

HIGHWAY STANDARDS, SEE SHEET NO. 2

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
PLANS FOR PROPOSED
FEDERAL AID HIGHWAY
FAU ROUTE 3573 (CENTER DRIVE)
WOODRIDGE DRIVE TO JANES AVENUE
RESURFACING
SECTION: 11-00068-00-RS
PROJECT NO: M-9003(882)
VILLAGE OF WOODRIDGE
DUPAGE COUNTY
JOB NO: C-91-120-12



LOCATION OF SECTION INDICATED THUS

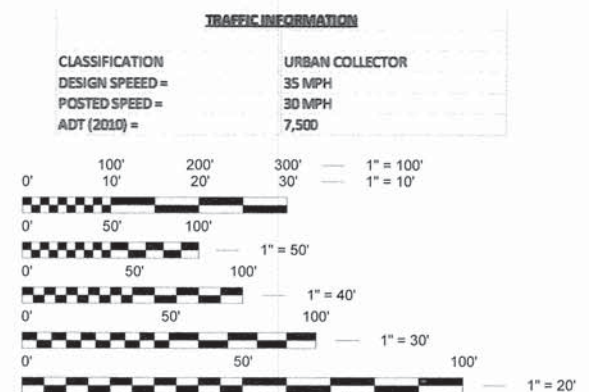
| INDEX OF SHEETS | |
|-----------------|--|
| SHEET # | DESCRIPTION |
| 1 | COVER |
| 2 | GENERAL NOTES AND STANDARDS |
| 3 | SUMMARY OF QUANTITIES |
| 4 | EXISTING AND PROPOSED TYPICAL CROSS SECTIONS |
| 5 | SCHEDULES OF QUANTITIES |
| 6-8 | RESURFACING AND PAVEMENT MARKING PLANS |
| 9 | CENTER DRIVE/PLAZA DRIVE DETECTOR LOOP PLAN |
| 10 | CENTER DRIVE/JANES AVENUE DETECTOR LOOP PLAN |
| 11 | TC 10 TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS (DISTRICT 1) |
| 12 | TC 13 TYPICAL PAVEMENT MARKINGS (DISTRICT 1) |
| 13 | TC 16 PAVEMENT MARKING LETTER AND SYMBOLS FOR TRAFFIC STAGING (DISTRICT 1) |
| 14 | BD 24 CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (DISTRICT 1) |
| 15 | BD 32 BUTT JOINT AND HMA TAPER DETAILS (DISTRICT 1) |
| 16 | TS 05 STANDARD TRAFFIC SIGNAL DESIGN DETAILS (DISTRICT 1) |
| 17 | TS 07 DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING (DISTRICT 1) |

PROJECT IS LOCATED IN THE VILLAGE OF WOODRIDGE



Joshua D. Strait 12/12/13
 ILLINOIS LICENSED PROFESSIONAL ENGINEER, NO. 062-060441
 LICENSE VALID THROUGH NOVEMBER 30, 2015

PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P. E. 847-705-4406 SCHAUMBURG, IL

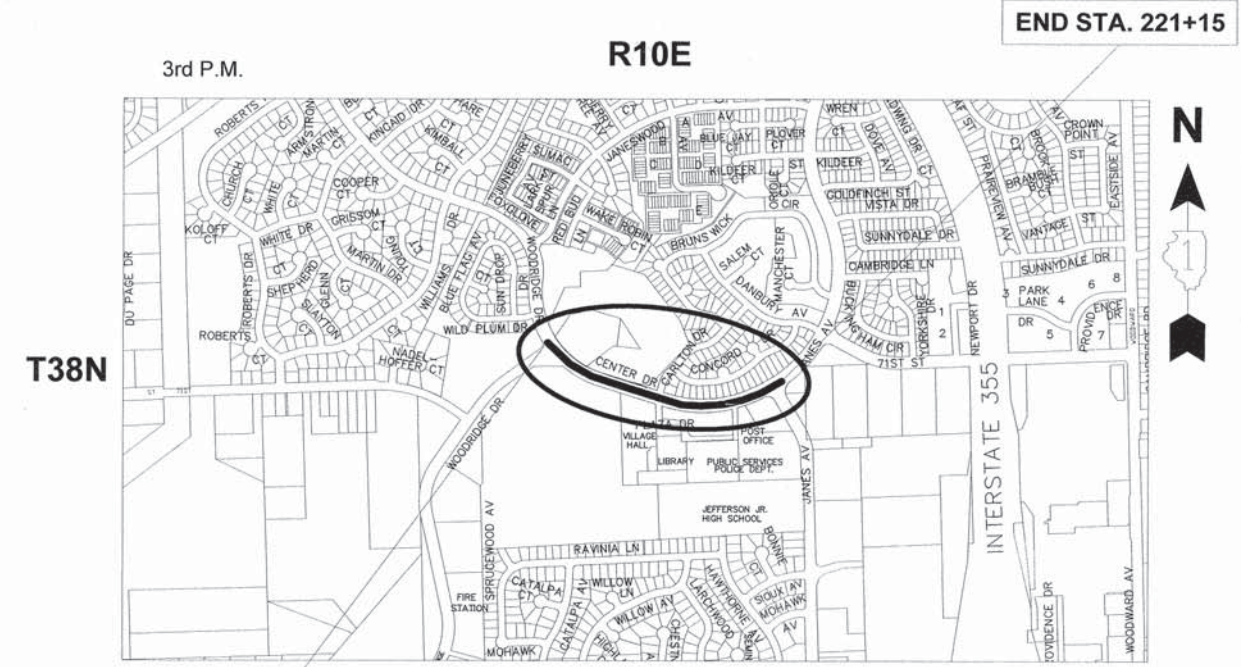


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

PLANS PREPARED BY:
VILLAGE OF WOODRIDGE
DEPARTMENT OF PUBLIC WORKS

J.U.L.I.E
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
 1-800-892-0123 OR 811

CONTRACT 63894



LOCATION MAP IS NOT TO SCALE

BEGIN STA. 200+77

GROSS AND NET LENGTH =
 1,956 FEET (0.37 MI)

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

Approved 12/12/13, 2013
Cl. B.
 VILLAGE OF WOODRIDGE, DIRECTOR OF PUBLIC WORKS

Passed December 17, 2013
C. Strait
 DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS

Released for Bid
 Based on Limited Review December 17, 2013
John Fortman
 DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINOIS

| F.A.U. | COUNTY | SECTION | TOTAL SHEETS | SHEET # |
|--------|--------|----------------|--------------|---------|
| 3573 | DUPAGE | 11-00068-00-RS | 17 | 1 |

GENERAL NOTES

SPECIFICATIONS, STANDARDS AND SPECIAL PROVISIONS:

ALL CONSTRUCTION SHALL BE PERFORMED IN ACCORDANCE WITH THE ILLINOIS DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED JANUARY 1, 2012 (HEREINAFTER REFERRED TO AS THE STANDARD SPECIFICATIONS); THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS ", ADOPTED JANUARY 1, 2014; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS, THE "DETAILS" ON THE PLANS AND THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS. ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED AS THE LATEST STANDARD OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION.

PROJECT COORDINATION:

THE CONTRACTOR SHALL NOTIFY THE ENGINEER AT 630-719-4753 AT LEAST 72 HOURS IN ADVANCE OF BEGINNING WORK AND SHALL COORDINATE ALL CONSTRUCTION OPERATIONS WITH THE ENGINEER. ATTENTION IS CALLED TO SECTION 701 OF THE STANDARD SPECIFICATIONS AND THE SPECIAL PROVISIONS FOR TRAFFIC CONTROL AND PROTECTION. THE STORAGE OF EQUIPMENT AND/OR MATERIALS WITHIN THE RIGHT-OF-WAY OF ANY STREET AND/OR PARK PROPERTY SHALL REQUIRE PRIOR APPROVAL OF THE ENGINEER.

TRAFFIC CONTROL AND MAINTENANCE:

THE CONTRACTOR SHALL SCHEDULE HIS WORK SO THAT ONLY ONE TEMPORARY LANE CLOSURE ON ONE STREET IN ONE DIRECTION IS IMPLEMENTED AT A TIME. A LANE CLOSURE WILL ONLY BE PERMITTED DURING CONSTRUCTION OPERATIONS AND IN ACCORDANCE WITH THE APPLICABLE IDOT STANDARDS. THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN ALL SIGNS, BARRICADES AND OTHER TRAFFIC CONTROL DEVICES, INCLUDING FLAGGERS, REQUIRED TO MAINTAIN TRAFFIC FLOW.

SAW CUTTING FOR REMOVAL:

THE CONTRACTOR SHALL MAKE ALL FULL DEPTH SAW CUT AT THE EDGE OF PAVEMENT ADJACENT TO THE REMOVAL OF ALL CONCRETE CURB OR COMBINATION CONCRETE CURB AND GUTTER. THE CONTRACTOR SHALL MAKE ALL FULL DEPTH SAW CUTS REQUIRED FOR THE REMOVAL OF CONCRETE CURB AND GUTTERS, SIDEWALKS, AND DRIVEWAYS AS SPECIFIED, OR AS DIRECTED BY THE ENGINEER. THE COST SHALL BE CONSIDERED INCLUDED IN THE COST FOR REMOVAL OF THE SPECIFIED ITEM IN THE CONTRACT.

BASE COURSE CLEANING:

PRIOR TO APPLYING THE HOT-MIX ASPHALT PRIME COAT, THE BASE SURFACE INCLUDING GUTTERS SHALL BE CLEANED OF LOOSE GRINDINGS, LEAVES, OF ALL DUST, DIRT, WEEDS AND OTHER FOREIGN MATERIALS. ALL CRACK FILL MATERIAL SHALL BE REMOVED IN ITS ENTIRETY ALONG THE CURB LINE. COST TO BE CONSIDERED INCLUDED IN THE COST OF THE HOT-MIX ASPHALT SURFACE WORK.

CLEAN-UP AND DISPOSAL:

THE CONTRACTOR SHALL MAINTAIN THE SITE IN A CLEAN AND ORDERLY MANNER. DEBRIS AND ANY SURPLUS MATERIAL SHALL BE REMOVED AND RESTORATION SHALL PROCEED AS THE WORK PROCEEDS. IF THE ENGINEER SO DIRECTS, THE CONTRACTOR SHALL STOP ALL OTHER WORK AND CONCENTRATE ON CLEAN-UP AND RESTORATION. DEBRIS AND SURPLUS MATERIAL SHALL BE DISPOSED OF BY THE CONTRACTOR OFF-SITE.

CONSTRUCTION LIMITS:

THE CONTRACTOR SHALL CONFINE OPERATIONS WITHIN THE CONSTRUCTION LIMITS OF THE PROJECT. ANY DAMAGE INSIDE AND OUTSIDE OF SAID CONSTRUCTION LIMITS SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.

PROPERTY ACCESS:

DURING CONSTRUCTION, ALL SIGNS, BARRICADES, AND TEMPORARY PAVEMENT MARKINGS WILL CONFORM TO THE ILLINOIS MUTCD. VEHICULAR ACCESS TO LOCAL PROPERTIES WILL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION, EXCEPT WHEN CONSTRUCTION OPERATIONS OCCUR DIRECTLY ON OR IN FRONT OF DRIVEWAYS. IN THIS CASE, FLAG PERSONS WILL BE USED TO DIRECT AND CONTROL TRAFFIC.

CURB AND GUTTER REPLACEMENT:

THE MINIMUM THICKNESS OF THE PROPOSED GUTTER FLAG SHALL BE 10" UNLESS OTHERWISE STATED IN THE PLANS OR DIRECTED BY THE ENGINEER.

HIGHWAY STANDARDS

| STANDARD # | DESCRIPTION |
|------------|---|
| 000001-06 | STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS |
| 424001-07 | PERPENDICULAR CURB RAMPS FOR SIDEWALKS |
| 424021-02 | DEPRESSED CORNER FOR SIDEWALKS |
| 442201-03 | CLASS C AND D PATCHES |
| 606001-05 | CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER |
| 701101-04 | OFF-ROAD OPERATIONS, MULTILANE, 15' TO 24" FROM PAVEMENT EDGE |
| 701427-02 | LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS LESS THAN OR EQUAL TO 40MPH |
| 701601-09 | URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN |
| 701701-09 | URBAN LANE CLOSURE, MULTILANE INTERSECTION |
| 701801-05 | SIDEWALK, CORNER OR CROSSWALK CLOSURE |
| 701901-03 | TRAFFIC CONTROL DEVICES |
| 780001-04 | TYPICAL PAVEMENT MARKINGS |
| 886001-01 | DETECTOR LOOP INSTALLATIONS |
| 886006-01 | TYPICAL LAYOUTS FOR DETECTION LOOPS |

COMMITMENT

SPECIAL NOTE:

DUE TO AN ANNUAL SUMMER EVENT THAT TAKES PLACE WITHIN THE CENTER DRIVE RIGHT OF WAY, NO WORK SHALL COMMENCE PRIOR TO MONDAY, JUNE 23, 2014.

VILLAGE OF WOODRIDGE
DEPARTMENT OF PUBLIC WORKS
1 Plaza Drive, Woodridge IL 60517 (630) 719-4753

| | |
|---------------|-----------|
| DESIGNED - AF | REVISED - |
| DRAWN - AF | |
| CHECKED - | |
| DATE - | |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES
AND STANDARDS

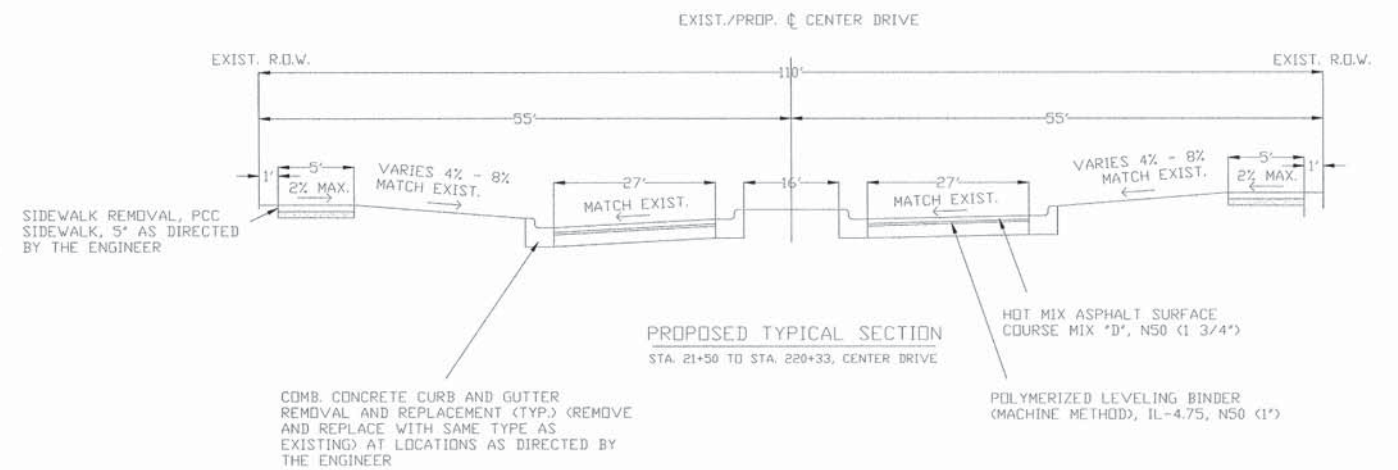
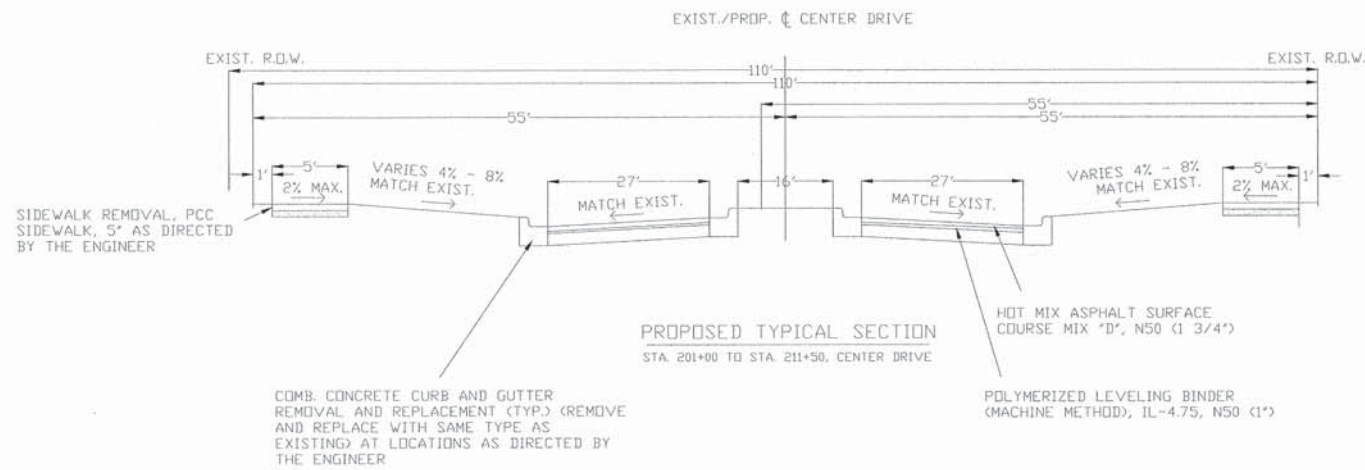
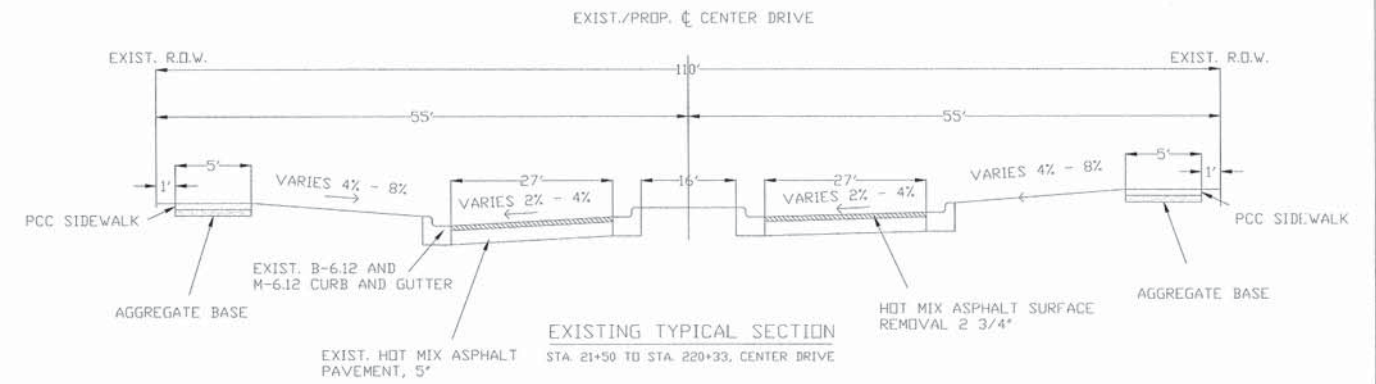
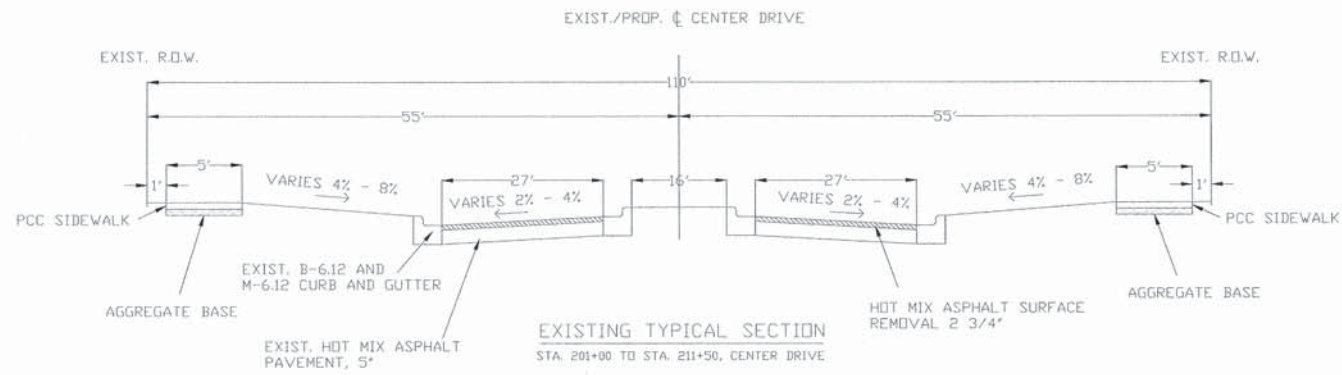
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|-------------|----------------|--------|---|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 3573 | 11-00068-00-RS | DUPAGE | 17 | 2 |
| ILLINOIS | | | CONTRACT NO. 63894 FEDERAL AID PROJECT | |

SCALE: NONE STA. TO STA.

| CODE NO | ITEM | UNIT | TOTAL QUANTITY |
|----------|--|--------|------------------------|
| | | | CONSTRUCTION CODE 0005 |
| 20201200 | REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL | CU YD | 20 |
| 20700220 | POROUS GRANULAR EMBANKMENT | CU YD | 20 |
| 21101615 | TOPSOIL FURNISH AND PLACE, 4" | SQ YD | 400 |
| 25000110 | SEEDING, CLASS 1A | ACRE | 0.18 |
| 25000400 | NITROGEN FERTILIZER NUTRIENT | POUND | 15 |
| 25000500 | PHOSPHORUS FERTILIZER NUTRIENT | POUND | 15 |
| 25000600 | POTASSIUM FERTILIZER NUTRIENT | POUND | 15 |
| 28000510 | INLET FILTERS | EACH | 25 |
| 40600100 | BITUMINOUS MATERIALS (PRIME COAT) | GALLON | 2830 |
| 40600300 | AGGREGATE (PRIME COAT) | TON | 43 |
| 40600400 | MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS | TON | 40 |
| 40600827 | POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 | TON | 793 |
| 40600982 | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT | SQ YD | 150 |
| 40603335 | HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 | TON | 1387 |
| 42400200 | PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH | SQ FT | 6126 |
| 42400800 | DETECTABLE WARNINGS | SQ FT | 242 |
| 44000160 | HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4" | SQ YD | 14150 |
| 44000600 | SIDEWALK REMOVAL | SQ FT | 6126 |
| 44003100 | MEDIAN REMOVAL | SQ FT | 120 |
| 44201705 | CLASS D PATCHES, TYPE II, 5 INCH | SQ YD | 25 |
| 44201709 | CLASS D PATCHES, TYPE III, 5 INCH | SQ YD | 50 |
| 44201711 | CLASS D PATCHES, TYPE IV, 5 INCH | SQ YD | 150 |
| 60107600 | PIPE UNDERDRAINS 4" | FOOT | 75 |
| 60255500 | MANHOLES TO BE ADJUSTED | EACH | 1 |

| CODE NO | ITEM | UNIT | TOTAL QUANTITY |
|------------|--|--------|------------------------|
| | | | CONSTRUCTION CODE 0005 |
| 60260100 | INLETS TO BE ADJUSTED | EACH | 9 |
| 60262700 | INLETS TO BE RECONSTRUCTED | EACH | 1 |
| 67100100 | MOBILIZATION | L SUM | 1 |
| 70102630 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701601 | L SUM | 1 |
| 70102635 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701701 | L SUM | 1 |
| 70102640 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701801 | L SUM | 1 |
| 70106800 | CHANGEABLE MESSAGE SIGN | CAL MO | 2 |
| 70300100 | SHORT TERM PAVEMENT MARKING | FOOT | 500 |
| 70300210 | TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS | SQ FT | 150 |
| 70300220 | TEMPORARY PAVEMENT MARKING - LINE 4" | FOOT | 2500 |
| 70300280 | TEMPORARY PAVEMENT MARKING - LINE 24" | FOOT | 534 |
| 70301000 | WORK ZONE PAVEMENT MARKING REMOVAL | SQ FT | 200 |
| * 78000100 | THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS | SQ FT | 150 |
| * 78000200 | THERMOPLASTIC PAVEMENT MARKING - LINE 4" | FOOT | 4412 |
| * 78000400 | THERMOPLASTIC PAVEMENT MARKING - LINE 6" | FOOT | 727 |
| * 78000600 | THERMOPLASTIC PAVEMENT MARKING - LINE 12" | FOOT | 1350 |
| * 78000650 | THERMOPLASTIC PAVEMENT MARKING - LINE 24" | FOOT | 283 |
| * 88600600 | DETECTOR LOOP REPLACEMENT | FOOT | 1060 |
| Z0004562 | COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT | FOOT | 1066 |

* INDICATES SPECIALTY ITEM



HOT-MIX ASPHALT MIXTURE REQUIREMENTS:

| MIXTURE TYPE | AIR VOIDS @Ndes |
|--|-----------------|
| STREETS | |
| HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5mm) 1 3/4" | 4% @ 50 Gyr. |
| POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50 1" | 3.5% @ 50 Gyr. |
| PATCHING CLASS D PATCHES (HMA BINDER IL-19mm) 5" (2 LIFTS) | 4% @ 70 Gyr. |

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
 THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
 FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

**NOTE:
MILLING DONE PRIOR TO PATCHING**

CROSS SECTIONS ARE NOT TO SCALE

| | | | | | | | | | |
|--|---------------|-----------|---|--|-------------|----------------|--------|---------------------|-----------|
| VILLAGE OF WOODRIDGE DEPARTMENT OF PUBLIC WORKS 1 Plaza Drive, Woodridge IL 60517 (830) 719-4752 | DESIGNED - AF | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | TYPICAL CROSS SECTIONS SCALE: NONE STA. TO STA. | F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| | DRAWN - AF | | | | 3573 | 11-00068-00-RS | DUPAGE | 17 | 4 |
| | CHECKED - | | | | | | | CONTRACT NO. 63894 | |
| | DATE - | | | | | ILLINOIS | | FEDERAL AID PROJECT | |

| CURB AND GUTTER REMOVAL AND REPLACEMENT SCHEDULE | | | |
|--|--------|------|---|
| LOC # | STA. | SIDE | Z0004562 COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT LENGTH (FT) |
| 1 | 201+75 | L | 42 |
| 2 | 201+75 | R | 21 |
| 3 | 201+80 | L | 10 |
| 4 | 201+80 | R | 25 |
| 5 | 201+85 | R | 15 |
| 6 | 202+10 | R | 10 |
| 7 | 202+15 | R | 11 |
| 8 | 202+75 | R | 23 |
| 9 | 202+80 | R | 7 |
| 10 | 203+00 | R | 60 |
| 11 | 203+15 | R | 45 |
| 12 | 203+55 | R | 27 |
| 13 | 203+60 | R | 30 |
| 14 | 204+00 | L | 62 |
| 15 | 204+00 | R | 52 |
| 16 | 204+15 | R | 15 |
| 17 | 204+40 | R | 20 |
| 18 | 204+50 | L | 26 |
| 19 | 204+60 | R | 16 |
| 20 | 205+00 | R | 8 |
| 21 | 205+30 | R | 30 |
| 22 | 205+50 | L | 24 |
| 23 | 205+95 | R | 10 |
| 24 | 207+70 | R | 15 |
| 25 | 208+30 | L | 36 |
| 26 | 208+30 | R | 76 |
| 27 | 209+90 | R | 18 |
| 28 | 210+50 | L | 35 |
| 29 | 210+60 | R | 29 |
| 30 | 211+20 | L | 14 |
| 31 | 211+30 | L | 13 |
| 32 | 213+90 | R | 21 |
| 33 | 214+50 | L | 15 |
| 34 | 214+50 | R | 15 |
| 35 | 215+15 | R | 10 |
| 36 | 215+95 | R | 22 |
| 37 | 216+00 | R | 10 |
| 38 | 216+05 | L | 13 |
| 39 | 217+40 | R | 30 |
| 40 | 217+50 | R | 38 |
| 41 | 218+95 | L | 45 |
| 42 | 220+00 | L | 12 |
| 43 | 220+45 | L | 10 |
| 44 | 220+95 | L | 10 |
| 45 | 221+00 | R | 10 |
| | | | 1086 |
| | | | FEET |

| DRAINAGE AND UTILITY STRUCTURE SCHEDULE | | | | | |
|---|------|---|---------------------------------------|--|--|
| STA | SIDE | 60255500 MANHOLES TO BE ADJUSTED (EACH) | 60260100 INLETS TO BE ADJUSTED (EACH) | 60262700 INLETS TO BE RECONSTRUCTED (EACH) | COMMENTS |
| 204+50 | LT | | 1 | | DETERIORATED RINGS |
| 204+60 | LT | | | | DETERIORATED RINGS |
| 208+55 | RT | | 1 | | DETERIORATED RINGS, PW TO SUPPLY NEW F&G |
| 214+40 | LT | | 1 | | DETERIORATED RINGS |
| 214+40 | RT | | 1 | | DETERIORATED RINGS, PW TO SUPPLY NEW F&G |
| 214+50 | LT | | 1 | | DETERIORATED RINGS |
| 214+50 | RT | | 1 | | DETERIORATED RINGS |
| 220+50 | LT | | 1 | | DETERIORATED RINGS |
| 221+00 | LT | | 1 | | DETERIORATED RINGS |
| 221+00 | RT | | 1 | | DETERIORATED RINGS, PW TO SUPPLY NEW F&G |
| VARIOUS LOC. | | 1 | | | |
| VARIOUS LOC. | | | | 1 | |
| | | 1 | 9 | 1 | |

| SIDEWALK REMOVAL AND REPLACEMENT SCHEDULE | | | | | |
|---|--------|------|------------------------------------|---------------------------------------|------------------------------------|
| LOC # | STA | SIDE | 42400200 PCC SIDEWALK, 5" (SQ. FT) | 42400800 DETECTABLE WARNINGS (SQ. FT) | 44000600 SIDEWALK REMOVAL (SQ. FT) |
| 1 | 201+20 | R | 48.75 | 16 | 48.75 |
| 2 | 201+25 | L | 30 | | 30 |
| 3 | 201+25 | R | 102 | | 102 |
| 4 | 201+50 | L | 44 | 8 | 44 |
| 5 | 201+50 | R | 129.15 | 20 | 129.15 |
| 6 | 201+95 | L | 30 | | 30 |
| 7 | 202+10 | L | 60 | | 60 |
| 8 | 202+10 | R | 60 | | 60 |
| 9 | 202+50 | R | 120 | | 120 |
| 10 | 203+00 | R | 60 | | 60 |
| 11 | 203+55 | L | 60 | | 60 |
| 12 | 203+85 | L | 60 | | 60 |
| 13 | 203+90 | R | 117 | | 117 |
| 14 | 204+00 | R | 60 | | 60 |
| 15 | 204+30 | L | 276 | | 276 |
| 16 | 204+30 | R | 69 | | 69 |
| 17 | 204+55 | R | 132 | | 132 |
| 18 | 205+05 | L | 30 | | 30 |
| 19 | 205+10 | R | 63 | | 63 |
| 20 | 205+55 | R | 32.4 | | 32.4 |
| 21 | 205+80 | R | 66 | | 66 |
| 22 | 206+10 | R | 61.8 | | 61.8 |
| 23 | 206+40 | L | 81 | 8 | 81 |
| 24 | 206+50 | R | 30 | | 30 |
| 25 | 207+00 | L | 30 | 8 | 30 |
| 26 | 207+10 | R | 33 | | 33 |
| 27 | 207+10 | L | 30 | | 30 |
| 28 | 207+20 | R | 57 | | 57 |
| 29 | 207+55 | L | 30 | | 30 |
| 30 | 207+55 | R | 64.8 | | 64.8 |
| 31 | 207+80 | L | 30 | | 30 |
| 32 | 207+90 | R | 151.2 | | 151.2 |
| 33 | 208+10 | L | 30 | | 30 |
| 34 | 208+10 | R | 30 | | 30 |
| 35 | 208+20 | L | 54 | | 54 |
| 36 | 208+20 | R | 30 | | 30 |
| 37 | 208+60 | L | 90 | | 90 |
| 38 | 208+60 | R | 25.2 | | 25.2 |
| 39 | 208+80 | R | 87 | | 87 |
| 40 | 209+00 | L | 87 | | 87 |
| 41 | 209+30 | L | 60 | | 60 |
| 42 | 209+30 | R | 30.6 | | 30.6 |
| 43 | 209+65 | L | 60 | | 60 |
| 44 | 209+70 | R | 25.2 | | 25.2 |
| 45 | 210+10 | L | 175 | | 175 |
| 46 | 210+40 | R | 154.2 | | 154.2 |
| 47 | 210+50 | L | 146 | 16 | 146 |
| 48 | 210+50 | R | 135.6 | 30 | 135.6 |
| 49 | 211+25 | L | 135 | 16 | 135 |
| 50 | 211+35 | L | 48 | 8 | 48 |
| 51 | 211+35 | R | 184 | 32 | 184 |
| 52 | 211+90 | R | 72.5 | | 72.5 |
| 53 | 212+30 | R | 100 | | 100 |
| 54 | 212+35 | L | 25 | | 25 |
| 55 | 212+50 | L | 25 | | 25 |
| 56 | 212+50 | R | 50 | | 50 |
| 57 | 212+70 | R | 25 | | 25 |
| 58 | 212+75 | L | 135 | | 135 |
| 59 | 212+90 | R | 25 | | 25 |
| 60 | 213+60 | R | 101 | | 101 |
| 61 | 214+00 | R | 50 | | 50 |
| 62 | 214+20 | R | 76 | | 76 |
| 63 | 214+55 | R | 25 | | 25 |
| 64 | 214+85 | R | 50 | | 50 |
| 65 | 215+00 | L | 100 | | 100 |
| 66 | 215+05 | R | 28 | | 28 |
| 67 | 215+85 | L | 25 | | 25 |
| 68 | 215+90 | R | 25 | | 25 |
| 69 | 216+05 | R | 27.5 | | 27.5 |
| 70 | 216+75 | R | 132.5 | 8 | 132.5 |
| 71 | 217+25 | R | 130 | 16 | 130 |
| 72 | 217+60 | R | 28.5 | | 28.5 |
| 73 | 218+20 | R | 47.5 | | 47.5 |
| 74 | 218+55 | R | 78 | | 78 |
| 75 | 218+65 | R | 25 | | 25 |
| 76 | 218+85 | R | 25 | | 25 |
| 77 | 219+00 | R | 50 | | 50 |
| 78 | 219+45 | R | 25 | | 25 |
| 79 | 219+85 | R | 361 | | 361 |
| 80 | 219+90 | L | 50 | | 50 |
| 81 | 220+05 | L | 28 | 8 | 28 |
| 82 | 220+05 | R | 98 | 20 | 98 |
| 83 | 220+50 | R | 65 | 16 | 65 |
| 84 | 220+90 | L | 50 | | 50 |
| 85 | 221+10 | L | 25 | | 25 |
| 86 | 221+10 | R | 108 | 12 | 108 |
| | | | 6126 | 242 | 6126 |
| | | | SQ. FT | SQ. FT | SQ. FT |

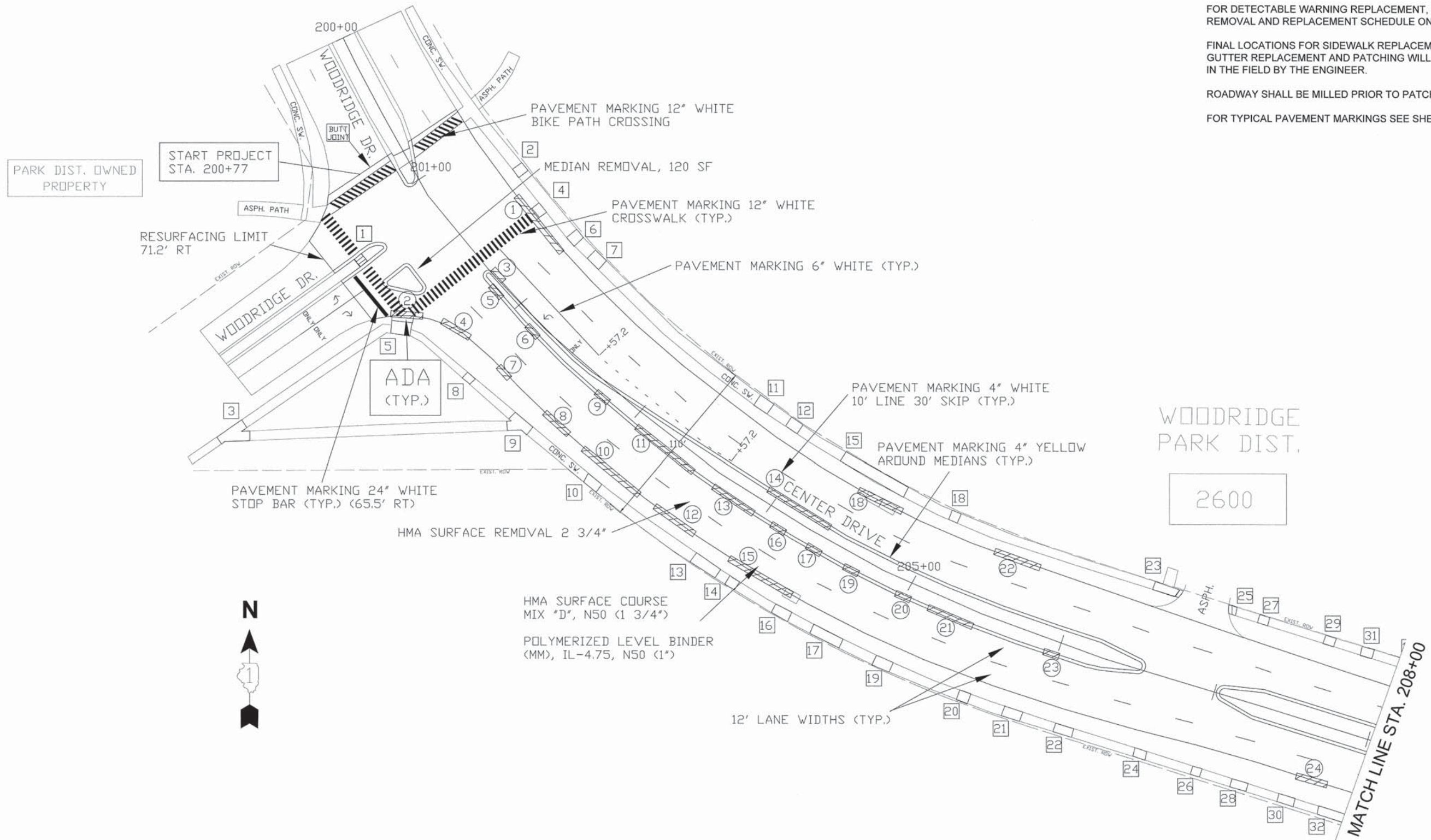
ALL SIDEWALK CROSSING RAMPS AND DETECTABLE WARNINGS ARE SCHEDULED FOR REPLACEMENT AS PART OF THIS PROJECT.

FOR DETECTABLE WARNING REPLACEMENT, SEE SIDEWALK REMOVAL AND REPLACEMENT SCHEDULE ON SHEET 5.

FINAL LOCATIONS FOR SIDEWALK REPLACEMENT, CURB AND GUTTER REPLACEMENT AND PATCHING WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

ROADWAY SHALL BE MILLED PRIOR TO PATCHING.

FOR TYPICAL PAVEMENT MARKINGS SEE SHEET 12.



| | | | | | | | | | | | |
|--|---------------|-----------|---|---|------------------------------|----------------------------|--------|--------------|-----------|--|--|
| VILLAGE OF WOODRIDGE DEPARTMENT OF PUBLIC WORKS 1 Plaza Drive, Woodridge IL 60517 (815) 719-4753 | DESIGNED - AF | REVISED - | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | RESURFACING PLAN AND PAVEMENT MARKING PLAN | F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. | | |
| | DRAWN - AF | | | | 3573 | 11-00068-00-RS | DUPAGE | 17 | 6 | | |
| | CHECKED - | | | | CONTRACT NO. 63894 | | | | | | |
| | DATE - | | | | ILLINOIS FEDERAL AID PROJECT | | | | | | |
| | | | | | SCALE: 1"=30' | STA. 200+00 TO STA. 208+00 | | | | | |



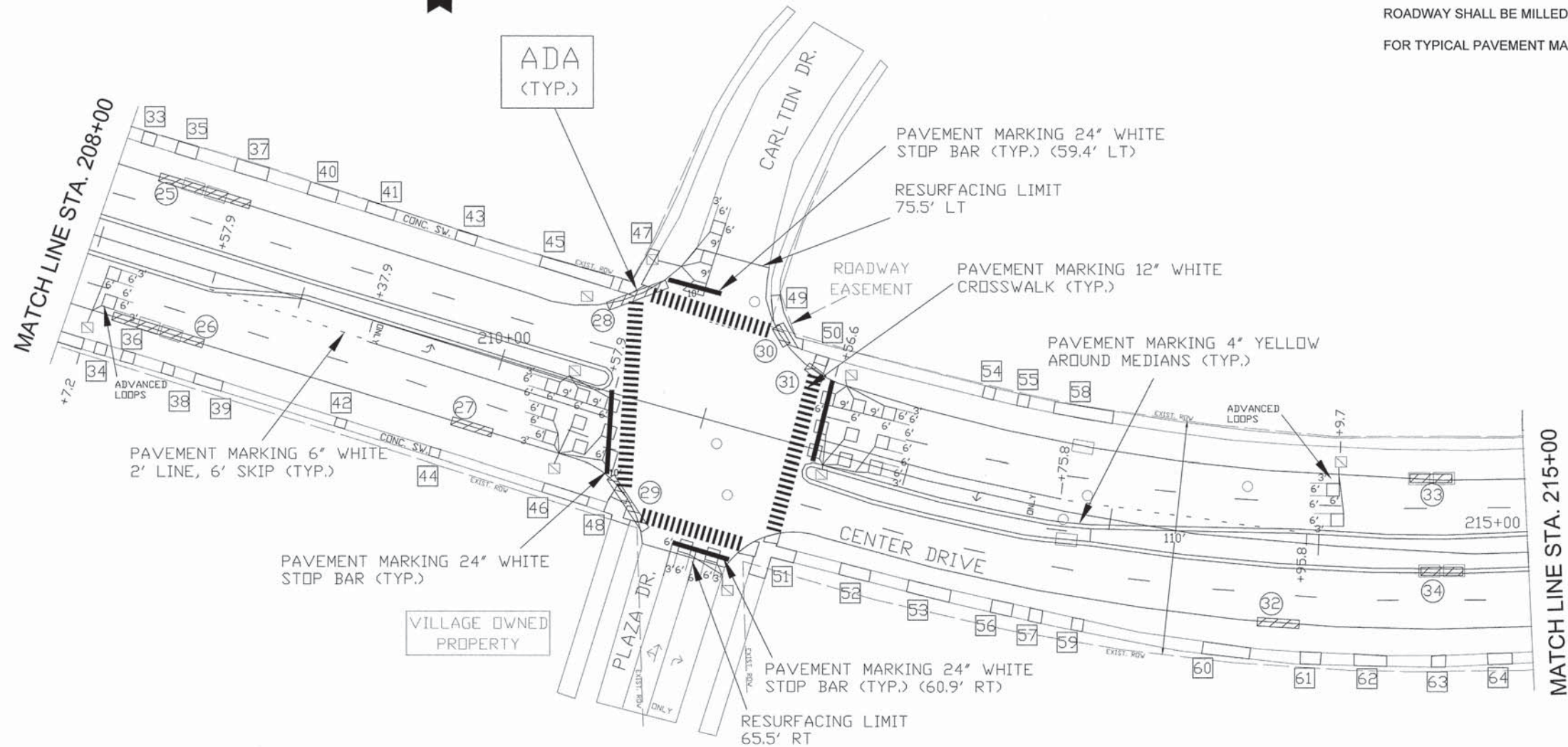
ALL SIDEWALK CROSSING RAMPS AND DETECTABLE WARNINGS ARE SCHEDULED FOR REPLACEMENT AS PART OF THIS PROJECT.

FOR DETECTABLE WARNING REPLACEMENT, SEE SIDEWALK REMOVAL AND REPLACEMENT SCHEDULE ON SHEET 5.

FINAL LOCATIONS FOR SIDEWALK REPLACEMENT, CURB AND GUTTER REPLACEMENT AND PATCHING WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

ROADWAY SHALL BE MILLED PRIOR TO PATCHING.

FOR TYPICAL PAVEMENT MARKINGS SEE SHEET 12.



WOODRIDGE
 - VILLAGE HALL
 - PUBLIC WORKS
 - LIBRARY
 - POST OFFICE

VILLAGE OF WOODRIDGE
 DEPARTMENT OF PUBLIC WORKS
 1 Plaza Drive, Woodridge IL 60517 (815) 719-4753

DESIGNED - AF
 DRAWN - AF
 CHECKED -
 DATE -

REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

RESURFACING PLAN AND
 PAVEMENT MARKING PLAN

SCALE: 1"=30' STA. 208+00 TO STA. 215+00

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|----------------|--------|---|-----------|
| 3573 | 11-00068-00-RS | DUPAGE | 17 | 7 |
| ILLINOIS | | | CONTRACT NO. 63894 FEDERAL AID PROJECT | |



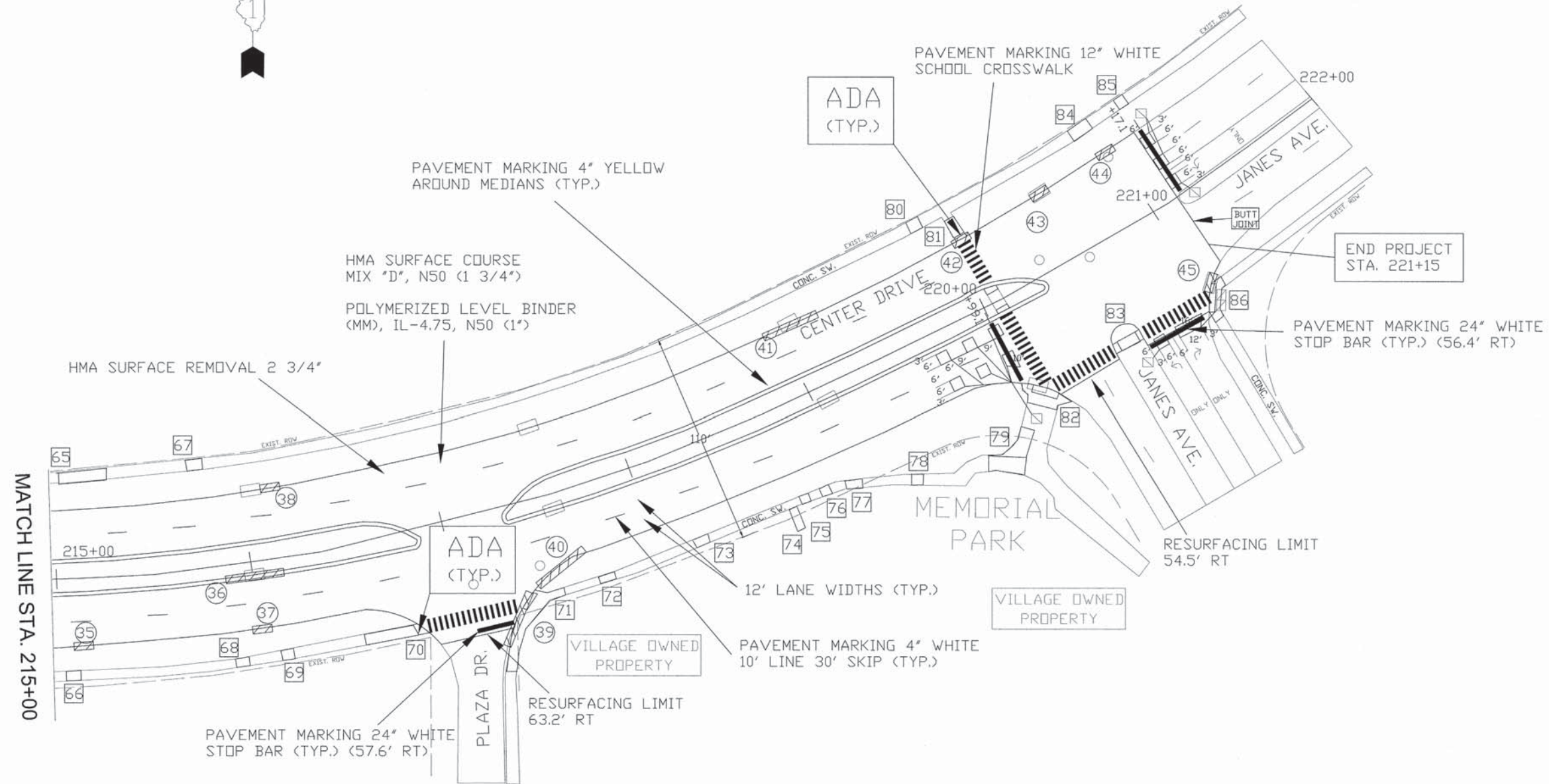
ALL SIDEWALK CROSSING RAMPS AND DETECTABLE WARNINGS ARE SCHEDULED FOR REPLACEMENT AS PART OF THIS PROJECT.

FOR DETECTABLE WARNING REPLACEMENT, SEE SIDEWALK REMOVAL AND REPLACEMENT SCHEDULE ON SHEET 5.

FINAL LOCATIONS FOR SIDEWALK REPLACEMENT, CURB AND GUTTER REPLACEMENT AND PATCHING WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

ROADWAY SHALL BE MILLED PRIOR TO PATCHING.

FOR TYPICAL PAVEMENT MARKINGS SEE SHEET 12.



VILLAGE OF WOODRIDGE
DEPARTMENT OF PUBLIC WORKS
1 Plaza Drive, Woodridge IL 60517 (830) 719-4753

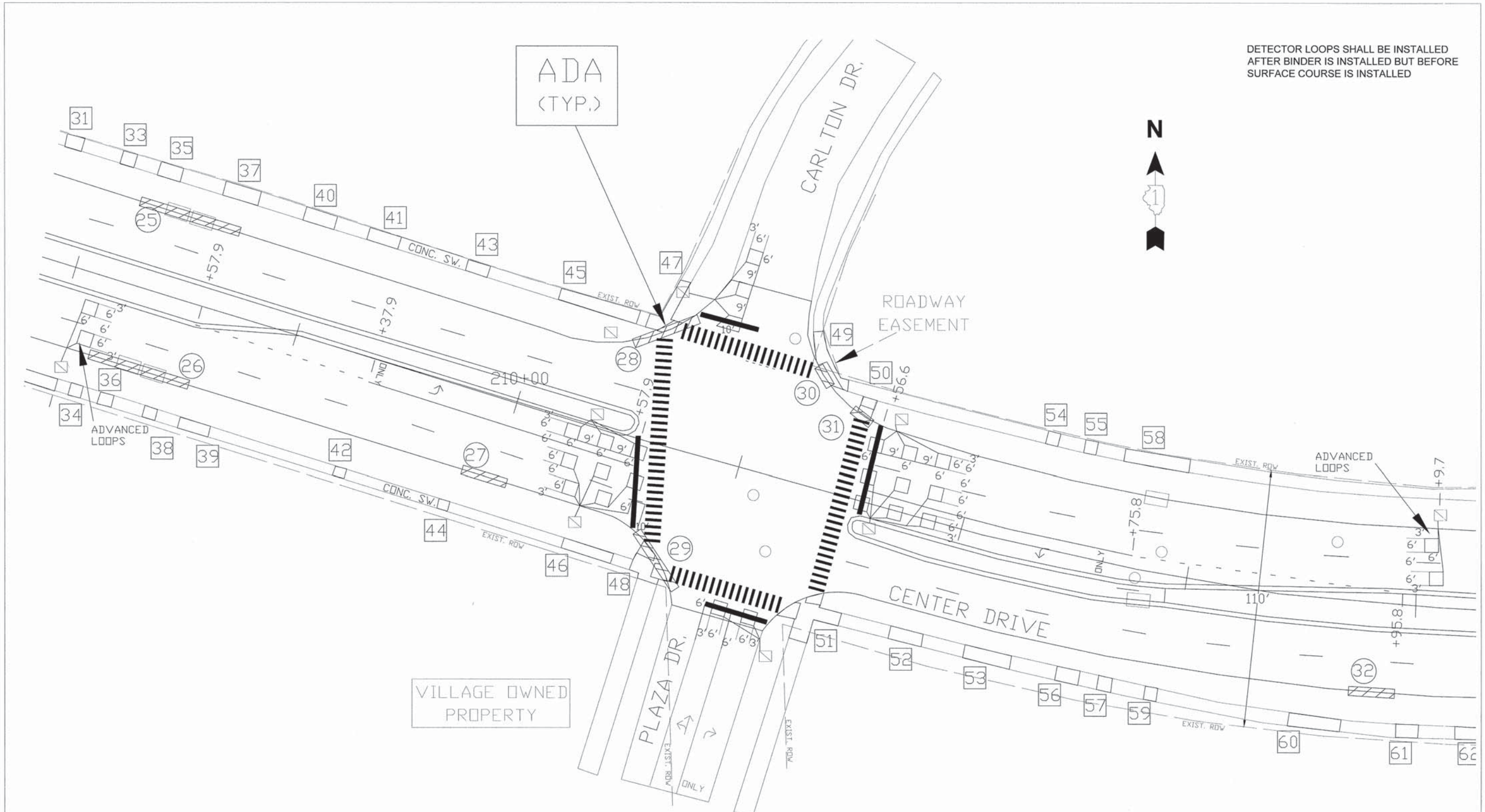
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| DESIGNED - AF | REVISED - |
| DRAWN - AF | |
| CHECKED - | |
| DATE - | |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

RESURFACING PLAN AND
PAVEMENT MARKING PLAN
SCALE: 1"=30'
STA. 215+00 TO STA. 222+00

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|-------------|----------------|--------|---|-----------|
| 3573 | 11-00068-00-RS | DUPAGE | 17 | 8 |
| ILLINOIS | | | CONTRACT NO. 63894 FEDERAL AID PROJECT | |

DETECTOR LOOPS SHALL BE INSTALLED
AFTER BINDER IS INSTALLED BUT BEFORE
SURFACE COURSE IS INSTALLED



VILLAGE OF WOODRIDGE
DEPARTMENT OF PUBLIC WORKS
1 Plaza Drive, Woodridge IL 60517 (630) 719-4793

| | |
|---------------|-----------|
| DESIGNED - AF | REVISED - |
| DRAWN - AF | |
| CHECKED - | |
| DATE - | |

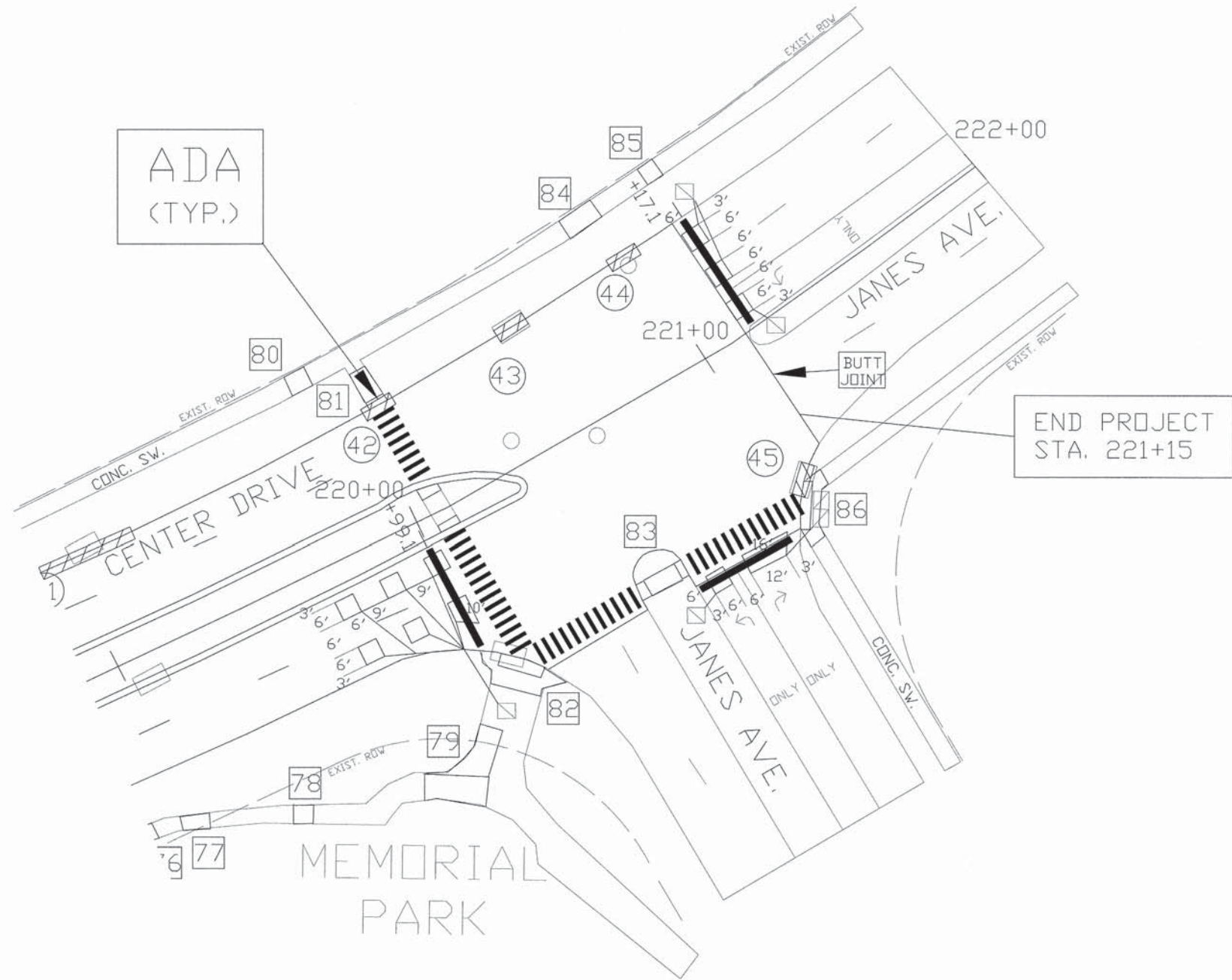
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PLAZA DRIVE, CARLTON DRIVE
DETECTOR LOOP PLAN
SCALE: 1"=20' STA. 210+50

| | | | | |
|------------|----------------|--------|---|-----------|
| FA.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 3573 | 11-00068-00-RS | DUPAGE | 17 | 9 |
| ILLINOIS | | | CONTRACT NO. 63894 FEDERAL AID PROJECT | |



DETECTOR LOOPS SHALL BE INSTALLED
AFTER BINDER IS INSTALLED BUT BEFORE
SURFACE COURSE IS INSTALLED



VILLAGE OF WOODRIDGE
DEPARTMENT OF PUBLIC WORKS
1 Plaza Drive, Woodridge IL 60517 (815) 718-4753

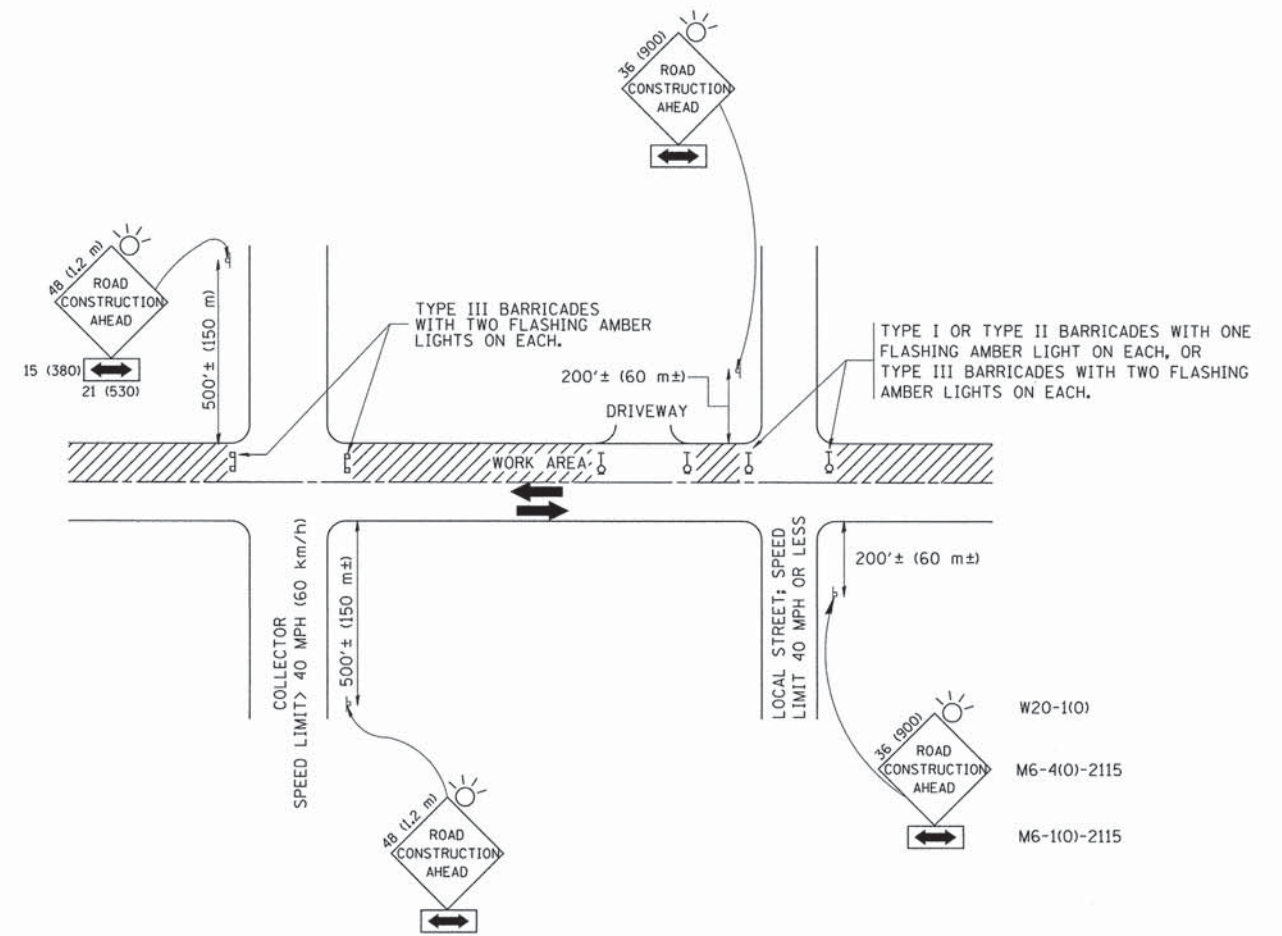
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|---------------|-----------|
| DESIGNED - AF | REVISED - |
| DRAWN - AF | |
| CHECKED - | |
| DATE - | |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

JANES AVENUE, CENTER DRIVE
DETECTOR LOOP PLAN

SCALE: 1"=20' STA. 220+00

| | | | | |
|-------------|----------------|--------|---|-----------|
| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 3573 | 11-00068-00-RS | DUPAGE | 17 | 10 |
| ILLINOIS | | | CONTRACT NO. 63894 FEDERAL AID PROJECT | |



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

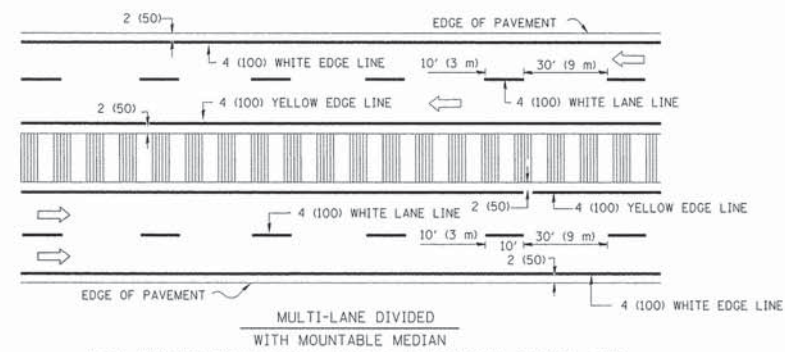
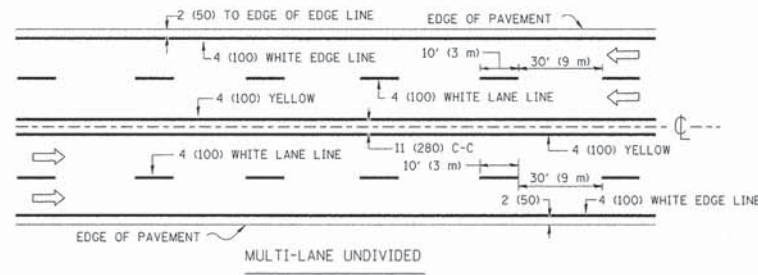
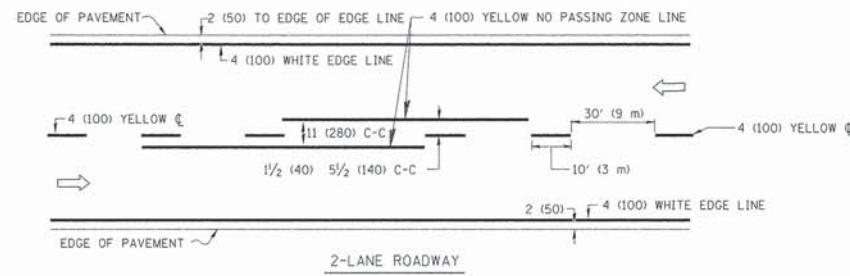
D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

| | | | |
|--|----------------------------|----------------|---------------------------------|
| FILE NAME = W:\diststd\22x34\td18.dgn | USER NAME = gaglionobt | DESIGNED - LHA | REVISED - J. OBERLE 10-18-95 |
| | | DRAWN - | REVISED - A. HOUSEH 03-06-96 |
| | PLOT SCALE = 50.000' / IN. | CHECKED - | REVISED - A. HOUSEH 10-15-96 |
| | PLOT DATE = 1/4/2000 | DATE - 06-89 | REVISED - T. RAMMACHER 01-06-00 |

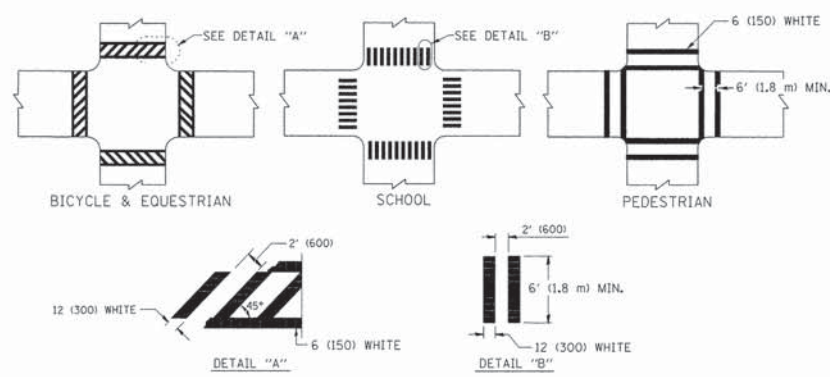
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| | | | | | | | | |
|--|--|--|--|---|----------------|--------------------|-----------------|--------------|
| TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS | | | | F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| SCALE: NONE | | | | 3573 | 11-00068-00-RS | DuPage | 17 | 11 |
| SHEET NO. 1 OF 1 SHEETS | | | | TC-10 | | CONTRACT NO. 63894 | | |
| STA. TO STA. | | | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

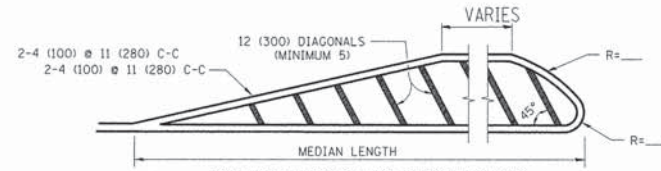
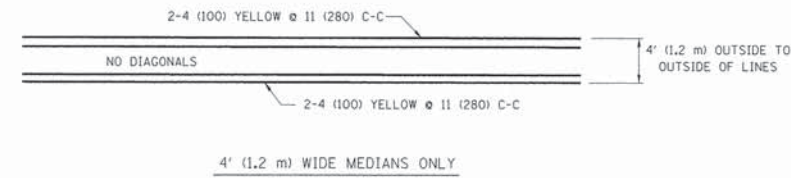


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

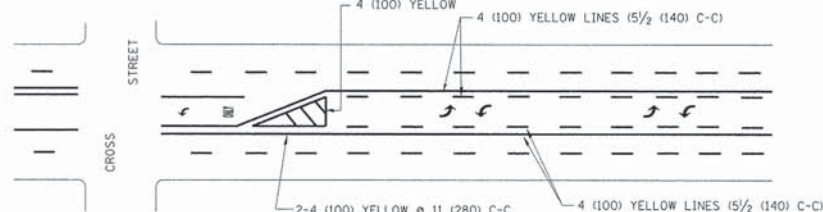


TYPICAL CROSSWALK MARKING

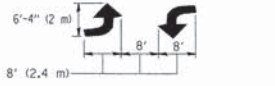


FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.
 DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
 150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

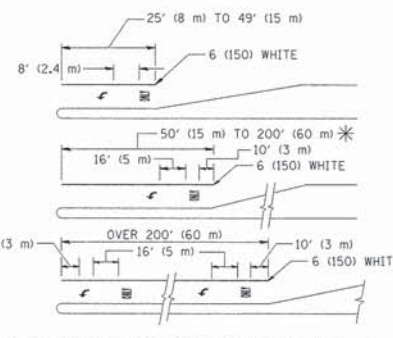


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

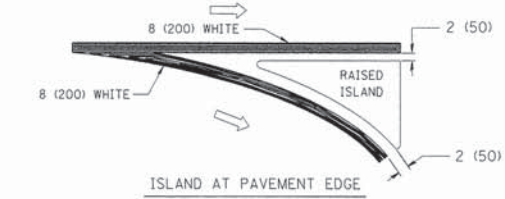
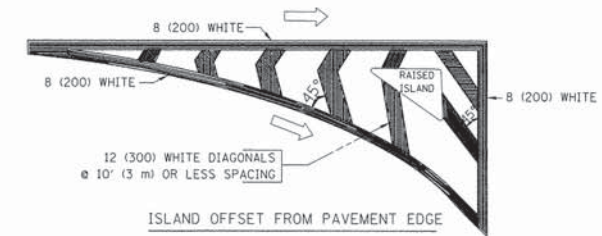
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
 AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
 * TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

| TYPE OF MARKING | WIDTH OF LINE | PATTERN | COLOR | SPACING / REMARKS |
|---|--|---------------------------------|---|--|
| CENTERLINE ON 2 LANE PAVEMENT | 4 (100) | SKIP-DASH | YELLOW | 10' (3 m) LINE WITH 30' (9 m) SPACE |
| CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT | 2 @ 4 (100) | SOLID | YELLOW | 11 (280) C-C |
| NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS | 4 (100) 2 @ 4 (100) | SOLID SOLID | YELLOW YELLOW | 5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN |
| LANE LINES | 4 (100) 5 (125) | SKIP-DASH SKIP-DASH | WHITE WHITE | 10' (3 m) LINE WITH 30' (9 m) SPACE |
| DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS) | SAME AS LINE BEING EXTENDED | SKIP-DASH | SAME AS LINE BEING EXTENDED | 2' (600) LINE WITH 6' (1.8 m) SPACE |
| EDGE LINES | 4 (100) | SOLID | YELLOW-LEFT WHITE-RIGHT | OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB |
| TURN LANE MARKINGS | 6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m)) | SOLID | WHITE | SEE TYPICAL TURN LANE MARKING DETAIL |
| TWO WAY LEFT TURN MARKING | 2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW | SKIP-DASH AND SOLID IN PAIRS | YELLOW WHITE | 10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL |
| CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL) | 2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90° | SOLID SOLID SOLID | WHITE WHITE WHITE | NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS. |
| STOP LINES | 24 (600) | SOLID | WHITE | PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE |
| PAINTED MEDIANS | 2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS | SOLID | YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC | 11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING. |
| GORE MARKING AND CHANNELIZING LINES | 8 (200) WITH 12 (300) DIAGONALS @ 45° | SOLID | WHITE | DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h)) |
| RAILROAD CROSSING | 24 (600) TRANSVERSE LINES; "RR" 15 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X" | SOLID | WHITE | SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²) |
| SHOULDER DIAGONALS | 12 (300) @ 45° | SOLID | WHITE - RIGHT YELLOW - LEFT | 50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h)) |

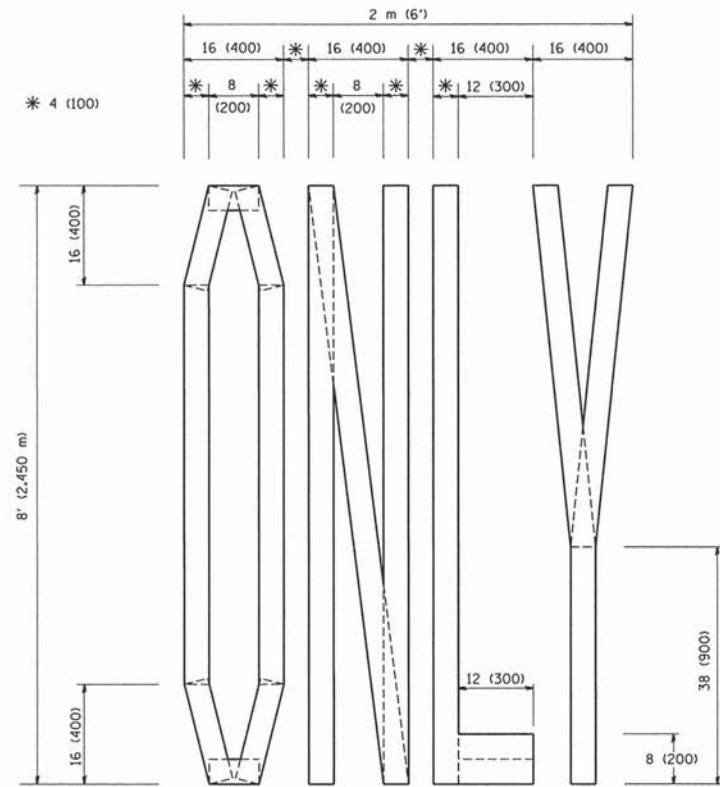
FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

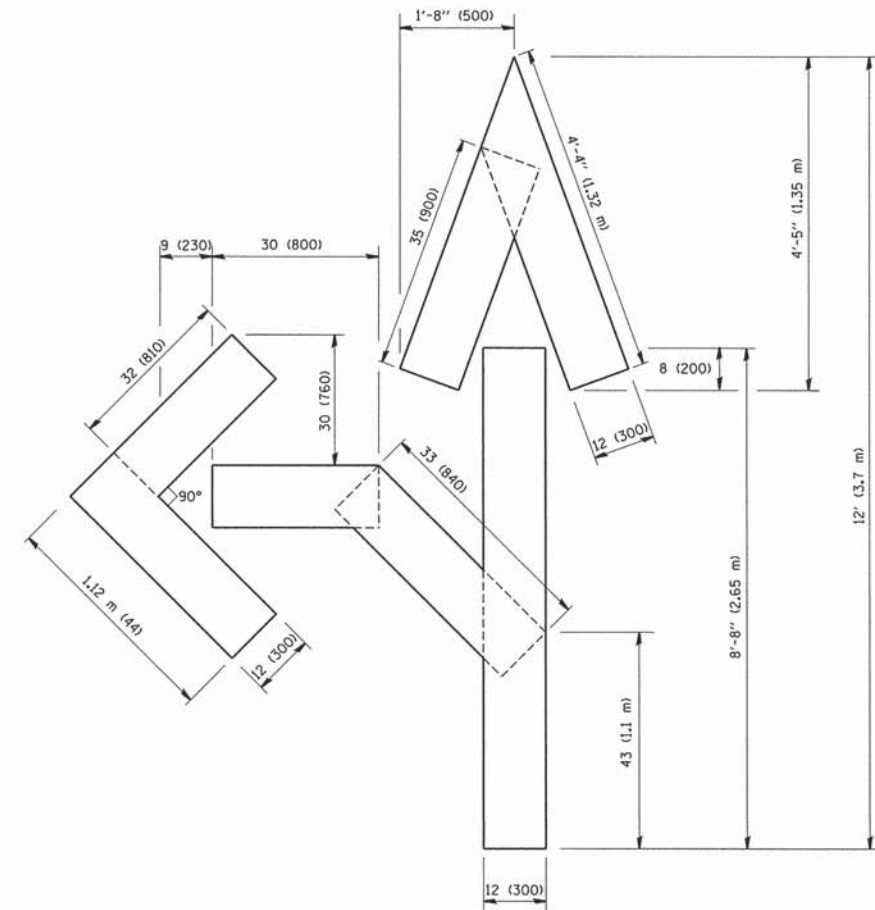
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| | PLOT SCALE = 50,000' / IN. | CHECKED - | REVISED - |
| | PLOT DATE = 9/9/2009 | DATE - 03-19-90 | REVISED - |

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

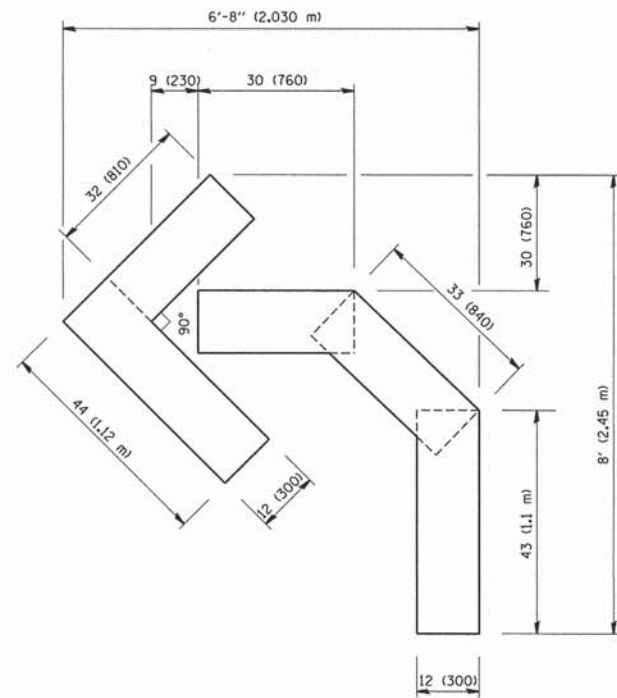
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|---------------------------|--|-------------|----------------|--------------------|---|-----------|
| DISTRICT ONE | | F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| TYPICAL PAVEMENT MARKINGS | | 3573 | 11-00068-00-RS | DuPage | 17 | 12 |
| SCALE: NONE | | TC-13 | | CONTRACT NO. 63894 | | |
| SHEET NO. 1 OF 1 SHEETS | | STA. | TO STA. | | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | |



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

| | | | |
|--|-----------------------------|-----------------|--------------------------------|
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| | | DRAWN - | REVISED -T. RAMMACHER 11-04-97 |
| | PLOT SCALE = 50.0000' / IN. | CHECKED - | REVISED -T. RAMMACHER 03-02-98 |
| | PLOT DATE = 1/4/2008 | DATE = 09-18-94 | REVISED -E. GOMEZ 08-28-00 |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

| PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING | | | |
|---|-------------------------|------|---------|
| SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. | TO STA. |

| F.A.I. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|----------------|--------|--------------------|-----------|
| 3573 | 11-00068-00-RS | DuPage | 17 | 13 |
| TC-16 | | | CONTRACT NO. 63894 | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

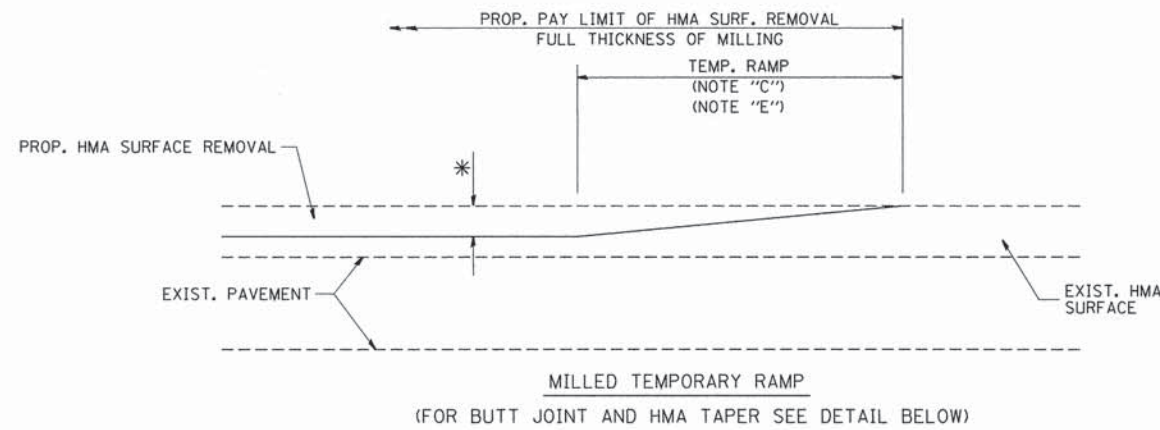
⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

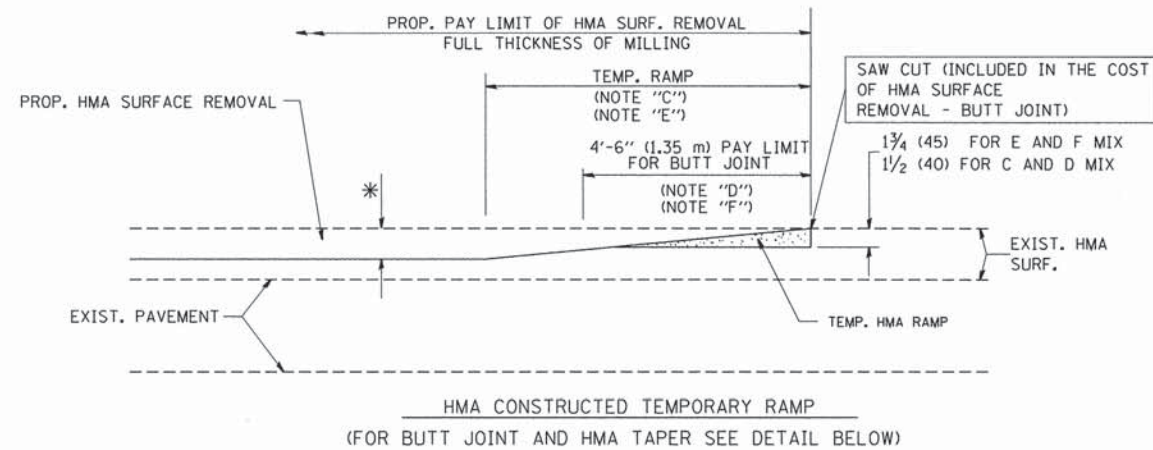
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

| | | | | | | | | | | | | |
|--|-----------------------|----------------------|-----------------------------|---|--|-------------------------|--------------|---|---------|--------|-----------------|--------------|
| FILE NAME * | USER NAME * drvakosgn | DESIGNED - A. HOUSEH | REVISED - R. SHAH 10-03-96 | STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION | CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT | | | F.A.D. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| cr\pw_work\p\dot\drvakosgn\d0108315\bd24.dgn | | DRAWN - | REVISED - A. ABBAS 03-21-97 | | 3573 | 11-00068-00-RS | DuPage | 17 | 14 | | | |
| PLOT SCALE = 50.000 * / IN. | | CHECKED - | REVISED - M. GOMEZ 01-22-01 | | BD600-06 (BD-24) | | | CONTRACT NO. 63894 | | | | |
| PLOT DATE = 12/15/2009 | | DATE - 03-11-94 | REVISED - R. BORO 12-15-09 | | SCALE: NONE | SHEET NO. 1 OF 1 SHEETS | STA. TO STA. | FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

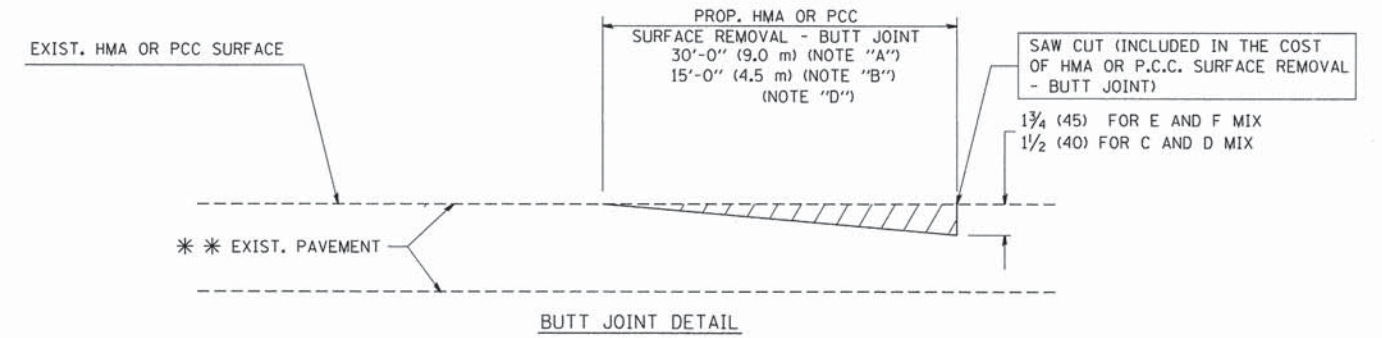


OPTION 1

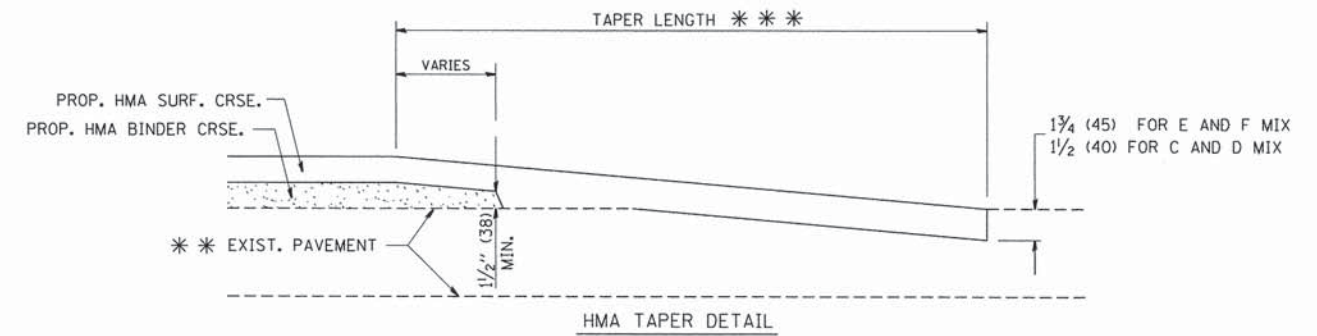


OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

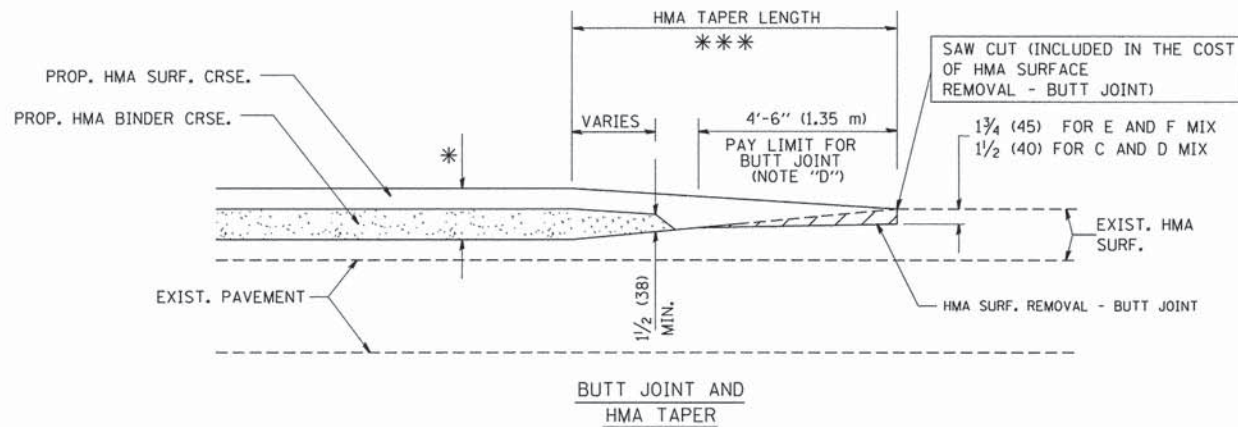
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

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| FILE NAME = W:\d\stata\22x34\bd32.dgn | USER NAME = gaglianobt | DESIGNED - M. DE YONG | REVISED - R. SHAH 10-25-94 |
| | | DRAWN - | REVISED - A. ABBAS 03-21-97 |
| | | CHECKED - | REVISED - M. GOMEZ 04-06-01 |
| | | DATE - 06-13-90 | REVISED - R. BORO 01-01-07 |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

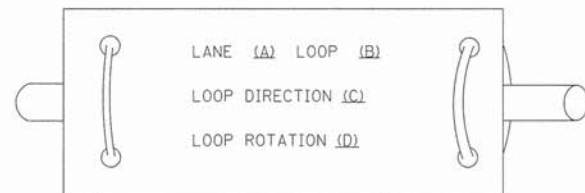
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| F.A.U. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---|----------------|--------|--------------------|-----------|
| 3573 | 11-00068-00-RS | DuPage | 17 | 15 |
| BD400-05 BD32 | | | CONTRACT NO. 63894 | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

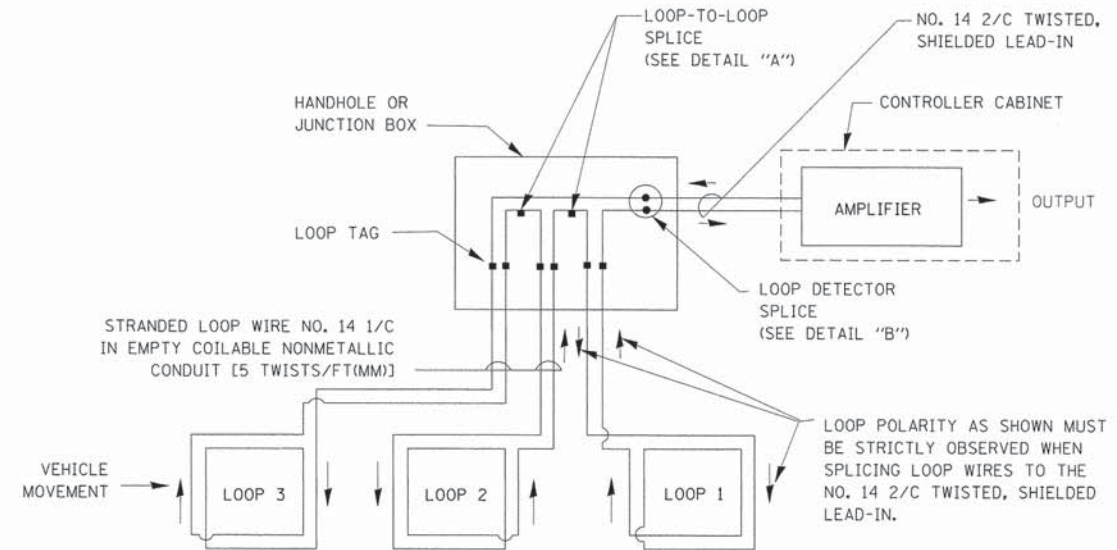
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

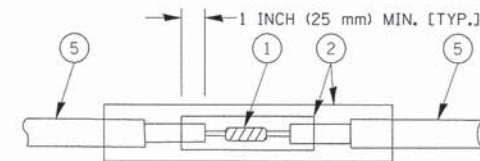


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

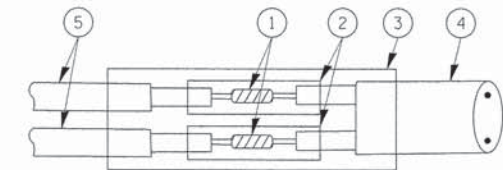


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

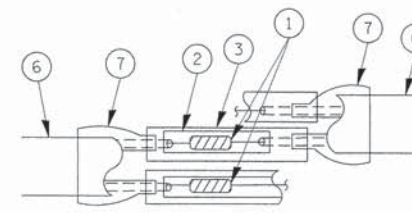


**DETAIL "A"
LOOP-TO-LOOP SPLICE**

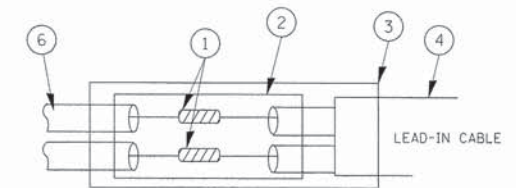


**DETAIL "B"
LOOP-TO-CONTROLLER SPLICE**

TYPE I LOOP



**DETAIL "A"
LOOP-TO-LOOP SPLICE**



**DETAIL "B"
LOOP-TO-CONTROLLER SPLICE**

LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PRE-FORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS, TYCO CBR-2 OR APPROVED EQUAL

| | | | |
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| es:\pv_work\FWIDOT\BAUERDL\d0108315\ss05.dgn | | DRAWN - BCK | REVISED - |
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| | | DATE - 10-28-09 | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

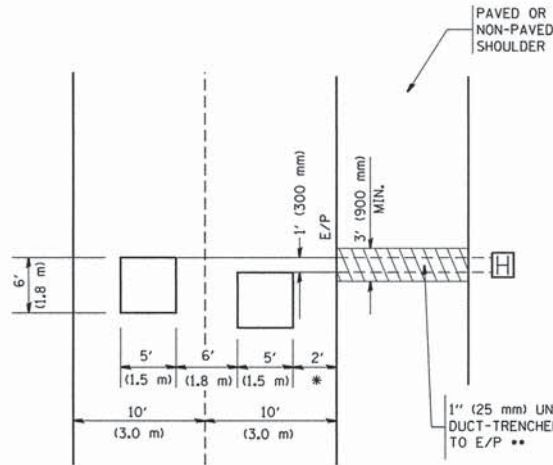
**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

| | | | | |
|---|----------------|--------|--------------------|-----------|
| F.A. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 3573 | 11-00068-00-RS | DuPage | 17 | 16 |
| TS-05 | | | CONTRACT NO. 63894 | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |

SCALE: NONE SHEET NO. 1 OF 6 SHEETS STA. TO STA.

LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

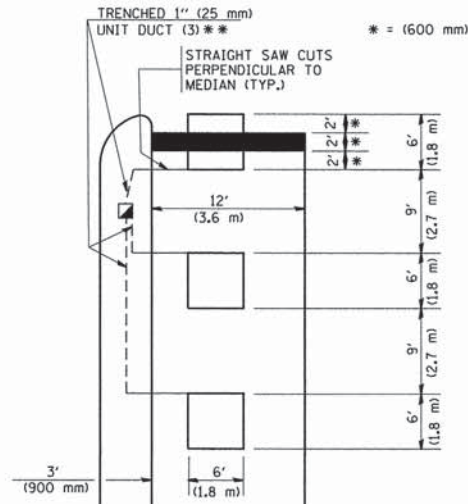


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



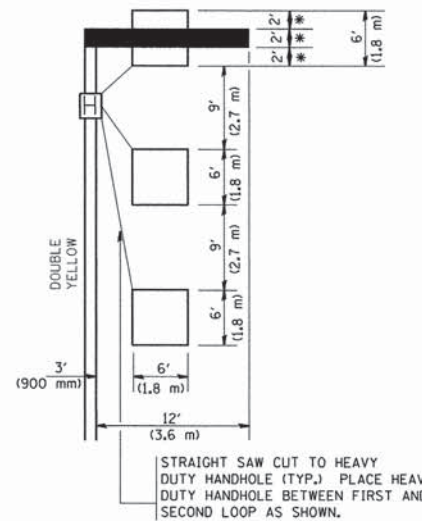
* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

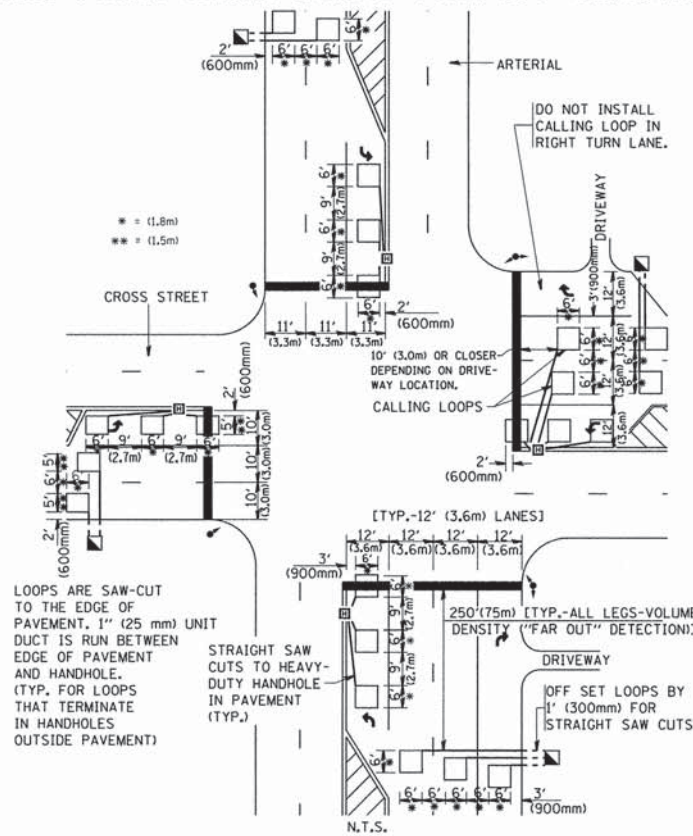
**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

* = (600 mm)



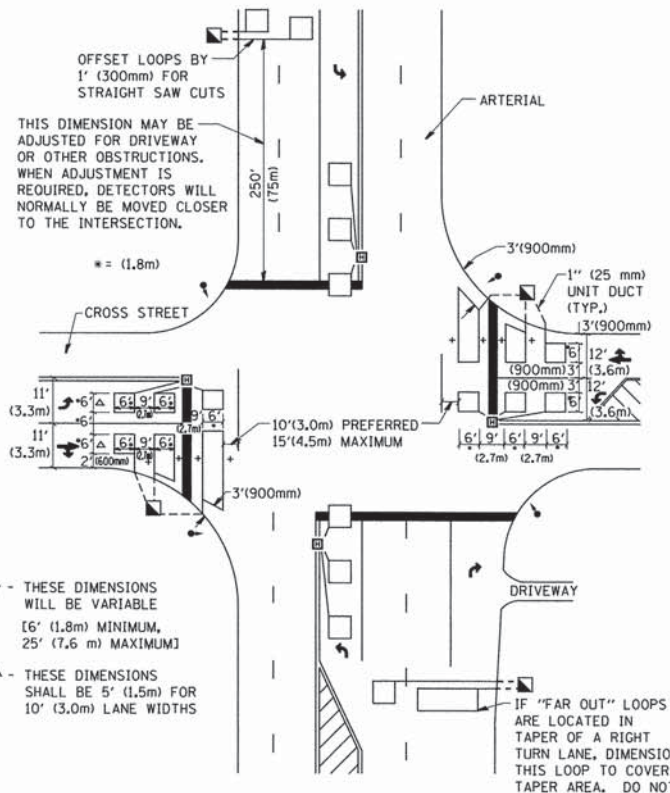
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1
N.T.S.**

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2
N.T.S.**

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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USER NAME = gaglianobt
PLOT SCALE = 50.0000 "/ IN.
PLOT DATE = 1/4/2008

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| DESIGNED - | REVISED - |
| DRAWN - | REVISED - |
| CHECKED - R.K.F. | REVISED - |
| DATE - | REVISED - |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

| | | | | |
|---|----------------|--------|--------------------|-----------|
| F.A.I.T. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 3573 | 11-00068-00-RS | DuPage | 17 | 17 |
| TS-07 | | | CONTRACT NO. 63894 | |
| FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT | | | | |