

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS
808 09-00481-00-WR	CHAMPAIGN	ILLINOIS	193 2
STA.	TO STA.		
FED. ROAD DIST. NO. 5		FED. AID PROJECT	CONTRACT NO. 9506

HIGHWAY STANDARDS

- 000001-06 STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
- 001001-02 AREAS OF REINFORCEMENT BARS
- 001006 DECIMAL OF AN INCH AND OF A FOOT
- 280001-07 TEMPORARY EROSION CONTROL SYSTEMS
- 424001-07 PERPENDICULAR CURB RAMPS FOR SIDEWALKS
- 424026-01 ENTRANCE/ALLEY PEDESTRIAN CROSSINGS
- 542301-03 PRECAST REINFORCED CONCRETE FLARED END SECTIONS
- 601001-04 SUB-SURFACE DRAINS
- 601101-01 CONCRETE HEADWALL FOR PIPE DRAINS
- 602301-04 INLET - TYPE A
- 602306-03 INLET - TYPE B
- 602401-03 MANHOLE - TYPE A
- 602406-06 MANHOLE - TYPE A 6 FOOT DIAMETER
- 602411-04 MANHOLE - TYPE A 7 FOOT DIAMETER
- 602421-04 MANHOLE - TYPE A 9 FOOT DIAMETER
- 602601-03 PRECAST REINFORCED CONCRETE FLAT SLAB TOP
- 602701-02 MANHOLE STEPS
- 604001-03 FRAME AND LIDS - TYPE 1
- 604006-04 FRAME AND LIDS GRATE - TYPE 3
- 604011-04 FRAME AND LIDS GRATE - TYPE 3V
- 604036-02 GRATE - TYPE B
- 604061-02 FRAME & GRATE - TYPE 12
- 604091-02 FRAME & GRATE - TYPE 24
- 604101-01 MEDIAN INLET FOR 24" REINFORCED CONCRETE PIPE
- 606001-05 COMBINATION CONCRETE CURB AND GUTTER
- 606006-02 OUTLETS FOR CONCRETE CURB AND CUTTER - TYPE B6.24
- 606301-04 PC CONCRETE ISLANDS AND MEDIANS
- 606306-03 CORRUGATED PC MEDIANS
- 606401-01 PAVED DITCH
- 701006-05 OFF-ROAD OPERATIONS, 2L, 2W, 15' TO EOP-45 MPS ±
- 701011-04 OFF-ROAD MOVING OPERATIONS, 2L, 2W, DAY ONLY-45 MPH ±
- 701201-04 LANE CLOSURE, 2L, 2W, DAY ONLY
- 701206-03 LANE CLOSURE 2L 2W - NIGHT ONLY 45 MPH+
- 701301-04 LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
- 701306-03 LANE CLOSURE 2L, 2W, SLOW MOVING OPERATIONS DAY ONLY, FOR SPEEDS > 45 MPH
- 701326-04 LANE CLOSURE, 2L, 2W, PAVEMENT WIDENING FOR SPEEDS ≥ 45 MPH
- 701701-09 URBAN LANE CLOSURE, MULTILANE INTERSECTION
- 701901-03 TRAFFIC CONTROL DEVICES
- 704001-07 TEMPORARY CONCRETE BARRIER
- 720001-01 SIGN PANEL MOUNTING DETAILS
- 720006-04 SIGN PANEL ERECTION DETAILS
- 720016-03 MAST ARM MOUNTED STREET NAME SIGNS
- 728001-01 TELESCOPING STEEL SIGN SUPPORT
- 780001-04 TYPICAL PAVEMENT MARKINGS
- 805001-01 ELECTRICAL SERVICE INSTALLATION
- 814001-02 DETAILS HANDHOLES
- 857001-01 STANDARD PHASE DESIGNATION DIAGRAMS & PHASE SEQ.
- 862001-01 UNINTERRUPTABLE POWER SUPPLY
- 873001-02 TRAFFIC SIGNAL GROUND & BOND
- 876001-03 PEDESTRIAN PUSHBUTTON POST
- 877011-05 STEEL COMBINATION MAST ARM ASSEMBLY & POLE 16' THRU 55'
- 877012-02 STEEL COMBINATION MAST ARM ASSEMBLY & POLE 56' THRU 75'
- 878001-09 CONCRETE FOUNDATION DETAILS
- 880006-01 TRAFFIC SIGNAL MOUNT DETAILS - POST & BRACKET MOUNT
- 886001-01 DETECTOR LOOP INSTALLATIONS
- 886001-01 TYPICAL LAYOUTS FOR DETECTION LOOPS
- BLR 21-9 TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES FOR CONSTRUCTION ON RURAL LOCAL HIGHWAYS

GENERAL NOTES



1. CONSTRUCTION METHODS AND MATERIALS FOR THE OFF-SITE ROAD CONSTRUCTION AND ASSOCIATED IMPROVEMENTS SHALL COMPLY WITH THE IDOT PERMIT AND THE JANUARY 1, 2012 EDITION OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION'S "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", "HIGHWAY STANDARDS", THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES", AND THE "SPECIAL PROVISIONS" INCLUDED IN THE BID DOCUMENTS.
2. EXISTING STATE-OWNED AND MAINTAINED UTILITY LINES ARE SHOWN ON THE PLANS TO INDICATE THEIR PRESENCE AND APPROXIMATE LOCATION. THE CONTRACTOR SHALL NOTIFY THE DISTRICT OPERATIONS ENGINEER TWO WEEKS PRIOR TO COMMENCING ANY EXCAVATION IN THE VICINITY OF THESE LINES. THE STATE WILL THEN LOCATE AND MARK THE HORIZONTAL LOCATIONS OF THE LINES AND PROVIDE ANY AVAILABLE INFORMATION AS TO THEIR DEPTH. SHOULD ANY OF THE LINES BE DAMAGED BY THE CONTRACTOR'S OPERATION, THE CONTRACTOR SHALL REPAIR THEM TO THE SATISFACTION OF THE ENGINEER AND AT NO COST TO THE STATE.

ALSO THERE MAY BE UTILITIES PRESENT WHICH WERE INSTALLED BY THE STATE BUT ARE MAINTAINED BY OTHERS (CITY, TOWN, ETC.) THE APPROXIMATE LOCATIONS OF THESE LINES ARE ALSO SHOWN ON THE PLANS ALONG WITH THE NAME OF THE MAINTAINING AGENCY. THE CONTRACTOR SHALL COORDINATE THE LOCATING OF THESE LINES WITH THE LOCAL AGENCY PRIOR TO COMMENCING ANY EXCAVATION OR BORING IN THEIR VICINITY. SHOULD THESE LINES BE DAMAGED BY THE CONTRACTOR'S OPERATIONS, THE CONTRACTOR SHALL REPAIR THEM TO THE SATISFACTION OF, AND AT NO COST TO, THE LOCAL AGENCY AND THE STATE.
3. UTILITY LINES WERE PLOTTED FROM INFORMATION FURNISHED BY THE VARIOUS UTILITY COMPANIES INVOLVED AND THE ACCURACY SHOULD BE CONSIDERED APPROXIMATE ONLY.

UTILITY COMPANIES MAY BE ADJUSTING THEIR FACILITIES DURING CONSTRUCTION. THE CONTRACTOR SHALL COOPERATE WITH THESE ORGANIZATIONS WHILE THESE ADJUSTMENTS ARE BEING PERFORMED.
J.U.L.I.E. - JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS SYSTEM (800)892-0123.
4. ALL ELEVATIONS SHOWN IN THE PLANS ARE BASED ON NORTH AMERICAN VERTICAL DATUM OF 1988. (NAVD 88)
5. FOR INLETS AND MANHOLES LOCATED IN THE CURB; STATIONS, OFFSETS AND RIM ELEVATIONS ARE PROVIDED AT THE EDGE OF PAVEMENT. ALL OTHER INLET AND MANHOLE STATIONS, OFFSETS AND RIM ELEVATIONS ARE GIVEN TO THE CENTER OF THE STRUCTURE. PIPE END SECTION STATIONS, OFFSETS AND INVERTS ARE PROVIDED AT THE END OF THE FLARED END SECTION.
6. THE TOP FOUR INCHES OF ALL DISTURBED AREAS TO BE LANDSCAPED SHALL MEET THE REQUIREMENTS OF ARTICLE 1081.05(c) OF THE IDOT STANDARD SPECIFICATION FOR TOPSOIL.
7. COARSE AGGREGATE GRADATION CA-10 MAY BE USED WHENEVER COARSE AGGREGATE CA-6 IS SPECIFIED IN THE STANDARD SPECIFICATIONS.
8. ALL REMOVAL LIMITS OF PAVEMENT, CURBS, DRIVEWAYS, ETC. SHALL BE SAWCUT FULL-DEPTH.
9. FOR SEPARATION REQUIREMENTS BETWEEN PROPOSED STORM SEWER AND PROPOSED WATER MAIN LOCATIONS, REFER TO SECTION 41-2.01 OF THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS. GASKETED JOINTS FOR STORM SEWERS MAY BE REQUIRED. COORDINATE STORM SEWER INSTALLATION WITH WATER MAIN INSTALLATION.
10. THE EXISTING ROAD SIGNS THAT INTERFERE WITH CONSTRUCTION WILL BE REMOVED OR RELOCATED AS DIRECTED BY THE ENGINEER ACCORDING TO ARTICLE 107.25 OF THE STANDARD SPECIFICATIONS. AFTER THE CONSTRUCTION IS COMPLETED, THE CONTRACTOR WILL RE-ERECT THE SIGNS AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO COMPENSATION WILL BE ALLOWED.
11. ITEMS TO BE REMOVED, INCLUDING BUT NOT LIMITED TO VEGETATION, TREES, STRUCTURES, PAVEMENT, CURBS AND GUTTERS, DRAINAGE STRUCTURES, UTILITIES, AND SIGNAGE ARE DESIGNATED ON THE PLAN DRAWINGS.
12. TREES THAT INTERFERE WITH THE CONSTRUCTION OPERATIONS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER. ANY TREE DUE TO ITS LOCATION AND DEEMED SUITABLE FOR SAVING BY THE ENGINEER SHALL BE PROTECTED DURING CLEARING AND SUBSEQUENT CONSTRUCTION OPERATIONS.
13. GRADING SHALL BE DONE BY HAND AROUND LIGHT POLES, UTILITY POLES, SIGN POSTS, SHRUBS, TREES OR OTHER NATURAL OR MAN-MADE OBJECTS WHERE SHALLOW FILLS OR CUTS ARE ADJACENT TO THE ITEMS. IT IS THE INTENT THAT THE LIMITS OF CONSTRUCTION BE SUCH AS TO PRESERVE IN THE ORIGINAL STATE AS MUCH AREA OF TEMPORARY EASEMENTS AS POSSIBLE. THE DECISION AS TO ITEMS TO REMAIN IN PLACE SHALL BE AS DIRECTED BY THE ENGINEER.

THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE CONTRACT UNIT PRICE PER CUBIC YARD FOR EARTH EXCAVATION AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
14. TEMPORARY EROSION CONTROL SEEDING IS INCLUDED IN THIS CONTRACT TO SEED NEW EARTH SLOPES DURING TIME PERIODS WHEN PERMANENT SEEDING IS NOT ALLOWED. SOME OR ALL OF THE TEMPORARY EROSION CONTROL SEEDING WILL BE DELETED IF IT IS POSSIBLE TO PLACE PERMANENT SEEDING ON EARTH SLOPES AT THE TIME OF THEIR COMPLETION.
15. THE CONTRACTOR SHALL NOTIFY THE URBANA & CHAMPAIGN SANITARY DISTRICT AT LEAST 48 HOURS IN ADVANCE OF ANY CONNECTION TO, OR WORK ON, AN URBAN & CHAMPAIGN SANITARY DISTRICT COLLECTOR SEWER OR INTERCEPTOR SEWER, AND WILL ONLY PERFORM THAT WORK IN THE PRESENCE OF AN URBANA & CHAMPAIGN SANITARY DISTRICT REPRESENTATIVE.

16. THE EXISTING TIE BARS BETWEEN THE EXISTING PAVEMENT AND EXISTING MEDIANS, GUTTERS AND/OR COMBINATION CURB AND GUTTERS THAT ARE FOUND SUITABLE FOR REUSE SHALL BE CLEANED, STRAIGHTENED AND INCORPORATED INTO THE NEW CONSTRUCTION. ANY EXISTING TIE BARS THAT ARE FOUND UNSUITABLE TO BE INCORPORATED INTO THE PROPOSED CONSTRUCTION DUE TO EXCESSIVE RUSTING OR DISTRESS SHALL BE REMOVED FLUSH WITH THE FACE OF THE EXISTING CONCRETE AND DISPOSED OF OUTSIDE THE LIMITS OF THE RIGHT-OF-WAY IN ACCORDANCE WITH ARTICLE 202.03 OF THE STANDARD SPECIFICATIONS.

THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCLUDED IN THE VARIOUS REMOVAL PAY ITEMS AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
13. BEFORE ORDERING PIPE CULVERTS AND/OR STORM SEWERS, THE CONTRACTOR SHALL CONSULT THE ENGINEER FOR THE EXACT LENGTHS.
14. THE BITUMINOUS PLUG SHOWN ON STANDARD 601001 FOR SHOULDER REMOVAL AND REPLACEMENT SHALL BE CONSTRUCTED OF A BITUMINOUS MIXTURE OF 75% CA-11 AND 25% CA-16 WITH 2.8 +/- 0.2% OF ASPHALT CEMENT. THE ASPHALT CEMENT SHALL BE PG 64-22.
15. CALCIUM CHLORIDE SHALL BE APPLIED FULL WIDTH TO THE PROPOSED AGGREGATE ROADWAYS AND/OR DETOURS FOR THE SOLE PURPOSE OF CONTROLLING DUST. THIS WORK SHALL BE PERFORMED ON ALL FINAL SURFACES AS WELL AS ANY REMAINING AGGREGATE SURFACES THAT WOULD LAY OVER ANY PARTICULAR WEEKEND.

THE FOLLOWING APPLICATION RATE HAS BEEN USED TO CALCULATE THIS ESTIMATED QUANTITY: 5 LBS./SQ.YD.

ANY ADDITIONAL QUANTITY FOR MAINTENANCE WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 WHEN SPECIFIED BY THE ENGINEER.
16. THE RESIDENT ENGINEER SHALL CONTACT THE PROGRAM DEVELOPMENT CHIEF OF SURVEYS PRIOR TO THE PRE-CONSTRUCTION CONFERENCE FOR INSTRUCTION AS TO SETTING OF TEMPORARY OR PERMANENT TIES FOR CENTERLINE ALIGNMENT CONTROL SURVEY MARKERS (PC'S, PT'S, AND PI'S). PROJECT IMPLEMENTATION PERSONNEL WILL BE RESPONSIBLE FOR LAYOUT OF THESE MARKERS.
17. SHORT TERM PAVEMENT MARKING SHALL BE APPLIED TO THE PAVEMENT AFTER ANY OF THE FOLLOWING: COLD MILLING AND/OR PLACING BITUMINOUS MATERIALS (PRIME COAT), LEVELING BINDER (MACHINE METHOD), BINDER AND SURFACE COURSES. SHORT TERM PAVEMENT MARKING PLACED ON THE SURFACE, SHALL COINCIDE WITH THE FINAL PAVEMENT STRIPING. SHORT TERM PAVEMENT MARKING PLACED PRIOR TO THE SURFACE SHALL COINCIDE WITH THE EXISTING PAVEMENT MARKINGS.
18. RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH STANDARD 781001, AND THE DETAILS SHOWN IN THE PLANS. IF THERE IS ANY DISCREPANCY BETWEEN THE STANDARD AND THE DETAILS IN THE PLANS, THE DETAILS IN THE PLANS SHALL GOVERN. THE FINAL PAVEMENT MARKINGS SHALL BE IN PLACE PRIOR TO PLACING THE RAISED REFLECTIVE PAVEMENT MARKERS AND THE RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED MIDWAY IN THE 30 FOOT (9 m) SPACE BETWEEN THE DASHED CENTERLINE STRIPES (WHEN APPLICABLE).
19. FACTORS USED FOR ESTIMATING PLAN QUANTITIES ARE AS FOLLOWS AND SHALL NOT BE USED FOR THE BASIS OF FINAL QUANTITIES:

ALL HOT MIX ASPHALT	2.016 TONS/CU. YD.
ALL AGGREGATE	2.05 TONS/CU. YD.
BITUMINOUS MATERIALS:	
ON PAVEMENT	0.09 GAL./SQ. YD.
INTERMEDIATE LIFTS (FOG COAT)	
ON AGGREGATE SURFACE	0.04 GAL./SQ. YD.
AGGREGATE (PRIME COAT)	0.0015 TONS/ SQ. YD.
RIPRAP	1.50 TONS/CU. YD.

△ Rev. 2 - 12 - 14

REVISIONS	
NAME	DATE

GENERAL NOTES AND HIGHWAY STANDARDS IL. RTE 130

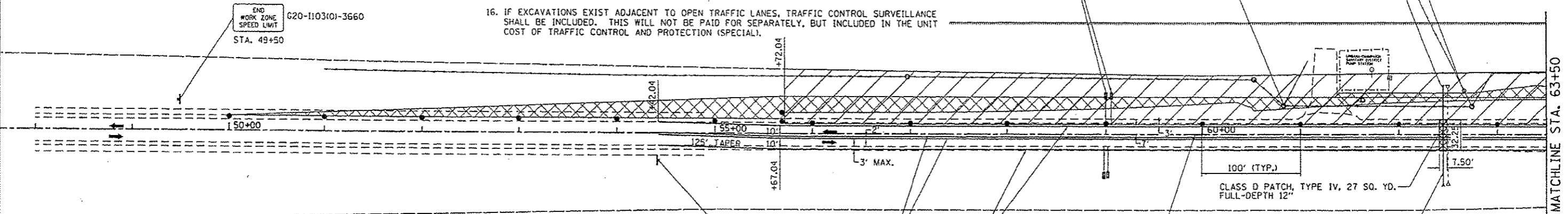
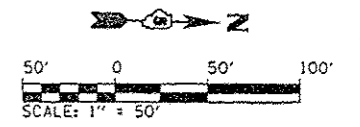
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MAINTENANCE OF TRAFFIC GENERAL NOTES

1. THE CONTRACTOR SHALL COORDINATE MAINTENANCE OF TRAFFIC OF THIS PROJECT WITH OTHER PROJECTS IN ADJACENT SECTIONS. SEE TRAFFIC CONTROL SPECIAL PROVISIONS FOR COORDINATION REQUIREMENTS.
2. ALL ADVANCE "ROAD WORK" SIGNS, W20-1 SERIES, AS SHOWN ON THE PLANS, REFERENCED IN THE STANDARDS OR DIRECTED BY THE ENGINEER, SHALL BE EQUIPPED WITH MONODIRECTIONAL FLASHING LIGHT. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST FOR TRAFFIC CONTROL AND PROTECTION, (SPECIAL).
3. EXISTING PAVEMENT MARKINGS IN CONFLICT WITH MAINTENANCE OF TRAFFIC STRIPING SHALL BE REMOVED OR MASKED USING A REMOVABLE BLACK LINE TAPE.
4. ALL DRUMS, VERTICAL PANELS AND BARRICADES PLACED IMMEDIATELY ADJACENT TO THE EDGE OF TRAVELED WAY SHALL BE EQUIPPED WITH STEADY BURN MONODIRECTIONAL LAMPS.
5. ROUGH GROOVED SURFACE AND FRESH OIL SIGNS ARE REQUIRED WHERE APPLICABLE. THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST FOR TRAFFIC CONTROL AND PROTECTION, (SPECIAL).
6. REMOVAL OF TEMPORARY CONCRETE BARRIER SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT COST PER FOOT FOR TEMPORARY CONCRETE BARRIER.
7. EXCAVATION, INCLUDING SHOULDER BASE COURSE, FOR CONSTRUCTION OF TEMPORARY PAVEMENT SHALL BE PAID AS EARTH EXCAVATION REGARDLESS OF SOIL TYPE OR SUITABILITY.
8. EMBANKMENT MATERIAL FOR TEMPORARY PAVEMENT WIDENING TO ACCOMMODATE A MAX. 1:2 SLOPE SHALL BE INCLUDED IN THE COST FOR EARTH EXCAVATION.
9. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES SUCH THAT WORK WILL ONLY BE CONDUCTED ON ONE SIDE OF IL ROUTE 130 AT ANY TIME.
10. THE CONTRACTOR SHALL RELOCATE OR COVER ALL EXISTING, TEMPORARY, AND PROPOSED SIGNS THAT CONFLICT WITH THE CURRENT CONSTRUCTION STAGE. SIGNS THAT DO NOT CONFLICT WITH THE CURRENT CONSTRUCTION STAGE AND ARE NOT MARKED FOR RELOCATION SHALL REMAIN AS PREVIOUSLY CONFIGURED. THIS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT COST OF TRAFFIC CONTROL AND PROTECTION, (SPECIAL).
11. AT LOCATIONS WHERE FULL DEPTH TEMPORARY PAVEMENT IS TO BE PLACED IN AN EXISTING MEDIAN, ISLAND OR SHOULDER, THE REMAINING GORE OR SHOULDER SHALL BE LEFT INTACT FOLLOWING CONSTRUCTION OF TEMPORARY PAVEMENT.
12. TEMPORARY RAMPS SHALL BE 40:1 MINIMUM.
13. WHEN PAVEMENT DROP-OFF IS TO REMAIN ADJACENT TO A LIVE TRAFFIC LANE, BARRICADES WITH PIPE EXTENSIONS SHALL BE USED TO ACHIEVE THE CORRECT ELEVATION.
14. ALL COSTS FOR FURNISHING, ERECTING, RELOCATING, MAINTAINING, AND REMOVING ALL TRAFFIC CONTROL ITEMS AS SHOWN IN THE MAINTENANCE OF TRAFFIC PLANS AND IN THE TRAFFIC CONTROL STANDARDS LISTED IN THE TRAFFIC CONTROL SPECIAL PROVISION, SHALL BE INCLUDED IN THE COST FOR TRAFFIC CONTROL AND PROTECTION, (SPECIAL). LUMP SUM ITEMS SHALL INCLUDE SIGNS, DRUMS, BARRICADES, AND ALL OTHER EQUIPMENT, HARDWARE, AND LABOR NECESSARY TO MAINTAIN THE LANE SHIFTS AND/OR CLOSURES. ITEMS SUCH AS TEMPORARY PAVEMENT, TEMPORARY CONCRETE BARRIER, TEMPORARY PAVEMENT MARKING, REMOVAL OF PAVEMENT MARKINGS, AND IMPACT ATTENUATORS, WILL BE PAID FOR SEPARATELY.
15. THE CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE AS REQUIRED OR AS DIRECTED BY THE ENGINEER THROUGHOUT THE CONSTRUCTION ZONE FOR THE PERIOD OF THE CONSTRUCTION. THIS WORK SHALL BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION, (SPECIAL). IN ADDITION TO THE PROPOSED TEMPORARY DRAINAGE STRUCTURES SHOWN ON THE PLANS, THE EXISTING DRAINAGE SYSTEM MAY ALSO BE USED THROUGHOUT CONSTRUCTION STAGING.
16. IF EXCAVATIONS EXIST ADJACENT TO OPEN TRAFFIC LANES, TRAFFIC CONTROL SURVEILLANCE SHALL BE INCLUDED. THIS WILL NOT BE PAID FOR SEPARATELY, BUT INCLUDED IN THE UNIT COST OF TRAFFIC CONTROL AND PROTECTION (SPECIAL).

P.A.P. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
B08	09-00481-00-WR	CHAMPAIGN	193	68
STA. 48+50		TO STA. 63+50		
FED. ROAD DIST. NO. 5 (ILLINOIS)		FED. AID PROJECT		CONTRACT NO. 91506



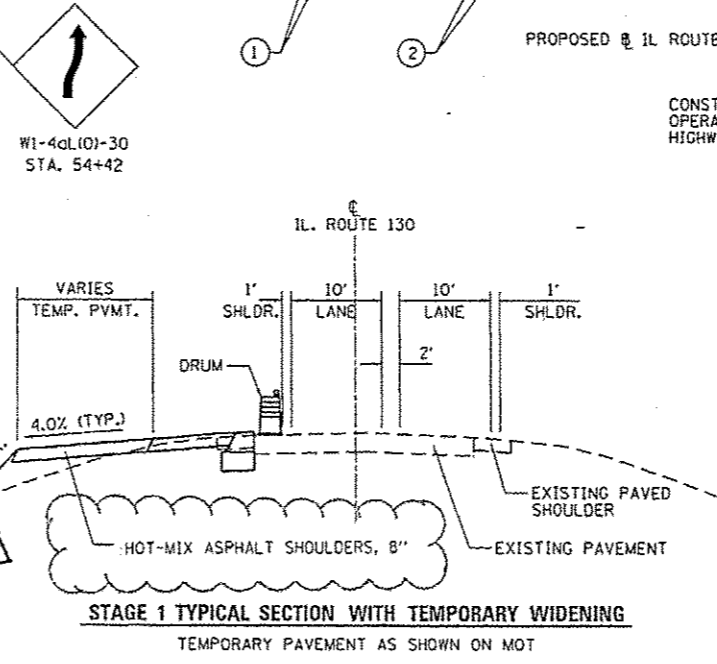
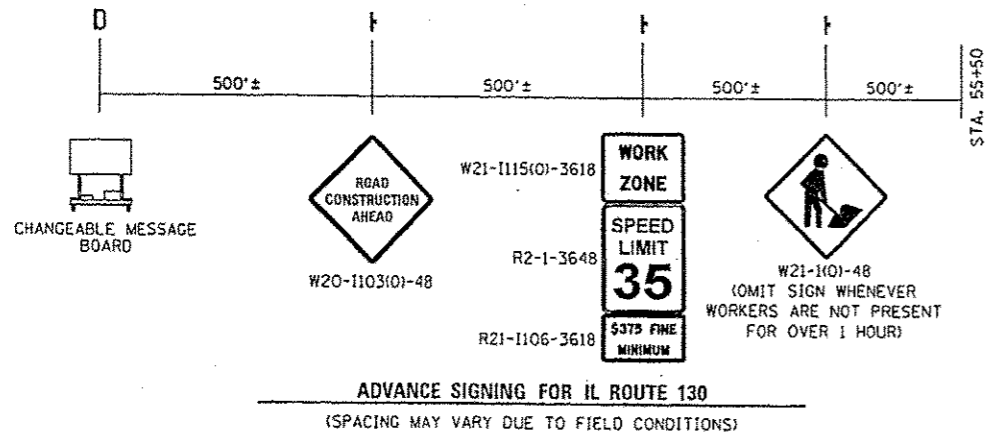
CONSTRUCT MANHOLE WITH RIM ELEVATION MATCHING TEMPORARY PAVEMENT IN TEMPORARY CONDITION. MANHOLE RIM WILL BE ADJUSTED TO FINAL CONDITION AFTER TEMPORARY PAVEMENT HAS BEEN REMOVED.

REPLACE PROPOSED FLARED-END SECTION WITH PIPE CULVERT, CLASS A, TYPE 2, 24", 6 L.F. IN TEMPORARY CONDITION. REMOVE TEMPORARY PIPE CULVERT AFTER TEMPORARY PAVEMENT HAS BEEN REMOVED AND REPLACE WITH PROPOSED FLARED-END SECTION.

CONSTRUCT MANHOLE WITH RIM ELEVATION MATCHING TEMPORARY PAVEMENT IN TEMPORARY CONDITION. MANHOLE RIM WILL BE ADJUSTED TO FINAL CONDITION AFTER TEMPORARY PAVEMENT HAS BEEN REMOVED.

REMOVE EXISTING UNDERDRAIN OUTLET STRUCTURES AND EXTEND WITH PIPE UNDERDRAIN, 4", 10 L.F. (EACH) IN TEMPORARY CONDITION. AFTER TEMPORARY PAVEMENT HAS BEEN REMOVED AT THE COMPLETION OF STAGE 2, REMOVE TEMPORARY UNDERDRAIN AND REINSTALL EXISTING PIPE UNDERDRAIN OUTLET STRUCTURES.

CONSTRUCT PERMANENT PIPE CULVERT DURING PRE-STAGE 1 OPERATIONS USING APPROPRIATE IDOT LANE CLOSURE HIGHWAY STANDARD.



STAGE I NOTES

1. THE CONTRACTOR SHALL CONSTRUCT PROPOSED PAVEMENT WIDENING, DRAINAGE ITEMS, CURB AND GUTTER AND HMA PAVING WITHIN THE WORK ZONE.
2. PAVEMENT WIDENING SHALL BE CONSTRUCTED UTILIZING IDOT HIGHWAY STANDARD 701326.
3. THE CONTRACTOR SHALL INSTALL ALL NECESSARY PAVEMENT MARKING AND TRAFFIC CONTROL SIGNAGE FOR SHIFTING THE TRAFFIC FOR STAGE 2 CONSTRUCTION.
4. ACCESS TO ALL ADJACENT PROPERTIES SHALL BE MAINTAINED AT ALL TIMES, UNLESS DIRECTED BY THE ENGINEER.

Rev. 2-12-14

LEGEND

- WORK ZONE
- TEMPORARY PAVEMENT - FULL DEPTH
- IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 2
- TEMPORARY CONCRETE BARRIER WALL (32 INCH HEIGHT)
- TYPE II BARRICADE, DRUM OR VERTICAL BARRICADE WITH STEADY BURNING MONODIRECTIONAL LIGHT
- PORTABLE CHANGEABLE MESSAGE BOARD MESSAGE: "CAUTION: ROAD WORK AHEAD" OR AS DIRECTED BY IDOT
- TYPE III BARRICADE W/FLASHING LIGHTS
- TEMPORARY CONSTRUCTION/TRAFFIC CONTROL SIGN
- DIRECTION OF TRAFFIC FLOW

TEMPORARY PAVEMENT MARKINGS

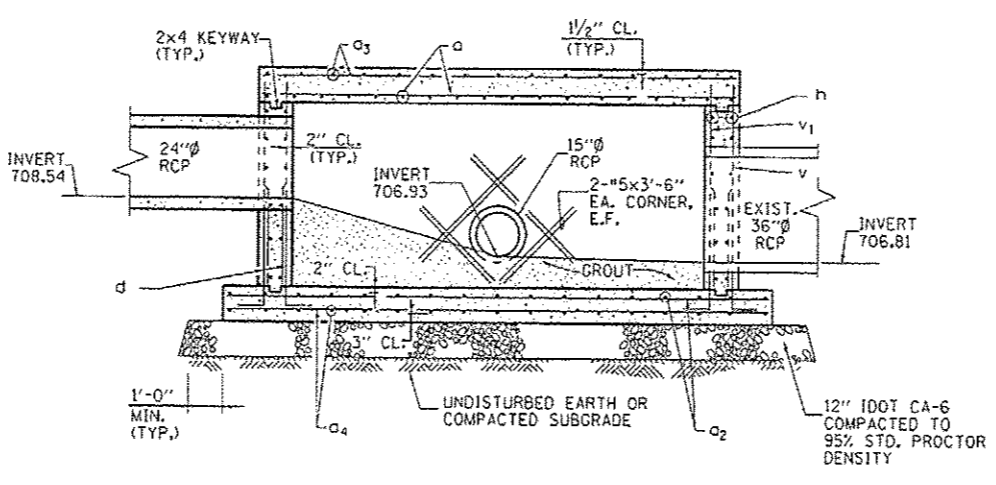
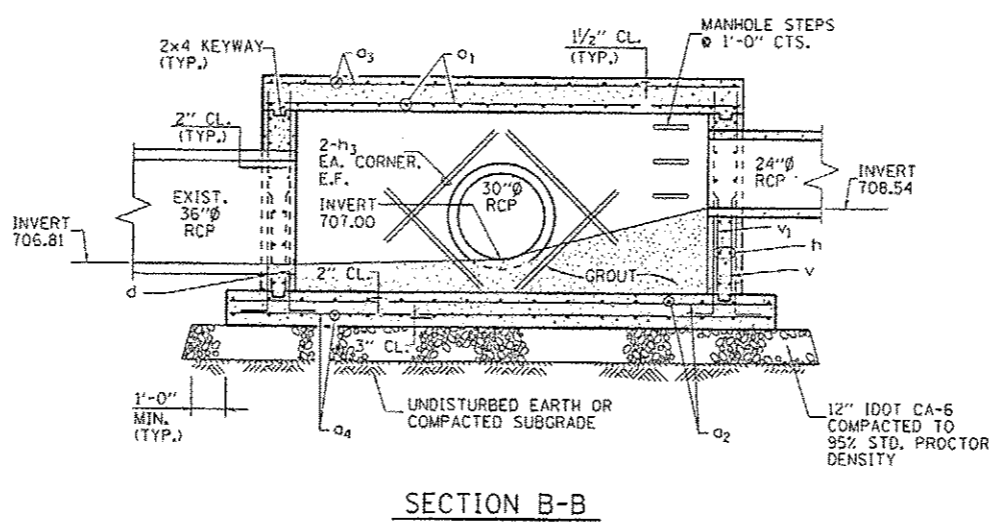
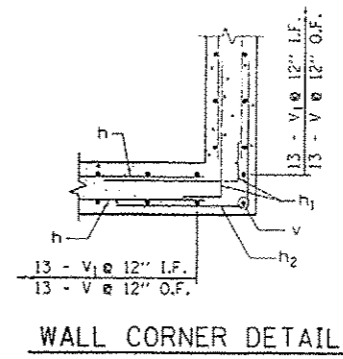
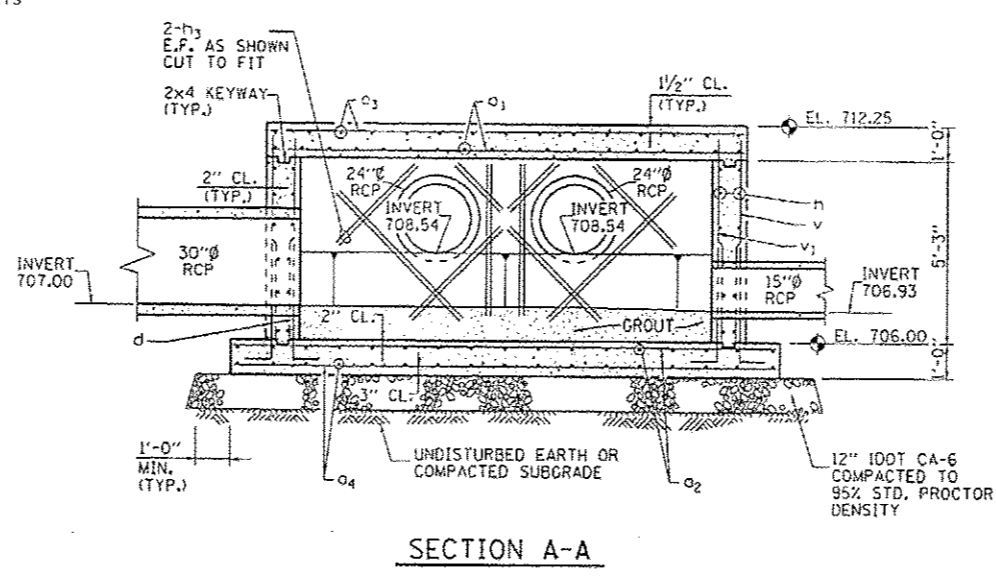
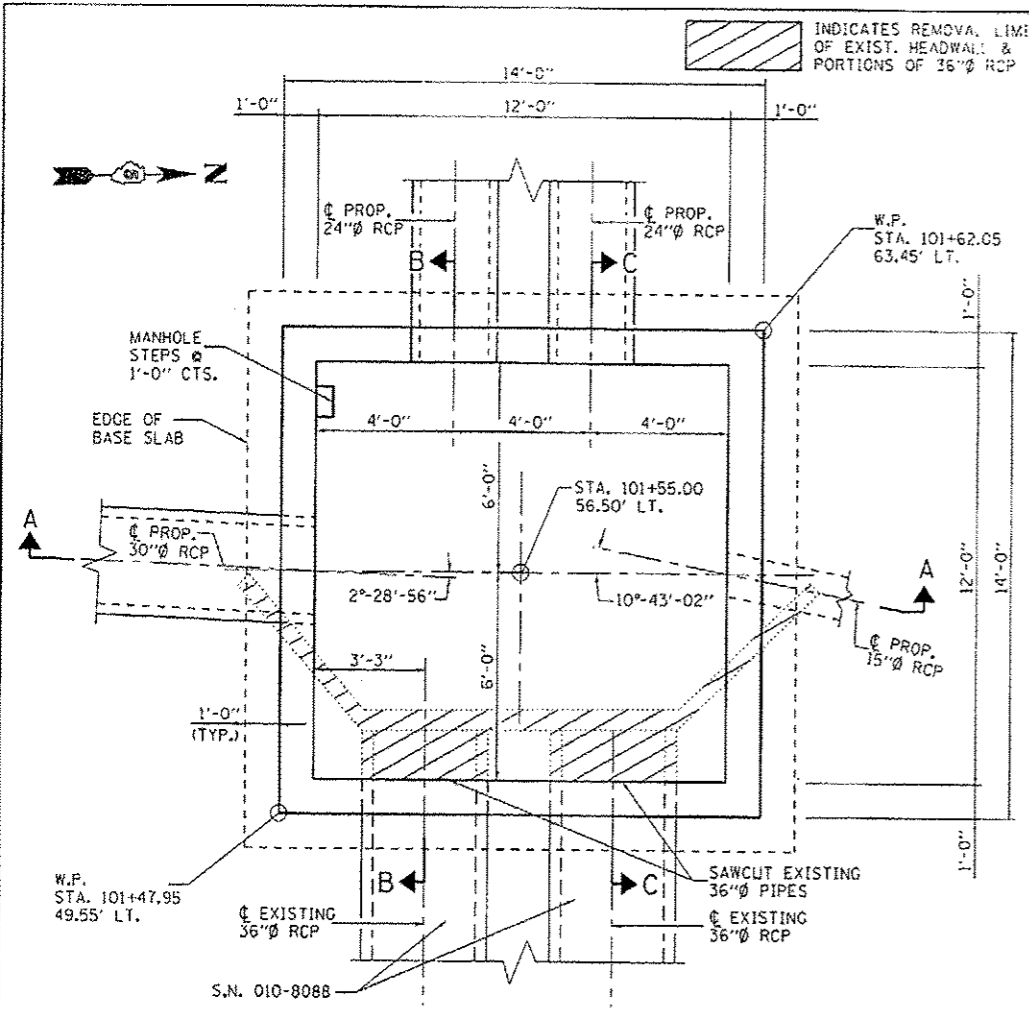
- ① 4" YELLOW (SOLID)
- ② 4" WHITE (SOLID)
- ③ 4" WHITE (SKIP-DASH) 25' LINE - 25' SPACE
- ④ 4" WHITE (DOT-DASH) 2' LINE - 6' SPACE
- ⑤ 12" WHITE (SOLID)
- ⑥ 24" WHITE (SOLID)
- ⑦ TEMPORARY PAVEMENT MARKING LETTERS & SYMBOLS (WHITE)

REVISIONS	
NAME	DATE

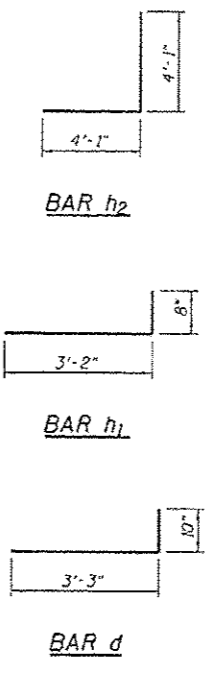
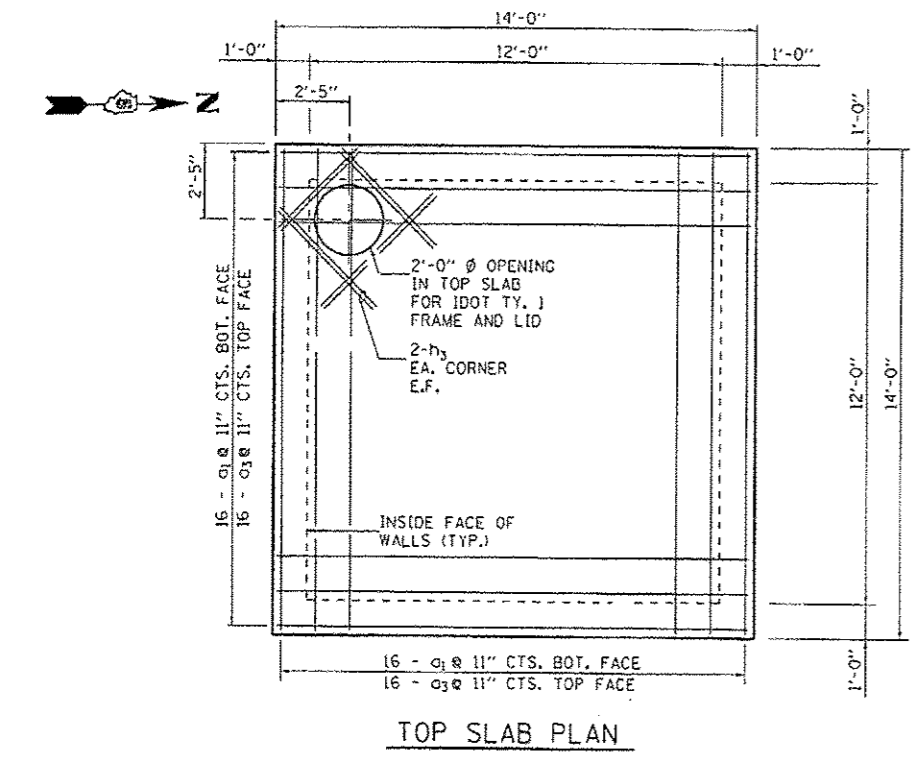
MAINTENANCE OF TRAFFIC STAGE 1
IL. ROUTE 130

SCALE: 1" = 50'
DATE: 12/09/13
DRAWN BY: BMB
CHECKED BY: KLT

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* PROVIDE #4 CORNER BARS 3'-0" x 3'-0" OUTSIDE CORNERS (TYP.)



BILL OF MATERIAL

BAR LIST				
Bar	No.	Size	Length	Shape
a ₁	30	#6	13'-9"	—
a ₂	34	#6	15'-6"	—
a ₃	30	#4	13'-9"	—
a ₄	34	#4	15'-6"	—
d	100	#5	4'-1"	—
h	48	#4	12'-0"	—
h ₁	48	#4	3'-10"	—
h ₂	24	#4	8'-2"	—
h ₃	120	#5	4'-0"	—
v	56	#5	5'-1"	—
v ₁	52	#5	5'-9"	—
TOTAL BILL OF MATERIAL				
Item	Unit	Total		
Concrete Structures	Cu. Yd.	26.9*		
Reinforcement Bars, Epoxy Coated	Pound	4215*		
Granular Culvert Backfill	Cu. Yd.	7.5		

* QUANTITIES SHOWN FOR INFORMATION ONLY, CONCRETE STRUCTURES AND REINFORCEMENT BARS SHALL BE INCLUDED IN LUMP SUM UNIT COST FOR JUNCTION BOX.

NOTES:

- ALL CONCRETE SHALL BE CLASS S1 AND IN ACCORDANCE WITH SECTION 503 OF THE IDOT STANDARD SPECIFICATIONS.
- ALL REINFORCEMENT BARS SHALL BE AASHTO M-31 OR M322, GRADE 60 AND IN ACCORDANCE WITH SECTION 508 OF THE IDOT STANDARD SPECIFICATIONS.
- MANHOLE STEPS SHALL BE ACCORDING TO STANDARD 602701.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR DIVERTING THE WATER FLOW FROM THE CONSTRUCTION AREA USING A METHOD MEETING THE APPROVAL OF THE ENGINEER.
- STRUCTURE EXCAVATION SHALL BE PAID FOR AS EARTH EXCAVATION.

Rev. 2-12-14

REVISIONS	
NAME	DATE

JUNCTION BOX
"5-33"
STA. 101+55, LT.

DESIGN STRESSES

f'c = 3,500 psi
fy = 60,000 psi

SCALE: AS SHOWN
DATE: 12/09/13

DRAWN BY: GLD
CHECKED BY: WLB