

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

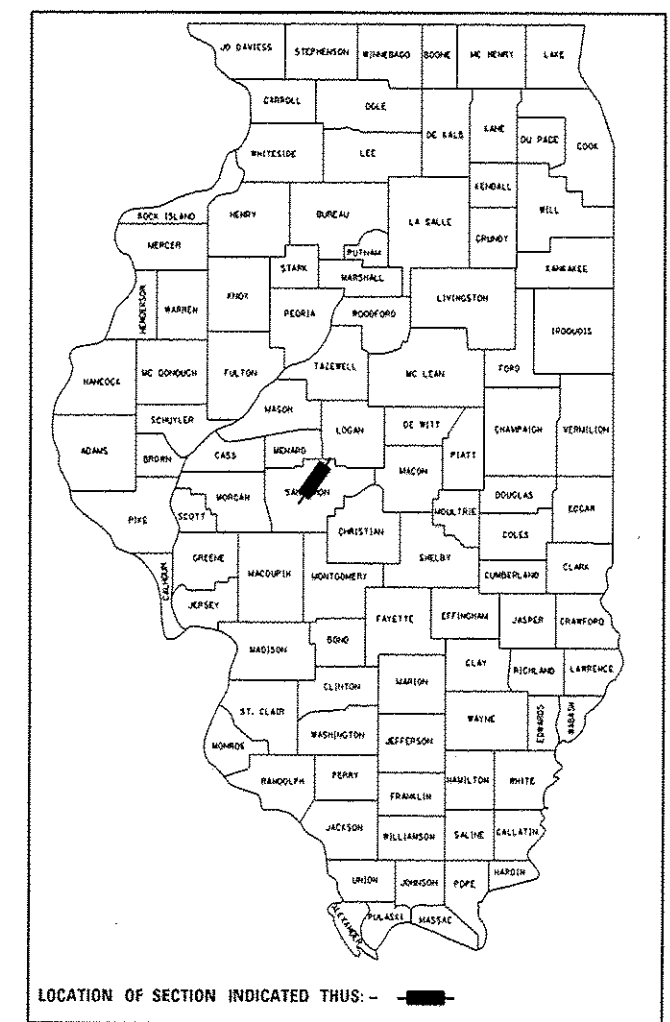
**PROPOSED
HIGHWAY PLANS**

FAP ROUTE 666 (I-55 BUS / PEORIA RD)
SECTION (26)RS, I
PROJECT ACNHPP-ACHSIP-0666(023)
RESURFACING AND
INTERSECTION IMPROVEMENT
SANGAMON COUNTY

C-96-068-12

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
666	(26)RS, I	SANGAMON	116	1
		ILLINOIS	CONTRACT NO. 72F50	

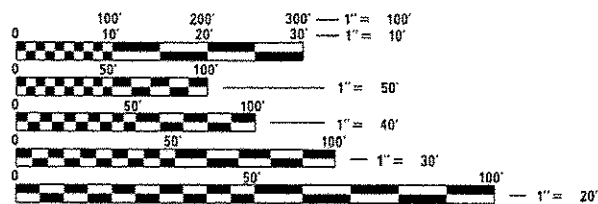
116-1+1 = 116 TOTAL SHEETS
D-96-068-12



FOR INDEX OF SHEETS, SEE SHEET NO. 2

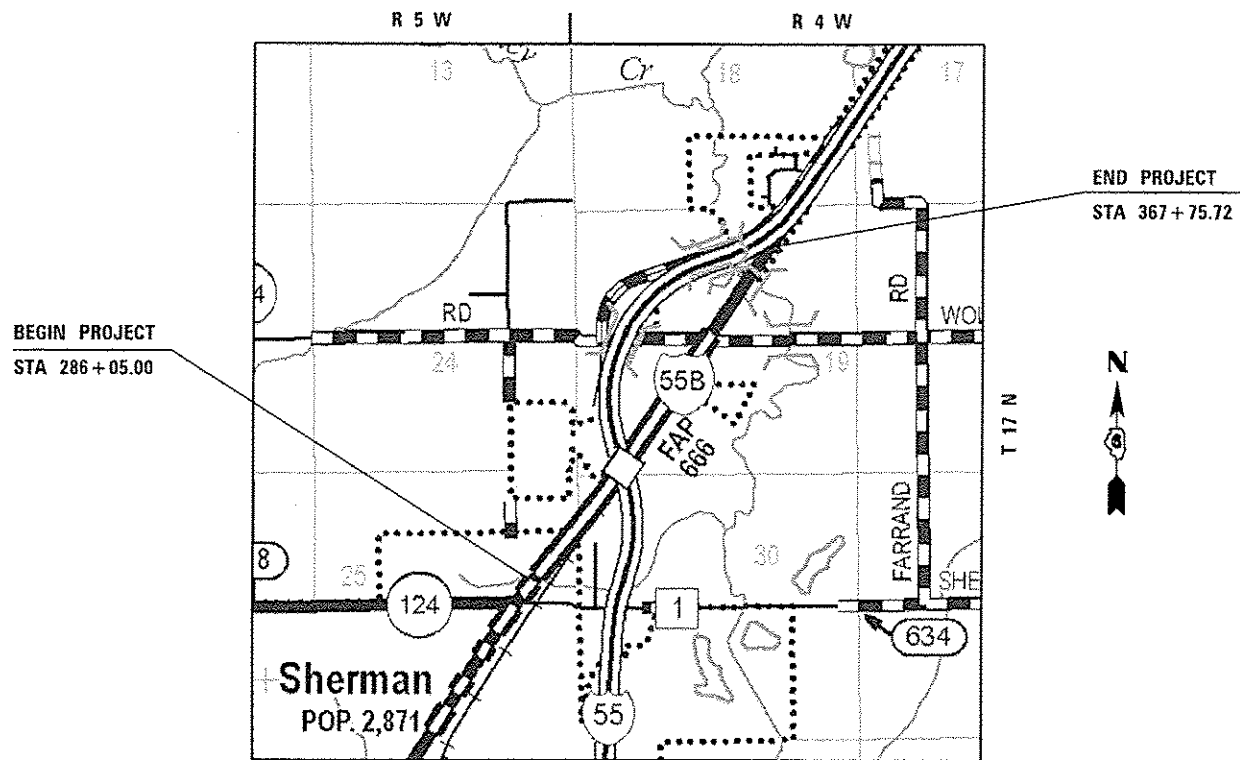
HIGHWAY STANDARDS

000001-06	630301-06	701451-02	BLR 21-9
001001-02	631011-09	701456-03	
280001-07	631032-08	701701-09	
353001-04	635006-03	701901-03	
442101-07	635011-02	720001-01	
442201-03	701006-05	720006-04	
542301-03	701101-04	720011-01	
602401-03	701201-04	780001-04	
604001-03	701301-04	781001-03	
606001-05	701306-03	782001	
606301-04	701311-03	825001-01	
630001-10	701326-04	830021-02	
	701427-02	836001-02	
	701106-02	805001-02	
		821101	
		838001	



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811



PROJECT ENGINEER: JEFF MYERS 217-782-4761
SQUAD LEADER: ED KERN 217-524-7547

GROSS LENGTH = 8,170.7 FT. = 1.56 MILE
NET LENGTH = 8,170.7 FT. = 1.56 MILE

CONTRACT NO. 72F50

ADT'S
BL-55: 8700 (2013)
SU% = 5.81%, 506
MU% = 3.16%, 275
PV = 7919 TF = 2.0

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED *December 5 20 13*

Roger L. Mickell
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

Jan 24 20 14
John D. Boranzello PE, EIT
ENGINEER OF DESIGN AND ENVIRONMENT

Jan 24 20 14
Omer Osman, PE, EIT
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

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OF THE STATE OF ILLINOIS

GENERAL NOTES

- ALL OF THE DISTURBED AREAS WITHIN THE RIGHT OF WAY NOT COVERED BY SURFACING MATERIAL SHALL BE SEEDED.
- CONTRACTOR SHALL PROVIDE ADEQUATE DRAINAGE AND EROSION PROTECTION FOR THE DURATION OF THIS PROJECT.
- SEEDING WILL NOT BE PERMITTED AT ANY TIME WHEN THE GROUND IS FROZEN, WET OR IN AN UNTILLABLE CONDITION. LOCATIONS TO BE SEEDED SHALL BE DETERMINED BY THE ENGINEER.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING UTILITY PROPERTY FROM CONSTRUCTION OPERATIONS AS OUTLINED IN ARTICLE 107.31 OF THE STANDARD SPECIFICATIONS. THE JULIE NUMBER IS 800-892-0123. A MINIMUM OF FORTY-EIGHT HOURS ADVANCE NOTICE IS REQUIRED. ANY DAMAGE TO THE UNDERGROUND FACILITIES, CAUSED BY THE CONTRACTOR SHALL BE REPAIRED TO THE SATISFACTION OF THE DEPARTMENT AT THE CONTRACTOR'S EXPENSE, INCLUDING TEMPORARY REPAIRS WHICH MAY BE REQUIRED TO KEEP THE FACILITY OPERATIONAL WHILE MATERIAL IS BEING OBTAINED TO MAKE PERMANENT REPAIRS.
- THE LOCATIONS OF THOSE BURIED AND ABOVEGROUND UTILITIES SHOWN ARE APPROXIMATE, ARE SHOWN FOR CONTRACTOR INFORMATIONAL USE ONLY, AND ARE NOT TO BE REFERENCED FOR CONSTRUCTIONS PURPOSES. THE IMPLIED PRESENCE OR ABSENCE OF UTILITIES IS NOT TO BE CONSTRUED BY THE OWNER, ENGINEER, CONTRACTOR, OR SUBCONTRACTORS TO BE AN ACCURATE AND COMPLETE REPRESENTATION OF UTILITIES THAT MAY OR MAY NOT EXIST ON THE CONSTRUCTION SITE. BURIED AND ABOVEGROUND UTILITY LOCATION, IDENTIFICATION, AND MARKING ARE THE SOLE RESPONSIBILITY OF THE CONTRACTOR. REROUTING, DISCONNECTION, PROTECTION, ETC. OF ANY UTILITIES MUST BE COORDINATED BETWEEN THE CONTRACTOR, UTILITY COMPANY, AND OWNER. SITE SAFETY, INCLUDING THE AVOIDANCE OF HAZARDS ASSOCIATED WITH BURIED AND ABOVEGROUND UTILITIES, REMAINS THE SOLE RESPONSIBILITY OF THE CONTRACTOR.
- THE THICKNESS OF HOT-MIX ASPHALT MIXTURES SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HOT-MIX ASPHALT MIXTURE IS PLACED.
- FOR STABILIZATION, ALL TYPE III BARRICADES SHALL REQUIRE A MINIMUM OF FOUR SANDBAGS PER BARRICADE.
- WHERE SECTION OR SUBSECTION MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL MONUMENTS UNTIL AN AUTHORIZED SURVEYOR OR AGENT HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR HAVING AN AUTHORIZED SURVEYOR RE-ESTABLISH ANY SECTION OR SUBSECTION MONUMENTS DESTROYED BY HIS OPERATIONS.
- THE FOLLOWING RATES OF APPLICATION HAVE BEEN USED IN CALCULATING PLAN QUANTITIES:

GRANULAR MATERIALS	2.05 TONS/CU YD
RIPRAP	1.75 TON/CU YD
BITUMINOUS MATERIAL (PRIME COAT)	0.00038 TON/SQ YD
AGGREGATE (PRIME COAT)	0.002 TON/SQ YD
HOT-MIX ASPHALT SURFACE COURSE	112 LBS/SQ YD/IN
HOT-MIX ASPHALT BINDER COURSE	112 LBS/SQ YD/IN
NITROGEN FERTILIZER NUTRIENT	90 LBS./ACRE
PHOSPHORUS FERTILIZER NUTIRENT	90 LBS./ACRE
POTASSIUM FERTILIZER NUTIRENT	90 LBS./ACRE
AGRICULTURAL GROUND LIMESTONE	2.0 TON/ACRE
- ALL ELEVATIONS REFER TO U.S.G.S. MEAN SEA LEVEL DATUM.
- ANY REFERENCE TO A STANDARD IN THESE PLANS SHALL BE INTERPRETED TO MEAN THE EDITION AS INDICATED BY THE SUBNUMBER LISTED ON THE INDEX OF SHEETS OR THE COPY OF THE STANDARD INCLUDED IN THESE PLANS.
- UNLESS NOTED OTHERWISE, STATIONS AND OFFSETS REFER TO CENTERLINE OF PROJECT.
- IN ADDITION TO FIELD SURVEYS AND AERIAL SURVEYS, PLAN DIMENSIONS AND DETAILS RELATIVE TO THE EXISTING FACILITIES HAVE BEEN TAKEN FROM EXISTING PLANS AND ARE SUBJECT TO CONSTRUCTION VARIATIONS. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY SUCH DIMENSIONS AND DETAILS IN THE FIELD. SUCH VARIATIONS SHALL NOT BE A CAUSE FOR ADDITIONAL COMPENSATION DUE TO A CHANGE IN THE SCOPE OF THE WORK. HOWEVER, THE CONTRACTOR WILL BE PAID FOR THE QUANTITY ACTUALLY FURNISHED AT THE UNIT PRICE BID FOR THE WORK.

- THE GRADING AND SHAPING OF THE DITCHES SHALL BE DONE IN SUCH A MANNER TO ESTABLISH POSITIVE FLOW BETWEEN DRAINAGE STRUCTURES, OR BETWEEN THE EXISTING ELEVATION AT THE UPSTREAM END OF THE GRADING TO THE DOWNSTREAM DRAINAGE STRUCTURE.
- BEFORE ORDERING PIPE CULVERTS, THE CONTRACTOR SHALL CONSULT THE ENGINEER FOR EXACT LENGTHS.
- THE CONNECTIONS OF THE PROPOSED PIPE CULVERTS TO EXISTING PIPES, CULVERTS, OR STRUCTURES WILL BE TO THE SATISFACTION OF THE ENGINEER, AND WILL BE INCLUDED IN THE COST OF THE PIPE CULVERTS.
- HOT-MIX ASPHALT RESURFACING OF THE EXISTING PAVEMENT AND SHOULDERS SHALL BE DONE IN A MANNER THAT MAINTAINS A MINIMUM VERTICAL CLEARANCE OF 16 FEET AT ALL OVERHEAD STRUCTURES. THE MINIMUM VERTICAL CLEARANCE SHALL BE MAINTAINED FROM OUTSIDE TO OUTSIDE OF THE PAVED SHOULDERS.

PRIOR TO THE START OF RESURFACING OPERATIONS, THE CONTRACTOR, IN THE PRESENCE OF THE ENGINEER, SHALL MEASURE AND DOCUMENT THE EXISTING VERTICAL CLEARANCE AT ALL OVERHEAD STRUCTURES. MEASUREMENTS SHALL BE TAKEN AT THE OUTSIDE EDGES OF THE PAVED SHOULDERS, AT THE EDGE OF EACH LANE, AND AT ANY SPLICE PLATES OVER THE SHOULDERS OR PAVEMENT. IF NECESSARY, THE ENGINEER SHALL MAKE ADJUSTMENTS TO THE RESURFACING THICKNESS SHOWN IN THE PLANS TO MAINTAIN THE REQUIRED MINIMUM VERTICAL CLEARANCE.

FOLLOWING PLACEMENT OF THE SURFACE COURSE AND HMA SHOULDERS, THE CONTRACTOR, IN THE PRESENCE OF THE ENGINEER, SHALL MEASURE AND DOCUMENT THE VERTICAL CLEARANCE AS DESCRIBED ABOVE. IF THE MINIMUM VERTICAL CLEARANCE IS LESS THAN 16 FEET, THE CONTRACTOR, AT HIS OWN EXPENSE, SHALL REMOVE AND REPLACE THE HMA SURFACE COURSE AS DIRECTED BY THE ENGINEER.

THE COST OF THIS WORK SHALL BE INCLUDED IN THE COST OF THE VARIOUS HOT-MIX ASPHALT ITEMS.

HOT-MIX ASPHALT SHOULDERS SHALL BE RAMPED IN THE SAME MANNER AS THE HMA BINDER AND SURFACE COURSES.

- ALL SAW CUTS, NECESSARY TO COMPLETE THE WORK DETAILED IN THESE PLANS, SHALL BE INCLUDED IN THE COST FOR THE VARIOUS PAY ITEMS INVOLVED. THE MINIMUM SAW CUT DEPTH IN THE PAVEMENT SHALL BE 1 1/2" UNLESS OTHERWISE SPECIFIED IN A DETAIL SHOWN IN THE PLANS.
- THE ENGINEER SHALL BE THE SOLE JUDGE CONCERNING CURING TIME FOR THE VARIOUS BITUMINOUS LIFTS.
- ANY REFERENCES OR CALL-OUTS IN THESE PLANS TO "BITUMINOUS CONCRETE" SHALL BE INTERPRETED TO MEAN "HOT-MIX ASPHALT."

COMMITMENTS:
THERE ARE NO COMMITMENTS ON THIS PROJECT.

INDEX OF SHEETS

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38-39	TRANSITION TAPER DETAIL SHEETS
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* 49-56	LIGHTING PLAN SHEETS
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59-63A	DETAIL SHEETS
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65-68	STORM WATER POLUTION PREVENTION PLAN
69-109	CROSS SECTION SHEETS - BL 55
110-111	CROSS SECTION SHEETS - WOLF CREEK ROAD
112-116	CROSS SECTION SHEETS - SUDDUTH ROAD

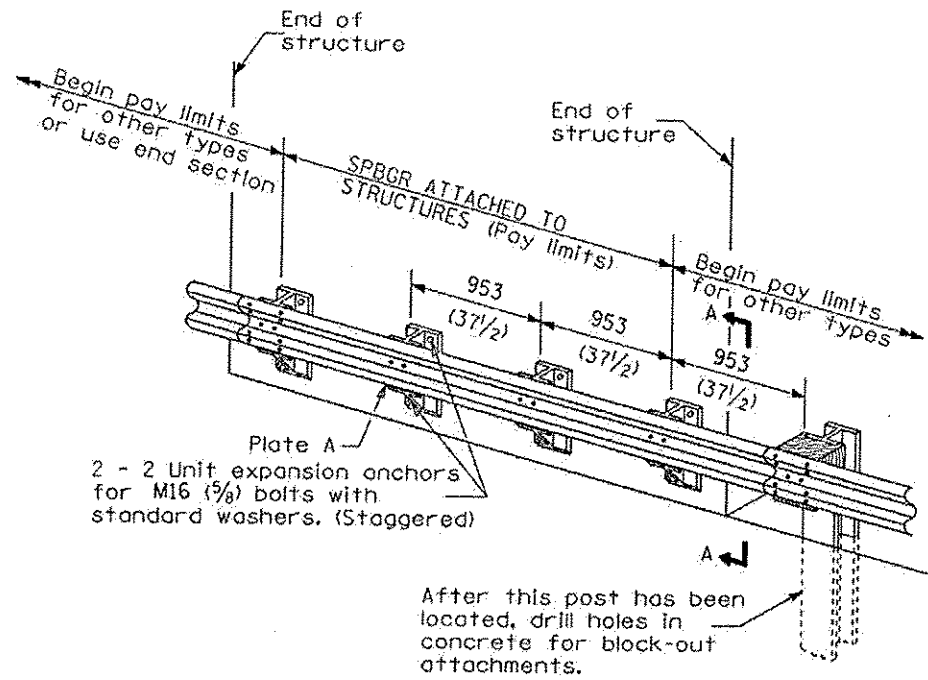
* SHEET 55 DELETED

DISTRICT SIX	
EXAMINED <u>11/27</u>	20 <u>13</u>
<i>Bill McBoyer</i> OPERATIONS ENGINEER	
EXAMINED <u>Nov 19</u>	20 <u>13</u>
<i>Tommy L. L.</i> PROJECT IMPLEMENTATION ENGINEER	
EXAMINED <u>Nov. 25</u>	20 <u>13</u>
<i>QRMLJ</i> PROGRAM DEVELOPMENT ENGINEER	

The following mixture requirements are applicable for this project:

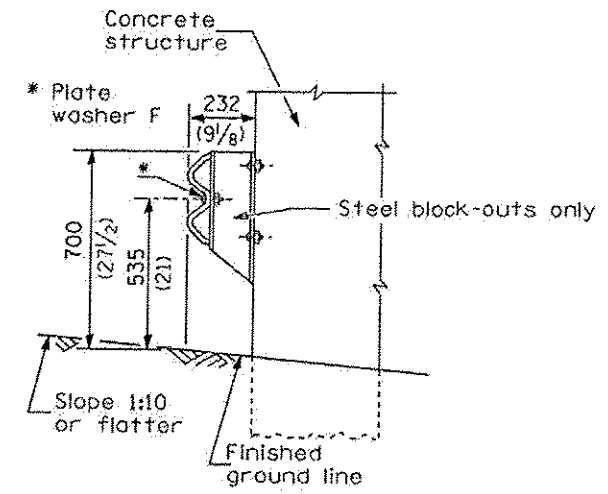
Mixture Number	1	2	3	4
Mixture Use(s)	HMA SURFACE	HMA BASE CSE, PATCHING, HMA BINDER	HMA SHOULDERS (SURFACE LIFT)	HMA SHOULDERS, 6 1/2"
AC/PG:	PG 64-22	PG 64-22	PG 64-22	PG 64-22
Design Air Voids:	4.0% @ N70	4.0% @ N70	4.0% @ N50	4.0% @ N50
Mixture Composition: (Gradation Mixture)	IL 9.5	IL 19.0	IL 9.5	IL 19.0
Friction Aggregate:	MIX "D"	N/A	MIX "C"	N/A

1 2-14-14

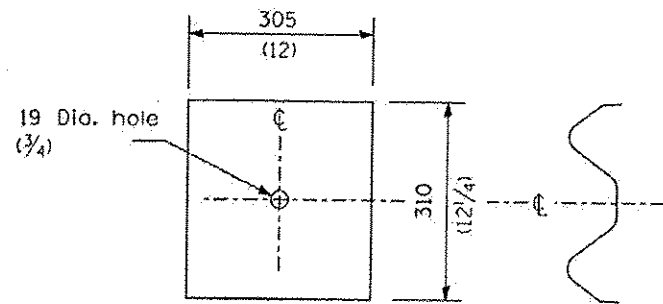


SPBGR ATTACHED TO STRUCTURES

953 (37 1/2) Block-out spacing



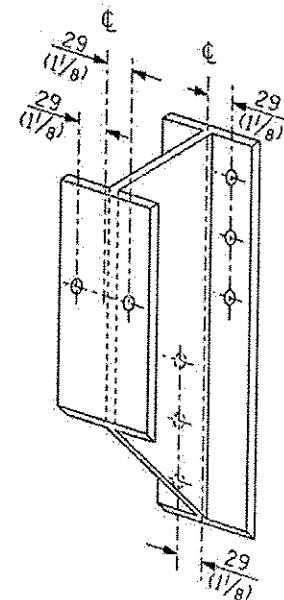
SECTION A-A



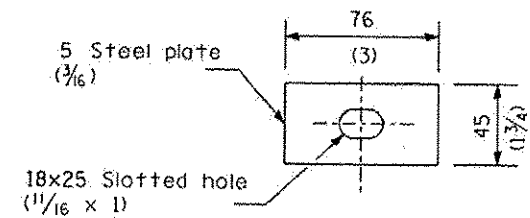
NOTE

Plate A shall be placed between rail element and block-out at non-splice mounting points only when steel block-outs are used.

PLATE A



STEEL BLOCK-OUT DETAIL



NOTE

Plate washer F shall be used on type A guardrail only where specified. Plate washer F shall be used at all other locations where rail element is bolted to a block-out unless otherwise noted.

PLATE WASHER F

△ SHEET ADDED 2-14-14

FILE NAME *	USER NAME * karnes	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETAIL SHEET FOR GUARDRAIL ATTACHED TO STRUCTURES		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
c:\pwork\pwork\karnes\0366010\0672F	DB-alt-Guardrail Detail	DRAWN -	REVISED -				666	(26)RS.1	SANGAMON	110	63A		
PLOT SCALE * 1/8" = 1'-0"		CHECKED -	REVISED -				SCALE: NTS		SHEET 1 OF 1 SHEETS		STA.	TO STA.	CONTRACT NO. 72F50
PLOT DATE * Feb-07-2014 10:35:00AM		DATE -	REVISED -				ILLINOIS FED. AID PROJECT						