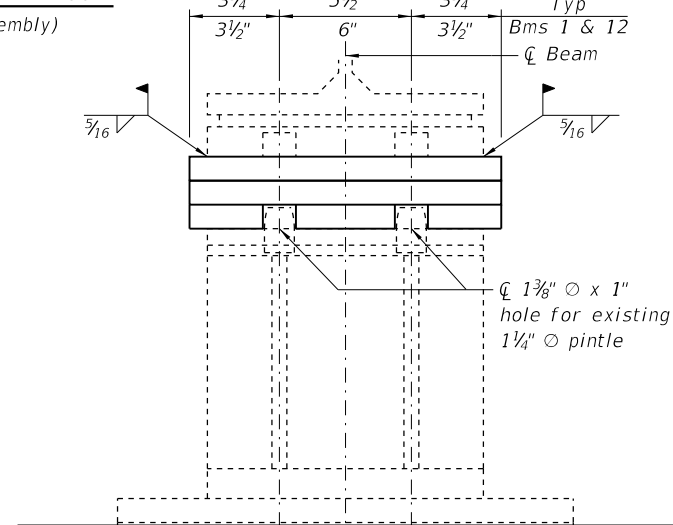


SECTION B-B

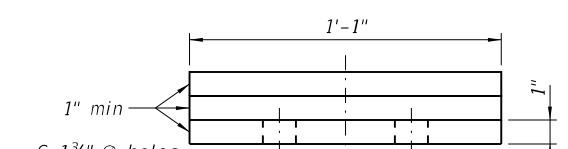


ELEVATION STEEL EXTENSION

(24 Req'd Thus)



SECTION C-C



1 3/8" holes

ELEVATION STEEL EXTENSION

(12 Req'd)

Note:
The dimensions, quantity, and orientation of all existing bearing components and proposed steel extensions shown on this sheet are based on existing plans. The Contractor shall field verify the existing bearing details and notify the Engineer of any variation from the design plans prior to fabrication of the steel extensions.



USER NAME = HBrlerton	DESIGNED - HB	REVISED - 02/10/2021 JJI
CHECKED - JJI	REVIS	
PLOT SCALE =	DRAWN - HB	REVIS
PLOT DATE = 2/18/2021	CHECKED - JJI	REVIS

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

UNIT 3 - BEARING EXTENSION DETAILS
STRUCTURE NO. 016-0631

SHEET NO. 46 OF 59 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2018-126-BR	COOK	194	101
CONTRACT NO. 62H51				
ILLINOIS FED. AID PROJECT				

REVISED SHEET 2/19/2021

Salvage: Existing steel railing and posts. See General Notes sheet 2 of 27.

WATERWAY INFORMATION

Drainage Area = 21 sq. mi.			Existing Overtopping Elev. = 624.3		at Sta. 63+27.4				
			Proposed Overtopping Elev. = 624.4		at Sta. 62+94.6				
Flood Event	Freq. Yr.	Discharge Ft/s	Waterway Opening-ft ²		Natural H.W.E. ft.	Head - Ft.		Headwater Elev. ft.	
			Exist.	Prop.		Exist.	Prop.	Exist.	Prop.
	2	1160.67	343	478	616.10	0.03	0.01	616.13	616.11
	10	1438.05	468	489	617.72	0.17	0.10	617.89	617.82
Design	50	1836.33	529	557	618.60	0.19	0.12	618.79	618.72
Base	100	1931.96	568	591	619.04	0.19	0.12	619.23	619.16
Scour Check	200	2068.58	610	647	619.72	0.18	0.10	619.90	619.82
Max. Calc.	500	2200	677	722	620.60	0.17	0.10	620.77	620.70

10-Year velocity through existing structure = 3.05 ft/s
10-Year velocity through proposed structure = 3.01 ft/s

DESIGN SCOUR ELEVATION TABLE

Event/Limit State	Design Scour Elevations (ft.)				Item 113
	W. Abut.	Pier 1	Pier 2	E. Abut.	
Q100	617.48	606.29	606.29	617.50	8
Q200	617.48	606.29	606.29	617.50	
Design	617.48	606.29	606.29	617.50	
Check	617.48	606.29	606.29	617.50	

SEISMIC DATA

Seismic Performance Zone (SPZ) = 1
Design Spectral Acceleration at 1.0 sec. (SD1) = 0.062
Design Spectral Acceleration at 0.2 sec. (SDS) = 0.114
Soil Site Class = C

DESIGN SPECIFICATIONS

2017 AASHTO LRFD Bridge Design Specifications, 8th Edition

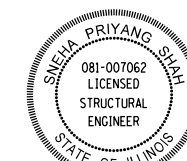
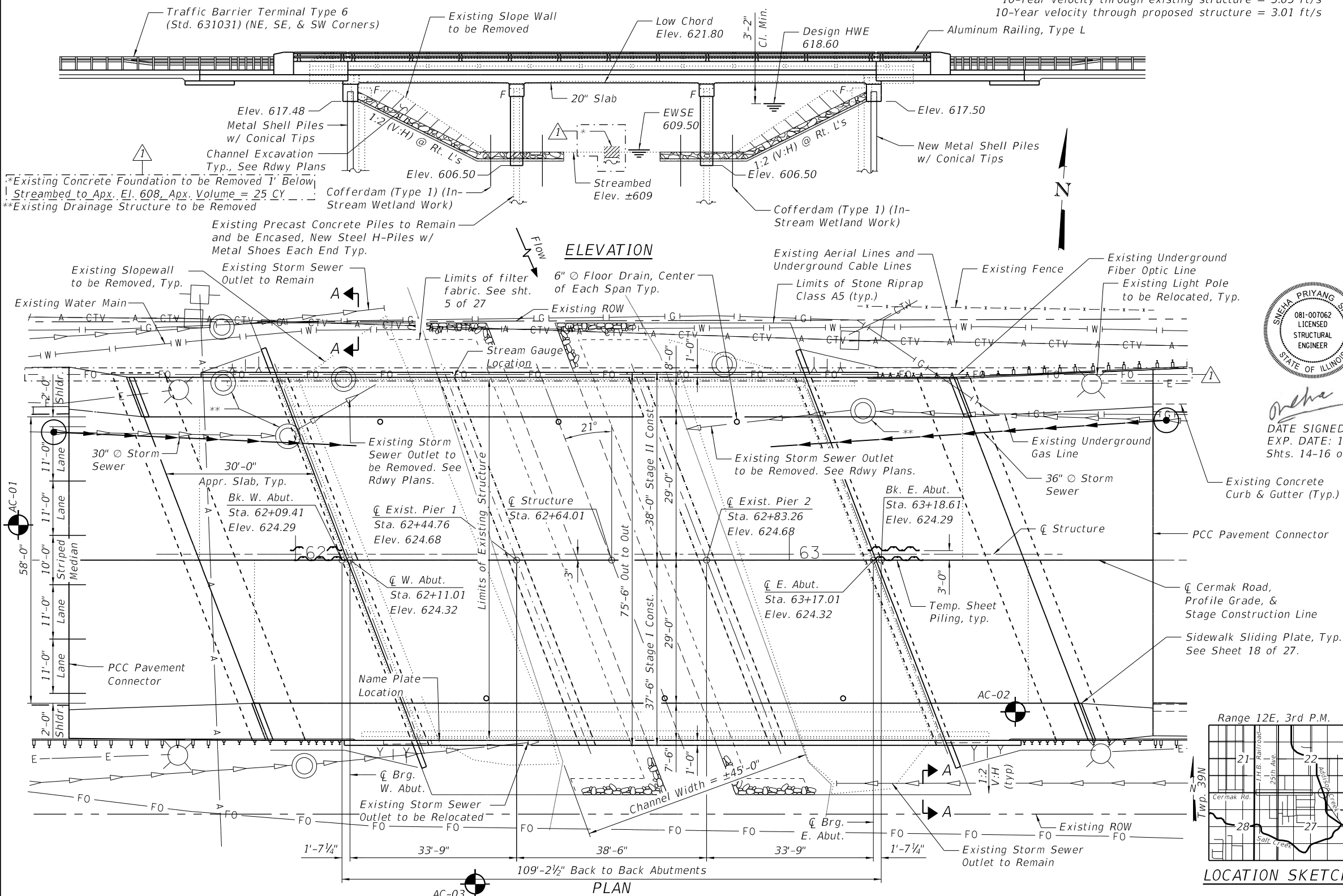
DESIGN STRESSES

FIELD UNITS

$f'_c = 5,000 \text{ psi}$ (Superstructure)
 $f'_c = 4,000 \text{ psi}$ (Approach Slabs)
 $f'_c = 3,500 \text{ psi}$
 $f_y = 60,000 \text{ psi}$ (Reinforcement)

LOADING HL-93

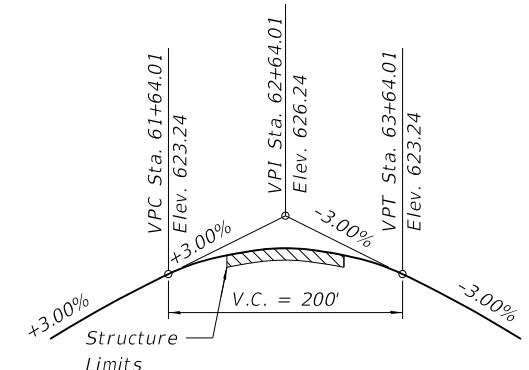
Allow 50#/sq. ft. for future wearing surface.



DATE SIGNED: 09-10-2020
EXP. DATE: 11-30-2022
Shts. 14-16 of 27



DATE SIGNED: 09-10-2020
EXP. DATE: 11-30-2022
Shts. 1-13, 17-27 of 27




EXISTING & PROPOSED
CERMAK ROAD PROFILE

(Along ζ of Roadway)

GENERAL PLAN & ELEVATION
CERMAK ROAD OVER ADDISON CREEK
F.A.U. RTE 1453 - SECTION 2018-126-BR
COOK COUNTY
STATION 62+64.01
STRUCTURE NO. 016-0633

1 REVISED SHEET 2/22/2021



USER NAME = Winson	DESIGNED - HB	REVISED -  02/19/2021 JJ
	CHECKED - JJ	REVISED -
PLOT SCALE =	DRAWN - HB	REVISED -
PLOT DATE = 2/19/2021	CHECKED - JJ	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SHEET NO. 1 OF 27 SHEETS

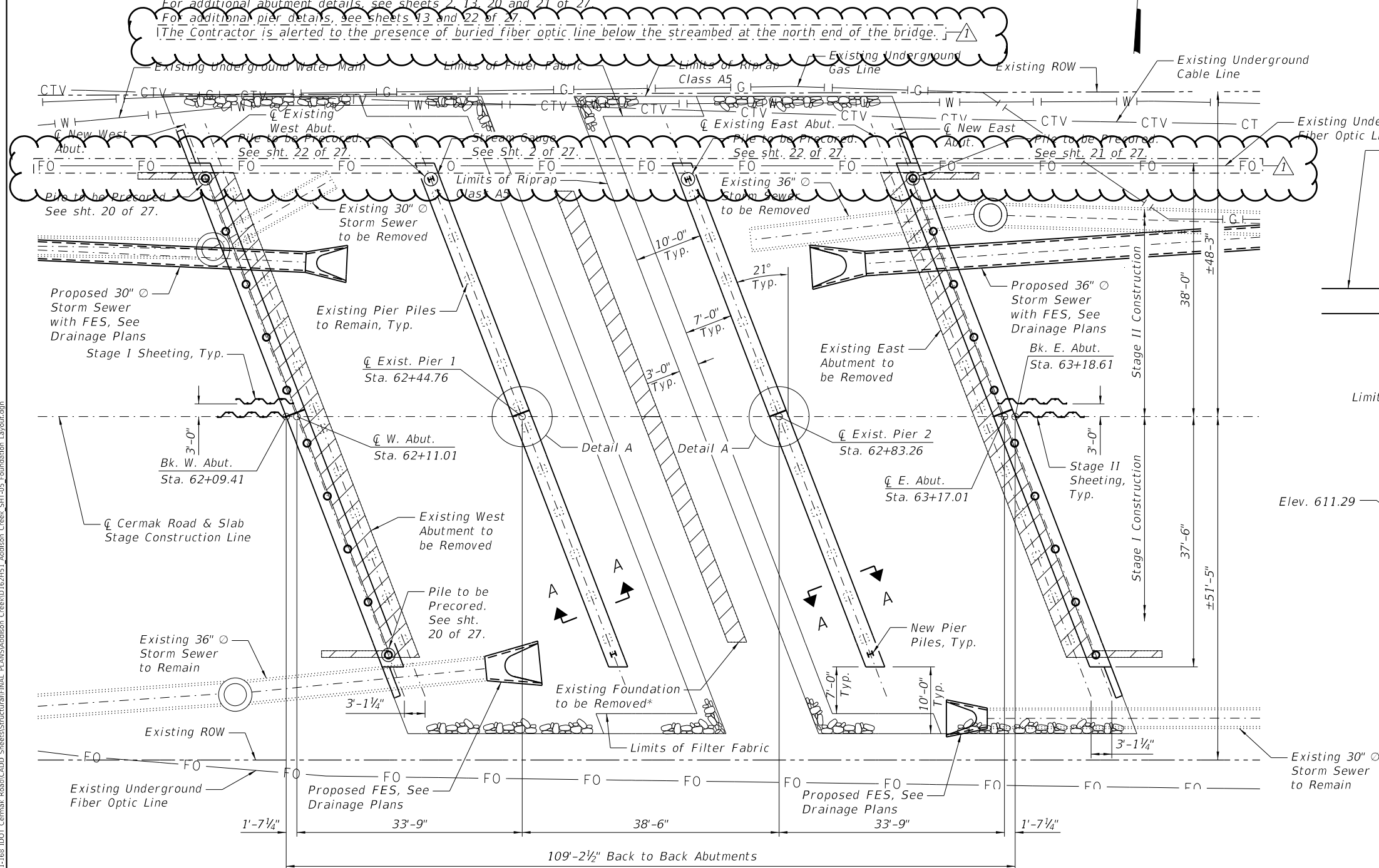
F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2018-126-BR	COOK	194	147
		CONTRACT NO. 62H51		
ILLINOIS		FED. AID PROJECT		

		CONTRACT
ILLINOIS	FED. AID PROJECT	

LEGEND

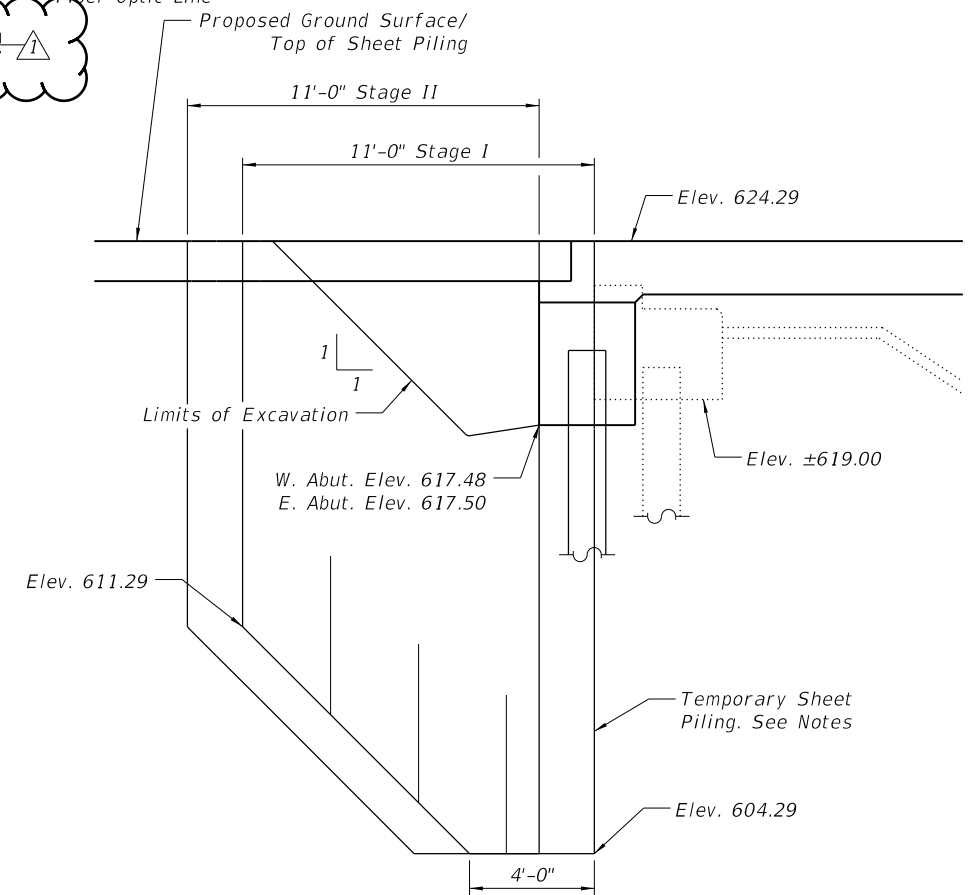
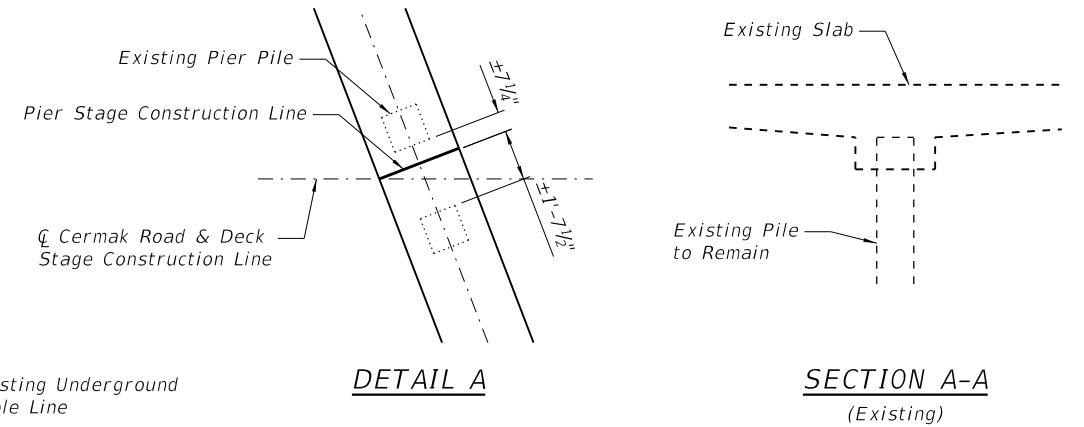


Notes:
Abutment piles are to be driven after removal of existing storm sewers and before installation of proposed storm sewers.
See roadway plans for removal and storm sewer details.
Substructure stage construction lines differ from superstructure stage construction line. See sheets 20 thru 22 for details.
Temporary sheet piling shall be Grade 50 with a minimum effective section modulus of 14.0 in. /ft.
If the Contractor chooses to alter the temporary sheet piling design requirements shown on the plans, a design submittal including plan details and calculations will be required for review and acceptance by the Engineer.
Existing slopewall removal shall not damage existing storm sewers that are to remain.
For additional riprap details, see sheet 2 of 27.
For additional abutment details, see sheets 2, 13, 20 and 21 of 27.
For additional pier details, see sheets 13 and 22 of 27.
The Contractor is alerted to the presence of buried fiber optic line below the streambed at the north end of the bridge.



PLAN

*Cost included with Removal of Existing Structures.



TEMPORARY SHEET PILING DETAILS

BILL OF MATERIAL

Item	Unit	Total
Temporary Sheet Piling	Sq. Ft.	782

REVISED SHEET 2/22/2021



USER NAME = HBrlerton	DESIGNED - HB	REVISED - 02/19/2021 JJI
CHECKED - JJI	REVISOR -	
PLOT SCALE =	DRAWN - HB	REVISOR -
PLOT DATE = 2/19/2021	CHECKED - JJI	REVISOR -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

FOUNDATION LAYOUT
STRUCTURE NO. 016-0633

SHEET NO. 5 OF 27 SHEETS

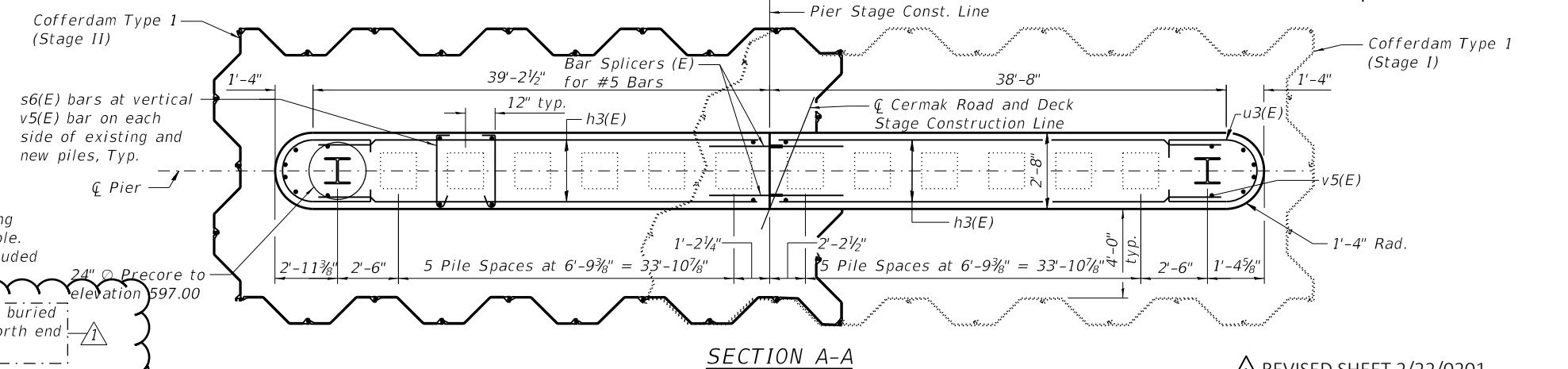
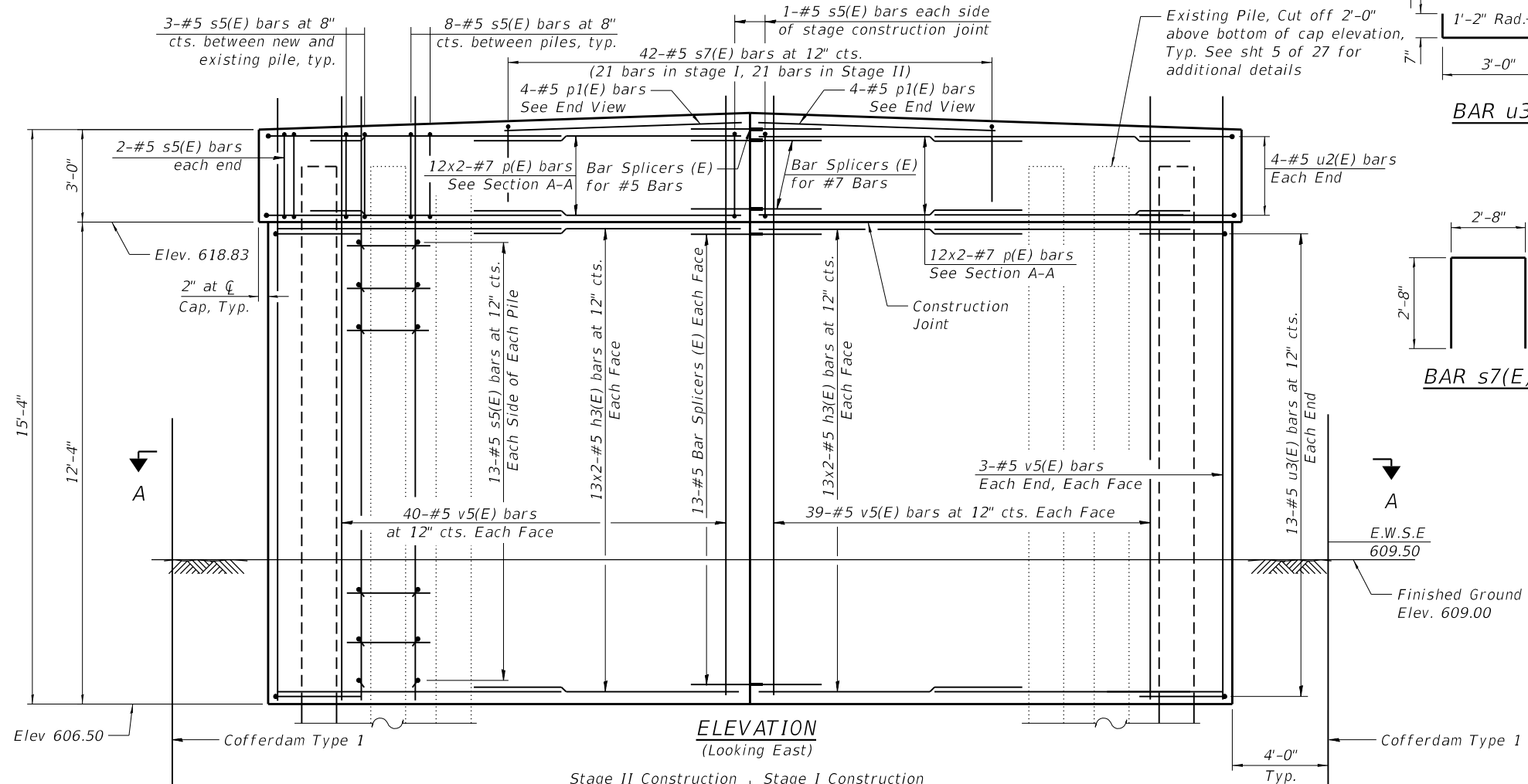
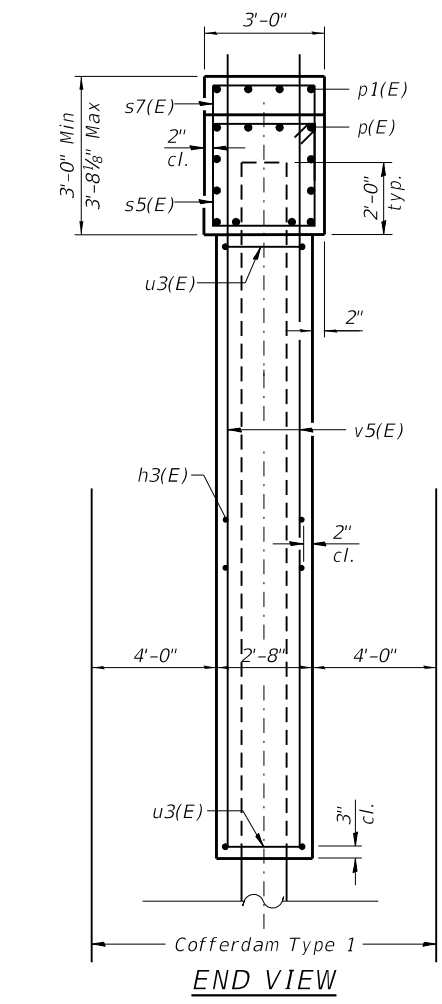
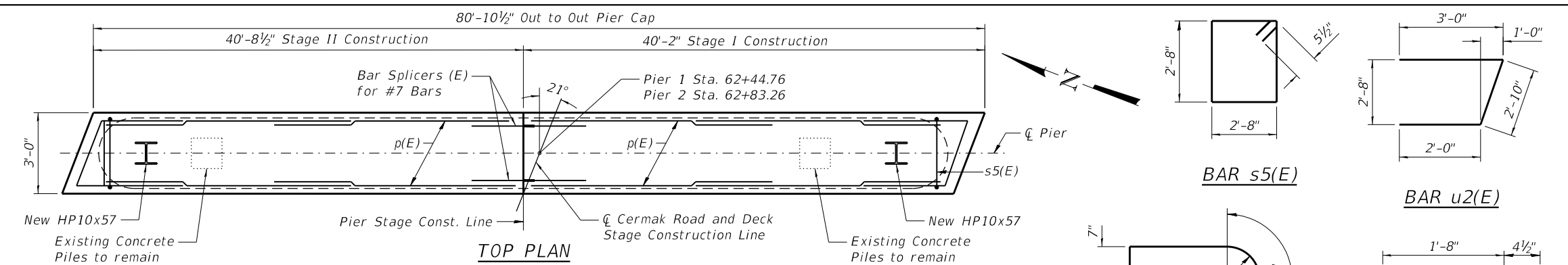
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1453	2018-126-BR	COOK	194	151
CONTRACT NO. 62H51				
ILLINOIS FED. AID PROJECT				

PIER 1 PILE DATA

Type: HP10x57 with Pile Shoes
Nominal Required Bearing: 327 kips
Factored Resistance Available: 180 kips
Est. Length: 45'
No. Production Piles: 2

PIER 2 PILE DATA

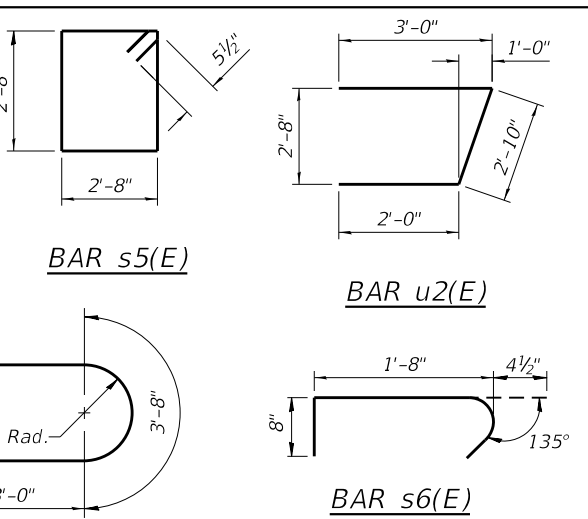
Type: HP10x57 with Pile Shoes
Nominal Required Bearing: 327 kips
Factored Resistance Available: 180 kips
Est. Length: 45'
No. Production Piles: 1
No. Test Piles: 1



MINIMUM BAR LAP

#5 bar = 3'-2"
#7 bar = 4'-5"

Notes:
The northernmost pile of each pier shall be precored to an elevation of 597.00. Pile driving shall begin from the bottom of the precore hole. Cost of precoring and backfilling shall be included in the item Driving Piles.
For details of piles, see sheet 23 of 27.
The Contractor is alerted to the presence of buried fiber optic line below the streambed at the north end of the bridge.



PIER 1 BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h3(E)	104	#5	21'-5"	—
p(E)	48	#7	23'-1"	—
p1(E)	8	#5	21'-0"	—
s5(E)	90	#5	11'-7"	□
s6(E)	364	#4	2'-9"	┐
s7(E)	42	#5	8'-0"	└
u2(E)	8	#5	7'-10"	┘
u3(E)	26	#5	10'-10"	┘
v5(E)	170	#5	16'-9"	—
Cofferdam Excavation			Cu. Yd.	109
Concrete Structures			Cu. Yd.	128.6
Reinforcement Bars, Epoxy Coated			Pound	10,200
Furnishing Steel Piles HP10x57			Foot	90
Driving Piles			Foot	90
Pile Shoes			Each	2
Cofferdam (Type 1) (In-Stream/Wetland Work)			Each	2

PIER 2 BILL OF MATERIAL

Bar	No.	Size	Length	Shape
h3(E)	104	#5	21'-5"	—
p(E)	48	#7	23'-1"	—
p1(E)	8	#5	21'-0"	—
s5(E)	90	#5	11'-7"	□
s6(E)	364	#4	2'-9"	┐
s7(E)	42	#5	8'-0"	└
u2(E)	8	#5	7'-10"	┘
u3(E)	26	#5	10'-10"	┘
v5(E)	170	#5	16'-9"	—
Cofferdam Excavation			Cu. Yd.	109
Concrete Structures			Cu. Yd.	128.6
Reinforcement Bars, Epoxy Coated			Pound	10,200
Furnishing Steel Piles HP10x57			Foot	45
Driving Piles			Foot	45
Test Pile Steel HP10x57			Each	1
Pile Shoes			Each	2
Cofferdam (Type 1) (In-Stream/Wetland Work)			Each	2