

CONSTRUCTION PLANS FOR TAYLORVILLE MUNICIPAL AIRPORT

CITY OF TAYLORVILLE TAYLORVILLE, ILLINOIS

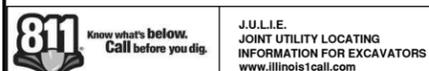
ILLINOIS PROJECT TAZ - 4342 S.B.G. PROJECT 3-17-SBGP-105

BITUMINOUS OVERLAY/RECONSTRUCTION OF NORTH AND SOUTHWEST APRON: INCLUDING CLEAN AND SEAL, CRACK REPAIR, MARKING, TIEDOWN ADJUSTMENT, FULL DEPTH RECONSTRUCTION OF AREAS, INSTALL TRENCH DRAINS

JANUARY 6, 2017

SUMMARY OF QUANTITIES			
ITEM NO.	Description	UNIT	QUANTITY
AR150510	ENGINEER'S FIELD OFFICE	LS	1
AR152410	UNCLASSIFIED EXCAVATION	CY	500
AR152480	SHOULDER ADJUSTMENT	SY	1,000
AR152540	SOIL STABILIZATION FABRIC	SY	500
AR162570	DETECTOR LOOP	LS	1
AR201660	BITUMINOUS CRACK REPAIR	LF	2,500
AR201661	CLEAN & SEAL BITUMINOUS CRACKS	LF	2,700
AR201670	CRACK CONTROL FABRIC	SY	1,150
AR208612	12" AGGREGATE BASE COURSE	SY	500
AR209606	CRUSHED AGG. BASE COURSE - 6"	SY	850
AR401610	BITUMINOUS SURFACE COURSE	TON	1,725
AR401650	BITUMINOUS PAVEMENT MILLING	SY	14,100
AR401655	BUTT JOINT CONSTRUCTION	SY	860
AR401910	REMOVE & REPLACE BIT. PAVEMENT	SY	20
AR501506	6" PCC PAVEMENT	SY	360
AR501604	4" PCC SIDEWALK	SF	150
AR501690	PCC SIDEWALK REMOVAL	SF	150
AR501900	REMOVE PCC PAVEMENT	SY	360
AR510510	TIE DOWN	EACH	15
AR510511	TIE DOWN-TYPE A	EACH	48
AR510900	REMOVE TIE DOWN	EACH	63
AR602510	BITUMINOUS PRIME COAT	GAL	250
AR603510	BITUMINOUS TACK COAT	GAL	2,250
AR620520	PAVEMENT MARKING-WATERBORNE	SF	1,700
AR620525	PAVEMENT MARKING-BLACK BORDER	SF	1,800
AR705546	6" NON PERFORATED UNDERDRAIN	LF	160
AR705620	UNDERDRAIN END SECTION	EACH	1
AR705645	UNDERDRAIN CONNECTION	EACH	1
AR751001	TRENCH DRAIN	LF	460
AR751410	INLET	EACH	1
AR751940	ADJUST INLET	EACH	2
AR901510	SEEDING	AC	0.2
AR908525	KNITTED STRAW MAT	SY	1,000

Sheet List Table		
Sheet No.	Sheet Index	Sheet Title
1	GC000	COVER SHEET
2	GC001	SITE PLAN
3	GC002	CONSTRUCTION SAFETY PHASING PLAN
4	GC003	CONSTRUCTION ACTIVITY PLAN 1
5	GC004	CONSTRUCTION ACTIVITY PLAN 2
6	GC005	CONSTRUCTION SAFETY PHASING PLAN NOTES
7	CD101	EXISTING CONDITIONS AND REMOVALS
8	CD501	EXISTING TYPICAL SECTIONS
9	CP100	PROPOSED TYPICAL SECTIONS
10	CP101	PROPOSED IMPROVEMENTS 1
11	CP102	PROPOSED IMPROVEMENTS 2
12	CP103	PROPOSED IMPROVEMENTS 3
13	CP501	PAVEMENT REPAIR DETAILS
14	CU501	DRAINAGE DETAILS
15	CU502	UNDERDRAIN DETAILS
16	CS101	PROPOSED STAKING PLAN 1
17	CS102	PROPOSED STAKING PLAN 2
18	CM101	PAVEMENT MARKING & TURFING PLAN 1
19	CM102	PAVEMENT MARKING & TURFING PLAN 2
20	CM501	PAVEMENT MARKING AND TIE DOWN DETAILS
21	CJ501	JOINTING DETAILS
22	EI501	DETECTOR LOOP DETAILS



THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTORS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

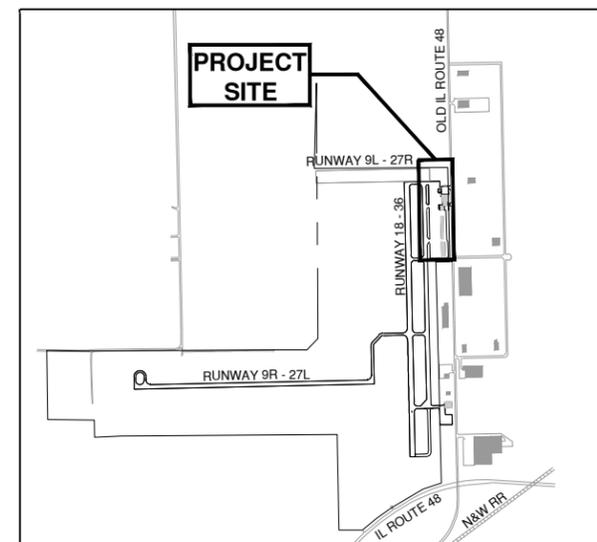
DESIGN INFORMATION
GEOMETRIC CRITERIA
AIRPLANE DESIGN GROUP II
TAXIWAY DESIGN GROUP II
PAVEMENT DESIGN CRITERIA
AIRCRAFT SINGLE WHEEL GEAR
DEPARTURE WEIGHT = 12,500 LBS.
100 ANNUAL DEPARTURES

CALL J.U.L.I.E.
BEFORE EXCAVATING
1-800-892-0123

TOWNSHIP: 13 NORTH
RANGE: 2 WEST OF THE 4TH P.M.
SECTION: 32
COUNTY: CHRISTIAN
CIVIL TOWNSHIP: TAYLORVILLE



LOCATION MAP



SITE PLAN

January 11, 2017

 Christopher B. Groth
 Exp. 11-30-2017

CITY OF TAYLORVILLE
TAYLORVILLE MUNICIPAL AIRPORT
TAYLORVILLE, ILLINOIS

APPROVED: *Shag Brotherton*
MAYOR
DATE: 1/12/17

APPROVED: *Phyllis J. Peabody*
CITY CLERK
DATE: 1/12/17

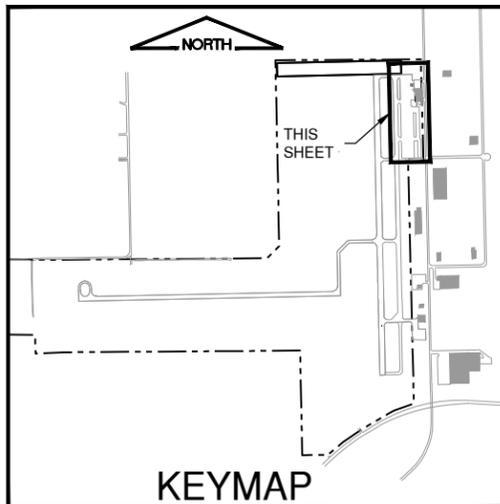
CMT
CRAWFORD MURPHY & TILLY, INC.
CONSULTING ENGINEERS
■ SPRINGFIELD, IL ■ AURORA, IL ■ ST. LOUIS, MO

SUBMITTED BY: *Chris B. Groth*
DATE: January 11, 2017

CMT JOB NUMBER 13050-01

LEGEND

	BITUMINOUS PAVEMENT MILLING - 0" - 2"		EXISTING STAKE MOUNTED TAXIWAY LIGHT		EXISTING DUCT MARKER
	FULL DEPTH PCC PAVEMENT REMOVAL		EXISTING CATCH BASIN		EXISTING PAVING MARKING
	UNCLASSIFIED EXCAVATION / FULL DEPTH AGGREGATE REMOVAL [NO GEOTEXTILE]		EXISTING UNDERDRAIN CLEANOUT		EXISTING STORM SEWER
	BUTT JOINT CONSTRUCTION		GATE POST TO REMAIN		EXISTING AIRFIELD ELECTRIC CIRCUIT
	FULL DEPTH PCC SIDEWALK REMOVAL		EXISTING TIE DOWN		EXISTING UNDERGROUND ELECTRIC
R	REMOVALS		EXISTING OVERHEAD LIGHT		EXISTING WATER LINE
RR	REMOVE AND REPLACE		EXISTING IRON PIN		EXISTING GAS LINE
	SAWCUT		EXISTING SURVEY MONUMENT		EXISTING TELEPHONE LINE
			EXISTING WATER VALVE/METER		EXISTING FENCE
			EXISTING MONITORING WELL		EXISTING DUCT

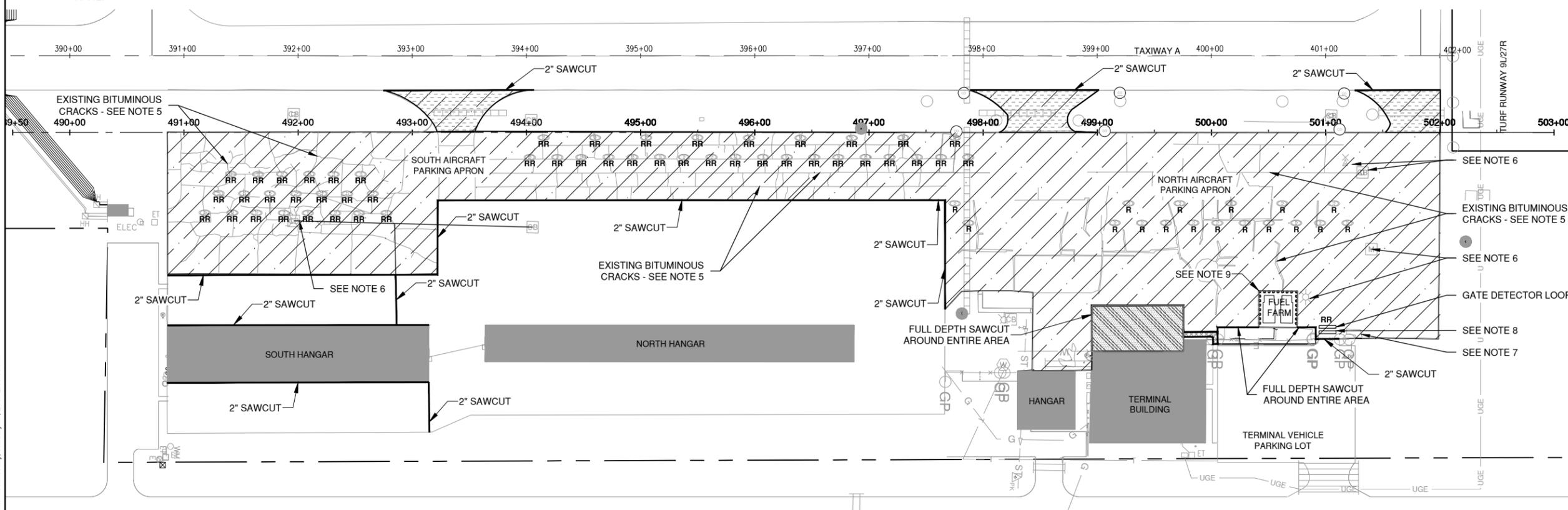


NOTES:

- EXISTING BUILDINGS HAVE UTILITY CONNECTIONS, SLIDING DOOR HARDWARE AND DRAINAGE HARDWARE THAT EXTEND BEYOND THE FOOTPRINT OF THE BUILDING (NOT SHOWN). THE CONTRACTOR MAY BE REQUIRED TO REMOVE THIS HARDWARE DURING CONSTRUCTION IN ORDER TO PROPERLY PROCEED WITH OPERATIONS AND WILL BE REQUIRED TO REPLACE THESE ITEMS TO PRE-CONSTRUCTION CONDITIONS AND TO THE SATISFACTION OF THE ENGINEER AND THE AIRPORT ONCE WORK HAS BEEN COMPLETED. ANY ADDITIONAL TIME AND EFFORT FOR THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT.
- THE MILLING OPERATIONS SHALL REMAIN A FIXED DISTANCE FROM THE FUEL FARM ISLAND AND THE FACE AND SIDE OF THE BUILDINGS WHERE THERE ARE NO BUILDING DOORS. THE RESIDENT ENGINEER WILL DETERMINE THE ACCEPTABLE OFFSET FOR THE MILLING EQUIPMENT. MILLING OPERATIONS AT THE BUILDING DOORS SHALL BE MILLED UP TO THE BUILDING FOUNDATION. MILLING ALONG THE BUILDING FOUNDATION MAY REQUIRE SPECIAL EQUIPMENT CAPABLE OF MILLING IN CLOSE PROXIMITY OF A VERTICAL FACE OR THE USE OF HAND TOOLS. COSTS ASSOCIATED WITH THIS MILLING SHALL BE INCIDENTAL TO THE MILLING PAY ITEM.
- THE CONTRACTOR SHALL NOT DAMAGE EXISTING BUILDING FOUNDATIONS OR BUILDING DOORS. DAMAGE CAUSED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.
- SAWCUT SHOWN SHALL BE INCIDENTAL TO THE MILLING OPERATIONS. ADDITIONAL AREAS NOT SHOWN MAY REQUIRE SAWCUT. NO ADDITIONAL PAYMENTS WILL BE MADE FOR SAWCUT.
- THE RESIDENT ENGINEER SHALL DETERMINE WHICH BITUMINOUS CRACKS WILL NEED TO BE EXTRA DEPTH REPAIRED. POTENTIAL LOCATIONS DETAILED ON SHEETS CP101 AND CP102.
- THE CONTRACTOR SHALL TAKE CARE TO PAVE AROUND OR OVER THESE ITEMS AND ANY DAMAGE DUE TO THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED TO PRE-CONSTRUCTION CONDITIONS TO THE SATISFACTION OF THE AIRPORT AND THE ENGINEER AT THE EXPENSE OF THE CONTRACTOR.
- THE AREA AROUND THE GATE OPERATOR SHALL HAVE THE TOP TWO (2") INCHES OF AGGREGATE REMOVED AND BE PAVED MATCHING THE SURROUNDING AREA.
- THE EXISTING ELECTRIC GATE DETECTOR LOOPS ON THE AIRSIDE SHALL BE REMOVED DURING MILLING OPERATIONS AND THIS SHALL BE CONSIDERED INCIDENTAL TO THE MILLING PAY ITEM. THE DETECTOR LOOPS SHALL BE REPLACED PER THE DETAILS SHOWN ON THE DETECTOR LOOP DETAILS SHEET. THE REPLACEMENT AND INSTALLATION OF THE NEW DETECTOR LOOPS SHALL BE PAID FOR UNDER ITEM AR162570 DETECTOR LOOP, LUMP SUM.
- EXTREME CAUTION SHALL BE EXERCISED DURING PAVING OPERATIONS IN THE VICINITY OF THE FUEL FARM. UNDERGROUND UTILITIES ARE PRESENT IN THESE AREAS THAT SHALL NOT BE DISTURBED.

THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE PROJECT ENGINEER ASSUMES ANY RESPONSIBILITY WHATEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE, AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION.

IT SHALL BE THE CONTRACTORS RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER, AND THE RESIDENT ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.



100% SUBMITTAL
JANUARY 6, 2017

**BITUMINOUS OVERLAY/
RECONSTRUCTION OF NORTH
AND SOUTHWEST APRON**

OWNER



**TAYLORVILLE MUNICIPAL
AIRPORT**
TAYLORVILLE, ILLINOIS

MARK	DATE	DESCRIPTION
SBG NO. 3-17-SBGP-105		
IL PROJ. NO. TAZ-4342		
CMT PROJECT NO: 13050-01-01		
CAD DWG FILE: TAZ-4342-CD101.DWG		
DESIGNED BY: PRR		
DRAWN BY: CMT		
CHECKED BY: RLV		
APPROVED BY: CBG		
COPYRIGHT: CRAWFORD, MURPHY & TILLY, INC. 2015		

**EXISTING
CONDITIONS AND
REMOVALS**

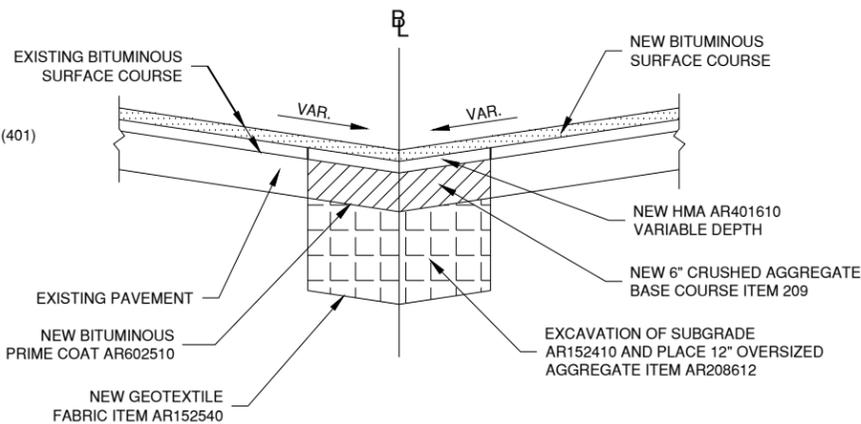


License No. 184-00613

CONSULTANTS

LEGEND

- PROPOSED LIMITS OF IMPROVEMENTS
- PROPOSED BITUMINOUS CRACK REPAIR
- PROPOSED TIE DOWN MOORING EYE - TYPE A
- PROPOSED TIE DOWN MOORING EYE
- PROPOSED 2" BITUMINOUS SURFACE COURSE OVERLAY (401)
PROPOSED TACK COAT (603)
- PROPOSED BUTT JOINT CONSTRUCTION (401)
- ADJUST INLET (AR751940)
- NEW INLET (AR751910)



SUBGRADE REPAIR DETAIL

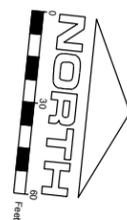
N.T.S.

NOTES:

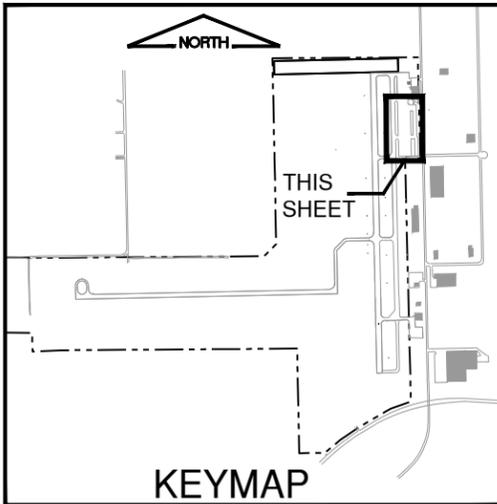
1. LOCATION OF SUBGRADE REPAIR SHALL BE DETERMINED AT THE TIME OF CONSTRUCTION AND APPROVED BY THE RESIDENT ENGINEER.
2. LENGTH AND WIDTH OF SUBGRADE REPAIR SHALL VARY DEPENDENT ON THE AFFECT AREA IN NEED OF REPAIR.
3. THE FOLLOWING SHALL BE THE STEPS TAKEN TO REPAIR THE SUBGRADE:
 - 3.A. REMOVE ADDITIONAL 2" OF BITUMINOUS SURFACE COURSE - (IF NEEDED)
 - 3.B. REMOVE EXISTING AGGREGATE SUBBASE IN AREAS THAT REQUIRE SUBGRADE REPAIR (ASSUMED 6" DEPTH).
 - 3.C. EXCAVATE 18" DEPTH OF SUBGRADE AR152410.
 - 3.D. COMPACT EXPOSED SUBGRADE.
 - 3.E. PLACE GEOTEXTILE FABRIC ITEM AR152540 ON TOP OF COMPACTED SUBGRADE.
 - 3.F. PLACE AND COMPACT OVERSIZED AGGREGATE ITEM AR208612 AT A THICKNESS OF 12".
 - 3.G. PLACE AND COMPACT 6" CRUSHED AGGREGATE ITEM 209, MEETING THE GRADES OF THE ADJACENT EXISTING AGGREGATE LAYER.
4. APPLY PRIME COAT AND CONSTRUCT BITUMINOUS LAYER AS SPECIFIED IN THE PLANS.
5. METHODS OF CONSTRUCTION SHALL MEET THE REQUIREMENTS SPECIFIED IN THE SPECIAL PROVISIONS.

NOTES:

1. A QUANTITY OF AR152511 SUBGRADE REPAIR HAS BEEN ALLOCATED TO THIS PROJECT TO BE USED AS NECESSARY AT THE DISCRETION OF THE RESIDENT ENGINEER. LOCATIONS ARE DETAILED BELOW.
2. TWO METHODS OF EXISTING CRACK REPAIR ARE TO BE UTILIZED: BITUMINOUS CRACK REPAIR AND JOINT SEALING FILLER CRACK REPAIR. POTENTIAL LOCATIONS ARE DETAILED BELOW. THIS QUANTITY MAY FLUCTUATE DURING THE CONSTRUCTION PROCESS.



KEYMAP



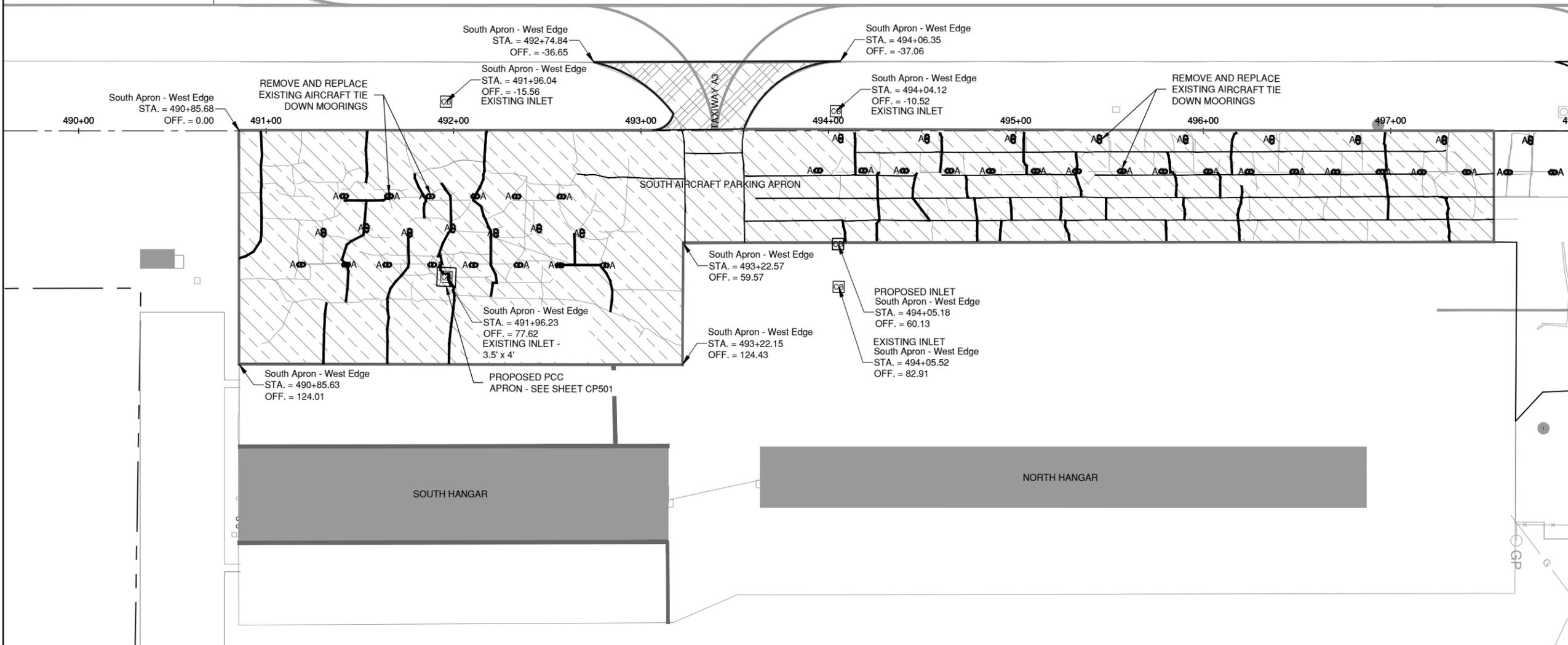
100% SUBMITTAL
JANUARY 6, 2017

BITUMINOUS OVERLAY/ RECONSTRUCTION OF NORTH AND SOUTHWEST APRON

OWNER



TAYLORVILLE MUNICIPAL
AIRPORT
TAYLORVILLE, ILLINOIS

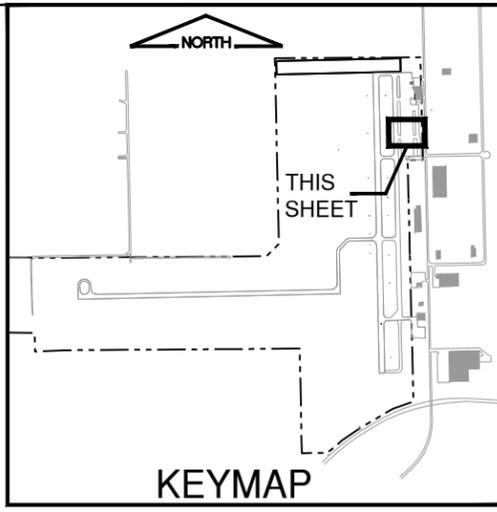
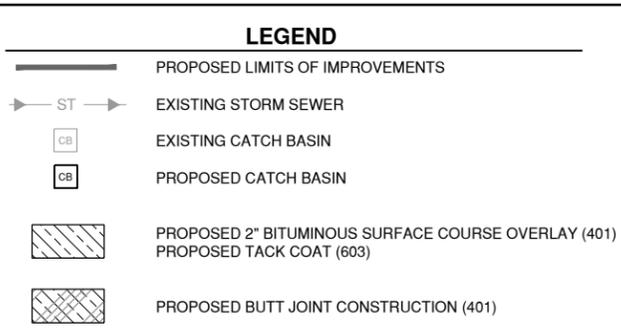
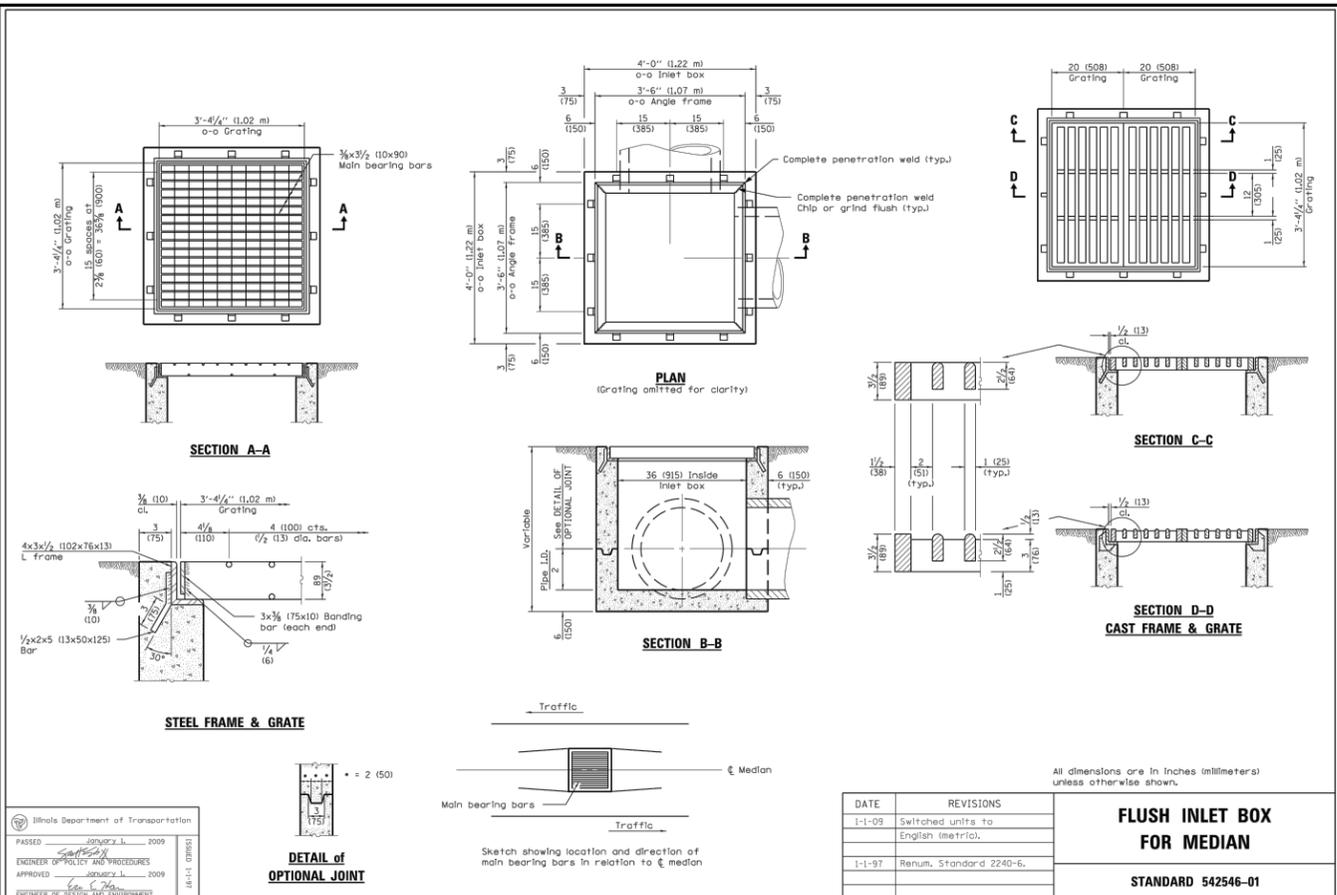


MARK | DATE | DESCRIPTION

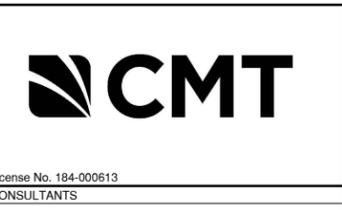
SBG NO. 3-17-SBGP-105
IL PROJ. NO. TAZ-4342
CMT PROJECT NO: 13050-01-01
CAD DWG FILE: TAZ-4342-CP102.DWG
DESIGNED BY: PRR
DRAWN BY: CMT
CHECKED BY: RLV
APPROVED BY: CBG
COPYRIGHT: CRAWFORD, MURPHY & TILLY, INC. 2015

SHEET TITLE PROPOSED IMPROVEMENTS 2

CP102
SHEET 11 OF 22



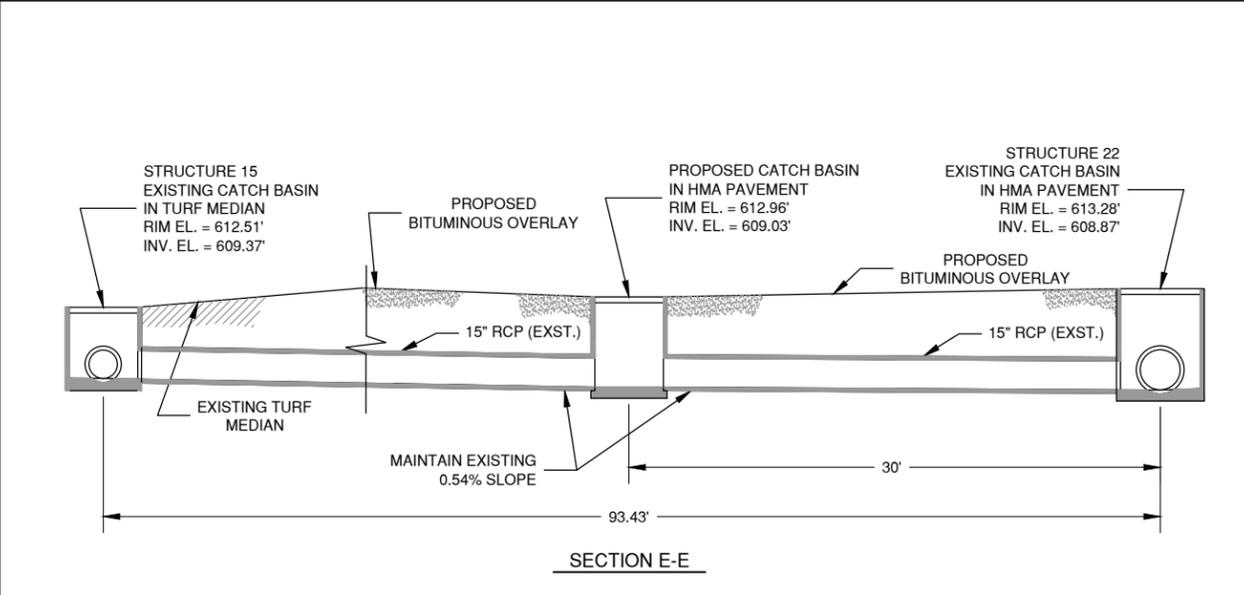
NOTE:
CONTRACTOR SHALL GRADE PROPOSED PAVEMENT SUCH THAT WATER DRAINS TO PROPOSED CATCH BASIN. SEE CS101 AND CS102.



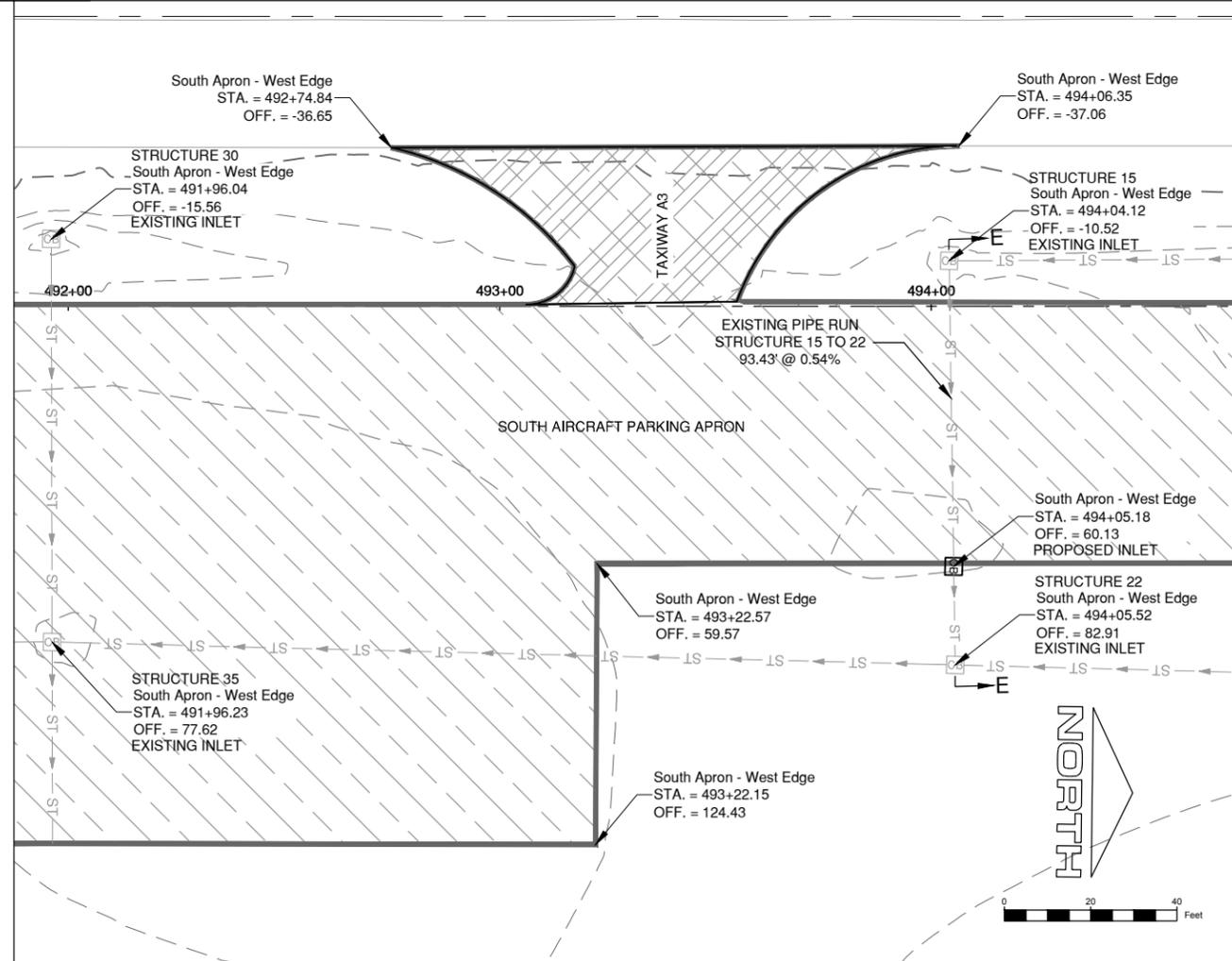
100% SUBMITTAL
JANUARY 6, 2017

BITUMINOUS OVERLAY/
RECONSTRUCTION OF NORTH
AND SOUTHWEST APRON

OWNER
TAYLORVILLE MUNICIPAL AIRPORT
TAYLORVILLE, ILLINOIS



- NOTES**
- CATCH BASIN STRUCTURE IS TO CONFORM TO IDOT STANDARD 542546-01, AVAILABLE IN IDOT HIGHWAY STANDARDS, LATEST EDITION.
 - CAST-IN-PLACE BASE SHOWN. BASE SHALL BE SQUARE. IF BASE IS PRECAST INTEGRAL WITH WALLS, THE FOOTPRINT OF THE BASE IS NOT REQUIRED TO EXTEND BEYOND THE OUTER EDGE OF THE WALLS.
 - FOR ADDITIONAL CONFIGURATIONS, MAINTAIN A MINIMUM OF 12 INCHES OF CONCRETE BETWEEN VERTICAL EDGES OF PIPE OPENINGS.
 - 12 INCH MINIMUM RISER HEIGHT ABOVE ALL PIPES.
 - PIPE RUNS AND STRUCTURES ARE TO MAINTAIN THE EXISTING 0.54% SLOPE.



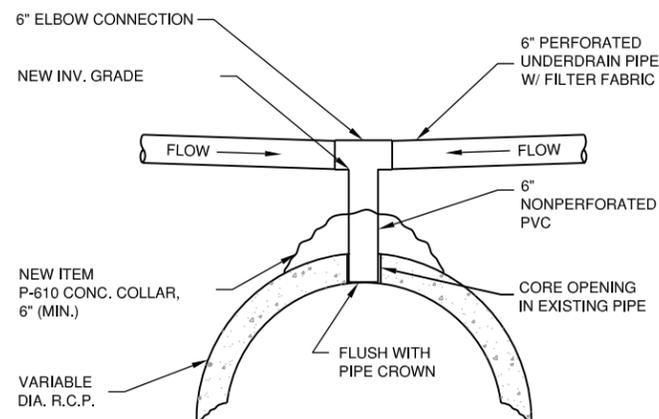
MARK DATE DESCRIPTION

SBG NO. 3-17-SBGP-105		
IL PROJ. NO. TAZ-4342		
CMT PROJECT NO: 13050-01-01		
CAD DWG FILE: TAZ-4342-CU501.DWG		
DESIGNED BY: PRR		
DRAWN BY: CMT		
CHECKED BY: RLV		
APPROVED BY: CBG		
COPYRIGHT: CRAWFORD, MURPHY & TILLY, INC. 2015		

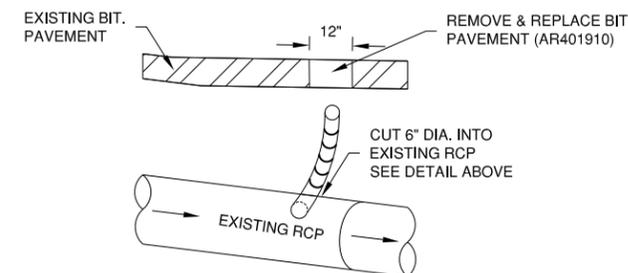
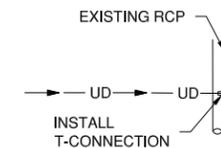
SHEET TITLE
DRAINAGE DETAILS

CU501
SHEET 14 OF 22

Path: K:\Taylorville\13050-01\Drawings\Sheet\RAFT\TAZ-4342-CU501.dwg
Date: Monday, January 16, 2017 7:32:22 PM

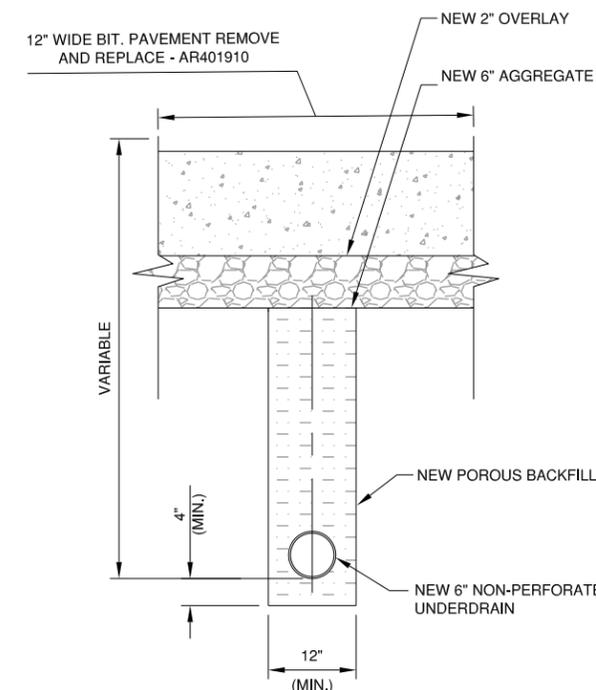


UNDERDRAIN DIRECT TOP CONNECTION DETAIL
N.T.S.



UNDERDRAIN DIRECT CONNECTION DETAIL
N.T.S.

NOTE:
DURING UNDERDRAIN CONSTRUCTION, CARE SHALL BE TAKEN TO ENSURE GOOD DRAINAGE BETWEEN THE SUB-BASE AND THE POROUS BACKFILL MATERIAL. ALL COSTS FOR THIS WORK SHALL BE CONSIDERED INCIDENTAL.

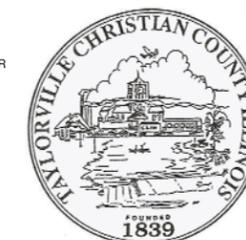


TYPICAL UNDERDRAIN DETAIL - BELOW PAVEMENT
N.T.S.

100% SUBMITTAL
JANUARY 6, 2017

BITUMINOUS OVERLAY/
RECONSTRUCTION OF NORTH
AND SOUTHWEST APRON

OWNER



TAYLORVILLE MUNICIPAL
AIRPORT

TAYLORVILLE, ILLINOIS

MARK	DATE	DESCRIPTION

SBG NO. 3-17-SBGP-105

IL PROJ. NO. TAZ-4342

CMT PROJECT NO: 13050-01-01

CAD DWG FILE: TAZ-4342-CU502.DWG

DESIGNED BY: PRR

DRAWN BY: CMT

CHECKED BY: RLV

APPROVED BY: CBG

COPYRIGHT: CRAWFORD, MURPHY & TILLY, INC. 2015

SHEET TITLE

**UNDERDRAIN
DETAILS**

CU502
SHEET 15 OF 22

100% SUBMITTAL
JANUARY 6, 2017

BITUMINOUS OVERLAY/
RECONSTRUCTION OF NORTH
AND SOUTHWEST APRON

OWNER



TAYLORVILLE MUNICIPAL
AIRPORT

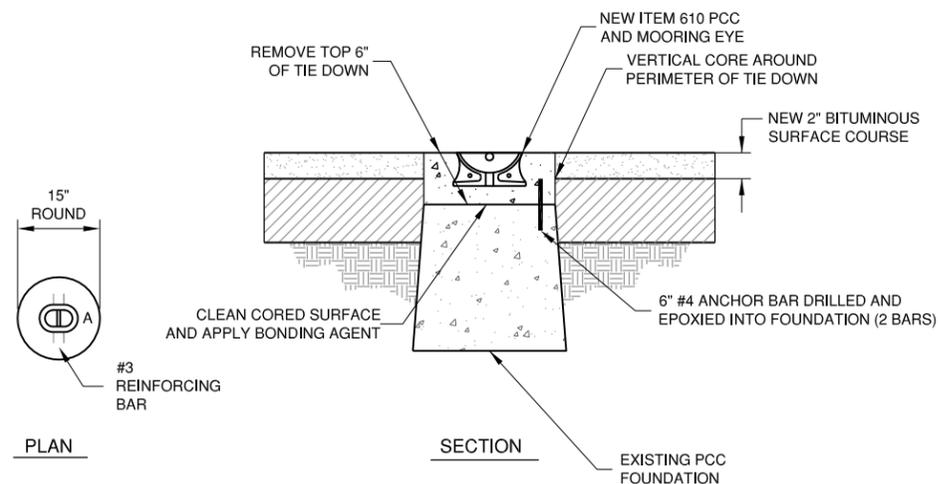
TAYLORVILLE, ILLINOIS

MARK	DATE	DESCRIPTION

SBG NO. 3-17-SBGP-105
IL PROJ. NO. TAZ-4342
CMT PROJECT NO: 13050-01-01
CAD DWG FILE: TAZ-4342-CM501.DWG
DESIGNED BY: PRR
DRAWN BY: CMT
CHECKED BY: RLV
APPROVED BY: CBG
COPYRIGHT: CRAWFORD, MURPHY & TILLY, INC. 2015

SHEET TITLE
**PAVEMENT MARKING
AND TIE DOWN
DETAILS**

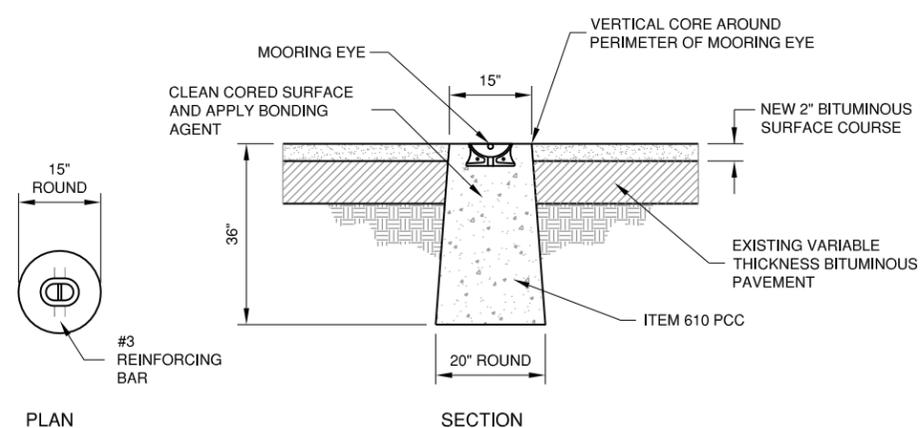
CM501
SHEET 20 OF 22



NOTE: THE MOORING EYE SHALL BE REMOVED PRIOR TO MILLING THE PAVEMENT AND INSTALLED AFTER PAVING OPERATIONS ARE COMPLETE.

TIE DOWN-TYPE A DETAIL

N.T.S.



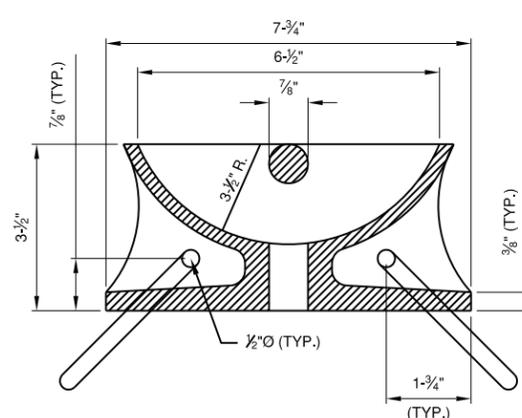
NOTE: THE MOORING EYE SHALL BE INSTALLED AFTER PAVING OPERATIONS ARE COMPLETE.

TIE DOWN DETAIL

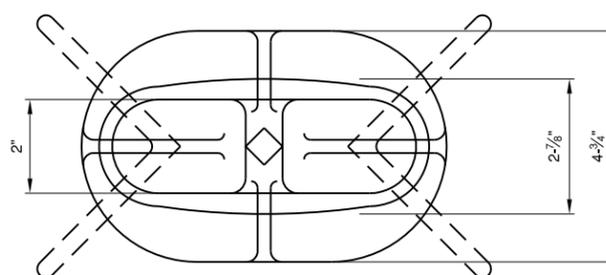
N.T.S.

MOORING NOTES

- MOORING CASTINGS SHALL BE NEENAH SEMI-STEEL AIRPORT MOORING EYE CATALOG NO. EJ5996 OR APPROVED EQUIVALENT.
- ANCHOR RODS SHALL BE #3 DEFORMED REBAR, 15" LONG AND SHALL BE BENT DOWNWARD AT 45 DEGREES.
- THE MOORING EYE SHALL BE INSTALLED AFTER PAVING OPERATIONS ARE COMPLETE BY CORING THROUGH NEW PAVEMENT.

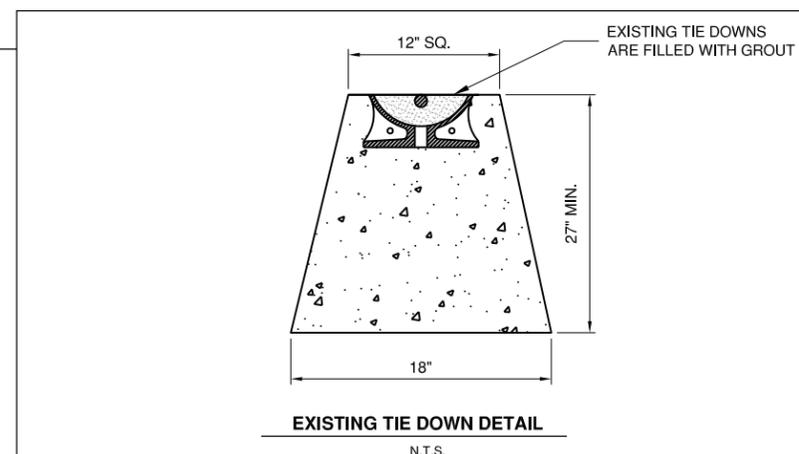


SECTION B-B



MOORING EYE DETAILS

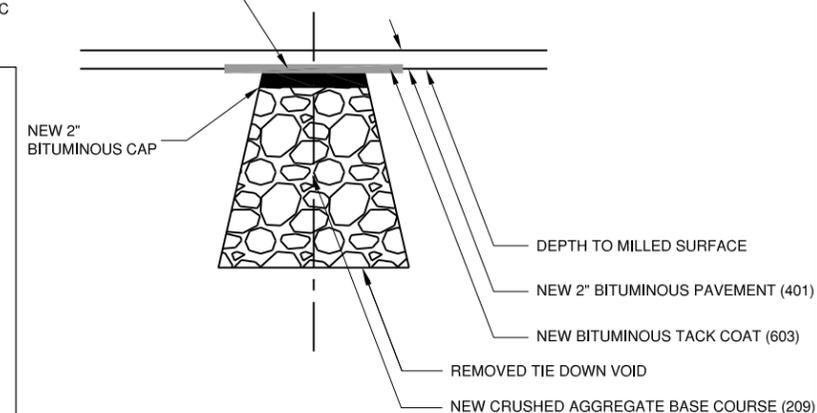
N.T.S.



EXISTING TIE DOWN DETAIL

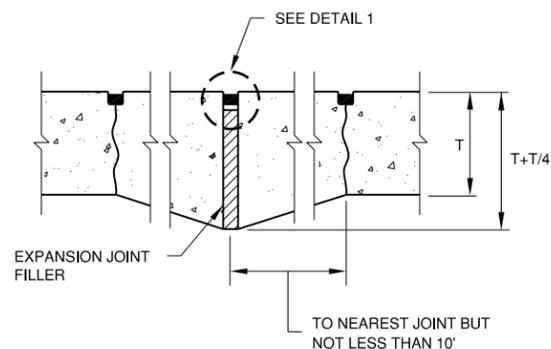
N.T.S.

NEW 4' x 4' SHEET CRACK CONTROL FABRIC (AR201670) OVER TIEDOWN REMOVALS - TO BE PAID FOR UNDER ITEM AR201670 CRACK CONTROL FABRIC



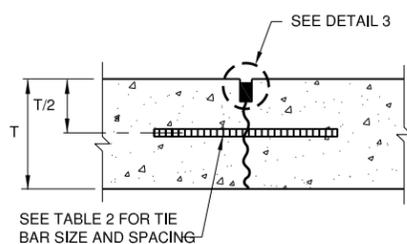
TIE DOWN BACKFILL DETAIL

N.T.S.



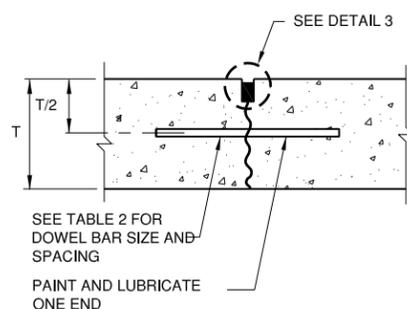
TYPE A THICKENED ISOLATION

SYMBOL — A — **T**



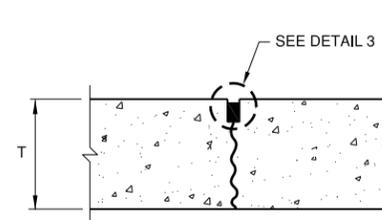
TYPE B HINGED (TIED) CONTRACTION

SYMBOL — B —



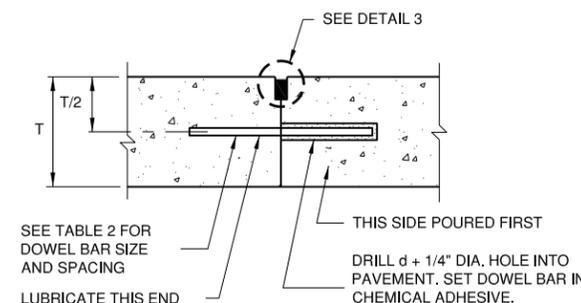
TYPE C DOWELED CONTRACTION

SYMBOL — C —



TYPE D DUMMY CONTRACTION

SYMBOL — D —



TYPE E DOWELED CONSTRUCTION

SYMBOL — E —

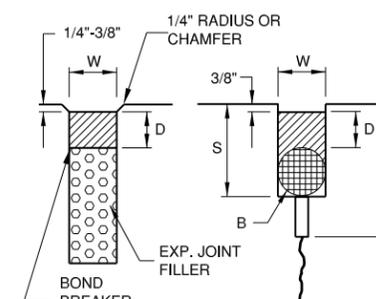
PAVEMENT THICKNESS T - INCHES	DEPTH OF CONTRACTION JOINT INITIAL SAW CUT I, INCHES $I=(T/3) \pm 1/4"$
5	1.67"
6	2.00"
7	2.33"
8	2.67"
9	3.00"
10	3.33"
11	3.67"
12	4.00"
13	4.33"
14	4.67"
15	5.00"
16	5.33"
17	5.67"
18	6.00"

PAVEMENT THICKNESS T - INCHES	DOWEL BAR DETAILS			TIE BAR DETAILS		
	DIA.	LENGTH	SPACING	BAR SIZE	LENGTH	SPACING
5	5/8"	12"	12"	#4	24"	30"
6	3/4"	18"	12"	#5	30"	30"
7	3/4"	18"	12"	#5	30"	30"
8	1"	19"	12"	#5	30"	30"
9	1"	19"	12"	#5	30"	30"
10	1"	19"	12"	#5	30"	30"
11	1"	19"	12"	#5	30"	30"
12	1"	19"	12"	#5	30"	30"
13	1 - 1/4"	20"	15"	#5	30"	30"
14	1 - 1/4"	20"	15"	#5	30"	30"
15	1 - 1/4"	20"	15"	#5	30"	30"
16	1 - 1/4"	20"	15"	#5	30"	30"
17	1 - 1/2"	20"	18"	#5	30"	30"
18	1 - 1/2"	20"	18"	#5	30"	30"

	DETAIL 1	DETAIL 2	DETAIL 3	DETAIL 4	DETAIL 5
	HOT/ COLD POUR	HOT/ COLD POUR	SILICONE	PRE FORMED	HOT/ COLD POUR
W=WIDTH OF SEALANT RESERVOIR (IN.)	3/4	1/2	1/2	3/8" COMPRESSED	3/16"
D=DEPTH OF SEALANT RESERVOIR (IN.)	3/4	1/2	1/4	N/A	I
B=BACKER ROD DIAMETER (IN.)	N/A	5/8	5/8	N/A	N/A
S=SECOND SAWCUT DEPTH (IN.) MINIMUM	N/A	1-3/8	1-3/8	1-1/2	N/A

JOINT NOTES

- ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
- THE INITIAL SAWCUT FOR ALL LONGITUDINAL & TRANSVERSE CONTRACTION JOINTS SHALL BE SAWED AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT.
- ALL TIE BARS & MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER APPROVED METHODS TO PREVENT SHIFTING DURING & AFTER CONCRETE PLACEMENT.
- TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH THE SPECIFICATIONS.
- THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSIONS OF THE SECOND SAWCUT WILL NOT BE ALLOWED.



DETAIL 1 DETAIL 3
JOINT SEALING DETAILS

100% SUBMITTAL
JANUARY 6, 2017

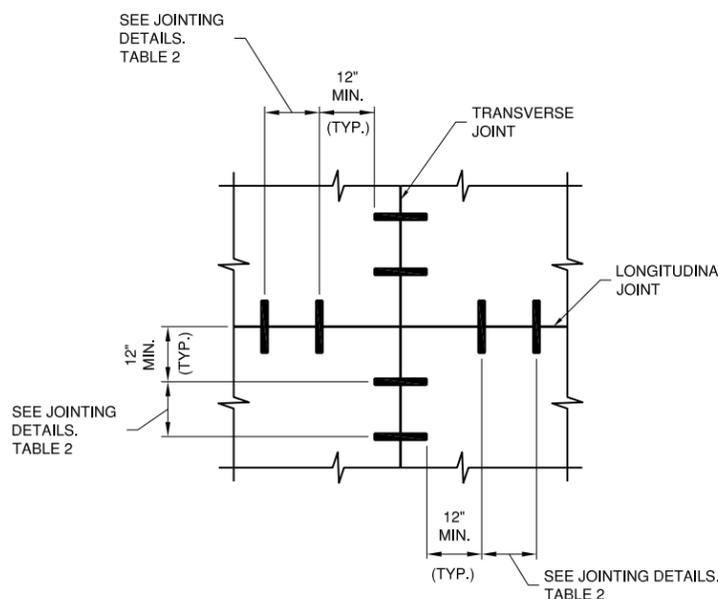
**BITUMINOUS OVERLAY/
RECONSTRUCTION OF NORTH
AND SOUTHWEST APRON**

OWNER

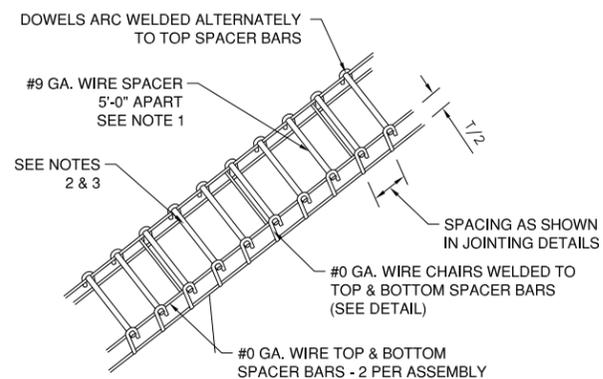


**TAYLORVILLE MUNICIPAL
AIRPORT**

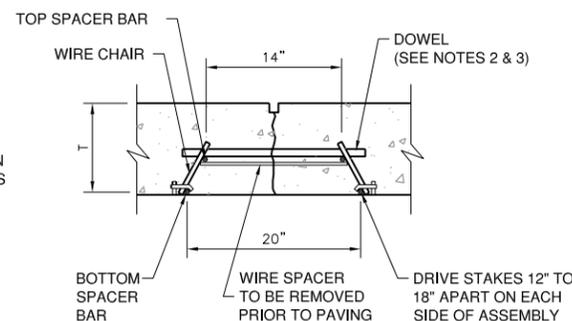
TAYLORVILLE, ILLINOIS



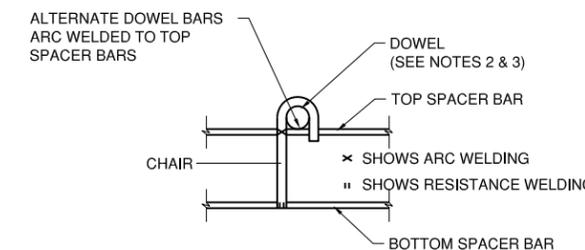
DOWEL PLACEMENT DETAIL
N.T.S.



DOWEL BASKET ASSEMBLY DETAIL
N.T.S.



DOWEL BAR INSTALLATION DETAIL
N.T.S.



**TYPICAL DOWEL BASKET ELEVATION
DETAIL SHOWING CHAIR**

DOWEL BASKET DETAILS
N.T.S.

DOWEL BASKET NOTES

- #9 GA. WIRE SPACER BAR ARC WELDED TO THE BOTTOM OF TOP SPACER BAR. (MAY BE MECHANICALLY ATTACHED IN LIEU OF WELDING) 3 REQUIRED PER UNIT. THIS WIRE MUST BE CUT OR REMOVED PRIOR TO PAVING.
- DOWEL BAR DIAMETER, LENGTH & SPACING SHALL BE AS SHOWN IN TABLE 2.
- DOWELS SHALL BE EPOXY COATED FULL LENGTH OF DOWEL. IMMEDIATELY PRIOR TO PAVING, THE FREE END OF EACH DOWEL SHALL BE LUBRICATED OR OILED, FOR HALF THE LENGTH OF THE DOWEL.

MARK | DATE | DESCRIPTION

SBG NO. 3-17-SBGP-105
IL PROJ. NO. TAZ-4342
CMT PROJECT NO: 13050-01-01
CAD DWG FILE: TAZ-4342-CJ501.DWG
DESIGNED BY: PRR
DRAWN BY: CMT
CHECKED BY: RLV
APPROVED BY: CBG
COPYRIGHT: CRAWFORD, MURPHY & TILLY, INC. 2015

JOINTING DETAILS

