

26 of 27

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0856	12-00154-00-RS	DUPAGE	26	1

CONTRACT NO. 63893

INDEX OF SHEETS 02-28-14 LETTING ITEM 014 STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION

DIVISION OF HIGHWAYS

PLANS FOR PROPOSED FEDERAL AID HIGHWAY

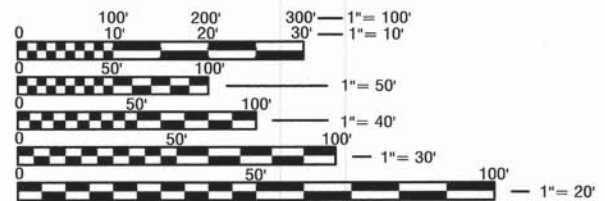
FAP ROUTE 0856 (NAPER BLVD.)
 GARTNER ROAD TO CHICAGO AVE
 RESURFACING
 SECTION 12-00154-00-RS
 PROJECT NO. M-4003(079)
 CITY OF NAPERVILLE
 DUPAGE COUNTY
 JOB NO: C-91-580-12



SHEET NO.	DESCRIPTION
1	TITLE
2	GENERAL NOTES, STANDARDS
3-4	SUMMARY OF QUANTITIES
5-6	TYPICAL SECTIONS
6-10	PROPOSED ROADWAY PLANS
11-13	PAVEMENT MARKING PLANS
14-26	DISTRICT ONE DETAILS

TRAFFIC DATA

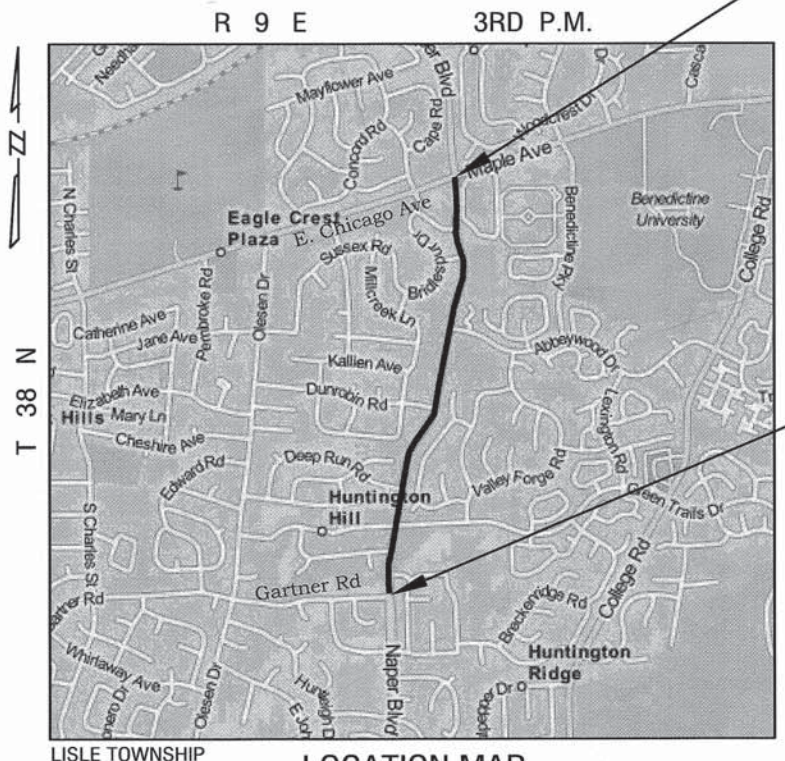
2008 ADT = 29,700
 2030 ADT = 42,000
 POSTED SPEED LIMIT = 40 MPH
 DESIGN DESIGNATION: MINOR ARTERIAL



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
 1-800-892-0123 OR 811

CONTRACT NO. 63893



LOCATION MAP NOT TO SCALE

PROJECT ENDS
 STATION 160+80.00

PROJECT BEGINS
 STATION 99+72.00

GROSS AND NET LENGTH = 6,118.00 FT (1.16 MILES)



Peter J. Zibille
 Expires: DATE: 11/15

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION
 DIVISION OF HIGHWAYS

Approved Dec 4 2013
[Signature]
 CITY OF NAPERVILLE, CITY ENGINEER

Passed DECEMBER 17 2013
[Signature]
 DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS

Released for Bid
 Based on Limited Review December 18 2013
[Signature]
 DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

PRINTED BY THE AUTHORITY
 OF THE STATE OF ILLINOIS

PLANS PREPARED BY:



Naperville
 400 South Eagle Street
 Naperville, IL 60540
 630-420-6100

PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P.E. 847-705-4406 SCHAUMBURG, IL

GENERAL NOTES

SPECIFICATIONS, STANDARDS, AND SPECIAL PROVISIONS:

ALL CONSTRUCTION SHALL BE PERFORMED IN ACCORDANCE WITH THE ILLINOIS DEPARTMENT OF TRANSPORTATION "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED JANUARY 1, 2012. (HEREINAFTER REFERRED TO AS THE STANDARD SPECIFICATIONS); THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1, 2014; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS; THE "DETAILS" ON THE PLANS AND THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS. ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED AS THE LATEST STANDARD OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION.

PROJECT COORDINATION:

THE CONTRACTOR SHALL NOTIFY THE ENGINEER AT LEAST 72 HOURS IN ADVANCE OF BEGINNING WORK AND SHALL COORDINATE ALL CONSTRUCTION OPERATIONS WITH THE ENGINEER. ATTENTION IS CALLED TO SECTION 701 OF THE STANDARD SPECIFICATIONS AND THE SPECIAL PROVISIONS FOR TRAFFIC CONTROL AND PROTECTION. THE STORAGE OF EQUIPMENT AND/OR MATERIALS WITHIN THE RIGHT-OF-WAY SHALL REQUIRE PRIOR WRITTEN APPROVAL OF THE ENGINEER.

PERMITTING:

THE CONTRACTOR MUST OBTAIN ALL APPLICABLE PERMITS FOR THE COMPLETION OF THE WORK, INCLUDING BUT NOT LIMITED TO DUPAGE COUNTY DOT, VILLAGE OF LISLE, AND LISLE TOWNSHIP. THE COST OF ALL PERMITS SHALL BE INCLUDED IN THE COST OF MOBILIZATION.

SAFETY:

PER ARTICLE 701.120F OF THE STANDARD SPECIFICATIONS, ALL CONSTRUCTION PERSONNEL SHALL BE REQUIRED TO WEAR FLUORESCENT SAFETY VESTS OF THE SPECIFIED COLOR/S AT ALL TIMES WHILE ON THE CONSTRUCTION SITE.

TRAFFIC CONTROL AND MAINTENANCE:

THE CONTRACTOR SHALL SCHEDULE HIS WORK SO THAT ONLY ONE TEMPORARY LANE CLOSURE IN EACH DIRECTION IS IMPLEMENTED AT A TIME. A LANE CLOSURE WILL ONLY BE PERMITTED DURING CONSTRUCTION OPERATIONS AND IN ACCORDANCE WITH THE APPLICABLE IDOT STANDARD. THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN ALL SIGNS, BARRICADES, AND OTHER TRAFFIC CONTROL DEVICES INCLUDING FLAGGERS REQUIRED TO MAINTAIN TRAFFIC FLOW. THE CONTRACTOR SHALL SCHEDULE HIS WORK SO THAT NO HOLES IN THE PAVEMENT, AS DETERMINED BY THE ENGINEER, REMAIN OPEN OVER NIGHT. ACCESS TO ADJUTING PROPERTY SHALL BE PROVIDED AT ALL TIMES DURING CONSTRUCTION OF THIS PROJECT. TIME REQUIRED FOR CONSTRUCTION AT DRIVEWAYS SHALL BE LIMITED TO THE MINIMUM TIME REQUIRED FOR SAID CONSTRUCTION AND, IF REQUIRED, TEMPORARY AGGREGATE SURFACE FOR DRIVEWAY ACCESS SHALL BE PROVIDED.

UTILITY COORDINATION:

THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ALL WORK WITH ALL UTILITIES PER ARTICLE 107.31 OF THE STANDARD SPECIFICATIONS INCLUDING, BUT NOT LIMITED TO, THE DUPAGE COUNTY WATER COMMISSION, DUPAGE COUNTY DEPARTMENT OF PUBLIC WORKS, AND COMMONWEALTH EDISON

PAVEMENT STRIPING:

BEFORE BEGINNING WORK, THE CONTRACTOR SHALL RETAIN AND RECORD (FOR FUTURE REFERENCES) ALL EXISTING PAVEMENT MARKING LINES IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. LOCATIONS OF ALL PROPOSED STRIPING SHALL BE AS DIRECTED BY THE ENGINEER.

SAW CUTTING FOR REMOVAL:

THE CONTRACTOR SHALL BE REQUIRED TO MAKE A FULL DEPTH SAW CUT AT THE EDGE OF PAVEMENT ADJACENT TO THE REMOVAL OF ALL CONCRETE CURB OR COMBINATION CONCRETE CURB AND GUTTER. THE CONTRACTOR SHALL MAKE ALL FULL DEPTH SAW CUTS REQUIRED FOR THE REMOVAL OF CONCRETE CURB AND GUTTERS, SIDEWALKS, AND DRIVEWAYS AS SPECIFIED, OR AS DIRECTED BY THE ENGINEER. THE COST SHALL BE CONSIDERED INCLUDED IN THE COST FOR REMOVAL OF THE SPECIFIED ITEM IN THE CONTRACT.

COMMITMENTS:

THE CONTRACTOR SHALL COMPLETE ALL WORK TO THE SATISFACTION OF THE ENGINEER BY THE END OF THE DAY ON AUGUST 15, 2014. THE SCHEDULE COMPLIES WITH THE NAPERVILLE COMMUNITY UNIT SCHOOL DISTRICT #203 SCHEDULE SO NO WORK OCCURS WHILE SCHOOL IS IN SESSION.

WORKING HOURS

THE WORKING HOURS WILL BE LIMITED TO BETWEEN 7 AM AND 3 PM. HOWEVER, BETWEEN 7 AM AND 9 AM, NO LANE CLOSURES WILL BE ALLOWED IN THE NORTH BOUND DIRECTION.

BASE COURSE CLEANING:

PRIOR TO APPLYING THE BITUMINOUS PRIME COAT, THE BASE SURFACE INCLUDING GUTTERS SHALL BE CLEANED OF LOOSE GRINDINGS, LEAVES, OF ALL DUST, DIRT, WEEDS, AND OTHER FOREIGN MATERIALS. ALL CRACK FILL MATERIAL SHALL BE REMOVED IN ITS ENTIRETY ALONG THE CURB LINE. COST TO BE INCLUDED IN THE COST OF THE HOT-MIX ASPHALT SURFACE WORK.

BASE PATCHING AND REPAIR:

PRIOR TO THE PLACEMENT OF PATCHES FOR BASE REPAIR, THE SUBGRADE SHALL BE INSPECTED BY THE ENGINEER. IF ADDITIONAL SUBBASE REPAIR IS NECESSARY, THE AREA SHALL BE UNDERCUT AND BACKFILLED WITH AGGREGATE SUBGRADE IMPROVEMENT MATERIAL AS APPROVED BY THE ENGINEER.

CLEAN-UP AND DISPOSAL:

THE CONTRACTOR SHALL MAINTAIN THE SITE IN A CLEAN AND ORDERLY MANNER. DEBRIS AND ANY SURPLUS MATERIAL SHALL BE REMOVED AND RESTORATION SHALL PROCEED AS THE WORK PROCEEDS. IF THE ENGINEER SO DIRECTS, THE CONTRACTOR SHALL STOP ALL OTHER WORK AND CONCENTRATE ON CLEAN-UP AND RESTORATION. DEBRIS AND SURPLUS MATERIAL SHALL BE DISPOSED OF BY THE CONTRACTOR AT AN APPROVED OFF-SITE DISPOSAL AREA.

CONSTRUCTION LIMITS:

THE CONTRACTOR SHALL CONFINE OPERATIONS WITHIN THE DEDICATED ROADWAY RIGHTS-OF-WAY. ANY DAMAGE OUTSIDE OF THE LIMITS OF OPERATION SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.

LANDSCAPING RESTORATION:

ALL LANDSCAPING DAMAGED DURING CONSTRUCTION SHALL BE RESTORED WITH A MINIMUM OF 4" OF PULVERIZED TOP SOIL, SODDING, AND FERTILIZER NUTRIENTS.

PUBLIC UTILITY LOCATIONS:

THE LOCATION OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, AND ANY OTHER PUBLIC UTILITIES AS SHOWN ON THE PLANS IS APPROXIMATE AND THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR. THIS WORK SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

CURB AND GUTTER REPLACEMENT:

THE MINIMUM THICKNESS OF THE PROPOSED GUTTER FLAG SHALL BE 10" UNLESS OTHERWISE STATED IN THE PLANS OR DIRECTED BY THE ENGINEER. DISTURBED PAVEMENT AND GROUND AREAS SHALL BE RESTORED IMMEDIATELY FOLLOWING REPLACEMENT OPERATIONS AND IN ALL CASES WITHIN THREE (3) WORKING DAYS FROM THE DATE THE CURB AND GUTTER WAS CAST. THE ENGINEER SHALL STOP THE CONTRACTOR FROM FURTHER REMOVAL OPERATIONS AT ANY TIME HE DETERMINES THE RESTORATION SHALL RESULT IN THE ENFORCEMENT OF LIQUIDATED DAMAGES IN THE AMOUNT SPECIFIED IN ARTICLE 108.09 OF THE STANDARD SPECIFICATIONS.

REMOVAL OF EXISTING PAVEMENT AND APPURTENANCES:

WHEN PORTIONS OF EXISTING PAVEMENTS OR APPURTENANCES ARE TO REMAIN IN PLACE, OR ADJACENT EXISTING PAVEMENTS OR APPURTENANCES ARE TO REMAIN IN PLACE, THE CONTRACTOR SHALL FORM A PERPENDICULAR STRAIGHT JOINT BY FULL-DEPTH MACHINE SAWING AT THE ENDS AND ALL EDGES OF PORTIONS TO BE REMOVED TO PREVENT SURFACE SPALLING WHEN THE EXISTING PAVEMENT OR APPURTENANCE TO REMAIN IN PLACE SHALL BE REPAIRED OR REMOVED AND REPLACED BY THE CONTRACTOR AT HIS/HER OWN EXPENSE, AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE MEASURED FOR PAYMENT BUT SHALL BE CONSIDERED INCLUDED IN THE ITEM BEING REMOVED.

PHASE 3 CONSTRUCTION ENGINEERING

PHASE 3 CONSTRUCTION ENGINEERING WILL BE PERFORMED BY THE CITY OF NAPERVILLE.

STATE STANDARDS

STANDARD NO.	DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
424001-07	CURB RAMPS FOR SIDEWALKS
442201-05	CLASS C AND D PATCHES
606001-05	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701427-02	LANE CLOSURE, MULTILANE, INTERMITTENT, OR MOVING OPER. FOR SPEEDS 40 MPH
701602-07	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701606-09	URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-09	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-05	LANE CLOSURE MULTILANE 1W OR 2W CROSSWALK OR SIDEWALK CLOSURE
701901-03	TRAFFIC CONTROL DEVICES
780001-04	TYPICAL PAVEMENT MARKINGS
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUTS FOR DETECTION LOOPS

DISTRICT 1 STANDARDS

STANDARD NO.	DESCRIPTION
BD-01	DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER \geq 15' (4.5 m)
BD-02	DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB < 15' (4.5 m)
BD-08	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING
BD-22	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
BD-24	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT
BD-32	BUTT JOINT AND HMA TAPER DETAILS
TC-10	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
TC-13	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
TC-14	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)
TC-16	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING
TC-22	ARTERIAL ROAD INFORMATION SIGN
TC-26	DRIVEWAY ENTRANCE SIGNING
TS-05	STANDARD TRAFFIC SIGNAL DESIGN DETAILS
TS-07	DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING

FILE NAME = 04-TypSections_Mopar_LAPP.dwg

DESIGNED	-	PJZ	REVISED	-
DRAWN	-	MCP	REVISED	-
CHECKED	-	RJK	REVISED	-
DATE	-	12/23/13	REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES & STANDARDS

SCALE: N.T.S.

SHEET NO. 1 OF 1 SHEETS

STA. -- TO STA. --

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0856	12-00154-00-RS	DUPAGE	26	2
CONTRACT NO. 63893				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

SUMMARY OF QUANTITIES

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY 0005
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	204	204
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	266	266
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	5	5
25000500	PHOSPHOROUS FERTILIZER NUTRIENT	POUND	5	5
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	5	5
25200110	SODDING, SALT TOLERANT	SQ YD	266	266
30300001	AGGREGATE SUBGRADE IMPROVEMENT	CU YD	204	204
35101500	AGGREGATE BASE COURSE, TYPE B	CU YD	2	2
40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	50	50
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	10	10
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	TON	1,582	1,582
40600895	CONSTRUCTING TEST STRIP	EACH	2	2
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	290	290
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	403	403
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	57	57
40603595	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F", N90	TON	4,217	4,217
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	3,220	3,220
42400800	DETECTABLE WARNINGS	SQ FT	230	230
44000160	HOT-MIX ASPHALT SURFACE REMOVAL, 2 3/4"	SQ YD	37,650	37,650
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	465	465
44000600	SIDEWALK REMOVAL	SQ FT	2,760	2,760

CODE NO.	ITEM	UNIT	TOTAL QUANTITY	ROADWAY 0005
44002224	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 6"	SQ YD	1,200	1,200
44201789	CLASS D PATCHES, TYPE II, 12 INCH	SQ YD	200	200
44201794	CLASS D PATCHES, TYPE III, 12 INCH	SQ YD	400	400
44201796	CLASS D PATCHES, TYPE IV, 12 INCH	SQ YD	600	600
67100100	MOBILIZATION	L SUM	1	1
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1
70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	L SUM	1	1
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	6	6
70300100	SHORT TERM PAVEMENT MARKING	FOOT	4,070	4,070
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SQ FT	1,104	1,104
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	34,856	34,856
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	3,134	3,134
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	2,176	2,176
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	520	520
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	552	552
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	17,428	17,428
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1,567	1,567
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	1,088	1,088
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	280	280

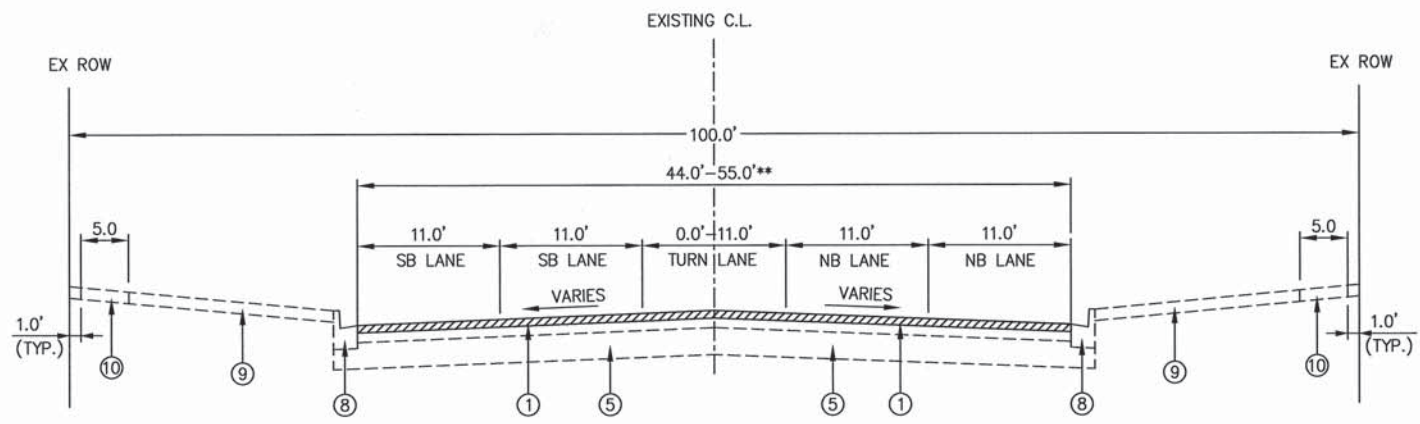
* = SPECIALTY ITEM

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0856	12-00154-00-RS	DUPAGE	26	3
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 63893	

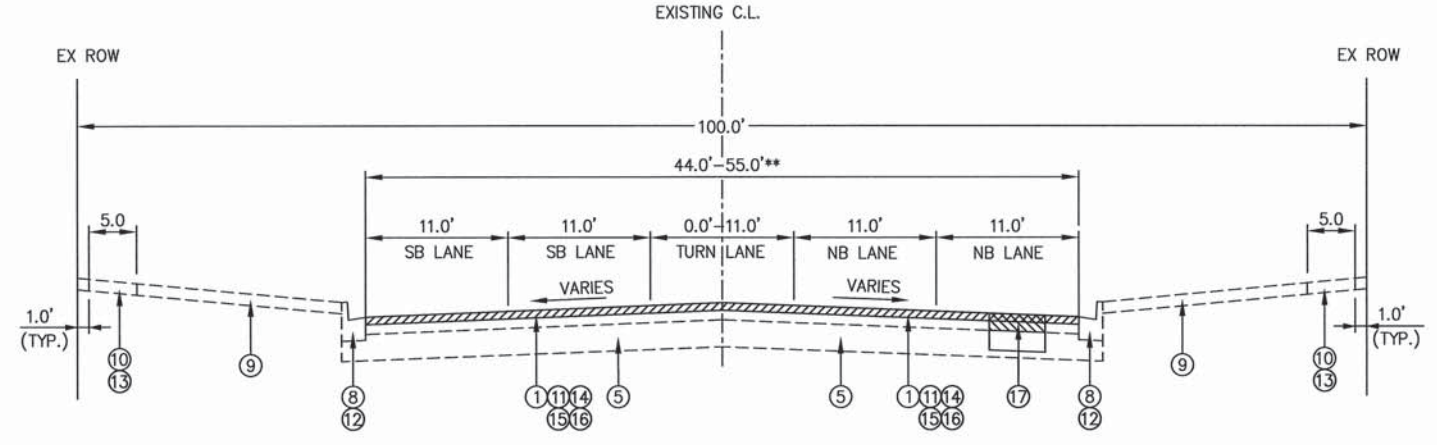
SCALE: N.T.S. SHEET NO. 1 OF 2 SHEETS STA. -- TO STA. --



EXISTING TYPICAL SECTION NO. 1

NAPER BOULEVARD
 STA 99+72 TO STA 110+61
 STA 120+61 TO STA 145+52

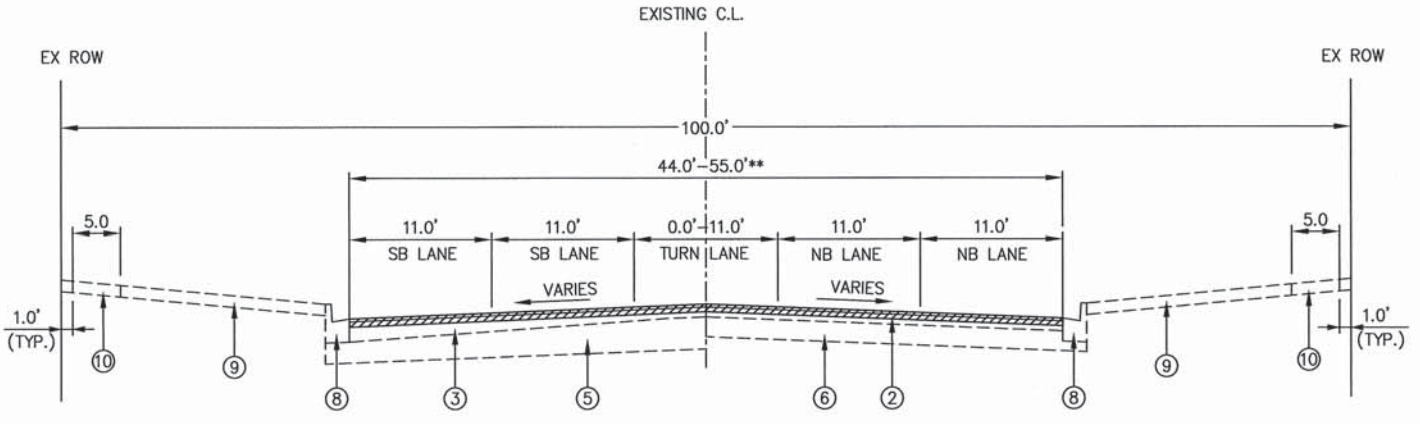
**** EXISTING PAVEMENT WIDTHS:**
 STA 99+83 TO STA 110+61 - 55.0'
 STA 120+61 TO STA 123+35 - 44.0'
 STA 123+35 TO STA 126+35 - 44.0' TO 55.0'
 STA 126+35 TO STA 128+85 - 55.0'
 STA 128+85 TO STA 131+35 - 55.0' TO 44.0'
 STA 131+35 TO STA 135+62 - 44.0'
 STA 135+62 TO STA 137+27 - 44.0' TO 55.0'
 STA 137+27 TO STA 145+52 - 55.0'



PROPOSED TYPICAL SECTION NO. 1

NAPER BOULEVARD
 STA 99+72 TO STA 110+61
 STA 120+61 TO STA 145+52

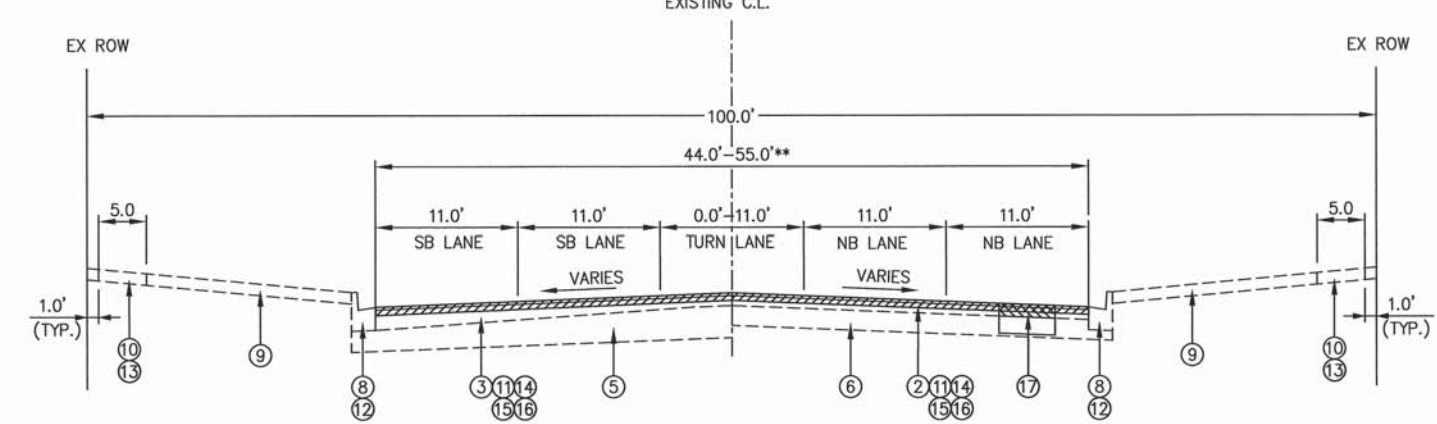
**** PROPOSED PAVEMENT WIDTHS:**
 STA 99+83 TO STA 110+61 - 55.0'
 STA 120+61 TO STA 123+35 - 44.0'
 STA 123+35 TO STA 126+35 - 44.0' TO 55.0'
 STA 126+35 TO STA 128+85 - 55.0'
 STA 128+85 TO STA 131+35 - 55.0' TO 44.0'
 STA 131+35 TO STA 135+62 - 44.0'
 STA 135+62 TO STA 137+27 - 44.0' TO 55.0'
 STA 137+27 TO STA 145+52 - 55.0'



EXISTING TYPICAL SECTION NO. 2

NAPER BOULEVARD
 STA 110+61 TO STA 120+61

**** EXISTING PAVEMENT WIDTHS:**
 STA 110+61 TO STA 115+11 - 55.0'
 STA 115+11 TO STA 118+11 - 55.0' TO 44.0'
 STA 118+11 TO STA 120+61 - 44.0'



PROPOSED TYPICAL SECTION NO. 2

NAPER BOULEVARD
 STA 110+61 TO STA 120+61

**** PROPOSED PAVEMENT WIDTHS:**
 STA 110+61 TO STA 115+11 - 55.0'
 STA 115+11 TO STA 118+11 - 55.0' TO 44.0'
 STA 118+11 TO STA 120+61 - 44.0'

LEGEND

EXISTING:

- ① EXISTING HMA SURFACE COURSE, 2.5" AND HMA BINDER COURSE, 3.5"
- ② EXISTING HMA SURFACE COURSE, 3.0" AND HMA BINDER COURSE, 1.5"
- ③ EXISTING HMA SURFACE COURSE, 3.0" AND HMA BINDER COURSE, 1.5"-10.0"
- ④ EXISTING HMA SURFACE COURSE, 2.25" AND HMA BINDER COURSE, 8.25"
- ⑤ EXISTING AGGREGATE BASE COURSE, 22" OR CEMENT AGGREGATE MATERIALS (CAM) BASE COURSE, 11.5", LOCATIONS VARY.
- ⑥ EXISTING CEMENT AGGREGATE MATERIALS (CAM), 11.5"
- ⑦ EXISTING AGGREGATE BASE COURSE, 6"
- ⑧ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ⑨ EXISTING TOPSOIL
- ⑩ EXISTING PCC SIDEWALK, 5"

PROPOSED:

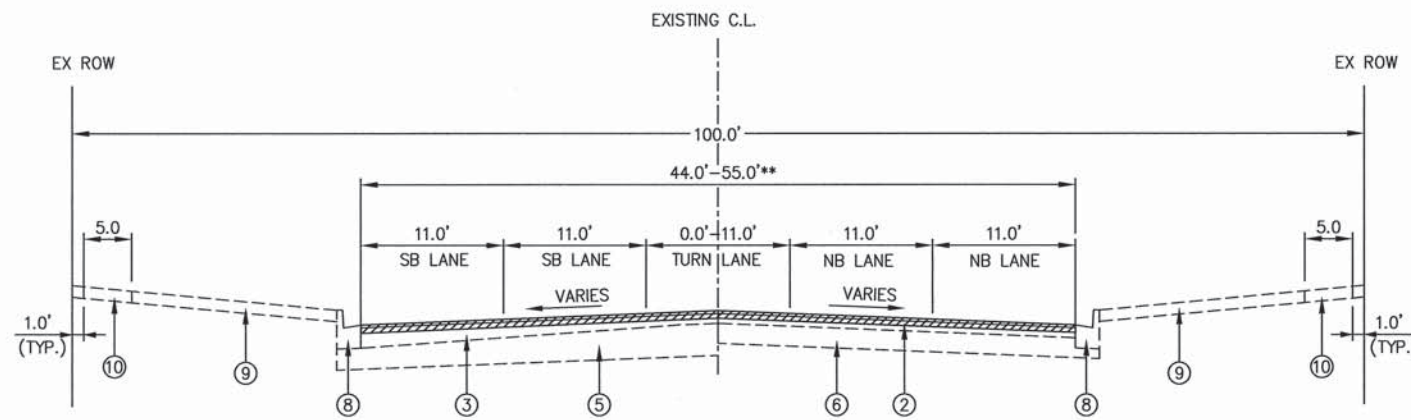
- ⑪ HMA SURFACE REMOVAL, 2-3/4" (44000159) (SEE EXISTING)
- ⑫ COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (Z0004562) AS DIRECTED BY ENGINEER
- ⑬ PCC SIDEWALK 5" (42400200), WITH AGG. BASE COURSE, TYPE B 2" (35101500) AND SIDEWALK REMOVAL (44000600) AS DIRECTED BY ENGINEER
- ⑭ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F" N90, 2" (40603595)
- ⑮ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4" (40600827)
- ⑯ BITUMINOUS MATERIALS (PRIME COAT) (40600100)
- ⑰ CLASS D PATCH, AS DIRECTED BY ENGINEER (SEE NOTE 2)

NOTE 1: CONTRACTOR SHALL PATCH BEFORE MILLING. SEE PATCHING DETAIL BD400-04 (BD-22) FOR INCLUDED ITEMS TO PATCHING COST AND FOR ADDITIONAL PATCHING DETAILS.

NOTE 2: AT LOCATIONS OF UNSUITABLE MATERIAL, AS DETERMINED BY THE ENGINEER, THE SUBGRADE TREATMENT SHALL CONSIST OF EXCAVATION OF UNSUITABLE MATERIAL TO A DEPTH OF 10 INCHES BELOW EXISTING PAVEMENT BASE AND PLACING AGGREGATE SUBGRADE IMPROVEMENT.

FILE NAME: 04-TypSections_Naper_LAPP.dwg

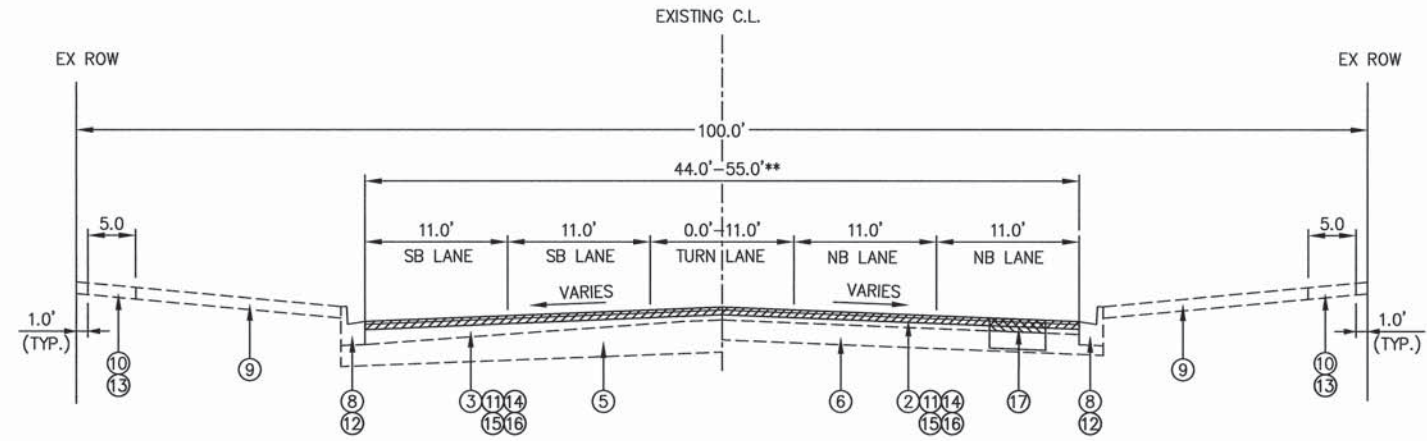
DESIGNED - PJZ	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	NAPER BOULEVARD TYPICAL SECTIONS		F.A.P. RTE. 0856	SECTION 12-00154-00-RS	COUNTY DUPAGE	TOTAL SHEETS 26	SHEET NO. 5
			SCALE: N.T.S.	SHEET NO. 1 OF 2 SHEETS	STA. - TO STA. -	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			
DRAWN - PJZ	REVISED -				CONTRACT NO. 63893				
CHECKED - RJK	REVISED -								
DATE - 12/23/13	REVISED -								



EXISTING TYPICAL SECTION NO. 2

NAPER BOULEVARD
STA 110+61 TO STA 120+61

**** EXISTING PAVEMENT WIDTHS:**
STA 110+61 TO STA 115+11 - 55.0'
STA 115+11 TO STA 118+11 - 55.0' TO 44.0'
STA 118+11 TO STA 120+61 - 44.0'



PROPOSED TYPICAL SECTION NO. 2

NAPER BOULEVARD
STA 110+61 TO STA 120+61

**** PROPOSED PAVEMENT WIDTHS:**
STA 110+61 TO STA 115+11 - 55.0'
STA 115+11 TO STA 118+11 - 55.0' TO 44.0'
STA 118+11 TO STA 120+61 - 44.0'

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ Ndes
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, (IL-9.5 mm) MIX "F", N90 (2")	4% @ 90 Gyr.
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, (3/4")	3.5% @ 50 Gyr.
CLASS D PATCHES 12" (IN 3 LIFTS) (HMA BINDER IL-19 mm)	4% @ 70 Gyr.
<u>DRIVEWAYS</u> HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm); 3"	4% @ 50 Gyr.

1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN
2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.
3. FOR "PERCENT OF RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

LEGEND

EXISTING:

- ① EXISTING HMA SURFACE COURSE, 2.5" AND HMA BINDER COURSE, 3.5"
- ② EXISTING HMA SURFACE COURSE, 3.0" AND HMA BINDER COURSE, 1.5"
- ③ EXISTING HMA SURFACE COURSE, 3.0" AND HMA BINDER COURSE, 1.5"-10.0"
- ④ EXISTING HMA SURFACE COURSE, 2.25" AND HMA BINDER COURSE, 8.25"
- ⑤ EXISTING AGGREGATE BASE COURSE, 22" OR CEMENT AGGREGATE MATERIALS (CAM) BASE COURSE, 11.5", LOCATIONS VARY.
- ⑥ EXISTING CEMENT AGGREGATE MATERIALS (CAM), 11.5"
- ⑦ EXISTING AGGREGATE BASE COURSE, 6"
- ⑧ EXISTING COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12
- ⑨ EXISTING TOPSOIL
- ⑩ EXISTING PCC SIDEWALK, 5"

PROPOSED:

- ⑪ HMA SURFACE REMOVAL, 2-3/4" (44000159) (SEE EXISTING)
- ⑫ COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT (Z0004562) AS DIRECTED BY ENGINEER
- ⑬ PCC SIDEWALK 5" (42400200), WITH AGG. BASE COURSE, TYPE B 2" (35101500) AND SIDEWALK REMOVAL (44000600) AS DIRECTED BY ENGINEER
- ⑭ POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "F" N90, 2" (40603595)
- ⑮ POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50, 3/4" (40600827)
- ⑯ BITUMINOUS MATERIALS (PRIME COAT) (40600100)
- ⑰ CLASS D PATCH, AS DIRECTED BY ENGINEER (SEE NOTE 2)

NOTE 1: CONTRACTOR SHALL PATCH BEFORE MILLING. SEE PATCHING DETAIL BD400-04 (BD-22) FOR INCLUDED ITEMS TO PATCHING COST AND FOR ADDITIONAL PATCHING DETAILS.

NOTE 2: AT LOCATIONS OF UNSUITABLE MATERIAL, AS DETERMINED BY THE ENGINEER, THE SUBGRADE TREATMENT SHALL CONSIST OF EXCAVATION OF UNSUITABLE MATERIAL TO A DEPTH OF 10 INCHES BELOW EXISTING PAVEMENT BASE AND PLACING AGGREGATE SUBGRADE IMPROVEMENT.

FILE NAME - 04-TypSections_Naper_LAPP.dwg

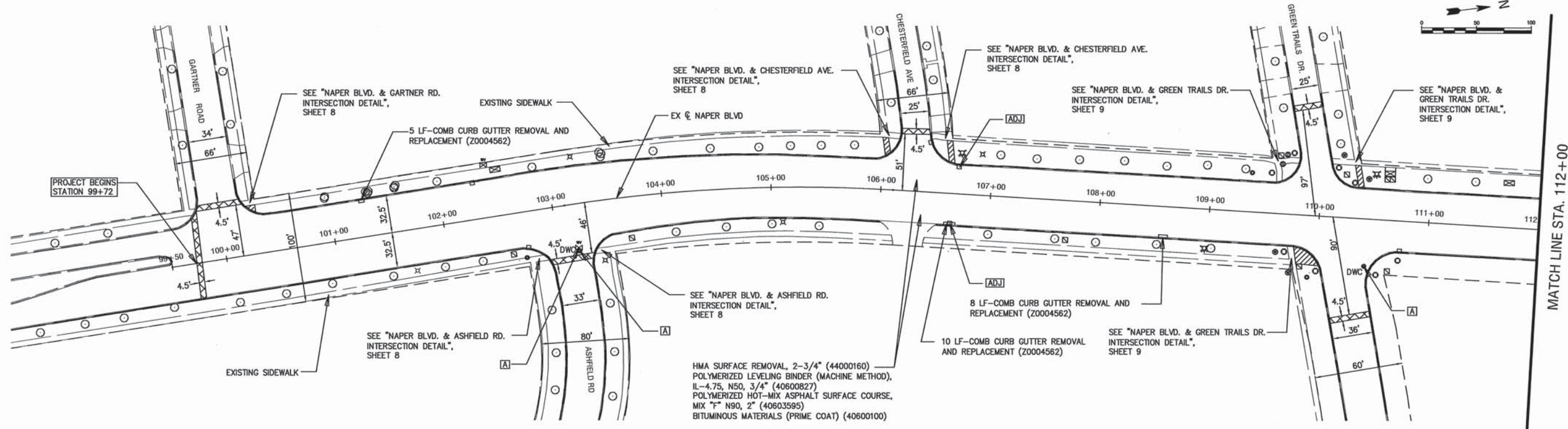
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DRAWN -	PJZ	REVISED -	
CHECKED -	RJK	REVISED -	
DATE -	12/23/13	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

NAPER BOULEVARD
TYPICAL SECTIONS

SCALE: N.T.S. SHEET NO. 2 OF 2 SHEETS STA. -- TO STA. --

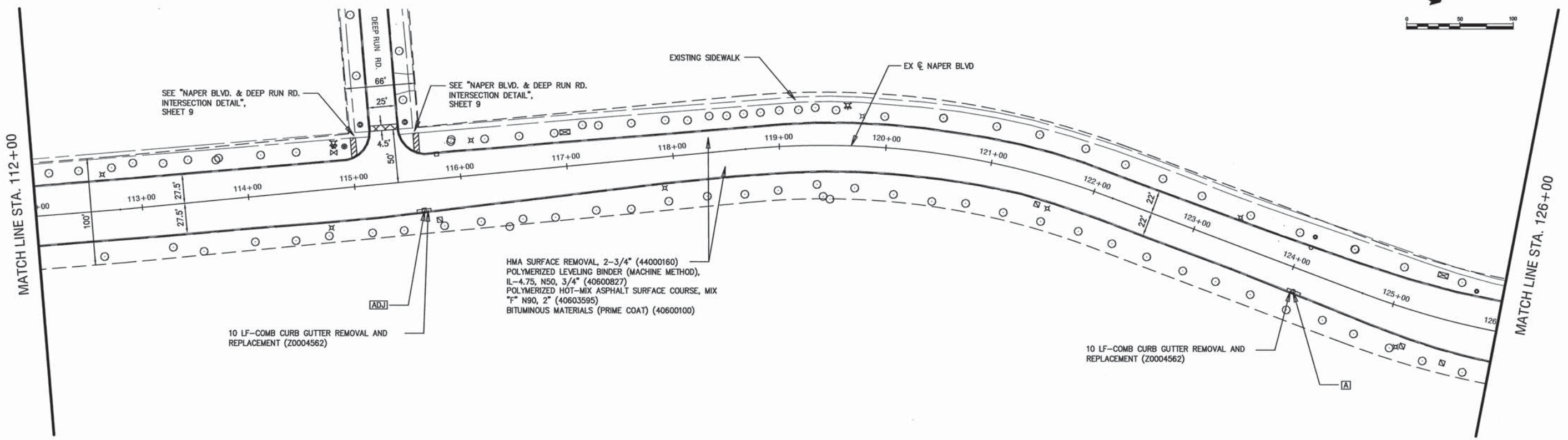
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0856	12-00154-00-RS	DUPAGE	26	6
CONTRACT NO. 63893				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:
 1. THE ACTUAL LOCATION OF PAVEMENT PATCHING WILL BE DETERMINED IN THE FIELD, DURING CONSTRUCTION, BY THE RESIDENT ENGINEER.

LEGEND:

[A]	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL) (X6030310)
[ADJ]	DRAINAGE & UTILITY STRUCTURES ADJUSTMENT (SPECIAL) (Z0017500)
[Hatched Box]	SIDEWALK REMOVAL (44000600) *NO REPLACEMENT*
[Cross-hatched Box]	HMA SURFACE REMOVAL-BUTT JOINT (40600982)



FILE NAME - \$FILES

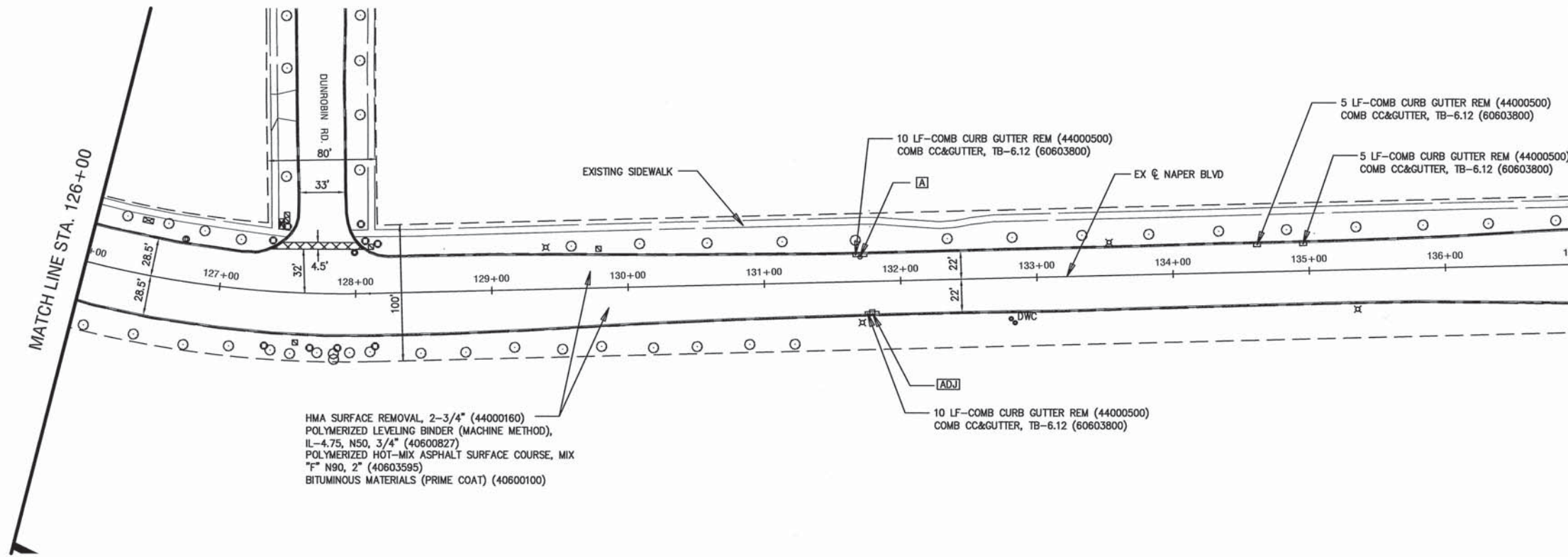
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DRAWN	-	MCP	REVISED	-
CHECKED	-	RJK	REVISED	-
DATE	-	11/20/2013	REVISED	-

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

NAPER BOULEVARD
 PROPOSED ROADWAY PLAN

SCALE: 1" = 50 FT SHEET NO. 1 OF 4 SHEETS STA. 99+50.00 TO STA. 126+00.00

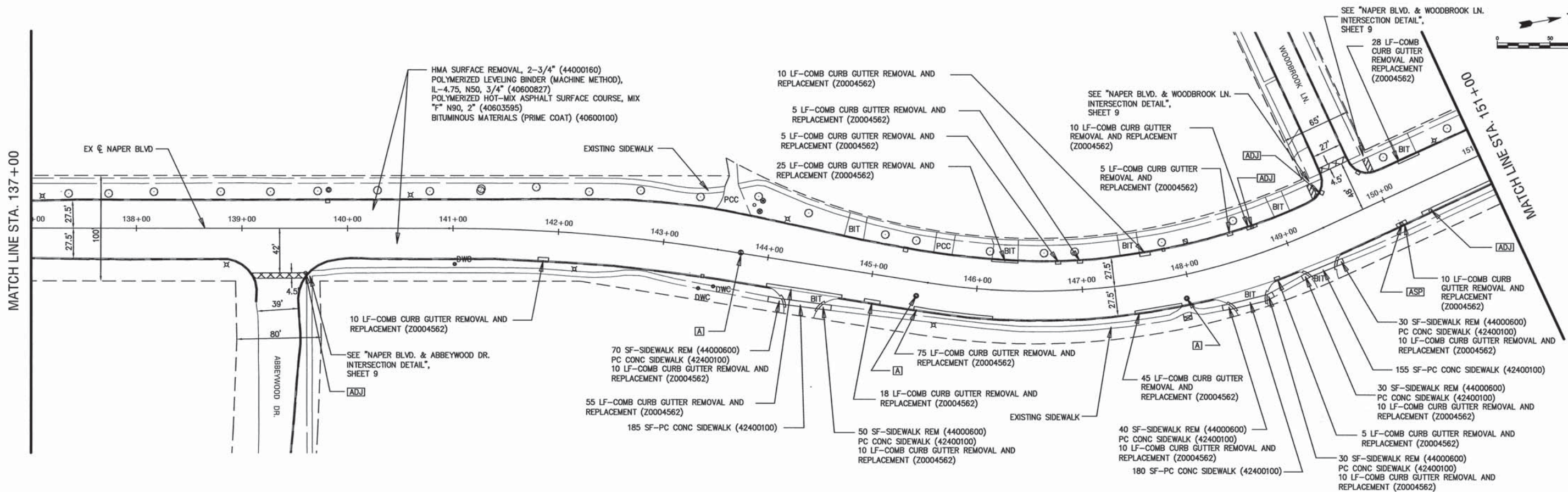
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0856	12-00154-00-RS	DUPAGE	26	7
CONTRACT NO. 63893				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:
 1. THE ACTUAL LOCATION OF PAVEMENT PATCHING WILL BE DETERMINED IN THE FIELD, DURING CONSTRUCTION, BY THE RESIDENT ENGINEER.

LEGEND:

[A]	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL) (X6030310)
[ADJ]	DRAINAGE & UTILITY STRUCTURES ADJUSTMENT (SPECIAL) (Z0017500)
[Hatched]	SIDEWALK REMOVAL (44000600) *NO REPLACEMENT*
[X]	HMA SURFACE REMOVAL-BUTT JOINT (40600982)



FILE NAME - 8745

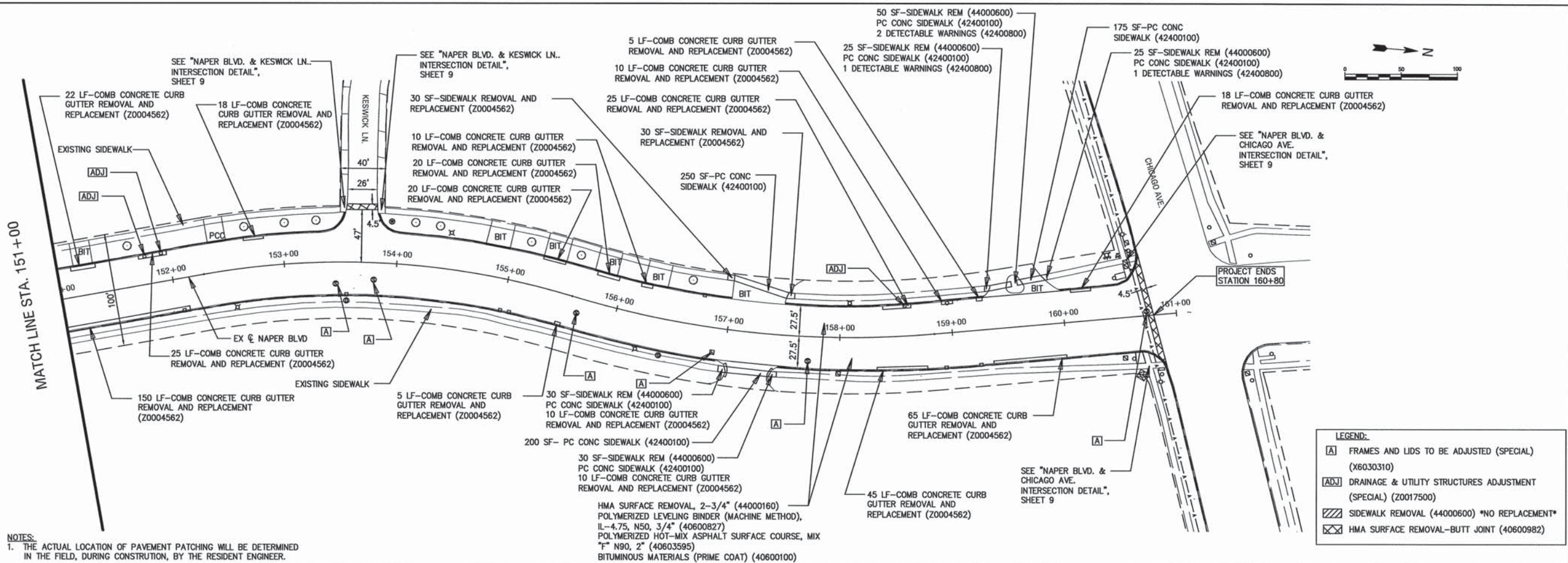
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DRAWN -	MCP	REVISED -	
CHECKED -	RJK	REVISED -	
DATE -	11/20/2013	REVISED -	

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

NAPER BOULEVARD
 PROPOSED ROADWAY PLAN

SCALE: 1" = 50 FT SHEET NO. 2 OF 4 SHEETS STA. 126+00.00 TO STA. 151+00.00

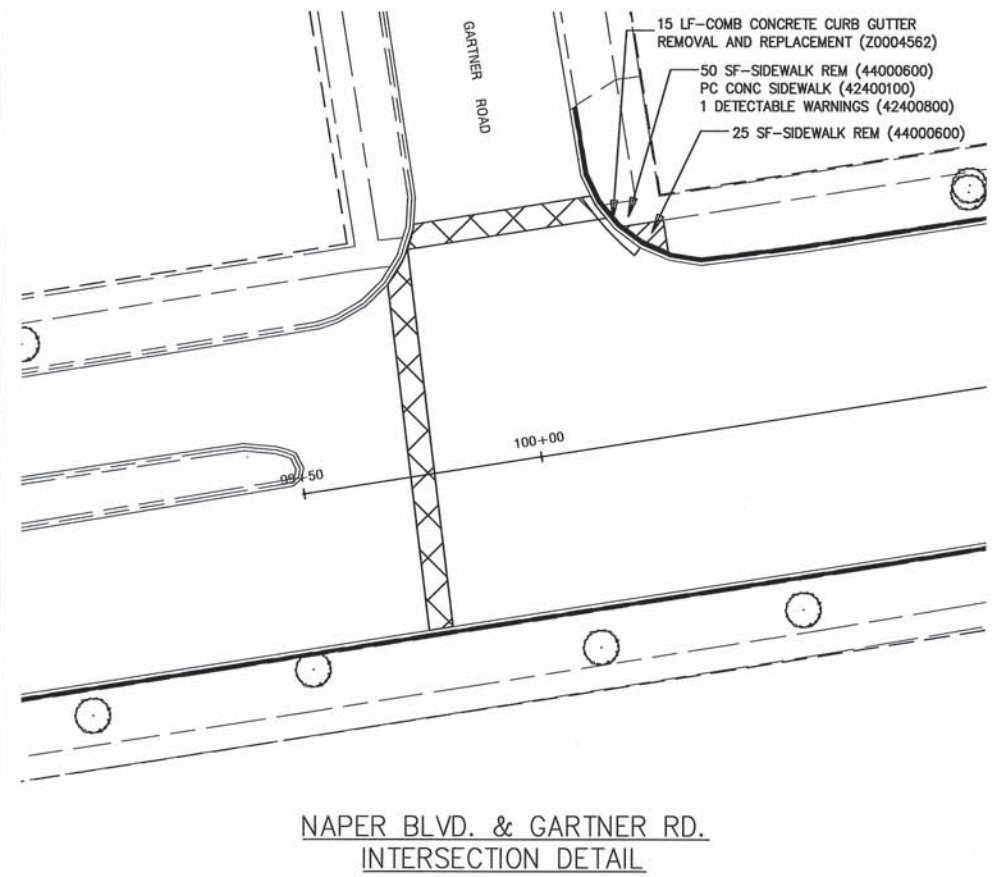
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CONTRACT NO. 63893				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



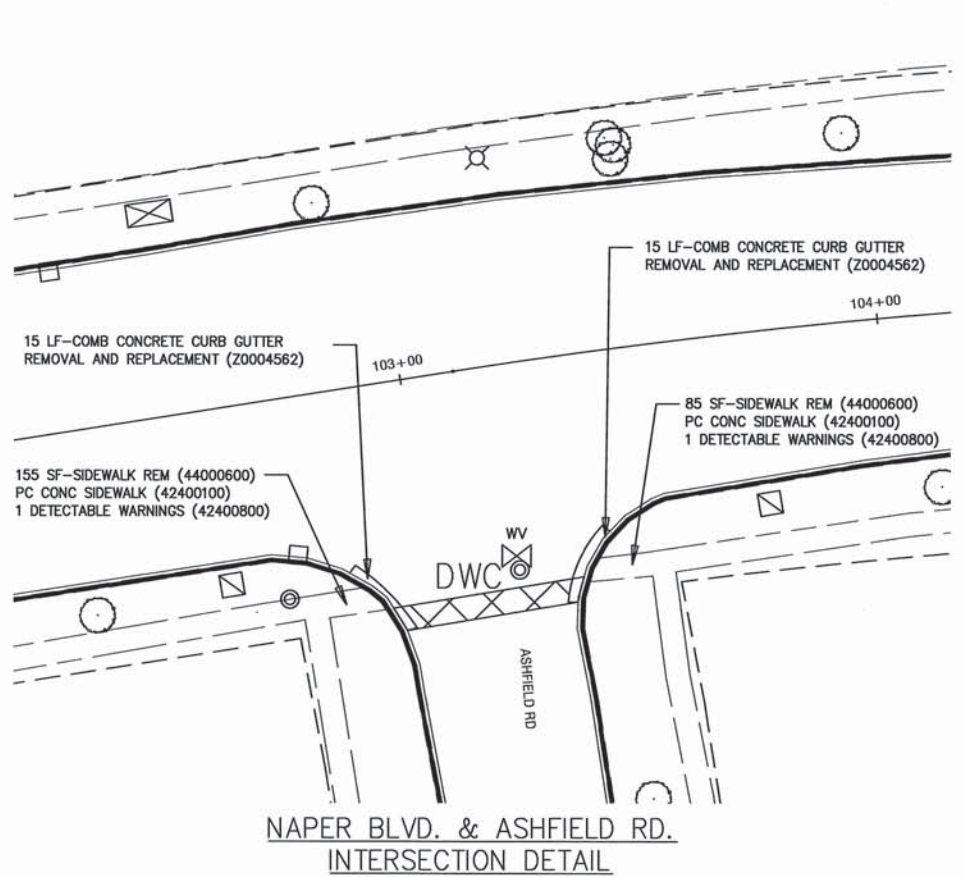
NOTES:
1. THE ACTUAL LOCATION OF PAVEMENT PATCHING WILL BE DETERMINED IN THE FIELD, DURING CONSTRUCTION, BY THE RESIDENT ENGINEER.

LEGEND:

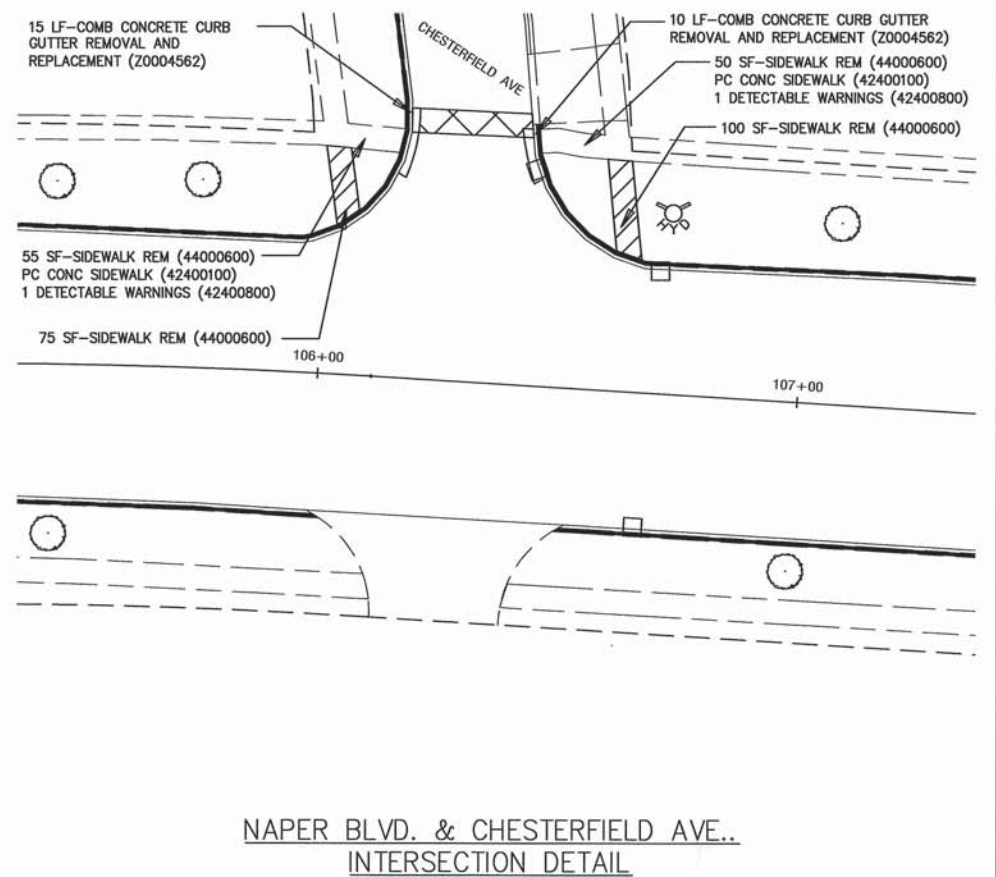
[A]	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL) (X6030310)
[ADJ]	DRAINAGE & UTILITY STRUCTURES ADJUSTMENT (SPECIAL) (Z0017500)
[Hatched]	SIDEWALK REMOVAL (44000600) *NO REPLACEMENT*
[Cross-hatched]	HMA SURFACE REMOVAL-BUTT JOINT (40600982)



NAPER BLVD. & GARTNER RD.
INTERSECTION DETAIL



NAPER BLVD. & ASHFIELD RD.
INTERSECTION DETAIL



NAPER BLVD. & CHESTERFIELD AVE..
INTERSECTION DETAIL

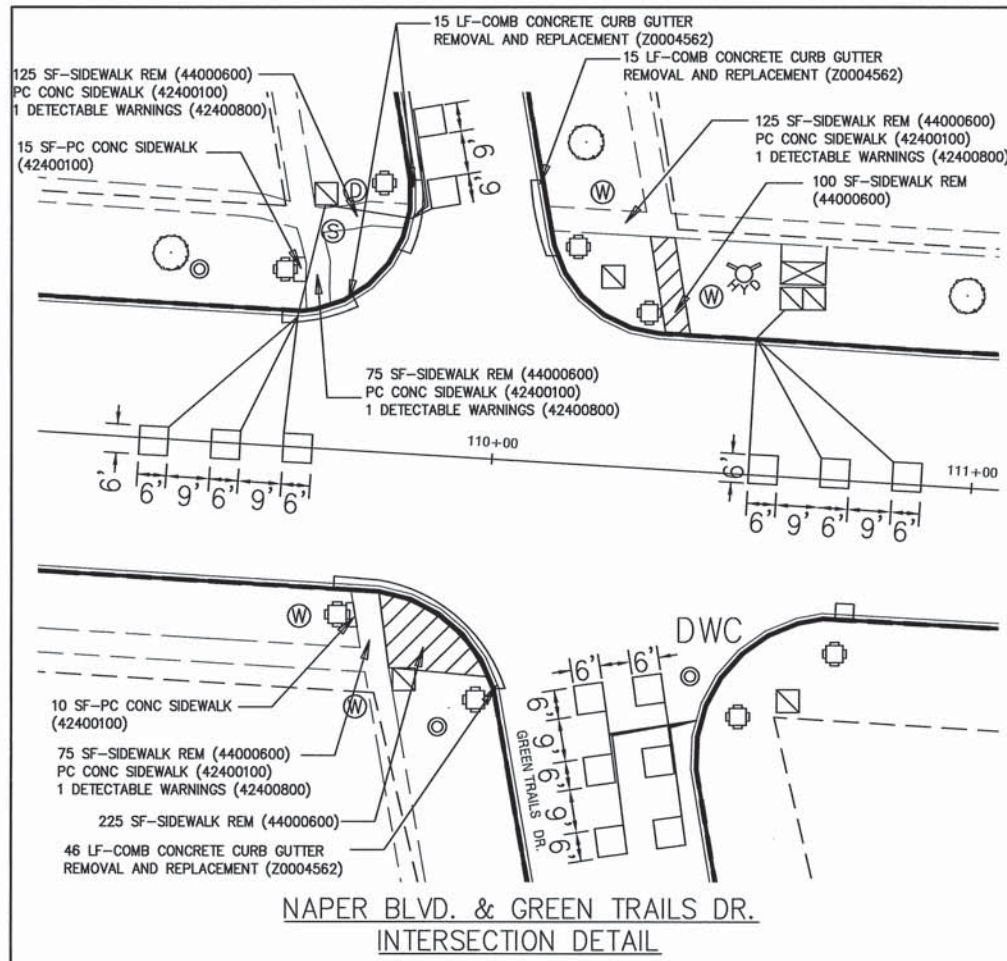
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

NAPER BOULEVARD
PROPOSED ROADWAY PLAN

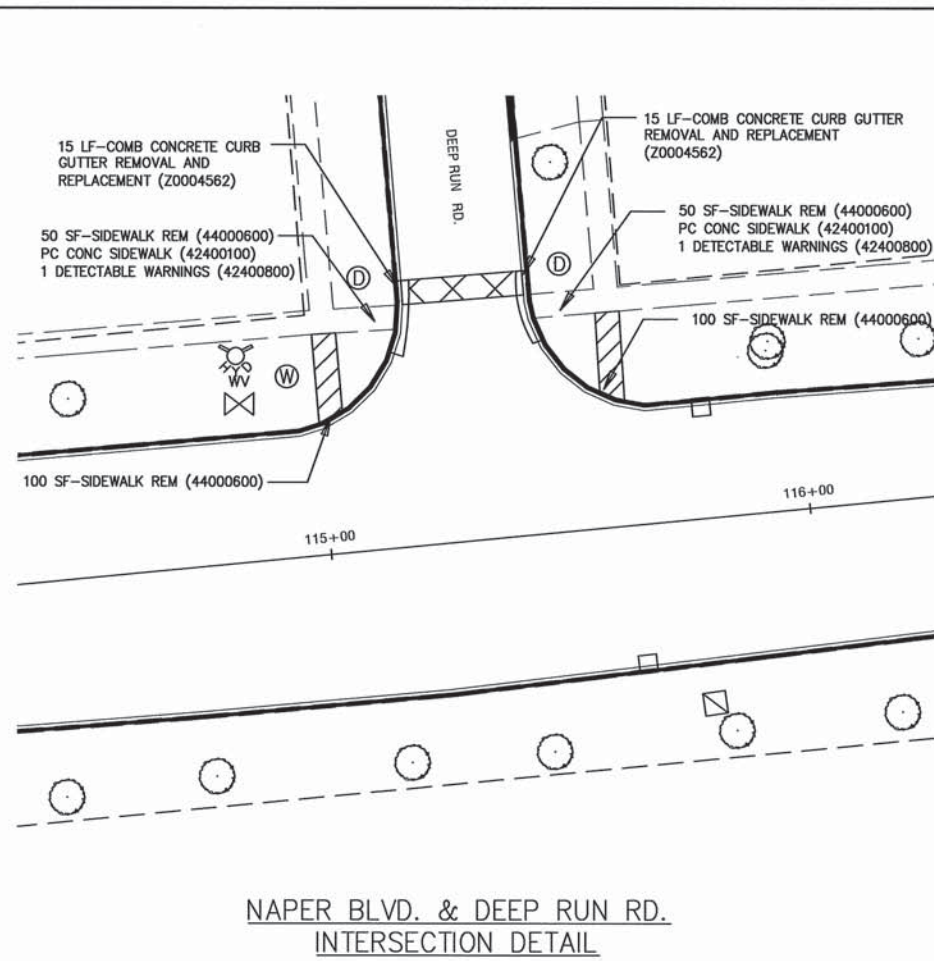
SCALE: 1" = 50 FT SHEET NO. 3 OF 4 SHEETS STA. 151+00.00 TO STA. 161+10.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 63893			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	

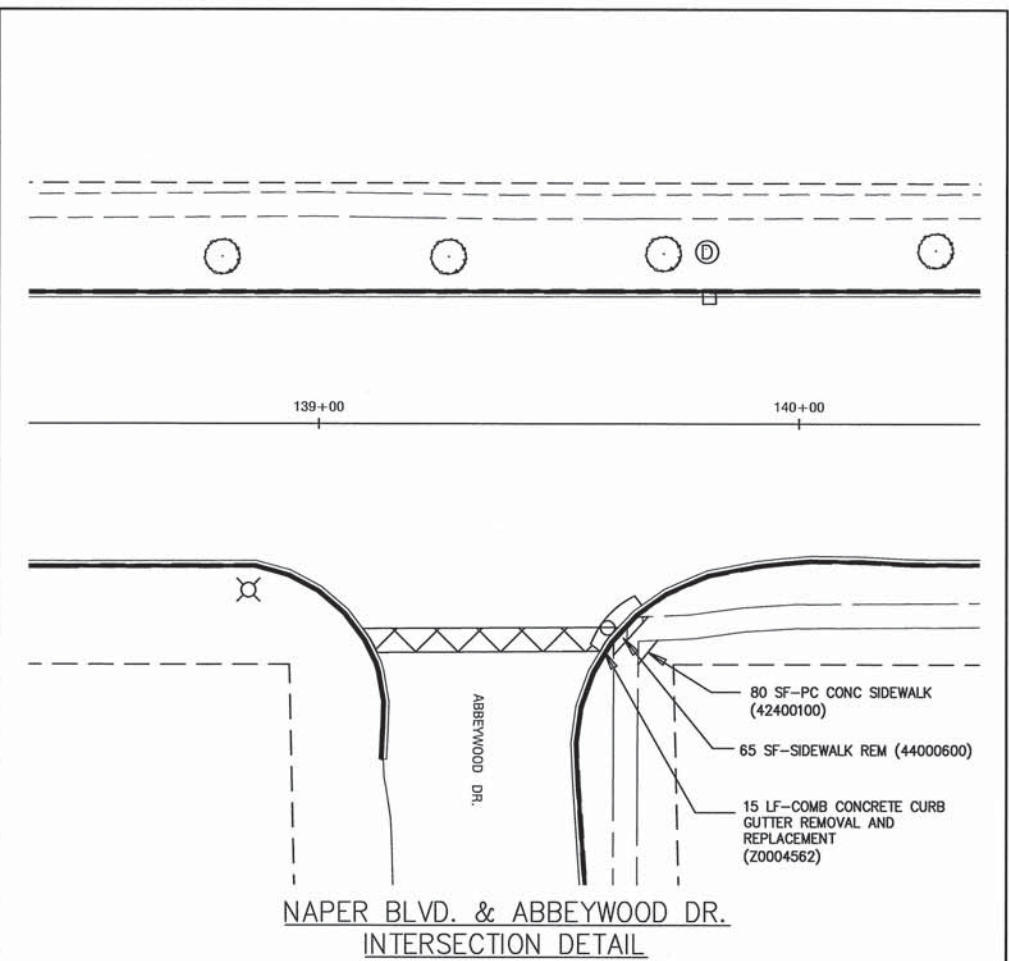
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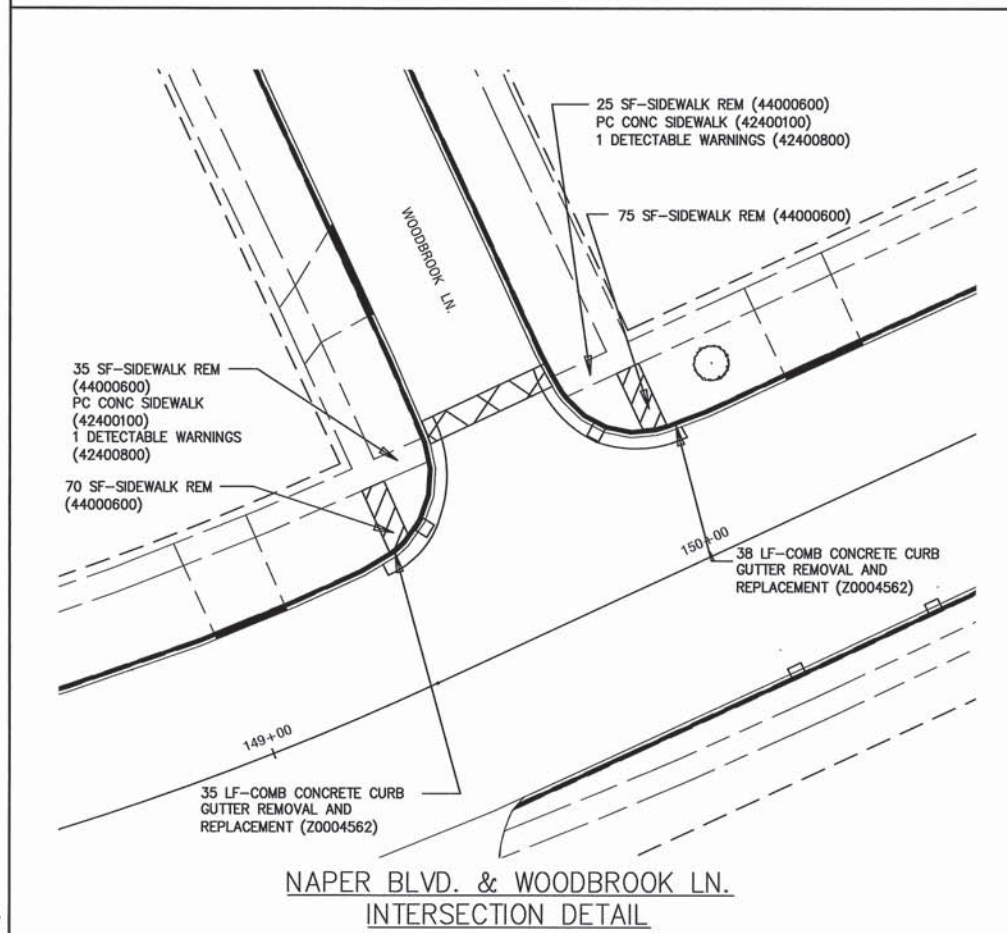
NAPER BLVD. & GREEN TRAILS DR.
INTERSECTION DETAIL



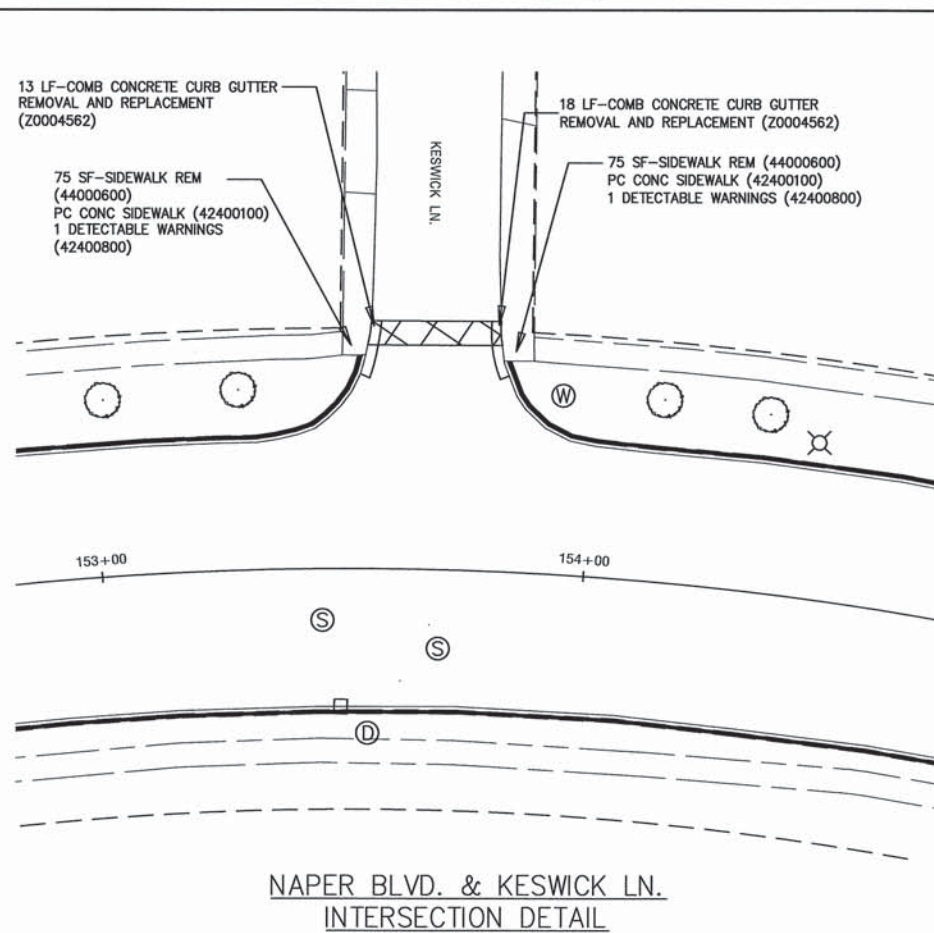
NAPER BLVD. & DEEP RUN RD.
INTERSECTION DETAIL



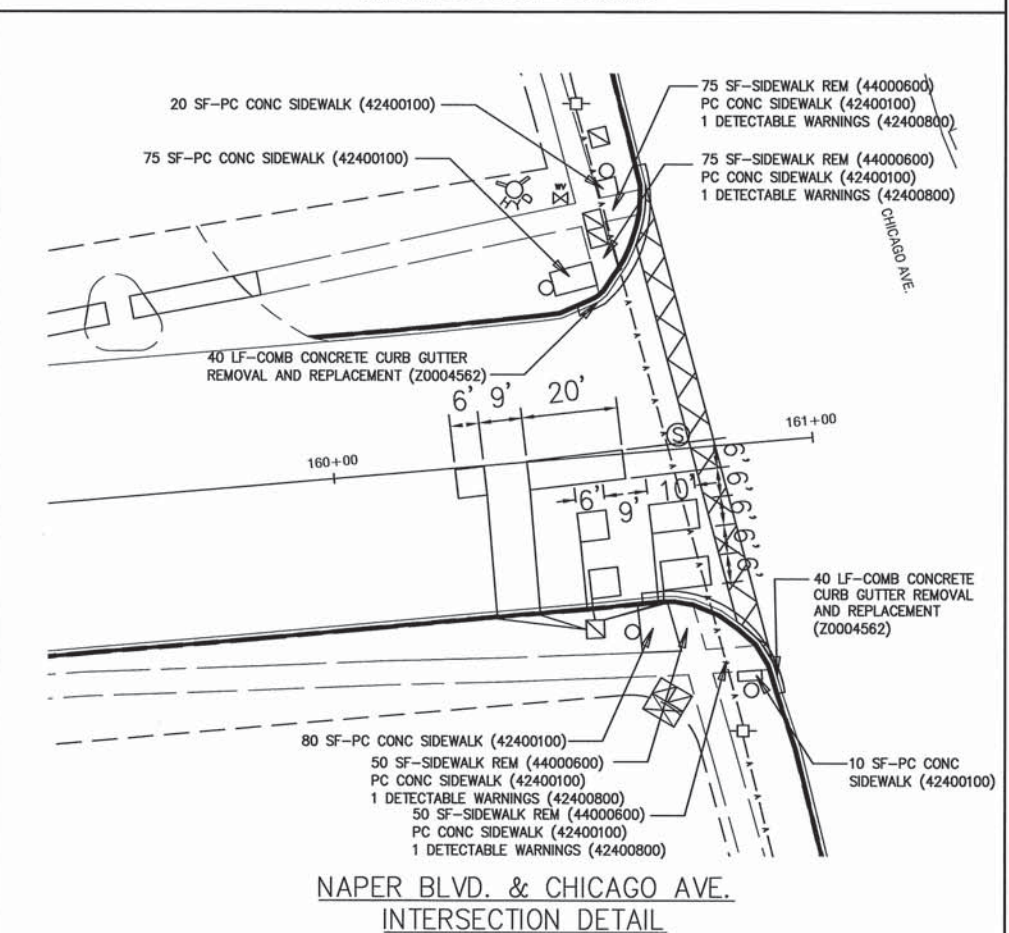
NAPER BLVD. & ABBEYWOOD DR.
INTERSECTION DETAIL



NAPER BLVD. & WOODBROOK LN.
INTERSECTION DETAIL



NAPER BLVD. & KESWICK LN.
INTERSECTION DETAIL



NAPER BLVD. & CHICAGO AVE.
INTERSECTION DETAIL

FILE NAME = \$FILES

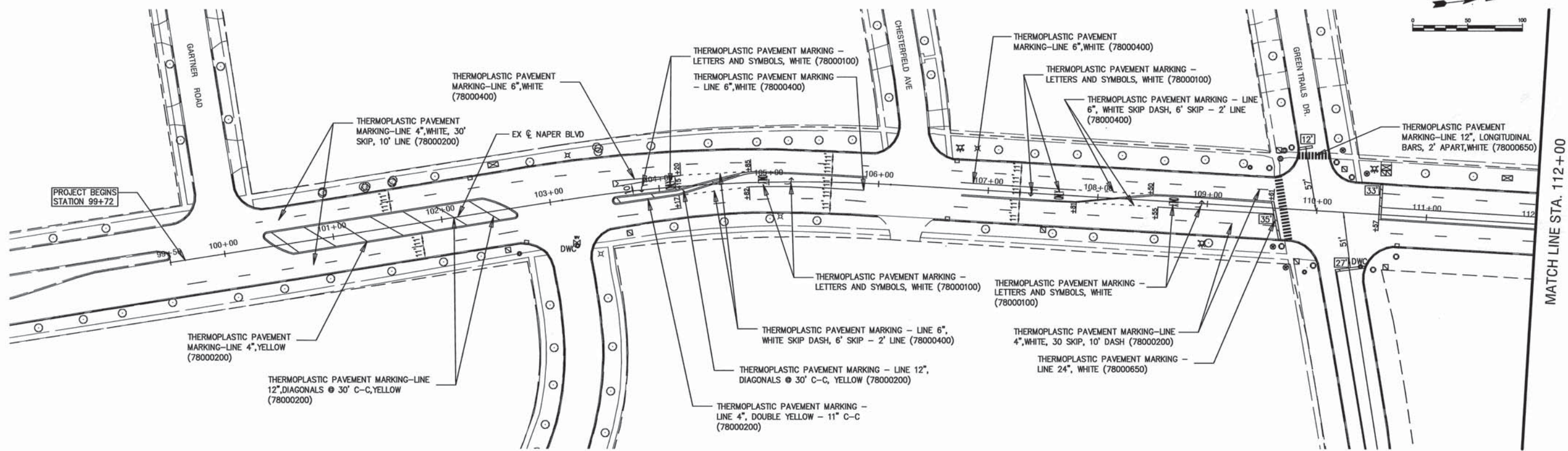
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DRAWN -	MCP	REVISED -	
CHECKED -	RJK	REVISED -	
DATE -	11/20/2013	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

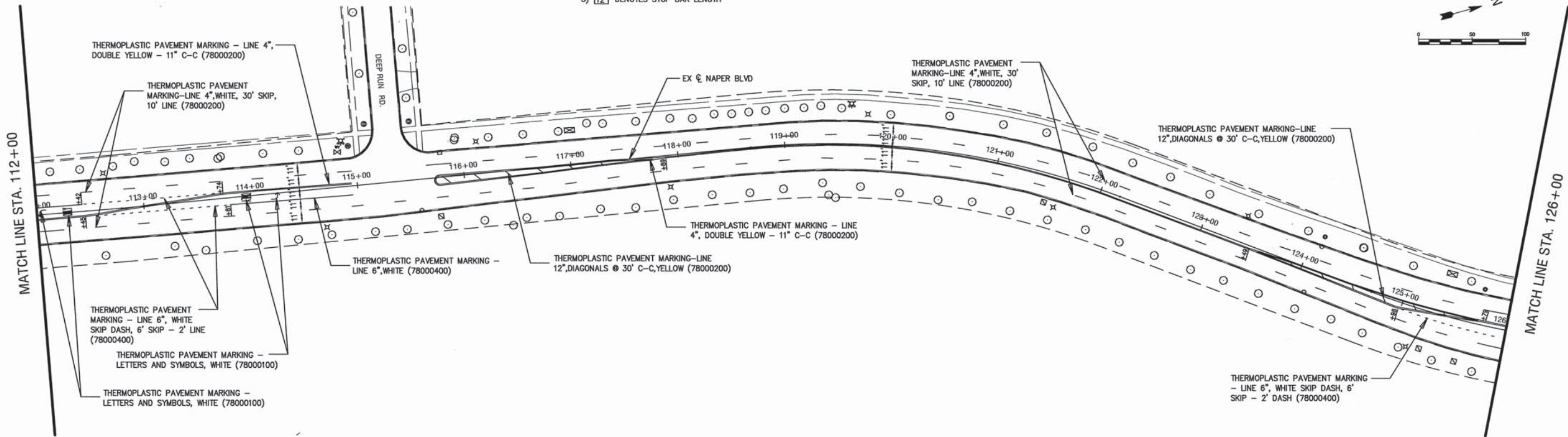
NAPER BOULEVARD
PROPOSED ROADWAY PLAN

SCALE: 1" = 50 FT SHEET NO. 4 OF 4 SHEETS STA. 151+00.00 TO STA. 161+10.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0856	12-00154-00-RS	DUPAGE	26	10
CONTRACT NO. 63893				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



- NOTES: 1) SEE SHEET 20 TC-13 DISTRICT ONE TYPICAL PAVEMENT MARKINGS FOR GUIDANCE
 2) SEE SHEET 25 TS-07 DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING FOR GUIDANCE
 3) [12] DENOTES STOP BAR LENGTH



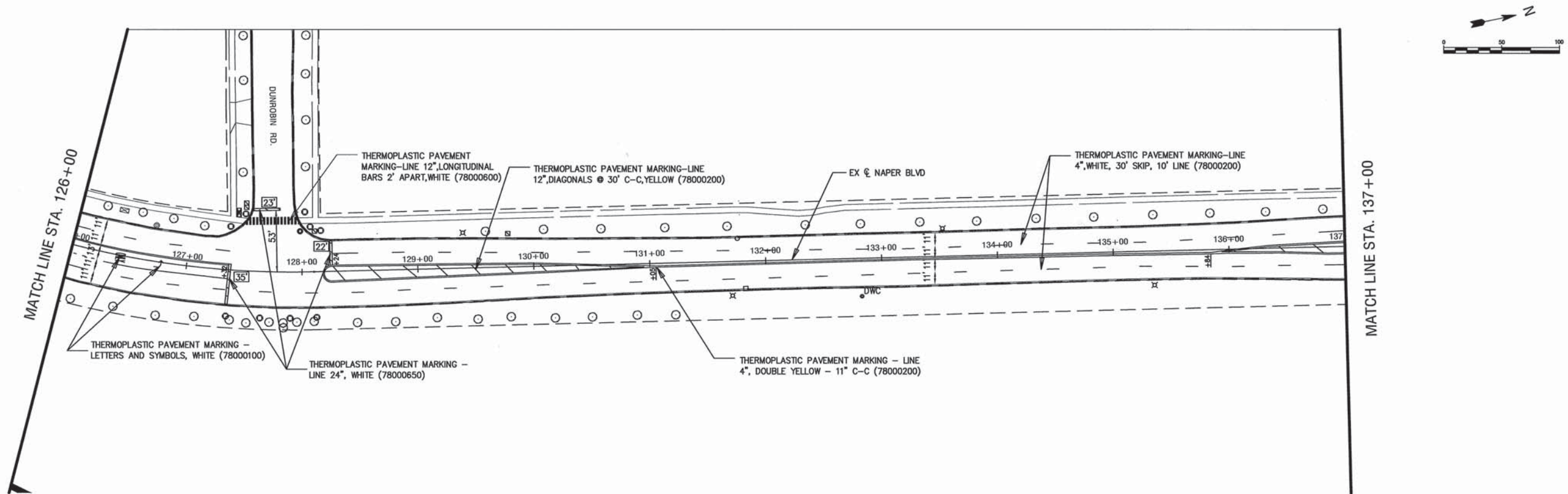
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DRAWN -	MCP	REVISED -	
CHECKED -	RJK	REVISED -	
DATE -	11/20/2013	REVISED -	

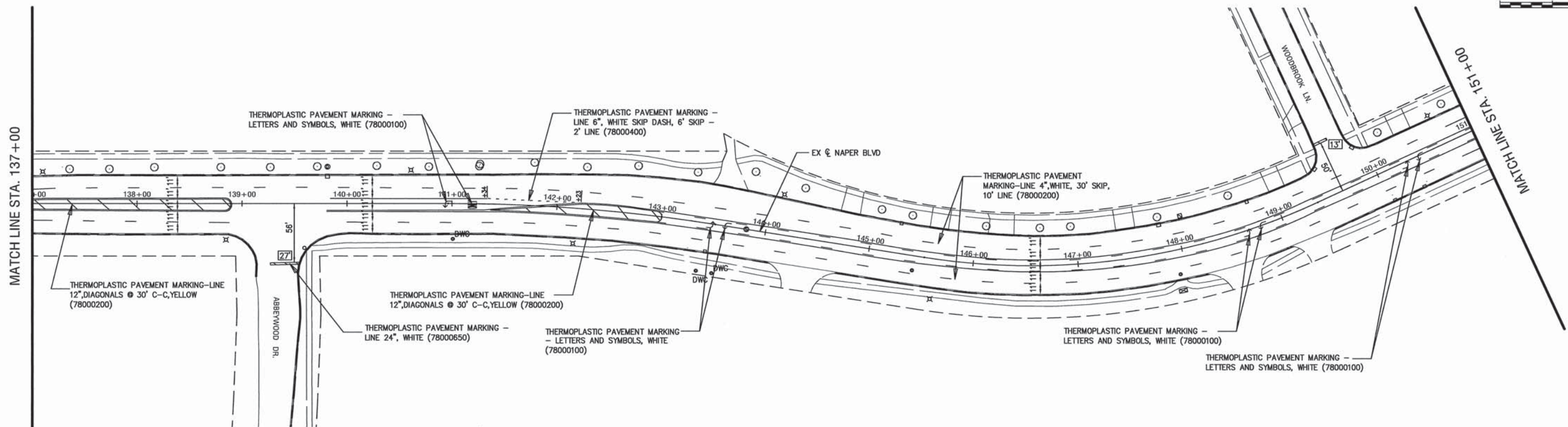
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

NAPER BOULEVARD PAVEMENT MARKING PLAN	
SCALE: 1" = 50 FT	SHEET NO. 1 OF 3 SHEETS
STA. 99+50.00	TO STA. 126+00.00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0856	12-00154-00-RS	DUPAGE	26	11
CONTRACT NO. 63893				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



- NOTES: 1) SEE SHEET 20 TC-13 DISTRICT ONE TYPICAL PAVEMENT MARKINGS FOR GUIDANCE
 2) SEE SHEET 25 TS-06 DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING FOR GUIDANCE
 3) [12] DENOTES STOP BAR LENGTH



FILE NAME - 0856

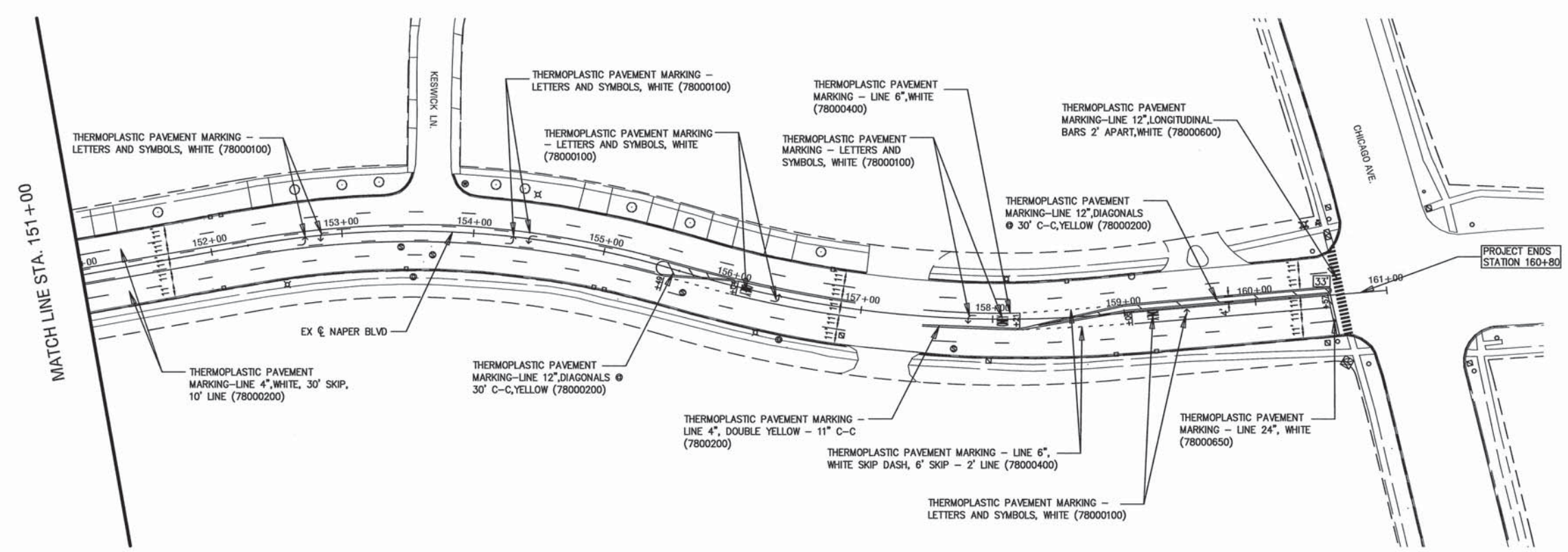
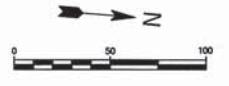
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DRAWN -	MCP	REVISED -	
CHECKED -	RJK	REVISED -	
DATE -	11/20/2013	REVISED -	

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

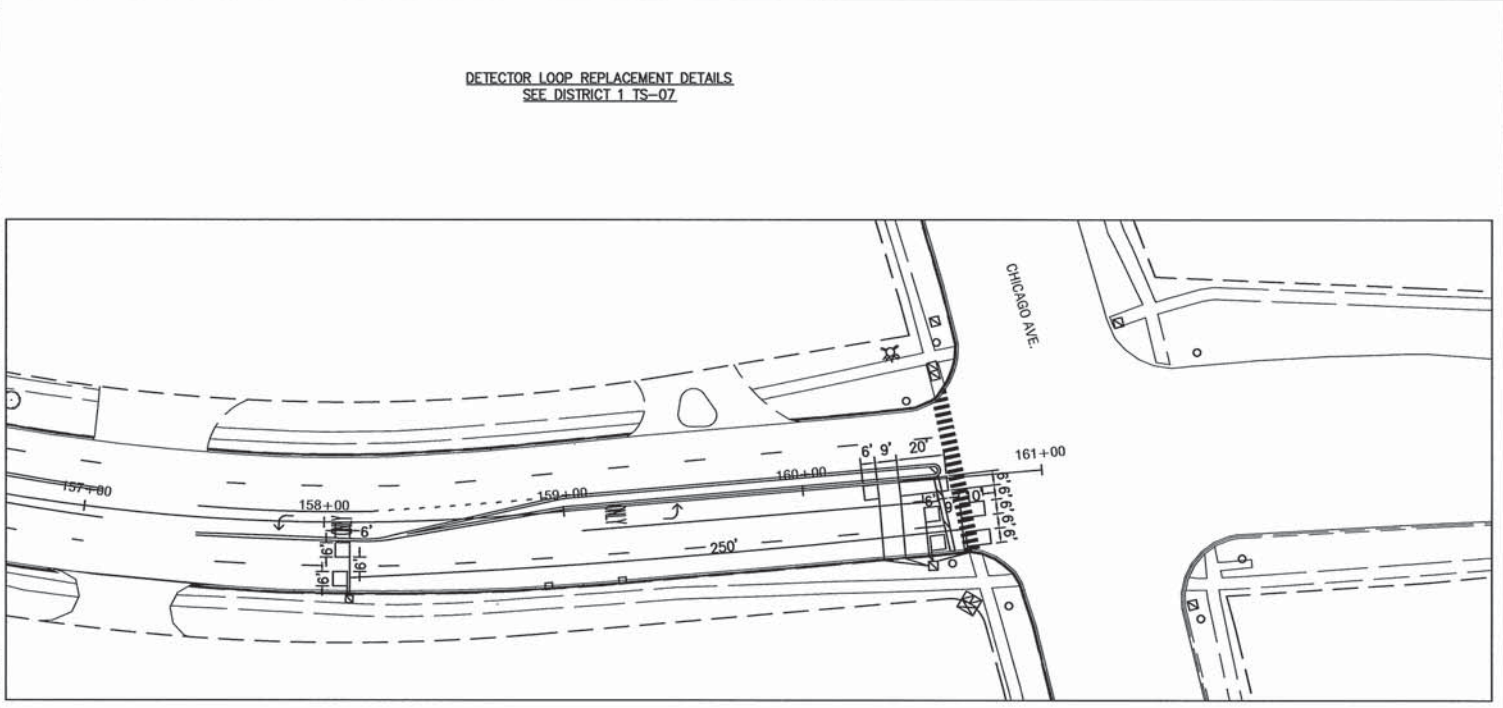
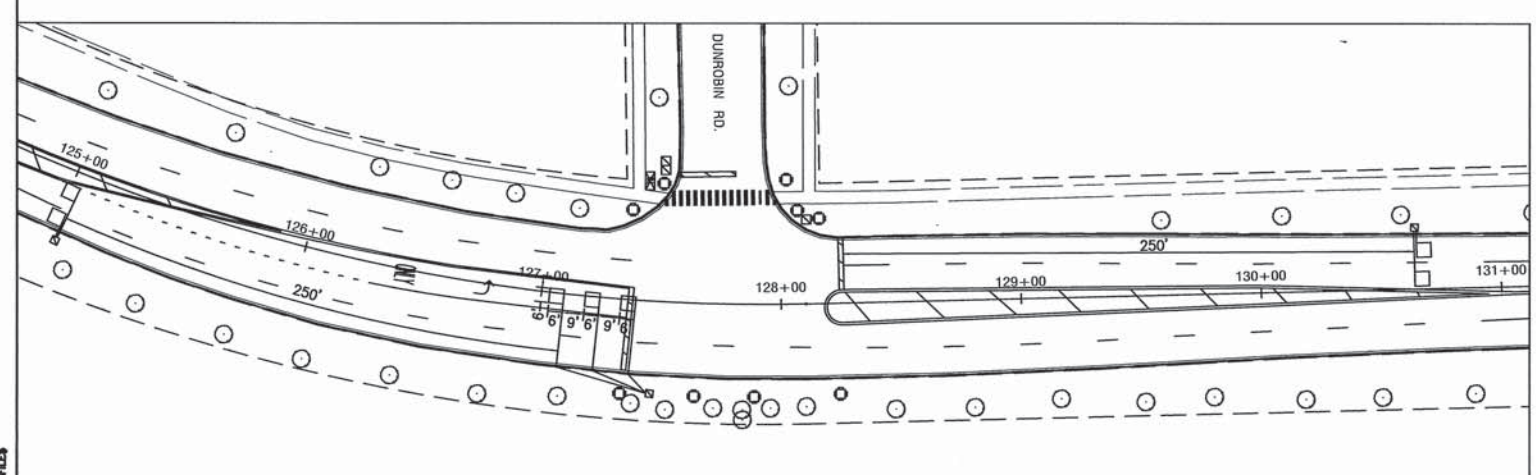
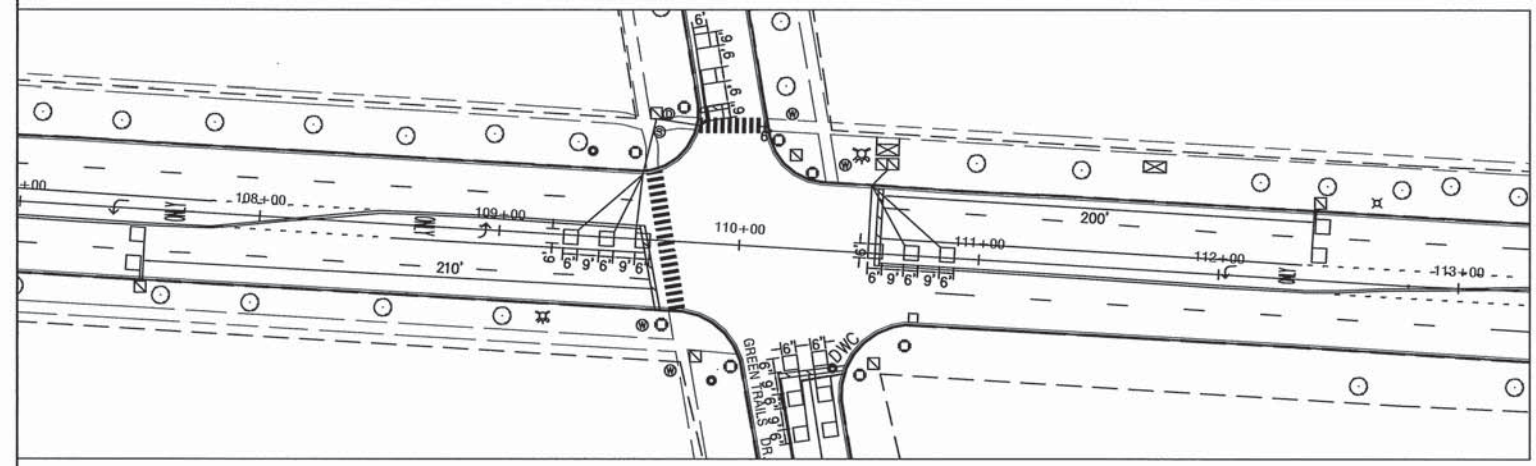
NAPER BOULEVARD
 PAVEMENT MARKING PLAN

SCALE: 1" = 50 FT SHEET NO. 2 OF 3 SHEETS STA. 126+00.00 TO STA. 151+00.00

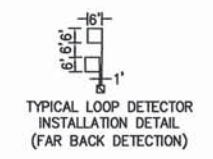
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0856	12-00154-00-RS	DUPAGE	26	12
CONTRACT NO. 63893				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



- NOTES:
- 1) SEE SHEET 20 TC-13 DISTRICT ONE TYPICAL PAVEMENT MARKINGS FOR GUIDANCE
 - 2) SEE SHEET 25 TS-06 DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING FOR GUIDANCE
 - 3) [12] DENOTES STOP BAR LENGTH



NOTE: CHICAGO AVE. DETECTOR LOOP INSTALLATION BASED ON DESIGN BY CIORBA GROUP, 1988



FILE NAME = 8113

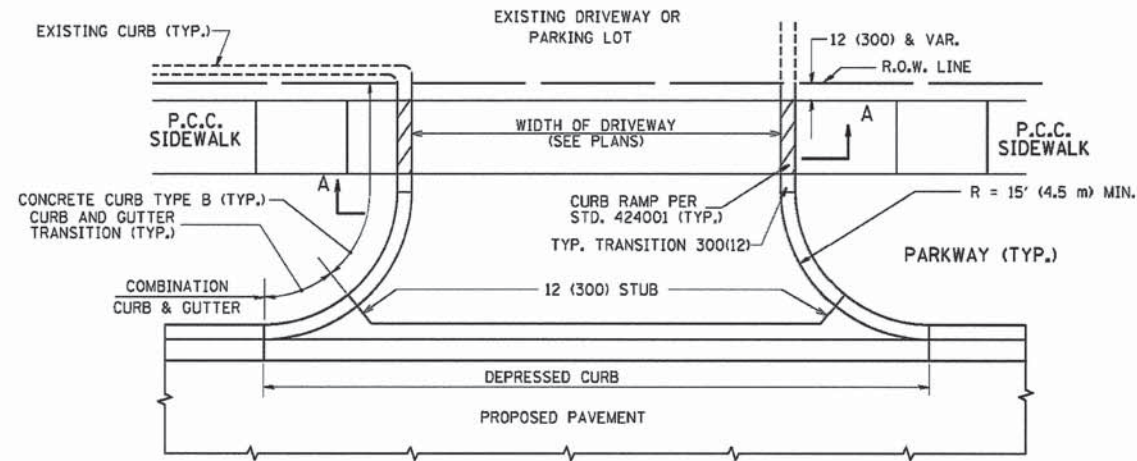
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DATE	-	11/20/2013	REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

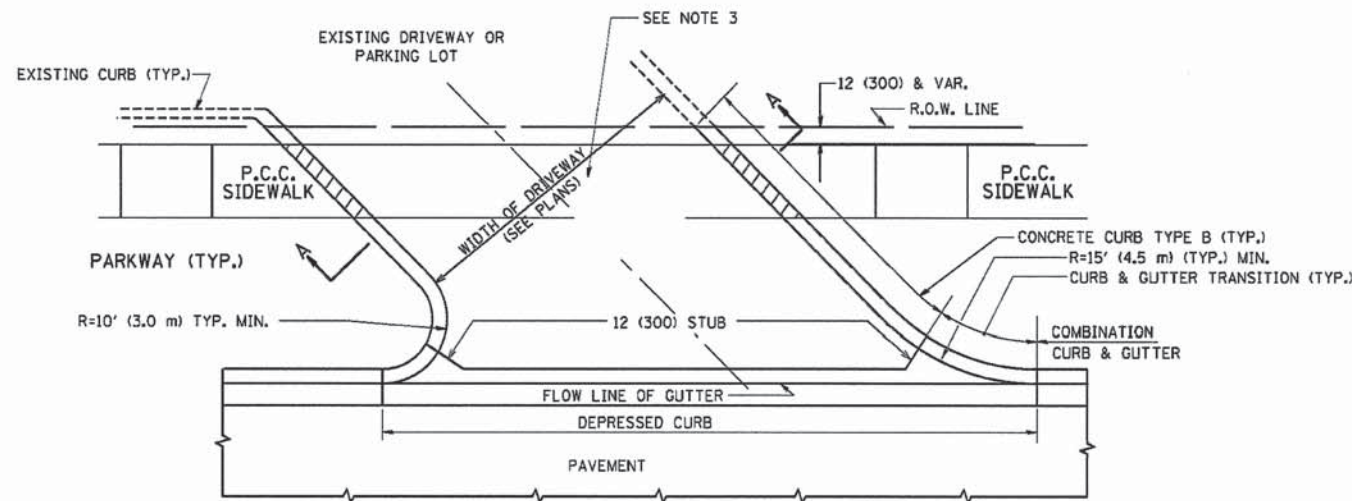
NAPER BOULEVARD
PAVEMENT MARKING PLAN

SCALE: 1" = 50 FT SHEET NO. 3 OF 3 SHEETS STA. 151+00.00 TO STA. 161+10.00

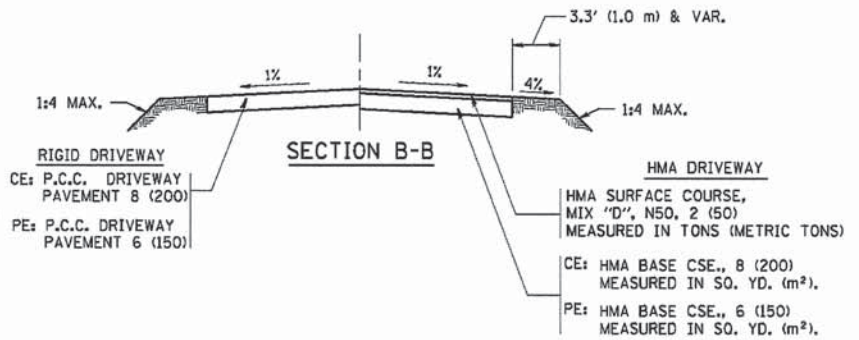
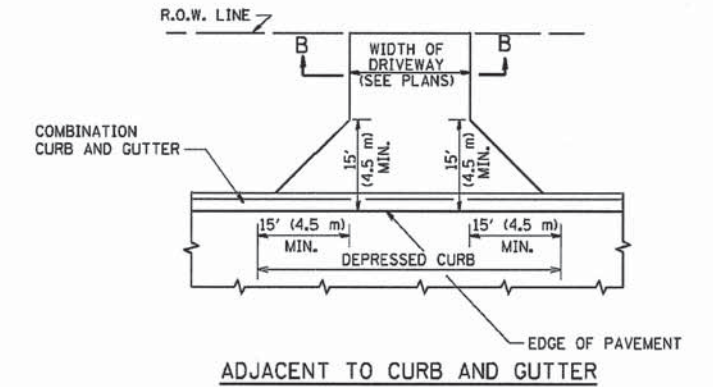
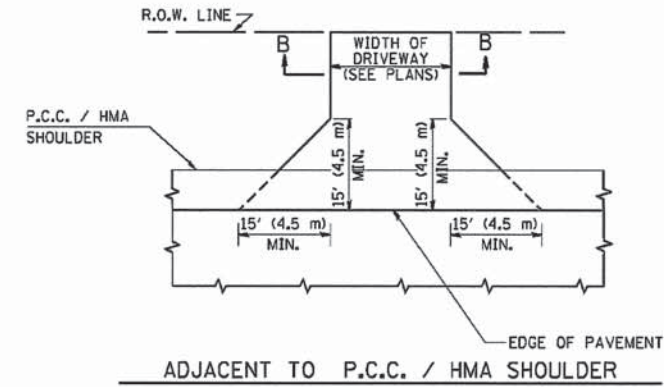
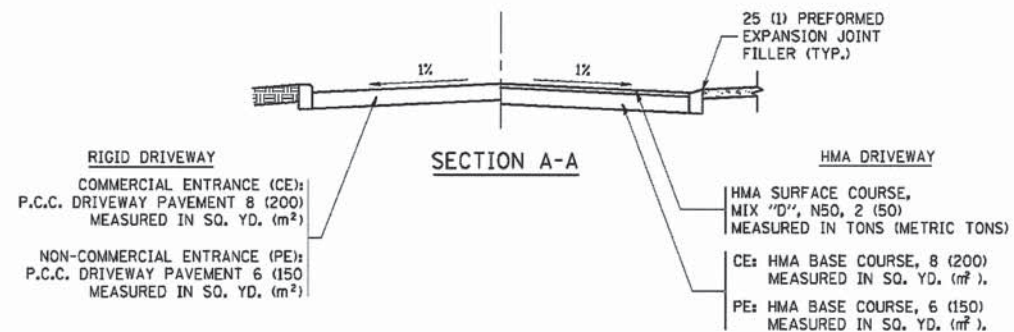
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0856	12-00154-00-RS	DUPAGE	26	13
CONTRACT NO. 63893				
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



WITH CONCRETE CURB, TYPE B



WITH CONCRETE CURB, TYPE B



RURAL FIELD ENTRANCE (FE)

HMA SURFACE COURSE,
MIX "D", N50, 2 (50)
MEASURED IN TONS (METRIC TONS)

AGGREGATE BASE CSE., TYPE B, 8 (200)
MEASURED IN SQ. YD. (m²)

GENERAL NOTES:

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATIONS IN THE PERMIT HANDBOOK. DRIVEWAYS SHALL BE REPLACED IN KIND, UNLESS OTHERWISE NOTED ON THE PLANS.

COMMERCIAL DRIVEWAYS SHALL BE CONSTRUCTED WITH CONCRETE CURB, TYPE B RETURNS EXCEPT WHEN THE SIDEWALK EDGE IS 4 FEET (1.2 METERS) OR LESS FROM THE BACK OF CURB, CONSTRUCT A FLARE DRIVEWAY WITHOUT CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

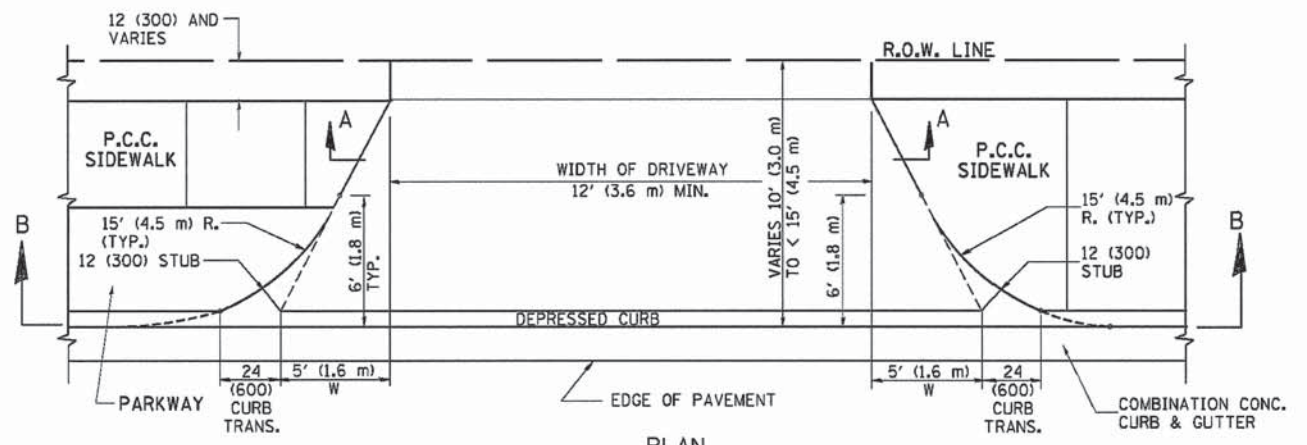
WHEN THE P.C.C. SIDEWALK EXTENDS THROUGH THE DRIVEWAY, THE THICKNESS OF THE SIDEWALK IN THE DRIVEWAY AREA SHALL BE THE SAME AS THE DRIVEWAY THICKNESS. SIDEWALK WILL BE PAID FOR AS P.C.C. SIDEWALK OF THE THICKNESS SPECIFIED. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

FILE NAME =	USER NAME = lshah	DESIGNED - R. SHAH	REVISED - P. LaFLUER 04-15-03
01\pwork\p\p\p\lshah\0853\5\bd01.dgn		DRAWN -	REVISED - R. BORO 01-01-07
	PLOT SCALE = 50.0000' / 1"	CHECKED -	REVISED - R. BORO 06-11-08
	PLOT DATE = 9/5/2011	DATE - 11-04-95	REVISED - R. BORO 09-06-11

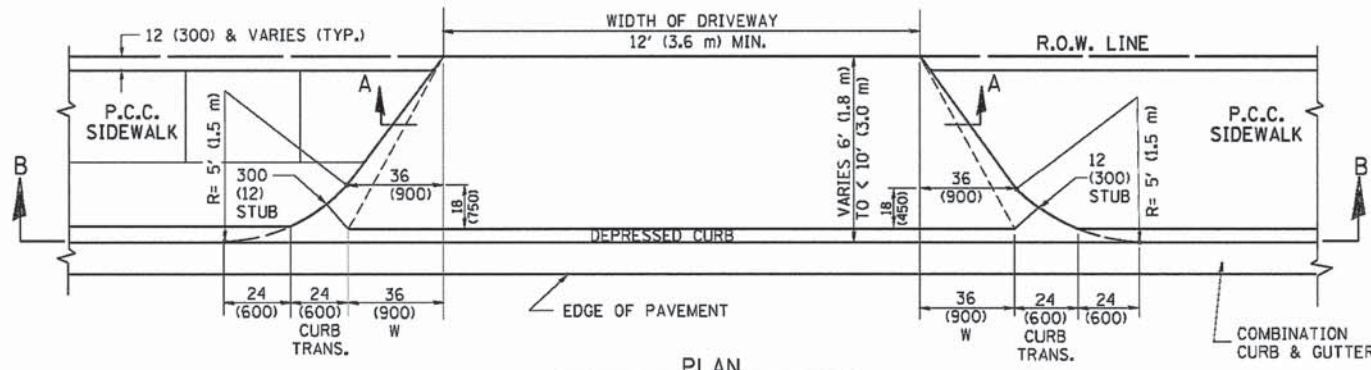
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS - DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 m)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

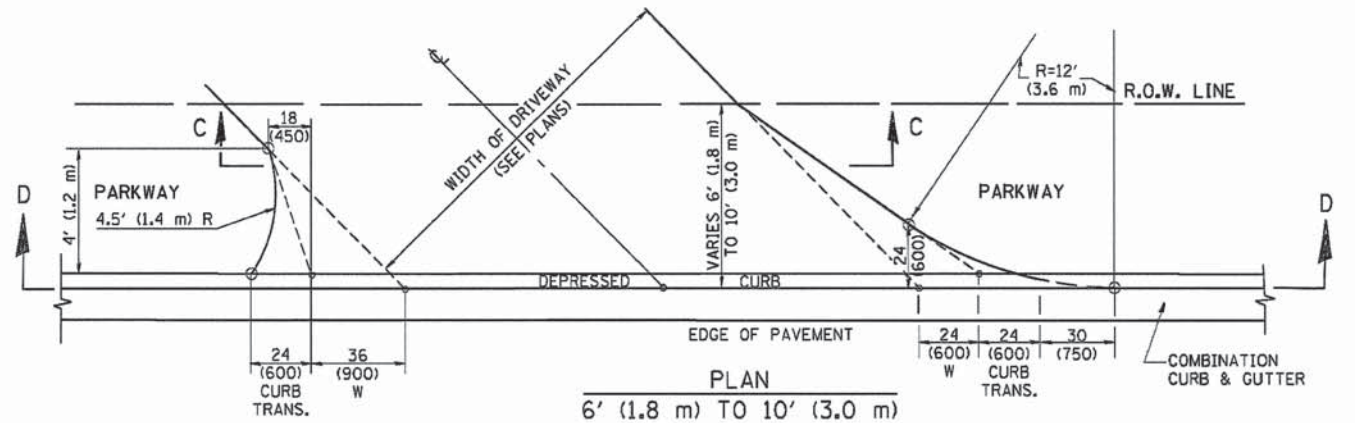
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BD0156-07 (BD-01)			CONTRACT NO.	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				



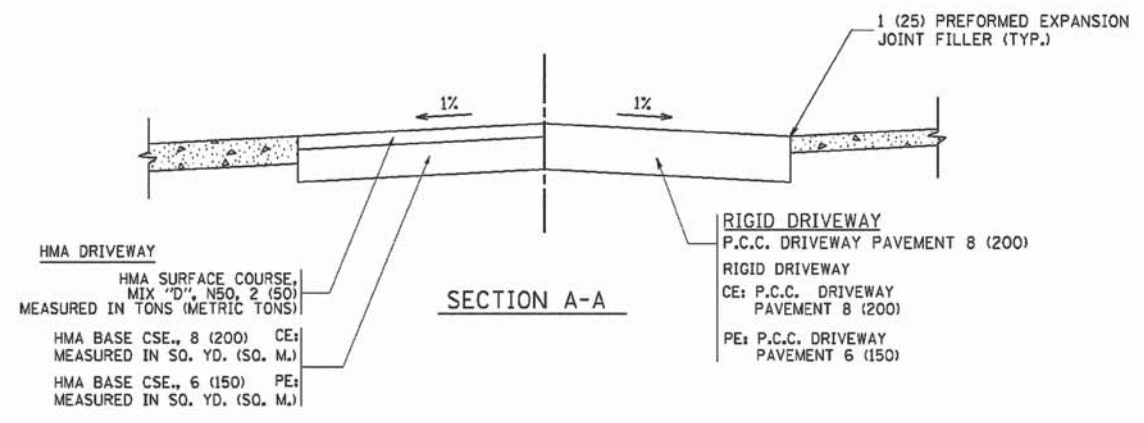
PLAN
10' (3.0 m) TO < 15' (4.5 m)



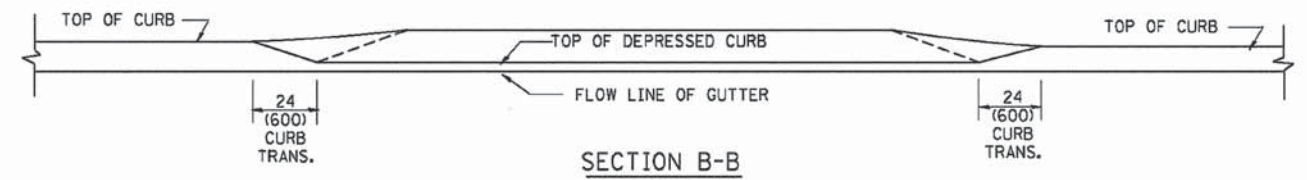
PLAN
6' (1.8 m) TO < 10' (3.0 m)



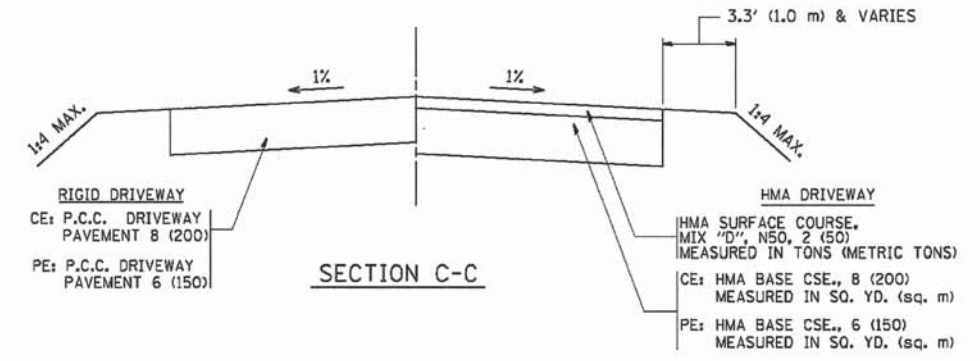
PLAN
6' (1.8 m) TO 10' (3.0 m)



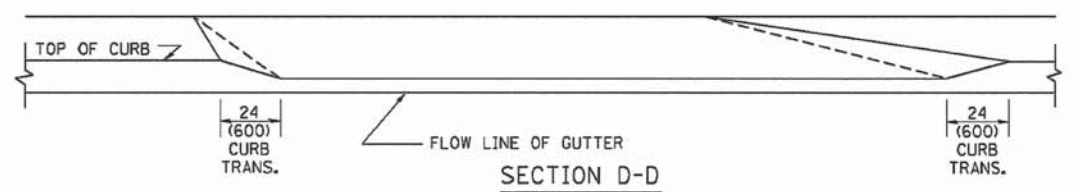
SECTION A-A



SECTION B-B



SECTION C-C



SECTION D-D

GENERAL NOTES

DRIVEWAY SLOPES, LOCATIONS, & GEOMETRIC LAYOUT SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE "HANDBOOK FOR POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS". FOR FURTHER LAYOUT REQUIREMENTS, REFER TO ILLUSTRATION 10 IN THE PERMIT HANDBOOK. WHERE SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED WITH RIGID PAVEMENT. WHERE NO SIDEWALKS EXIST, DRIVEWAYS SHALL BE REPLACED IN KIND. SIDEWALK CROSS SLOPE THRU DRIVEWAY AREA TO BE A MAXIMUM OF 1:50.

WHEN THE DISTANCE BETWEEN R.O.W. AND THE BACK OF CURB IS EQUAL TO OR LESS THAN 8' (2.4 m), THE P.C.C. SIDEWALK SHALL EXTEND TO THE BACK OF CURB.

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC PERMIT OFFICE AT 847/ 705-4131 FOR ANY QUESTIONS ON DRIVEWAYS SHOWN IN THE PLANS; SPECIFICALLY IN REFERENCE TO ADDITIONAL AND/OR RELOCATION/REMOVAL OF A DRIVEWAY.

COMBINATION CONCRETE CURB & GUTTER SHALL BE MEASURED STRAIGHT ACROSS THE DRIVEWAY. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THE CURB & GUTTER TRANSITION.

THE 1 (25) PREFORMED EXPANSION JOINT FILLER WILL NOT BE PAID SEPARATELY, BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE P.C.C. DRIVEWAY PAVEMENT OR P.C.C. SIDEWALK.

"W" VARIES FROM 36 (900) TO 5' (1.5 m) PROPORTIONAL TO THE LENGTH (L), FROM 6' (1.8 m) TO 10' (3 m).

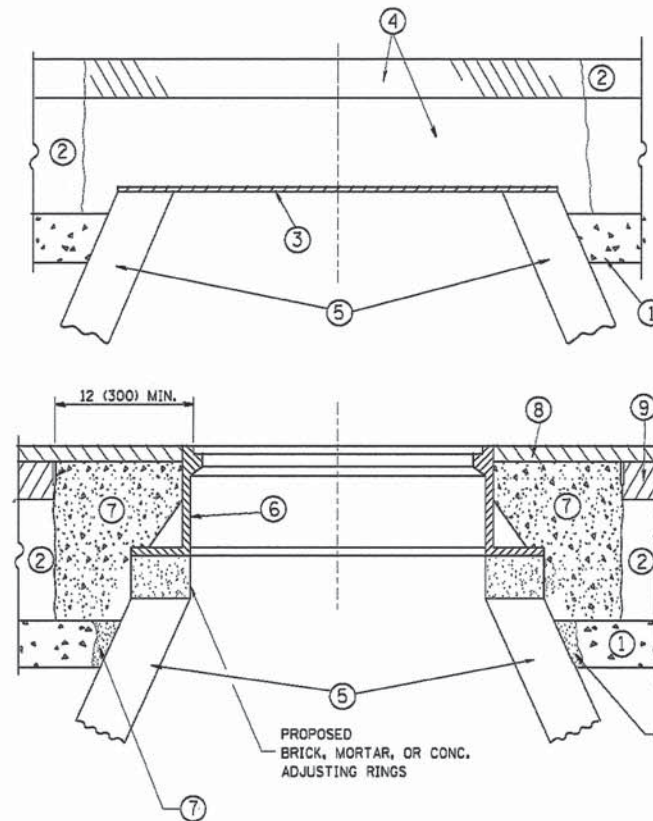
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE NOTED.

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	PLOT SCALE = 58.0208' / 1"	CHECKED -	REVISED - R. BORO 01-01-07
	PLOT DATE = 10/28/2011	DATE - 11-06-95	REVISED - R. BORO 09-06-11

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DRIVEWAY DETAILS	
DISTANCE BETWEEN ROW AND FACE OF CURB < 15' (4.5 m)	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE. 0856	SECTION 12-00154-00-RS	COUNTY DUPAGE	TOTAL SHEETS 26	SHEET NO. 15
BD400-02 (BD-02)			CONTRACT NO.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

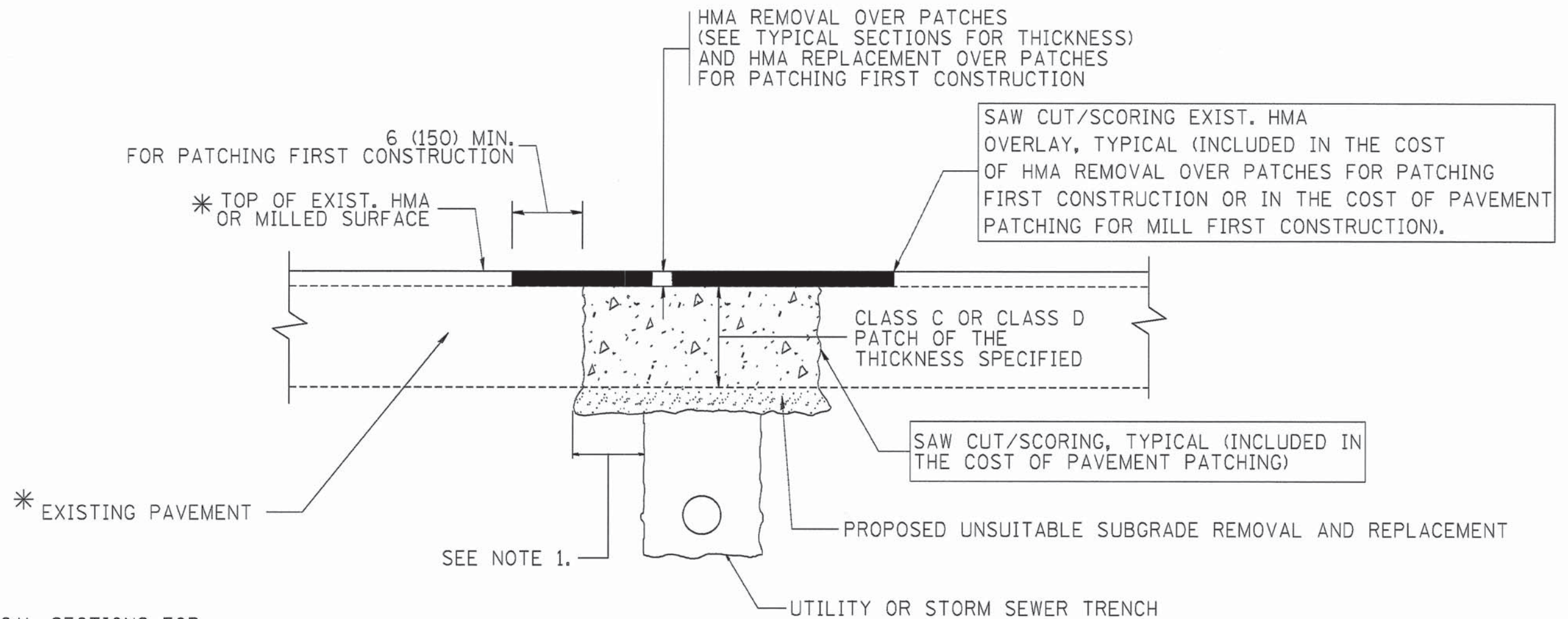
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		CHECKED -	REVISED - R. BORO 03-09-11
		DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0856	12-00154-00-RS	DUPAGE	26	16
BD600-03 (BD-8)			CONTRACT NO.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

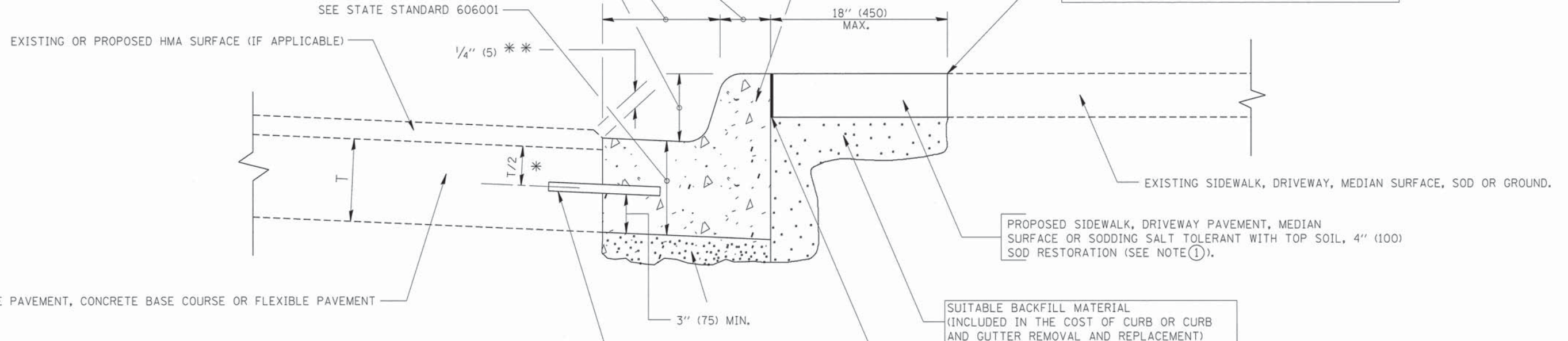
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	PLOT SCALE = 50.000' / IN.	DRAWN -	REVISED - R. BORO 01-01-07		0856	12-00154-00-RS	DUPAGE	26	17			
	PLOT DATE = 10/27/2008	CHECKED -	REVISED - R. BORO 09-04-07		BD400-04 (BD-22)			CONTRACT NO.				
	DATE - 10-25-94	REVISOR -	REVISED - K. ENG 10-27-08		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.



EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

** IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY. SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

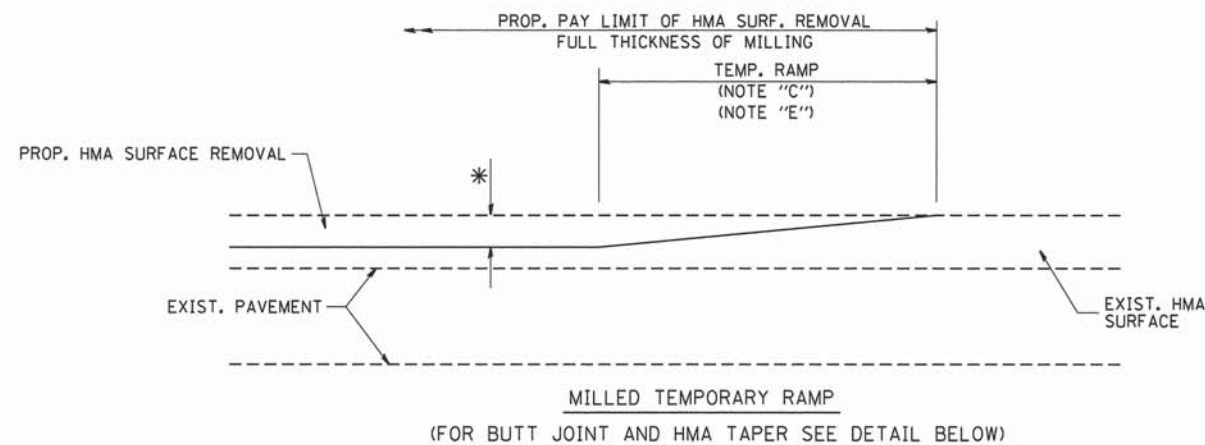
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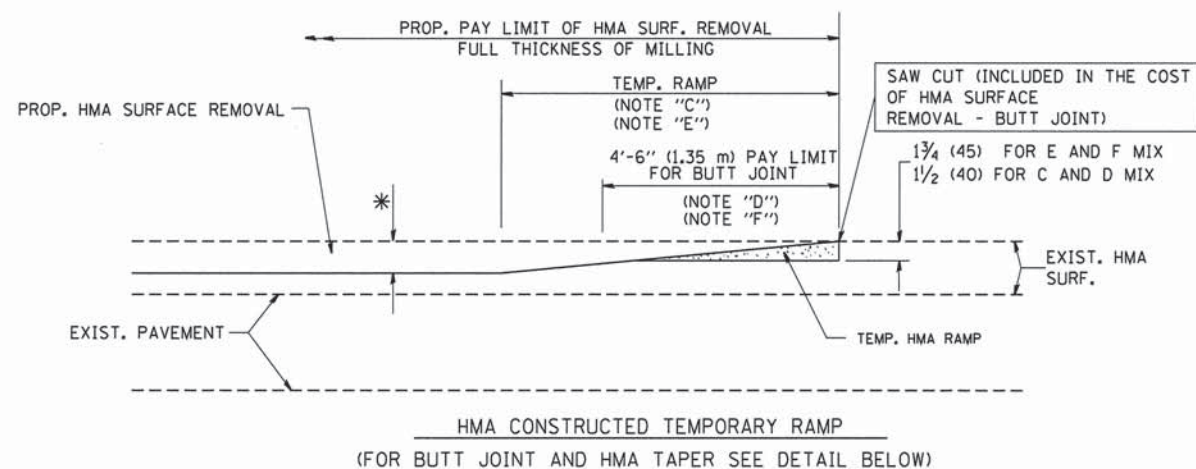
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

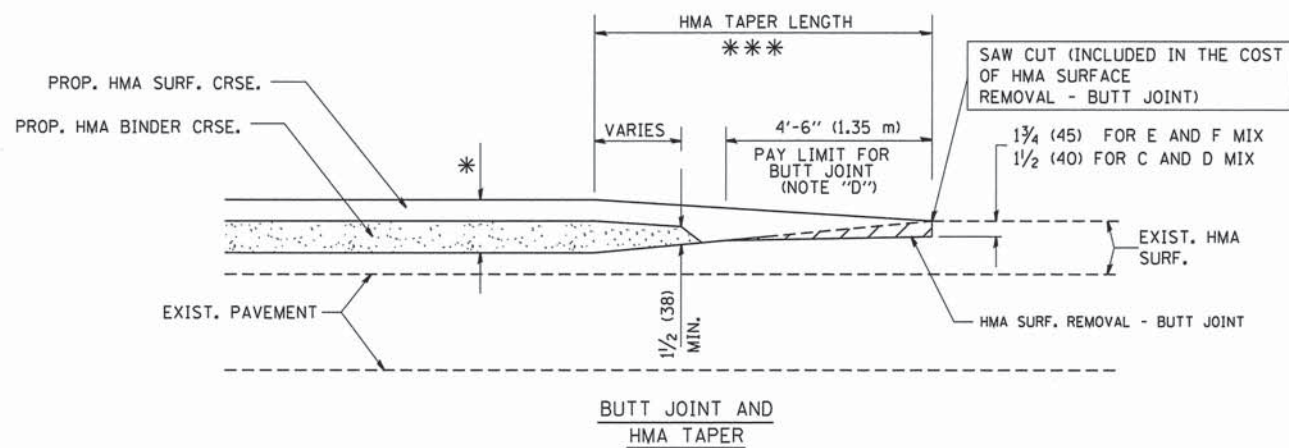
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BD600-06 (BD-24)		CONTRACT NO.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



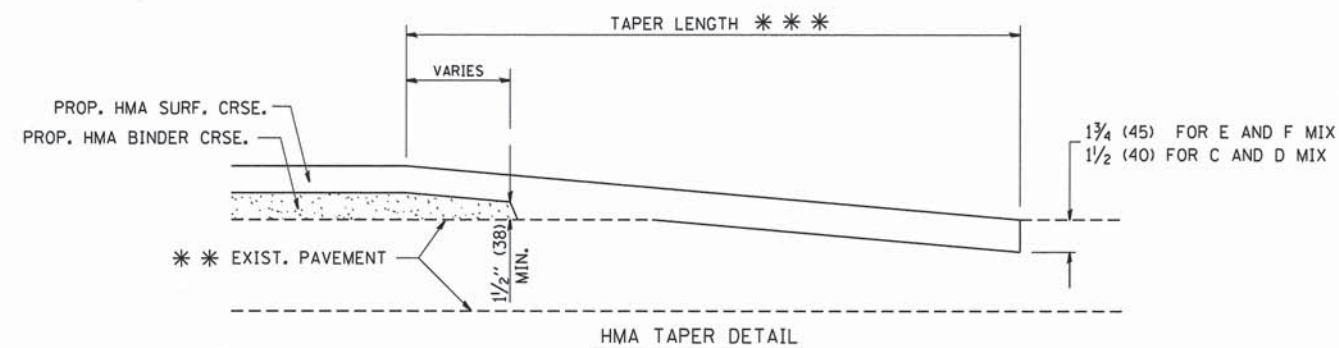
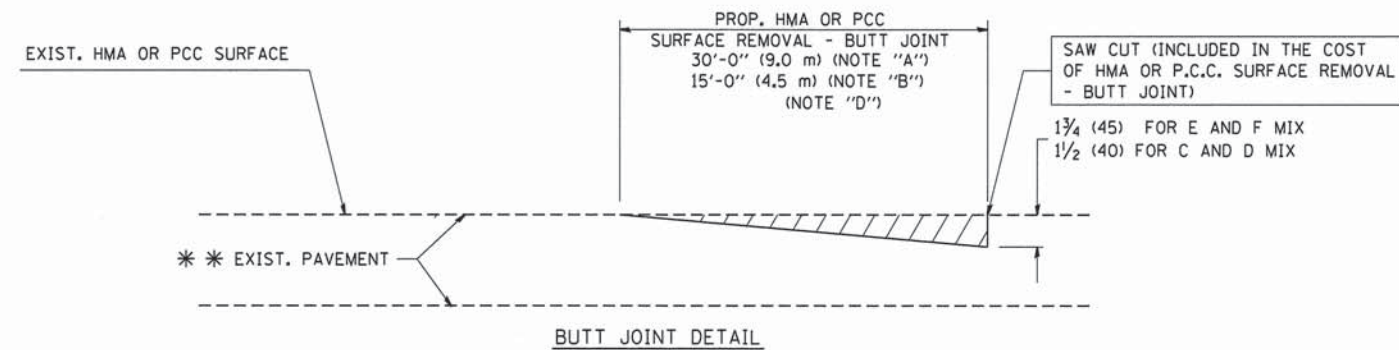
OPTION 1



**OPTION 2
TYPICAL TEMPORARY RAMP**



**TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING**



**TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY**

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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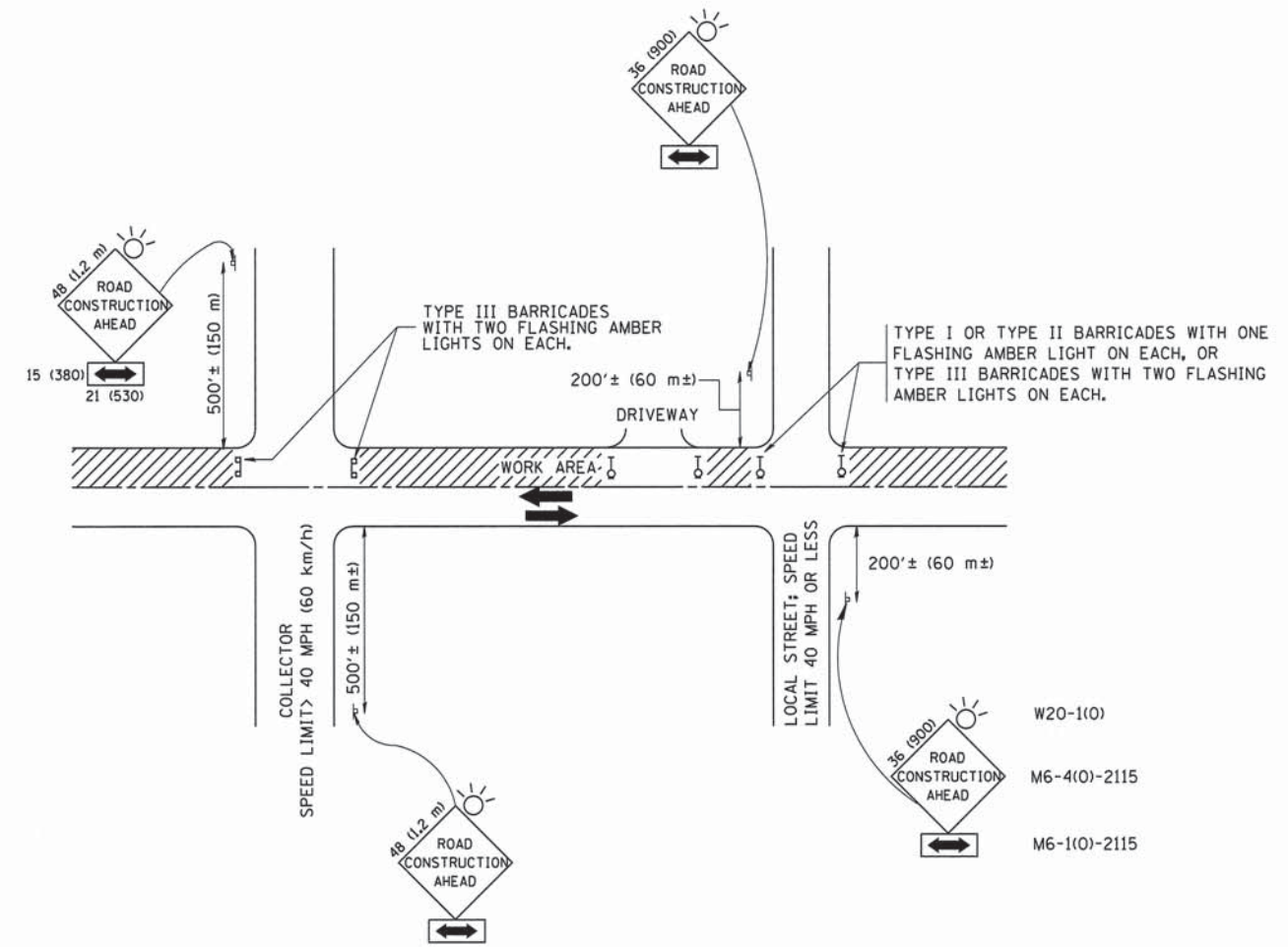
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PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - M. GOMEZ 04-06-01
		REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**BUTT JOINT AND
HMA TAPER DETAILS**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0856	12-00154-00-RS	DUPAGE	26	19
BD400-05 BD32			CONTRACT NO.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

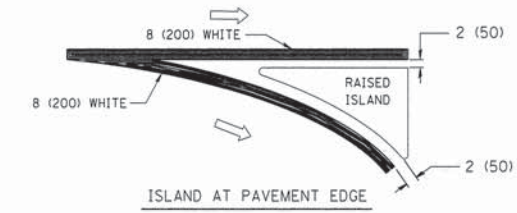
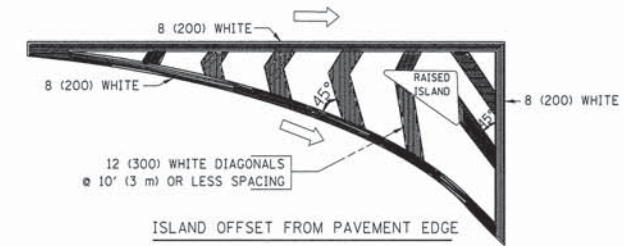
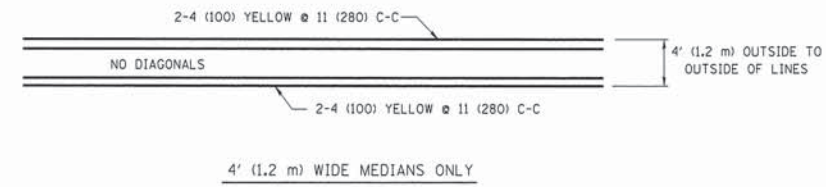
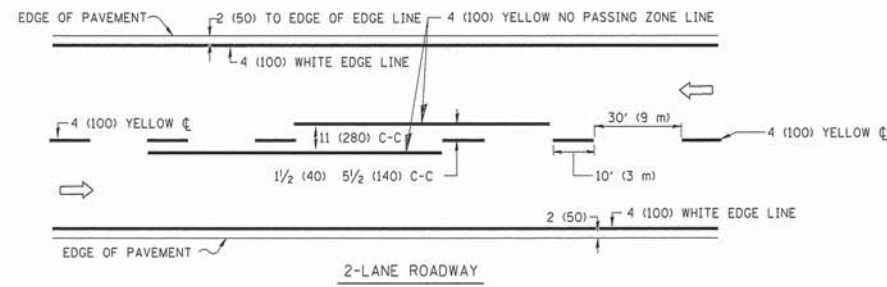
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		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

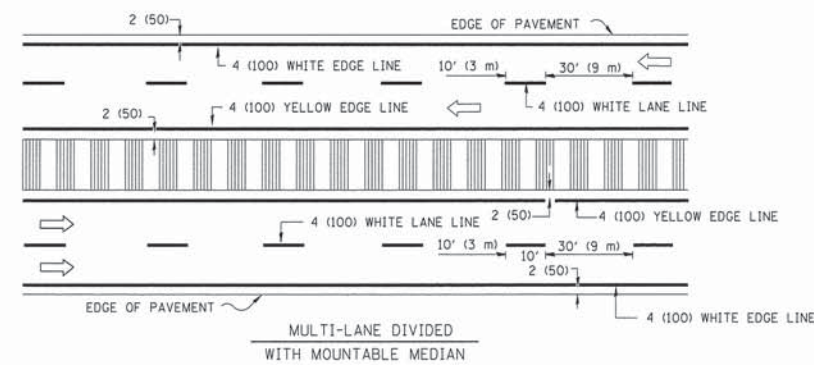
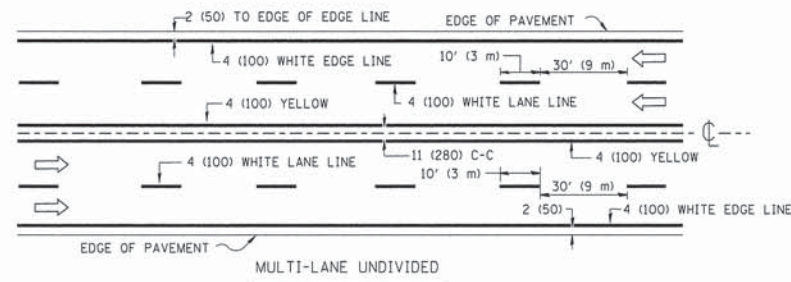
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0856	12-00154-00-RS	DUPAGE	26	20
TC-10			CONTRACT NO.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

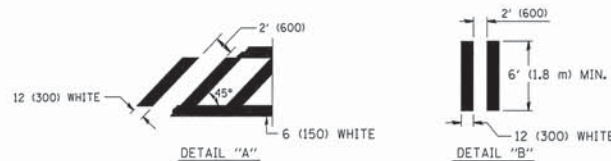
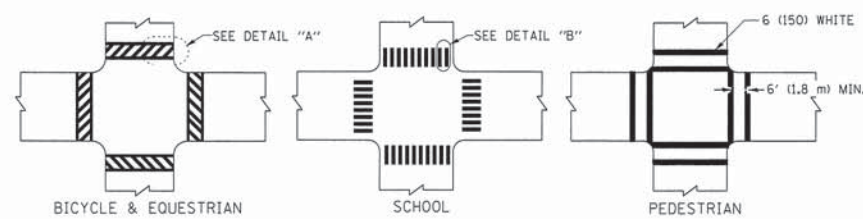


TYPICAL ISLAND MARKING

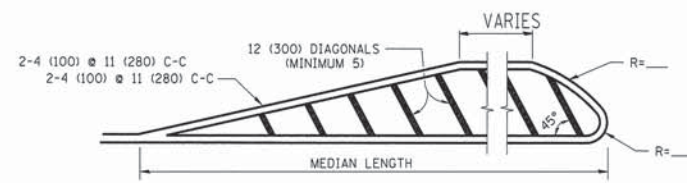


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING



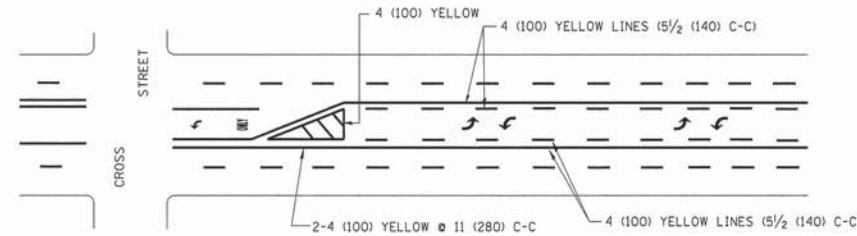
TYPICAL CROSSWALK MARKING



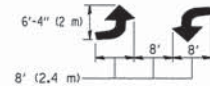
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

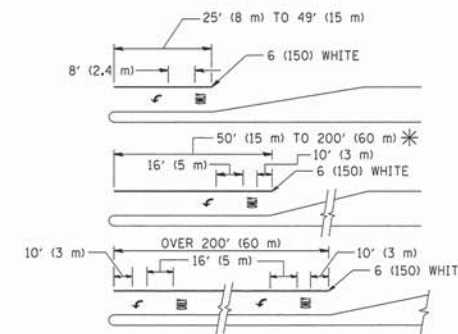


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" 15 6' (1.8 m) LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R": 3.6 SQ. FT. (0.33 m ²) EACH "X": 54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

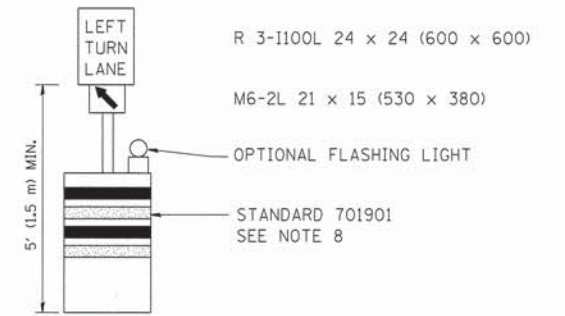
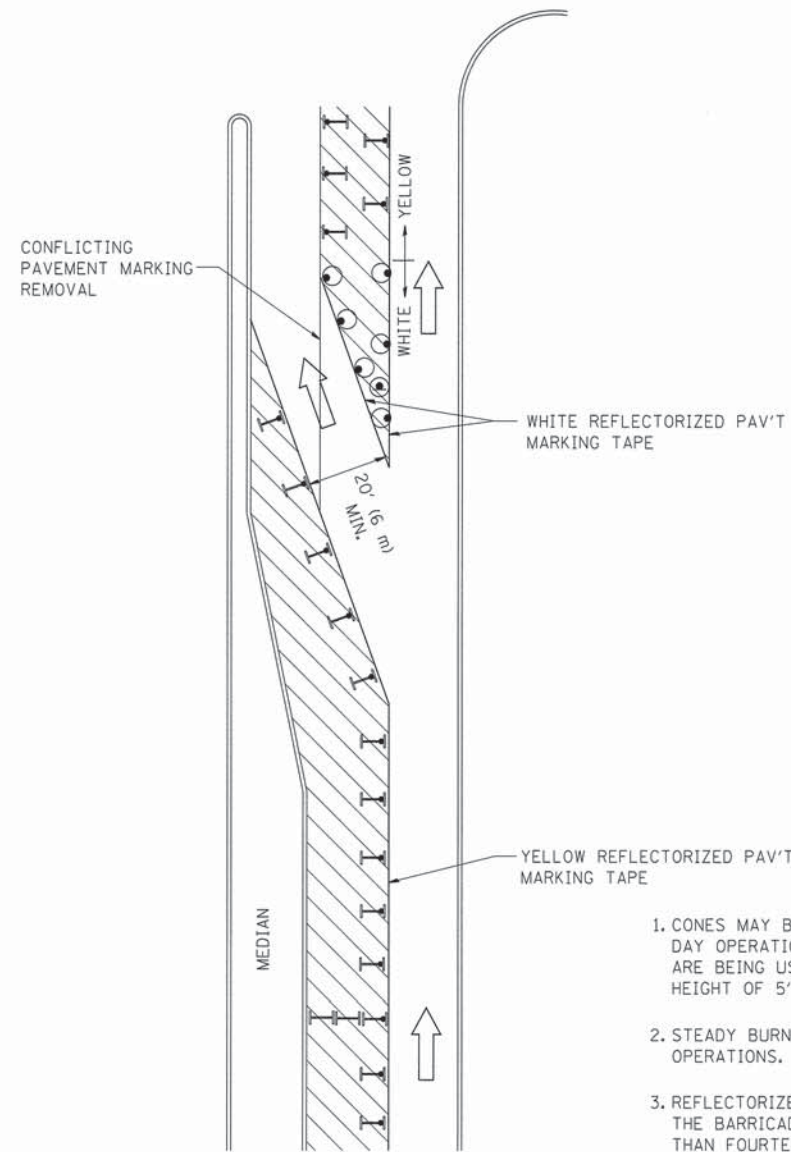
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
TYPICAL PAVEMENT MARKINGS

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0856	12-00154-00-RS	DUPAGE	26	21
TC-13		CONTRACT NO.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				









GENERAL NOTES

1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 REQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

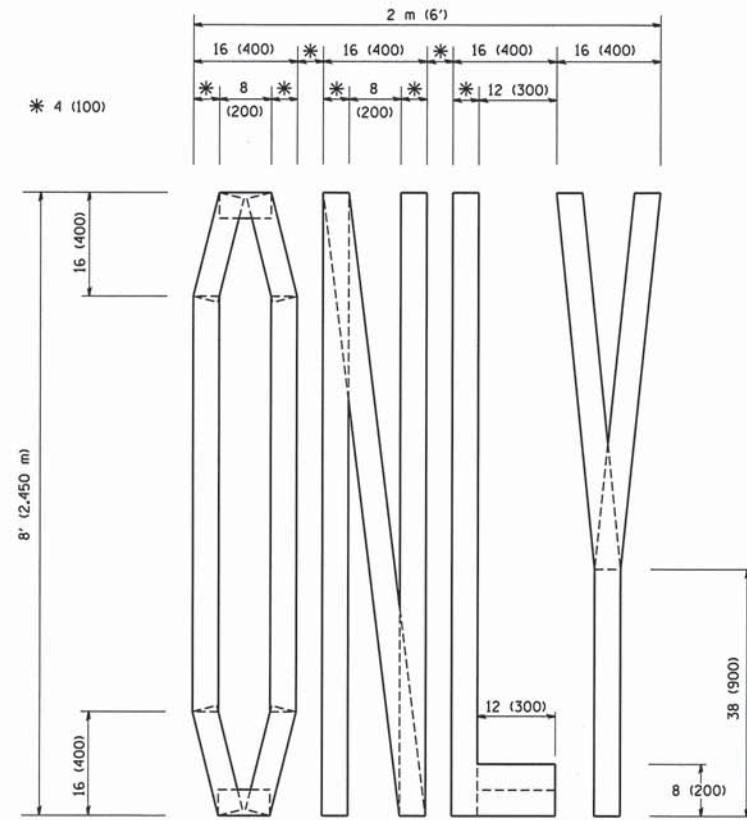
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

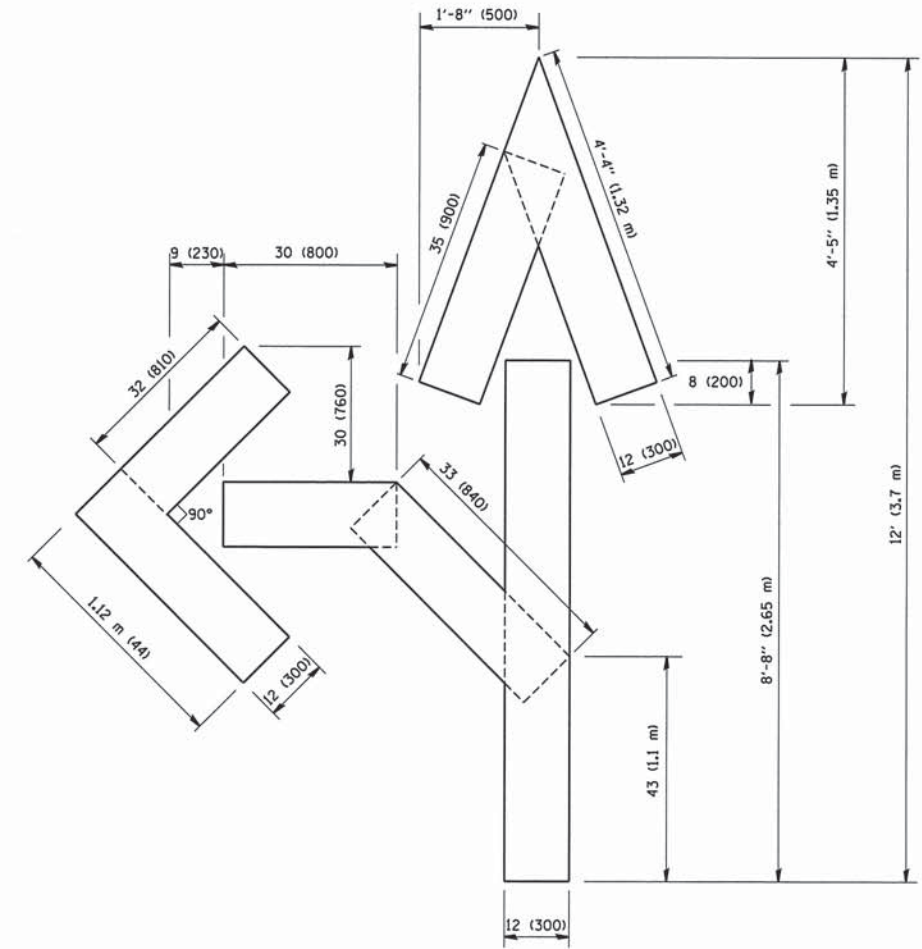
**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

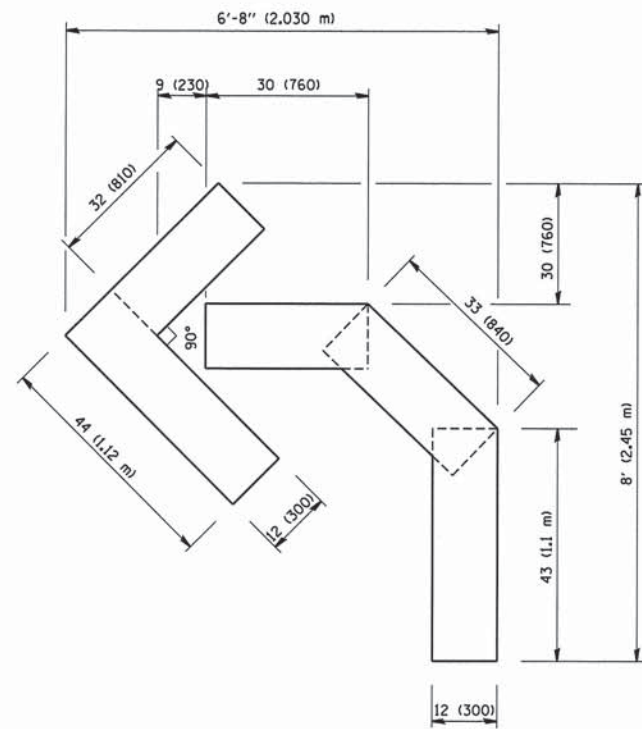
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0856	12-00154-00-RS	DUPAGE	26	22
TC-14			CONTRACT NO.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



QUANTITY
 4 (100) LINE = 64.1 ft. (19.7 m)
 21.1 sq. ft. (1.97 sq. m)



QUANTITY
 4 (100) LINE = 82.5 ft. (25.3 m)
 27.5 sq. ft. (2.53 sq. m)



QUANTITY
 4 (100) LINE = 45.5 ft. (13.9 m)
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

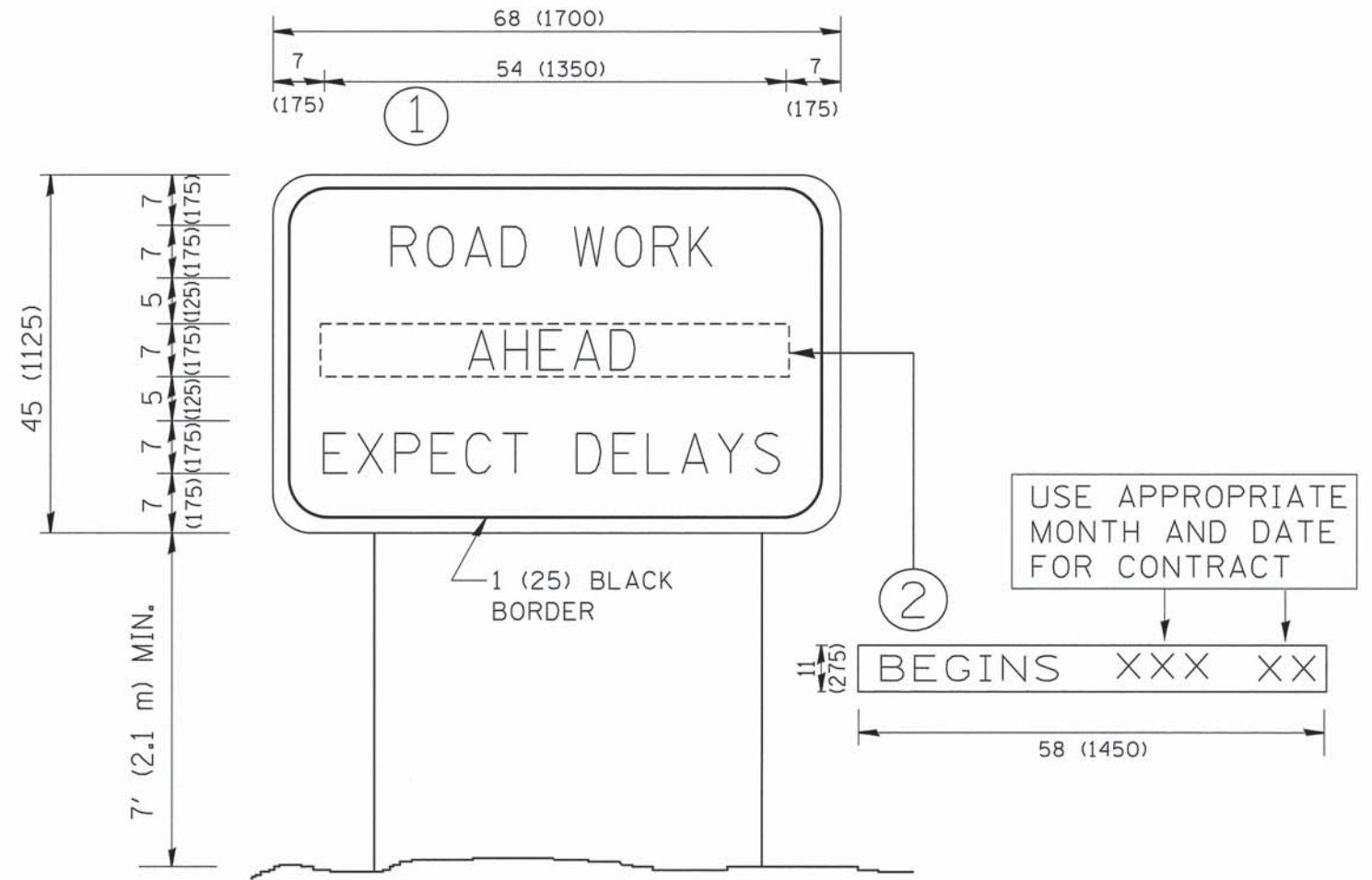
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		DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS
 FOR TRAFFIC STAGING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0856	12-00154-00-RS	DUPAGE	26	23
TC-16			CONTRACT NO.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

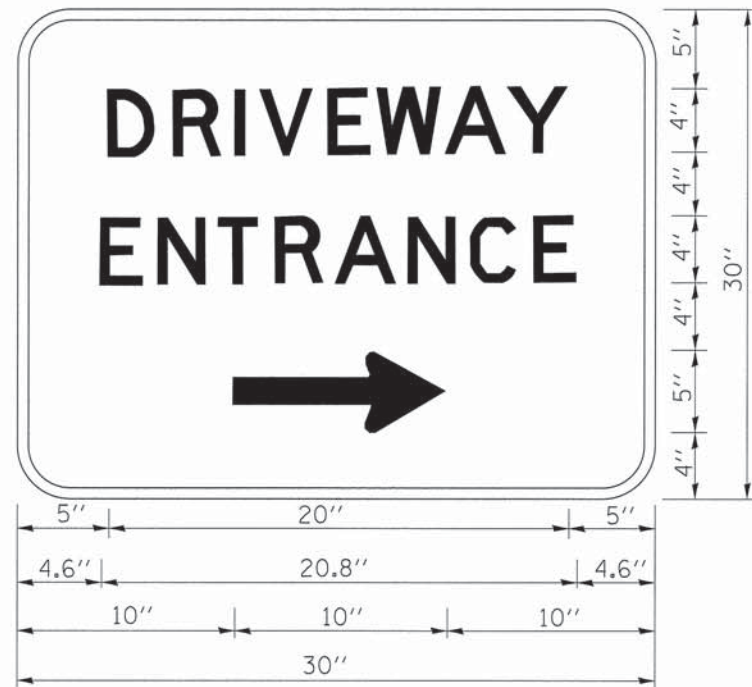
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		CHECKED -	REVISED - T. RAMMACHER 02-02-99
		DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	12-00154-00-RS	DUPAGE	26	24
TC-22		CONTRACT NO.		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

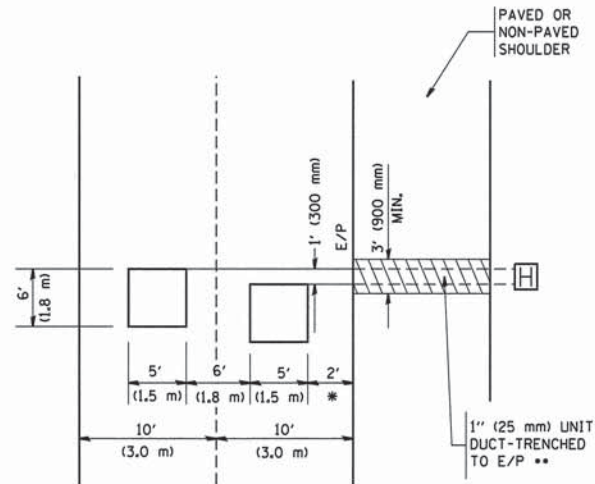
NOTES:

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN) SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

FILE NAME =	USER NAME = geglianobt	DESIGNED -	REVISED - C. JUCIUS 02-15-07	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DRIVEWAY ENTRANCE SIGNING				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.

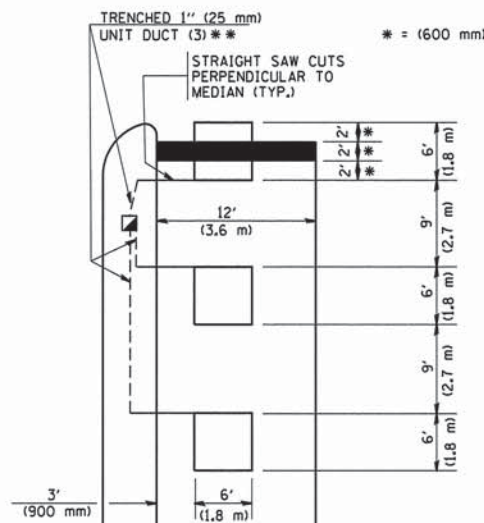


* = (600 mm)

** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**

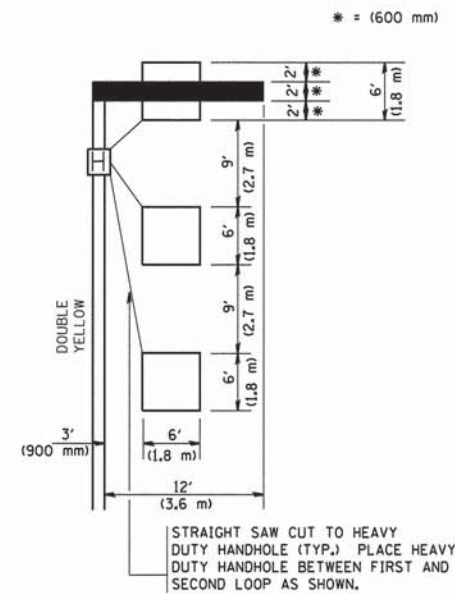
HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.



** UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

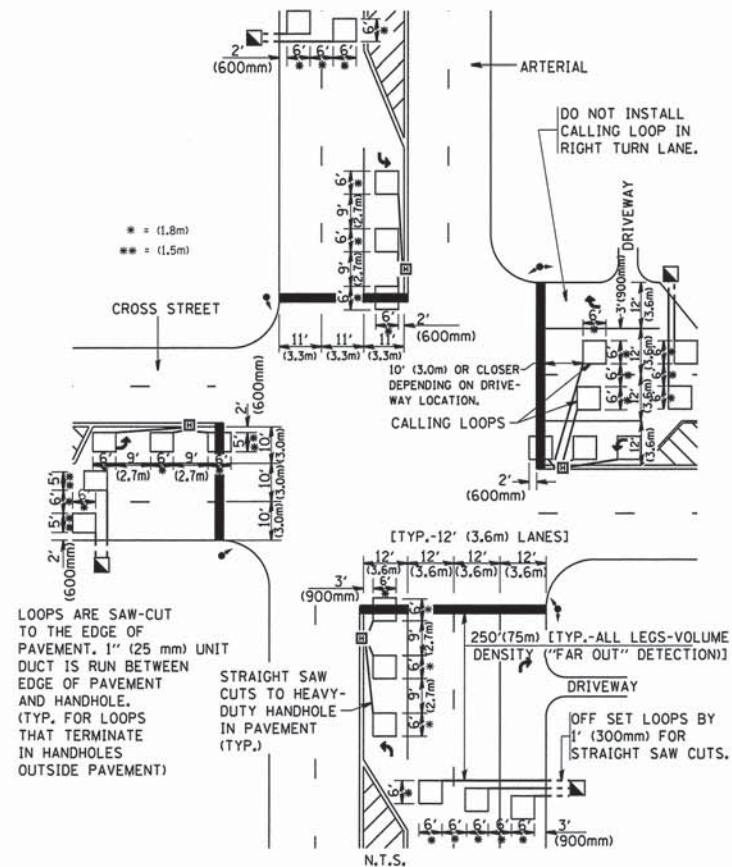
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)**



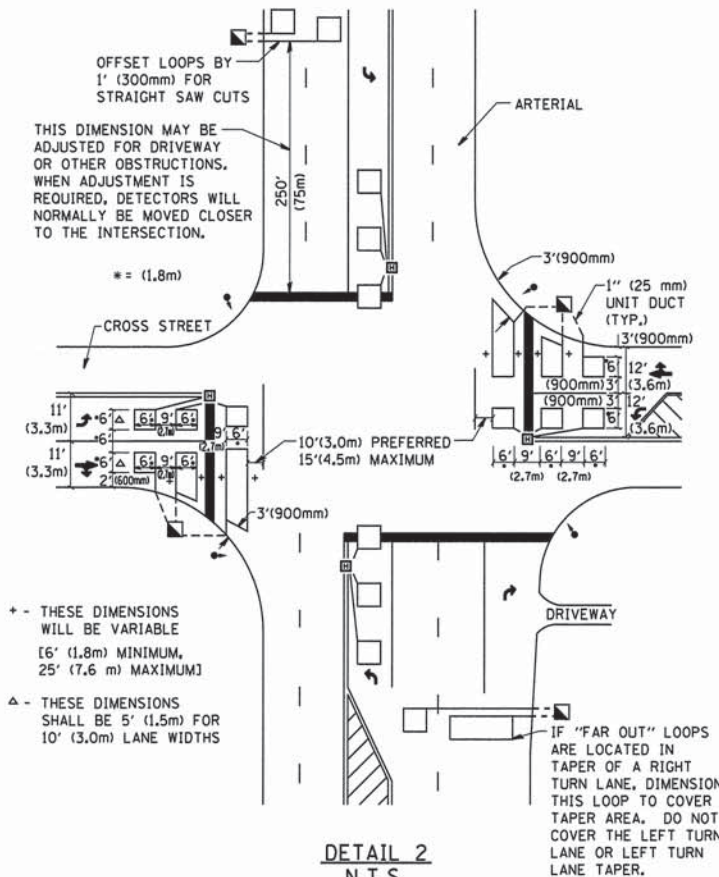
NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1
N.T.S.**

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2
N.T.S.**

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

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USER NAME = gegljanobt
PLOT SCALE = 50,0000 ' / IN.
PLOT DATE = 1/4/2008

DESIGNED -
DRAWN -
CHECKED - R,K,F.
DATE -

REVISED -
REVISED -
REVISED -
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT 1 - DETECTOR LOOP INSTALLATION
DETAILS FOR ROADWAY RESURFACING**

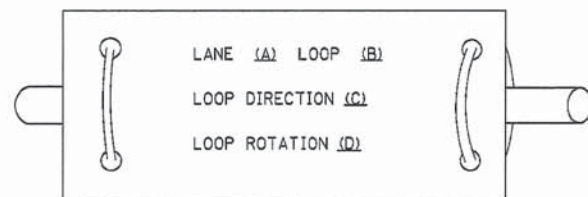
SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TS-07			CONTRACT NO.	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

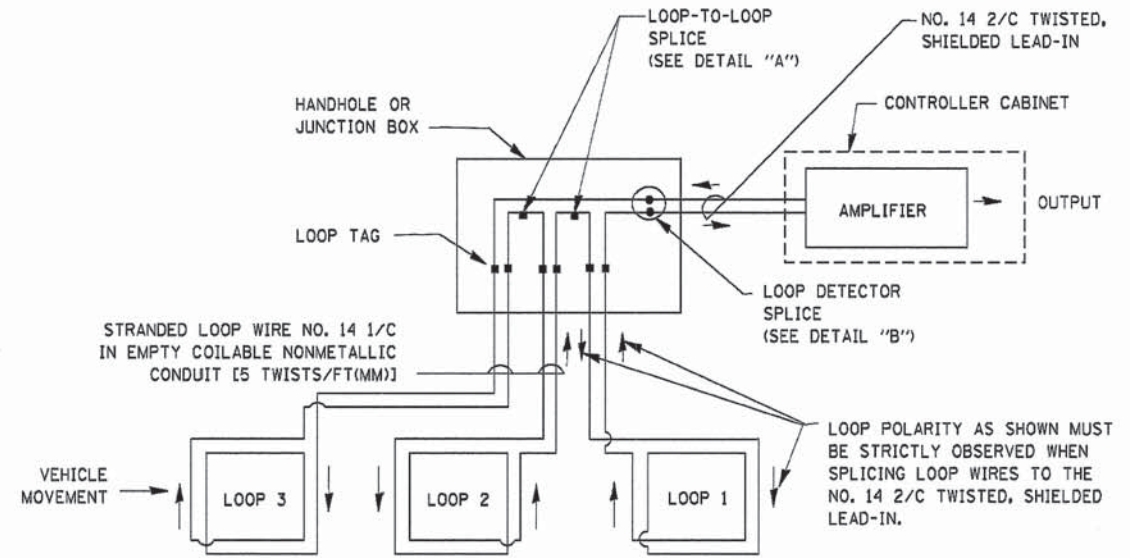
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

LOOP LEAD-IN CABLE TAG

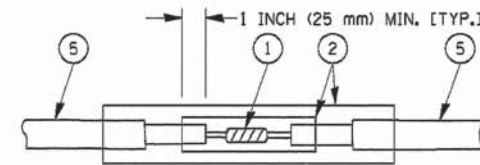


- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

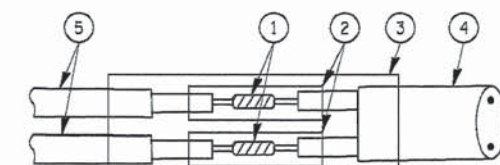


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

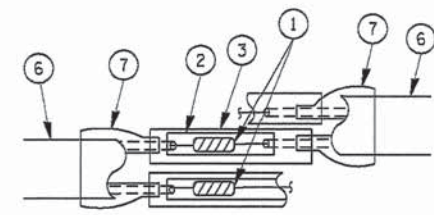


DETAIL "A" LOOP-TO-LOOP SPLICE

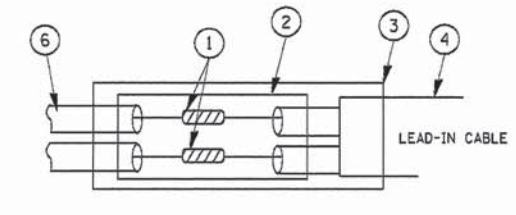


DETAIL "B" LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A" LOOP-TO-LOOP SPLICE



DETAIL "B" LOOP-TO-CONTROLLER SPLICE

LOOP DETECTOR SPLICE

- 1 WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- 2 WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- 3 WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- 4 NO. 14 2/C TWISTED, SHIELDED CABLE.
- 5 LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- 6 PRE-FORMED LOOP
- 7 XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

FILE NAME =	USER NAME = bauerdl	DESIGNED - DAD	REVISED -
as\pwork\PH1DOT\BAUERDL\d8188315\ts05.dgn		DRAWN - BCK	REVISED -
		CHECKED - DAD	REVISED -
		DATE - 10-28-09	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
0856	12-00154-00-RS	DUPAGE	26A	26A
TS-05			CONTRACT NO.	
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				

SCALE: NONE SHEET NO. 1 OF 6 SHEETS STA. TO STA.