

PROGRAM AND OFFICE ENGINEER: CHARLES F. RIDDLE, P.E. 847-705-4406 SCHAUMBURG, IL

FOR INDEX OF SHEETS, SEE SHEET NO. 2

02-28-14 LETTING ITEM 012

STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION

DIVISION OF HIGHWAYS

# PLANS FOR PROPOSED FEDERAL AID HIGHWAY

F.A.U. 4058 (JANDUS CUT OFF ROAD)  
UNION PACIFIC TO F.A.P. 305 (US ROUTE 14)  
RECONSTRUCTION OF JANDUS CUT OFF ROAD  
SECTION: 13-00063-00-PV  
PROJECT: M-4003(240)  
VILLAGE OF CARY  
MCHENRY COUNTY  
JOB: C-91-087-14

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
1321	13-00063-00-PV	MCHENRY	79	1
FED. ROAD DIST. NO.		ILLINOIS	CONTRACT NO. 63886	

79+1-80

**HIGHWAY CLASSIFICATION**

US ROUTE 14 - OTHER PRINCIPAL ARTERIAL  
JANDUS CUT OFF ROAD (WEST LEG) - MAJOR COLLECTOR (NON-URBAN)

**DESIGN DESIGNATION**

2700(30) ARTERIAL 22.15 (PCC-20)

**TRAFFIC DATA**

US ROUTE 14  
2012 ADT = 31,700

JANDUS CUT OFF ROAD  
2012 ADT = 13,550

**POSTED SPEED LIMIT**

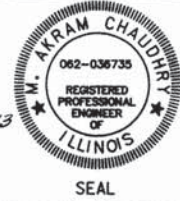
US ROUTE 14 = 35 MPH  
JANDUS CUT OFF ROAD = 25 MPH

**DESIGN SPEED LIMIT**

US ROUTE 14 = 35 MPH  
JANDUS CUT OFF ROAD = 25 MPH

PROJECT LOCATED IN THE  
VILLAGE OF CARY

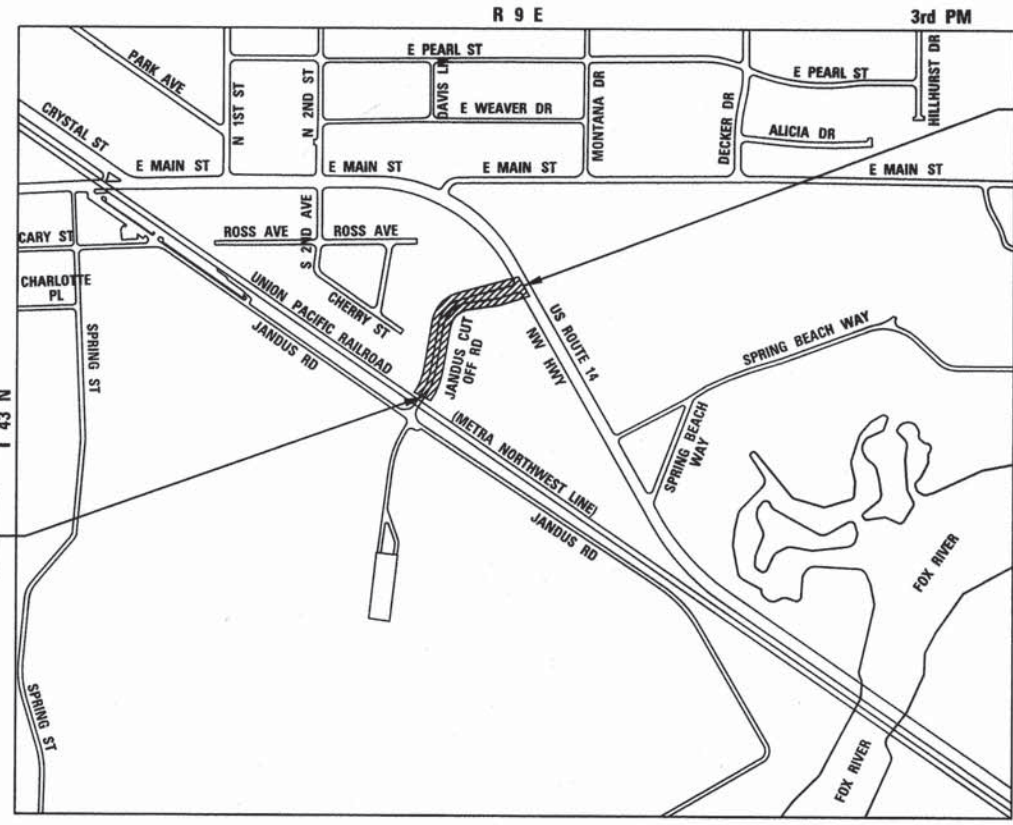
PROFESSIONAL ENGINEER'S SIGN & SEAL



*Akram Chaudhry* 12-26-2013  
AKRAM CHAUDHRY, P.E.  
EXPIRES: 11-30-2015

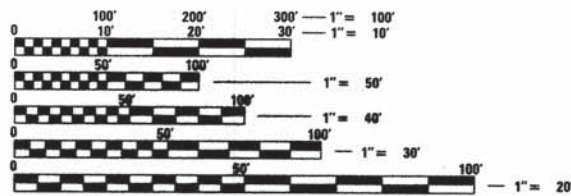


LOCATION OF SECTION INDICATED THUS: - [shaded area]



LOCATION MAP

PROJECT GROSS AND NET LENGTH:  
JANDUS CUT OFF ROAD = 897 FT (0.17 MI)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123 OR 811

PROJECT ENGINEER: BENJAMIN HARTMAN, P.E.  
PROJECT MANAGER: JACK MELHUISE, P.E.

CONTRACT NO. 63886

END PROJECT  
JANDUS CUT OFF ROAD  
STA 20+79.80

BEGIN PROJECT  
JANDUS CUT OFF ROAD  
STA 11+82.76

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
DIVISION OF HIGHWAYS

APPROVED *[Signature]* 2013  
VILLAGE OF CARY, PUBLIC WORKS DIRECTOR

PASSED *[Signature]* DEC 26 2013  
DISTRICT 1 ENGINEER OF LOCAL ROAD AND STREETS

RELEASING FOR BID  
BASED ON LIMITED  
REVIEW *[Signature]* DECEMBER 26, 2013  
DEPUTY DIRECTOR OF HIGHWAYS, REGION 1 ENGINEER

420 NORTH FRONT STREET, SUITE 100 | MCHENRY, ILLINOIS 60050  
Phone: 815.385.1778 | Toll Free: 800.728.7805 | Fax: 815.385.1781 | HRGreen.com  
ILLINOIS PROFESSIONAL DESIGN FIRM #184-001322

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OF THE STATE OF ILLINOIS

COMPANY NAME: [blank]  
 PROJECT CONTACT: [blank]  
 CLIENT: [blank]  
 DATE PLOTTED: 12/25/2013 9:59:33 AM  
 PLOT NUMBER: 8620253-cv-cop  
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**GENERAL NOTES 73-79 CROSS SECTIONS**

1. ALL REFERENCES TO "STANDARD SPECIFICATIONS" IN THESE GENERAL NOTES SHALL BE INTERPRETED TO MEAN "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION, JANUARY 1, 2002.
2. ALL REFERENCES TO "ENGINEER" SHALL BE INTERPRETED TO MEAN THE RESIDENT ENGINEER.
3. BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION) AT 8-1-1 AND THE VILLAGE OF CARY AT (847) 639-0003 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION IS REQUIRED).
4. ALL ELEVATIONS SHOWN ON THE PLANS ARE ON THE USGS (NAD 83) DATUM.
5. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS, PROPERTY CORNERS, AND REFERENCE MARKERS UNTIL THE OWNER, HIS AGENT, OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.
6. ALL RADII FOR PROPOSED CURB AND GUTTER ARE TO THE EDGE OF PAVEMENT, UNLESS OTHERWISE NOTED. CURB AND GUTTER ELEVATIONS SHOWN ALONG RETURNS AND AT POINTS OF CURVATURE, ETC. ARE TO THE EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
7. OFFSET LOCATIONS GIVEN IN THE PLANS FOR STRUCTURES, EDGE OF PAVEMENT, ETC. ARE FROM THE ROADWAY CENTERLINE.
8. THE BITUMINOUS MATERIAL PRIME COAT QUANTITIES HAVE BEEN DETERMINED USING A RATE OF 0.1 GALLONS / SQ YD.

9. SAW CUTTING OF PAVEMENTS, SIDEWALK, ETC. SHALL BE TO FULL DEPTH AND SHALL RESULT IN A CLEAN STRAIGHT EDGE ON THE PORTION REMAINING. ALL SAW CUTTING SHALL BE CONSIDERED INCLUDED IN THE ITEM REMOVED.

10. DRIVEWAY OFFSETS ARE TO BE CONSTRUCTED TO THE R.O.W. OR EASEMENT UNLESS OTHERWISE NOTED.

11. REMOVAL OF EXISTING COMBINATION CURB AND GUTTER, REGARDLESS OF CURB AND GUTTER TYPE, SHALL BE PAID FOR AS "COMBINATION CURB AND GUTTER REMOVAL".

12. THE ELEVATIONS SHOWN ON THE PLANS ARE FINISHED GRADES OF PROPOSED PAVEMENT, UNLESS OTHERWISE NOTED.

13. ALL SIGNS TO BE REMOVED SHALL REMAIN THE PROPERTY OF THE VILLAGE AND SHALL BE RETURNED TO THE VILLAGE UPON REMOVAL. CONTACT VILLAGE PUBLIC WORKS AT 847-639-0003 OR 454 CARY WOODS CIRCLE, CARY, IL.

14. ALL A.D.A. RAMP PANELS REMOVED UNDER SIDEWALK REMOVAL SHALL REMAIN THE PROPERTY OF THE VILLAGE AND SHALL BE RETURNED TO THE VILLAGE UPON REMOVAL.

15. THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR AT 847-705-4470 A MINIMUM OF 72 HOURS PRIOR TO THE PLACEMENT OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.

16. THE RESIDENT ENGINEER SHALL CONTACT THE QA MATERIALS TESTING CONSULTANT 48 HOURS IN ADVANCE OF PLACING MATERIALS.

17. THE CONTRACTOR SHALL DISPOSE OF ALL EXCESS EXCAVATION, SUITABLE AND UNUSABLE MATERIALS OFFSITE AND AT AN APPROVED LOCATION IN A MANNER THAT PUBLIC OR PRIVATE PROPERTY WILL NOT BE DAMAGED OR ENDANGERED. THIS WORK IS CONSIDERED INCLUDED IN THE COST OF THE PROJECT.

18. NO EXCAVATIONS WILL BE PERMITTED TO REMAIN OPEN OVER ANY WEEKEND AND NO EXCAVATIONS SHALL BE LEFT OPEN OVERNIGHT IN ANY RESIDENTIAL AREA.

19. SPECIAL ATTENTION IS DRAWN TO THE FACT THAT ARTICLE 105.06 OF THE STANDARD SPECIFICATIONS REQUIRES THE CONTRACTOR TO HAVE A COMPETENT SUPERINTENDENT ON THE PROJECT SITE AT ALL TIMES, IRRESPECTIVE OF THE AMOUNT OF WORK SUBLET. THE SUPERINTENDENT SHALL BE CAPABLE OF READING AND UNDERSTANDING THE PLANS AND SPECIFICATIONS, SHALL HAVE FULL AUTHORITY TO EXECUTE ORDERS TO EXPEDITE THE PROJECT, SHALL BE RESPONSIBLE FOR SCHEDULING AND HAVE CONTROL OF ALL WORK AS THE AGENT OF THE CONTRACTOR. FAILURE TO COMPLY WITH THIS PROVISION WILL RESULT IN A SUSPENSION OF WORK AS PROVIDED IN ARTICLE 107.07.

20. THE ENGINEER AND VILLAGE ARE NOT RESPONSIBLE FOR THE CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCES OR PROCEDURES, TIME OF PERFORMANCE, PROGRAMS OR FOR ANY SAFETY PRECAUTIONS USED BY THE CONTRACTOR. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR EXECUTION OF HIS WORK IN ACCORDANCE WITH THE CONTRACT DOCUMENTS AND SPECIFICATIONS.

21. THE INDISCRIMINATE USE OF FIRE HYDRANTS, EXISTING STREAMS, CREEKS, WETLANDS, OR PONDS IS STRICTLY PROHIBITED. THE CONTRACTOR SHALL PROVIDE A WATER TRUCK AND DRIVER AS REQUIRED TO OBTAIN AND TRANSPORT THIS WATER. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING WATER FROM AN APPROVED SOURCE. IF THIS WATER IS FROM A SOURCE OTHER THAN HIS YARD, WRITTEN APPROVAL FROM THE AGENCY HAVING JURISDICTION FOR THE SOURCE OF THE WATER MUST BE RECEIVED BY THE CONTRACTOR PRIOR TO THE USE OF THE WATER.

22. THE CONTRACTOR SHALL BE RESPONSIBLE FOR POSTING "FRESH OIL" SIGNS (48"x48" MINIMUM) AS NEEDED FOR THE PROJECT. THE CONTRACTOR SHALL INSTALL AND MAINTAIN TEMPORARY SIGNS IN THE PARKWAY TWENTY-FOUR (24) HOURS PRIOR TO STARTING WORK ON EACH STREET. THE SIGNS SHALL READ "FRESH OIL, TRAVEL AT YOUR OWN RISK." THE CONTRACTOR SHALL REMOVE THE SIGNS AND REINSTALL THEM AS NEEDED. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT BE INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (SPECIAL)

23. THE CONTRACTOR SHALL FILL THE HOLES CREATED BY THE REMOVAL OF THE DRIVEWAY PAVEMENT WHERE NEW GUTTER OR SIDEWALK IS TO BE INSTALLED WITH AGGREGATE BASE COURSE (CA-6 CRUSHED) SO THAT THE RESIDENTS CAN USE THEIR DRIVEWAYS UNTIL THE START OF INSTALLATION OF THE CONCRETE GUTTER AND/OR SIDEWALK. THE COST OF THE AGGREGATE BASE COURSE WILL BE INCLUDED IN THE COST OF THE ITEM OF WORK BEING CONSTRUCTED.

24. THE CONTRACTOR SHALL BE RESPONSIBLE FOR SWEEPING AND CLEANING STREETS OF ANY DEBRIS AND MATERIAL THAT HAS ACCUMULATED AS A RESULT OF THE CONSTRUCTION ACTIVITY. A MECHANICAL SWEEPER, MECHANICALLY DRIVEN AIR AND HANDWORK WITH SHOVEL AND BROOM SHALL BE UTILIZED TO PROVIDE A CLEAN STREET FOR THE MOTORING PUBLIC. WITHIN 24 HOURS OF PLACING PRIME COAT AND THE LAYING OF HMA, THE CONTRACTOR SHALL SWEEP THE PAVEMENT AND REMOVE STANDING WATER, EARTH, WEEDS, LEAVES, DIRT, CONSTRUCTION DEBRIS, AND ALL LOOSE MATERIAL.

25. CONSTRUCTION WORK MAY BE PERFORMED MONDAY THRU SATURDAY DURING THE HOURS OF 7:00 A.M. TO 7:00 P.M. NO WORK MAY BE PERFORMED PRIOR OR BEYOND THIS PERIOD WITHOUT PRIOR WRITTEN APPROVAL FROM THE VILLAGE. NO COMPENSATION WILL BE PAID FOR ANY INCONVENIENCE, DELAY, OR LOSS EXPERIENCED BY THE CONTRACTOR BECAUSE OF ADJUSTMENTS TO THEIR NORMAL SCHEDULE.

26. THE CONTRACTOR SHALL BE RESPONSIBLE FOR HIRING AN ENVIRONMENTAL FIRM WITH AT LEAST FIVE DOCUMENTED LEAKING UNDERGROUND STORAGE TANK (LUST) CLEANUPS OR THAT IS PRE-QUALIFIED IN HAZARDOUS WASTE BY THE DEPARTMENT TO REMEDIATE THE SOIL CONTAMINATION AND MONITOR FOR WORKER PROTECTION.

27. THE STEEL MAST ARM ASSEMBLIES, POLES, SIGNAL POSTS, SIGNAL HOUSINGS, AND BRACKETS SHALL BE PAINTED WITH TWO COATS OF BLACK PAINT.

**STORM SEWERS, WATER MAINS, AND UTILITIES**

1. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL UTILITIES PRIOR TO CONSTRUCTION TO DETERMINE THE LOCATION OF ALL UTILITY EQUIPMENT. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS IN ACCORDANCE WITH THE STANDARD SPECIFICATIONS IF UTILITY RELOCATION, ADJUSTMENT, OR PROTECTION IS NECESSARY.

2. THE LOCATION OF EXISTING DRAINAGE STRUCTURES, STORM SEWERS, WATER MAINS, SANITARY SEWERS, AND ANY OTHER PUBLIC OR PRIVATE UTILITIES AS SHOWN ON THE PLANS IS APPROXIMATE AND THEIR EXACT LOCATION IS TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR.

3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND AND SURFACE UTILITIES EVEN THOUGH THEY MIGHT NOT BE SHOWN ON THE PLANS. ANY UTILITY PROPERTY DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER.

4. ALL UTILITY COMPANIES SHALL BE NOTIFIED AT LEAST 48 HOURS PRIOR TO THE START OF CONSTRUCTION.

5. THE CONTRACTOR SHALL USE ALL NECESSARY PRECAUTIONS AND PROTECTION MEASURES REQUIRED TO MAINTAIN EXISTING UTILITIES, SEWERS, AND APPURTENANCES THAT MUST BE KEPT IN OPERATION.

6. ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT, ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.

7. WHEN EXISTING DRAINAGE FACILITIES ARE DISTURBED, THE CONTRACTOR SHALL PROVIDE AND MAINTAIN, IN AN OPERATING CONDITION, TEMPORARY OUTLETS AND CONNECTIONS FOR ALL DRAINS, SEWERS, AND CATCH BASINS. THE CONTRACTOR SHALL PROVIDE FACILITIES WHICH HAVE THE CAPACITY TO RECEIVE AND DISCHARGE THE STORM WATER FLOW RATES NORMALLY ACCEPTED AND RELEASED BY THE EXISTING DRAINAGE FACILITIES. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.

8. ALL FRAMES, GRATES, LIDS, AND BOXES SCHEDULED TO BE REMOVED FROM EXISTING STRUCTURES SHALL REMAIN THE PROPERTY AND BE DELIVERED TO THE VILLAGE, AS APPLICABLE. CONTACT VILLAGE PUBLIC WORKS AT 847-639-0003 OR 454 CARY WOODS CIRCLE, CARY, IL.

9. ANY ITEMS DAMAGED DURING REMOVAL OPERATIONS SHALL BE REPLACED BY THE CONTRACTOR AT HIS EXPENSE. THE COST OF SALVAGING EXISTING FRAMES, GRATES, LIDS, OR BOXES AND/OR STOCKPILING THEM ON THE JOB SITE FOR DELIVERY TO THE VILLAGE OF CARY, AS APPLICABLE SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT, AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. CONTACT PUBLIC WORKS AT 847-639-0003.

10. RIM ELEVATIONS GIVEN ON THE PLANS ARE ONLY TO ASSIST THE CONTRACTOR IN DETERMINING THE APPROXIMATE OVERALL HEIGHT OF THE STRUCTURE. RIM ELEVATIONS FOR STRUCTURES IN THE CURB ARE TO EDGE OF PAVEMENT. FRAMES ON ALL NEW, ADJUSTED OR RECONSTRUCTED STRUCTURES WILL BE ADJUSTED TO THE FINAL ELEVATION OF THE AREA IN WHICH THEY ARE LOCATED AS PART OF THE STRUCTURE, ADJUSTMENT OR RECONSTRUCTION COST.

11. ONLY PRECAST CONCRETE ADJUSTMENT RINGS, MAXIMUM OF 12" IN HEIGHT, WILL BE ALLOWED IN THE ADJUSTMENT OR RECONSTRUCTION OF CATCH BASIN, MANHOLE, INLET AND VALVE VAULT STRUCTURES. COMMON BRICK WILL NOT BE ALLOWED. THE RINGS SHALL BE INCLUDED IN THE COST OF WORK BEING PERFORMED.

12. THE VILLAGE PUBLIC WORKS DEPARTMENT WILL PROVIDE A WATER METER SO THAT WATER CAN BE USED FOR DUST CONTROL, MIXING MORTAR, ETC. THE CONTRACTOR WILL PAY FOR THE QUANTITY OF WATER USED AND WILL NOT BE REIMBURSED FOR THE USE OF VILLAGE WATER.

13. THE THICKNESSES OF HOT MIX ASPHALT MIXTURES SHOWN IN THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACES OR BASES ON WHICH THE HOT MIX ASPHALT MIXTURES ARE TO BE PLACED.

14. WHERE NEW WORK MEETS EXISTING FEATURES TO REMAIN, FIELD CHECK ALL DIMENSIONS AND ELEVATIONS BEFORE PROCEEDING WITH CONSTRUCTION, NOTIFY ENGINEER IMMEDIATELY OF ANY DISCREPANCIES.

15. TYPE "B" CURB RAMPS SHALL BE INSTALLED AT ALL INTERSECTING STREETS AND CURBED DRIVEWAYS PER CURRENT IDOT STANDARDS AT LOCATIONS WHERE SIDEWALK IS SHOWN ON THE PLAN.

16. THE CONTRACTOR WILL BE REQUIRED TO TEMPORARILY RESET ALL MAILBOXES WHICH INTERFERE WITH CONSTRUCTION OPERATIONS AND AFTER COMPLETION OF ROADWAY CONSTRUCTIONS, TO SET THEM IN THEIR PERMANENT LOCATIONS PER UNITED STATES POST OFFICE REQUIREMENTS. THIS WORK SHALL BE DONE IN CONFORMANCE WITH ARTICLE 107.20 OF THE STANDARDS SPECIFICATIONS, AND THE COST WILL BE CONSIDERED INCLUDED IN THE CONTRACT.

17. ALL WATER MAIN CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS", LATEST EDITION, AND REVISIONS THERETO. THESE IMPROVEMENT PLANS AND DETAILS, SPECIAL PROVISIONS AND IN ACCORDANCE WITH CODES AND ORDINANCES OF THE VILLAGE OF CARY.

18. ALL WATER MAINS SHALL BE DUCTILE IRON PIPE CLASS 52 WITH EITHER MECHANICAL OR PUSH-ON JOINTS AND SHALL CONFORM TO ANSI A21.51-96, AWWA C151 AND ANSI A21.11-00, AWWA C111. ALL PIPE AND FITTINGS SHALL BE MADE IN THE UNITED STATES.

19. ALL WATER MAIN FITTINGS SHALL BE COMPACT DUCTILE IRON AND SHALL CONFORM TO AWWA/ANSI C153/A21.53-00. FITTINGS SHALL BE U.L. LISTED CLASS 350, TYLER GRIFFIN, CLOW. ALL FITTINGS SHALL BE MADE IN THE UNITED STATES.

20. ALL PIPE AND FITTINGS SHALL BE CEMENT LINED IN ACCORDANCE WITH AWWA/ANSI C104/A21.4-95.

21. ALL FITTINGS SHALL BE MECHANICAL JOINT AND SHALL BE RESTRAINED WITH MEGA LUGS UNLESS OTHERWISE SHOWN ON THE DRAWINGS.

22. LONG RADIUS CURVES, EITHER HORIZONTAL OR VERTICAL MAY BE LAID WITH STANDARD PIPE AT DEFLECTIONS AT THE JOINTS. MAXIMUM DEFLECTIONS AT PIPE JOINTS AND LAYING RADIUS FOR THE VARIOUS PIPE LENGTHS SHALL BE IN ACCORDANCE WITH AWWA C800-99. WHEN RUBBER GASKETED PIPE IS LAID ON A CURVE, THE PIPE SHALL BE JOINTED IN A STRAIGHT ALIGNMENT AND THEN DEFLECTED TO THE CURVED ALIGNMENT. TRENCHES SHALL BE MADE WIDER ON CURVES FOR THIS PURPOSE.

23. COUPLINGS FOR CONNECTIONS TO EXISTING ASBESTOS CEMENT WATER MAINS SHALL BE FORD OR CASCADE, BOTH WITH ALLOY BOLTS AND FUSION BONDED EPOXY COATING.

**SANITARY SEWER CONSTRUCTION**

1. ALL SANITARY SEWER CONSTRUCTIONS SHALL BE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTIONS IN ILLINOIS", LATEST EDITION, AND REVISIONS THERETO, THE NOTES ON THE PLANS AND IN ACCORDANCE WITH CODES AND ORDINANCES OF THE VILLAGE OF CARY.

2. THE FINAL ADJUSTMENTS OF CASTINGS WILL BE ACCOMPLISHED BY USE OF PRECAST CONCRETE ADJUSTING RINGS SET IN SEALANT. MORTAR JOINTS WILL NOT BE ALLOWED. TOTAL HEIGHT OF ADJUSTING RINGS USED SHALL NOT EXCEED EIGHT (8") INCHES IN THE VILLAGE OF CARY.

3. IN THE VILLAGE OF CARY, THREE (3) ADJUSTMENT RINGS TOTALING EIGHT (8") INCHES IN HEIGHT MAY BE USED. NO MORE THAN TWO (2) RINGS MAY BE PRECAST CONCRETE. THE TOP RING IN PAVED AREAS WITH CROWN ADJUSTMENTS SHALL BE RUBBER.

4. AN APPROVED EXTERNAL ADAPTOR SEAL SHALL BE INSTALLED IN ALL MANHOLES.

5. SANITARY SEWER SERVICES SHALL BE CONSTRUCTED ACCORDING TO THE DETAILS ON THE PLANS.

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**INDEX, GENERAL NOTES, AND LIST OF STATE STANDARDS**

SCALE: NTS SHEET NO. 1 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	2
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 63886	

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	DRAWN - DMS	REVISED -
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- WHEN CONNECTING TO AN EXISTING SANITARY MAIN WHEN A TEE OR WYE IS NOT PROVIDED, A SEWER SADDLE MUST BE INSTALLED. THE MINIMUM DISTANCE BETWEEN FITTINGS IS FOUR (4) FEET CENTER TO CENTER. DISRUPTION OF ANY EXISTING SANITARY MAIN BY BREAKING OR CUTTING IN A WYE/TEE IS PROHIBITED UNLESS THE EXISTING MAIN IS CRACKED OR BROKEN AT THE POINT OF CONNECTION WITH THE SERVICE SADDLE. THE ENGINEER SHALL DETERMINE THE EXISTING MAIN REPAIR OR REPLACEMENT REQUIRED ON A CASE BY CASE BASIS PRIOR TO CONNECTION, CONSTRUCTION OR INSTALLATION.
- ALL SANITARY SEWERS WILL BE SUBJECT TO AN AIR EXFILTRATION TEST. TELEVISION TEST AND DEFLECTION TEST TO BE PERFORMED BY THE CONTRACTOR UNDER THE SUPERVISION OF THE ENGINEER. ALL TESTING WILL BE DONE IN CONFORMANCE WITH THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS", LATEST EDITION, AND VILLAGE OF CARY REQUIREMENTS.
- VACUUM TESTING OF EACH MANHOLE SHALL BE CARRIED OUT IMMEDIATELY AFTER ASSEMBLY AND PRIOR TO BACKFILLING, ALL LIFT HOLES SHALL BE PLUGGED WITH AN APPROVED NON-SHRINK GROUT.
- NO GROUT WILL BE PLACED IN THE HORIZONTAL JOINTS BEFORE TESTING. ALL PIPES ENTERING THE MANHOLE SHALL BE PLUGGED, TAKING CARE TO SECURELY BRACE THE PLUGS FROM BEING DRAWN INTO THE MANHOLE.
- THE TESTING HEAD SHALL BE PLACED AT THE INSIDE OF THE TOP OF THE FRAME SECTION AND THE SEAL INFLATED IN ACCORDANCE WITH THE MANUFACTURERS RECOMMENDATION.
- A VACUUM OF 10 INCHES OF MERCURY SHALL BE DRAWN AND THE VACUUM PUMP SHUT OFF WITH THE VALVES CLOSED, THE TIME SHALL BE MEASURED FOR THE VACUUM TO DROP TO 9 INCHES. THE MANHOLE SHALL PASS IF THE TIME IS GREATER THAN 60 SECONDS FOR A 48 INCH DIAMETER MANHOLE, 75 SECONDS FOR A 60 INCH MANHOLE AND 90 SECONDS FOR A 72 INCH MANHOLE. IF THE MANHOLE FAILS THE INITIAL TEST, NECESSARY REPAIRS SHALL BE MADE WITH A NON-SHRINK GROUT WHILE THE VACUUM IS STILL BEING DRAWN. RETESTING SHALL PROCEED UNTIL A SATISFACTORY TEST IS OBTAINED.
- ALL COVERS USED FOR SANITARY SEWERS SHALL HAVE A MACHINED SURFACE AND A WATERTIGHT RUBBER GASKET SEAL. FULL FRAME SEAL SHALL BE AN APPROVED SEALANT. TROWEL MASTIC OVER FULL SURFACE BETWEEN FRAMES, ADJUSTING RINGS AND CONES.

**BACKFILL**

- STORM SEWER, WATER MAIN, AND SANITARY SEWER SHALL BE BACK FILLED IN ACCORDANCE WITH ARTICLE 550.07, METHOD 1 ONLY.
- PROVIDE TRENCH BACKFILL FOR ALL UTILITY LINES WITHIN 2' OF PAVED AREAS. ALL TRENCH BACKFILL QUANTITIES FOR STORM SEWER HAVE BEEN COMPUTED AND SHALL BE PAID FOR IN ACCORDANCE WITH THE STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS BUREAU OF CONSTRUCTION TRENCH BACKFILL TABLE, BASED ON PIPE SIZE. DEPTH WILL BE CALCULATED AS INVERT DEPTH FROM SURFACE ELEVATION.
- TRENCH BACKFILL AND AGGREGATE SHOULDER MATERIAL SHALL CONSIST OF CA-6 MATERIAL.

**SIGNING AND STRIPING**

- SEE IDOT STANDARD DETAIL 780001, DISTRICT ONE DETAIL AND PLAN SHEETS FOR PAVEMENT MARKING DETAILS.
- SIGNS SHALL NOT BE MOVED OR COVERED UNTIL PROGRESS OF WORK NECESSITATES IT.
- THE CONTRACTOR WILL BE REQUIRED TO TEMPORARILY RESET ALL SUCH SIGNS THAT INTERFERE WITH HIS CONSTRUCTION OPERATIONS. ALL SUCH SIGNS MUST BE MAINTAINED STRAIGHT AND CLEAN FOR THE DURATION OF THE TEMPORARY SETTING AND MUST BE RE-ERECTED AT A TEMPORARY LOCATION IN A WORKMANLIKE MANNER AND BE VISIBLE TO THE TRAFFIC FOR WHICH IT IS INTENDED. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.
- ALL SIGNS SHALL BE INSTALLED OR RELOCATED IN PERMANENT LOCATIONS AS THE ROADWAY IS COMPLETED. THIS WORK SHALL BE PAID FOR USING THE APPROPRIATE PAY ITEMS.
- ALL REMOVED SIGNS SHALL BE RETURNED TO THE VILLAGE OF CARY. CONTACT VILLAGE PUBLIC WORKS AT 847-639-0003 OR 454 CARY WOODS CIRCLE, CARY, IL.
- LONGER POSTS MAY BE REQUIRED AT SOME TEMPORARY OR PERMANENT SIGN LOCATIONS TO MAINTAIN PROPER SIGN ELEVATIONS. THIS WORK SHALL BE PAID FOR IN ACCORDANCE WITH SECTION 729 OF THE STANDARD SPECIFICATIONS.

**TRAFFIC CONTROL**

- SEE TRAFFIC CONTROL PLANS FOR GENERAL NOTES CONCERNING TRAFFIC CONTROL AND PROTECTION.

**EROSION CONTROL PLANS**

- SEE EROSION CONTROL PLANS FOR GENERAL NOTES CONCERNING EROSION CONTROL.

**EARTHWORK**

- PRIOR TO ANY EMBANKMENT PLACEMENT ALL VEGETATION AND UNSTABLE MATERIAL SHOULD BE REMOVED TO DEPTH ENCOUNTERED AND REPLACED WITH SUITABLE EMBANKMENT MATERIAL.
- SUBGRADE SHALL BE PREPARED IN ACCORDANCE WITH ARTICLE 301.03 OF THE STANDARD SPECIFICATIONS BEFORE REMOVAL OF ANY UNSTABLE MATERIAL.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR HIRING AN ENVIRONMENTAL FIRM WITH AT LEAST FIVE CLEANUPS OR THAT IS PRE-QUALIFIED IN HAZARDOUS WASTE BY THE DEPARTMENT TO REMEDIATE THE SOIL CONTAMINATION AND MONITOR FOR WORKER PROTECTION. THE GENERATOR NUMBER FOR MCHENRY COUNTY IS 1118995010.

**STATE STANDARDS**

STANDARD NO.	LIST OF DESCRIPTION
000001-06	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
424001-07	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
424006-01	DIAGONAL CURB RAMPS FOR SIDEWALKS
424011-01	CORNER PARALLEL CURB RAMPS FOR SIDEWALKS
424021-02	DEPRESSED CORNER FOR SIDEWALKS
424026-01	ENTRANCE / ALLEY PEDESTRIAN CROSSINGS
442201-03	CLASS C AND D PATCHES
542301-03	PRECAST REINFORCED CONCRETE FLARED END SECTION
602001-02	CATCH BASIN, TYPE A
602301-04	INLET, TYPE A
602401-03	MANHOLE, TYPE A
602501-02	VALVE VAULT, TYPE A
602601-03	PRECAST REINFORCED CONCRETE FLAT SLAB TOP
602701-02	MANHOLE STEPS
604001-03	FRAME AND LIDS, TYPE 1
604036-02	GRATE TYPE 8
604051-03	FRAME AND GRATE, TYPE 11
606001-05	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
606201-02	TYPE B GUTTER (INLET, OUTLET AND ENTRANCE)
701006-05	OFF-RD OPERATIONS, 2L, 2W, 15' TO 24" FROM PAVEMENT EDGE
701011-04	OFF-RD MOVING OPERATIONS, 2L, 2W, DAY ONLY
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE, 2L, 2W, MOVING OPERATIONS - DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W UNDIVIDED
701701-09	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-05	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-03	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-04	SIGN PANEL ERECTION DETAILS
720011-01	METAL POSTS FOR SIGNS, MARKERS AND DELINEATORS
729001-01	APPLICATIONS OF TYPES A & B METAL POSTS (FOR SIGNS AND MARKERS)
780001-04	TYPICAL PAVEMENT MARKINGS
781001-03	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
805001-01	ELECTRICAL SERVICE INSTALLATION DETAILS
814001-02	HANDHOLES
814006-02	DOUBLE HANDHOLES
825021-02	LIGHTING CONTROLLER, BASE MOUNTED, 240V
830001-02	LIGHT POLE ALUMINUM MAST ARM
857001-01	STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCES
862001-01	UNINTERRUPTABLE POWER SUPPLY (UPS)
873001-02	TRAFFIC SIGNAL GROUNDING & BONDING
877001-05	STEEL MAST ARM ASSEMBLY AND POLE 16' THROUGH 55'
878001-09	CONCRETE FOUNDATION DETAILS
880001-01	SPAN WIRE MOUNTED SIGNALS AND FLASHING BEACON INSTALLATION
880006-01	TRAFFIC SIGNAL MOUNTING DETAILS
886001-01	DETECTOR LOOP INSTALLATION

**DISTRICT ONE DETAILS**

BD-01	DRIVEWAY DETAILS
BD-03	DISTANCE BETWEEN R.O.W. AND FACE OF CURB & EDGE OF SHOULDER >= 15' (4.5 M)
BD-07	OUTLET FOR CONCRETE CURB AND GUTTER
BD-12	6060095 CLASS SI CONCRETE (OUTLET) CU YD
BD-22	DETAIL OF STORM SEWER CONNECTION TO EXISTING SEWER
TC-10	MANHOLE WITH RESTRICTOR PLATE
TC-11	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT
TC-13	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS
TC-16	TYPICAL APPLICATIONS
TC-22	RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLow RESISTANT)
TC-26	DISTRICT ONE TYPICAL PAVEMENT MARKINGS
	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING
	ARTERIAL ROAD INFORMATION SIGN
	DRIVEWAY ENTRANCE SIGNING

COMPANY NAME: HRGreen.com  
 PROJECT CONTACT: Illinois Professional Design Firm # 184-001322  
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PLOT DATE = 1/13/2014	CHECKED - TEH	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**INDEX, GENERAL NOTES, AND LIST OF STATE STANDARDS**

SCALE: NTS SHEET NO. 2 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
405B	13-00063-00-PV	MCHENRY	79	3
CONTRACT NO. 63886				
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				



PAYCODE	ITEM DESCRIPTION	UNIT	TOTAL	ROADWAY 80%FEDERAL 20%STATE 0004	SIGNALS 80%FEDERAL 20%STATE 0021	LIGHTING 80%FEDERAL 20%STATE 0021	SANITARY 100% LOCAL 0043
20200100	EARTH EXCAVATION	CU YD	1530	1,530			
20201200	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	CU YD	2620	2,620			
20800150	TRENCH BACKFILL	CU YD	241	73			168
21101615	TOPSOIL FURNISH AND PLACE, 4"	SQ YD	3375	3,375			
25000115	SEEDING, CLASS 1B	ACRE	0.75	0.75			
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	63	63			
25000500	PHOSPHORUS FERTILIZER NUTRIENT	POUND	63	63			
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	63	63			
25100630	EROSION CONTROL BLANKET	SQ YD	3380	3,380			
28000250	TEMPORARY EROSION CONTROL SEEDING	POUND	200	200			
28000305	TEMPORARY DITCH CHECKS	FOOT	24	24			
28000400	PERIMETER EROSION BARRIER	FOOT	1595	1,595			
28000500	INLET AND PIPE PROTECTION	EACH	6	6			
28000510	INLET FILTERS	EACH	10	10			
28100105	STONE RIPRAP, CLASS A3	SQ YD	55	55			
28200200	FILTER FABRIC	SQ YD	65	65			
30300112	AGGREGATE SUBGRADE IMPROVEMENT 12"	SQ YD	4210	4,210			
31101200	SUBBASE GRANULAR MATERIAL, TYPE B 4"	SQ YD	1265	1,265			
35102000	AGGREGATE BASE COURSE, TYPE B 8"	SQ YD	75	75			
35501316	HOT-MIX ASPHALT BASE COURSE, 8"	SQ YD	775	775			
40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	263	263			
40600100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	1355	1,355			
40600895	CONSTRUCTING TEST STRIP	EACH	2	2			
40603335	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	155	155			
40701881	HOT-MIX ASPHALT PAVEMENT (FULL-DEPTH), 10"	SQ YD	3770	3,770			
42300200	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 6 INCH	SQ YD	1035	1,035			
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	1035	1,035			
42400800	DETECTABLE WARNINGS	SQ FT	60	60			
44000100	PAVEMENT REMOVAL	SQ YD	2715	2,715			
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	1045	1,045			
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	360	360			
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SQ YD	100	100			
50300225	CONCRETE STRUCTURES	CU YD	23.2	23.2			
50300285	FORM LINER TEXTURED SURFACE	SQ FT	770.0	770.0			
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	1740.0	1,740.0			
54213657	PRECAST REINFORCED CONCRETE FLARED END SECTIONS 12"	EACH	1	1			
550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	456	456			
550A0090	STORM SEWERS, CLASS A, TYPE 1 18"	FOOT	183	183			
56109424	DUCTILE IRON WATER MAIN FITTINGS 12" 45.00 DEGREE BEND	EACH	2	2			
56101158	DUCTILE IRON WATER MAIN REDUCER, 12" X 6"	EACH	1	1			
56100065	DUCTILE IRON WATER MAIN TEE, 12" X 12"	EACH	1	1			
56105200	WATER VALVES 12"	EACH	3	3			
56400500	FIRE HYDRANTS TO BE REMOVED	EACH	1	1			
56400820	FIRE HYDRANT WITH AUXILIARY VALVE AND VALVE BOX	EACH	1	1			
60107600	PIPE UNDERDRAINS 4"	FOOT	416	416			
60200805	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 8 GRATE	EACH	3	3			
60201105	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 11 FRAME AND GRATE	EACH	6	6			
60218400	MANHOLES, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	2	2			
60236200	INLETS, TYPE A, TYPE 8 GRATE	EACH	2	2			

PAYCODE	ITEM DESCRIPTION	UNIT	TOTAL	ROADWAY 80%FEDERAL 20%STATE 0004	SIGNALS 80%FEDERAL 20%STATE 0021	LIGHTING 80%FEDERAL 20%STATE 0021	SANITARY 100% LOCAL 0043
60236800	INLETS, TYPE A, TYPE 11 FRAME AND GRATE	EACH	3	3			
60250200	CATCH BASINS TO BE ADJUSTED	EACH	1	1			
60249010	VALVE VAULTS, TYPE A, 6'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	3	3			
60600095	CLASS SI CONCRETE (OUTLET)	CU YD	6.0	6.0			
60600605	CONCRETE CURB, TYPE B	FOOT	230	230			
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	1605	1,605			
60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	140	140			
66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	650	650			
66900450	SPECIAL WASTE PLANS AND REPORT	L SUM	1	1			
66900530	SOIL DISPOSAL ANALYSIS	EACH	2	2			
67100100	MOBILIZATION	L SUM	1	1			
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	18	18			
70300100	SHORT TERM PAVEMENT MARKING	FOOT	2650	2,650			
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	10000	10,000			
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	60	60			
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	4340	4,340			
72000100	SIGN PANEL - TYPE 1	SQ FT	40	25	15		
72000200	SIGN PANEL - TYPE 2	SQ FT	27		27		
72900100	METAL POST - TYPE A	FOOT	44	44			
78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	240	240			
78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	4180	4,180			
78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	910	910			
78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	175	175			
78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	105	105			
78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	80	80			
78300100	PAVEMENT MARKING REMOVAL	SQ FT	2340	2,340			
80400100	ELECTRIC SERVICE INSTALLATION	EACH	1			1	
80400200	ELECTRIC UTILITY SERVICE CONNECTION	L SUM	1			1	
80500020	SERVICE INSTALLATION-POLE MOUNTED	EACH	2		1	1	
81028200	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	885		730	155	
81028210	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.	FOOT	45		45		
81028220	UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	85		85		
81028240	UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	407		407		
81028350	UNDERGROUND CONDUIT, PVC, 2" DIA.	FOOT	15			15	
81400100	HANDHOLE	EACH	2		2		
81400200	HEAVY-DUTY HANDHOLE	EACH	4		4		
81400300	DOUBLE HANDHOLE	EACH	2		2		
81603055	UNIT DUCT, 600V, 3-1C NO.8, 1/C NO.8 GROUND, (XLP-TYPE USE), 1" DIA. POLYETHYLENE	FOOT	961			961	
81702400	ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 3-1/C NO. 2	FOOT	15			15	
82500350	LIGHTING CONTROLLER, BASE MOUNTED, 240VOLT, 100AMP	EACH	1			1	
83007500	LIGHT POLE, ALUMINUM, 35 FT. M.H., 12 FT. MAST ARM	EACH	6			6	
83600352	LIGHT POLE FOUNDATION, METAL, 11 1/2" BOLT CIRCLE, 8 5/8" X 6"	EACH	6			6	
83800105	BREAKAWAY DEVICE, TRANSFORMER BASE, 11.5 INCH BOLT CIRCLE	EACH	6			6	
85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1			1	
85100500	PAINT NEW TRAFFIC SIGNAL POST	EACH	3			3	
85100600	PAINT NEW MAST ARM AND POLE, UNDER 40 FOOT	EACH	1			1	
85100701	PAINT NEW MAST ARM AND POLE, 40 FOOT AND OVER	EACH	3			3	
86400100	TRANSCEIVER - FIBER OPTIC	EACH	1			1	
87300925	ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1C	FOOT	859			859	

† SPECIALTY ITEM  
\* SPECIAL PROVISION

COMPANY NAME: HRGreen.com  
PROJECT CONTACT: HRGreen.com  
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: NTS	SHEET NO. 1 OF 2 SHEETS	STA. TO STA.	F.A.U. RTE. 4058	SECTION 13-00063-00-PV	COUNTY MCHENRY	TOTAL SHEETS 79	SHEET NO. 4
						CONTRACT NO. 63886	
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT							



PAYCODE	ITEM DESCRIPTION	UNIT	TOTAL	ROADWAY 80%FEDERAL 20%STATE 0004	SIGNALS 80%FEDERAL 20%STATE 0021	LIGHTING 80%FEDERAL 20%STATE 0021	SANITARY 100% LOCAL 0043
+ 87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	2,191		2,191		
+ 87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	1,387		1,387		
+ 87301245	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	2,026		2,026		
+ 87301255	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	885		885		
+ 87301805	ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C	FOOT	71		71		
+ 87301900	ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTUOR, NO. 6 1C	FOOT	982		982		
+ 87502480	TRAFFIC SIGNAL POST, GALVANIZED STEEL 14 FT.	EACH	1		1		
+ 87502500	TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	2		2		
+ 87700160	STEEL MAST ARM ASSEMBLY AND POLE, 24 FT.	EACH	1		1		
+ 87700240	STEEL MAST ARM ASSEMBLY AND POLE, 40 FT.	EACH	1		1		
+ 87700250	STEEL MAST ARM ASSEMBLY AND POLE, 42 FT.	EACH	1		1		
+ 87700400	STEEL MAST ARM ASSEMBLY AND POLE, 60 FT.	EACH	1		1		
+ 87800100	CONCRETE FOUNDATION, TYPE A	FOOT	12.0		12.0		
+ 87800150	CONCRETE FOUNDATION, TYPE C	FOOT	4.0		4.0		
+ 87800400	CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	10.0		10.0		
+ 87800415	CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	26.0		26.0		
+ 87800420	CONCRETE FOUNDATION, TYPE E 42-INCH DIAMETER	FOOT	21.0		21.0		
+ 88030020	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	7		7		
+ 88030050	SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	1		1		
+ 88030100	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	2		2		
+ 88030110	SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	3		3		
+ 88030240	SIGNAL HEAD, LED, 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED	EACH	1		1		
+ 88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	4		4		
+ 88200110	TRAFFIC SIGNAL BACKPLATE, LOUVERED	EACH	10		10		
+ 88500100	INDUCTIVE LOOP DETECTOR	EACH	7		7		
+ 88600100	DETECTOR LOOP, TYPE I	FOOT	633.0		633.0		
+ 88700200	LIGHT DETECTOR	EACH	2		2		
+ 88700300	LIGHT DETECTOR AMPLIFIER	EACH	1		1		
+ 88800100	PEDESTRIAN PUSH-BUTTON	EACH	4		4		
+ 89000100	TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1		1		
+ 89502300	REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	7193.5		7,193.5		
+ 89502375	REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1		1		
+ 89502380	REMOVE EXISTING HANDHOLE	EACH	7		7		
+ 89502382	REMOVE EXISTING DOUBLE HANDHOLE	EACH	1		1		
+ 89502385	REMOVE EXISTING CONCRETE FOUNDATION	EACH	8		8		
Z0013798	CONSTRUCTION LAYOUT	L SUM	1	1			
• Z0018700	DRAINAGE STRUCTURE TO BE REMOVED	EACH	1	1			
• Z0019500	DRYWELL	EACH	1	1			
• Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	113	113			
+ Z0033044	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1		1		
+ Z0058300	SANITARY SEWER, TYPE 1, 8"	FOOT	155				155
+ Z0058500	SANITARY SEWER, TYPE 1, 12"	FOOT	162				162
+ Z0062456	TEMPORARY PAVEMENT	SQ YD	790	790			
• Z0073510	TEMPORARY TRAFFIC SIGNAL TIMING	EACH	1		1		
Δ Z0076600	TRAINEES	HOUR	500	500			
Δ Z0076604	TRAINEES TRAINING PROGRAM GRADUATE	HOUR	500	500			
+ X0324085	EMERGENCY VEHICLE PRIORITY SYSTEM SENSOR CABLE, NO. 20 3/C	FOOT	588.5		588.5		
+ X0325003	REMOVE EXISTING VALVE AND VAULT	EACH	1	1			
+ X0327367	STEEL CASING PIPE, BORED AND JACKED, 24"	FOOT	123				123
• X4022000	TEMPORARY ACCESS (COMMERCIAL ENTRANCE)	EACH	10	10			
• X4023000	TEMPORARY ACCESS (ROAD)	EACH	4	4			
X5121800	PERMANENT STEEL SHEET PILING	SQ FT	2002	2,002			

PAYCODE	ITEM DESCRIPTION	UNIT	TOTAL	ROADWAY 80%FEDERAL 20%STATE 0004	SIGNALS 80%FEDERAL 20%STATE 0021	LIGHTING 80%FEDERAL 20%STATE 0021	SANITARY 100% LOCAL 0043
+ X5610712	WATER MAIN REMOVAL, 12"	FOOT	100	100			
+ X5611106	DUCTILE IRON WATER MAIN, CLASS 52 WITH POLYETHYLENE ENCASEMENT, 6"	FOOT	10	10			
+ X5611112	DUCTILE IRON WATER MAIN, CLASS 52 WITH POLYETHYLENE ENCASEMENT, 12"	FOOT	144	144			
• X6020094	MANHOLES, TYPE A, 6'-DIAMETER, TYPE 1 FRAME, CLOSED LID, RESTRICTOR PLATE	EACH	1	1			
+ X6022810	MANHOLES, SANITARY, 4'-DIAMETER, TYPE 1 FRAME, CLOSED LID	EACH	5				5
• X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	L SUM	1	1			
+ X8570226	FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL	EACH	1		1		
+ X8620200	UNINTERRUPTIBLE POWER SUPPLY, SPECIAL	EACH	1		1		
+ X8710024	FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM24F	FOOT	871.5		871.5		
+ XX007797	LUMINAIRE (SPECIAL)	EACH	6			6	

+ SPECIALTY ITEM  
• SPECIAL PROVISION  
XCONSTRUCTION TYPE CODE 0042

Δ 0042

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	5
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	

SCALE: NTS SHEET NO. 2 OF 2 SHEETS STA. TO STA.

CONTRACT NO. 63886

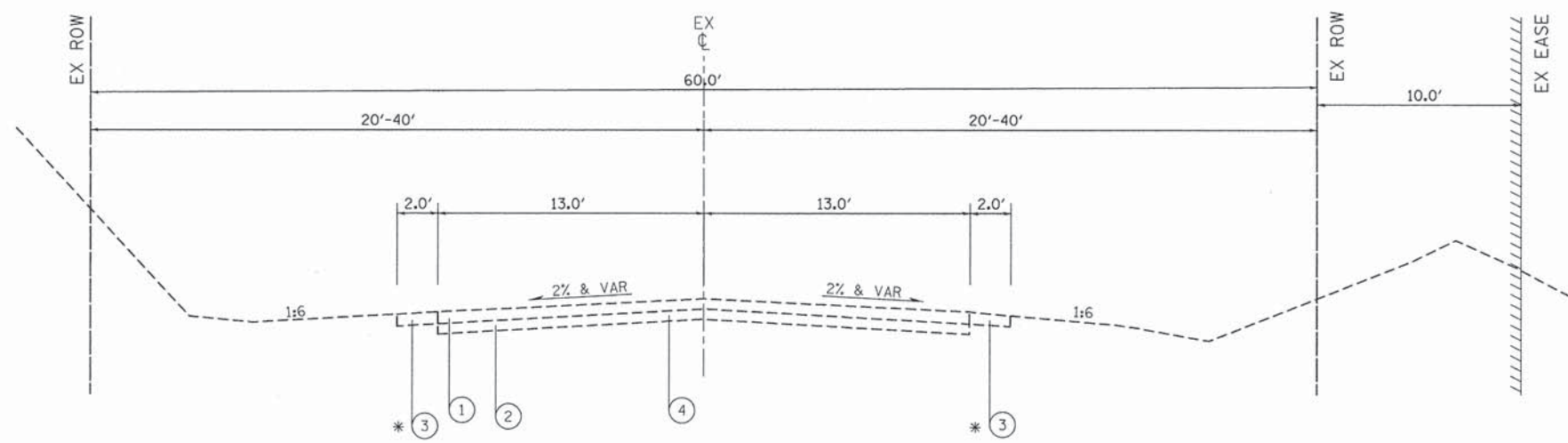
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DATE - 1/13/2014

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**EXISTING TYPICAL SECTION LEGEND**

- ① HOT-MIX ASPHALT, THICKNESS VARIES 6" TO 8"
- ② AGGREGATE SUB BASE, 6"
- ③ AGGREGATE SHOULDER
- ④ PAVEMENT REMOVAL

**NOTE**  
SEE REMOVAL PLAN FOR ADDITIONAL INFORMATION

**EXISTING TYPICAL SECTION**  
**JANDUS CUTOFF ROAD**  
STA 101+09 TO STA 109+94

\* EX CURB & GUTTER STA 108+38 TO 109+54 LT  
STA 108+37 TO 109+75 RT

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**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**EXISTING TYPICAL SECTIONS**

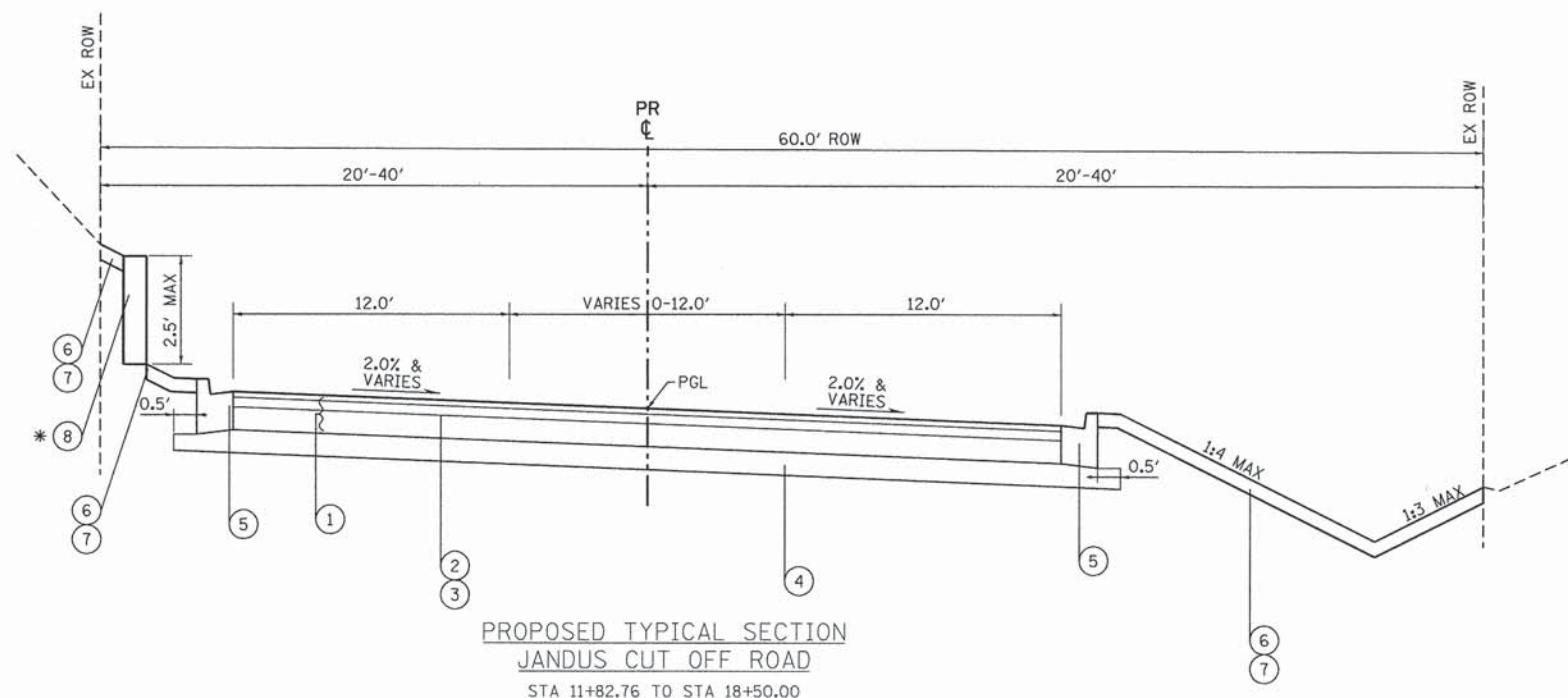
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	6
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 63886	



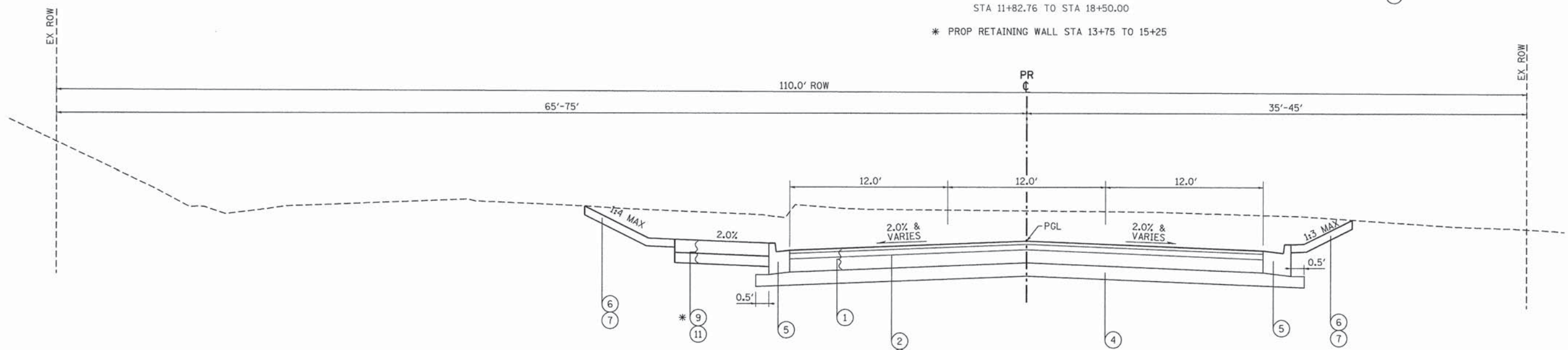
**PROPOSED TYPICAL SECTION LEGEND**

- ① HOT-MIX ASPHALT PAVEMENT (FULL DEPTH), 10"
- ② BITUMINOUS MATERIALS (PRIME COAT)
- ③ AGGREGATE (PRIME COAT)
- ④ AGGREGATE SUBGRADE IMPROVEMENT, 12"
- ⑤ COMB CONC C&G, TYPE B-6.12
- ⑥ TOPSOIL FURNISH AND PLACE, 4"
- ⑦ SEEDING, CLASS 1B
- ⑧ CONCRETE CAST-IN-PLACE RETAINING WALL
- ⑨ PC CONC SIDEWALK 5
- ⑩ OMIT
- ⑪ SUB-BASE GRANULAR MATERIAL, TYPE B 4"



**PROPOSED TYPICAL SECTION  
JANDUS CUT OFF ROAD  
STA 11+82.76 TO STA 18+50.00**

\* PROP RETAINING WALL STA 13+75 TO 15+25



**PROPOSED TYPICAL SECTION  
JANDUS CUT OFF ROAD  
STA 18+50.00 TO STA 20+79.80**

\* PROP SIDEWALK STA 18+50 TO 20+46

NOTE: THE CONTRACTOR SHALL MILL BEFORE PATCHING (VERIFY FROM US14 AS-BUILTS)

HOT-MIX ASPHALT MIXTURE REQUIREMENTS	
MIXTURE TYPE	AIR VOIDS @ Ndes
DRIVEWAYS	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm); 2"	4% @ 50 GYR
HMA BASE COURSE, (HMA BINDER IL-19mm); CE 8" (IN 3 LIFTS)	4% @ 50 GYR
FULL-DEPTH HOT-MIX ASPHALT PAVEMENT, 10"	
JANDUS CUT OFF ROAD	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5 mm); 2"	4% @ 70 GYR
HOT-MIX ASPHALT BINDER COURSE, IL 19.0, N70; 2.25"	4% @ 70 GYR
HMA BASE COURSE, (HMA BINDER IL-19mm); 5.75" (IN 2 LIFTS)	4% @ 70 GYR
TEMPORARY PAVEMENT	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL 9.5 mm); 2"	4% @ 50 GYR
HOT-MIX ASPHALT BINDER COURSE, (HMA BINDER IL-19 mm) 6" (IN 2 LIFTS)	4% @ 50 GYR
PATCHING	
CLASS D PATCHES (HMA BINDER IL-19mm); 12" (IN 2 LIFTS)	4% @ 70 GYR

**JANDUS CUTOFF ROAD  
PAVEMENT CROSS SLOPE TABLE**

STATION	LEFT	RIGHT
11+82.76	-2.0%	-2.0%
13+30.00	-2.0%	-2.0%
13+80.00	0.0%	-2.0%
14+00.00	2.0%	-2.0%
17+45.00	2.0%	-2.0%
17+65.00	0.0%	-2.0%
18+15.00	-2.0%	-2.0%
20+52.77	3.5%	-3.5%

- NOTES:**
- DEPRESSED CURB SHALL BE USED AT ALL DRIVEWAYS.
  - LOCATIONS AND DEPTHS OF IMPROVED SUBGRADE AND FABRIC ARE ESTIMATED. ACTUAL DEPTHS AND LOCATIONS TO BE VERIFIED IN THE FIELD DURING CONSTRUCTION.
  - HOT-MIX ASPHALT SURFACE COURSE SHALL EXTEND 1/4" ABOVE EDGE OF CURB AND GUTTER.

NOTES:  
 1. THE UNIT WEIGHT TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.  
 2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

COMPANY NAME: HRGreen.com  
 PROJECT CONTACT: J. J. S.  
 CLIENT: ILLINOIS DEPARTMENT OF TRANSPORTATION  
 FILE NAME: 13-00063-00-PV-02.dgn  
 PLOT DRIVER: plot.dwt  
 PEN TABLE: standard-trans.tbl



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PLOT SCALE = NTS	DRAWN - DMS	REVISED -
PLOT DATE = 12/24/2013	CHECKED - TEH	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PROPOSED TYPICAL SECTIONS**

SCALE: NTS SHEET NO. 2 OF 2 SHEETS STA. TO STA.

F.A.U. RTE. 4058	SECTION 13-00063-00-PV	COUNTY MCHENRY	TOTAL SHEETS 79	SHEET NO. 7
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT		CONTRACT NO. 63886		



PJANDUS	STAGE 1 (CU YD)				STAGE 2 (CU YD)				STAGE 3 (CU YD)			
	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	EARTH EXCAVATION	EMBANKMENT	AGGREGATE SUBGRADE IMPROVEMENT	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	EARTH EXCAVATION	EMBANKMENT	AGGREGATE SUBGRADE IMPROVEMENT	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL	EARTH EXCAVATION	EMBANKMENT	AGGREGATE SUBGRADE IMPROVEMENT
12+00.00	0.0	0.0	0.0	0.0	0+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0
12+50.00	0.0	26.5	0.4	0.0	0.0	51.4	1.4	0.0	0.0	0.0	0.0	0.0
13+00.00	0.0	43.3	0.5	0.0	0.0	39.1	1.0	0.0	0.0	0.0	0.0	0.0
13+03.49	0.0	3.7	0.0	0.0	0.0	3.5	0.0	0.0	0.0	0.0	0.0	0.0
13+50.00	0.0	46.5	1.1	0.0	0.0	31.4	0.2	0.0	0.0	0.0	0.0	0.0
14+00.00	0.0	48.1	2.1	0.0	0.0	15.3	1.1	0.0	0.0	0.0	0.0	0.0
14+50.00	0.0	36.9	3.5	0.0	0.0	15.3	1.9	0.0	0.0	0.0	0.0	0.0
15+00.00	0.0	30.1	2.5	0.0	0.0	19.4	3.1	0.0	0.0	0.0	0.0	0.0
15+50.00	0.0	39.8	0.5	0.0	0.0	19.2	3.8	0.0	0.0	0.0	0.0	0.0
16+00.00	0.0	35.6	0.6	0.0	0.0	19.1	2.7	0.0	0.0	0.0	0.0	0.0
16+50.00	0.0	13.6	0.3	0.0	0.0	10.1	1.0	0.0	0.0	31.4	2.9	0.0
16+95.47	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	50.3	5.0	0.0
17+00.00	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	4.3	0.4	0.0
17+50.00	0.0	22.5	0.4	0.0	0.0	12.4	0.6	0.0	0.0	23.4	1.8	0.0
17+92.07	0.0	47.0	0.5	0.0	0.0	17.5	0.5	0.0	0.0	0.0	0.0	0.0
18+00.00	0.0	10.2	0.0	0.0	0.0	2.1	0.1	0.0	0.0	0.0	0.0	0.0
18+50.00	0.0	78.0	0.2	0.0	0.0	14.7	1.8	0.0	0.0	0.0	0.0	0.0
18+98.38	0.0	100.4	0.3	0.0	0.0	20.2	4.3	0.0	0.0	0.0	0.0	0.0
19+00.00	0.0	3.7	0.0	0.0	0.0	0.7	0.2	0.0	0.0	0.0	0.0	0.0
19+50.00	0.0	132.0	0.3	0.0	0.0	9.7	4.6	0.0	0.0	0.0	0.0	0.0
20+00.00	0.0	171.1	0.1	0.0	0.0	10.7	1.1	0.0	0.0	0.0	0.0	0.0
20+50.00	0.0	175.2	0.0	0.0	0.0	42.3	0.6	0.0	0.0	0.0	0.0	0.0
	0.0	1064.1	13.2	0.0	0.0	354.1	30.0	0.0	0.0	109.4	10.0	0.0

COMPANY NAME: HRGreen  
 PROJECT CONTACT: JJS  
 CLIENT: ILLINOIS DEPARTMENT OF TRANSPORTATION  
 DATE PLOTTED: 12/15/2013 9:06:13 AM  
 FILE NAME: 86120253-sch-01.dgn  
 PLOT DRIVER: pdfplot  
 PEN TABLE: standard-trans.tbl



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	DRAWN - DMS	REVISED -
PLOT SCALE = NTS	CHECKED - TEH	REVISED -
PLOT DATE = 12/15/2013	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**SCHEDULE OF QUANTITIES**

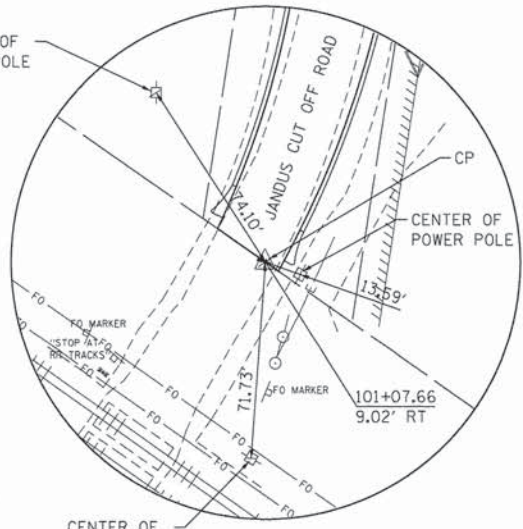
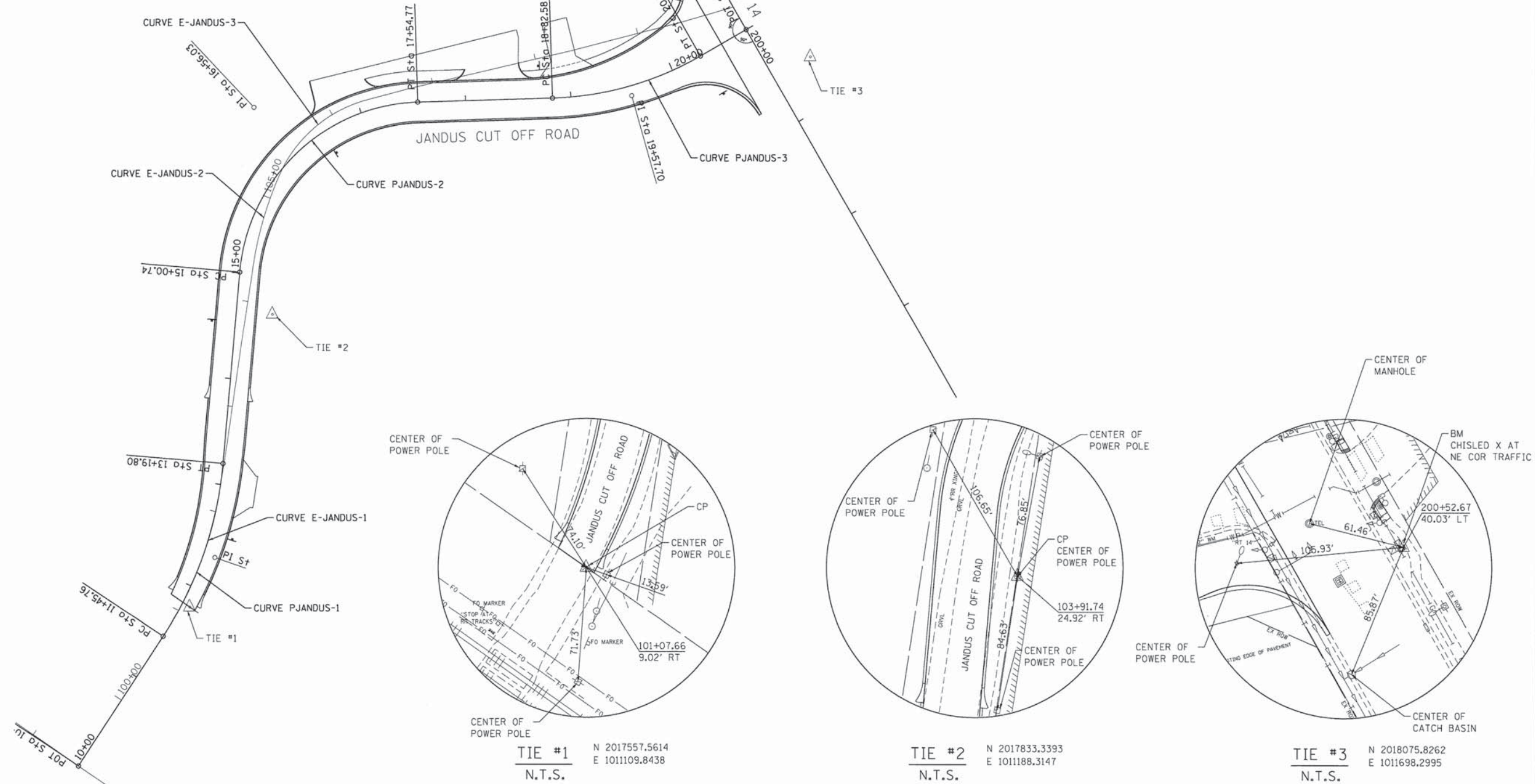
SCALE: NTS SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
405B	13-00063-00-PV	MCHENRY	79	8
CONTRACT NO.			63886	
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

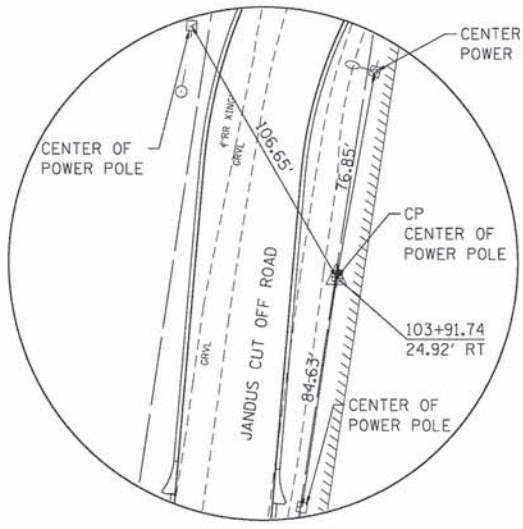


CURVE	PROPOSED			EXISTING		
	PJANDUS-1	PJANDUS-2	PJANDUS-3	E-JANDUS-1	E-JANDUS-2	E-JANDUS-3
P.I.	12+34.62	16+56.03	19+57.70	101+84.42	104+81.04	105+86.59
Δ	28° 29' 24" (LT)	83° 10' 19" (RT)	28° 07' 03" (LT)	11° 06' 23" (LT)	18° 14' 31" (RT)	45° 45' 19" (RT)
D	16° 22' 13"	32° 44' 26"	19° 05' 55"	13° 26' 42"	17° 28' 49"	44° 55' 32"
R	350.00'	175.00'	300.00'	426.15'	327.78'	127.54'
T	88.86'	155.30'	75.13'	41.43'	52.62'	53.81'
L	174.04'	254.03'	147.22'	82.60'	104.36'	101.85'
E	11.10'	58.97'	9.26'	2.01'	4.20'	10.89'
e	N.C.	2%	N.C.	N.C.	N.C.	N.C.
P.C.	11+45.76	15+00.74	18+82.58	101+42.99	104+28.42	105+32.78
P.T.	13+19.80	17+54.77	20+29.80	102+25.59	105+32.78	106+34.63

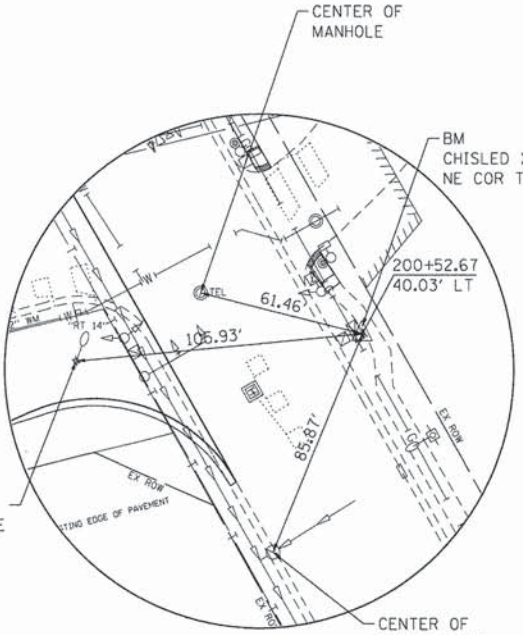
**BENCHMARKS**  
 CHISLED "X" ON THE CONTROL BOX PAD AT THE NORTHEAST CORNER OF ROUTE 14 AND JANDUS CUT OFF ROAD. ELEVATION=793.51  
 RAILROAD SPIKE IN THE SOUTH FACE OF THE POWER POLE LOCATED AT THE NORTHWEST CORNER OF ROUTE 14 AND JANDUS CUT OFF ROAD. ELEVATION=798.79  
 RAILROAD SPIKE IN THE NORTH FACE OF THE POWER POLE LOCATED ON THE SOUTH SIDE OF JANDUS CUT OFF ROAD AND THE THIRD POLE EAST OF THE RAILROAD TRACKS. ELEVATION=799.92



**TIE #1** N 2017557.5614  
 E 1011109.8438  
 N.T.S.



**TIE #2** N 2017833.3393  
 E 1011188.3147  
 N.T.S.



**TIE #3** N 2018075.8262  
 E 1011698.2995  
 N.T.S.

COMPANY NAME: HRGreen  
 PROJECT CONTACT: bhar-tme  
 DATE PLOTTED: 12/15/2013 9:06:47 AM  
 FILE NAME: 86120253-11a-01.dgn  
 PLOT DRIVER: pdfplot  
 PEN TABLE: standard-trans.tbl



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PLOT SCALE = NTS	DRAWN - DMS	REVISED -
PLOT DATE = 12/15/2013	CHECKED - TEH	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**ALIGNMENT, TIES, AND BENCHMARKS**

SCALE: NTS SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	9
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 63886	



**PRE-STAGE**

**CONSTRUCTION**

1. CONSTRUCT TEMPORARY PAVEMENT WIDENING AT LOCATIONS SHOWN ON THE STAGE 1 PLANS. THIS WORK SHALL BE COMPLETED UNDER TRAFFIC IN ACCORDANCE WITH STANDARD 701006-05.
2. CONSTRUCT THE STORM SEWER IMPROVEMENTS FROM THE EXISTING INLET/ NEW MANHOLE LOCATED AT STA 19+45 SOUTH TO STA 17+87.5. CONSTRUCT THE STORM SEWER MANHOLES AND CATCH BASINS AND CROSSROAD PIPES AT STA 17+85 AND STA 19+40. INSTALL TEMPORARY COVERS OVER STRUCTURES AND PIPES.
3. BACKFILL TRENCHES AND PROVIDE TEMPORARY PAVEMENT ACROSS DRIVEWAYS AS NECESSARY TO MAINTAIN ACCESS.
4. PROVIDE TEMPORARY SWALES/DITCHES TO MAINTAIN DRAINAGE AND PROVIDE EROSION CONTROL.

**MAINTENANCE OF TRAFFIC**

1. REMOVE OR OBLITERATE ALL CONFLICTING PAVEMENT MARKINGS.
2. MAINTAIN EXISTING TRAFFIC LANES ON JANDUS CUT OFF ROAD IN ACCORDANCE WITH STANDARD 701006-04 AND THE DETAILS SHOWN ON THE PLANS.

**STAGE 1**

**CONSTRUCTION**

1. RELOCATE JANDUS CUT OFF ROAD TRAFFIC TO THE SOUTH SIDE OF THE EXISTING ROADWAY AND TEMPORARY PAVEMENT INSTALLED IN PRESTAGE. (ONE LANE IN EACH DIRECTION).
2. SAWCUT AND REMOVE EXISTING PAVEMENT TO CONSTRUCT PORTIONS OF TRAVEL LANES BETWEEN STA 12+63.32 TO STA 16+44.17 (SOUTHBOUND) AND STA 17+22.48 TO STA 20+52.76 (NORTHBOUND).
3. CONSTRUCT RETAINING WALL FROM STA 13+75.00 TO STA 15+30.95.
4. CONSTRUCTION CURB AND GUTTER AND DRAINAGE STRUCTURES FROM STA 12+63.32 TO STA 16+44.17 (SOUTHBOUND) AND STA 17+22.48 TO STA 20+52.76 (NORTHBOUND). CONTRACTOR SHALL SCHEDULE CONSTRUCTION SUCH THAT BINDER COURSE IS COMPLETED WITHIN 14 DAYS OF CURB AND GUTTER CONSTRUCTION UNLESS ANOTHER TIME FRAME IS AUTHORIZED BY THE ENGINEER.
5. CONSTRUCT GUTTER OUTLETS AT STA 13+80.
6. CONSTRUCT PROPOSED STORM SEWER, WATER MAIN AND SANITARY SEWER MAINS WITHIN WORK ZONE AREA. (SEE DRAINAGE AND UTILITY PLANS FOR LOCATIONS). INSTALL TEMPORARY COVERS OVER STRUCTURES AND PIPES.
7. CONSTRUCT THE SOUTH LEG OF THE INTERSECTION OF THE PROPOSED RELOCATED JANDUS CUT OFF ROAD AND HIGHWAY 14. CONSTRUCTION SHALL BE STAGED SO THAT ACCESS IS MAINTAINED AT ALL TIMES AT THE EXISTING INTERSECTION OF HIGHWAY 14 AND JANDUS CUT OFF ROAD.

**MAINTENANCE OF TRAFFIC**

REMOVE OR OBLITERATE ALL CONFLICTING PAVEMENT MARKINGS.

1. MAINTAIN TRAFFIC LANES ON JANDUS CUT OFF ROAD IN ACCORDANCE WITH STANDARD 701006-04 AND THE DETAILS SHOWN ON THE PLANS.
2. INSTALL TEMPORARY TRAFFIC SIGNALS WITH COVERED HEADS.
3. MAINTAIN TRAFFIC LANES ON US ROUTE 14 OFF ROAD IN ACCORDANCE WITH STANDARD 701101-04 AND THE DETAILS SHOWN ON THE PLANS.

**STAGE 2**

**CONSTRUCTION**

1. SHIFT NORTHBOUND TRAFFIC ONTO PROPOSED NORTHBOUND TRAVEL LANE CONSTRUCTED IN STAGE 1.
2. SAWCUT AND REMOVE EXISTING PAVEMENT TO CONSTRUCT PORTIONS OF TRAVEL LANES BETWEEN STA 12+63.74 TO STA 16+44.17 AND 17+22.48 TO STA 20+08.69.
3. CONSTRUCT PROPOSED STORM SEWER, WATER MAIN AND SANITARY SEWER MAINS WITHIN WORK ZONE AREA. (SEE DRAINAGE AND UTILITY PLANS FOR LOCATIONS). INSTALL TEMPORARY COVERS OVER STRUCTURES AND PIPES.

**MAINTENANCE OF TRAFFIC**

1. ACTIVATE TEMPORARY TRAFFIC SIGNALS.
2. REMOVE OR OBLITERATE ALL CONFLICTING PAVEMENT MARKINGS.
3. MAINTAIN TRAFFIC LANES ON JANDUS CUT OFF ROAD IN ACCORDANCE WITH STANDARD 701502-05 AND THE DETAILS SHOWN ON THE PLANS.

**STAGE 3**

**CONSTRUCTION**

1. SHIFT NORTHBOUND TRAFFIC ONTO PROPOSED NORTHBOUND TRAVEL LANE CONSTRUCTED IN STAGE 2.
2. SHIFT SOUTHBOUND TRAFFIC ONTO PROPOSED SOUTHBOUND TRAVEL LANE CONSTRUCTED IN STAGE 2.
3. SAWCUT AND REMOVE EXISTING PAVEMENT TO CONSTRUCT PORTIONS OF TRAVEL LANES BETWEEN STA 11+82.76 TO STA 13+41.00 (NORTHBOUND) AND STA 16+44.17 TO STA 17+86.05 (SOUTHBOUND) AND STA 20+08.69 TO STA 20+52.76 (SOUTHBOUND).
4. CONSTRUCT PROPOSED STORM SEWER, WATER MAIN AND SANITARY SEWER MAINS WITHIN WORK ZONE AREA. (SEE DRAINAGE AND UTILITY PLANS FOR LOCATIONS). INSTALL TEMPORARY COVERS OVER STRUCTURES AND PIPES.
5. CONSTRUCTION CURB AND GUTTER AND DRAINAGE STRUCTURES FROM STA 12+63.82 TO STA 13+41.00 (NORTHBOUND) AND STA 16+44.17 TO STA 17+86.05 (SOUTHBOUND) AND STA 20+08.69 TO STA 20+52.76 (SOUTHBOUND). CONTRACTOR SHALL SCHEDULE CONSTRUCTION SUCH THAT BINDER COURSE IS COMPLETED WITHIN 14 DAYS OF CURB AND GUTTER CONSTRUCTION UNLESS ANOTHER TIME FRAME IS AUTHORIZED BY THE ENGINEER.
6. CONSTRUCT THE NORTH LEG OF THE INTERSECTION OF THE PROPOSED RELOCATED JANDUS CUT OFF ROAD AND HIGHWAY 14. CONSTRUCTION SHALL BE STAGED SO THAT ACCESS IS MAINTAINED AT ALL TIMES AT THE EXISTING INTERSECTION OF HIGHWAY 14 AND JANDUS CUT OFF ROAD.
7. REMOVE TEMPORARY PAVEMENT CONSTRUCTED IN PRESTAGE AND GRADE FINAL DITCH ON EAST SIDE OF ROAD AND PROVIDE EROSION CONTROL.

**MAINTENANCE OF TRAFFIC**

1. MODIFY TEMPORARY TRAFFIC SIGNALS WHERE APPLICABLE.
2. INSTALL PERMANENT TRAFFIC SIGNALS.
3. REMOVE OR OBLITERATE ALL CONFLICTING PAVEMENT MARKINGS.
4. MAINTAIN TRAFFIC LANES ON JANDUS CUT OFF ROAD IN ACCORDANCE WITH STANDARD 701006-04 AND THE DETAILS SHOWN ON THE PLANS.

**STAGE 4**

**CONSTRUCTION**

1. SHIFT SOUTHBOUND TRAFFIC ONTO PROPOSED SOUTHBOUND TRAVEL LANE CONSTRUCTED IN STAGE 3.
2. SAWCUT AND REMOVE EXISTING PAVEMENT TO CONSTRUCT TRAVEL LANES BETWEEN STA 15+85.77 TO STA 17+22.48 (NORTHBOUND).
3. CONSTRUCTION CURB AND GUTTER AND DRAINAGE STRUCTURES FROM STA 15+85.77 TO STA 17+22.48. CONTRACTOR SHALL SCHEDULE CONSTRUCTION SUCH THAT BINDER COURSE IS COMPLETED WITHIN 14 DAYS OF CURB AND GUTTER CONSTRUCTION UNLESS ANOTHER TIME FRAME IS AUTHORIZED BY THE ENGINEER.
4. SAWCUT AND REMOVE EXISTING PAVEMENT TO CONSTRUCT TRAVEL LANES AND CURB AND GUTTER BETWEEN STA 11+82.76 TO STA 12+63.82. CONTRACTOR SHALL SCHEDULE CONSTRUCTION SUCH THAT BINDER COURSE IS COMPLETED WITHIN 14 DAYS OF CURB AND GUTTER CONSTRUCTION UNLESS ANOTHER TIME FRAME IS AUTHORIZED BY THE ENGINEER.
5. CONSTRUCT REMAINDER OF STORM SEWER WITHIN DITCH AREAS.
6. INSTALL REMAINING STORM SEWER STRUCTURES AND PIPES AND GRADE FINAL DITCHES AND PROVIDE EROSION CONTROL AND FINAL LANDSCAPING.

**MAINTENANCE OF TRAFFIC**

1. RELOCATE JANDUS CUT OFF ROAD TRAFFIC TO PERMANENT LANE ASSIGNMENTS AND ACTIVATE PERMANENT TRAFFIC SIGNALS.
2. REMOVE OR OBLITERATE ALL CONFLICTING PAVEMENT MARKINGS.
3. MAINTAIN TRAFFIC LANES ON JANDUS CUT OFF ROAD IN ACCORDANCE WITH STANDARD 701006-04 AND THE DETAILS SHOWN ON THE PLANS.

**SUGGESTED MAINTENANCE OF TRAFFIC – SIGN LEGEND**

**KEEP RIGHT** R4-7A-2430  
**ROAD CONSTRUCTION AHEAD** W20-1(O)-48  
**END WORK ZONE SPEED LIMIT** G20-1103(O)-3624  
**LEFT LANE CLOSED AHEAD** W20-5L(O)-48  
**SPEED LIMIT 25** W3-5(O)-48  
**WORK ZONE SPEED LIMIT 35** W2-1-1115(O)-3618  
**PHOTO ENFORCED \$375 FINE MINIMUM** R2-1-3648  
**DRIVEWAY ENTRANCE** M6-1(O)-2115  
**STOP AT LINE** M6-3030 WHITE ON GREEN  
**18 x 6 WHITE ON RED**  
**WORK ZONE SPEED LIMIT 25** W2-1-1115(O)-3618  
**PHOTO ENFORCED \$375 FINE MINIMUM** R2-1-3648

**SUGGESTED MAINTENANCE OF TRAFFIC – GENERAL NOTES:**

1. THE CONTRACTOR SHALL FURNISH, PLACE, AND MAINTAIN THREE (3) CHANGEABLE MESSAGE SIGNS. EXACT SIGN LOCATIONS SHALL BE DETERMINED BY THE ENGINEER (SEE LOCATION MAP AT RIGHT FOR APPROXIMATE LOCATIONS). TWO WEEKS PRIOR TO THE CONTRACTOR BEGINNING ANY WORK THE SIGNS SHALL BE ACTIVATED AND NOTE THE DATE CONSTRUCTION BEGINS AND THAT DELAYS CAN BE EXPECTED. THE SIGN MESSAGES SHALL BE MODIFIED THROUGHOUT THE PROJECT TO ALERT THE TRAVELING PUBLIC WHEN SIGNIFICANT CHANGES OCCUR (LANE CLOSURES, NEW LANE CONFIGURATION, ETC.).
2. A MINIMUM OF ONE THROUGH LANE IN EACH DIRECTION SHALL REMAIN OPEN ON JANDUS CUT OFF ROAD AT ALL TIMES. TEMPORARY TRAFFIC LANES SHALL BE A MINIMUM OF 10' WIDE.
3. TEMPORARY RAMPS SHALL BE PROVIDED IN ACCORDANCE WITH SECTION 406 OF THE STANDARD SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.
4. ACCESS TO ALL PROPERTIES SHALL BE MAINTAINED AT ALL TIMES. ENTRANCE RECONSTRUCTION SHALL BE EITHER STAGED OR ALTERNATED WITH OTHER ENTRANCES PROVIDING ACCESS TO THE SAME PROPERTY. COMMERCIAL ENTRANCE CLOSURES RESULTING IN A COMPLETE LOSS OF ACCESS TO ANY BUSINESS WILL NOT BE PERMITTED AT ANY TIME. THE CONTRACTOR SHALL PROVIDE PROPERTY OWNERS WITH VERBAL AND WRITTEN NOTICE A MINIMUM OF 48 HOURS PRIOR TO BEGINNING WORK ON AN ENTRANCE. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT.
5. CONSTRUCTION ON OR ADJACENT TO AN ENTRANCE, RESULTING IN A TEMPORARY CLOSURE TO THAT ENTRANCE, SHALL BE EXPEDITED AND THE ENTRANCE REOPENED TO TRAFFIC AS SOON AS POSSIBLE. DURING THE TEMPORARY CLOSURE, THE CONTRACTOR SHALL PROVIDE TYPE III BARRICADES WITH "ROAD CLOSED" SIGNS AT THE LOCATIONS DESIGNATED BY THE ENGINEER. TEMPORARY CLOSURE AT ENTRANCES WILL NOT BE PERMITTED DURING THE WEEKEND.
6. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO PROVIDE ADEQUATE DRAINAGE DURING CONSTRUCTION. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE COST OF THE TRAFFIC CONTROL AND PROTECTION.
7. EXISTING OR PROPOSED TRAFFIC SIGNS CONFLICTING WITH THE TEMPORARY TRAFFIC CONTROL SHALL BE TEMPORARILY REMOVED OR COVERED AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE COST OF THE TRAFFIC CONTROL AND PROTECTION.

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED MAINTENANCE OF TRAFFIC – GENERAL NOTES  
JANDUS CUTOFF ROAD**

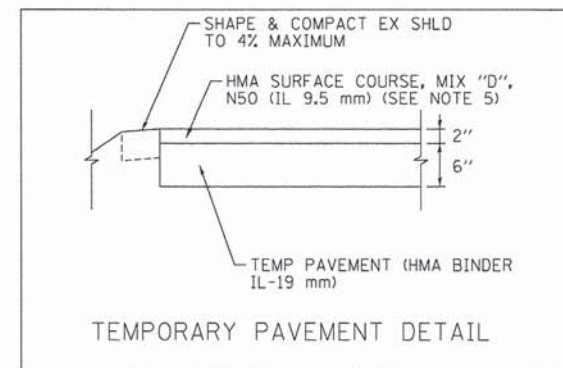
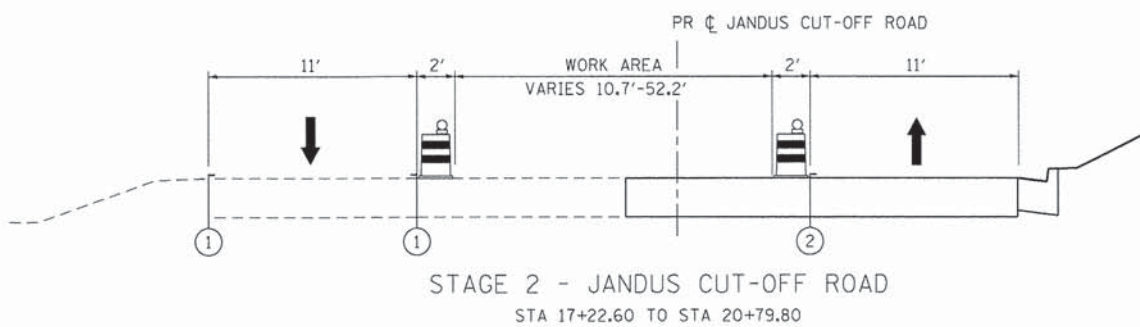
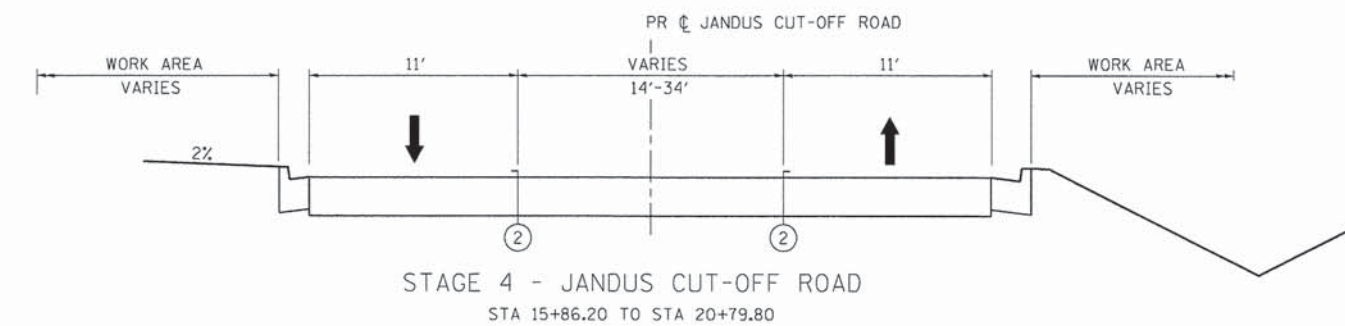
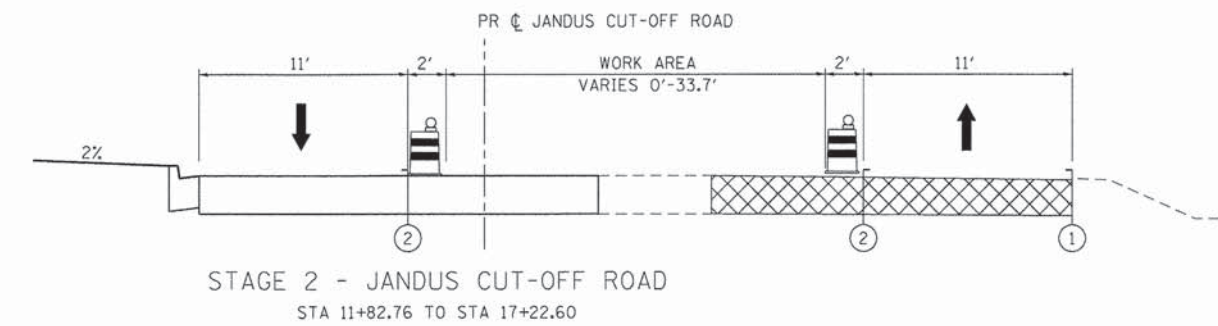
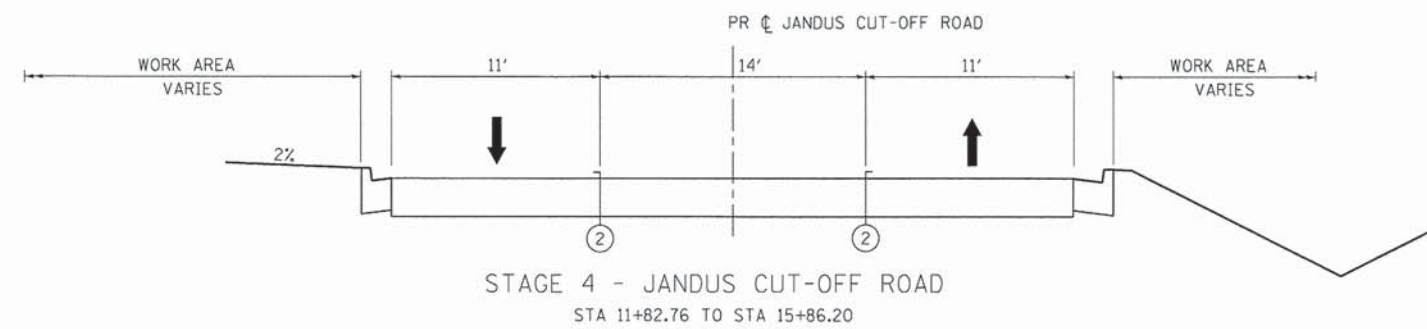
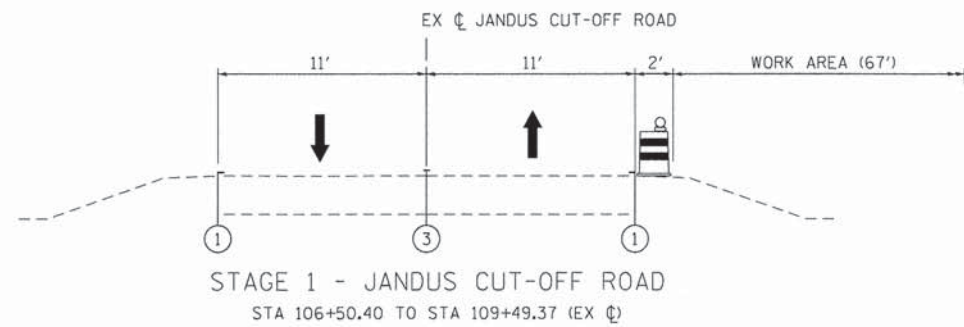
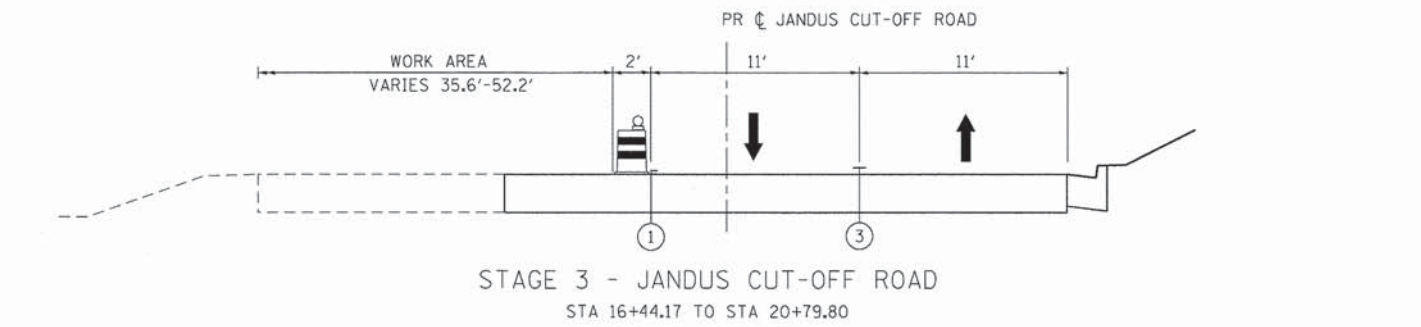
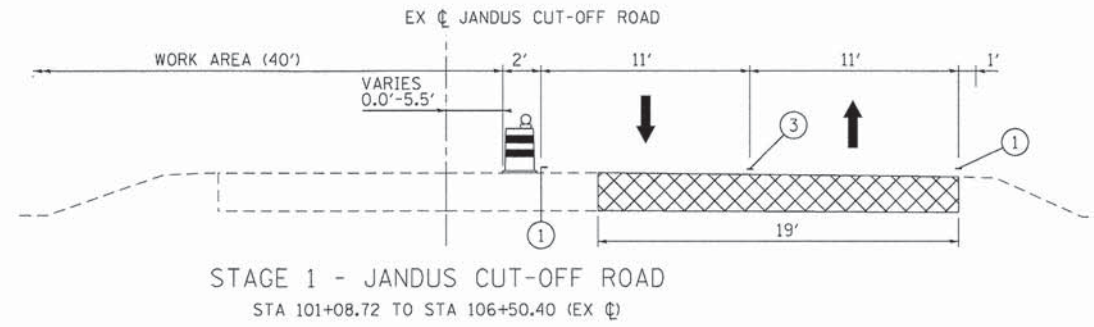
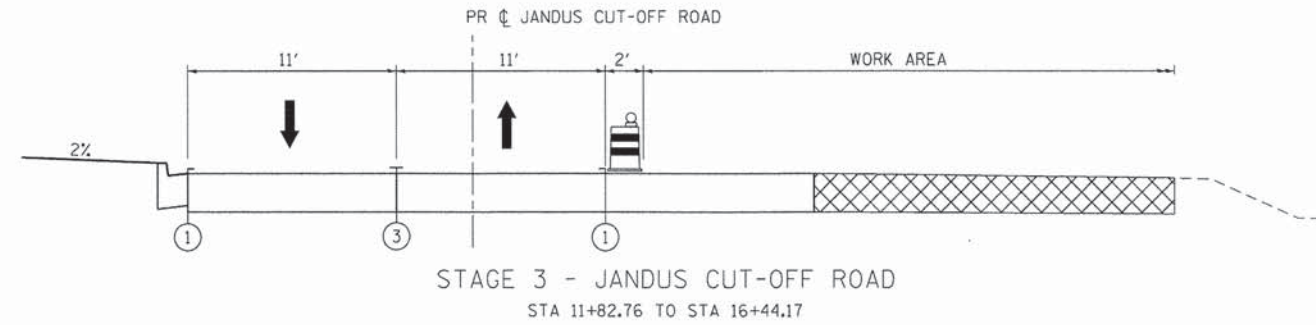
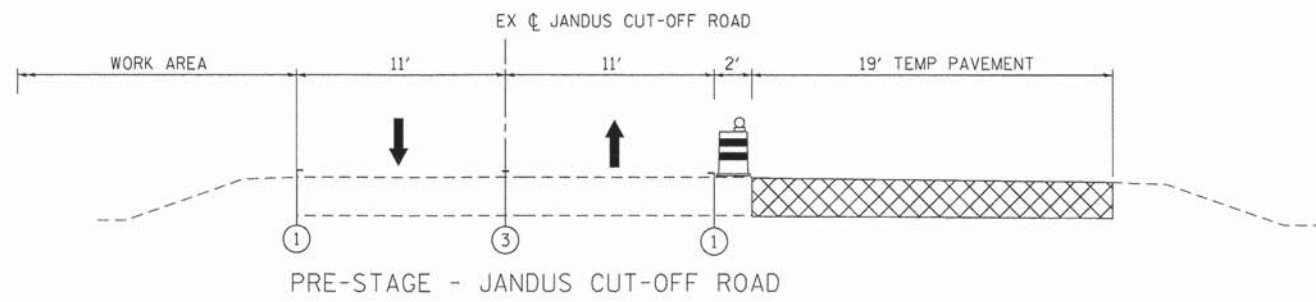
SCALE: NTS SHEET NO. 1 OF 7 SHEETS STA. TO STA.

F.A.U. RTEL.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	10
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	CONTRACT NO. 63886

HRGreen.com Illinois Professional Design Firm #184-001322	USER NAME = jme1hus	DESIGNED - JJS	REVISED -
HRGreen	PLOT SCALE = NTS	DRAWN - DMS	REVISED -
	PLOT DATE = 12/26/2013	CHECKED - TEH	REVISED -
		DATE -	REVISED -

COMPANY NAME: HRGreen.com  
 PROJECT CONTACT: Illinois Professional Design Firm  
 CLIENT: #184-001322  
 DATE PLOTTED: 12/26/2013 8:55:03 AM  
 FILE NAME: 86120253-mch-0601.dgn  
 PLOT NUMBER: 63886-10  
 PEN TABLE: 63886-10.tbl





- LEGEND**
- TEMPORARY PAVEMENT HMA BINDER IL-19MM, 8"
  - DIRECTION OF TRAFFIC
  - TYPE II BARRICADE OR DRUM WITH MONO-DIRECTIONAL STEADY BURNING LIGHT 50 FT C-C, 20 FT C-C ON TAPERS AND 12 FT ON RADII
  - TEMPORARY PAVEMENT MARKING - LINE 4" (WHITE)
  - TEMPORARY PAVEMENT MARKING - LINE 4" (YELLOW)
  - TEMPORARY PAVEMENT MARKING - LINE 4" (DBL YELLOW)

COMPANY NAME: HRGreen.com  
 PROJECT CONTACT: bhartha  
 CLIENT: HRGreen  
 DATE PLOTTED: 12/05/2013 8:06:48 AM  
 PLOT NUMBER: 8612023-mattyp-01.dgn  
 PEN TABLE: Standard-Trans.tbl



USER NAME = bhartha	DESIGNED - JJS	REVISED -
PLOT SCALE = 1"=50'	DRAWN - DMS	REVISED -
PLOT DATE = 12/05/2013	CHECKED - TEH	REVISED -
	DATE -	REVISED -

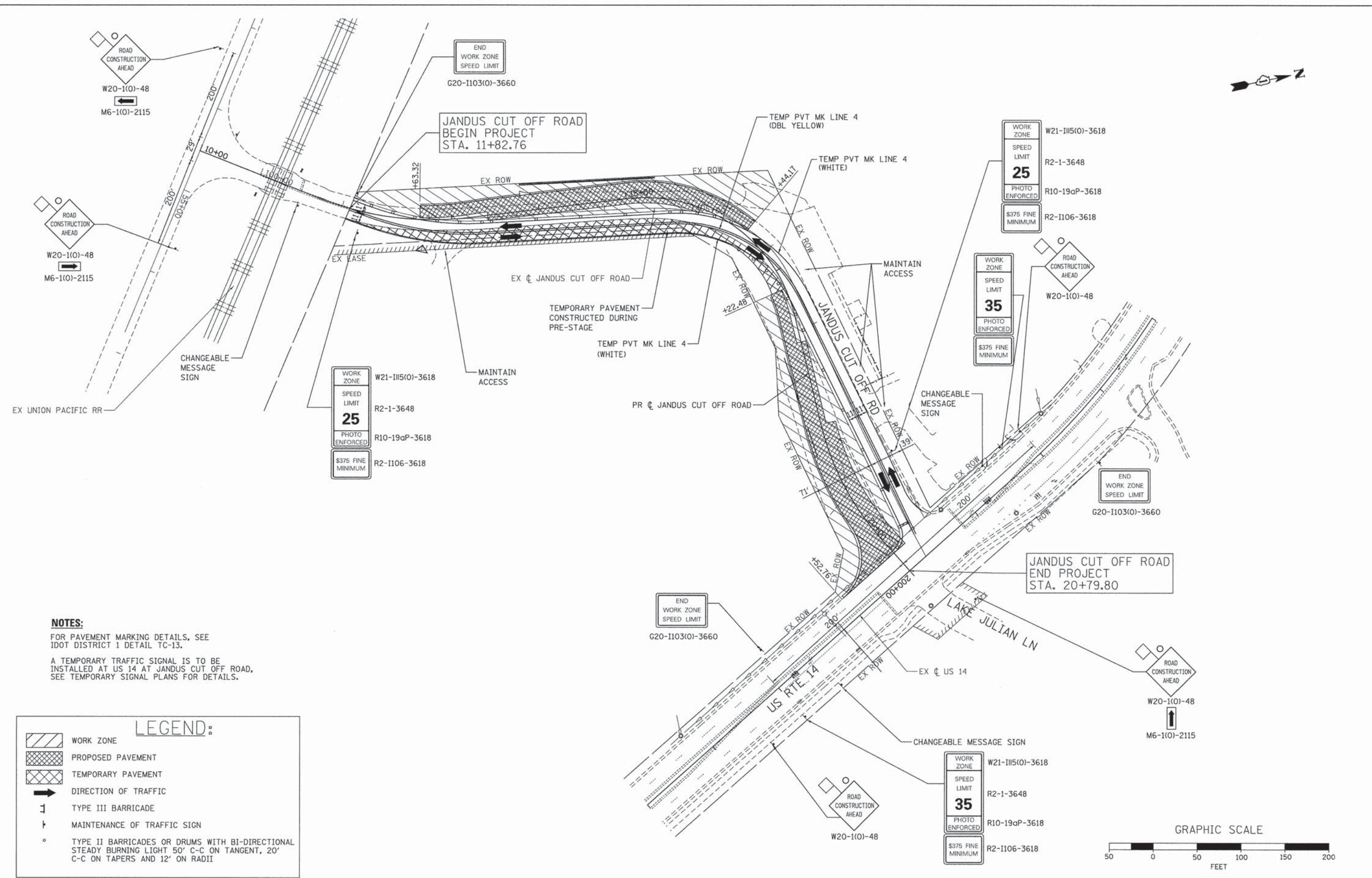
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED MAINTENANCE OF TRAFFIC - TYPICAL SECTIONS  
JANDUS CUTOFF ROAD**

SCALE: 1"=50' SHEET NO. 2 OF 7 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	11
CONTRACT NO. 63886				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				

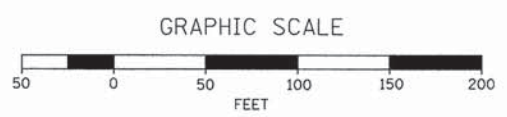




**NOTES:**  
 FOR PAVEMENT MARKING DETAILS, SEE IDOT DISTRICT 1 DETAIL TC-13.  
 A TEMPORARY TRAFFIC SIGNAL IS TO BE INSTALLED AT US 14 AT JANDUS CUT OFF ROAD, SEE TEMPORARY SIGNAL PLANS FOR DETAILS.

**LEGEND:**

- WORK ZONE
- PROPOSED PAVEMENT
- TEMPORARY PAVEMENT
- DIRECTION OF TRAFFIC
- TYPE III BARRICADE
- MAINTENANCE OF TRAFFIC SIGN
- TYPE II BARRICADES OR DRUMS WITH BI-DIRECTIONAL STEADY BURNING LIGHT 50' C-C ON TANGENT, 20' C-C ON TAPERS AND 12' ON RADII



12/24/2013 10:36:04 AM  
 6620253-mst1-01.dgn  
 pef,djt  
 s:randor@trans.tn



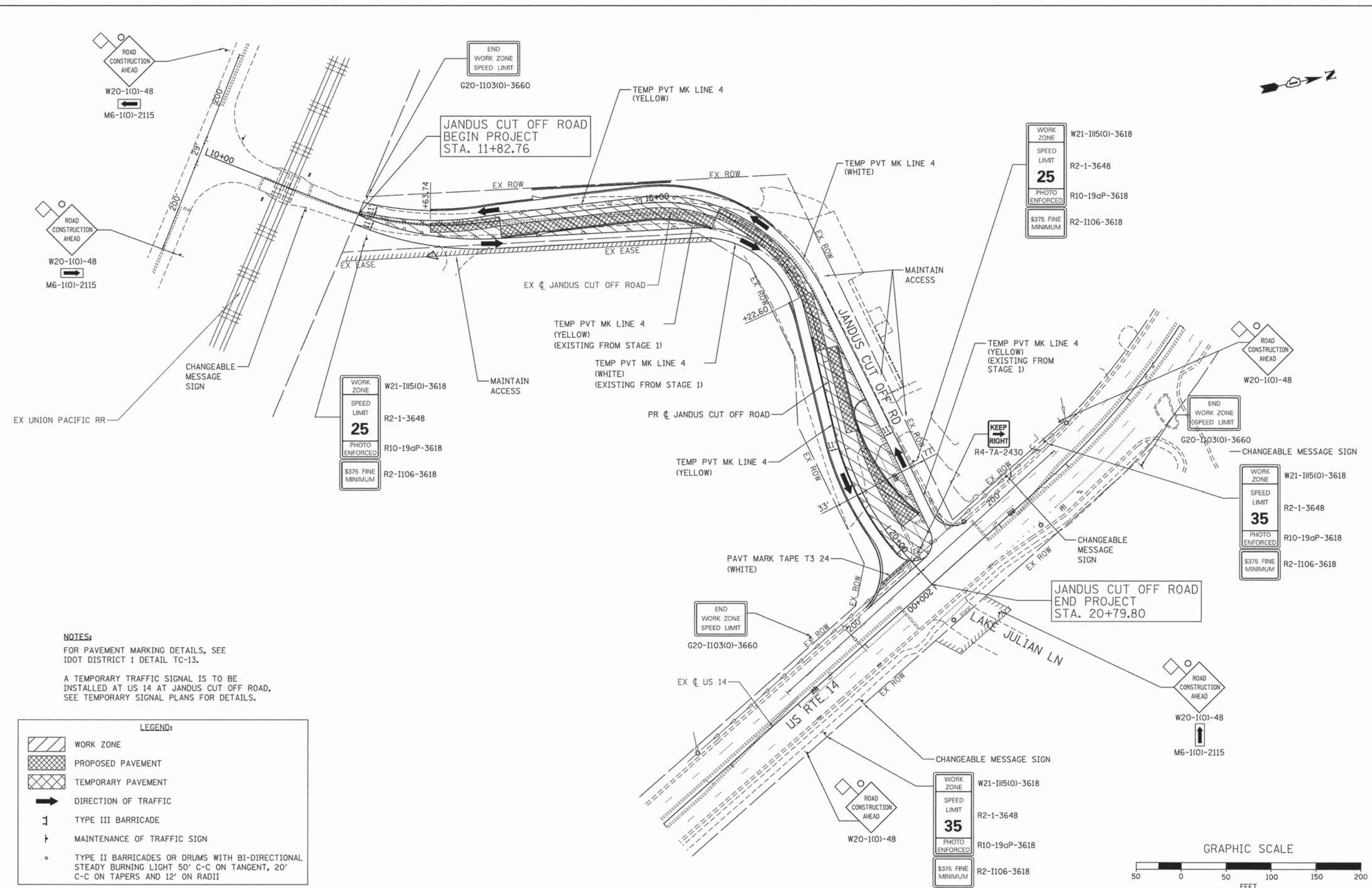
USER NAME = jmelhus	DESIGNED - JJS	REVISED -
DRAWN - DMS	REVISED -	
CHECKED - TEH	REVISED -	
DATE -	REVISED -	
PLOT SCALE = 1"=50'		
PLOT DATE = 12/24/2013		

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**SUGGESTED MAINTENANCE OF TRAFFIC - STAGE 1  
 JANDUS CUTOFF ROAD**  
 SCALE: 1"=50' SHEET NO. 4 OF 7 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	12
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 63886	

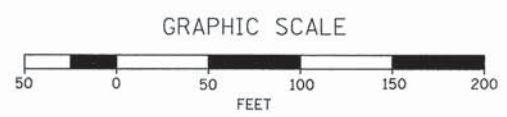




**NOTES:**  
 FOR PAVEMENT MARKING DETAILS, SEE IDOT DISTRICT 1 DETAIL TC-13.  
 A TEMPORARY TRAFFIC SIGNAL IS TO BE INSTALLED AT US 14 AT JANDUS CUT OFF ROAD, SEE TEMPORARY SIGNAL PLANS FOR DETAILS.

**LEGEND:**

- WORK ZONE
- PROPOSED PAVEMENT
- TEMPORARY PAVEMENT
- DIRECTION OF TRAFFIC
- TYPE III BARRICADE
- MAINTENANCE OF TRAFFIC SIGN
- TYPE II BARRICADES OR DRUMS WITH BI-DIRECTIONAL STEADY BURNING LIGHT 50' C-C ON TANGENT, 20' C-C ON TAPERS AND 12' ON RADII



COMPANY NAME: HRGreen.com  
 PROJECT CONTACT: jme@hrgreen.com  
 CLIENT: MCHENRY COUNTY  
 DATE PLOTTED: 12/24/2013 10:56:08 AM  
 PLOT DRIVER: jme@hrgreen.com  
 PEN TABLE: standard-trans.tbl  
 06/22/2013 10:56:08 AM  
 06/22/2013 10:56:08 AM

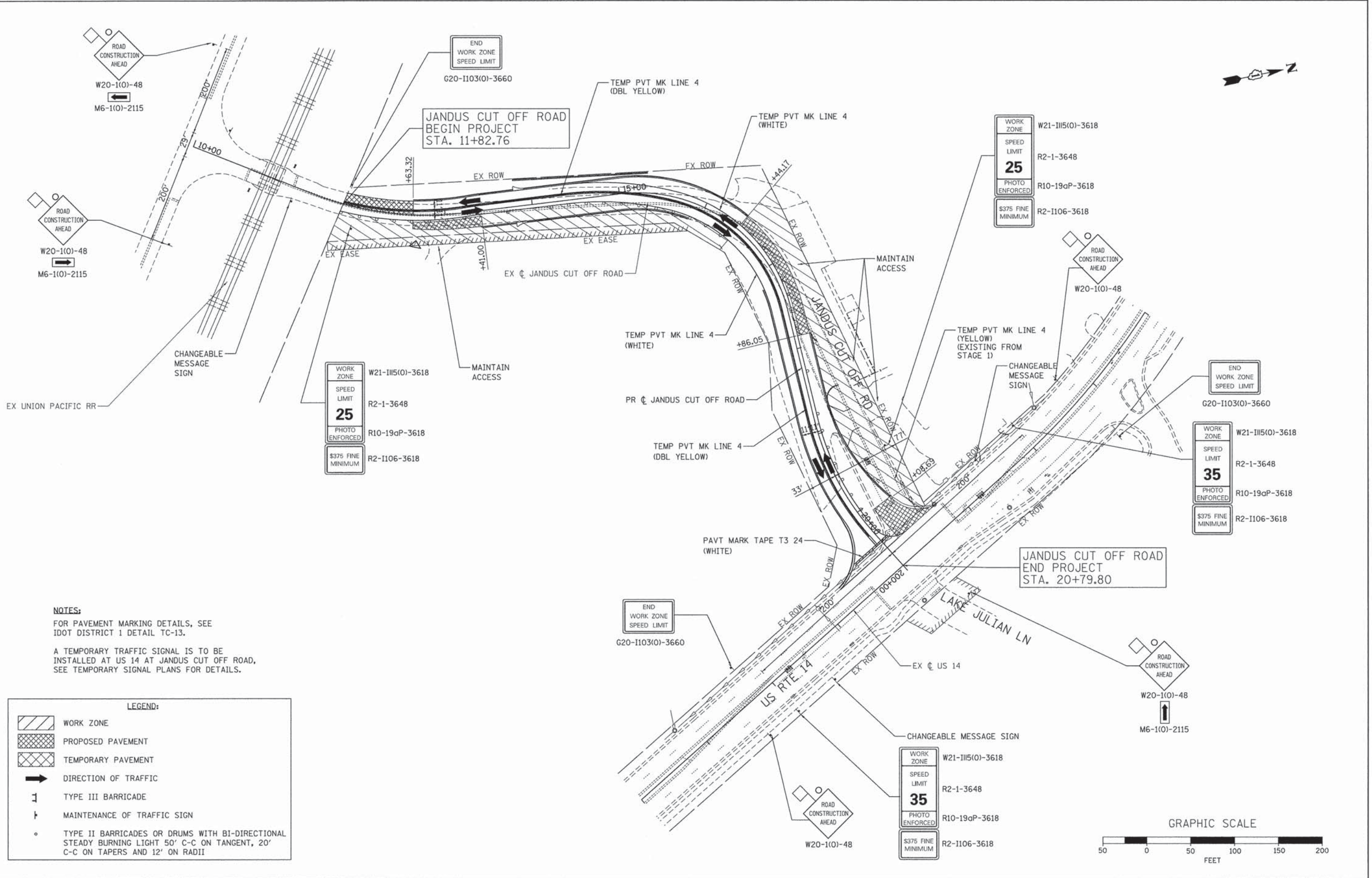
	USER NAME = jme@hrgreen.com DESIGNED - JJS DRAWN - DMS CHECKED - TEH DATE - 12/24/2013	DESIGNED - JJS DRAWN - DMS CHECKED - TEH DATE - 12/24/2013	REVISED - REVISED - REVISED - REVISED -
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**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**SUGGESTED MAINTENANCE OF TRAFFIC - STAGE 2**  
**JANDUS CUTOFF ROAD**  
 SCALE: 1"=50'    SHEET NO. 5 OF 7 SHEETS    STA.    TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	13
FED. ROAD DIST. NO.	ILLINOIS FED. AID PROJECT	CONTRACT NO. 63886		

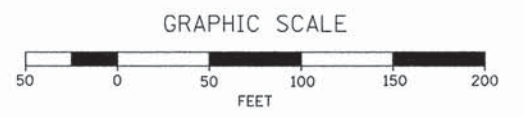




**NOTES:**  
 FOR PAVEMENT MARKING DETAILS, SEE IDOT DISTRICT 1 DETAIL TC-13.  
 A TEMPORARY TRAFFIC SIGNAL IS TO BE INSTALLED AT US 14 AT JANDUS CUT OFF ROAD, SEE TEMPORARY SIGNAL PLANS FOR DETAILS.

**LEGEND:**

- WORK ZONE
- PROPOSED PAVEMENT
- TEMPORARY PAVEMENT
- DIRECTION OF TRAFFIC
- TYPE III BARRICADE
- MAINTENANCE OF TRAFFIC SIGN
- TYPE II BARRICADES OR DRUMS WITH BI-DIRECTIONAL STEADY BURNING LIGHT 50' C-C ON TANGENT, 20' C-C ON TAPERS AND 12' ON RADII



COMPANY NAME: HRGreen.com  
 PROJECT CONTACT: jme1hus  
 CLIENT: HRGreen  
 DATE PLOTTED: 12/24/2013 10:35:12 AM  
 FILE NAME: 8620253-mo13-01.dgn  
 PLOT DRIVER: pctdpr  
 PLOT TABLE: standard-forms.tbl



USER NAME = jme1hus	DESIGNED - JJS	REVISED -
DRAWN - DMS	REVISOR -	
CHECKED - TEH	REVISOR -	
DATE -	REVISOR -	

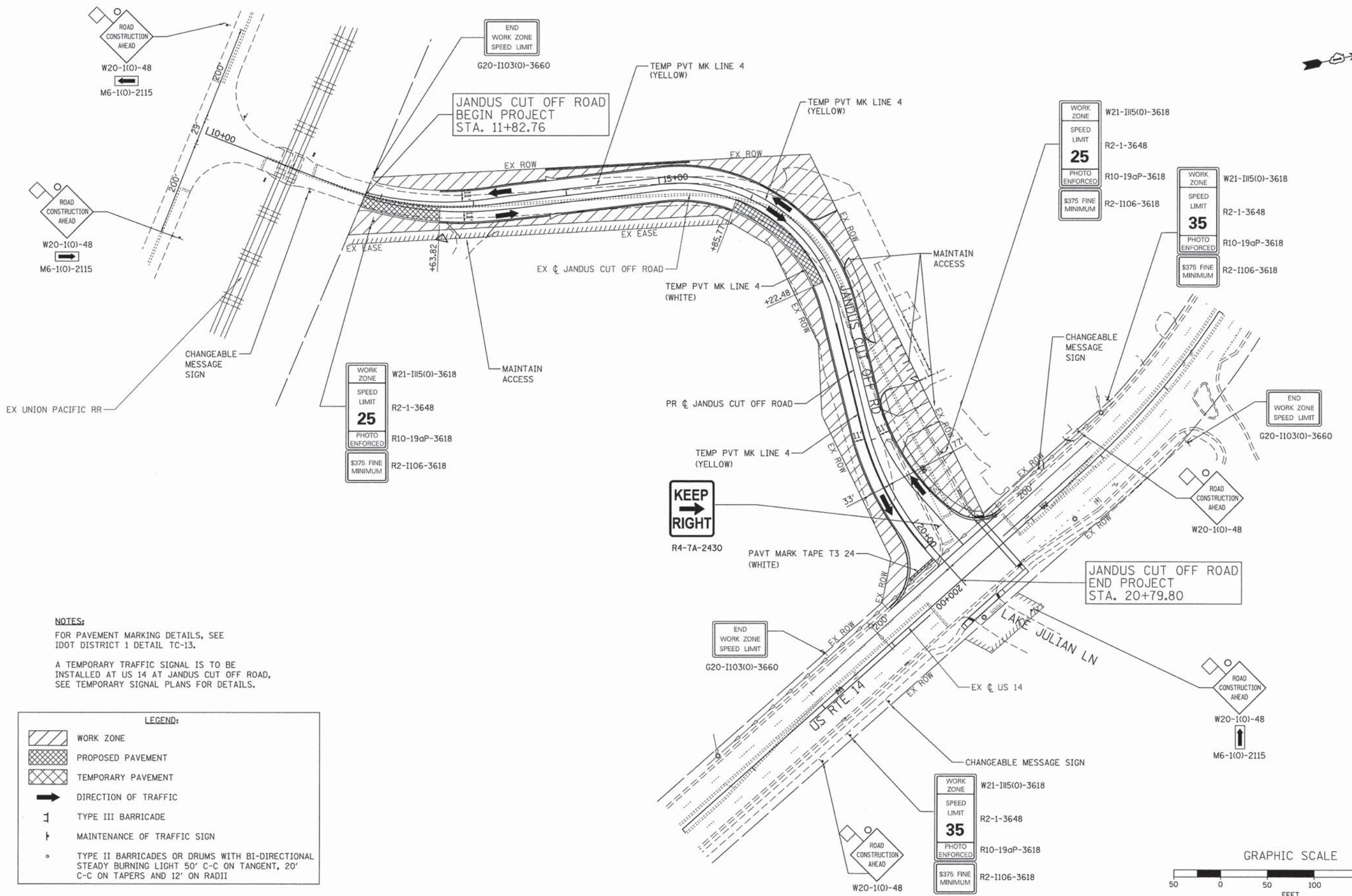
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**SUGGESTED MAINTENANCE OF TRAFFIC - STAGE 3  
 JANDUS CUTOFF ROAD**

SCALE: 1"=50' SHEET NO. 6 OF 7 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	14
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 63886	

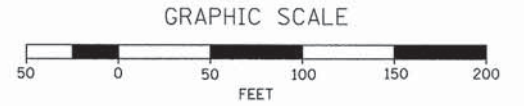




**NOTES:**  
 FOR PAVEMENT MARKING DETAILS, SEE IDOT DISTRICT 1 DETAIL TC-13.  
 A TEMPORARY TRAFFIC SIGNAL IS TO BE INSTALLED AT US 14 AT JANDUS CUT OFF ROAD, SEE TEMPORARY SIGNAL PLANS FOR DETAILS.

**LEGEND:**

	WORK ZONE
	PROPOSED PAVEMENT
	TEMPORARY PAVEMENT
	DIRECTION OF TRAFFIC
	TYPE III BARRICADE
	MAINTENANCE OF TRAFFIC SIGN
	TYPE II BARRICADES OR DRUMS WITH BI-DIRECTIONAL STEADY BURNING LIGHT 50' C-C ON TANGENT, 20' C-C ON TAPERS AND 12' ON RADII



COMPANY NAME: HRGreen  
 PROJECT CONTACT: J. J. Smith  
 DATE PLOTTED: 12/26/2013 8:57:19 AM  
 FILE NAME: 86200633-mo14-01.dgn  
 PLOT DATE: 12/26/2013  
 PEN TABLE: Standard-Trans.tbl

HRGreen.com Illinois Professional Design Firm # 134-001322	USER NAME = jsmith	DESIGNED - JJS	REVISED -
	PLOT SCALE = 1"=50'	DRAWN - DMS	REVISED -
	PLOT DATE = 12/26/2013	CHECKED - TEH	REVISED -
		DATE -	REVISED -

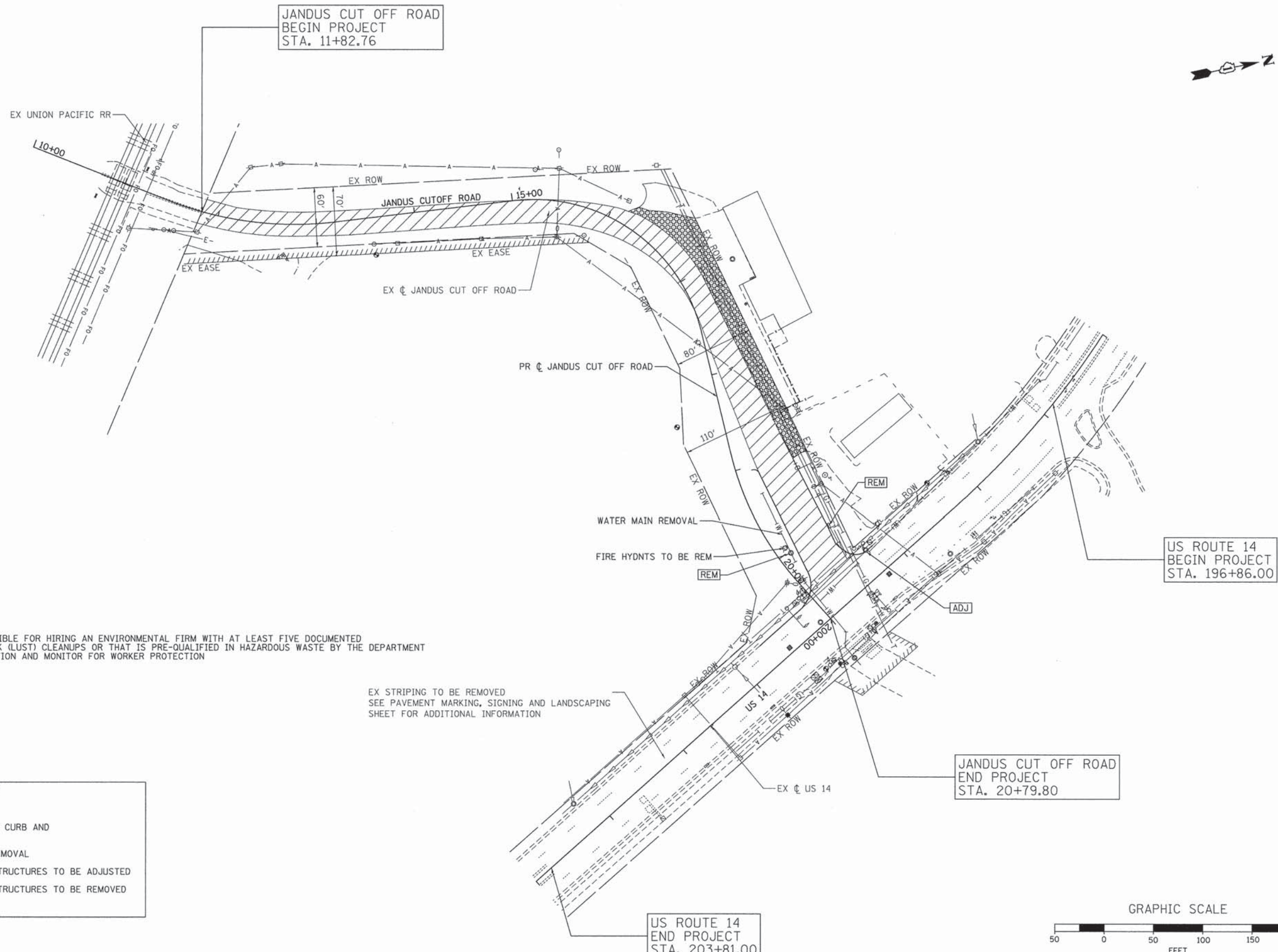
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**SUGGESTED MAINTENANCE OF TRAFFIC - STAGE 4  
 JANDUS CUTOFF ROAD**

SCALE: 1"=50' SHEET NO. 7 OF 7 SHEETS STA. TO STA.

F.A.U. RTE. 4058	SECTION 13-00063-00-PV	COUNTY MCHENRY	TOTAL SHEETS 79	SHEET NO. 15
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 63886	



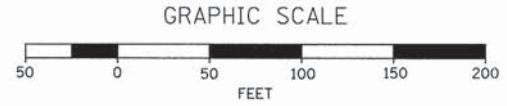


**NOTE:**  
 THE CONTRACTOR SHALL BE RESPONSIBLE FOR HIRING AN ENVIRONMENTAL FIRM WITH AT LEAST FIVE DOCUMENTED LEAKING UNDERGROUND STORAGE TANK (LUST) CLEANUPS OR THAT IS PRE-QUALIFIED IN HAZARDOUS WASTE BY THE DEPARTMENT TO REMEDIATE THE SOIL CONTAMINATION AND MONITOR FOR WORKER PROTECTION

EX STRIPING TO BE REMOVED  
 SEE PAVEMENT MARKING, SIGNING AND LANDSCAPING SHEET FOR ADDITIONAL INFORMATION

**LEGEND:**

	PAVEMENT REMOVAL
	COMBINATION CONCRETE CURB AND GUTTER REMOVAL
	DRIVEWAY PAVEMENT REMOVAL
	DRAINAGE & UTILITY STRUCTURES TO BE ADJUSTED
	DRAINAGE & UTILITY STRUCTURES TO BE REMOVED



COMPANY NAME: HRGreen.com  
 PROJECT CONTACT: [Name]  
 CLIENT: [Name]  
 DATE PLOTTED: 12/24/2013 10:35:50 AM  
 PLOT NUMBER: 8620253-rm-01.dgn  
 PEN TABLE: Standard-Trans.tbl



USER NAME = jme1hus	DESIGNED - JJS	REVISED -
DRAWN - DMS	CHECKED - TEH	REVISED -
PLOT SCALE = 1"=50'	DATE -	REVISED -
PLOT DATE = 12/24/2013		

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**REMOVAL PLAN**

SCALE: 1"=50' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	16
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 63886	



**NOTES:**

- ① FOR EXISTING AND PROPOSED UTILITIES, SEE DRAINAGE AND UTILITY SHEETS.
- ② LOCATION OF GUTTER OUTLETS REFER TO EDGE OF PAVEMENT STATION AND OFFSET UNLESS OTHERWISE NOTED.

PROP. CURVE PJANDUS-1  
 PI STA. = 12+34.62  
 $\Delta = 28^\circ 29' 24''$  (LT)  
 $D = 16^\circ 22' 13''$   
 $R = 350.00'$   
 $T = 88.86'$   
 $L = 174.04'$   
 $E = 11.10'$   
 $e = N.C.$   
 $T.R. = N/A$   
 $S.E. RUN = N/A$   
 $P.C. STA = 11+45.76$   
 $P.T. STA = 13+19.80$

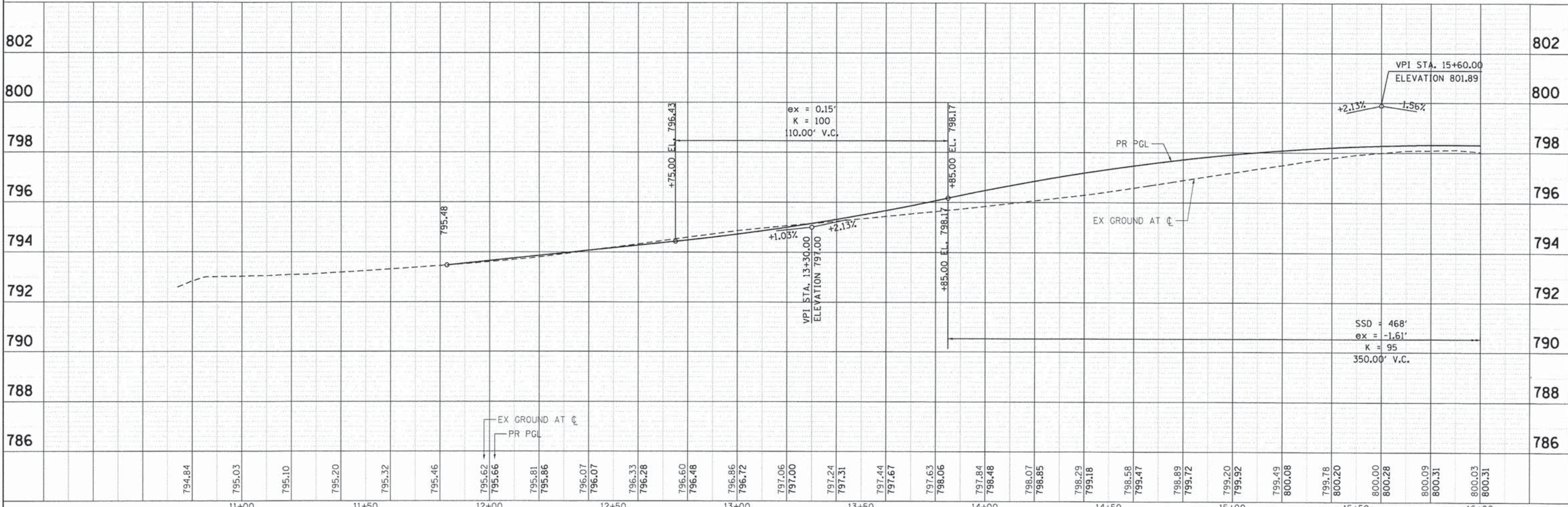
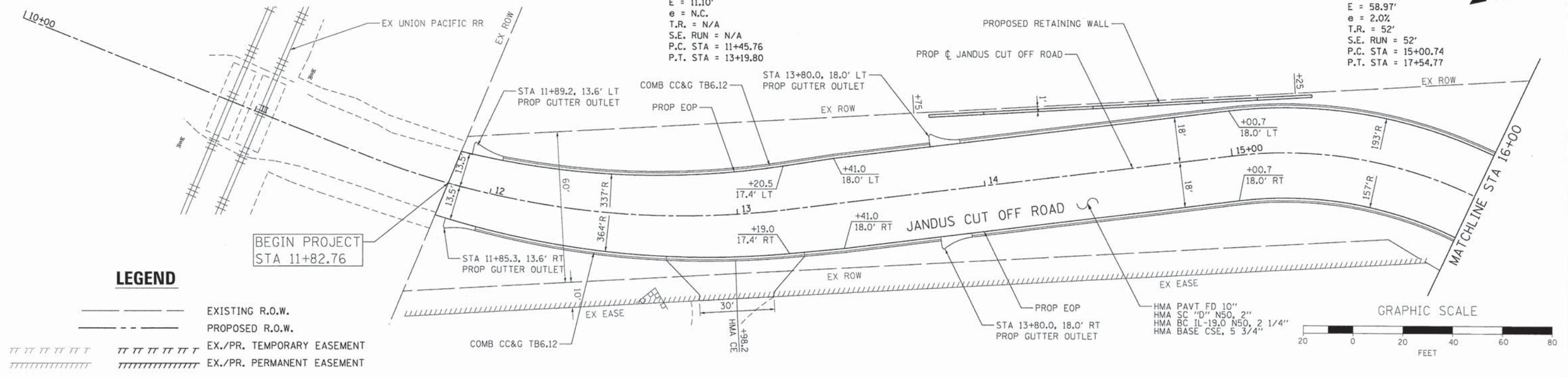
PROP. CURVE PJANDUS-2  
 PI STA. = 16+56.03  
 $\Delta = 83^\circ 10' 19''$  (RT)  
 $D = 32^\circ 44' 26''$   
 $R = 175.00'$   
 $T = 155.30'$   
 $L = 254.03'$   
 $E = 58.97'$   
 $e = 2.0%$   
 $T.R. = 52'$   
 $S.E. RUN = 52'$   
 $P.C. STA = 15+00.74$   
 $P.T. STA = 17+54.77$



PLAN	NO.
DATE	
BY	
REVISED	
ALIGNED	
CHECKED	
DATE	
BY	
NO.	

PROFILE	NO.
DATE	
BY	
REVISED	
GRADES	
CHECKED	
DATE	
BY	
NO.	

PROJECT CONTACT:  
 CLIENT:  
 DATE PLOTTED:  
 FILE NAME:  
 PLOT DRIVER:  
 PEN TABLE:



11+00	11+50	12+00	12+50	13+00	13+50	14+00	14+50	15+00	15+50	16+00																																					
794.84	795.03	795.10	795.20	795.32	795.46	795.62	795.66	795.81	795.86	796.07	796.07	796.33	796.28	796.60	796.48	796.86	796.72	797.06	797.00	797.24	797.31	797.44	797.67	797.63	798.06	797.84	798.48	798.07	798.85	798.29	799.18	798.58	799.47	798.89	799.72	799.20	799.92	799.49	800.08	799.78	800.20	800.00	800.28	800.09	800.31	800.03	800.31

**HRGreen.com**  
 Illinois Professional Design Firm  
 # 184-001322

USER NAME = bhar.tmo	DESIGNED - -	REVISED - -
FILE NAME = 86120253-PNP-01.dgn	DRAWN - -	REVISED - -
PLOT SCALE = 1"=20'	CHECKED - -	REVISED - -
PLOT DATE = 12/5/2013	DATE - -	REVISED - -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN AND PROFILE  
 JANDUS CUTOFF ROAD**  
 SCALE: 1"=20' SHEET NO. 1 OF 3 SHEETS STA. 11+82.76 TO STA. 16+00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	17
CONTRACT NO. 63886			ILLINOIS FED. AID PROJECT	



PROP. CURVE PJANDUS-2  
 PI STA. = 16+56.03  
 $\Delta = 83^\circ 10' 19''$  (RT)  
 $D = 32^\circ 44' 26''$   
 $R = 175.00'$   
 $T = 155.30'$   
 $L = 254.03'$   
 $E = 58.97'$   
 $e = 2.02'$   
 $T.R. = 52'$   
 $S.E. RUN = 52'$   
 $P.C. STA = 15+00.74$   
 $P.T. STA = 17+54.77$

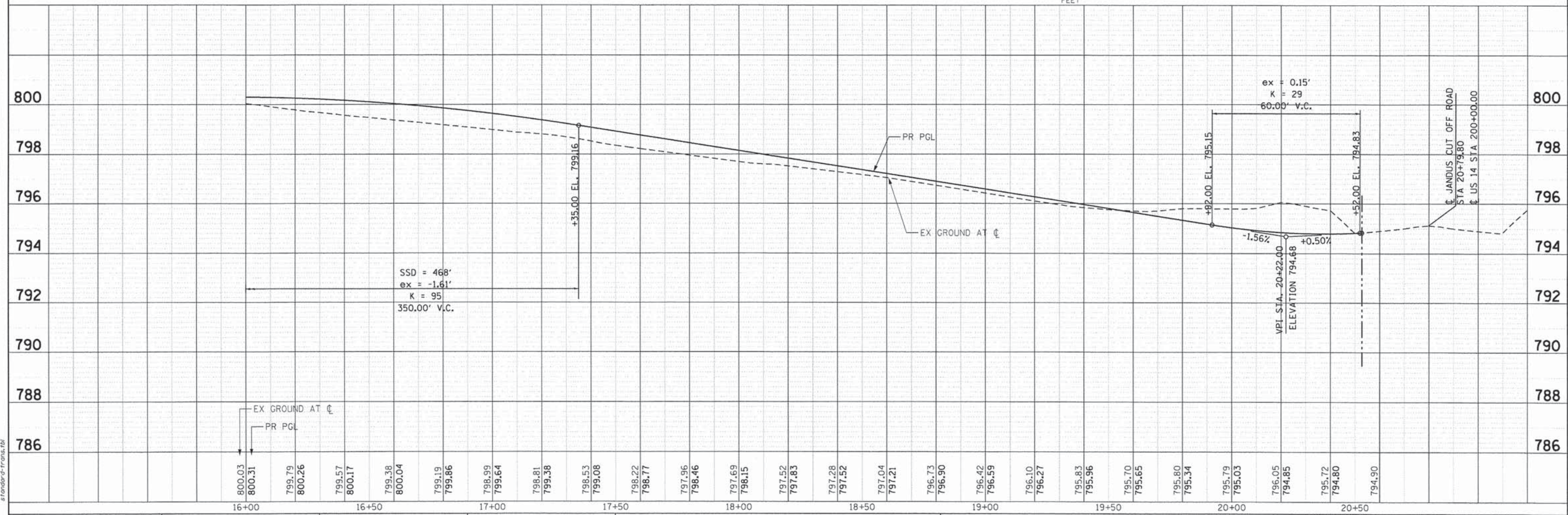
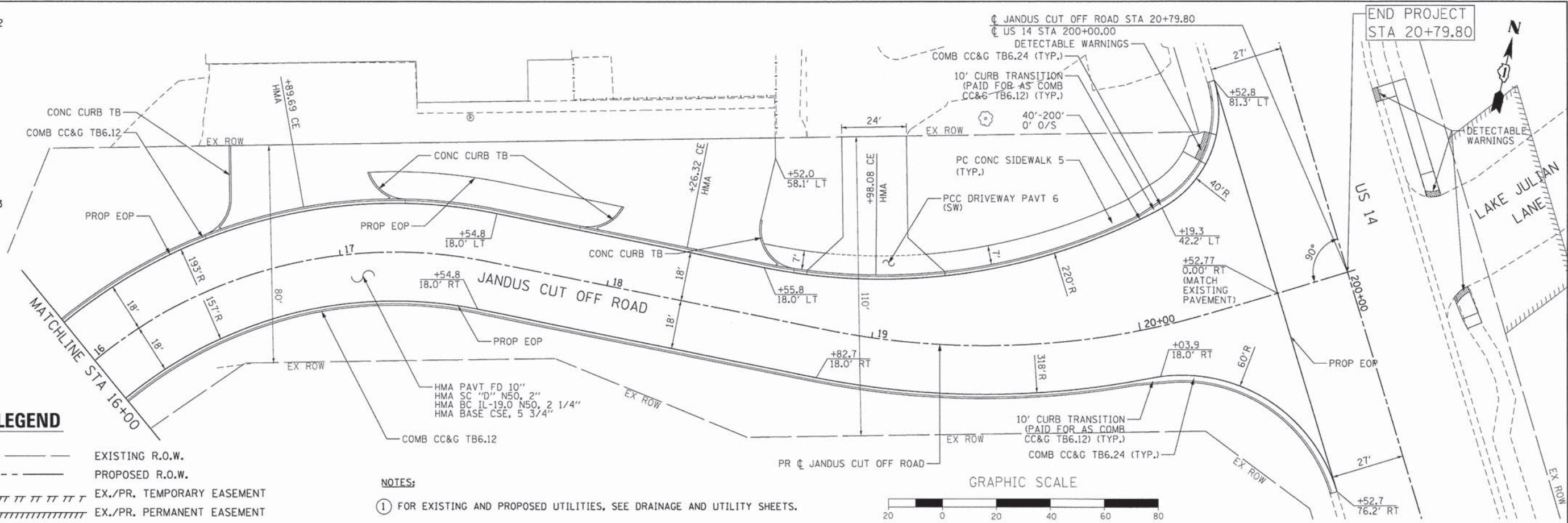
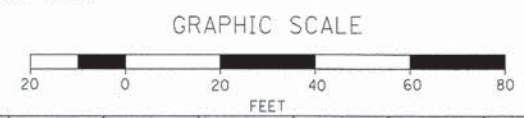
PROP. CURVE PJANDUS-3  
 PI STA. = 19+57.70  
 $\Delta = 28^\circ 07' 03''$  (LT)  
 $D = 19^\circ 05' 55''$   
 $R = 300.00'$   
 $T = 75.13'$   
 $L = 147.22'$   
 $E = 9.26'$   
 $e = N.C.$   
 $T.R. = N/A$   
 $S.E. RUN = N/A$   
 $P.C. STA. = 18+82.58$   
 $P.T. STA. = 20+29.80$

**LEGEND**

- EXISTING R.O.W.
- - - PROPOSED R.O.W.
- ||||| EX./PR. TEMPORARY EASEMENT
- ||||| EX./PR. PERMANENT EASEMENT

**NOTES:**

- ① FOR EXISTING AND PROPOSED UTILITIES, SEE DRAINAGE AND UTILITY SHEETS.



DATE	
BY	
PROF. FILE	
SUBMITTED	
PLOTTED	
GRADES CHECKED	
STRUCTURE NOTATIONS CHECKED	
NOTE BOOK NO.	

COMPANY NAME: HRGreen.com  
 PROJECT CONTACT: bhar@hrgreen.com  
 CLIENT: HRGreen  
 DATE PLOTTED: 12/25/2013 8:02:23 AM  
 FILE NUMBER: 86120253-PNP-02.dgn  
 PLOT NUMBER: 16+00-20+00  
 PLOT TABLE: 16+00-20+00

USER NAME = bhar@hrgreen.com	DESIGNED -	REVISED -
FILE NAME = 86120253-PNP-02.dgn	DRAWN -	REVISED -
PLOT SCALE = 1"=20'	CHECKED -	REVISED -
PLOT DATE = 12/5/2013	DATE -	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN AND PROFILE  
 JANDUS CUTOFF ROAD

SCALE: 1"=20' SHEET NO. 2 OF 3 SHEETS STA. 16+00 TO STA. 20+79.80

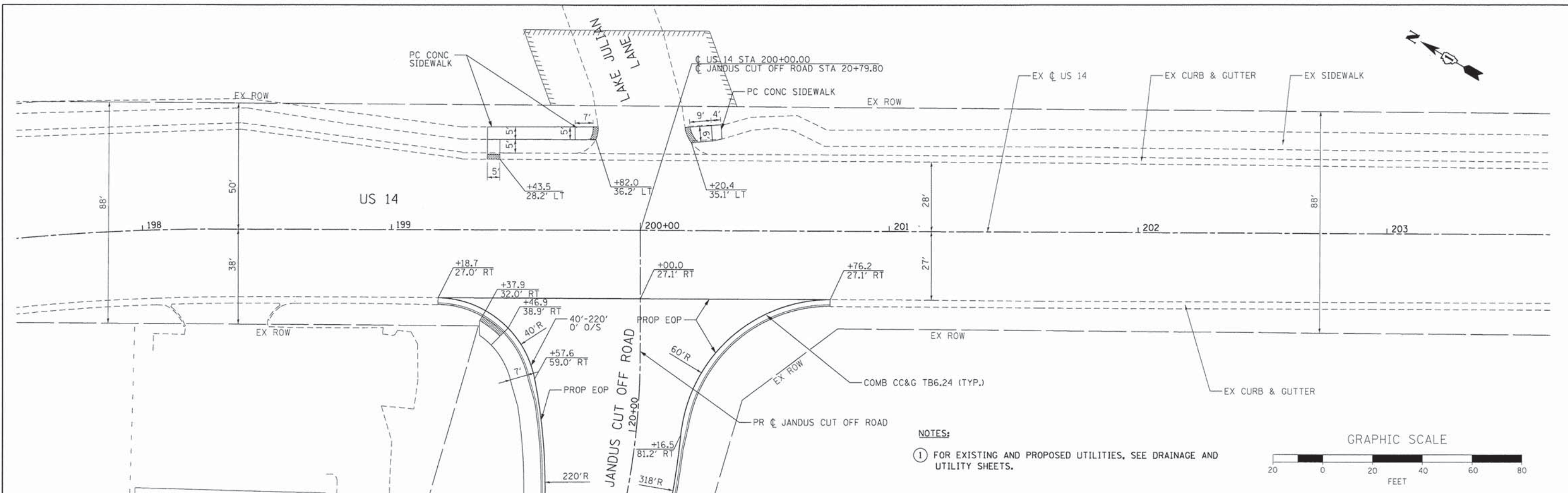
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	18
CONTRACT NO. 63886			ILLINOIS FED. AID PROJECT	



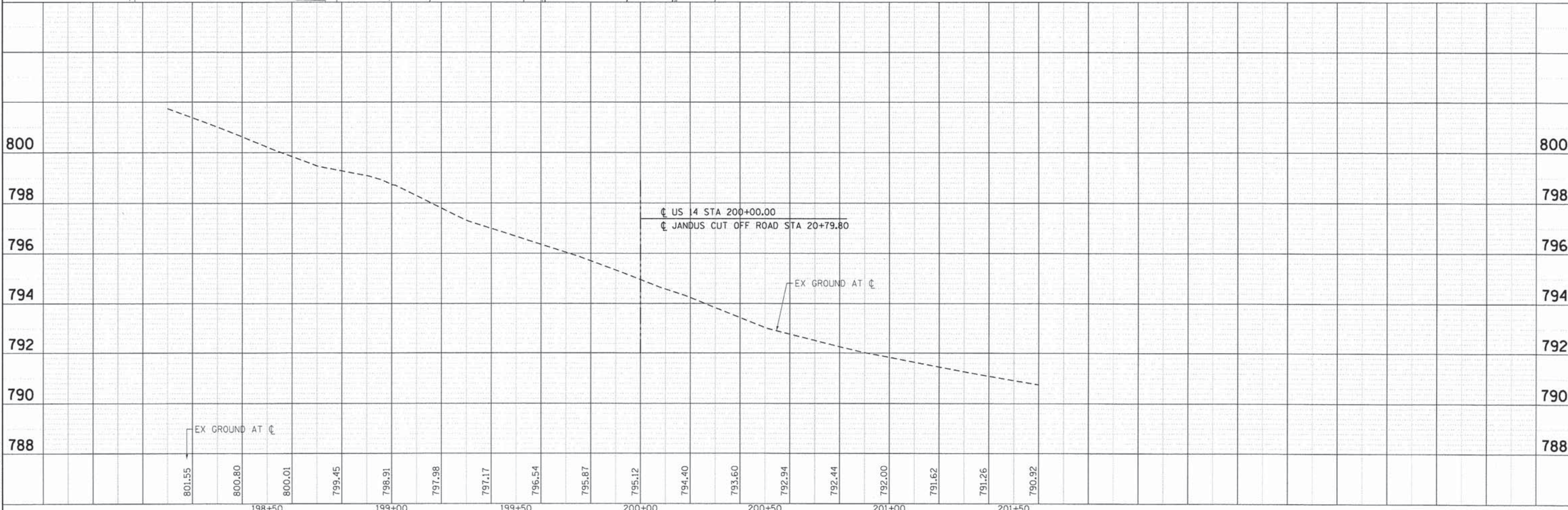
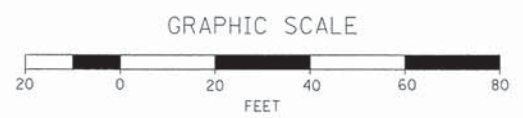
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	PLOTTED		
	ALIGNED		
	CHECKED		
	NO.		
	FILE NAME		

PROFILE	DESIGNED	BY	DATE
	PLOTTED		
	GRADES		
	CHECKED		
	NO.		
	FILE NAME		

COMPANY NAME: HRGreen  
 PROJECT CONTACT: HRGreen.com  
 CLIENT: Illinois Professional Design Firm  
 DATE PLOTTED: 12/5/2013 9:07:28 AM  
 FILE NAME: 86120253-PNP-03.dgn  
 PLOT NUMBER: 03 of 03  
 PLOT DATE: 12/5/2013



**NOTES:**  
 ① FOR EXISTING AND PROPOSED UTILITIES, SEE DRAINAGE AND UTILITY SHEETS.



USER NAME = bhar-tina FILE NAME = 86120253-PNP-03.dgn PLOT SCALE = 1"=20' PLOT DATE = 12/5/2013	DESIGNED - DRAWN - CHECKED - DATE -	REVISED - REVISED - REVISED - REVISED -	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>ROADWAY PLAN AND PROFILE</b> <b>U.S. ROUTE 14</b>	F.A. RTE. 4058 SECTION 13-00063-00-PV COUNTY MCHENRY CONTRACT NO. 63886	TOTAL SHEETS 79 SHEET NO. 19	ILLINOIS FED. AID PROJECT
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SCALE: 1"=20' SHEET NO. 3 OF 3 SHEETS STA. 197+50 TO STA. 203+50



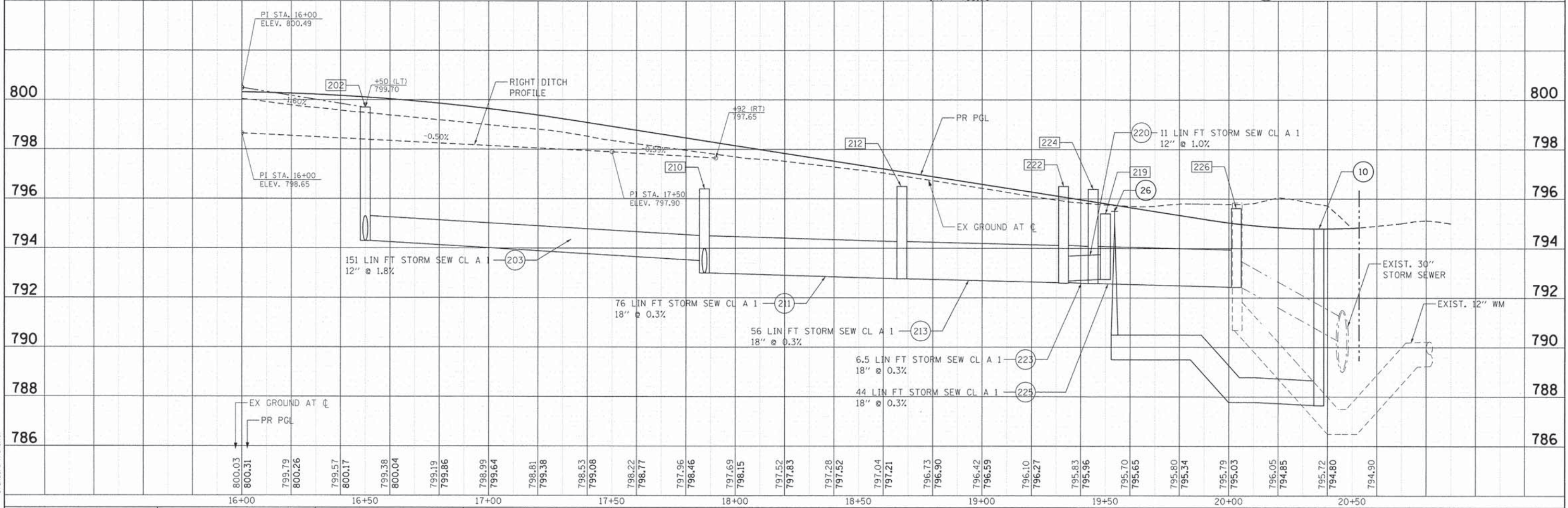
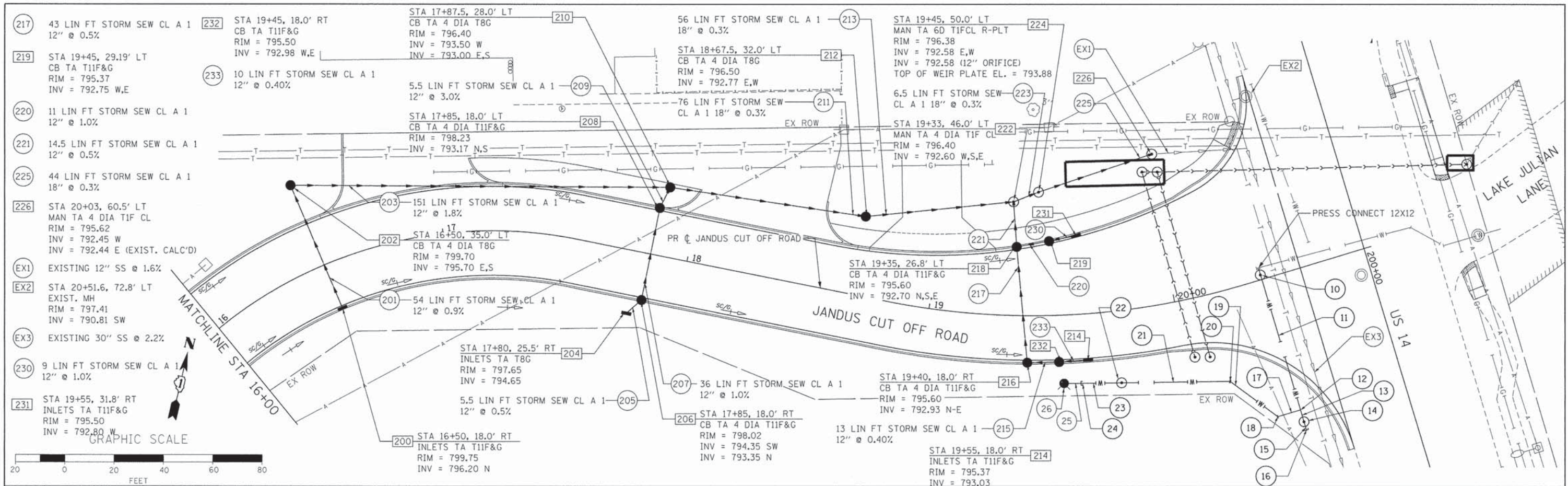




PLAN	DATE
SURVEYED	
ALIGNED	
CHECKED	
RT. OF WAY CHECKED	
FILE NAME	
NO.	

PROFILE	DATE
SURVEYED	
UPDATES CHECKED	
MARKS CHECKED	
STRUCTURE NOTATIONS CHFD	
NOTE BOOK	
NO.	

COMPANY NAME: HRGreen.com  
 PROJECT CONTACT: Illinois Professional Design Firm #184-001322  
 CLIENT: 1/13/2014 12:55:08 PM  
 DATE PLOTTED: 86120253-DPP-02.dgn  
 FILE NAME: pdf.plt  
 PLOT DRIVER: standard-trans.tbl  
 PEN TABLE:



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798.65	799.70	799.90	799.64	798.81	799.38	798.53	799.08	798.22	798.77
797.96	798.46	797.69	798.15	797.52	797.83	797.28	797.52	797.04	797.21
796.73	796.90	796.42	796.59	796.10	796.27	795.83	795.96	795.70	795.65
795.80	795.34	795.79	795.03	796.05	794.85	795.72	794.80	794.90	

**HRGreen.com**  
 Illinois Professional Design Firm  
 #184-001322

USER NAME = tjeilton	DESIGNED -	REVISED -
FILE NAME = 86120253-DPP-02.dgn	DRAWN -	REVISED -
PLOT SCALE = 1"=20'	CHECKED -	REVISED -
PLOT DATE = 1/13/2014	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**JANDUS CUTOFF ROAD  
 DRAINAGE AND UTILITIES PLAN AND PROFILE**

SCALE: 1"=20'    SHEET NO. 2 OF 2 SHEETS    STA.    TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	21
CONTRACT NO. 63886			ILLINOIS FED. AID PROJECT	







**PROPOSED DRAINAGE STRUCTURES**

LOCATION				INLETS TYPE A		CATCH BASINS TYPE A 4' DIAMETER		MANHOLES TYPE A		DRY-WELL	PRECAST REINF CONC FLARED END SECTIONS
ID NUMBER	STATION	OFFSET (FOOT)		TYPE 8 GRATE (EACH)	TYPE 11 F & G (EACH)	TYPE 8 GRATE (EACH)	TYPE 11 F & G (EACH)	4' DIAMETER T1F CL (EACH)	6' DIAMETER T1F CL R-PLT (EACH)	4' DIAMETER TYPE 8 GRATE (EACH)	12" (EACH)
JANDUS CUT OFF ROAD											
100	13+27	23	RT	1							
102	12+25	25	RT								1
103	12+85	27	LT							1	
200	16+50	18	RT		1						
202	16+50	37.5	LT			1					
204	17+80	25.5	RT	1							
206	17+85	18	RT				1				
208	17+85	18	LT				1				
210	17+87.5	28	LT			1					
212	18+67.5	32	LT			1					
214	19+55	18	RT		1						
216	19+40	18	RT				1				
218	19+35	26.8	LT				1				
219	19+50	30.0	LT		1						
222	19+33	46	LT					1			
224	19+45	50	LT						1		
226	20+03	60.5	LT					1			
SUBTOTAL JANDUS CUT OFF ROAD				2	3	3	4	2	1	1	1
TOTAL				2	3	3	4	2	1	1	1

**WATER MAIN SCHEDULE**

1 - 9	UNUSED	21	DI WM CL52 POLY EN 12 40 LIN FT
10	PRESS CONNECT 12X12 WATER VALVES 12 VV TA 6 DIA T1F CL STA. 20+36.4, 1.9' LT RIM = 794.79	22	WATER VALVES 12 VV TA 6 DIA T1F CL STA. 19+75.0, 29.0' RT RIM = 795.00
11	DI WM CL52 POLY EN 12 54 LIN FT	23	DI WM CL52 POLY EN 12 13 LIN FT
12	DI WAT MN TEE, 12X 12 STA. 20+36.3, 55.0' RT	24	DI WAT MN RD, 12 X 6 STA. 19+60.0, 27.9' RT
13	DI WM CL52 POLY EN 12 2 LIN FT	25	DI WM CL52 POLY EN 6 5 LIN FT
14	WATER VALVES 12 VV TA 6 DIA T1F CL STA. 20+36.3, 60.0' RT RIM = 794.00	26	FIRE HYD W/AUX V & VB STA. 19+53.7, 27' RT BURY ELEV. = 795.50
15	DI WM CL52 POLY EN 12 2 LIN FT		
16	PLUG WATER MAIN 12 STA. 20+36.3, 63.5' RT		
17	DI WM CL52 POLY EN 12 9 LIN FT		
18	DI WT MNF 12 45.0 DB STA.20+27.2, 55.0' RT		
19	DI WM CL52 POLY EN 12 24 LIN FT		
20	DI WT MNF 12 45.0 DB STA.20+14.2, 36.2' RT		

**PROPOSED DRAINAGE PIPES**

LOCATION					STORM SEWERS CLASS A, TYPE 1		TRENCH BACKFILL
ID NUMBER	UPSTREAM STATION	UPSTREAM STRUCTURE ID NUMBER	DOWNSTREAM STATION	DOWNSTREAM STRUCTURE ID NUMBER	12" (FOOT)	18" (FOOT)	(CU YD)
JANDUS CUT OFF ROAD							
101	13+27	100	12+25	102	101		0.0
201	16+50	200	16+50	202	56		5.8
203	16+50	202	17+87.5	210	151		26.1
205	17+80	204	17+85	206	5.5		0.5
207	17+85	206	17+85	208	36		7.0
209	17+85	208	17+87.5	210	5.5		0.7
211	17+87.5	210	18+67.5	212		76	11.8
213	18+67.5	212	19+33	222		56	10.1
215	19+55	214	19+40	216	13		1.7
217	19+40	216	19+35	218	43		5.7
220	19+50	219	19+35	218	11		1.5
221	19+35	218	19+33	222	14.5		1.9
223	19+33	222	19+45	224		7	0.0
225	19+45	224	20+03	226		44	0.0
SUBTOTAL JANDUS CUT OFF ROAD					437	183	73
TOTAL					437	183	73

8612053  
VILLAGE OF CARY  
8612053-DPP-SCH-01.dgn

COMPANY NAME: HRGreen.com  
PROJECT CONTACT: Illinois Professional Design Firm #194-001322  
CLIENT: JANDUS CUT OFF ROAD  
DATE PLOTTED: 1/13/2014 12:25:02 PM  
PLT NAME: 8612053-DPP-SCH-01.dgn  
PEN TABLE: Standard-Trans.tbl



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	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

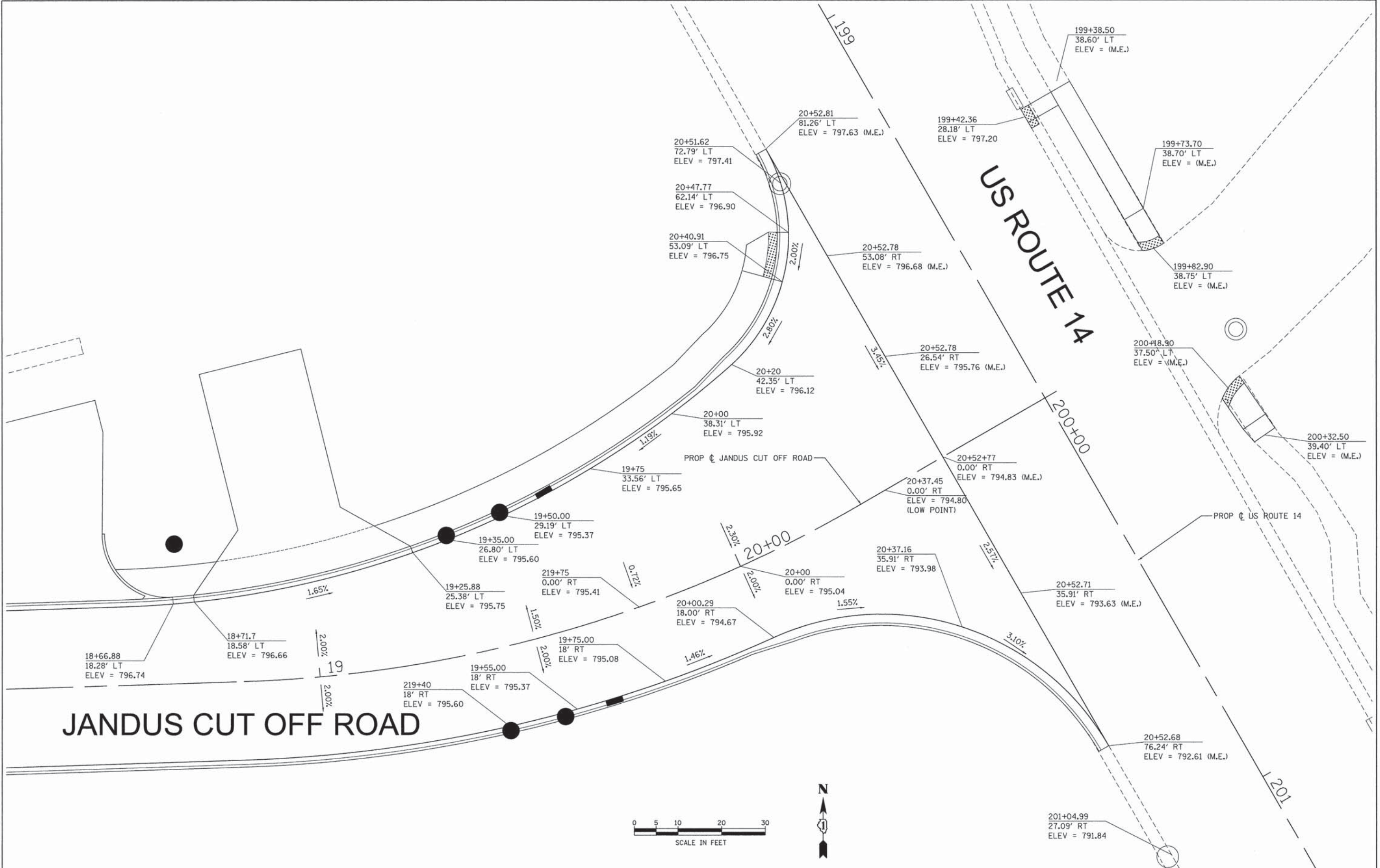
**JANDUS CUTOFF ROAD  
DRAINAGE SCHEDULE**

SCALE: N.T.S. SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	23
CONTRACT NO. 63886				

FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT





# JANDUS CUT OFF ROAD

# US ROUTE 14

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

US ROUTE 14 AT JANDUS CUTOFF ROAD  
INTERSECTION GRADING PLAN

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	24
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
CONTRACT NO.			63886	

USER NAME = jmelhus	DESIGNED - JJS	REVISED -
DATE PLOTTED = 12/24/2013 10:36:07 AM	DRAWN - DMS	REVISED -
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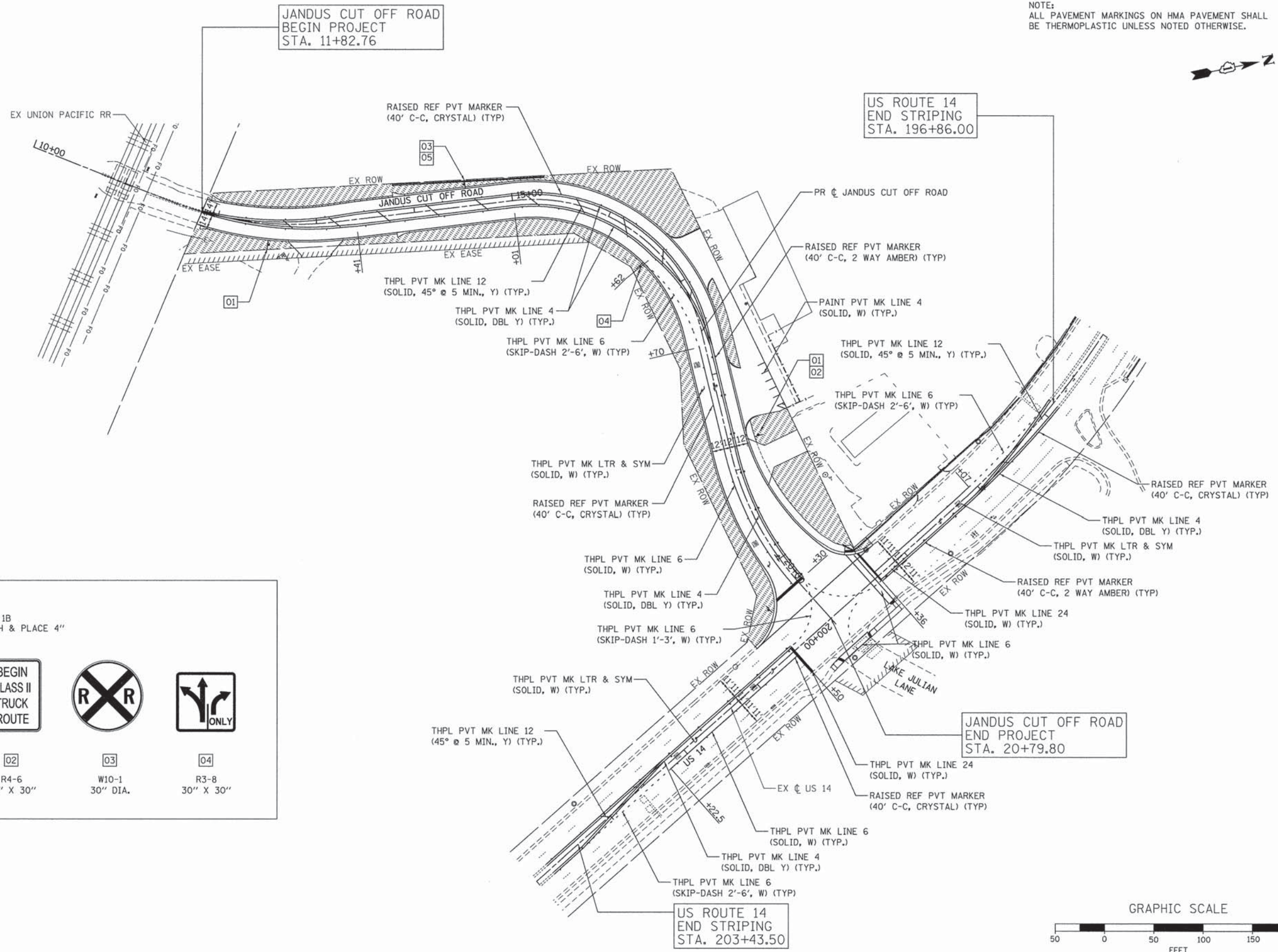
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COMPANY NAME: HRGreen.com  
PROJECT CONTACT: Illinois Professional Design Firm #184-001322  
DATE PLOTTED: 12/24/2013 10:36:07 AM  
FILE NAME: 18120253-PEP-01.dgn  
PLOT DRIVER: pldrvr  
FOR TABLE:





NOTE:  
ALL PAVEMENT MARKINGS ON HMA PAVEMENT SHALL  
BE THERMOPLASTIC UNLESS NOTED OTHERWISE.



**LEGEND**

SEEDING, CLASS 1B  
TOPSOIL FURNISH & PLACE 4"

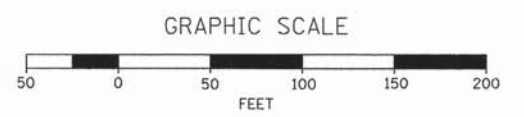
**SPEED LIMIT 25**  
01 R2-1  
30" X 36"

**BEGIN CLASS II TRUCK ROUTE**  
02 R4-6  
24" X 30"

**R X R**  
03 W10-1  
30" DIA.

**ONLY**  
04 R3-8  
30" X 30"

05 W10-9P  
36" X 36"



COMPANY NAME: HRGreen.com  
PROJECT CONTACT: jmelhus  
DATE PLOTTED: 12/24/2013 10:36:11 AM  
FILE NAME: 13-00063-pmk-01.dgn  
PLOT SCALE: 1"=50'  
PEN TABLE: Standard-Trans.tbl

HRGreen.com  
Illinois Professional Design Firm  
#184-001322

USER NAME = jmelhus	DESIGNED - JJS	REVISED -
DRAWN - DMS	CHECKED - TEH	REVISED -
DATE = 12/24/2013		

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING, SIGNING AND LANDSCAPE PLAN**

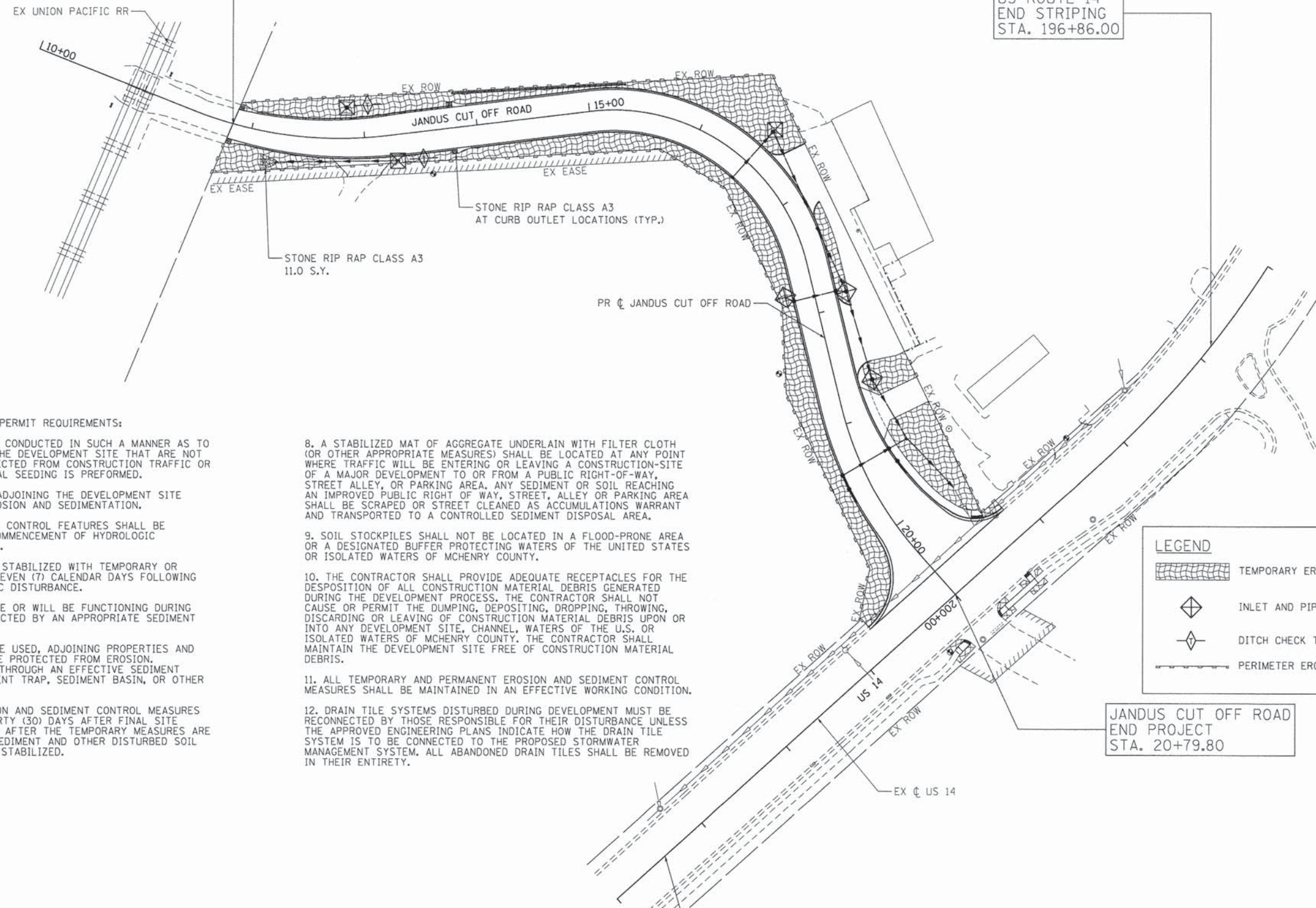
SCALE: 1"=50' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE. 4058	SECTION 13-00063-00-PV	COUNTY MCHENRY	TOTAL SHEETS 79	SHEET NO. 25
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 63886	



JANDUS CUT OFF ROAD  
BEGIN PROJECT  
STA. 11+82.76

US ROUTE 14  
END STRIPING  
STA. 196+86.00



- INLET FILTERS**
- STA. 16+50, 18' RT
  - STA. 17+85, 18.0' RT
  - STA. 19+33, 46.0' LT
  - STA. 19+35, 26.8' LT
  - STA. 19+40, 18' RT
  - STA. 19+50, 29.2' LT
  - STA. 19+55, 18' RT
  - STA. 199+27.2, 28.2' RT
  - STA. 201+05.0, 28.1' RT

- INLET AND PIPE PROTECTION**
- STA. 12+85, 27' LT
  - STA. 13+27, 23' RT
  - STA. 16+50, 37.5' LT
  - STA. 17+80, 25.5' RT
  - STA. 17+87.5, 28.0' LT
  - STA. 18+67.5, 32.0' LT

- DITCH CHECK TEMPORARY**
- STA. 13+05, 28.5' LT
  - STA. 13+50, 23.5' RT

**LEGEND**

- TEMPORARY EROSION CONTROL SEEDING
- INLET AND PIPE PROTECTION
- DITCH CHECK TEMPORARY
- PERIMETER EROSION BARRIER, SPECIAL (TYP.)

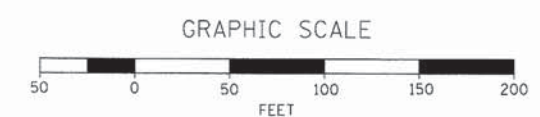
**MCHENRY COUNTY STORMWATER PERMIT REQUIREMENTS:**

1. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. AREAS OF THE DEVELOPMENT SITE THAT ARE NOT TO BE GRADED SHALL BE PROTECTED FROM CONSTRUCTION TRAFFIC OR OTHER DISTURBANCE UNTIL FINAL SEEDING IS PREFORMED.
2. PROPERTIES AND CHANNELS ADJOINING THE DEVELOPMENT SITE SHALL BE PROTECTED FROM EROSION AND SEDIMENTATION.
3. SOIL EROSION AND SEDIMENT CONTROL FEATURES SHALL BE CONSTRUCTED PRIOR TO THE COMMENCEMENT OF HYDROLOGIC DISTURBANCE OF UPLAND AREAS.
4. DISTURBED AREAS SHALL BE STABILIZED WITH TEMPORARY OR PERMANENT MEASURES WITHIN SEVEN (7) CALENDAR DAYS FOLLOWING THE END OF ACTIVE HYDROLOGIC DISTURBANCE.
5. ALL STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY AN APPROPRIATE SEDIMENT CONTROL MEASURE.
6. IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION. DISCHARGES SHALL BE ROUTED THROUGH AN EFFECTIVE SEDIMENT CONTROL MEASURE (E.G., SEDIMENT TRAP, SEDIMENT BASIN, OR OTHER APPROPRIATE MEASURES).
7. ALL TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN THIRTY (30) DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED. TRAPPED SEDIMENT AND OTHER DISTURBED SOIL AREAS SHALL BE PERMANENTLY STABILIZED.

8. A STABILIZED MAT OF AGGREGATE UNDERLAIN WITH FILTER CLOTH (OR OTHER APPROPRIATE MEASURES) SHALL BE LOCATED AT ANY POINT WHERE TRAFFIC WILL BE ENTERING OR LEAVING A CONSTRUCTION-SITE OF A MAJOR DEVELOPMENT TO OR FROM A PUBLIC RIGHT-OF-WAY, STREET ALLEY, OR PARKING AREA. ANY SEDIMENT OR SOIL REACHING AN IMPROVED PUBLIC RIGHT OF WAY, STREET, ALLEY OR PARKING AREA SHALL BE SCRAPED OR STREET CLEANED AS ACCUMULATIONS WARRANT AND TRANSPORTED TO A CONTROLLED SEDIMENT DISPOSAL AREA.
9. SOIL STOCKPILES SHALL NOT BE LOCATED IN A FLOOD-PRONE AREA OR A DESIGNATED BUFFER PROTECTING WATERS OF THE UNITED STATES OR ISOLATED WATERS OF MCHENRY COUNTY.
10. THE CONTRACTOR SHALL PROVIDE ADEQUATE RECEPTACLES FOR THE DESPOSITION OF ALL CONSTRUCTION MATERIAL DEBRIS GENERATED DURING THE DEVELOPMENT PROCESS. THE CONTRACTOR SHALL NOT CAUSE OR PERMIT THE DUMPING, DEPOSITING, DROPPING, THROWING, DISCARDING OR LEAVING OF CONSTRUCTION MATERIAL DEBRIS UPON OR INTO ANY DEVELOPMENT SITE, CHANNEL, WATERS OF THE U.S. OR ISOLATED WATERS OF MCHENRY COUNTY. THE CONTRACTOR SHALL MAINTAIN THE DEVELOPMENT SITE FREE OF CONSTRUCTION MATERIAL DEBRIS.
11. ALL TEMPORARY AND PERMANENT EROSION AND SEDIMENT CONTROL MEASURES SHALL BE MAINTAINED IN AN EFFECTIVE WORKING CONDITION.
12. DRAIN TILE SYSTEMS DISTURBED DURING DEVELOPMENT MUST BE RECONNECTED BY THOSE RESPONSIBLE FOR THEIR DISTURBANCE UNLESS THE APPROVED ENGINEERING PLANS INDICATE HOW THE DRAIN TILE SYSTEM IS TO BE CONNECTED TO THE PROPOSED STORMWATER MANAGEMENT SYSTEM. ALL ABANDONED DRAIN TILES SHALL BE REMOVED IN THEIR ENTIRETY.

JANDUS CUT OFF ROAD  
END PROJECT  
STA. 20+79.80

US ROUTE 14  
END STRIPING  
STA. 203+43.50



COMPANY NAME: HRGreen  
 PROJECT CONTACT: HRGreen  
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 FILE NAME: 86120253-ero-01.dgn  
 PLOT DRIVER: pdf.plt  
 PEN TABLE: standard-trans.tbl



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	DRAWN - DMS	REVISED -
PLOT SCALE = 1"=50'	CHECKED - TEH	REVISED -
PLOT DATE = 12/5/2013	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**EROSION CONTROL PLAN**

SCALE: 1"=50' SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
405B	13-00063-00-PV	MCHENRY	79	26
CONTRACT NO. 63886				

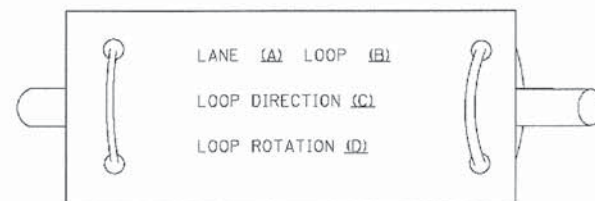
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT



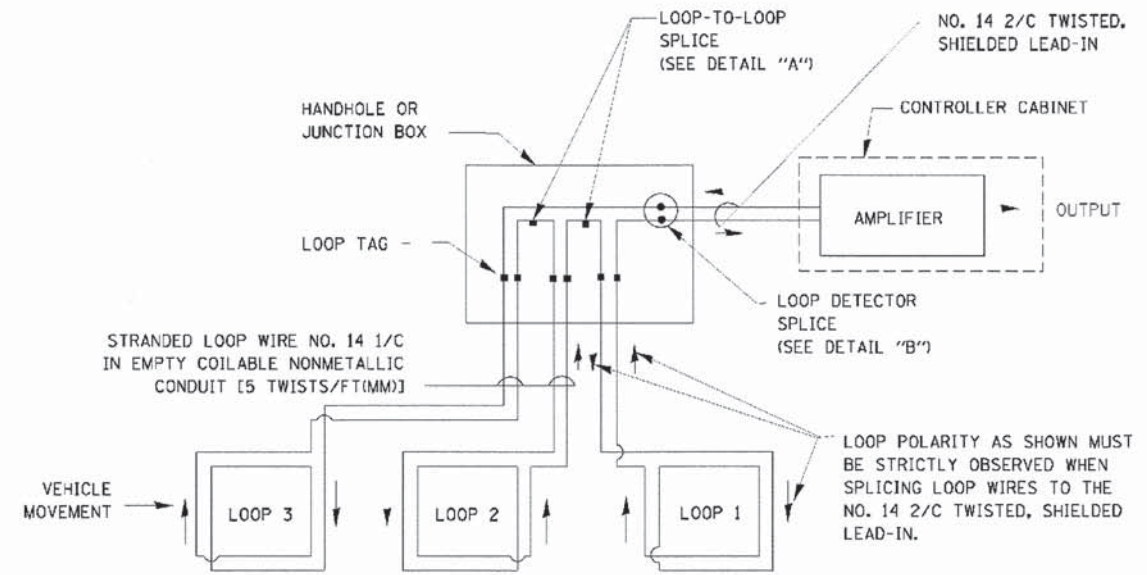
**LOOP DETECTOR NOTES**

- EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
- THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
- EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
- ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
- IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
- LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
- PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

**LOOP LEAD-IN CABLE TAG**

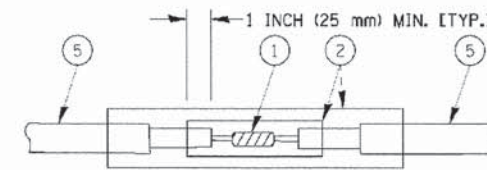


- LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

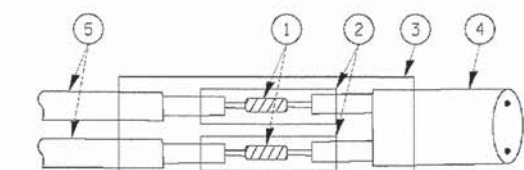


**DETECTOR LOOP WIRING SCHEMATIC**

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

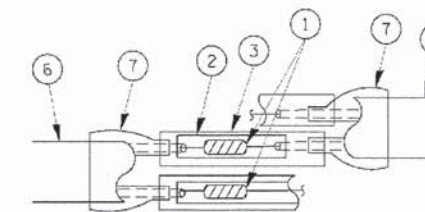


DETAIL "A" LOOP-TO-LOOP SPLICE

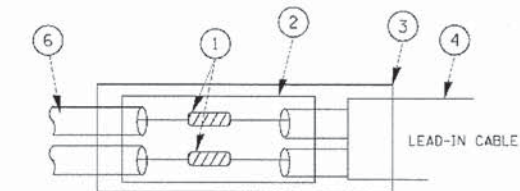


DETAIL "B" LOOP-TO-CONTROLLER SPLICE

**TYPE I LOOP**



DETAIL "A" LOOP-TO-LOOP SPLICE



DETAIL "B" LOOP-TO-CONTROLLER SPLICE

**PREFORMED LOOP**

**LOOP DETECTOR SPLICE**

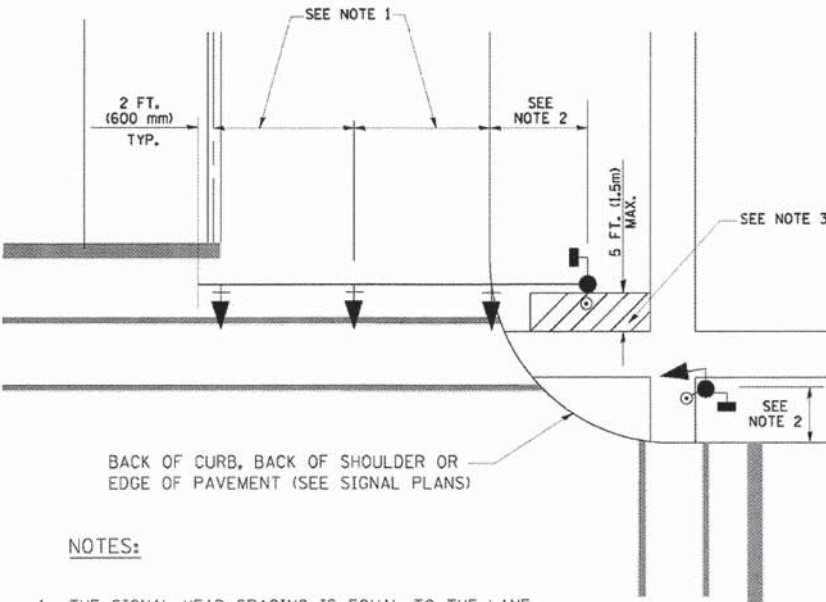
- WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH.
- WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- NO. 14 2/C TWISTED, SHIELDED CABLE.
- LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- PREFORMED LOOP
- XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO CBR-2 OR APPROVED EQUAL

COMPANY NAME: HRGreen.com  
 PROJECT CONTACT: Illinois Professional Design Firm # 184-001322  
 DATE PLOTTED: 12/5/2013 9:08:08 AM  
 FILE NAME: 86120253-sig-de101.dgn  
 PLOT DRIVER: pdf.plt  
 PEN TABLE: standard-trans.tbl

	DESIGNED - DAD DRAWN - BCK CHECKED - DAD DATE - 10/28/09	REVISED - REVISED - REVISED - REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS</b>		F.A. RTE. 405B	SECTION 13-0063-00-PV	COUNTY MCHENRY	TOTAL SHEETS 79	SHEET NO. 27
	PLCT SCALE - 20.0000' / 1" / 4" PLCT DATE - 10/28/2009	SCALE: N.T.S.		SHEET NO. 1 OF 22 SHEETS	STA. TO STA.	FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT	CONTRACT NO. 63886			



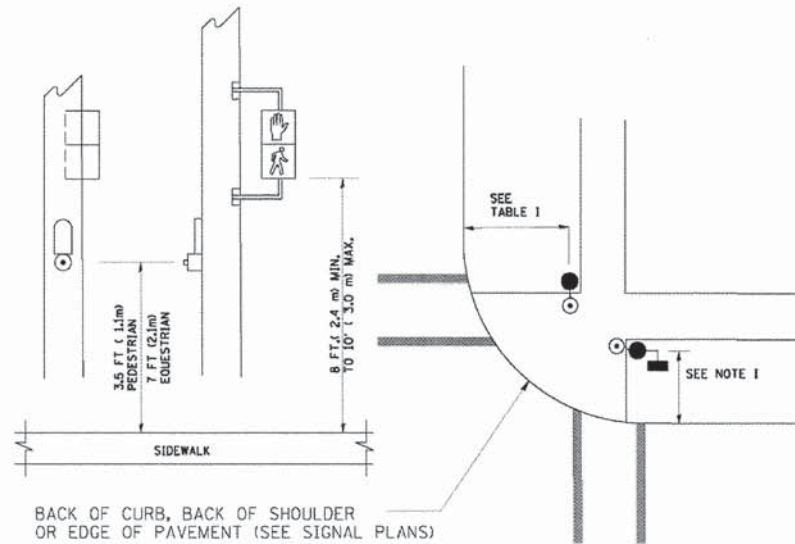
**TRAFFIC SIGNAL MAST ARM AND SIGNAL POST**  
 MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.



**NOTES:**

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

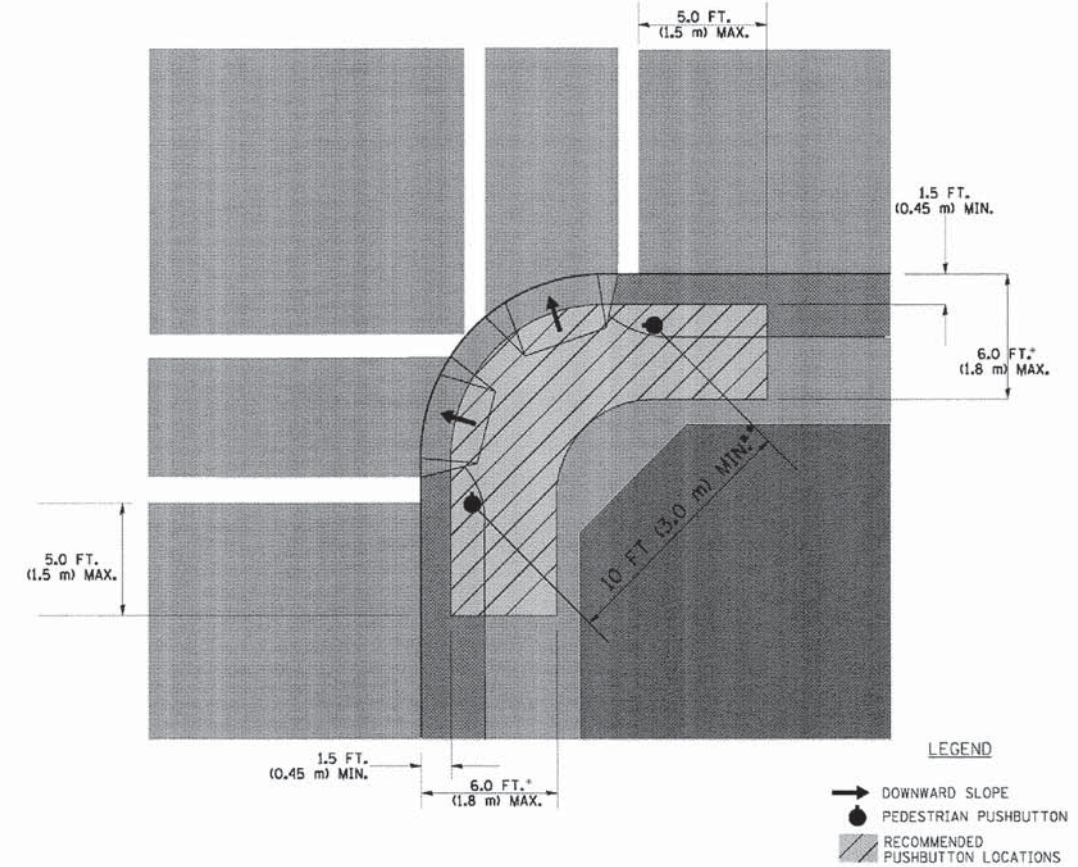
**PEDESTRIAN SIGNAL POST AND PEDESTRIAN PUSH BUTTON POST**



**NOTES:**

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

**RECOMMENDED PUSHBUTTON LOCATIONS**



**LEGEND**

- DOWNWARD SLOPE
- PEDESTRIAN PUSHBUTTON
- ▨ RECOMMENDED PUSHBUTTON LOCATIONS

- WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

**NOTES:**

1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

**TRAFFIC SIGNAL EQUIPMENT OFFSET**

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

**NOTES:**

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD AFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

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 PROJECT CONTACT: Illinois Professional Design Firm  
 CLIENT: 12/5/2013 9:08:17 AM #184-001322  
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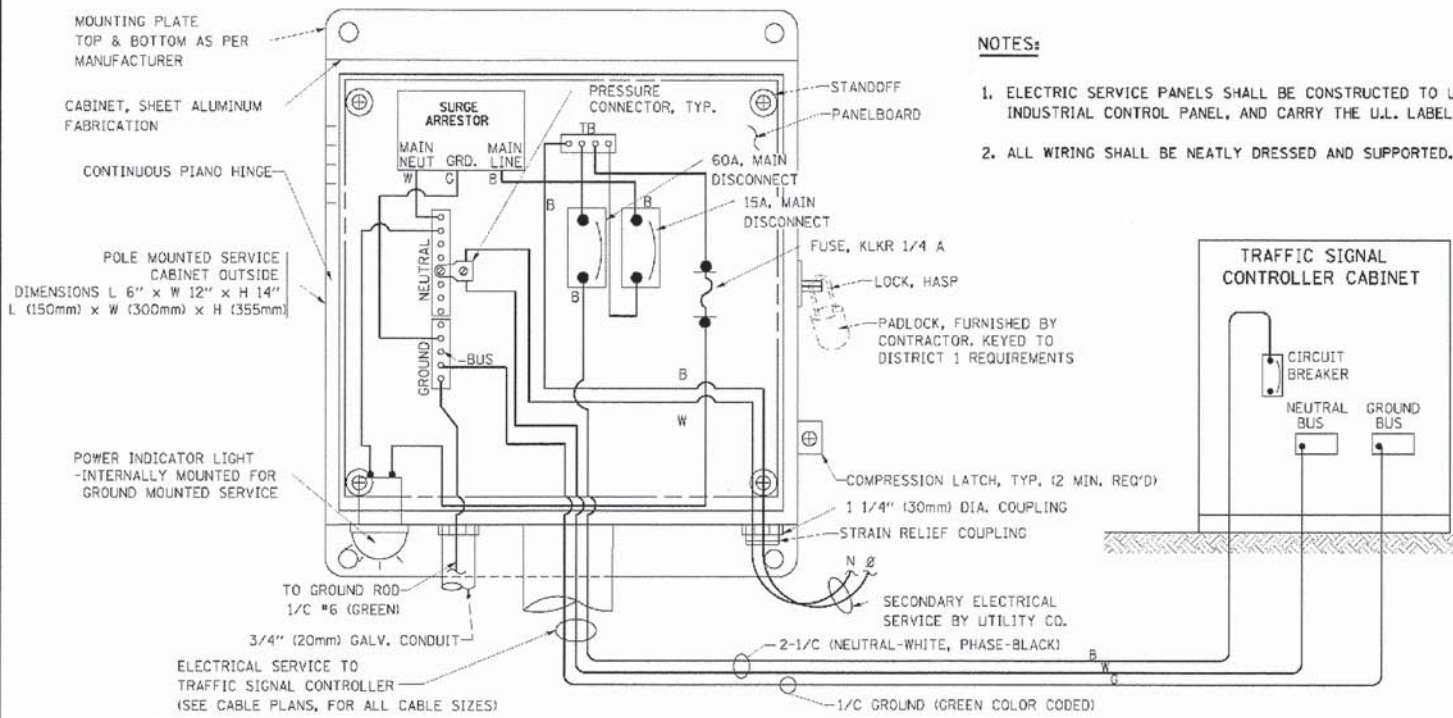
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 DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE  
 STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

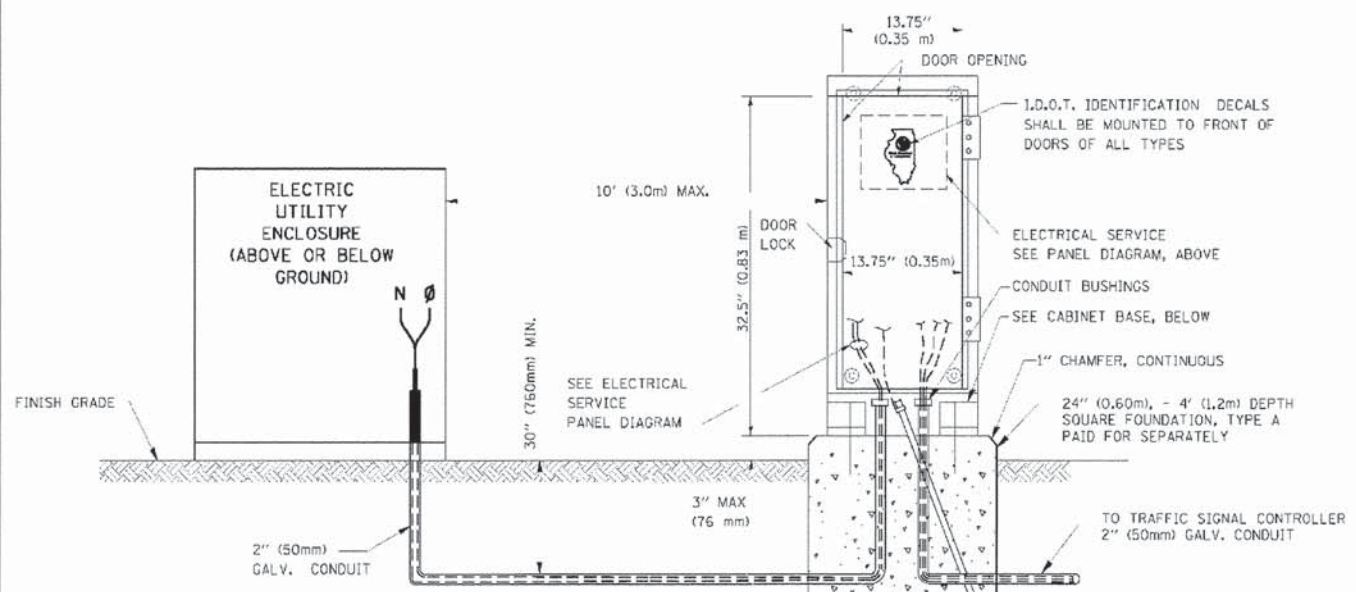
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405B	13-00063-00-PV	MCHENRY	79	28
CONTRACT NO. 63886			ILLINOIS FED. AID PROJECT	

SCALE: N.T.S. SHEET NO. 2 OF 22 SHEETS STA. TO STA.



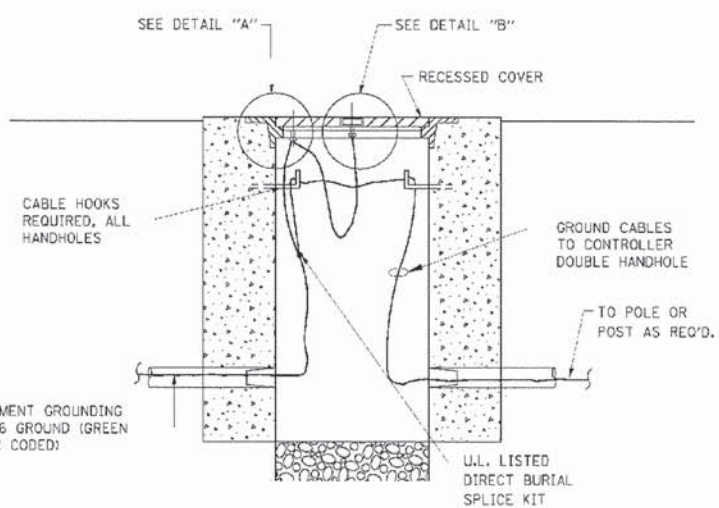
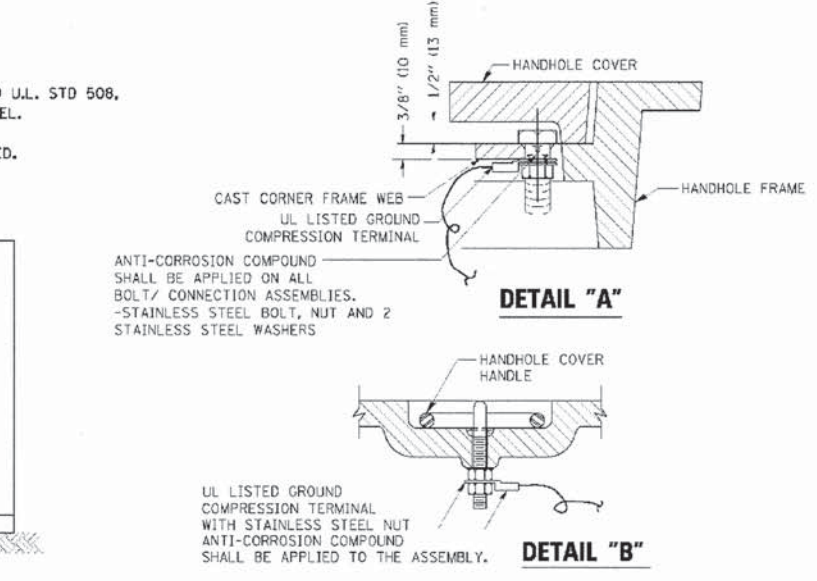


**ELECTRICAL SERVICE – PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE)**  
**SERVICE INSTALLATION POLE MOUNT (SHOWN)**  
 (NOT TO SCALE)

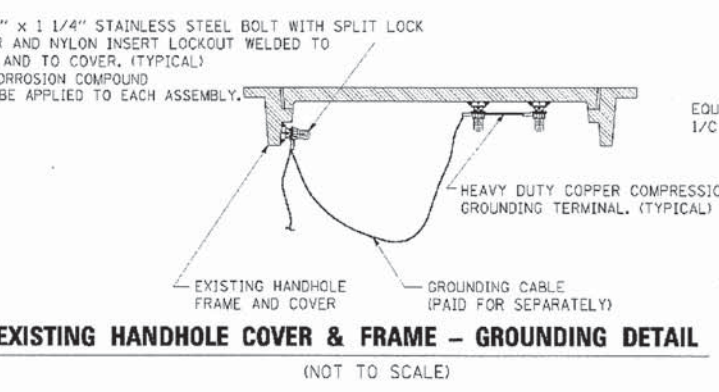


**SERVICE INSTALLATION GROUND MOUNT**  
 (NOT TO SCALE)

**CABINET – BASE BOLT PATTERN**  
 (NOT TO SCALE)

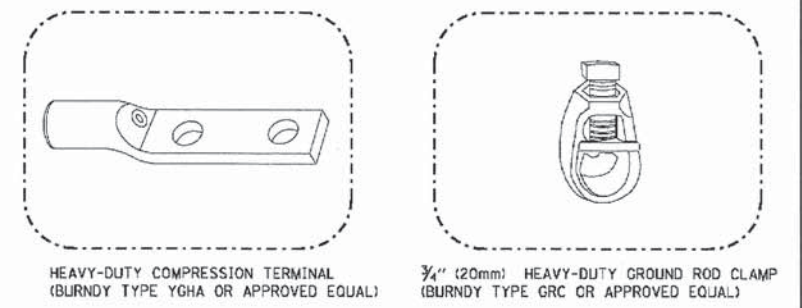


**HANDHOLE COVER & FRAME – GROUNDING DETAIL**  
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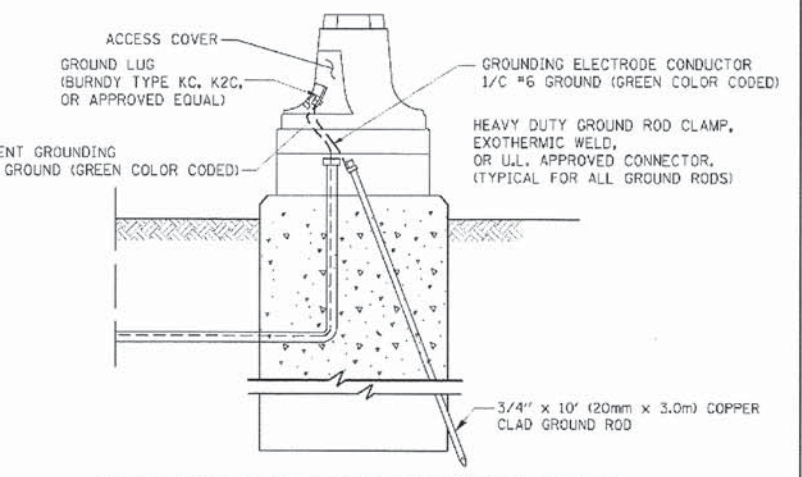


**EXISTING HANDHOLE COVER & FRAME – GROUNDING DETAIL**  
 (NOT TO SCALE)

- NOTES:**
- GROUNDING SYSTEM**
1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS. THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD. ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
  2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
  3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
  4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



- NOTES:**
- ALL CLAMPS SHALL BE BRONZE OR COPPER, U.L. APPROVED.
  - GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



**MAST ARM POLE /POST-GROUNDING DETAIL**  
 (NOT TO SCALE)

12/15/2013 8:00:37 AM  
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 CLIENT: ILLINOIS DEPARTMENT OF TRANSPORTATION  
 PLOT DRIVER: pldr01  
 PEN TABLE: standard-trans.tbl

**HRGreen.com**  
 Illinois Professional Design Firm  
 # 184-001322

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PLOT SCALE = N.T.S.	DRAWN - DMS	REVISED - BUR, TRAFFIC 1-2-04
PLOT DATE = 12/5/2013	CHECKED - TEH	REVISED -
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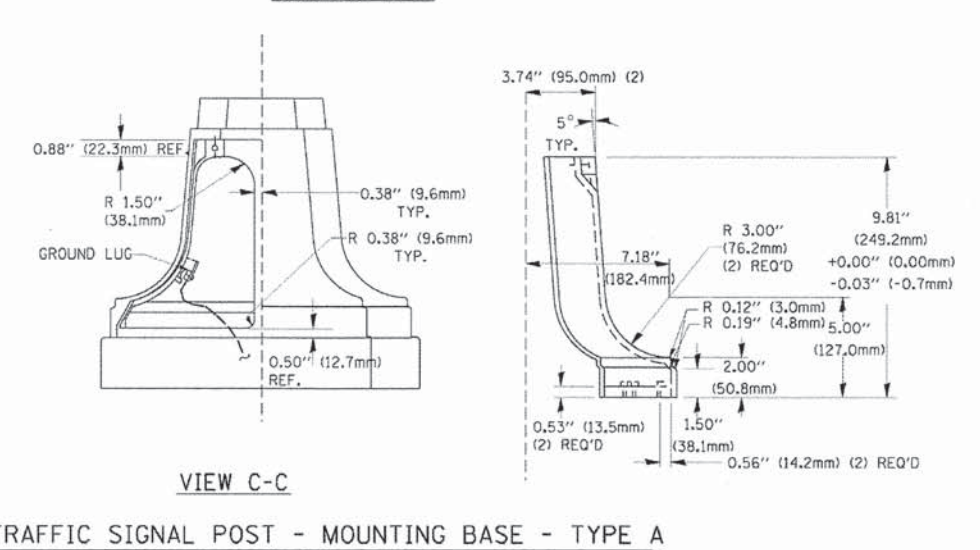
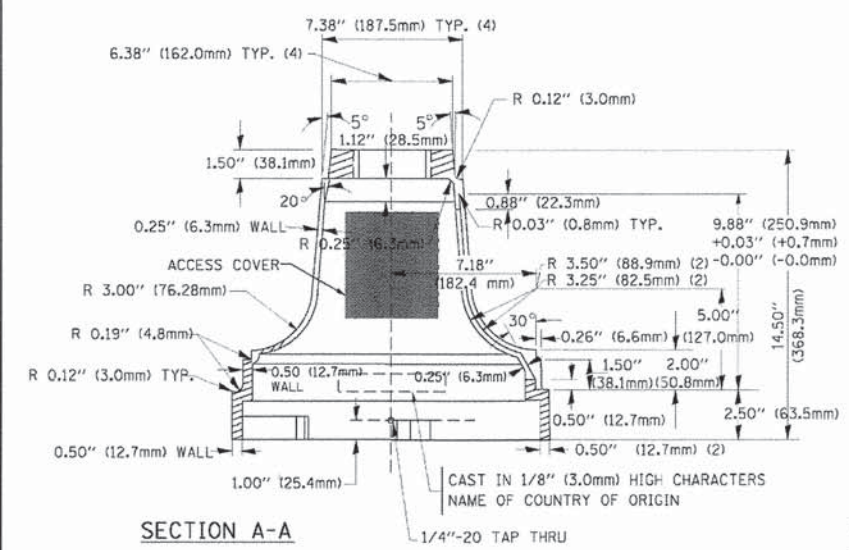
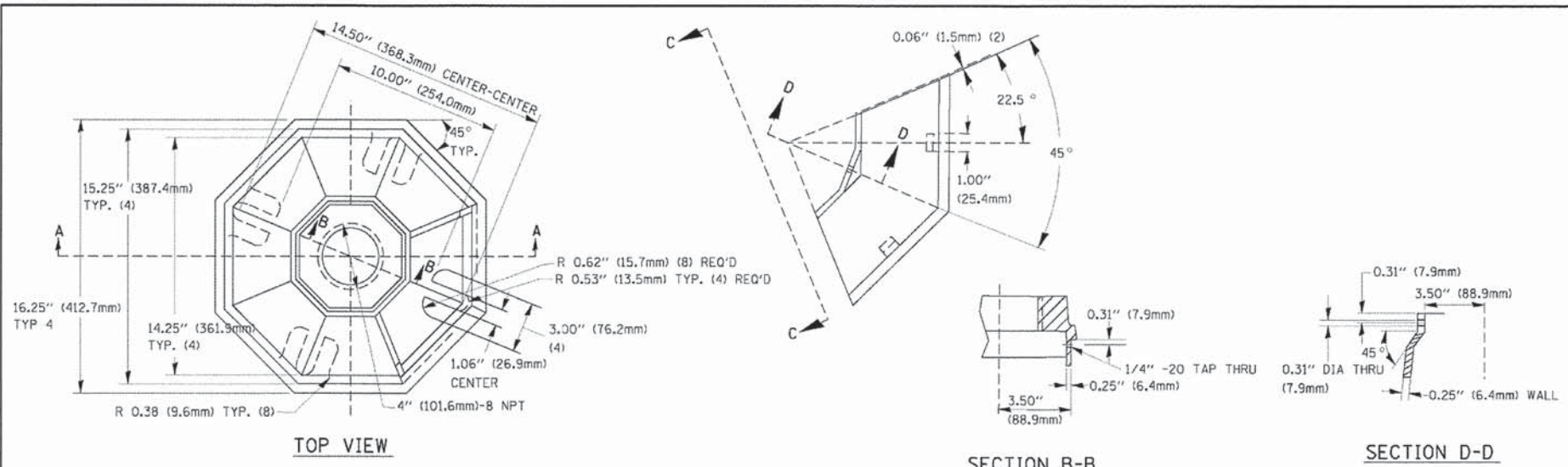
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE**  
**STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

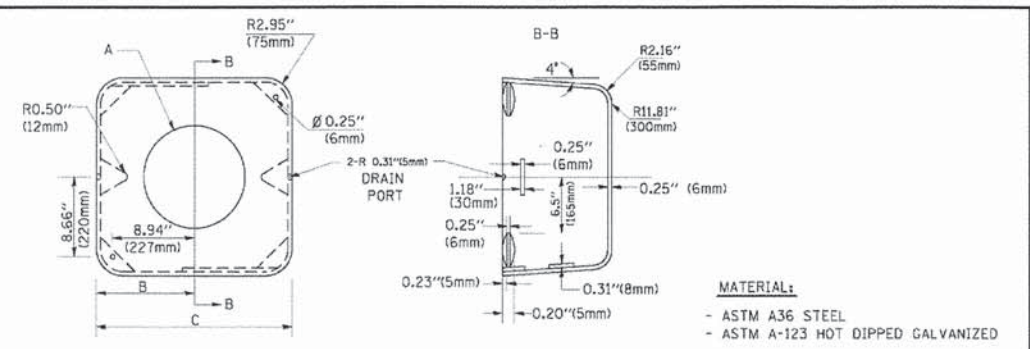
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	29
CONTRACT NO.				63886
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				





VIEW C-C  
TRAFFIC SIGNAL POST - MOUNTING BASE - TYPE A

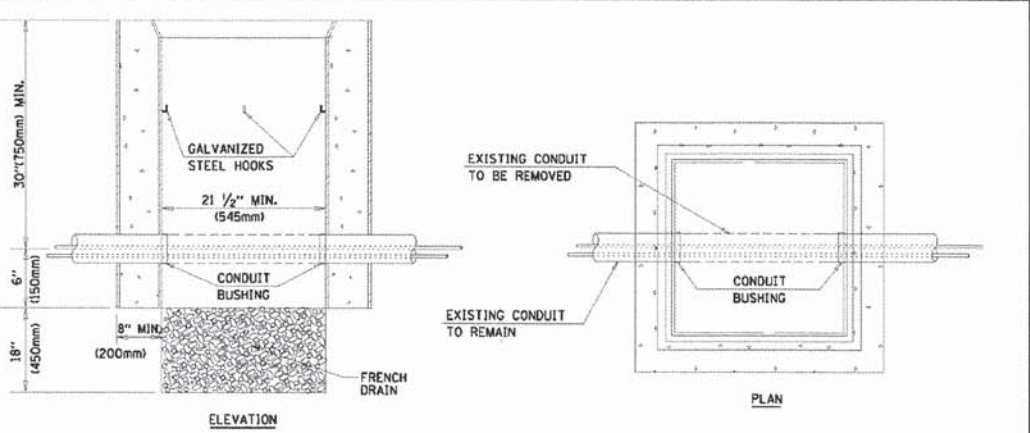
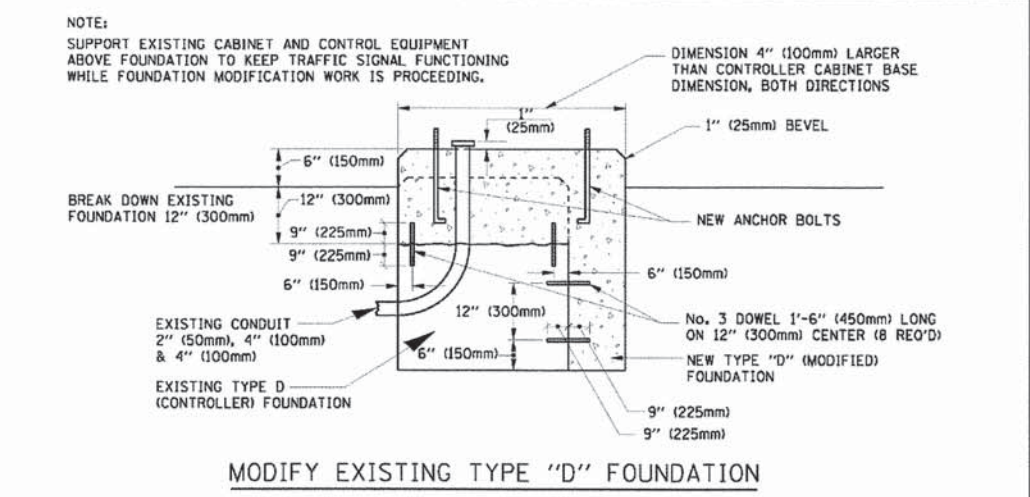


A	B	C	HEIGHT	WEIGHT
VARIES	9.5\"(241mm)	19\"(483mm)	7\"(178mm) - 12\"(300mm)	53 lbs (24kg)
VARIES	10.75\"(273mm)	21.5\"(546mm)	7\"(178mm) - 12\"(300mm)	68 lbs (31 kg)
VARIES	13.0\"(330mm)	26\"(660mm)	7\"(178mm) - 12\"(300mm)	81 lbs (37 kg)
VARIES	18.5\"(470mm)	37\"(940mm)	7\"(178mm) - 12\"(300mm)	126 lbs (57 kg)

**SHROUD**

NOTES:

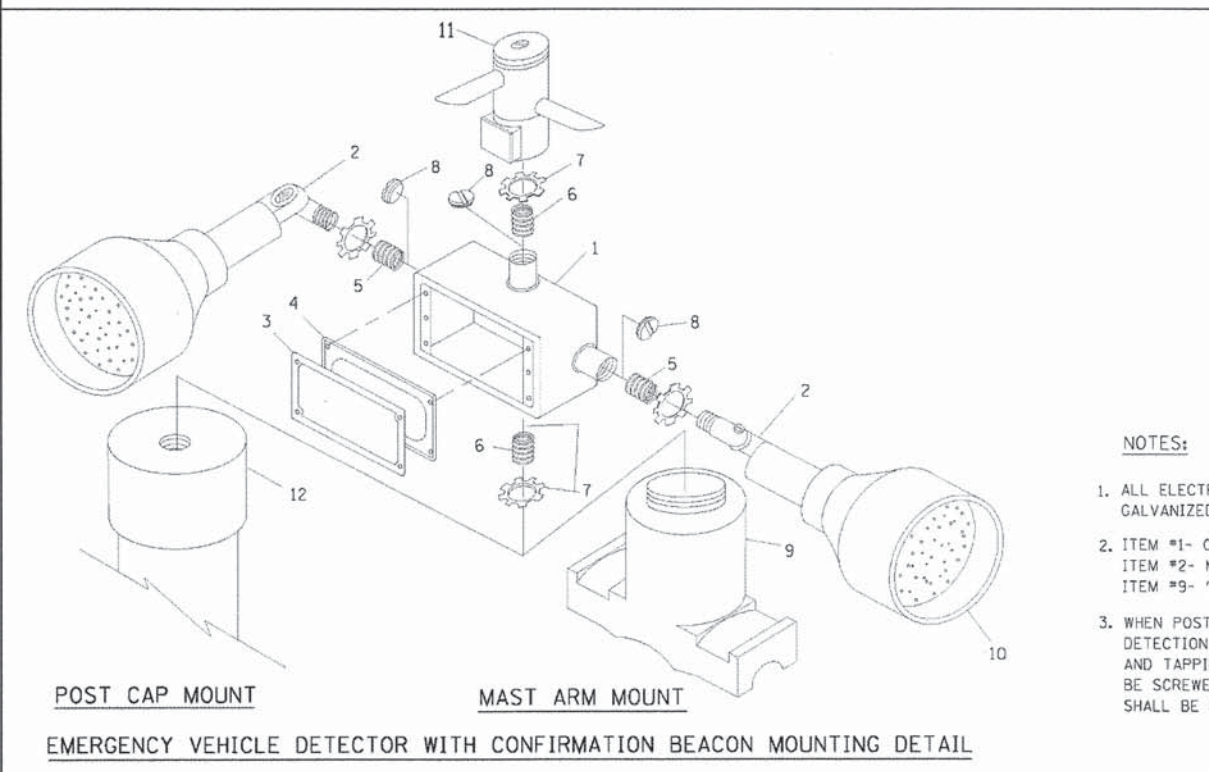
- DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
- THE SUPPLIER SHALL VERIFY THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
- THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.



**HANDHOLE TO INTERCEPT EXISTING CONDUIT**

NOTES:

- HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
- REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCIDENTAL TO THE HANDHOLE.

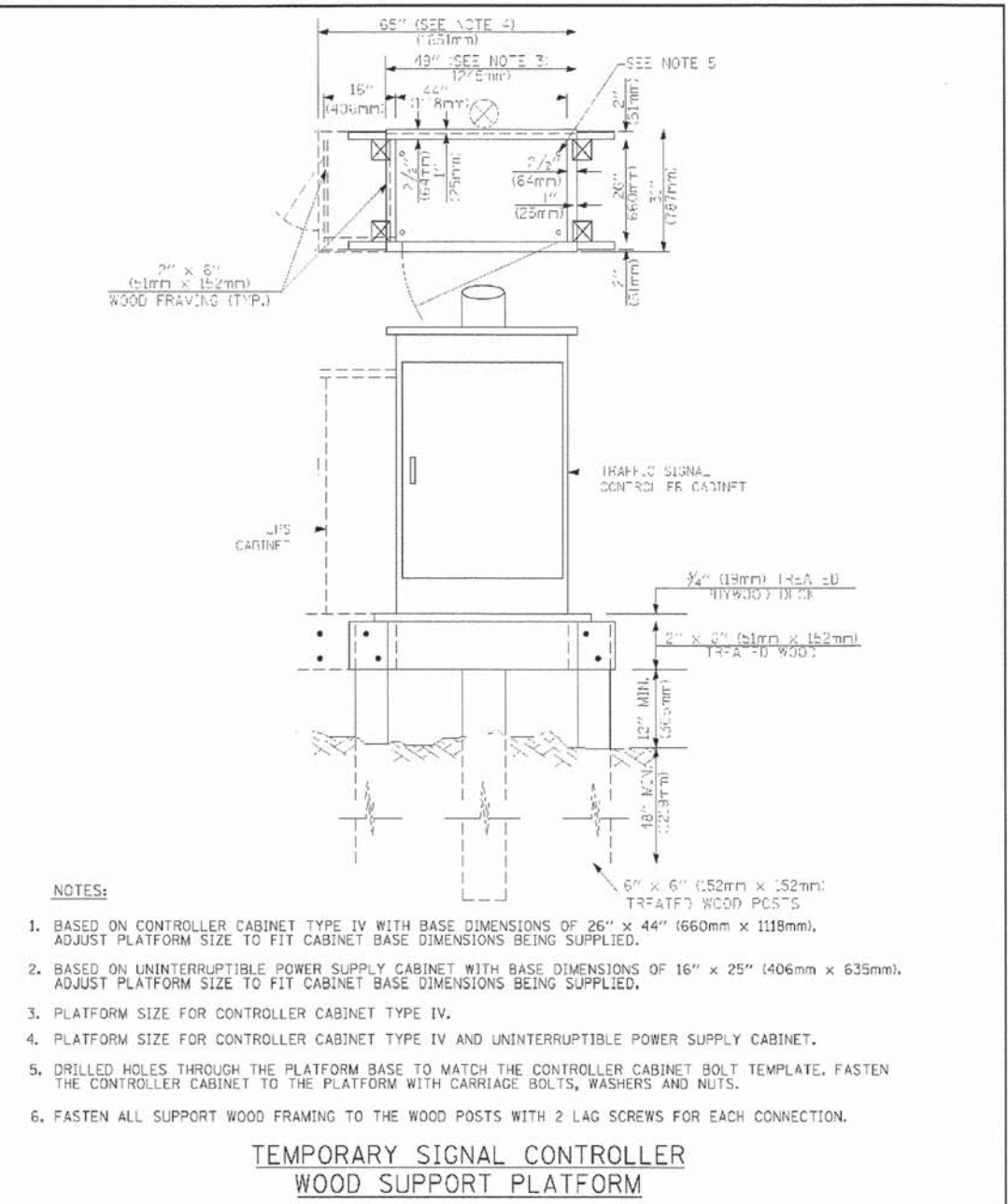
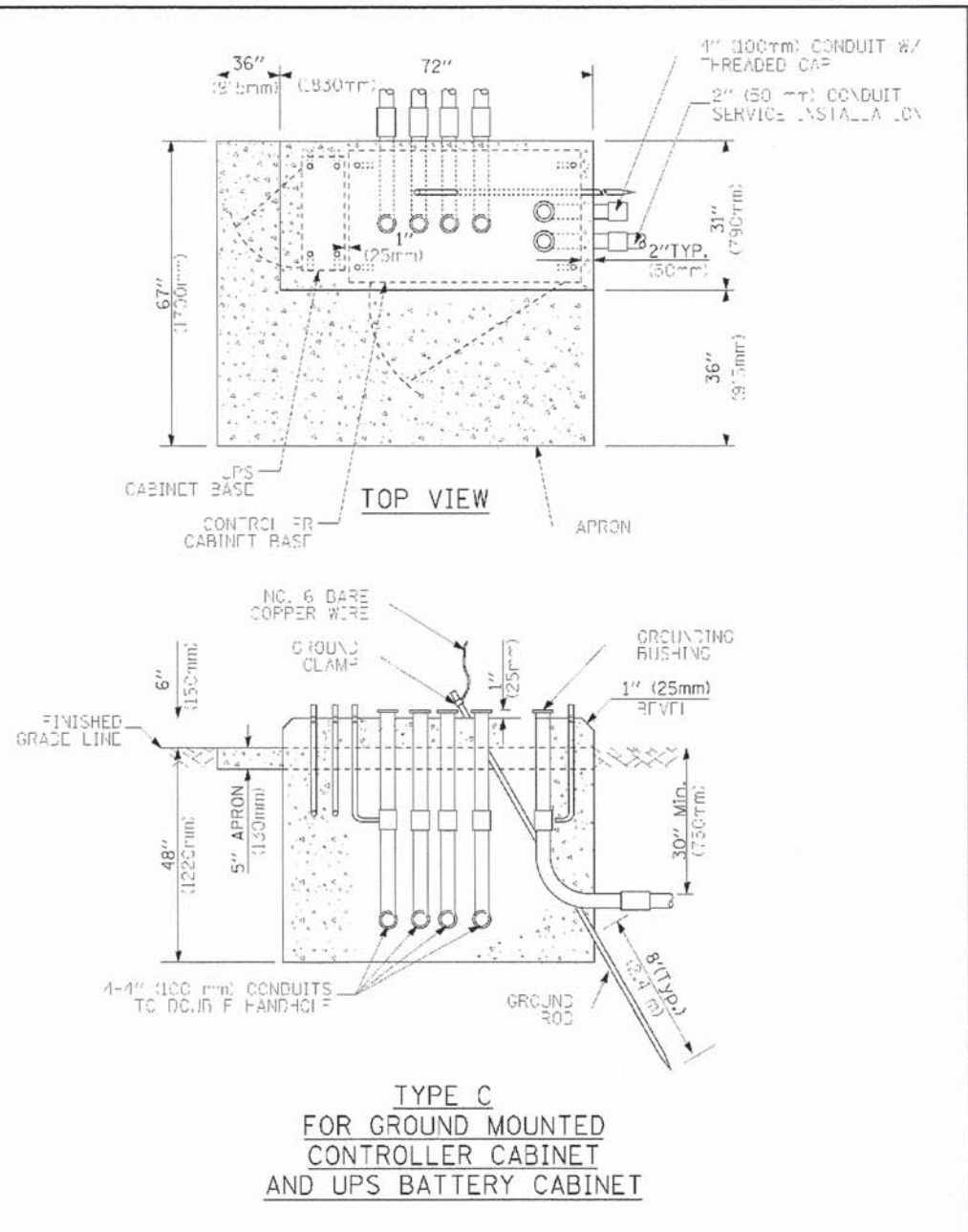
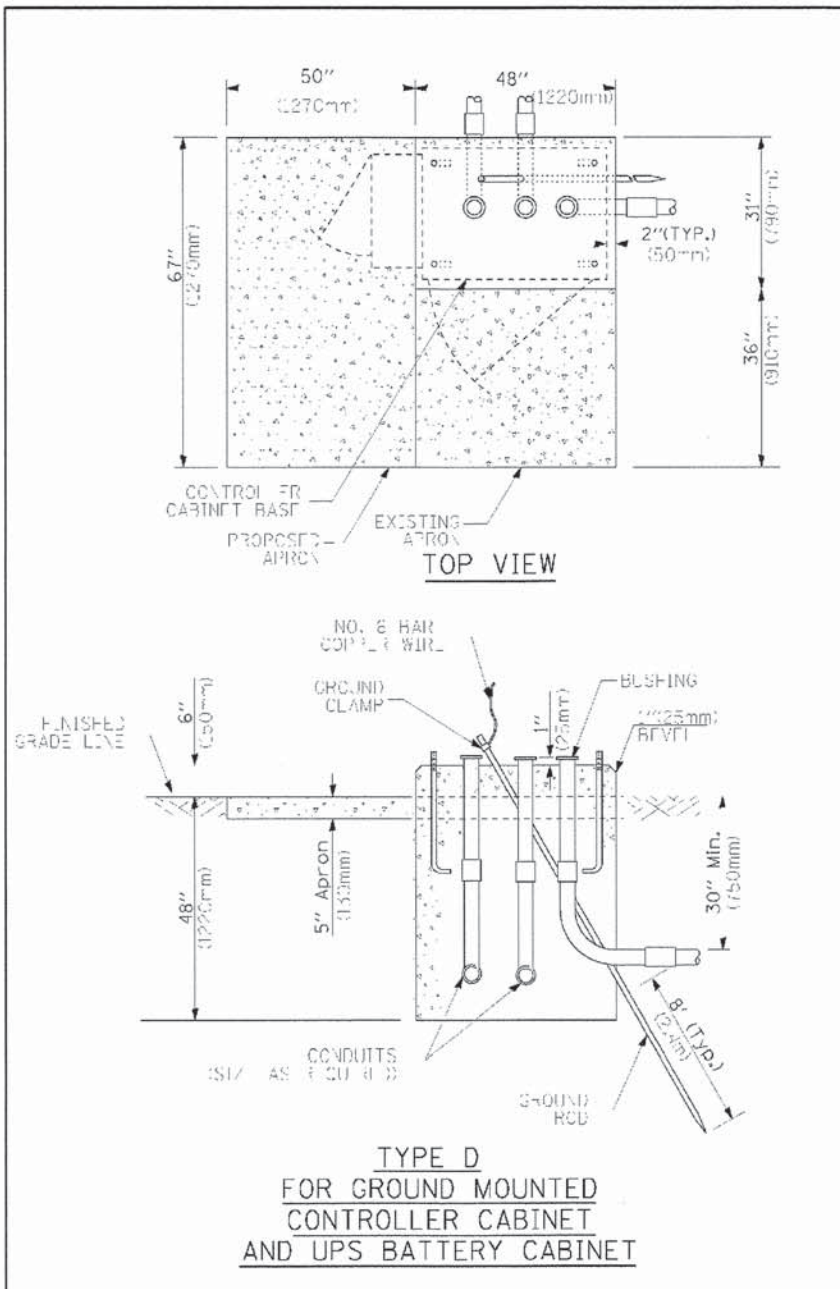


ITEM NO.	IDENTIFICATION
1	OUTLET BOX - GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4\"(19 mm) CLOSE NIPPLE
7	3/4\"(19 mm) LOCKNUT
8	3/4\"(19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

NOTES:

- ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
- ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT  
ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT  
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
- WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTOR UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4\"(19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.





- NOTES:**
1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm), ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
  2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm), ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
  3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
  4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
  5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE, FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
  6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

**TEMPORARY SIGNAL CONTROLLER WOOD SUPPORT PLATFORM**

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

**CABLE SLACK**

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

**VERTICAL CABLE LENGTH**

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

**DEPTH OF FOUNDATION**

MAST ARM LENGTH	FOUNDATION DEPTH	FOUNDATION DIAMETER	SPIRAL DIAMETER	QUANTITY OF REBARS	SIZE OF REBARS
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 55' (16.8 m) and less than 65' (19.8 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

- NOTES:**
1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
  2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
  3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
  4. For mast arm assemblies with dual arms refer to state standard 878001.

**DEPTH OF MAST ARM FOUNDATIONS, TYPE E**

COMPANY NAME: HRGreen.com  
 PROJECT CONTACT: bhar@hrgreen.com  
 CLIENT: ILLINOIS DEPARTMENT OF TRANSPORTATION  
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 FILE NAME: 86120253-sig-det106.dgn  
 PLOT DATE: 12/25/2013  
 PLOT SCALE: N.T.S.  
 PLOT DATE: 12/25/2013



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PLOT SCALE: N.T.S.	DRAWN: DMS	REVISED: BUR, TRAFFIC 1-2-02
PLOT DATE: 12/25/2013	CHECKED: TEH	REVISED: -
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**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

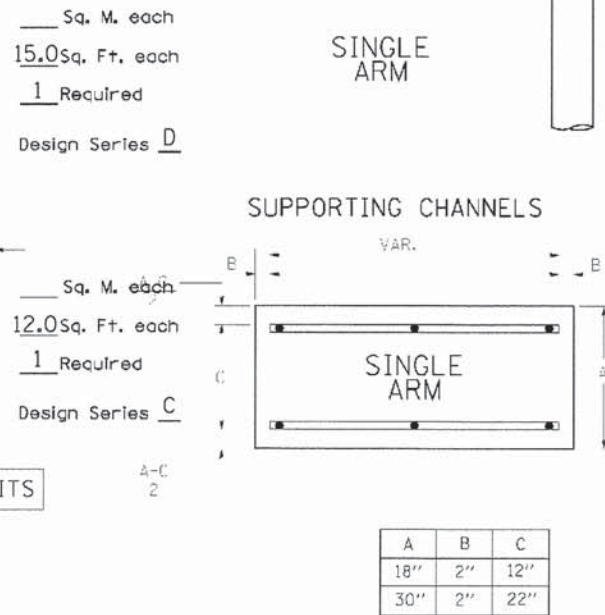
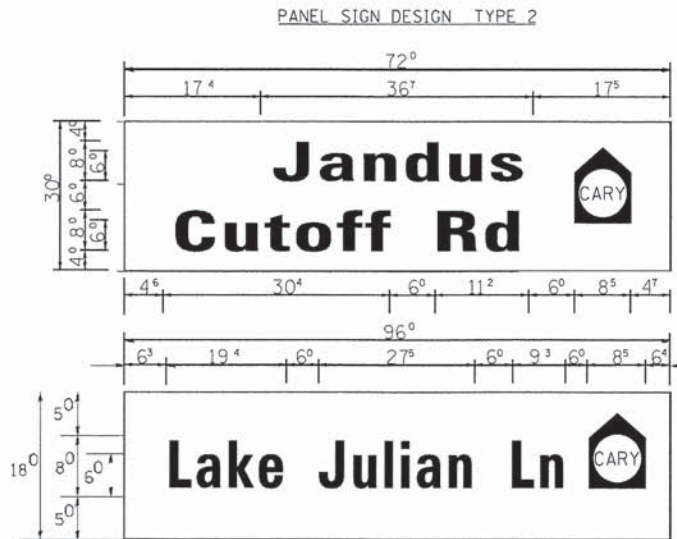
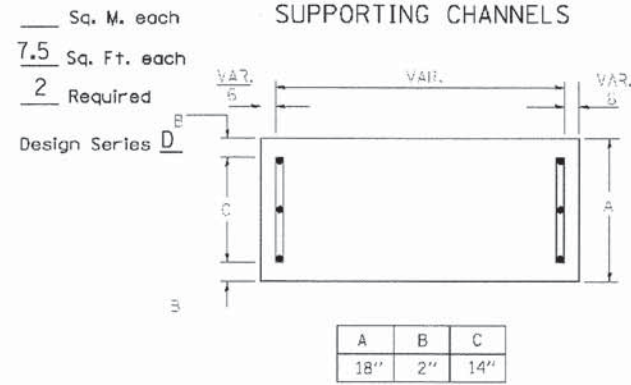
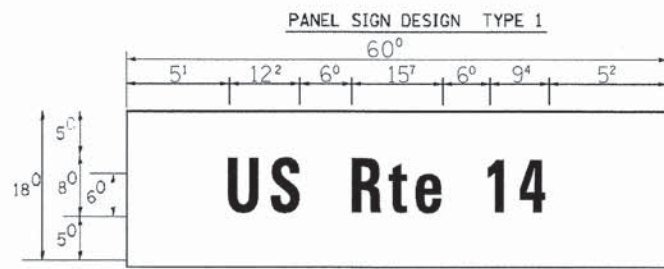
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						CONTRACT NO. 63886	
						FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT	



# TRAFFIC SIGNAL LEGEND

ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED	ITEM	REMOVAL	EXISTING	PROPOSED
CONTROLLER CABINET				EMERGENCY VEHICLE LIGHT DETECTOR				ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 1/C, UNLESS NOTED OTHERWISE			
RAILROAD CONTROL CABINET				CONFIRMATION BEACON				COAXIAL CABLE			
COMMUNICATIONS CABINET				HANDHOLE				VENDOR CABLE FOR CAMERA			
MASTER CONTROLLER		LVC		HEAVY DUTY HANDHOLE				COPPER INTERCONNECT CABLE, NO. 18 3 PAIR TWISTED, SHIELDED			
MASTER MASTER CONTROLLER		FMM		DOUBLE HANDHOLE				FIBER OPTIC CABLE NO. 62.5/125, MM12F			
UNINTERRUPTIBLE POWER SUPPLY		FUPS		JUNCTION BOX				FIBER OPTIC CABLE NO. 62.5/125, MM12F 5M12F			
SERVICE INSTALLATION, (P) POLE OR (G) GROUND MOUNT				GALVANIZED STEEL CONDUIT IN TRENCH (T) OR PUSHED (P)				FIBER OPTIC CABLE NO. 62.5/125, MM12F 5M12F			
TELEPHONE CONNECTION (P) POLE OR (G) GROUND MOUNT				TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE				FIBER OPTIC CABLE NO. 62.5/125, (NUMBER OF FIBERS & TYPE TO BE NOTED ON PLANS)			
STEEL MAST ARM ASSEMBLY AND POLE				COMMON TRENCH				GROUND ROD AT (C) CONTROLLER, (H) HANDHOLE, (P) POST, (M) MAST ARM, OR (S) SERVICE			
ALUMINUM MAST ARM ASSEMBLY AND POLE				COILABLE NONMETALLIC CONDUIT (EMPTY)				CONTROLLER CABINET AND FOUNDATION TO BE REMOVED			
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE				SYSTEM ITEM		S		STEEL MAST ARM POLE AND FOUNDATION TO BE REMOVED			
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH PTZ CAMERA				INTERSECTION ITEM		I		ALUMINUM MAST ARM POLE AND FOUNDATION TO BE REMOVED			
SIGNAL POST				REMOVE ITEM	R			STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE AND FOUNDATION TO BE REMOVED			
TEMPORARY WOOD POLE (CLASS 5 OR BETTER) 45 FOOT (13.7m) MINIMUM				RELOCATE ITEM	HI			SIGNAL POST AND FOUNDATION TO BE REMOVED			
GUY WIRE				ABANDON ITEM	A			INTERSECTION & SAMPLING (SYSTEM) DETECTOR			
SIGNAL HEAD				12" (300mm) TRAFFIC SIGNAL SECTION				SAMPLING (SYSTEM) DETECTOR			
SIGNAL HEAD CONSTRUCTION STAGES (NUMBERS INDICATE THE CONSTRUCTION STAGE)				12" (300mm) RED WITH 8" (200mm) YELLOW AND GREEN TRAFFIC SIGNAL FACE				EXISTING INTERSECTION LOOP DETECTOR PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
SIGNAL HEAD WITH BACKPLATE				SIGNAL FACE				EXISTING PREFORMED INTERSECTION LOOP DETECTOR PROPOSED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
SIGNAL HEAD OPTICALLY PROGRAMMED				SIGNAL FACE WITH BACKPLATE, "P" INDICATES PROGRAMMED HEAD				PREFORMED INTERSECTION AND SAMPLING (SYSTEM) DETECTOR			
FLASHER INSTALLATION (S DENOTES SOLAR POWER)				12" (300mm) PEDESTRIAN SIGNAL HEAD WALK/DON'T WALK SYMBOL				PREFORMED SAMPLING (SYSTEM) DETECTOR			
PEDESTRIAN SIGNAL HEAD				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, OUTLINED				<b>RAILROAD SYMBOLS</b>			
PEDESTRIAN PUSHBUTTON DETECTOR				12" (300mm) PEDESTRIAN SIGNAL HEAD INTERNATIONAL SYMBOL, SOLID				EXISTING			PROPOSED
ACCESSIBLE PEDESTRIAN PUSHBUTTON DETECTOR				PEDESTRIAN SIGNAL HEAD, INTERNATIONAL SYMBOL, WITH COUNTDOWN TIMER				RAILROAD CONTROL CABINET			
ILLUMINATED SIGN "NO LEFT TURN"				RADIO INTERCONNECT				RAILROAD CANTILEVER MAST ARM			
ILLUMINATED SIGN "NO RIGHT TURN"				RADIO REPEATER				FLASHING SIGNAL			
DETECTOR LOOP, TYPE I				DENOTES NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE, ALL DETECTOR LOOP CABLE TO BE SHIELDED				CROSSING GATE			
PREFORMED DETECTOR LOOP				GROUND CABLE IN CONDUIT NO. 6 SOLID COPPER (GREEN)				CROSSBUCK			
MICROWAVE VEHICLE SENSOR											
VIDEO DETECTION CAMERA											
VIDEO DETECTION ZONE											
PAN, TILT, ZOOM CAMERA											
WIRELESS DETECTOR SENSOR											
WIRELESS ACCESS POINT											





NOTE: SIGN DIMENSIONS ARE IN ENGLISH UNITS

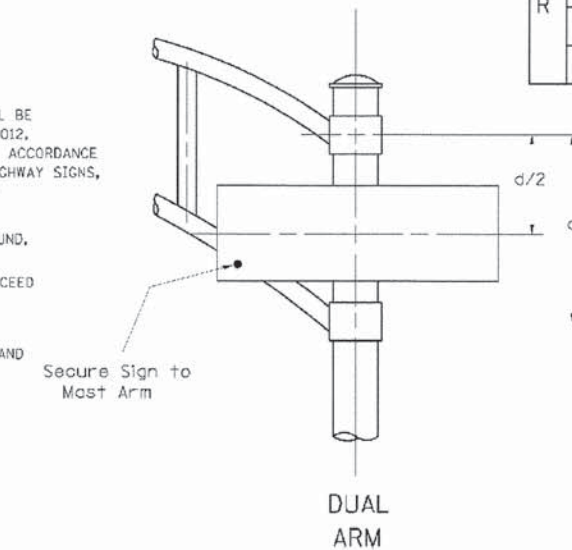
GENERAL NOTES

- WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 877001, 877002, 877006, 877011 AND 877012, AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" x 8'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
- ALL SIGNS SHALL HAVE A WHITE REFLECTORIZED LEGEND AND BORDER ON A GREEN REFLECTORIZED BACKGROUND, TYPE A SHEETING.
- THE SIGN LENGTH SHOULD BE INCREASED IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHOULD NOT EXCEED 8'-0".
- ALL BORDERS SHALL BE 3/4" WIDE AND CORNER RADIUS SHALL BE 2-1/4".
- SIGNIFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS. LOCAL SUPPLIERS OF THE SIGNIFIX ALUMINUM CHANNEL FRAMING SYSTEM ARE:

\* J.O. HERBERT CO. MIDLOTHIAN, VA. \* WESTERN REMAC INC. WOODRIDGE, IL.

PARTS LISTING:  
SIGN CHANNEL PART #HPN053 (MED. CHANNEL)  
SIGN SCREWS 1/4" x 14 x 1" H.W.H. #3  
SELF TAPPING WITH NEOPRENE WASHER  
BRACKETS PART #HPN034 (UNIVERSAL)  
CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING

OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BACKET OF THE ABOVE PRODUCT.



SIGNIFIX ALUMINUM CHANNEL FRAMING SYSTEM shall be used. See Note #5.

Upper Case To Lower Case  
Spacing Chart 8-6 Inch Series "C & D"

SERIES	SECOND LETTER																	
	acde		goq		bhikl		mnp ru		f w		j		s t		v y		x z	
	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D
A W X	12	14	14	15	12	14	06	10	11	14	06	10	11	12	12	14		
B	14	15	20	21	14	15	11	12	14	15	12	14	12	14	16	17		
C E G	14	15	20	21	12	14	06	10	12	14	12	14	14	15	14	15		
D O Q R	14	15	20	21	14	15	06	10	12	14	12	14	14	15	14	15		
F	05	06	14	15	06	10	05	06	06	10	06	10	06	10	11	12		
H I M N	20	21	22	24	20	21	14	15	16	17	16	17	20	21	20	21		
J U	20	21	20	21	16	17	14	15	16	17	16	17	16	17	20	21		
K L	11	12	16	17	11	12	05	06	11	12	11	12	11	12	12	14		
P	12	14	14	15	12	14	05	06	11	12	11	12	12	14	12	14		
S	12	14	16	17	12	14	06	10	12	14	12	14	12	14	12	14		
T	11	12	16	17	06	10	06	10	11	12	11	12	11	12	12	14		
V	06	10	14	15	11	12	06	10	12	14	12	14	12	14	12	14		
Y	05	06	14	15	06	10	05	06	05	07	05	06	06	10	11	12		
Z	16	17	22	24	16	17	12	14	16	17	16	17	16	17	20	21		

Lower Case To Lower Case  
Spacing Chart 6 Inch Series "C & D"

SERIES	SECOND LETTER																	
	acde		goq		bhikl		mnp ru		f w		j		s t		v y		x z	
	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D
ad h g i j	16	17	22	24	16	17	12	14	14	15	14	15	16	17	16	17		
l m n q u																		
b f k o p s	12	14	16	17	11	12	05	06	11	12	11	12	12	14	12	14		
c e	12	14	16	17	12	14	06	10	12	14	12	14	12	14	12	14		
r	06	10	12	14	06	10	03	03	05	06	05	06	06	10	06	10		
t z	12	14	16	17	12	14	06	10	11	12	11	12	12	14	12	14		
v y	11	12	14	15	11	12	05	06	06	10	06	10	11	12	11	12		
w	11	12	14	15	11	12	05	06	11	12	11	12	11	12	12	14		
x	12	14	16	17	11	12	05	06	11	12	11	12	11	12	12	14		

Number To Number  
Spacing Chart 8 Inch Series "C & D"

SERIES	SECOND NUMBER																			
	0		1		2		3		4		5		6		7		8		9	
	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D	C	D
0 9	16	17	16	17	14	15	12	14	14	15	14	15	16	17	12	14	16	17	16	17
1	20	21	20	21	20	21	16	17	14	15	20	21	20	21	14	15	20	21	20	21
2 3 4	14	15	14	15	14	15	12	14	12	14	14	15	14	15	11	12	16	17	14	15
5	14	15	14	15	14	15	11	12	11	12	14	15	14	15	11	12	14	15	14	15
6	16	17	14	15	14	15	12	15	12	14	14	15	14	15	11	12	14	15	14	15
7	12	14	12	14	14	15	12	15	05	06	12	14	14	15	11	12	14	15	12	14
8	16	17	16	17	14	15	12	15	12	14	14	15	16	17	12	14	16	17	14	15

EXAMPLE, 2<sup>3</sup> DENOTES 3"

UPPER AND LOWER CASE  
LETTER WIDTHS

LETTERS	6 INCH UPPER CASE LETTERS				8 INCH UPPER CASE LETTERS				LETTERS	6 INCH LOWER CASE LETTERS	
	SERIES		SERIES		SERIES		SERIES			SERIES	
	C	D	C	D	C	D	C	D		C	D
A	3 <sup>6</sup>	5 <sup>0</sup>	5 <sup>0</sup>	6 <sup>5</sup>	a	3 <sup>5</sup>	4 <sup>2</sup>				
B	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	b	3 <sup>5</sup>	4 <sup>2</sup>				
C	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	c	3 <sup>5</sup>	4 <sup>1</sup>				
D	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	d	3 <sup>5</sup>	4 <sup>2</sup>				
E	3 <sup>0</sup>	3 <sup>5</sup>	4 <sup>0</sup>	4 <sup>7</sup>	e	3 <sup>5</sup>	4 <sup>2</sup>				
F	3 <sup>0</sup>	3 <sup>5</sup>	4 <sup>0</sup>	4 <sup>7</sup>	f	2 <sup>3</sup>	2 <sup>6</sup>				
G	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	g	3 <sup>5</sup>	4 <sup>2</sup>				
H	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	h	3 <sup>5</sup>	4 <sup>2</sup>				
I	0 <sup>7</sup>	0 <sup>7</sup>	1 <sup>1</sup>	1 <sup>2</sup>	i	1 <sup>1</sup>	1 <sup>1</sup>				
J	3 <sup>0</sup>	3 <sup>6</sup>	4 <sup>0</sup>	5 <sup>0</sup>	j	2 <sup>0</sup>	2 <sup>2</sup>				
K	3 <sup>2</sup>	4 <sup>1</sup>	4 <sup>3</sup>	5 <sup>4</sup>	k	3 <sup>5</sup>	4 <sup>2</sup>				
L	3 <sup>0</sup>	3 <sup>5</sup>	4 <sup>0</sup>	4 <sup>7</sup>	l	1 <sup>1</sup>	1 <sup>1</sup>				
M	3 <sup>7</sup>	4 <sup>5</sup>	5 <sup>1</sup>	6 <sup>1</sup>	m	6 <sup>0</sup>	7 <sup>0</sup>				
N	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	n	3 <sup>5</sup>	4 <sup>2</sup>				
O	3 <sup>4</sup>	4 <sup>2</sup>	4 <sup>5</sup>	5 <sup>5</sup>	o	3 <sup>6</sup>	4 <sup>3</sup>				
P	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	p	3 <sup>5</sup>	4 <sup>2</sup>				
Q	3 <sup>4</sup>	4 <sup>2</sup>	4 <sup>5</sup>	5 <sup>5</sup>	q	3 <sup>5</sup>	4 <sup>2</sup>				
R	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	r	2 <sup>6</sup>	3 <sup>2</sup>				
S	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	s	3 <sup>6</sup>	4 <sup>2</sup>				
T	3 <sup>0</sup>	3 <sup>5</sup>	4 <sup>0</sup>	4 <sup>7</sup>	t	2 <sup>7</sup>	3 <sup>2</sup>				
U	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	u	3 <sup>5</sup>	4 <sup>2</sup>				
V	3 <sup>5</sup>	4 <sup>4</sup>	4 <sup>7</sup>	6 <sup>0</sup>	v	4 <sup>2</sup>	4 <sup>7</sup>				
W	4 <sup>4</sup>	5 <sup>2</sup>	6 <sup>0</sup>	7 <sup>0</sup>	w	5 <sup>5</sup>	6 <sup>4</sup>				
X	3 <sup>4</sup>	4 <sup>0</sup>	4 <sup>5</sup>	5 <sup>3</sup>	x	4 <sup>4</sup>	5 <sup>1</sup>				
Y	3 <sup>6</sup>	5 <sup>0</sup>	5 <sup>0</sup>	6 <sup>6</sup>	y	4 <sup>6</sup>	5 <sup>3</sup>				
Z	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>	z	3 <sup>6</sup>	4 <sup>3</sup>				

NUMBER	6 INCH SERIES		8 INCH SERIES	
	C	D	C	D
1	1 <sup>2</sup>	1 <sup>4</sup>	1 <sup>5</sup>	2 <sup>0</sup>
2	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>
3	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>
4	3 <sup>5</sup>	4 <sup>3</sup>	4 <sup>7</sup>	5 <sup>7</sup>
5	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>
6	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>
7	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>
8	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>
9	3 <sup>2</sup>	4 <sup>0</sup>	4 <sup>3</sup>	5 <sup>3</sup>
0	3 <sup>4</sup>	4 <sup>2</sup>	4 <sup>5</sup>	5 <sup>5</sup>

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PLOT DATE = 12/5/2013	CHECKED - TEH	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE  
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

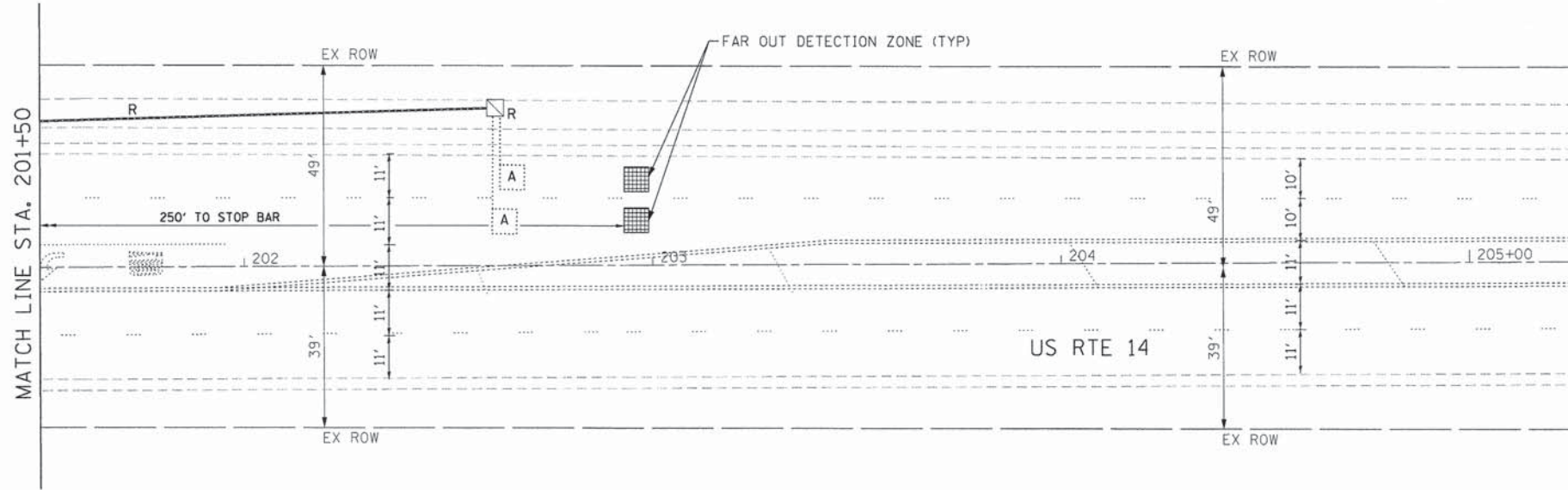
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CONTRACT NO. 63886				
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				

SCALE: N.T.S. SHEET NO. 7 OF 22 SHEETS STA. TO STA.

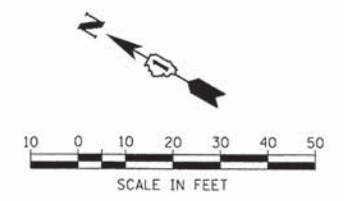








**LEGEND:**  
 DETECTION ZONE



COMPANY NAME: HRGreen  
 PROJECT CONTACT: bhar@hrgreen.com  
 CLIENT: ILLINOIS DEPARTMENT OF TRANSPORTATION  
 FILE NAME: 13-00063-00-PV-02.dgn  
 PLOT DRIVER: pldrvr  
 PEN TABLE: standard-trans.tbl



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PLOT DATE = 12/5/2013	CHECKED - TEH	REVISED -
	DATE -	REVISED -

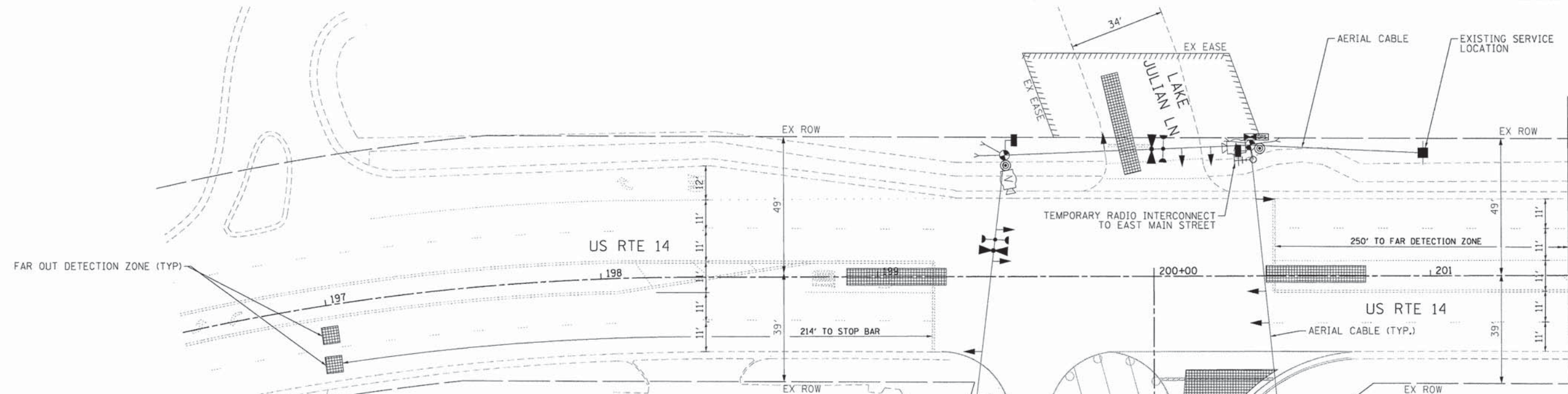
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION AND REMOVE  
 EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN-STAGE 1  
 US RTE 14 AT JANDUS CUT OFF ROAD**

SCALE: 1" = 20' SHEET NO. 9 OF 22 SHEETS STA. 201+50 TO STA. END

F.A. RTE. 4058	SECTION 13-00063-00-PV	COUNTY MCHENRY	TOTAL SHEETS 79	SHEET NO. 35
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 63886	





MATCH LINE STA. 201+50

**NOTES FOR TEMPORARY TRAFFIC SIGNALS**

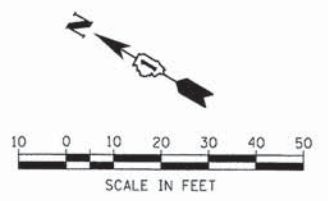
1. ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
2. ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1, INSTALLED IN A NEMA TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
3. ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE LED AND 12" (300mm) DIAMETER. HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. PEDESTRIAN SIGNALS SHALL INCLUDE SOLID INTERNATIONAL SYMBOLS. PEDESTRIAN SIGNALS WITH COUNTDOWN TIMERS SHALL BE USED WHEN THE EXISTING INSTALLATION UTILIZES COUNTDOWN TYPE OR AS DIRECTED BY THE ENGINEER. COUNTDOWN TYPE PEDESTRIAN SIGNALS ARE NOT TO BE INSTALLED AT A RAILROAD INTERSECTION. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
4. ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
5. ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
6. THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL, AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
7. UNINTERRUPTABLE POWER SUPPLY (UPS) SYSTEMS SHALL BE INSTALLED AND MADE OPERATIONAL AT TEMPORARY TRAFFIC SIGNAL INSTALLATIONS WHERE UPS IS INSTALLED AT THE EXISTING TRAFFIC SIGNAL, TEMPORARY TRAFFIC SIGNALS AT RAILROAD INTERSECTIONS, AND TEMPORARY TRAFFIC SIGNALS AT INTERSECTIONS WITH FIRE STATION ACTUATED EMERGENCY VEHICLE PRE-EMPTION, OR WHEN INDICATED ON THE PLANS.
8. TRAFFIC SIGNAL MANAGEMENT SYSTEMS SHALL BE MAINTAINED IN OPERATION AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. REQUIRED EQUIPMENT SHALL BE AS SHOWN ON THE PLANS AND THE CONTRACTOR SHALL PLACE THE EQUIPMENT IN OPERATION TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE TRAFFIC SIGNAL MANAGEMENT SYSTEM.
9. DETECTION AT TEMPORARY TRAFFIC SIGNALS SHALL BE INCLUDED FOR ALL APPROACHES OF THE INTERSECTION UNLESS INDICATED OTHERWISE ON THE PLANS. THE DETECTION SYSTEM MUST MEET THE SPECIFICATIONS OF DISTRICT 1 AND THE CONTRACTOR SHALL PLACE THE DETECTORS INTO OPERATION TO THE SATISFACTION OF THE ENGINEER.
10. WHEN PAN, TILT, ZOOM CAMERAS ARE INSTALLED AT THE EXISTING INTERSECTION OR ARE CALLED FOR IN THE PLANS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING AND MAINTAINING THE CAMERAS TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE CAMERAS.

- LEGEND:**
- WORK ZONE
  - DETECTION ZONE
  - PROPOSED STAGE 2 PAVEMENT

- NOTE**
- FINAL DETECTION CAMERA POSITIONS TO BE DETERMINED BY THE CONTRACTOR/VENDOR TO ENSURE CORRECT ZONE COVERAGE, INCLUDING FAR-OUT DETECTION IS WORKING PROPERLY.
- NOTE**
- THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.
- NOTE**
- THE TEMPORARY RADIO INTERCONNECT SHALL NOT BE REMOVED UNTIL FIBER INTERCONNECT IS INSTALLED AND OPERATIONAL.

**RESTORATION OF WORK AREA**

RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAYS SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD AND ALL DAMAGE TO UNMOWED LAWNS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.



COMPANY NAME: HRGreen  
 PROJECT CONTACT: HRGreen.com  
 CLIENT: Illinois Professional Design Firm  
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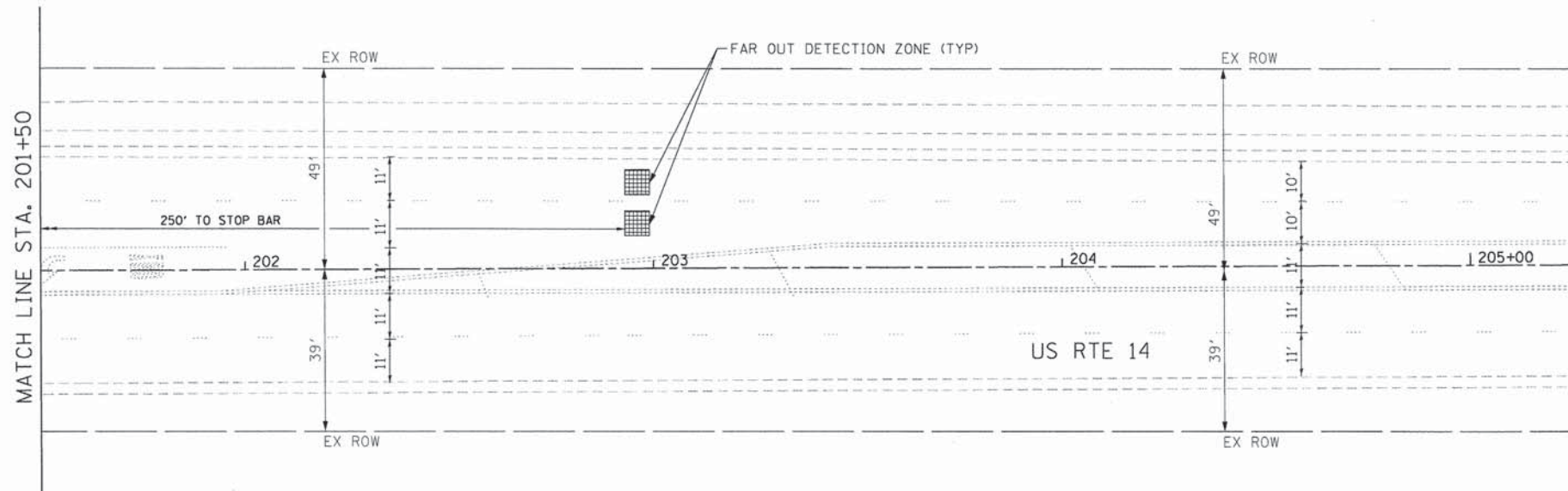
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	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

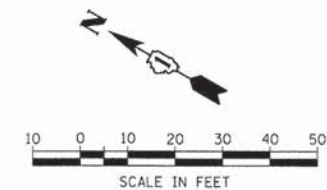
<b>TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN</b>	
<b>STAGE 2</b>	
<b>US RTE 14 AT JANDUS CUT OFF ROAD</b>	
SCALE: 1" = 20'	SHEET NO. 10 OF 22 SHEETS
STA. 196+50	TO STA. 201+50

F.A. RTE. 4058	SECTION 13-00063-00-PV	COUNTY MCHENRY	TOTAL SHEETS 79	SHEET NO. 36
CONTRACT NO. 63886			ILLINOIS FED. AID PROJECT	





LEGEND:  
 DETECTION ZONE



COMPANY NAME: HRGreen  
 PROJECT CONTACT: HRGreen.com  
 CLIENT: Illinois Professional Design Firm  
 DATE PLOTTED: 12/15/2013 8:05:30 AM  
 FILE PLOTTED: 13-00063-00-PV.dgn  
 PLOT DRIVER: bhartha  
 PEN TABLE: standard-trans.tbl



USER NAME = bhartha	DESIGNED - JJS	REVISED -
	DRAWN - DMS	REVISED -
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PLOT DATE = 12/15/2013	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

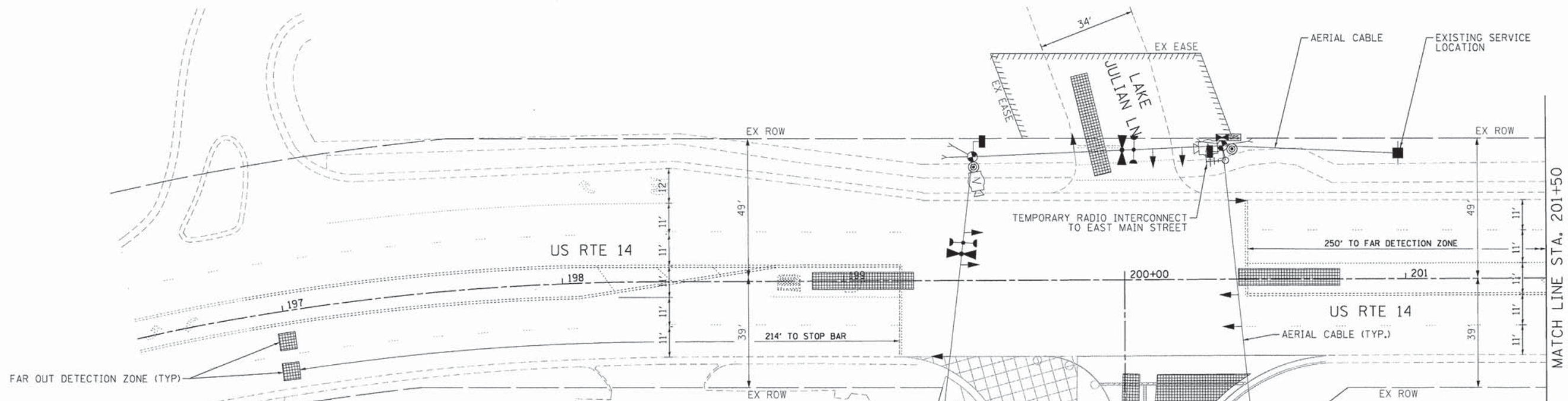
**TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN  
 STAGE 2  
 US RTE 14 AT JANDUS CUT OFF ROAD**

SCALE: 1" = 20' SHEET NO. 11 OF 22 SHEETS STA. 201+50 TO STA. END

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	37
CONTRACT NO.			63886	

FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT





**NOTES FOR TEMPORARY TRAFFIC SIGNALS**

1. ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
2. ONLY CONTROLLERS SUPPLIED BY ONE OF THE DISTRICT APPROVED CLOSED LOOP EQUIPMENT MANUFACTURERS WILL BE APPROVED FOR USE AT TEMPORARY SIGNAL LOCATIONS. ALL CONTROLLERS USED FOR TEMPORARY TRAFFIC SIGNALS SHALL BE FULLY ACTUATED NEMA MICROPROCESSOR BASED WITH RS232 DATA ENTRY PORTS COMPATIBLE WITH EXISTING MONITORING SOFTWARE APPROVED BY IDOT DISTRICT 1, INSTALLED IN A NEMA TS2 CABINET. ONLY ONE BRAND OF CONTROLLER WILL BE ACCEPTED FOR ANY ONE CONTRACT.
3. ALL TRAFFIC SIGNAL SECTIONS AND PEDESTRIAN SIGNAL SECTIONS SHALL BE LED AND 12" (300mm) DIAMETER. HEADS SHALL BE PLACED AS INDICATED ON THE TEMPORARY TRAFFIC SIGNAL PLAN OR AS DIRECTED BY THE ENGINEER. PEDESTRIAN SIGNALS SHALL INCLUDE SOLID INTERNATIONAL SYMBOLS. PEDESTRIAN SIGNALS WITH COUNTDOWN TIMERS SHALL BE USED WHEN THE EXISTING INSTALLATION UTILIZES COUNTDOWN TYPE OR AS DIRECTED BY THE ENGINEER. COUNTDOWN TYPE PEDESTRIAN SIGNALS ARE NOT TO BE INSTALLED AT A RAILROAD INTERSECTION. THE CONTRACTOR SHALL FURNISH ENOUGH CABLE SLACK TO RELOCATE HEADS TO ANY POSITION ON THE SPAN WIRE OR AT LOCATIONS ILLUSTRATED ON THE PLANS FOR CONSTRUCTION STAGING. THE TEMPORARY TRAFFIC SIGNAL SHALL REMAIN IN OPERATION DURING ALL SIGNAL HEAD RELOCATIONS. EACH TEMPORARY TRAFFIC SIGNAL HEAD SHALL HAVE ITS OWN CABLE FROM THE CONTROLLER CABINET TO THE SIGNAL HEAD.
4. ALL EXISTING STREET NAME AND INTERSECTION REGULATORY SIGNS SHALL BE REMOVED FROM EXISTING POLES, RELOCATED AND SECURELY FASTENED TO THE SPAN WIRE OR WOOD POLE AS DIRECTED BY THE ENGINEER.
5. ANY TEMPORARY SIGNAL WITHIN AN EXISTING CLOSED LOOP TRAFFIC SIGNAL SYSTEM SHALL BE INTERCONNECTED TO THAT SYSTEM USING SIMILAR BRAND CONTROL EQUIPMENT.
6. THE TEMPORARY TRAFFIC SIGNAL SHALL HAVE THE SIGNAL HEAD DISPLAYS, SIGNAL HEAD PLACEMENTS AND CONTROLLER PHASING MATCH THE EXISTING TRAFFIC SIGNAL, AT THE TIME OF THE TURN ON, IF NO TRAFFIC STAGING IS IN PLACE OR WILL NOT BE STAGED ON THE DAY OF THE TURN ON.
7. UNINTERRUPTABLE POWER SUPPLY (UPS) SYSTEMS SHALL BE INSTALLED AND MADE OPERATIONAL AT TEMPORARY TRAFFIC SIGNAL INSTALLATIONS WHERE UPS IS INSTALLED AT THE EXISTING TRAFFIC SIGNAL, TEMPORARY TRAFFIC SIGNALS AT RAILROAD INTERSECTIONS, AND TEMPORARY TRAFFIC SIGNALS AT INTERSECTIONS WITH FIRE STATION ACTUATED EMERGENCY VEHICLE PRE-EMPTION, OR WHEN INDICATED ON THE PLANS.
8. TRAFFIC SIGNAL MANAGEMENT SYSTEMS SHALL BE MAINTAINED IN OPERATION AS INDICATED ON THE PLANS OR AS DIRECTED BY THE ENGINEER. REQUIRED EQUIPMENT SHALL BE AS SHOWN ON THE PLANS AND THE CONTRACTOR SHALL PLACE THE EQUIPMENT IN OPERATION TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE TRAFFIC SIGNAL MANAGEMENT SYSTEM.
9. DETECTION AT TEMPORARY TRAFFIC SIGNALS SHALL BE INCLUDED FOR ALL APPROACHES OF THE INTERSECTION UNLESS INDICATED OTHERWISE ON THE PLANS. THE DETECTION SYSTEM MUST MEET THE SPECIFICATIONS OF DISTRICT 1 AND THE CONTRACTOR SHALL PLACE THE DETECTORS INTO OPERATION TO THE SATISFACTION OF THE ENGINEER.
10. WHEN PAN, TILT, ZOOM CAMERAS ARE INSTALLED AT THE EXISTING INTERSECTION OR ARE CALLED FOR IN THE PLANS, THE CONTRACTOR SHALL BE RESPONSIBLE FOR INSTALLING AND MAINTAINING THE CAMERAS TO THE SATISFACTION OF THE ENGINEER AND THE AGENCY RESPONSIBLE FOR THE CAMERAS.

**RESTORATION OF WORK AREA**

RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAYS SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD AND ALL DAMAGE TO UNMOWED LAWNS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

**LEGEND:**

- WORK ZONE
- DETECTION ZONE
- PROPOSED STAGE 3 PAVEMENT

**NOTE**

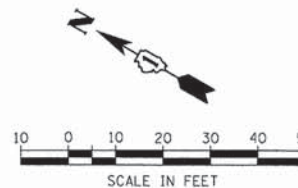
FINAL DETECTION CAMERA POSITIONS TO BE DETERMINED BY THE CONTRACTOR/VENDOR TO ENSURE CORRECT ZONE COVERAGE, INCLUDING FAR-OUT DETECTION IS WORKING PROPERLY.

**NOTE**

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

**NOTE**

THE TEMPORARY RADIO INTERCONNECT SHALL NOT BE REMOVED UNTIL FIBER INTERCONNECT IS INSTALLED AND OPERATIONAL.



COMPANY NAME: HRGreen.com  
 PROJECT CONTACT: HRGreen Professional Design Firm  
 CLIENT: #184-001322  
 DATE PLOTTED: 12/5/2013 3:09:33 AM  
 FILE NAME: 86120253-1stg-ph-sig3-01.dgn  
 PLOT DRIVER: pdf24pr  
 PLOT TABLE: standard-Trans.ctb

HRGreen.com  
 HRGreen Professional Design Firm  
 #184-001322

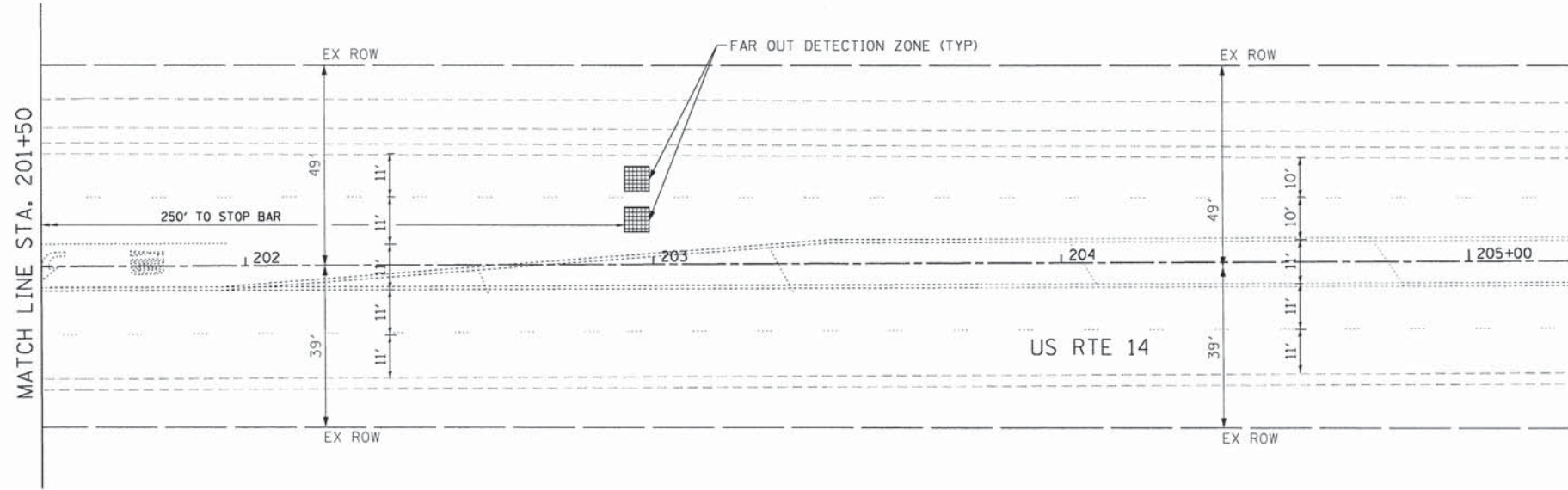
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PLOT DATE = 12/5/2013	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

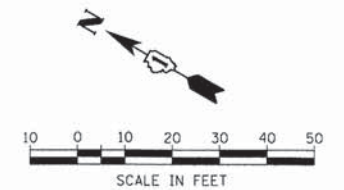
**TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN**  
**STAGE 3**  
**US RTE 14 AT JANDUS CUT OFF ROAD**  
 SCALE: 1" = 20' SHEET NO. 12 OF 22 SHEETS STA. 196+50 TO STA. 201+50

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	38
CONTRACT NO. 63886				
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				





LEGEND:  
 DETECTION ZONE



COMPANY NAME: HRGreen  
 PROJECT CONTACT: HRGreen.com  
 CLIENT: Illinois Professional Design Firm  
 DATE PLOTTED: 12/5/2013 8:05:35 AM  
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 PLOT NUMBER: 13  
 PLOT DATE: 12/5/2013



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PLOT DATE = 12/5/2013	DATE -	REVISED -

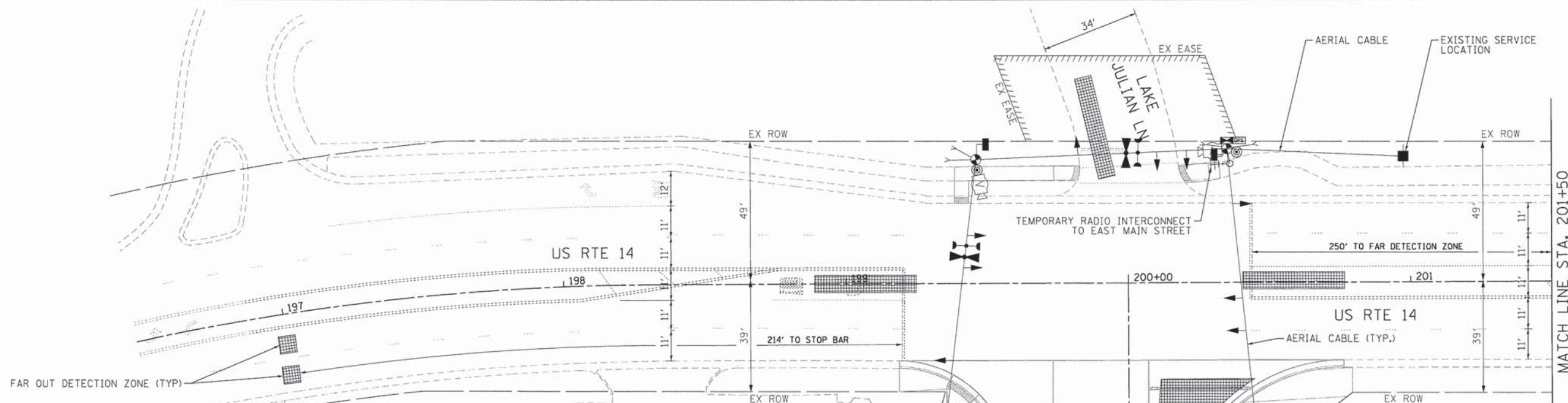
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN  
 STAGE 3  
 US RTE 14 AT JANDUS CUT OFF ROAD**

SCALE: 1" = 20'    SHEET NO. 13 OF 22 SHEETS    STA. 201+50 TO STA. END

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	39
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 63886	





**NOTES FOR TEMPORARY TRAFFIC SIGNALS**

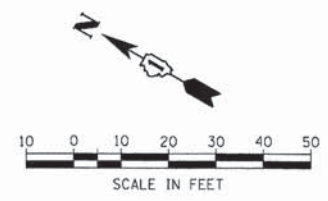
1. ALL CONTROL EQUIPMENT INCLUDING EMERGENCY PRE-EMPTION AND COMMUNICATION DEVICES FOR THE TEMPORARY TRAFFIC SIGNAL(S) SHALL BE FURNISHED BY THE CONTRACTOR.
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- LEGEND:**
- WORK ZONE
  - DETECTION ZONE
  - PROPOSED STAGE 4 PAVEMENT

- NOTE**  
FINAL DETECTION CAMERA POSITIONS TO BE DETERMINED BY THE CONTRACTOR/VENDOR TO ENSURE CORRECT ZONE COVERAGE, INCLUDING FAR-OUT DETECTION IS WORKING PROPERLY.
- NOTE**  
THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.
- NOTE**  
THE TEMPORARY RADIO INTERCONNECT SHALL NOT BE REMOVED UNTIL FIBER INTERCONNECT IS INSTALLED AND OPERATIONAL.

**RESTORATION OF WORK AREA**

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COMPANY NAME: HRGreen  
 PROJECT CONTACT: HRGreen.com  
 CLIENT: Illinois Professional Design Firm  
 DATE PLOTTED: 12/5/2013 9:09:39 AM  
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**HRGreen**  
 HRGreen.com  
 Illinois Professional Design Firm  
 # 184-001322

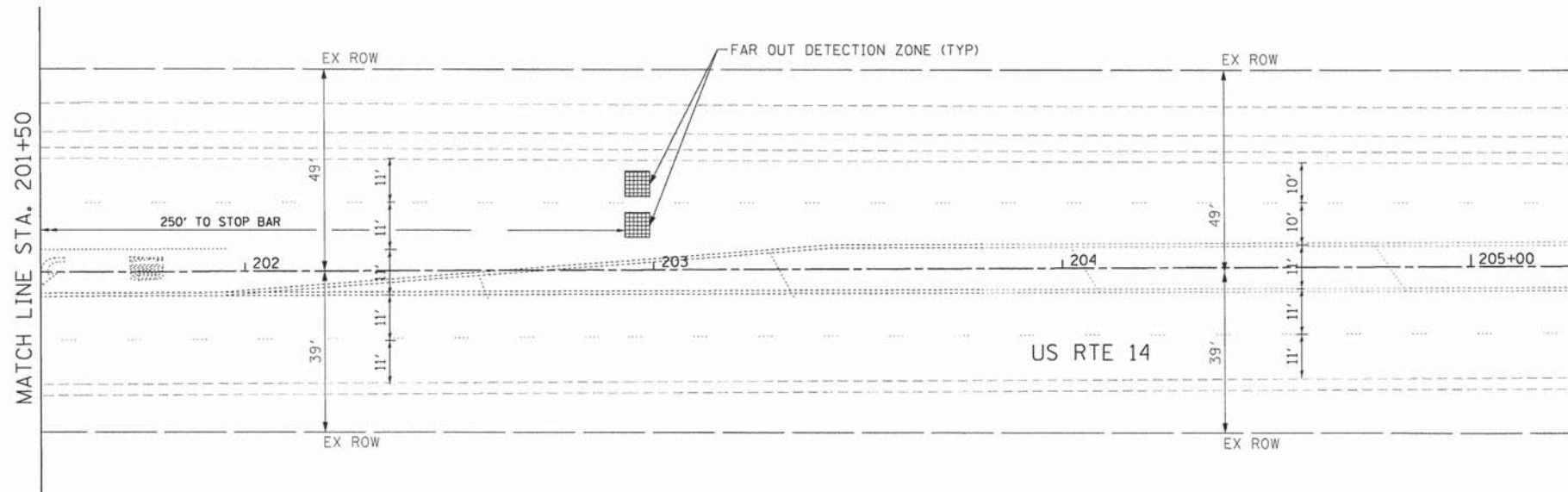
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	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

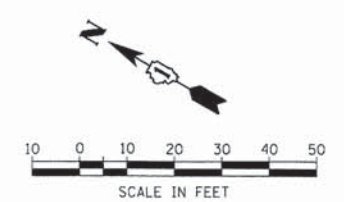
**TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN**  
**STAGE 4**  
**US RTE 14 AT JANDUS CUT OFF ROAD**  
 SCALE: 1" = 20' SHEET NO. 14 OF 22 SHEETS STA. 196+50 TO STA. 201+50

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	40
CONTRACT NO. 63886				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				





LEGEND:  
 DETECTION ZONE



COMPANY NAME: HRGreen  
 PROJECT CONTACT: bhartha  
 CLIENT: Illinois Department of Transportation  
 DATE PLOTTED: 12/5/2013 3:09:42 AM  
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PLOT SCALE = 1" = 20'	CHECKED - TEH	REVISED -
PLOT DATE = 12/5/2013	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**TEMPORARY TRAFFIC SIGNAL INSTALLATION PLAN  
 STAGE 4  
 US RTE 14 AT JANDUS CUT OFF ROAD**

SCALE: 1" = 20'    SHEET NO. 15 OF 22 SHEETS    STA. 201+50 TO STA. END

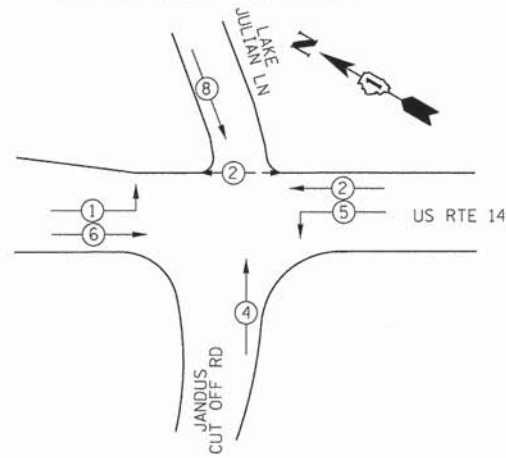
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	41
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 63886	



**TEMPORARY CONTROLLER SEQUENCE**

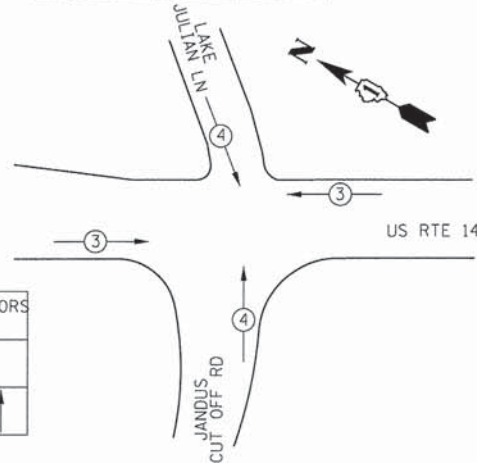
**CONTROLLER SEQUENCE LEGEND**

- ⊙ → DUAL ENTRY PHASE
- ⊙ → OVERLAP
- ⊙ → PEDESTRIAN PHASE
- NUMBER REFERRING TO ASSOCIATED PHASE



**PHASE DESIGNATION DIAGRAM**

**TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE**

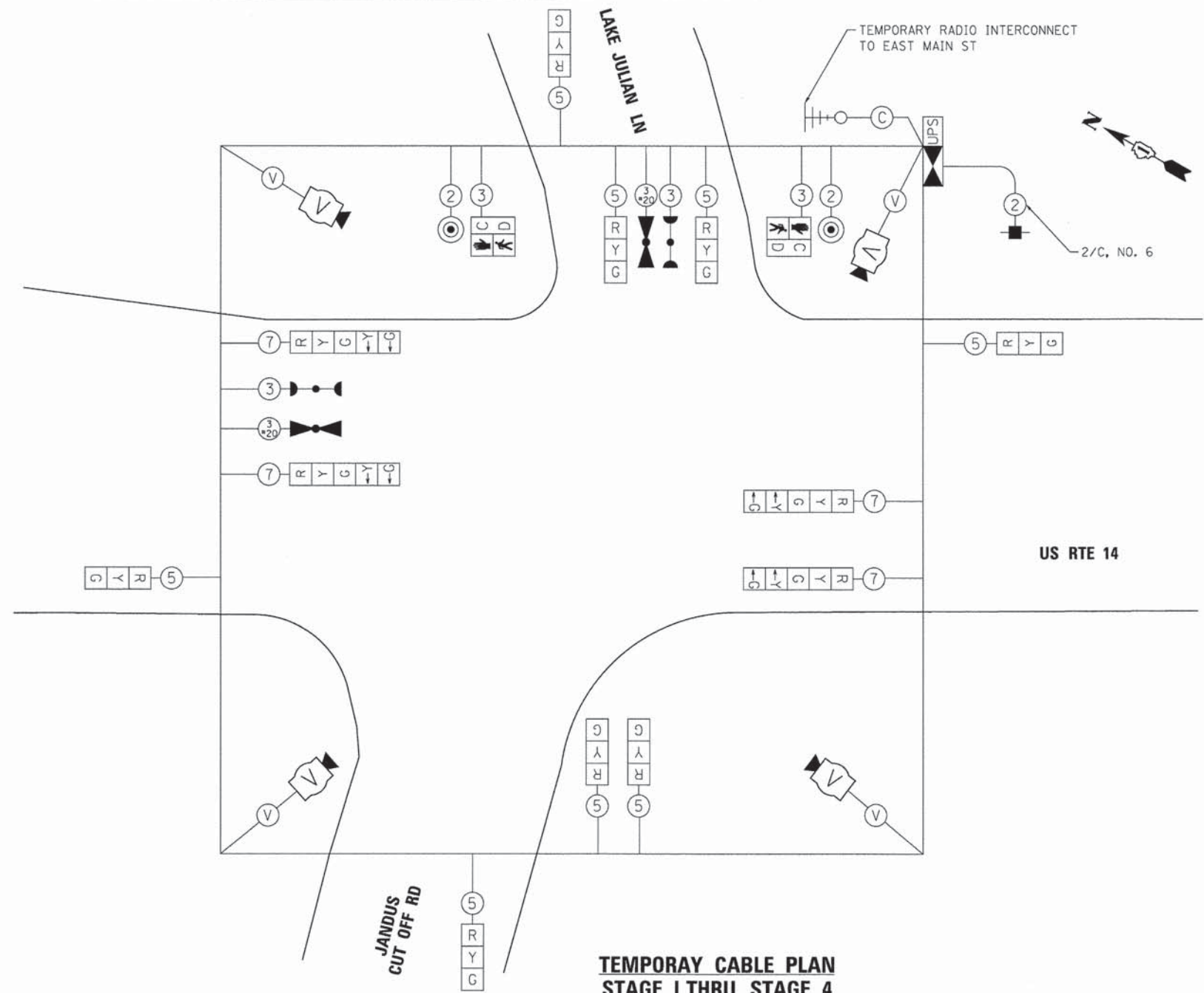


TEMPORARY EMERGENCY VEHICLE PREEMPTORS			
TEMPORARY EMERGENCY VEHICLE PREEMPTORS	3	4	
MOVEMENT	← →	↑ ↓	

I.D.O.T TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					TOTAL WATTAGE
TYPE	NO. LAMPS	WATTAGE		% OPERATION	
		INCAND.	LED		
SIGNAL (RED)	12		17	0.50	102
(YELLOW)	12		25	0.25	75
(GREEN)	12		15	0.25	45
ARROW (YELLOW)	4		12	0.10	4.8
ARROW (GREEN)	4		12	0.10	4.8
PEDESTRIAN SIGNAL	2		25	1.00	50
CONTROLLER	1		100	1.00	100
TEMPORARY UPS	1		25	1.00	25
VIDEO SYSTEM	1		15	1.00	15.0
TOTAL =					421.60

ENERGY COSTS TO: VILLAGE OF CARY  
655 VILLAGE HALL DRIVE  
CARY, IL 60013

ENERGY SUPPLY CONTACT: NORA FERNANDEZ  
PHONE: (866) 639-3532  
COMPANY: COMMONWEALTH EDISON



**TEMPORARY CABLE PLAN STAGE I THRU STAGE 4**

**RESTORATION OF WORK AREA**

RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAYS SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD AND ALL DAMAGE TO UNMOWED LAWNS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

**NOTE**

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

12/25/2013 9:08:44 AM  
86120253-FSP-cdb.dgn  
PLOT TABLE  
PLOT DATE = 12/5/2013



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PLOT SCALE = N.T.S.	DRAWN - DMS	REVISED -
	CHECKED - TEH	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**

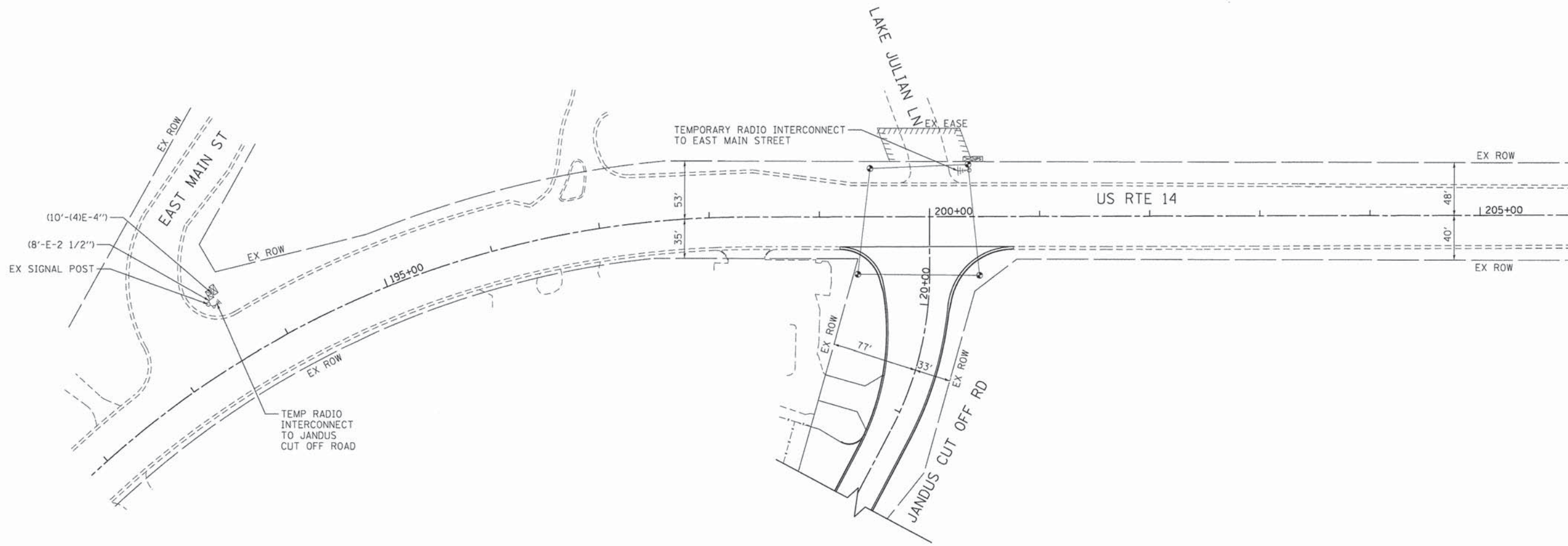
**TEMPORARY CABLE PLAN AND TEMPORARY PHASE DESIGNATION DIAGRAM-STAGE 1 THRU 4 US RTE 14 AT JANDUS CUT OFF ROAD**

SCALE: N.T.S.	SHEET NO. 16 OF 22 SHEETS	STA. TO STA.	F.A. RTE. 4058	SECTION 13-00063-00-PV	COUNTY MCHENRY	TOTAL SHEETS 79	SHEET NO. 42
						CONTRACT NO. 63886	

F.A. RTE. 4058	SECTION 13-00063-00-PV	COUNTY MCHENRY	TOTAL SHEETS 79	SHEET NO. 42
CONTRACT NO. 63886				

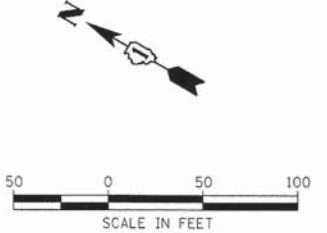
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT





NOTE:  
 THE TRAFFIC SIGNAL CONTROL EQUIPMENT  
 FOR THIS PROJECT SHALL BE "ECONOLITE" TO  
 MATCH THE EXISTING ADJACENT SYSTEM.

**RESTORATION OF WORK AREA**  
 RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL  
 TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE,  
 TRENCH BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE  
 ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS,  
 SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE  
 TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD AND ALL  
 DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH  
 STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.



COMPANY NAME: HRGreen.com  
 PROJECT CONTACT: Illinois Professional Design Firm  
 CLIENT: #194-001322  
 DATE PLOTTED: 12/5/2013 9:09:48 AM  
 FILE NAME: 86120253-18g-int.dgn  
 PLOT NUMBER: 17  
 PLOT TABLE: 17



USER NAME = bhartna	DESIGNED - JJS	REVISED -
PLOT SCALE = 1" = 50'	DRAWN - DMS	REVISED -
PLOT DATE = 12/5/2013	CHECKED - TEH	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

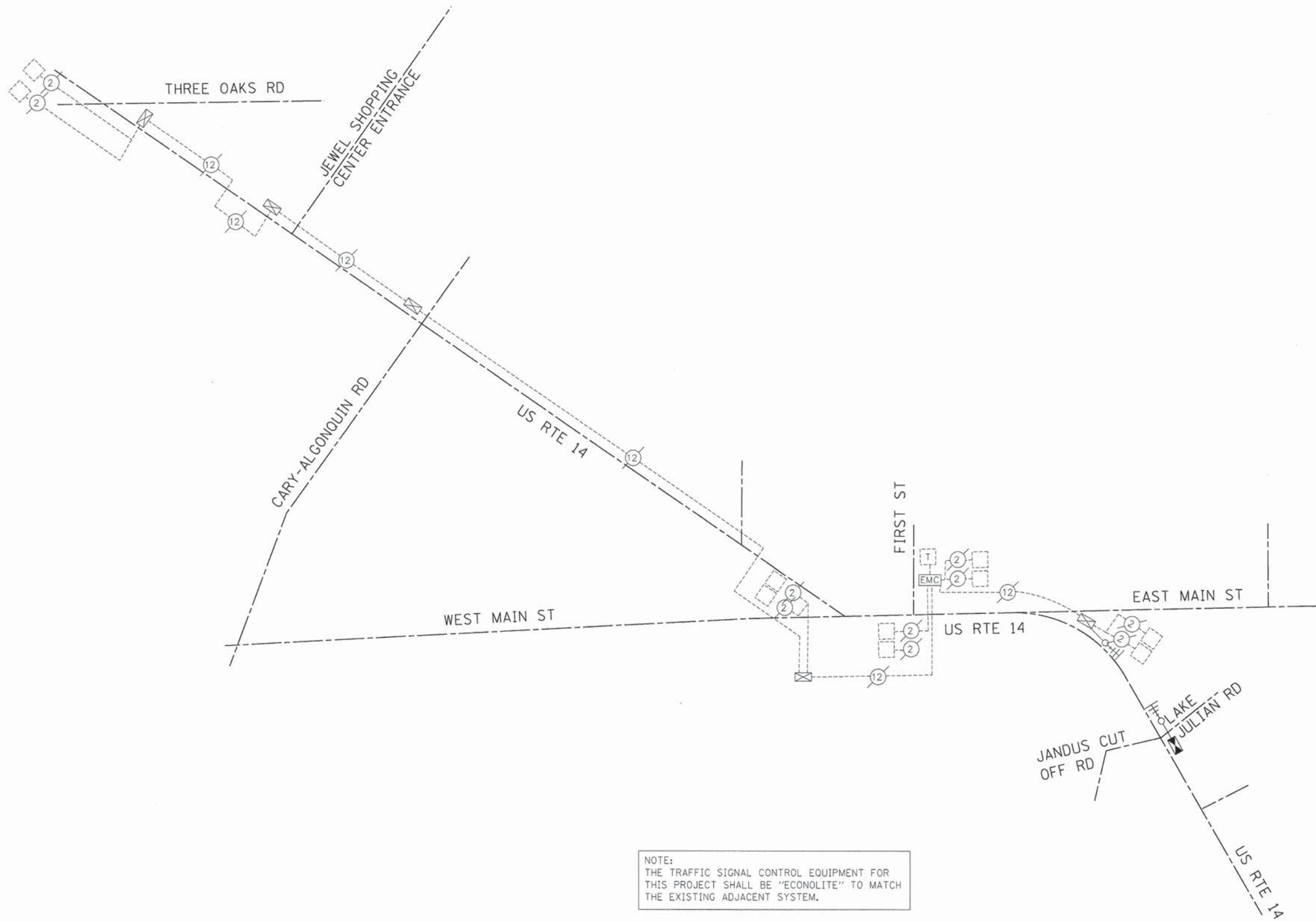
**TEMPORARY INTERCONNECT PLAN  
 US RTE 14 FROM EAST MAIN STREET TO JANDUS CUTOFF ROAD**

SCALE: 1" = 50' SHEET NO. 17 OF 22 SHEETS STA. 191+80 TO STA. 205+80

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	43
FED. ROAD DIST. NO.			ILLINOIS FED. AID PROJECT	
			CONTRACT NO. 63886	

86120253-18g-int.dgn





NOTE:  
 THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR  
 THIS PROJECT SHALL BE "ECONOLITE" TO MATCH  
 THE EXISTING ADJACENT SYSTEM.

COMPANY NAME: HRGreen  
 PROJECT CONTACT: bhartha  
 CLIENT: HRGreen.com  
 DATE PLOTTED: 12/25/2013 9:09:50 AM  
 FILE NAME: 86120253-1a1g-11n1sch.dgn  
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 PLOT TABLE: standard-trans.tbl



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	DRAWN - DMS	REVISED -
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PLOT DATE = 12/5/2013	DATE -	REVISED -

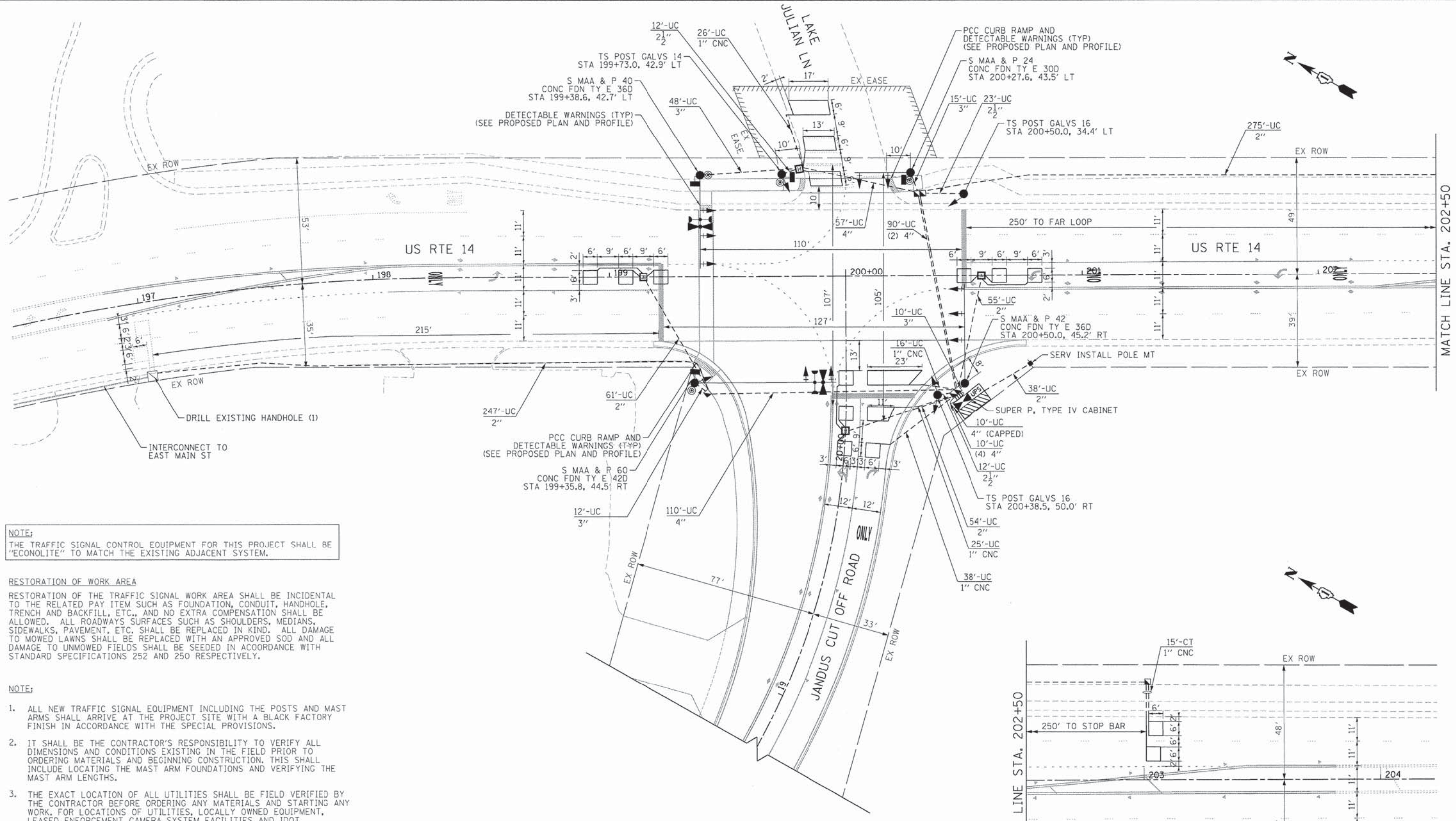
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**TEMPORARY INTERCONNECT SCHEMATIC  
 US RTE 14 FROM THREE OAKS ROAD TO JANDUS CUTOFF ROAD**

SCALE: N.T.S. SHEET NO. 18 OF 22 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	44
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 63886	

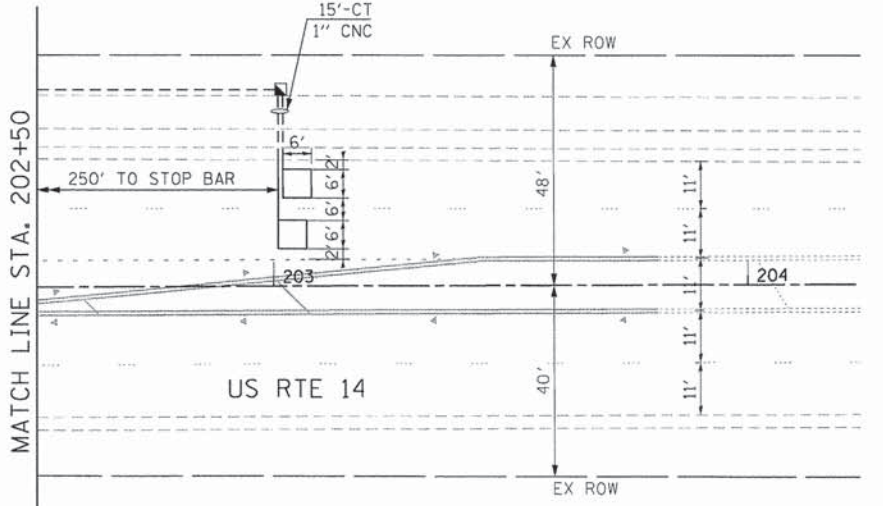




**NOTE:**  
 THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

**RESTORATION OF WORK AREA**  
 RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH AND BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAYS SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDING IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.

- NOTE:**
1. ALL NEW TRAFFIC SIGNAL EQUIPMENT INCLUDING THE POSTS AND MAST ARMS SHALL ARRIVE AT THE PROJECT SITE WITH A BLACK FACTORY FINISH IN ACCORDANCE WITH THE SPECIAL PROVISIONS.
  2. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION. THIS SHALL INCLUDE LOCATING THE MAST ARM FOUNDATIONS AND VERIFYING THE MAST ARM LENGTHS.
  3. THE EXACT LOCATION OF ALL UTILITIES SHALL BE FIELD VERIFIED BY THE CONTRACTOR BEFORE ORDERING ANY MATERIALS AND STARTING ANY WORK. FOR LOCATIONS OF UTILITIES, LOCALLY OWNED EQUIPMENT, LEASED ENFORCEMENT CAMERA SYSTEM FACILITIES AND IDOT UNDERGROUND FACILITIES, CONTACT THE LOCAL COUNTIES, MUNICIPALITIES AND IDOT FOR LOCATES. THE CONTRACTOR SHALL CALL 'JULIE' AT (800) 892-0123 OR 811, IN THE CITY OF CHICAGO CONTACT DIGGER AT (312) 744-7000 FOR FIELD LOCATIONS OF BURIED UTILITIES (48 HOURS NOTIFICATION REQUIRED).
  4. THE CONTRACTOR SHALL CHECK THE PROPOSED TRAFFIC SIGNAL EQUIPMENT LOCATIONS FOR OVERHEAD UTILITY CONFLICTS. THE CONTRACTOR SHALL COORDINATE ANY CONFLICTS WITH THE UTILITY COMPANIES AND THE RESIDENT ENGINEER BEFORE ORDERING MATERIALS.
  5. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, LOCAL GOVERNMENT AGENCIES AND IDOT.



COMPANY NAME: HRGreen.com  
 PROJECT CONTACT: Illinois Professional Design Firm #184-01322  
 DATE PLOTTED: 12/5/2013 9:09:53 AM  
 FILE NAME: 86120253-sfp-pln.dgn  
 PLOT NUMBER: 19  
 PEN TABLE: standard.ctb

USER NAME = bhar.tma	DESIGNED - JJS	REVISED -
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PLOT DATE = 12/5/2013	DATE -	REVISED -

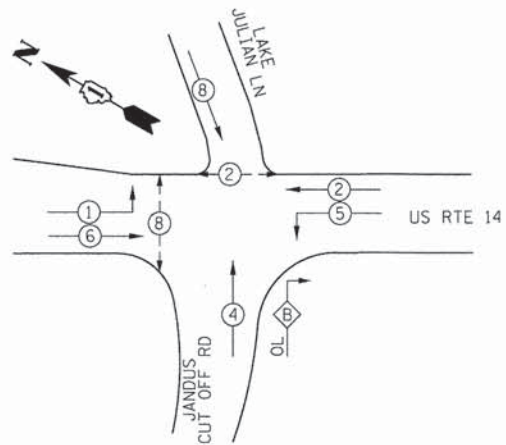
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

TRAFFIC SIGNAL MODERNIZATION PLAN	
US RTE 14 AT JANDUS CUTOFF ROAD/LAKE JULIAN LANE	
SCALE: 1" = 20'	SHEET NO. 19 OF 22 SHEETS
STA. 196+50	TO STA. 202+50

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	45
CONTRACT NO. 63886				
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				



**CONTROLLER SEQUENCE**



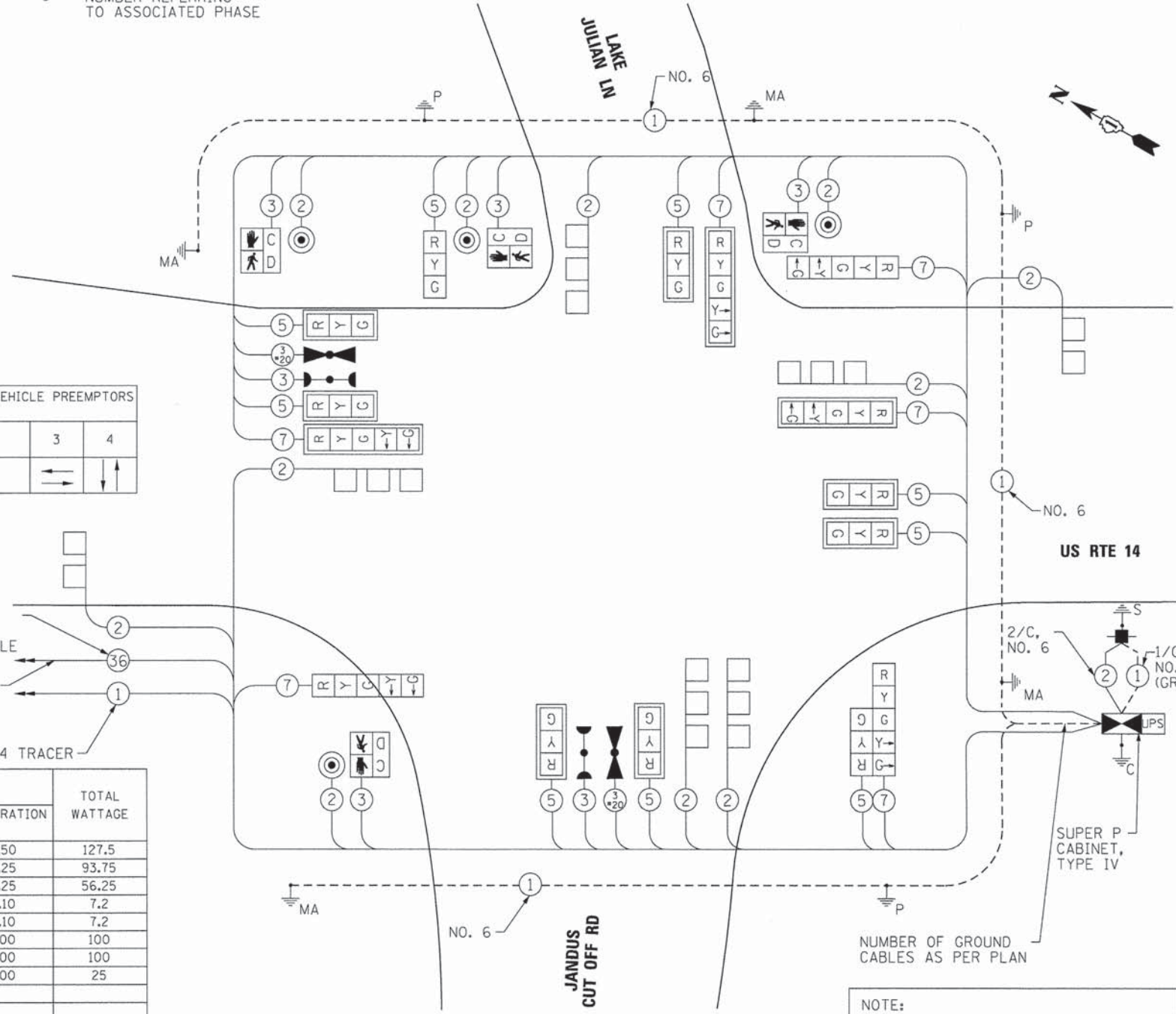
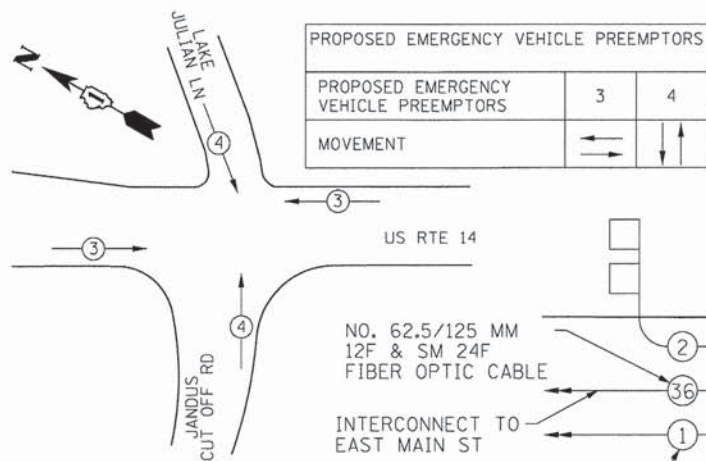
**CONTROLLER SEQUENCE LEGEND**

- ⊙ - DUAL ENTRY PHASE
- ⊙ - OVERLAP
- ⊙ - PEDESTRIAN PHASE
- \* - NUMBER REFERRING TO ASSOCIATED PHASE

**PHASE DESIGNATION DIAGRAM**

OVERLAP PHASE	PERMISSIVE PHASE	PROTECTED PHASE
B	= 4	+ 5

**EMERGENCY VEHICLE PREEMPTION SEQUENCE**



**CABLE PLAN**

NOTE: THE END OF THE TRACER CABLE SHALL BE CONTINUOUS AND EXTEND INTO THE CONTROLLER CABINET.

NOTE: THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

**SCHEDULE OF QUANTITIES**

ITEM	UNIT	TOTAL
SIGN PANEL - TYPE 1	SQ FT	15.0
SIGN PANEL - TYPE 2	SQ FT	27.0
SERVICE INSTALLATION - POLE MOUNTED	EACH	1
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	730.0
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.	FOOT	45.0
UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	85.0
UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	407.0
HANDHOLE	EACH	2
HEAVY-DUTY HANDHOLE	EACH	4
DOUBLE HANDHOLE	EACH	2
PAINT NEW TRAFFIC SIGNAL POST	EACH	3
PAINT NEW MAST ARM AND POLE, UNDER 40 FOOT	EACH	1
PAINT NEW MAST ARM AND POLE, 40 FOOT AND OVER	EACH	3
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	2,191.0
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	1,387.0
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	2,025.5
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 7C	FOOT	884.5
ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2 C	FOOT	70.5
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	981.5
TRAFFIC SIGNAL POST, GALVANIZED STEEL 14 FT.	EACH	1
TRAFFIC SIGNAL POST, GALVANIZED STEEL 16 FT.	EACH	2
STEEL MAST ARM ASSEMBLY AND POLE, 24 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 40 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 42 FT.	EACH	1
STEEL MAST ARM ASSEMBLY AND POLE, 60 FT.	EACH	1
CONCRETE FOUNDATION, TYPE A	FOOT	12.0
CONCRETE FOUNDATION, TYPE C	FOOT	4.0
CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	10.0
CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	26.0
CONCRETE FOUNDATION, TYPE E 42-INCH DIAMETER	FOOT	21.0
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	7
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	1
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, BRACKET MOUNTED	EACH	2
SIGNAL HEAD, LED, 1-FACE, 5-SECTION, MAST-ARM MOUNTED	EACH	3
SIGNAL HEAD, LED, 2-FACE, 1-3 SECTION, 1-5 SECTION, BRACKET MOUNTED	EACH	1
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	4
TRAFFIC SIGNAL BACKPLATE, LOUVERED	EACH	10
INDUCTIVE LOOP DETECTOR	EACH	7
DETECTOR LOOP, TYPE I	FOOT	633.0
* LIGHT DETECTOR	EACH	2
* LIGHT DETECTOR AMPLIFIER	EACH	1
PEDESTRIAN PUSH-BUTTON	EACH	4
TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	5,315.5
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
REMOVE EXISTING HANDHOLE	EACH	7
REMOVE EXISTING DOUBLE HANDHOLE	EACH	1
REMOVE EXISTING CONCRETE FOUNDATION	EACH	8
TEMPORARY TRAFFIC SIGNAL TIMING	EACH	1
* EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO. 20 3/C	FOOT	588.5
FULL-ACTUATED CONTROLLER AND TYPE IV CABINET, SPECIAL	EACH	1
UNINTERRUPTABLE POWER SUPPLY, SPECIAL	EACH	1

\* 100% OF EMERGENCY VEHICLE PREEMPTION EQUIPMENT COST SHALL BE PAID FOR BY THE VILLAGE OF CARY.

I.D.O.T TRAFFIC SIGNAL INSTALLATION ELECTRICAL SERVICE REQUIREMENTS					
TYPE	NO. LAMPS	WATTAGE		% OPERATION	TOTAL WATTAGE
		INCAND.	LED		
SIGNAL (RED)	15		17	0.50	127.5
(YELLOW)	15		25	0.25	93.75
(GREEN)	15		15	0.25	56.25
ARROW (YELLOW)	6		12	0.10	7.2
ARROW (GREEN)	6		12	0.10	7.2
PEDESTRIAN SIGNAL	4		25	1.00	100
CONTROLLER	1		100	1.00	100
UPS	1		25	1.00	25
TOTAL =					516.90

ENERGY COSTS TO: VILLAGE OF CARY  
655 VILLAGE HALL DRIVE  
CARY, IL 60013

ENERGY SUPPLY CONTACT: NORA FERNANDEZ  
PHONE: (866) 639-3532  
COMPANY: COMMONWEALTH EDISON

COMPANY NAME: HRGreen.com  
PROJECT CONTACT: Illinois Professional Design Firm  
DATE PLOTTED: 12/5/2013 9:09:57 AM  
FILE NAME: 86120253-slp-cab.dgn  
PLOT DRIVER: pdfLdr  
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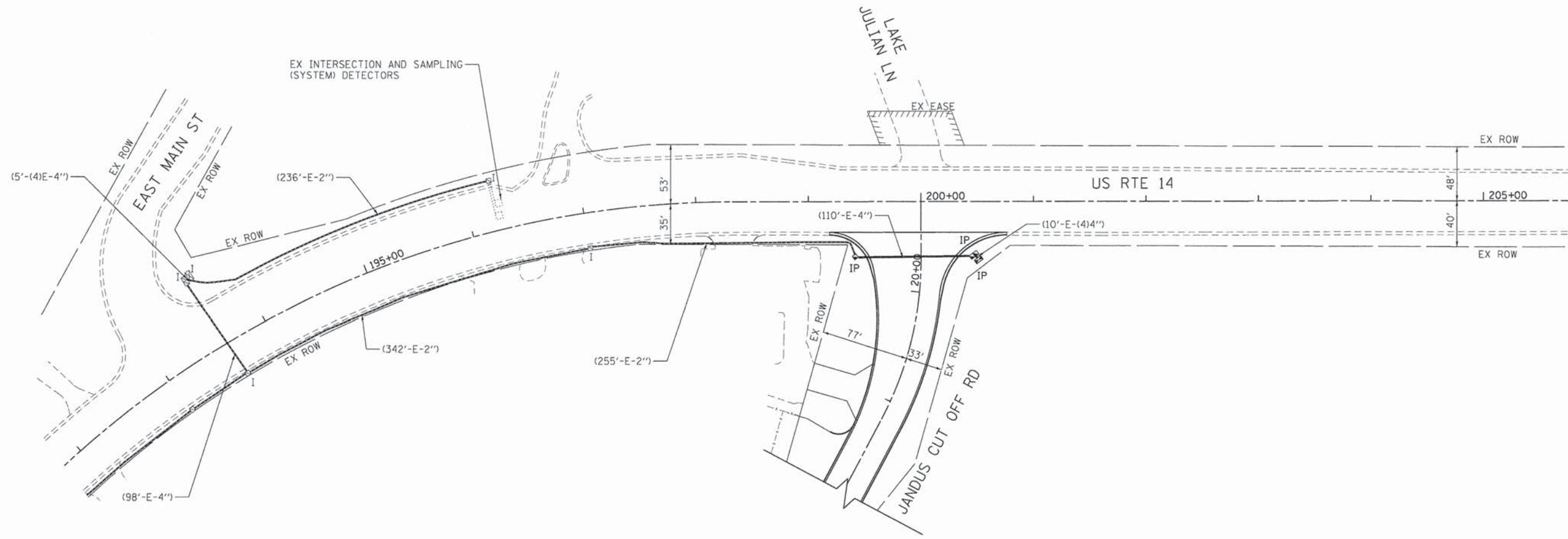
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PLOT DATE = 12/5/2013	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

CABLE PLAN, PHASE DESIGNATION DIAGRAM, EMERGENCY VEHICLE PREEMPTION SEQUENCE AND SCHEDULE OF QUANTITIES  
US RTE 14 AT JANDUS CUT OFF ROAD/LAKE JULIAN LANE

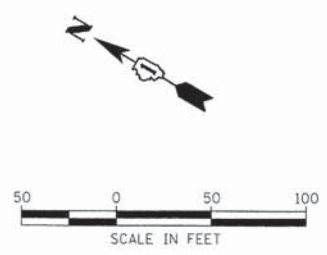
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FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 63886	





**NOTE:**  
 THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

**RESTORATION OF WORK AREA**  
 RESTORATION OF THE TRAFFIC SIGNAL WORK AREA SHALL BE INCIDENTAL TO THE RELATED PAY ITEM SUCH AS FOUNDATION, CONDUIT, HANDHOLE, TRENCH BACKFILL, ETC., AND NO EXTRA COMPENSATION SHALL BE ALLOWED. ALL ROADWAY SURFACES SUCH AS SHOULDERS, MEDIANS, SIDEWALKS, PAVEMENT, ETC. SHALL BE REPLACED IN KIND. ALL DAMAGE TO MOWED LAWNS SHALL BE REPLACED WITH AN APPROVED SOD AND ALL DAMAGE TO UNMOWED FIELDS SHALL BE SEEDED IN ACCORDANCE WITH STANDARD SPECIFICATIONS 252 AND 250 RESPECTIVELY.



COMPANY NAME: HRGreen  
 PROJECT CONTACT: [Name]  
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	DRAWN - DMS	REVISED -
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PLOT DATE = 12/5/2013	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**INTERCONNECT PLAN  
 US RTE 14 FROM EAST MAIN STREET TO JANDUS CUTOFF ROAD**  
 SCALE: 1" = 50' SHEET NO. 21 OF 22 SHEETS STA. 191+80 TO STA. 205+80

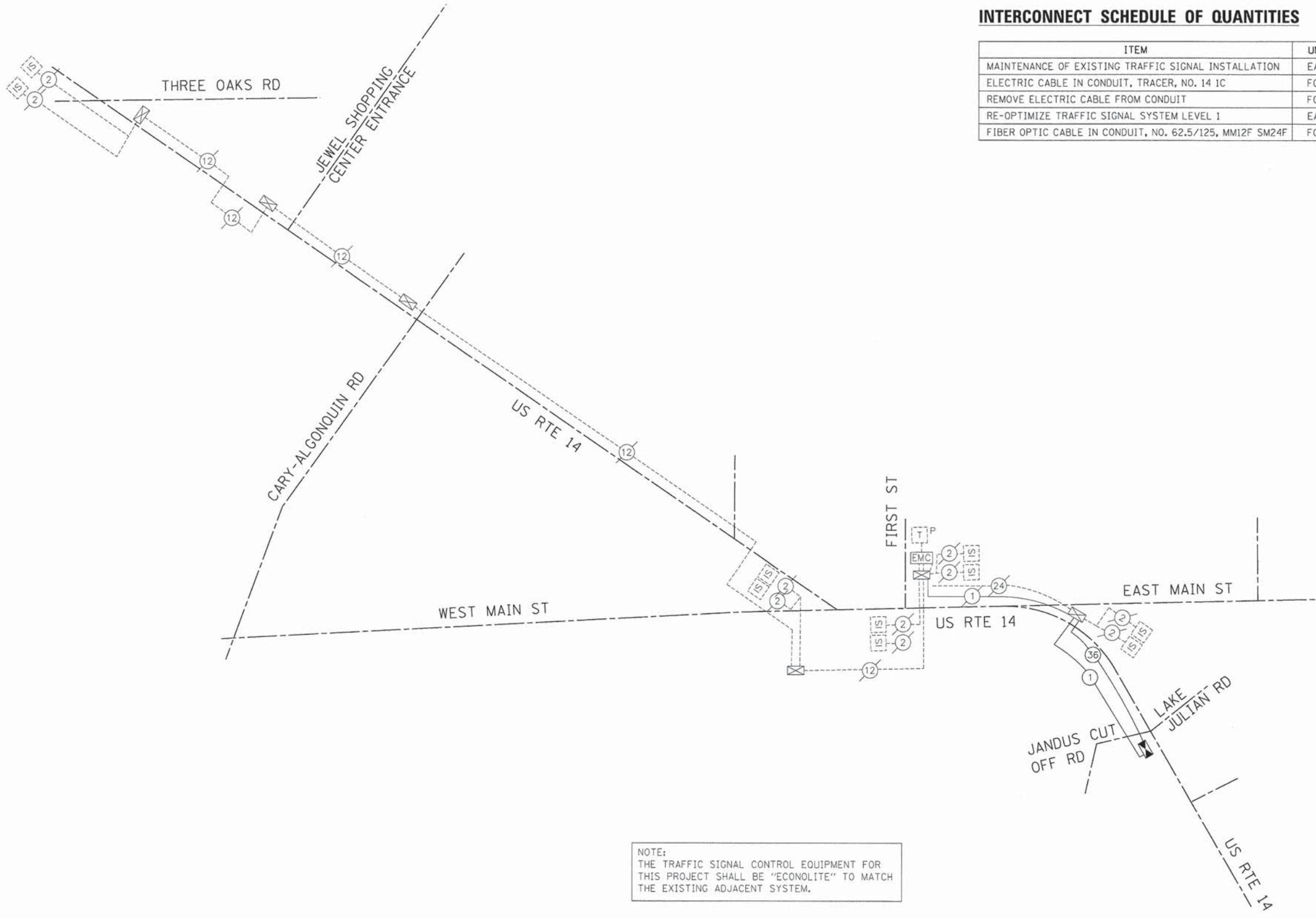
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	47
CONTRACT NO. 63886				

86120253  
 86120253-sig-int.dgn



**INTERCONNECT SCHEDULE OF QUANTITIES**

ITEM	UNIT	TOTAL
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
ELECTRIC CABLE IN CONDUIT, TRACER, NO. 14 IC	FOOT	858.5
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	1878.0
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1
FIBER OPTIC CABLE IN CONDUIT, NO. 62.5/125, MM12F SM24F	FOOT	871.5



NOTE:  
 THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

COMPANY NAME: HRGreen  
 PROJECT CONTACT: bhortna  
 CLIENT: HRGreen  
 FILE NAME: 12/05/2013 09:00:07 AM  
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 STANDARD: standard-trans.tbl



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PLOT DATE = 12/5/2013	DATE -	REVISED -

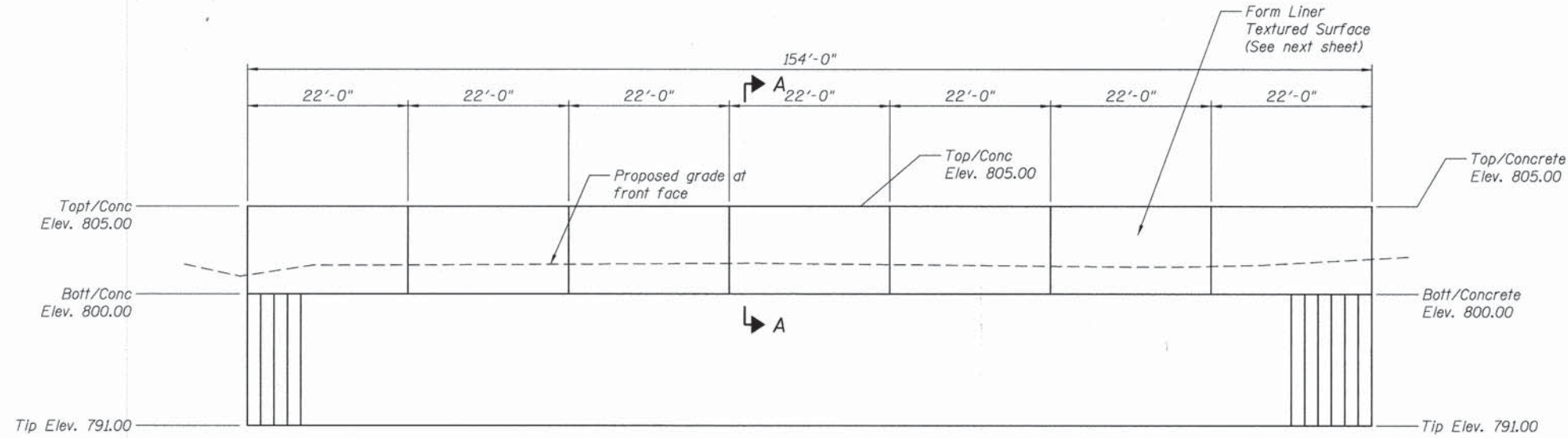
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**INTERCONNECT SCHEMATIC  
 US ROUTE 14  
 FROM THREE OAKS ROAD TO JANDUS CUT OFF ROAD/LAKE JULIAN LANE**

F.A. RTE. 4058	SECTION 13-00063-00-PV	COUNTY MCHENRY	TOTAL SHEETS 79	SHEET NO. 48
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 63886	

SCALE: N.T.S. SHEET NO. 22 OF 22 SHEETS STA. TO STA.





**ELEVATION**  
Looking West at front face

**CURVE DATA**

Prop. Curve PJANDUS-2  
 PI STA. = 16+56.03  
 $\Delta = 83^\circ 10' 19''$  (RT)  
 $D = 32^\circ 44' 26''$   
 $R = 175.00'$   
 $T = 155.30'$   
 $L = 254.03'$   
 $E = 58.97'$   
 $e = 2.0\%$   
 $T.R. = 52'$   
 $S.E. RUN = 52'$   
 $P.C. STA = 15+00.74$   
 $P.T. STA = 17+54.77$

**INDEX OF SHEETS**

- S-1 General Plan and Elevation
- S-2 Bill of Material and Details
- S-3 Soil Boring Logs

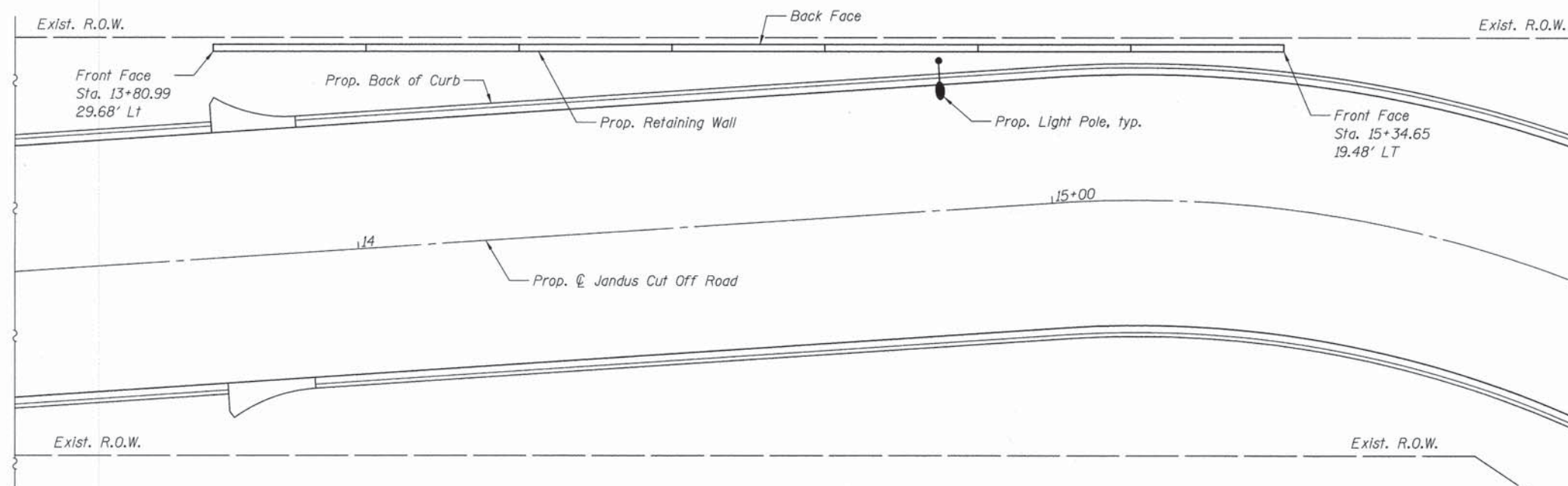
**DESIGN STRESSES**

**FIELD UNITS (CONCRETE)**

$f'_c = 3,500$  psi  
 $f_y = 60,000$  psi (Reinforcement)  
 $f_y = 38,000$  psi min. (Steel Sheet Pile)  
 $f_y = 36,000$  psi min. (Steel Plate)

**DESIGN SPECIFICATIONS**

2012 AASHTO LRFD Bridge  
 Design Specifications, 6th Edition



**PLAN**

◆ = Soil Boring Location



To the best of my knowledge, information and belief, this design is structurally adequate for the design loading shown on the plans. The design is an economical one for the style of structure and complies with requirements of the current "AASHTO LRFD Bridge Design Specifications".

*Robert G. Danks*  
 Structural Engineer Expires: 11/30/2014  
 HR Green, Inc.

**GENERAL PLAN AND ELEVATION**  
**RETAINING WALL**  
**JANDUS CUT OFF ROAD**  
**(F.A.U. 4058)**  
**SECTION NO. 12-00061-00-PV**  
**McHENRY COUNTY**  
**STA. 13+75.00 TO STA. 15+34.65**

COMPANY NAME: HRGreen.com  
 PROJECT CONTACT: VILLAGE OF CARY  
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 DATE PLOTTED: 12/6/2013 9:56:28 AM  
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**HRGreen**  
 HRGreen.com  
 Illinois Professional Design Firm  
 #194-001322

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CHECKED - RGD	CHECKED - RGD	REVISED -
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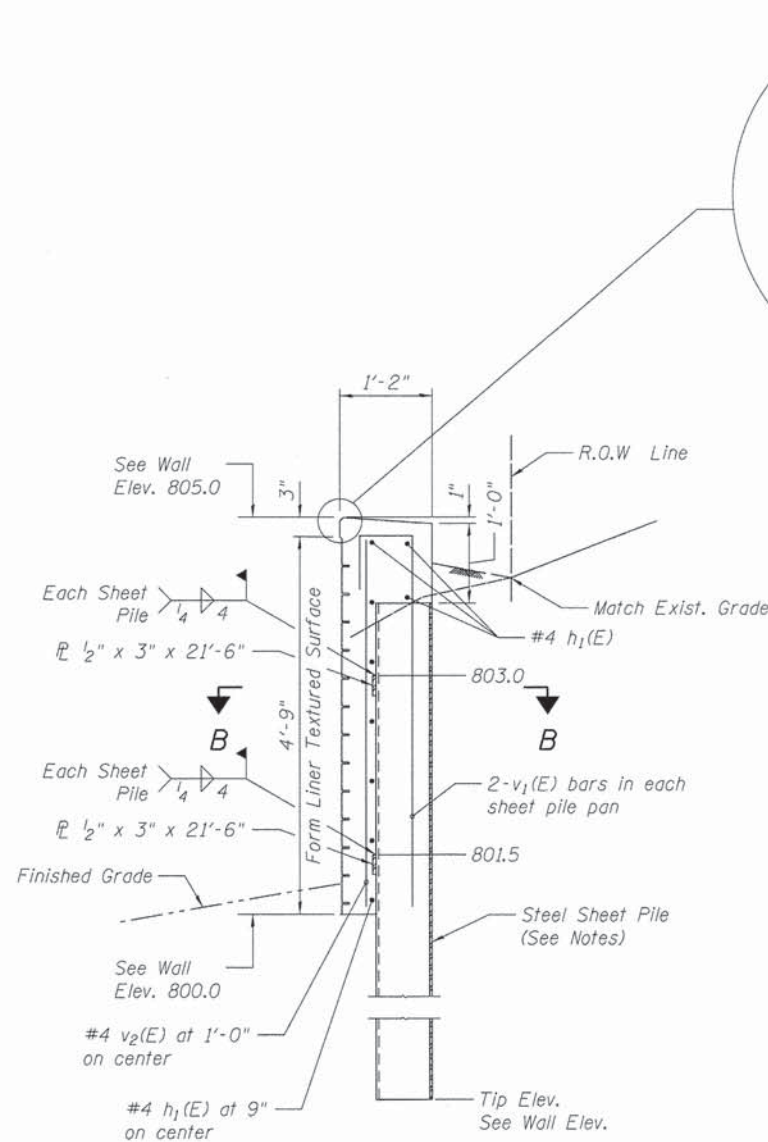
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**GENERAL PLAN AND ELEVATION**

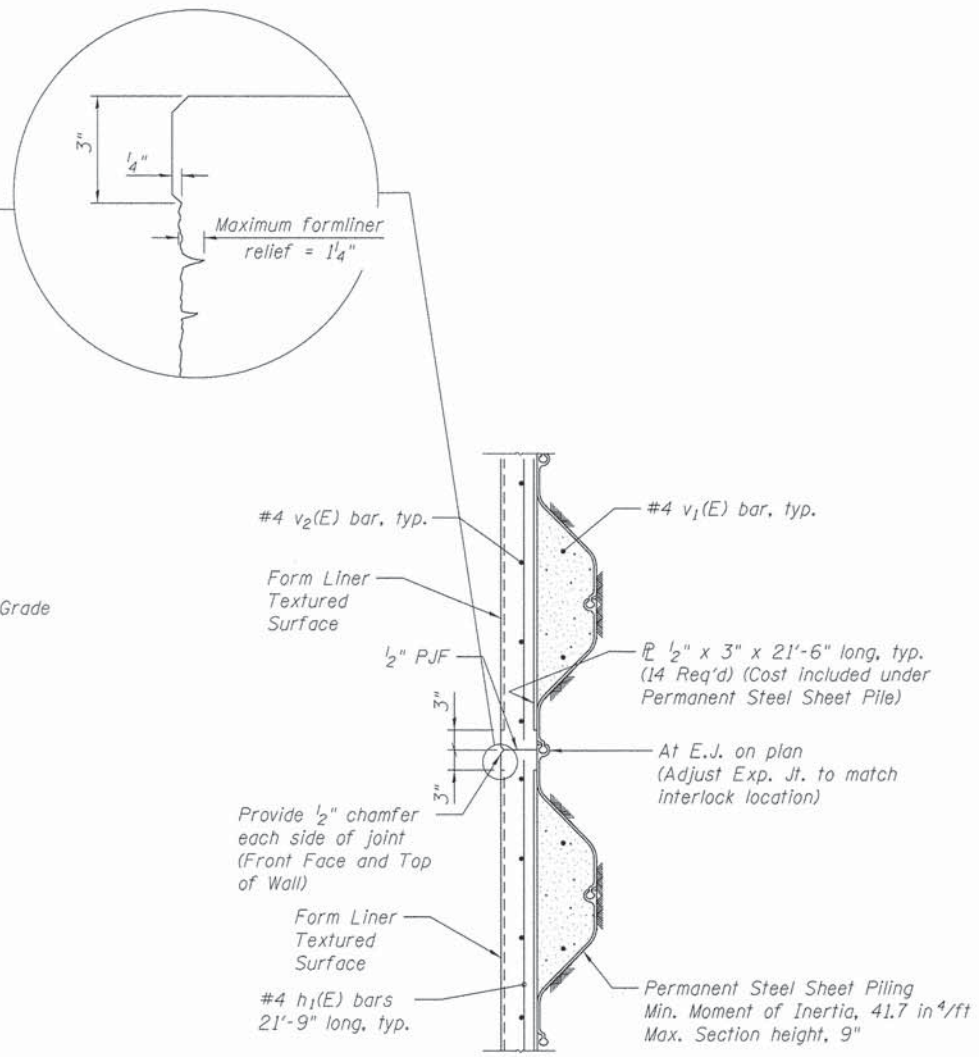
SHEET NO. S-1 OF S-3 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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			CONTRACT NO. 63886	
ILLINOIS FED. AID PROJECT #FEDPROJNO#				

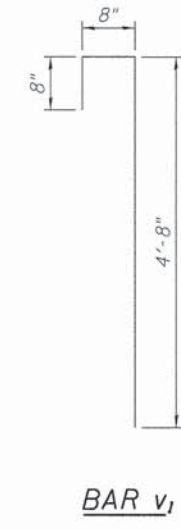




SECTION A-A



SECTION B-B



BAR v1

**BILL OF MATERIAL**

Bar	No.	Size	Length	Shape
h <sub>1</sub> (E)	63	#4	21'-9"	—
v <sub>1</sub> (E)	84	#4	6'-0"	⌊
v <sub>2</sub> (E)	155	#4	4'-8"	—
Reinforcement Bars, Epoxy Coated			Pound	1,740
Concrete Structures			Cu Yd	23.2
Permanent Steel Sheet Piling			Sq Ft	2,002
Form Liner Textured Surface			Sq Ft	770

**NOTES:**

- Cut v (E) and v<sub>1</sub>(E) bars in field to maintain clear cover from edge of concrete.
- Stations and offsets based on proposed PGL for Jandus Cut Off Road.
- Reinforcement bars shall conform to the requirements of ASTM A 706 Gr 60. See Supplemental Specifications and Recurring Special Provisions.
- Acceptable permanent steel sheet piles include: Sz-15, CZB4RD, Z65, PZ-22 See Special Provision for Permanent Steel Sheet Piling (LRFD)

COMPANY NAME: HRGreen  
 PROJECT CONTACT: VILLAGE OF CARY  
 CLIENT: 12/5/2013 8:02:12 AM  
 DATE PLOTTED: 8620253-Hor-Gem.dgn  
 FILE NAME: pld.dwg  
 PLOT DRIVER: standard-true.ctb

**HRGreen**  
 HRGreen.com  
 Illinois Professional Design Firm  
 #184-001322

USER NAME = bharima	DESIGNED - KMA	REVISED -
CHECKED - RGD	REVISIONS	
PLOT SCALE = N.T.S.	DRAWN - WJH	REVISED -
PLOT DATE = 12/5/2013	CHECKED -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**BILL OF MATERIAL AND DETAILS**  
**JANDUS CUTOFF ROAD**  
 SHEET NO. S-2 OF S-3 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	50
				CONTRACT NO. 63886
				ILLINOIS FED. AID PROJECT #FEDPROJN05



PROJECT **Jandus Cutoff Road Realignment, Sta. 10+00 to 19+50, Cary, IL**

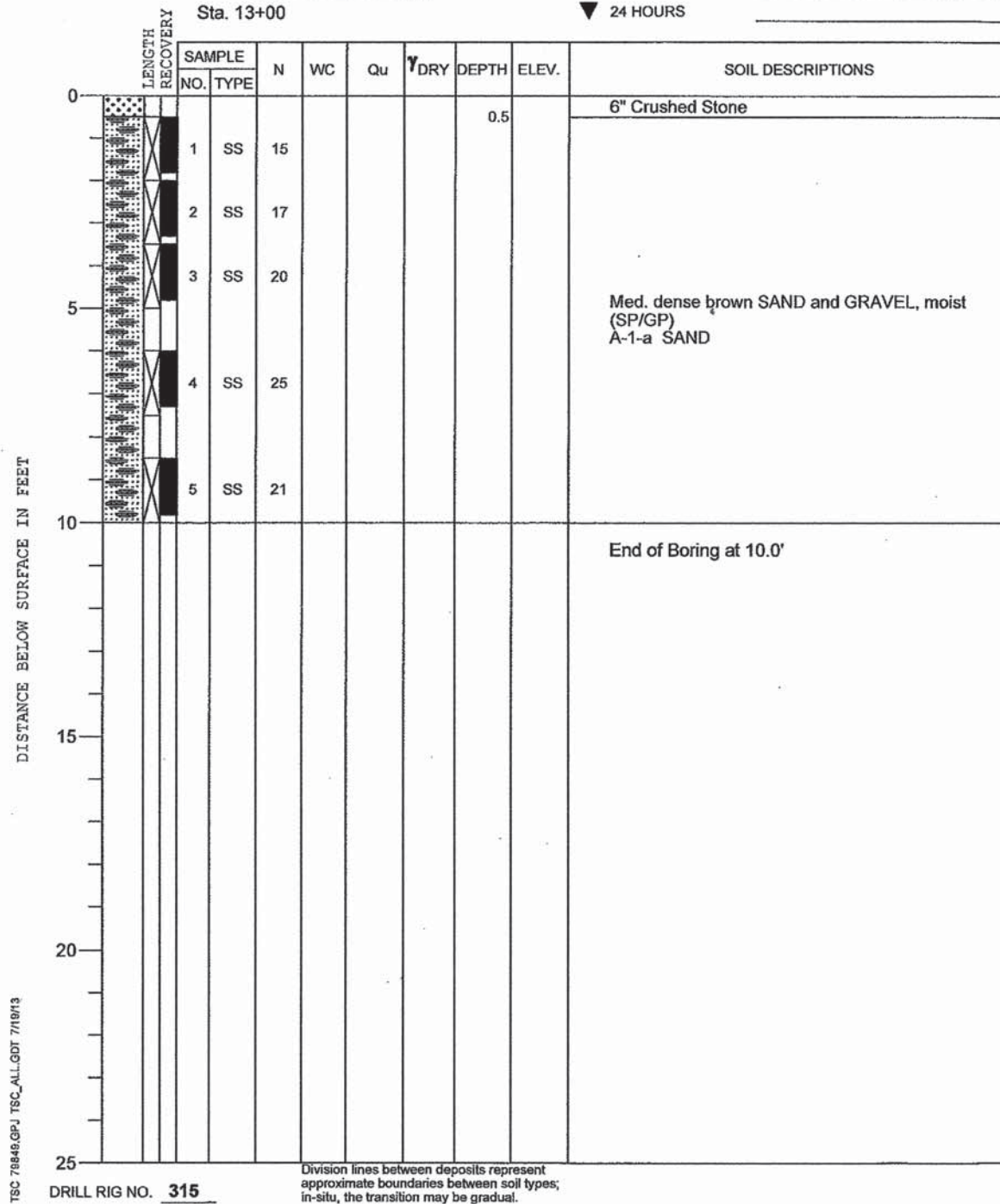
CLIENT **HR Green, Inc., McHenry, IL**



BORING **1** DATE STARTED **6-19-13** DATE COMPLETED **6-19-13** JOB **L-79,849**

ELEVATIONS  
 GROUND SURFACE 796.86  
 END OF BORING \_\_\_\_\_

WATER LEVEL OBSERVATIONS  
 ▽ WHILE DRILLING Dry  
 ▽ AT END OF BORING Dry  
 ▽ 24 HOURS \_\_\_\_\_



COMPANY NAME: VILLAGE OF CARY  
 PROJECT CONTACT: 12/24/2013 10:35:59 AM  
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 FILE NAME: 13-00063-00-PV-51.dwg  
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HRGreen.com  
 Illinois Professional Design Firm  
 #184-001322

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PLOT DATE = 12/24/2013	CHECKED -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**RETAINING WALL SOIL BORING LOG  
 JANDUS CUTOFF ROAD**

SHEET NO. S-3 OF S-3 SHEETS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	51
CONTRACT NO. 63886				
[ILLINOIS] FED. AID PROJECT #FEDPROJNO#				



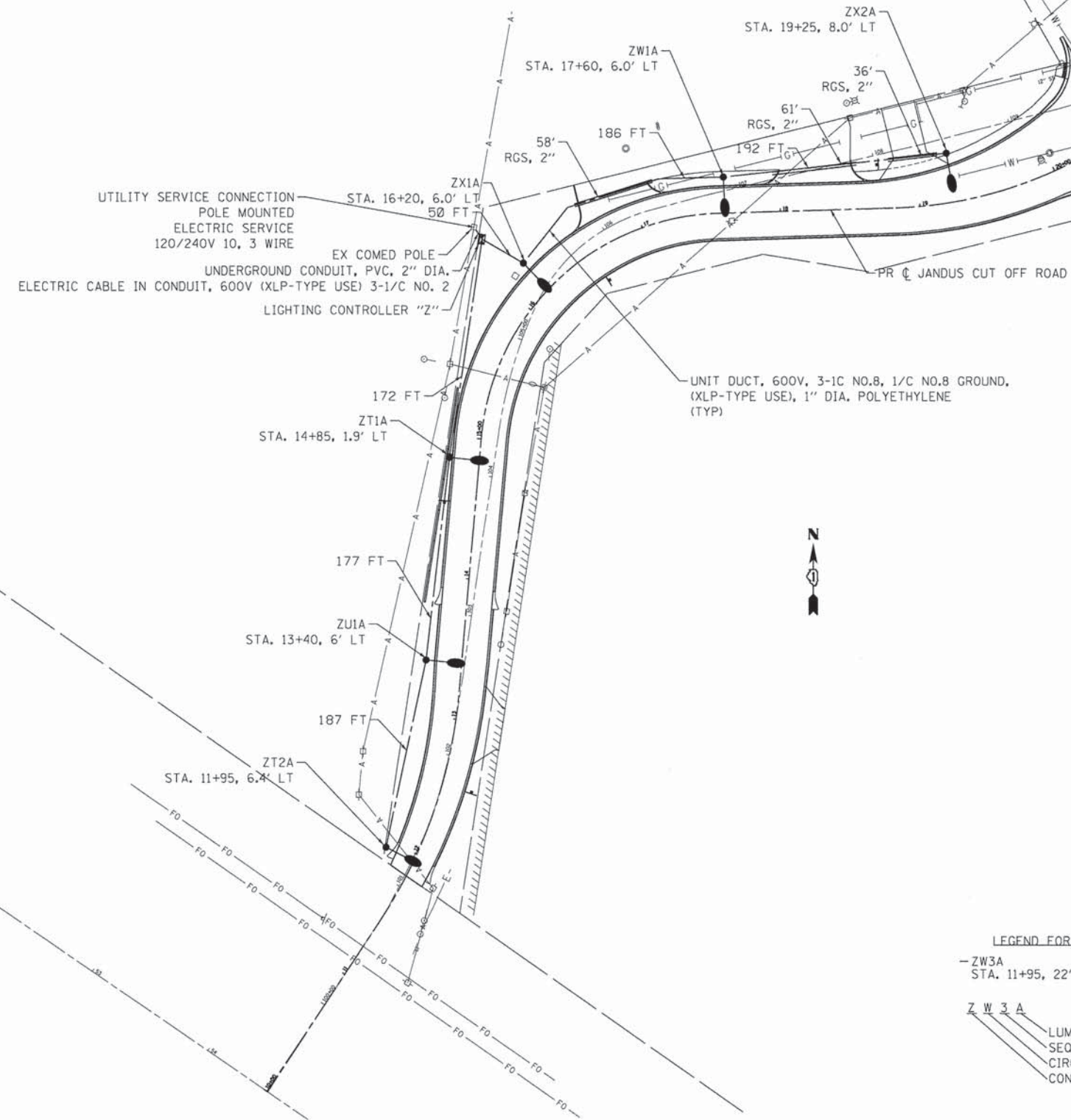
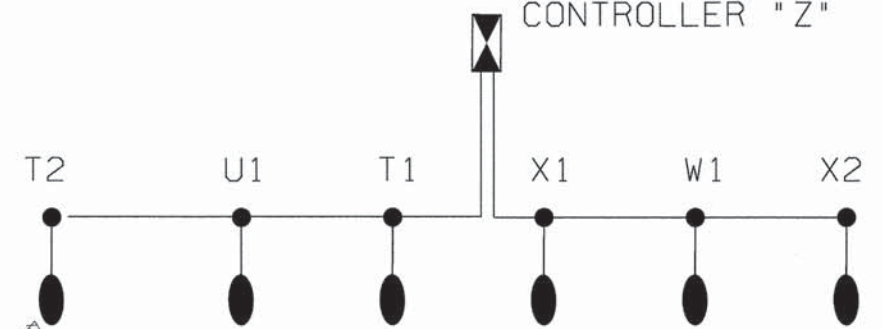




LOAD TABULATION LIGHTING CONTROLLER "Z"

CIRCUIT	LED 180W	WATTS
X	2	360
W	1	180
U	1	180
T	2	360
TOTAL		1080

CIRCUIT LINE DIAGRAM  
CONTROLLER "Z"



LEGEND FOR LIGHT POLE CALLOUT  
 - ZW3A (LIGHT POLE REFERENCE NAME)  
 STA. 11+95, 22' LT (STATION, OFFSET FROM BACK OF CURB)  
 Z W 3 A  
 LUMINAIRE TYPE  
 SEQUENCE NUMBER  
 CIRCUIT IDENTIFIER  
 CONTROLLER IDENTIFIER

12/5/2013 8:02:59 AM  
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	DATE -	REVISED -

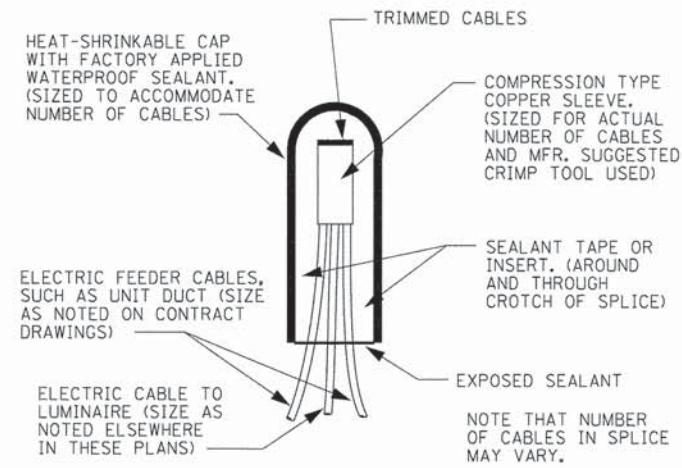
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

LIGHTING PLAN  
JANDUS CUTOFF ROAD

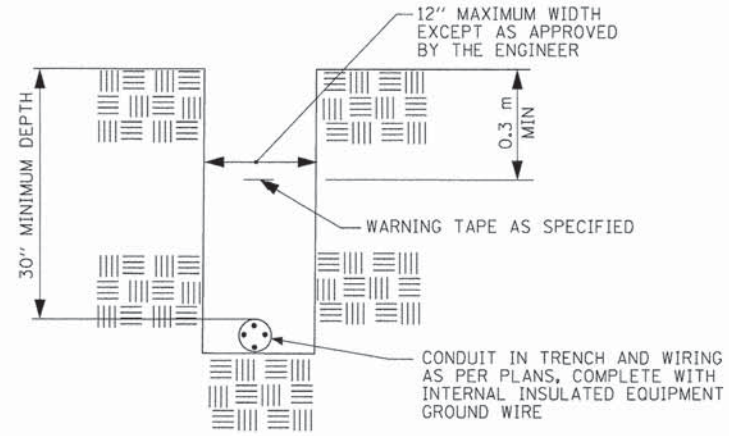
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FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT			CONTRACT NO. 63886	

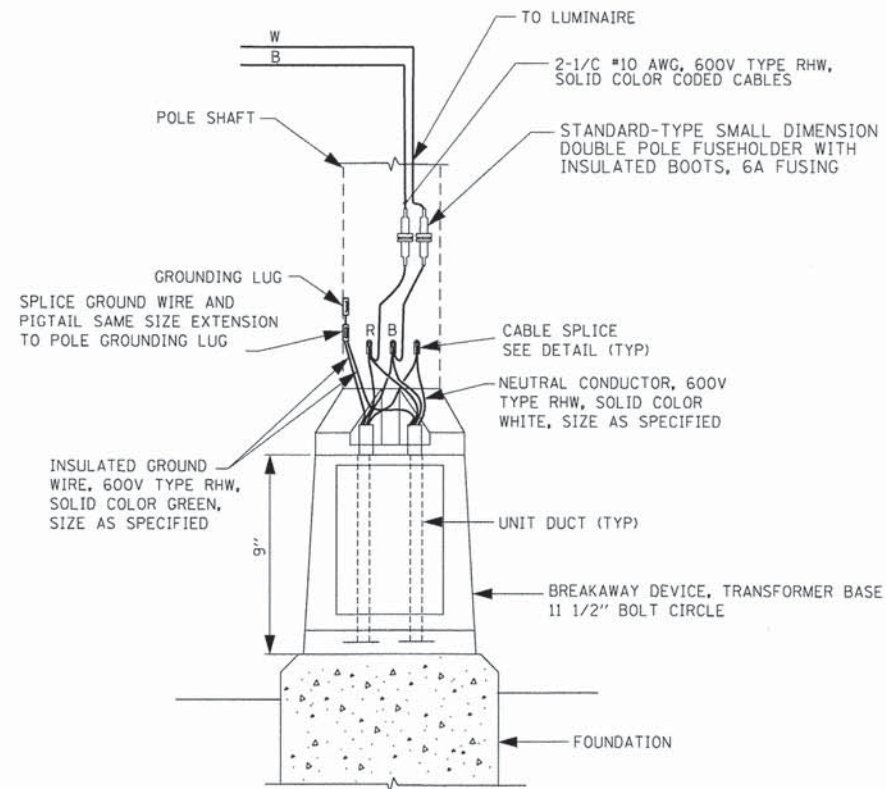




**SPLICING DETAIL**  
N.T.S.



**TYPICAL CONDUIT IN TRENCH DETAIL**  
N.T.S.



**POLE BASE WIRING DIAGRAM**  
**SINGLE MAST ARM**  
N.T.S.

COMPANY NAME: HRGreen.com  
PROJECT CONTACT: Illinois Professional Design Firm # 184-011322  
CLIENT: HRGreen  
DATE PLOTTED: 12/5/2013 3:03:32 AM  
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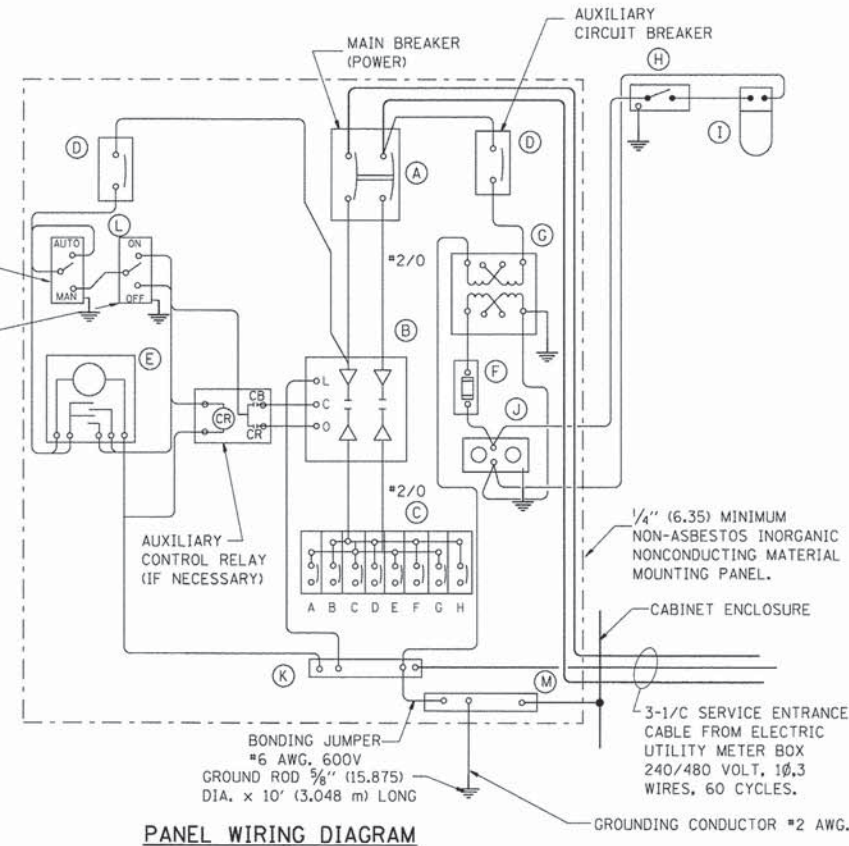
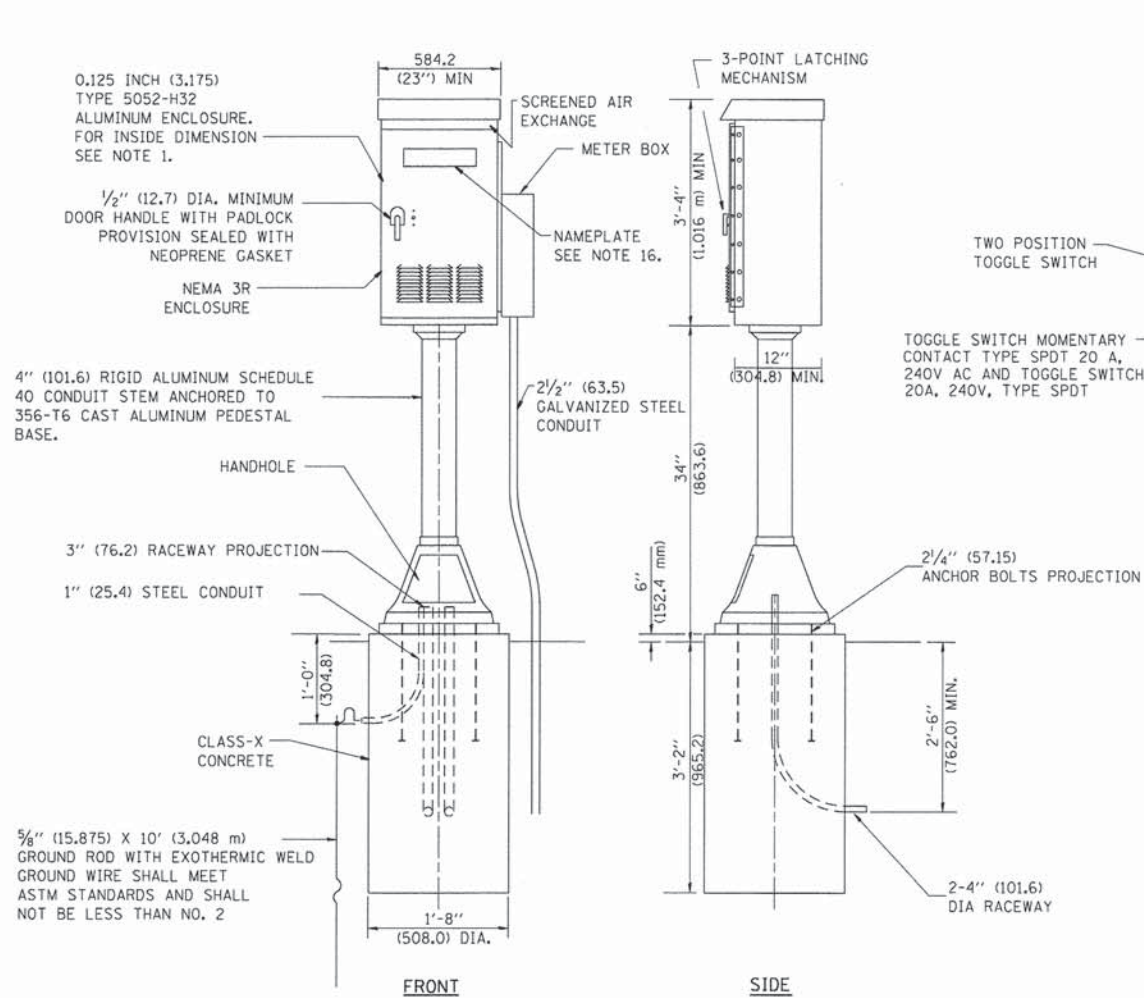
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**LIGHTING DETAILS**  
**JANDUS CUTOFF ROAD**

SCALE: SHEET NO. OF SHEETS STA. TO STA.

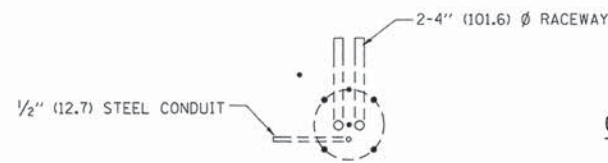
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4058	13-00063-00-PV	MCHENRY	79	54
CONTRACT NO. 63886				
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT				





PANEL EQUIPMENT

BILL OF MATERIAL		
ITEM	QUANTITY	DESCRIPTION
A	1	MAIN CIRCUIT BREAKER, 2 POLE, 600 VOLT 100 AMP. FRAME, 100 AMP. NON-INTERCHANGEABLE TRIP INTERRUPTING RATING NEMA-22000 AMP. AT 480 VOLT.
B	1	REMOTE CONTROL SWITCH, ELECTRICALLY OPERATED, MECHANICALLY HELD, 2 POLE, SINGLE THROW, 100 AMP., 600 VOLTS CONTROL CIRCUIT 240 VOLT.
C	8	CIRCUIT BREAKERS, 1 POLE, 277V., 100 AMP., FRAME 50 AMP. NON-INTERCHANGEABLE TRIP INTERRUPTING RATING NEMA-10,000 AMP. AT 240 V.
D	2	CONTROL CIRCUIT-CIRCUIT BREAKER, 1 POLE, 240 V., 100 AMP. FRAME, 15 AMP. NON-INTERCHANGEABLE TRIP INTERRUPTING RATING NEMA-5000 AMP. AT 240 V.
E	1	ASTRONOMIC MICROPROCESSOR-BASED 2-CHANNEL CONTROLLER [TIME SWITCH].
F	1	20 A., 120 V. FUSE.
G	1	1.5 KVA, SINGLE PHASE, ENCAPSULATED TRANSFORMER 240 X 480 / 120 X 240 VOLT, 60 Hz.
H	1	SPST 20A SWITCH ON DOOR, TO TURN LIGHT ON WHEN DOOR IS OPEN,
I	1	INCANDESCENT LIGHTING FIXTURE ENCLOSED AND GASKETED WITH 60 WATT, 120 V. LAMP.
J	1	20 A., 120 V., DUPLEX RECEPTACLE, GFCI.
K	1	COPPER GROUND BUS 1/4" (6.35) X 1" (25.4) X 12" (304.8 mm) LONG MOUNTED ON PANEL WITH LUGS AND 4 SPARE LUGS
L	1	TOGGLE SWITCHES MOUNTED IN 4" (101.6) X 4" (101.6 mm) BOX.
M	1	COPPER GROUND BUS 1/4" (6.35) X 1" (25.4) X 12" (304.8 mm) LONG MOUNTED ON PANEL WITH LUGS AND SPARE LUGS



FOUNDATION PLAN

CONTROL CABINET

NOTES:

- ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
- UNLESS OTHERWISE INDICATED, THE CABINET SHALL BE MOUNTED ATOP A 4-INCH (101.6 mm) RIGID ALUMINUM SCHEDULE 40 CONDUIT STEM ANCHORED TO A CAST ALUMINUM PEDESTAL BASE.
- IN FRONT OF CONTROL CABINET DOOR, REMOVE VEGETATION AND 2" (50.8 mm) TOP SOIL, LEVEL THE AREA AND ON TOP, PLACE LENGTH WISE PARALLEL TO CONTROL CABINET, A CONCRETE PAD 36" (914.4 mm) x 60" (1524 mm) x 4" (101 mm) MIN. SIZE. THE COST OF LABOR AND MATERIALS ARE INCLUDED IN THE COST OF THE CONTROLLER.
- DOOR SHALL BE CONSTRUCTED FROM SAME TYPE OF MATERIAL AND THICKNESS AS CABINET.
- DOOR SHALL BE EQUIPPED WITH THREE POINT LATCHING MECHANISM WITH NYLON ROLLERS AT TOP THE BOTTOM.
- DOOR HINGE SHALL BE A HEAVY GAUGE CONTINUOUS HINGE WITH A 1/4" (6.35 mm) DIA. STAINLESS STEEL HINGE PIN.
- ALL EXTERNAL HARDWARE SHALL BE STAINLESS STEEL.
- CONTROL WIRING TO BE #12 AWG, 600V, TYPE "SIS" GRAY SWITCH BOARD WIRE, STRANDED COPPER.
- METER BOX SHALL BE MOUNTED ON THE SIDE OF CONTROL CABINET, NEAR TO THE SERVICE POLE.
- CABINETS SHALL BE PRIMED AND PAINTED AS SPECIFIED.
- THE HEADS OF CONNECTORS SCREWS SHALL BE PAINTED WHITE FOR NEUTRAL BAR CONNECTION AND GREEN FOR GROUND BAR CONNECTORS.
- ALL WIRING WITHIN THE CABINET SHALL BE COLOR CODED AS INDICATED.  
R = RED BL = BLUE W = WHITE  
B = BLACK Y = YELLOW G = GREEN
- PROVIDE SEALING GROMMETS FOR ALL OPEN WIRING EXTENDED FROM DEVICES IN BOXES OR CABINETS WITHIN THE CONTROL CABINET.
- ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.
- THE CONTROLLER SHALL BE CONSTRUCTED TO U.L. STD. 508 AND BEAR THE U.L. LABEL "ENCLOSED INDUSTRIAL CONTROL PANEL".
- 12" (304.8) X 16" (406.4 mm) STAINLESS STEEL EXTERIOR NAMEPLATE SHALL BE ENGRAVED TO "STATE OF ILLINOIS LIGHTING CONTROLS" UNLESS OTHERWISE SPECIFIED.

COMPANY NAME:  
PROJECT CONTACT:  
CLIENT:  
DATE PLOTTED:  
FILE NAME:  
PLOT SCALE:  
PLOT DATE:

FILE NAME =	USER NAME = bharta	DESIGNED -	REVISED - D. DREW 12-02-93
		DRAWN -	REVISED - R. TOMSONS 08-19-04
		CHECKED - R. GUPTA	REVISED -
		DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

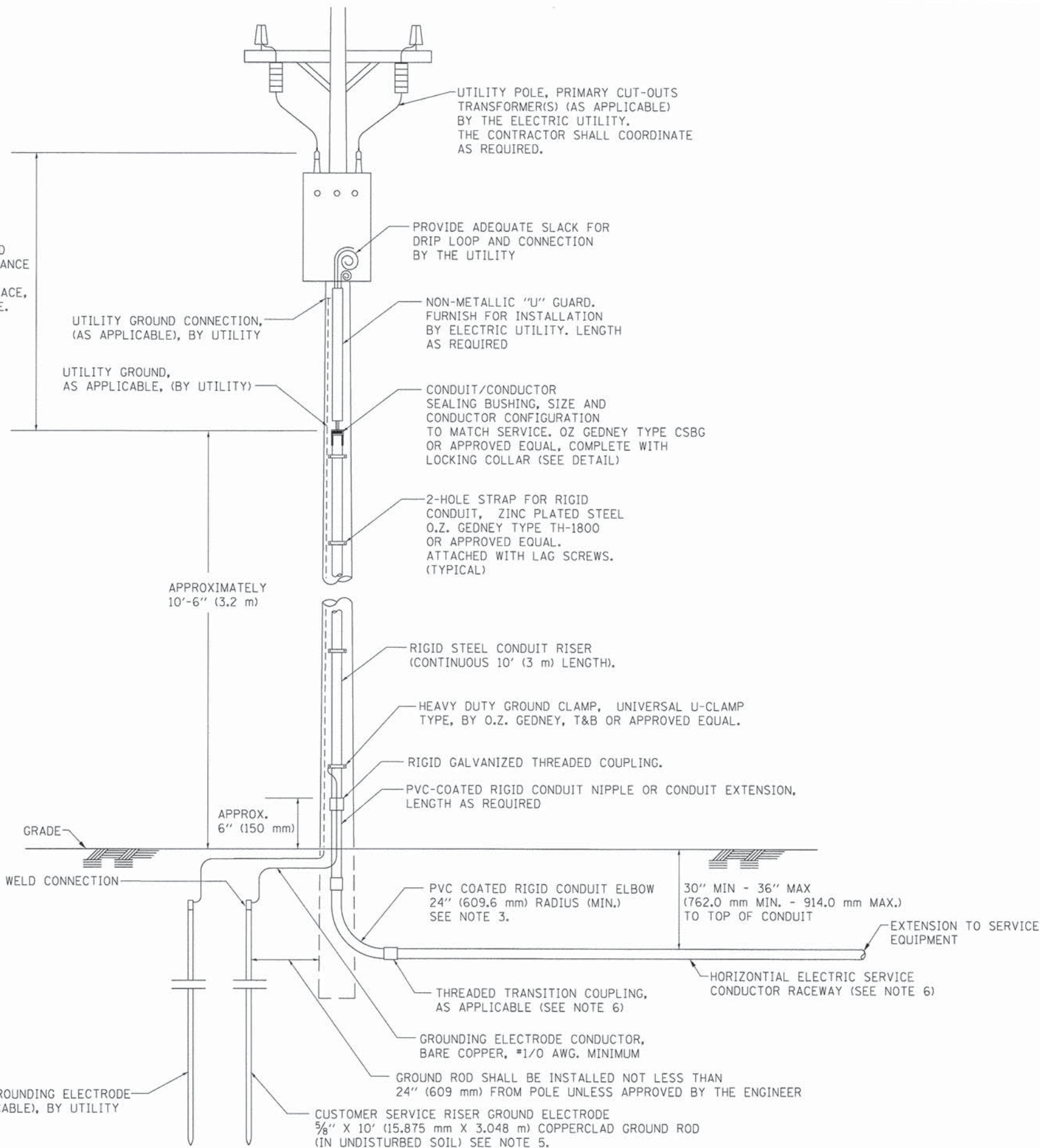
LIGHTING CONTROLLER  
PEDESTAL MOUNT

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	55
BE-210			CONTRACT NO. 63886	
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				



ASCERTAIN AND ASSURE CLEARANCE FROM UTILITY SECONDARY SPACE, AS APPLICABLE.

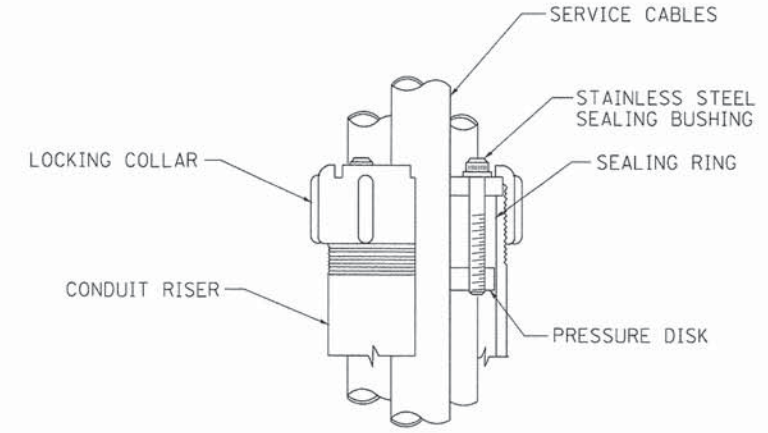


**APPLICATION**

THIS DETAIL APPLIES FOR LOW VOLTAGE ELECTRIC SERVICE (660 V OR LESS) FROM AN OVERHEAD UTILITY SUPPLY TO SEPERATLY-MOUNTED SERVICE EQUIPMENT.

**NOTES**

- SERVICE VOLTAGE SHALL BE AS INDICATED ELSEWHERE IN THE DRAWINGS.
- UNLESS OTHERWISE INDICATED, ITEMS AND WORK SHALL BE INCLUDED AND PAID AS PART OF THE ELECTRIC UTILITY SERVICE INSTALLATION PAY ITEM.
- CONDUIT AND CONNECTOR DIAMETER SHALL MATCH THE DIAMETER OF THE SERVICE CONDUCTOR RACEWAY AS INDICATED ON THE PLANS.
- PVC COATED RACEWAYS AND ACCESSORIES SHALL BE CAREFULLY INSTALLED WITH MFR RECOMMENDED TOOLS AND PROCEDURES TO AVOID DAMAGE. ANY DAMAGE SHALL BE REPAIRED WITH COMPATIBLE PVC TOUCH-UP MATERIAL TO THE SATISFACTION OF THE ENGINEER OR THE DAMAGED MATERIAL SHALL BE REPLACED AT NO ADDITIONAL COST.
- THE CONTRACTOR SHALL OBTAIN INSPECTION AND APPROVAL BY THE ENGINEER OF SERVICE RISER GROUND ELECTRODE, RISER ELBOW, NIPPLE AND CONNECTION TO SERVICE CONDUCTOR RACEWAY EXTENSION BEFORE BACKFILL AND SHALL ALSO OBTAIN INSPECTION OF SERVICE RISER AND SEALING BUSHING BEFORE UTILITY "U" GUARD INSTALLATION AND SERVICE CONNECTION.
- THE HORIZONTAL ELECTRIC SERVICE CONDUCTOR RACEWAY SHALL BE AS INDICATED AND SHALL BE MEASURED SEPARATELY FOR PAYMENT. WHEN THE RACEWAY IS PVC-COATED RIGID GALVANIZED STEEL, THE COUPLING SHALL BE THE SAME. WHEN THE RACEWAY IS PVC CONDUIT (IN CONCRETE), THE COUPLING SHALL BE A METALIC TO NON METALIC ADAPTER. WHEN THE RACEWAY IS ENCASED IN CONCRETE, THE CONCRETE SHALL EXTEND TO COVER THE COUPLING.
- PLANS AND DETAILS INDICATE THE GENERAL NATURE AND REQUIREMENTS. THEY DO NOT SHOW EVERY ACCESSORY AND ATTACHMENT, AND THEY DO NOT RELIEVE THE CONTRACTOR OF THE REQUIREMENTS OF THE SPECIFICATIONS AND SPECIAL PROVISIONS TO ASCERTAIN UTILITY REQUIREMENTS AND TO COORDINATE ACCORDINGLY, FURNISHING ALL ITEMS AND WORK NOT PROVIDED BY THE UTILITY, BUT NECESSARY FOR A COMPLETE SERVICE INSTALLATION IS REQUIRED AND SHALL BE INCLUDED IN THE ELECTRIC UTILITY SERVICE INSTALLATION PAY ITEM.



**SEALING BUSHING DETAIL**

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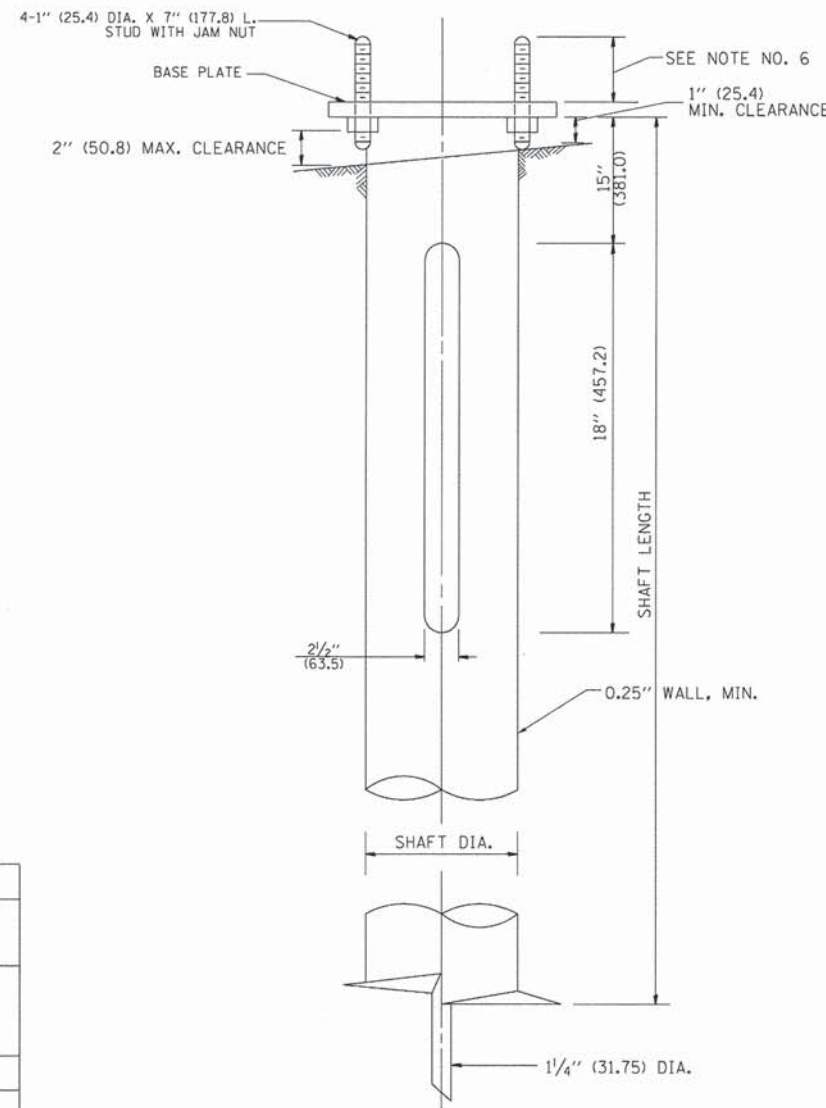
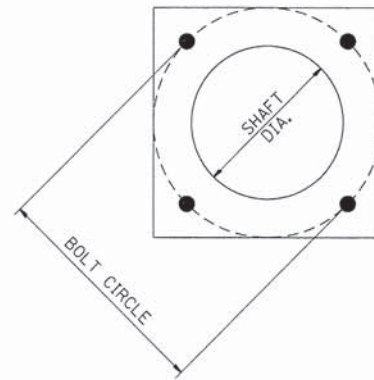
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>ELECTRIC SERVICE INSTALLATION</b>		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
AERIAL, REMOTE DISCONNECT		4058	13-00063-00-PV	MCHENRY	79	56
SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA.	TO STA.	

<b>BE-220</b>		CONTRACT NO. 63886	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			





**HELIX FOUNDATION SIZE**

POLE MOUNTING HEIGHT	BOLT CIRCLE	SHAFT DIAMETER	SHAFT LENGTH	BASEPLATE
30 FT.	11 1/2"	8 5/8"	6 FT.	12"x12"x1"
31 FT.-35 FT.	11 1/2"	8 5/8"	6 FT.	12"x12"x1"
36 FT.-40 FT.	15"	8 5/8"	6 FT.	15"x15"x1 1/4"
41 FT.-45 FT.	15"	8 5/8"	6 FT.	15"x15"x1 1/4"
46 FT.-50 FT.	15"	10"	8 FT.	15"x15"x1 1/4"

**METAL HELIX FOUNDATION MATERIALS**

ITEM	MATERIAL REQUIREMENT
BASEPLATE	AASHTO M 270M, GRADE 36 (M270M, GRADE 250)
SHAFT	ASTM A 252, GRADE 2 (PHOSPHOROUS 0.04% MAXIMUM, SULFUR 0.05% MAXIMUM)
HELIX SCREW	AASHTO M 183 (ASTM A 635)
PILOT POINT	AASHTO M 270 (ASTM A 575)
ANCHOR RODS/STUDS	AASHTO M 314 (ASTM F 1554)
HEXAGON NUTS	AASHTO M 291M (ASTM A 563) GRADE DH, OR AASHTO M 292 (ASTM A 194) GRADE 2H
WASHERS	AASHTO M 293 (ASTM F 436)

**NOTES:**

- ALL DIMENSION IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
- ALL MATERIAL SHALL BE GALVINIZED ACCORDING TO AASHTO M111, UNLESS OTHERWISE SPECIFIED.
- ALL WELDS SHALL BE CONTINUOUS AND NOT LESS THAN 1/4" (6.35 mm) FILLET WELDS. THE WELDED FOUNDATION SHALL BE CAPABLE OF WITHSTANDING 10,000 FT/LBS (13558.18 n.m) OF INSTALLATION TORQUE APPLIED ABOUT THE AXIS OF THE FOUNDATION.
- THE HELIX FOUNDATION SHAFT SHALL BE INSTALLED VERTICAL AND THE BASE PLATE SHALL BE IN LEVEL. THE BREAKAWAY COUPLINGS AND HARDWARE SHALL NOT BE USED TO ALIGN THE POLE INSTALLATION.
- THE CABLE TRENCH SHALL BE BACKFILLED AND FIRMLY COMPACTED BEFORE THE INSTALLATION OF THE LIGHT POLE.
- THE CONTRACTOR SHALL COORDINATE EXTENSION OF ANCHOR BOLTS ABOVE TOP OF THE BASE PLATE WITH THE BREAKAWAY DEVICE MANUFACTURER'S REQUIREMENTS.
- ANY VOIDS WITHIN THE METAL FOUNDATION SHALL BE FILLED WITH FINE AGGREGATE.
- METAL FOUNDATIONS SHALL BE INSTALLED IN UNDISTURBED SOIL. PREDRILLING A PILOT HOLE AND/OR BACKFILLING AROUND THE FOUNDATION IS NOT ALLOWED.
- THE METAL FOUNDATION SHALL NOT BE INSTALLED TO A TORQUE WHICH EXCEEDS THE MANUFACTURER'S MAXIMUM TORQUE RATING NOR SHALL IT BE INSTALLED TO AN INSTALLATION TORQUE VALUE OF LESS THAN 3,500 FT LB (4,750 KNM). METAL FOUNDATIONS THAT ARE NOT INSTALLED TO FULL INSTALLATION DEPTH OR DO NOT ACHIEVE THE MINIMUM INSTALLATION TORQUE SHALL BE REMOVED AND REPLACED WITH A CONCRETE FOUNDATION AT NO ADDITIONAL COST.
- THE BASEPLATE SHALL BE PERPENDICULAR TO THE SHAFT AXIS ( $\pm 1^\circ$ ) AND THE HOLE CENTERLINE SHALL BE CONCENTRIC ( $\pm 0.188$ ) TO THE SHAFT AXIS.
- THE PILOT POINT AND SHAFT AXIS SHALL BE CONCENTRIC ( $\pm 0.125$ ) AND IN LINE ( $\pm 2^\circ$ ).
- THE BASEPLATE SHALL BE STAMPED WITH THE MANUFACTURERS NAME AND DATE OF MANUFACTURE.

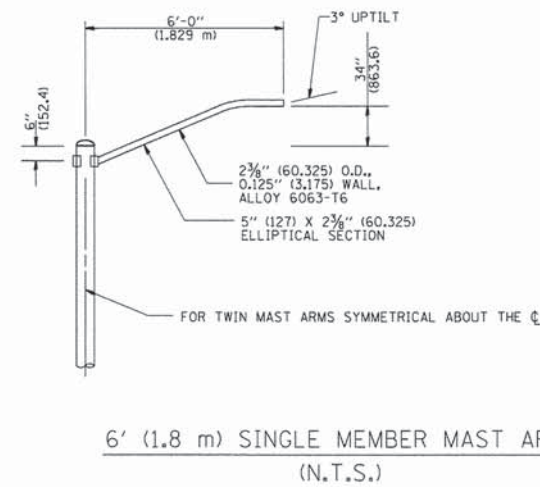
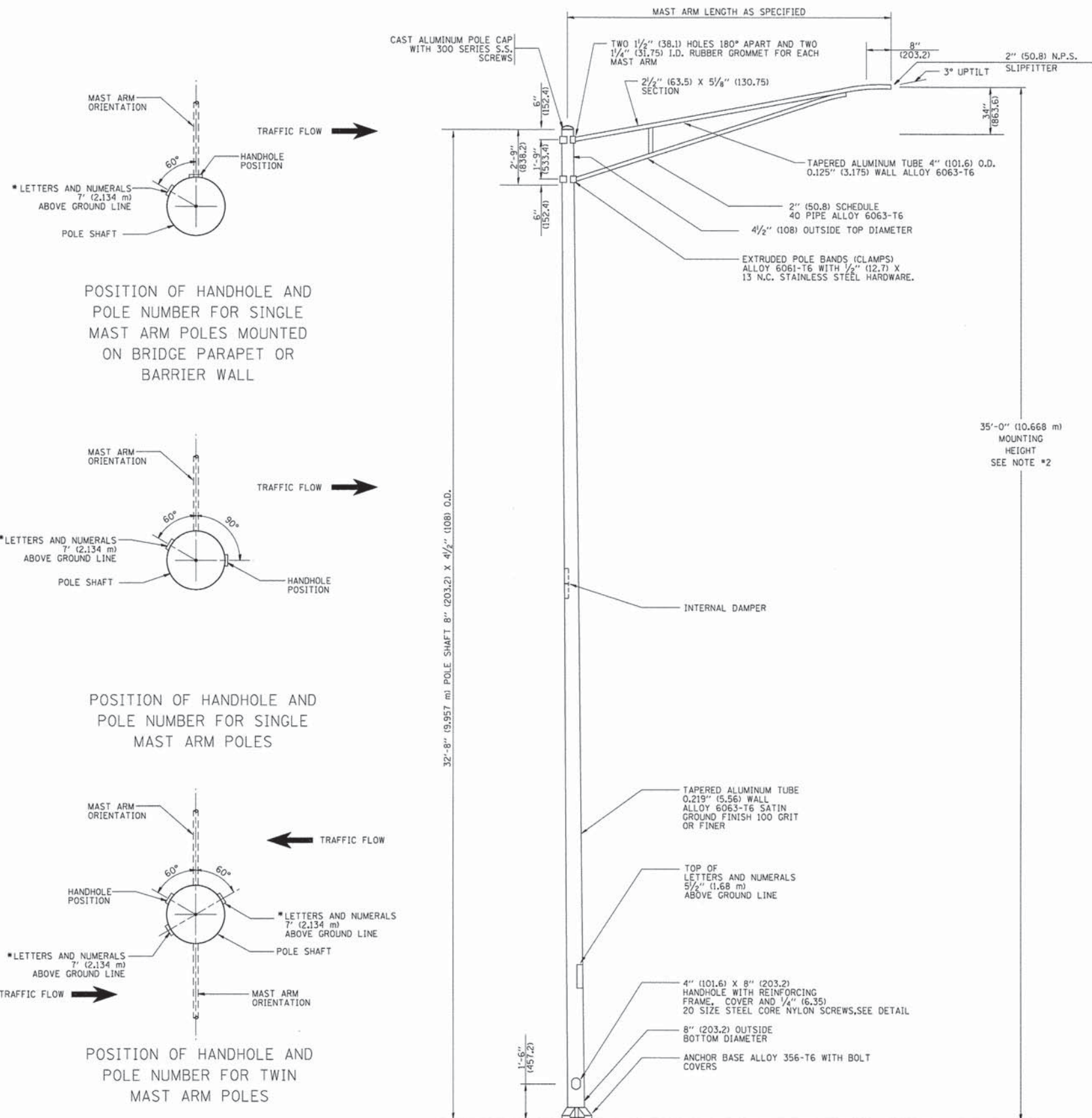
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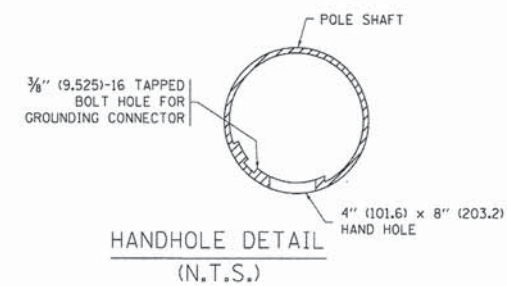
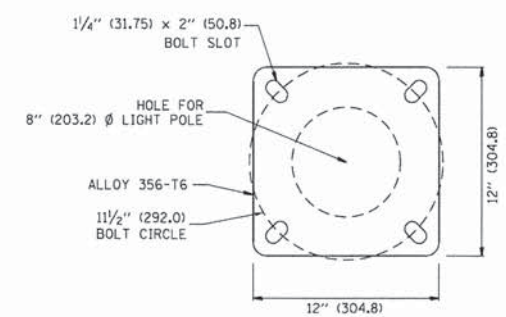
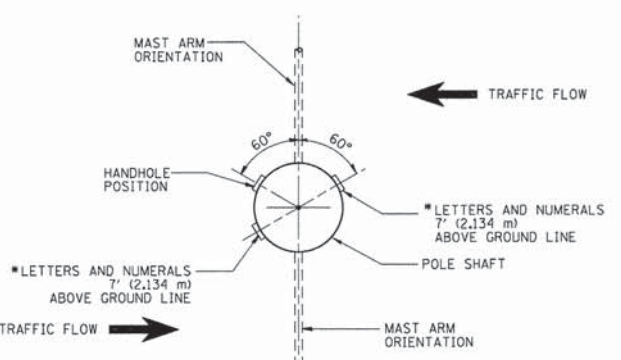
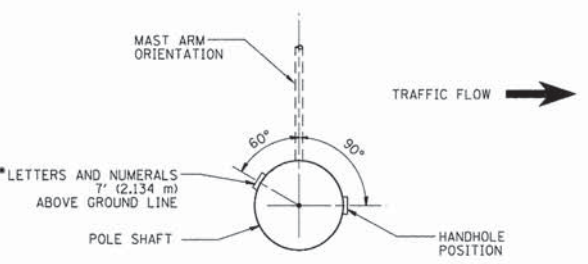
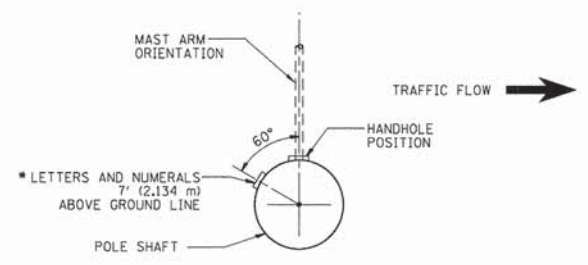
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

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						4058	13-00063-00-PV	MCHENRY	79	57
						<b>BE-305</b>		CONTRACT NO.		63886
FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT										





- NOTES:**
1. ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
  2. MOUNTING HEIGHT IS DEFINED AS THE DISTANCE FROM THE CENTERLINE OF THE TENON TO THE BOTTOM OF THE ANCHOR BASE.
  3. THE LIGHT POLE WILL MEET AASHTO DESIGN CRITERIA AS SPECIFIED.
  4. THE INSTALLING CONTRACTOR WILL PROVIDE A UL LISTED GROUNDING CONNECTOR, BURNDY K2C23, T&B SP4DL OR APPROVED EQUAL.
  5. LIGHT POLES WILL NOT BE INSTALLED WITHOUT MAST ARMS AND LUMINAIRES.
  6. LIGHT POLES WILL BE SET PLUMB ON THE FOUNDATION WITHOUT THE USE OF LEVELING NUTS, WASHERS OR SHIMS.
  7. LIGHTING UNIT IDENTIFICATION NUMBERS SHALL BE INSTALLED BEFORE THE LIGHTING UNIT IS ENERGIZED.



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PROJECT CONTACT:  
CLIENT:  
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

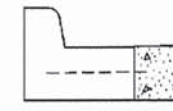
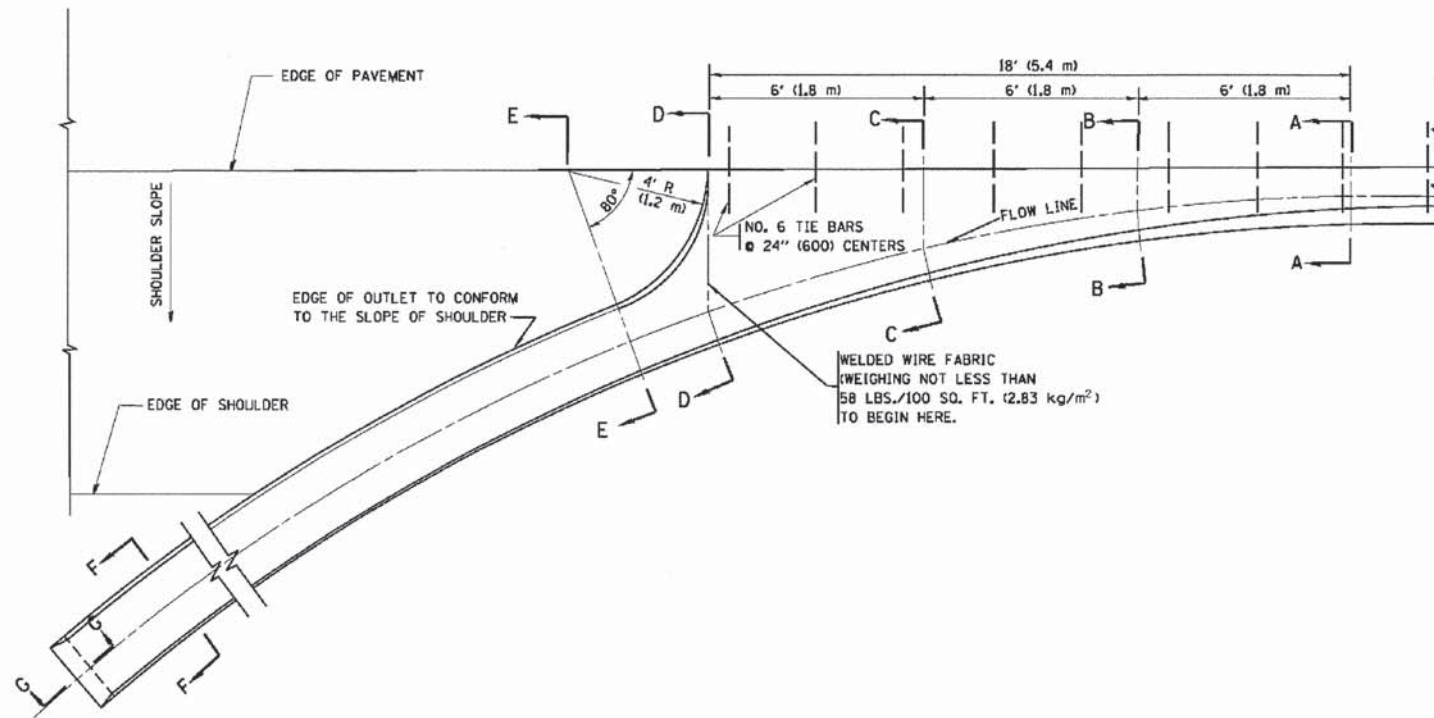
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<b>35'-0" (10.668 m) MOUNTING HEIGHT</b>	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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FED. ROAD DIST. NO. 1 [ILLINOIS] FED. AID PROJECT				



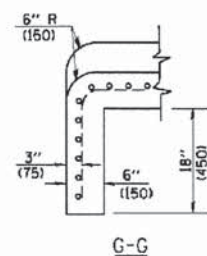
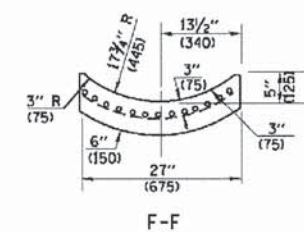
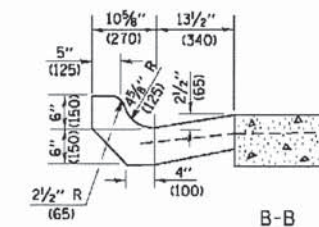
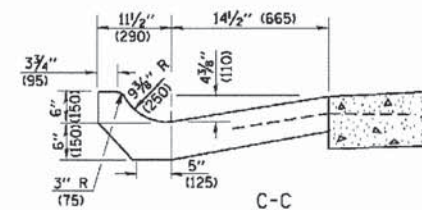
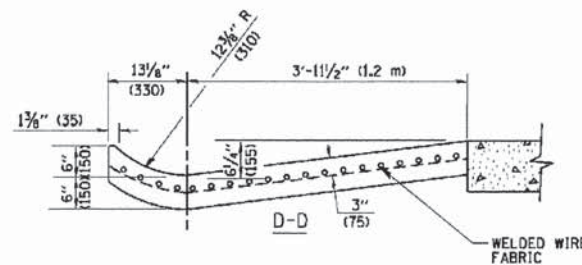
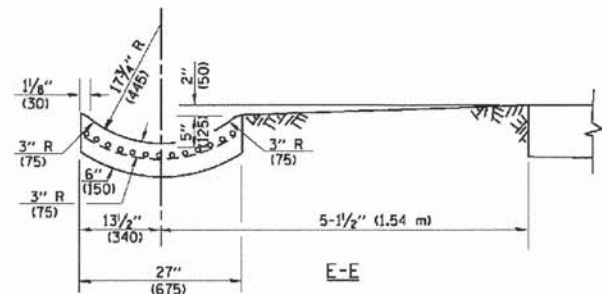






A-A \*

\* DIMENSIONS OF THE CURB & GUTTER AT SECTION A-A ARE SHOWN ON STATE STANDARD 606001. FOR DETAILS OF OUTLET FOR CONCRETE CURB & GUTTER, TYPE B-6.24 (B-15.60) SEE STATE STANDARD 606006.



**GENERAL NOTES**

GUTTER OUTLET SHALL BE TIED TO THE PAVEMENT IN ACCORDANCE WITH DETAILS FOR LONGITUDINAL CONSTRUCTION JOINT SHOWN ON STANDARD 420001.

TIE BARS SHALL BE NO. 20 (NO.6) AT 24\"/>

IF THE AVERAGE GRADE OF PAVEMENT FOR THE DISTANCE FROM SECTION A-A TO D-D EXCEEDS 2%, THIS DISTANCE SHALL BE INCREASED 6' (1.8 m) FOR EACH 1% INCREASE IN GRADE.

**QUANTITIES**

FOR SECTION A-A TO E-E AND CURTAIN WALL =  
 1.25 CU. YDS. (0.96 m<sup>3</sup>) CLASS SI CONCRETE (OUTLET) FOR 9\"/>

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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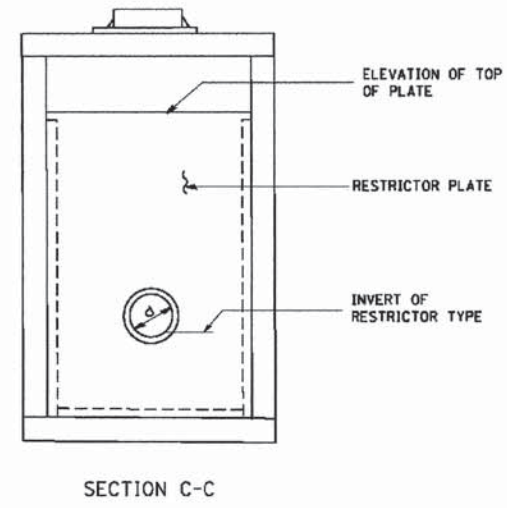
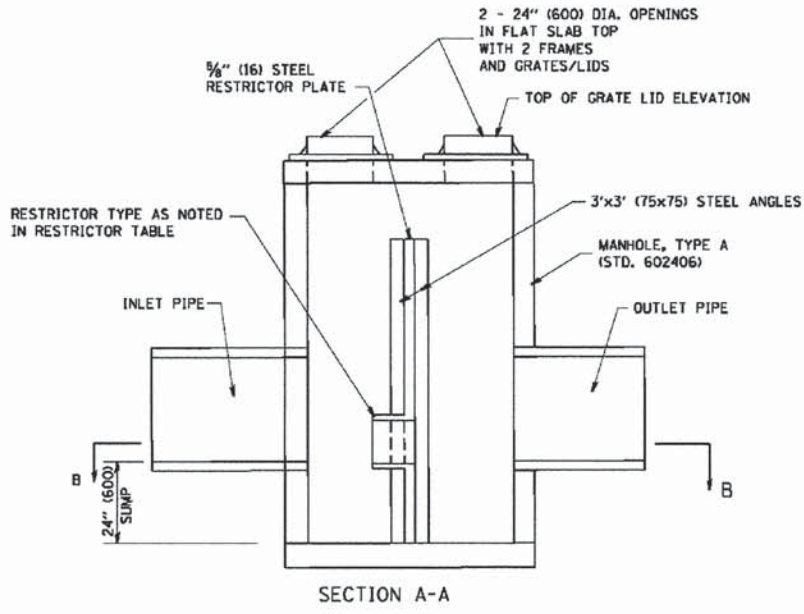
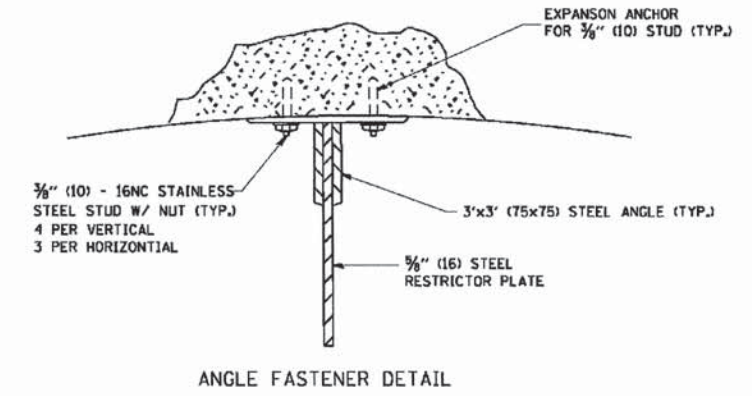
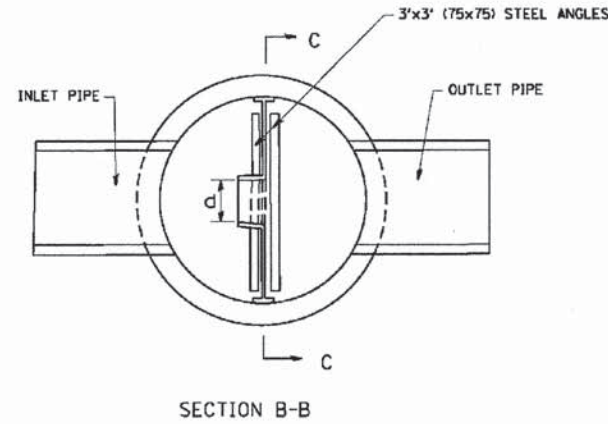
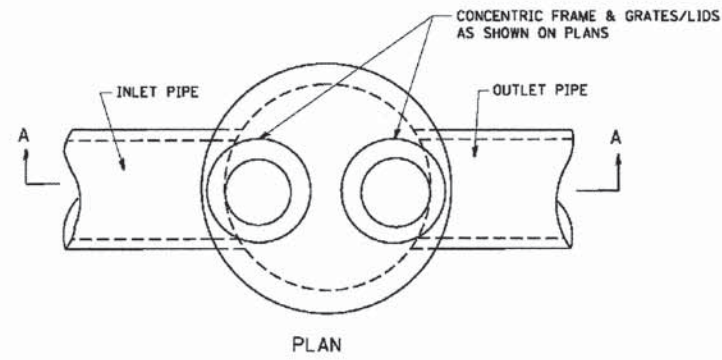
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**OUTLET FOR CONCRETE  
CURB AND GUTTER**

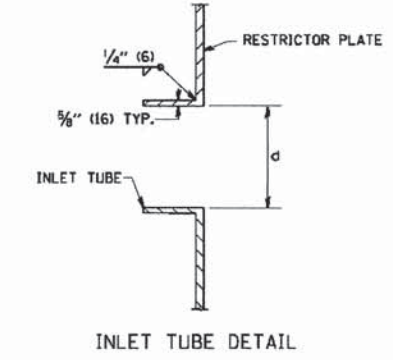
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F.A. RTE. 4058	SECTION 13-00063-00-PV	COUNTY MCHENRY	TOTAL SHEETS 79	SHEET NO. 60
BD600-01 (BD-03)		CONTRACT NO.	63886	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

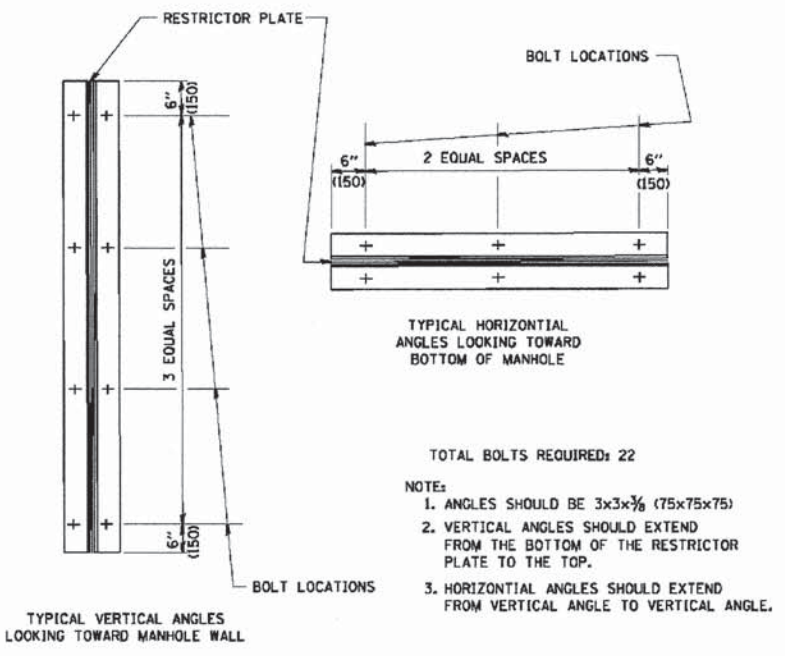




- NOTES:
1. ALL STEEL ANGLES AND PLATES TO BE GALVANIZED AFTER FABRICATION.
  2. ALL RESTRICTOR PLATES, ANGLES AND HARDWARE TO BE INCLUDED IN THE COST OF THE MANHOLE.
  3. BASIS OF PAYMENT: "MANHOLES, TYPE A, 6 FT. (1.8 m)-DIAMETER, TYPE I FRAME, CLOSED LID, RESTRICTOR PLATE" EACH



STATION	MANHOLE DIAMETER	FRAME AND GRATE	RESTRICTOR TYPE	INSIDE RESTRICTOR TYPE DIAMETER in. (mm) (d)	INVERT OF RESTRICTOR TYPE	ELEVATION OF TOP OF PLATE OVERFLOW
19+45, 50.0 LT	6'	T1CL	3	12"	792.58	793.88



- TOTAL BOLTS REQUIRED: 22
- NOTE:
1. ANGLES SHOULD BE 3x3x3/8 (75x75x75)
  2. VERTICAL ANGLES SHOULD EXTEND FROM THE BOTTOM OF THE RESTRICTOR PLATE TO THE TOP.
  3. HORIZONTAL ANGLES SHOULD EXTEND FROM VERTICAL ANGLE TO VERTICAL ANGLE.

RESTRICTOR TYPE					
1	2	3	4	5	6
RE-ENTRANT TUBE	SHARP EDGED	SQUARE EDGED	RE-ENTRANT TUBE	SQUARE EDGED	ROUNDED
LENGTH: 1/2 TO 1 DIA.		STREAM CLEARS SIDES	LENGTH: 2-1/2 DIA.	LENGTH: 2-1/2 DIA.	
C=.52	C=.61	C=.61	C=.73	C=.82	C=.98

VALUES OF "C" FOR CIRCULAR AND SQUARE ORIFICES

COMPANY NAME:  
PROJECT CONTACT:  
CLIENT:  
DATE PLOTTED:  
PLOT DRIVER:  
PEN TABLE:

FILE NAME =	USER NAME = gng10nabt	DESIGNED - R. SHAH	REVISED - R. SHAH 10-25-94
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	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - M. GOMEZ 01-08-01
	PLOT DATE = 1/4/2008	DATE - 09-09-94	REVISED -

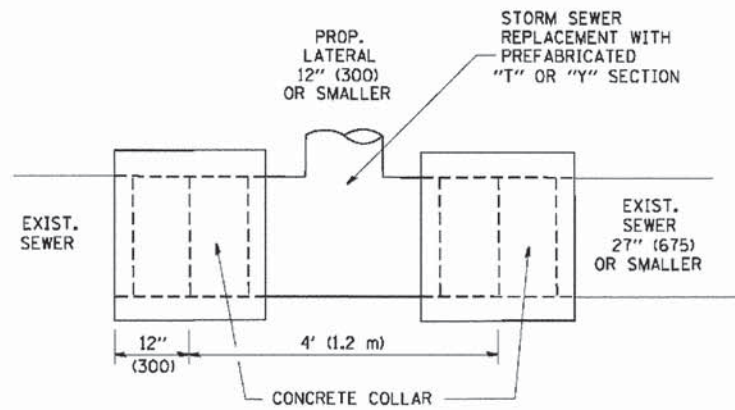
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.
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F.A. RTE. 4058	SECTION 13-00063-00-PV	COUNTY MCHENRY	TOTAL SHEETS 79	SHEET NO. 60A
BD600-04 (BD-12)		CONTRACT NO. 63886		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

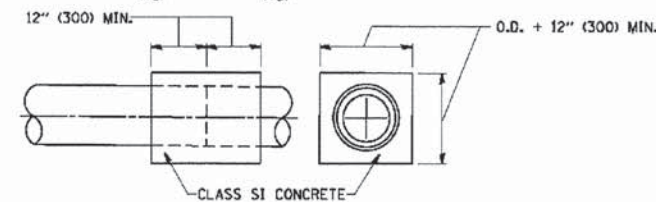
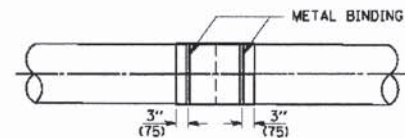
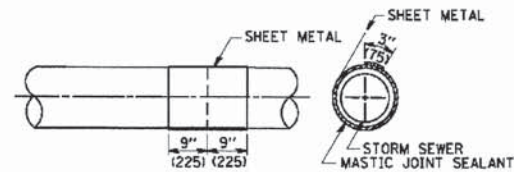
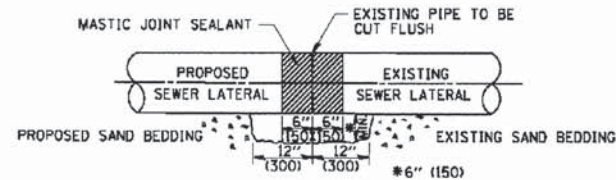
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.





DETAIL "A"

LATERAL CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER

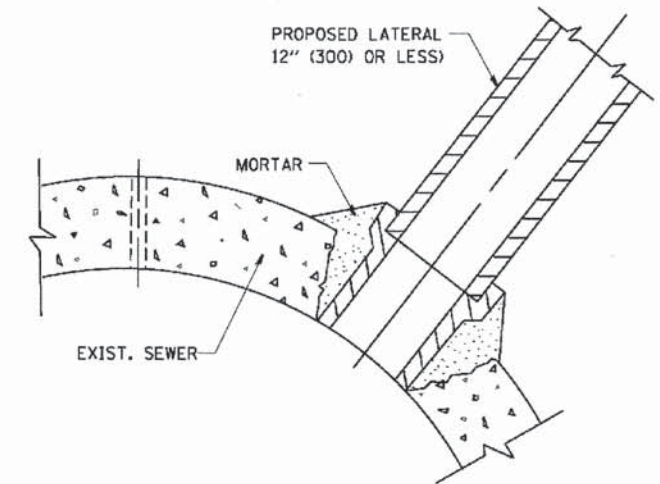


DETAIL "B"

CLASS SI CONCRETE COLLAR

CONSTRUCTION SEQUENCE

- CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
- APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
- BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12" x 6" (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
- CUT A PIECE OF SHEET METAL GAGE NO. 19 L1 (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 3" (75) LONG.
- WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
- LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
- PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
- WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OZZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
- PLACE CLASS SI CONCRETE AROUND THE JOINT.



DETAIL "C"

PROPOSED LATERAL CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER

NOTES

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

- THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
  - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
  - PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

GENERAL

CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER. ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.

CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS, THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.

REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.

TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.

CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

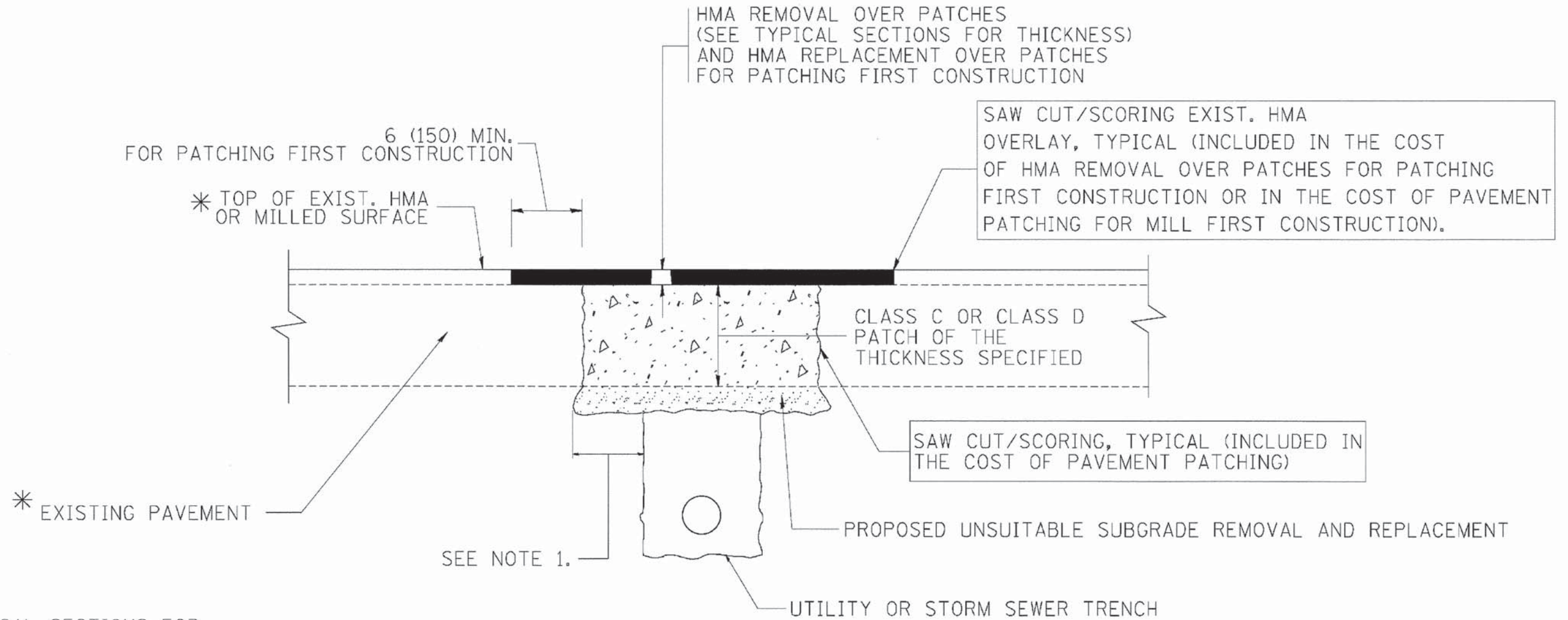
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PROJECT CONTACT:  
CLIENT:  
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			REVISED - R. SHAH 10-25-94
			REVISED - R. SHAH 06-12-96

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

DETAIL OF STORM SEWER CONNECTION TO EXISTING SEWER		F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	4058	13-00063-00-PV	MCHENRY	79	61
STA. TO STA.		BD500-01 (BD-7)		CONTRACT NO.		63886
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT						





\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE SECTION 442 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, ADOPTED JANUARY 1, 2007.

**SEQUENCE OF CONSTRUCTION (PATCHING FIRST)**

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

**SEQUENCE OF CONSTRUCTION (MILLING FIRST)**

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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PROJECT CONTACT:  
CLIENT:  
DESIGNER:  
FILE NAME:  
PLOT DRIVER:  
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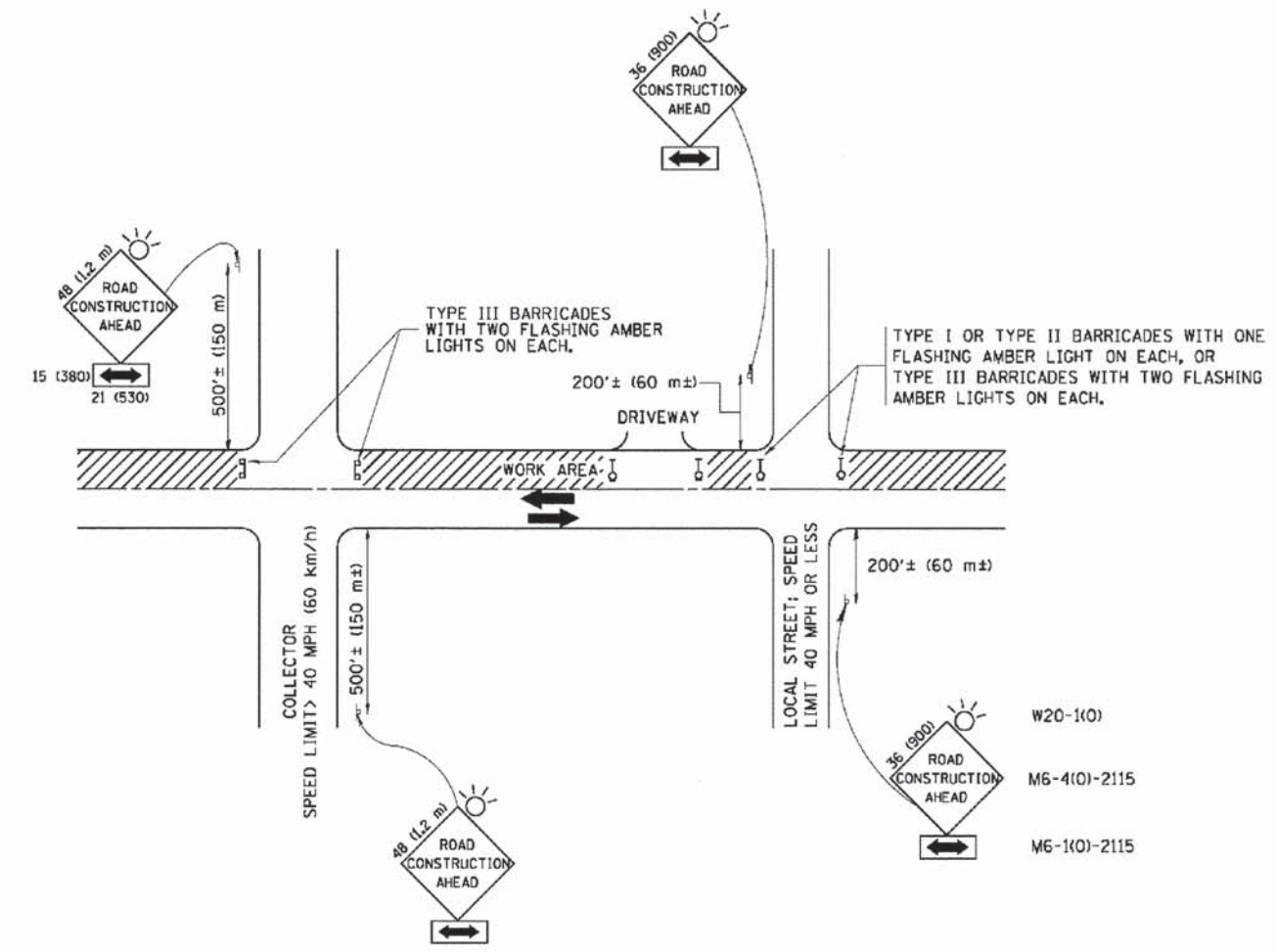
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT PATCHING FOR  
HMA SURFACED PAVEMENT**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	62
<b>BD400-04 (BD-22)</b>		<b>CONTRACT NO.</b>	63886	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				





### TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

#### NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS:
  1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
  2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
    - a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
    - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
 

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (Inches) unless otherwise shown.

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PROJECT CONTACT:  
CLIENT:  
DATE PLOTTED:  
FILE NAME:  
PLOT DATE:  
PLOT SCALE:  
FOR TABLES:

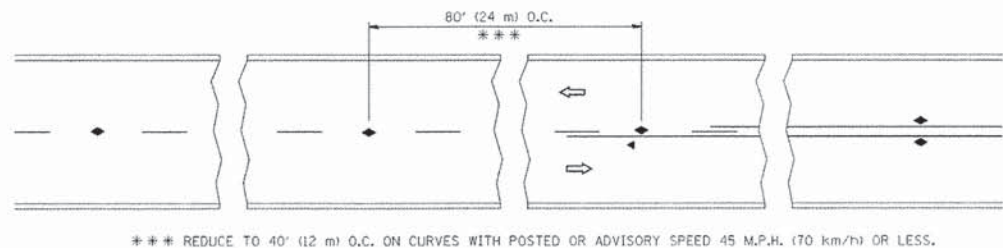
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### STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

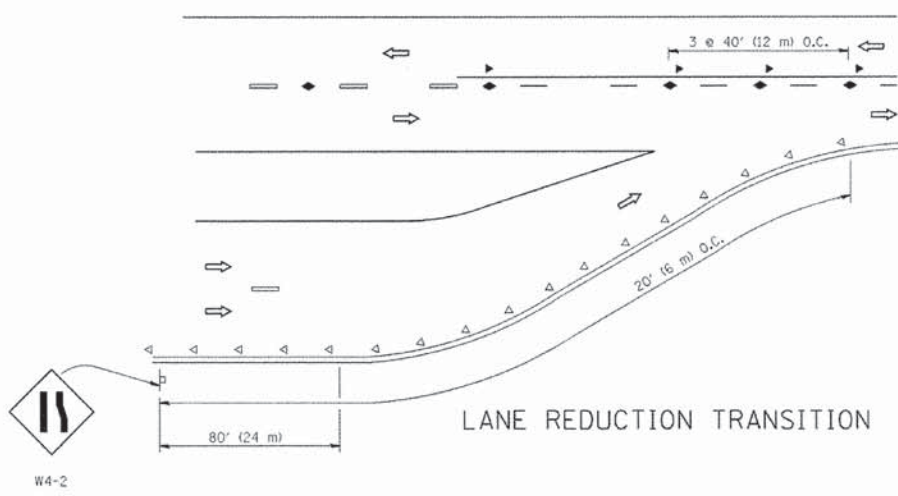
TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	63
TC-10			CONTRACT NO. 63886	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				

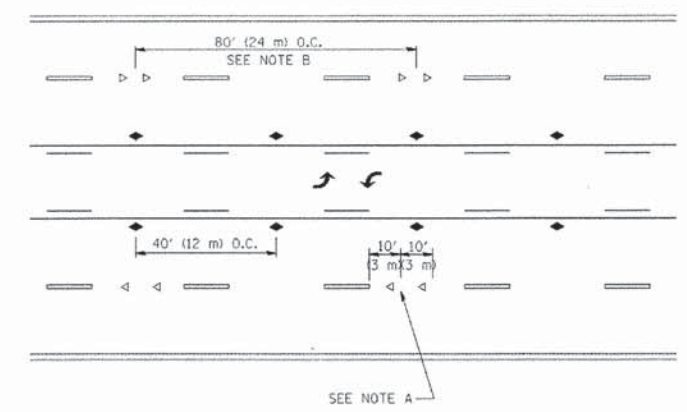




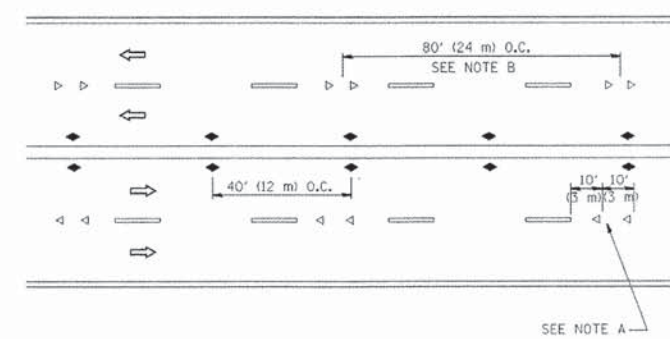
TWO-LANE/TWO-WAY



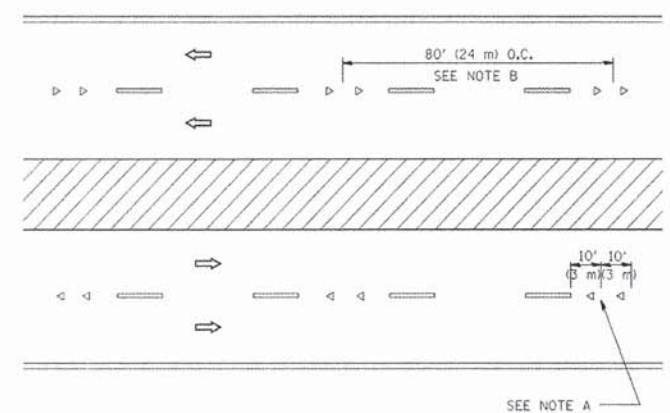
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

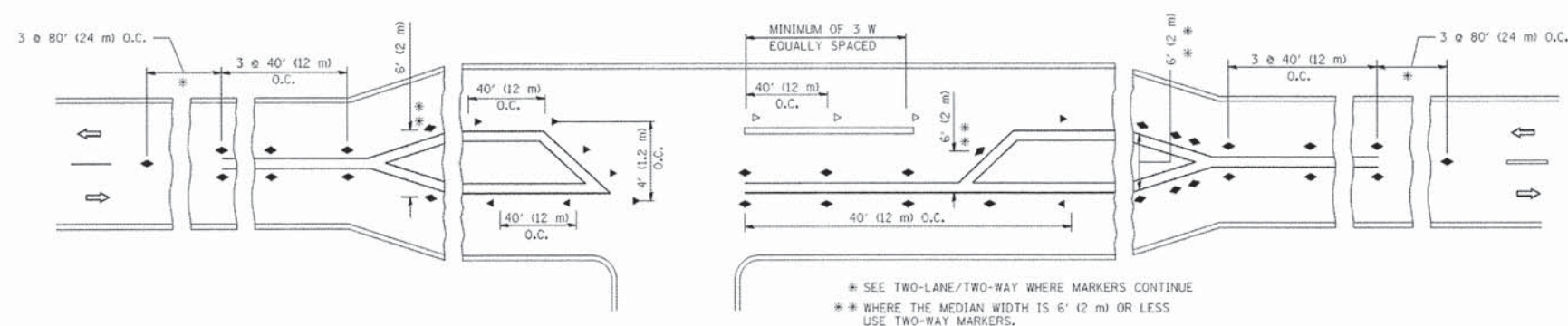
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◁ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

COMPANY NAME: 12/5/2013 9:10:09 AM  
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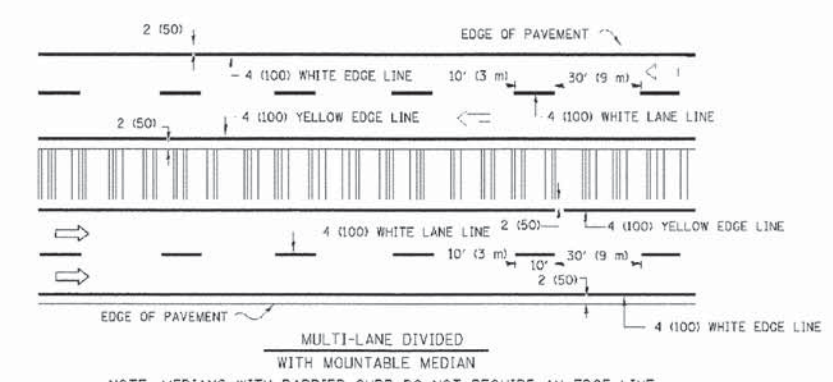
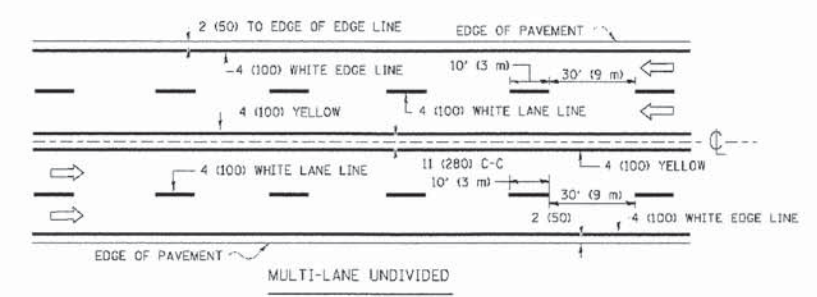
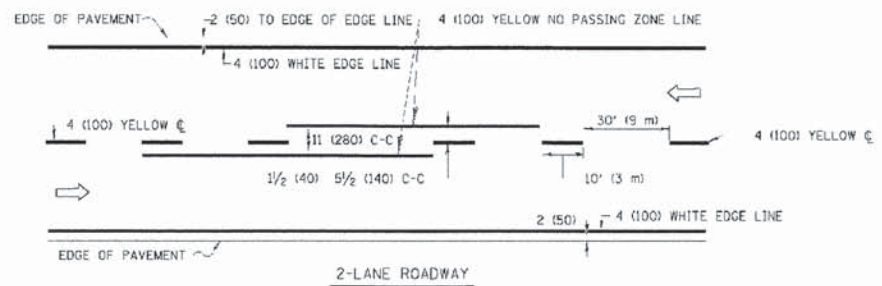
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

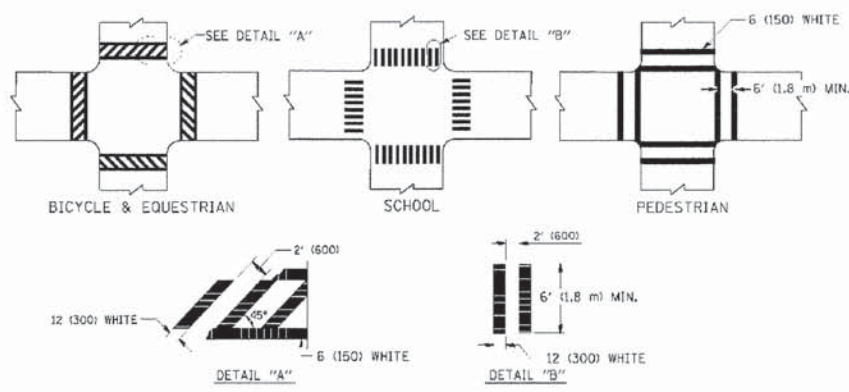
TYPICAL APPLICATIONS			
RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
MCHEMRY	VILLAGE OF CARY	86120253	79	64
TC-11			CONTRACT NO.	63886
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

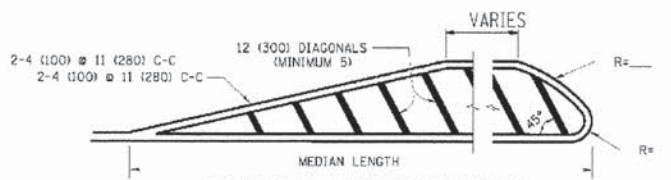
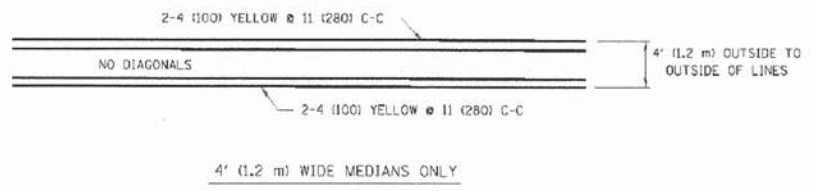




NOTE: MEDIANS WITH BARRIER CURBS DO NOT REQUIRE AN EDGE LINE  
**TYPICAL LANE AND EDGE LINE MARKING**

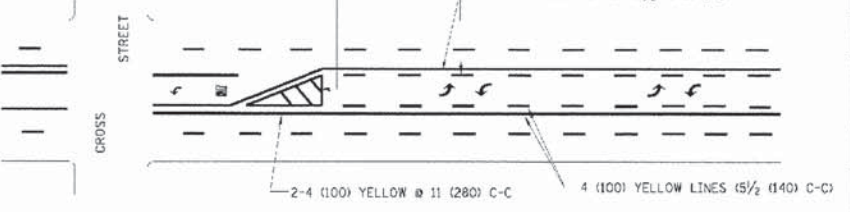


**TYPICAL CROSSWALK MARKING**

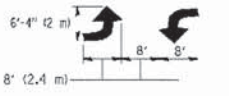


FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.  
 DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
 75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
 150' (45 m) C-C (OVER 45MPH (70 km/h))

**MEDIANS OVER 4' (1.2 m) WIDE**

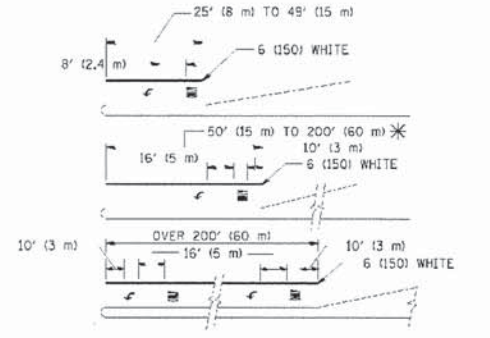


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



**MEDIAN WITH TWO-WAY LEFT TURN LANE**

**TYPICAL PAINTED MEDIAN MARKING**

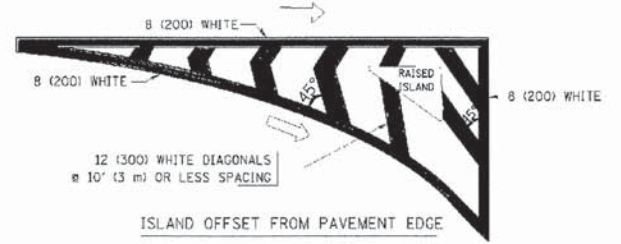


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
 \* AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>) ONLY AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

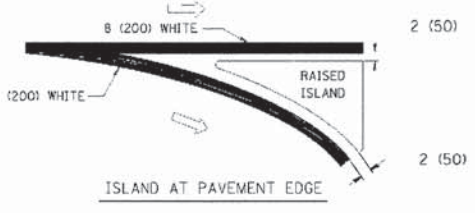
\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

**TYPICAL LEFT (OR RIGHT) TURN LANE**

**TYPICAL TURN LANE MARKING**



**ISLAND OFFSET FROM PAVEMENT EDGE**



**ISLAND AT PAVEMENT EDGE**

**TYPICAL ISLAND MARKING**

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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 PROJECT CONTACT:   
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		CHECKED -	REVISED -
		DATE -	REVISED -

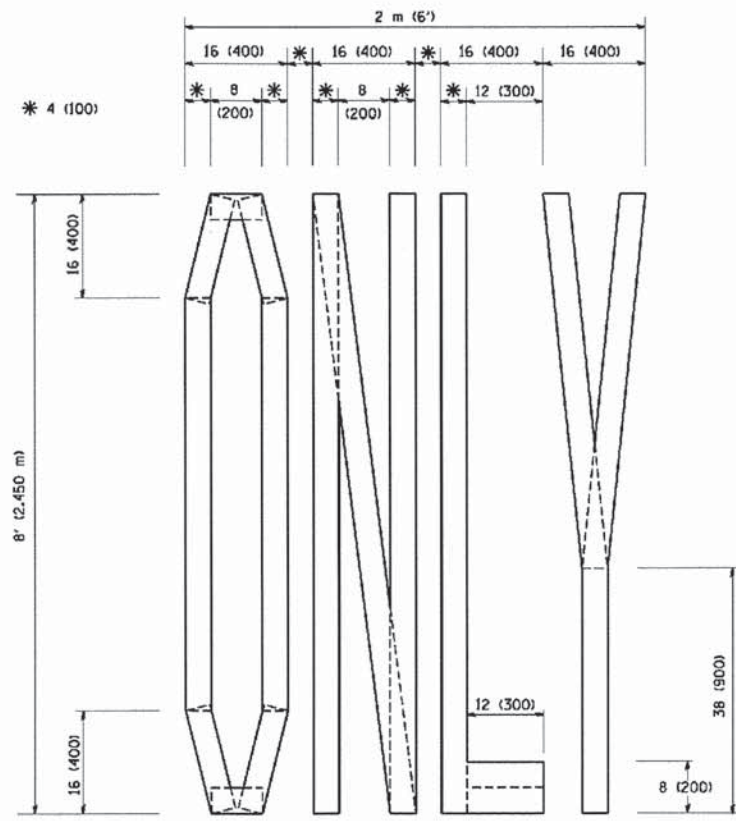
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**DISTRICT ONE  
 TYPICAL PAVEMENT MARKINGS**

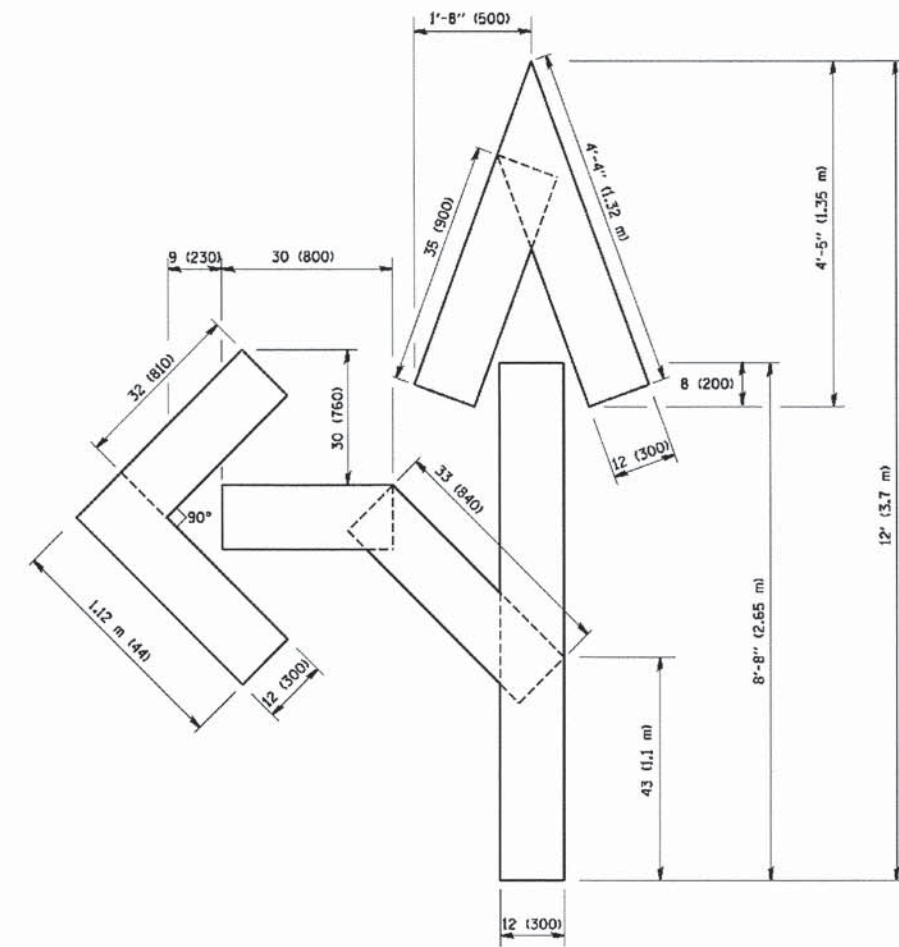
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F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS NO.
4058	13-00063-00-PV	MCHENRY	79 65
<b>TC-13</b>		<b>CONTRACT NO. 63886</b>	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			

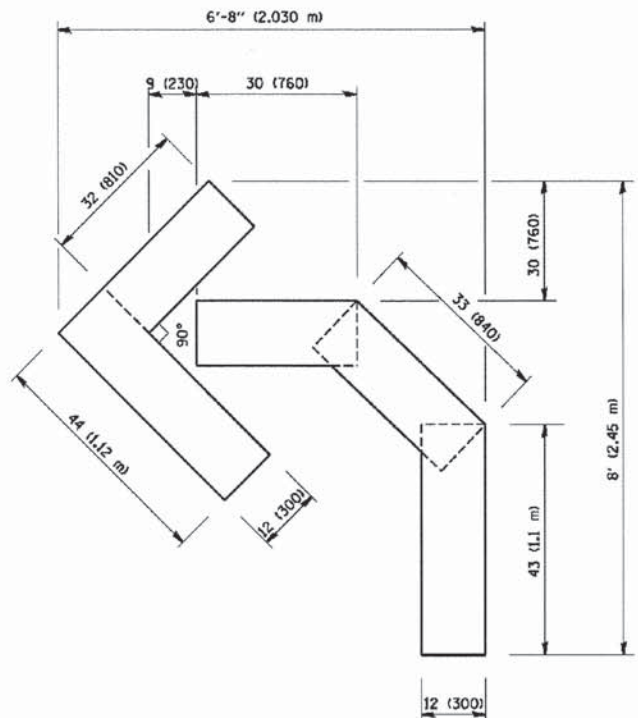




QUANTITY  
 4 (100) LINE = 64.1 ft. (19.7 m)  
 21.1 sq. ft. (1.97 sq. m)



QUANTITY  
 4 (100) LINE = 82.5 ft. (25.3 m)  
 27.5 sq. ft. (2.53 sq. m)



QUANTITY  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.39 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

COMPANY NAME:  
 PROJECT CONTACT:  
 CLIENT:  
 DATE PLOTTED:  
 PLOT NUMBER:  
 PLOT BROWSER:  
 PEN TABLE:

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 USER NAME = gogliano  
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 PLOT DATE = 1/4/2008

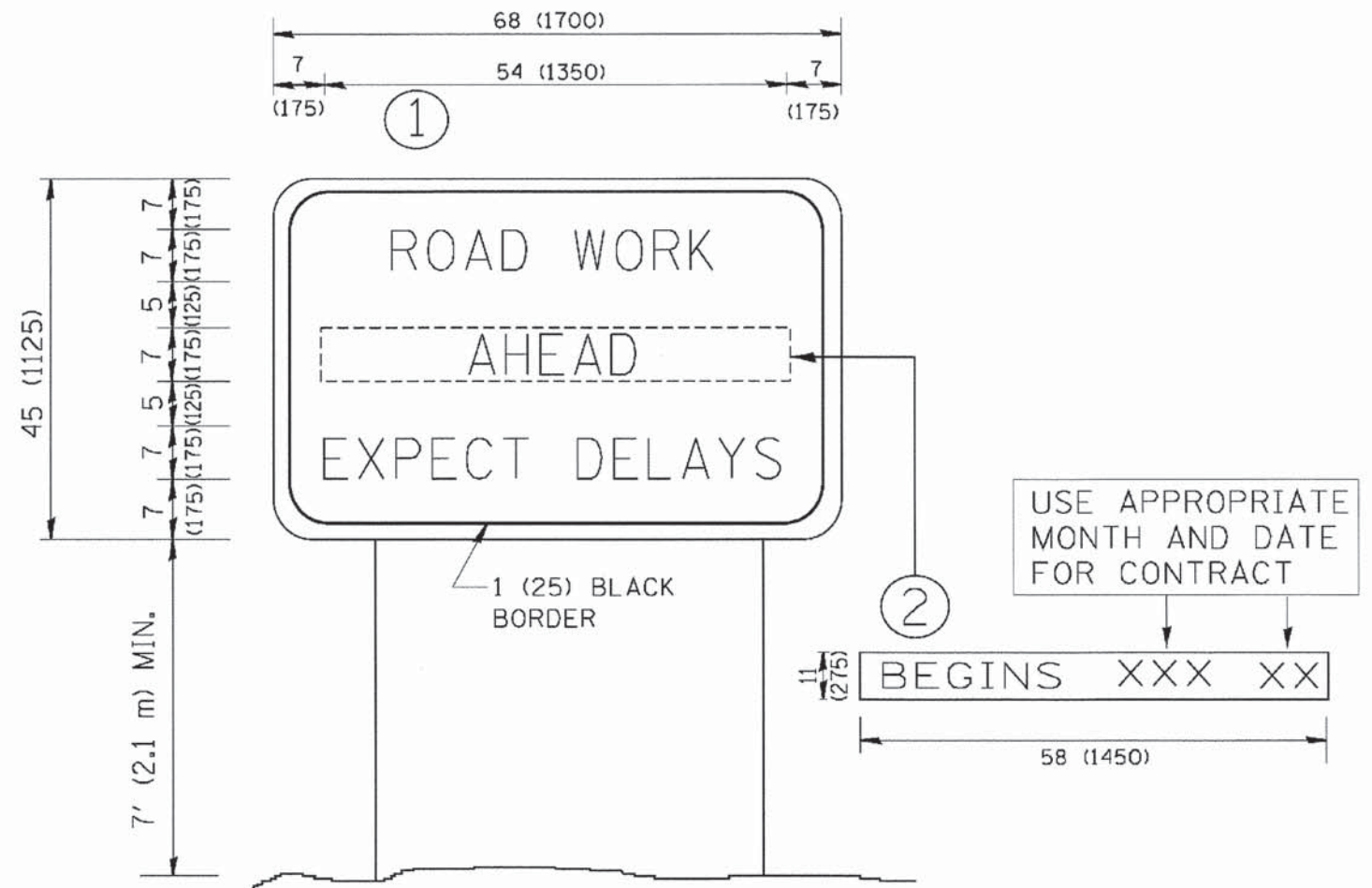
DESIGNED -	REVISED - T. RAMMACHER 06-05-96
DRAWN -	REVISED - T. RAMMACHER 11-04-97
CHECKED -	REVISED - T. RAMMACHER 03-02-98
DATE - 09-18-94	REVISED - E. GOMEZ 08-28-00

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING			
SCALE: NONE	SHEET NO. 1	OF 1 SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	66
TC-16			CONTRACT NO. 63886	
FED. ROAD DIST. NO. J ILLINOIS FED. AID PROJECT				





**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

COMPANY NAME:  
 PROJECT CONTACT:  
 CLIENT:  
 DATE PLOTTED:  
 PLOT NUMBER:  
 PEN TABLE:

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	PLOT DATE = 1/4/2008	DATE -	REVISED - C. JUJUS 01-31-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD  
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	67
TC-22			CONTRACT NO. 63886	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				





3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED  
 "DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

**NOTES:**

1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE  
 PLACED BACK-TO-BACK; ONE WITH A RIGHT HAND ARROW (SHOWN)  
 SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY  
 AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE  
 FAR LEFT SIDE OF THE DRIVEWAY.
3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

COMPANY NAME:  
 PROJECT CONTACT:  
 CLIENT:  
 DATE PLOTTED:  
 PLOT DRIVER:  
 PEN TABLE:

FILE NAME =  
 W:\data\td\22x34\te26.dgn

USER NAME = geglienobt	DESIGNED -	REVISED - C. JUCIUS 02-15-07
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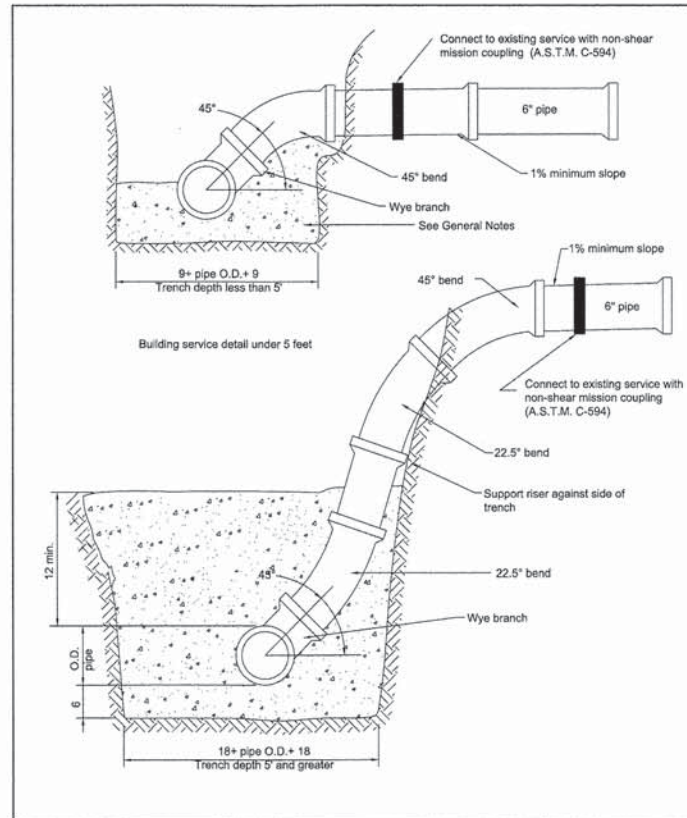
STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

DRIVEWAY ENTRANCE SIGNING

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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TC-26			CONTRACT NO. 63886	
FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				

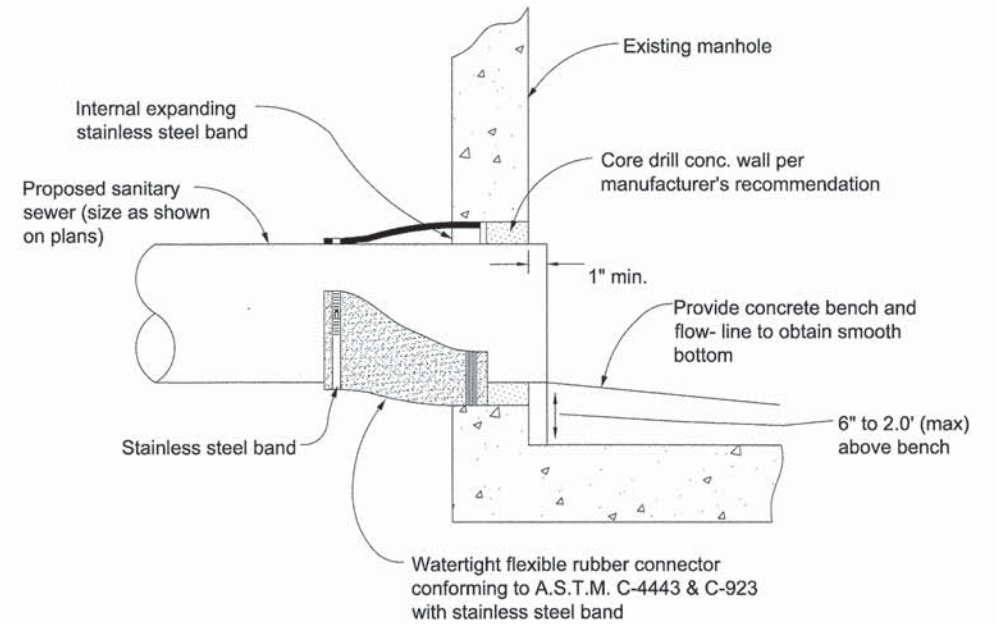




**General Notes:**

1. All dimensions are in inches unless otherwise specified.
2. Depths: 12' or less should comply with SDR 26 PVC sewer pipe per A.S.T.M. D-3034.
3. Installations between 12' and 20' deep shall be SDR 21.
4. Installations over 20' deep shall be class 52 ductile iron.
5. New services to be PVC only.
6. Bedding should be CA-7 and backfill should be CA-6.
7. Service inverts shall be at or above the crown of the mainline sewer.

Sanitary Service



Note: Fill annular space with grout

Connection to Existing Manhole

COMPANY NAME: HRGreen.com  
 PROJECT CONTACT: Illinois Professional Design Firm #184-001322  
 DATE PLOTTED: 12/5/2013 3:11:51 AM  
 FILE NAME: 86120253-001-03.dgn  
 PLOT DRIVER: pdf24pt  
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PLOT DATE = 12/5/2013	DATE -	REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

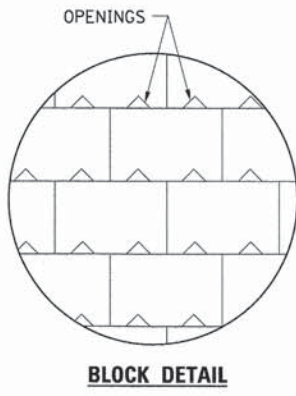
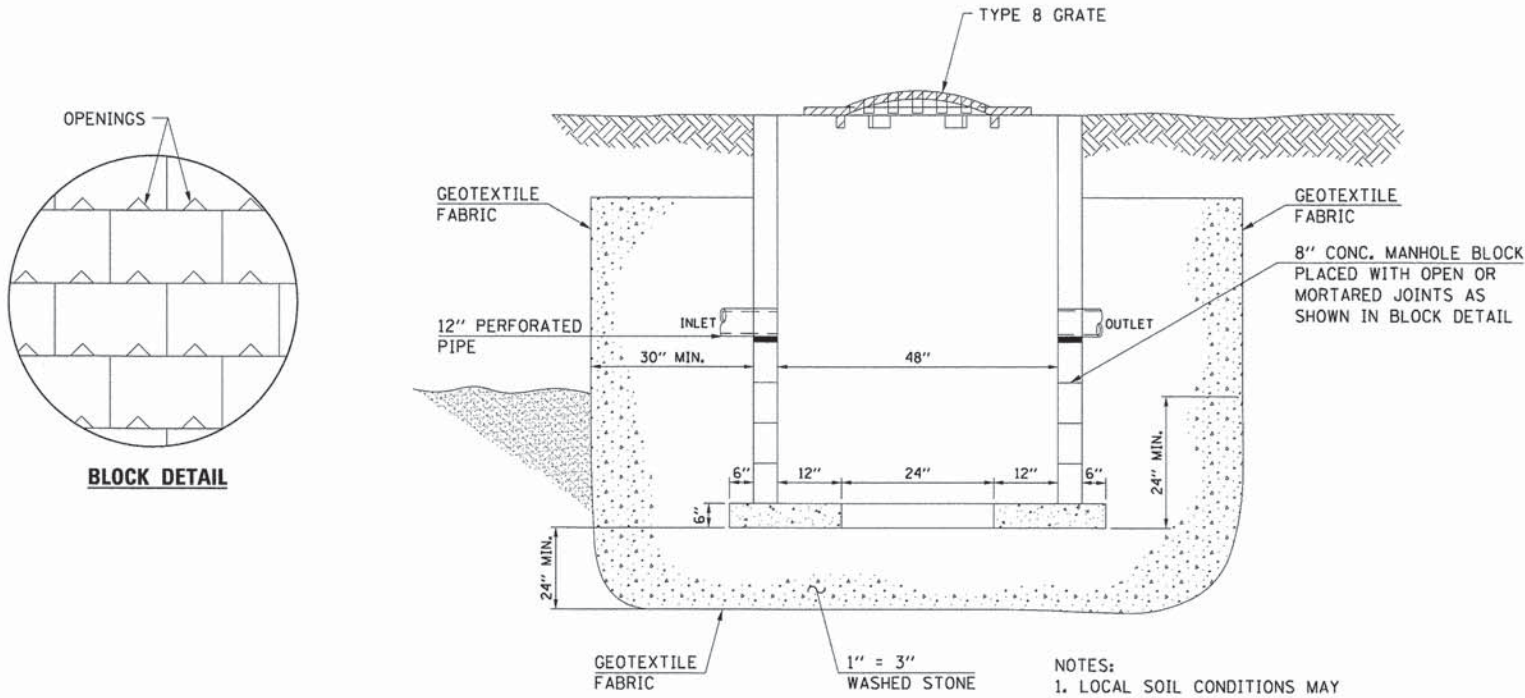
VILLAGE OF CARY  
 DETAILS

SCALE: SHEET NO. OF SHEETS STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	69
FED. ROAD DIST. NO.			CONTRACT NO. 63886	
[ILLINOIS] FED. AID PROJECT				



**DRY WELL MANHOLE DETAIL**



- NOTES:
1. LOCAL SOIL CONDITIONS MAY NECESSITATE A DEEPER EXCAVATION TO PENETRATE GRANULAR SUBSOILS. MINIMUM DEPTH OF WELL SHALL BE 6 FEET.
  2. STRUCTURE TO BE EXCAVATED AFTER PERMANENT STABILIZATION IS COMPLETE.

COMPANY NAME: HRGreen  
 PROJECT CONTACT: bhartha  
 CLIENT: VILLAGE OF CARY  
 DATE: 12/5/2013  
 PLOT SCALE: 1" = 3"  
 PLOT DRIVER: strander-trans.tbl  
 PEN TABLE: strander-trans.tbl

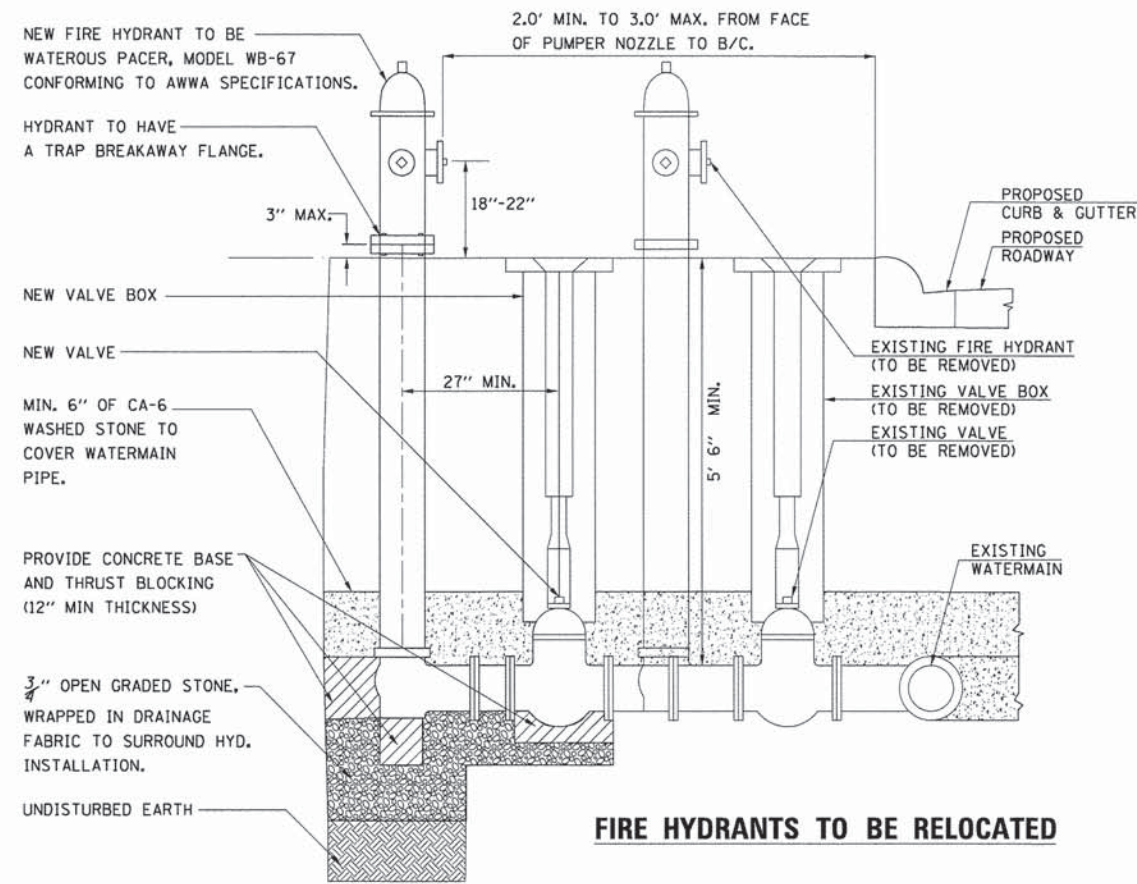
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	PLOT DATE = 12/5/2013	CHECKED -	REVISED -
		DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>VILLAGE OF CARY                  DETAILS</b>			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	70
FED. ROAD DIST. NO.			CONTRACT NO. 63886	
[ILLINOIS] FED. AID PROJECT				





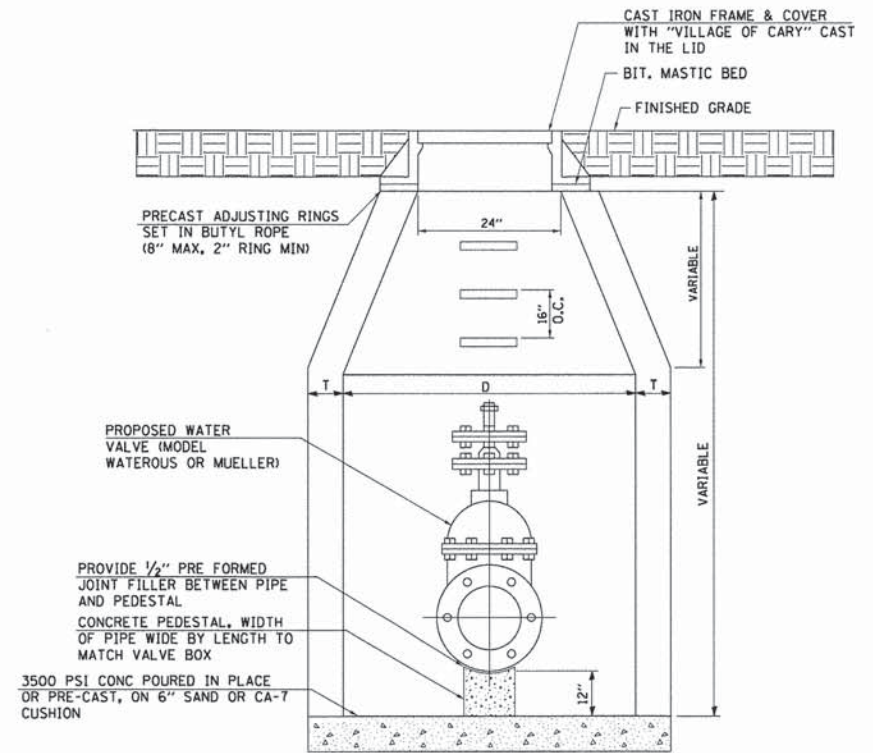
**FIRE HYDRANTS TO BE RELOCATED**

**SEQUENCE OF CONSTRUCTION:**

1. CLOSE EXISTING 12" VALVES. CONTACT VILLAGE OF CARY WATER & SEWER DEPARTMENT A MINIMUM OF 48 HOURS PRIOR TO ANY DISRUPTION IN SERVICE TO SCHEDULE ALL SHUT-DOWNS.
2. REMOVE EXISTING HYDRANT, VALVE, AND VALVE BOX FROM TEE.
3. INSTALL NEW HYDRANT, VALVE, AND VALVE BOX AT NEW LOCATION.
4. REOPEN EXISTING 12" VALVES.
5. BACKFILL.
6. FLUSH AND TEST FOR CHLORINE RESIDUAL AND PROVIDE TEST.

**NOTES:**

1. NEW VALVE BOX SHALL BE "TYLER" OR APPROVED EQUAL.
2. ALL NUTS, BOLTS, & THREADED RODS SHALL BE STAINLESS STEEL.
3. TRENCH BACKFILL SHALL BE PROVIDED FOR TRENCHES MADE IN THE SUBGRADE OF THE PROPOSED IMPROVEMENT OR WITHIN 2' OF THE PROPOSED CURB IN ACCORDANCE WITH SECTION 208 OF THE STANDARD SPECIFICATIONS.
4. NEW VALVE BOXES SHALL BE LOCATED OUTSIDE OF PAVED AREAS.



**VALVE VAULT**

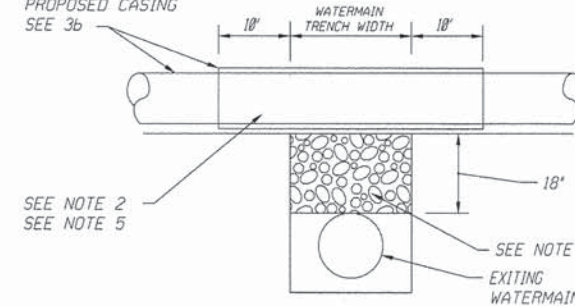
	D	T (min.)	NOTE:
Precast Reinforced Concrete Section	1.2 m (4'-0")	100 (4)	VALVE VAULT DIA. SHALL BE 48" FOR VALVES 8" OR SMALLER, AND 60" FOR VALVES 12" OR LARGER AND SHALL BE PRECAST CONCRETE SECTIONS.
	1.5 m (5'-0")	125 (5)	

**WATER AND SEWER SEPARATION REQUIREMENTS DETAIL**

EXISTING WATERMAIN BELOW PROPOSED SEWER LINE WITH 18" MINIMUM SEPARATION.

NOTE: CLASS IV MATERIAL TO BE COMPACTED TO 95% OF STANDARD PROCTOR MAXIMUM DENSITY.

PROPOSED SEWER SEE 3a, OR PROPOSED CASING SEE 3b



**NOTES:**

1. IF SELECT GRANULAR BACKFILL EXISTS; REMOVE WITHIN WIDTH OF PROPOSED SEWER TRENCH AND REPLACE WITH SELECT EXCAVATED MATERIAL (CLASS IV) AND COMPACT.
2. OMIT SELECT GRANULAR CRADLE AND GRANULAR BACKFILL TO ONE (1) FOOT OVER TOP OF PIPE AND USE SELECT EXCAVATED MATERIAL (CLASS IV) AND COMPACT FOR 10 FEET ON EITHER SIDE OF WATER MAIN.
3. a. CONSTRUCT 10 FEET OF PROPOSED SEWER WITH WATERMAIN QUALITY PIPE AND PRESSURE TEST, OR; b. USE 10 FEET OF CASING FOR PROPOSED SEWER AND SEAL ENDS OF CASING.
4. POINT LOADS SHALL NOT BE ALLOWED BETWEEN SEWER OR SEWER CASING AND WATER MAIN.
5. PROVIDE ADEQUATE SUPPORT FOR EXISTING WATERMAIN TO PREVENT DAMAGE DUE TO SETTLEMENT OF SEWER TRENCH.

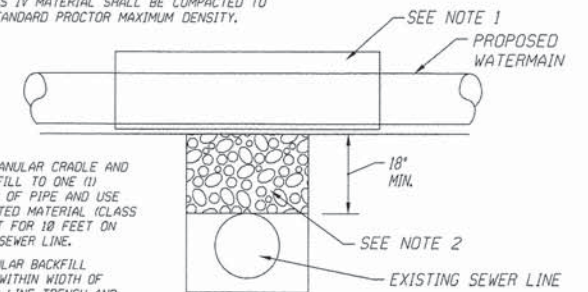
▲ TAKEN FROM THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS - SEE PAGES 156-159.

PROPOSED WATERMAIN ABOVE EXISTING SEWER LINE WITH 18" MINIMUM SEPARATION.

NOTE: CLASS IV MATERIAL SHALL BE COMPACTED TO 95% OF STANDARD PROCTOR MAXIMUM DENSITY.

**NOTES:**

1. OMIT SELECT GRANULAR CRADLE AND GRANULAR BACKFILL TO ONE (1) FOOT OVER TOP OF PIPE AND USE SELECT EXCAVATED MATERIAL (CLASS IV) AND COMPACT FOR 10 FEET ON EITHER SIDE OF SEWER LINE.
2. IF SELECT GRANULAR BACKFILL EXISTS; REMOVE WITHIN WIDTH OF EXISTING SEWER LINE TRENCH AND REPLACE WITH SELECT EXCAVATED MATERIAL (CLASS IV) AND COMPACT.

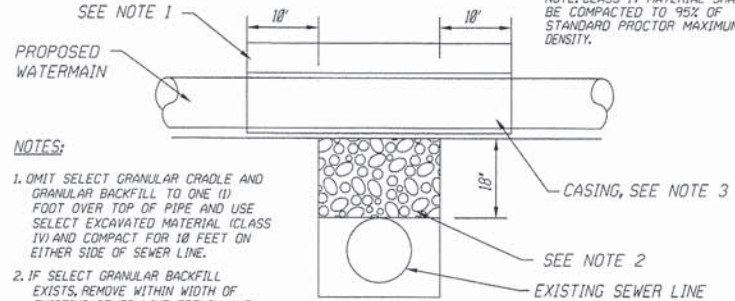


PROPOSED WATER MAIN ABOVE EXISTING SEWER LINE WITH LESS THAN 18" SEPARATION.

NOTE: CLASS IV MATERIAL SHALL BE COMPACTED TO 95% OF STANDARD PROCTOR MAXIMUM DENSITY.

**NOTES:**

1. OMIT SELECT GRANULAR CRADLE AND GRANULAR BACKFILL TO ONE (1) FOOT OVER TOP OF PIPE AND USE SELECT EXCAVATED MATERIAL (CLASS IV) AND COMPACT FOR 10 FEET ON EITHER SIDE OF SEWER LINE.
2. IF SELECT GRANULAR BACKFILL EXISTS; REMOVE WITHIN WIDTH OF EXISTING SEWER LINE TRENCH AND REPLACE WITH SELECT EXCAVATED MATERIAL (CLASS IV) AND COMPACT.



3. USE A CASING FOR PROPOSED WATER MAIN AND SEAL ENDS OF CASING.
4. POINT LOADS SHALL NOT BE ALLOWED BETWEEN WATER MAIN OR WATERMAIN AND SEWER.

U-5

COMPANY NAME: HRGreen  
 PROJECT CONTACT: bhar-tina  
 CLIENT: VILLAGE OF CARY  
 DATE: 12/5/2013  
 FILE NAME: 13-00063-00-PV.dwg  
 PLOT DRIVER: default  
 PEN TABLE: s:\standard\tracns.tbl



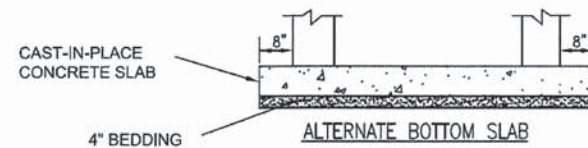
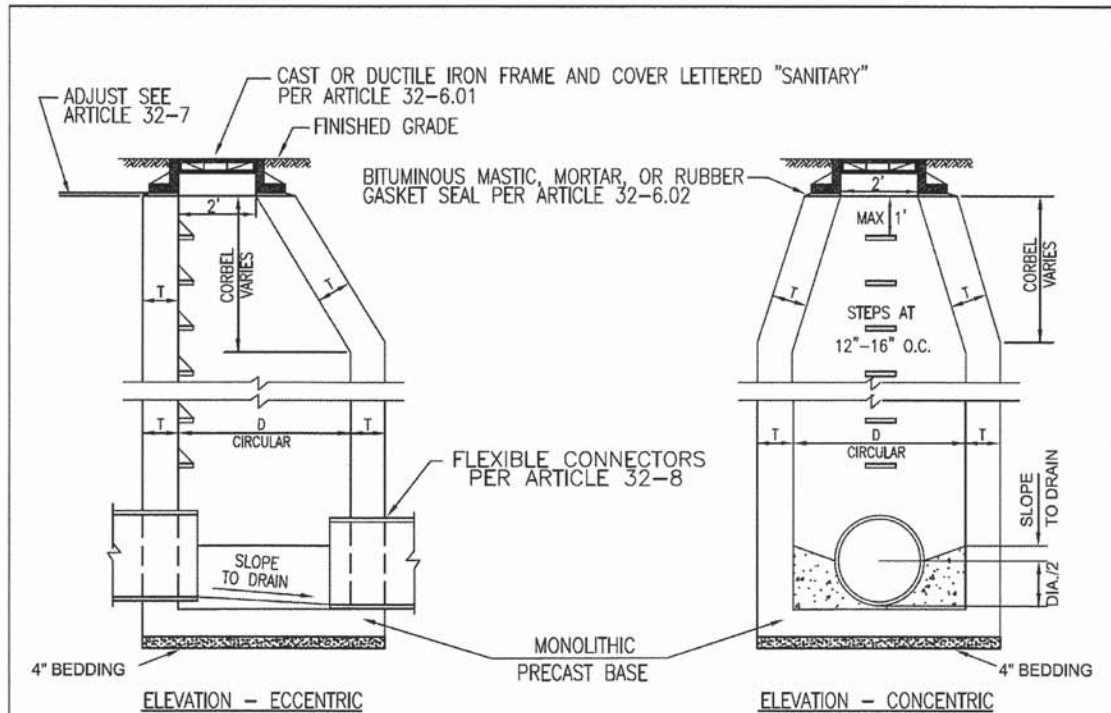
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	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**VILLAGE OF CARY  
 DETAILS**

SCALE:	SHEET NO. OF SHEETS	STA. TO STA.	F.A. RTE. 4058	SECTION 13-00063-00-PV	COUNTY MCHENRY	TOTAL SHEETS 79	SHEET NO. 71
						CONTRACT NO. 63886	
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT							





ALT. MATERIALS FOR WALLS	D	T (MIN.)
CONCRETE MASONRY UNITS	4 FEET	5 IN
	5 FEET	5 IN
	6 FEET	5 IN
BRICK MASONRY	4 FEET	8 IN
	5 FEET	8 IN
	6 FEET	8 IN
PRECAST REIN. CONCRETE SECT.	4 FEET	4 IN
	5 FEET	5 IN
	6 FEET	6 IN
CAST-IN-PLACE CONCRETE.	4 FEET	6 IN
	5 FEET	6 IN
	6 FEET	6 IN

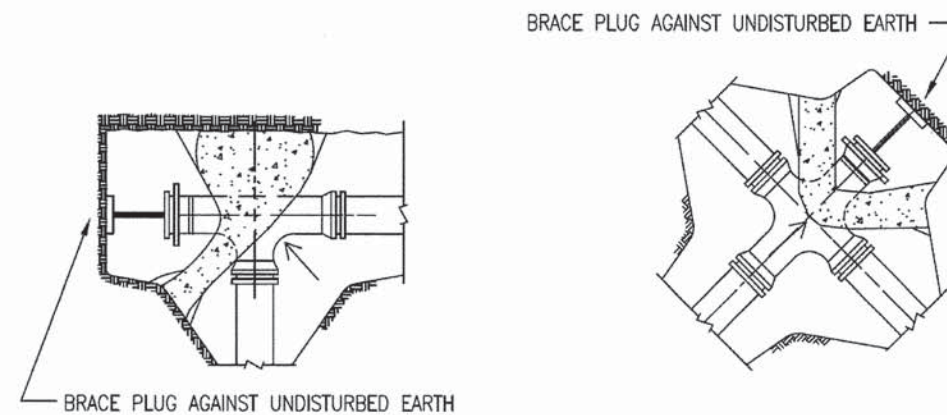
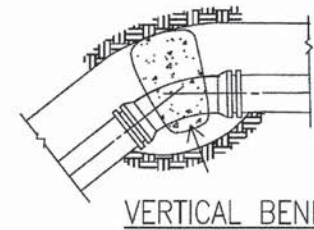
NOTE: SANITARY MANHOLES SUBJECT TO SATURATED SOIL CONDITIONS OR SURFACE SUBMERGENCE SHALL BE EQUIPPED WITH CHIMNEY SEALS AND WATER TIGHT MANHOLE COVERS

NOTE: MANHOLE STEPS WHEN REQUIRED, SHALL BE FURNISHED AND INSTALLED AS SHOWN ON THE PLANS IN ACCORDANCE WITH ARTICLE 32-5

**TYPICAL SANITARY MANHOLE "A"**

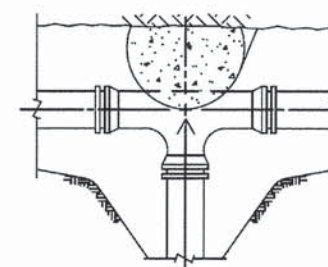
REFERENCE SECTION 32 FOR SANITARY SEWER MANHOLE SPECIFICATIONS

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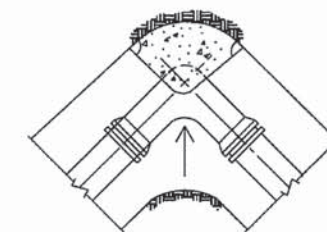


**PLUGGED TEE**

**PLUGGED CROSS**



**TEE**



**90° ELBOW**

NOTE: ALL BLOCKS BEAR AGAINST UNDISTURBED EARTH. ARROWS INDICATE DIRECTION OF THRUST. ALL BLOCKS TO BE 3000 P.S.I CONCRETE. ALL FITTINGS SHOWN IN PLAN EXCEPT VERTICAL BEND.

**TYPICAL THRUST BLOCK INSTALLATIONS**

STANDARD DRAWING NO.12

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 PROJECT CONTACT: HRGreen.com  
 CLIENT: VILLAGE OF CARY  
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 PLOTTER: HP DesignJet T1100e  
 PEN TABLE: Standard.ctb



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PLOT DATE = 12/5/2013	CHECKED -	REVISED -
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STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

VILLAGE OF CARY  
 DETAILS

SCALE: SHEET NO. OF SHEETS STA. TO STA.

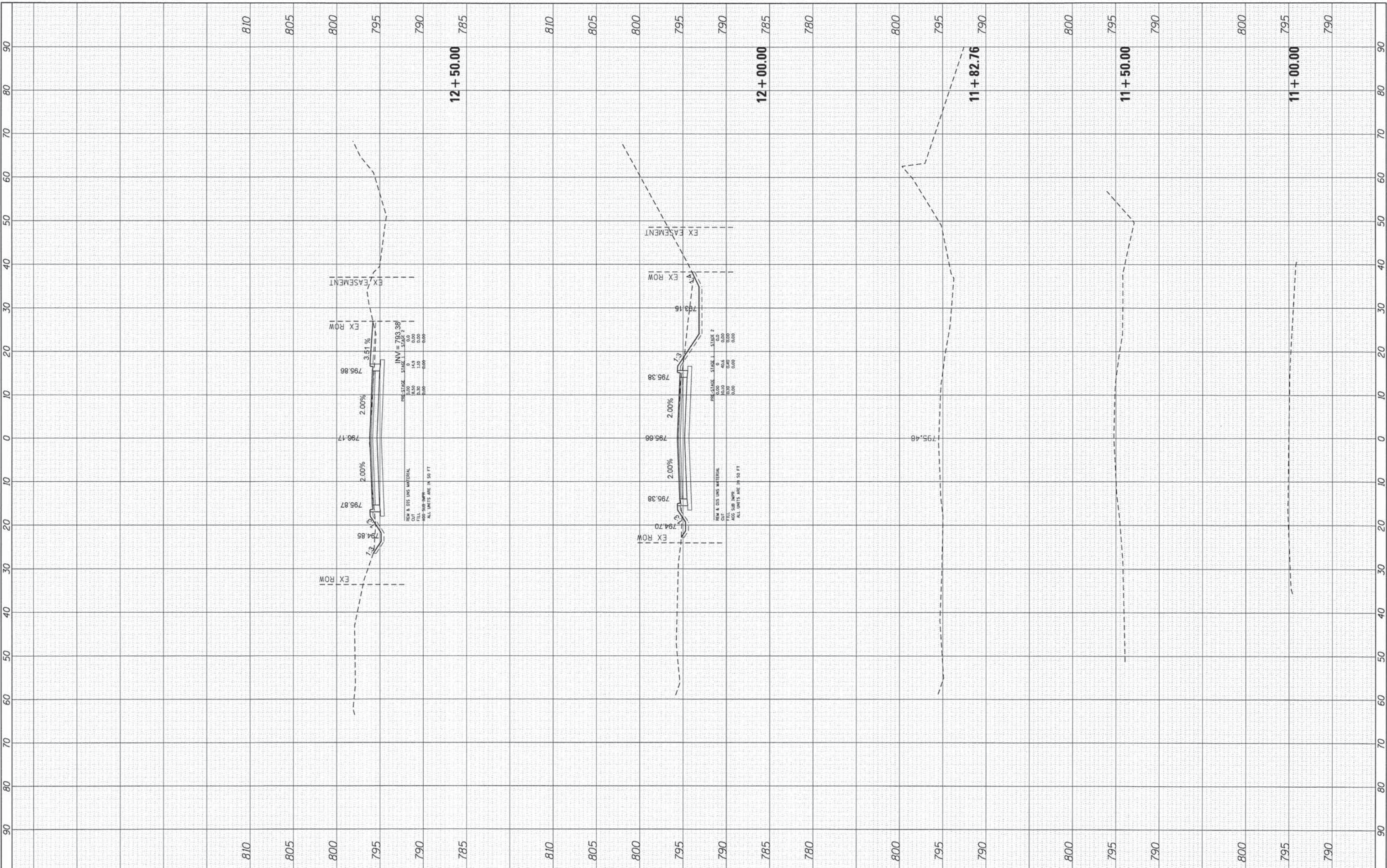
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4058	13-00063-00-PV	MCHENRY	79	72
FED. ROAD DIST. NO. [ILLINOIS] FED. AID PROJECT			CONTRACT NO. 63886	



FINAL SURVEY NO.	BY	DATE
CHECKED		
PLOTTED		
TEMPLATE		
AREAS CHECKED		

ORIGINAL SURVEY NO.	BY	DATE
CHECKED		
PLOTTED		
TEMPLATE		
AREAS CHECKED		

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 PLOT DRIVER: pdfplot  
 PEN TABLE: standard-trans.tbl



**HRGreen.com**  
 Illinois Professional Design Firm  
 # 184-001322

USER NAME = bhar-ma	DESIGNED - JJS	REVISED -
PLOT SCALE = 1"=10'	DRAWN - DMS	REVISED -
PLOT DATE = 12/5/2013	CHECKED - THE	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>CROSS SECTIONS    JANDUS CUTOFF ROAD</b>		
SCALE: 1"=10'	SHEET 1 OF 7 SHEETS	STA. 11+00.00 TO STA. 12+50.00

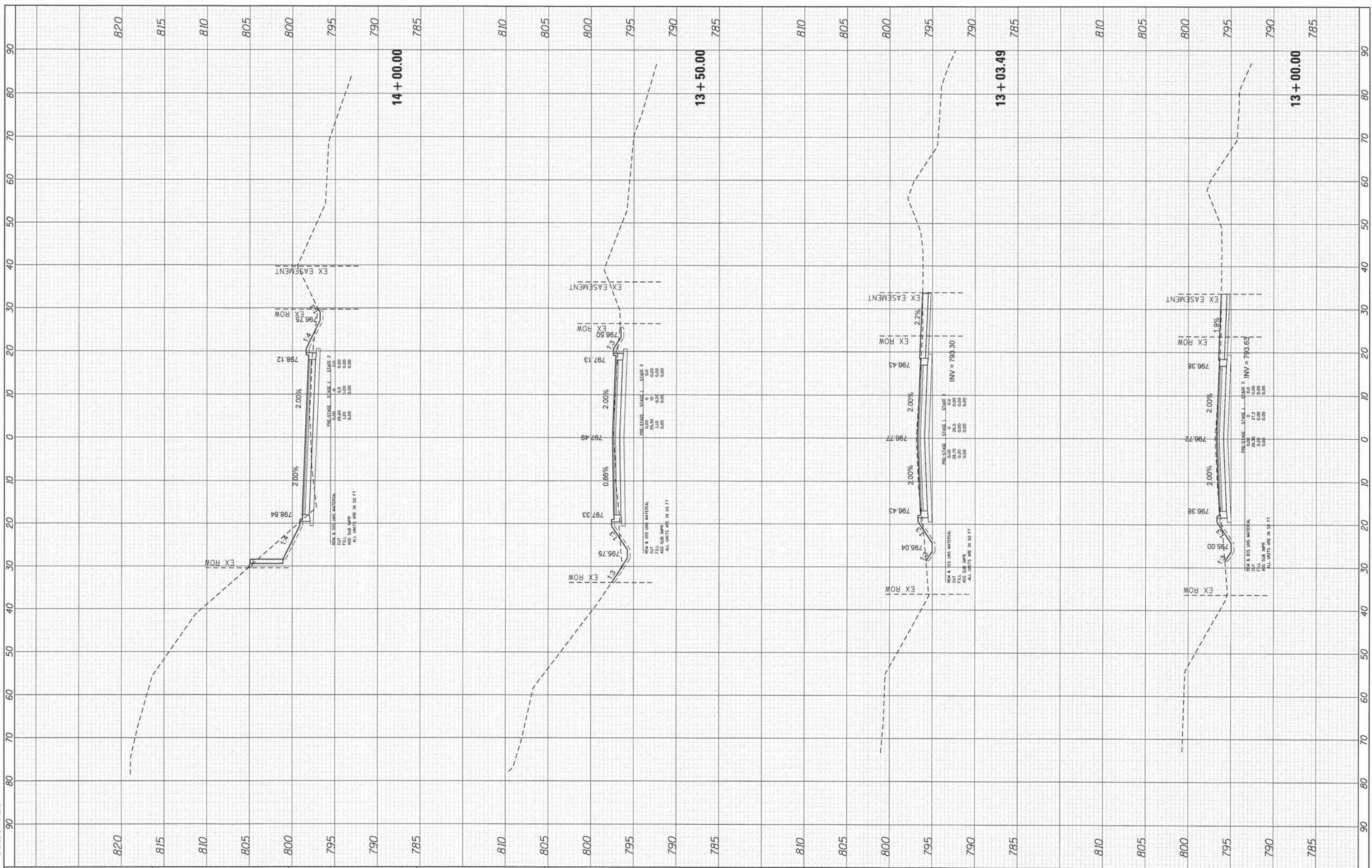
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	73
CONTRACT NO. 63886			[ILLINOIS] FED. AID PROJECT 63886	



FINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
NOTE BOOK		
AREAS CHECKED		
NO.		

ORIGINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
NOTE BOOK		
AREAS CHECKED		
NO.		

COMPANY NAME: HRGreen.com  
 PROJECT CONTACT: Illinois Professional Design Firm  
 #184-001322  
 DATE PLOTTED: 12/5/2013 9:02:36 AM  
 FILE NAME: 86120253-XSC-SHT-Jandus.dgn  
 PLOT DRIVER: pdf.plt  
 PEN TABLE: standard-trans.tbl



USER NAME = bhar-tia
DESIGNED - JJS
DRAWN - DMS
CHECKED - THE
DATE - 12/5/2013

REVISIONS
REVISED -
REVISED -
REVISED -
REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**CROSS SECTIONS  
JANDUS CUTOFF ROAD**

SCALE: 1"=10'    SHEET 2 OF 7 SHEETS    STA. 13+00.00 TO STA. 14+00.00

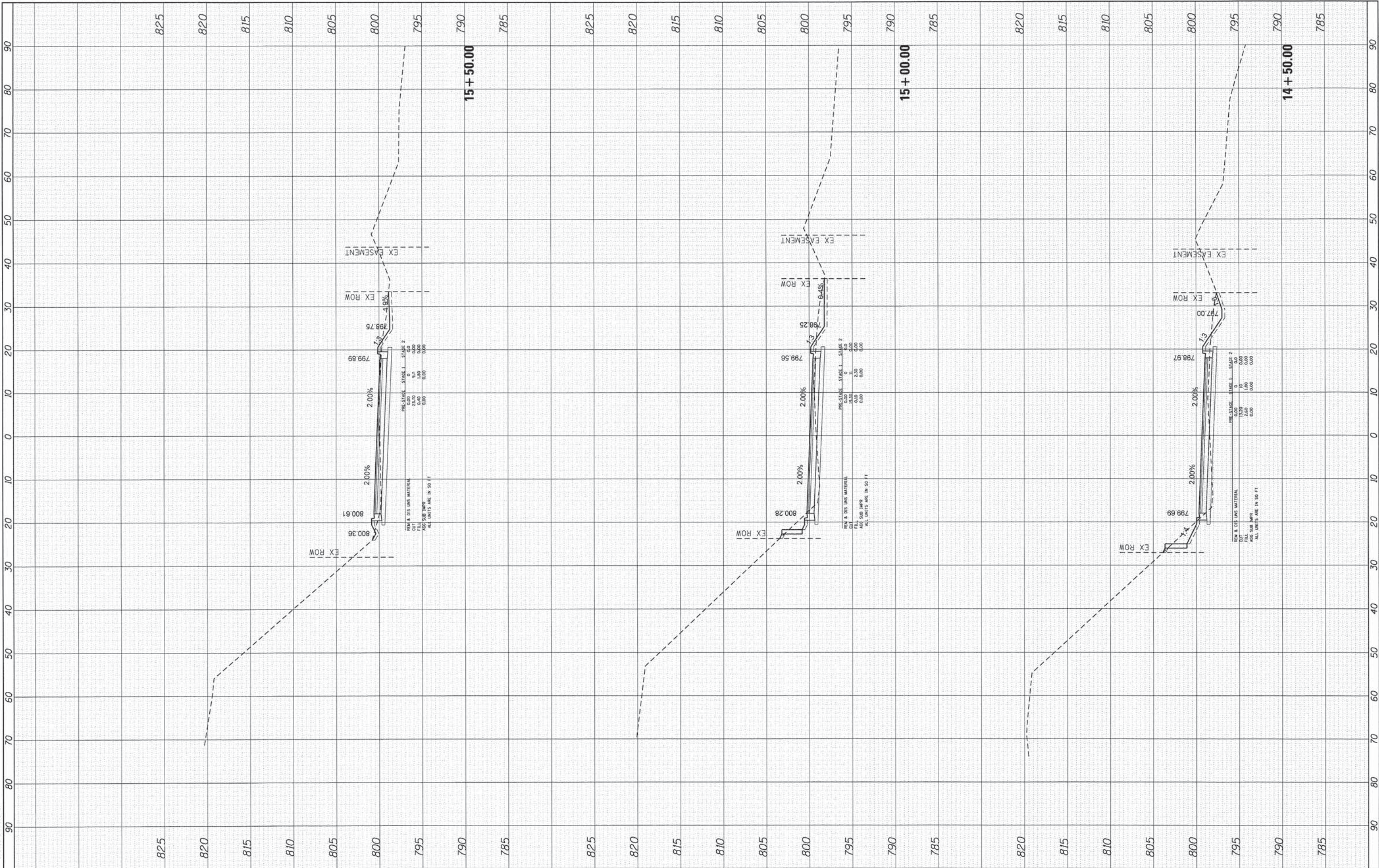
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	74
				CONTRACT NO. 63886
				ILLINOIS FED. AID PROJECT 63886



FINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
NOTE BOOK		
AREAS CHECKED		
NO.		

ORIGINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
NOTE BOOK		
AREAS CHECKED		
NO.		

COMPANY NAME: HRGreen.com  
 PROJECT CONTACT: Illinois Professional Design Firm  
 # 184-001322  
 CLIENT: 12/5/2013 9:12:42 AM  
 DATE PLOTTED: 86120253-ASC-SHT - Jandus.dgn  
 FILE NAME: pdfcut1  
 PLOT DRIVER: standard-trans.tn  
 PEN TABLE:



**HRGreen**  
 HRGreen.com  
 Illinois Professional Design Firm  
 # 184-001322

USER NAME = bhar-tia	DESIGNED - JJS	REVISED -
	DRAWN - DMS	REVISED -
	CHECKED - THE	REVISED -
	DATE -	REVISED -
PLOT SCALE = 1"=10'		
PLOT DATE = 12/5/2013		

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>CROSS SECTIONS    JANDUS CUTOFF ROAD</b>		
SCALE: 1"=10'	SHEET 3 OF 7 SHEETS	STA. 14+50.00 TO STA. 15+50.00

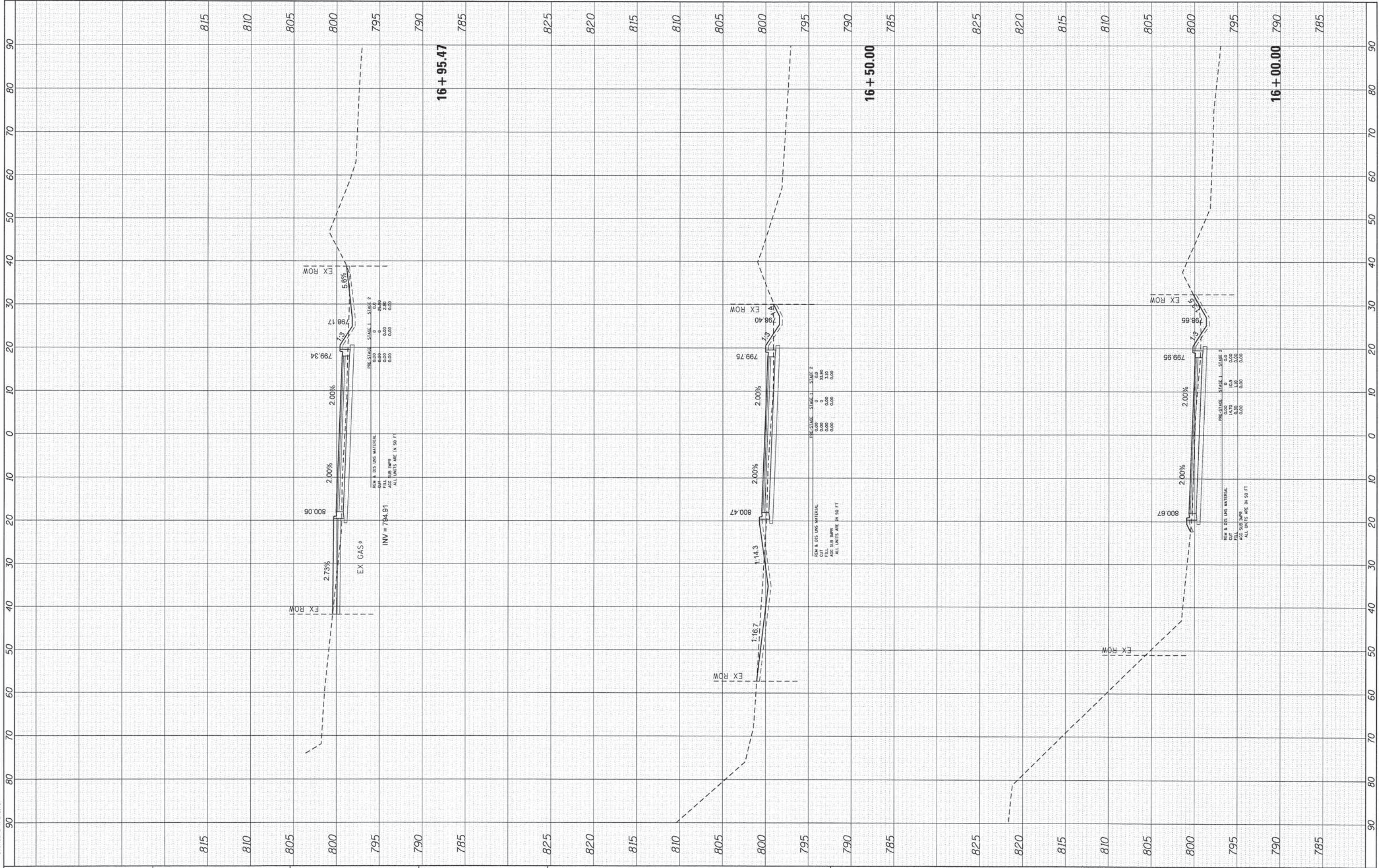
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
405B	13-00063-00-PV	MCHENRY	79	75
CONTRACT NO. 63886			ILLINOIS FED. AID PROJECT 63886	



FINAL SURVEYED	DATE
SURVEY	BY
PLOTTED	
NOTE BOOK	
AREAS CHECKED	
NO.	

ORIGINAL SURVEYED	DATE
SURVEY	BY
PLOTTED	
NOTE BOOK	
AREAS CHECKED	
NO.	

COMPANY NAME: HRGreen  
 PROJECT CONTACT: 12/5/2013 9:02:47 AM  
 CLIENT: 86120253-ASC-SHT-Jandus.dgn  
 DATE PLOTTED: paf,jaf  
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 PLOT DRIVER: standard-trons.tbl



USER NAME = bhar-una
PLOT SCALE = 1"=10'
PLOT DATE = 12/5/2013

DESIGNED - JJS	REVISED -
DRAWN - DMS	REVISED -
CHECKED - THE	REVISED -
DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS
JANDUS CUTOFF ROAD
SCALE: 1"=10'
SHEET 4 OF 7 SHEETS
STA. 16+00.00 TO STA. 16+95.47

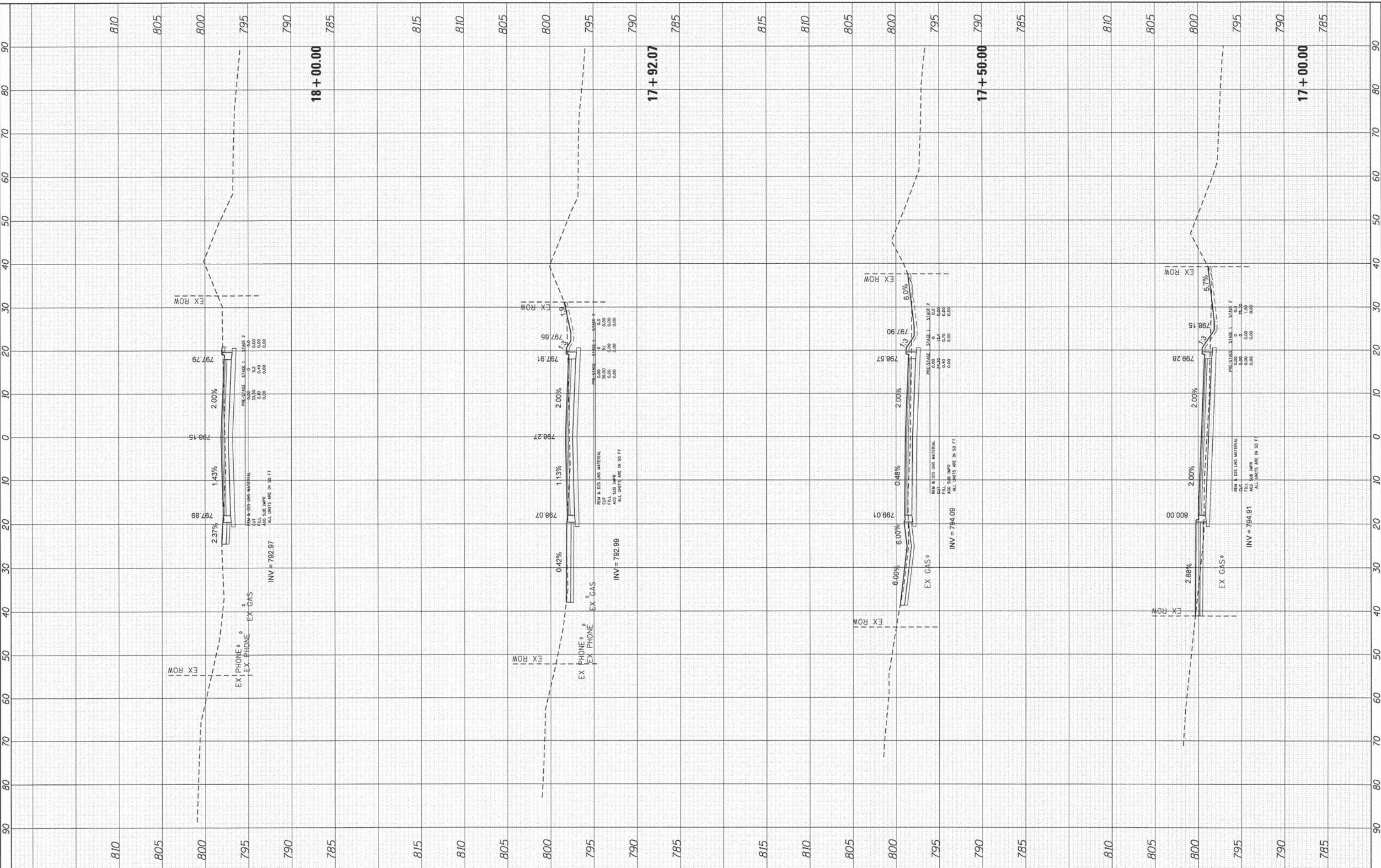
F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	76
CONTRACT NO. 63886				
ILLINOIS FED. AID PROJECT 63886				



FINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS CHECKED		

ORIGINAL SURVEY	SURVEYED	BY	DATE
NOTE BOOK	PLOTTED		
NO.	TEMPLATE		
	AREAS CHECKED		

COMPANY NAME: HRGreen.com  
 PROJECT CONTACT: 12/5/2013 9:12:52 AM  
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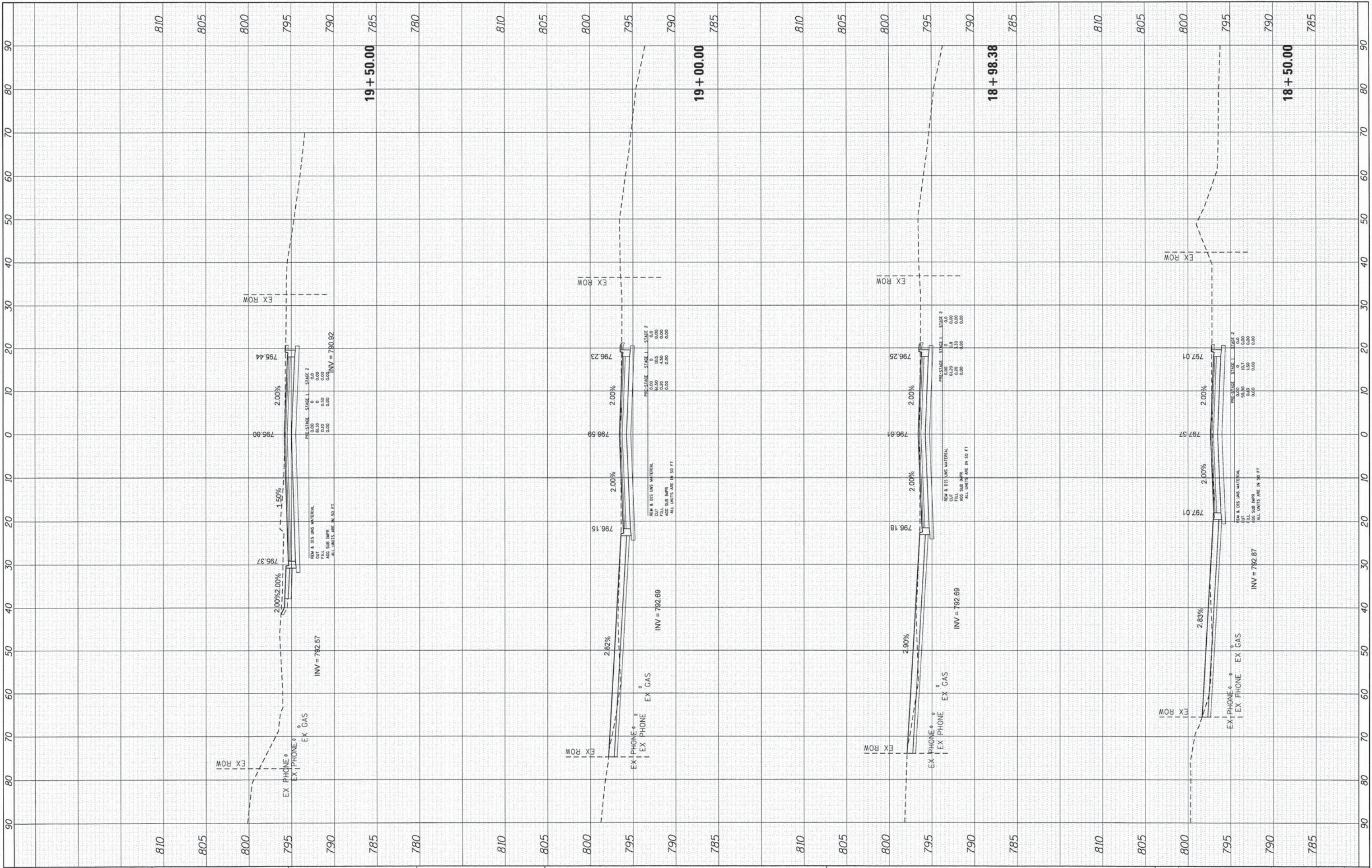
	USER NAME = bhar-tia DESIGNED - JJS DRAWN - DMS CHECKED - THE DATE -	REVISED - REVISED - REVISED - REVISED -	<b>STATE OF ILLINOIS          DEPARTMENT OF TRANSPORTATION</b>	<b>CROSS SECTIONS          JANDUS CUTOFF ROAD</b>		F.A. RTE. 4058 SECTION 13-00063-00-PV COUNTY MCHENRY TOTAL SHEETS 79 SHEET NO. 77	CONTRACT NO. 63886 ILLINOIS FED. AID PROJECT 63886
	PLOT SCALE = 1"=10' PLOT DATE = 12/5/2013	SCALE: 1"=10' SHEET 5 OF 7 SHEETS STA. 17+00.00 TO STA. 18+00.00		STA. 17+00.00 TO STA. 18+00.00			



FINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
NOTE BOOK		
AREAS CHECKED		
NO.		

ORIGINAL SURVEY	BY	DATE
SURVEYED		
PLOTTED		
NOTE BOOK		
AREAS CHECKED		
NO.		

COMPANY NAME: HRGreen.com  
 PROJECT CONTACT: 12/5/2013 9:05:57 AM  
 CLIENT: 86120253-XSC-SHF-VanDusen.dgn  
 DATE PLOTTED: pdf\_caf  
 FILE NAME: s:\standard-trans.tbl  
 PLOT DRIVER:  
 PER TABLE:



USER NAME = bhar-ma
DESIGNED - JJS
DRAWN - DMS
CHECKED - THE
DATE -

REVISIONS
REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION

CROSS SECTIONS  
 JANDUS CUTOFF ROAD

SCALE: 1"=10'
SHEET 6 OF 7 SHEETS
STA. 18+50.00 TO STA. 19+50.00

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
4058	13-00063-00-PV	MCHENRY	79	78
CONTRACT NO. 63886			ILLINOIS FED. AID PROJECT 63886	



