

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3778	144 RS-5	WILL & COOK	37	1
		ILLINOIS	CONTRACT NO. 60H93	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

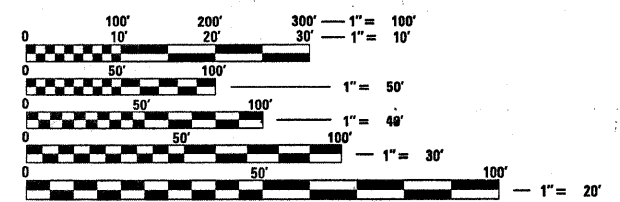
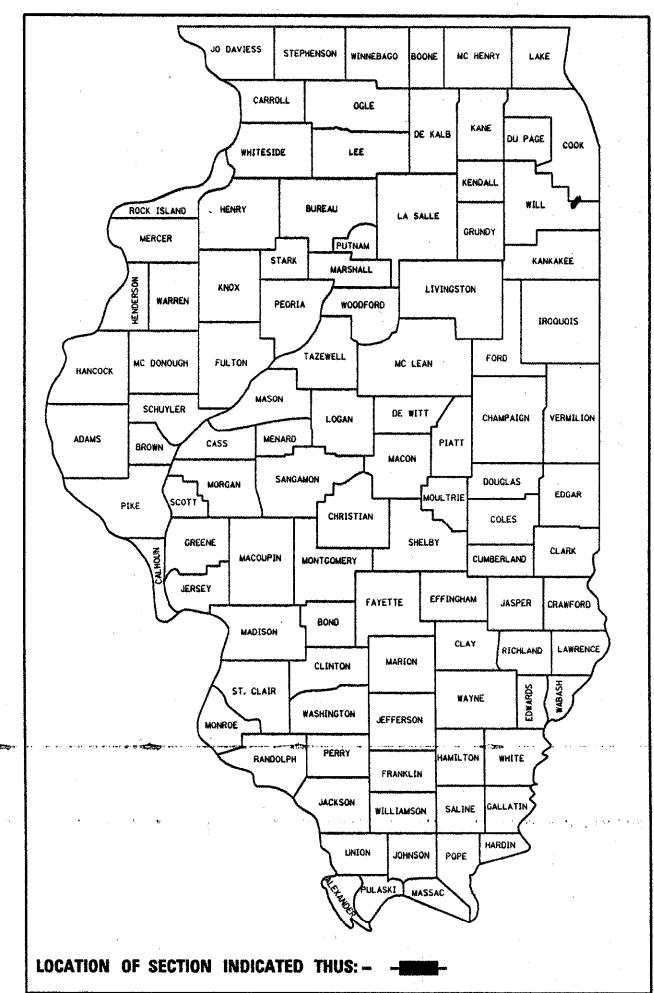
**PROPOSED
HIGHWAY PLANS**

**FAU 3778 (GOVERNORS HIGHWAY)
IL 50 (CICERO AVE.) TO US 30 (LINCOLN HWY.)
SECTION: 144 RS-5
RESURFACING (MAINTENANCE)
WILL & COOK COUNTIES
C-91-833-09**

FOR INDEX OF SHEETS, SEE SHEET NO. 2

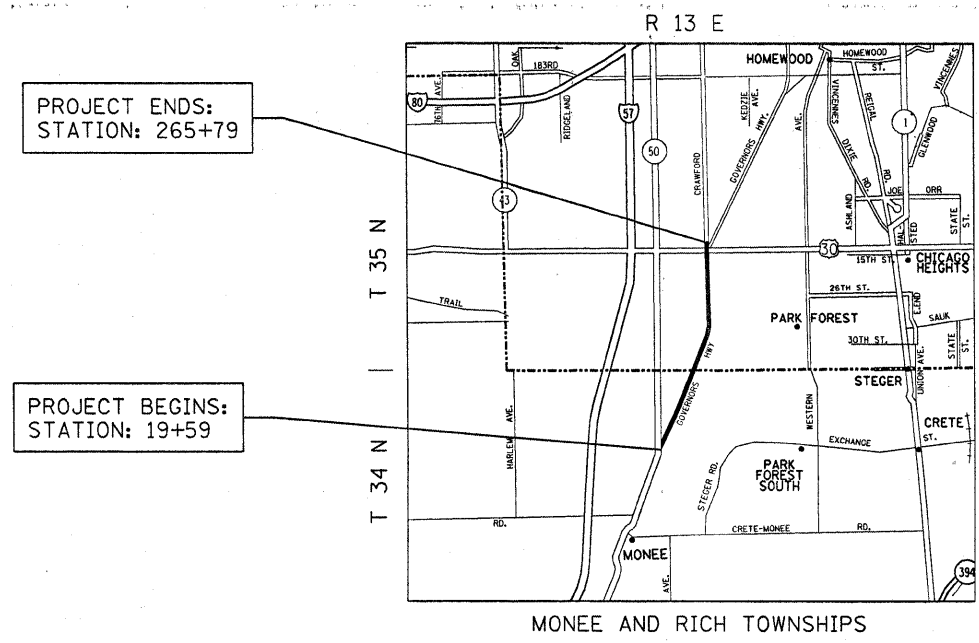
PROJECT LOCATED IN THE VILLAGE OF UNIVERSITY PARK IN WILL COUNTY AND THE VILLAGES OF RICHTON PARK AND MATTESON IN COOK COUNTY

D-91-833-09



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811



TRAFFIC DATA
2006 ADT = 18,800
SPEED LIMIT = 45-55 MPH

GROSS AND NET LENGTH OF PROJECT = 24,620 FEET = 4.66 MILES

PROJECT ENGINEER: KARI SMITH (847) 705-4437
PROJECT MANAGER: KEN ENG
CONTRACT NO. 60H93

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED December 8, 2009

Diane M. O'Keefe
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

January 29, 2010
Scott E. Still P.E. RD
Acting ENGINEER OF DESIGN AND ENVIRONMENT

January 29, 2010
Christine M. Reed RD
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

INDEX OF SHEETS

STATE STANDARDS

GENERAL NOTES

<u>SHEET NO.</u>	<u>DESCRIPTION</u>	<u>STANDARD NO.</u>	<u>DESCRIPTION</u>
1	TITLE SHEET	000001-05	TYPICAL SYMBOLS, ABBREVIATIONS AND PATTERNS
2	INDEX OF SHEETS, STATE STANDARDS & GENERAL NOTES	420001-07	PAVEMENT JOINTS
3-4	SUMMARY OF QUANTITIES	420701-02	PAVEMENT FABRIC
5-7	EXISTING AND PROPOSED TYPICAL SECTIONS	442101-07	CLASS B PATCHES
8-16	ROADWAY AND PAVEMENT MARKING PLANS	442201-03	CLASS C AND D PATCHES
17-23	DETECTOR LOOP REPLACEMENT PLANS	604001-03	FRAME AND LIDS, TYPE 1
24	DRIVEWAY DETAILS DISTANCE BETWEEN ROW AND FACE OF CURB < 15'	604086-02	FRAME AND GRATE, TYPE 23
25	DETAILS FOR FRAME AND LIDS ADJUSTMENT WITH MILLING	604091-02	FRAME AND GRATE, TYPE 24
26	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT	606001-04	CONCRETE CURB AND COMBINATION CONCRETE CURB AND GUTTER
27	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	606306-03	CORRUGATED PC CONCRETE MEDIANS
28	BUTT JOINT AND HMA TAPER	630301-05	SHOULDER WIDENING FOR TYPE 1 (SPECIAL) GUARDRAIL TERMINALS
29	HMA TAPER AT EDGE OF P.C.C. PAVEMENT	635006-03	REFLECTOR AND TERMINAL MARKER PLACEMENT
30	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS AND DRIVEWAYS	701426-03	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATIONS
31	TYPICAL APPLICATIONS: RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)	701606-06	URBAN LANE CLOSURE, MULTILANE 2W WITH MOUNTABLE MEDIAN
32	DISTRICT ONE TYPICAL PAVEMENT MARKINGS	701701-00	URBAN LANE CLOSURE, MULTILANE INTERSECTION
33	TRAFFIC CONTROL AND PROTECTION OF TURN BAYS (TO REMAIN OPEN TO TRAFFIC)	701901-01	TRAFFIC CONTROL DEVICES
34	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING	886001-01	DETECTOR LOOP INSTALLATION
35	ARTERIAL INFORMATION SIGNING	886006-01	TYPICAL LAYOUT FOR DETECTION LOOPS
36	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN		
37	DISTRICT 1 DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING		

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT (800) 892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. (48 HOUR NOTIFICATION REQUIRED)

THE CONTRACTOR WILL NOT BE ABLE TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT THE WRITTEN PERMISSION OF THE DEPARTMENT.

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS UNLESS OTHERWISE SPECIFIED.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC, THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS, AND 1 INCH WHERE THE SPEED LIMIT IS OVER 45 MPH. WITH WRITTEN APPROVAL FROM THE RESIDENT ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM OF 1:3 (V:H).

WHEN ARTIFICIAL LIGHTING IS UTILIZED IN NIGHT OPERATIONS, THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISABILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.

ALL PAVEMENT PATCHING AND CURB AND GUTTER REMOVAL AND REPLACEMENT LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

DOUBLE LANE MARKERS ARE TO BE USED AS SHOWN ON THE DISTRICT ONE DETAIL "TYPICAL APPLICATIONS - RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)" SHOWN IN THE PLANS.

THE RESIDENT ENGINEER SHALL CONTACT MS. CORA MATHIS, AREA TRAFFIC FIELD ENGINEER, AT (815) 485-6475 A MINIMUM OF 2 WEEKS PRIOR TO PLACEMENT OF FINAL PAVEMENT MARKINGS.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE INSTALLATION OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGES OF UNIVERSITY PARK, RICHTON PARK AND MATTESON.

FILE NAME =	USER NAME = sm:thkl	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GOVERNORS HWY (IL 50 TO US 30) INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\pwork\FWIDDT\SMITHKL\0146173\0183	09-sht-plendgn	DRAWN -	REVISED -			3778	144 RS-5	WI & COOK	37	2	
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -			CONTRACT NO. 60H93					
	PLOT DATE = 12/5/2009	DATE -	REVISED -			SCALE:	SHEET NO.	OF	SHEETS	STA.	TO STA.

SUMMARY OF QUANTITIES			URBAN 100% STATE	CONSTRUCTION TYPE CODE					SUMMARY OF QUANTITIES			URBAN 100% STATE	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	WILL COUNTY 1000	COOK COUNTY 1000				CODE NO	ITEM	UNIT	TOTAL QUANTITIES	WILL COUNTY 1000	COOK COUNTY 1000				
20201006	GRADING AND SHAPING SHOULDERS	UNIT	360	195	165				60300310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	13	3	10				
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	3		3				60404940	FRAMES AND GRATES, TYPE 23	EACH	3		3				
20400800	FURNISHED EXCAVATION	CU YD	9		9				60404950	FRAMES AND GRATES, TYPE 24	EACH	5	2	3				
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	27	5	22				60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	4	2	2				
25000210	SEEDING, CLASS 2A	ACRE	0.01		0.01				60619600	CONCRETE MEDIAN, TYPE SB-6.12	SO FT	650		650				
25200110	SODDING, SALT TOLERANT	SQ YD	10	5	5				60624600	CORRUGATED MEDIAN	SO FT	895		895				
40600200	BITUMINOUS MATERIALS (PRIME COAT)	TON	100	43	57				*63100167	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) TANGENT	EACH	1		1				
40600300	AGGREGATE (PRIME COAT)	TON	496	213	283				63200310	GUARDRAIL REMOVAL	FOOT	50		50				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	187	80	107				67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	3	3				
40600895	CONSTRUCTING TEST STRIP	EACH	2	1	1				67100100	MOBILIZATION	L SUM	1	0.5	0.5				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	360	115	245				70100320	TRAFFIC CONTROL AND PROTECTION, STANDARD 701422	L SUM	1	0.5	0.5				
40600985	PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT	SO YD	465		465				70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	0.5	0.5				
40603310	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", NSO	TON	1	0.5	0.5				70300100	SHORT-TERM PAVEMENT MARKING	FOOT	24790	10630	14160				
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	10415	4470	5945				70300210	TEMPORARY PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	510	40	470				
42001200	PAVEMENT FABRIC	SO YD	265		265				70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	94735	44000	50735				
42001300	PROTECTIVE COAT	SO YD	545	10	535				70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	3260	260	3000				
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SQ YD	7	2	5				70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	880		880				
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	45		45				70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	505	115	390				
44000158	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/4"	SO YD	120690	53205	67485				70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SO FT	8265	3545	4720				
44000600	SIDEWALK REMOVAL	SO FT	45		45				*78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	510	40	470				
44001700	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	150	50	100				*78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	94735	44000	50735				
44003100	MEDIAN REMOVAL	SO FT	1545		1545				*78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	3260	260	3000				
44200956	CLASS B PATCHES, TYPE II, 9 INCH	SO YD	180		180				*78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	880		880				
44200962	CLASS B PATCHES, TYPE III, 9 INCH	SO YD	265		265				*78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	505	115	390				
44201789	CLASS D PATCHES, TYPE II, 12 INCH	SO YD	640	640					*78008210	POLYUREA PAVEMENT MARKING TYPE I - LINE 4"	FOOT	330		330				
44201794	CLASS D PATCHES, TYPE III, 12 INCH	SO YD	1280	1280					*78008230	POLYUREA PAVEMENT MARKING TYPE I - LINE 6"	FOOT	40		40				
44201796	CLASS D PATCHES, TYPE IV, 12 INCH	SO YD	215	215					*78008270	POLYUREA PAVEMENT MARKING TYPE I - LINE 24"	FOOT	25		25				
44201851	CLASS D PATCHES, TYPE II, 17 INCH	SO YD	810		810				*78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	1680	735	945				
44201855	CLASS D PATCHES, TYPE III, 17 INCH	SO YD	1620		1620													
44201857	CLASS D PATCHES, TYPE IV, 17 INCH	SO YD	270		270													
44213200	SAW CUTS	FOOT	1600		1600													
48102100	AGGREGATE WEDGE SHOULDER, TYPE B	TON	715	385	330													
55039700	STORM SEWERS TO BE CLEANED	FOOT	830	380	450													
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	2		2													
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	5	2	3													

*SPECIALTY ITEMS

SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE						SUMMARY OF QUANTITIES			CONSTRUCTION TYPE CODE							
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	WILL COUNTY 1000	COOK COUNTY 1000					CODE NO	ITEM	UNIT	TOTAL QUANTITIES	WILL COUNTY 1000	COOK COUNTY 1000				
* 78200420	GUARDRAIL MARKERS, TYPE B	EACH	1		1														
* 78201000	TERMINAL MARKER - DIRECT APPLIED	EACH	1		1														
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	1350	590	760														
* 85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	2	1	1														
* 87301305	ELECTRIC CABLE IN CONDUIT, LEAD-IN, NO. 14 1 PAIR	FOOT	1964	520	1444														
* 87900200	DRILL EXISTING HANDHOLE	EACH	3	1	2														
* 88500100	INDUCTIVE LOOP DETECTOR	EACH	5	3	2														
* 88600100	DETECTOR LOOP, TYPE I	FOOT	233	141	92														
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	1800	66	1734														
* 89502300	REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	1670	470	1200														
X0322256	TEMPORARY INFORMATION SIGNING	SO FT	51.4	25.7	25.7														
44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	11	4	7														
40600826	POLYMERIZED LEVELING BINDER (MACHINE METHOD), 1L-4.75, N50	TON	5090	2195	2895														
X4400100	PORTLAND CEMENT CONCRETE SURFACE REMOVAL (VARIABLE DEPTH)	SO YD	295		295														
Z0017202	DOWEL BARS 1 1/2"	EACH	780		780														
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	30	10	20														
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	L SUM	1	0.5	0.5														
Z0004530	HOT-MIX ASPHALT DRIVEWAY PAVEMENT, 8"	SQ YD	7	2	5														
*SPECIALTY ITEMS										*SPECIALTY ITEMS **NON-PARTICIPATION ITEM									

LEGEND

- ① EXIST. PCC BASE COURSE, 9"(\pm)
- ② EXIST. HOT-MIX ASPHALT SURFACE COURSE (BEFORE MILLING), 5 1/4"(\pm)
- ③ EXIST. CONCRETE CURB AND GUTTER
- ④ EXIST. AGGREGATE SHOULDER
- ⑤ PROP. HOT-MIX ASPHALT SURFACE REMOVAL - 2 1/4"
- ⑥ PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"
- ⑦ PROP. POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 3/4"
- ⑧ PROP. CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (LOCATIONS TO BE DETERMINED BY THE RESIDENT ENGINEER)
- ⑨ PROP. AGGREGATE WEDGE SHOULDER, TYPE B
- ⑩ PROP. GRADING AND SHAPING SHOULDERS

NOTES:

1. SEE ROADWAY AND PAVEMENT MARKING PLAN SHEETS FOR LOCATIONS OF LEFT AND RIGHT TURN LANES, MEDIANS, CONCRETE CURB AND GUTTER TYPE AND AGGREGATE SHOULDERS.
2. PAVEMENT PATCHING SHALL BE DONE AFTER MILLING OF ROADWAY (SEE BD-22).

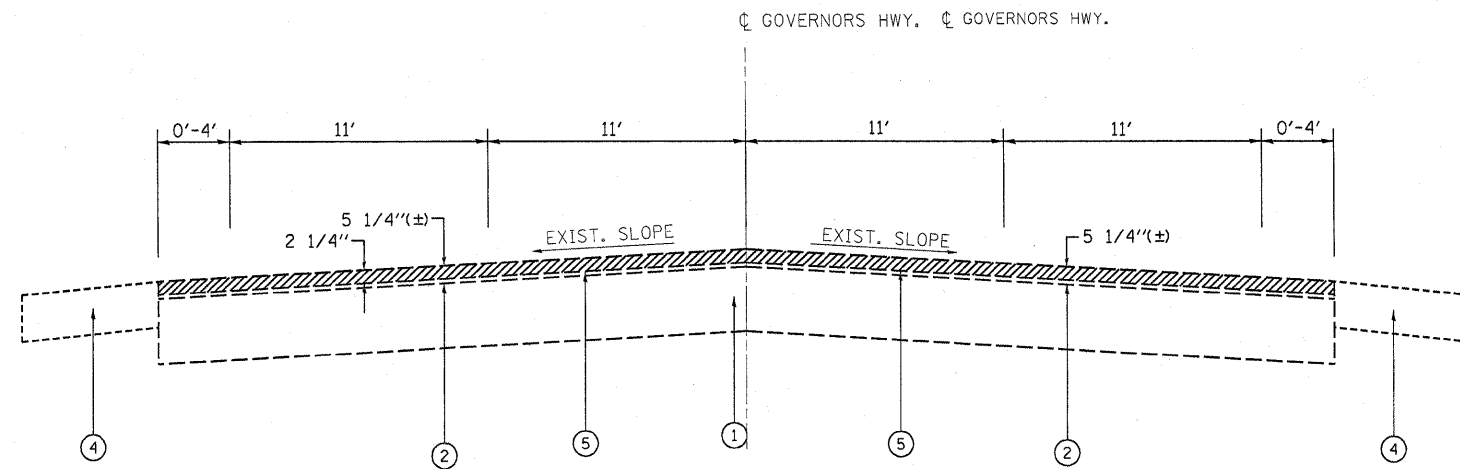
HOT-MIX ASPHALT MIXTURE REQUIREMENTS

	MIXTURE TYPE	AIR VOIDS (%)
ROADWAY	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2"	4% @ 70 GYR
	POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 3/4"	4% @ 50 GYR
PATCHES	CLASS D PATCHES, (HMA BINDER IL-19.0 MM), 12" & 17"	4% @ 70 GYR
DRIVEWAY	HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19 MM), 6" OR 8"	4% @ 50 GYR
	HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50 (IL 9.5 MM), 2"	4% @ 50 GYR

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN

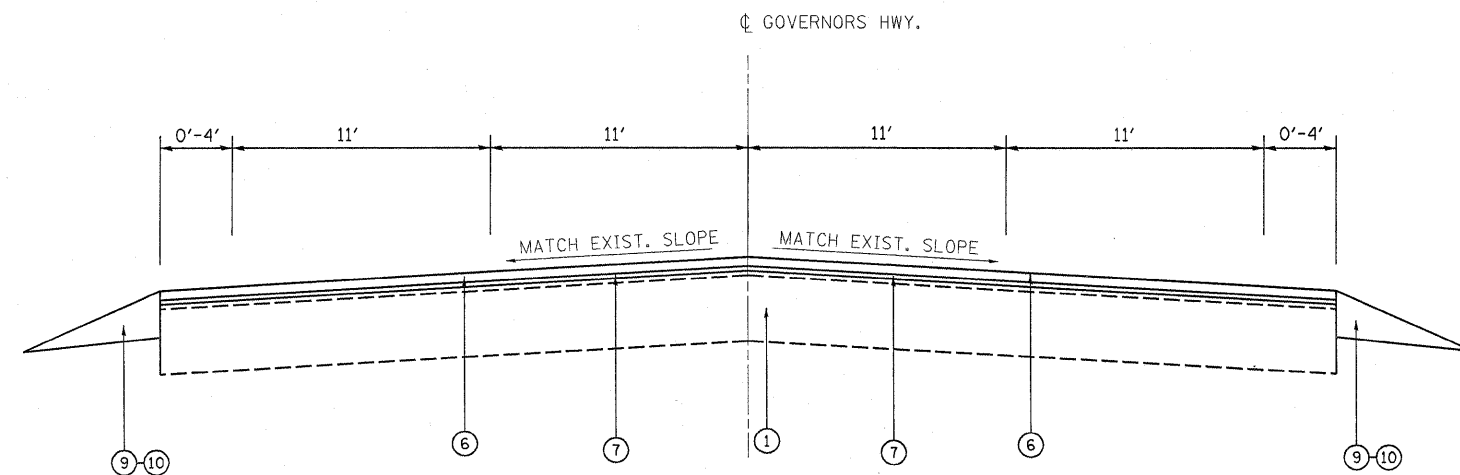
THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 70-22 AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

FOR "PERCENT RAP" SEE DISTRICT ONE SPECIAL PROVISIONS



EXISTING TYPICAL SECTION GOVERNORS HWY.

STATION: 19+59 TO 121+73



PROPOSED TYPICAL SECTION GOVERNORS HWY.

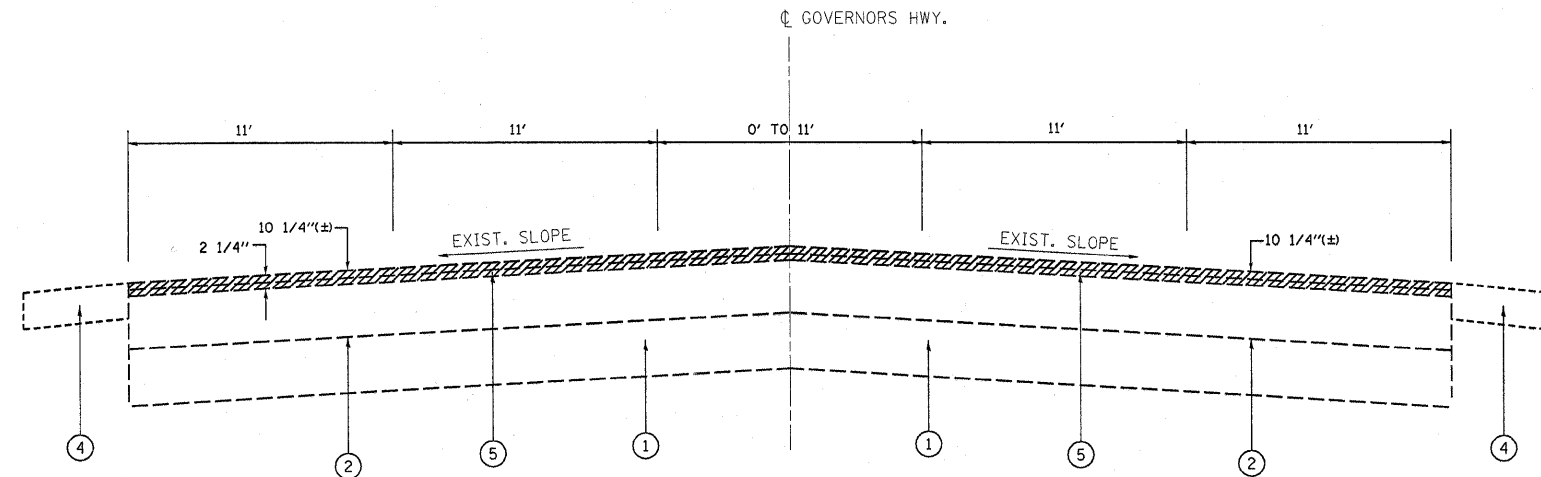
STATION: 19+59 TO 121+73

LEGEND

- ① EXIST. PCC BASE COURSE, 9''(±)
- ② EXIST. HOT-MIX ASPHALT SURFACE COURSE (BEFORE MILLING), 10 1/4''(±)
- ③ EXIST. CONCRETE CURB AND GUTTER
- ④ EXIST. AGGREGATE SHOULDER
- ⑤ PROP. HOT-MIX ASPHALT SURFACE REMOVAL - 2 1/4''
- ⑥ PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2''
- ⑦ PROP. POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 3/4''
- ⑧ PROP. CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (LOCATIONS TO BE DETERMINED BY THE RESIDENT ENGINEER)
- ⑨ PROP. AGGREGATE WEDGE SHOULDER, TYPE B
- ⑩ PROP. GRADING AND SHAPING SHOULDERS

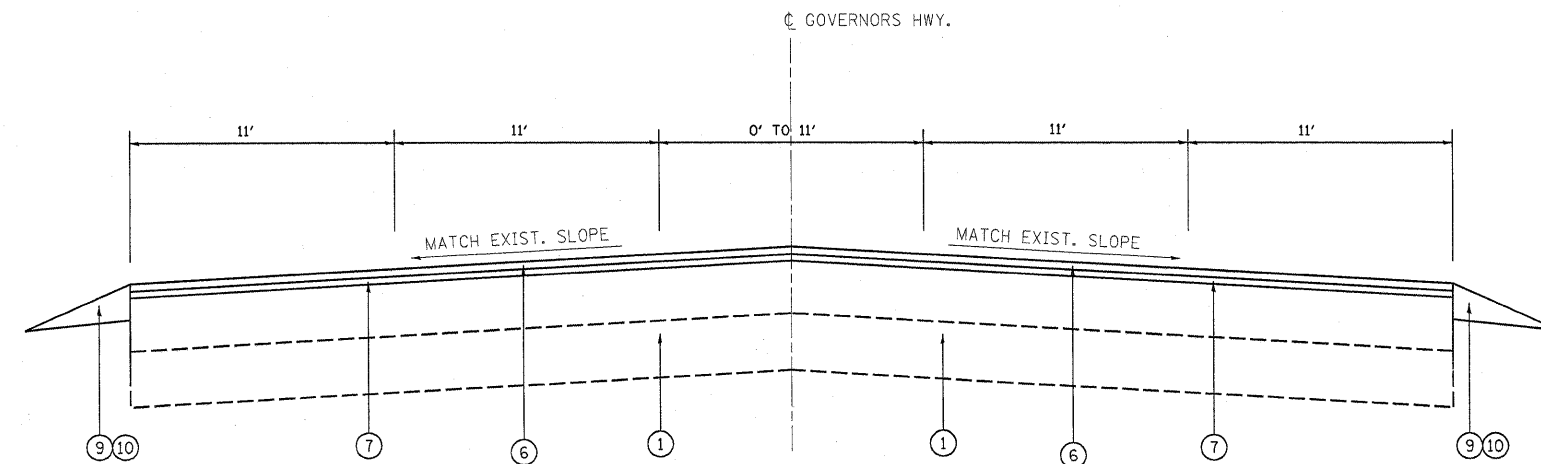
NOTES:

1. SEE ROADWAY AND PAVEMENT MARKING PLAN SHEETS FOR LOCATIONS OF LEFT AND RIGHT TURN LANES, MEDIANS, CONCRETE CURB AND GUTTER TYPE AND AGGREGATE SHOULDERS.
2. PAVEMENT PATCHING SHALL BE DONE AFTER MILLING OF ROADWAY (SEE BD-22).



EXISTING TYPICAL SECTION
GOVERNORS HWY.

STATION:
121+73 TO 247+65

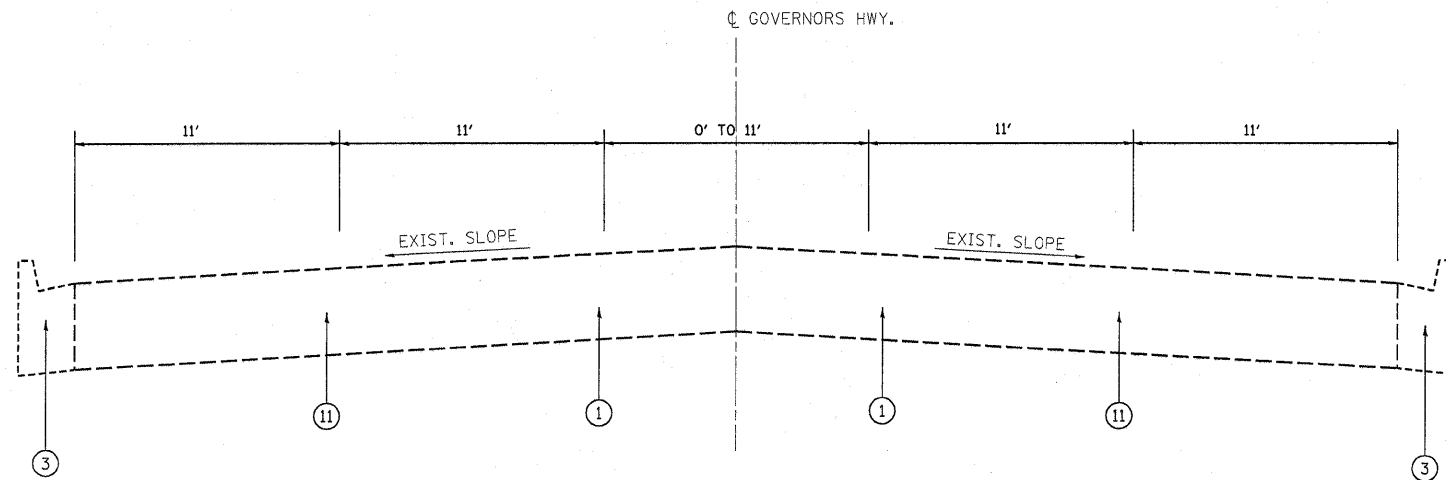


PROPOSED TYPICAL SECTION
GOVERNORS HWY.

STATION:
121+73 to 247+65

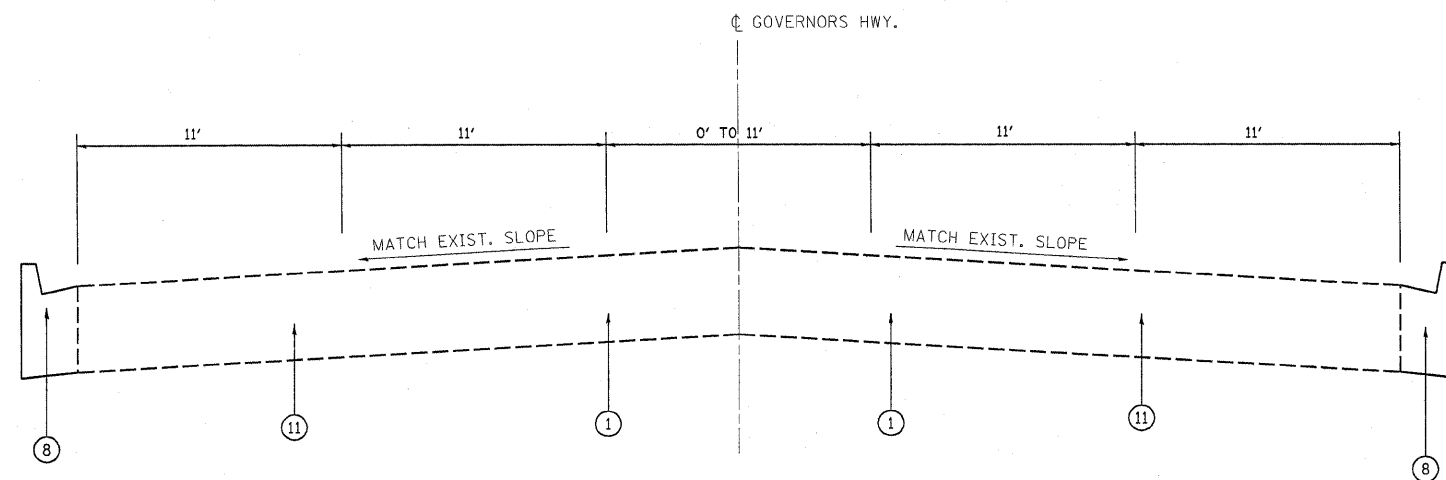
FILE NAME =	USER NAME = smthkl	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GOVERNORS HWY (IL 50 TO US 30) EXISTING AND PROPOSED TYPICAL SECTIONS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca\pw_work\PWIDOT\SMITHKL\0146173\0183309-sht-plen.dgn	PLOT SCALE = 50,0000 ' / IN.	DRAWN -	REVISED -			3778	144 RS-5	WILL & COOK	37	6
PLOT DATE = 12/5/2009	DATE -	CHECKED -	REVISED -			CONTRACT NO. 60H93		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		
						SCALE:	SHEET NO. OF SHEETS	STA.	TO STA.	

LEGEND



EXISTING TYPICAL SECTION
GOVERNORS HWY.

STATION:
247+65 TO 265+79



PROPOSED TYPICAL SECTION
GOVERNORS HWY.

STATION:
247+65 TO 265+79

- ① EXIST. PCC BASE COURSE, 9''(±)
- ② EXIST. HOT-MIX ASPHALT SURFACE COURSE (BEFORE MILLING), 10 1/4''(±)
- ③ EXIST. CONCRETE CURB AND GUTTER
- ④ EXIST. AGGREGATE SHOULDER
- ⑤ PROP. HOT-MIX ASPHALT SURFACE REMOVAL - 2 1/4''
- ⑥ PROP. HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 1/2''
- ⑦ PROP. POLYMERIZED LEVELING BINDER (MM), IL-4.75, N50, 3/4''
- ⑧ PROP. CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (LOCATIONS TO BE DETERMINED BY THE RESIDENT ENGINEER)
- ⑨ PROP. AGGREGATE WEDGE SHOULDER, TYPE B
- ⑩ PROP. GRADING AND SHAPING SHOULDERS
- ⑪ PROP. CLASS B PATCHING

NOTES:

1. SEE ROADWAY AND PAVEMENT MARKING PLAN SHEETS FOR LOCATIONS OF LEFT AND RIGHT TURN LANES, MEDIANS, CONCRETE CURB AND GUTTER TYPE AND AGGREGATE SHOULDERS.
2. CLASS B PATCHING TO BE DONE FROM STA. 247+65 TO 265+79.

FILE NAME =	USER NAME = smthkl	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GOVERNORS HWY (IL 50 TO US 30) EXISTING AND PROPOSED TYPICAL SECTIONS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ca\pwwork\PWIDOT\SMTHKL\d0146173\0183309-aht-plen.dgn	PLOT SCALE = 50.0000' / IN.	DRAWN -	REVISED -			3778	144 RS-5	WILL & COOK	37	7	
	PLOT DATE = 12/5/2009	CHECKED -	REVISED -			SCALE: SHEET NO. OF SHEETS STA. TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT CONTRACT NO. 60H93			
		DATE -	REVISED -								

IL. RTE. 50
(CICERO AVE.)

PROJECT BEGINS:
STATION: 19+59

GOVERNORS HWY

PROP. HMA SURFACE REMOVAL, 2 1/4"
PROP. HMA SURFACE COURSE, MIX "D", N70, 1 1/2"
PROP. POLYMERIZED LEVELING BINDER (MM), N50, 3/4"

PROP. 4" YELLOW THERMOPLASTIC
CENTERLINE 2 @ 11" C-C (TYP.)

PROP. 4" YELLOW THERMOPLASTIC
MEDIAN EDGE LINE (TYP.)

EXISTING
CURB & GUTTER

PROP. 4" WHITE THERMOPLASTIC
EDGE LINE (TYP.)

PROP. 24" WHITE THERMOPLASTIC
STOP BAR (TYP.)

15+00

20+00

25+00

MATCHLINE STA. 30+00



MCNAMARA RD.

GOVERNORS HWY

PROP. HMA SURFACE REMOVAL, 2 1/4"
PROP. HMA SURFACE COURSE, MIX "D", N70, 1 1/2"
PROP. POLYMERIZED LEVELING BINDER (MM), N50, 3/4"

PROP. AGGREGATE WEDGE SHOULDERS
PROP. GRADING AND SHAPING SHOULDERS

PROP. 4" WHITE THERMOPLASTIC
SKIP-DASH (10' DASH, 30' SKIP) (TYP.)

PROP. 4" YELLOW THERMOPLASTIC
CENTERLINE 2 @ 11" C-C (TYP.)

EXISTING AGG. SHOULDER

30+00

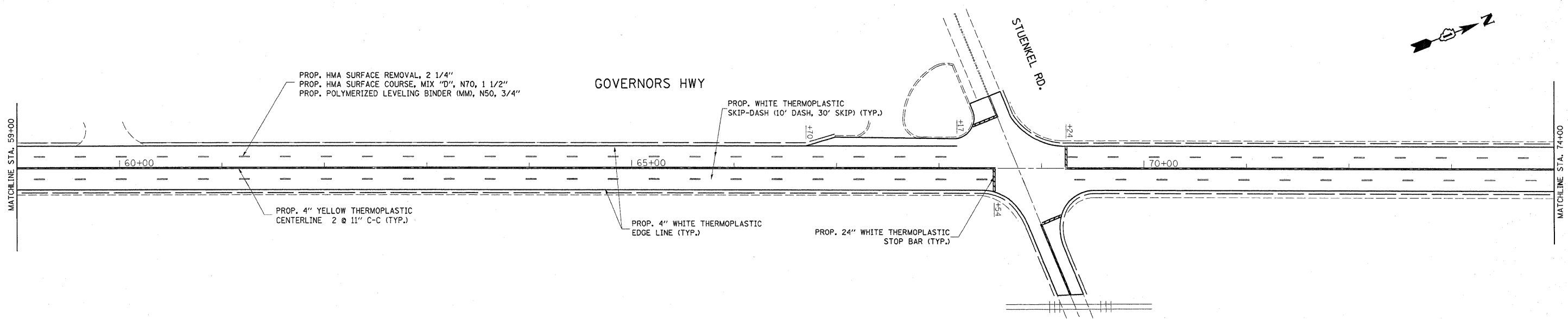
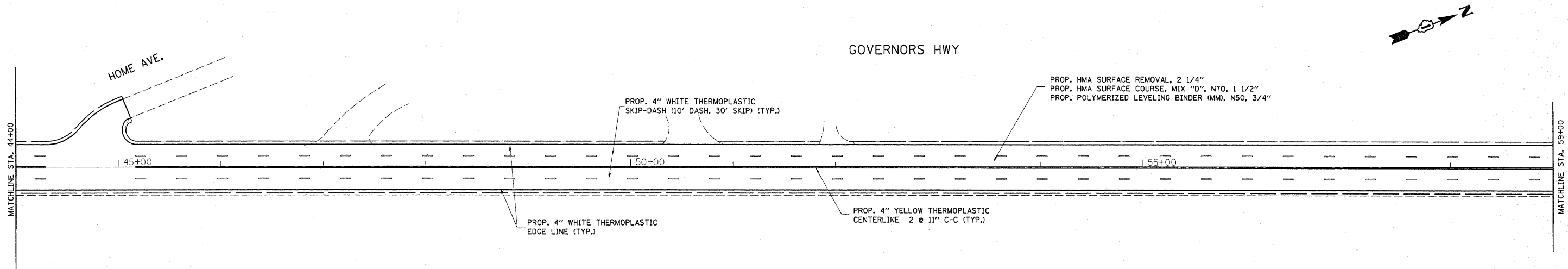
35+00

40+00

MATCHLINE STA. 30+00

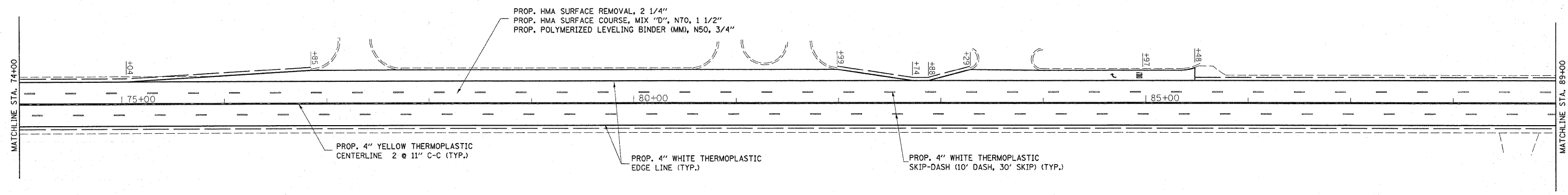
MATCHLINE STA. 44+00

FILE NAME =	USER NAME = smthkl	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY & PAVEMENT MARKING PLAN GOVERNORS HWY (IL. RTE. 50 - U.S. RTE. 30)			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwork\pwork\smthkl\d0146173\018330	9-shit-plan.dgn	DRAWN -	REVISED -		3778	144 RS-5	WILL & COOK	37	8			
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	PLOT DATE = 12/14/2009	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							

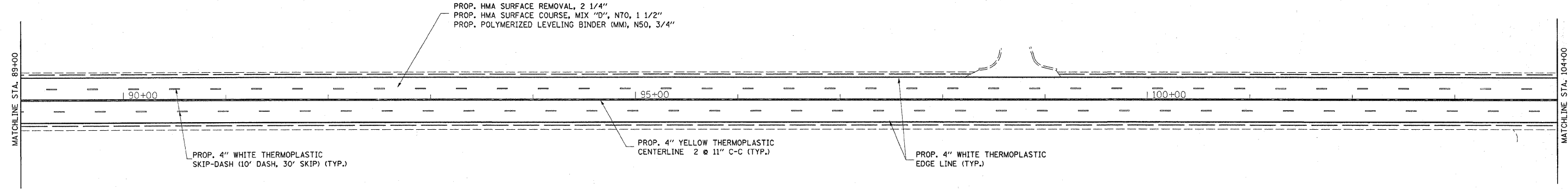


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PLOT SCALE = 50,0000 ' / IN.		CHECKED -	REVISED -			CONTRACT NO. 60H93				
PLOT DATE = 12/14/2009		DATE -	REVISED -			ILLINOIS FED. AID PROJECT				
					SCALE: 1"=50'	SHEET NO. OF SHEETS STA. TO STA.				

GOVERNORS HWY

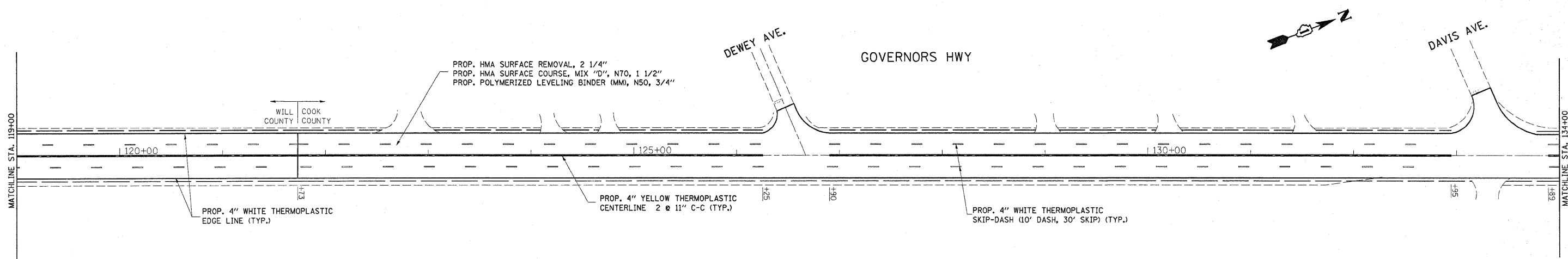
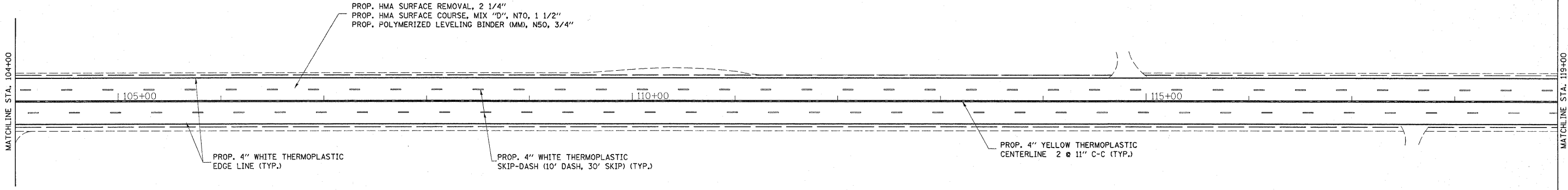
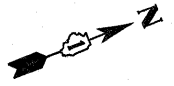


GOVERNORS HWY

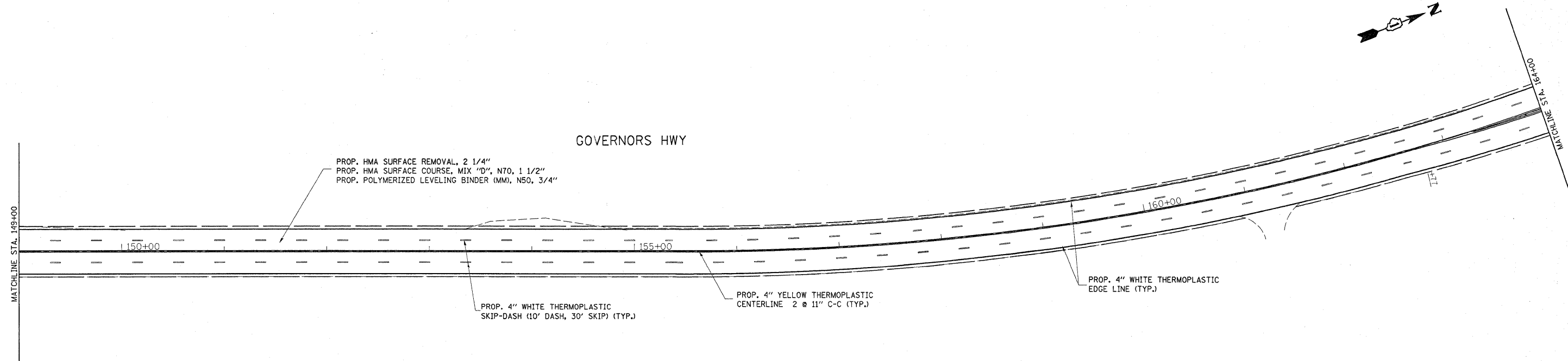
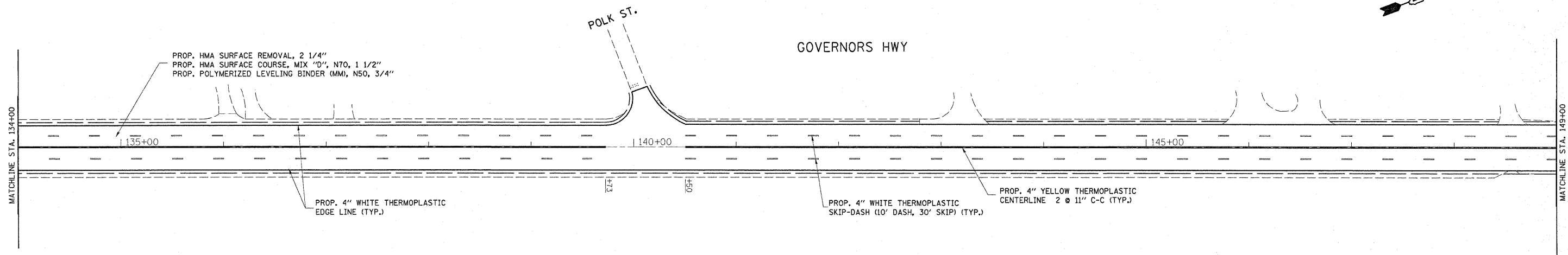


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	PLOT DATE = 12/14/2009	DATE -	REVISED -		SCALE: 1"=50'			SHEET NO.	OF	SHEETS	STA.	TO STA.	CONTRACT NO. 60H93
	ILLINOIS FED. AID PROJECT												

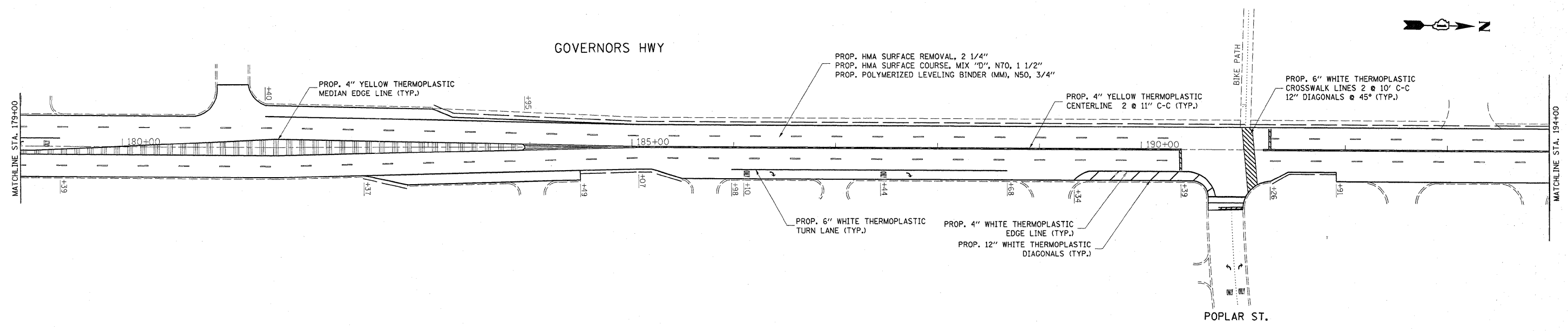
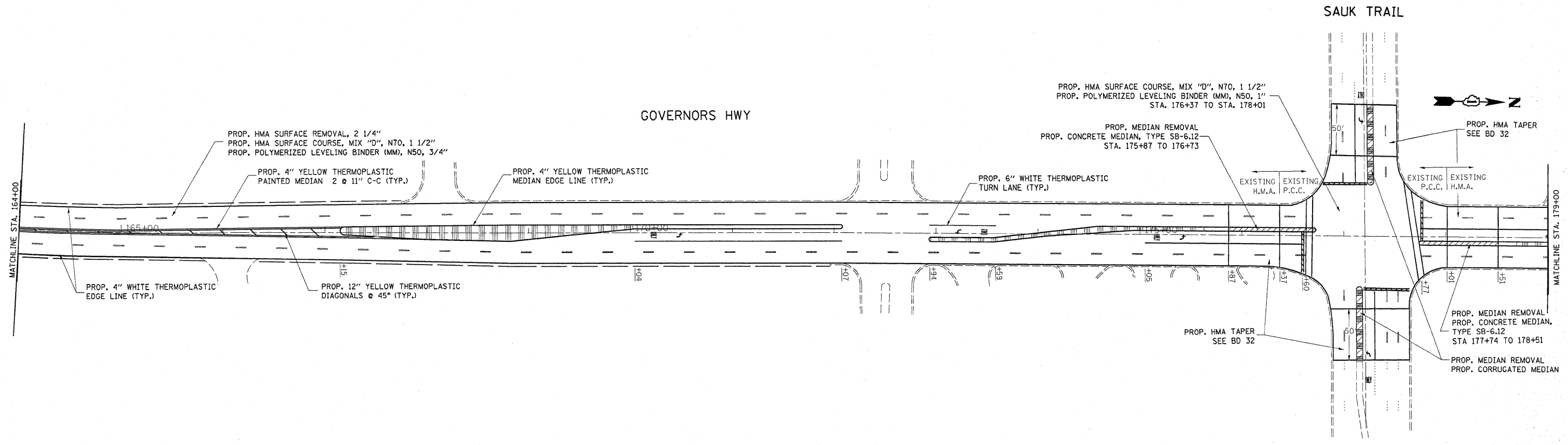
GOVERNORS HWY



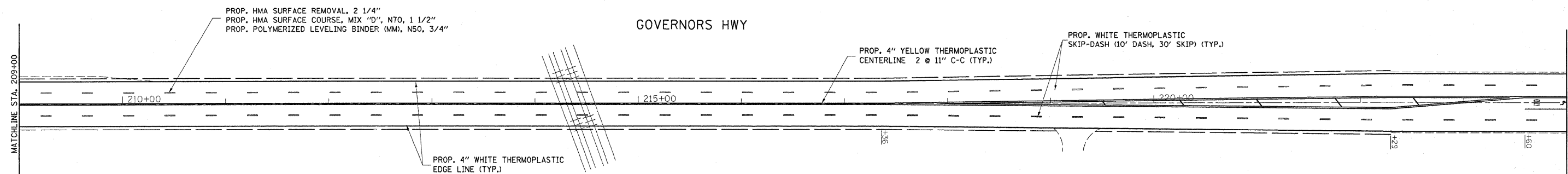
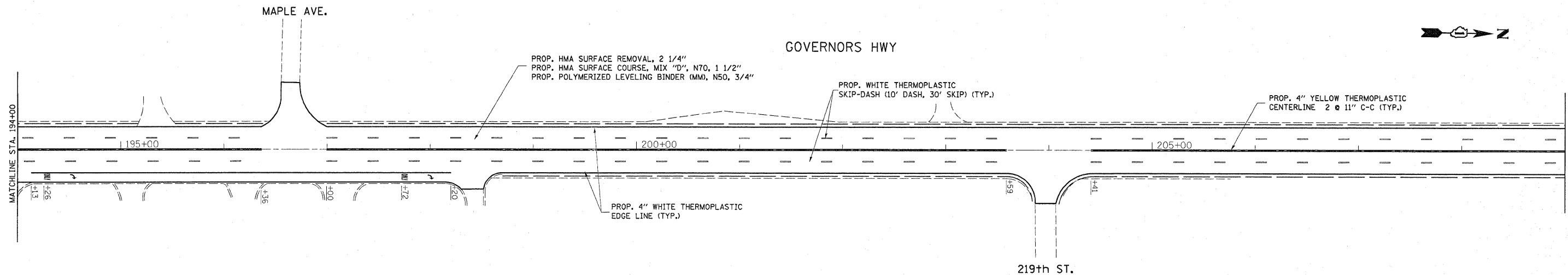
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	PLOT DATE = 12/14/2009	DATE -	REVISED -		CONTRACT NO. 60H93								



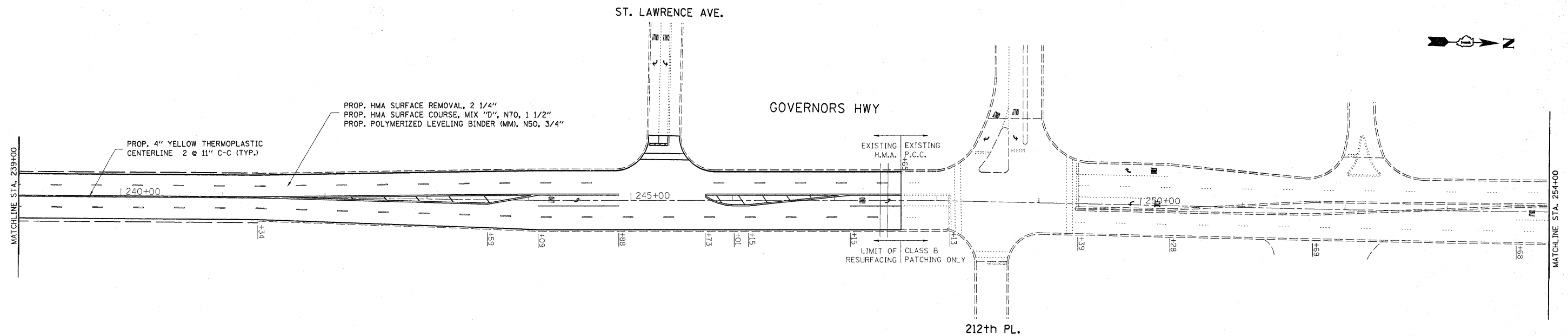
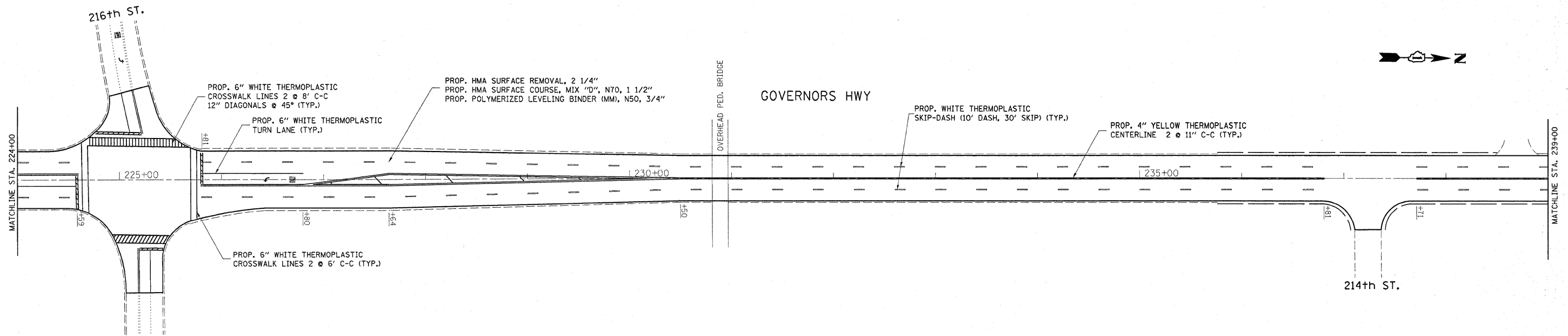
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PLOT DATE = 12/14/2009	DATE -	CHECKED -	REVISED -		SCALE: 1"=50'			SHEET NO. OF SHEETS STA. TO STA.		CONTRACT NO. 60H93		
					ILLINOIS FED. AID PROJECT							



FILE NAME =	USER NAME = smthkl	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY & PAVEMENT MARKING PLAN GOVERNORS HWY (IL RTE. 50 - U.S. RTE. 30)			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
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		CHECKED -	REVISED -								CONTRACT NO. 60H93		
		DATE -	REVISED -								ILLINOIS FED. AID PROJECT		



FILE NAME =	USER NAME = smthkl	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY & PAVEMENT MARKING PLAN GOVERNORS HWY (IL. RTE. 50 - U.S. RTE. 30)			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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		DATE -	REVISED -					ILLINOIS FED. AID PROJECT				



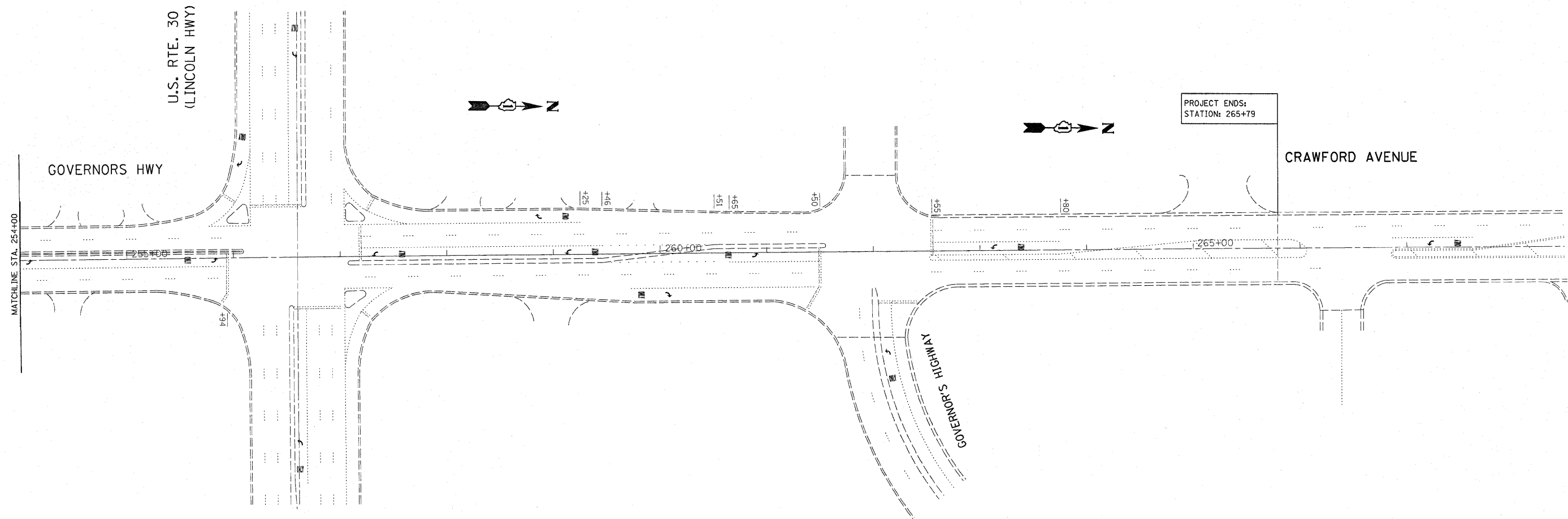
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	PLOT DATE = 12/14/2009	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY & PAVEMENT MARKING PLAN
GOVERNORS HWY (IL RTE. 50 - U.S. RTE. 30)**

SCALE: 1"=50' SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3778	144 RS-5	WILL & COOK	37	15
CONTRACT NO. 60H93				
ILLINOIS FED. AID PROJECT				



FILE NAME =	USER NAME = am1thk1	DESIGNED -	REVISED -
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	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -
	PLOT DATE = 12/14/2009	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY & PAVEMENT MARKING PLAN
GOVERNORS HWY (IL. RTE. 50 - U.S. RTE. 30)**

SCALE: 1"=50' SHEET NO. OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3778	144 RS-5	WILL & COOK	37	16
CONTRACT NO. 60H93				
ILLINOIS FED. AID PROJECT				

SCHEDULE OF QUANTITIES FOR MAGNETIC DETECTOR REPLACEMENTS

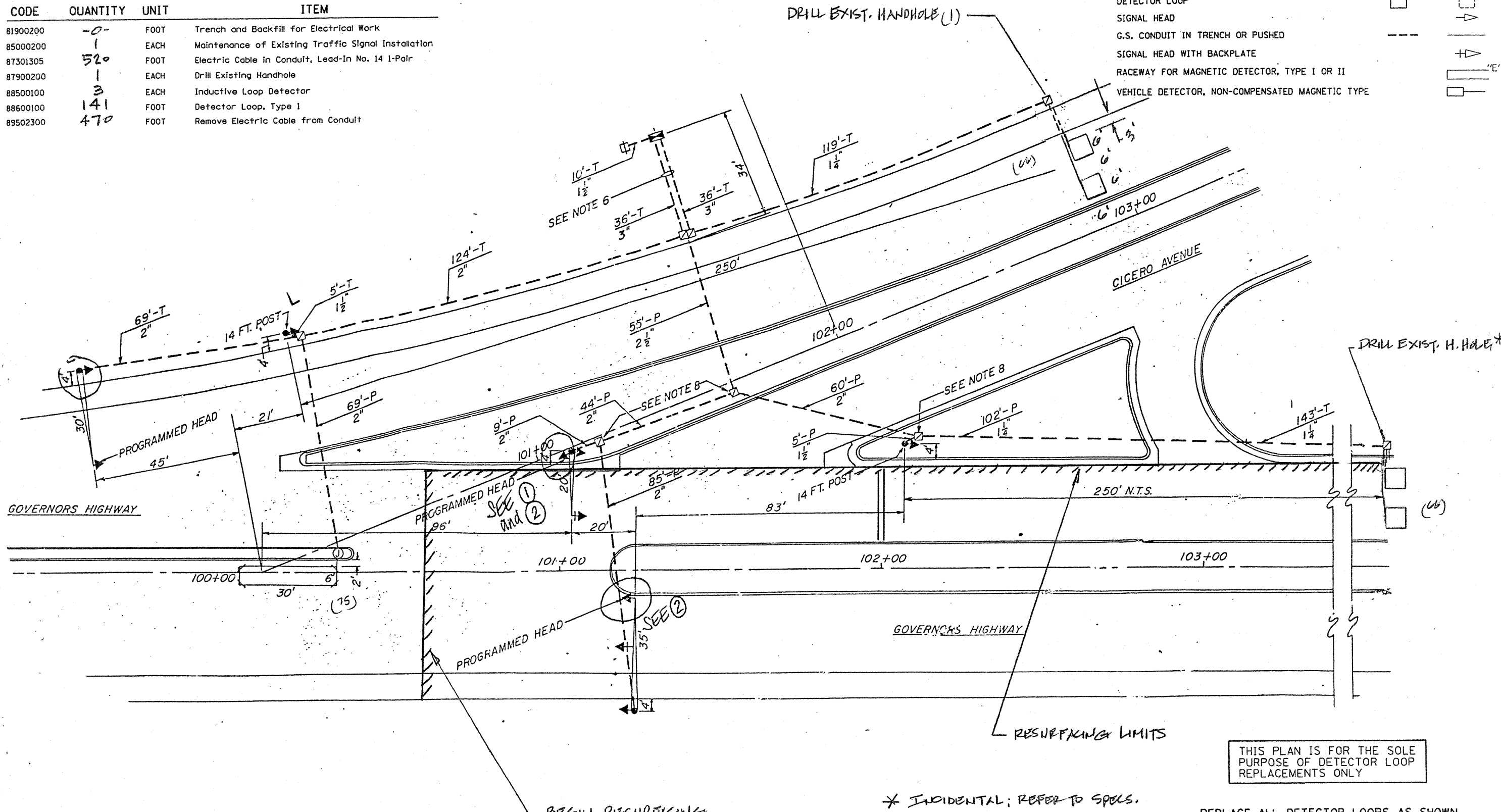
(• When Quantities Below Are Shown)

CODE	QUANTITY	UNIT	ITEM
81900200	-0-	FOOT	Trench and Backfill for Electrical Work
85000200	1	EACH	Maintenance of Existing Traffic Signal Installation
87301305	520	FOOT	Electric Cable in Conduit, Lead-In No. 14 1-Pair
87900200	1	EACH	Drill Existing Handhole
88500100	3	EACH	Inductive Loop Detector
88600100	141	FOOT	Detector Loop, Type 1
89502300	470	FOOT	Remove Electric Cable from Conduit

All Magnetic Detectors will be replaced at a signalized intersection; even if only one magnetic detector is within the resurfacing limits. The "Pay Items" shown above will be paid as per the loop replacement specifications direct for "Magnetic Detectors" during roadway resurfacing projects.

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP		
SIGNAL HEAD		
G.S. CONDUIT IN TRENCH OR PUSHED		
SIGNAL HEAD WITH BACKPLATE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	66	FOOT	DETECTOR LOOP, REPLACEMENT

SCHEDULE OF QUANTITIES FOR MAGNETIC DETECTOR REPLACEMENTS
(• When Quantities Below Are Shown)

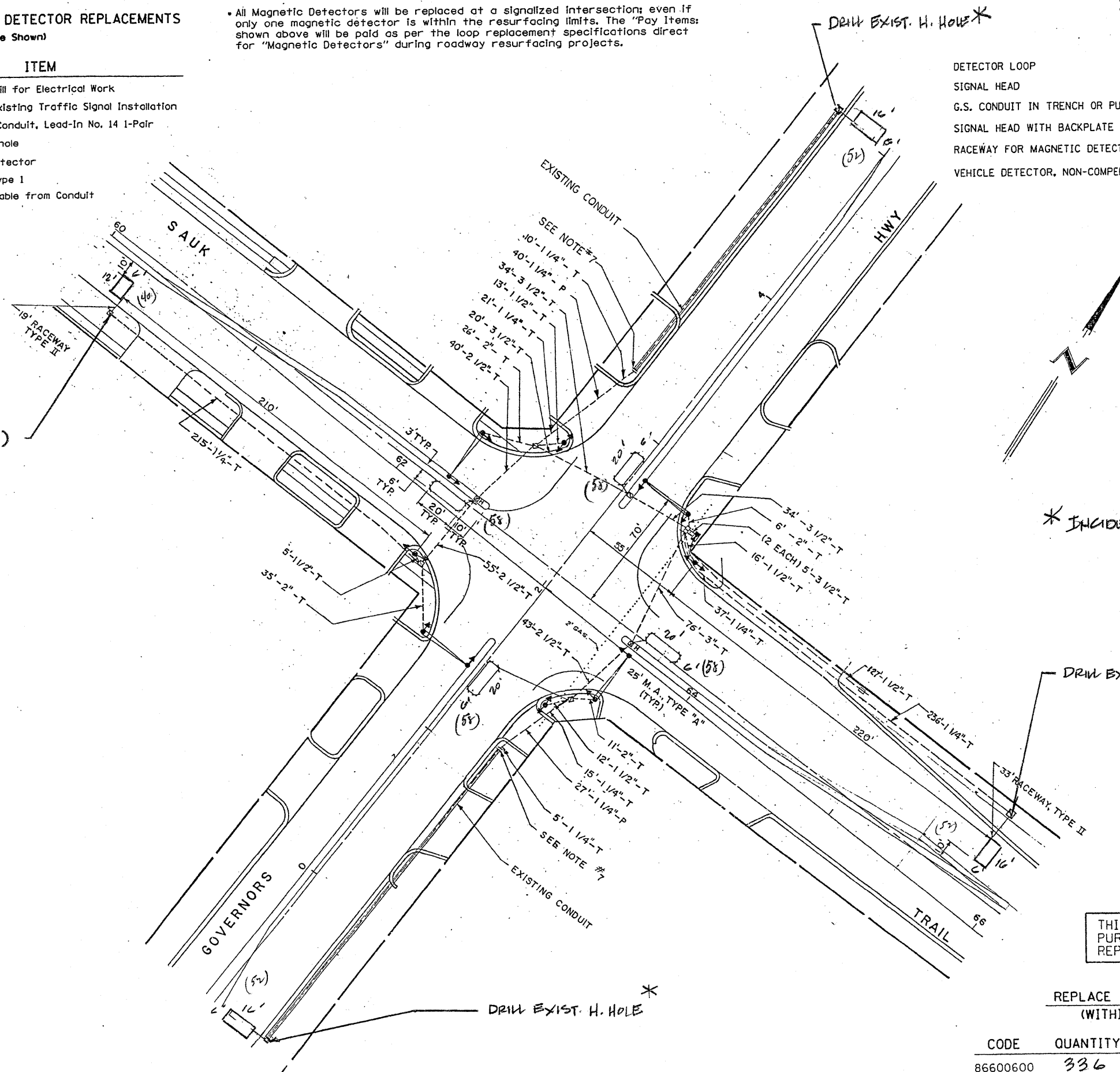
CODE	QUANTITY	UNIT	ITEM
81900200	0	FOOT	Trench and Backfill for Electrical Work
85000200	1	EACH	Maintenance of Existing Traffic Signal Installation
87301305	1444	FOOT	Electric Cable In Conduit, Lead-In No. 14 1-Pair
87900200	2	EACH	Drill Existing Handhole
88500100	2	EACH	Inductive Loop Detector
88600100	92	FOOT	Detector Loop, Type I
89502300	1200	FOOT	Remove Electric Cable from Conduit

• All Magnetic Detectors will be replaced at a signalized intersection; even if only one magnetic detector is within the resurfacing limits. The "Pay Items" shown above will be paid as per the loop replacement specifications direct for "Magnetic Detectors" during roadway resurfacing projects.

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP		
SIGNAL HEAD		
G.S. CONDUIT IN TRENCH OR PUSHED		
SIGNAL HEAD WITH BACKPLATE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		

DRILL EXIST. HANDHOLE (1)



* INCIDENTAL; REFER TO SPECS.

THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

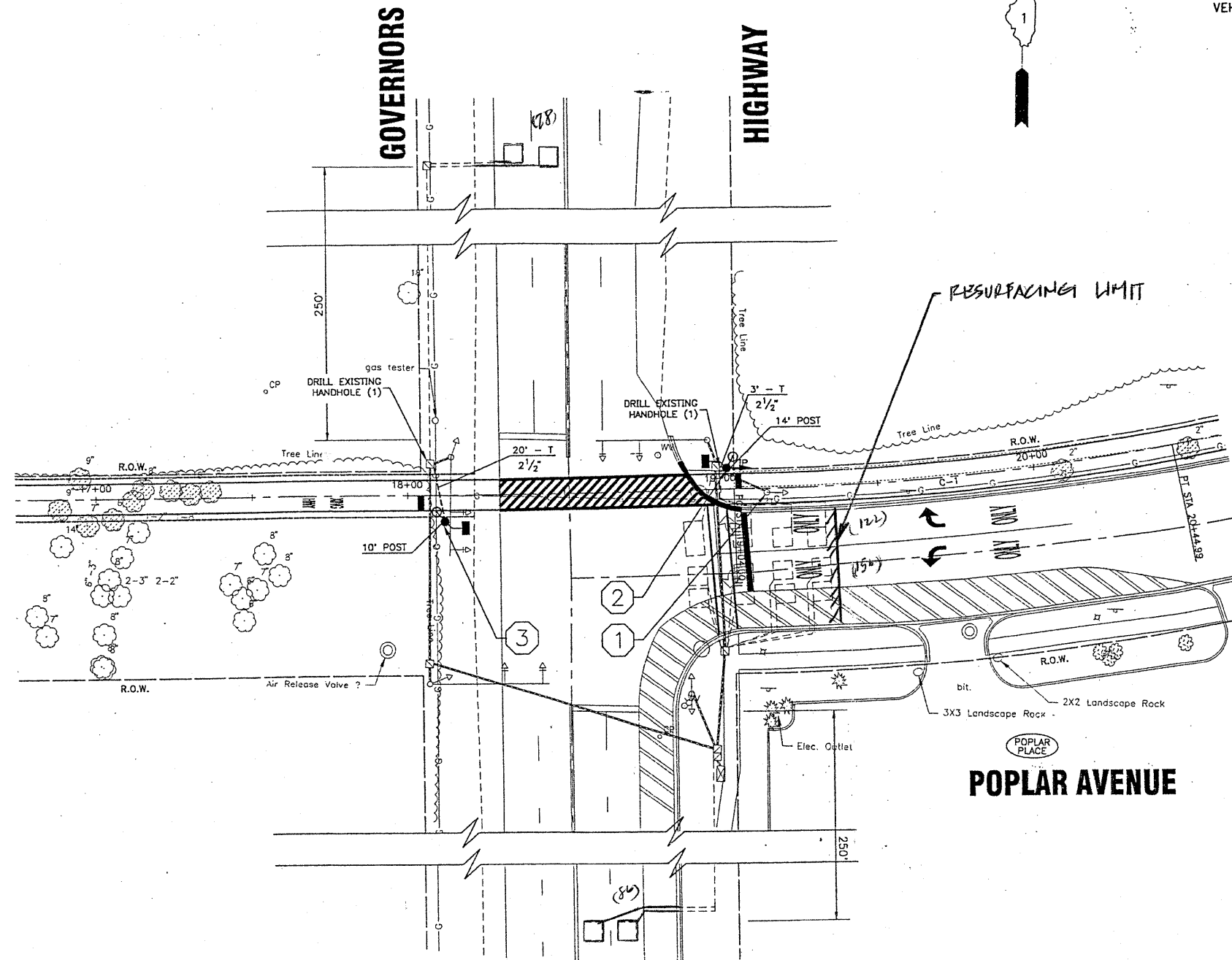
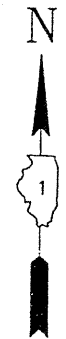
REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	336	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME =	USER NAME = kxanthaphixaybc	DESIGNED - BCK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE - DETECTOR LOOP REPLACEMENT GOVERNORS HWY @ SAUK TRAIL	F.A.D. RTE. 3778	SECTION 144 R5-5	COUNTY Cook	TOTAL SHEETS 37	SHEET NO. 18	
CONTRACT NO.	DATE	CHECKED - DAD	REVISED -			SCALE: NONE	SHEET NO. OF SHEETS STA. TO STA.	FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	60493	
PLOT SCALE = 39.9360' / IN.		DRAWN - BCK	REVISED -								
PLOT DATE = 4/3/2009			REVISED -								

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP		
SIGNAL HEAD		
G.S. CONDUIT IN TRENCH OR PUSHED		
SIGNAL HEAD WITH BACKPLATE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	442	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME =	USER NAME = konthaphixaybo	DESIGNED - BCK	REVISED -
at\pwork\PWIDOT\KANTHAPHIXAYBC\d01126	4\traffic.legend.v7.dgn	DRAWN - BCK	REVISED -
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PLOT DATE = 4/3/2009	DATE	REVISED -	REVISED -

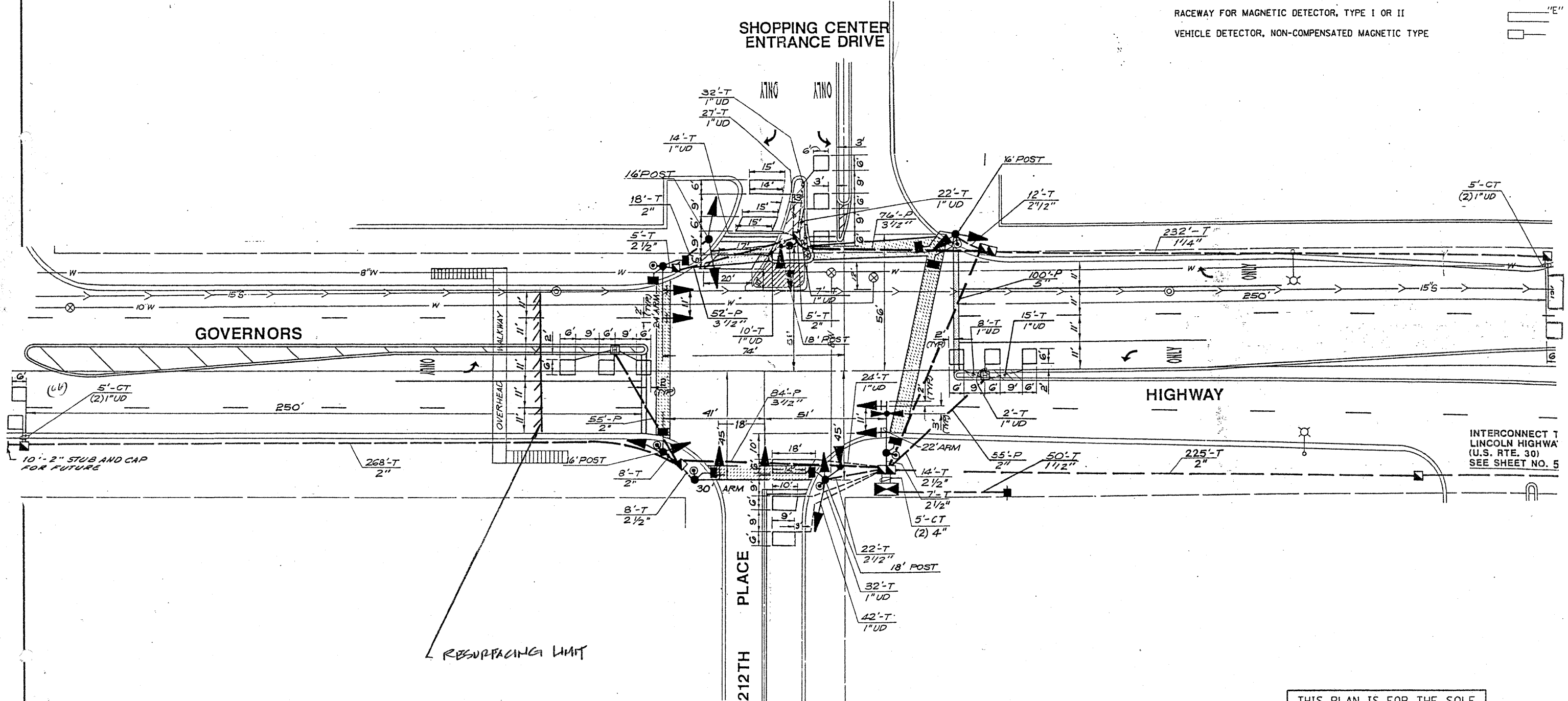
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE - DETECTOR LOOP REPLACEMENT
Governor's Hwy. @ Poplar Av.

F.A.V. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3718	144 RS-5	COOK	37	19
SCALE: NONE		SHEET NO. OF SHEETS		STA. TO STA.
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT		CONTRACT NO. 60493

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP		
SIGNAL HEAD		
G.S. CONDUIT IN TRENCH OR PUSHED		
SIGNAL HEAD WITH BACKPLATE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

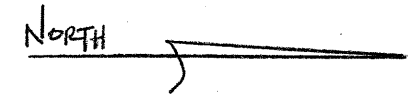
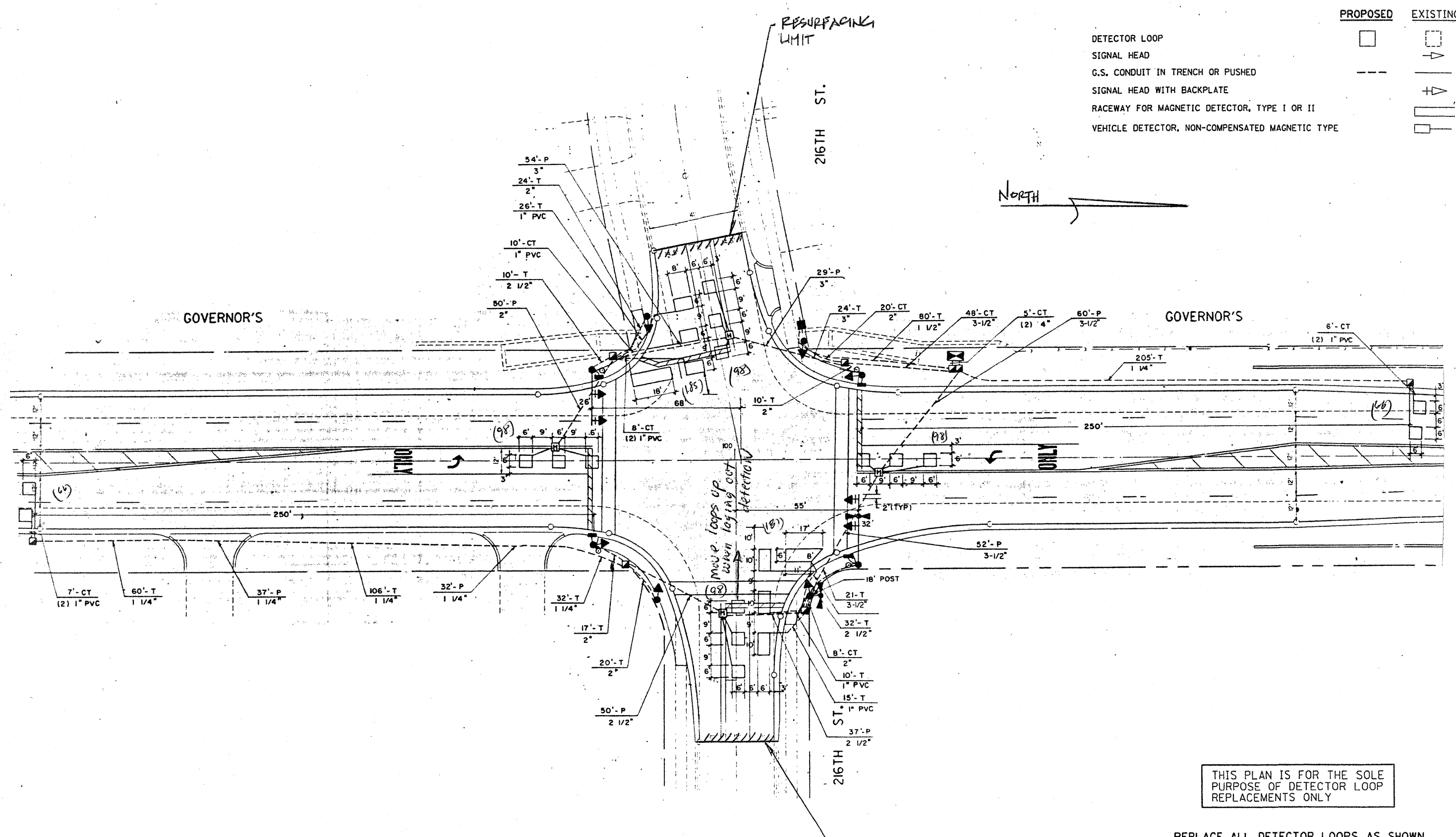
REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	666	FOOT	DETECTOR LOOP, REPLACEMENT

FILE NAME = c:\pwwork\FWIDOT\KANTHAPHIXAYBC\d01126	USER NAME = kanthaphixayba \traffic\legend.v7.dgn	DESIGNED - BCK DRAWN - BCK	REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE - DETECTOR LOOP REPLACEMENT GOVERNOR'S HWY. @ 212TH PLACE	F.A.U. RTE. 3778	SECTION 144 RS-5	COUNTY Cook	TOTAL SHEETS 37	SHEET NO. 20
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TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP		
SIGNAL HEAD		
G.S. CONDUIT IN TRENCH OR PUSHED		
SIGNAL HEAD WITH BACKPLATE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		



THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600	890	FOOT	DETECTOR LOOP, REPLACEMENT

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PLOT SCALE = 3/4" = 1' IN.	CHECKED - DAD	REVISED -	REVISED -
PLOT DATE = 4/3/2009	DATE -	REVISED -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

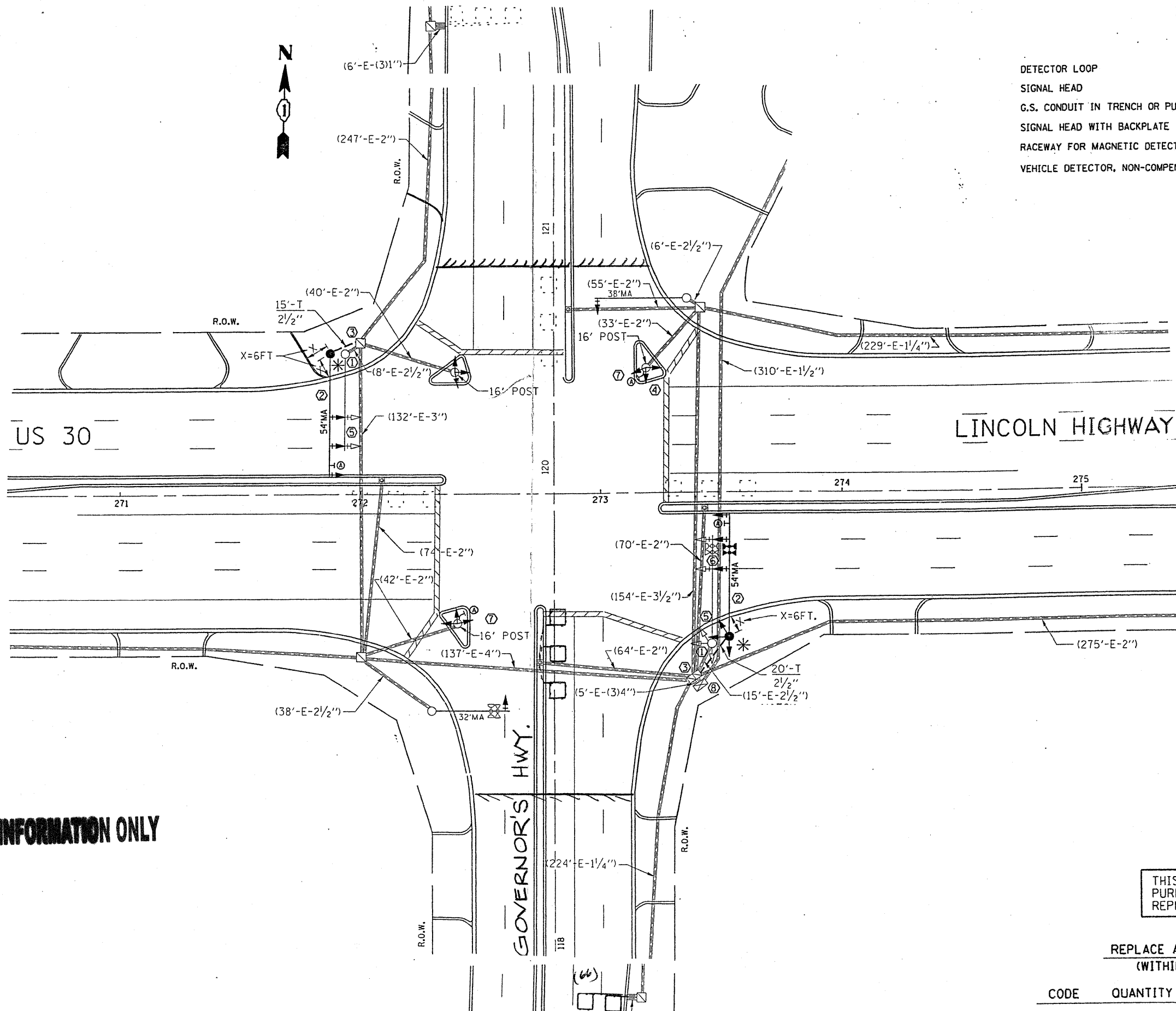
DISTRICT ONE - DETECTOR LOOP REPLACEMENT
GOVERNOR'S HWY. @ 216TH STREET

F.A.W. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
378	144 RS-5	COOK	87	21
FED. ROAD DIST. NO.		ILLINOIS FED. AID PROJECT	CONTRACT NO.	
			80H93	

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.

TRAFFIC SIGNAL LEGEND

	PROPOSED	EXISTING
DETECTOR LOOP		
SIGNAL HEAD		
G.S. CONDUIT IN TRENCH OR PUSHED		
SIGNAL HEAD WITH BACKPLATE		
RACEWAY FOR MAGNETIC DETECTOR, TYPE I OR II		
VEHICLE DETECTOR, NON-COMPENSATED MAGNETIC TYPE		



FOR INFORMATION ONLY

THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENTS ONLY

REPLACE ALL DETECTOR LOOPS AS SHOWN (WITHIN THE RESURFACING LIMITS)

CODE	QUANTITY	UNIT	ITEM
86600600		FOOT	DETECTOR LOOP, REPLACEMENT

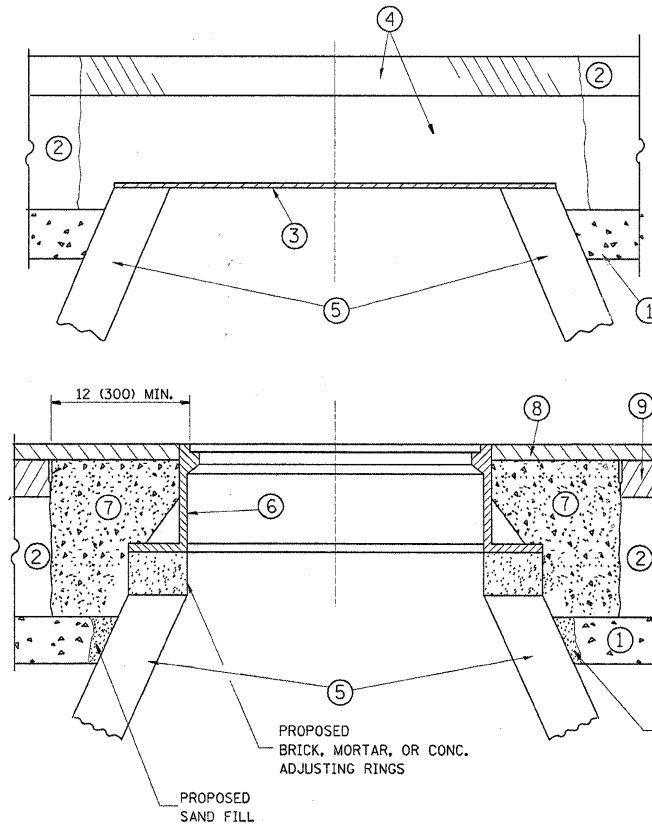
FILE NAME =	USER NAME = kanthaphixaybc	DESIGNED - BCK	REVISED -
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE - DETECTOR LOOP REPLACEMENT
U.S. ROUTE 30 @ GOVERNOR'S HWY.

F.A.D. RTE. 3778	SECTION 144 R5-5	COUNTY COOK	TOTAL SHEETS 37	SHEET NO. 22
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 60H93	

SCALE: NONE SHEET NO. OF SHEETS STA. TO STA.



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS SI CONCRETE, OR HMA SURFACE COURSE OR HMA BINDER COURSE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS.

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS SI CONCRETE, HMA SURFACE COURSE OR HMA BINDER COURSE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT: THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "FRAMES AND LIDS TO BE ADJUSTED, SPECIAL"
NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

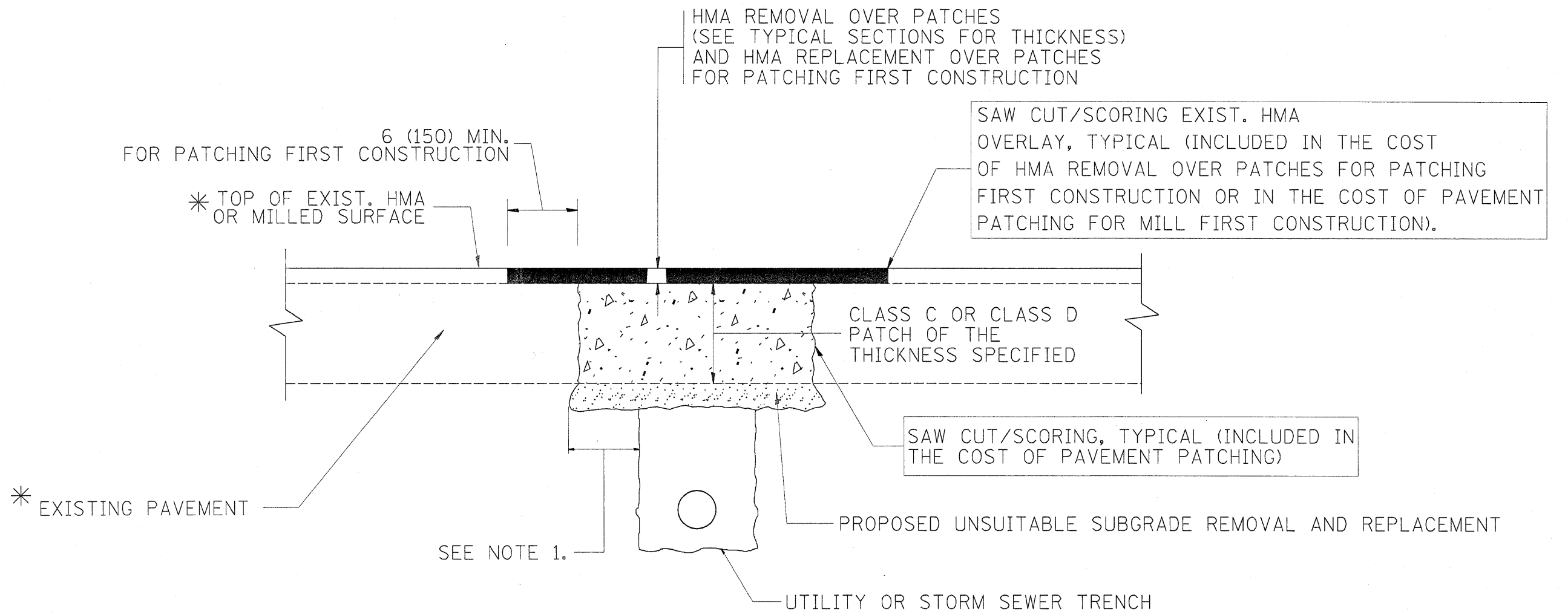
DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = smthkl	DESIGNED - R. SHAH	REVISED - R. SHAH 03-10-95
of:\pwork\p\WIDOT\SMITHKL\d0146173\Dist	ed.dgn	DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.3963' / IN.	CHECKED -	REVISED - R. WIEDEMAN 05-14-04
	PLOT DATE = 8/11/2009	DATE - 10-25-94	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING		F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	3778	144 RS-5	WILL & COOK	37	25
	STA. TO STA.	BD600-03 (BD-8)		CONTRACT NO. 60H93		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = smthkl	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
or\pwork\pwork\SMITHKL\0146173\01st	DRAWN -	REVISED - R. BORO 01-01-07	3778			144 RS-5	WILL & COOK	37	26	
PLOT SCALE = 50.3963' / IN.	CHECKED -	REVISED - R. BORO 09-04-07	BD400-04 (BD-22)			CONTRACT NO. 60H93				
PLOT DATE = 8/11/2009	DATE - 10-25-94	REVISED - K. ENG 10-27-08	SCALE: NONE			SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.
SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

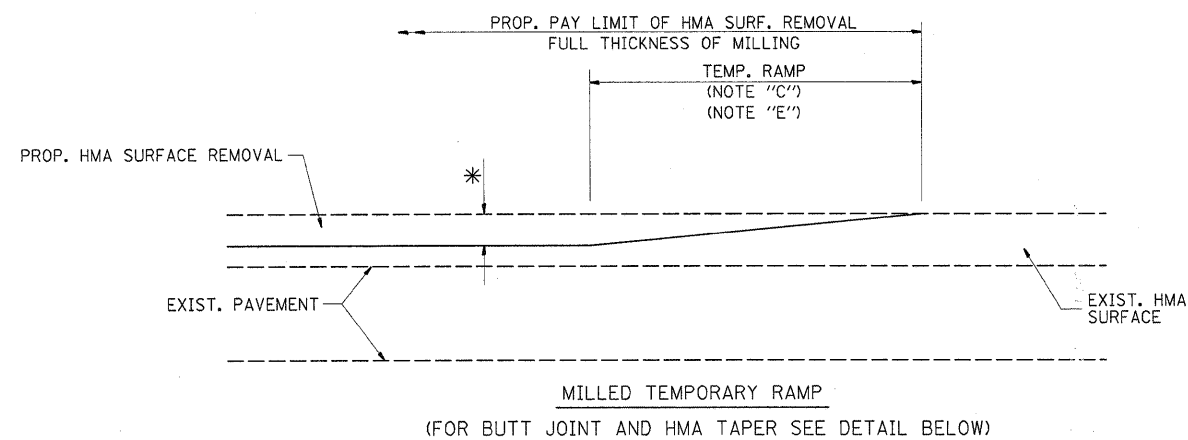
BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

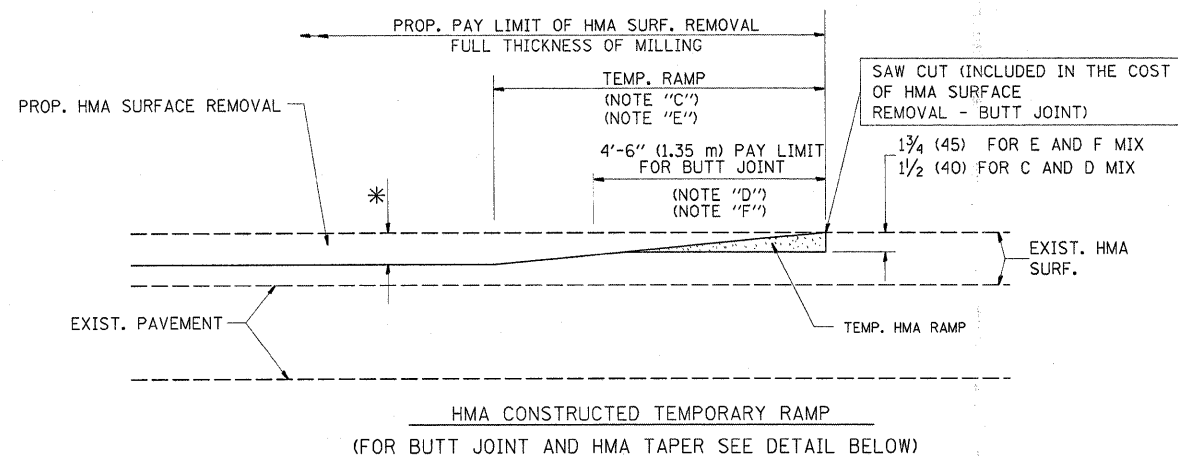
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

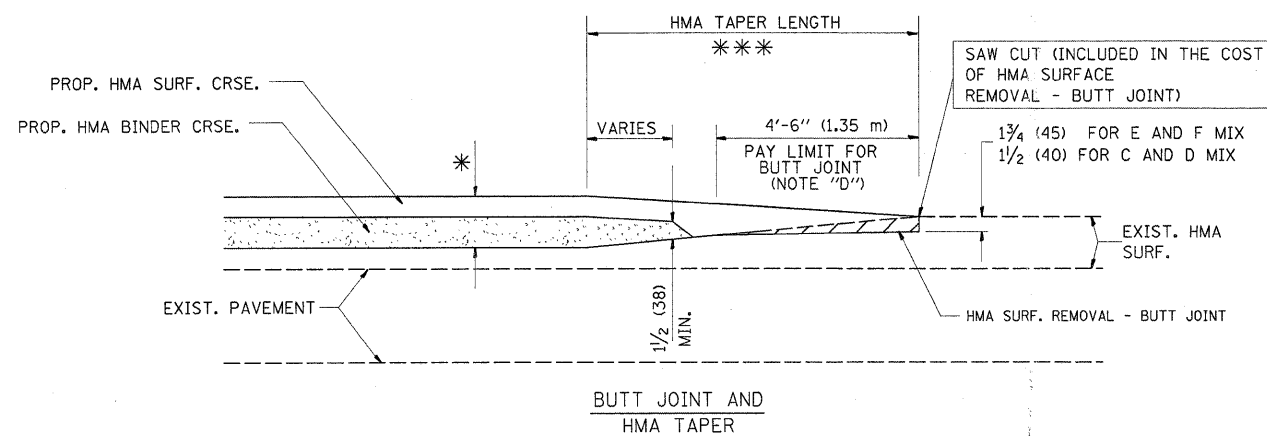
FILE NAME =	USER NAME = smthkl	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ci:\pw_work\PM\ID\1\SM\THKL\d0146173\01st.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97		3778	144 RS-5	WILL & COOK	37	27			
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED - M. GOMEZ 01-22-01		BD600-06 (BD-24)			CONTRACT NO. 60H93				
PLOT DATE = 12/16/2009		DATE - 03-11-94	REVISED - R. BORO 12-15-09		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			



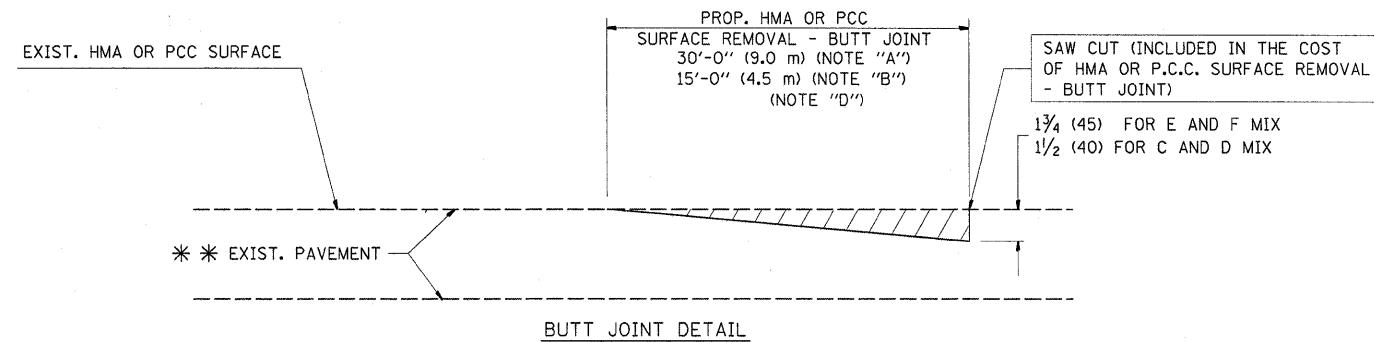
OPTION 1



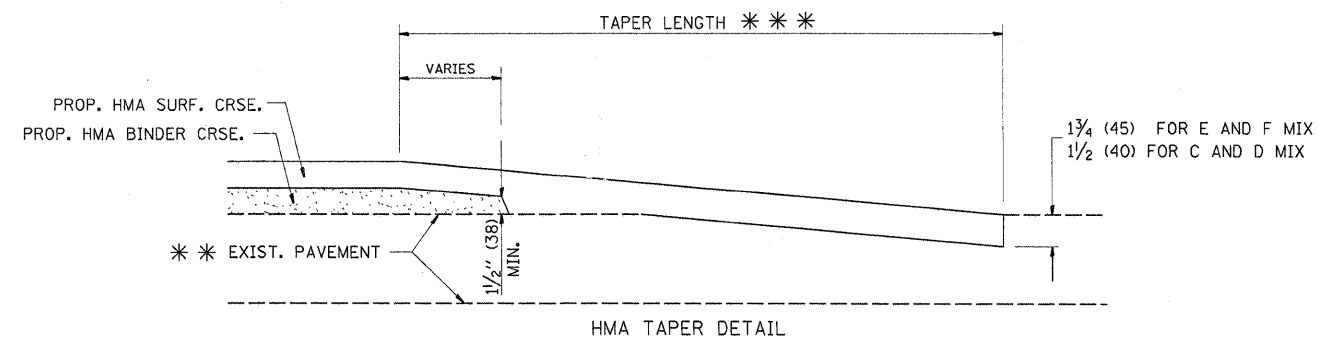
OPTION 2
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

* * * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- * * * 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

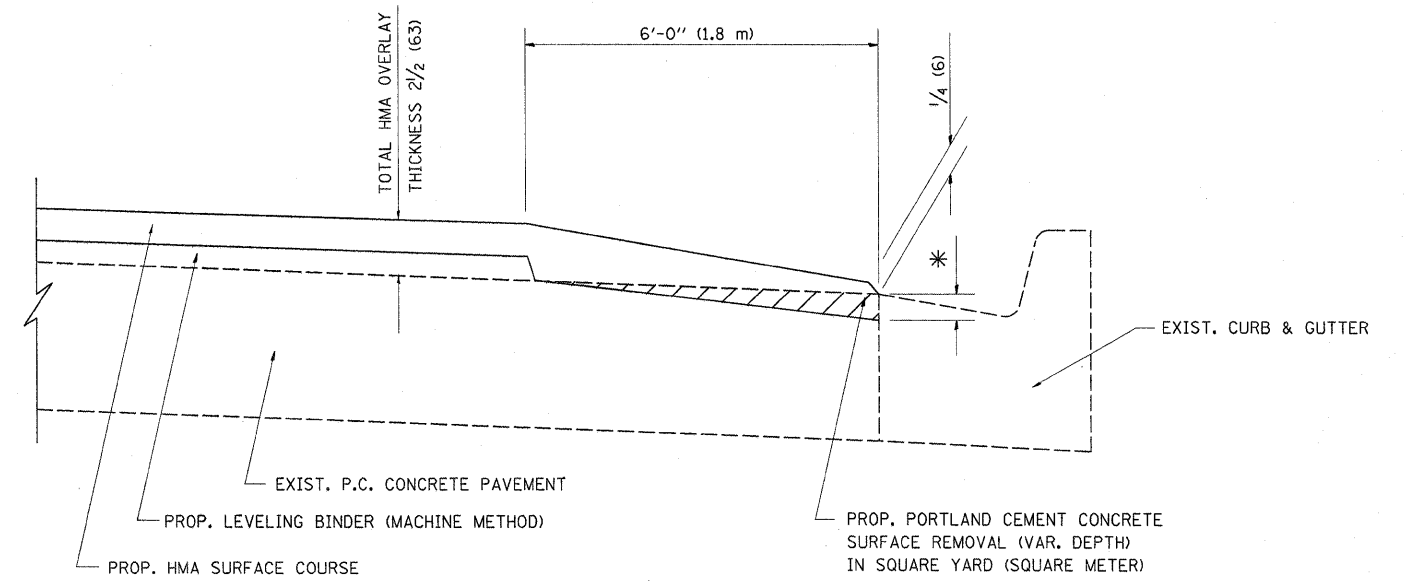
FILE NAME =	USER NAME = sm1thkl	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
cd:\pw_work\PW1007\SMITHKL\d0146173\Dist\dgn		DRAWN -	REVISED - A. ABBAS 03-21-97
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED - M. GOMEZ 04-06-01
PLOT DATE = 8/11/2009		DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND
HMA TAPER DETAILS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3778	144 RS-5	WILL & COOK	37	28
BD400-05 BD32		CONTRACT NO. 60H93		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



HMA TAPER AT
EDGE OF P.C.C PAVEMENT

HMA SURFACE	THICKNESS	LEVELING BINDER	THICKNESS	* MILLING AT GUTTER FLAG
C OR D	1/2 (38)	1 (25)		1/4 (33)
F	1 3/4 (44)	3/4 (19)		1/2 (38)

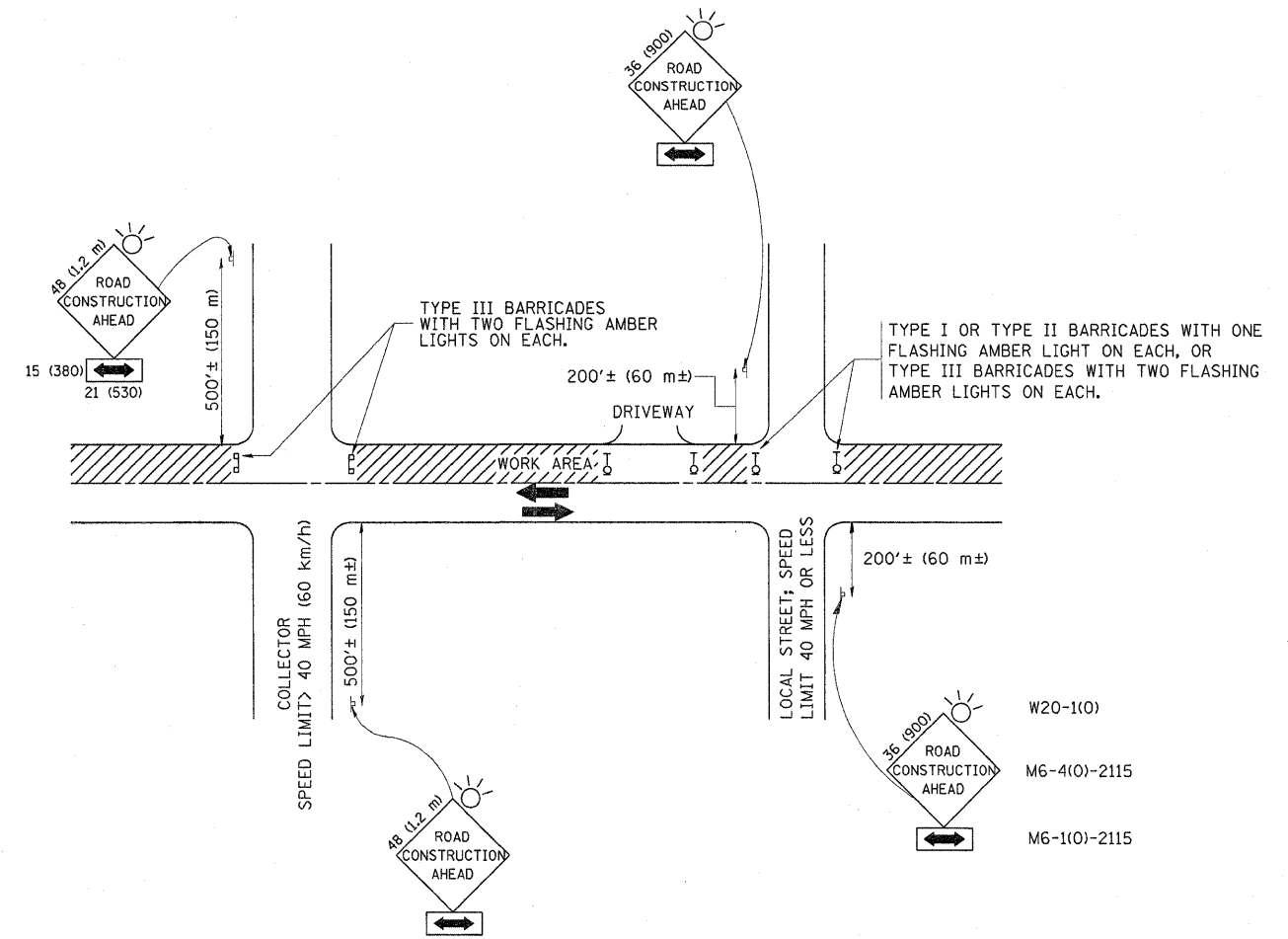
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = smthkl	DESIGNED - R. SHAH	REVISED - R. SHAH 10-25-94
cd:\pw_work\PWIDOT\SMITHKL\vd8146173\01st.dgn		DRAWN - JIS	REVISED - A. ABBAS 05-05-99
	PLOT SCALE = 50.0000' / IN.	CHECKED - A. ABBAS	REVISED - E. GOMEZ 12-21-00
	PLOT DATE = 8/11/2009	DATE - 09-10-94	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

HMA TAPER AT EDGE OF P.C.C PAVEMENT			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3778	144 RS-5	WILL & COOK	37	29
BD400-06 (BD33)		CONTRACT NO. 60H93		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

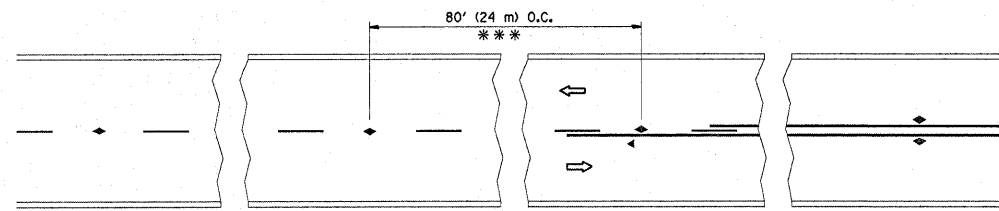
NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
 - 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 70150L, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
 - C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
 - D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

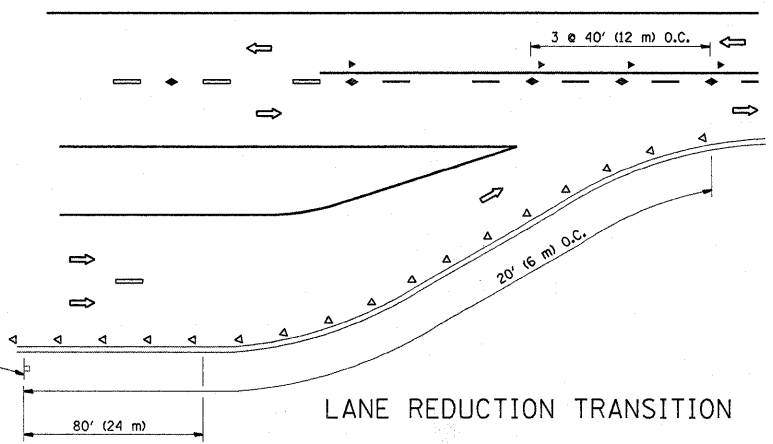
All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME =	USER NAME = smthkl	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pw\work\PWIDOT\SMITHKL\d8146173\Dist		DRAWN -	REVISED - A. HOUSEH 03-06-96		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	3778	144 RS-5	WILL & COOK	37	30
		CHECKED -	REVISED - A. HOUSEH 10-15-96					TC-10		CONTRACT NO. 60H93		
		DATE - 06-89	REVISED - T. RAMMACHER 01-06-00					FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

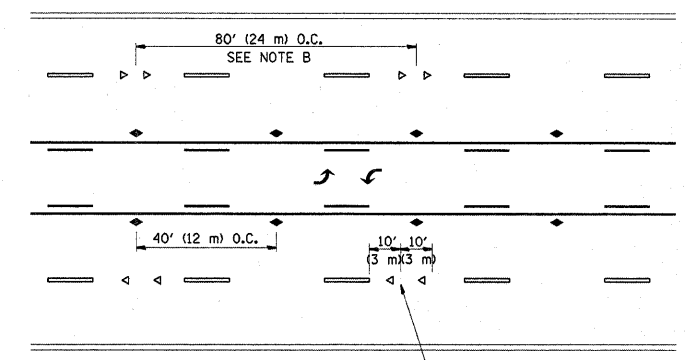


*** REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

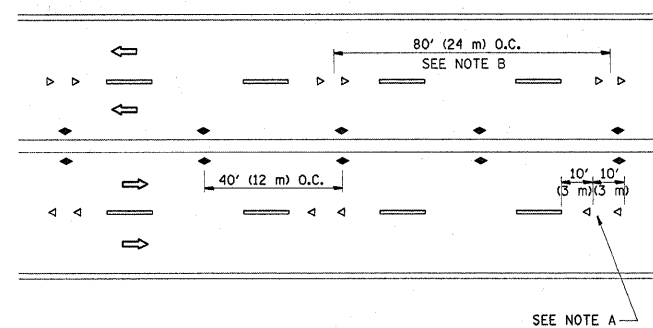
TWO-LANE/TWO-WAY



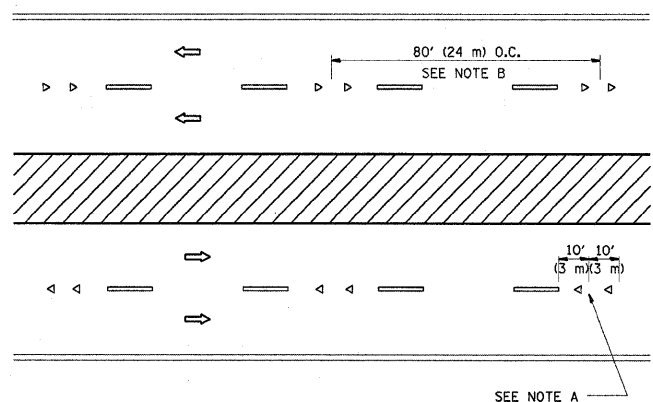
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

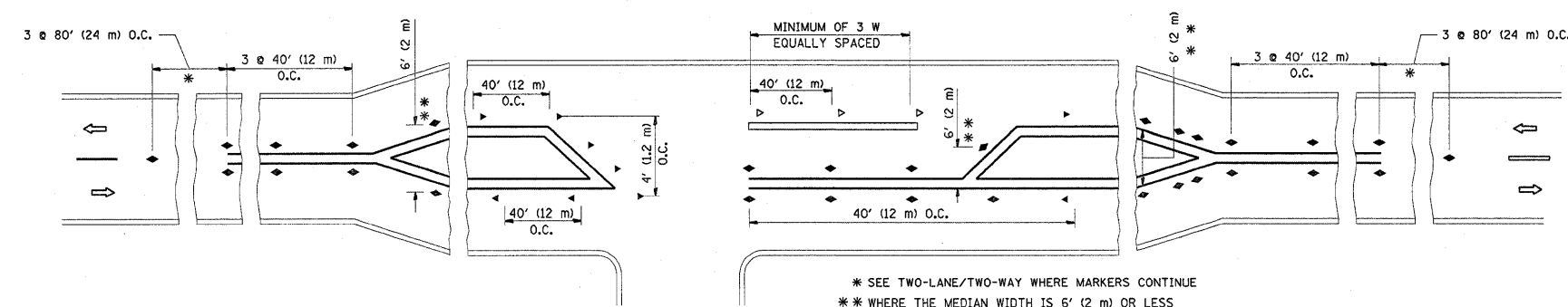
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHOULD BE INCLUDED IN THE PLANS.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

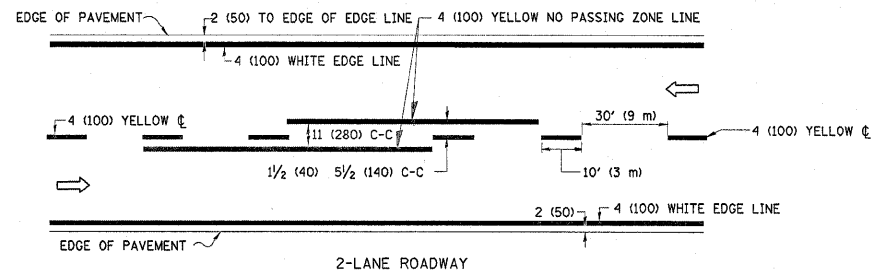


LEFT TURN

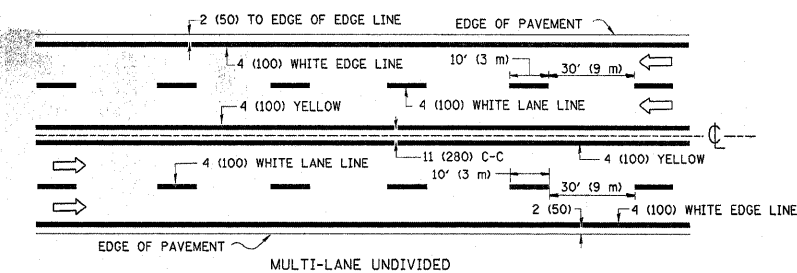
* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE
 ** WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

All dimensions are in inches (millimeters) unless otherwise shown.

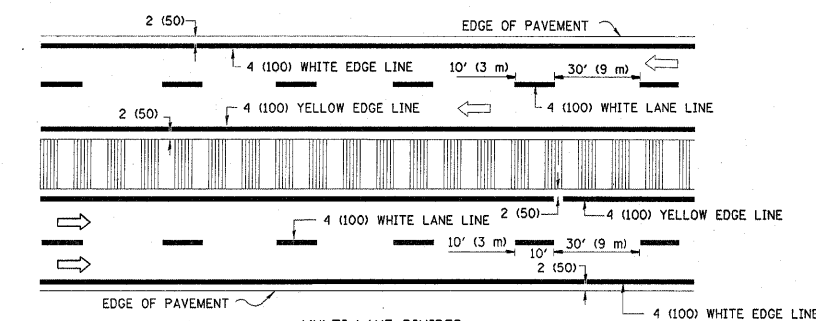
FILE NAME =	USER NAME = smthkl	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ca\pwwork\pwwid\SMITHKL\d0146173\d1e5\d.dgn		DRAWN -	REVISED - T. RAMMACHER 03-12-99		3778	144 RS-5	WILL & COOK	37	31			
PLOT SCALE = 50.0000' / IN.		CHECKED -	REVISED - T. RAMMACHER 01-06-00		TC-11				CONTRACT NO. 60H93			
PLOT DATE = 12/15/2009		DATE -	REVISED - C. JUCIUS 09-09-09		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			



2-LANE ROADWAY



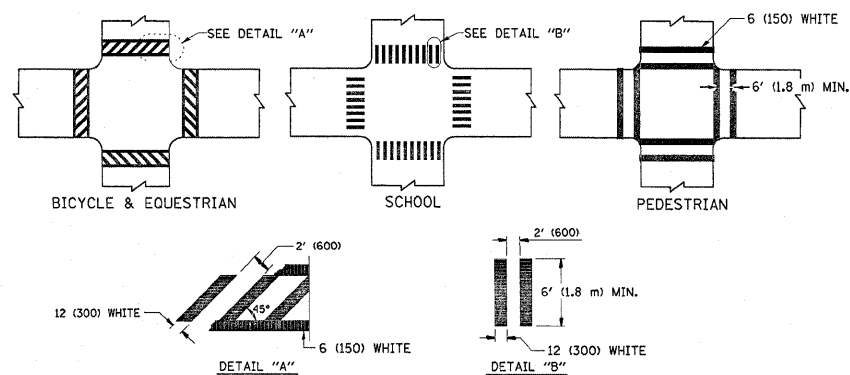
MULTI-LANE UNDIVIDED



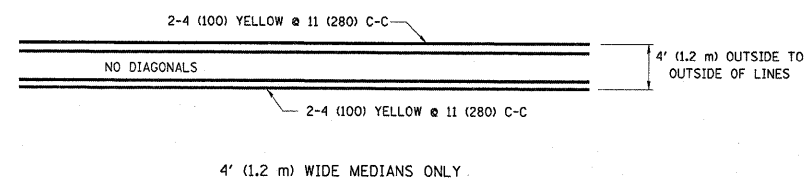
MULTI-LANE DIVIDED WITH MOUNTABLE MEDIAN

NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

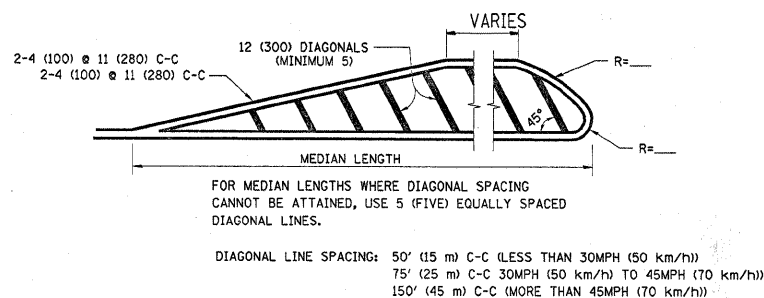
TYPICAL LANE AND EDGE LINE MARKING



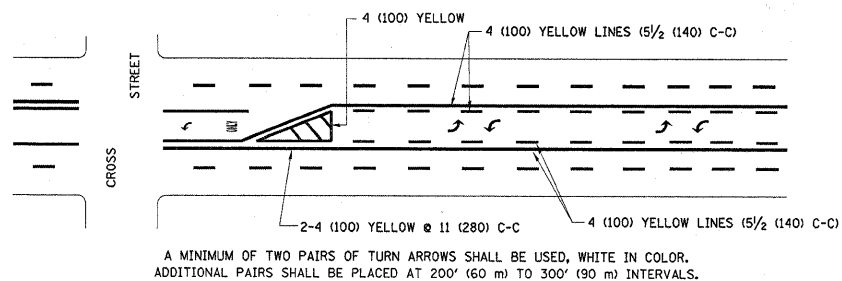
TYPICAL CROSSWALK MARKING



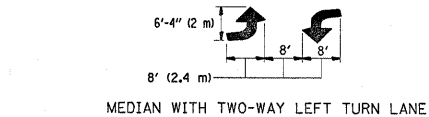
4' (1.2 m) WIDE MEDIANS ONLY



MEDIANS OVER 4' (1.2 m) WIDE

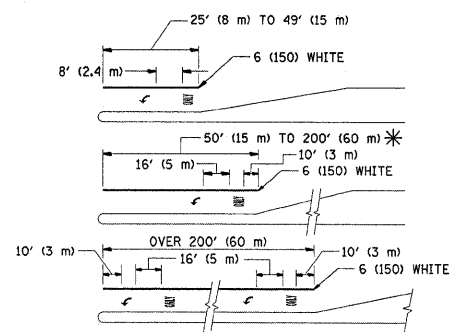


TYPICAL PAINTED MEDIAN MARKING



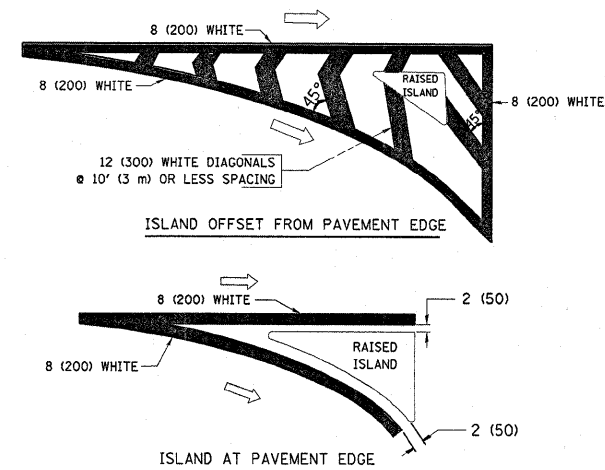
MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL LEFT (OR RIGHT) TURN LANE



TYPICAL TURN LANE MARKING

FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "RR"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

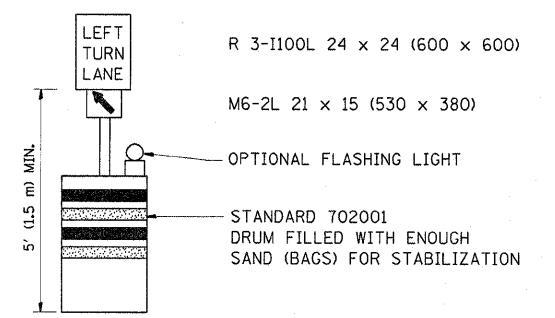
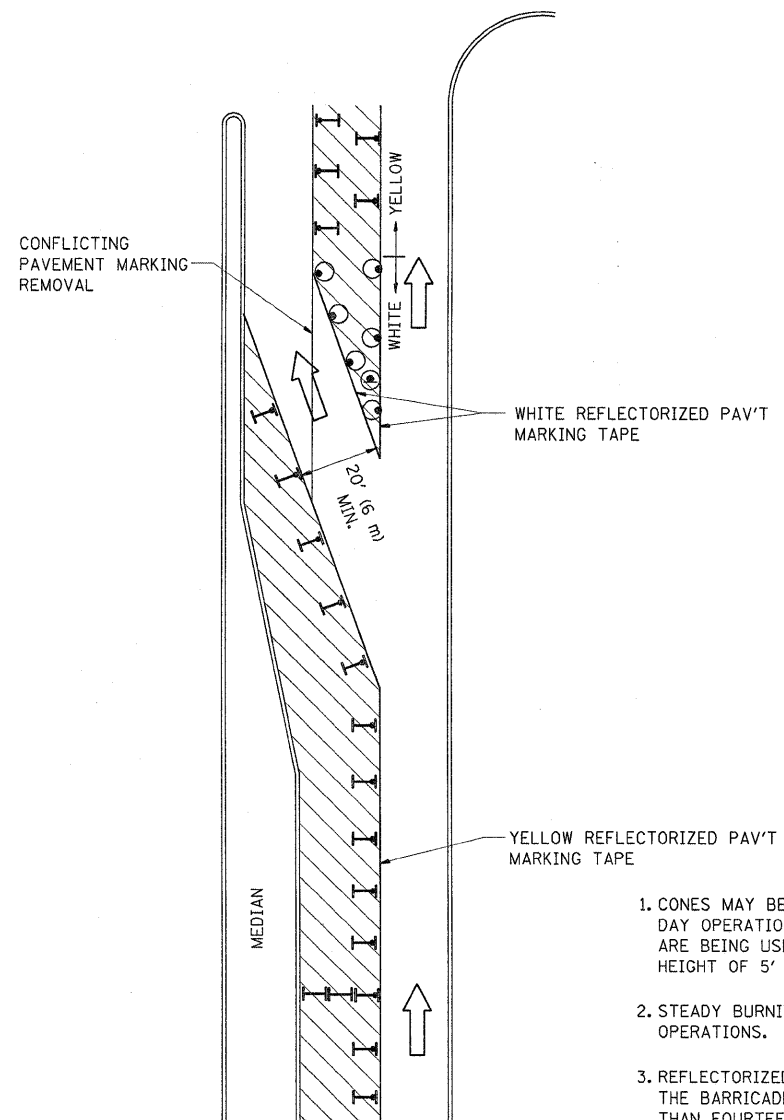
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	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -
	PLOT DATE = 12/16/2009	DATE - 03-19-90	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3778	144 RS-5	WILL & COOK	37	30
TC-13		CONTRACT NO. 60H93		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

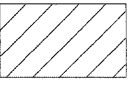
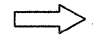






GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM BT 725 IS REQUIRED.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

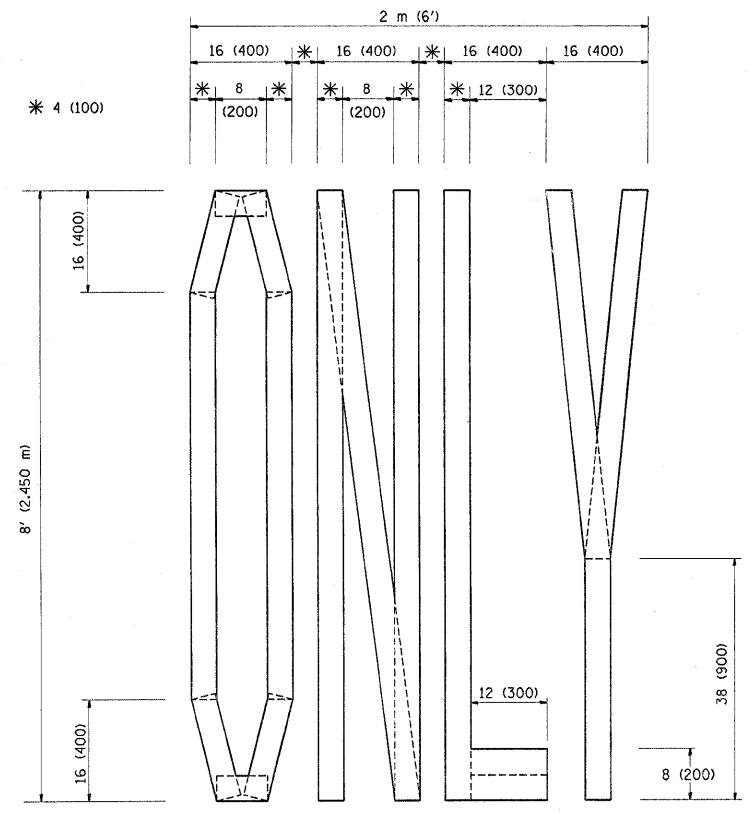
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	PLT SCALE = 50.0000' / IN.	CHECKED -	REVISED - A. HOUSEH 10-12-96
	PLT DATE = 8/11/2009	DATE -	REVISED -T. RAMMACHER 01-06-00

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

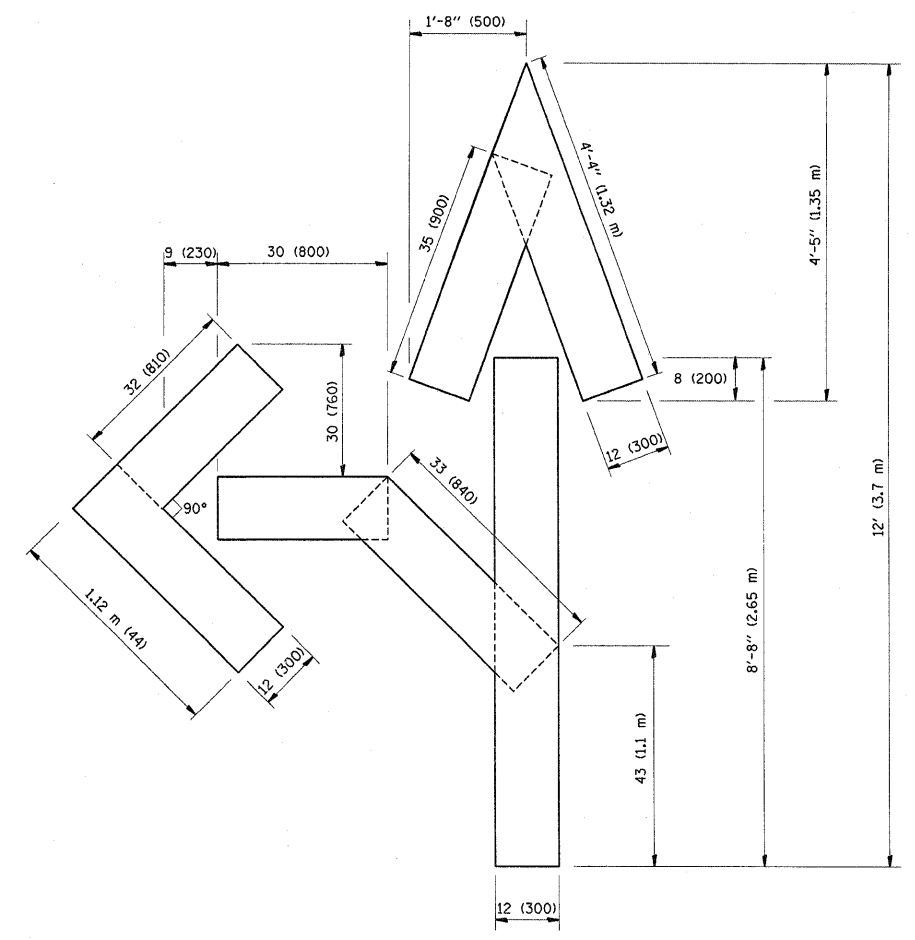
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

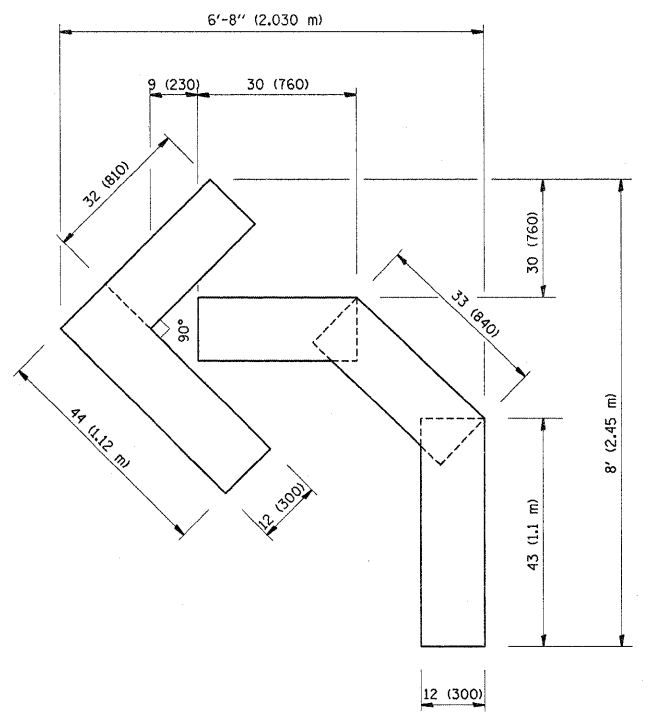
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3778	144 RS-5	WILL & COOK	37	33
TC-14		CONTRACT NO. 60H93		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

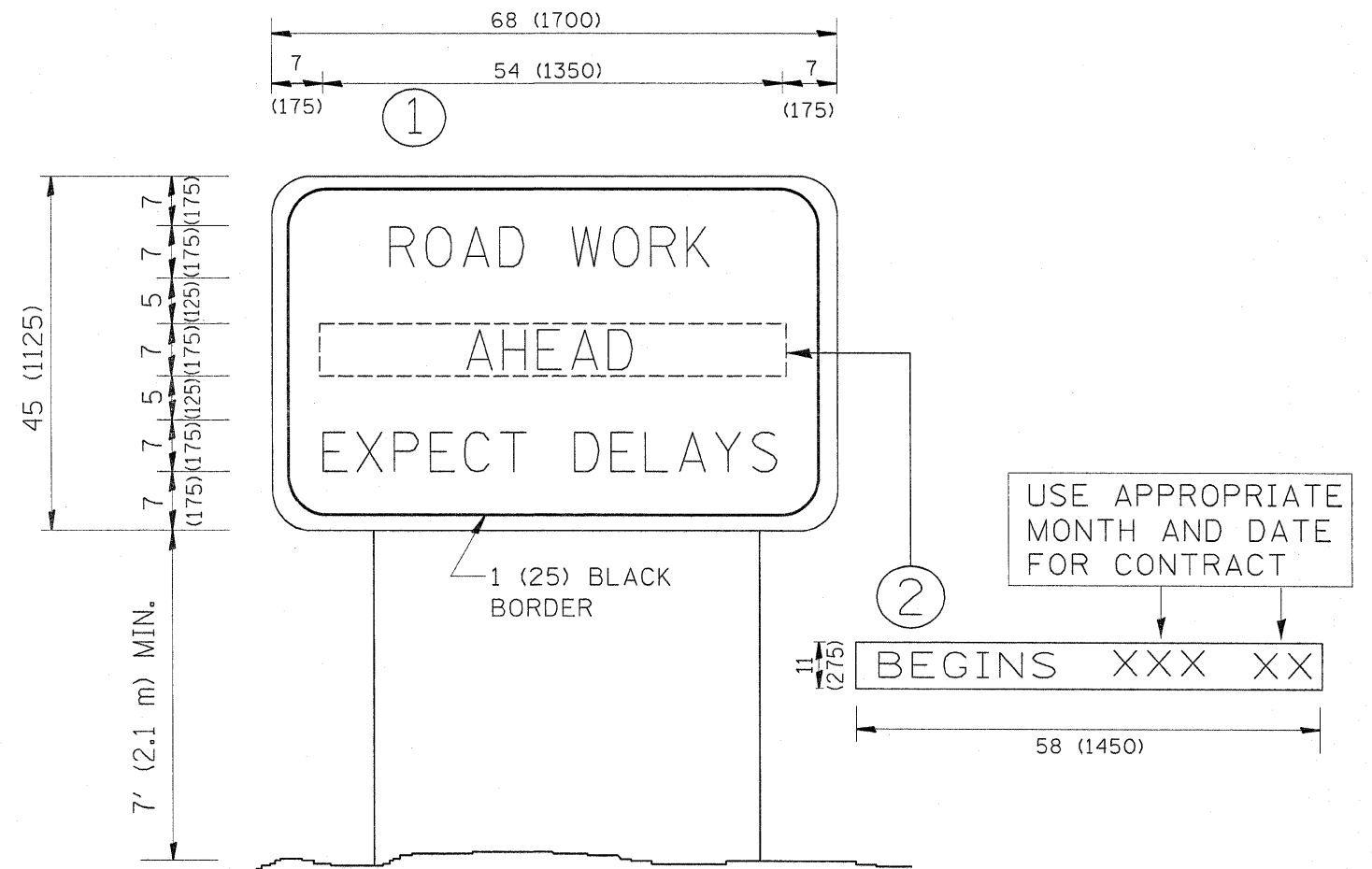
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = smthkl	DESIGNED -	REVISED -T. RAMMACHER 06-05-96
ca:\pw_work\PWIDOT\SMITHKL\d0146173\Dist	td.dgn	DRAWN -	REVISED -T. RAMMACHER 11-04-97
PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED -T. RAMMACHER 03-02-98	
PLOT DATE = 8/11/2009	DATE = 09-18-94	REVISED -E. GOMEZ 08-28-00	

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING			
SCALE: NONE	SHEET NO. 1	OF 1 SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3778	144 RS-5	WILL & COOK	37	34
TC-16			CONTRACT NO. 60H93	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = smithkl	DESIGNED -	REVISED - R. MIRS 09-15-97
es:\pw\work\VPWIDOT\SMITHKL\d0146173\Dist\d.dgn		DRAWN -	REVISED - R. MIRS 12-11-97
		CHECKED -	REVISED - T. RAMMACHER 02-02-99
		DATE -	REVISED - C. JUCIUS 01-31-07

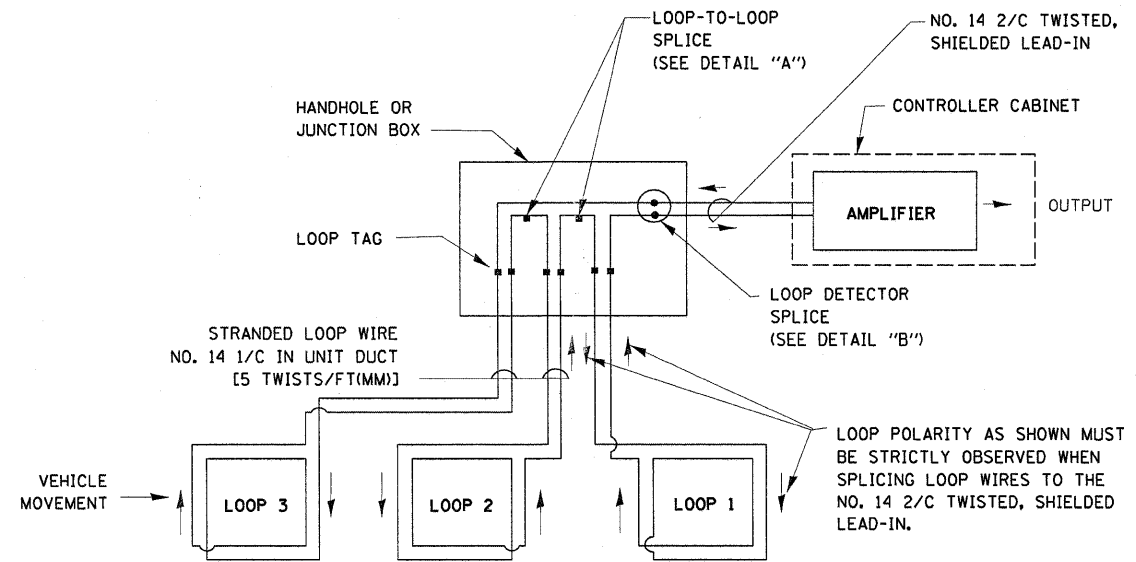
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ARTERIAL ROAD INFORMATION SIGN	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3778	144 RS-5	WILL & COOK	37	36
TC-22			CONTRACT NO. 60H93	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LOOP DETECTOR NOTES

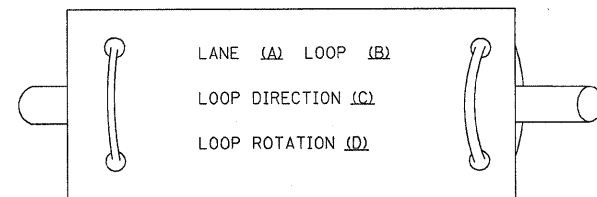
1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE UNIT DUCT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). UNIT DUCT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVESHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.



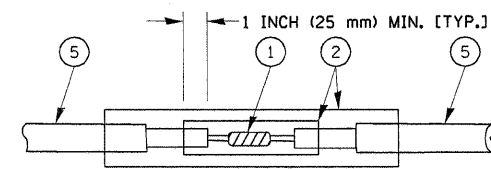
DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

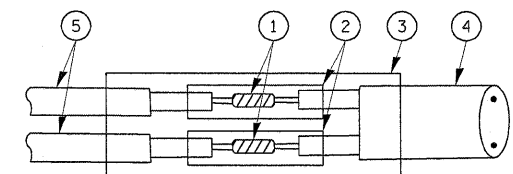
LOOP LEAD-IN CABLE TAG



- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.



**DETAIL "A"
LOOP-TO-LOOP SPLICE**



**DETAIL "B"
LOOP-TO-CONTROLLER SPLICE**

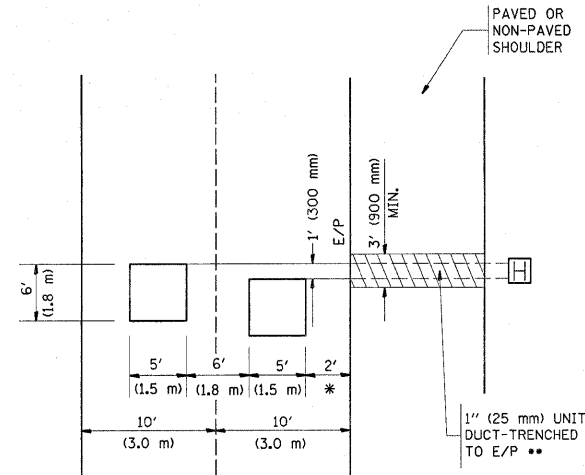
LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.

FILE NAME =	USER NAME = smsthl	DESIGNED - D.A.D.	REVISED - 11-12-01	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cd:\pw\work\PWIDOT\SMITHKL\d8146173\Dist1std.dgn	DRAWN - R.W.P.	REVISED - BUR. TRAFFIC 01-01-02	3778			144 RS-5	WILL & COOK	37	36	
PLOT SCALE = 50.0000 ' / IN.	CHECKED - D.A.Z.	REVISED -	TS-05			CONTRACT NO. 60H93				
PLOT DATE = 8/11/2009	DATE - 05-30-00	REVISED -	SCALE: NONE SHEET NO. 1 OF 4 SHEETS STA. TO STA.			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

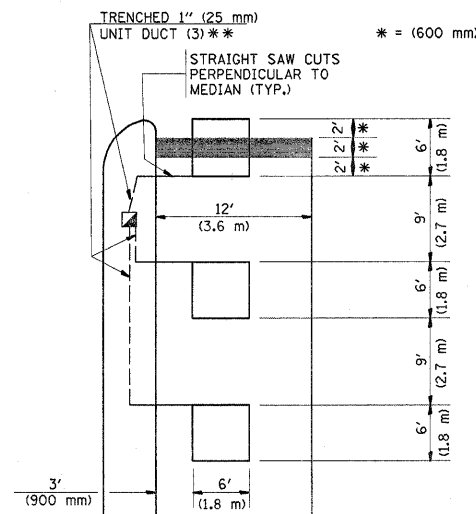


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

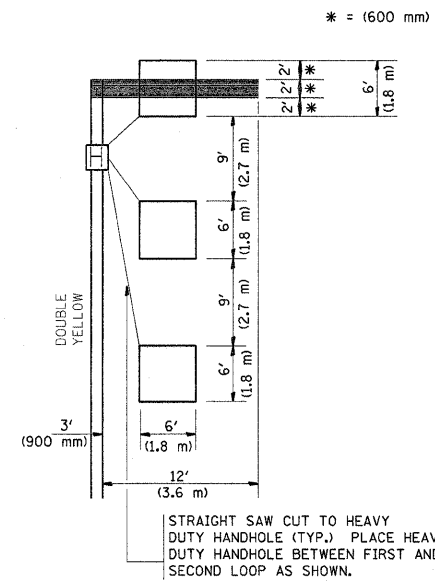


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



* = (600 mm)

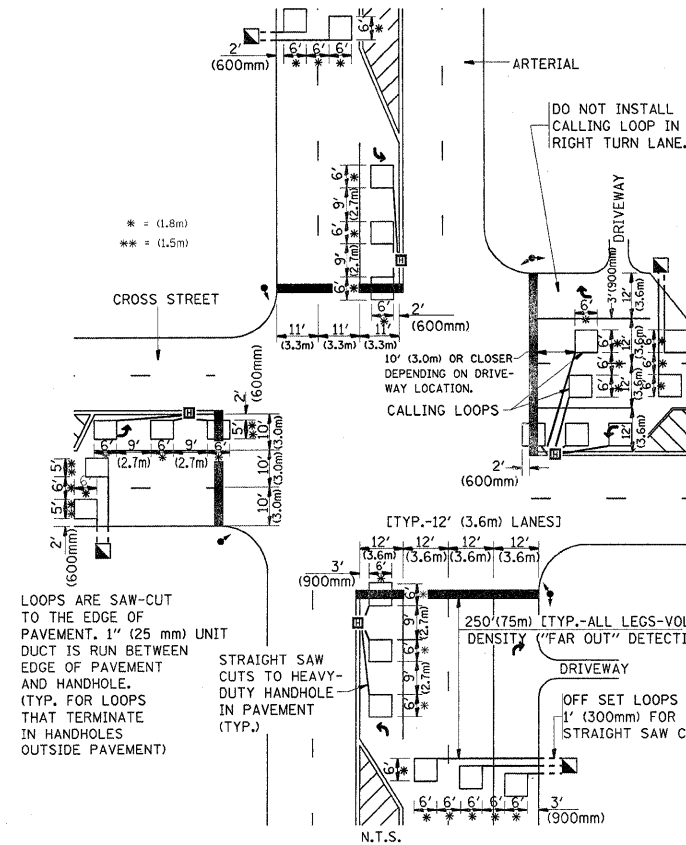
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

NOTES:

VEHICLES LOOP DETECTORS

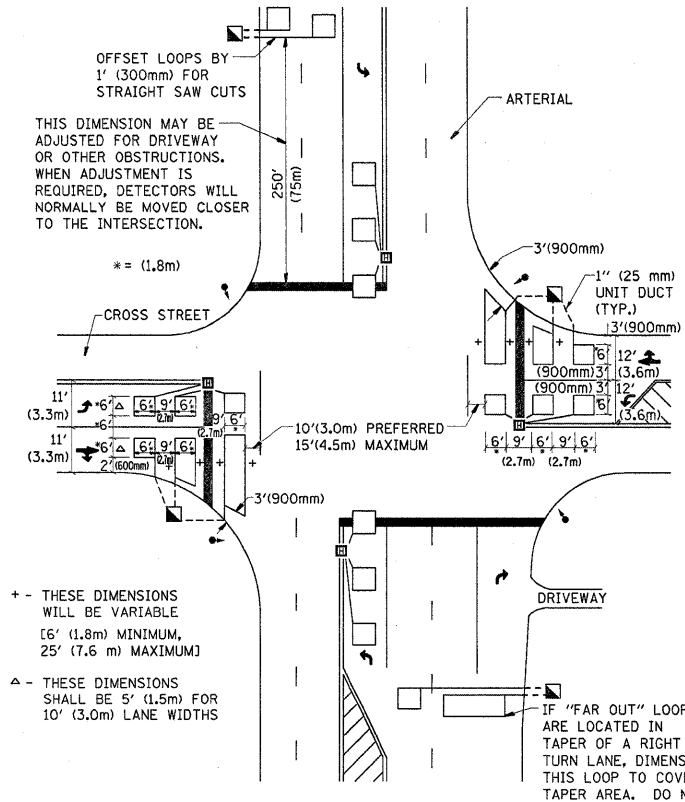
- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1
N.T.S.**

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2
N.T.S.**

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DIMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME =	USER NAME = sm:thkl	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING	F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ca:\pw_work\VPWIDOT\SMITHKL\08146173\Dist1.dgn		DRAWN -	REVISED -			3778	144 RS-5	WILL & COOK	37	37	
PLOT SCALE = 50.0000' / IN.		CHECKED - R.K.F.	REVISED -			TS-07		(CONTRACT NO. 60H93)			
PLOT DATE = 8/11/2009		DATE -	REVISED -			SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	