

02-28-14 LETTING ITEM 082

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

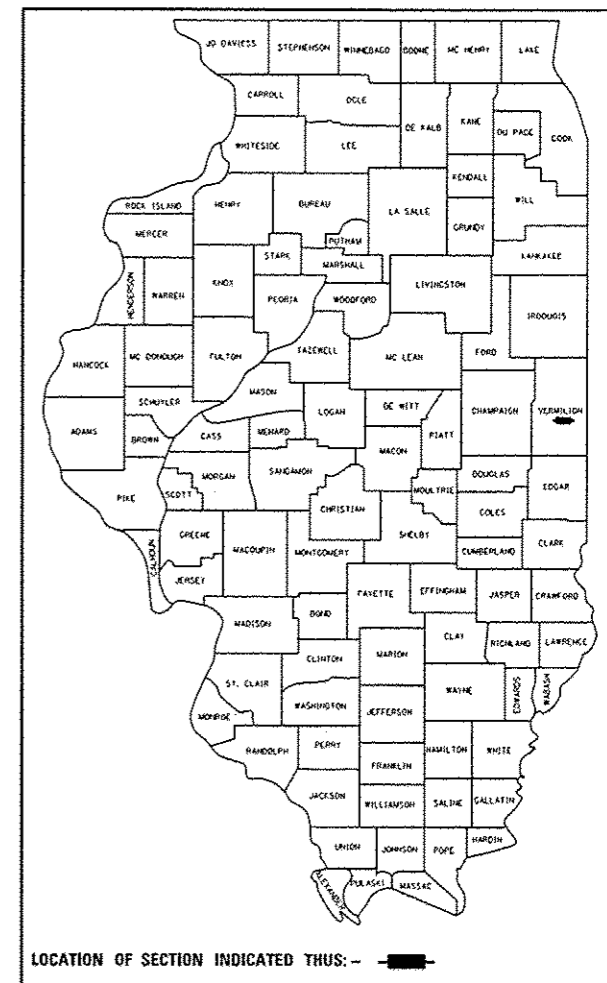
FAP 729 (US 150)
SECTION (34Z-1B) I-2

BRIDGE JOINT REPAIR OVER
NORTH FORK VERMILION RIVER
IN DANVILLE
VERMILION COUNTY

C-95-009-12

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
729	(34Z-1B) I-2	VERMILION	25	1
		ILLINOIS	CONTRACT NO. 70971	

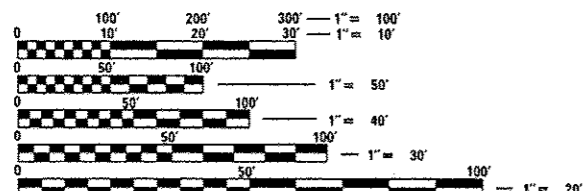
D95-009-12



FOR INDEX OF SHEETS, SEE SHEET NO.
FOR SUMMARY OF QUANTITIES, SEE SHEET NO.

CURRENT TRAFFIC DATA	
2012 ADT =	14,100
2032 ADT =	16,200
P.U. & P.C. % =	96.2
SU % =	2.5
MU % =	1.3

DESIGN DESIGNATION
O.P.A.



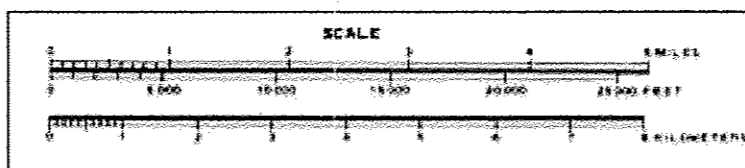
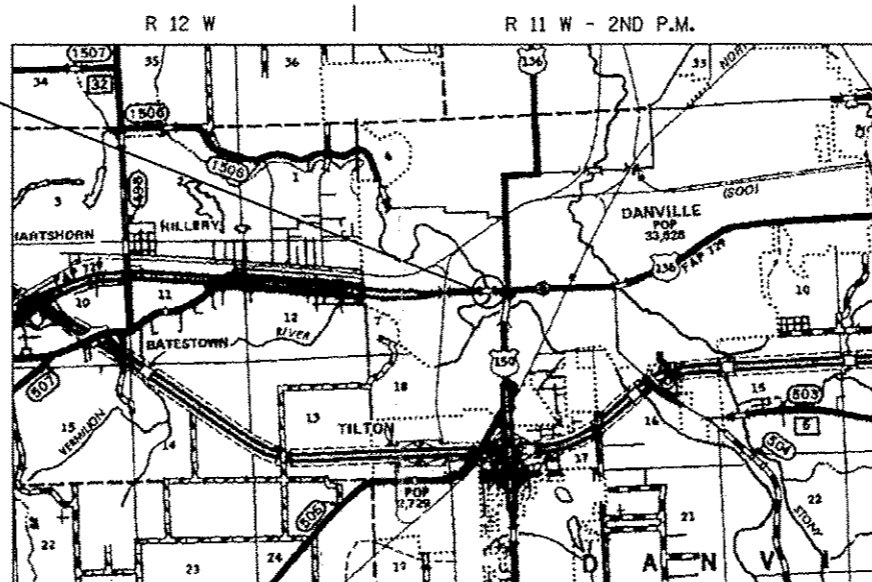
FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811 DANVILLE TOWNSHIP

PROJECT ENGINEER: TIM BRANDENBURG
217-465-4181

CONTRACT NO. 70971

EXISTING S.N. 092-0053
F.A.P. 729 (US-150)
STATION 616+30.50
OVER NORTH FORK
VERMILION RIVER
PROPOSED BRIDGE
JOINT REPAIRS



GROSS LENGTH = 511.0 FT. = 0.097 MILE
NET LENGTH = 14.7 FT. = 0.003 MILE

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED OCTOBER 4, 2013

Joseph F. Grove
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

Jan 24 2014
John D. Baranzelli, P.E.
acting ENGINEER OF DESIGN AND ENVIRONMENT

Jan 24 2014
Omer Osman, P.E.
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

INDEX OF SHEETS

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	INDEX OF SHEETS
2	LIST OF STANDARDS
3	GENERAL NOTES
3	COMMITMENTS
4 - 5	SUMMARY OF QUANTITIES
6	GENERAL PLAN & ELEVATION; SN 092-0053
7 - 13	STRUCTURE REPAIR PLANS; SN 092-0053
14 - 19	AS-BUILT PLANS; SN 092-0053 (FOR INFORMATION ONLY)
20 - 23	STAGING PLANS
24	WIDTH RESTRICTION SIGNING
25	DETECTOR LOOP RELOCATION

LIST OF STANDARDS

STANDARD NO.	DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001001-02	AREAS OF REINFORCEMENT BARS
001006	DECIMAL OF AN INCH AND OF A FOOT
701101-04	OFF-ROAD OPERATIONS, MULTILANE, 15' TO 24" FROM PAVEMENT EDGE
701606-09	URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701801-05	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-03	TRAFFIC CONTROL DEVICES
704001-07	TEMPORARY CONCRETE BARRIER
780001-04	TYPICAL PAVEMENT MARKINGS
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUTS FOR DETECTOR LOOPS

FILE NAME *	USER NAME = ceerlockjd	DESIGNED - TJB	REVISED - 09/20/2013	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	INDEX OF SHEETS / HIGHWAY STANDARDS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\p\work\p\dot\ceerlockjd\299453\	72971-Sht-Gennote.dgn	DRAWN -	REVISED -			729	(342-1B) 1-2	VERMILION	25	2	
PLOT SCALE * 40.0000 1/ in.	CHECKED -	REVISED -	REVISED -			CONTRACT NO. T0971					
PLOT DATE * 9/23/2013	DATE -	REVISED -	REVISED -			SCALE: SHEET OF SHEETS STA. TO STA. ILLINOIS FED. AID PROJECT					

GENERAL NOTES

G.N.-100

ENGLISH UNITS OF MEASUREMENT SHALL GOVERN OVER AND SUPERSEDE ANY METRIC UNITS SHOWN IN THIS CONTRACT. WHERE INCLUDED, METRIC UNITS ARE FOR INFORMATION ONLY.

G.N.-100A

ELECTRONIC FILES AND/OR ELECTRONIC SURVEY INFORMATION INCLUDING CADD FILES WILL NOT BE AVAILABLE TO THE CONTRACTOR.

G.N.-105.07

EXISTING STATE-OWNED AND MAINTAINED UTILITY LINES ARE SHOWN ON THE PLANS TO INDICATE THEIR PRESENCE AND APPROXIMATE LOCATION. THE CONTRACTOR SHALL NOTIFY THE DISTRICT OPERATIONS ENGINEER TWO WEEKS PRIOR TO COMMENCING ANY EXCAVATION IN THE VICINITY OF THESE LINES. THE STATE WILL THEN LOCATE AND MARK THE HORIZONTAL LOCATIONS OF THE LINES AND PROVIDE ANY AVAILABLE INFORMATION AS TO THEIR DEPTH. SHOULD ANY OF THE LINES BE DAMAGED BY THE CONTRACTOR'S OPERATION, THE CONTRACTOR SHALL REPAIR THEM TO THE SATISFACTION OF THE ENGINEER AND AT NO COST TO THE STATE.

ALSO THERE MAY BE UTILITIES PRESENT WHICH WERE INSTALLED BY THE STATE BUT ARE MAINTAINED BY OTHERS (CITY, TOWN, ETC.) THE APPROXIMATE LOCATIONS OF THESE LINES ARE ALSO SHOWN ON THE PLANS ALONG WITH THE NAME OF THE MAINTAINING AGENCY. THE CONTRACTOR SHALL COORDINATE THE LOCATING OF THESE LINES WITH THE LOCAL AGENCY PRIOR TO COMMENCING ANY EXCAVATION OR BORING IN THEIR VICINITY. SHOULD THESE LINES BE DAMAGED BY THE CONTRACTOR'S OPERATIONS, THE CONTRACTOR SHALL REPAIR THEM TO THE SATISFACTION OF, AND AT NO COST TO, THE LOCAL AGENCY AND THE STATE.

G.N.-107.37

UTILITY LINES WERE PLOTTED FROM INFORMATION FURNISHED BY THE VARIOUS UTILITY COMPANIES INVOLVED (QUALITY LEVEL C &/OR QUALITY LEVEL D) AND THE ACCURACY SHOULD BE CONSIDERED APPROXIMATE ONLY.

UTILITY COMPANIES MAY BE ADJUSTING THEIR FACILITIES DURING CONSTRUCTION. THE CONTRACTOR SHALL COOPERATE WITH THESE ORGANIZATIONS WHILE THESE ADJUSTMENTS ARE BEING PERFORMED. J.U.L.I.E. - JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS SYSTEM (800) 892-0123 OR 811.

NO COMMITMENTS

FILE NAME =	USER NAME = ceerlockjd	DESIGNED - TJB	REVISED - 09/20/2013	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	GENERAL NOTES /COMMITMENTS	F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
er\pvc\work\pav\dot\ceerlockjd\102994831\1029971-5kt-Gennote.dgn		DRAWN -	REVISED -			729	(34Z-1B) 1-2	VERMILION	25	3	
PLOT SCALE = 40.0000 1" = 40'		CHECKED -	REVISED -			CONTRACT NO. 70971					
PLOT DATE = 9/23/2013		DATE -	REVISED -			ILLINOIS FED. AID PROJECT					
				SCALE:							
				SHEET		OF SHEETS STA. TO STA.					

FAP 729 (US 150)
 VERMILION COUNTY
 URBAN MULTILANE
 100% STATE
 BRIDGE JOINT REPAIR
 STA. 616+30.50
 S.N. 092-0053

CONSTRUCTION TYPE CODE: 0014

CODE NO	ITEM DESCRIPTION	UNIT	QUANTITY
50102400	CONCRETE REMOVAL	CU YD	13.2
50104650	SLOPE WALL REMOVAL	SQ YD	187.0
50300255	CONCRETE SUPERSTRUCTURE	CU YD	13.6
50300300	PROTECTIVE COAT	SQ YD	36.0
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	2270.0
50800515	BAR SPLICERS	EACH	12.0
51100300	SLOPE WALL 6 INCH	SQ YD	187.0
52000110	PREFORMED JOINT STRIP SEAL	FOOT	144.0
67100100	MOBILIZATION	L SUM	1.0
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1.0
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1.0
70400100	TEMPORARY CONCRETE BARRIER	FOOT	1175.0
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	1125.0

13

FILE NAME =	USER NAME = pwrlockjd	DESIGNED - TJB	REVISED - 09/20/13	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\pwr\work\pwrlockjd\0229453\070971-5ht-500.dgn	PLOT SCALE = 40,0000' / in.	DRAWN -	REVISED -						729	(342-1B) 1-2	VERMILION	25	4
MODELNAME =	PLOT DATE = 9/23/2013	CHECKED -	REVISED -		SCALE:	SHEET	OF	SHEETS	STA.	TO STA.	CONTRACT NO. 70971		
										ILLINOIS FED. AID PROJECT			

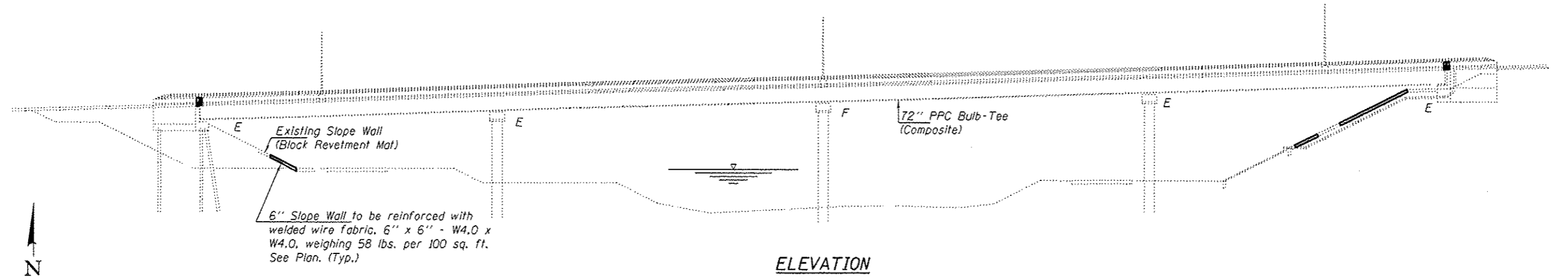
FAP 729 (US 150)
 VERMILION COUNTY
 URBAN MULTILANE
 100% STATE
 BRIDGE JOINT REPAIR
 STA. 616+30.50
 S.N. 092-0053

CONSTRUCTION TYPE CODE: 0014

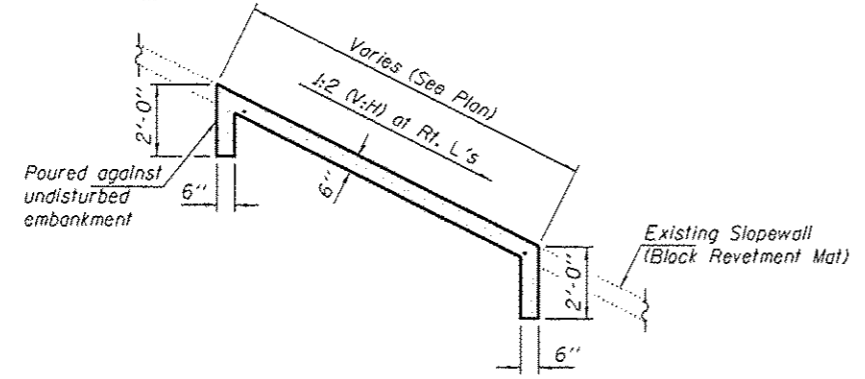
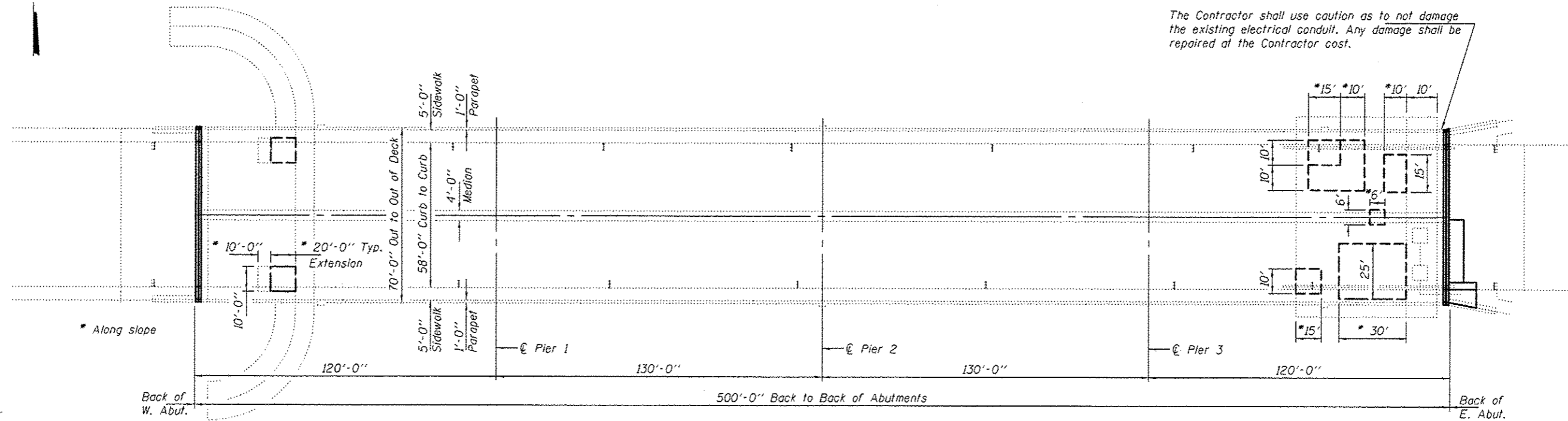
CODE NO	ITEM DESCRIPTION	UNIT	QUANTITY
70600250	IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE), TEST LEVEL 3	EACH	2.0
70600350	IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE), TEST LEVEL 3	EACH	2.0
81500100	GULFBOX JUNCTION	EACH	1.0
88600100	DETECTOR LOOP, TYPE I	FOOT	73.0
X7015005	CHANGEABLE MESSAGE SIGN	CAL DA	56.0
X7200201	WIDTH RESTRICTION SIGNING	L SUM	1.0
Z0001700	APPROACH SLAB REPAIR (FULL DEPTH)	SQ YD	28.4

* SPECIALTY ITEMS

FILE NAME #	USER NAME # cearlockjd	DESIGNED - TJB	REVISED - 09/20/2013	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SUMMARY OF QUANTITIES	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
c:\p\work\p\ridot\cearlockjd\029945310	70971-Sht-500.dgn	DRAWN -	REVISED -			729	(34Z-JB) 1-2	VERMILION	25	5	
MODELNAME#	PLOT SCALE = 48,0000' / in.	CHECKED -	REVISED -			CONTRACT NO. 70971					
	PLOT DATE = 9/23/2013	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					
				SCALE: SHEET OF SHEETS STA. TO STA.							



The Contractor shall use caution as to not damage the existing electrical conduit. Any damage shall be repaired at the Contractor cost.



GENERAL NOTES

All structural steel shall conform to AASHTO Classification M-270 Gr. 36, unless otherwise noted.

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

The Contractor shall use extreme care during concrete removal so as not to damage the PPC Bulb-Tee Beam.

Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.

The deck surface shall have its final finish tined according to Article 420.09(e)(1) of the Standard Specifications. Cost Included with Concrete Superstructure.

Joint openings shall be adjusted according to Article 520.04 of the Standard Specifications when the deck is poured at an ambient temperature other than 50° F.

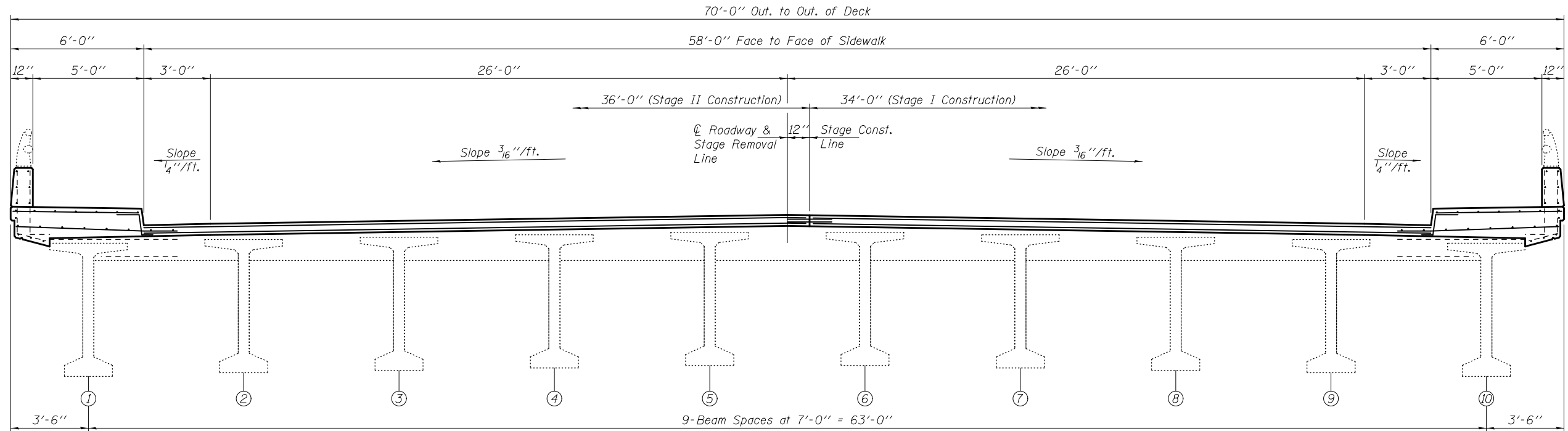
Reinforcement bars designated (E) shall be epoxy coated.

TOTAL BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Concrete Removal	Cu. Yd.	13.2
Concrete Superstructure	Cu. Yd.	13.6
Reinforcement Bars, Epoxy Coated	Pound	2270
Bar Splicers	Each	12
Preformed Joint Strip Seal	Foot	144
Slope Wall Removal	Sq. Yd.	187.0
Slope Wall, 6 inch	Sq. Yd.	187.0
Approach Slab Repair (Full Depth)	Sq. Yd.	28.4

Note: All materials required for approach slab are included with Approach Slab Repair (Full Depth).

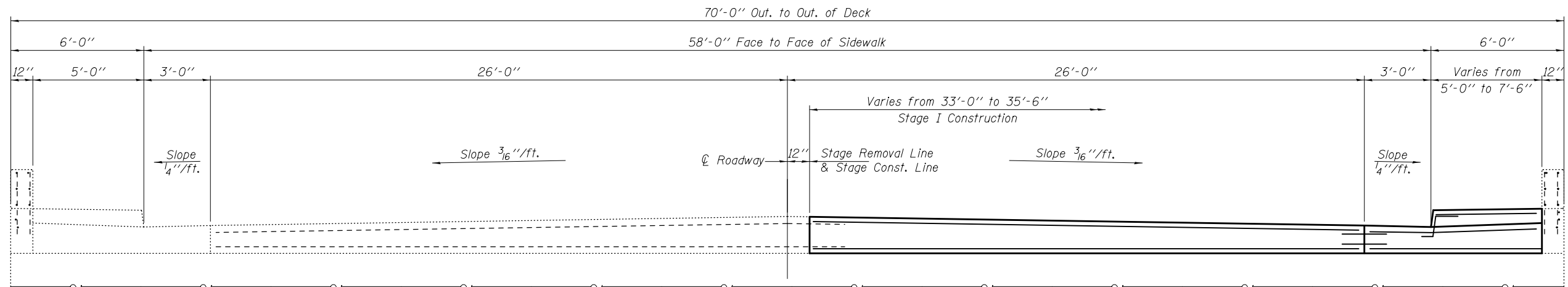




CROSS-SECTION THRU BRIDGE DECK

(Looking East)

Note:
For sidewalk and parapet
dimensions see sheet 4 of 8.



CROSS-SECTION THRU EAST APPROACH

(Looking East)

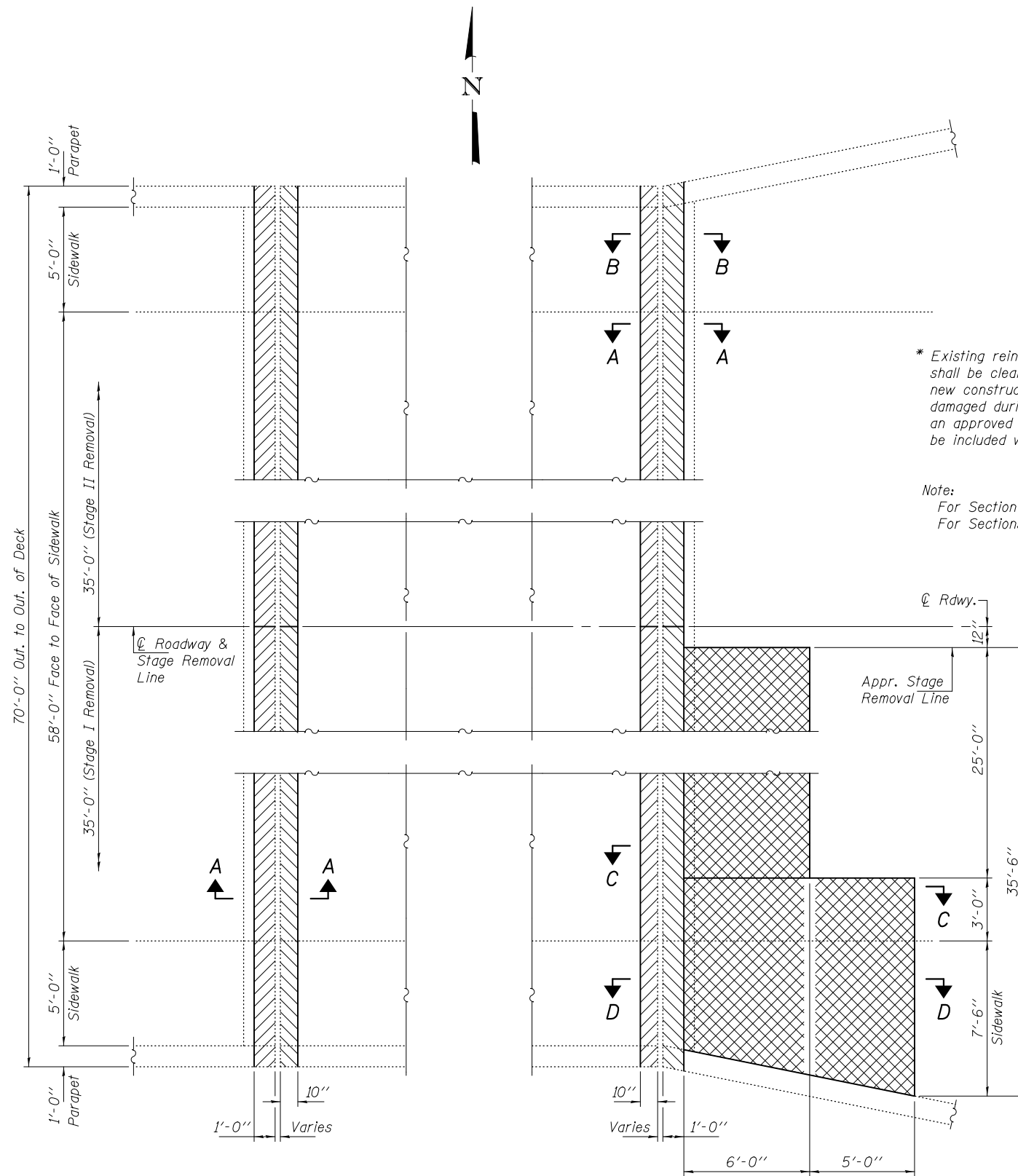
DESIGNED ADY	EXAMINED <i>Timothy A. [Signature]</i>	DATE JANUARY 17, 2014
CHECKED ARS	PASSED <i>[Signature]</i>	REVISOR
DRAWN Kyle M. Steffen	ACTING ENGINEER OF BRIDGES AND STRUCTURES	REVISOR
CHECKED ADY ARS		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

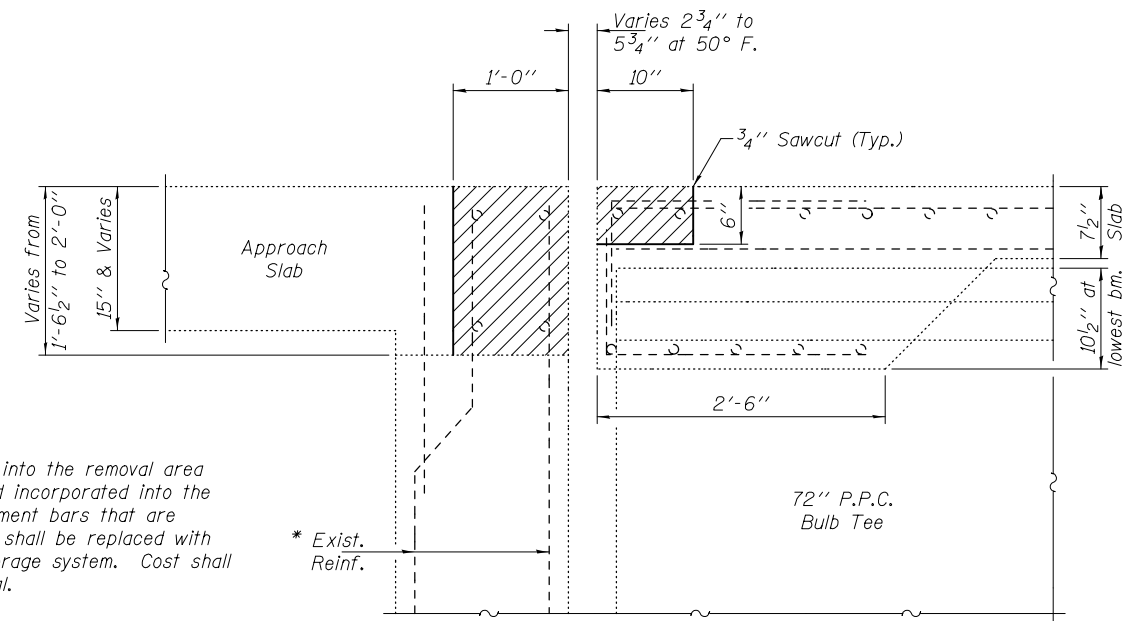
TYPICAL CROSS SECTIONS
SN 092-0053

SHEET NO. 2 OF 8 SHEETS

F.A.P. RTE. 729	SECTION (34Z-1B)I-2	COUNTY VERMILION	TOTAL SHEETS 25	SHEET NO. 7
ILLINOIS FED. AID PROJECT			CONTRACT NO. 70971	



REMOVAL PLAN

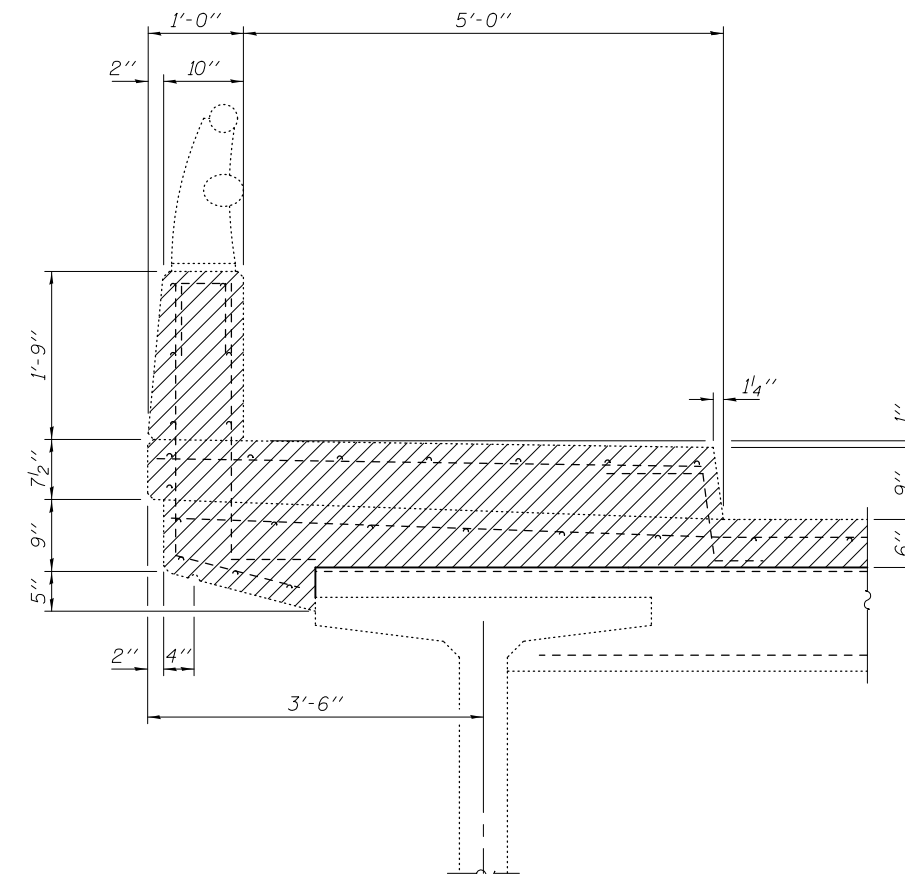


SECTION A-A

* Existing reinforcement extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost shall be included with Concrete Removal.

Note:
For Section B-B see sheet 5 of 8.
For Sections C-C & D-D see sheet 6 of 8.

- Hatched areas indicate Concrete Removal
- Cross-Hatched areas indicate Approach Slab Repair (Full Depth)



SECTION THRU SIDEWALK

DESIGNED ADY	EXAMINED <i>Timothy A. [Signature]</i>	DATE JANUARY 17, 2014
CHECKED ARS	PASSED <i>[Signature]</i>	REVISED
DRAWN Kyle M. Steffen	ACTING ENGINEER OF BRIDGES AND STRUCTURES	REVISED
CHECKED ADY ARS	ACTING ENGINEER OF STRUCTURAL SERVICES	

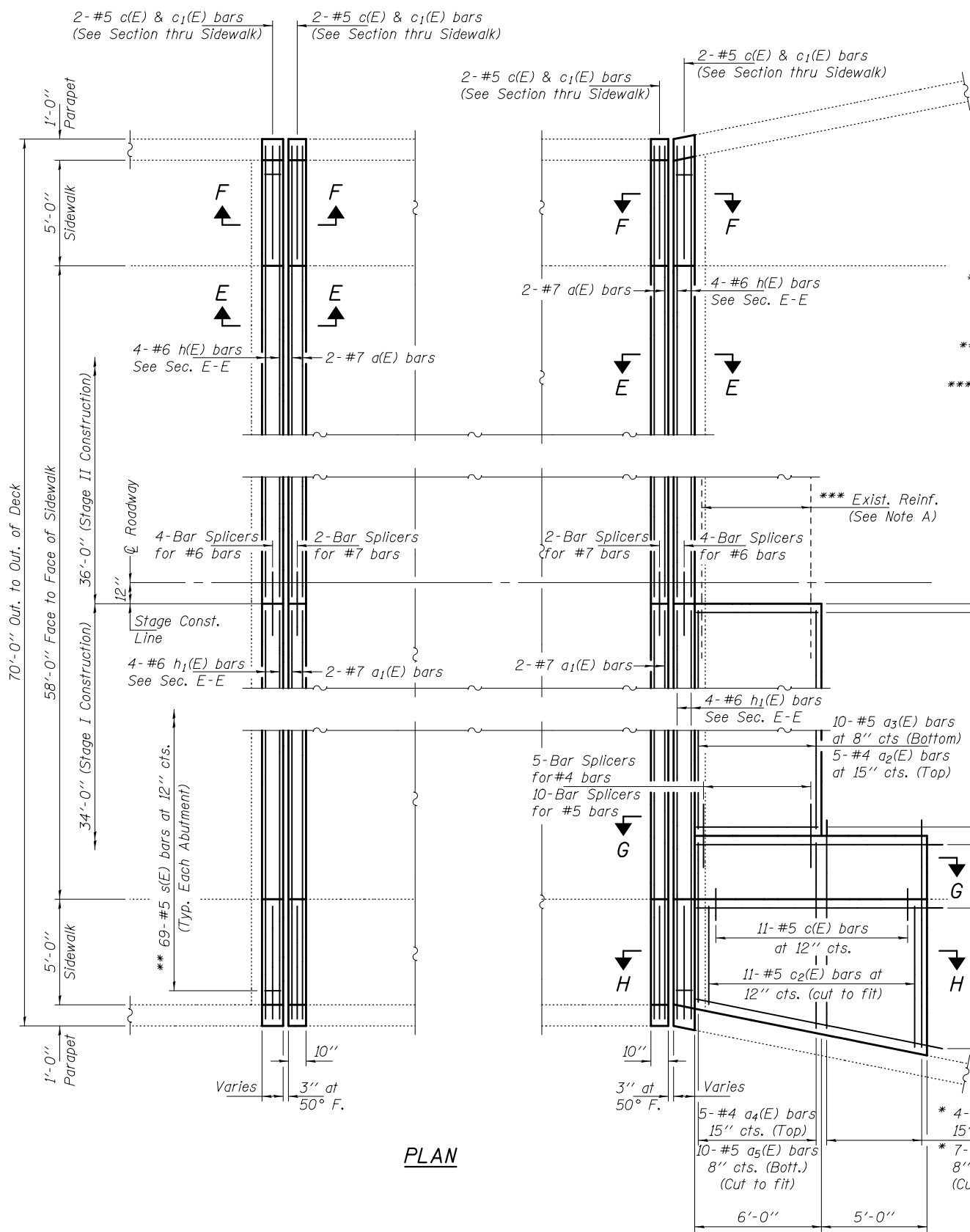
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CONCRETE REMOVAL DETAILS
SN 092-0053**

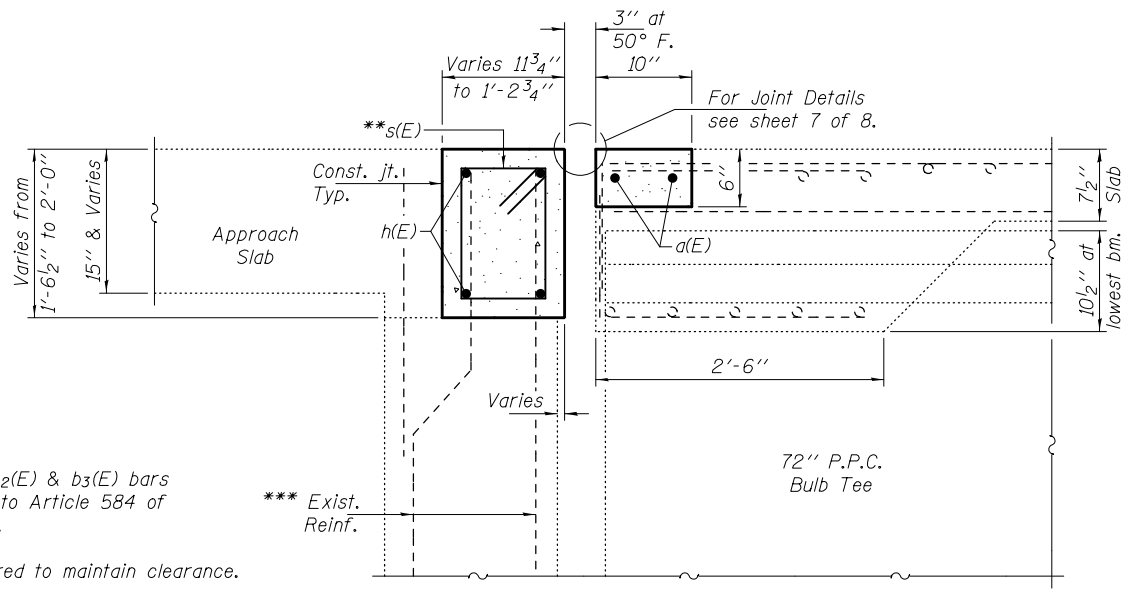
SHEET NO. 3 OF 8 SHEETS

F.A.P. RTE. 729	SECTION (34Z-1B)I-2	COUNTY VERMILION	TOTAL SHEETS 25	SHEET NO. 8
CONTRACT NO. 70971				

ILLINOIS FED. AID PROJECT



PLAN



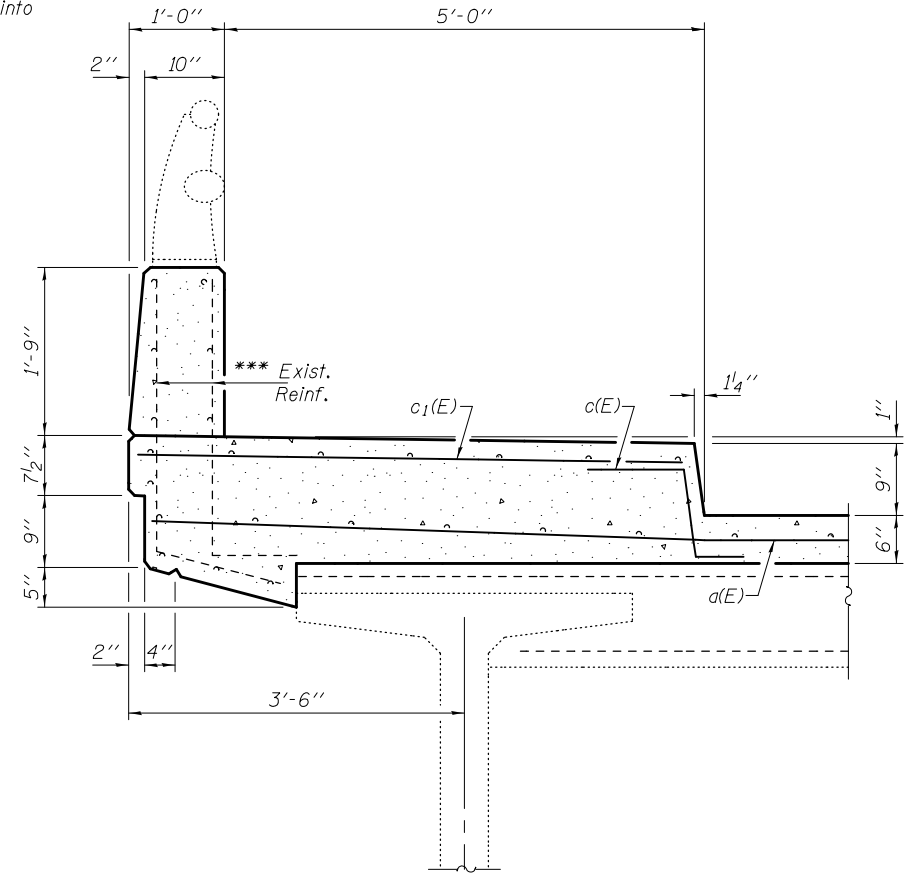
SECTION E-E

* Epoxy grout $a_6(E)$, $a_7(E)$, $b_2(E)$ & $b_3(E)$ bars in 9" min. holes according to Article 584 of the Standard Specifications.

** Tilt #5 $s(E)$ bars as required to maintain clearance.

*** Existing reinforcement extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost shall be included with Concrete Removal.

Note A:
Existing reinforcement shall extend into new approach pavement 2'-7" min.



SECTION THRU SIDEWALK

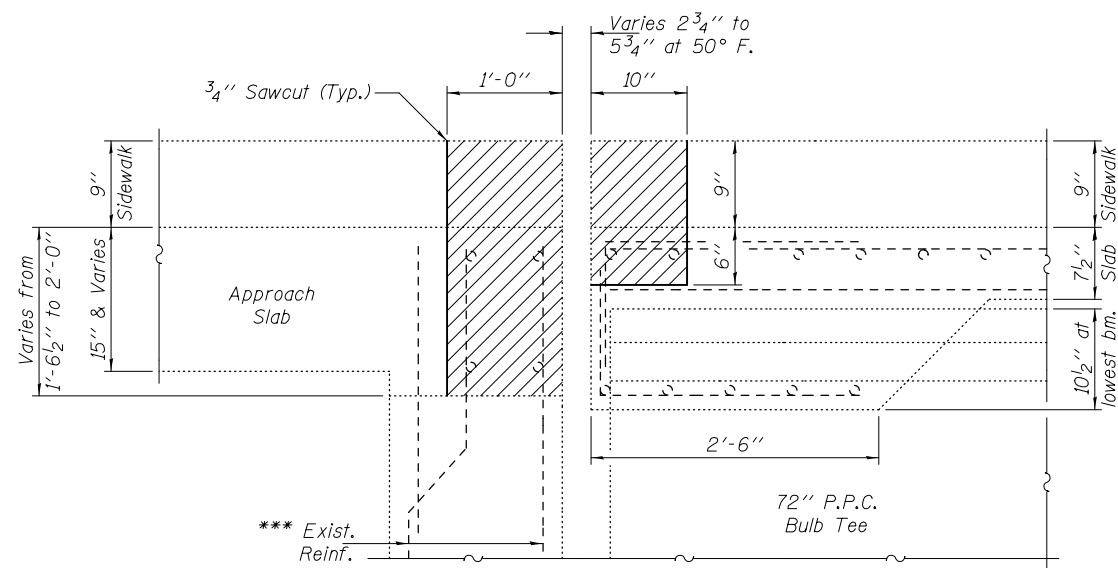
Note:
For Section F-F and Bill of Material for Bridge see sheet 5 of 8.
For Sections G-G & H-H and Bill of Material for Approach see sheet 6 of 8.

DESIGNED ADY	EXAMINED <i>Timothy A. [Signature]</i>	DATE JANUARY 17, 2014
CHECKED ARS	PASSED <i>[Signature]</i>	REVISED
DRAWN Kyle M. Steffen	ACTING ENGINEER OF BRIDGES AND STRUCTURES	REVISED
CHECKED ADY ARS		

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

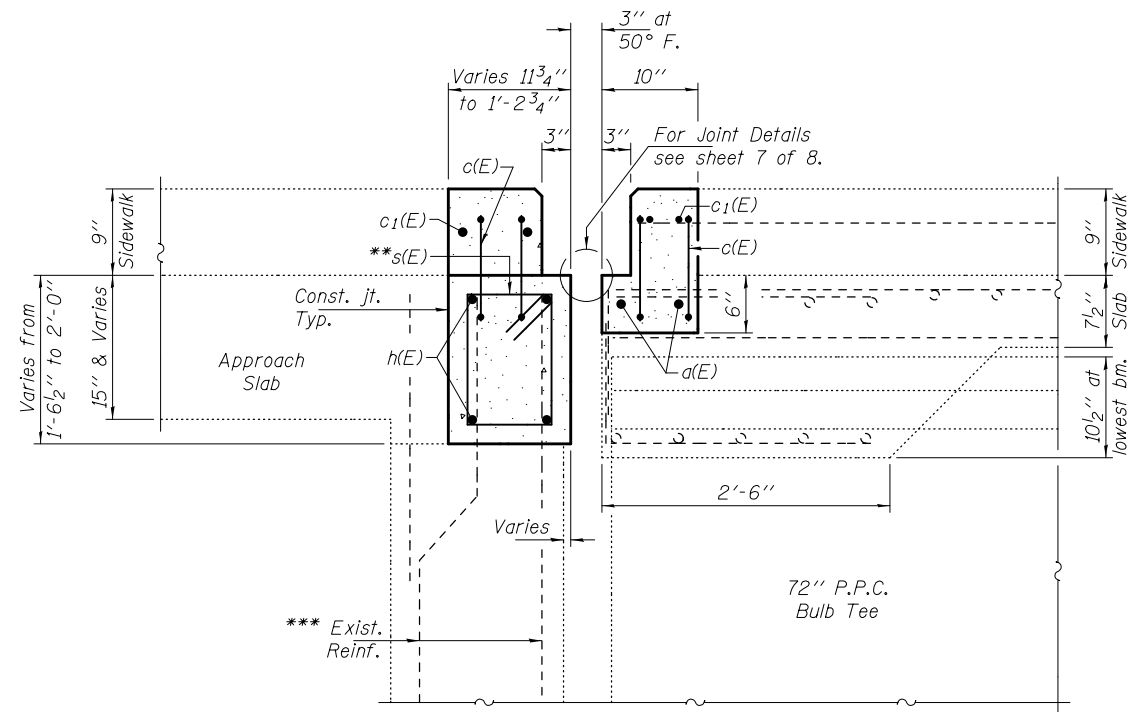
**CONCRETE REPLACEMENT DETAILS
SN 092-0053**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
729	(34Z-1B)I-2	VERMILION	25	9
CONTRACT NO. 70971			ILLINOIS FED. AID PROJECT	



SECTION B-B

- Hatched areas indicate Concrete Removal

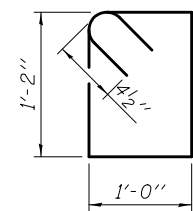


SECTION F-F

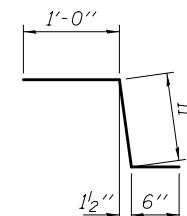
Sliding Plates on sidewalk not shown for clarity. See details on sheet 7 of 8.

** Tilt #5 s(E) bars as req'd to maintain clearance.

*** Existing reinforcement extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost shall be included with Concrete Removal.



BAR s(E)



BAR c(E)

BILL OF MATERIAL (BRIDGE)

Bar	No.	Size	Length	Shape
a(E)	4	#7	35'-7"	—
a ₁ (E)	4	#7	33'-7"	—
c(E)	16	#5	2'-5"	⌒
c ₁ (E)	16	#5	5'-8"	—
h(E)	8	#6	35'-7"	—
h ₁ (E)	8	#6	33'-7"	—
s(E)	138	#5	5'-1"	⊞
Concrete Removal			Cu. Yd.	13.2
Concrete Superstructure			Cu. Yd.	13.6
Bar Splicers			Each	12
Reinforcement Bars, Epoxy Coated			Pounds	2270

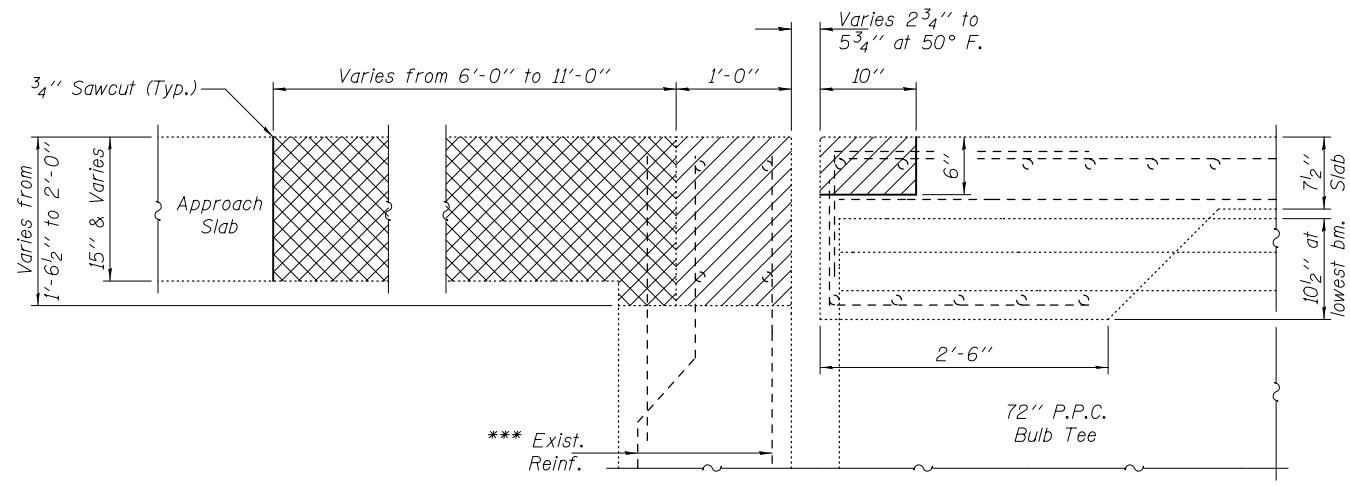
DESIGNED ADY	EXAMINED	DATE JANUARY 17, 2014
CHECKED ARS	PASSED	REVISED
DRAWN Kyle M. Steffen	ACTING ENGINEER OF BRIDGES AND STRUCTURES	REVISED
CHECKED ADY ARS		

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

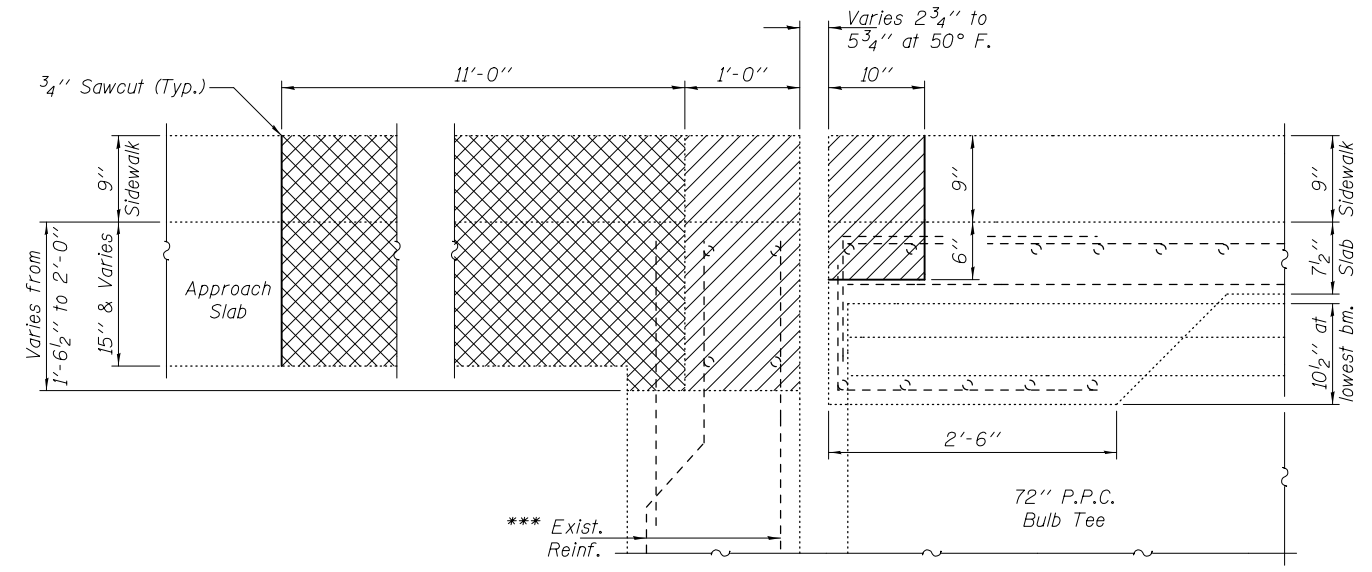
**REPAIR DETAILS
SN 092-0053**

SHEET NO. 5 OF 8 SHEETS

F.A.P. RTE. 729	SECTION (342-1B)I-2	COUNTY VERMILION	TOTAL SHEETS 25	SHEET NO. 10
CONTRACT NO. 70971			ILLINOIS FED. AID PROJECT	

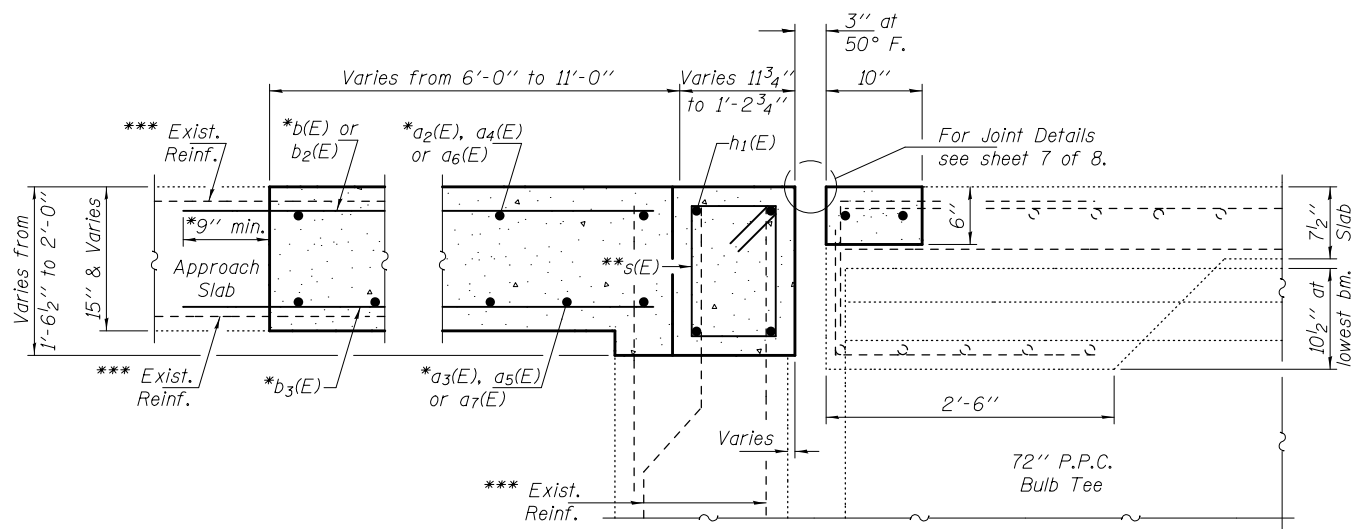


SECTION C-C

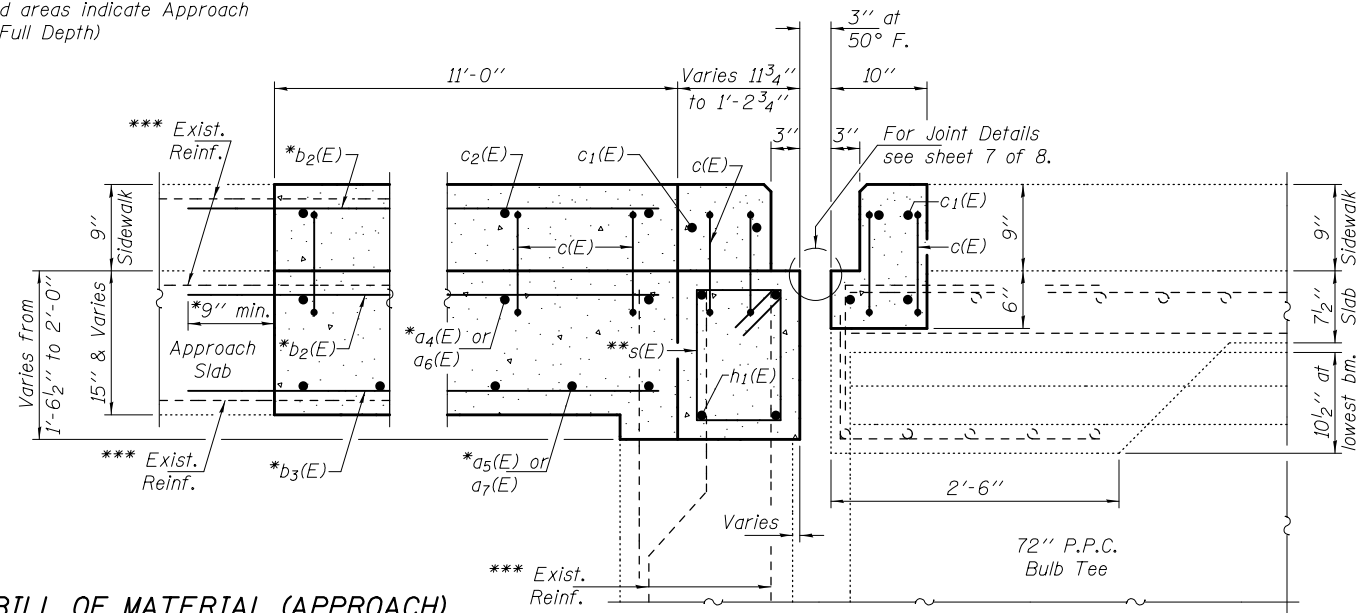


SECTION D-D

- Hatched areas indicate Concrete Removal
 - Cross-Hatched areas indicate Approach Slab Repair (Full Depth)



SECTION G-G

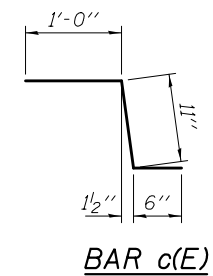


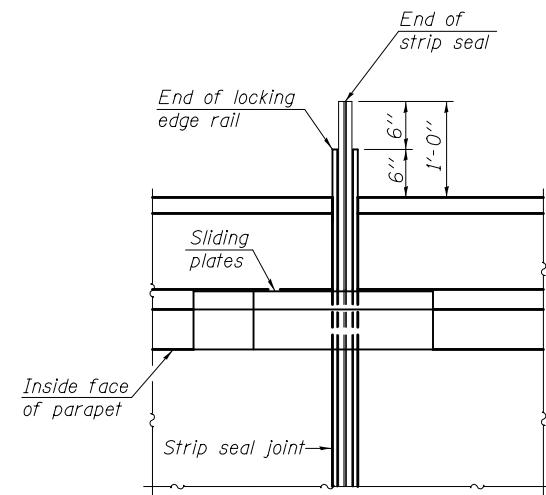
SECTION H-H

***** BILL OF MATERIAL (APPROACH)

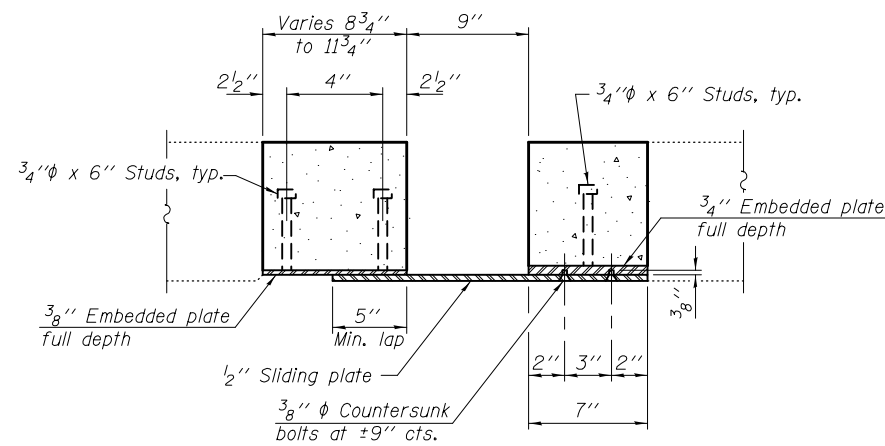
Bar	No.	Size	Length	Shape
a ₂ (E)	5	#4	23'-7"	—
a ₃ (E)	10	#5	23'-7"	—
a ₄ (E)	5	#4	8'-0"	—
a ₅ (E)	10	#5	8'-0"	—
a ₆ (E)	4	#4	11'-1"	—
a ₇ (E)	7	#5	11'-1"	—
b(E)	21	#5	5'-8"	—
b ₁ (E)	56	#9	5'-8"	—
b ₂ (E)	17	#4	11'-7"	—
b ₃ (E)	13	#5	11'-7"	—
c(E)	11	#5	2'-5"	┌
c ₂ (E)	11	#5	6'-8"	┌
Bar Splicers			Each	15
Reinforcement Bars, Epoxy Coated			Pounds	2070

* Epoxy grout a₆(E), a₇(E), b₂(E) & b₃(E) bars in 9" min. holes according to Article 584 of the Standard Specifications.
 ** Tilt #5 s(E) bars as required to maintain clearance.
 *** Existing reinforcement extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost shall be included with Concrete Removal.
 **** For information only. All materials required for approach slab are included with Approach Slab Repair (Full Depth).

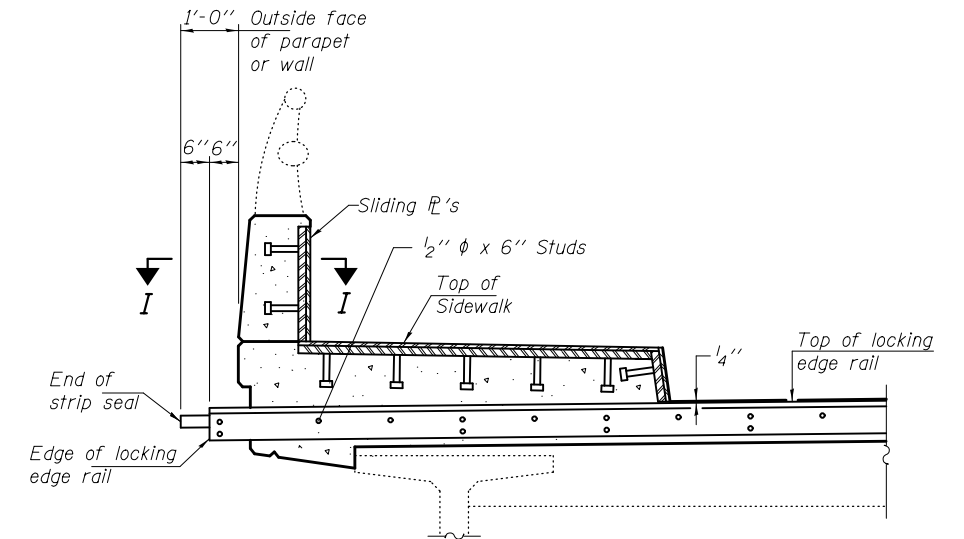




PLAN

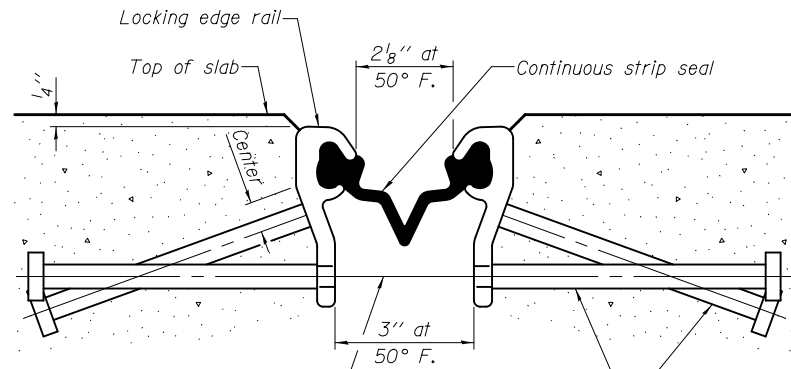


SECTION I-I



TYPICAL END TREATMENT AT SIDEWALK OR MEDIAN

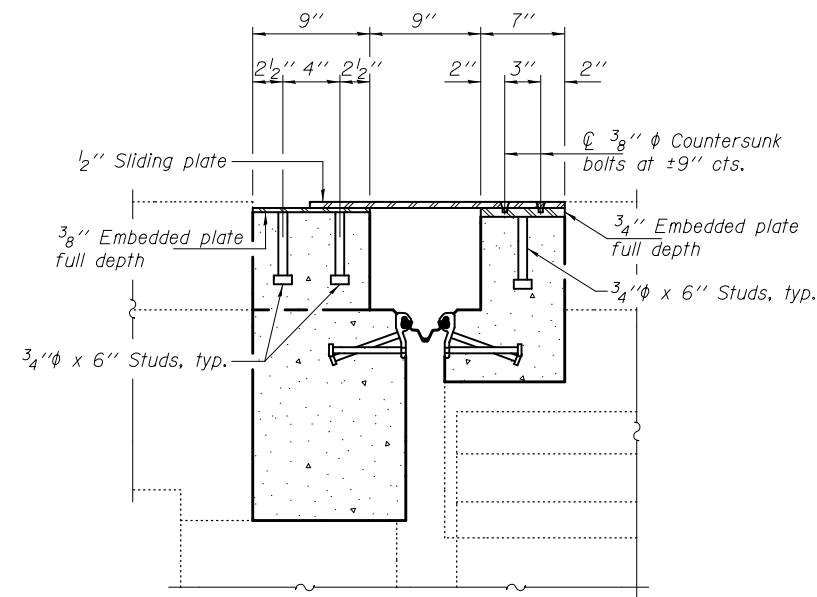
Shorter plates with a single row of studs at 12" cts. may be necessary on medians which are shallower than 9". See manufacturer's recommendation.



7/16" φ holes at 4'-0" cts. for 3/8" φ bolts. All bolts shall be burned, sawed, or chipped off flush with the plates after forms are removed, typ.

Place 1/2" φ x 6" granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded at 1'-0" alt. cts.

SECTION THRU STRIP SEAL JOINT



SECTION THRU SIDEWALK SHOWING SLIDING PLATES

(Reinforcement not shown for clarity.)
See Section F-F on sheet 5 of 8.

Notes:

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the Locking Edge Rails. Open or "bebed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The Locking Edge Rails depicted are conceptual only, except for the minimum dimensions shown. The actual configuration of the Locking Edge Rails and matching strip seal may vary from manufacturer to manufacturer. Flanged edge rails will not be allowed. Locking Edge Rails may be spliced at slope discontinuities.

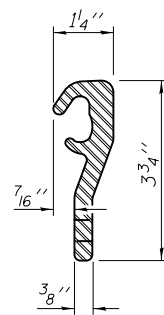
The manufacturer's recommended installation methods shall be followed.

The joint opening and deck dimensions detailed on the superstructure are based on a rolled rail expansion joint. If the Contractor elects to use the welded rail expansion joint, the opening and deck dimensions shall be modified according to the dimensions detailed on this sheet. Required modifications shall be made at no additional cost to the State.

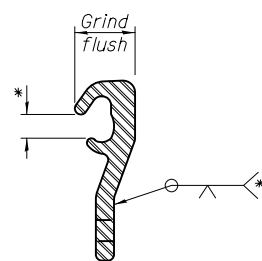
All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

Maximum space between rail segments at stage lines shall be 3/16", sealed with a suitable sealant.

Parapet plates and anchorage studs for skews > 30° included in the cost of Preformed Joint Strip Seal.



LOCKING EDGE RAIL



LOCKING EDGE RAIL SPLICE

* Omit weld at seal opening.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	144

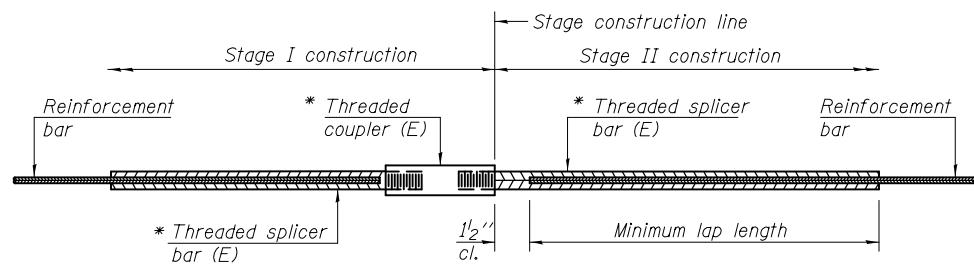
DESIGNED ADY	EXAMINED <i>Timothy A. [Signature]</i>	DATE JANUARY 17, 2014
CHECKED ARS	PASSED <i>[Signature]</i>	REVISIONS
DRAWN Kyle M. Steffen	ACTING ENGINEER OF BRIDGES AND STRUCTURES	
CHECKED ADY ARS		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PREFORMED JOINT STRIP SEAL DETAILS
SN 092-0053

SHEET NO. 7 OF 8 SHEETS

F.A.P. RTE. 729	SECTION (342-1B)I-2	COUNTY VERMILION	TOTAL SHEETS 25	SHEET NO. 12
				CONTRACT NO. 70971
ILLINOIS FED. AID PROJECT				



STANDARD BAR SPLICER ASSEMBLY

Minimum Lap Lengths					
Bar size to be spliced	Table 1	Table 2	Table 3	Table 4	Table 5
3, 4	1'-5"	1'-11"	2'-1"	2'-4"	2'-3"
5	1'-9"	2'-5"	2'-7"	2'-11"	2'-10"
6	2'-1"	2'-11"	3'-1"	3'-6"	3'-4"
7	2'-9"	3'-10"	4'-2"	4'-8"	4'-6"
8	3'-8"	5'-1"	5'-5"	6'-2"	5'-10"
9	4'-7"	6'-5"	6'-10"	7'-9"	7'-5"

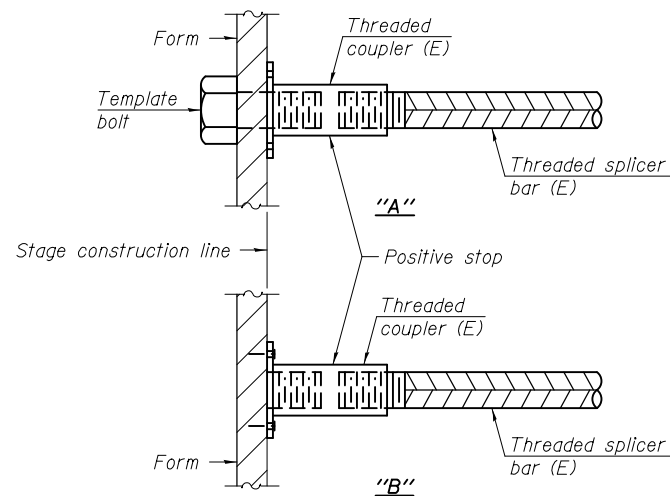
- Table 1: Black bar, 0.8 Class C
- Table 2: Black bar, Top bar lap, 0.8 Class C
- Table 3: Epoxy bar, 0.8 Class C
- Table 4: Epoxy bar, Top bar lap, 0.8 Class C
- Table 5: Epoxy bar, Top bar lap, Class B

Threaded splicer bar length = min. lap length + 1/2" + thread length

* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

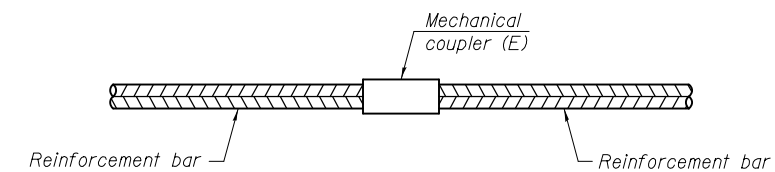
Location	Bar size	No. assemblies required	Table for minimum lap length
W. Abut. (Hatchblock)	#6	4	3
W. Abut. (Deck)	#7	2	3
E. Abut. (Hatchblock)	#6	4	3
E. Abut. (Deck)	#7	2	3
** Approach slab	#4	5	3
** Approach slab	#5	10	3

** For information only. All materials required for approach slab are included with Approach Slab Repair (Full Depth).



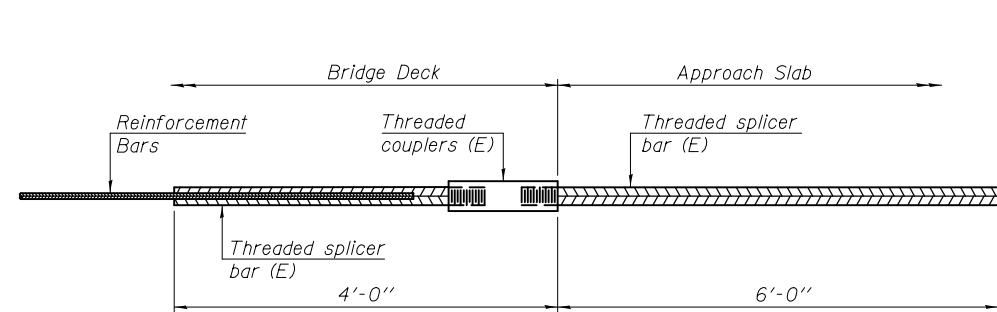
INSTALLATION AND SETTING METHODS

"A": Set bar splicer assembly by means of a template bolt.
 "B": Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E): Indicates epoxy coating.



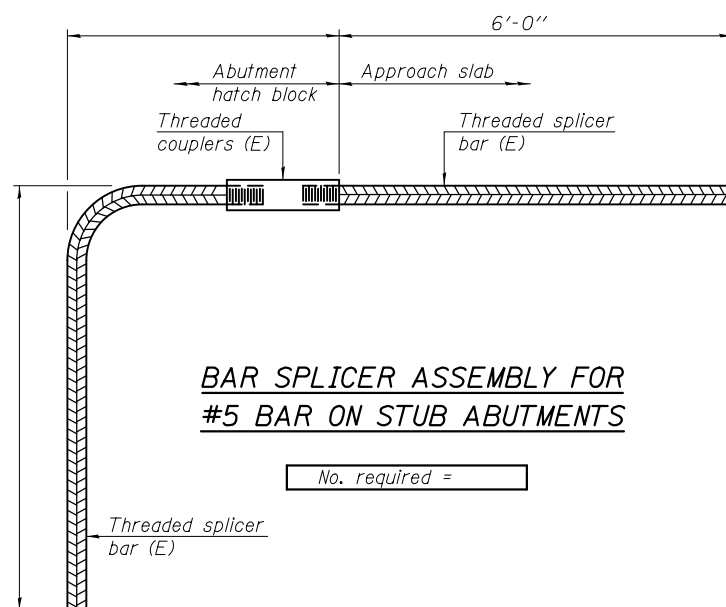
STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required



BAR SPLICER ASSEMBLY FOR #5 BAR ON INTEGRAL OR SEMI-INTEGRAL ABUTMENTS

No. required =



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

No. required =

NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
 See special provision for Mechanical Splicers.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1

7-1-10

DESIGNED ADY	EXAMINED	DATE JANUARY 17, 2014
CHECKED ARS	PASSED	
DRAWN Kyle M. Steffen		
CHECKED ADY ARS		

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
SN 092-0053

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
729	(34Z-1B)I-2	VERMILION	25	13
CONTRACT NO. 70971				

SHEET NO. 8 OF 8 SHEETS

ILLINOIS FED. AID PROJECT

Bench Mark: *A1 southwest corner of bridge, southeast corner of bridge name plate set in hand rail, high point. Elev. 563.54"

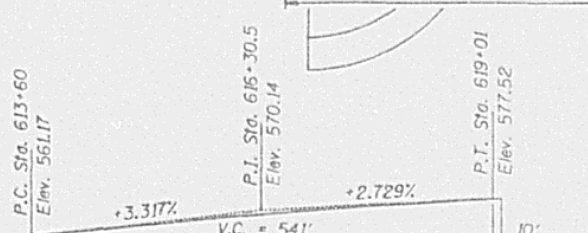
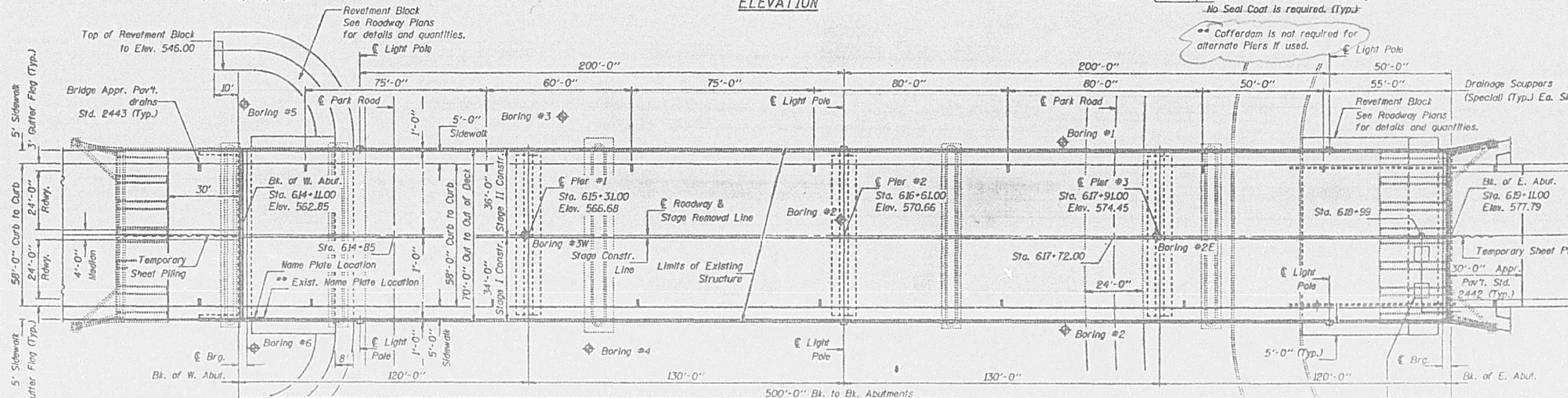
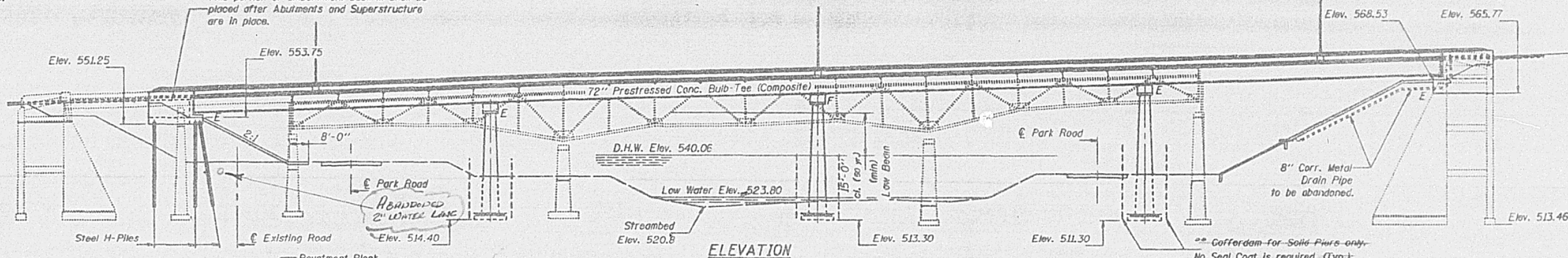
Existing Structure: Constructed in 1947 as F.A. Route 11. Structure is a six span structure; steel truss, plate girder and I-beam bridge on open abutments. The length Bk. to Bk. of Abutts. is 551'-0" with Out to Out width of 69'-6". Stage construction is to be utilized to maintain two lanes traffic at all times. No skew.

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DATE	BY	CHECKED	DATE	SHEET NO.
F.A.P. 729	DBR	VERMILION	04	46
ILLINOIS FED. AID PROJECT				33 SHEETS

No Salvage:

This portion of embankment backfill shall be placed after Abutments and Superstructure are in place.



STATION 616+30.50
REBUILT 199 BY
STATE OF ILLINOIS
F.A.P. RT. 729 SEC. (34Z-1)DBR
F.A. PROJ#F-S7F-12(11)
LOADING HS20
STR. NO. 092-0053

NAME PLATE
See Std. 2113

** Existing Name Plate to be cleaned and relocated. Cost incidental to "Name Plates".

WATERWAY INFORMATION

Drainage Area = 307 sq. mi. Low Grade Elev. 550.72 @ Sta. 608+84.19

Flood	Freq. Yr.	0 C.F.S.	Opening Sq. Ft.	Nat. Exist.	Prop. Exist.	Head - Ft. Exist.	Prop. Exist.	Headwater El.
Design	50	14970	4470	4338	540.06	0.22	0.23	540.28/540.29
Base	100	16890	4935	4756	541.16	0.24	0.25	541.40/541.41
Overtopping	-	-	-	-	-	-	-	-
Max. Calc.	500	21360	5936	5657	543.49	0.26	0.28	543.75/543.77

DESIGN SPECIFICATIONS

AASHTO (1989) & 1990, 1991 Interim Specifications & Seismic Retrofitting Guidelines for Highways Bridges

LOADING HS 20-44

Allow 25#/sq. ft. for future wearing surface.

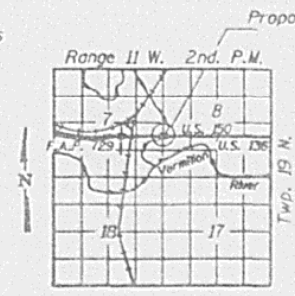
DESIGN STRESSES

FIELD UNITS

f'c = 3,500 psi
fy = 60,000 psi (Reinf.)

PRECAST PRESTRESSED UNITS

f'c = 6,000 psi
f'cl = 5,000 psi
f's = 270,000 psi (1/2" Low-Relaxation Strands)
f'si = 201,960 psi (1/2" Low-Relaxation Strands)



LOCATION SKETCH

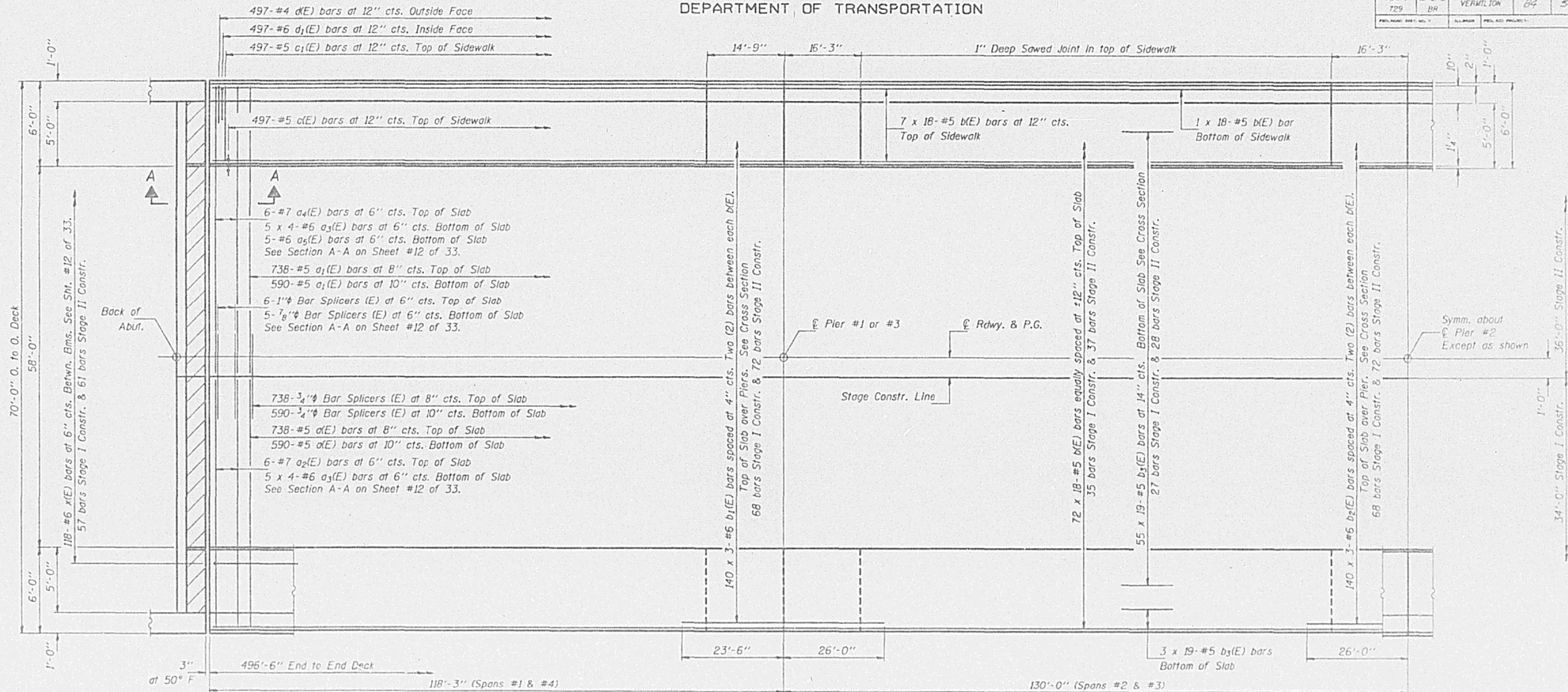
GENERAL PLAN
U.S. ROUTE 150 - MAIN STREET OVER
NORTH FORK VERMILION RIVER
F.A.P. ROUTE 729 SECTION (34Z-1)DBR
STATION 616+30.50
VERMILION COUNTY
STRUCTURE NUMBER 092-0053

DESIGNED: Steve [Signature]
CHECKED: W.S. W.D.C.
DRAWN: John F. Schneller Jr.
APPROVED: Ralph E. Anderson
DATE: FEBRUARY 16 1995



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	SHEETS	SHEET NO.
F.A.P. 729	(342-D) BR	VERMILION	64	54
SHEET NO. 9 33 SHEETS				



MIN. BAR LAPS

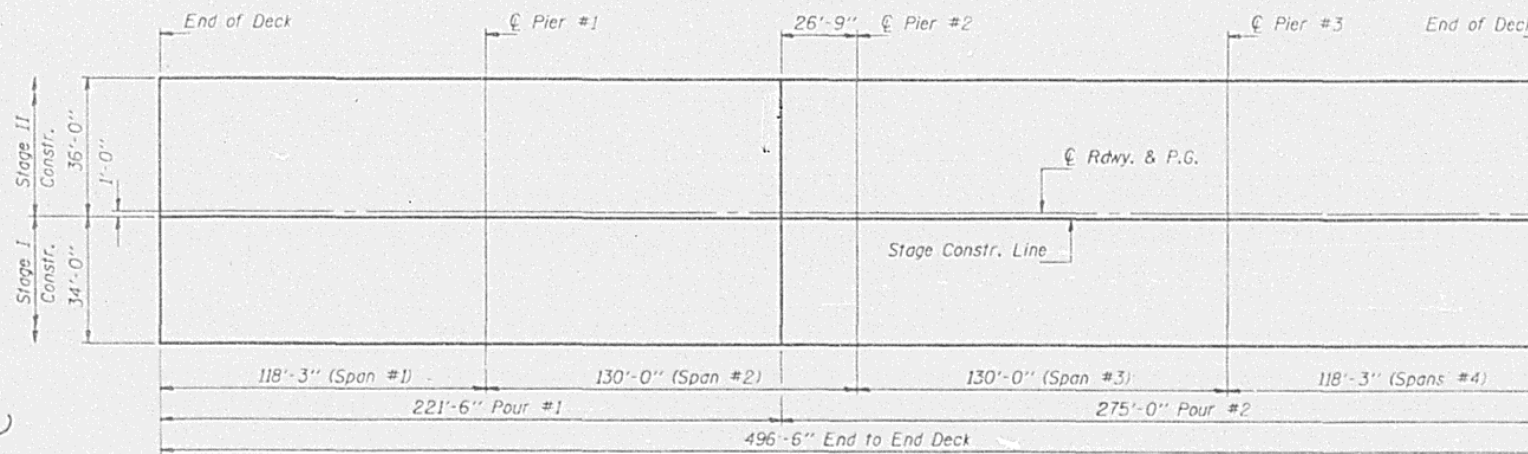
- #5 bar = 2'-2"
- #6 bar = 2'-7"

Notes: See sheet #10 & #11 of 33 for Superstructure Details and Bill of Material.
See Sheet #12 & #13 of 33 for Sections at Abutments and Piers
See Sheet #5 of 33 for Bar Splicer Details.
See Sheet #1 of 33 for Drainage Scupper spacing.
See Sheets #15 & #16 of 33 for Drainage Scupper Details.
Reinforcement bars designated (E) shall be epoxy coated.
Bars indicated thus 20 x 3-#5 etc. indicates 20 lines of bars with 3 lengths per line.

DESIGNED *Steve Johnson*
CHECKED *John F. Schneller Jr.*
DRAWN *John F. Schneller Jr.*
CHECKED *SPM GHA*

EXAMINED *Greg J. Kaspa*
PASSED *Ralph E. Anderson*
APPROVED _____
DIRECTOR OF HIGHWAYS

PARTIAL PLAN



POURING SEQUENCE DETAIL

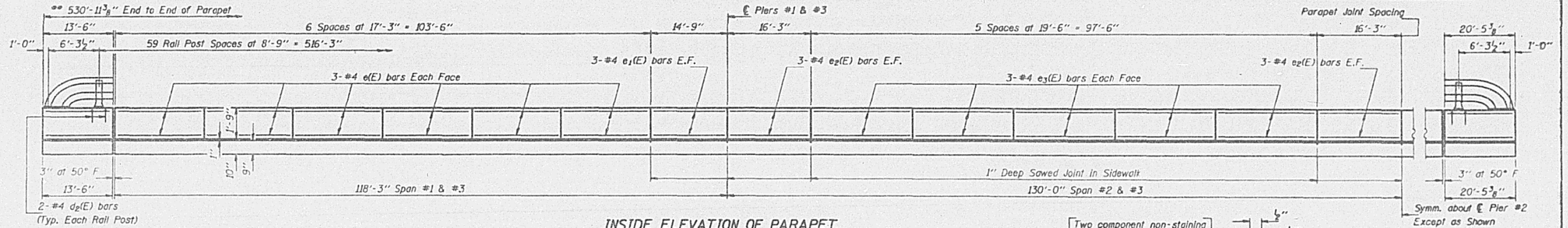
Notes: When the deck pour is stopped for the day at the Transverse Banded Construction Joint in the Deck Pouring Sequence as shown, the next pour shall not be made until both of the following requirements are met:
1.) At least 72 hours shall have elapsed from the end of the previous pour.
2.) The concrete strength shall have attained a minimum modulus of rupture of 650 p.s.i. or a minimum compressive strength of 3500 p.s.i.
The contractor, as an alternative to the pouring sequence may pour the deck simultaneously from each end of the bridge.

SUPERSTRUCTURE
F.A.P. RT. 729 SEC. (342-D)BR
VERMILION COUNTY
STA. 616+30.50

FILE NAME =	USER NAME = ceerlockjd	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	AS BUILT FOR INFORMATION ONLY	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
ce:\pwwork\pwwork\ceerlockjd\0299453\0570971-SHT-DETAILS.dgn		DRAWN -	REVISED -			729	(342-1B) I-2	VERMILION	25	15	
PLOT SCALE = 48.0000' / in.		CHECKED -	REVISED -			CONTRACT NO. 70971					
PLOT DATE = 9/23/2013		DATE -	REVISED -			ILLINOIS FED. AID PROJECT					

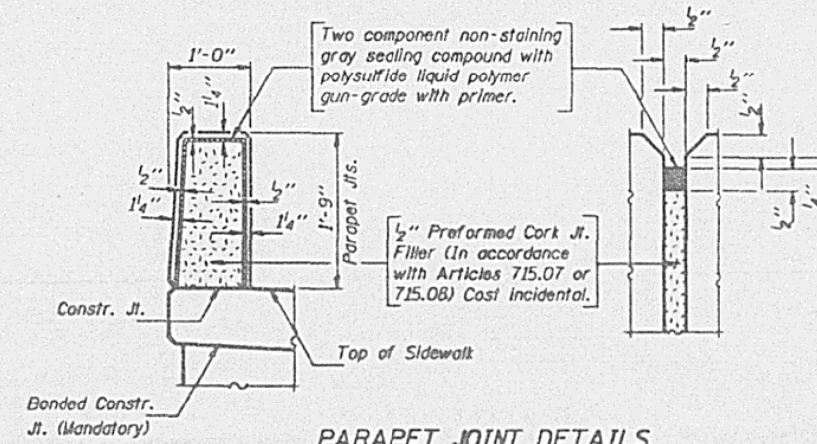
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROJECT NO.	SECTION	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.P. 729	(342-D) BR	VERMILION	84 55	33 SHEETS

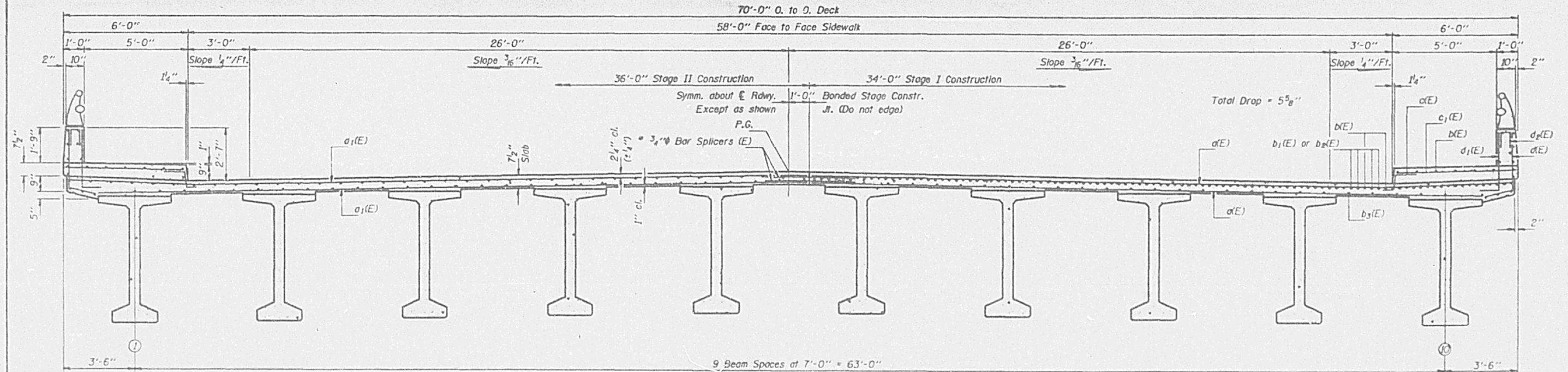


INSIDE ELEVATION OF PARAPET
(Looking North)
** Measured along inside face of Parapet.

Notes: See sheet #11 of 33 for Superstructure Details and Bill of Material.
See Sheet #12 & #13 of 33 for Sections at Abutments and Piers
See Sheet #5 of 33 for Bar Splicer Details.
Reinforcement bars designated (E) shall be epoxy coated.



PARAPET JOINT DETAILS



CROSS SECTION
(Looking East)

* Bar Splicers (E) shall be tied with double the number of ties normally used.

SUPERSTRUCTURE DETAILS
F.A.P. RT. 729 SEC. (342-D) BR
VERMILION COUNTY
STA. 616+30.50

DESIGNED <i>John F. Schneller Jr.</i>	EXAMINED <i>John J. Kasper</i>
CHECKED <i>R. A. Karch</i>	PASSED <i>Ralph E. Anderson</i>
DRAWN <i>John F. Schneller Jr.</i>	APPROVED _____
CHECKED <i>J.P.L.</i>	DIRECTOR OF HIGHWAYS

FILE NAME =	USER NAME = ceerlockjd	DESIGNED -	REVISED -
et:\pwork\pwork\ceerlockjd\0299453\0570971-SHT-DETAILS.dgn		DRAWN -	REVISED -
	PLOT SCALE = 48.0000 / in.	CHECKED -	REVISED -
	PLOT DATE = 9/23/2013	DATE -	REVISED -

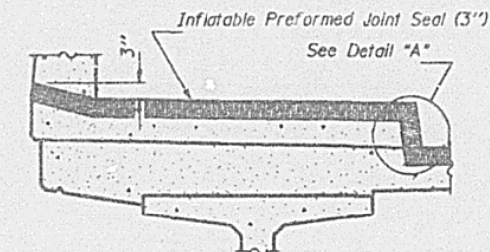
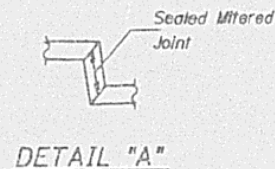
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

AS BUILT FOR INFORMATION ONLY			
SCALE:	SHEET	OF SHEETS	STA. TO STA.

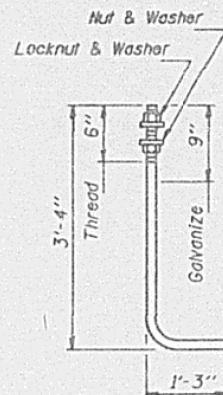
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
729	(342-1B) I-2	VERMILION	25	16
CONTRACT NO. 70971				
ILLINOIS FED. AID PROJECT				

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

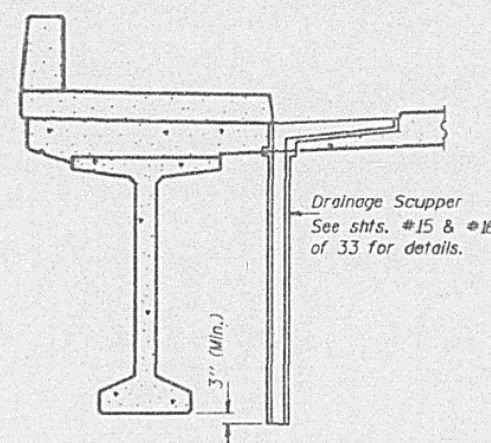
PROJECT NO.	SECTION	COUNTY	SHEET NO.	TOTAL SHEETS
F.A.P. 729	(34Z-1B)	VERMILION	11	33



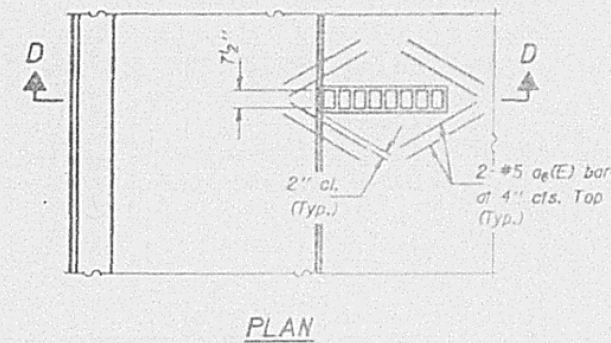
TYPICAL END OF SEAL TREATMENT



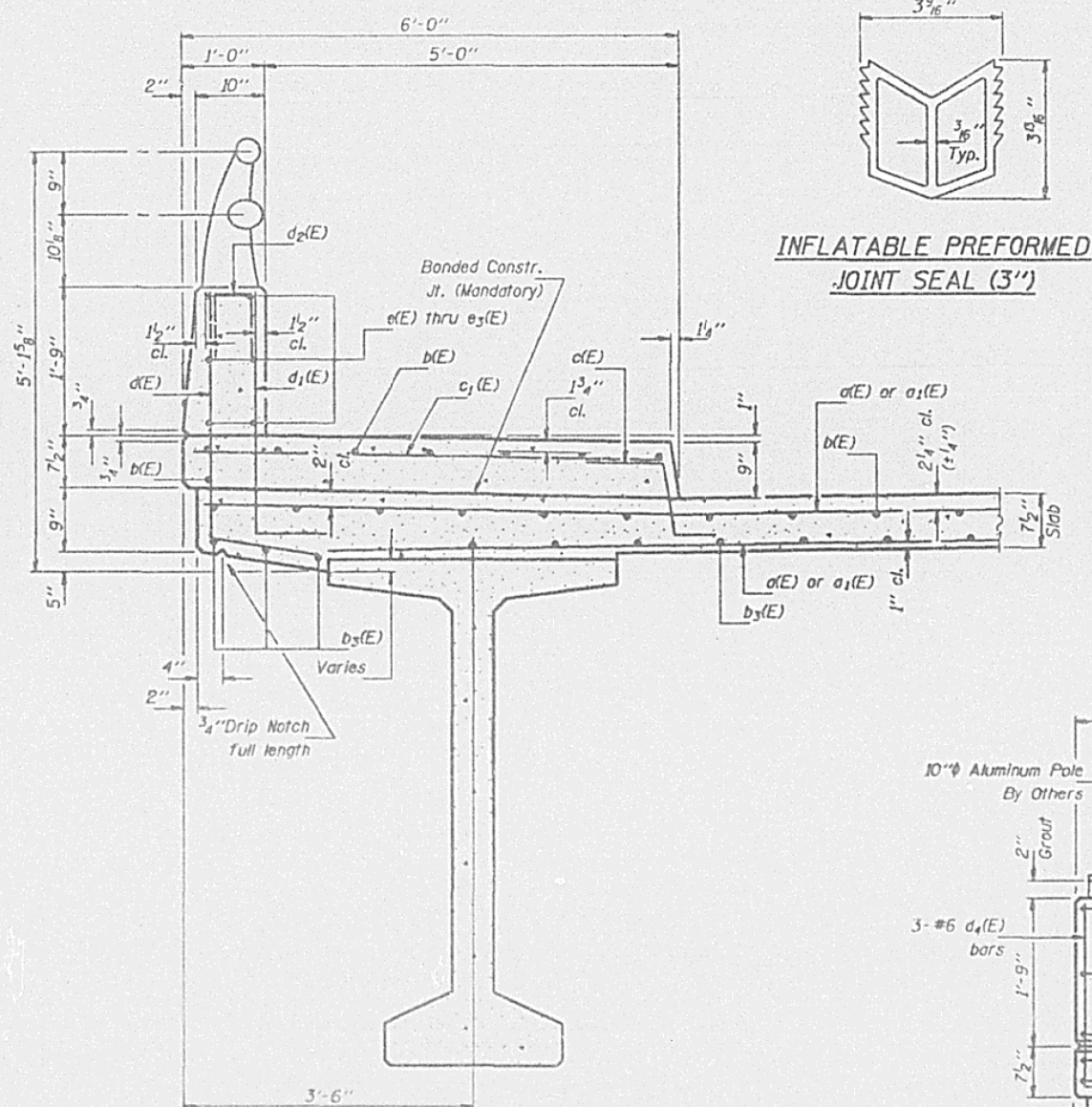
1/4" ANCHOR BOLT



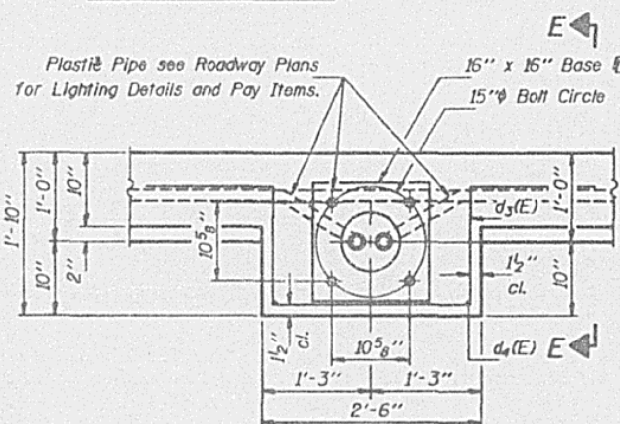
SECTION D-D



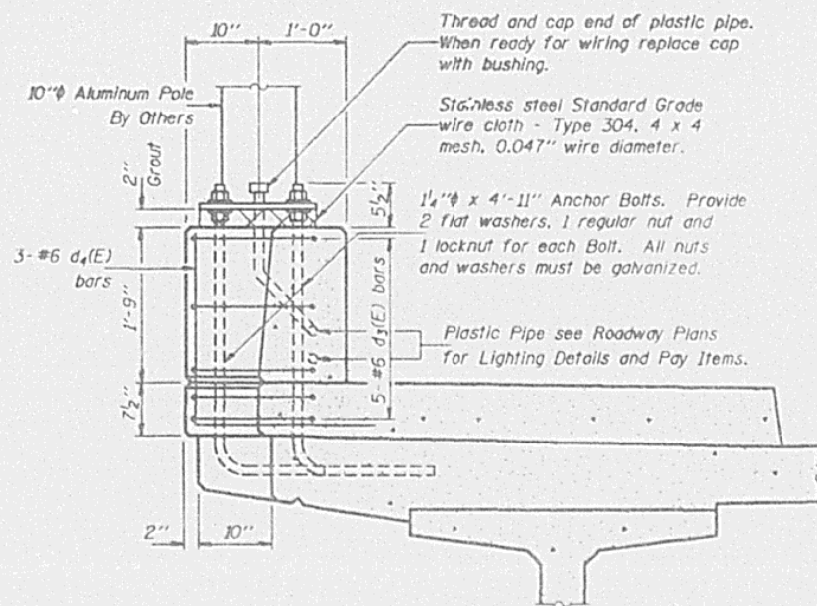
PLAN



SECTION THRU SIDEWALK

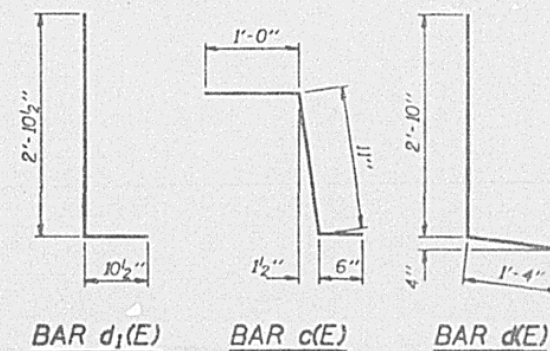


PLAN



SECTION E-E

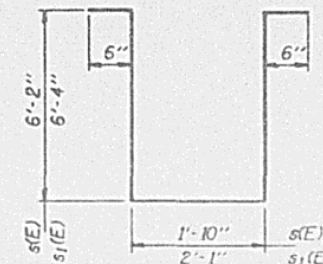
Cost of anchor bolts, nuts, washers and plastic pipe is incidental.



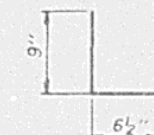
BAR d₁(E)

BAR c(E)

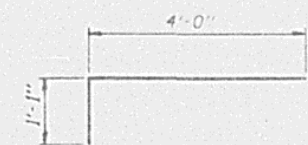
BAR d(E)



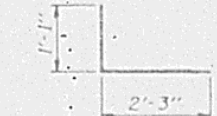
BARS s(E) & s₁(E)



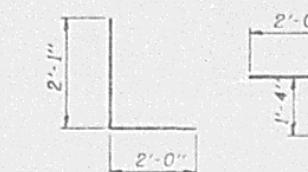
BAR d₂(E)



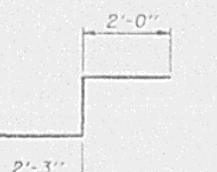
BAR x(E)



BAR x₁(E)



BAR d₄(E)



BAR d₃(E)

SUPERSTRUCTURE
BILL OF MATERIAL

Bar No.	Size	Length	Shape
a(E)	1328 #5	33'-7"	
a ₁ (E)	1328 #5	35'-7"	
a ₂ (E)	12 #7	33'-7"	
a ₃ (E)	80 #6	5'-9"	
a ₄ (E)	12 #7	35'-7"	
a ₅ (E)	10 #6	4'-0"	
a ₆ (E)	112 #5	2'-0"	
b(E)	1584 #5	29'-9"	
b ₁ (E)	840 #6	18'-3"	
b ₂ (E)	420 #6	19'-1"	
b ₃ (E)	1159 #5	28'-3"	
c(E)	994 #5	2'-5"	
c ₁ (E)	994 #5	5'-8"	
d(E)	994 #4	4'-2"	
d ₁ (E)	994 #6	3'-9"	
d ₂ (E)	240 #4	2'-0 1/2"	
d ₃ (E)	30 #6	8'-11"	
d ₄ (E)	18 #6	4'-1"	
e(E)	144 #4	17'-0"	
e ₁ (E)	24 #4	14'-6"	
e ₂ (E)	48 #4	15'-0"	
e ₃ (E)	120 #4	16'-3"	
m(E)	192 #4	6'-3"	
m ₁ (E)	48 #6	4'-7"	
m ₂ (E)	30 #6	6'-2"	
m ₃ (E)	24 #4	4'-1"	
m ₄ (E)	5 #6	3'-2"	
s(E)	72 #4	15'-2"	
s ₁ (E)	36 #4	15'-9"	
x(E)	236 #6	5'-1"	
x ₁ (E)	236 #6	3'-4"	
Reinforcement Bars (Epoxy Coated)		Lbs.	244,170
Class X Concrete Superstructure		Cu. Yd.	1,259

Reinforcement bars designated (E) shall be epoxy coated.

SUPERSTRUCTURE DETAILS
F.A.P. RT. 729 SEC. (34Z-1B)
VERMILION COUNTY
STA. 616+30.50

DESIGNED: Steven Nguyen
CHECKED: A.R. Schuch
DRAWN: John F. Schueller Jr.
CHECKED: SPM

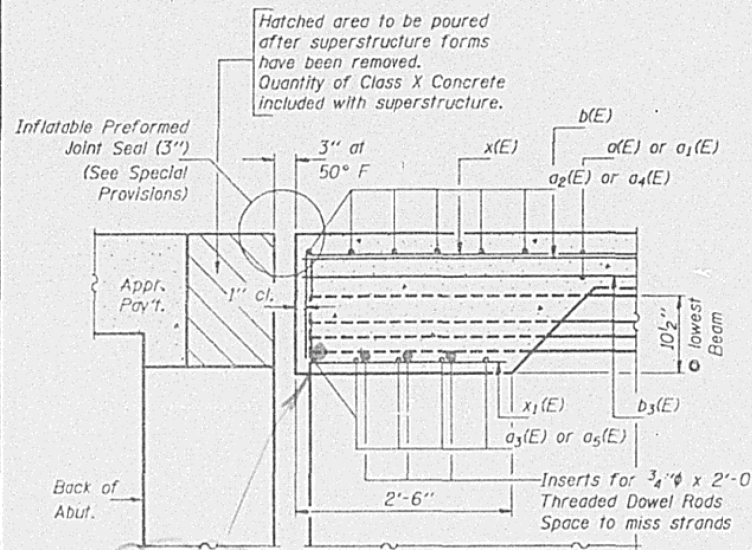
EXAMINED: [Signature]
PASSED: [Signature]
APPROVED: [Signature]

Feb 13 1993

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROUTE NO.	SECTION	COUNTY	STA.	SHEET NO.
F.A.P. 729	(34Z-D) BR	VERMILION	64	57
33 SHEETS				

TYPICAL - ALL BEAMS - ALL SPANS:
#5 BENT BARS USED AS STIRRUPS WHERE FILLETS EXCEED 3"



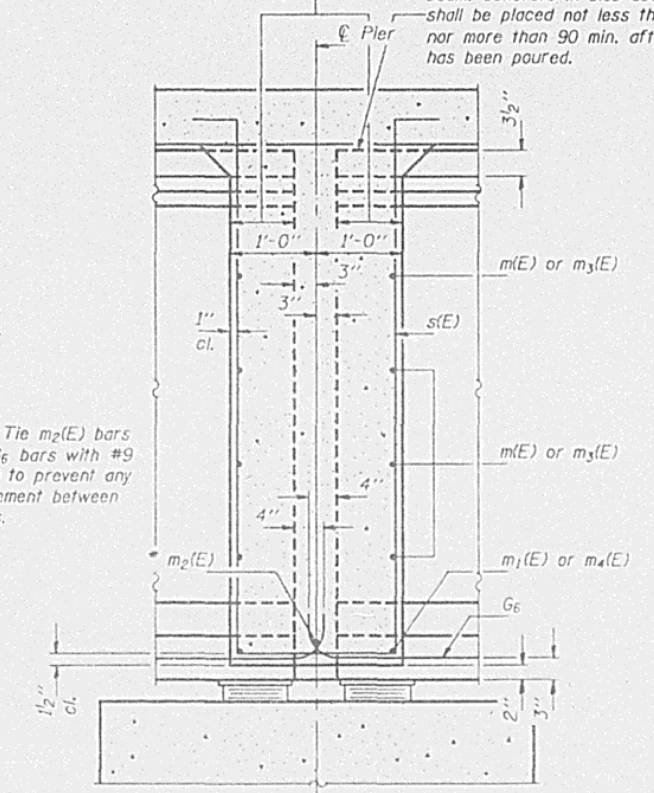
SECTION A-A

ADDITIONAL #3(E) BAR

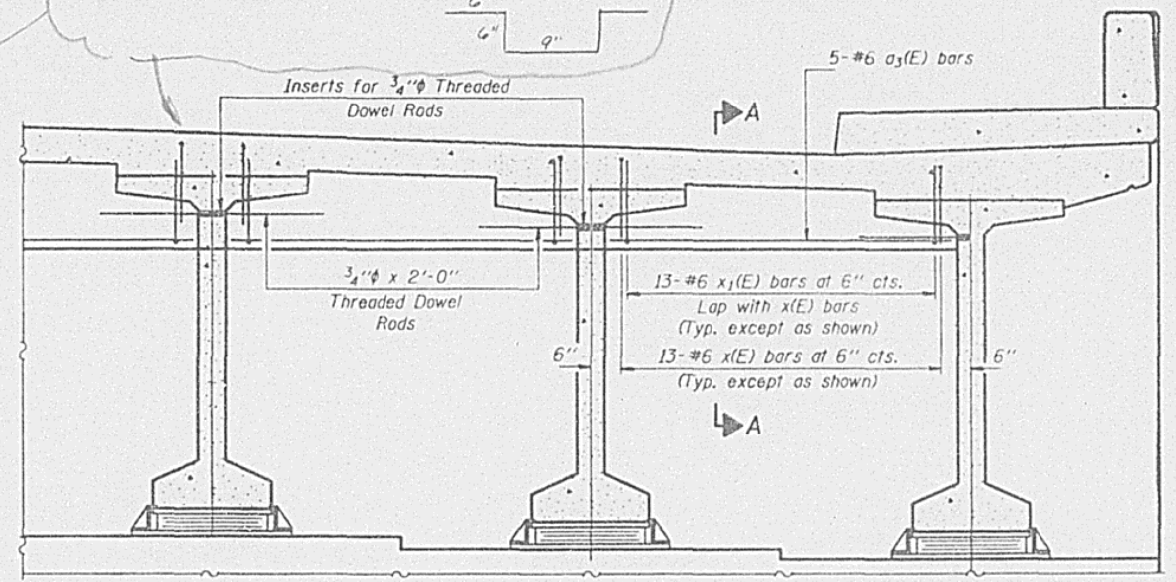
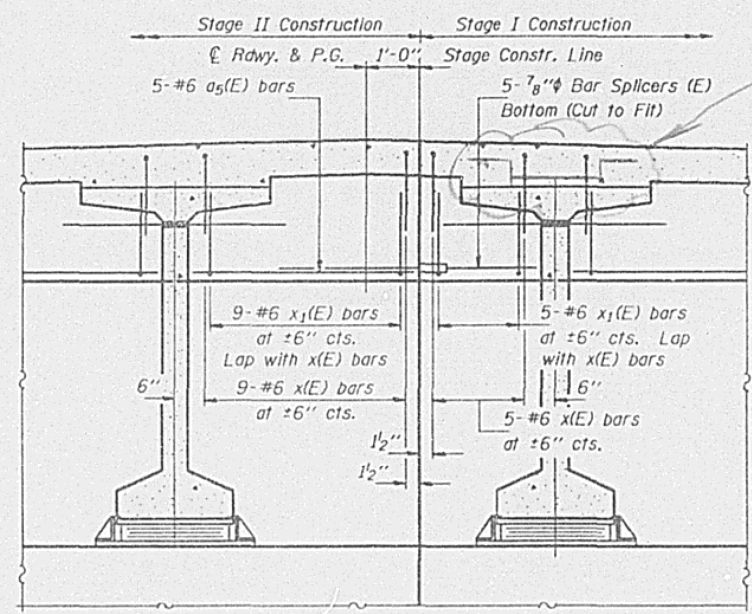
Roofing Felt shall be Bonded to side of beams embedded into Diaphragm. Cost Incidental

Pour diaphragm flush with Top of beam. Concrete in slab above this line shall be placed not less than 45 min. nor more than 90 min. after diaphragm has been poured.

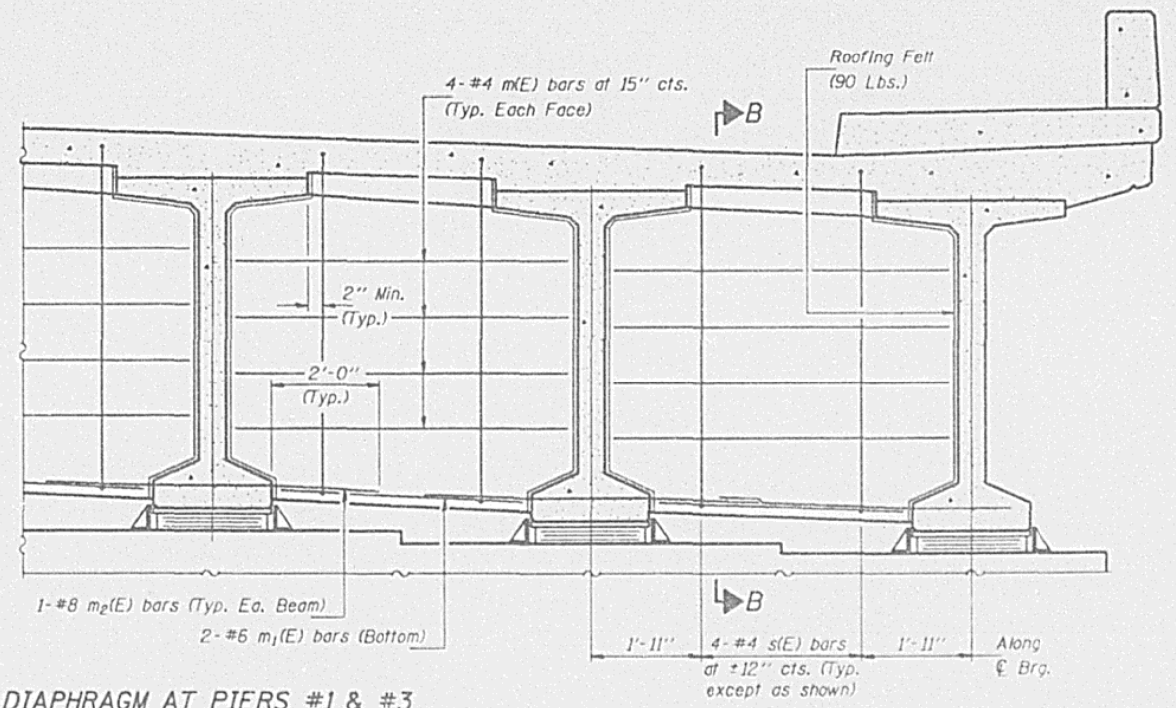
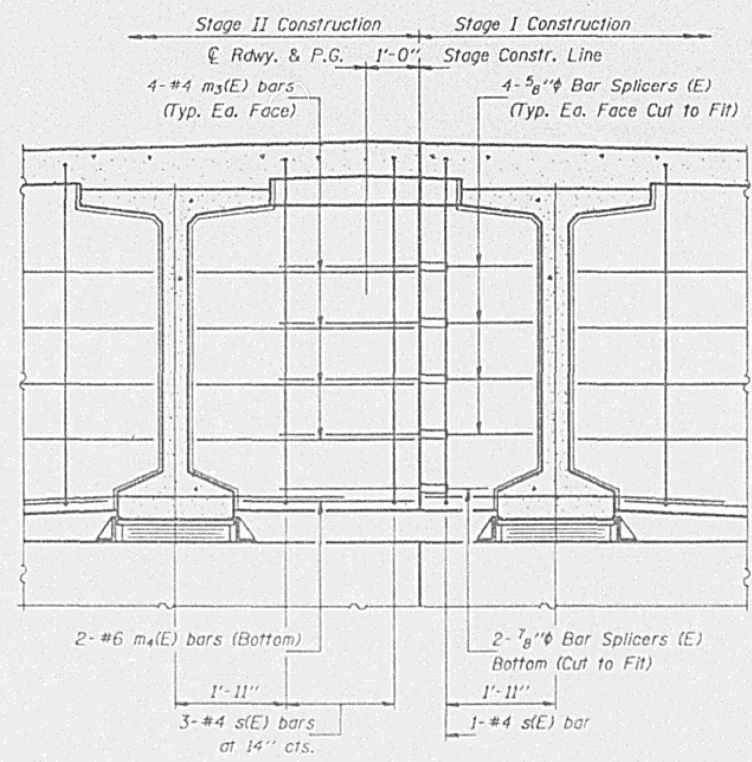
* Tie m2(E) bars to G6 bars with #9 wire to prevent any movement between bars.



SECTION B-B



DIAPHRAGM AT ABUTMENTS



DIAPHRAGM AT PIERS #1 & #3

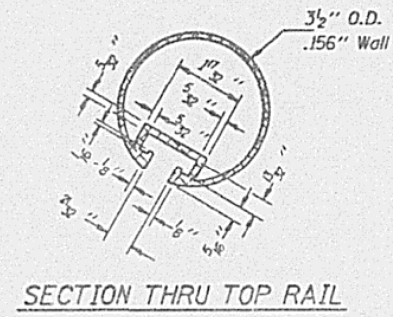
Notes: Reinforcement bars shown on this sheet are included in Bill of Material on sheet #11 of 33. Class X Concrete in Diaphragms is included with "Class X Concrete Superstructure" on sheet #11 of 33. For Inflatable Preformed Joint Details see sheet #11 of 33. For details of s(E), x(E) and x1(E) bars see sheet #11 of 33. For Bar Splicer details see sheet #5 of 33.

DESIGNED <i>Stan Nizajic</i>	EXAMINED <i>Orsi J. Kapor</i>
CHECKED <i>Mike R. Kibben</i>	PASSED <i>Ralph E. Anderson</i>
DRAWN <i>John F. Scheller Jr.</i>	APPROVED _____
CHECKED <i>SPW GHA</i>	DIRECTOR OF HIGHWAYS

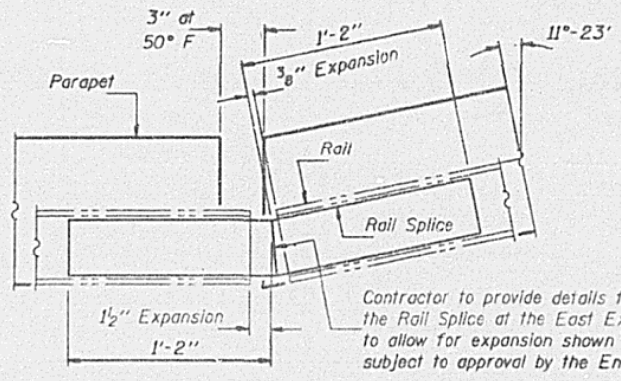
DIAPHRAGM DETAILS
F.A.P. RT. 729 SEC. (34Z-D)BR
VERMILION COUNTY
STA. 616+30.50

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

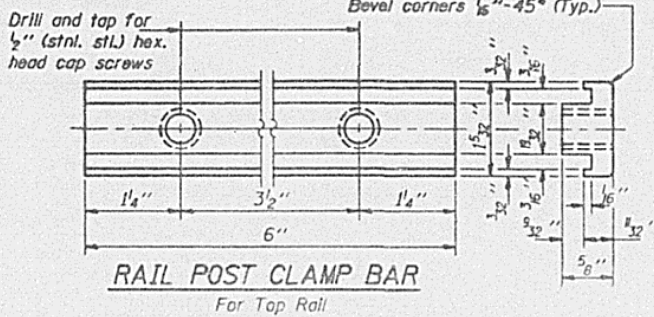
PROJECT NO.	SECTION	COUNTY	DATE	SHEET NO.	SHEET TOTAL
F.A.P. 729	34Z-D BR	VERMILION	64	59	33 SHEETS



SECTION THRU TOP RAIL

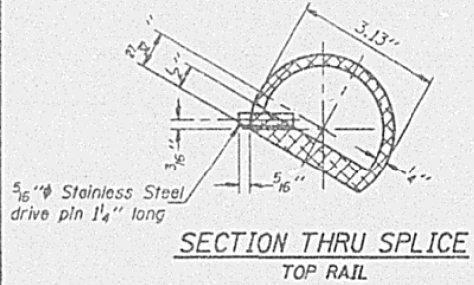


RAIL SPLICE DETAIL AT EAST EXPANSION JOINT

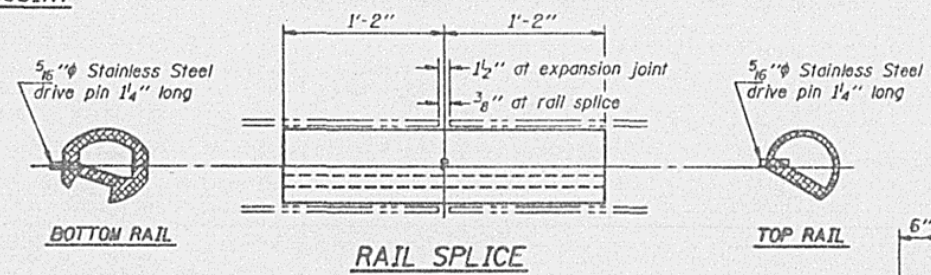


RAIL POST CLAMP BAR
For Top Rail

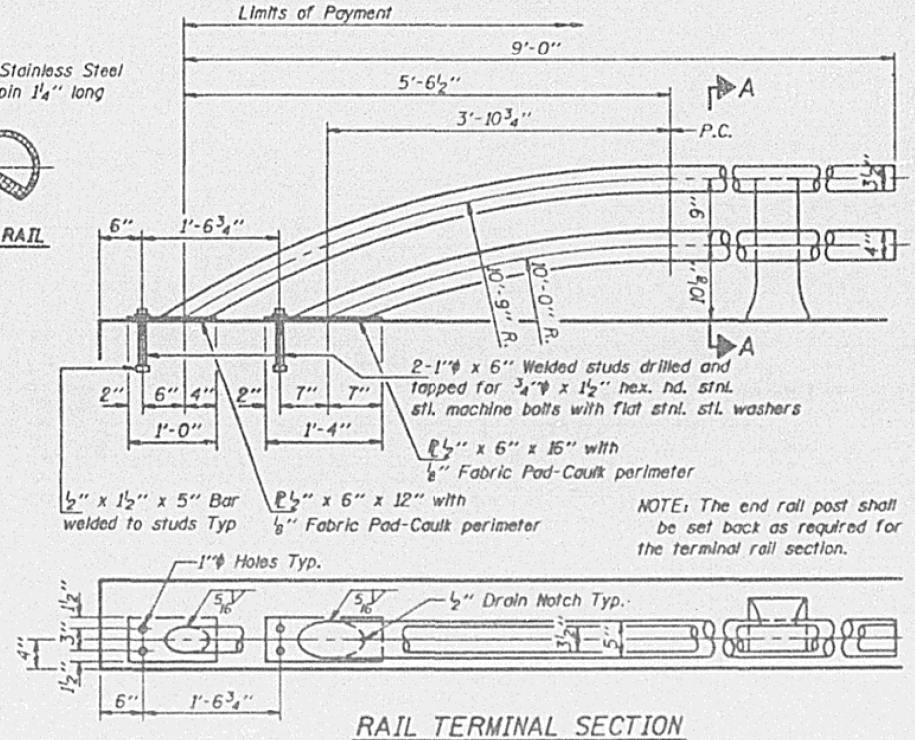
Notes: All Posts shall be normal to parapet.
All Aluminum Alloy Extruded Rail shall be supplied in modular lengths of 30 feet, except at the end of bridge or over open joints in bridge deck where the rail shall be attached to a minimum of 2 posts. If the rail is on a horizontal curve of 2300 foot radius or less, the modular lengths may be reduced but shall be attached to a minimum of 2 posts.
All joints in rail shall be spliced per detail.
Provide 1-1/8" and 2-1/8" Aluminum Shims for 25% of the Posts.
Rail elements shall be parallel to Grade-high spots will be ground and low spots shimmed.
Railing shall be in accordance with Section 508 of the Standard Specifications, except as noted, and will be paid for at the contract unit price per lineal foot for ALUMINUM RAILING, TYPE L.
Aluminum alloy rail shall conform to ASTM B221 alloy 6061-T6 or 6351-T5 with min. yield 35 ksi, min. tensile 38 ksi, and elongation of 10% in 2 inches.



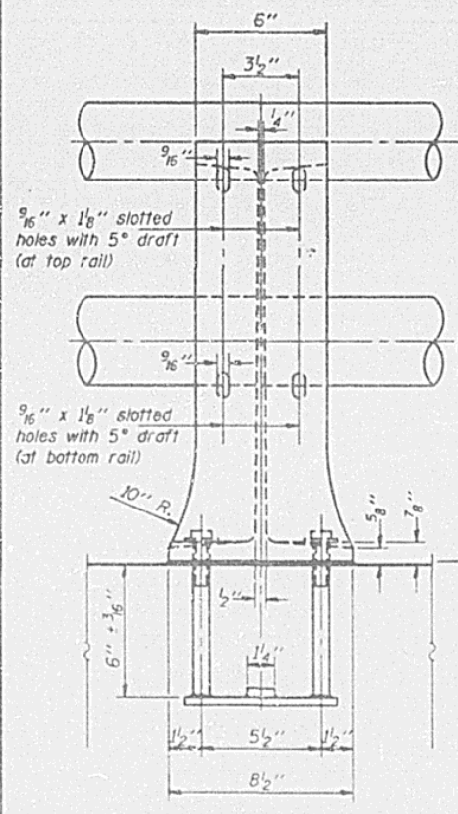
SECTION THRU SPLICE
TOP RAIL



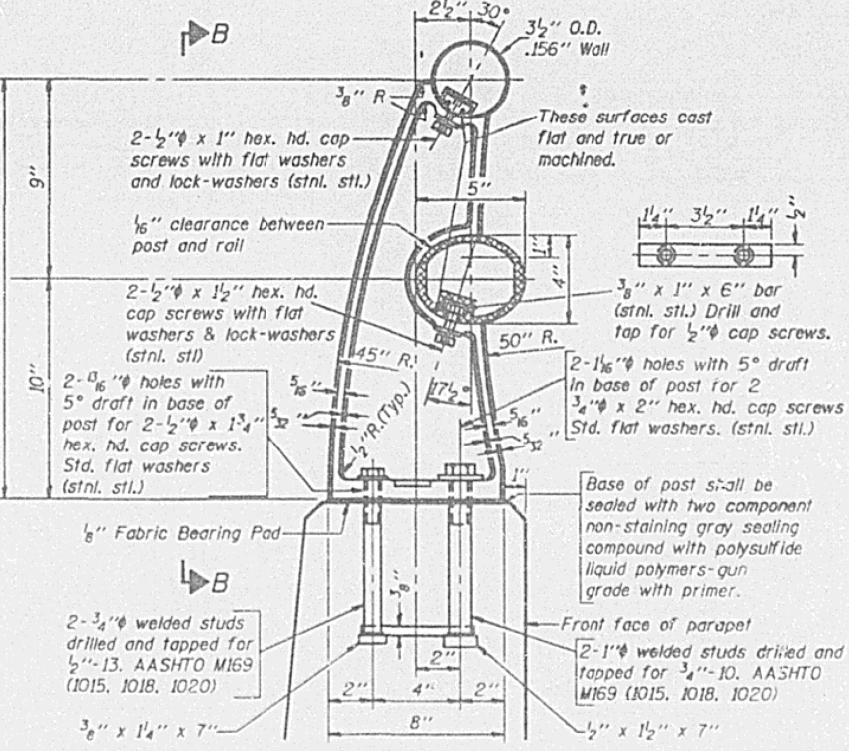
RAIL SPLICE



RAIL TERMINAL SECTION

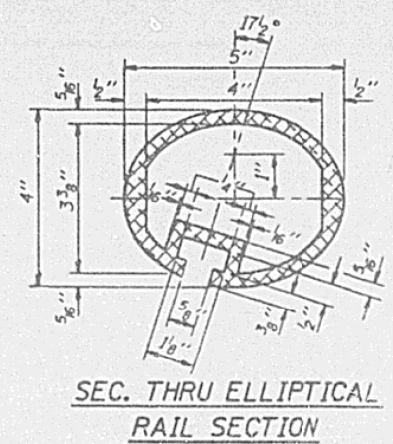


VIEW B-B

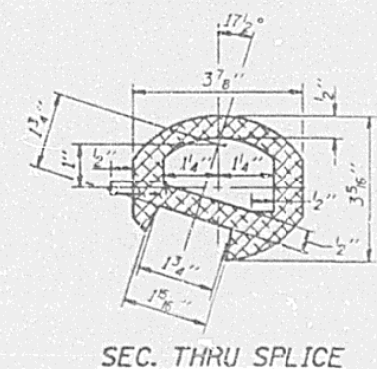


SECTION A-A

RAIL POST DETAILS



SEC. THRU ELLIPTICAL
RAIL SECTION



SEC. THRU SPLICE

DESIGNED *Steven Myerson*
CHECKED *H.A. Albrecht*
DRAWN *John F. Schneller Jr.*
CHECKED *SPM*

EXAMINED *Raj D. Kava*
DESIGNER OF PUBLIC DESIGN
PREPARED *Ralph E. Walker*
ENGINEER OF BRIDGES AND STRUCTURES
APPROVED _____
DIRECTOR OF HIGHWAYS

Feb 18 1973

R-20 12-31-87

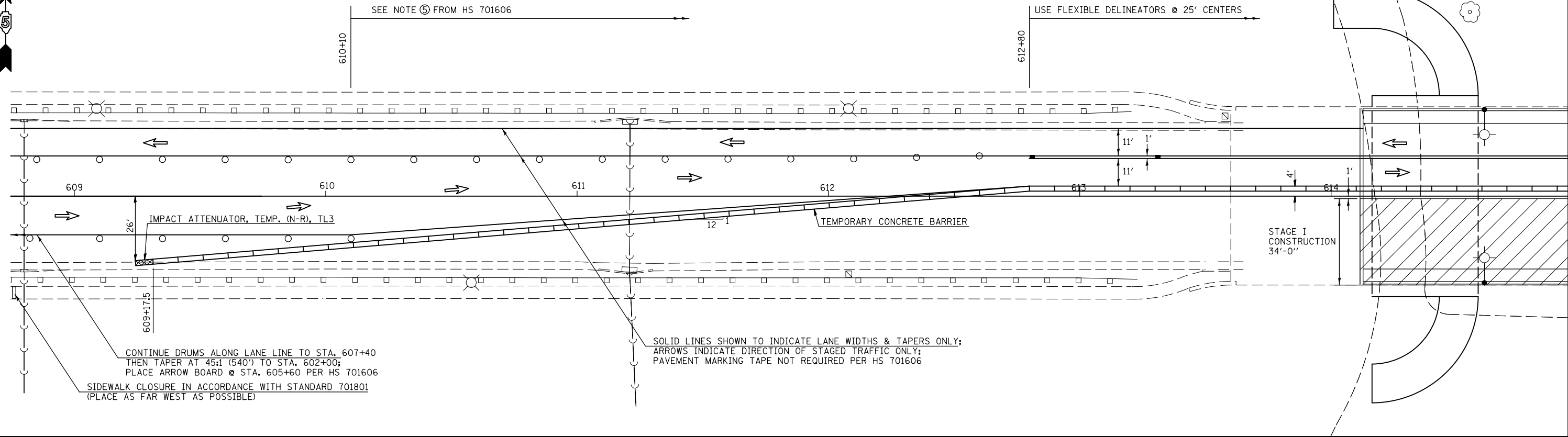
BILL OF MATERIAL

Item	Unit	Quantity
Aluminum Railing, Type L	Lin. Ft.	1058

TYPE L
ALUMINUM RAILING
F.A.P. RT. 729 SEC. (34Z-1)BR
VERMILION COUNTY
STA. 616+30.50



STAGE I TRAFFIC CONTROL



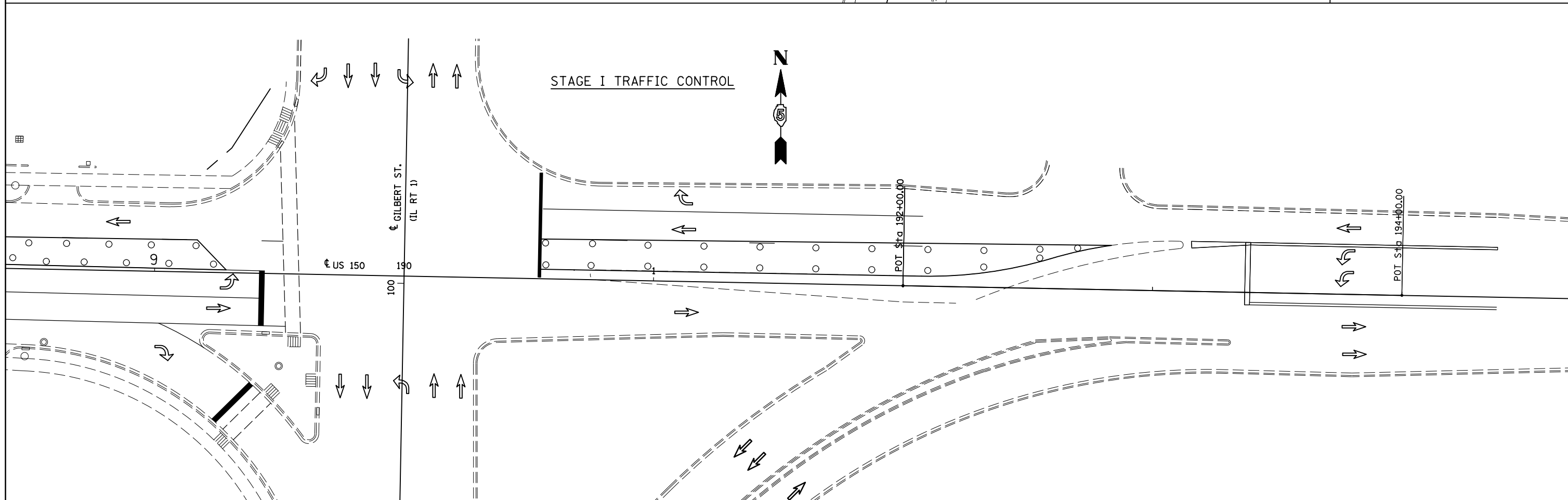
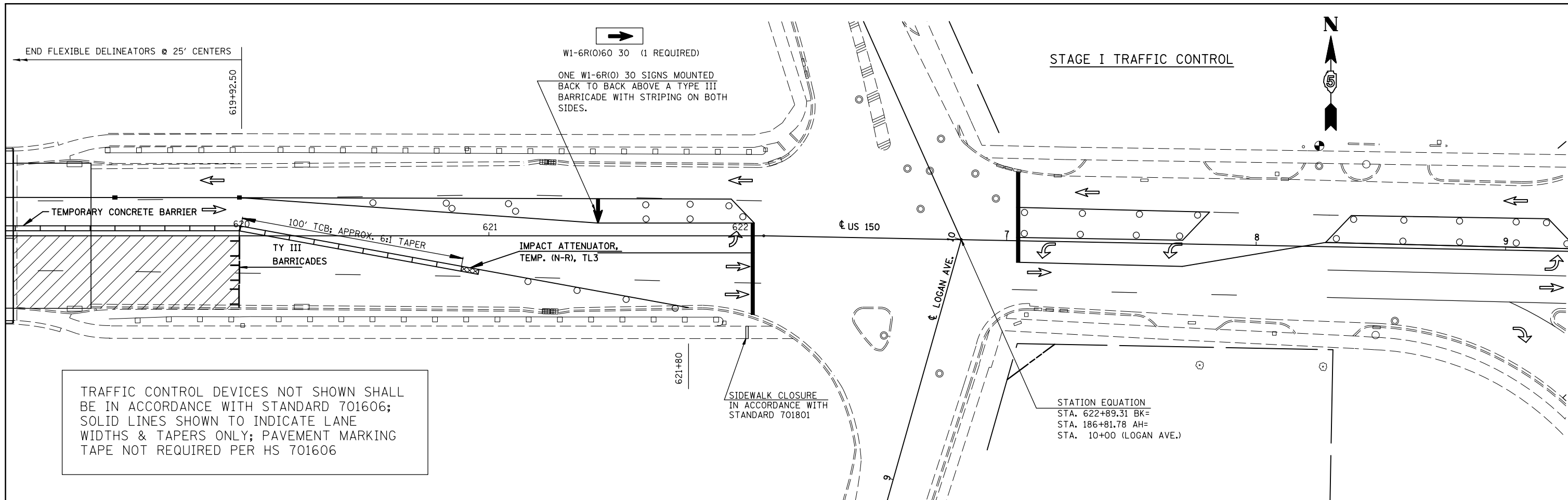
TRAFFIC CONTROL DEVICES NOT SHOWN SHALL BE IN ACCORDANCE WITH STANDARD 701606; SOLID LINES SHOWN TO INDICATE LANE WIDTHS & TAPERS ONLY; PAVEMENT MARKING TAPE NOT REQUIRED PER HS 701606

FILE NAME =	USER NAME = ceerlockjd	DESIGNED - TJB	REVISED - 09/20/2013
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Default	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 9/23/2013	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGE I TRAFFIC CONTROL			
SCALE:	SHEET	OF	SHEETS
	STA.		TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
729	(34Z-1B) I-2	VERMILION	25	20
CONTRACT NO. 70971				
ILLINOIS FED. AID PROJECT				



FILE NAME =	USER NAME = ceerlockjd	DESIGNED - TJB	REVISED - 09/20/2013
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Default	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 9/23/2013	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

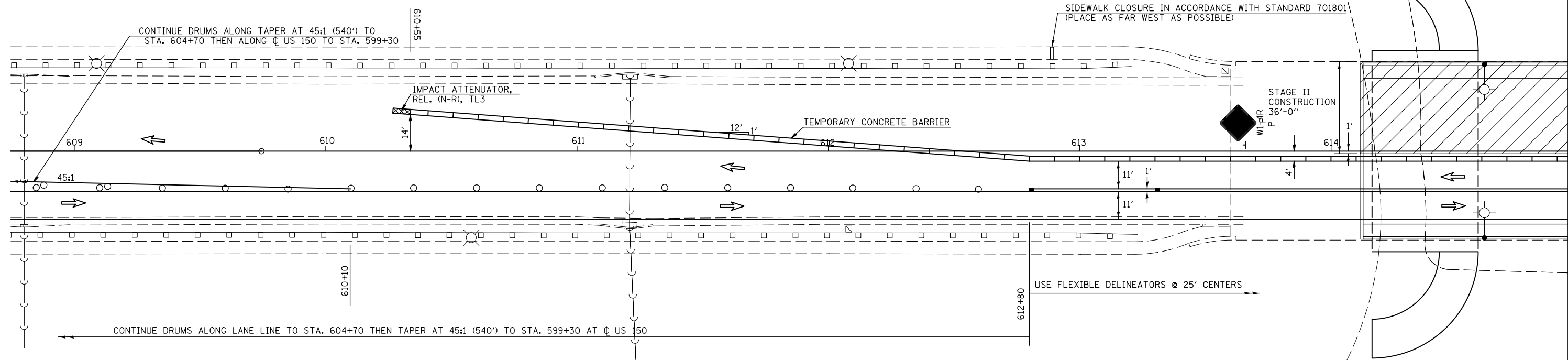
STAGE I TRAFFIC CONTROL				
SCALE:	SHEET	OF	SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
729	(342-1B) I-2	VERMILION	25	21
CONTRACT NO. 70971				
ILLINOIS FED. AID PROJECT				

TRAFFIC CONTROL DEVICES NOT SHOWN SHALL BE IN ACCORDANCE WITH STANDARD 701606; SOLID LINES SHOWN TO INDICATE LANE WIDTHS & TAPERS ONLY; PAVEMENT MARKING TAPE NOT REQUIRED PER HS 701606



STAGE II TRAFFIC CONTROL

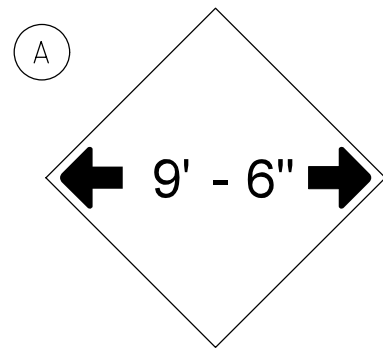


FILE NAME =	USER NAME = ceerlockjd	DESIGNED - TJB	REVISED - 09/20/2013
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Default	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 9/23/2013	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGE II TRAFFIC CONTROL				
SCALE:	SHEET	OF	SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
729	(342-1B) I-2	VERMILION	25	22
CONTRACT NO. 70971				
ILLINOIS FED. AID PROJECT				

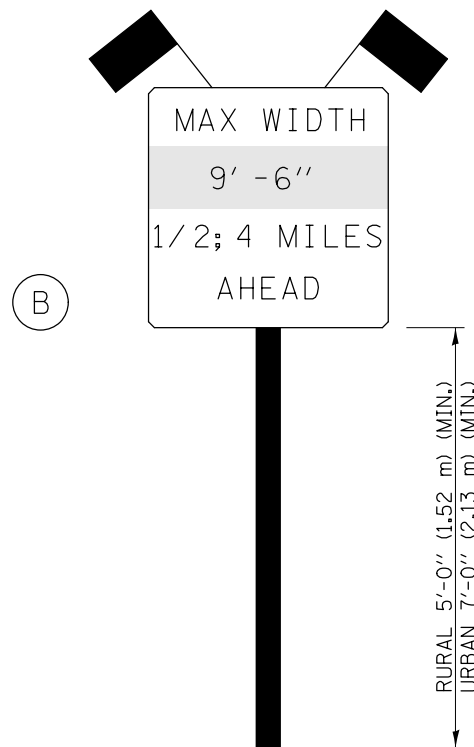


W12-2(0)-48"x48"(1200x1200)

SIGN (A) 2 SIGNS - W12-2(0)-48"x48"(1200x1200) ARE TO BE PLACED AS SHOWN IN THE PLANS OR AS DIRECTED BY THE ENGINEER.

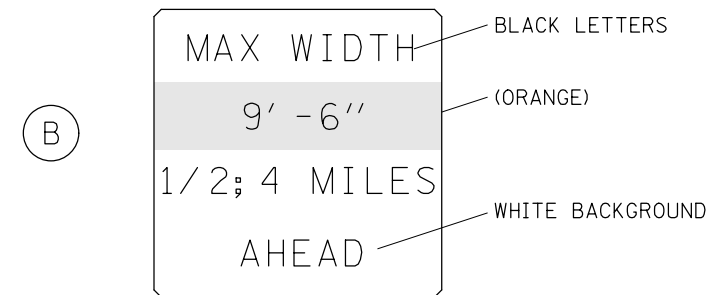
SIGN (B) 2 SIGNS - (SIGN PANEL, TYPE II) AS SHOWN ARE TO BE PLACED AS SHOWN IN THE PLANS OR AS DIRECTED BY THE ENGINEER.

- SIGN (1)
- SIGN (2)
- SIGN (3)
- SIGN (4)



(B)

SIGN PANEL, TYPE II



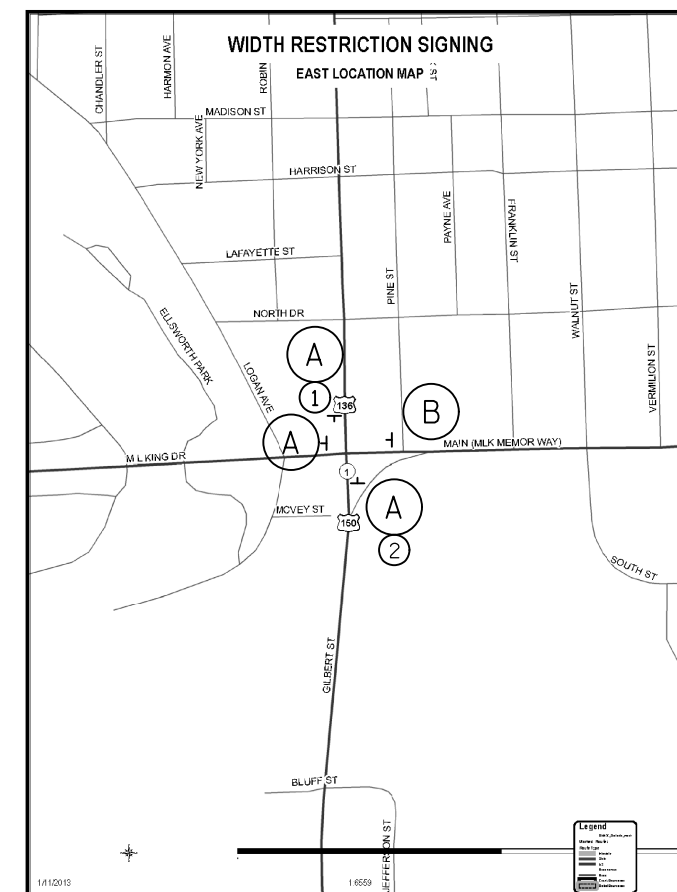
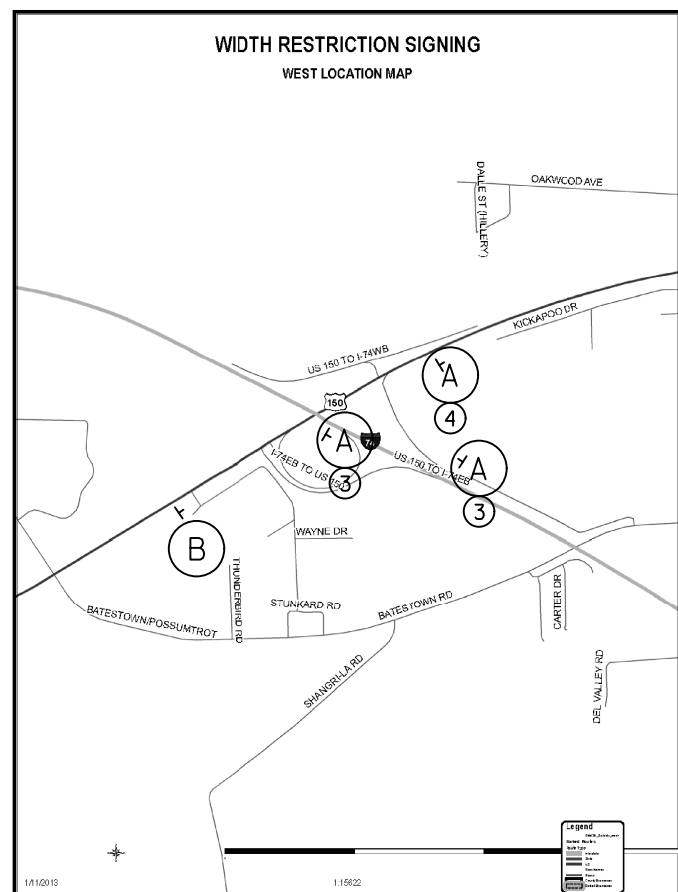
(B)

W12-I103(0)-48"x48"(1200x1200)
"D" LETTERS/NUMBERS



GENERAL NOTES

1. ALL TRAFFIC CONTROL DEVICES SHALL BE FURNISHED, ERECTED AND MAINTAINED BY THE CONTRACTOR.
2. ALL (B) SIGNS SHALL HAVE FLAGS INSTALLED UNLESS OTHERWISE DIRECTED.
3. LOCATIONS OF TRAFFIC CONTROL DEVICES MAY BE ADJUSTED BY THE ENGINEER.
4. ALL TRAFFIC CONTROL SHOWN ON THIS SHEET SHALL BE PAID FOR AT THE CONTRACT LUMP SUM PRICE FOR WIDTH RESTRICTION SIGNING.
5. ALL SIGNS SHALL BE POST MOUNTED UNLESS OTHERWISE DIRECTED.
6. ALL SIGNS SHOWN ORANGE (O) SHALL BE FLUORESCENT ORANGE.
7. ALL SIGNS SHOWN SHALL CONSIST OF THE CURRENT RETROREFLECTIVE SHEETING REQUIREMENTS AS OUTLINED IN SECTION 1106.01 OF THE STANDARD SPECIFICATIONS BOOK.



FILE NAME =	USER NAME = ceerlockjd	DESIGNED - TJB	REVISED - 09/20/2013
ei:\pw\work\p\idot\ceerlockjd\0299453\0570971-5ht-Staging.dgn		DRAWN -	REVISED -
	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -
MODELNAME	PLOT DATE = 9/23/2013	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

WIDTH RESTRICTION SIGNING

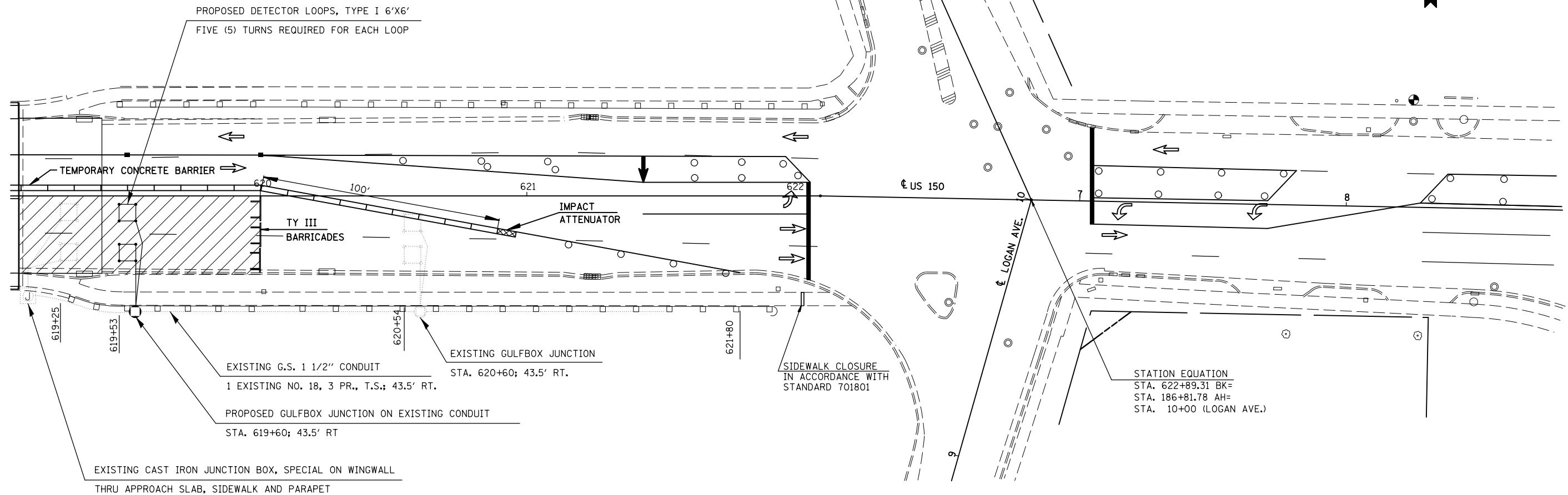
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
729	(342-1B) I-2	VERMILION	25	24
CONTRACT NO. 70971				

ILLINOIS FED. AID PROJECT



STAGE I TRAFFIC CONTROL



DETECTOR LOOP, TYPE I INSTALLATION SHALL BE COMPLETED DURING STAGE I TRAFFIC CONTROL SET-UP.

RESIDENT ENGINEER SHOULD CONTACT DAVE BURKYBILE CONCERNING EXTENDING MAINLINE GREEN TIME WITH ONE LANE TRAFFIC; SEE SPECIAL PROVISIONS

BILL OF MATERIALS

ITEM	UNIT	TOTAL
DETECTOR LOOP, TYPE I	FOOT	73.0
GULFBOX JUNCTION	EACH	1.0