

F.A.I. SHEET NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	1
ILLINOIS CONTRACT NO. 60W29				

*356 + 38 = 394

FOR INDEX OF SHEETS AND STANDARDS SEE SHEET NO. 2 AND 3

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

**PROPOSED
HIGHWAY PLANS**

PEORIA STREET BRIDGE AT
I-290/CONGRESS PARKWAY
(CIRCLE INTERCHANGE)
SECTION 2013-011R
BRIDGE REPLACEMENT,
ROADWAY RECONSTRUCTION, LIGHTING,
CTA STATION REHABILITATION
PROJECT: ACNHPP-0290(200)
COOK COUNTY
C-91-231-13

DESIGN DESIGNATIONS:	POSTED /DESIGN SPEEDS:
PEORIA STREET	N/A
WB I-290	45 /50 MPH
EB I-290	45 /50 MPH
NW RAMP	35 /35 MPH
EN RAMP	30 /30 MPH
ES RAMP	40 /40 MPH
O(2040) LOCAL STREET (URBAN)	
54,000(2040) INTERSTATE	
33,000(2040) INTERSTATE	
36,000(2040) INTERSTATE	
31,000(2040) INTERSTATE	
44,000(2040) INTERSTATE	

02-28-14 LETTING ITEM 117

Jennifer M. Golemba 10/10/13
JENNIFER M. GOLEMBA DATE
062-058708 LICENSE EXPIRES 11/30/2013
LICENSED PROFESSIONAL ENGINEER SHEET RANGE

William D. Stermer 10/10/13
WILLIAM D. STERMER DATE
062-060936 LICENSE EXPIRES 11/30/2013
LICENSED PROFESSIONAL ENGINEER SHEET RANGE

Dajin Liu 10/10/13
DAJIN LIU DATE
081-005944 LICENSE EXPIRES 11/30/2014
LICENSED STRUCTURAL ENGINEER SHEET RANGE

Oren X. Skidelsky 10/10/13
OREN X. SKIDELSKY DATE
062-053762 LICENSE EXPIRES 11/30/2013
LICENSED PROFESSIONAL ENGINEER SHEET RANGE

Charles Roy Rowe 10/10/13
CHARLES ROY ROWE DATE
001-014357 LICENSE EXPIRES 11/30/2014
LICENSED ARCHITECT SHEET RANGE

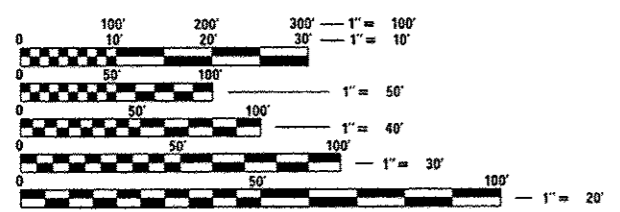


LOCATION OF SECTION INDICATED THIS: -

AECOM

TranSystems

AAA ENGINEERING

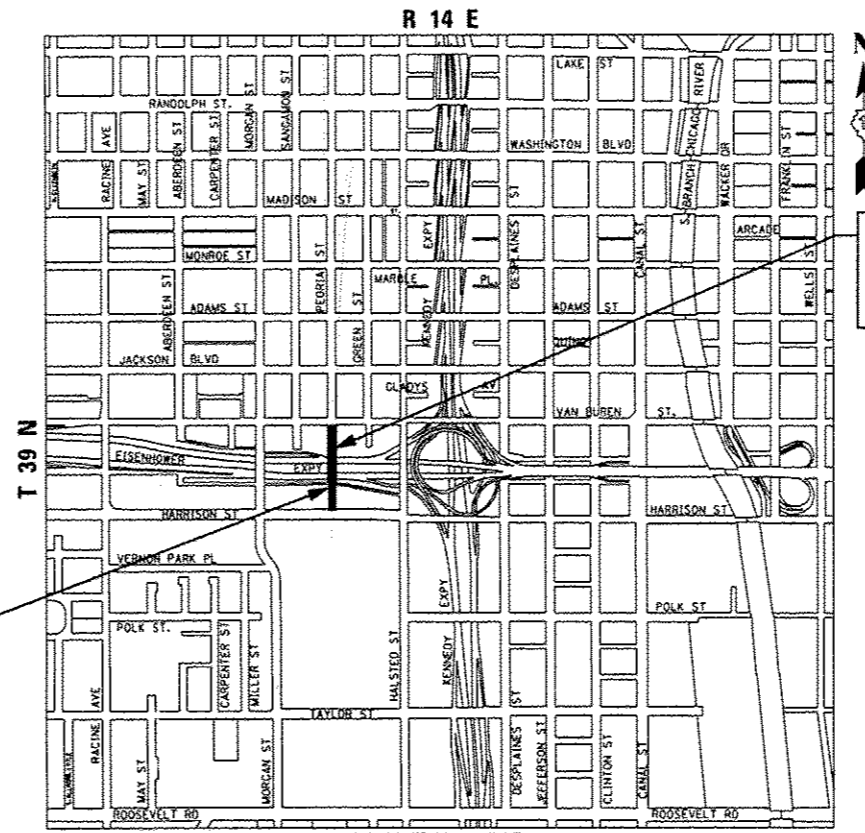


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

C.U.A.N.
CHICAGO UTILITY ALERT NETWORK
1-312-744-7000

NPDES PERMIT INFORMATION	
NPDES Disturbed	
Area =	1.99 Acres
Approximate Location of Roadway is :	
Longitude	87° 38' 58.22" W
Latitude	41° 52' 31.87" N

PEORIA STREET BRIDGE
SN 016-1708
STA 3702 + 57.64 TO
STA 3705 + 30.73



LOCATION MAP

NOT TO SCALE

THE PROJECT IS LOCATED IN THE CITY OF CHICAGO

GROSS LENGTH = 687.95 FT (0.130 MILES)

NET LENGTH = 687.95 FT (0.130 MILES)

PROJECT ENGINEER: ADDIS ABEBAW
PROJECT MANAGER: BRIAN KUTTAB

CONTRACT NO. 60W29

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED *October 15* 20 *13*

John D. Baranzelli, P.E.
DEPUTY DIRECTOR OF HIGHWAYS, REGION ENGINEER

John D. Baranzelli, P.E.
ENGINEER OF DESIGN AND ENVIRONMENT

Omer Osman, P.E.
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

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OF THE STATE OF ILLINOIS

DISTRICT 1 DESIGN /CONSULTANT SERVICES: BRIAN KUTTAB, P.E. (847)705-4431 SCHAUMBURG, ILLINOIS

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016029-sh1-Genote-01.dgn
 USER NAME = BAW\jort
 PLOT SCALE = 100,0000 / in.
 PLOT DATE = 12/19/2013

DESIGNED - JLV	REVISED - 12/18/2013
DRAWN - BAW	REVISED -
CHECKED - JMC	REVISED -
DATE - 10/30/2013	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX OF DRAWINGS AND HIGHWAY STANDARDS
 SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	2
CONTRACT NO. 60W29			ILLINOIS FED. AID PROJECT	

GENERAL NOTES

1. SEVENTY-TWO (72) HOURS BEFORE STARTING EXCAVATION, THE CONTRACTOR SHALL CALL DIGGER (CHICAGO UTILITY ALERT NETWORK) AT (312) 744-7000 TO HAVE THE LOCATION OF EXISTING UNDERGROUND UTILITIES MARKED IN THE FIELD.
2. A MINIMUM OF SEVENTY-TWO (72) HOURS PRIOR TO ANY PLACEMENT OR RELOCATION OF MAINTENANCE OF TRAFFIC DEVICES, CONTACT ILLINOIS DEPARTMENT OF TRANSPORTATION (IDOT) DISTRICT 1 BUREAU OF TRAFFIC.
3. THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS TO PERFORM WORK.
4. TEN FOOT TRANSITIONS SHALL BE USED TO MATCH PROPOSED ITEMS OF WORK TO EXISTING ITEMS IN THE FIELD UNLESS OTHERWISE SHOWN. THE TRANSITIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR THE PROPOSED ITEM OF WORK SPECIFIED.
5. ALL ELEVATIONS IN THE PLANS ARE BASED UPON THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88), UNLESS OTHERWISE NOTED.
6. THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE OR CITY PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT OR CITY OF CHICAGO.
7. THE ENGINEER'S FIELD OFFICE FOR THIS CONTRACT NO. 60W29 SHALL BE LOCATED AT 900 SOUTH DES PLAINES STREET, CHICAGO. CONTRACTOR COORDINATION WITH THE MORGAN STREET CONTRACT 60W25 AND HALSTED STREET/HARRISON STREET CONTRACT 60W26 IS REQUIRED. SEE THE SPECIAL PROVISION FOR ENGINEER'S FIELD OFFICE, TYPE A (SPECIAL) FOR DETAILS.
8. THE IDOT HIGHWAY STANDARDS LATEST REVISION NUMBERS SHALL APPLY TO THIS PROJECT.
9. EXCEPT WHERE DESIGNATED OTHERWISE, THE LOCATIONS AND/OR DEPTHS OF UNDERGROUND UTILITIES SHOWN HAVE BEEN TAKEN FROM OFFICE RECORD INFORMATION FURNISHED BY THE UTILITY OWNERS AND THE SUE SURVEYS. ALL UNDERGROUND UTILITIES MUST BE CONSIDERED APPROXIMATE.
10. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN IN THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER. THIS WORK WILL BE AT THE CONTRACTOR'S EXPENSE.
11. THE CONTRACTOR SHALL COORDINATE WORK WITH RAILROADS AND UTILITIES IN ADVANCE OF WORKING IN THE VICINITY OF THEIR FACILITIES, AND ALLOW SUFFICIENT TIME FOR THEM TO PERFORM ADJUSTMENTS TO THEIR FACILITIES IN ACCORDANCE WITH THE CONTRACTOR'S SCHEDULE. COORDINATION EFFORTS SHALL BE INCLUDED IN THE COST OF THE CONTRACT BID PRICE.
12. THE CONTRACTOR MUST CALL THE IDOT ELECTRICAL MAINTENANCE CONTRACTOR TO LOCATE IDOT FACILITY CABLES.
13. CHICAGO TRANSIT AUTHORITY (CTA) REPRESENTATIVE WILL BE MR. RICK HERNDOLBER, MANAGER, CAPITAL CONSTRUCTION. CONTRACTOR SHALL NOTIFY CTA REPRESENTATIVE AT 312-681-3921 AT LEAST 72 HOURS IN ADVANCE OF THE TIME HE INTENDS TO ENTER UPON THE CTA RIGHT-OF-WAY FOR THE PERFORMANCE OF ANY WORK.
14. PLAN DIMENSIONS AND DETAILS RELATIVE TO CHICAGO TRANSIT AUTHORITY (CTA) TRACK ARE SUBJECT TO NOMINAL CONSTRUCTION VARIATIONS. THE CONTRACTOR SHALL FIELD VERIFY THE MINIMUM HORIZONTAL CLEARANCE DIMENSIONS FROM CENTERLINE OF TRACK TO THE FACE OF THE PROPOSED PIER PRIOR TO CONSTRUCTION OR ORDERING OF MATERIALS. THE ALLOWABLE HORIZONTAL PERMANENT CLEARANCE SHALL BE 7'-2" MINIMUM FROM CENTERLINE OF TRACK TO EDGE OF PIER, AND THE ALLOWABLE HORIZONTAL TEMPORARY CLEARANCE DURING CONSTRUCTION SHALL BE 6'-1" FROM CENTERLINE OF TRACK TO EDGE OF PIER. A MINIMUM VERTICAL CLEARANCE OF 14'-6" OF THE HIGH RUNNING RAIL OF THE CTA TRACK SHALL BE PROVIDED AT ALL TIMES.
15. THE PROPOSED EMBANKMENT SHALL BE BENCHED INTO THE EXISTING SLOPES TO THE SATISFACTION OF THE ENGINEER. SEE DISTRICT 1 DETAILS FOR BENCHING DETAIL.
16. THE CONTRACTOR SHALL NOTIFY THE CHICAGO TRANSIT AUTHORITY AT LEAST THREE WEEKS PRIOR TO REMOVAL OF THE FOLLOWING ITEMS FROM THE EXISTING BRICK STATION HOUSE ON THE BRIDGE: TURNSTYLES, FARE VENDING MACHINES, AUTOMATED TELLER MACHINES, TRASH RECEPTACLES, BENCHES, BIKE RACK (INSIDE THE GLASS BUILDING), SPEAKERS AND CAMERAS. THE CONTRACTOR SHALL STORE THESE ITEMS FOR RELOCATION TO THE RENOVATED GLASS BUILDING AS SHOWN ON THE PLANS AND APPROVED BY THE ENGINEER. THE REMOVAL, TRANSPORTATION, STORAGE AND PLACEMENT OF THESE ITEMS WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN COST OF REMOVAL OF EXISTING STRUCTURES. ANY ITEM THAT IS DAMAGED DURING REMOVAL, TRANSPORT, STORAGE OR PLACEMENT SHALL BE REPLACED AT THE CONTRACTOR'S OWN EXPENSE.
17. ALL ROADWAY WIDTHS AND RADIUS SHOWN ON THE PLANS ARE TO THE EDGE OF PAVEMENT UNLESS OTHERWISE NOTED.
18. DRAINAGE
 - a. ALL STORM SEWER CONNECTIONS WITH PIPES 27 INCHES IN DIAMETER AND SMALLER SHALL BE MADE WITH PRECAST TEES OR ELBOW PIPES. FOR PROPOSED STORM SEWERS LARGER THAN 27 INCHES IN DIAMETER, OPENINGS OF THE SPECIFIED DIAMETER SHALL BE IN THE PIPE AT THE TIME IT IS MANUFACTURED. PRECAST "TEE" AND "ELBOW" PIPE CONNECTIONS FOR PROPOSED STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST FOR THE STORM SEWERS.
 - b. COMBINED SEWER, (WATER MAIN REQUIREMENTS) SHALL BE USED AT LOCATIONS WHERE LATERAL SEPARATION BETWEEN THE SEWER AND WATER MAIN IS LESS THAN 10 FT AND THE VERTICAL SEPARATION IS LESS THAN 1.5 FT. COMBINED SEWER (WATER MAIN REQUIREMENTS) SHALL ALSO BE USED TO CONNECT INLETS, TYPE A, TYPE 1 FRAME OPEN LID (CITY OF CHICAGO) TO CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, OPEN LID (CITY OF CHICAGO). DUCTILE IRON PIPE WITH RUBBER GASKET JOINTS SHALL BE USED FOR ALL COMBINED SEWER, (WATER MAIN REQUIREMENTS).
 - c. OFFSETS AND TOP OF FRAME OR LID ELEVATIONS FOR STRUCTURES WHICH ARE LOCATED ADJACENT TO CURB AND GUTTER OR CURB ARE GIVEN AT THE EDGE OF PAVEMENT. OFFSETS AND TOP OF FRAME OR LID ELEVATIONS FOR STRUCTURES NOT LOCATED IN THE GUTTER ARE GIVEN TO THE CENTER OF THE GRATE OR LID. STRUCTURES ADJACENT TO CURB SHALL BE TURNED SO THAT THE FRAME IS CLOSEST TO THE CENTER LINE OF THE ROAD UNLESS OTHERWISE NOTED ON THE PLANS.
 - d. DRAINAGE GRADES SHALL BE VERIFIED IN FIELD PRIOR TO INSTALLATION OF DRAINAGE ITEMS.
 - e. CONNECTING EXISTING DRAINAGE SYSTEMS TO PROPOSED DRAINAGE SYSTEMS SHALL BE INCLUDED IN THE COST OF THE PROPOSED DRAINAGE ITEMS. CARE SHALL BE TAKEN BY THE CONTRACTOR TO NOT DAMAGE THE EXISTING DRAINAGE SYSTEMS. ANY DAMAGE CAUSED BY THE CONTRACTOR TO THE EXISTING DRAINAGE SYSTEMS SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR AT THE CONTRACTOR'S OWN EXPENSE AND TO THE SATISFACTION OF THE ENGINEER. NO ADDITIONAL COMPENSATION SHALL BE PROVIDED.
 - f. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING POSITIVE DRAINAGE ON THE JOB SITE DURING CONSTRUCTION.

NOTE:

BOXED ITEMS ARE INCLUDED IN THE COST OF THE CONTRACT.

19. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
20. THE THICKNESS OF THE HOT-MIX ASPHALT MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HOT-MIX ASPHALT MIXTURE IS PLACED.
21. WHEN THE PAVEMENT CONSTRUCTED IS PORTLAND CEMENT CONCRETE BASE COURSE OR PORTLAND CEMENT CONCRETE PAVEMENT, IT SHALL NOT BE OPENED TO TRAFFIC, INCLUDING CONSTRUCTION TRAFFIC, UNTIL AFTER THE SPECIFIED CURING PERIOD AS DEFINED IN ARTICLE 701.17(c) OF THE STANDARD SPECIFICATIONS AND UNTIL THE JOINTS HAVE BEEN SEALED.
22. PROTECTIVE COAT SHALL BE APPLIED IN ACCORDANCE WITH ARTICLE 420.18 OF THE STANDARD SPECIFICATIONS TO CONCRETE MEDIAN SURFACES AND BARRIER AND ALL EXPOSED SURFACES OF CURBS AND GUTTERS.
23. THE ENTIRE AREA WHICH IS TO RECEIVE BITUMINOUS MATERIALS (PRIME COAT) SHALL BE SWEEP CLEAN BEFORE THE MATERIAL APPLICATION. SWEEPINGS SHALL NOT BE DEPOSITED IN THE GUTTER OR ON THE CURB, PARKWAY, OR SIDEWALK, BUT SHALL BE PICKED UP AND DISPOSED OF PROPERLY BEYOND THE LIMITS OF THE PROJECT ON THE SAME DAY THAT SWEEPING IS DONE. THIS WORK SHALL BE INCLUDED IN THE COST OF BITUMINOUS MATERIALS (PRIME COAT) AND NO ADDITIONAL COMPENSATION SHALL BE ALLOWED.
24. ALL REINFORCEMENT BARS, DOWEL BARS, AND TIE BARS SHALL BE EPOXY COATED UNLESS OTHERWISE NOTED IN THE PLANS.
25. IF ANY UNUSUAL MATERIALS ARE UNCOVERED OR THERE ARE SUSPICIONS OF EXISTING UNDERGROUND STORAGE TANKS, THE GENERAL CONTRACTOR IS REQUIRED TO HIRE AN ENVIRONMENTAL FIRM WITH AT LEAST FIVE (5) DOCUMENTED LEAKING UNDERGROUND STORAGE TANK (LUST) CLEANUPS OR THAT IS PREQUALIFIED IN HAZARDOUS WASTE BY THE DEPARTMENT TO REMEDIATE THE SOIL CONTAMINATION AND MONITOR FOR WORKER PROTECTION.
26. THE CONTRACTOR IS RESPONSIBLE FOR PROTECTING EXISTING PLANT MATERIAL.
27. WHERE SECTION, SUBSECTION, SUBDIVISION OR PROPERTY MONUMENTS ARE ENCOUNTERED, THE ENGINEER SHALL BE NOTIFIED BEFORE SUCH MONUMENTS ARE REMOVED. THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL PROPERTY MARKERS AND MONUMENTS UNTIL THE OWNER, AN AUTHORIZED SURVEYOR, OR AGENT HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATION.
28. FOR STORM SEWER CONSTRUCTED UNDER THE ROADWAY, BACKFILLING METHODS TWO AND THREE AUTHORIZED UNDER THE PROVISIONS OF ARTICLE 550.07 OF THE STANDARD SPECIFICATIONS WILL NOT BE ALLOWED.
29. ABANDONED UNDERGROUND UTILITIES THAT CONFLICT WITH CONSTRUCTION SHALL BE DISPOSED OF OUTSIDE THE LIMITS OF THE RIGHT-OF-WAY ACCORDING TO ART. 202.03 OF THE STANDARD SPECIFICATIONS AND AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF EARTH EXCAVATION.
30. A QUANTITY OF HIGH-EARLY STRENGTH PORTLAND CEMENT CONCRETE BASE COURSE IS INCLUDED IN THE PLANS. THE ENGINEER SHALL APPROVE THE USE OF THIS MATERIAL PRIOR TO PLACEMENT.
31. A QUANTITY OF 100 FEET OF EXPLORATION TRENCH 52" DEPTH HAS BEEN INCLUDED IN THE PLANS FOR THE PURPOSE OF IDENTIFYING ANY BURIED OBSTACLE. THE ENGINEER SHALL APPROVE THE LOCATIONS OF EXPLORATION TRENCH 52" DEPTH BEFORE ANY EXCAVATION MAY BEGIN.
32. SAW CUT (FULL DEPTH) SHALL BE REQUIRED AT THE JOINT BETWEEN PAVEMENT, SIDEWALK, CURB, CURB AND GUTTER, MEDIAN, DRIVEWAY PAVEMENT, HOT-MIX ASPHALT SURFACES TO BE REMOVED AND THAT LEFT IN PLACE OR AS DIRECTED BY THE ENGINEER. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE REMOVAL ITEMS.
33. THE DEPARTMENT HAS NOT OBTAINED ANY PERMITS FOR OFFSITE BORROW OR WASTE/USE (BWU) AREAS, PRIOR TO WORKING IN BWU AREAS, IF THE CONTRACTOR CHOOSES TO USE ACTIVITIES REQUIRING PERMITS IT IS THE CONTRACTOR'S RESPONSIBILITY TO SECURE THE PROPER PERMITS. IN ADDITION TO THE BORROW REVIEW (BDE 2289) AND USE/WASTE REVIEW (BDE 2290) SUBMITTALS, THE CONTRACTOR WILL NEED TO SUBMIT AN EROSION AND SEDIMENT CONTROL (ESC) PLAN FOR EVERY BWU SITE TO THE DEPARTMENT FOR ACCEPTANCE. GUIDELINES FOR ACCEPTABLE BWU PRACTICES CAN BE FOUND IN SECTION 11.5.A AND B OF THE SWPPP. THE COST OF ALL MATERIALS AND LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS TO PREPARE AND IMPLEMENT ESC PLANS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
34. DEPARTMENTS WILL REQUIRE A MINIMUM OF THREE (3) WEEKS TO REVIEW ANY SUBMITTALS REQUIRING THE DEPARTMENT'S APPROVAL. NO ADDITIONAL COMPENSATION AND NO EXTENSION OF CALENDAR DAYS WILL BE MADE DUE TO THE DEPARTMENT REQUIRING MORE TIME TO REVIEW SUBMITTALS.
35. THE CONTRACTOR SHALL NOTIFY THE VICE CHANCELLOR FOR ADMINISTRATIVE SERVICES AT THE UNIVERSITY OF ILLINOIS AT CHICAGO 312-413-1404 AT LEAST THREE WEEKS PRIOR TO REMOVAL OF ALL BICYCLE RACKS, PARK BENCHES, FLAG POLES, TRASH CANS, LIGHT POLES, FENCING, SIGNING, MONUMENTS AND GUARDRAIL AS NOTED ON THE PLANS. THE CONTRACTOR SHALL MOVE THESE ITEMS TO EITHER ONE OF THE FOLLOWING DESIGNATED STORAGE LOCATIONS: 1515 WEST 15TH STREET, CHICAGO OR 1140 SOUTH MORGAN STREET, CHICAGO. THESE ITEMS SHALL BE RETRIEVED FROM THE DESIGNATED STORAGE LOCATION AND PLACED IN THE FINAL LOCATIONS AS APPROVED BY THE ENGINEER. IF NO FINAL LOCATION HAS BEEN APPROVED, THE ITEMS SHALL REMAIN IN STORAGE. THE REMOVAL, TRANSPORTATION AND PLACEMENT OF THESE ITEMS, EXCEPT FOR FENCING AND GUARDRAIL, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN CONTRACT BID PRICE. THE REMOVAL AND TRANSPORTATION OF FENCING AND GUARDRAIL WILL BE PAID FOR AS FENCE REMOVAL AND GUARDRAIL REMOVAL, RESPECTIVELY. ANY ITEM THAT IS DAMAGED DURING REMOVAL, TRANSPORT OR PLACEMENT SHALL BE REPLACED AT THE CONTRACTOR'S OWN EXPENSE.
36. THE CONTRACTOR SHALL ENSURE THAT NO CONSTRUCTION ACTIVITY OCCURS BEYOND THE TEMPORARY CHAIN LINK FENCE AND/OR LIMITS OF CONSTRUCTION ALONG PEORIA STREET.
37. THE CONTRACTOR SHALL PLACE TEMPORARY FENCE AROUND THE WORK AREA WITHIN THE CTA RIGHT-OF-WAY AS SHOWN ON THE EROSION CONTROL PLANS. THE CONTRACTOR SHALL RESTORE THIS AREA TO ITS EXISTING CONDITION UPON COMPLETION OF THE WORK. RESTORATION OF THE CTA RIGHT-OF-WAY SHALL BE INCLUDED IN THE COST OF THE TEMPORARY FENCE.
38. THE CONTRACTOR SHALL AVOID WORK THAT WILL CAUSE EXCESSIVE NOISE ADJACENT TO 412 S PEORIA ST (COLLEGE OF URBAN PLANNING & PUBLIC AFFAIRS) AND 400 S PEORIA ST (UIC ART AND DESIGN HALL) DURING FINALS WEEK MAY 5, 2014 TO MAY 9, 2014. IN ADDITION, A WEEK IN DECEMBER 2014 AND MAY 2015 WILL HAVE THE SAME RESTRICTIONS. THESE DATES SHOULD BE CONFIRMED WITH THE UNIVERSITY ONCE THE CAMPUS SCHEDULE FOR THAT TIMEFRAME HAS BEEN ESTABLISHED. NO ADDITIONAL TIME EXTENSION WILL BE GRANTED FOR DELAYS INCURRED AS A RESULT OF ANY WORK STOPPAGE.
39. CARE SHALL BE TAKEN TO NOT DAMAGE THE BRIDGE DECK LATEX CONCRETE OVERLAY ONCE IT HAS BEEN PLACED IN STAGE 2. ANY DAMAGE TO THE BRIDGE DECK LATEX CONCRETE OVERLAY SHALL BE REPAIRED AT THE CONTRACTOR'S OWN EXPENSE.

FILE PATH: c:\11888029\p\int\lans\constr\plan\plan\GENERAL NOTES.dwg USER: BAVITOT DATE: 12/19/2013



0100W29.sht-000note-03.dgn	DESIGNED - JLV	REVISED - A 12/18/2013
USER NAME = BAVITOT	DRAWN - BAW	REVISED -
PLOT SCALE = 100.0000' / 1"	CHECKED - JMC	REVISED -
PLOT DATE = 12/19/2013	DATE - 10/30/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

GENERAL NOTES		SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
SCALE: NONE	SHEET 1 OF 1 SHEETS	2013-011R	COOK	356	4
		CONTRACT NO. 60W29			
		ILLINOIS FED. AID PROJECT			

FILE PATH : p:\11382924\p\11382924\11382924.dwg
 PROJECT : TRANSPORTATION\11382924\11382924.dwg
 USER : JTRubley
 PLOT SCALE : 20.0000' / 1"

CODE NO.	ITEM	UNIT	URBAN TOTAL QUANTITY	CONSTRUCTION CODE					
				ROADWAY	ROADWAY	BRIDGE	LIGHTING	OTHER	
				90% FED	100% STATE	90% FED	90% FED	90% FED	
				10% STATE	100% STATE	10% STATE	10% STATE	10% STATE	
				0004	0004	0011	0021	0044	
URBAN	URBAN	016-1708	URBAN	URBAN					
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	125	125					
44000300	CURB REMOVAL	FOOT	50	50					
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	774	774					
44000600	SIDEWALK REMOVAL	SQ FT	16,344	16,344					
44001980	CONCRETE BARRIER REMOVAL	FOOT	507	507					
50100100	REMOVAL OF EXISTING STRUCTURES	EACH	1			1			
50157300	PROTECTIVE SHIELD	SQ YD	1,394			1,394			
50200100	STRUCTURE EXCAVATION	CU YD	3368	75		3,293			
50300225	CONCRETE STRUCTURES	CU YD	1060.1	28.9		1031.2			
50300254	RUBBED FINISH	SQ FT	4065			4065			
50300255	CONCRETE SUPERSTRUCTURE	CU YD	209.3			209.3			
50300285	FORM LINER TEXTURED SURFACE	SQ FT	1020			1020			
50300300	PROTECTIVE COAT	SQ YD	395			395			
50500105	FURNISHING AND ERECTING STRUCTURAL STEEL	L SUM	1			1			
50500505	STUD SHEAR CONNECTORS	EACH	6,806			6,806			

* DENOTES SPECIALTY ITEM

** DENOTES NON-PARTICIPATING ITEM



DESIGNED - JLV
 DRAWN - BAW
 CHECKED - JMG
 DATE - 10/30/2013

REVISED -
 REVISED -
 REVISED -
 REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: NONE SHEET 4 OF 17 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	9
CONTRACT NO. 60W29			ILLINOIS FED. AID PROJECT	

Rev.

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CODE NO.	ITEM	UNIT	URBAN TOTAL QUANTITY	CONSTRUCTION CODE					
				ROADWAY	ROADWAY	BRIDGE	LIGHTING	OTHER	
				90% FED		90% FED	90% FED	90% FED	
				10% STATE	100% STATE	10% STATE	10% STATE	10% STATE	
				0004	0004	0011	0021	0044	
URBAN	URBAN	016-1708	URBAN	URBAN					
50800105	REINFORCEMENT BARS	POUND	292,640			292,640			
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	168,910	2,130		166,780			
51500100	NAME PLATES	EACH	1			1			
51602000	PERMANENT CASING	FOOT	1,554			1,554			
* 51603000	DRILLED SHAFT IN SOIL	CU YD	1,318.9			1,318.9			
* 51604000	DRILLED SHAFT IN ROCK	CU YD	38.2			38.2			
52100010	ELASTOMERIC BEARING ASSEMBLY, TYPE I	EACH	18			18			
52100020	ELASTOMERIC BEARING ASSEMBLY, TYPE II	EACH	9			9			
52100520	ANCHOR BOLTS, 1"	EACH	18			18			
52100530	ANCHOR BOLTS, 1 1/4"	EACH	36			36			
550A0330	STORM SEWERS, CLASS A, TYPE 2 10"	FOOT	6	6					
550A0050	STORM SEWERS, CLASS A, TYPE 1 12"	FOOT	100	100					
550A0340	STORM SEWERS, CLASS A, TYPE 2 12"	FOOT	18	18					
55100400	STORM SEWER REMOVAL 10"	FOOT	22	22					
58700300	CONCRETE SEALER	SQ FT	8459	337		8,122			

* DENOTES SPECIALTY ITEM ** DENOTES NON-PARTICIPATING ITEM



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 USER NAME = JTRubley
 PLOT SCALE = 20.0000' / 1" = 20'
 PLOT DATE = 11/6/2013

DESIGNED - JLV
 DRAWN - BAW
 CHECKED - JMG
 DATE - 10/30/2013

REVISED -
 REVISED -
 REVISED -
 REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
 SCALE: NONE SHEET 5 OF 17 SHEETS STA. TO STA.

F.A.I. RTE. SECTION COUNTY TOTAL SHEETS SHEET NO.
 90/94/290 2013-01R COOK 356 10
 CONTRACT NO. 60W29
 ILLINOIS FED. AID PROJECT

Rev.

CODE NO.	ITEM	UNIT	URBAN TOTAL QUANTITY	CONSTRUCTION CODE				
				ROADWAY	ROADWAY	BRIDGE	LIGHTING	OTHER
				90% FED		90% FED	90% FED	90% FED
				10% STATE	100% STATE	10% STATE	10% STATE	10% STATE
				0004	0004	0011	0021	0044
URBAN	URBAN	016-1708	URBAN	URBAN				
59100100	GEOCOMPOSITE WALL DRAIN	SQ YD	535	37		490		
60200105	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	5	5				
60201310	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 20 FRAME AND GRATE	EACH	2	2				
60200210	CATCH BASINS, TYPE C, TYPE 20 FRAME AND GRATE	EACH	1	1				
60250200	CATCH BASINS TO BE ADJUSTED	EACH	5	5				
60255500	MANHOLES TO BE ADJUSTED	EACH	7	7				
60500040	REMOVING MANHOLES	EACH	1	1				
60500050	REMOVING CATCH BASINS	EACH	4	4				
60500060	REMOVING INLETS	EACH	2	2				
60500405	FILLING VALVE VAULTS	EACH	1	1				
60605000	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	FOOT	188	188				
60618300	CONCRETE MEDIAN SURFACE, 4 INCH	SQ FT	73	73				
63200310	GUARDRAIL REMOVAL	FOOT	305	305				
66400105	CHAIN LINK FENCE, 4'	FOOT	5			5		
66400305	CHAIN LINK FENCE, 6'	FOOT	58	58				
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	10,400	10,400				

* DENOTES SPECIALTY ITEM

** DENOTES NON-PARTICIPATING ITEM

FILE PATH: c:\projects\60w29\60w29.dgn
 USER: jtrubley
 PLOT SCALE: 1/8" = 1'-0"
 PLOT DATE: 11/6/2013



DESIGNED - JLV	REVISED -
DRAWN - BAW	REVISED -
CHECKED - JMG	REVISED -
DATE - 10/30/2013	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: NONE	SHEET 6	OF 17 SHEETS	STA.	TO STA.
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	11
CONTRACT NO. 60W29				
ILLINOIS FED. AID PROJECT				

Rev.

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CODE NO.	ITEM	UNIT	URBAN TOTAL QUANTITY	CONSTRUCTION CODE					
				ROADWAY 90% FED	ROADWAY 100% STATE	BRIDGE 90% FED	LIGHTING 90% FED	OTHER 90% FED	
				10% STATE	10% STATE	10% STATE	10% STATE	10% STATE	
				0004	0004	0011	0021	0044	
				URBAN	URBAN	016-1708	URBAN	URBAN	
* 66900450	SPECIAL WASTE PLANS AND REPORTS	L SUM	1	1					
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	4	4					
67100100	MOBILIZATION	L SUM	1	1					
70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	259	259					
70106800	CHANGEABLE MESSAGE SIGN	CAL MO	35	35					
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SQ FT	3880	3880					
70400100	TEMPORARY CONCRETE BARRIER	FOOT	1463	1463					
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	463	463					
70600260	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	2	2					
70600332	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3	EACH	1	1					
* 72000100	SIGN PANEL - TYPE 1	SQ FT	28	28					
* 72000200	SIGN PANEL - TYPE 2	SQ FT	35	35					
* 72000300	SIGN PANEL - TYPE 3	SQ FT	1031		1031				

* DENOTES SPECIALTY ITEM ** DENOTES NON-PARTICIPATING ITEM



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 USER NAME = JTRubley
 PLOT SCALE = 28.0000' / 1" =
 PLOT DATE = 11/6/2013

DESIGNED - JLV REVISED -
 DRAWN - BAW REVISED -
 CHECKED - JMC REVISED -
 DATE - 10/30/2013 REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
 SCALE: NONE SHEET 7 OF 17 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	12
CONTRACT NO. 60W29			ILLINOIS FED. AID PROJECT	

Rev.

CODE NO.	ITEM	UNIT	URBAN TOTAL QUANTITY	CONSTRUCTION CODE				
				ROADWAY	ROADWAY	BRIDGE	LIGHTING	OTHER
				90% FED		90% FED	90% FED	90% FED
				10% STATE	100% STATE	10% STATE	10% STATE	10% STATE
				0004	0004	0011	0021	0044
URBAN	URBAN	016-1708	URBAN	URBAN				
* 84200804	REMOVAL OF POLE FOUNDATION	EACH	15				15	
* 87000885	ELECTRIC CABLE ASSEMBLY IN CONDUIT, 600V (XLP-TYPE TC) 2/C NO. 6 AND NO. 8	FOOT	1478				1478	
* 87301727	ELECTRIC CABLE IN CONDUIT, COMMUNICATION NO. 19 6C	FOOT	1478				1478	
* 87900200	DRILL EXISTING HANDHOLE	EACH	11				11	
* 89502380	REMOVE EXISTING HANDHOLE	EACH	3				3	
* X0322141	REMOVE TEMPORARY WOOD POLE	EACH	4				4	
* K0029634	WEED CONTROL, PRE-EMERGENT GRANULAR HERBICIDE	POUND	1	1				
X0322400	PILE EXTRACTION	EACH	77			77		
* K1003679	MULCH	CYD	14	14				
* X0324198	REMOVAL OF ASBESTOS CEMENT CONDUIT	FOOT	885				885	
* X0325207	TELEVISION INSPECTION OF SEWER	FOOT	1546	1546				
* X0325815	REMOVE EXISTING CABLE	FOOT	340				340	
* X0326326	CABLE IN CONDUIT, TRIPLEX, 2-1/C NO. 6 AND 1-1/C NO. 8 GROUND	FOOT	1115				1115	
X0326382	CONCRETE BARRIER BASE (SPECIAL)	FOOT	467	467				
X0326486	DECORATIVE RAILING (PARAPET MOUNTED)	FOOT	496			496		
** X0326801	COMBINED SEWERS TO BE CLEANED	FOOT	453		453			
X0326935	CROSSHOLE SONIC LOGGING	EACH	4			4		
* X0327004	TEMPORARY WOOD POLE, 60 FT., CLASS 4	EACH	4				4	

* DENOTES SPECIALTY ITEM

** DENOTES NON-PARTICIPATING ITEM



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 USER NAME = JTRubiny
 PLOT SCALE = 28.0000' / 1" =
 PLOT DATE = 11/6/2013

DESIGNED - JLV
 DRAWN - BAW
 CHECKED - JMG
 DATE - 10/30/2013

REVISED -
 REVISED -
 REVISED -
 REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
 SCALE: NONE SHEET 11 OF 17 SHEETS STA. TO STA.

F.A.J. RTE. 90/94/290
 SECTION 2013-011R
 COUNTY COOK
 TOTAL SHEETS 356 SHEET NO. 16
 CONTRACT NO. 60W29
 ILLINOIS FED. AID PROJECT

Rev.

FILE PATH: \\p0111\share\proj\1160\1160.dwg
 USER: jtrubley
 PLOT SCALE: 20.0000' / 1"

CODE NO.	ITEM	UNIT	URBAN TOTAL QUANTITY	CONSTRUCTION CODE					
				ROADWAY 90% FED	ROADWAY 100% STATE	BRIDGE 90% FED	LIGHTING 90% FED	OTHER 90% FED	
				ROADWAY 10% STATE	ROADWAY 10% STATE	BRIDGE 10% STATE	LIGHTING 10% STATE	OTHER 10% STATE	
				0004	0004	0011	0021	0044	
				URBAN	URBAN	016-1708	URBAN	URBAN	
X0327357	CONSTRUCTION VIBRATION MONITORING	L SUM	1	1					
X0327614	COMBINED SEWER REMOVAL 12"	FOOT	4	4					
X0327615	COMBINED SEWER REMOVAL 8"	FOOT	85	85					
X0327616	MAINTAINING ITS DURING CONSTRUCTION	CAL MO	15				15		
* X0370049	UNDERGROUND CONDUIT, PVC, 2" DIA., SCHEDULE 80 (CDOT)	FOOT	635				635		
X0370069	COMBINED SEWER (EXTRA STRENGTH VITRIFIED CLAY PIPE) 8 INCH (CDOT)	FOOT	110	110					
X0370070	COMBINED SEWER, (WATER MAIN REQUIREMENTS), 8 INCH CDOT	FOOT	30	30					
X0370080	COMBINATION CURB AND GUTTER TYPE B V.12 (CDOT)	FOOT	238.5	238.5					
X0370081	SAND CUSHION, 4 INCH, (CDOT)	SQ FT	4844	4844					
** X0370085	CLEAN MANHOLE OR HANDHOLE (CDOT)	EACH	4		4				
X0370135	CONCRETE CURB, TYPE B (SPECIAL) (CDOT)	FOOT	807.6	807.6					
X0539800	TREE GRATES	EACH	3	3					
X4240800	DETECTABLE WARNINGS (SPECIAL)	SQ FT	12	12					
X5011100	FOUNDATION REMOVAL	EACH	8			8			
X5091730	BRIDGE FENCE RAILING (SPECIAL)	FOOT	129			129			

* DENOTES SPECIALTY ITEM

** DENOTES NON-PARTICIPATING ITEM



DESIGNED - JLV
 DRAWN - BAW
 CHECKED - JMG
 DATE - 10/30/2013

REVISED -
 REVISED -
 REVISED -
 REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: NONE SHEET 12 OF 17 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	17
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60W29	

Rev.

CODE NO.	ITEM	UNIT	URBAN TOTAL QUANTITY	CONSTRUCTION CODE				
				ROADWAY	ROADWAY	BRIDGE	LIGHTING	OTHER
				90% FED		90% FED	90% FED	90% FED
				10% STATE	100% STATE	10% STATE	10% STATE	10% STATE
				0004	0004	0011	0021	0044
URBAN	URBAN	016-1708	URBAN	URBAN				
X0370201	UNDERGROUND CONDUIT, PVC, 3/4" DIA., SCHEDULE 80 (CDOT)	FOOT	775				775	
* X0370075	UNDERGROUND CONDUIT, PVC, 4" DIA., SCHEDULE 80 (CDOT)	FOOT	2565				2565	
* X0370186	ELECTRICAL MANHOLE, 3' X 4' X 4', 30" FRAME AND LID (CDOT)	EACH	2				2	
* 83000023	LIGHT POLE, ALUMINUM, 18 FT. M.H., 8 FT. DAVIT ARM	EACH	12				12	
* 83000024	LIGHT POLE, ALUMINUM, 18 FT. M.H., 8 FT. DAVIT ARM - TWN	EACH	2				2	
X0327688	REMOVAL OF ETU BOLLARD FOUNDATION	EACH	2				2	
* X0370076	ROD AND CLEAN DUCT IN EXISTING CONDUIT SYSTEM	FOOT	160				160	
** X0327689	COMBINED SEWERS TO BE CLEANED, SPECIAL	FOOT	930		930			
* X0324571	MAINTENANCE OF STREET LIGHTING SYSTEM (CITY OF CHICAGO)	LSUM	1				1	
X0327690	TELEVISION INSPECTION OF SEWER, SPECIAL	FOOT	1,860	1,860				
X0370187	CONCRETE (CTA)	LSUM	1					1
X0370188	DEMOLITION (CTA)	LSUM	1					1
X0370189	STRUCTURAL STEEL AND METAL DECK (CTA)	LSUM	1					1
X0370190	MASONRY (CTA)	LSUM	1					1
X0370200	STATION RENOVATION (CTA)	LSUM	1					1
X0370191	ELEVATOR (CTA)	LSUM	1					1

* DENOTES SPECIALTY ITEM

** DENOTES NON-PARTICIPATING ITEM

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 PLOT SCALE = 20.0000' / 1" in.
 PLOT DATE = 11/6/2013

DESIGNED - JLV
 DRAWN - BAW
 CHECKED - JMC
 DATE - 10/30/2013
 REVISED -
 REVISED -
 REVISED -
 REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
 SCALE: NONE SHEET 16 OF 17 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-01R	COOK	356	21
CONTRACT NO. 60W29			ILLINOIS FED. AID PROJECT	

Rev.

CODE NO.	ITEM	UNIT	URBAN TOTAL QUANTITY	CONSTRUCTION CODE						
				ROADWAY	ROADWAY	BRIDGE	LIGHTING	OTHER		
				90% FED	100% STATE	90% FED	90% FED	90% FED		
				10% STATE	100% STATE	10% STATE	10% STATE	10% STATE		
				0004	0004	0011	0021	0044		
URBAN	URBAN	016-1708	URBAN	URBAN						
* X0370192	MECHANICAL AND PLUMBING (CTA)	LSUM	1							1
* X0370193	ELECTRICAL AND COMMUNICATIONS (CTA)	LSUM	1							1
X0370199	EARTHWORK, MICROPILES AND DRILLED SHAFTS (CTA)	LSUM	1							1
X0370194	EARTHWORK, EXTERIOR IMPROVEMENTS AND UTILITIES WORK (CTA)	LSUM	1							1
X0327649	SOIL RETENTION SYSTEM	SQ FT	104			104				
X5040500	PRECAST CONCRETE DECK PANELS	SQ FT	15,272			15,272				
Z0006012	BRIDGE DECK LATEX CONCRETE OVERLAY, 2 1/4 INCHES	SQ YD	1,614			1,614				
Ø Z0076600	TRAINEES	HOUR	2500	2500						
Ø Z0076604	TRAINEES - TRAINING PROGRAM GRADUATE	HOUR	2500	2500						
* X0370195	RELOCATE DUCT BANK (CTA)	L SUM	1							1
* X0370196	BALLASTED TRACK CONSTRUCTION (CTA)	EACH	4							4
* X0370197	MAINTENANCE OF TRANSIT OPERATIONS (CTA)	L SUM	1							1
** Z0005305	BOX CULVERT TO BE CLEANED	FOOT	144	144						
X0370198	EXPLORATORY TRENCHING (CTA)	FOOT	560							560

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* DENOTES SPECIALTY ITEM

** DENOTES NON-PARTICIPATING ITEM



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 USER NAME = J77ublay
 PLOT SCALE = 20.0000 1/1 in.
 PLOT DATE = 11/6/2013

DESIGNED - JLV
 DRAWN - BAW
 CHECKED - JMC
 DATE - 10/30/2013

REVISED -
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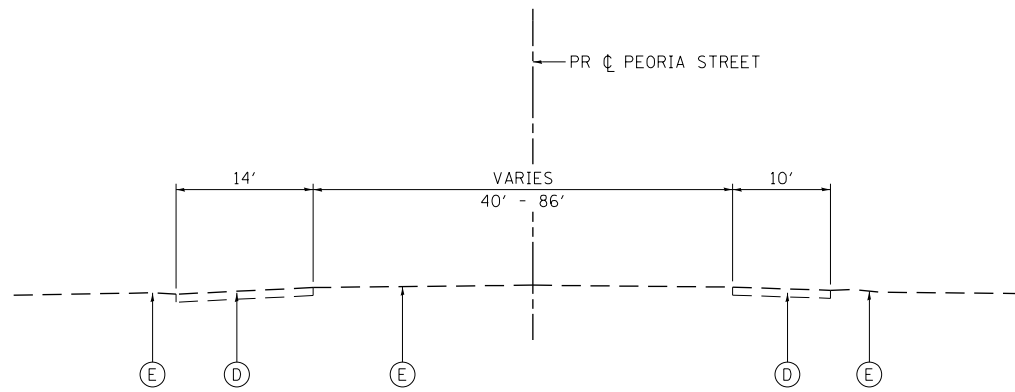
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
 SCALE: NONE SHEET 17 OF 17 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-01R	COOK	356	22
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60W29	

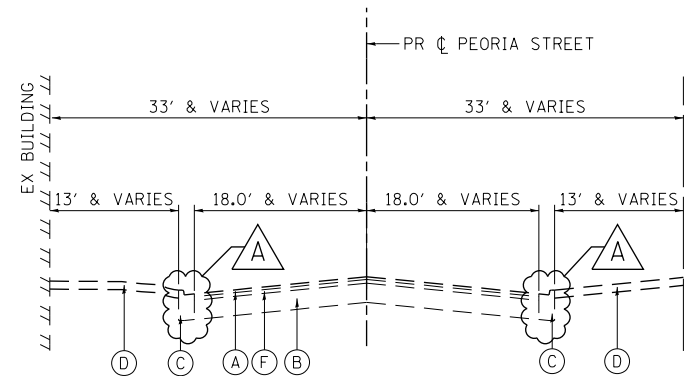
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EXISTING TYPICAL SECTION

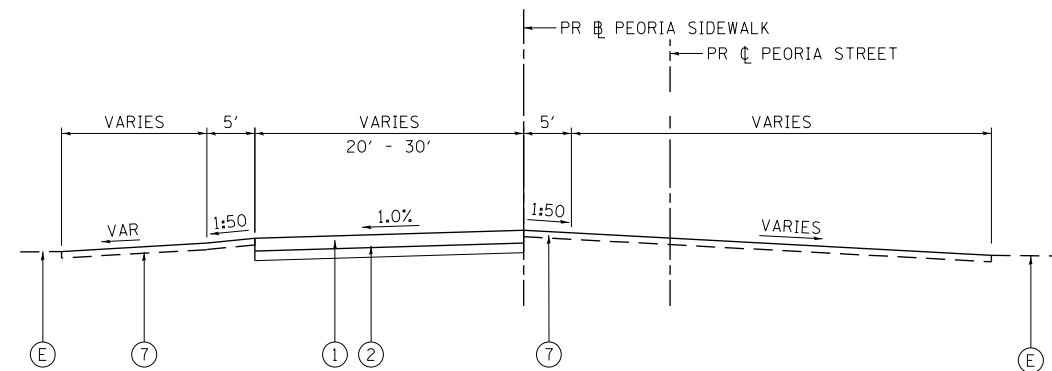
STA 3700+11.87 TO STA 3702+75.98
 EXISTING BRIDGE (SN 016-2082) INCLUDING APPROACH PAVEMENT
 STA 3702+75.98 TO STA 3705+17.14



EXISTING TYPICAL SECTION

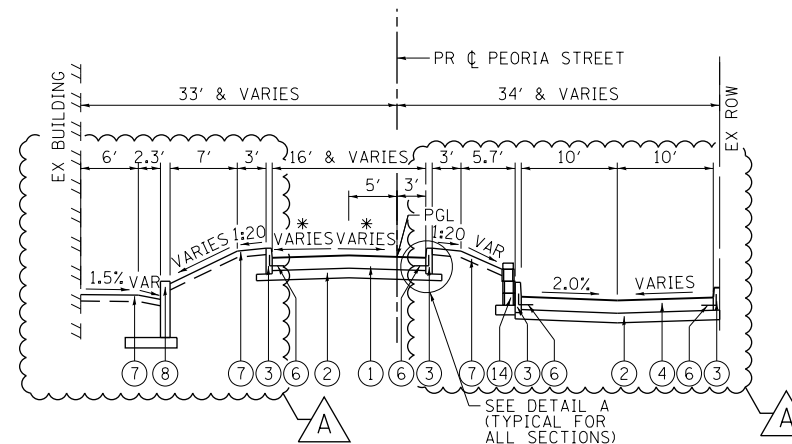
STA 3705+17.14 TO STA 3706+90.99
 EXISTING BRIDGE (SN 016-2082) INCLUDING APPROACH PAVEMENT
 STA 3702+75.98 TO STA 3705+17.14

- EXISTING**
- (A) HOT-MIX ASPHALT SURFACE COURSE, 1 3/4"
 - (B) PORTLAND CEMENT CONCRETE PAVEMENT, 12"
 - (C) CONCRETE CURB AND GUTTER
 - (D) PORTLAND CEMENT CONCRETE SIDEWALK
 - (E) GROUND SURFACE (ASSUME EXISTING 4" TOPSOIL DEPTH)
 - (F) HOT-MIX ASPHALT BINDER COURSE 2 1/4"



PROPOSED TYPICAL SECTION

STA 3700+11.87 TO STA 3702+38.64
 PROPOSED BRIDGE (SN 016-1708) INCLUDING APPROACH PAVEMENT
 STA 3702+38.64 TO STA 3705+49.73



PROPOSED TYPICAL SECTION

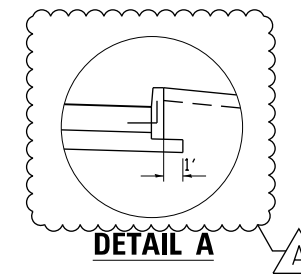
STA 3705+49.73 TO STA 3707+04.98
 • SEE NOTE 2
 PROPOSED BRIDGE (SN 016-1708) INCLUDING APPROACH PAVEMENT
 STA 3702+38.64 TO STA 3705+49.73

PROPOSED

- (1) PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH
- (2) SUB-BASE GRANULAR MATERIAL, TYPE B, 6"
- (3) CONCRETE CURB, TYPE B (SPECIAL) (CDOT)
- (4) PORTLAND CEMENT CONCRETE SIDEWALK (SEE ROADWAY PLAN & PROFILE FOR DEPTHS)
- (5) SAND CUSHION, 4 INCH, (CDOT)
- (6) #5 TIE BARS, 30" LONG AT 30" C-C
 (INCLUDED IN PRICE FOR BID FOR CONCRETE CURB, TY B (SPECIAL) (CDOT))
- (7) TOPSOIL FURNISH AND PLACE, 4"
- (8) RETAINING WALL (SEE RETAINING WALL PLANS)
- (9) TEMPORARY PAVEMENT (SEE TYPICAL SHT 3 FOR DETAILS)
- (10) SUB-BASE GRANULAR MATERIAL, TYPE B, 4"
- (11) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (12) CONCRETE BARRIER WALL (SPECIAL)
- (13) CONCRETE BARRIER BASE (SPECIAL)
- (14) SEGMENTAL CONCRETE BLOCK WALL

NOTES:

1. AS INDICATED ON THE ROADWAY PLANS, TYPICALLY AT LEAST 6' (4' MIN) OF PROPOSED SIDEWALK WIDTH MUST HAVE A CROSS SLOPE OF 1:64 OR LESS.
2. TYPICAL LOCAL ROADWAY CROSS SLOPE VARIES BETWEEN 1.4% AND 3.6% AS INDICATED ON THE PLANS. SEE ROADWAY AND GRADING PLAN DETAILS FOR ROADWAY SLOPE TRANSITIONS.



DETAIL A



D160W29-sht-Typical-01.dgn	DESIGNED - JLV	REVISED - 12/18/2013
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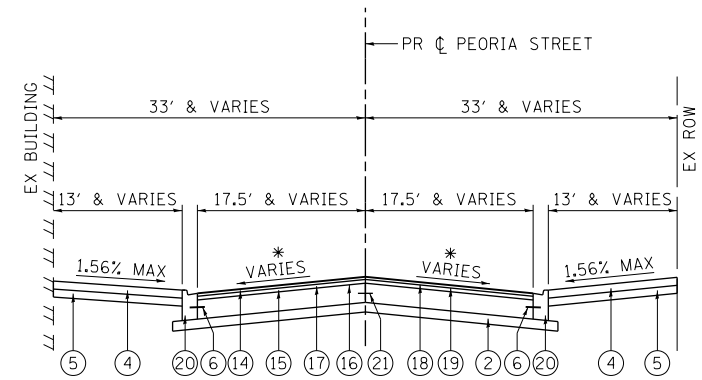
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS
 PEORIA STREET**

SCALE: NONE SHEET 1 OF 3 SHEETS STA. TO STA.

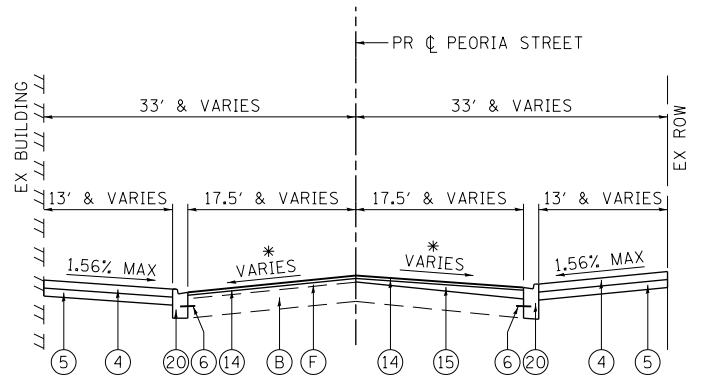
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	23
CONTRACT NO. 60W29			ILLINOIS FED. AID PROJECT	

A



PROPOSED TYPICAL SECTION

STA 3706+61.25 TO STA 3706+90.99



PROPOSED TYPICAL SECTION

STA 3706+90.99 TO STA 3707+45.28

EXISTING

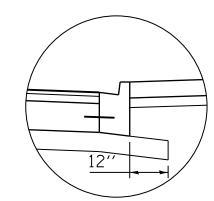
- (A) HOT-MIX ASPHALT SURFACE COURSE, 1 3/4"
- (B) PORTLAND CEMENT CONCRETE PAVEMENT, 12"
- (C) CONCRETE CURB
- (D) PORTLAND CEMENT CONCRETE SIDEWALK
- (E) GROUND SURFACE (ASSUME EXISTING 4" TOPSOIL DEPTH)
- (F) HOT-MIX ASPHALT BINDER COURSE, 2 1/4"

PROPOSED

- (1) PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH
- (2) SUB-BASE GRANULAR MATERIAL, TYPE B, 6"
- (3) CONCRETE CURB, TYPE B (SPECIAL) (CDOT)
- (4) PORTLAND CEMENT CONCRETE SIDEWALK (SEE ROADWAY PLAN & PROFILE FOR DEPTHS)
- (5) SAND CUSHION, 4 INCH, (CDOT)
- (6) #5 TIE BARS, 30" LONG AT 30" C-C (INCLUDED IN PRICE FOR BID FOR COMBINATION CURB AND GUTTER TYPE B V.12 (CDOT))
- (7) TOPSOIL FURNISH AND PLACE, 4"
- (8) RETAINING WALL (SEE RETAINING WALL PLANS)
- (9) TEMPORARY PAVEMENT (SEE TYPICAL SHT 3 FOR DETAILS)
- (10) SUB-BASE GRANULAR MATERIAL, TYPE B, 4"
- (11) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (12) CONCRETE BARRIER WALL (SPECIAL)
- (13) CONCRETE BARRIER BASE (SPECIAL)
- (14) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70, 1 3/4"
- (15) HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70, VARIES 2 1/4" TO 5 1/4"
- (16) PAVEMENT FABRIC
- (17) PORTLAND CEMENT CONCRETE BASE COURSE, 9"
- (18) BITUMINOUS MATERIALS (PRIME COAT)
- (19) AGGREGATE (PRIME COAT)
- (20) COMBINATION CURB AND GUTTER TYPE B V.12 (CDOT)
- (21) #5 TIE BARS, 30" LONG AT 30" C-C (LONGITUDINAL CONSTRUCTION JOINT) (INCLUDED IN PRICE FOR BID FOR PCC BASE COURSE)

NOTES:

1. AS INDICATED ON THE ROADWAY PLANS, TYPICALLY AT LEAST 6' (4' MIN) OF PROPOSED SIDEWALK WIDTH MUST HAVE A CROSS SLOPE OF 1:64 OR LESS.
2. TYPICAL LOCAL ROADWAY CROSS SLOPE VARIES BETWEEN 1.4% AND 3.6% AS INDICATED ON THE PLANS. SEE ROADWAY AND GRADING PLAN DETAILS FOR ROADWAY SLOPE TRANSITIONS.



DETAIL A

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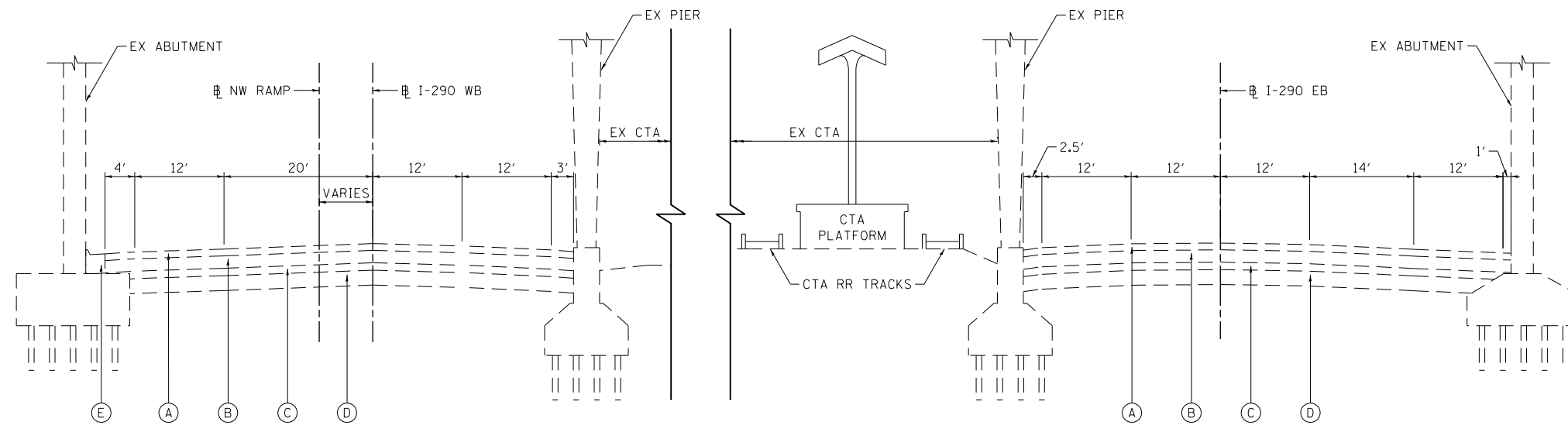


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PLOT DATE = 12/19/2013	DATE - 10/30/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TYPICAL SECTIONS PEORIA STREET	
SCALE: NONE	SHEET 1A OF 3 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	23A
CONTRACT NO. 60W29			ILLINOIS FED. AID PROJECT	



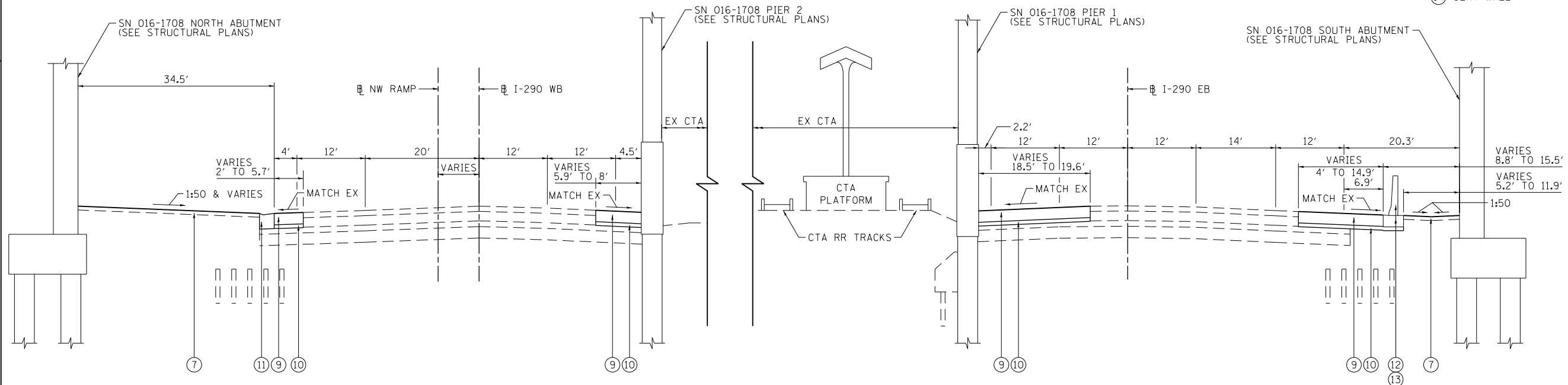
**EXISTING TYPICAL SECTION
I-290 EXPRESSWAY UNDER PEORIA STREET BRIDGE
(LOOKING EAST)**

EXISTING

- (A) HOT-MIX ASPHALT PAVEMENT, 5 1/2" - 5 3/4"
- (B) PORTLAND CEMENT CONCRETE PAVEMENT, 10"
- (C) GRANULAR SUB-BASE, 6"
- (D) PREPARED SUBGRADE, 12"
- (E) CONCRETE CURB AND GUTTER

PROPOSED

- (1) PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH
- (2) SUB-BASE GRANULAR MATERIAL, TYPE B, 6"
- (3) CONCRETE CURB, TYPE B (SPECIAL) (CDOT)
- (4) PORTLAND CEMENT CONCRETE SIDEWALK (SEE ROADWAY PLAN & PROFILE FOR DEPTHS)
- (5) SAND CUSHION, 4 INCH, (CDOT)
- (6) #5 TIE BARS, 30" LONG AT 30" C-C (INCLUDED IN PRICE FOR BID FOR CURB)
- (7) TOPSOIL FURNISH AND PLACE, 4"
- (8) RETAINING WALL (SEE RETAINING WALL PLANS)
- (9) TEMPORARY PAVEMENT (SEE TYPICAL SHT 3 FOR DETAILS)
- (10) SUB-BASE GRANULAR MATERIAL, TYPE B, 4"
- (11) COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24
- (12) CONCRETE BARRIER WALL (SPECIAL)
- (13) CONCRETE BARRIER BASE (SPECIAL)
- (14) SEAT WALL



**PROPOSED TYPICAL SECTION
I-290 EXPRESSWAY UNDER PEORIA STREET BRIDGE
(LOOKING EAST)**

NOTES:

1. AS INDICATED ON THE ROADWAY PLANS, TYPICALLY AT LEAST 6' (4' MIN) OF PROPOSED SIDEWALK WIDTH MUST HAVE A CROSS SLOPE OF 1:64 OR LESS.
2. TYPICAL LOCAL ROADWAY CROSS SLOPE VARIES BETWEEN 1.4% AND 3.6% AS INDICATED ON THE PLANS. SEE ROADWAY AND GRADING PLAN DETAILS FOR ROADWAY SLOPE TRANSITIONS.

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PLOT DATE = 10/29/2013	DATE - 10/30/2013	REVISED -

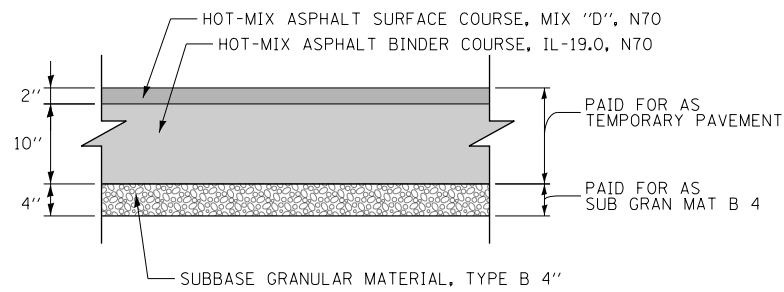
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

TYPICAL SECTIONS			
I-290			
SCALE: NONE	SHEET 2	OF 3 SHEETS	STA. TO STA.

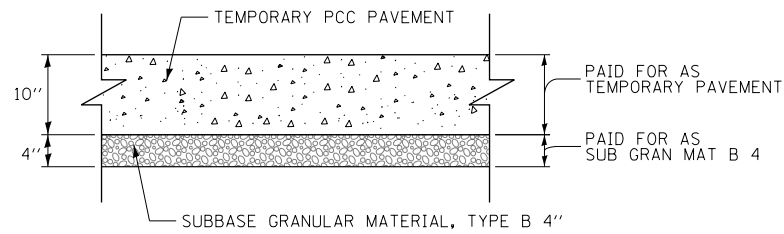
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	24
CONTRACT NO. 60W29			ILLINOIS FED. AID PROJECT	

TEMPORARY PAVEMENT DETAILS:

DETAIL A: FULL DEPTH TEMPORARY HMA PAVEMENT



DETAIL B: TEMPORARY PCC PAVEMENT



TEMPORARY PAVEMENT GENERAL NOTES:

1. THE CONTRACTOR SHALL HAVE THE OPTION OF USING HMA OR PCC SECTION FOR TEMPORARY PAVEMENT.
2. TEMPORARY HMA TEMPORARY PAVEMENT SHALL CONSIST OF TWO ITEMS: HMA BINDER COURSE AND HMA SURFACE COURSE.
3. PORTLAND CEMENT CONCRETE TEMPORARY PAVEMENT SHALL CONSIST OF CLASS PV CONCRETE MEETING THE REQUIREMENTS OF ARTICLE 1020 OF THE STANDARD SPECIFICATIONS. PORTLAND CEMENT CONCRETE TEMPORARY PAVEMENT DOES NOT REQUIRE DOWEL BARS.

HOT MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS (%) @NDES
PAVEMENT RESURFACING	
PEORIA STREET	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5 mm): 1-3/4"	4% @ 70 GYR
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70: VARIES 2-1/4" TO 5-1/4" (IN 1 OR 2 LIFTS)	4% @ 70 GYR
PAVEMENT RECONSTRUCTION	
PEORIA STREET	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5 mm): 1-3/4"	4% @ 70 GYR
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70: 2-1/4"	4% @ 70 GYR
UNIVERSITY OF ILLINOIS AT CHICAGO PARKING LOT	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5 mm): 2"	4% @ 70 GYR
HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19 mm): 8"	4% @ 70 GYR
TEMPORARY PAVEMENT (IF HMA OPTION IS SELECTED BY CONTRACTOR)	
I-290	
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5 mm): 2"	4% @ 70 GYR
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70: 10" (IN 4 LIFTS)	4% @ 70 GYR

1. THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
2. THE "AC TYPE" FOR POLYMERIZED HMA MIXTURES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.

AGGREGATE SUBGRADE IMPROVEMENT AND GEOTECHNICAL FABRIC FOR GROUND STABILIZATION

AN ESTIMATED QUANTITY OF UNDERCUT, AGGREGATE SUBGRADE IMPROVEMENT (ASI) AND GEOTECHNICAL FABRIC HAS BEEN INCLUDED. AGGREGATE SUBGRADE IMPROVEMENT (ASI), HAS BEEN PROVIDED FOR SOILS THAT TEND TO BE UNSUITABLE OR UNSTABLE. GEOTECHNICAL FABRIC FOR GROUND STABILIZATION IS TO BE PLACED BELOW THE ASI. THE ACTUAL NEED FOR REMOVAL AND REPLACEMENT WITH ASI WILL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION BY THE GEOTECHNICAL ENGINEER. ALL POTENTIALLY UNSTABLE SOILS SHOULD BE TESTED WITH EITHER A STATIC OR DYNAMIC CONE PENETROMETER AND TREATED IN ACCORDANCE WITH ARTICLE 301.04 AND THE UNDERCUT GUIDELINES IN THE ILLINOIS DEPARTMENT OF TRANSPORTATION (IDOT) DISTRICT I ASI SPECIAL PROVISION. IF UNSTABLE AND/OR UNSUITABLE MATERIAL IS ENCOUNTERED, THE SOIL SHALL BE REMOVED AND REPLACED WITH ASI AS DETERMINED BY THE GEOTECHNICAL ENGINEER. IF UNSTABLE AND/OR UNSUITABLE MATERIAL IS NOT ENCOUNTERED, THEN THE QUANTITY SHALL BE DEDUCTED AND NO ADDITIONAL COMPENSATION WILL BE DUE TO THE CONTRACTOR.

I-290

STRUCTURAL DESIGN TRAFFIC:	YEAR	<u>2015</u>
PV= _____	SU= _____	MU= _____
ROAD/STREET CLASSIFICATION:	CLASS	<u>I</u>
PERCENT OF STRUCTURAL DESIGN TRAFFIC IN DESIGN LANE:		
P= _____	S= _____	M= _____
TRAFFIC FACTOR:	ACTUAL TF= _____	AC TYPE= _____
	MINIMUM TF= _____	
PG GRADE:	BINDER= _____	SURFACE= _____
SUBGRADE SUPPORT RATING		
SSR= <u>POOR</u>		

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PLOT DATE = 12/19/2013	DATE - 10/30/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TYPICAL SECTIONS
TEMPORARY PAVEMENT**

SCALE: NONE SHEET 3 OF 3 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	25
CONTRACT NO. 60W29				
ILLINOIS FED. AID PROJECT				

REMOVAL SCHEDULE

LOCATION	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	TREE REMOVAL (OVER 15 UNITS DIAMETER)	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	PAVEMENT REMOVAL	DRIVEWAY PAVEMENT REMOVAL	CURB REMOVAL	COMBINATION CURB AND GUTTER REMOVAL	SIDEWALK REMOVAL	CONCRETE BARRIER REMOVAL	APPROACH SLAB REMOVAL	FENCE REMOVAL	GUARDRAIL REMOVAL
	UNIT	UNIT	SQ YD	SQ YD	SQ YD	FOOT	FOOT	SQ FT	FOOT	SQ YD	FOOT	FOOT
PEORIA STREET	1,230	0	19	1,306	125	50	682	16,344		184	44	305
I-290 WB				276			92		186			
I-290 EB				576					321			
TOTAL	1,230	0	19	2,158	125	50	774	16,344	507	184	44	305

TREE REMOVAL SCHEDULE

STATION	OFFSET	LT/RT	NO. STEMS	SIZE DIAMETER	TREE REMOVAL 6-15	TREE REMOVAL OVER 15	TREE PRUNING 1- 10	TREE PRUNING OVER 10
				INCH	UNIT	UNIT	EACH	EACH
3700+15.42	25.25	LT	1	18				1
3700+15.46	53.09	LT	1	10			1	
3700+16.37	25.45	RT	1	20				1
3700+30.30	91.42	RT	3	6,7,12	25			
3700+34.22	59.00	RT	3	6,6,6	18			
3700+39.50	79.40	RT	3	6,6,9	21			
3700+45.16	63.42	LT	5	6,6,6,6,6	30			
3700+49.29	55.16	RT	5	6,6,6,6,6	30			
3700+53.73	75.13	RT	4	5,8,9,9	31			
3700+59.48	133.29	LT	1	7	7			
3700+59.77	58.02	LT	4	4,4,4,4	16			
3700+59.95	122.32	LT	1	8	8			
3700+60.02	109.36	LT	1	8	8			
3700+60.11	97.78	LT	1	10	10			
3700+64.62	52.20	RT	2	8,8	16			
3700+66.62	140.54	LT	1	14	14			
3700+69.01	73.35	RT	1	9	9			
3700+69.96	132.92	LT	1	6	6			
3700+72.44	109.55	LT	1	9	9			
3700+72.55	123.70	LT	1	9	9			
3700+74.06	51.97	LT	3	10,10,10	30			
3700+78.21	77.62	LT	3	4,5,11	20			
3700+80.12	49.02	RT	2	8,8	16			
3700+85.52	49.49	LT	3	4,4,4	12			
3700+85.66	69.31	RT	3	5,7,11	23			
3700+92.13	73.30	LT	3	8,10,12	30			
3700+96.23	44.66	RT	4	8,8,8,8	32			
3700+99.77	46.06	LT	3	8,8,8	24			
3701+05.87	71.23	LT	3	3,3,3	9			
3701+11.43	41.22	RT	4	8,8,8,8	32			
3701+16.12	41.83	LT	3	6,6,6	18			
3701+18.52	62.28	RT	2	9,10	19			
3701+27.43	38.28	RT	3	6,6,6	18			
3701+31.86	38.74	LT	3	6,6,6	18			
3701+32.56	59.57	RT	3	6,9,11	26			
3701+35.99	135.64	LT	1	9	9			
3701+37.89	63.45	LT	4	5,6,7,14	32			
3701+42.18	35.08	RT	4	8,8,8,8	32			
3701+48.13	34.46	LT	4	8,8,8,8	32			
3701+49.05	56.55	RT	2	8,12	20			
3701+58.95	30.70	RT	3	6,6,6	18			
3701+63.88	52.52	RT	2	6,9	15			
3701+64.02	30.40	LT	4	8,8,8,8	32			

STATION	OFFSET	LT/RT	NO. STEMS	SIZE DIAMETER	TREE REMOVAL 6-15	TREE REMOVAL OVER 15	TREE PRUNING 1- 10	TREE PRUNING OVER 10
				INCH	UNIT	UNIT	EACH	EACH
3701+68.51	56.18	LT	3	9,10,11	30			
3701+74.23	28.37	RT	3	6,6,6	18			
3701+78.32	27.02	LT	4	6,6,6,6	24			
3701+80.62	49.23	RT	3	6,8,11	25			
3701+83.94	52.93	LT	2	3,3	6			
3701+93.83	23.83	LT	4	6,6,6,6	24			
3701+95.58	44.95	RT	3	5,6,10	21			
3701+99.25	49.56	LT	3	3,3,4	10			
3702+08.12	20.22	LT	2	6,6	12			
3702+09.21	42.26	RT	3	5,7,9	21			
3702+13.55	58.55	RT	3	6,6,9	21			
3702+14.38	45.92	LT	3	5,8,8	21			
3702+15.68	17.92	RT	2	6,6	12			
3702+23.11	39.33	RT	2	4,4	8			
3702+23.59	17.23	LT	2	6,6	12			
3702+25.17	58.73	LT	4	2,3,4,4	13			
3702+30.05	41.86	LT	2	3,6	9			
3702+32.05	66.92	RT	4	5,6,6,7			4	
3702+34.27	40.04	RT	3	4,5,5	14			
3702+36.55	53.91	RT	4	2,3,3,4	12			
3702+40.31	66.63	RT	6	2,3,3,3,4,4			1	
3702+40.25	38.92	LT	6	3,3,4,4,4,5	23			
3702+42.94	48.95	LT	6	2,2,5,5,5,6	25			
3702+43.48	61.34	LT	5	3,3,3,4,6			5	
3702+49.41	72.96	LT	3	5,6,7			3	
3705+09.32	33.04	RT	1	10	10			
3705+16.01	65.13	LT	1	7			1	
3705+25.00	63.51	LT	1	19				1
3705+26.06	37.47	LT	1	12	12			
3705+26.67	56.70	RT	1	6	6			
3705+30.05	60.98	LT	1	19				1
3705+34.17	73.29	RT	1	5			1	
3705+36.37	60.15	RT	2	4,4	8			
3705+39.96	42.77	RT	1	9	9			
3705+43.76	57.86	LT	1	3			1	
3705+68.55	22.01	LT	1	10	10			
3706+08.85	21.59	LT	1	6	6			
3706+17.85	23.44	RT	1	12	12			
3706+58.14	23.48	RT	1	12	12			
3706+93.59	20.98	LT	1	8			1	
3707+12.44	22.09	RT	1	6			1	
3707+33.10	21.55	LT	1	4			1	

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PLOT DATE = 12/19/2013	DATE - 10/30/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES

SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	26
CONTRACT NO. 60W29			ILLINOIS FED. AID PROJECT	

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ROADWAY SCHEDULE

LOCATION	TREE TRUNK PROTECTION	TREE ROOT PRUNING	TREE PRUNING (1 TO 10 INCH DIAMETER)	TREE PRUNING (OVER 10 INCH DIAMETER)	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	AGGREGATE SUBGRADE IMPROVEMENT	SUBBASE GRANULAR MATERIAL, TYPE B, 4"	SUBBASE GRANULAR MATERIAL, TYPE B, 6"	PORTLAND CEMENT CONCRETE BASE COURSE 9"	HIGH-EARLY-STRENGTH PORTLAND CEMENT CONCRETE BASE COURSE 9"	BITUMINOUS MATERIALS (PRIME COAT)	AGGREGATE (PRIME COAT)	HOT-MIX ASPHALT BINDER COURSE, IL-19.0 N70	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	PAVEMENT FABRIC	PROTECTIVE COAT	BRIDGE APPROACH PAVEMENT CONNECTOR (PCC)	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	COMBINATION CURB AND GUTTER TYPE B V.12 (CDOOT)	SAND CUSHION, 4 INCH, (CDOOT)	CONCRETE CURB, TYPE B (SPECIAL) (CDOOT)	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.24	DETECTABLE WARNINGS (SPECIAL)	CONCRETE MEDIAN SURFACE, 4 INCH	TEMPORARY PAVEMENT	CONCRETE BARRIER BASE (SPECIAL)	CONCRETE BARRIER WALL (SPECIAL)	CONCRETE BARRIER TRANSITION (SPECIAL)	BARRIER WALL MARKERS, TYPE C	CHAIN LINK FENCE, 6'	BOLLARDS	TREE GRATES	HOT-MIX ASPHALT BASE COURSE 8"					
	EACH	EACH	EACH	EACH	SQ YD	CU YD	SQ YD	SQ YD	SQ YD	SQ YD	TON	TON	TON	TON	SQ YD	SQ YD	SQ YD	SQ YD	SQ YD	SQ FT	FOOT	FOOT	FOOT	SQ FT	SQ FT	SQ YD	FOOT	FOOT	FOOT	FOOT	EACH	FOOT	EACH	EACH	SQ YD				
PEORIA STREET	24	24	20	4	150	50		1,448	108		0.3	0.2	34	39	98	133	28	1,290	4,771	238.5	4,844	807.6		12	73														
I-290 WB							233																188				197	90	30	60	2								
I-290 EB							802																			652	377	287	90	8									
HIGH-EARLY-STRENGTH PCC BASE CSE 9" (ASSUME 10% OF PEORIA STREET PCC BASE CSE 9")									-10	10																													
TOTAL	24	24	20	4	150	50	1,035	1,448	98	10	0.3	0.2	34	39	98	133	28	1,290	4,771	238.5	4,844	807.6	188	12	73	849	467	317	150	10	58	2	3			68			

NOTES:
1. ADDITIONAL BARRIER WALL MARKERS, TYPE C QUANTITY PROVIDED ON THE SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN SCHEDULE.



EARTHWORK SCHEDULE

LOCATION	EARTH EXCAVATION	REMOVAL AND DISPOSAL OF UNSUITABLE MATERIAL		(1) (3) EXCAVATION TO BE USED IN EMBANKMENT (15% SHRINKAGE) [EARTH EXCAVATION * 0.85]	(1) EMBANKMENT	(1) EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)	FURNISHED EXCAVATION	NON-SPECIAL WASTE DISPOSAL
		EXISTING TOPSOIL	(2) UNDERCUT					
		CU YD	CU YD					
PEORIA STREET END AREAS (SOUTH OF I-290) (4)	248	435	50	211	438	-227		
PEORIA STREET END AREAS (NORTH OF I-290) (4)	110	35		94	582	-489		
PEORIA STREET SOUTH ABUTMENT	112	0		95	216	-121		
PEORIA STREET PIER 1	0	0		0	110	-110	1,235	10,400
PEORIA STREET PIER 2	0	0		0	68	-68		
PEORIA STREET NORTH ABUTMENT	800	0		680	737	-57		
PEORIA STREET SIPHON	0	0		0	0	0		
HAUL ROAD / TEMP SIDEWALK CONNECTION	0	0		0	160	-160		
TOTAL	1,270	520		1,080	2,311		1,235	10,400
NON-SPECIAL WASTE ADJUSTMENT (5)				-65			65	
ADJUSTED TOTAL	1,270	520		1,015	2,311		1,300	10,400

NOTES:
 (1) NOT A PAY ITEM
 (2) A QUANTITY OF 50 CUBIC YARDS HAS BEEN ASSUMED FOR UNDERCUT.
 (3) QUANTITIES FOR EARTH EXCAVATION AVAILABLE FOR EMBANKMENT HAVE BEEN ADJUSTED TO EXCLUDE ESTIMATED VOLUME OF CONTAMINATED EARTH EXCAVATION REQUIRING DISPOSAL AS NON-SPECIAL WASTE.
 (4) END AREAS SHOWN ON CROSS SECTIONS ARE NOT INTENDED TO DELINEATE AREAS OF NON-SPECIAL WASTE. ACTUAL VOLUMES SHALL BE DETERMINED IN THE FIELD AT THE TIME OF CONSTRUCTION. ESTIMATED QUANTITY ADJUSTMENTS HAVE BEEN APPLIED AFTER STANDARD EARTHWORK COMPUTATIONS.
 (5) A QUANTITY HAS BEEN ASSUMED FOR NON-SPECIAL WASTE DISPOSAL BASED ON THE PRELIMINARY SOIL INVESTIGATION LOCATIONS. SEE SPECIAL PROVISIONS REGARDING LOCATIONS AND REQUIREMENTS FOR PROPER HANDLING AND DISPOSAL.



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES			
SCALE: NONE	SHEET 2	OF 2 SHEETS	STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	27
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60W29	



EXISTING DRAINAGE STRUCTURE SCHEDULE

STATION	OFFSET	CATCH BASINS TO BE ADJUSTED	MANHOLES TO BE ADJUSTED	REMOVING MANHOLES	REMOVING CATCH BASINS	REMOVING INLETS	CATCH BASIN TO BE RECONSTRUCTED (SPECIAL) (SEE NOTE 1)	COMMENTS	
		EACH	EACH	EACH	EACH	EACH	EACH		
PEORIA STREET									
3700+03.04	0.00'		1					EX RIM EL = 592.97	PR RIM EL = 592.97
3700+03.25	72.63' RT	1						EX RIM EL = 592.33	PR RIM EL = 592.33
3700+96.17	108.43' LT	1						EX RIM EL = 593.35	PR RIM EL = 593.50
3702+19.67	17.09' RT		1					EX RIM EL = 594.48	PR RIM EL = 594.48
3702+48.07	56.16' RT				1				
3702+48.25	63.30' LT	1						EX RIM EL = 593.33	PR RIM EL = 593.33
3705+74.79	17.47' LT					1			
3705+76.67	16.74' RT					1			
3705+84.38	1.61' LT			1					
3706+27.90	8.0' RT		1					EX RIM EL = 593.15	PR RIM EL = 594.19
EB I-290									
343+73.70	42.40' RT				1				
344+98.10	35.00' RT		1					EX RIM EL = 577.21	PR RIM EL = 577.21
345+25.20	17.70' LT		1					EX RIM EL = 577.47	PR RIM EL = 577.47
345+25.50	37.30' RT						1		
345+36.90	42.90' RT				1				
345+39.80	26.20' LT	1						EX RIM EL = 577.21	PR RIM EL = 577.21
346+18.40	26.40' LT		1					EX RIM EL = 577.03	PR RIM EL = 577.03
WB I-290									
345+19.10	28.90' RT		1					EX RIM EL = 577.20	PR RIM EL = 577.27
345+27.40	28.80' RT	1						EX RIM EL = 577.19	PR RIM EL = 577.25
345+37.80	35.40' LT				1				
TOTALS		5	7	1	4	2	1		

NOTES:

- SEE SPECIFICATIONS FOR A DESCRIPTION OF THIS WORK.
- STATION, OFFSET, AND ELEVATION FOR STRUCTURES LOCATED ALONG THE SINGLE FACE BARRIER WALL ARE GIVEN AT THE TOE OF THE WALL.

EXISTING PIPE SCHEDULE

DRAINAGE AND UTILITY SHEET NO.	LOCATION	STORM SEWER REMOVAL	COMBINED SEWER REMOVAL			TRENCH BACKFILL	TELEVISION INSPECTION OF SEWER	TELEVISION INSPECTION OF SEWER, SPECIAL	COMBINED SEWERS TO BE CLEANED	COMBINED SEWERS TO BE CLEANED, SPECIAL	STORM SEWERS TO BE CLEANED		WATER MAIN REMOVAL, 12"
		FOOT	FOOT			CU YD	FOOT	FOOT	FOOT	FOOT	FOOT		FOOT
		10"	8"	12"	54"						10"	7-2 3/8"W x 8'-0"H	12"
1	PEORIA STREET	8	85	4	95	263	906	1,860	453	930			103
8,10	WB I-290	5					416				64	144	
9,11	EB I-290	9					224				112		
	TOTAL	22	85	4	95	263	1,546	1,860	453	930	176	144	103

NOTES:

- ADDITIONAL TRENCH BACKFILL QUANTITY PROVIDED ON THE DRAINAGE AND UTILITY SCHEDULE FOR THE PROPOSED STORM SEWER AND COMBINED SEWER.
- THE QUANTITIES PROVIDED FOR TELEVISION INSPECTION OF SEWER AND TELEVISION INSPECTION OF SEWER, SPECIAL CONSIST OF WORK BEFORE AND AFTER CONSTRUCTION.
- COMBINED SEWERS TO BE CLEANED, SPECIAL AND TELEVISION INSPECTION OF SEWER, SPECIAL QUANTIFIES EACH OF THE THREE EXISTING SIPHON PIPES SEPARATELY.

EXISTING UTILITY STRUCTURE SCHEDULE

STATION	OFFSET	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	REMOVE EXISTING HANDHOLE	FILLING VALVE VAULTS	COMMENTS
		EACH	EACH	EACH	
3700+71.3	139.7' LT	1			UNIVERSITY OF ILLINOIS CHICAGO COMMUNICATIONS MANHOLE, PR RIM=593.85
3700+76.0	128.4' LT	1			UNIVERSITY OF ILLINOIS CHICAGO ELECTRIC POWER HANDHOLE, PR RIM=593.80
3702+37.1	11.8' LT		1		CITY OF CHICAGO ELECTRICAL MANHOLE OR HANDHOLE
3705+31.9	14.6' LT		1		CITY OF CHICAGO ELECTRICAL MANHOLE OR HANDHOLE
3706+30.0	22.7' RT			1	CITY OF CHICAGO WATER MAIN STRUCTURE
3706+30.1	12.3' LT		1		CITY OF CHICAGO ELECTRICAL MANHOLE OR HANDHOLE
3707+01.0	30.1' LT	1			CITY OF CHICAGO ELECTRICAL MANHOLE OR HANDHOLE, PR RIM=593.25
TOTALS		3	3	1	

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PLOT DATE = 12/19/2013	DATE - 10/30/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DRAINAGE SCHEDULES

SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	28
CONTRACT NO. 60W29			ILLINOIS FED. AID PROJECT	



PROPOSED DRAINAGE STRUCTURE SCHEDULE																	
STRUCTURE	LOCATION	OFFSET	EDGE OF PAVEMENT (EOP), EDGE OF SHOULDER (EOS), OR CENTER OF STRUCTURE (COS)	RIM (ELEV)	N INVERT (ELEV)	W INVERT (ELEV)	E INVERT (ELEV)	S INVERT (ELEV)	NE INVERT (ELEV)	NW INVERT (ELEV)	SE INVERT (ELEV)	SW INVERT (ELEV)	CATCH BASINS, TYPE A, 4-DIAMETER, TYPE 20 FRAME AND GRATE	CATCH BASINS, TYPE C, TYPE 20 FRAME AND GRATE	INLETS, TYPE A, TYPE 1 FRAME, OPEN LID (CITY OF CHICAGO)	CATCH BASINS, TYPE A, 4-DIAMETER, TYPE 1 FRAME, OPEN LID (CITY OF CHICAGO)	
																	EACH
PEORIA STREET																	
S-01C (5)	3701+21.39	108.48' LT	COS	592.42	MATCH (EX)			MATCH (EX)									1
S-01D (5)	3701+45.19	108.54' LT	COS	593.03	MATCH (EX)			MATCH (EX)									1
S-02	3702+48.07	56.16' RT	COS	593.74			MATCH (EX)	MATCH (EX)									1
S-03	3706+12.90	12.11' LT	EOP	594.71			589.21										1
S-04	3706+32.90	12.00' LT	EOP	593.98			588.48										1
S-06	3706+45.93	1.97' RT	EOP	593.66			590.41								1		
S-07	3706+49.03	21.40' RT	COS	593.19		588.69				587.69							1
S-09	3706+85.99	17.50' RT	EOP	592.86		587.36											1
WB I-290																	
S-32 (4)	345+37.80	35.40' RT	EOP	577.13	574.25 (3)			574.25 (EX)						1			
EB I-290																	
S-30	343+73.98	44.59' RT	EOS (1)	577.38	574.13 (EX)								1				
S-31	345+42.00	44.60' RT	EOS (1)	576.66				574.00		572.50 (3)			1				
													TOTAL	2	1	1	7

NOTES:

- STATION, OFFSET, AND ELEVATION FOR STRUCTURES LOCATED ALONG THE CONCRETE BARRIER WALL (SPECIAL) ARE GIVEN AT THE TOE OF THE WALL.
- ELEVATIONS NOTED AS EXISTING (EX) ARE CALCULATED FROM RECORD INFORMATION. CONTRACTOR SHALL CONFIRM IN THE FIELD, PRIOR TO FABRICATING/ORDERING ANY STRUCTURES THAT MAY BE IMPACTED.
- PROPOSED ELEVATIONS DETERMINED USING RECORD INFORMATION. CONTRACTOR SHALL CONFIRM IN THE FIELD THAT PROPOSED ELEVATIONS ARE NOT BELOW EXISTING INVERTS OF OUTLET PIPES AT THE PROPOSED STRUCTURE OR EXISTING DOWNSTREAM STRUCTURE, PRIOR TO FABRICATING STRUCTURES.
- THE SUMP OF THE PROPOSED STRUCTURE SHALL BE ADJUSTED TO MEET THE MINIMUM STRUCTURE HEIGHT SHOWN IN STANDARD 602011.
- PROPOSED DRAINAGE STRUCTURES S-01C AND S-01D SHALL BE FABRICATED IN ACCORDANCE WITH CITY OF CHICAGO DEPARTMENT OF WATER MANAGEMENT STANDARD DETAIL A.13.

PROPOSED DRAINAGE PIPE SCHEDULE										
PIPE	UPSTREAM STRUCTURE	DOWNSTREAM STRUCTURE	UPSTREAM INVERT ELEVATION	DOWNSTREAM INVERT ELEVATION	STORM SEWERS, CLASS A, TYPE 2		COMBINED SEWER (EXTRA STRENGTH VTRIFIED CLAY PIPE)		COMBINED SEWER, (WATER MAIN REQUIREMENTS)	TRENCH BACKFILL
					10"	12"	8"	12"		
					FOOT	FOOT	(CDOT) FOOT	(CDOT) FOOT		
PEORIA STREET										
P-01C	PR END CONNECTION TO EX PIPE	S-02	MATCH (EX)	MATCH (EX)			45			0
P-01D	S-02	PR END CONNECTION TO EX PIPE	MATCH (EX)	MATCH (EX)				4		0
P-02	S-03	PR END CONNECTION TO EX COMBINED SEWER	589.21	MATCH (EX CROWN)					15	12
P-04	S-04	PR END CONNECTION TO EX COMBINED SEWER	588.48	MATCH (EX CROWN)					15	11
P-05	S-06	S-07	590.41	588.69				20		3
P-06	S-07	PR END CONNECTION TO EX COMBINED SEWER	587.69	MATCH (EX CROWN)				25		17
P-09	S-09	PR END CONNECTION TO EX COMBINED SEWER	587.36	MATCH (EX CROWN)				20		14
SIPHON	SEE SIPHON RECONSTRUCTION SHEETS FOR DETAILS									572
WB I-290										
8" DRAIN PIPE	BRIDGE SCUPPER (PIER 2)	EX STRUCTURE STA 345+27.4, 27.6' RT	(SEE BRIDGE PLANS)	574.00						4
P-32	S-32	PR CONNECTION TO EX STORM SEWER	574.25 (EX)	EX PIPE	6					1
EB I-290										
P-31	S-31	EX STRUCTURE STA 345+25.50, 37.30' RT	572.50	572.35 (EX)		18				5
ROUNDED TOTAL					6	18	110	4	30	639

NOTES:

- ELEVATIONS NOTED AS EXISTING (EX) ARE CALCULATED FROM RECORD INFORMATION. CONTRACTOR SHALL CONFIRM IN THE FIELD, PRIOR TO FABRICATING/ORDERING ANY STRUCTURES THAT MAY BE IMPACTED.
- ADDITIONAL TRENCH BACKFILL QUANTITY PROVIDED ON THE EXISTING PIPE SCHEDULE.

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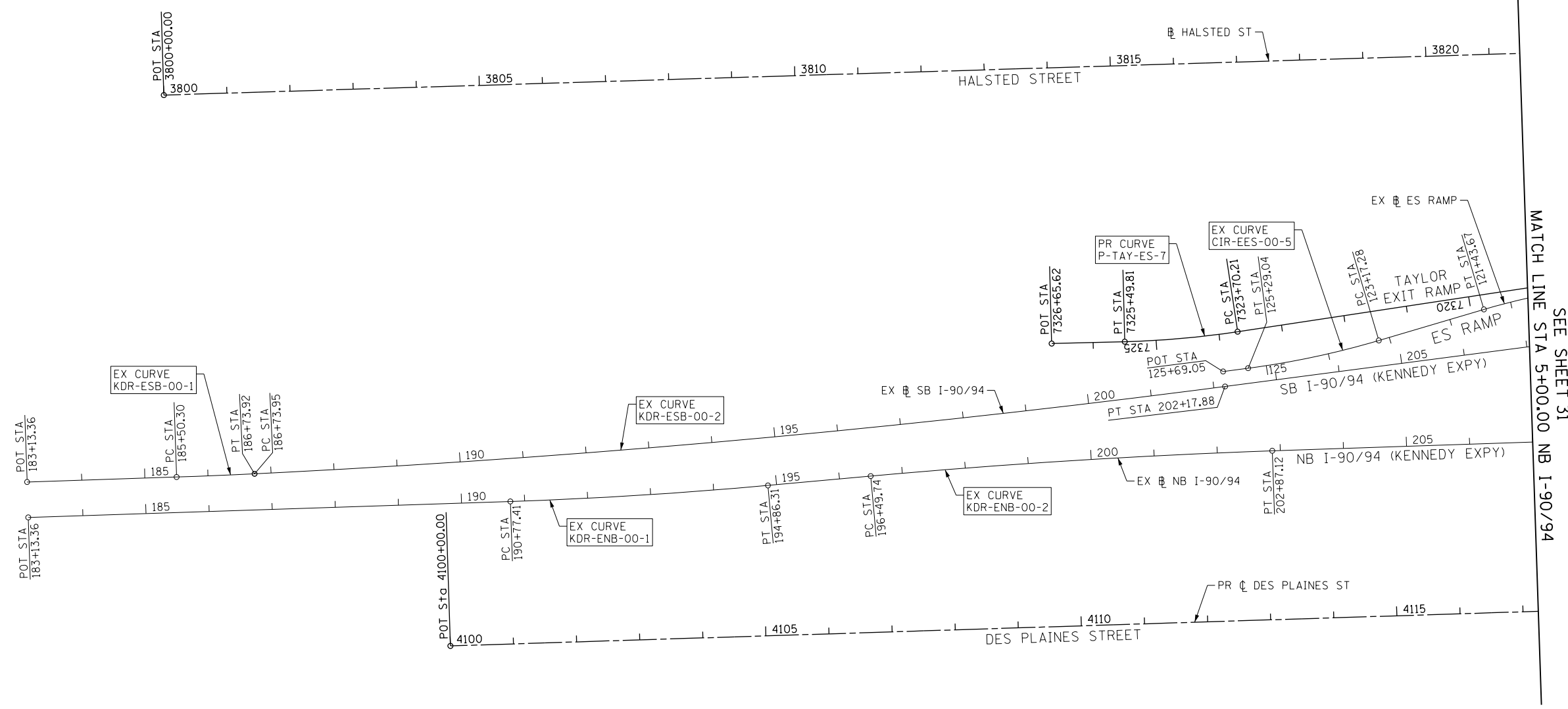
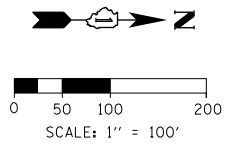
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DRAINAGE SCHEDULES

SCALE: NONE SHEET 2 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	29
CONTRACT NO. 60W29			ILLINOIS FED. AID PROJECT	



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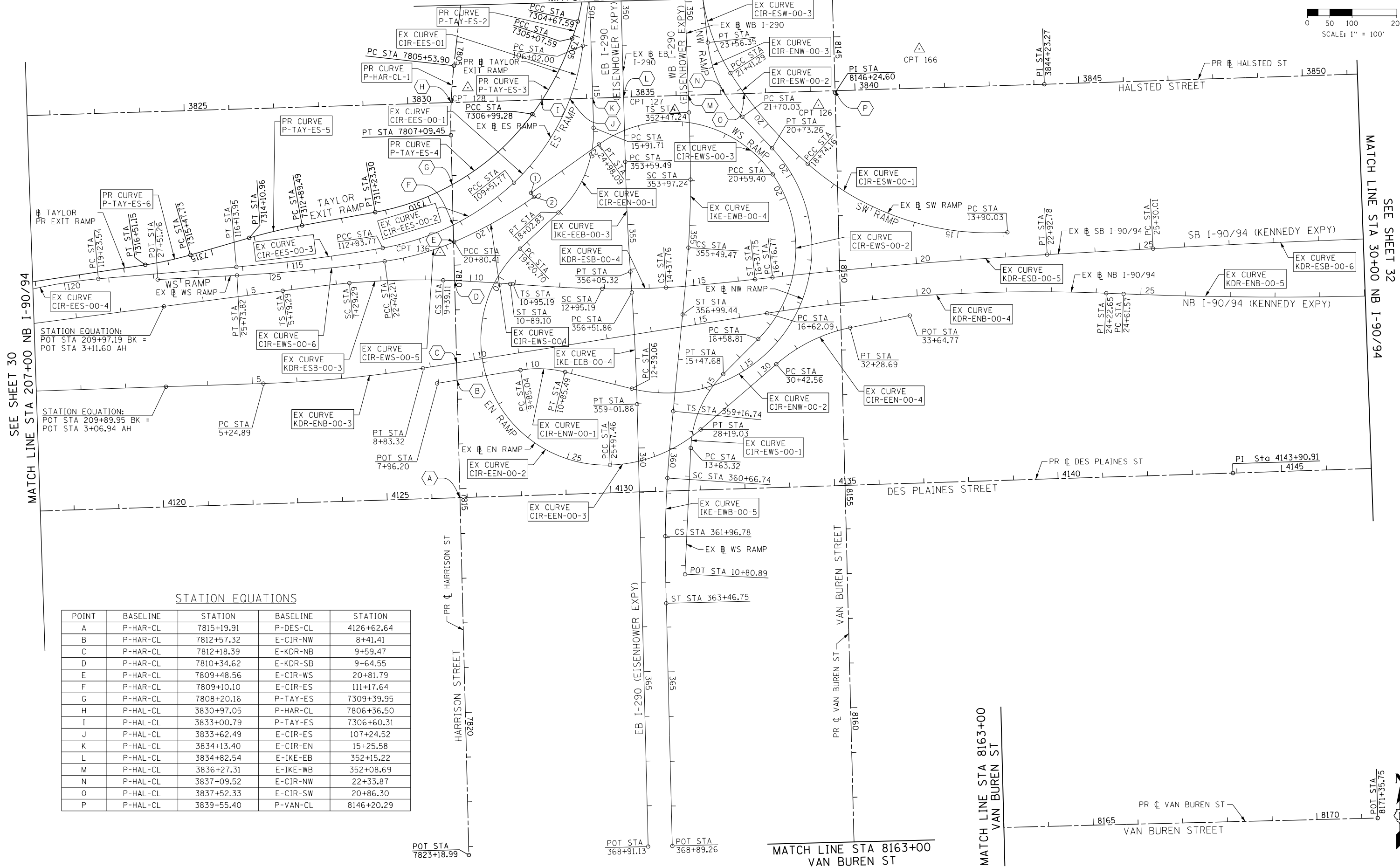
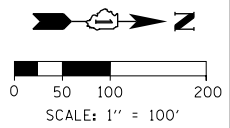
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ALIGNMENT, TIES AND BENCHMARKS			
SCALE: 1" = 100'	SHEET 1	OF 9 SHEETS	STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	30
CONTRACT NO. 60W29				
ILLINOIS FED. AID PROJECT				

SEE SHEET 33
MATCH LINE STA 350+00 EB I-290



STATION EQUATIONS

POINT	BASELINE	STATION	BASELINE	STATION
A	P-HAR-CL	7815+19.91	P-DES-CL	4126+62.64
B	P-HAR-CL	7812+57.32	E-CIR-NW	8+41.41
C	P-HAR-CL	7812+18.39	E-KDR-NB	9+59.47
D	P-HAR-CL	7810+34.62	E-KDR-SB	9+64.55
E	P-HAR-CL	7809+48.56	E-CIR-WS	20+81.79
F	P-HAR-CL	7809+10.10	E-CIR-ES	111+17.64
G	P-HAR-CL	7808+20.16	P-TAY-ES	7309+39.95
H	P-HAL-CL	3830+97.05	P-HAR-CL	7806+36.50
I	P-HAL-CL	3833+00.79	P-TAY-ES	7306+60.31
J	P-HAL-CL	3833+62.49	E-CIR-ES	107+24.52
K	P-HAL-CL	3834+13.40	E-CIR-EN	15+25.58
L	P-HAL-CL	3834+82.54	E-IKE-EB	352+15.22
M	P-HAL-CL	3836+27.31	E-IKE-WB	352+08.69
N	P-HAL-CL	3837+09.52	E-CIR-NW	22+33.87
O	P-HAL-CL	3837+52.33	E-CIR-SW	20+86.30
P	P-HAL-CL	3839+55.40	P-VAN-CL	8146+20.29

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SEE SHEET 30
MATCH LINE STA 207+00 NB I-90/94

SEE SHEET 32
MATCH LINE STA 30+00 NB I-90/94

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

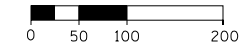
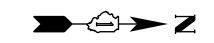
ALIGNMENT, TIES AND BENCHMARKS

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-01R	COOK	356	31
CONTRACT NO. 60W29			ILLINOIS FED. AID PROJECT	

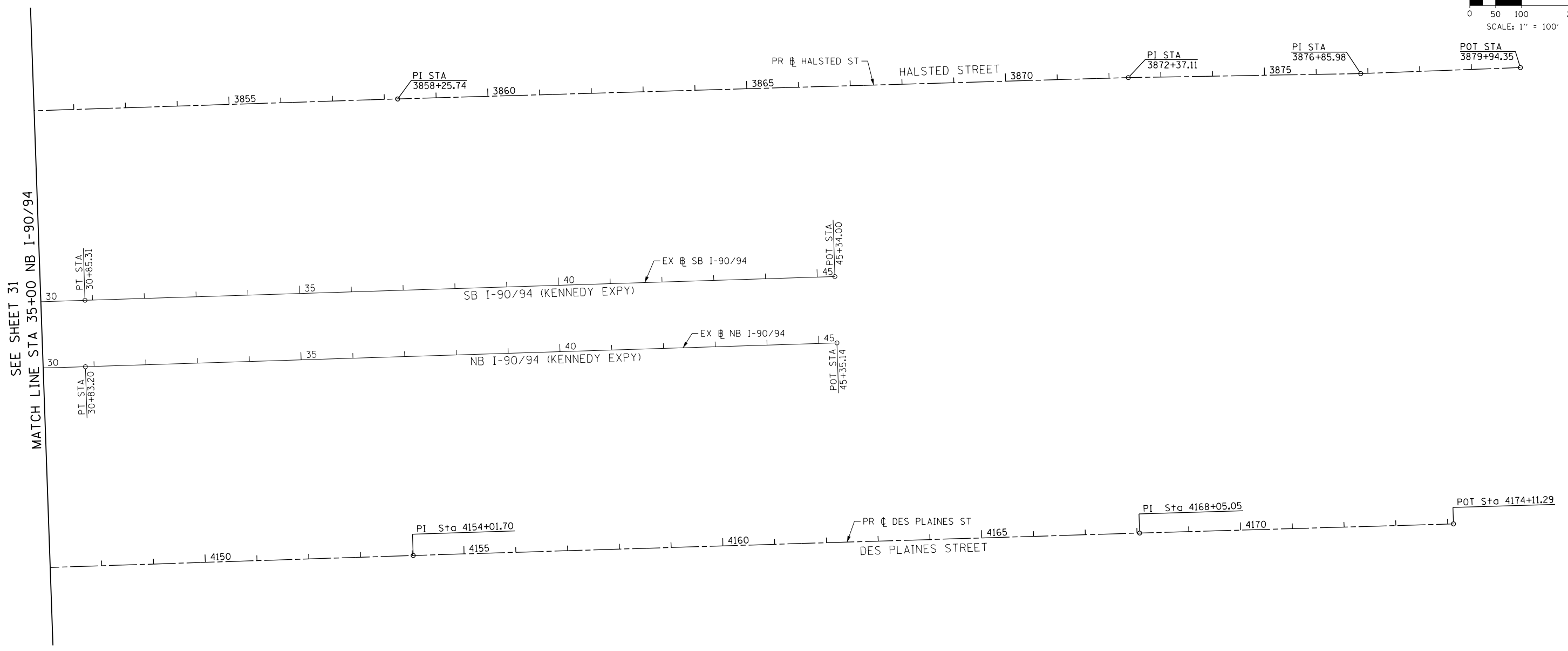


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SCALE: 1" = 100' SHEET 2 OF 9 SHEETS STA. TO STA.



SCALE: 1" = 100'



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SEE SHEET 31
MATCH LINE STA 35+00 NB I-90/94



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PLOT DATE = 10/28/2013

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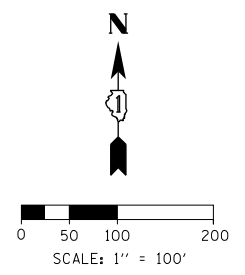
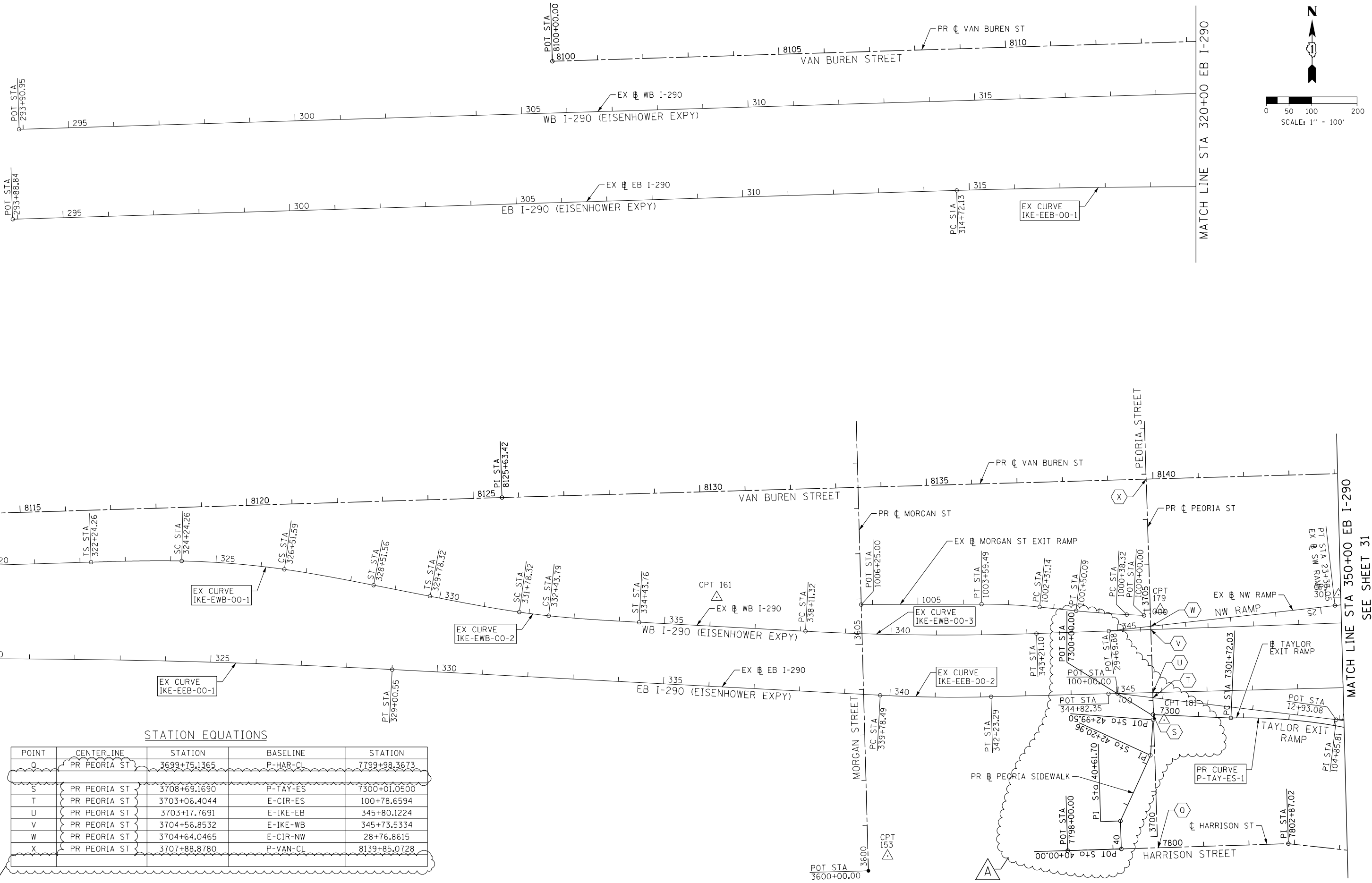
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ALIGNMENT, TIES AND BENCHMARKS

SCALE: 1" = 100'	SHEET 3	OF 9	SHEETS	STA.	TO STA.
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-01R	COOK	356	32
CONTRACT NO. 60W29			ILLINOIS FED. AID PROJECT	

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STATION EQUATIONS

POINT	CENTERLINE	STATION	BASELINE	STATION
Q	PR PEORIA ST	3699+75.1365	P-HAR-CL	7799+98.3673
S	PR PEORIA ST	3708+69.1690	P-TAY-ES	7300+01.0500
T	PR PEORIA ST	3703+06.4044	E-CIR-ES	100+78.6594
U	PR PEORIA ST	3703+17.7691	E-IKE-EB	345+80.1224
V	PR PEORIA ST	3704+56.8532	E-IKE-WB	345+73.5334
W	PR PEORIA ST	3704+64.0465	E-CIR-NW	28+76.8615
X	PR PEORIA ST	3707+88.8780	P-VAN-CL	8139+85.0728



DI60W29-Sht-4B-04.dgn	DESIGNED - JLV	REVISED - A 12/18/2013
USER NAME = BAWtor t	DRAWN - BAW	REVISED -
PLOT SCALE = 200.0000' / in.	CHECKED - JMG	REVISED -
PLOT DATE = 12/19/2013	DATE - 10/30/2013	REVISED -

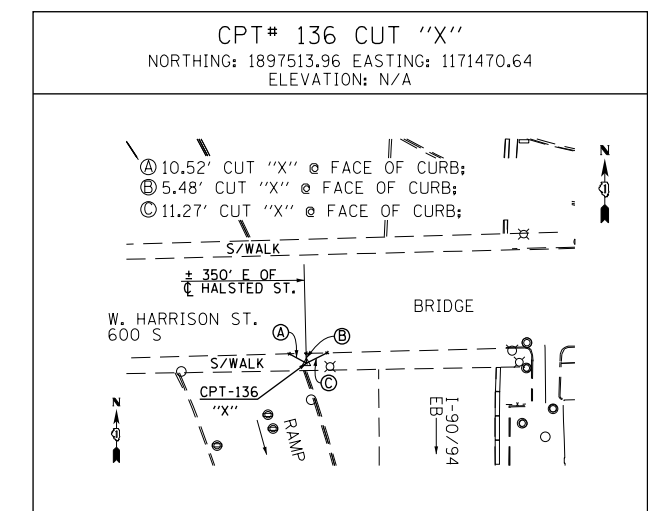
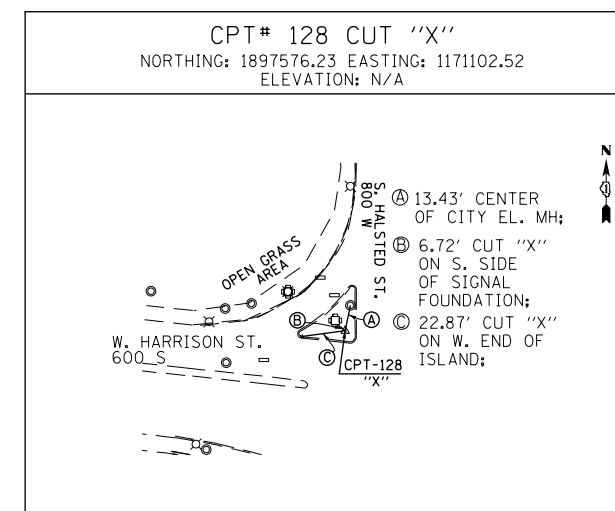
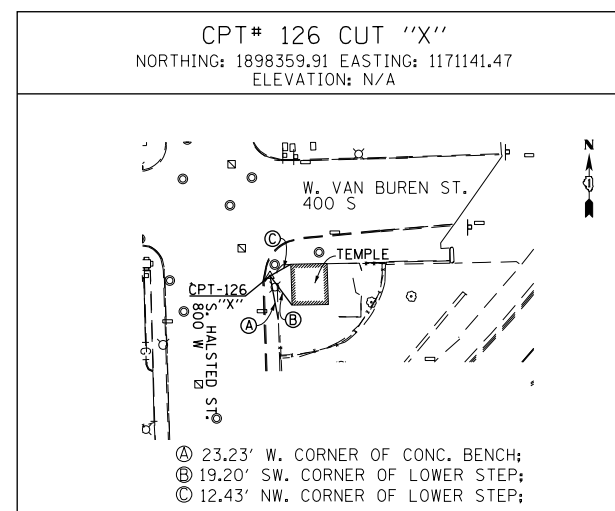
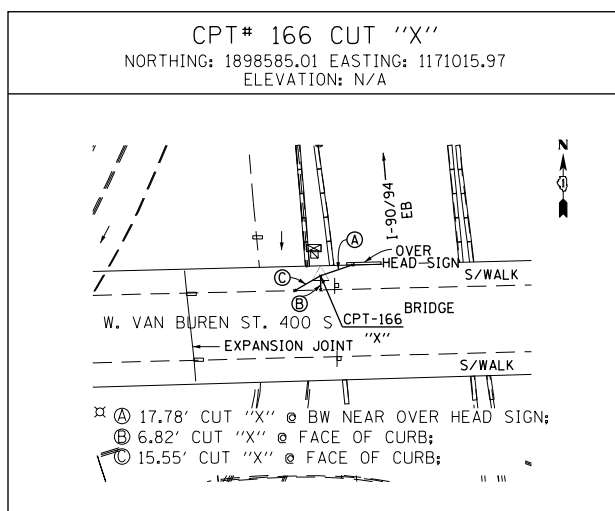
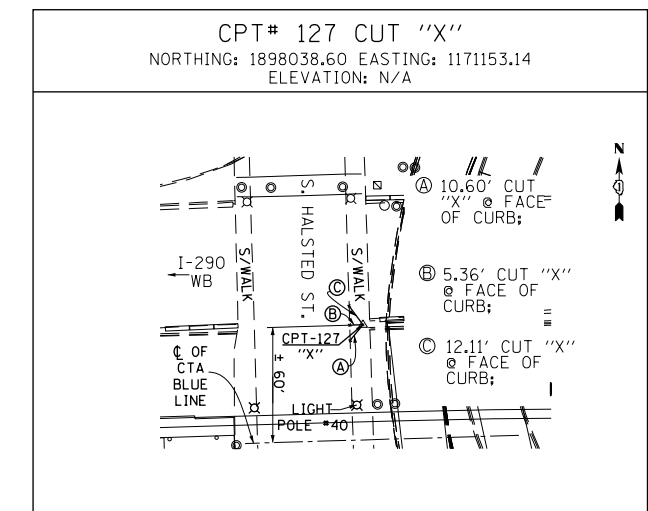
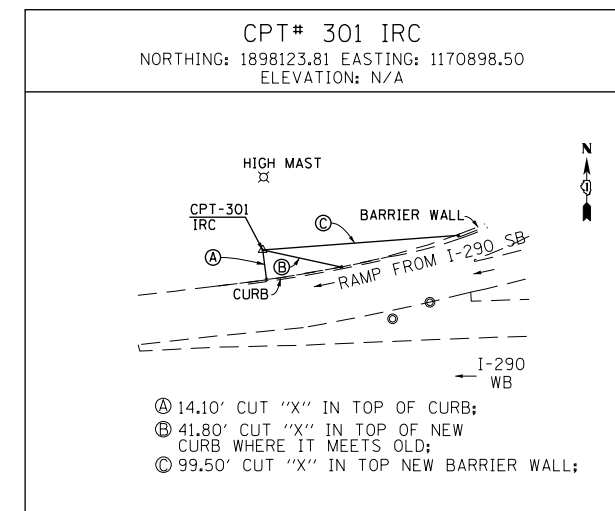
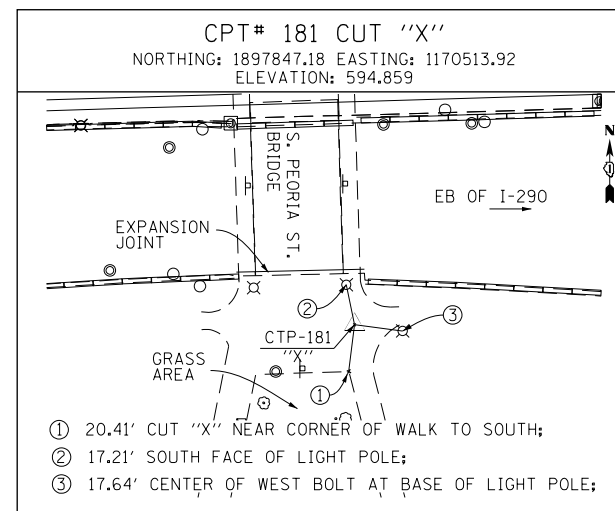
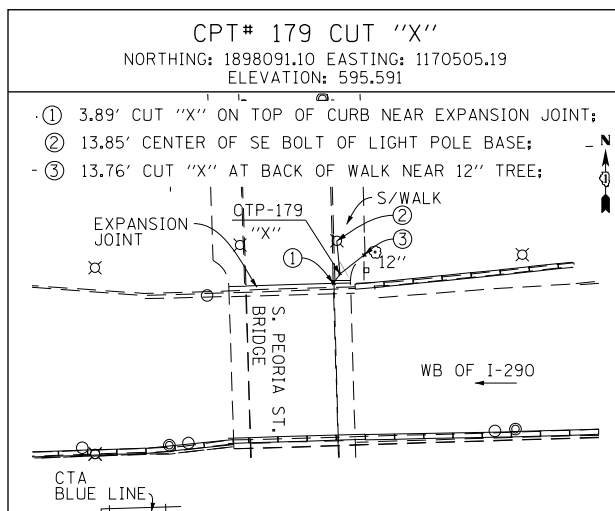
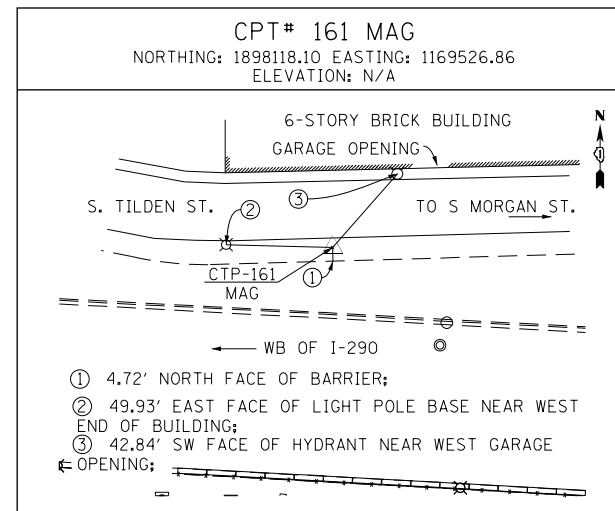
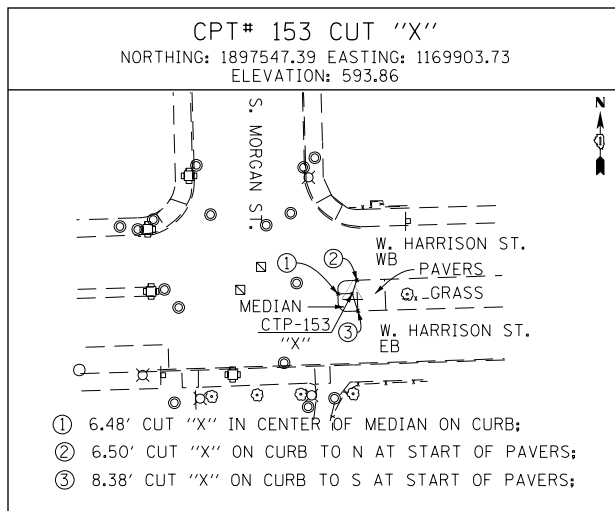
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ALIGNMENT, TIES AND BENCHMARKS	
SCALE: 1" = 100'	SHEET 4 OF 9 SHEETS STA. TO STA.

F.A.I. R.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	33
CONTRACT NO. 60W29			ILLINOIS FED. AID PROJECT	

SEE SHEET 31

BENCHMARKS		
MONUMENT	ELEVATION	DESCRIPTION
BM 1100	582.6782	SET "X" @ CL OF EAST PIER @ CL OF VAN BUREN ST.
BM 1140	579.3332	SET "X" ON WESTERLY JAYWALL OF I-90 @ CL OF VAN BUREN ST.
BM 1141	575.1320	SET "X" ON WESTERLY JAYWALL OF I-90 @ CL OF OUTBOUND I-290 OVERPASS.
BM 1142	575.9003	SET "X" ON WESTERLY JAYWALL OF I-90 @ CL OF INBOUND I-290 OVERPASS.
BM 1143	580.1100	SET "X" ON WESTERLY JAYWALL OF I-90 ±35' SOUTH OF HARRISON ST. OVERPASS.
BM 1144	585.7601	CHISEL "X" ON OVERHEAD SIGN BOLT. SIGN #D1. W. SIDE OF HARRISON. (CENTER OF 3 EAST BOLTS)
BM 1145	586.1391	CHISEL "X" ON WESTERLY BOLT OF H.M.L.P. CL OF I-90 ± 200' S. OF PUMP/LIFT STATION.
BM 1146	582.9574	SET "X" ON WESTERLY JAYWALL OF I-90 WESTBOUND ± OPPOSITE SIGN #D4
BM 1147	579.4454	SET "X" ON WESTERLY JAYWALL OF ACCIDENT INVESTIGATION SITE ±35' N. OF E. I-90/94 SIGN.
BM 1345	594.1988	"+" CUT WSW FLANGE BOLT FH W. SIDE DES PLAINES ST. ±200' S. OF S HARRISON ST.



FILE PATH = p:\388035-par\1\escom\1\local\1\160W29-sht-ATB-05.dgn



D160W29-sht-ATB-05.dgn
USER NAME = BAW1tor1
PLOT SCALE = 100.0000' / in.
PLOT DATE = 10/28/2013

DESIGNED - JLV
DRAWN - BAW
CHECKED - JMG
DATE - 10/30/2013

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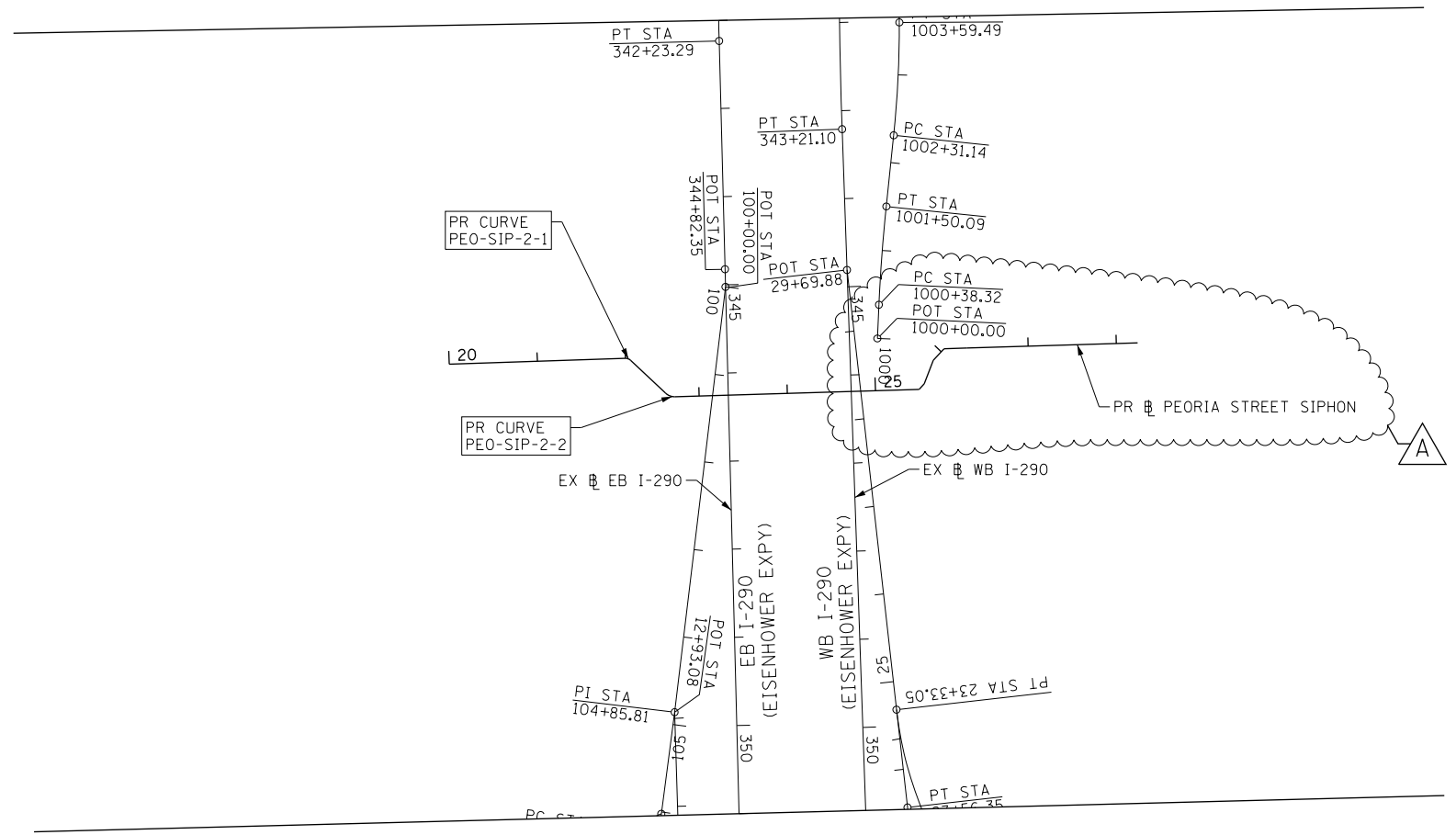
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ALIGNMENT, TIES AND BENCHMARKS

SCALE: NONE SHEET 5 OF 9 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	34
CONTRACT NO. 60W29				
ILLINOIS FED. AID PROJECT				

FILE PATH = p:\388035-p\m\1\escomon\me\local\PEO-SIP-2\Documents\01_Americas\Transportation\6269938_Circle\Phase\1\000_CAD\006_Roadway\Sheets\60W29_Sht-ATB-06.dgn



PEO-SIP-2				
POINT	DESCRIPTION	STATION	NORTHING	EASTING
POT		20+00.00	1,897,592.5361	1,170,498.6879
PC	PEO-SIP-2-1	22+00.13	1,897,792.5550	1,170,492.1566
PI	PEO-SIP-2-1	22+03.45	1,897,795.8806	1,170,492.0480
PT	PEO-SIP-2-1	22+06.43	1,897,798.3023	1,170,494.3298
PC	PEO-SIP-2-2	22+60.78	1,897,837.8544	1,170,531.5977
PI	PEO-SIP-2-2	22+66.99	1,897,842.3790	1,170,535.8609
PT	PEO-SIP-2-2	22+72.56	1,897,848.5928	1,170,535.6737
PI		25+49.46	1,898,125.3678	1,170,527.5345
PI		25+58.29	1,898,131.4107	1,170,520.9047
PI		25+86.65	1,898,141.4457	1,170,494.3806
PI		26+04.51	1,898,153.6828	1,170,481.3601
PI		26+30.31	1,898,179.4937	1,170,480.5615
PI		28+24.28	1,898,373.3447	1,170,474.6693

PROP. CURVE PEO-SIP-2-1
 PI STA. = 22+03.45
 $\Delta = 45^\circ 10' 01''$ (RT)
 D = 716' 11' 50"
 R = 8.00'
 T = 3.33'
 L = 6.31'
 E = 0.66'
 e = ----
 T.R. = ----
 P.C. STA = 22+00.13
 P.T. STA = 22+06.43

PROP. CURVE PEO-SIP-2-2
 PI STA. = 22+66.99
 $\Delta = 45^\circ 01' 21''$ (LT)
 D = 381' 58' 19"
 R = 15.00'
 T = 6.22'
 L = 11.79'
 E = 1.24'
 e = ----
 T.R. = ----
 P.C. STA = 22+60.78
 P.T. STA = 22+72.56



DI60W29-Sht-ATB-06.dgn	DESIGNED - JLV	REVISED - A 12/18/2013
USER NAME = BAW:tor t	DRAWN - BAW	REVISED -
PLOT SCALE = 200.0000' / in.	CHECKED - JMG	REVISED -
PLOT DATE = 12/19/2013	DATE - 10/30/2013	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ALIGNMENT, TIES AND BENCHMARKS

SCALE: 1" = 100' SHEET 6 OF 9 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	35
CONTRACT NO. 60W29				
ILLINOIS FED. AID PROJECT				

EXISTING CURVE DATA

EXIST. CURVE IKE-EWB-00-1 PI STA. = 325+38.06 $\Delta = 6^\circ 49' 10''$ (RT) D = 3' 00' 00" R = 1,909.89' T = 113.80' L = 227.32' E = 3.39' P.C. STA. = 324+24.26 P.T. STA. = 326+51.59	PI Sta. 323+57.62 $\Delta s = 2^\circ 59' 59.83''$ $Ds = 2^\circ 59' 59.83''$ T1 = 133.35' T2 = 66.68' Rs = 1,909.89' Ls = 200.00' 322+24.26 324+24.26	PI Sta. 327+18.26 $\Delta s = 2^\circ 59' 58.21''$ $Ds = 2^\circ 59' 59.83''$ T1 = 133.33' T2 = 66.67' Rs = 1,909.89' Ls = 199.97' 326+51.59 328+51.56	EXIST. CURVE IKE-EWB-00-2 PI STA. = 332+11.06 $\Delta = 1^\circ 57' 51''$ (LT) D = 3' 00' 00" R = 1,909.89' T = 32.74' L = 65.47' E = 0.28' P.C. STA. = 331+78.32 P.T. STA. = 332+43.79	PI Sta. 331+11.67 $\Delta s = 2^\circ 59' 59.83''$ $Ds = 2^\circ 59' 59.83''$ T1 = 133.35' T2 = 66.68' Rs = 1,909.89' Ls = 200.00' 329+78.32 331+78.32	PI Sta. 333+10.47 $\Delta s = 2^\circ 59' 58.21''$ $Ds = 2^\circ 59' 59.83''$ T1 = 133.33' T2 = 66.67' Rs = 1,909.89' Ls = 199.97' 332+43.79 334+43.76	EXIST. CURVE IKE-EWB-00-3 PI STA. = 340+66.38 $\Delta = 5^\circ 05' 52''$ (LT) D = 1' 00' 00" R = 5,729.58' T = 255.06' L = 509.78' E = 5.67' P.C. STA. = 338+11.32 P.T. STA. = 343+21.10	PI Sta. 333+10.47 $\Delta s = 2^\circ 59' 58.21''$ $Ds = 2^\circ 59' 59.83''$ T1 = 133.33' T2 = 66.67' Rs = 1,909.89' Ls = 200.00' 332+43.79 334+43.76	EXIST. CURVE IKE-EWB-00-4 PI STA. = 354+73.38 $\Delta = 3^\circ 48' 20''$ (RT) D = 2' 29' 59" R = 2,292.01' T = 76.14' L = 152.23' E = 1.26' P.C. STA. = 353+97.24 P.T. STA. = 355+49.47	PI Sta. 353+47.25 $\Delta s = 1^\circ 52' 29.47''$ $Ds = 2^\circ 29' 59.30''$ T1 = 100.01' T2 = 50.01' Rs = 2,292.01' Ls = 150.00' 352+47.24 353+97.24	PI Sta. 355+99.47 $\Delta s = 1^\circ 52' 28.12''$ $Ds = 2^\circ 29' 59.30''$ T1 = 99.99' T2 = 50.00' Rs = 2,292.01' Ls = 149.97' 355+49.47 356+99.44	EXIST. CURVE IKE-EWB-00-5 PI STA. = 361+31.77 $\Delta = 3^\circ 15' 02''$ (LT) D = 2' 29' 59" R = 2,292.01' T = 65.04' L = 130.04' E = 0.92' P.C. STA. = 360+66.74 P.T. STA. = 361+96.78	PI Sta. 360+16.74 $\Delta s = 1^\circ 52' 29.47''$ $Ds = 2^\circ 29' 59.30''$ T1 = 100.01' T2 = 50.01' Rs = 2,292.01' Ls = 150.00' 359+16.74 360+66.74	PI Sta. 362+46.77 $\Delta s = 1^\circ 52' 28.12''$ $Ds = 2^\circ 29' 59.30''$ T1 = 99.99' T2 = 50.00' Rs = 2,292.01' Ls = 149.97' 361+96.78 363+46.75
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EXIST. CURVE IKE-EEB-00-1 PI STA. = 321+86.75 $\Delta = 4^\circ 45' 41''$ (RT) D = 0' 20' 00" R = 17,188.74' T = 714.62' L = 1,428.42' E = 14.85' P.C. STA. = 314+72.13 P.T. STA. = 329+00.55	EXIST. CURVE IKE-EEB-00-2 PI STA. = 341+00.95 $\Delta = 4^\circ 29' 17''$ (LT) D = 1' 50' 00" R = 3,125.22' T = 122.46' L = 244.80' E = 2.40' P.C. STA. = 339+78.49 P.T. STA. = 342+23.29	EXIST. CURVE IKE-EEB-00-3 PI STA. = 354+82.42 $\Delta = 2^\circ 27' 30''$ (LT) D = 1' 00' 00" R = 5,729.65' T = 122.93' L = 245.82' E = 1.32' P.C. STA. = 353+59.49 P.T. STA. = 356+05.32	EXIST. CURVE IKE-EEB-00-4 PI STA. = 357+76.88 $\Delta = 2^\circ 30' 00''$ (RT) D = 1' 00' 00" R = 5,729.65' T = 125.02' L = 250.00' E = 1.36' P.C. STA. = 356+51.86 P.T. STA. = 359+01.86	EXIST. CURVE KDR-ENB-00-1 PI STA. = 192+81.92 $\Delta = 3^\circ 17' 58''$ (LT) D = 0' 48' 25" R = 7,100.63' T = 204.51' L = 408.90' E = 2.94' P.C. STA. = 190+77.41 P.T. STA. = 194+86.31	EXIST. CURVE KDR-ENB-00-2 PI STA. = 199+68.51 $\Delta = 3^\circ 11' 12''$ (RT) D = 0' 30' 00" R = 11,460.32' T = 318.77' L = 637.38' E = 4.43' P.C. STA. = 196+49.74 P.T. STA. = 202+87.12	EXIST. CURVE KDR-ENB-00-3 PI STA. = 7+04.34 $\Delta = 7^\circ 10' 07''$ (LT) D = 2' 00' 00" R = 2,864.80' T = 179.45' L = 358.43' E = 5.61' P.C. STA. = 5+24.89 P.T. STA. = 8+83.32	EXIST. CURVE KDR-ENB-00-4 PI STA. = 20+43.63 $\Delta = 11^\circ 24' 28''$ (RT) D = 1' 30' 00" R = 3,819.89' T = 381.54' L = 760.55' E = 19.01' P.C. STA. = 16+62.09 P.T. STA. = 24+22.65	EXIST. CURVE KDR-ENB-00-5 PI STA. = 27+72.52 $\Delta = 4^\circ 08' 39''$ (LT) D = 0' 40' 00" R = 8,594.42' T = 310.95' L = 621.63' E = 5.62' P.C. STA. = 24+61.57 P.T. STA. = 30+83.20
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EXIST. CURVE KDR-ESB-00-1 PI STA. = 186+12.11 $\Delta = 0^\circ 55' 38''$ (LT) D = 0' 45' 00" R = 7,640.04' T = 61.81' L = 123.62' E = 0.25' P.C. STA. = 185+50.30 P.T. STA. = 186+73.92	EXIST. CURVE KDR-ESB-00-2 PI STA. = 194+46.34 $\Delta = 4^\circ 37' 53''$ (LT) D = 0' 18' 00" R = 19,100.00' T = 772.39' L = 1,543.93' E = 15.61' P.C. STA. = 186+73.95 P.T. STA. = 202+17.88	PI Sta. 6+79.30 $\Delta s = 2^\circ 23' 14.33''$ $Ds = 3^\circ 10' 59.11''$ T1 = 100.01' T2 = 50.01' Rs = 1,800.01' Ls = 150.00' 5+79.29 7+29.29	PI Sta. 9+89.11 $\Delta s = 2^\circ 23' 13.76''$ $Ds = 3^\circ 10' 59.11''$ T1 = 100.00' T2 = 50.00' Rs = 1,800.01' Ls = 149.99' 9+39.11 10+89.10	EXIST. CURVE KDR-ESB-00-3 PI STA. = 8+34.32 $\Delta = 6^\circ 40' 44''$ (RT) D = 3' 10' 59" R = 1,910.09' T = 105.03' L = 209.82' E = 3.06' P.C. STA. = 7+29.29 P.T. STA. = 9+39.11	EXIST. CURVE KDR-ESB-00-4 PI STA. = 13+66.51 $\Delta = 4^\circ 16' 36''$ (LT) D = 2' 59' 59" R = 1,910.09' T = 71.32' L = 142.57' E = 1.33' P.C. STA. = 12+95.19 P.T. STA. = 14+37.76	PI Sta. 12+28.55 $\Delta s = 2^\circ 59' 58.71''$ $Ds = 2^\circ 59' 58.71''$ T1 = 133.35' T2 = 66.68' Rs = 1,910.09' Ls = 200.00' 10+95.19 12+95.19	PI Sta. 15+04.45 $\Delta s = 2^\circ 59' 58.17''$ $Ds = 2^\circ 59' 58.71''$ T1 = 133.35' T2 = 66.68' Rs = 1,910.09' Ls = 199.99' 14+37.76 16+37.75	EXIST. CURVE KDR-ESB-00-5 PI STA. = 19+84.85 $\Delta = 3^\circ 04' 48''$ (RT) D = 0' 30' 00" R = 11,459.27' T = 308.08' L = 616.01' E = 4.14' P.C. STA. = 16+76.77 P.T. STA. = 22+92.78	EXIST. CURVE KDR-ESB-00-6 PI STA. = 28+07.67 $\Delta = 1^\circ 23' 18''$ (RT) D = 0' 15' 00" R = 22,918.30' T = 277.67' L = 555.31' E = 1.68' P.C. STA. = 25+30.01 P.T. STA. = 30+85.31
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EXIST. CURVE CIR-ENW-00-1 PI STA. = 10+35.96 $\Delta = 23^\circ 06' 09''$ (RT) D = 23' 00' 00" R = 249.11' T = 50.91' L = 100.45' E = 5.15' P.C. STA. = 9+85.04 P.T. STA. = 10+85.49	EXIST. CURVE CIR-ENW-00-2 PI STA. = 23+51.29 $\Delta = 147^\circ 31' 02''$ (LT) D = 17' 41' 01" R = 324.00' T = 1,112.23' L = 834.20' E = 834.46' P.C. STA. = 12+39.06 P.T. STA. = 20+73.26	EXIST. CURVE CIR-ENW-00-3 PI STA. = 22+66.58 $\Delta = 37^\circ 04' 01''$ (RT) D = 19' 53' 40" R = 288.00' T = 96.55' L = 186.32' E = 15.75' P.C. STA. = 21+70.03 P.T. STA. = 23+56.35	EXIST. CURVE CIR-EWS-00-1 PI STA. = 14+59.92 $\Delta = 42^\circ 15' 04''$ (RT) D = 22' 55' 04" R = 250.00' T = 96.60' L = 184.36' E = 18.01' P.C. STA. = 13+63.32 P.T. STA. = 15+47.68	EXIST. CURVE CIR-EWS-00-2 PI STA. = 18+99.17 $\Delta = 79^\circ 41' 42''$ (LT) D = 19' 53' 40" R = 288.00' T = 240.36' L = 400.59' E = 87.12' P.C. STA. = 16+58.81 P.T. STA. = 20+59.40	EXIST. CURVE CIR-EWS-00-3 PI STA. = 23+40.00 $\Delta = 90^\circ 44' 22''$ (LT) D = 20' 41' 04" R = 277.00' T = 280.60' L = 438.69' E = 117.29' P.C. STA. = 20+59.40 P.T. STA. = 24+98.09	EXIST. CURVE CIR-EWS-00-4 PI STA. = 19+78.44 $\Delta = 12^\circ 22' 32''$ (RT) D = 6' 02' 40" R = 947.89' T = 102.77' L = 204.74' E = 5.55' P.C. STA. = 18+75.67 P.T. STA. = 20+80.41	EXIST. CURVE CIR-EWS-00-5 PI STA. = 21+61.79 $\Delta = 15^\circ 14' 17''$ (RT) D = 9' 25' 03" R = 608.40' T = 81.38' L = 161.81' E = 5.42' P.C. STA. = 20+80.41 P.T. STA. = 22+42.21	EXIST. CURVE CIR-EWS-00-6 PI STA. = 24+08.12 $\Delta = 4^\circ 57' 42''$ (RT) D = 1' 29' 46" R = 3,829.31' T = 165.91' L = 331.61' E = 3.59' P.C. STA. = 22+42.21 P.T. STA. = 25+73.82
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EXIST. CURVE CIR-EES-00-1 PI STA. = 107+84.38 $\Delta = 40^\circ 04' 47''$ (RT) D = 11' 27' 32" R = 500.01' T = 182.38' L = 349.77' E = 32.22' P.C. STA. = 106+02.00 P.T. STA. = 109+51.77	EXIST. CURVE CIR-EES-00-2 PI STA. = 111+22.58 $\Delta = 33^\circ 11' 59''$ (RT) D = 9' 59' 59" R = 572.97' T = 170.81' L = 332.00' E = 24.92' P.C. STA. = 109+51.77 P.T. STA. = 112+83.77	EXIST. CURVE CIR-EES-00-3 PI STA. = 114+48.97 $\Delta = 4^\circ 57' 10''$ (RT) D = 1' 30' 00" R = 3,819.73' T = 165.19' L = 330.18' E = 3.57' P.C. STA. = 112+83.77 P.T. STA. = 116+13.95	EXIST. CURVE CIR-EES-00-4 PI STA. = 120+33.98 $\Delta = 11^\circ 33' 10''$ (LT) D = 5' 14' 53" R = 1,091.74' T = 110.44' L = 220.13' E = 5.57' P.C. STA. = 119+23.54 P.T. STA. = 121+43.67	EXIST. CURVE CIR-EES-00-5 PI STA. = 124+23.38 $\Delta = 8^\circ 59' 59''$ (RT) D = 4' 15' 00" R = 1,348.15' T = 106.10' L = 211.76' E = 4.17' P.C. STA. = 123+17.28 P.T. STA. = 125+29.04
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EXIST. CURVE CIR-EEN-00-1 PI STA. = 17+04.03 $\Delta = 48^\circ 23' 08''$ (RT) D = 22' 55' 06" R = 250.00' T = 112.32' L = 211.12' E = 24.07' P.C. STA. = 15+91.71 P.T. STA. = 18+02.83	EXIST. CURVE CIR-EEN-00-2 PI STA. = 26+59.42 $\Delta = 138^\circ 29' 00''$ (LT) D = 20' 27' 46" R = 280.00' T = 738.72' L = 676.76' E = 510.01' P.C. STA. = 19+20.70 P.T. STA. = 25+97.46	EXIST. CURVE CIR-EEN-00-3 PI STA. = 27+12.77 $\Delta = 39^\circ 10' 08''$ (LT) D = 17' 40' 40" R = 324.11' T = 115.31' L = 221.57' E = 19.90' P.C. STA. = 25+97.46 P.T. STA. = 28+19.03	EXIST. CURVE CIR-EEN-00-4 PI STA. = 31+37.75 $\Delta = 29^\circ 37' 21''$ (RT) D = 15' 54' 55" R = 360.01' T = 95.19' L = 186.13' E = 12.37' P.C. STA. = 30+42.56 P.T. STA. = 32+28.69	EXIST. CURVE CIR-ESW-00-1 PI STA. = 16+45.46 $\Delta = 45^\circ 09' 26''$ (RT) D = 9' 19' 39" R = 614.27' T = 255.43' L = 484.13' E = 50.99' P.C. STA. = 13+90.03 P.T. STA. = 18+74.16	EXIST. CURVE CIR-ESW-00-2 PI STA. = 20+08.60 $\Delta = 16^\circ 01' 34''$ (RT) D = 5' 59' 58" R = 955.00' T = 134.44' L = 267.12' E = 9.42' P.C. STA. = 18+74.16 P.T. STA. = 21+41.29	EXIST. CURVE CIR-ESW-00-3 PI STA. = 22+38.85 $\Delta = 26^\circ 01' 11''$ (RT) D = 13' 34' 06" R = 422.28' T = 97.57' L = 191.77' E = 11.12' P.C. STA. = 21+41.29 P.T. STA. = 23+33.05
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EXISTING ALIGNMENTS

EX SB I-90/94 (KENNEDY EXPY)				
POINT	DESCRIPTION	STATION	NORTHING	EASTING
POT		183+13.36	1,894,226.7720	1,171,843.7613
PC	KDR-ESB-00-1	185+50.30	1,894,463.5855	1,171,835.9008
PI	KDR-ESB-00-1	186+12.11	1,894,525.3637	1,171,833.8503
PT	KDR-ESB-00-1	186+73.92	1,894,587.1007	1,171,830.8004
PC	KDR-ESB-00-2	186+73.95	1,894,587.1274	1,171,830.7991
PI	KDR-ESB-00-2	194+46.34	1,895,358.5739	1,171,792.6885
PT	KDR-ESB-00-2	202+17.88	1,896,124.4241	1,171,692.4111
POT	209+97.19 BK, 3+11.60 AH		1,896,897.1317	1,171,591.2358
TS	KDR-ESB-00-3B	5+79.29	1,897,162.5525	1,171,556.4826
PIS	KDR-ESB-00-3B	6+79.30	1,897,261.7152	1,171,543.4986
SC	KDR-ESB-00-3	7+29.29	1,897,311.5276	1,171,539.0772
PI	KDR-ESB-00-3	8+34.32	1,897,416.1453	1,171,529.7912
CS	KDR-ESB-00-3	9+39.11	1,897,521.1330	1,171,532.7356
PIS	KDR-ESB-00-3A	9+89.11	1,897,571.1183	1,171,534.1374
ST	KDR-ESB-00-3A	10+89.10	1,897,670.8779	1,171,541.1020
TS	KDR-ESB-00-4B	10+95.19	1,897,676.9607	1,171,541.5267
PIS	KDR-ESB-00-4B	12+28.55	1,897,809.9894	1,171,550.8139
SC	KDR-ESB-00-4	12+95.19	1,897,876.6634	1,171,551.9707
PI	KDR-ESB-00-4	13+66.51	1,897,947.9712	1,171,553.2078
CS	KDR-ESB-00-4	14+37.76	1,898,019.1728	1,171,549.1239
PIS	KDR-ESB-00-4A	15+04.45	1,898,085.7441	1,171,545.3056
ST	KDR-ESB-00-4A	16+37.75	1,898,218.2892	1,171,530.7143
PC	KDR-ESB-00-5	16+76.77	1,898,257.0735	1,171,526.4446
PI	KDR-ESB-00-5	19+84.85	1,898,563.3008	1,171,492.7332
PT	KDR-ESB-00-5	22+92.78	1,898,870.8971	1,171,475.5242
PC	KDR-ESB-00-6	25+30.01	1,899,107.7534	1,171,462.2729
PI	KDR-ESB-00-6	28+07.67	1,899,384.9870	1,171,446.7626
PT	KDR-ESB-00-6	30+85.31	1,899,662.5150	1,171,437.9735
POT		45+34.00	1,901,110.4797	1,171,392.1175

EX WB I-290 (EISENHOWER EXPY)				
POINT	DESCRIPTION	STATION	NORTHING	EASTING
POT		293+90.95	1,898,109.2305	1,165,314.6967
TS	IKE-EWB-00-1B	322+24.26	1,898,195.6075	1,168,146.6931
PIS	IKE-EWB-00-1B	323+57.62	1,898,199.6729	1,168,279.9836
SC	IKE-EWB-00-1	324+24.26	1,898,198.2148	1,168,346.6517
PI	IKE-EWB-00-1	325+38.06	1,898,195.7265	1,168,460.4198
CS	IKE-EWB-00-1	326+51.59	1,898,179.7466	1,168,573.0875
PIS	IKE-EWB-00-1A	327+18.26	1,898,170.3839	1,168,639.1010
ST	IKE-EWB-00-1A	328+51.56	1,898,144.7784	1,168,769.9517
TS	IKE-EWB-00-2B	329+78.32	1,898,120.4341	1,168,894.3575
PIS	IKE-EWB-00-2B	331+11.67	1,898,094.8247	1,169,025.2278
SC	IKE-EWB-00-2	331+78.32	1,898,085.4611	1,169,091.2512
PI	IKE-EWB-00-2	332+11.06	1,898,080.8638	1,169,123.6666
CS	IKE-EWB-00-2	332+43.79	1,898,077.3803	1,169,156.2204
PIS	IKE-EWB-00-2A	333+10.47	1,898,070.2861	1,169,222.5160
ST	IKE-EWB-00-2A	334+43.76	1,898,063.0562	1,169,355.6523
PC	IKE-EWB-00-3	338+11.32	1,898,043.1252	1,169,722.6716
PI	IKE-EWB-00-3	340+66.38	1,898,029.2947	1,169,977.3536
PT	IKE-EWB-00-3	343+21.10	1,898,038.1489	1,170,232.2572
TS	IKE-EWB-00-4B	352+47.24	1,898,070.2993	1,171,157.8387
PIS	IKE-EWB-00-4B	353+47.25	1,898,073.7709	1,171,257.7841
SC	IKE-EWB-00-4	353+97.24	1,898,073.8709	1,171,307.7891
PI	IKE-EWB-00-4	354+73.38	1,898,074.0231	1,171,383.9319
CS	IKE-EWB-00-4	355+49.47	1,898,069.1215	1,171,459.9169
PIS	IKE-EWB-00-4A	355+99.47	1,898,065.9031	1,171,509.8083
ST	IKE-EWB-00-4A	356+99.44	1,898,056.2063	1,171,609.3226
TS	IKE-EWB-00-5B	359+16.74	1,898,035.1324	1,171,825.5961
PIS	IKE-EWB-00-5B	360+16.74	1,898,025.4337	1,171,925.1303
SC	IKE-EWB-00-5	360+66.74	1,898,022.2149	1,171,975.0317
PI	IKE-EWB-00-5	361+31.77	1,898,018.0288	1,172,039.9323
CS	IKE-EWB-00-5	361+96.78	1,898,017.5295	1,172,104.9659
PIS	IKE-EWB-00-5A	362+46.77	1,898,017.1456	1,172,154.9595
ST	IKE-EWB-00-5A	363+46.75	1,898,019.6488	1,172,254.9138
POT		368+89.26	1,898,033.2310	1,172,797.2593

EX WS RAMP				
POINT	DESCRIPTION	STATION	NORTHING	EASTING
POT		10+80.89	1,898,061.5790	1,172,189.8288
PC	CIR-EWS-00-1	13+63.32	1,898,074.4485	1,171,907.6892
PI	CIR-EWS-00-1	14+59.92	1,898,078.8501	1,171,811.1924
PT	CIR-EWS-00-1	15+47.68	1,898,146.9909	1,171,742.7246
PC	CIR-EWS-00-2	16+58.81	1,898,225.3834	1,171,663.9559
PI	CIR-EWS-00-2	18+99.17	1,898,394.9331	1,171,493.5925
PCC	CIR-EWS-00-2/CIR-EWS-00-3	20+59.40	1,898,257.6484	1,171,296.3016
PI	CIR-EWS-00-3	23+40.00	1,898,097.3780	1,171,065.9783
PT	CIR-EWS-00-3	24+98.09	1,897,869.1423	1,171,229.2078
POT	26+84.69 BK = Sta 0+00.00 AH		1,897,717.3617	1,171,337.7582
PC	CIR-EWS-00-4, 0+11.04 BK = 18+75.67 AH		1,897,723.7342	1,171,346.7691
PI	CIR-EWS-00-4	19+78.44	1,897,640.1447	1,171,406.5568
PCC	CIR-EWS-00-4/CIR-EWS-00-5	20+80.41	1,897,545.6837	1,171,447.0403
PI	CIR-EWS-00-5	21+61.79	1,897,470.8805	1,171,479.0990
PCC	CIR-EWS-00-5/CIR-EWS-00-6	22+42.21	1,897,390.2813	1,171,490.3702
PI	CIR-EWS-00-6	24+08.12	1,897,225.9725	1,171,513.3475
PT	CIR-EWS-00-6	25+73.82	1,897,060.2922	1,171,522.0278
POT		27+51.26	1,896,883.0948	1,171,531.3115

EX NB I-90/94 (KENNEDY EXPY)				
POINT	DESCRIPTION	STATION	NORTHING	EASTING
POT		183+13.36	1,894,228.6347	1,171,899.8969
PC	KDR-ENB-00-1	190+77.41	1,894,992.2698	1,171,874.5496
PI	KDR-ENB-00-1	192+81.92	1,895,196.6626	1,171,867.7651
PT	KDR-ENB-00-1	194+86.31	1,895,400.3262	1,171,849.2283
PC	KDR-ENB-00-2	196+49.74	1,895,563.0812	1,171,834.4148
PI	KDR-ENB-00-2	199+68.51	1,895,880.5418	1,171,805.5204
PT	KDR-ENB-00-2	202+87.12	1,896,199.1177	1,171,794.3177
POT	209+89.95 BK, 3+06.94 AH		1,896,901.5590	1,171,770.9004
PC	KDR-ENB-00-3	5+24.89	1,897,119.3934	1,171,763.6385
PI	KDR-ENB-00-3	7+04.34	1,897,298.7419	1,171,757.6595
PT	KDR-ENB-00-3	8+83.32	1,897,475.9425	1,171,729.3467
PC	KDR-ENB-00-4	16+62.09	1,898,244.9598	1,171,606.4743
PI	KDR-ENB-00-4	20+43.63	1,898,621.7192	1,171,546.2763
PT	KDR-ENB-00-4	24+22.65	1,899,002.9421	1,171,561.7870
PC	KDR-ENB-00-5	24+61.57	1,899,041.8342	1,171,563.3694
PI	KDR-ENB-00-5	27+72.52	1,899,352.5300	1,171,576.0107
PT	KDR-ENB-00-5	30+83.20	1,899,663.3270	1,171,566.1658
POT		45+35.14	1,901,114.5368	1,171,520.1970

EX EB I-290 (EISENHOWER EXPY)				
POINT	DESCRIPTION	STATION	NORTHING	EASTING
POT		293+88.84	1,897,910.7311	1,165,300.7513
PC	IKE-EEB-00-1	314+72.13	1,897,974.3410	1,167,383.0682
PI	IKE-EEB-00-1	321+86.75	1,897,996.1608	1,168,097.3549
PT	IKE-EEB-00-1	329+00.55	1,897,958.6150	1,168,810.9878
PC	IKE-EEB-00-2	339+78.49	1,897,901.9806	1,169,887.4363
PI	IKE-EEB-00-2	341+00.95	1,897,895.5464	1,170,009.7311
PT	IKE-EEB-00-2	342+23.29	1,897,898.7017	1,170,132.1545
POT		344+82.35	1,897,905.3762	1,170,391.1241
PC	IKE-EEB-00-3	353+59.49	1,897,927.9756	1,171,267.9805
PI	IKE-EEB-00-3	354+82.42	1,897,931.1429	1,171,390.8707
PT	IKE-EEB-00-3	356+05.32	1,897,939.5781	1,171,513.5119
PC	IKE-EEB-00-4	356+51.86	1,897,942.7715	1,171,559.9401
PI	IKE-EEB-00-4	357+76.88	1,897,951.3501	1,171,684.6669
PT	IKE-EEB-00-4	359+01.86	1,897,954.4801	1,171,809.6491
POT		368+91.13	1,897,979.2472	1,172,798.6113

EX ES RAMP				
POINT	DESCRIPTION	STATION	NORTHING	EASTING
POT		100+00.00	1,897,905.8769	1,170,411.1180
PI		104+85.81	1,897,848.1878	1,170,893.4859
PC	CIR-EES-00-1	106+02.00	1,897,833.1375	1,171,008.7039
PI	CIR-EES-00-1	107+84.38	1,897,812.0320	1,171,189.8610
PCC	CIR-EES-00-1/CIR-EES-00-2	109+51.77	1,897,679.2448	1,171,314.8846
PI	CIR-EES-00-2	111+22.58	1,897,554.4446	1,171,431.5050
PCC	CIR-EES-00-2/CIR-EES-00-3	112+83.77	1,897,386.1594	1,171,460.7534
PI	CIR-EES-00-3	114+48.97	1,897,223.4066	1,171,489.0402
PT	CIR-EES-00-3	116+13.95	1,897,058.8194	1,171,503.1705
PC	CIR-EES-00-4	119+23.54	1,896,750.3679	1,171,529.6519
PI	CIR-EES-00-4	120+33.98	1,896,640.3327	1,171,539.0987
PT	CIR-EES-00-4	121+43.67	1,896,534.4186	1,171,570.3909
PC	CIR-EES-00-5	123+17.28	1,896,367.9252	1,171,619.5811
PI	CIR-EES-00-5	124+23.38	1,896,266.1759	1,171,649.6429
PT	CIR-EES-00-5	125+29.04	1,896,160.9766	1,171,663.4180
POT		125+69.05	1,896,121.3108	1,171,668.6915

EX NW RAMP				
POINT	DESCRIPTION	STATION	NORTHING	EASTING
POT		7+96.20	1,897,507.4719	1,171,763.3763
PC	CIR-ENW-00-1	9+85.04	1,897,693.9515	1,171,733.5850
PI	CIR-ENW-00-1	10+35.96	1,897,744.2285	1,171,725.5529
PT	CIR-ENW-00-1	10+85.49	1,897,793.6250	1,171,737.8925
PC	CIR-ENW-00-2	12+39.06	1,897,942.6154	1,171,775.1112
PI	CIR-ENW-00-2	23+51.29	1,899,021.6848	1,172,044.6691
PT	CIR-ENW-00-2	20+73.26	1,898,256.1989	1,171,237.7716
PC	CIR-ENW-00-3	21+70.03	1,898,189.5975	1,171,167.5671
PI	CIR-ENW-00-3	22+66.58	1,898,123.1471	1,171,097.5218
PT	CIR-ENW-00-3	23+56.35	1,898,112.3438	1,171,001.5775
POT		29+69.88	1,898,043.6940	1,170,391.8959

EX SW RAMP				
POINT	DESCRIPTION	STATION	NORTHING	EASTING
PC	CIR-ESW-00-1	13+90.03	1,898,782.0096	1,171,425.7159
PI	CIR-ESW-00-1	16+45.46	1,898,527.0935	1,171,441.8766
PCC	CIR-ESW-00-1/CIR-ESW-00-2	18+74.16	1,898,335.8773	1,171,272.5256
PI	CIR-ESW-00-2	20+08.60	1,898,235.2352	1,171,183.3917
PCC	CIR-ESW-00-2/CIR-ESW-00-3	21+41.29	1,898,163.1121	1,171,069.9371
PI	CIR-ESW-00-3	22+38.85	1,898,110.7700	1,170,987.5993
PT	CIR-ESW-00-3	23+33.05	1,898,099.8530	1,170,890.6455

EX EN RAMP				
POINT	DESCRIPTION	STATION	NORTHING	EASTING
POT		12+93.08	1,897,848.1878	1,170,893.4859
PC	CIR-EEN-00-1	15+91.71		

PROPOSED ALIGNMENTS

PR TAYLOR EXIT RAMP				
POINT	DESCRIPTION	STATION	NORTHING	EASTING
POT		7300+00.00	1,897,859.3643	1,170,489.3261
PC	P-TAY-ES-1	7301+72.03	1,897,851.8722	1,170,661.1900
PI	P-TAY-ES-1	7303+20.62	1,897,855.7720	1,170,809.7335
PCC	P-TAY-ES-1/P-TAY-ES-2	7304+67.59	1,897,821.8823	1,170,954.4121
PI	P-TAY-ES-2	7304+87.59	1,897,815.9728	1,170,973.5222
PCC	P-TAY-ES-2/P-TAY-ES-3	7305+07.59	1,897,809.4040	1,170,992.4159
PI	P-TAY-ES-3	7306+04.16	1,897,777.6920	1,171,083.6281
PCC	P-TAY-ES-3/P-TAY-ES-4	7306+99.28	1,897,720.4792	1,171,161.4226
PI	P-TAY-ES-4	7309+22.03	1,897,588.5048	1,171,340.8734
PT	P-TAY-ES-4	7311+23.30	1,897,369.3289	1,171,380.6447
PC	P-TAY-ES-5	7312+89.49	1,897,205.8036	1,171,410.3177
PI	P-TAY-ES-5	7313+50.28	1,897,145.9948	1,171,421.1705
PT	P-TAY-ES-5	7314+10.96	1,897,087.6162	1,171,438.1064
PC	P-TAY-ES-6	7315+47.73	1,896,956.2590	1,171,476.2136
PI	P-TAY-ES-6	7315+99.52	1,896,906.5239	1,171,490.6420
PT	P-TAY-ES-6	7316+51.15	1,896,855.3179	1,171,498.3687
PC	P-TAY-ES-7	7323+70.21	1,896,144.3089	1,171,605.6557
PI	P-TAY-ES-7	7324+60.13	1,896,055.3983	1,171,619.0718
PT	P-TAY-ES-7	7325+49.81	1,895,965.5123	1,171,621.4385
POT		7326+65.62	1,895,849.7437	1,171,624.4867

PR HALSTED STREET				
POINT	DESCRIPTION	STATION	NORTHING	EASTING
POT		3800+00.00	1,894,443.3678	1,171,230.8570
PI		3844+23.27	1,898,864.5442	1,171,094.8299
PI		3858+25.74	1,900,266.2756	1,171,049.3238
PI		3872+37.11	1,901,677.0451	1,171,008.1349
PI		3876+85.98	1,902,125.8500	1,171,000.3999
POT		3879+94.35	1,902,434.0036	1,170,988.7426

PR VAN BUREN STREET				
POINT	DESCRIPTION	STATION	NORTHING	EASTING
POT		8100+00.00	1,898,259.3220	1,166,490.9835
PI		8125+63.42	1,898,338.2206	1,169,053.1845
PI		8146+24.60	1,898,397.0221	1,171,113.5263
POT		8171+35.75	1,898,460.2841	1,173,623.8877

PR HARRISON STREET				
POINT	DESCRIPTION	STATION	NORTHING	EASTING
POT		7798+00.00	1,897,559.2104	1,170,301.2209
PI		7802+87.02	1,897,574.4743	1,170,788.0041
PC	P-HAR-CL-1	7805+53.90	1,897,545.0894	1,171,053.2542
PI	P-HAR-CL-1	7806+31.79	1,897,536.5125	1,171,130.6756
PT	P-HAR-CL-1	7807+09.45	1,897,538.4543	1,171,208.5464
POT		7823+18.99	1,897,578.5778	1,172,817.5931

PR DES PLAINES STREET				
POINT	DESCRIPTION	STATION	NORTHING	EASTING
POT		4100+00.00	1,894,897.3602	1,172,103.4101
PI		4143+90.91	1,899,286.0495	1,171,963.8051
PI		4154+01.70	1,900,296.2890	1,171,930.4175
PI		4168+05.05	1,901,698.9691	1,171,887.2201
POT		4174+11.29	1,902,304.9557	1,171,869.4907

PR MORGAN STREET				
POINT	DESCRIPTION	STATION	NORTHING	EASTING
POT		3600+00.00	1,897,559.2104	1,169,861.4881
POT		3618+77.63	1,899,391.6120	1,169,810.8410

PR PEORIA STREET				
POINT	DESCRIPTION	STATION	NORTHING	EASTING
POT		3700+00.00	1,897,590.2790	1,170,498.7199
POT		3717+00.00	1,899,287.4618	1,170,446.0167

PR PEORIA SIDEWALK				
POINT	DESCRIPTION	STATION	NORTHING	EASTING
POT		40+00.00	1,897,562.8859	1,170,418.4372
PI		40+63.78	1,897,626.6321	1,170,416.4383
POT		43+00.58	1,897,849.0192	1,170,497.7879

PROPOSED CURVE DATA

<p>PROP. CURVE P-TAY-ES-1 PI STA. = 7303+20.62 Δ = 14° 41' 14" (RT) D = 4° 58' 09" R = 1,153.00' T = 148.59' L = 295.56' E = 9.54' e = N/A T.R. = N/A S.E. RUN = N/A P.C. STA. = 7301+72.03 P.T. STA. = 7304+67.59</p>	<p>PROP. CURVE P-TAY-ES-2 PI STA. = 7304+87.59 Δ = 1° 59' 16" (RT) D = 4° 58' 09" R = 1,153.00' T = 20.00' L = 40.00' E = 7.24' e = N/A T.R. = N/A S.E. RUN = N/A P.C. STA. = 7304+67.59 P.T. STA. = 7305+07.59</p>	<p>PROP. CURVE P-TAY-ES-3 PI STA. = 7306+04.16 Δ = 17° 09' 39" (RT) D = 8° 57' 09" R = 640.00' T = 96.57' L = 191.69' E = 7.24' e = N/A T.R. = N/A S.E. RUN = N/A P.C. STA. = 7305+07.59 P.T. STA. = 7306+99.28</p>	<p>PROP. CURVE P-TAY-ES-4 PI STA. = 7309+22.03 Δ = 43° 22' 59" (RT) D = 10° 13' 53" R = 560.00' T = 222.76' L = 424.02' E = 42.68' e = N/A T.R. = N/A S.E. RUN = N/A P.C. STA. = 7306+99.28 P.T. STA. = 7311+23.30</p>	<p>PROP. CURVE P-TAY-ES-5 PI STA. = 7313+50.28 Δ = 5° 53' 34" (LT) D = 4° 51' 05" R = 1,181.00' T = 60.79' L = 121.46' E = 1.56' e = N/A T.R. = N/A S.E. RUN = N/A P.C. STA. = 7312+89.49 P.T. STA. = 7314+10.96</p>	<p>PROP. CURVE P-TAY-ES-6 PI STA. = 7315+99.52 Δ = 7° 35' 48" (RT) D = 7° 20' 44" R = 780.00' T = 51.79' L = 103.42' E = 1.72' e = N/A T.R. = N/A S.E. RUN = N/A P.C. STA. = 7315+47.73 P.T. STA. = 7316+51.15</p>	<p>PROP. CURVE P-TAY-ES-7 PI STA. = 7324+60.13 Δ = 7° 04' 21" (RT) D = 3° 56' 16" R = 1,455.00' T = 89.92' L = 179.61' E = 2.78' e = N/A T.R. = N/A S.E. RUN = N/A P.C. STA. = 7323+70.21 P.T. STA. = 7325+49.81</p>
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**PART OF FUTURE CONTRACT (BY OTHERS)
ALIGNMENT USED EXCLUSIVELY FOR AS
A REFERENCE FOR RETAINING WALL 13
SN 016-1802, PR TAYLOR EXIT RAMP
CURVE DATA ARE SHOWN FOR
INFORMATION ONLY**

<p>PROP. CURVE PEO-SIP-2-1 PI STA. = 22+03.45 Δ = 45° 10' 01" (RT) D = 716° 11' 50" R = 8.00' T = 3.33' L = 6.31' E = 0.66' e = ---- T.R. = ---- P.C. STA. = 22+00.13 P.T. STA. = 22+06.43</p>	<p>PROP. CURVE PEO-SIP-2-2 PI STA. = 22+66.99 Δ = 45° 01' 21" (LT) D = 381° 58' 19" R = 15.00' T = 6.22' L = 11.79' E = 1.24' e = ---- T.R. = ---- P.C. STA. = 22+60.78 P.T. STA. = 22+72.56</p>	<p>PROP. CURVE P-HAR-CL-1 PI STA. = 7806+31.79 Δ = 7° 45' 00" (LT) D = 4° 58' 56" R = 1,150.00' T = 77.90' L = 155.55' E = 2.64' e = N/A T.R. = N/A S.E. RUN = N/A P.C. STA. = 7805+53.90 P.T. STA. = 7807+09.45</p>
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ALIGNMENT, TIES AND BENCHMARKS

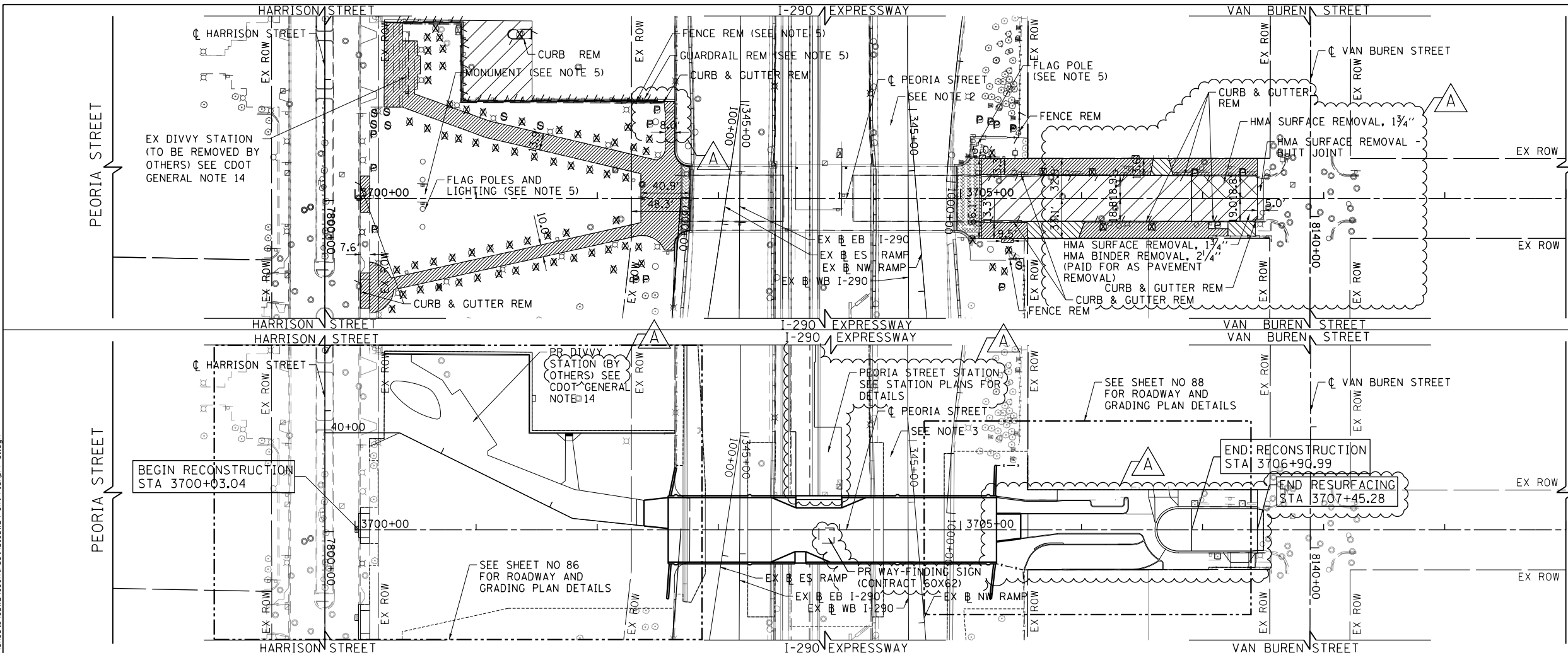
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F.A.I. RE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	38
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60W29	

PLAN	SURVEYED	DATE
	PLOTTED	BY
	NOTED	
	CHECKED	
	FILED	
	NO.	

PROFILE	SURVEYED	DATE
	PLOTTED	BY
	NOTED	
	CHECKED	
	FILED	
	NO.	

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PAVEMENT REMOVAL LEGEND

- PAVEMENT REMOVAL
- DRIVEWAY PAVEMENT REMOVAL
- SIDEWALK REMOVAL
- HMA SURFACE REMOVAL
- APPROACH SLAB REMOVAL
- LINEAR REMOVAL
- TREE REMOVAL
- TREE TRUNK PROTECTION, TREE ROOT PRUNING, TREE PRUNING
- SAPLING REMOVAL (NOT MEASURED FOR PAYMENT)

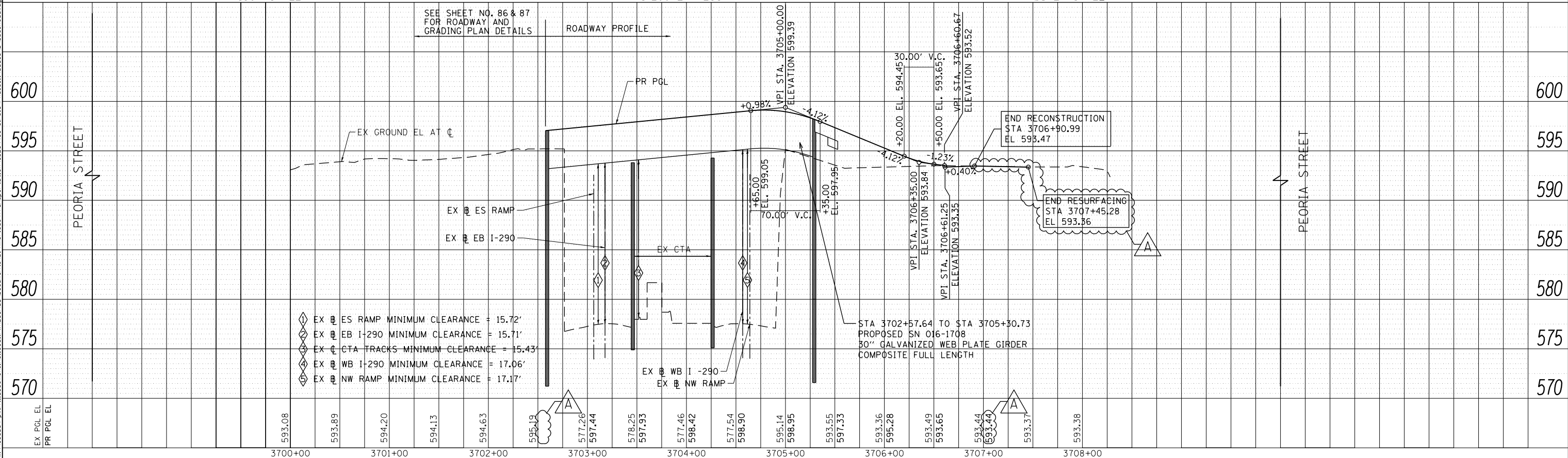
EXISTING

NOTES:

- SEE ROADWAY AND GRADING DETAIL SHEETS FOR GEOMETRIC INFORMATION, SHEET NO. 86 TO 89
- SEE I-290 REMOVAL PLAN SHEET NO. 40 TO 41 FOR ADDITIONAL INFORMATION.
- SEE I-290 ROADWAY PLAN SHEET NO. 42 TO 43 FOR ADDITIONAL INFORMATION.
- SEE ALIGNMENT AND TIES SHEET NO. 30 TO 34 FOR CURVE DATA.
- SEE NOTE 35 ON THE GENERAL NOTES SHEET FOR INFORMATION REGARDING REMOVAL AND STORAGE OF THE ITEM.
- ALL NEWSPAPER STANDS NEAR THE CTA STATION WILL BE REMOVED BY OTHERS.

PROPOSED

SCALE: 1"=50 FEET



EX PGL EL	PR PGL EL	3700+00	3701+00	3702+00	3703+00	3704+00	3705+00	3706+00	3707+00	3708+00															
593.08	593.89	594.20	594.13	594.63	595.19	577.26	597.44	578.25	597.93	577.46	598.42	577.54	598.90	595.14	598.95	593.55	597.33	593.36	595.28	593.49	593.65	593.44	593.44	593.37	593.38



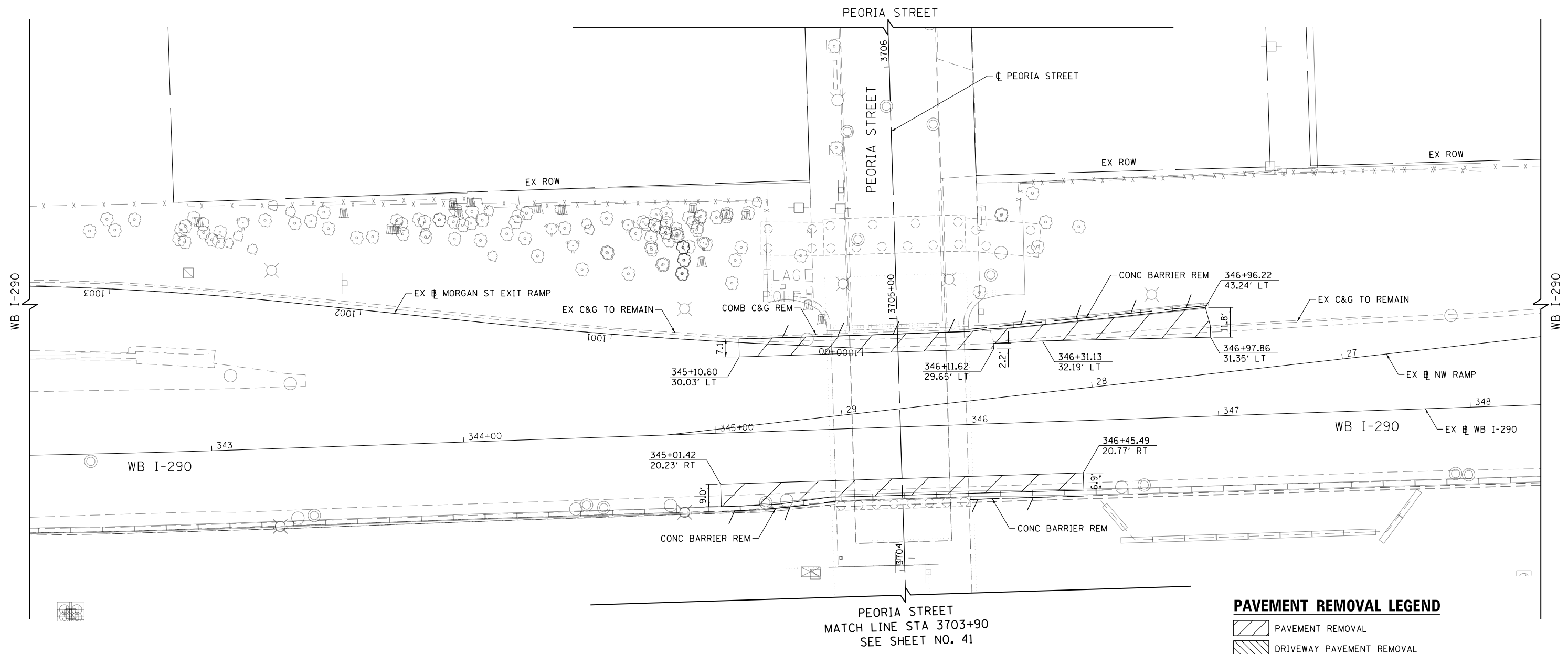
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PLOT DATE = 12/19/2013	DATE - 10/30/2013	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

ROADWAY PLAN AND PROFILE
PEORIA STREET

SCALE: 1"=50' SHEET 1 OF 5 SHEETS STA. 3700+00 TO STA. 3710+00

F.A.I. R.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	39
CONTRACT NO. 60W29			ILLINOIS FED. AID PROJECT	

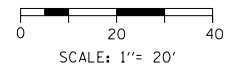


PAVEMENT REMOVAL LEGEND

- PAVEMENT REMOVAL
- DRIVEWAY PAVEMENT REMOVAL
- SIDEWALK REMOVAL
- HMA SURFACE REMOVAL
- APPROACH SLAB REMOVAL
- MEDIAN REMOVAL
- PAVED SHOULDER REMOVAL
- LINEAR REMOVAL
- X TREE REMOVAL
- P TREE TRUNK PROTECTION, TREE ROOT PRUNING, TREE PRUNING
- S SAPLING REMOVAL (NOT MEASURED FOR PAYMENT)

NOTES:

1. ALL STATIONS AND OFFSETS ARE MEASURED FROM EX. I-290 & RAMP ALIGNMENTS UNLESS OTHERWISE NOTED.
2. SEE PEORIA STREET PLAN AND PROFILE SHEET FOR ADDITIONAL REMOVAL INFORMATION.



PEORIA STREET
MATCH LINE STA 3703+90
SEE SHEET NO. 41

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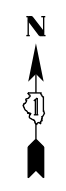


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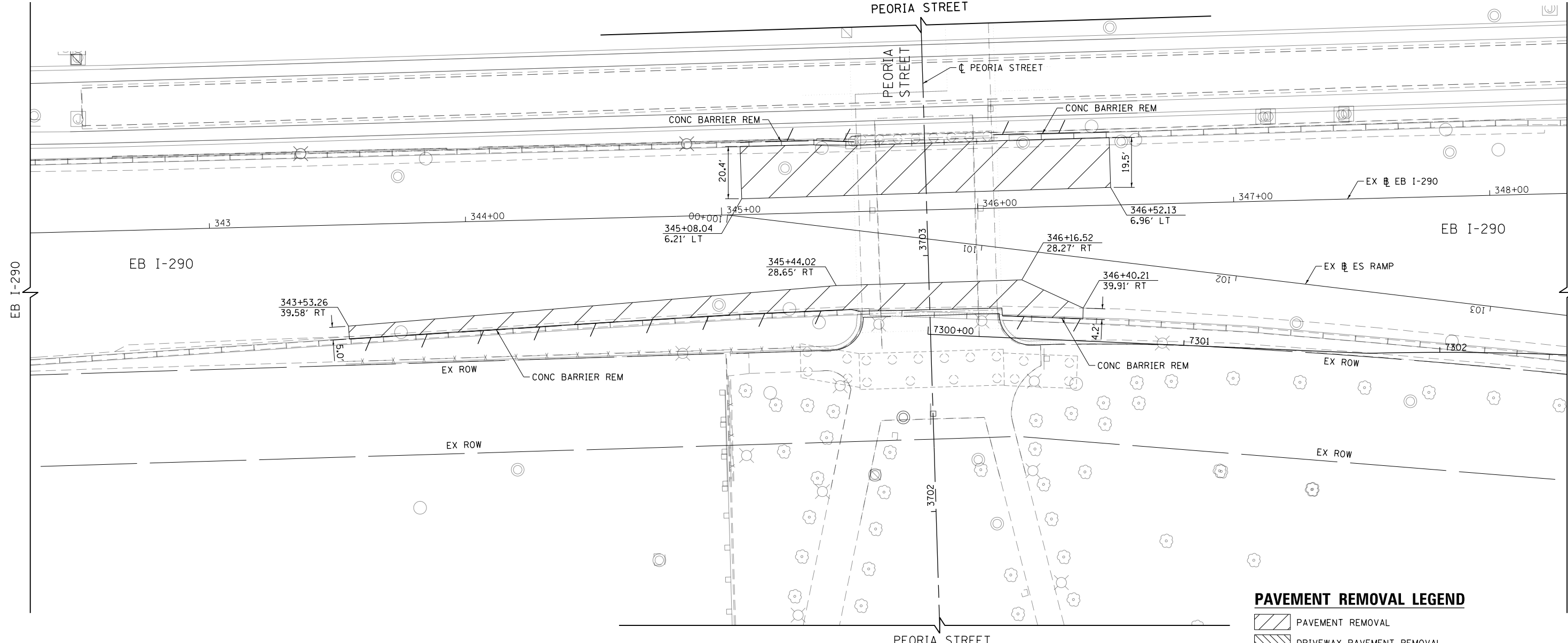
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY REMOVAL PLAN I-290 WESTBOUND	
SCALE: 1"=20'	SHEET 2 OF 5 SHEETS STA. TO STA.

F.A.I. RTE. 90/94/290	SECTION 2013-011R	COUNTY COOK	TOTAL SHEETS 356	SHEET NO. 40
CONTRACT NO. 60W29				
ILLINOIS FED. AID PROJECT				



SEE SHEET NO. 40
MATCH LINE STA 3703+90
PEORIA STREET

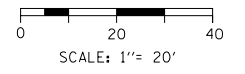


PAVEMENT REMOVAL LEGEND

- PAVEMENT REMOVAL
- DRIVEWAY PAVEMENT REMOVAL
- SIDEWALK REMOVAL
- HMA SURFACE REMOVAL
- APPROACH SLAB REMOVAL
- MEDIAN REMOVAL
- PAVED SHOULDER REMOVAL
- LINEAR REMOVAL
- TREE REMOVAL
- TREE TRUNK PROTECTION, TREE ROOT PRUNING, TREE PRUNING
- SAPLING REMOVAL (NOT MEASURED FOR PAYMENT)

NOTES:

1. ALL STATIONS AND OFFSETS ARE MEASURED FROM EX EB I-290 & RAMP ALIGNMENTS UNLESS OTHERWISE NOTED.
2. SEE PEORIA STREET PLAN AND PROFILE SHEET FOR ADDITIONAL REMOVAL INFORMATION.



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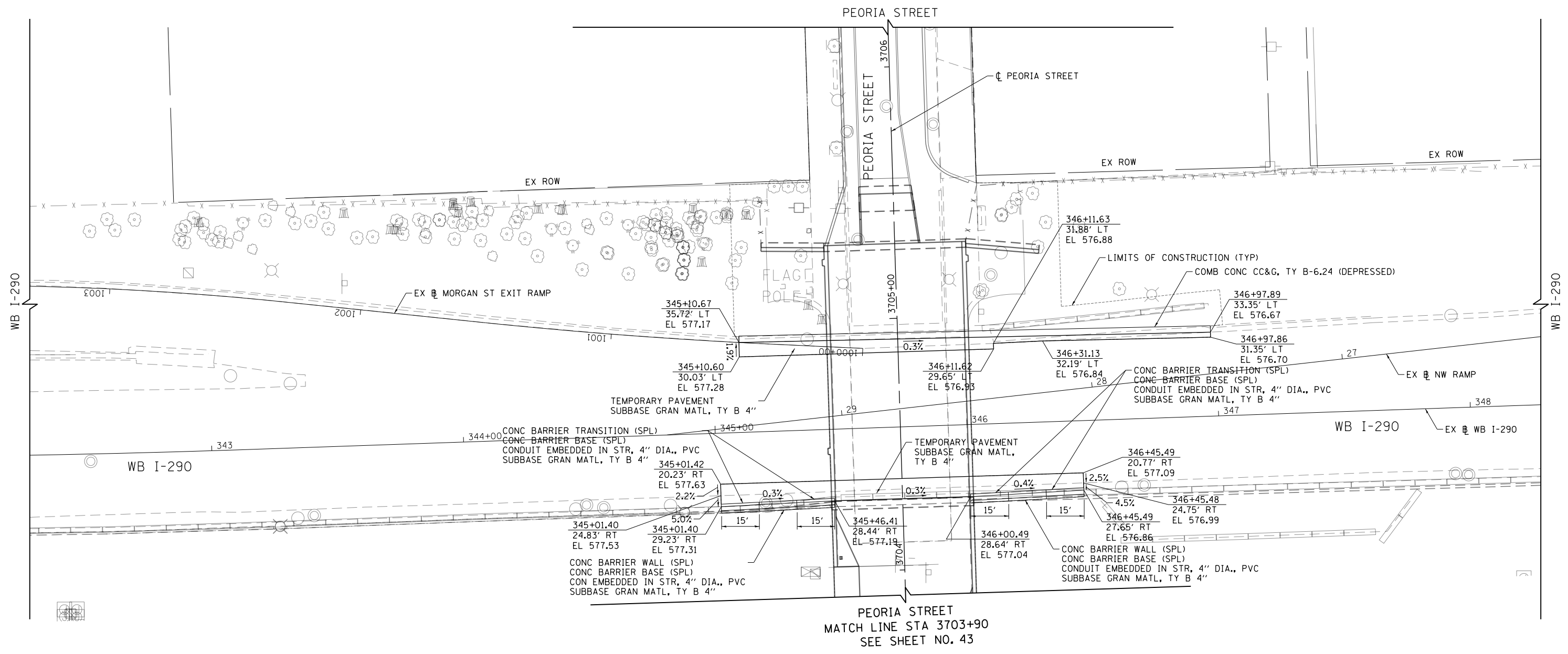
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PLOT DATE = 10/28/2013	DATE - 10/30/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY REMOVAL PLAN
I-290 EASTBOUND**

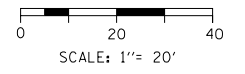
SCALE: 1"=20' SHEET 3 OF 5 SHEETS STA. TO STA.

F.A.I. RTE. 90/94/290	SECTION 2013-011R	COUNTY COOK	TOTAL SHEETS 356	SHEET NO. 41
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60W29	



NOTES:

1. ALL STATIONS AND OFFSETS ARE MEASURED FROM EX \square I-290 & RAMP ALIGNMENTS UNLESS OTHERWISE NOTED.
2. SEE PEORIA STREET PLAN AND PROFILE FOR ADDITIONAL PROPOSED INFORMATION.
3. SEE ROADWAY DETAIL SHEETS FOR CONCRETE BARRIER AND CONCRETE BARRIER TRANSITION LIMITS AND DETAILS.



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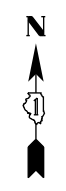
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DATE - 10/30/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

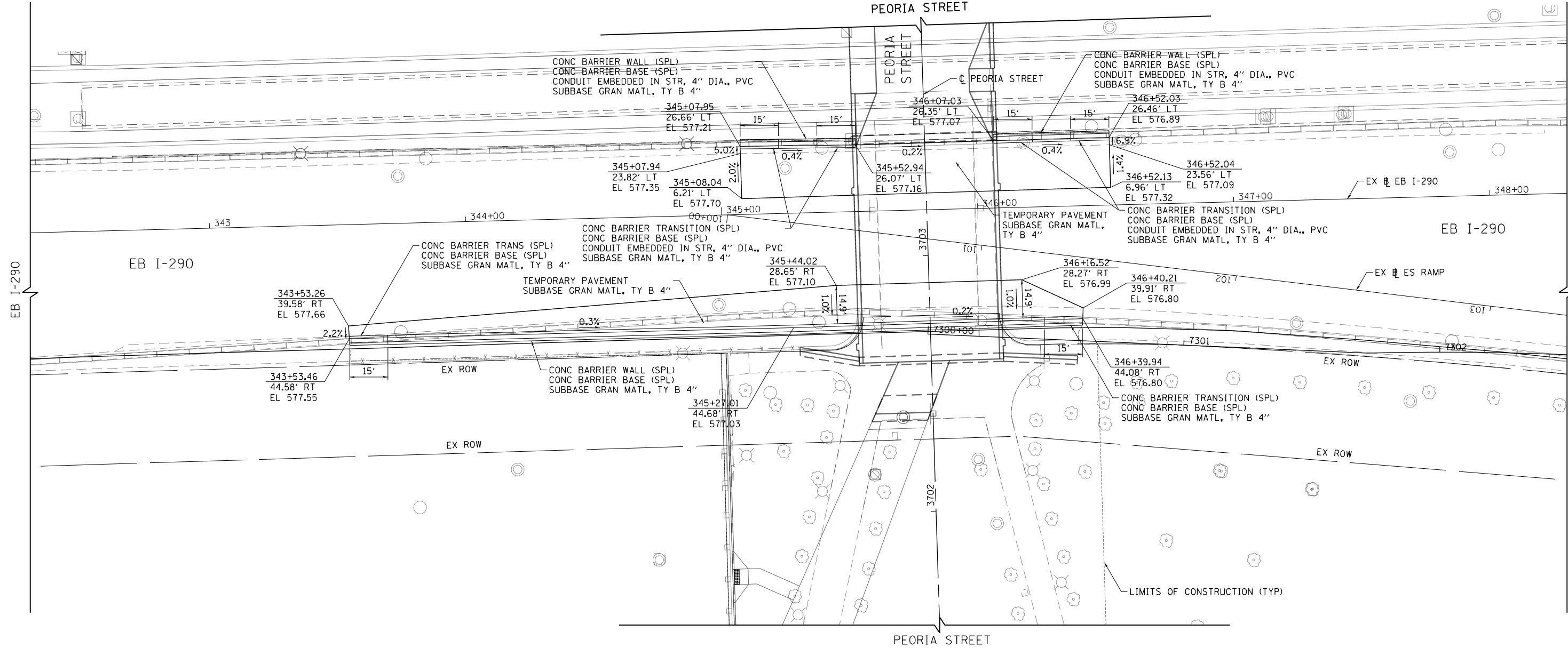
**ROADWAY PLAN
I-290 WESTBOUND**

SCALE: 1"=20' SHEET 4 OF 5 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	42
CONTRACT NO. 60W29			ILLINOIS FED. AID PROJECT	

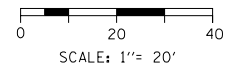


SEE SHEET NO. 42
MATCH LINE STA 3703+90
PEORIA STREET



NOTES:

1. ALL STATIONS AND OFFSETS ARE MEASURED FROM EX ROW I-290 & RAMP ALIGNMENTS UNLESS OTHERWISE NOTED.
2. SEE PEORIA STREET PLAN AND PROFILE FOR ADDITIONAL PROPOSED INFORMATION.
3. SEE ROADWAY DETAIL SHEETS FOR CONCRETE BARRIER AND CONCRETE BARRIER TRANSITION LIMITS AND DETAILS.



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PLOT DATE = 10/28/2013	DATE - 10/30/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN
I-290 EASTBOUND**

SCALE: 1"=20' SHEET 5 OF 5 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	43
CONTRACT NO. 60W29				
ILLINOIS FED. AID PROJECT				





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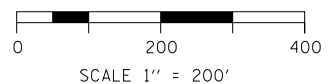
NOTES

1. THE PROPOSED DETOUR SIGNING SHALL BE INSTALLED PRIOR TO CLOSING THE PEORIA STREET BRIDGE. THE PROPOSED DETOUR SHALL BE UTILIZED DURING STAGES 1A AND 1B.
2. THE APPROXIMATE LOCATIONS OF THE PROPOSED SIGN BARRICADE TYPE III THAT ARE SHOWN ALONG PEORIA STREET ARE FOR STAGE 1A AND WILL NEED TO BE RELOCATED DURING STAGE 1B TO ALLOW FOR THE PROPOSED CONSTRUCTION THAT WILL TAKE PLACE DURING THAT STAGE.
3. THE CONTRACTOR IS RESPONSIBLE FOR COORDINATION WITH OTHER ROAD WORK IN THE AREA.
4. ANY EXISTING SIGNS THAT CONTRADICT THE DETOUR SIGNAGE AS SHOWN ON THE PLANS SHALL BE COVERED PRIOR TO OPENING THE DETOUR ROUTE. THIS WORK SHALL BE INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION (SPECIAL)" AND WILL NOT BE PAID FOR SEPARATELY. THE COVERS SHALL BE REMOVED AT THE TIME AT THE DETOUR ROUTE SIGNS ARE REMOVED.
5. ALL SIGNAGE SHALL BE IN ACCORDANCE WITH THE LATEST ILLINOIS STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, "THE QUALITY STANDARD FOR WORK ZONE TRAFFIC CONTROL DEVICES", THE DETAILS OF THESE PLANS, THE "2009 MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES", THE SPECIAL PROVISION FOR "TRAFFIC CONTROL AND PROTECTION (ARTERIALS)" AND HIGHWAY STANDARDS 701801 AND 701901.
6. THE SIGNAGE SHOWN ON THE PLANS IS DEEMED TO BE THE MINIMUM REQUIRED. THE CONTRACTOR SHALL MAKE ALL CHANGES DEEMED NECESSARY BY THE ENGINEER. ADDITIONS AND DELETIONS OF TRAFFIC CONTROL FOR THIS DETOUR SHALL BE CONSIDERED INCLUDED IN THE COST OF "TRAFFIC CONTROL AND PROTECTION (SPECIAL)" AND WILL NOT BE PAID FOR SEPARATELY.
7. ALL SIGNS SHOWN SHALL BE FURNISHED, ERECTED, AND MAINTAINED BY THE CONTRACTOR, AND SHALL BE POST-MOUNTED IN THE GROUND PER ARTICLE 701.14 OF THE STANDARD SPECIFICATIONS AND STANDARD 701901.
8. ALL DETOUR SIGNS SHALL BE REMOVED ONCE CONSTRUCTION IS COMPLETE AND THE ROAD IS REOPENED TO TRAFFIC.
9. THIS SHEET SHOWS A SCHEMATIC OF THE PEDESTRIAN DETOUR AND LOCATION OF THE DETOUR SIGNAGE. SOME OF THE SPECIFIC SIGNING DETAILS ARE ALSO SHOWN ON THE SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN SHEETS.

LEGEND

-  TEMPORARY SIGN
-  SIGN BARRICADE TYPE III
-  ROADWAY DIRECTION
-  DETOUR ROUTE

①		M4-9a (R) 30" X 24"	⑤		R11-1102L 24" X 30"
②		M4-9a (L) 30" X 24"	⑥		R11-1102R 24" X 30"
③		R11-1101 24" X 18"	* ⑦		4" BLACK LETTERS ON ORANGE REFLECTIVE BACKGROUND 48" X 36"
④		R9-11R 24" X 18"	* TO BE PAID FOR AS TEMPORARY INFORMATION SIGNING.		



DESIGNED - JDT	REVISED - 12/18/2013
USER NAME = BAW:tor t	DRAWN - JTR
DATE = 10/30/2013	CHECKED - JMG
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PLOT DATE = 12/19/2013	DATE - 10/30/2013

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PEDESTRIAN DETOUR PLAN
PEORIA STREET**

SCALE: 1"=200' SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	44
CONTRACT NO. 60W29			ILLINOIS FED. AID PROJECT	

TRAFFIC CONTROL GENERAL NOTES

1. TRAFFIC CONTROL AND PROTECTION SHALL BE PERFORMED IN ACCORDANCE WITH SECTION 701 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
2. TYPE III BARRICADES AND ROAD CLOSURE SIGNS SHALL BE POSITIONED AS SHOWN, ACCORDING TO HIGHWAY STANDARD 701901 AND AS DIRECTED BY THE ENGINEER.
3. TYPE A LOW INTENSITY FLASHING WARNING LIGHTS SHALL BE USED ON EACH SIGN IN ADVANCE OF THE WORK DURING HOURS OF DARKNESS.
4. ALL WARNING SIGNS SHALL BE A MINIMUM OF 48" x 48" AND HAVE A BLACK LEGEND AND BORDER ON A FLUORESCENT ORANGE REFLECTORIZED BACKGROUND.
5. ALL TYPE III BARRICADES UTILIZED FOR ROAD CLOSURES SHALL HAVE TWO LOW INTENSITY FLASHING LIGHTS MOUNTED ON TOP OF EACH BARRICADE.

6.

7. WHERE ARTIFICIAL LIGHTING IS UTILIZED IN NIGHT OPERATIONS, THE CONTRACTOR SHALL EXERCISE THE UTMOST PRECAUTIONS IN PREVENTING ADVERSE VISIBILITY TO THE MOTORING PUBLIC AND ADJOINING RESIDENTIAL AREAS.
8. EXACT LOCATION OF ALL WARNING SIGNS AND BARRICADES SHALL BE STAKED IN THE FIELD FOR APPROVAL BY THE ENGINEER PRIOR TO INSTALLATION.
9. PRIOR TO START OF CONSTRUCTION, ALL REQUIRED TRAFFIC CONTROL DEVICES SHALL BE IN PLACE.
10. COOPERATION BETWEEN CONTRACTORS WILL BE REQUIRED. THE CONTRACTOR SHALL COORDINATE TRAFFIC CONTROL WITH OTHER ADJACENT CONTRACTS WHICH MAY BE UNDER CONSTRUCTION DURING THE DURATION OF THIS PROJECT. NO ADDITIONAL COMPENSATION IS ALLOWED. ADJACENT CONTRACTS MAY CONSIST OF, BUT NOT LIMITED TO PROJECTS NEAR:
 - o CONTRACT 60F63 - FAI 90/94 (KENNEDY EXPRESSWAY) AT OHIO STREET
 - o CONTRACT 60W25 - MORGAN STREET BRIDGE AT I-290/CONGRESS PARKWAY (CIRCLE INTERCHANGE)
 - o CONTRACT 60W26 - FROM CIRCLE INTERCHANGE TO HARRISON STREET BRIDGE (WB) AND HALSTED STREET BRIDGE
 - o CONTRACT 60W28 - NORTHWEST FLYOVER BRIDGE AT I-90/94 AND I-290/CONGRESS PARKWAY (CIRCLE INTERCHANGE)
 - o CONTRACT 60W36 - TUNNEL BULKHEADING (CIRCLE INTERCHANGE)
 - o CONTRACT 60W71 - HARRISON STREET BRIDGE (EAST) AT I-90/94 (CIRCLE INTERCHANGE)
 - o UNIVERSITY OF ILLINOIS AT CHICAGO CONTRACT - CUPPA HALL RENOVATIONS
 - o AND OTHERS.

11. THE CONTRACTOR SHALL NOTIFY THE ENGINEER 28 DAYS PRIOR TO ANY ANTICIPATED CLOSURES.
12. A MINIMUM 11' LANE WIDTH SHALL BE MAINTAINED ON ALL INTERSTATE LANES OPEN TO TRAFFIC DURING CONSTRUCTION.
13. OFFSETS BETWEEN THE EDGE OF TRAVEL LANE AND THE FACE OF TEMPORARY CONCRETE BARRIER ARE ASSUMED TO BE 2 FT UNLESS DESIGNATED OTHERWISE.
14. THE CONTRACTOR SHALL BE ADVISED THAT THE WORK DURING PEAK HOURS SPECIFICATION TAKES PRECEDENCE OVER THE SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL.
15. THE CONTRACTOR SHALL VERIFY THE LOCATION OF ALL BUILDING ACCESS, COORDINATE WITH BUILDING OWNERS AND LOCAL AUTHORITIES AND PROVIDE FULL ACCESS TO BUSINESSES OR PROPERTIES DURING THEIR NORMAL WORKING HOURS IN ACCORDANCE WITH ADA AND APPLICABLE CODE REQUIREMENTS.
16. THE CONTRACTOR SHALL MAINTAIN TRAFFIC ON ALL STREETS EXCEPT WHERE NOTED ON THE PLANS AND PROHIBIT PARKING WITHIN FIFTY (50) FEET OF THE CONSTRUCTION AREA AT ALL TIMES. NOTIFY CDOT AND OEMC AT LEAST 72 HOURS BEFORE COMMENCING CONSTRUCTION.
17. PROVIDE CONTINUOUS TEMPORARY ACCESS TO ALL SIDE STREETS, ALLEYS, DRIVEWAYS, AND PARKING LOTS UNLESS SPECIFICALLY IDENTIFIED ON THE PLANS FOR TEMPORARY CLOSURE. LOTS WITH MORE THAN ONE DRIVEWAY MUST BE STAGED TO KEEP AT LEAST ONE DRIVEWAY OPEN AT ALL TIMES.
18. MAINTAIN ACCESS TO FIRE HYDRANTS, BUILDING STANDPIPES AND OTHER EMERGENCY FACILITIES WITHIN THE CONSTRUCTION ZONE.

19. THE CONTRACTOR SHALL COORDINATE WORK WITH PRIVATE UTILITY COMPANIES AND THEIR CONTRACTORS FOR ADJUSTMENTS, SUPPORTS, AND RELOCATION. UTILITY WORK WILL GENERALLY BE CONDUCTED PRIOR TO ROADWAY WORK, BUT CERTAIN ITEMS MAY NEED TO PROCEED IN CONCERT WITH CONSTRUCTION OPERATIONS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO CONTACT AFFECTED UTILITIES AND COORDINATE WORK WITHIN THE CONSTRUCTION SCHEDULE. THE EFFORT OF WHICH WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

20.

21. DEMOLITION AND RECONSTRUCTION OF PIER 1 AND PIER 2 AND THE SUPERSTRUCTURE OF THE PEORIA STREET BRIDGE SHALL BE COORDINATED WITH THE CTA. CTA TRACK ACCESS OCCURRENCES ARE ALLOWED DURING OFF-PEAK HOURS ONLY. ALL CTA TRACK ACCESS OCCURRENCES SHALL BE COORDINATED WITH AND PERFORMED IN ACCORDANCE WITH THE CTA FLAGGING AND COORDINATION SPECIAL PROVISION AND THE DISCRETION OF THE CTA.

22. THE CONTRACTOR SHALL CONTACT THE DISTRICT OF ARTERIAL TRAFFIC CONTROL SUPERVISOR AT (847) 705-4470 AND THE EXPRESSWAYS TRAFFIC CONTROL SUPERVISOR AT (847) 705-4155 A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

23. SIGN W21-1a SHALL BE TAKEN DOWN WHEN THE WORKERS ARE NOT PRESENT.

24. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING ACCESS RESTRICTIONS TO THE SITE AS MAY REQUIRED BY THE ENGINEER. TEMPORARY CHAIN LINK FENCE IS TO BE PLACED AROUND WORK ZONES TO RESTRICT ACCESS. REFER TO THE EROSION AND SEDIMENTATION CONTROL PLANS FOR THE LOCATIONS. ANY MODIFICATIONS TO THE SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN WHICH AFFECT THE EROSION AND SEDIMENTATION CONTROL PLANS MUST STILL RESTRICT ACCESS TO THE WORK ZONE THROUGH THE USE OF TEMPORARY CHAIN LINK FENCE. NO ADDITIONAL COMPENSATION WILL BE ALLOWED DUE TO CHANGES IN EROSION AND SEDIMENTATION CONTROL PLANS.

25. A 3'-6" DEFLECTION AREA IS REQUIRED FROM THE BACK SIDE OF THE TEMPORARY CONCRETE BARRIER WALL TO ANY OBSTRUCTION OR DROP OFF IN THE WORK ZONE. IF THIS 3'-6" DEFLECTION AREA CANNOT BE MAINTAINED, THE TEMPORARY CONCRETE BARRIER WALL SHALL BE ANCHORED TO THE PAVEMENT (EXCLUDING BRIDGE DECKS) THROUGH THE 3 ANCHORING HOLES ON THE TRAFFIC SIDE OF THE TEMPORARY CONCRETE BARRIER WALL. THIS WORK SHALL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE TEMPORARY CONCRETE BARRIER PAY ITEM.

26. THE CONTRACTOR SHALL BE REQUIRED TO REMOVE AND REPLACE ALL REFLECTORS FROM EXISTING RAISED REFLECTIVE PAVEMENT MARKERS WHICH CONFLICT WITH THE DESIGNATED TRAFFIC CONTROL PLANS. THE REMOVAL OF THE REFLECTOR SHALL BE PAID FOR AS RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REMOVAL. THE REPLACEMENT OF THE REFLECTOR SHALL BE PAID FOR AS REPLACEMENT REFLECTOR.

27. ALL TEMPORARY PAVEMENT MARKINGS SHOWING DETERIORATION AFTER 7 DAYS SHALL BE REPLACED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER. SUFFICIENT QUANTITIES FOR ONE PLACEMENT AND ONE REPLACEMENT HAVE BEEN PROVIDED FOR EACH STAGE. ALL MARKINGS THAT REQUIRE REPLACEMENT AFTER THE SECOND PLACEMENT SHALL BE REPLACED BY THE CONTRACTOR AT HIS OWN EXPENSE.

28. THE FURNISHING, INSTALLING, AND RELOCATION OF ALL TRAFFIC SIGNS SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES AND THE STANDARD SPECIFICATIONS. THIS WORK SHALL BE INCLUDED IN THE COST FOR TRAFFIC CONTROL AND PROTECTION, (SPECIAL) FOR THOSE SIGNS ALONG LOCAL STREETS AND INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS) FOR THOSE SIGNS ALONG EXPRESSWAYS. ALL CONFLICTING TRAFFIC SIGNS SHALL BE COVERED AS DIRECTED BY THE ENGINEER. THIS WORK SHALL BE INCLUDED IN THE COST TRAFFIC CONTROL AND PROTECTING, (SPECIAL) FOR THOSE SIGNS ALONG LOCAL STREETS AND INCLUDED IN THE COST OF TRAFFIC CONTROL AND PROTECTION (EXPRESSWAYS) FOR THOSE SIGNS ALONG EXPRESSWAYS.

29. THE EXISTING SCAFFOLDING CURRENTLY IN PLACE ADJACENT TO CUPPA HALL SHALL BE MAINTAINED IN PLACE DURING CONSTRUCTION UNTIL THE RENOVATIONS TO CUPPA HALL ARE COMPLETE. THE CONTRACTOR SHALL COORDINATE WITH THE UNIVERSITY OF ILLINOIS AT CHICAGO AT LEAST THREE WEEKS PRIOR TO BEGINNING WORK ADJACENT TO THE SCAFFOLDING.

30. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ENSURING DRAINAGE OF THE ROADWAY DURING ALL STAGES OF CONSTRUCTION. A QUANTITY OF 5 CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, OPEN LID AND 100' OF STORM SEWERS, CLASS A, TYPE 1 12 INCHES HAS BEEN PROVIDED FOR TEMPORARY USE. REMOVAL OF THESE ITEMS SHALL BE INCLUDED IN THEIR COST.

31. TYPE III BARRICADES SHALL BE PLACED AT BOTH ENDS OF CLOSED PORTIONS OF ROAD UNDER CONSTRUCTION. THE CONTRACTOR SHALL PROVIDE ENOUGH TYPE III BARRICADES TO CROSS THE ENTIRE ROADWAY AS DEPICTED IN THE PLANS.

32. ALL TYPE II/III BARRICADES, DRUMS, AND VERTICAL PANELS SHALL HAVE MONO-DIRECTIONAL OR BI-DIRECTIONAL STEADY BURNING LIGHTS AS PER CURRENT IDOT STANDARDS.

33. ALL TRAFFIC CONTROL DEVICES SHALL BE REMOVED, COVERED OR TURNED AWAY FROM TRAFFIC AS SOON AS THEY ARE NO LONGER NECESSARY. WHEN A SIGN IS COVERED, ITS POST SHALL HAVE A REFLECTIVE 3 INCH X 6 INCH DELINEATOR INSTALLED.

NOTE:

BOXED ITEMS ARE INCLUDED IN THE COST OF THE CONTRACT.

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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN			
GENERAL NOTES			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	45
CONTRACT NO. 60W29			ILLINOIS FED. AID PROJECT	

STAGE 1A

MAINTENANCE OF TRAFFIC AND CONSTRUCTION ALONG EB I-290 AND RAMPES ES & EN

1. MAINTAIN A MINIMUM OF 2 LANES OF TRAFFIC THROUGH TO CONGRESS PARKWAY AT ALL TIMES AS SHOWN ON THE PLANS.
2. MAINTAIN A MINIMUM OF 1 LANE OF TRAFFIC TO RAMP EN AND 2 LANES OF TRAFFIC TO RAMP ES AS SHOWN ON THE PLANS.
3. ALL TRAVEL LANES ON THE EAST END SHALL TIE INTO THE CURRENT STAGE OF TRAFFIC CONTROL FOR THE HALSTED STREET/HARRISON STREET PROJECT CONTRACT NO. 60W26 AND CONFORM TO ALL APPLICABLE IDOT DISTRICT 1 STANDARDS.
4. CLOSE THE OUTSIDE ADD LANE AT THE START OF THE LANE TAPER NEAR STATION 336+61 AND OUTSIDE (RIGHT) SHOULDER IN ADVANCE OF THE LANE CLOSURE. USE THE IDOT DISTRICT 1 STANDARD TC-17 FOR THE SHOULDER CLOSURE.
5. INSTALL THE TEMPORARY CONCRETE BARRIER ALONG THE OUTSIDE (RIGHT) SHOULDER OF EB I-290 AS SHOWN ON THE PLANS.

6. REMOVE THE EXISTING PEORIA STREET BRIDGE MOUNTED SIGNS. INSTALL THE PROPOSED EASTBOUND OVERHEAD SIGNS ONTO THE MORGAN STREET BRIDGE, UTILIZING THE BRIDGE MOUNTED SIGN SUPPORTS THAT WERE PREVIOUSLY INSTALLED AS PART OF THE MORGAN STREET PROJECT CONTRACT NO. 60W25. THE PROPOSED SIGNS SHALL BE INSTALLED AS SHOWN ON THE SIGNING PLANS.

7. REMOVE THE EXISTING SOUTH BRIDGE ABUTMENT. SEE THE BRIDGE CONSTRUCTION NOTES BELOW AND THE STRUCTURAL PLANS FOR DETAILS.
8. REMOVE THE EXISTING CONCRETE BARRIER WALL ALONG THE OUTSIDE (RIGHT) SHOULDER.
9. CONSTRUCT THE TEMPORARY PAVEMENT (TO REMAIN) AND CONCRETE BARRIER WALL AS SHOWN ON THE ROADWAY PLANS FOR USE IN STAGE 1B.

MAINTENANCE OF TRAFFIC AND CONSTRUCTION ALONG WB I-290 AND RAMPES NW & SW

1. MAINTAIN A MINIMUM OF 2 LANES OF TRAFFIC FROM CONGRESS PARKWAY AT ALL TIMES AS SHOWN ON THE PLANS.
2. MAINTAIN 1 LANE OF TRAFFIC FROM RAMP NW AND RAMP SW AT ALL TIMES AS SHOWN ON THE PLANS.
3. INSTALL THE TEMPORARY CONCRETE BARRIER ALONG THE OUTSIDE (RIGHT) AND INSIDE (LEFT) SHOULDER OF WB I-290 AS SHOWN ON THE PLANS. REDUCE THE TRAVEL LANES TO 11 FEET AND SHIFT THE LANES TO THE MIDDLE AS DETAILED ON THE TYPICAL SECTIONS.
4. ALL TRAVEL LANES ON THE EAST END SHALL TIE INTO THE CURRENT STAGE OF TRAFFIC CONTROL FOR HALSTED/HARRISON CONTRACT NO. 60W26 AND CONFORM TO ALL APPLICABLE IDOT DISTRICT 1 STANDARDS.

5. THE MORGAN STREET EXIT WILL REMAIN CLOSED AND ALL APPLICABLE SIGNS ALONG I-290, RAMP NW AND RAMP SW WARNING DRIVERS OF THIS CONDITION SHALL REMAIN IN PLACE FROM THE MORGAN STREET CONTRACT NO. 60W25 AS SHOWN ON THE PLANS.

6. REMOVE THE EXISTING PEORIA STREET BRIDGE MOUNTED SIGNS AND TEMPORARILY GROUND MOUNT THE ASHLAND AVE/PAULINA STREET GUIDE SIGN AS SHOWN ON THE PLANS.
7. BEGIN REMOVING THE EXISTING PIER 2 AND THE NORTH ABUTMENT. SEE THE BRIDGE CONSTRUCTION NOTES BELOW AND THE STRUCTURAL PLANS FOR DETAILS.
8. BEGIN REMOVING THE EXISTING CONCRETE BARRIER WALL ALONG THE INSIDE (LEFT) AND OUTSIDE (RIGHT) SHOULDER. BEGIN REMOVING THE EXISTING CURB AND GUTTER ALONG THE OUTSIDE (RIGHT) SHOULDER.
9. BEGIN CONSTRUCTING THE TEMPORARY PAVEMENT (TO REMAIN), CONCRETE BARRIER WALL AND CURB AND GUTTER AS SHOWN ON THE ROADWAY PLANS.

MAINTENANCE OF TRAFFIC AND CONSTRUCTION ALONG LOCAL ROADS

1. INSTALL THE TRAFFIC CONTROL AND EROSION AND SEDIMENTATION CONTROL ALONG PEORIA STREET AS SHOWN ON THE PLANS.
2. THE ENTRANCE TO THE PARKING LOT ON THE EAST SIDE OF PEORIA STREET, NORTH OF I-290 WILL BE CLOSED DURING CONSTRUCTION. ACCESS TO THE PARKING LOT WILL BE MAINTAINED VIA THE ALLEY OFF OF VAN BUREN STREET.
3. THE ALLEY ADJACENT TO CUPPA HALL SHALL REMAIN OPEN AT ALL TIMES EXCEPT FOR WHEN THE ALLEY ENTRANCE AND PROPOSED CUL-DE-SAC WILL BE RECONSTRUCTED DURING WHICH THE ACCESS WILL BE MAINTAINED VIA THE ALLEY ENTRANCE ON SANGAMON STREET. THE CONTRACTOR SHALL NOTIFY THE UNIVERSITY OF ILLINOIS AT CHICAGO AT LEAST THREE WEEKS PRIOR TO CLOSING ACCESS TO THE ALLEY.
4. A DETOUR SHALL BE POSTED FOR PEDESTRIANS WANTING TO CROSS THE PEORIA STREET BRIDGE AND/OR ACCESS THE PEORIA STREET STATION. ACCESS TO THE CTA STATION AND PLATFORM WILL BE PROVIDED AT MORGAN STREET. SEE THE PEDESTRIAN DETOUR PLAN FOR DETAILS. ACCESS TO BUILDING ENTRANCES SHALL BE MAINTAINED AT ALL TIMES DURING NORMAL WORKING HOURS IN ACCORDANCE WITH ADA AND LOCAL CODE REQUIREMENTS.

5. BEGIN RECONSTRUCTION OF THE SIPHON ALONG PEORIA STREET. FLOW THROUGH THE SIPHON SHALL BE MAINTAINED AT ALL TIMES.

6. BEGIN CONSTRUCTION OF THE NORTH END OF PEORIA STREET. CONSTRUCTION WILL CONTINUE THROUGH STAGE 1B. VEHICLE ACCESS TO PEORIA STREET FROM THE NORTH WILL BE RESTRICTED AS SHOWN ON THE PLANS.

7. CONSTRUCT THE STABILIZED CONSTRUCTION ENTRANCE TO ALLOW CONSTRUCTION ACCESS TO THE PEORIA STREET BRIDGE VIA HARRISON STREET.

8. BEGIN CONSTRUCTION OF THE SOUTH END OF PEORIA STREET. CONSTRUCTION WILL CONTINUE THROUGH STAGE 2.

BRIDGE CONSTRUCTION

1. PLACE THE TEMPORARY SHORING TO SUPPORT THE EXISTING CTA STATION ON THE WEST SIDE OF THE BRIDGE AND THE STAIRS ON THE EAST SIDE OF THE BRIDGE. THIS WORK MUST BE COMPLETED PRIOR TO BEGINNING REMOVAL OF THE EXISTING SUPERSTRUCTURE.
2. RELOCATE THE ITS COMMUNICATION AND POWER CABLE PRIOR TO BEGINNING REMOVAL OF PIER 1 AND PIER 2. SEE THE ITS PLANS FOR DETAILS.
3. REMOVE THE EXISTING BRIDGE SUPERSTRUCTURE.
4. REMOVE THE EXISTING AND CONSTRUCT THE PROPOSED SOUTH ABUTMENT. THIS WORK MUST BE COMPLETED PRIOR TO BEGINNING STAGE 1B.
5. BEGIN REMOVAL AND CONSTRUCTION OF PIER 2 AND THE NORTH ABUTMENT. REMOVAL AND CONSTRUCTION OF THE SIPHON SHALL BEGIN IN CONJUNCTION WITH THE REMOVAL OF THE NORTHEAST WINGWALL AND THE NORTH ABUTMENT. SEE THE SIPHON PLANS FOR DETAILS.
6. REFER TO THE CTA FLAGGING AND COORDINATION SPECIAL PROVISION FOR REQUIREMENTS WORKING ADJACENT TO OR ABOVE THE CTA RIGHT-OF-WAY.

CTA STATION CONSTRUCTION

1. CONTRACTOR SHALL WORK WITH THE CTA TO IDENTIFY ALL EXISTING UTILITIES WITHIN THE WORK AREA.
2. REMOVE THE CTA EQUIPMENT IN THE EXISTING BRICK HOUSE BUILDING ON THE BRIDGE PRIOR TO DEMOLITION. COORDINATE WITH THE CTA REGARDING CLOSING OF THE STATION AND STORING OF THE EQUIPMENT.
3. REMOVE THE EXISTING BRICK HOUSE BUILDING ON THE BRIDGE.
4. RELOCATE THE EXISTING ELECTRICAL AND COMMUNICATIONS CONDUITS MOUNTED TO THE UNDERSIDE OF THE PLATFORM AS SHOWN ON THE PLANS.

STAGE 1B

MAINTENANCE OF TRAFFIC AND CONSTRUCTION ALONG EB I-290 AND RAMPES ES & EN

1. MAINTAIN A MINIMUM OF 2 LANES OF TRAFFIC THROUGH TO CONGRESS PARKWAY AT ALL TIMES AS SHOWN ON THE PLANS.
2. MAINTAIN A MINIMUM OF 1 LANE OF TRAFFIC TO RAMP EN AND 2 LANES OF TRAFFIC TO RAMP ES AS SHOWN ON THE PLANS.
3. ALL TRAVEL LANES ON THE EAST END SHALL TIE INTO THE CURRENT STAGE OF TRAFFIC CONTROL FOR THE HALSTED STREET/HARRISON STREET PROJECT CONTRACT NO. 60W26 AND CONFORM TO ALL APPLICABLE IDOT DISTRICT 1 STANDARDS.
4. SHIFT THE LANES OF TRAFFIC SOUTH ONTO THE TEMPORARY PAVEMENT THAT WAS CONSTRUCTED IN STAGE 1A.
5. RELOCATE THE TEMPORARY CONCRETE BARRIER THAT WAS ALONG THE OUTSIDE (RIGHT) SHOULDER OF EB I-290 IN STAGE 1A TO THE LEFT EDGE OF TRAVELED WAY AS SHOWN ON THE PLANS.
6. REMOVE THE EXISTING PIER 1, LEAVING IN PLACE THE PORTION OF THE EXISTING FOOTING UNDER THE CTA TRACKS. SEE THE BRIDGE CONSTRUCTION NOTES BELOW AND REFER TO THE STRUCTURAL PLANS FOR ADDITIONAL DETAILS.
7. REMOVE THE EXISTING CONCRETE BARRIER WALL ALONG THE INSIDE (LEFT) SHOULDER.
8. CONSTRUCT THE TEMPORARY PAVEMENT (TO REMAIN) AND CONCRETE BARRIER WALL AS SHOWN ON THE ROADWAY PLANS.

MAINTENANCE OF TRAFFIC AND CONSTRUCTION ALONG WB I-290 AND RAMPES NW & SW

1. MAINTAIN THE TRAFFIC CONTROL FROM STAGE 1A.

MAINTENANCE OF TRAFFIC AND CONSTRUCTION ALONG LOCAL ROADS

1. INSTALL THE TRAFFIC CONTROL AND EROSION AND SEDIMENTATION CONTROL ALONG PEORIA STREET AS SHOWN ON THE PLANS.
2. THE ENTRANCE TO THE PARKING LOT ON THE EAST SIDE OF PEORIA STREET, NORTH OF I-290 WILL BE CLOSED DURING CONSTRUCTION. ACCESS TO THE PARKING LOT WILL BE MAINTAINED VIA THE ALLEY OFF OF VAN BUREN STREET.
3. THE ALLEY ADJACENT TO CUPPA HALL SHALL REMAIN OPEN AT ALL TIMES EXCEPT FOR WHEN THE ALLEY ENTRANCE AND PROPOSED CUL-DE-SAC WILL BE RECONSTRUCTED DURING WHICH THE ACCESS WILL BE MAINTAINED VIA THE ALLEY ENTRANCE ON SANGAMON STREET. THE CONTRACTOR SHALL NOTIFY THE UNIVERSITY OF ILLINOIS AT CHICAGO AT LEAST THREE WEEKS PRIOR TO CLOSING ACCESS TO THE ALLEY.
4. MAINTAIN THE PEDESTRIAN DETOUR FROM STAGE 1A
5. COMPLETE RECONSTRUCTION OF THE SIPHON ALONG PEORIA STREET.
6. COMPLETE CONSTRUCTION OF THE NORTH END OF PEORIA STREET, INCLUDING ALL DRAINAGE, EARTHWORK, LIGHTING AND LANDSCAPING ACTIVITIES. VEHICLE ACCESS TO PEORIA STREET FROM THE NORTH WILL BE RESTRICTED AS SHOWN ON THE PLANS.
7. CONSTRUCT THE STABILIZED CONSTRUCTION ENTRANCE TO ALLOW CONSTRUCTION ACCESS TO THE PEORIA STREET BRIDGE VIA HARRISON STREET.
8. CONTINUE CONSTRUCTION OF THE SOUTH END OF PEORIA STREET. CONSTRUCTION WILL CONTINUE THROUGH STAGE 2.
9. CONSTRUCT THE TEMPORARY SIDEWALK CONNECTION FROM THE BRIDGE TO THE EXISTING EAST SIDEWALK ON THE SOUTH END OF PEORIA STREET, INCLUDING THE TEMPORARY LIGHTING AS SHOWN ON THE PLANS. THIS TEMPORARY SIDEWALK CONNECTION WILL BE UTILIZED IN STAGE 2 TO PROVIDE PEDESTRIAN ACCESS ACROSS THE BRIDGE.

BRIDGE CONSTRUCTION

1. REMOVE THE EXISTING AND CONSTRUCT THE PROPOSED PIER 1. THIS WORK CANNOT BE STARTED UNTIL THE PROPOSED SOUTH ABUTMENT, CONCRETE BARRIER WALL AND TEMPORARY PAVEMENT WORK IS COMPLETED IN STAGE 1A.
2. COMPLETE REMOVAL AND CONSTRUCTION OF PIER 2 AND THE NORTH ABUTMENT.
3. COMPLETE CONSTRUCTION OF THE PROPOSED SUPERSTRUCTURE, INCLUDING BRIDGE RAILINGS, BRIDGE LIGHTING AND THE BRIDGE DECK LATEX CONCRETE OVERLAY.
4. REMOVE THE TEMPORARY SHORING AND RE-ATTACH THE CTA STATION ON THE WEST SIDE OF THE BRIDGE AND THE STAIRS ON THE EAST SIDE OF THE BRIDGE TO THE NEW FASCIA BEAMS.
5. REFER TO THE CTA FLAGGING AND COORDINATION SPECIAL PROVISION FOR REQUIREMENTS WORKING ADJACENT TO OR ABOVE THE CTA RIGHT-OF-WAY.

CTA STATION CONSTRUCTION

1. REMOVE THE CTA PA SPEAKERS, CCTV CAMERAS, COMMUNICATION SIGNAGE AND TELEPHONE EQUIPMENT AS SHOWN ON THE PLANS.
2. PROVIDE TEMPORARY CONDUITS AND CABLING FOR THE EXISTING ELECTRICAL, LIGHTING, CCTV, PA SYSTEM AND COMMUNICATION SYSTEMS MOUNTED TO THE UNDERSIDE OF THE PLATFORM AS SHOWN ON THE PLANS. THE PLATFORM LEVEL SYSTEMS SHALL BE MAINTAINED AT ALL TIMES.
- 3.

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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN
NARRATIVE

SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	46
CONTRACT NO. 60W29			ILLINOIS FED. AID PROJECT	

STAGE 2

MAINTENANCE OF TRAFFIC AND CONSTRUCTION ALONG EB I-290 AND RAMPS ES & EN

1. OPEN ALL LANES OF TRAFFIC TO THE PROPOSED LANE CONFIGURATION.

MAINTENANCE OF TRAFFIC AND CONSTRUCTION ALONG WB I-290 AND RAMPS NW & SW

1. OPEN ALL LANES OF TRAFFIC TO THE PROPOSED LANE CONFIGURATION.

MAINTENANCE OF TRAFFIC AND CONSTRUCTION ALONG LOCAL ROADS

1. INSTALL THE TRAFFIC CONTROL AND EROSION AND SEDIMENTATION CONTROL ALONG PEORIA STREET AS SHOWN ON THE PLANS.
2. OPEN THE BRIDGE TO PEDESTRIANS. PROHIBIT ACCESS TO THE CTA STATION ON THE WEST SIDE OF THE BRIDGE AS SHOWN ON THE PLANS. THE CTA STATION EXIT ONLY ACCESS ON THE EAST SIDE OF THE BRIDGE WILL BE REOPENED.
3. PEDESTRIAN ACCESS ACROSS THE BRIDGE WILL UTILIZE THE TEMPORARY SIDEWALK CONNECTION FROM THE BRIDGE TO THE EXISTING EAST SIDEWALK ON THE SOUTH END OF PEORIA STREET.
4. COMPLETE CONSTRUCTION OF THE SIDEWALK ON THE SOUTH END OF PEORIA STREET, INCLUDING ALL DRAINAGE, EARTHWORK, LIGHTING AND LANDSCAPING ACTIVITIES. THE SIDEWALK WILL BE OPENED TO PEDESTRIANS IN STAGE 3.

CTA STATION CONSTRUCTION

1. REHABILITATE THE CTA STATION GLASS BUILDING. SEE THE CTA STATION PLANS FOR DETAILS.
2. REMOVE AND REPLACE THE EXISTING CTA STATION PLATFORM, CANOPY AND STAIRS FOR THE NEW ELEVATOR AND STATION HOUSE EXTENSION. SEE THE CTA STATION PLANS FOR DETAILS.
3. ALL WORK SHALL BE SEQUENCED TO ALLOW THE PLATFORM TO REMAIN OPEN DURING CTA PEAK OPERATING HOURS. ALL WORK SHALL REQUIRE THE INSTALLATION OF TEMPORARY AND PERMANENT BARRICADES AND SIGNAGE TO RESTRICT ACCESS DURING WORK OPERATIONS AND SHALL FOLLOW THE CITY OF CHICAGO ADA REQUIREMENTS. SEE APPENDIX C OF THE SPECIAL PROVISIONS FOR MAINTENANCE OF TRANSIT OPERATIONS.

STAGE 3

MAINTENANCE OF TRAFFIC AND CONSTRUCTION ALONG EB I-290 AND RAMPS ES & EN

1. ALL LANES OF TRAFFIC WERE OPENED TO THE PROPOSED LANE CONFIGURATION IN STAGE 2.

MAINTENANCE OF TRAFFIC AND CONSTRUCTION ALONG WB I-290 AND RAMPS NW & SW

1. ALL LANES OF TRAFFIC WERE OPENED TO THE PROPOSED LANE CONFIGURATION IN STAGE 2.

MAINTENANCE OF TRAFFIC AND CONSTRUCTION ALONG LOCAL ROADS

1. INSTALL THE TRAFFIC CONTROL AND EROSION AND SEDIMENTATION CONTROL ALONG PEORIA STREET AS SHOWN ON THE PLANS.
2. REMOVE THE EXISTING EAST SIDEWALK ON THE SOUTH END OF PEORIA STREET AND COMPLETE ALL EARTHWORK AND LANDSCAPING ACTIVITIES.
3. RECONSTRUCT THE SECTIONS OF SIDEWALK AND CURB AND GUTTER ALONG HARRISON STREET AS SHOWN ON THE PLANS. TEMPORARILY CLOSE THE OUTSIDE LANE ALONG WESTBOUND HARRISON STREET ACCORDING TO HIGHWAY STANDARD 701601 WHEN PERFORMING THE WORK. THIS WORK SHALL BE PERFORMED ONLY DURING THE WEEKDAY HOURS OF 9:00 AM AND 3:00 PM, AFTER WHICH THE LANE SHALL BE IMMEDIATELY REOPENED TO TRAFFIC. THE CONTRACTOR SHALL COORDINATE WITH THE CITY OF CHICAGO AT LEAST THREE WEEKS PRIOR TO BEGINNING WORK WHICH WOULD REQUIRE A LANE CLOSURE.

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PLOT DATE = 12/19/2013	DATE - 10/30/2013	REVISED -

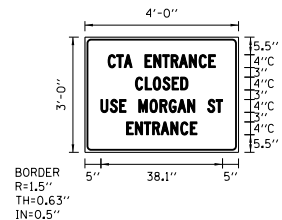
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN
NARRATIVE**

SCALE: NONE SHEET 2 OF 2 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-01R	COOK	356	47
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60W29	

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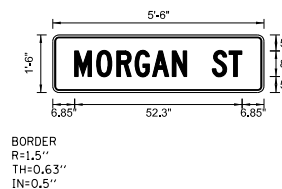
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Dimensions are in inches.tenths
Letter locations are panel edge to lower left corner M.U.T.C.D.: 2009 Edition

SIGN NUMBER	Temp Information Sign 1
WIDTH x HGHT.	4'-0" x 3'-0"
BORDER WIDTH	0.63"
CORNER RADIUS	1.5"
MOUNTING	Ground
BACKGROUND	TYPE: Reflective COLOR: Orange
LEGEND/BORDER	TYPE: Reflective COLOR: Black

SYMBOL	ROT	X	Y	WID	HT

LETTER POSITIONS (X)												LENGTH	SERIES/SIZE	
C	T	A	E	N	T	R	A	N	C	E				C 2000
7.2	10	12.2	14.8	18.8	21.5	24.3	26.9	29.5	32.6	35.7	38.7			4
C	L	O	S	E	D									C 2000
15.8	18.8	21.4	24.3	27.2	30									4
U	S	E	M	O	R	G	A	N	S	T				C 2000
5	7.9	10.8	12.9	16.9	20.3	23.5	26.3	29	32.1	34.4	38.4	41		4
E	N	T	R	A	N	C	E							C 2000
13	15.7	18.5	21.1	23.7	26.9	29.9	32.9							4

SIGN DETAIL
1:50



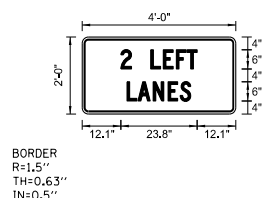
Panel Style: construction_guide.ssi
Dimensions are in inches.tenths
Letter locations are panel edge to lower left corner M.U.T.C.D.: 2009 Edition

SIGN NUMBER	Temp Information Sign 2
WIDTH x HGHT.	5'-6" x 1'-6"
BORDER WIDTH	0.63"
CORNER RADIUS	1.5"
MOUNTING	Ground
BACKGROUND	TYPE: Reflective COLOR: Orange
LEGEND/BORDER	TYPE: Reflective COLOR: Black

SYMBOL	ROT	X	Y	WID	HT

LETTER POSITIONS (X)												LENGTH	SERIES/SIZE	
M	O	R	G	A	N	S	T							C 2000
6.8	13.7	20	25.7	31.2	37.4	41.9	49.9	55.1						8

SIGN DETAIL
1:50



Panel Style: construction_guide.ssi
Dimensions are in inches.tenths
Letter locations are panel edge to lower left corner M.U.T.C.D.: 2009 Edition

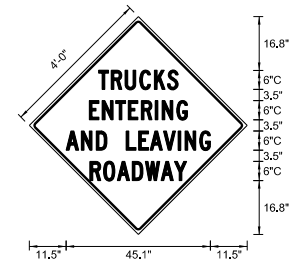
SIGN NUMBER	Temp Information Sign 3
WIDTH x HGHT.	4'-0" x 2'-0"
BORDER WIDTH	0.63"
CORNER RADIUS	1.5"
MOUNTING	Ground
BACKGROUND	TYPE: Reflective COLOR: Orange
LEGEND/BORDER	TYPE: Reflective COLOR: Black

SYMBOL	ROT	X	Y	WID	HT

LETTER POSITIONS (X)												LENGTH	SERIES/SIZE	
2	L	E	F	T										C 2000
12.1	15.5	21.5	25.4	29.4	32.9									6
L	A	N	E	S										C 2000
14.1	17.5	22.2	26.8	30.6										6

NOTES:
1. THE SIGNS SHOWN SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

SIGN DETAIL
1:50



Panel Style: construction_warning.ssi
Dimensions are in inches.tenths
Letter locations are panel edge to lower left corner M.U.T.C.D.: 2009 Edition

SIGN NUMBER	Temp Information Sign 4
WIDTH x HGHT.	5'-8" x 5'-8"
BORDER WIDTH	1.25"
CORNER RADIUS	3"
MOUNTING	Ground
BACKGROUND	TYPE: Reflective COLOR: Orange
LEGEND/BORDER	TYPE: Reflective COLOR: Black

SYMBOL	ROT	X	Y	WID	HT

LETTER POSITIONS (X)												LENGTH	SERIES/SIZE	
T	R	U	C	K	S									C 2000
21.7	25.6	29.9	34.5	39.1	43.1									6
E	N	T	E	R	I	N	G							C 2000
18.7	22.8	27	30.9	35	39.4	41.5	46.1							6

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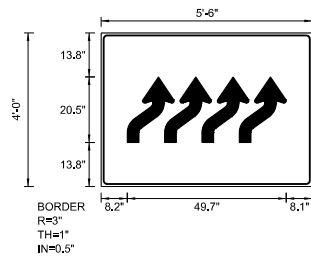
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PLOT DATE = 12/19/2013	DATE - 10/30/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN TEMPORARY INFORMATION SIGNS			
SCALE: NONE	SHEET 1 OF 1 SHEETS	STA. TO STA.	

F.A.I. RTE. 90/94/290	SECTION 2013-011R	COUNTY COOK	TOTAL SHEETS 356	SHEET NO. 48
CONTRACT NO. 60W29				ILLINOIS FED. AID PROJECT

SIGN DETAIL
1:50



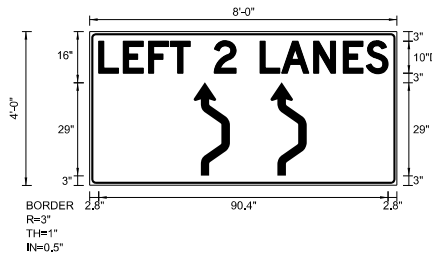
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Dimensions are in inches.tenths
Letter locations are panel edge to lower left corner M.U.T.C.D.: 2009 Edition

SIGN NUMBER	Temp Information Sign 5
WIDTH x HGHT.	5'-6" x 4'-0"
BORDER WIDTH	1"
CORNER RADIUS	3"
MOUNTING	Ground
BACKGROUND	TYPE: Reflective COLOR: Orange
LEGEND/BORDER	TYPE: Reflective COLOR: Black

SYMBOL	ROT	X	Y	WID	HT
W1_4cR	0	8.2	13.8	14.7	20.5
W1_4cR	0	19.8	13.8	14.7	20.5
W1_4cR	0	31.5	13.8	14.7	20.5
W1_4cR	0	43.2	13.8	14.7	20.5

LETTER POSITIONS (X)										LENGTH	SERIES/SIZE

SIGN DETAIL
1:50



Panel Style: construction_guide.ssi
Dimensions are in inches.tenths
Letter locations are panel edge to lower left corner M.U.T.C.D.: 2009 Edition

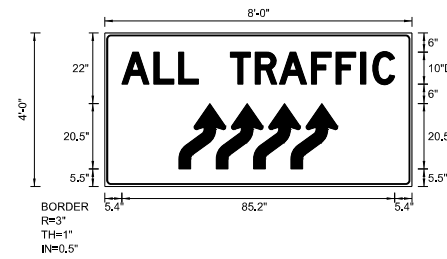
SIGN NUMBER	Temp Information Sign 7
WIDTH x HGHT.	8'-0" x 4'-0"
BORDER WIDTH	1"
CORNER RADIUS	3"
MOUNTING	Ground
BACKGROUND	TYPE: Reflective COLOR: Orange
LEGEND/BORDER	TYPE: Reflective COLOR: Black

SYMBOL	ROT	X	Y	WID	HT
W24_1bR	0	32.5	3	11.3	29
W24_1bR	0	56.5	3	11.3	29

LETTER POSITIONS (X)										LENGTH	SERIES/SIZE
L	E	F	T	2	L	A	N	E	S		D 2000
2.8	10.5	18.4	25.2	38.9	53.2	60	70	79.2	86.4	90.4	10



SIGN DETAIL
1:50



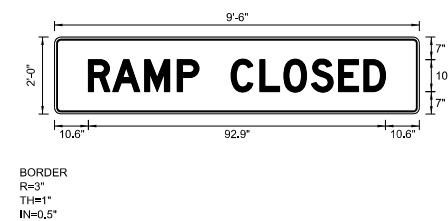
Panel Style: construction_guide.ssi
Dimensions are in inches.tenths
Letter locations are panel edge to lower left corner M.U.T.C.D.: 2009 Edition

SIGN NUMBER	Temp Information Sign 6
WIDTH x HGHT.	8'-0" x 4'-0"
BORDER WIDTH	1"
CORNER RADIUS	3"
MOUNTING	Ground
BACKGROUND	TYPE: Reflective COLOR: Orange
LEGEND/BORDER	TYPE: Reflective COLOR: Black

SYMBOL	ROT	X	Y	WID	HT
W1_4cR	0	23.2	5.5	14.7	20.5
W1_4cR	0	34.8	5.5	14.7	20.5
W1_4cR	0	46.5	5.5	14.7	20.5
W1_4cR	0	58.2	5.5	14.7	20.5

LETTER POSITIONS (X)										LENGTH	SERIES/SIZE	
A	L	L		T	R	A	F	F	I	C		D 2000
5.4	15.4	23.1	29.3	39.3	47	54.6	64.6	72.3	80	83.8	85.2	10

SIGN DETAIL
1:50



Panel Style: construction_guide.ssi
Dimensions are in inches.tenths
Letter locations are panel edge to lower left corner M.U.T.C.D.: 2009 Edition

SIGN NUMBER	Temp Information Sign 8
WIDTH x HGHT.	9'-6" x 2'-0"
BORDER WIDTH	1"
CORNER RADIUS	3"
MOUNTING	Overhead
BACKGROUND	TYPE: Reflective COLOR: Orange
LEGEND/BORDER	TYPE: Reflective COLOR: Black

SYMBOL	ROT	X	Y	WID	HT

LETTER POSITIONS (X)										LENGTH	SERIES/SIZE	
R	A	M	P		C	L	O	S	E	D		D 2000
10.6	18.2	28.2	38.4	45.2	55.2	64.2	71.7	80.3	88.8	96.7	92.9	10

NOTES:
1. THE SIGNS SHOWN SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

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PLOT DATE = 12/19/2013

DESIGNED - JDT	REVISED - 12/18/2013
DRAWN - JTR	REVISED -
CHECKED - JMG	REVISED -
DATE - 10/30/2013	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN
TEMPORARY INFORMATION SIGNS

SCALE: NONE SHEET 1A OF 1 SHEETS STA. TO STA.

F.A.I. RTE. 90/94/290	SECTION 2013-011R	COUNTY COOK	TOTAL SHEETS 356	SHEET NO. 48A
CONTRACT NO. 60W29			ILLINOIS FED. AID PROJECT	

SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN SCHEDULE

LOCATION	STATION	TO	STATION	TEMPORARY CONCRETE BARRIER FOOT	RELOCATE TEMPORARY CONCRETE BARRIER FOOT	BARRIER WALL MARKERS, TYPE C EACH	IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3 EACH	IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3 EACH	TEMPORARY EPOXY PAVEMENT MARKING - LINE 4" FOOT	TEMPORARY EPOXY PAVEMENT MARKING - LINE 5" FOOT	TEMPORARY EPOXY PAVEMENT MARKING - LINE 8" FOOT	TEMPORARY EPOXY PAVEMENT MARKING - LINE 12" FOOT	WORK ZONE PAVEMENT MARKING REMOVAL SQ FT	RAISED REFLECTIVE PAVEMENT MARKER, REFLECTOR REMOVAL EACH	REPLACEMENT REFLECTOR EACH	TEMPORARY INFORMATION SIGNING SQ FT	WOOD SIGN SUPPORT FOOT	STORM SEWERS, CLASS A, TYPE 1 12" FOOT	CATCH BASINS, TYPE A, 4-DIAMETER, TYPE 1 FRAME, OPEN LID EACH	REMOVE SIGN PANEL - TYPE 3 SQ FT	RELOCATE SIGN PANEL - TYPE 3 SQ FT	REMOVE GROUND MOUNTED SIGN SUPPORT EACH
TRAFFIC CONTROL GENERAL NOTES																		100	5			
PEORIA STREET STAGE 1A	3700+00.00	TO	3710+00.00																			
MAINLINE STAGE 1A																						
EB I-290	308+00.00	TO	353+00.00	463	37	1			1,406	225	2,199	8	1,047	270		186.75						
WB I-290	308+00.00	TO	353+00.00	875	70	1			1,281	622	3,143			117		41.25	45			181	181	2
PEORIA STREET STAGE 1B	3700+00.00	TO	3710+00.00																			
MAINLINE STAGE 1B																						
EB I-290	308+00.00	TO	353+00.00	88	463	7		1	3,337	1,556	7,003	33	1,441		270	108						
WB I-290	308+00.00	TO	353+00.00						1,281	622	3,143		1,391		117	19						
PEORIA STREET STAGE 2	3700+00.00	TO	3710+00.00																			
PEORIA STREET STAGE 4	3700+00.00	TO	3710+00.00																			
TOTAL				1,463	463	117	2	1	7,305	3,026	15,489	41	3,880	387	387	443	45	100	5	181	181	2

NOTES:
1. ADDITIONAL BARRIER WALL MARKERS, TYPE C QUANTITY PROVIDED ON THE ROADWAY SCHEDULE

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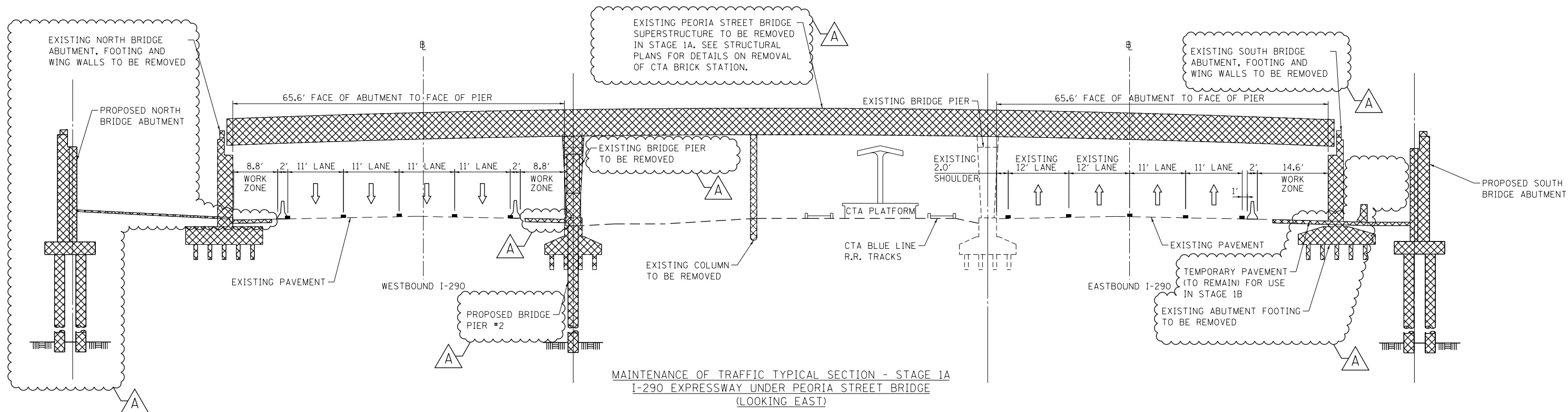
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN SCHEDULE

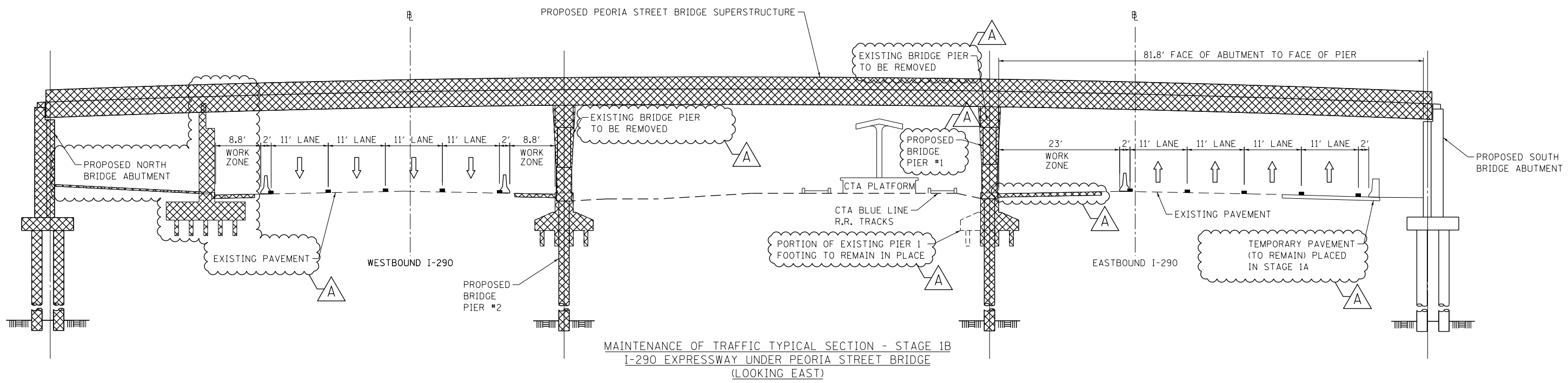
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F.A.I. RTE. 90/94/290	SECTION 2013-011R	COUNTY COOK	TOTAL SHEETS 356	SHEET NO. 49
CONTRACT NO. 60W29			ILLINOIS FED. AID PROJECT	

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MAINTENANCE OF TRAFFIC TYPICAL SECTION - STAGE 1A
I-290 EXPRESSWAY UNDER PEORIA STREET BRIDGE
(LOOKING EAST)



MAINTENANCE OF TRAFFIC TYPICAL SECTION - STAGE 1B
I-290 EXPRESSWAY UNDER PEORIA STREET BRIDGE
(LOOKING EAST)



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

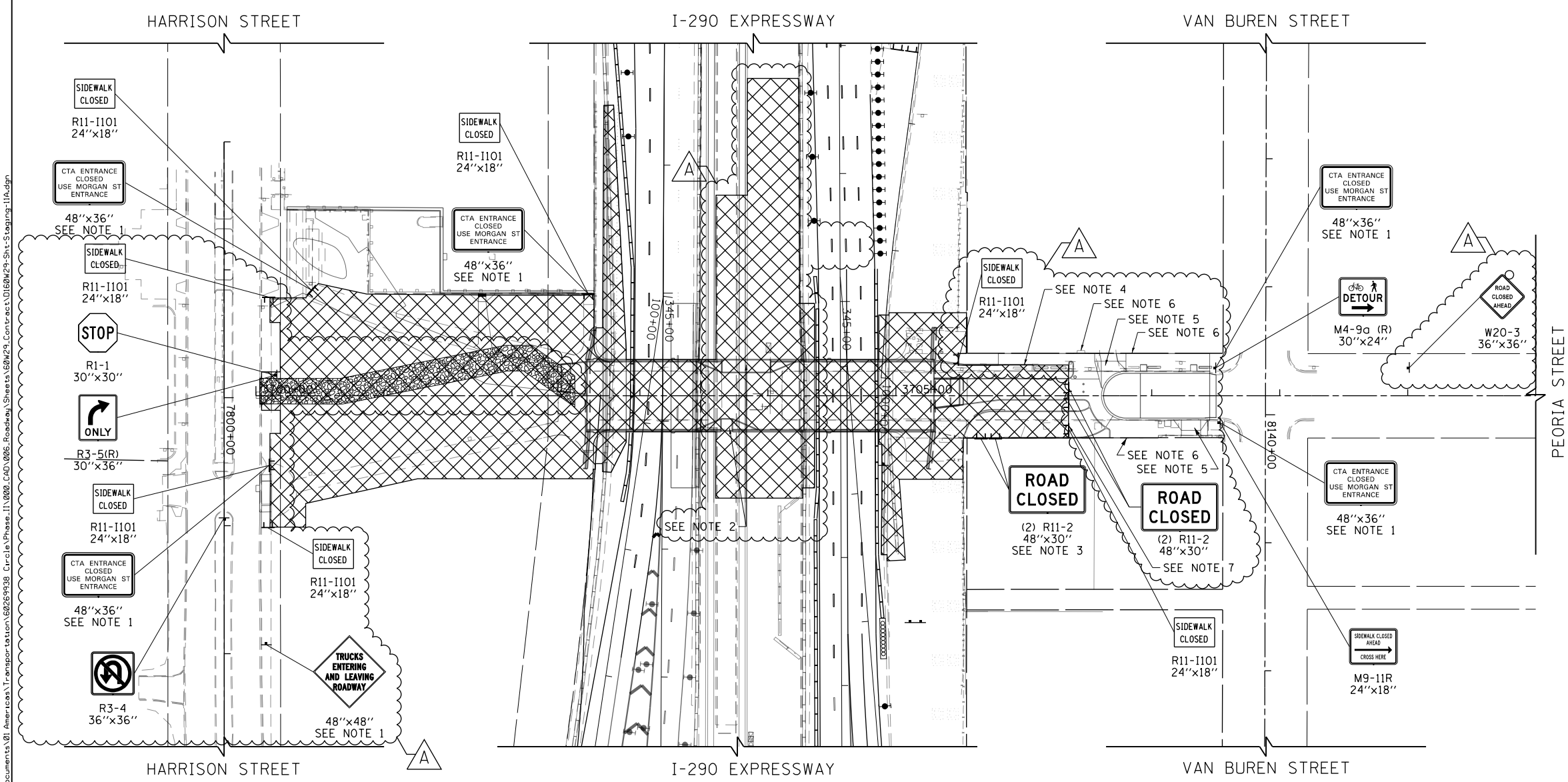
SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN			
TYPICAL SECTIONS			
SCALE: 1"=50'	SHEET 1	OF 1 SHEETS	STA. TO STA.

F.A.I. RTE. 90/94/290	SECTION 2013-011R	COUNTY COOK	TOTAL SHEETS 356	SHEET NO. 50
CONTRACT NO. 60W29				
ILLINOIS FED. AID PROJECT				



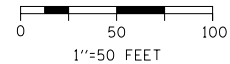
LEGEND

- WORK ZONE
- TEMPORARY IMPACT ATTENUATOR
- TEMPORARY CONCRETE BARRIER
- DIRECTION OF TRAFFIC FLOW
- SIGN
- ARROW BOARD
- TYPE III BARRICADE
- TYPE II BARRICADE OR DRUM WITH FLASHING LIGHT
- STABILIZED CONSTRUCTION ENTRANCE



NOTES:

1. SIGN PANEL TO BE PAID FOR AS "TEMPORARY INFORMATION SIGNING".
2. ACCESS TO THE CTA STATION AND PLATFORM WILL BE CLOSED DURING CONSTRUCTION OF THE BRIDGE. ACCESS TO THE BRIDGE VIA THE EAST STAIRCASE FROM THE CTA PLATFORM WILL BE REINSTATED IN STAGE 2. ACCESS TO THE CTA STATION WILL BE REINSTATED IN STAGE 3.
3. THE ENTRANCE TO THE PARKING LOT OFF OF PEORIA STREET WILL BE CLOSED DURING CONSTRUCTION. ACCESS TO THE PARKING LOT WILL BE MAINTAINED VIA THE ALLEY OFF OF VAN BUREN STREET.
4. THE EXISTING SCAFFOLDING MUST BE MAINTAINED IN PLACE DURING CONSTRUCTION UNTIL THE RENOVATIONS TO CUPPA HALL ARE COMPLETE. THE CONTRACTOR SHALL COORDINATE WITH THE UNIVERSITY OF ILLINOIS AT CHICAGO AT LEAST THREE WEEKS PRIOR TO BEGINNING WORK ADJACENT TO THE SCAFFOLDING.
5. THE ALLEY ADJACENT TO CUPPA HALL SHALL REMAIN OPEN AT ALL TIMES EXCEPT FOR WHEN THE ALLEY APRON AND PROPOSED CUL-DE-SAC WILL BE RECONSTRUCTED DURING WHICH TIME ACCESS WILL BE MAINTAINED VIA THE ALLEY ENTRANCE ON SANGAMON STREET. THE CONTRACTOR SHALL NOTIFY THE UNIVERSITY OF ILLINOIS AT CHICAGO AT LEAST THREE WEEKS PRIOR TO CLOSING ACCESS TO THE ALLEY.
6. PEDESTRIAN ACCESS TO BUILDING ENTRANCES SHALL BE MAINTAINED AT ALL TIMES DURING NORMAL WORKING HOURS IN ACCORDANCE WITH ADA AND APPLICABLE CODE REQUIREMENTS. THE CONTRACTOR SHALL NOTIFY THE BUILDING OWNERS AT LEAST THREE WEEKS PRIOR TO RECONSTRUCTING THE SIDEWALK ADJACENT TO EACH BUILDING ENTRANCE.
7. PEDESTRIAN ACCESS TO THE PARKING LOT SHALL BE MAINTAINED AT ALL TIMES.



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 DRAWN - JTR
 CHECKED - JMG
 DATE - 10/30/2013

REVISED - 12/18/2013
 REVISED -
 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN
 PEORIA STREET STAGE 1A**

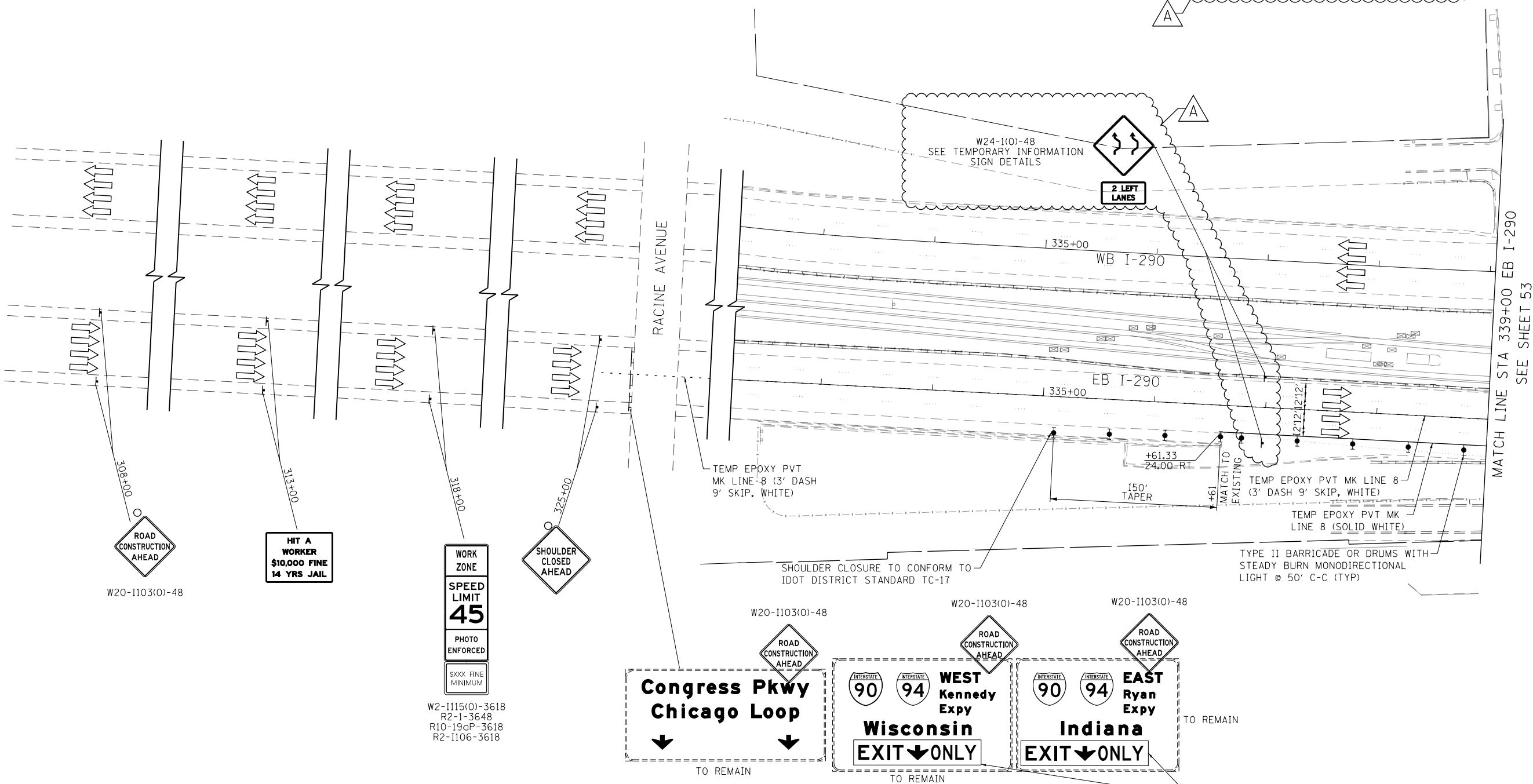
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	51
CONTRACT NO. 60W29				
ILLINOIS FED. AID PROJECT				

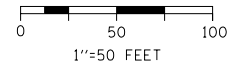
NOTES
1. FOR TEMPORARY INFORMATION SIGN DETAILS, SEE SHEETS 48 AND 48A

LEGEND

- WORK ZONE
- TEMPORARY IMPACT ATTENUATOR
- TEMPORARY CONCRETE BARRIER
- DIRECTION OF TRAFFIC FLOW
- SIGN
- ARROW BOARD
- TYPE III BARRICADE
- TYPE II BARRICADE OR DRUM WITH FLASHING LIGHT
- STABILIZED CONSTRUCTION ENTRANCE



INSTALL BLACK ON ORANGE TEMPORARY PANEL ONTO EXISTING SIGN FACE (TO BE PAID FOR AS TEMPORARY INFORMATION SIGNING)



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PLOT DATE = 12/19/2013	DATE - 10/30/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN
I-290 STAGE 1A**
SCALE: 1"=50' SHEET 2 OF 3 SHEETS STA. 308+00 TO STA. 339+00

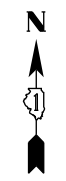
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	52
			CONTRACT NO. 60W29	
ILLINOIS FED. AID PROJECT				

NOTES

1. REMOVE EXISTING OVERHEAD SIGN STRUCTURE BRIDGE MOUNTED (WB-01-BM). RELOCATE EXISTING SIGN PANEL TO (STA 347+51, 58' LT). SIGN PANEL SHALL BE PLACED ON WOOD SIGN SUPPORTS. SIGN SHALL REMAIN FOR THE DURATION OF CONSTRUCTION OR UNTIL THE PROPOSED BRIDGE MOUNTED SIGN (WB-02-BM) IS INSTALLED. REFER TO SIGNING PLANS FOR DETAILS.
2. REMOVE EXISTING OVERHEAD SIGN STRUCTURE BRIDGE MOUNTED (EB-03-BM AND EB-04-BM) AFTER PROPOSED SIGNS (EB-01-BM AND EB-02-BM) HAVE BEEN PLACED IN PROPOSED LOCATION. REFER TO SIGNING PLANS FOR DETAILS.
3. TEMPORARY TRAFFIC CONTROL PAVEMENT MARKINGS SHALL MATCH TO THE CURRENT STAGE OF TRAFFIC CONTROL FOR CONTRACT NUMBER 60W26. FURTHER COORDINATION OF TEMPORARY MARKINGS AND DEVICES ALONG EASTBOUND AND WESTBOUND I-290 WILL BE REQUIRED.
4. THE STAGING WEAVE SHALL BE SIGNED AND MAINTAINED ACCORDING TO DISTRICT DETAIL TC-09. THE 2 DOUBLE REVERSE CURVES SIGN SHOWN REPLACES THE 4'x8' TEMPORARY INFORMATION SIGN IN TC-09 AT THE WEAVE POINT DUE TO SPACE RESTRICTIONS. IF THE RESIDENT ENGINEER DETERMINES THERE IS ENOUGH CLEARANCE BETWEEN THE RAIL AND TRAFFIC, TEMPORARY INFORMATION SIGN 7 SHALL BE USED IN PLACE OF THE SIGN CURRENTLY SHOWN IN THE PLANS. SEE SHEET 48A.

LEGEND

- WORK ZONE
- TEMPORARY IMPACT ATTENUATOR
- TEMPORARY CONCRETE BARRIER
- DIRECTION OF TRAFFIC FLOW
- SIGN
- ARROW BOARD
- TYPE III BARRICADE
- TYPE II BARRICADE OR DRUM WITH FLASHING LIGHT
- STABILIZED CONSTRUCTION ENTRANCE

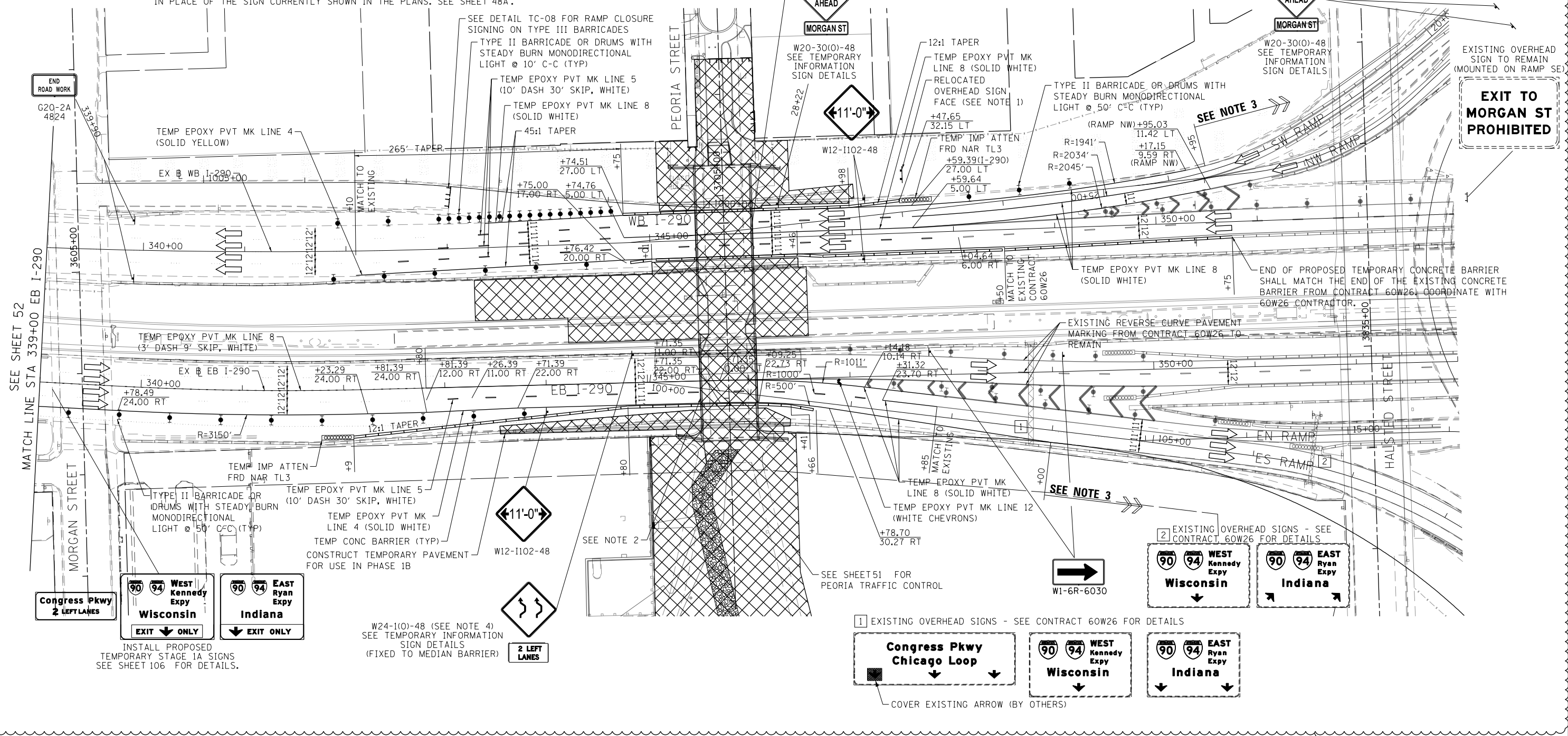


SEE NOTE 1
Exit 28B
Ashland Ave
Paulina St
3/4 MILE

RAMP CLOSED AHEAD
 STA. 12+75
 (SW RAMP)

MORGAN ST

W20-30(O)-48
 SEE TEMPORARY INFORMATION SIGN DETAILS



SEE SHEET 52
 MATCH LINE STA 339+00 EB I-290

END ROAD WORK
 G20-2A
 4824

EXIT TO MORGAN ST PROHIBITED

90 94 WEST Kennedy Expy Wisconsin
EXIT ONLY

90 94 EAST Ryan Expy Indiana
EXIT ONLY

INSTALL PROPOSED TEMPORARY STAGE 1A SIGNS SEE SHEET 106 FOR DETAILS.

W24-1(O)-48 (SEE NOTE 4)
 SEE TEMPORARY INFORMATION SIGN DETAILS (FIXED TO MEDIAN BARRIER)

2 LEFT LANES

EXISTING OVERHEAD SIGNS - SEE CONTRACT 60W26 FOR DETAILS

Congress Pkwy Chicago Loop

90 94 WEST Kennedy Expy Wisconsin

90 94 EAST Ryan Expy Indiana

COVER EXISTING ARROW (BY OTHERS)



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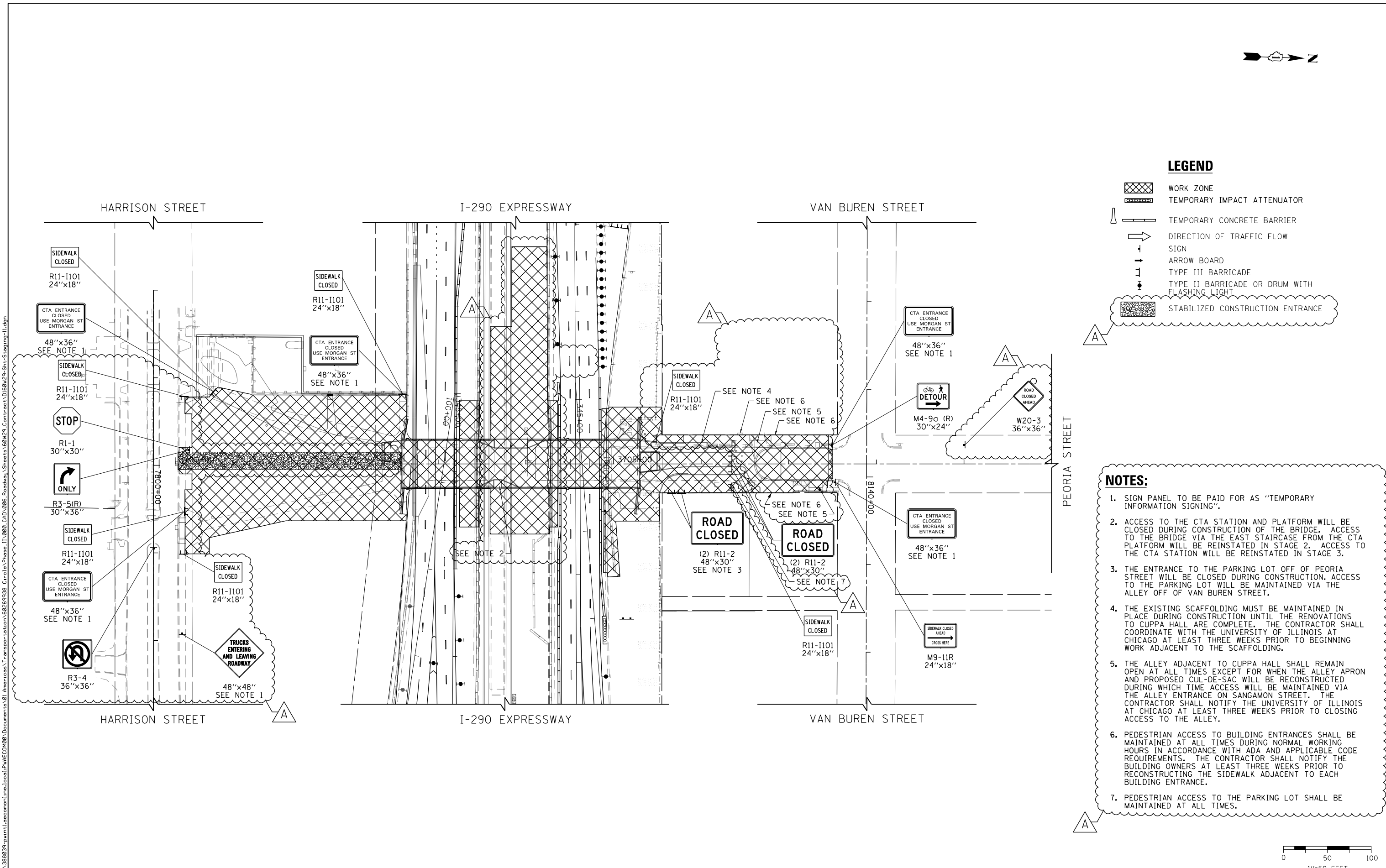
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN
I-290 STAGE 1A

SCALE: 1"=50' SHEET 3 OF 3 SHEETS STA. 339+00 TO STA. 353+00

F.A.I. RTE. 90/94/290	SECTION 2013-011R	COUNTY COOK	TOTAL SHEETS 356	SHEET NO. 53
CONTRACT NO. 60W29				ILLINOIS FED. AID PROJECT

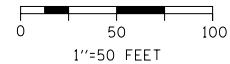


LEGEND

- WORK ZONE
- TEMPORARY IMPACT ATTENUATOR
- TEMPORARY CONCRETE BARRIER
- DIRECTION OF TRAFFIC FLOW
- SIGN
- ARROW BOARD
- TYPE III BARRICADE
- TYPE II BARRICADE OR DRUM WITH FLASHING LIGHT
- STABILIZED CONSTRUCTION ENTRANCE

NOTES:

1. SIGN PANEL TO BE PAID FOR AS "TEMPORARY INFORMATION SIGNING".
2. ACCESS TO THE CTA STATION AND PLATFORM WILL BE CLOSED DURING CONSTRUCTION OF THE BRIDGE. ACCESS TO THE BRIDGE VIA THE EAST STAIRCASE FROM THE CTA PLATFORM WILL BE REINSTATED IN STAGE 2. ACCESS TO THE CTA STATION WILL BE REINSTATED IN STAGE 3.
3. THE ENTRANCE TO THE PARKING LOT OFF OF PEORIA STREET WILL BE CLOSED DURING CONSTRUCTION. ACCESS TO THE PARKING LOT WILL BE MAINTAINED VIA THE ALLEY OFF OF VAN BUREN STREET.
4. THE EXISTING SCAFFOLDING MUST BE MAINTAINED IN PLACE DURING CONSTRUCTION UNTIL THE RENOVATIONS TO CUPPA HALL ARE COMPLETE. THE CONTRACTOR SHALL COORDINATE WITH THE UNIVERSITY OF ILLINOIS AT CHICAGO AT LEAST THREE WEEKS PRIOR TO BEGINNING WORK ADJACENT TO THE SCAFFOLDING.
5. THE ALLEY ADJACENT TO CUPPA HALL SHALL REMAIN OPEN AT ALL TIMES EXCEPT FOR WHEN THE ALLEY APRON AND PROPOSED CUL-DE-SAC WILL BE RECONSTRUCTED DURING WHICH TIME ACCESS WILL BE MAINTAINED VIA THE ALLEY ENTRANCE ON SANGAMON STREET. THE CONTRACTOR SHALL NOTIFY THE UNIVERSITY OF ILLINOIS AT CHICAGO AT LEAST THREE WEEKS PRIOR TO CLOSING ACCESS TO THE ALLEY.
6. PEDESTRIAN ACCESS TO BUILDING ENTRANCES SHALL BE MAINTAINED AT ALL TIMES DURING NORMAL WORKING HOURS IN ACCORDANCE WITH ADA AND APPLICABLE CODE REQUIREMENTS. THE CONTRACTOR SHALL NOTIFY THE BUILDING OWNERS AT LEAST THREE WEEKS PRIOR TO RECONSTRUCTING THE SIDEWALK ADJACENT TO EACH BUILDING ENTRANCE.
7. PEDESTRIAN ACCESS TO THE PARKING LOT SHALL BE MAINTAINED AT ALL TIMES.



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D160W29-Sht-Staging-11.dgn
 USER NAME = BAW1tor1
 PLOT SCALE = 100.0000' / in.
 PLOT DATE = 12/19/2013

DESIGNED - JDT
 DRAWN - JTR
 CHECKED - JMG
 DATE - 10/30/2013

REVISED - 12/18/2013
 REVISED -
 REVISED -
 REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN
 PEORIA STREET STAGE 1B**

SCALE: 1"=50' SHEET 1 OF 3 SHEETS STA. 3700+00 TO STA. 3710+00

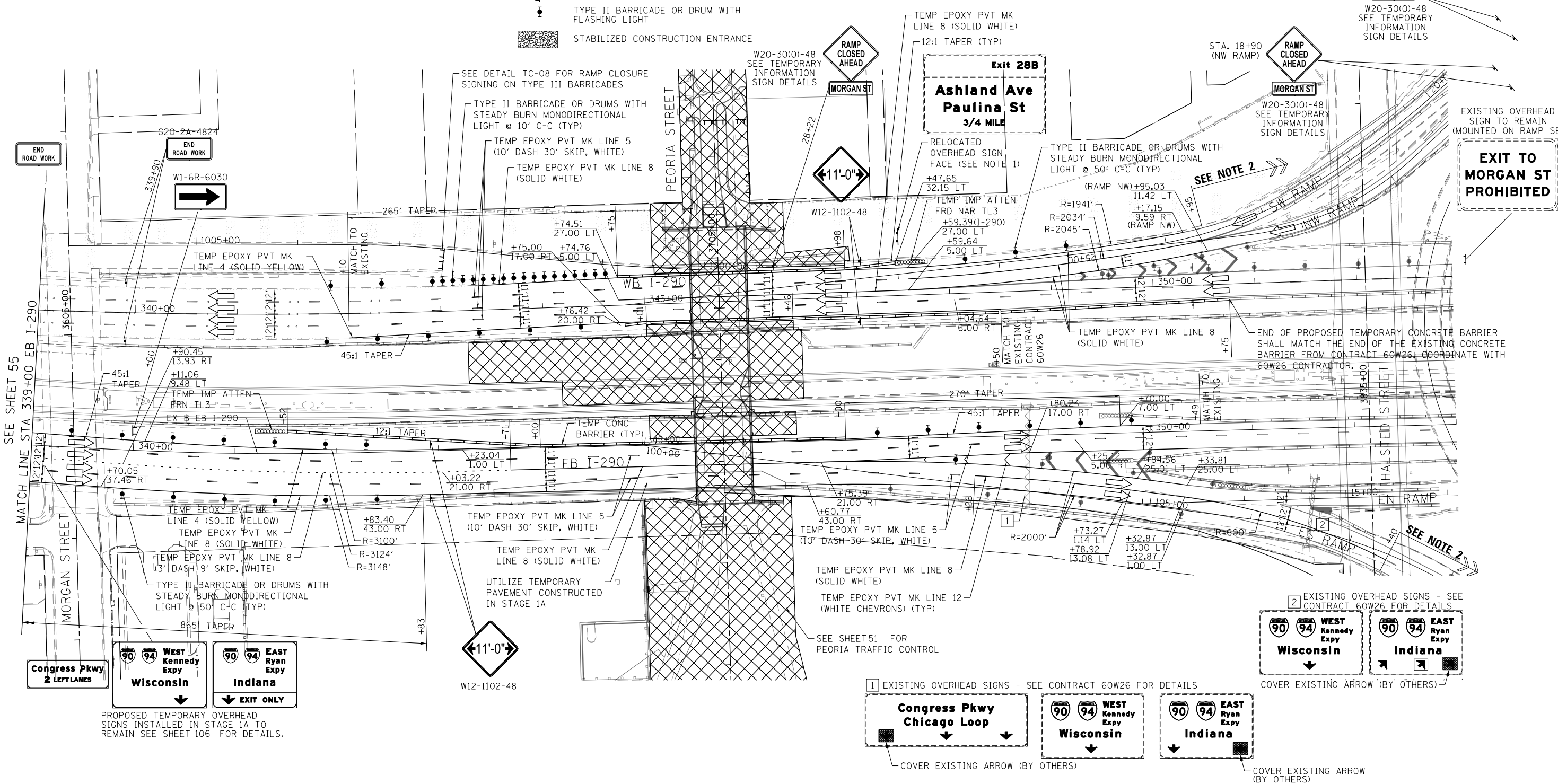
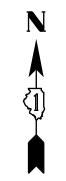
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	54
CONTRACT NO. 60W29			ILLINOIS FED. AID PROJECT	

NOTES

1. REMOVE EXISTING OVERHEAD SIGN STRUCTURE BRIDGE MOUNTED (WB-01-BM). RELOCATE EXISTING SIGN PANEL TO (STA 347+51, 58' LT). SIGN PANEL SHALL BE PLACED ON WOOD SIGN SUPPORTS. SIGN SHALL REMAIN FOR THE DURATION OF CONSTRUCTION OR UNTIL THE PROPOSED BRIDGE MOUNTED SIGN (WB-02-BM) IS INSTALLED. REFER TO SIGNING PLANS FOR DETAILS.
2. TEMPORARY TRAFFIC CONTROL PAVEMENT MARKINGS SHALL MATCH TO THE CURRENT STAGE OF TRAFFIC CONTROL FOR CONTRACT NUMBER 60W26. FURTHER COORDINATION OF TEMPORARY MARKINGS AND DEVICES ALONG EASTBOUND AND WESTBOUND I-290 WILL BE REQUIRED.

LEGEND

- WORK ZONE
- TEMPORARY IMPACT ATTENUATOR
- TEMPORARY CONCRETE BARRIER
- DIRECTION OF TRAFFIC FLOW
- SIGN
- ARROW BOARD
- TYPE III BARRICADE
- TYPE II BARRICADE OR DRUM WITH FLASHING LIGHT
- STABILIZED CONSTRUCTION ENTRANCE



SEE SHEET 55
MATCH LINE STA 339+00 EB I-290

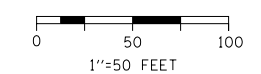
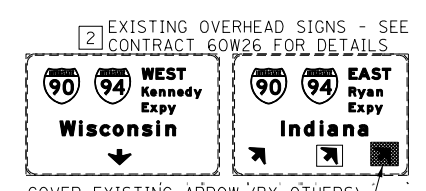
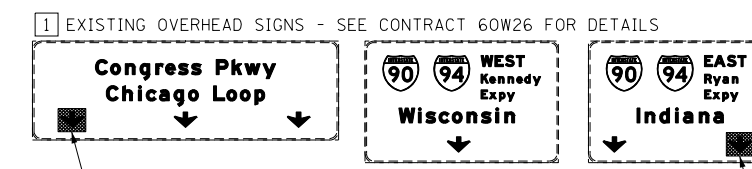
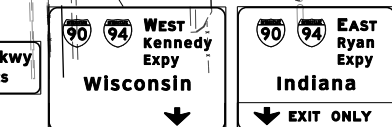
MORGAN STREET

PEORIA STREET

Exit 28B
**Ashland Ave
Paulina St
3/4 MILE**

MORGAN ST

EXISTING OVERHEAD SIGN TO REMAIN (MOUNTED ON RAMP SE)
EXIT TO MORGAN ST PROHIBITED



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PLOT DATE = 1/21/2014	DATE - 10/30/2013	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN
I-290 STAGE 1B
SCALE: 1"=50' SHEET 3 OF 3 SHEETS STA. 339+00 TO STA. 353+00

F.A.I. RTE. 90/94/290	SECTION 2013-011R	COUNTY COOK	TOTAL SHEETS 356	SHEET NO. 56
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60W29	

A

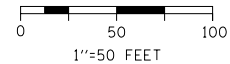
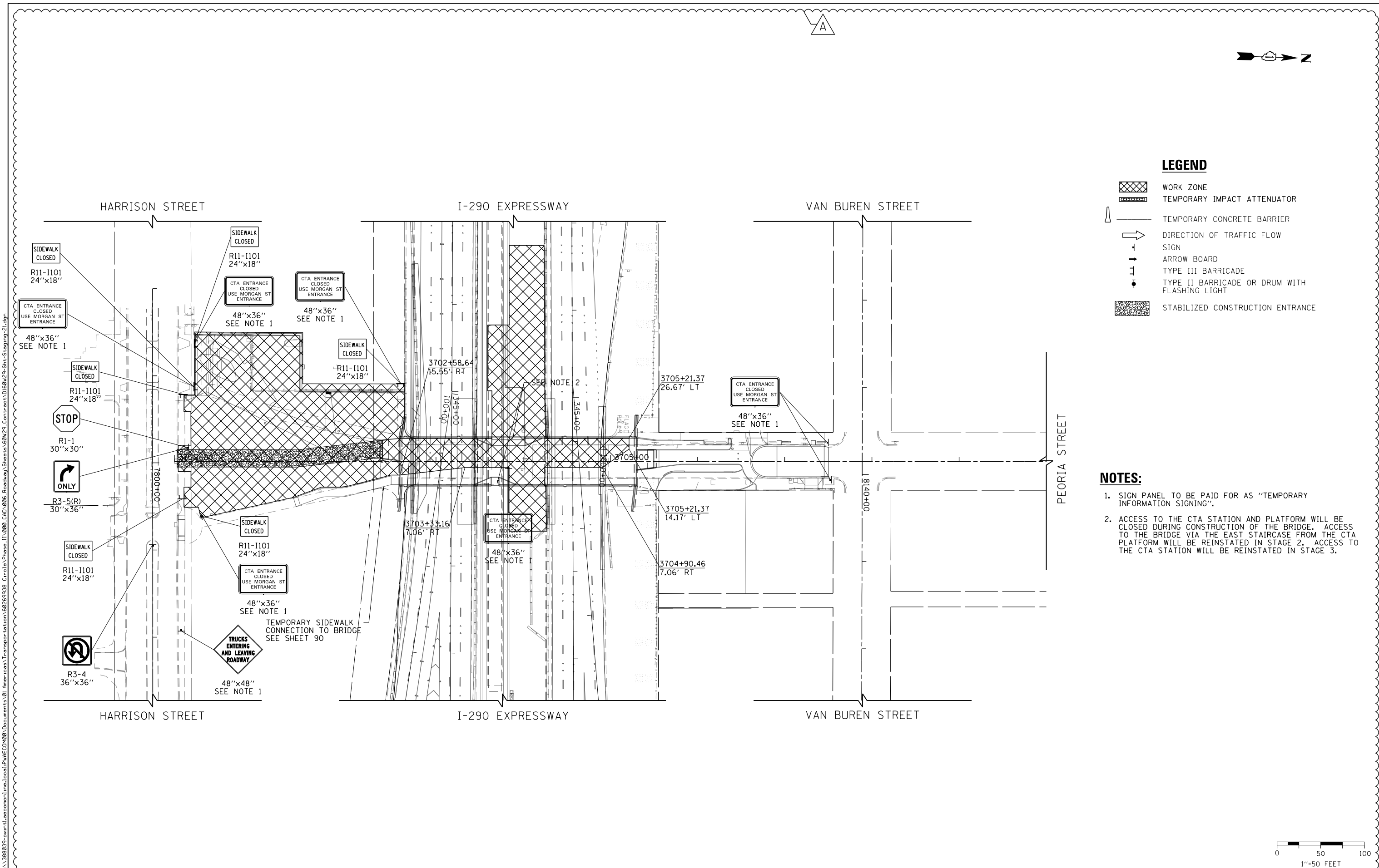


LEGEND

- WORK ZONE
- TEMPORARY IMPACT ATTENUATOR
- TEMPORARY CONCRETE BARRIER
- DIRECTION OF TRAFFIC FLOW
- SIGN
- ARROW BOARD
- TYPE III BARRICADE
- TYPE II BARRICADE OR DRUM WITH FLASHING LIGHT
- STABILIZED CONSTRUCTION ENTRANCE

NOTES:

1. SIGN PANEL TO BE PAID FOR AS "TEMPORARY INFORMATION SIGNING".
2. ACCESS TO THE CTA STATION AND PLATFORM WILL BE CLOSED DURING CONSTRUCTION OF THE BRIDGE. ACCESS TO THE BRIDGE VIA THE EAST STAIRCASE FROM THE CTA PLATFORM WILL BE REINSTATED IN STAGE 2. ACCESS TO THE CTA STATION WILL BE REINSTATED IN STAGE 3.



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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**










**SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN
PEORIA STREET STAGE 2**

SCALE: 1"=50' SHEET 1 OF 1 SHEETS STA. 3700+00 TO STA. 3710+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	57
CONTRACT NO. 60W29				
ILLINOIS FED. AID PROJECT				

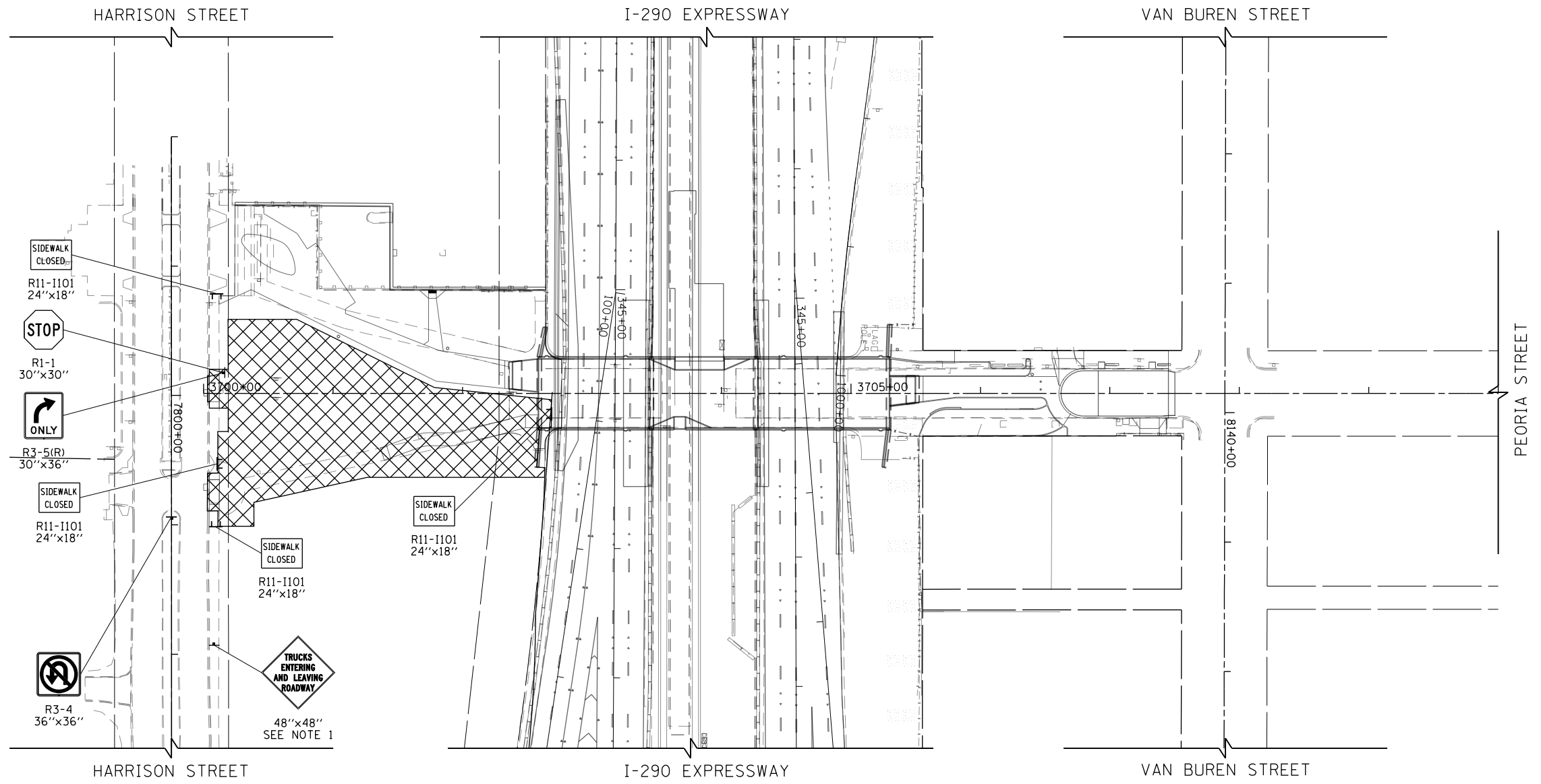


LEGEND

-  WORK ZONE
-  TEMPORARY IMPACT ATTENUATOR
-  TEMPORARY CONCRETE BARRIER
-  DIRECTION OF TRAFFIC FLOW
-  SIGN
-  ARROW BOARD
-  TYPE III BARRICADE
-  TYPE II BARRICADE OR DRUM WITH FLASHING LIGHT
-  STABILIZED CONSTRUCTION ENTRANCE


NOTES:

1. SIGN PANEL TO BE PAID FOR AS "TEMPORARY INFORMATION SIGNING".



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PLOT DATE = 12/19/2013	DATE - 10/30/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SUGGESTED STAGES OF CONSTRUCTION AND TRAFFIC CONTROL PLAN
PEORIA STREET STAGE 3**

SCALE: 1"=50' SHEET 1 OF 1 SHEETS STA. 3700+00 TO STA. 3710+00


F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	58
CONTRACT NO. 60W29			ILLINOIS FED. AID PROJECT	

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SHEET LEFT BLANK INTENTIONALLY



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PLOT DATE = 12/19/2013	DATE - 10/30/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-01R	COOK	356	59
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60W29	

EROSION CONTROL GENERAL NOTES

1. THE CONSTRUCTION LIMITS WILL BE STAKED AND APPROVED BY THE ENGINEER PRIOR TO COMMENCING CONSTRUCTION. THE CONSTRUCTION LIMITS MAY BE ADJUSTED BY THE ENGINEER TO PRESERVE TREES AND NO ADDITIONAL COMPENSATION WILL BE PAID TO THE CONTRACTOR FOR CHANGES IN CONSTRUCTION LIMITS.
2. EROSION CONTROL ITEMS ARE CONSIDERED HIGH PRIORITY ITEMS IN THIS CONTRACT. THE ENGINEER WILL IMPLEMENT ALL PROVISIONS OF SPECIFICATION TO NECESSARY ASSURE THAT EROSION CONTROL ITEMS ARE CONSTRUCTED AND MAINTAINED IN A TIMELY MANNER. THE CONTRACTOR SHALL INSTALL TEMPORARY EROSION CONTROL MEASURES PRIOR TO THE START OF CONSTRUCTION OPERATIONS WHICH WILL POTENTIALLY CREATE ERODIBLE CONDITIONS. PLACEMENT AND MAINTENANCE OF TEMPORARY EROSION CONTROL SYSTEMS WILL BE UTILIZED THROUGHOUT THE CONSTRUCTION LIMITS.
3. TEMPORARY EROSION CONTROL MEASURES SHALL BE CONSTRUCTED AS SHOWN ON THE PLANS AND AS DIRECTED BY THE ENGINEER. THE WORK SHALL BE COMPLETED IN ACCORDANCE WITH SECTION 280 OF THE STANDARD SPECIFICATIONS, CONTRACT SPECIAL PROVISIONS AND THE STORM WATER POLLUTION PREVENTION PLAN (SWPPP).
4. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN IN HIGHWAY STANDARD 280001-07.
5. THE EROSION CONTROL MEASURES SHOWN ARE BUT A GRAPHICAL REPRESENTATION OF SUGGESTED MEASURES. DEVIATIONS FROM THIS PLAN ARE TO BE EXPECTED PENDING A JOBSITE INSPECTION BETWEEN THE CONTRACTOR AND THE DEPARTMENT.
6. THE CONTRACTOR WILL BE REQUIRED TO IMPLEMENT AND MAINTAIN SEDIMENT CONTROL MEASURES PRIOR TO STRIPPING EXISTING VEGETATION.
7. THE CONTRACTOR SHALL INSPECT ALL EROSION AND SEDIMENT CONTROL MEASURES WEEKLY AND AFTER EACH RAINFALL, 0.5 INCHES OR GREATER IN A 24 HOUR PERIOD, OR EQUIVALENT SNOWFALL. ADDITIONALLY DURING WINTER MONTHS, ALL MEASURES SHOULD BE CHECKED AFTER EACH SIGNIFICANT SNOWMELT. ANY NECESSARY REPAIRS OR CLEANUP TO MAINTAIN THE EFFECTIVENESS OF SAID MEASURES SHALL BE MADE IMMEDIATELY.
8. TEMPORARY EROSION CONTROL SEEDING SHALL BE APPROVED PER SUB-STAGE AS SOON AS ROUGH GRADING IS COMPLETED IN A SECTION. STABILIZATION OF DISTURBED AREAS MUST BE INITIATED WITHIN 1 WORKING DAY OF TEMPORARY OR PERMANENT CESSATION OF EARTH DISTURBING ACTIVITIES AND SHALL BE COMPLETED AS SOON AS POSSIBLE BUT NO LATER THAN 14 DAYS FROM THE INITIATION OF STABILIZATION OF WORK IN AN AREA.
9. ANY AREA WHERE THERE IS NO PROPOSED GRADING THE EXISTING GROUND COVER SHALL REMAIN.
10. TEMPORARY STOCKPILE LOCATIONS SHALL BE APPROVED BY THE ENGINEER AND WILL REQUIRE SILT FENCE AND TEMPORARY SEEDING.
11. THE CONTRACTOR SHALL INSTALL AND MAINTAIN INLET FILTERS AT ALL EXISTING INLETS ADJACENT TO THE EDGE OF PAVEMENT PRIOR TO THE START OF PRE-STAGE WORK. THE INLET FILTERS SHALL BE MAINTAINED AT EACH SUBSEQUENT STAGE UNTIL NO LONGER REQUIRED OR AS DIRECTED BY THE ENGINEER.
12. DURING CONSTRUCTION OPERATIONS, WHEN ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DITCHES, GUTTERS OR DRAINAGE STRUCTURES SO THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, THE MATERIAL SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE PROJECT.
13. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL DRAINAGE STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE PROJECT.

14. THE CONTRACTOR SHALL IMMEDIATELY INSTALL AND MAINTAIN INLET FILTERS AT ALL NEW INLETS AND DRAINAGE STRUCTURES. THE INLET FILTERS SHALL BE MAINTAINED AT EACH SUBSEQUENT STAGE UNTIL COMPLETION OF STAGING OR UNTIL NO LONGER REQUIRED.

15. PERMANENT STABILIZATION SHALL BE INSTALLED ON ALL AREAS DISTURBED DURING EACH STAGE OF CONSTRUCTION PRIOR TO BEGINNING THE SUBSEQUENT STAGE. ALSO, ALL EROSION CONTROL MEASURES PLACED DURING CONSTRUCTION SHALL REMAIN IN PLACE AND BE MAINTAINED UNTIL COMPLETION OF STAGE 2 OR NO LONGER REQUIRED.











16. WHEN EXCAVATING, THE CONTRACTOR SHALL INSTALL, AS PER THE ILLINOIS URBAN MANUAL, EITHER A SUMP PIT OR A SEDIMENTATION BASIN IN ORDER TO PREVENT SEDIMENT FROM ENTERING THE EXISTING SEWER SYSTEMS OR DRAINAGE WAYS. IF THE CONTRACTOR INSTALLS A SUMP PIT AND WILL BE PUMPING WATER OUT OF THE PIT, THE CONTRACTOR SHALL BE REQUIRED TO INSTALL A FILTER BAG AT THE END OF THE PUMP DISCHARGE HOSE TO PREVENT SEDIMENT FROM ENTERING THE EXISTING DRAINAGE SYSTEMS.

17. MULCH, METHOD 4 AND SURFACE ROUGHENING SHALL BE USED FOR TEMPORARY STABILIZATION DURING WINTER INSTEAD OF TEMPORARY EROSION CONTROL SEEDING WHEN GRADING WILL OCCUR AFTER SEPTEMBER 30TH WHEN TEMPORARY SEED WILL NOT GERMINATE AND PROVIDE EROSION CONTROL PROTECTION UNTIL THE FOLLOWING SPRING. SURFACE ROUGHENING WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF MULCH, METHOD 4.

18. PERIMETER EROSION BARRIER SHALL BE PLACED 3 INCHES INSIDE OF TEMPORARY CHAIN LINK FENCE WHEN ADJACENT TO THE FENCE AS SHOWN ON THE PLANS.


19. VEHICLE GATES SHALL BE INSTALLED AT THE STABILIZED CONSTRUCTION ENTRANCE OFF OF HARRISON STREET AND AT THE NORTH END OF PEORIA STREET WHERE THE TEMPORARY CHAIN LINK FENCE IS SHOWN ON THE PLANS. THE CONTRACTOR SHALL LOCK THE GATES AT ALL TIMES WHEN NOT ON THE PROJECT SITE.

TEMPORARY EROSION CONTROL LEGEND

-  INLET FILTER
-  INLET FILTER (TO REMAIN FROM PREVIOUS STAGE)
-  PERIMETER EROSION BARRIER
-  MULCH, METHOD 2
-  TEMPORARY EROSION CONTROL SEEDING
-  MULCH, METHOD 4 SURFACE ROUGHENING
-  STABILIZED CONSTRUCTION ENTRANCE
-  LIMITS OF CONSTRUCTION
-  TEMPORARY CHAIN LINK FENCE
-  TEMPORARY FENCE

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PLOT DATE = 12/19/2013	DATE - 10/30/2013	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EROSION AND SEDIMENTATION CONTROL PLAN
GENERAL NOTES

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	60
CONTRACT NO. 60W29			ILLINOIS FED. AID PROJECT	

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TEMPORARY EROSION CONTROL SCHEDULE

STAGES	MULCH, METHOD 2 ACRE	MULCH, METHOD 4 ACRE	TEMPORARY EROSION CONTROL SEEDING POUND	PERIMETER EROSION BARRIER FOOT	INLET FILTERS EACH	TEMPORARY CHAIN LINK FENCE FOOT	TEMPORARY FENCE FOOT	STABILIZED CONSTRUCTION ENTRANCE SQ YD
STAGE 1A	0.95	-	95	966	29	811	226	568
STAGE 1B	0.90	-	90	-	5	-	-	430
STAGE 2	-	0.75	-	631	3	263	-	-
STAGE 3	0.56	-	56	565	-	-	-	-
TOTAL	2.50	0.75	241	2,163	37	1,075	226	998

PERMANENT EROSION CONTROL SCHEDULE

	POTASSIUM FERTILIZER NUTRIENT POUND	NITROGEN FERTILIZER NUTRIENT POUND	SEEDING, CLASS 2A ACRE	EROSION CONTROL BLANKET SQ YD	SODDING, SALT TOLERANT SQ YD	SUPPLEMENTAL WATERING UNIT	TOPSOIL FURNISH AND PLACE, 4" SQ YD	MOWING ACRE	MULCH CU YD	WEED CONTROL, PRE-EMERGENT GRANULAR HERBICIDE LB
PERMANENT CONDITION	81	81	0.25	611	4,610	207.6	5,221	0.25	14	1
TOTAL	81	81	0.25	611	4,610	207.6	5,221	0.25	14	1

INLET FILTER SCHEDULE

INLET FILTERS				
STAGE 1A				
	LOCATION		ALIGNMENT	AMOUNT (EACH)
	STATION	OFFSET		
STAGE 1A	3700+03.00	0.38' LT	PEORIA	1
STAGE 1A	3700+03.14	118.60' LT	PEORIA	1
STAGE 1A	3700+03.25	72.63' RT	PEORIA	1
STAGE 1A	3702+48.03	56.19' RT	PEORIA	1
STAGE 1A	3702+48.25	63.30' LT	PEORIA	1
STAGE 1A	3705+24.81	42.12' RT	PEORIA	1
STAGE 1A	3705+74.79	17.47' LT	PEORIA	1
STAGE 1A	3705+76.67	16.74' RT	PEORIA	1
STAGE 1A	3707+53.09	17.90' LT	PEORIA	1
STAGE 1A	3707+65.47	22.24' RT	PEORIA	1
STAGE 1A	3707+67.19	21.34' LT	PEORIA	1
STAGE 1A	3707+69.86	34.63' LT	PEORIA	1
STAGE 1A	343+73.66	42.44' RT	EB I-290	1
STAGE 1A	343+73.98	44.59' RT	EB I-290	1
STAGE 1A	345+25.53	37.26' RT	EB I-290	1
STAGE 1A	345+36.93	42.87' RT	EB I-290	1
STAGE 1A	345+39.79	25.23' LT	EB I-290	1
STAGE 1A	345+42.00	44.60' RT	EB I-290	1
STAGE 1A	346+62.28	25.27' LT	EB I-290	1
STAGE 1A	347+23.31	47.95' RT	EB I-290	1
STAGE 1A	347+83.48	26.10' LT	EB I-290	1
STAGE 1A	349+22.85	74.89' RT	EB I-290	1
STAGE 1A	345+27.44	27.59' RT	WB I-290	1
STAGE 1A	345+37.77	35.38' LT	WB I-290	1
STAGE 1A	345+37.78	36.13' LT	WB I-290	1
STAGE 1A	346+60.57	26.94' RT	WB I-290	1
STAGE 1A	347+93.22	26.18' RT	WB I-290	1
STAGE 1A	347+93.59	36.66' LT	WB I-290	1
STAGE 1A	349+20.23	41.75' LT	WB I-290	1
TOTAL				29

INLET FILTERS				
STAGE 1B				
	LOCATION		ALIGNMENT	AMOUNT (EACH)
	STATION	OFFSET		
STAGE 1B	3706+12.90	12.11' LT	PEORIA	1
STAGE 1B	3706+32.90	12.00' LT	PEORIA	1
STAGE 1B	3706+45.86	1.99' RT	PEORIA	1
STAGE 1B	3706+49.03	21.40' RT	PEORIA	1
STAGE 1B	3706+85.99	17.50' RT	PEORIA	1
TOTAL				5

INLET FILTERS				
STAGE 2				
	LOCATION		ALIGNMENT	AMOUNT (EACH)
	STATION	OFFSET		
STAGE 2	3700+96.17	108.43' LT	PEORIA	1
STAGE 2	3701+21.39	108.48' LT	PEORIA	1
STAGE 2	3701+45.19	108.54' LT	PEORIA	1
TOTAL				3



DI60W29-sht-Eros-Schedule-01.dgn
 USER NAME = BAW1tor t
 PLOT SCALE = 20.0000' / in.
 PLOT DATE = 12/19/2013

DESIGNED - JLV
 DRAWN - JSL
 CHECKED - JMG
 DATE - 10/30/2013

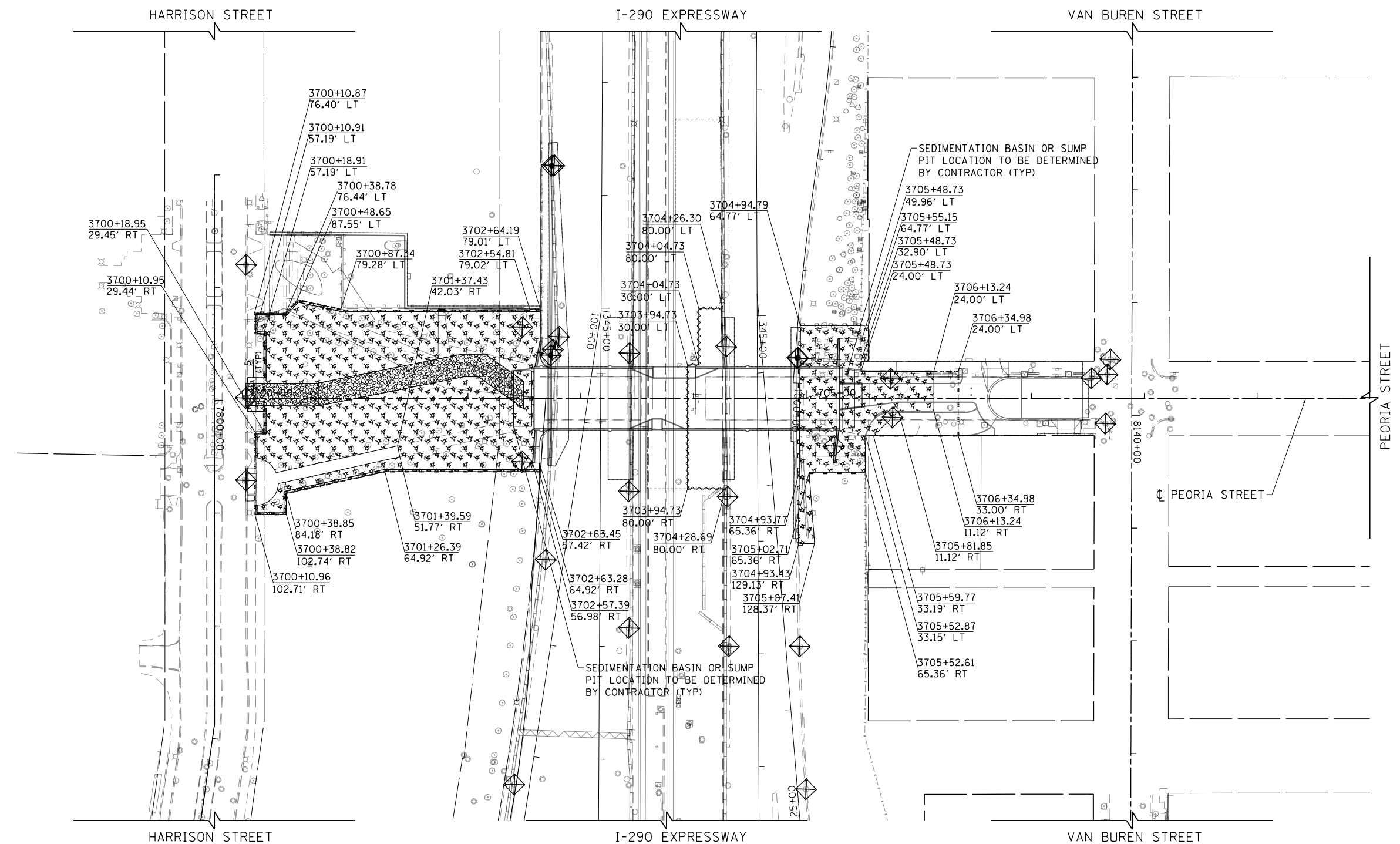
REVISED - 12/18/2013
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**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

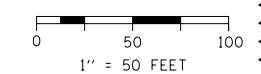
**EROSION AND SEDIMENTATION CONTROL PLAN
 SCHEDULES**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE. 90/94/290	SECTION 2013-011R	COUNTY COOK	TOTAL SHEETS 356	SHEET NO. 61
CONTRACT NO. 60W29			ILLINOIS FED. AID PROJECT	



NOTE:
FOR LEGEND SEE SHEET NO. 60



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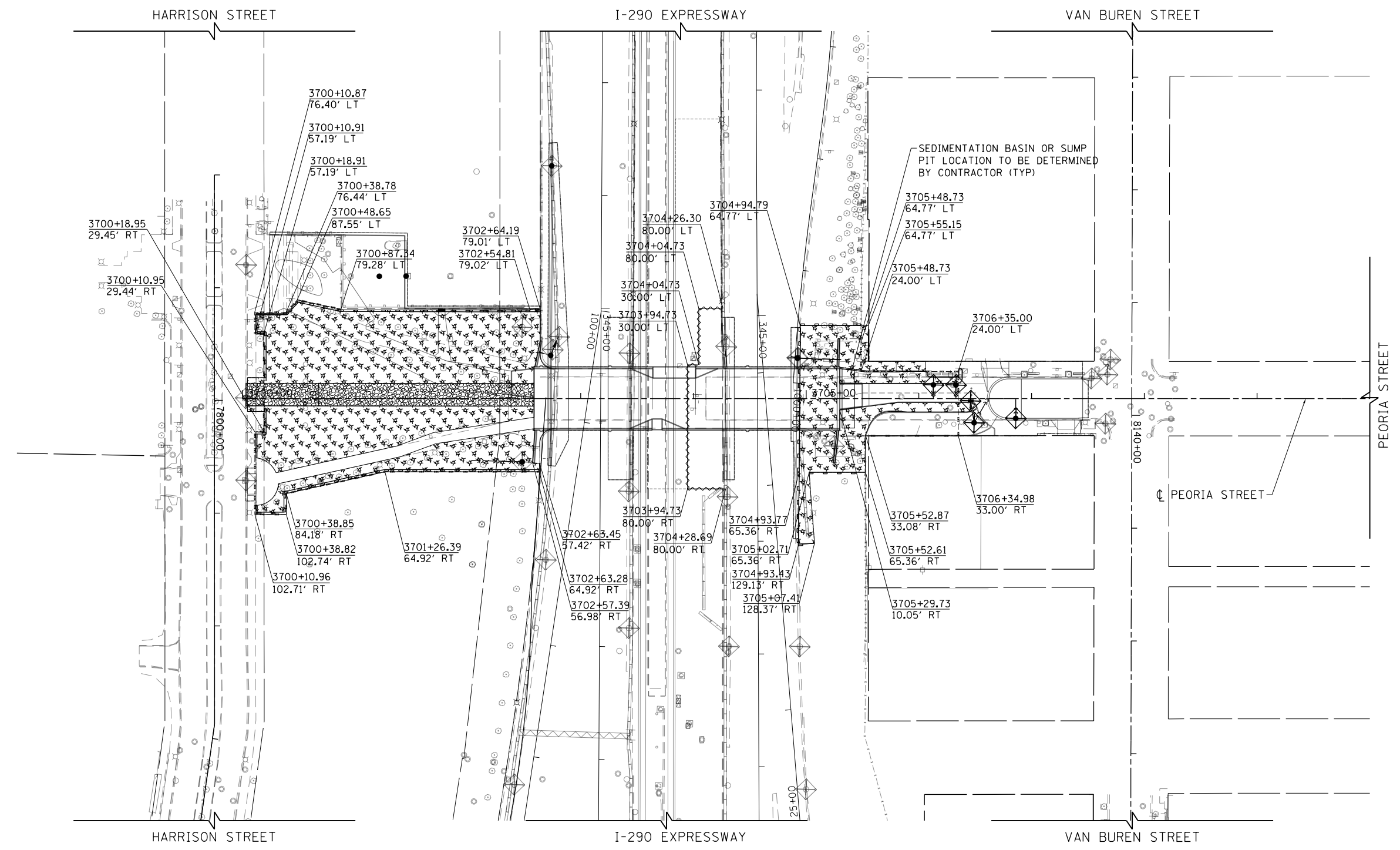
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DRAWN - BAW	REVISED -
CHECKED - JLV	REVISED -
DATE - 10/30/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

EROSION AND SEDIMENTATION CONTROL PLAN			
PEORIA STREET STAGE 1A			
SCALE: 1"=50'	SHEET 1	OF 1 SHEETS	STA. 3700+00 TO STA. 3710+00

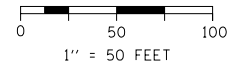
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-01R	COOK	356	62
CONTRACT NO. 60W29				
ILLINOIS FED. AID PROJECT				

A



NOTE:

FOR LEGEND SEE SHEET NO. 60



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PLOT DATE = 12/19/2013

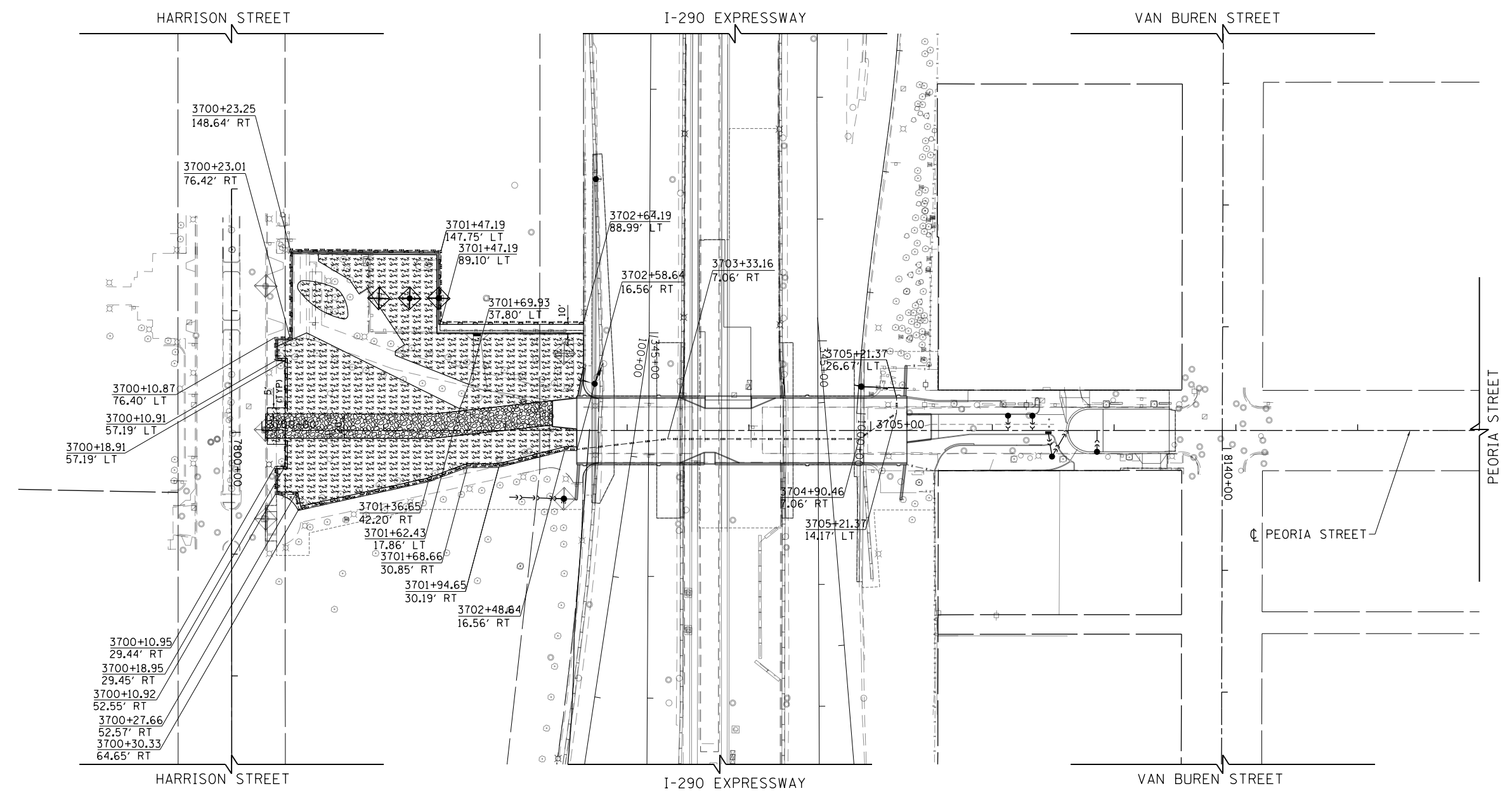
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DRAWN - BAW	REVISED -
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DATE - 10/30/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

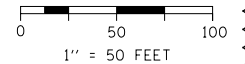
**EROSION AND SEDIMENTATION CONTROL PLAN
PEORIA STREET STAGE 1B**

SCALE: 1"=50' SHEET 1 OF 1 SHEETS STA. 3700+00 TO STA. 3710+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	63
CONTRACT NO. 60W29				
ILLINOIS FED. AID PROJECT				



NOTE:
FOR LEGEND SEE SHEET NO. 60



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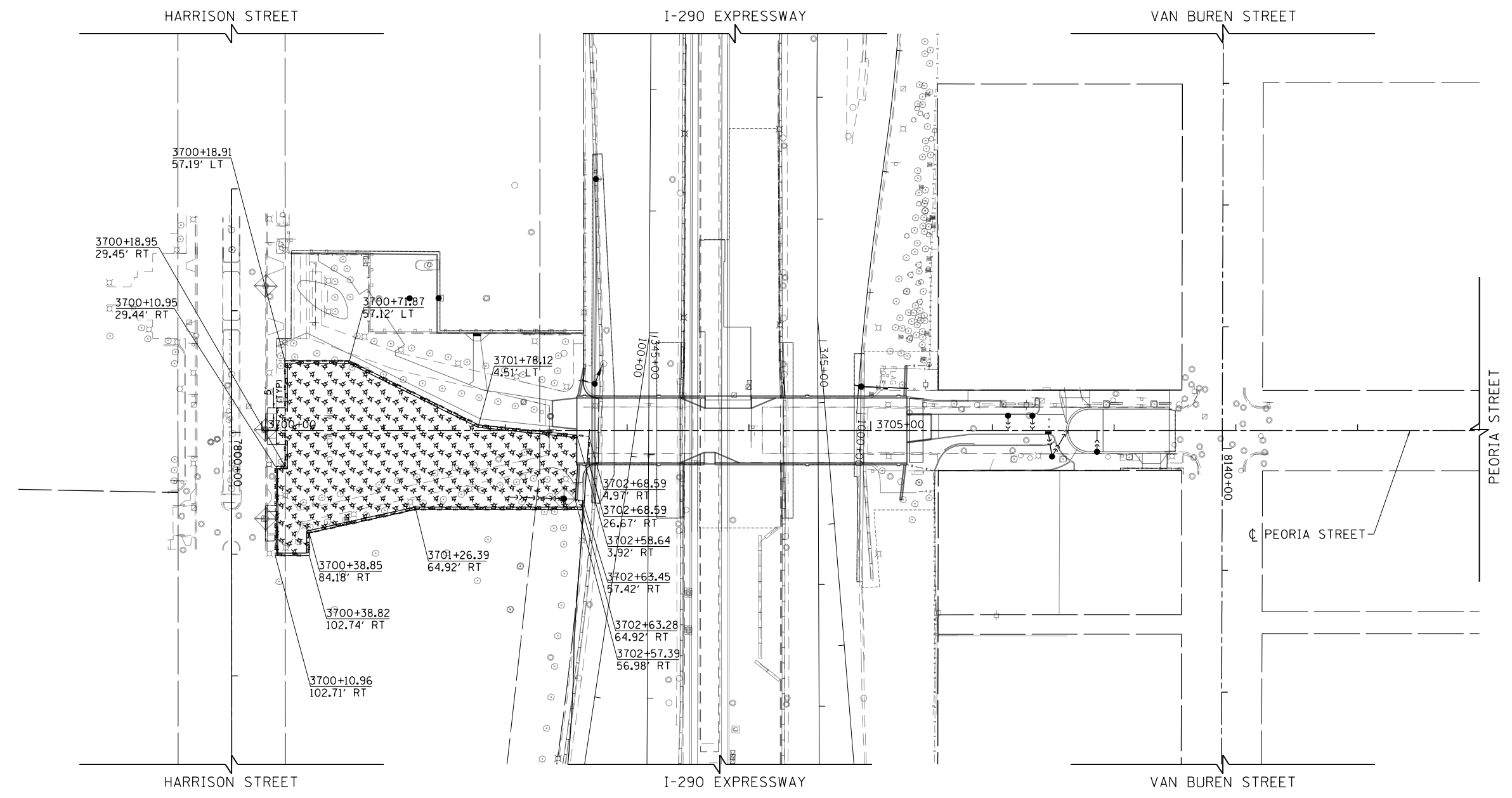
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PLOT DATE = 12/19/2013	DATE - 10/30/2013	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EROSION AND SEDIMENTATION CONTROL PLAN
PEORIA STREET STAGE 2

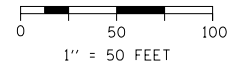
SCALE: 1"=50' SHEET 1 OF 1 SHEETS STA. 3700+00 TO STA. 3710+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	64
CONTRACT NO. 60W29				
ILLINOIS FED. AID PROJECT				



NOTE:

FOR LEGEND SEE SHEET NO. 60



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PLOT DATE = 12/19/2013	DATE - 10/30/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EROSION AND SEDIMENTATION CONTROL PLAN
PEORIA STREET STAGE 3**

SCALE: 1"=50' SHEET 1 OF 1 SHEETS STA. 3700+00 TO STA. 3710+00


F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	65
CONTRACT NO. 60W29				
ILLINOIS FED. AID PROJECT				

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SHEET LEFT BLANK INTENTIONALLY



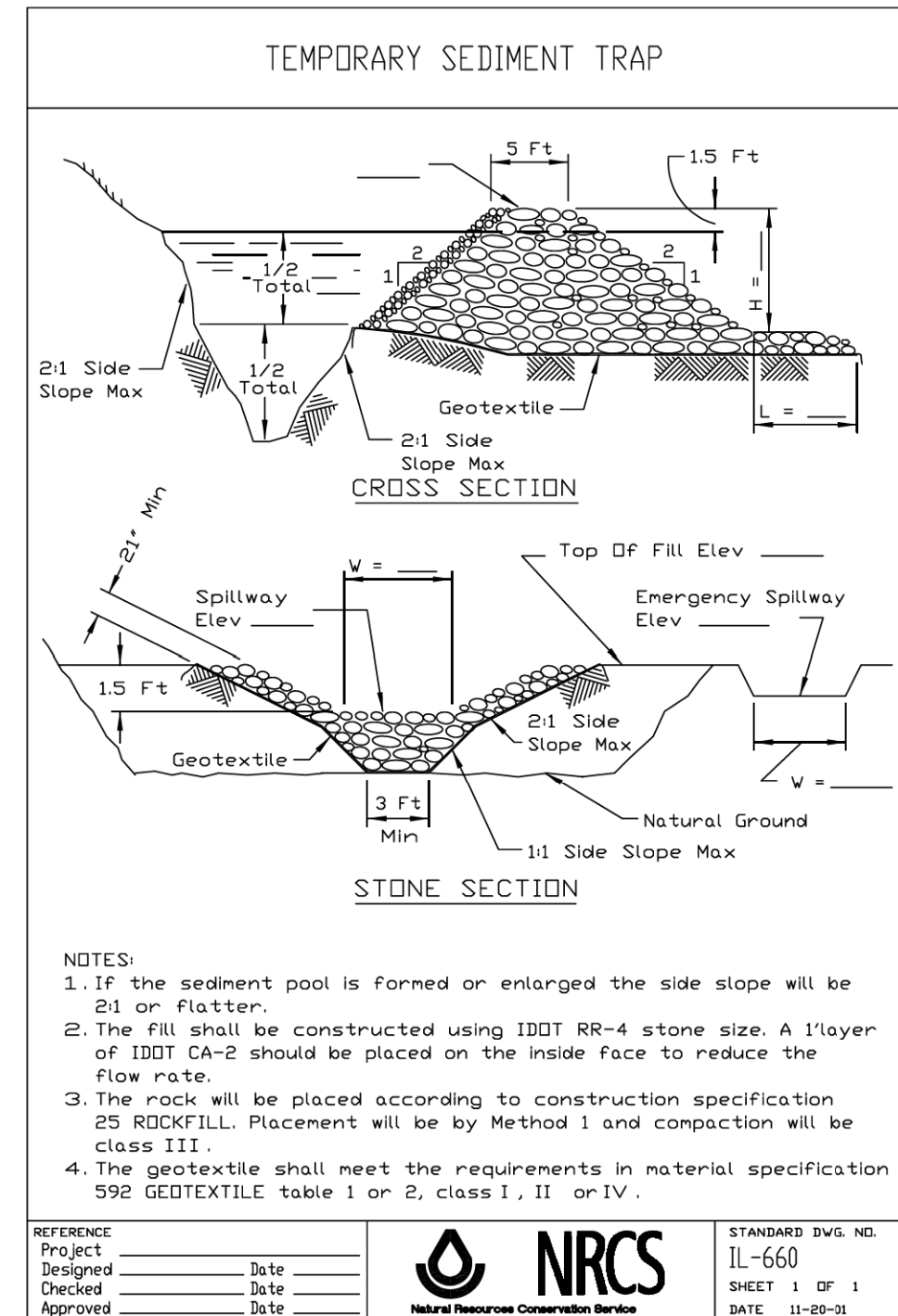
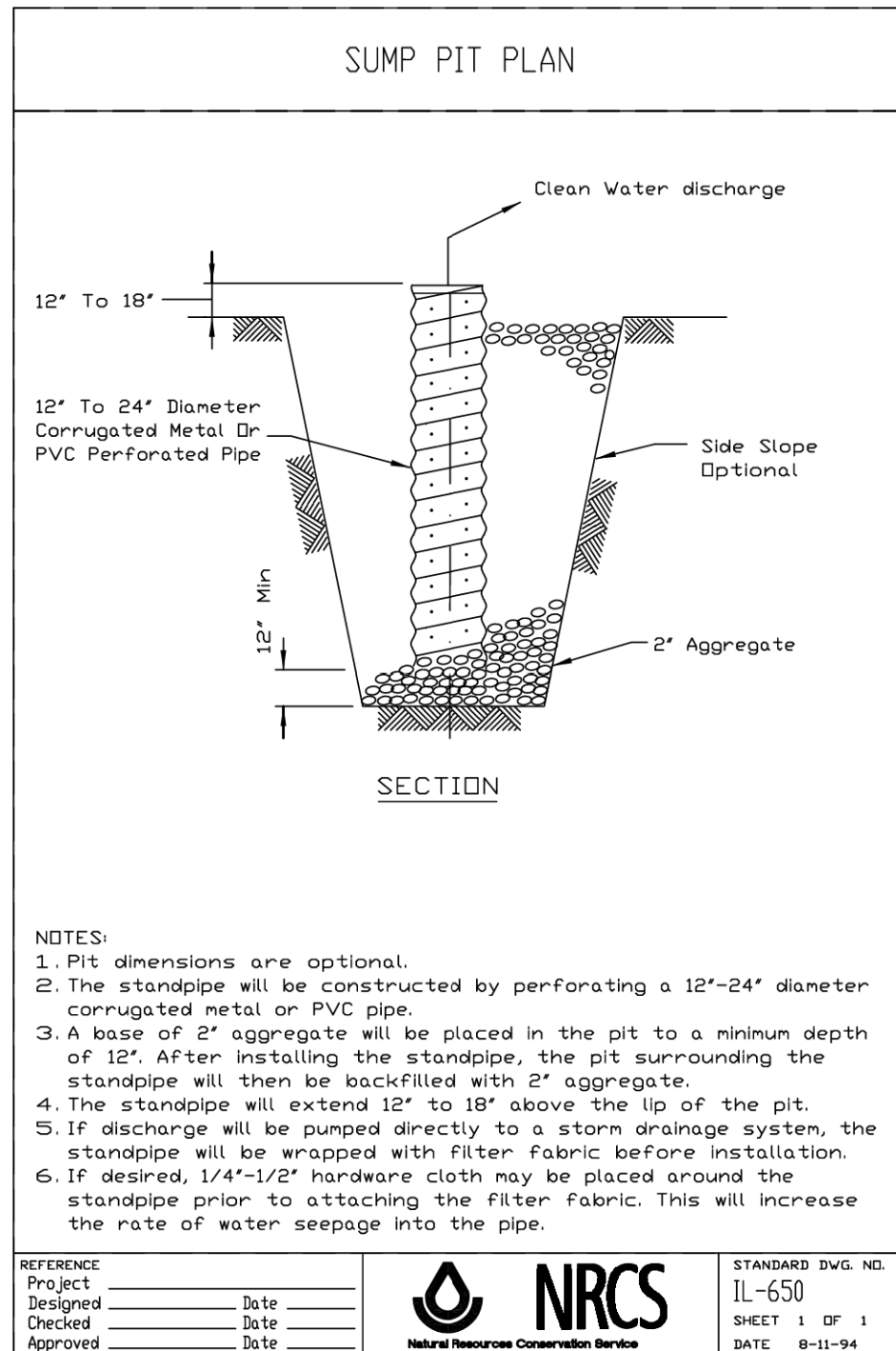
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PLOT DATE = 12/19/2013	DATE - 10/30/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-01R	COOK	356	66
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60W29	

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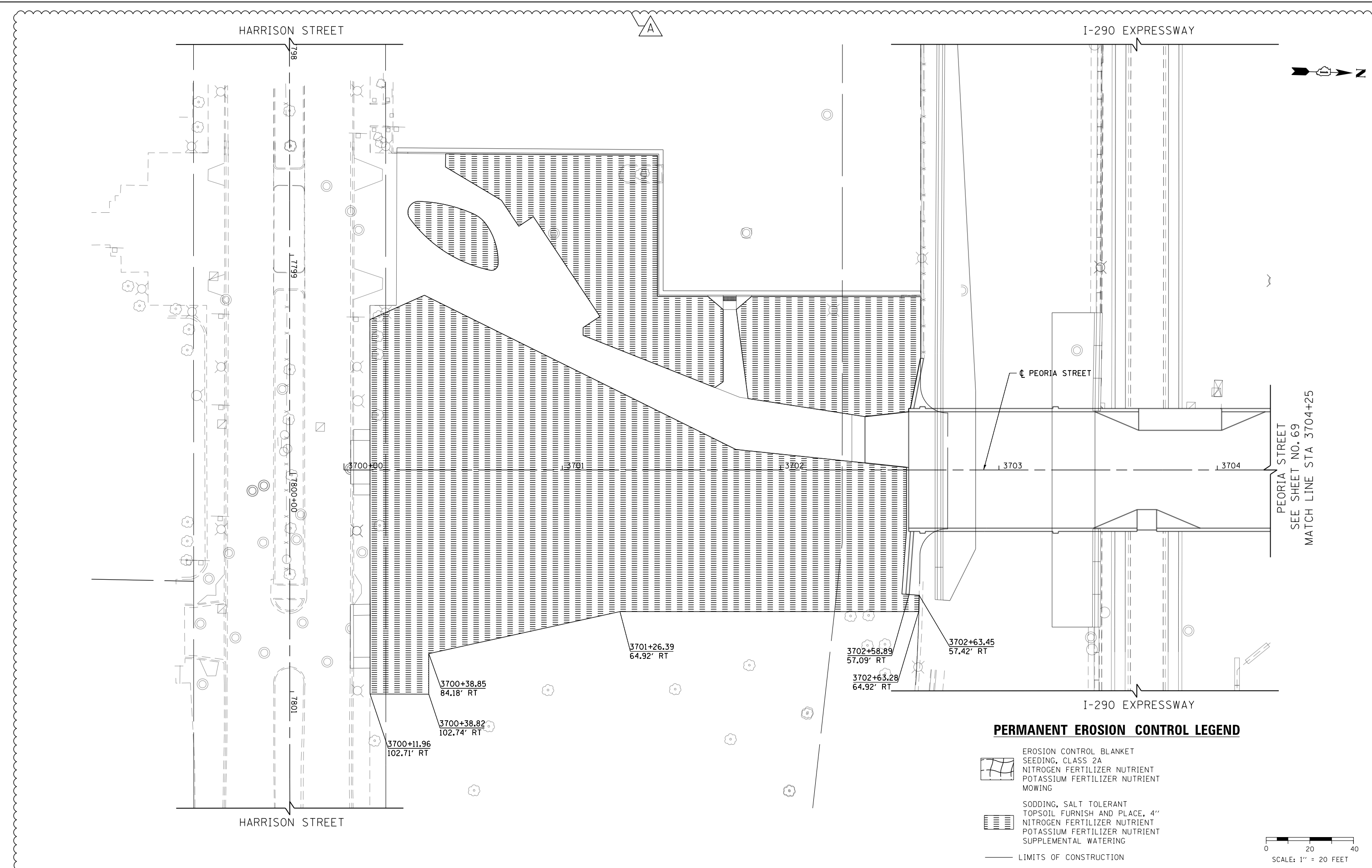
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EROSION AND SEDIMENTATION CONTROL PLAN
DETAILS**












SCALE: NONE SHEET 1 OF 1 SHEETS STA. _____ TO STA. _____

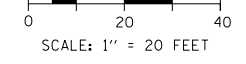
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90/94/290	2013-011R	COOK	356	67
CONTRACT NO. 60W29				
ILLINOIS FED. AID PROJECT				

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PERMANENT EROSION CONTROL LEGEND

-  EROSION CONTROL BLANKET
-  SEEDING, CLASS 2A
-  NITROGEN FERTILIZER NUTRIENT
-  POTASSIUM FERTILIZER NUTRIENT
-  MOWING
-  SODDING, SALT TOLERANT
-  TOPSOIL FURNISH AND PLACE, 4"
-  NITROGEN FERTILIZER NUTRIENT
-  POTASSIUM FERTILIZER NUTRIENT
-  SUPPLEMENTAL WATERING
-  LIMITS OF CONSTRUCTION



PEORIA STREET
SEE SHEET NO. 69
MATCH LINE STA 3704+25



D160W29-Sht-Lndscp-01.dgn
USER NAME = BAWtor t
PLOT SCALE = 40.0000' / in.
PLOT DATE = 12/19/2013

DESIGNED - JLJ	REVISED - A 12/18/2013
DRAWN - KAL	REVISED -
CHECKED - JMG	REVISED -
DATE - 10/30/2013	REVISED -

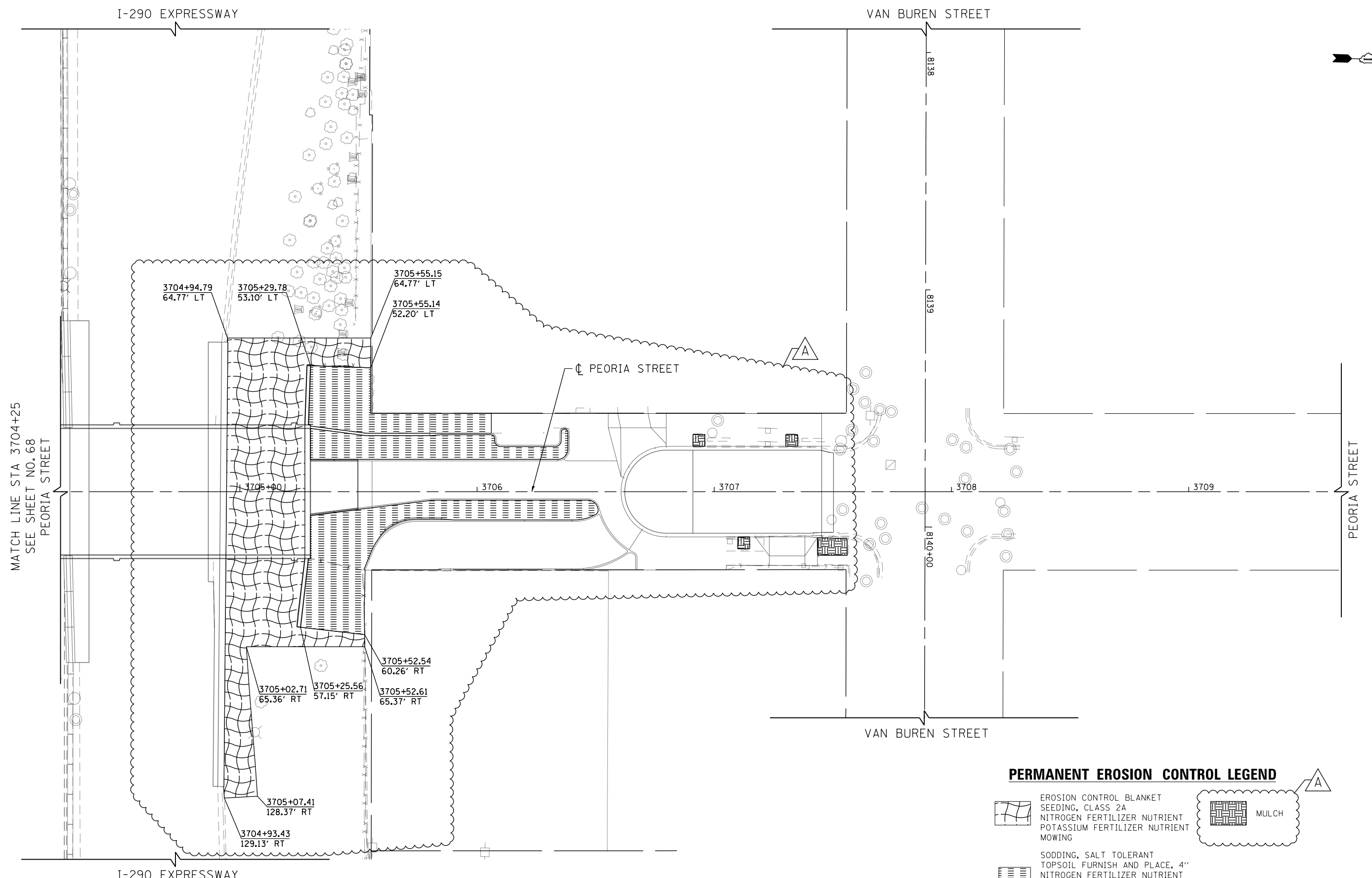
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PERMANENT EROSION AND SEDIMENTATION CONTROL PLAN			
PEORIA STREET			
SCALE: 1"=20'	SHEET 1	OF 2 SHEETS	STA. 3700+00 TO STA. 3704+25





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90/94/290	2013-011R	COOK	356	68
CONTRACT NO. 60W29				
ILLINOIS FED. AID PROJECT				

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MATCH LINE STA 3704+25
SEE SHEET NO. 68
PEORIA STREET




PERMANENT EROSION CONTROL LEGEND

-  EROSION CONTROL BLANKET
-  SODDING, SALT TOLERANT TOPSOIL FURNISH AND PLACE, 4" NITROGEN FERTILIZER NUTRIENT POTASSIUM FERTILIZER NUTRIENT SUPPLEMENTAL WATERING
-  MULCH
-  LIMITS OF CONSTRUCTION

0 20 40
SCALE: 1" = 20 FEET



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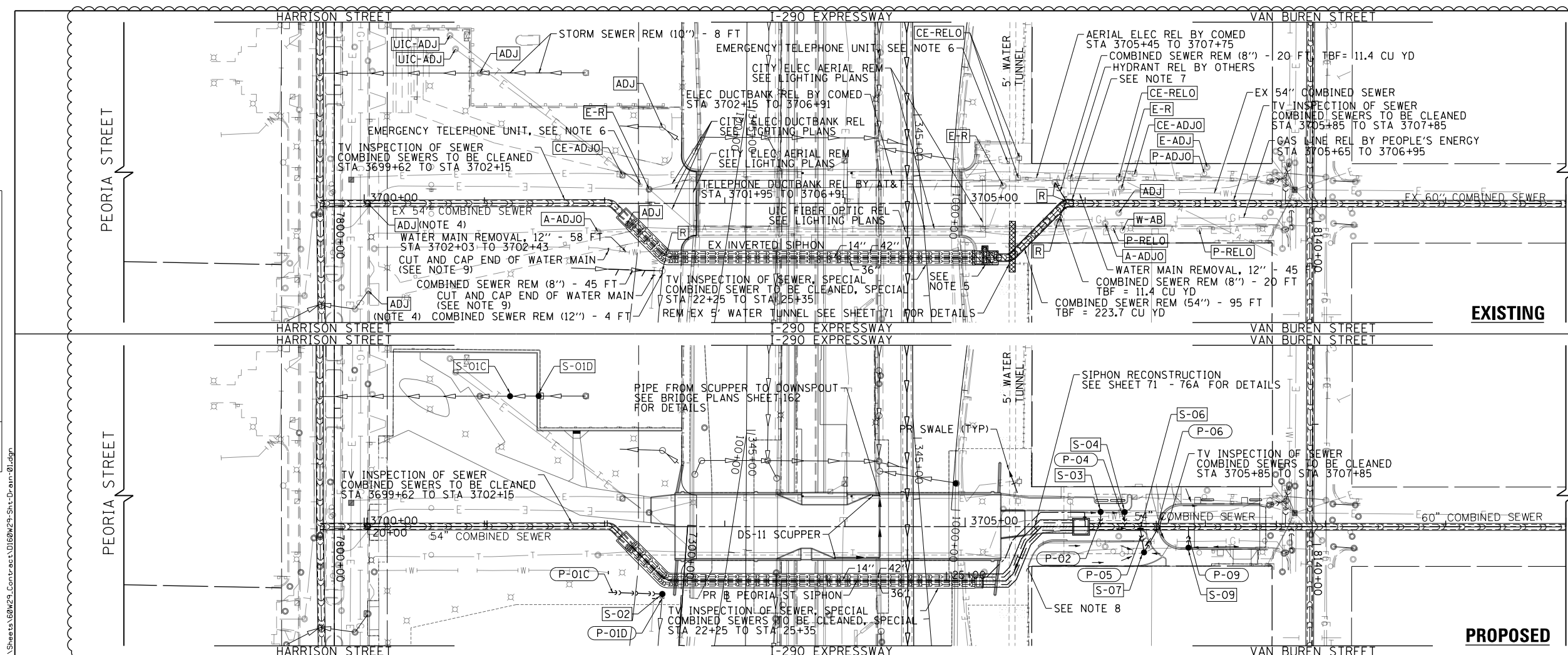
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PERMANENT EROSION AND SEDIMENTATION CONTROL PLAN
PEORIA STREET**
SCALE: 1"=20' SHEET 2 OF 2 SHEETS STA. 3704+70 TO STA. 3707+40

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-01R	COOK	356	69
CONTRACT NO. 60W29				
ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
	PLOTTED	
	NOTE BOOK	
	CHECKED	
	ALIGNMENT	
	STRUCTURE	
	BY	
	DATE	

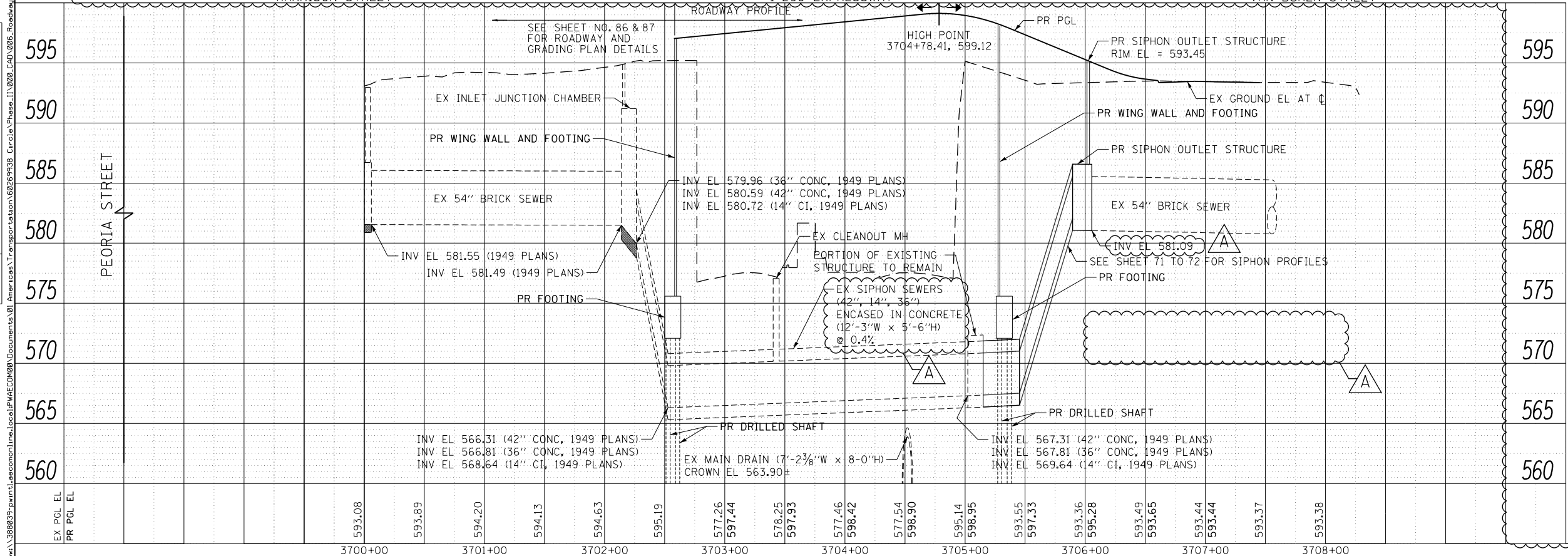
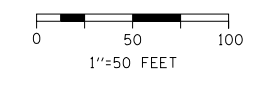
PROFILE	SURVEYED	DATE
	PLOTTED	
	NOTE BOOK	
	CHECKED	
	GRADES	
	STRUCTURE	
	BY	
	DATE	



ADJUSTMENT AND REMOVAL LEGEND

- A-ADJO** ATT ADJUST BY OTHERS
- CE-RELO** COMED RELOCATE BY OTHERS
- CE-ADJO** COMED ADJUST BY OTHERS
- P-RELO** PEOPLE'S ENERGY RELOCATE BY OTHERS
- E-R** CITY ELECTRIC REMOVAL
- E-ADJ** CITY ELECTRIC ADJUST
- W-ADJ** CITY WATER ABANDON BY CONTRACTOR (FILLING VALVE VAULTS)
- UIC-ADJ** UNIVERSITY OF ILLINOIS AT CHICAGO ADJUST BY CONTRACTOR
- R** REMOVAL BY CONTRACTOR
- ADJ** ADJUST BY CONTRACTOR

- ### NOTES:
- SEE SHEET NO. 28 TO 29 FOR DRAINAGE STRUCTURE AND PIPE SCHEDULES.
 - SEE SHEET NO. 77 TO 80 FOR I-290 DRAINAGE AND REMOVALS.
 - SEE STRUCTURAL SHEETS FOR DS-11 SCUPPER DETAILS.
 - COST OF PAVEMENT PATCHING TO BE INCLUDED IN COST OF STRUCTURE ADJUSTMENT OR PROPOSED STRUCTURE. SEE SHEET 86 FOR DIMENSIONS.
 - REMOVAL OF EXISTING SIPHON OUTLET STRUCTURE (TO BE PAID FOR AS REMOVAL OF EXISTING STRUCTURES)
 - FOR EMERGENCY TELEPHONE UNIT RELOCATION DETAILS SEE LIGHTING PLANS.
 - THE 54" COMBINED SEWER SHALL NOT BE REMOVED OR DISCONNECTED UNTIL THE TEMPORARY BYPASS LINES FOR THE SIPHON HAVE BEEN INSTALLED AND FLOW HAS BEEN DIVERTED TO THE TEMPORARY LINES. COORDINATION WITH THE CONSTRUCTION OF THE TEMPORARY SOIL RETENTION SYSTEMS WILL BE REQUIRED.
 - THE TEMPORARY SOIL RETENTION SYSTEM IN THIS AREA CANNOT BE CONSTRUCTED UNTIL THE TEMPORARY BYPASS LINES FOR THE SIPHON HAVE BEEN CONSTRUCTED AND FLOW IS DIVERTED TO THE TEMPORARY LINES.
 - THE CONTRACTOR SHALL CUT AND CAP THE WATER MAIN AT THE LIMITS SHOWN ON THE PLANS. THE WORK REQUIRED TO CAP THE ENDS OF THE EXISTING WATER MAIN SHALL BE INCLUDED IN THE COST OF WATER MAIN REMOVAL, 12", AND SHALL NOT BE PAID FOR SEPARATELY.
 - THE LIMITS FOR REMOVAL AND ABANDONMENT OF EXISTING WATER MAIN PIPES MUST BE DETERMINED AS PART OF STAGING REQUIREMENTS AND CONSTRUCTION LIMITS FOR SEWER AND WATER MAIN CONSTRUCTION.



EX PGL EL	PR PGL EL	3700+00	3701+00	3702+00	3703+00	3704+00	3705+00	3706+00	3707+00	3708+00								
		593.08	593.89	594.20	594.13	594.63	595.19	577.26 597.44	578.25 597.93	577.46 598.42	577.54 598.90	595.14 598.95	593.55 597.33	593.36 595.28	593.49 593.65	593.44 593.44	593.37	593.36



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PLOT DATE = 12/19/2013	DATE - 10/30/2013	REVISED -

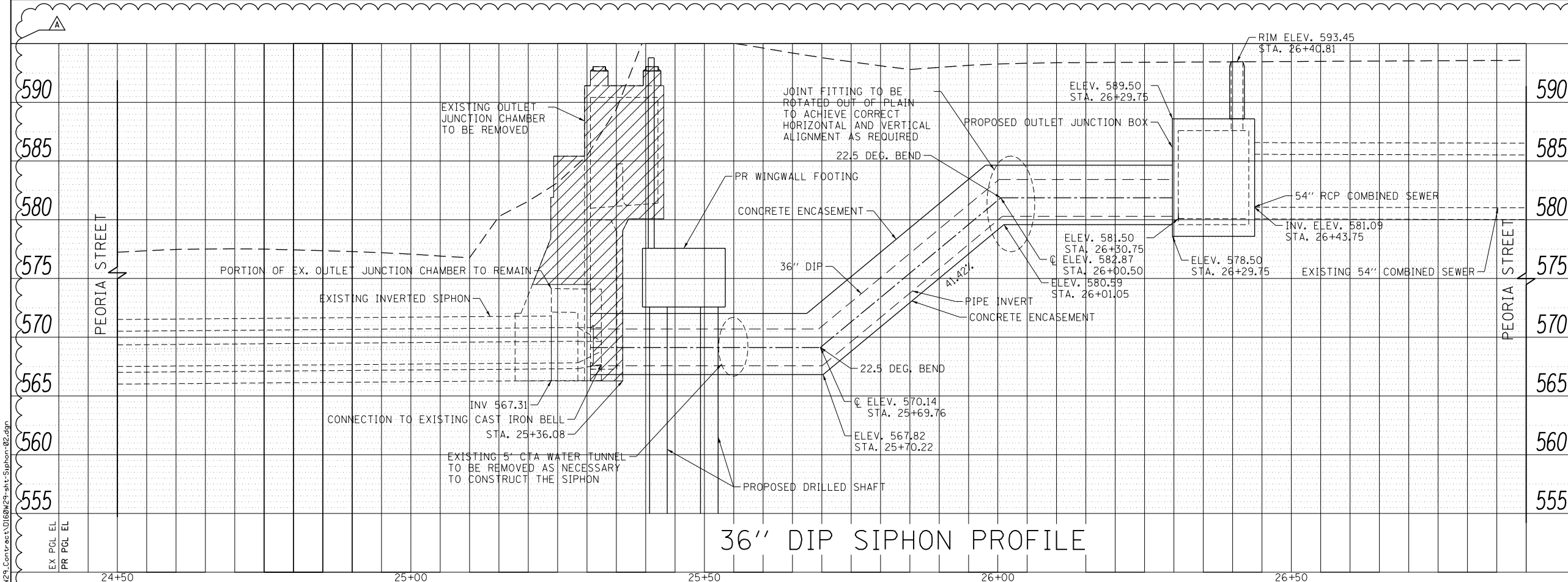
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DRAINAGE AND UTILITY PLAN AND PROFILE
PEORIA STREET**

SCALE: 1"=50' SHEET 1 OF 11 SHEETS STA. 3700+00 TO STA. 3710+00

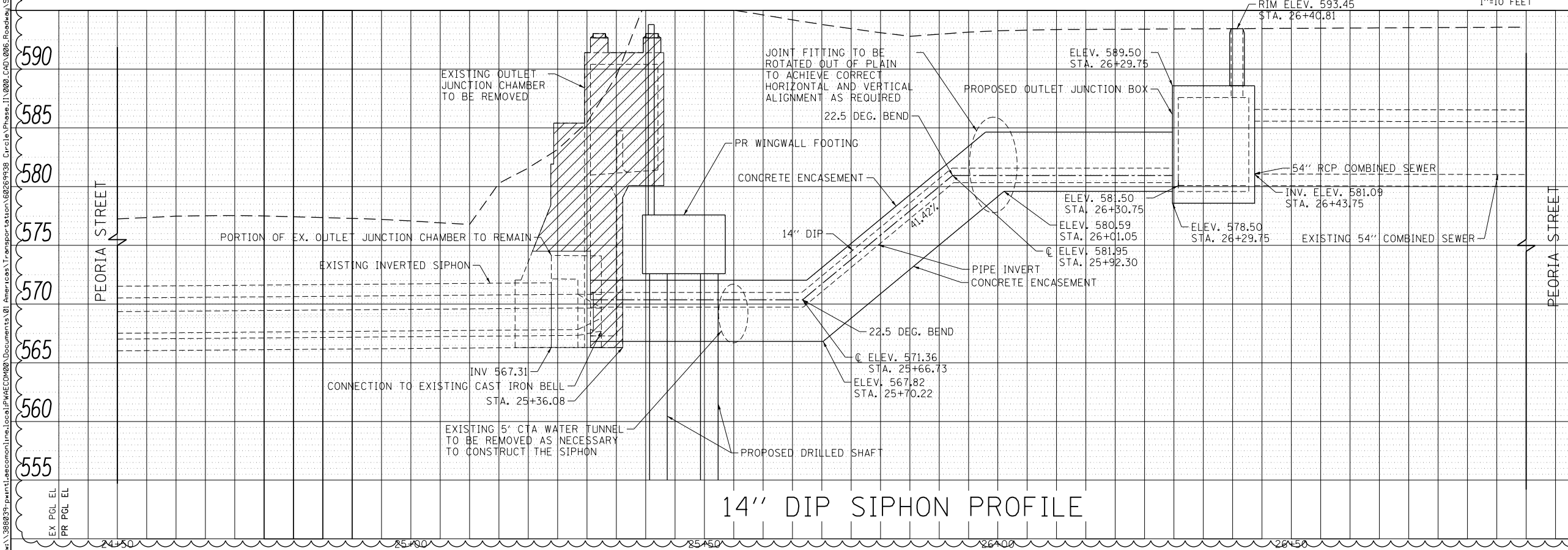
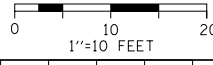
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	70
CONTRACT NO. 60W29			ILLINOIS FED. AID PROJECT	

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- NOTES:
1. ALL DIMENSIONS, ELEVATIONS AND PIPE SIZES SHOWN HAVE BEEN TAKEN FROM RECORD DRAWINGS TITLED (INCLUDED IN THE PLANS):

WEST ROUTE SUPER HIGHWAY
PLANS FOR PEORIA STREET BRIDGE
SECTION 2525.1B
F.A. ROUTE NO. 131 PROJECT UI261(29)
APPROVED JULY 1, 1949
 2. CONTRACTOR SHALL VERIFY ALL DIMENSIONS, ELEVATIONS AND SIZES PRIOR TO CONSTRUCTION OR DEMOLITION AND REPORT DISCREPANCIES TO THE ENGINEER BEFORE PROCEEDING.
 3. CONDITION OF THE EXISTING 54" BRICK COMBINED SEWER PIPE IS EXPECTED TO BE GOOD AND THE EXTERIOR SURFACE SUITABLE FOR APPLICATION OF AN ADHESIVE WATER STOP SYSTEM.
 4. ALL NEW SIPHON PIPE SHALL BE DUCTILE IRON PIPE BELLED JOINT.
 5. ALL FITTINGS SHALL BE DUCTILE IRON MECHANICAL JOINT FITTINGS.
 6. DESIGN CODES:
A. AMERICAN CONCRETE INSTITUTE ACI-318
B. AASHTO
 7. SEE SPECIAL PROVISION SIPHON RECONSTRUCTION FOR ADDITIONAL CONSTRUCTION REQUIREMENTS AND MATERIAL SPECIFICATIONS.
 8. DESIGN STRENGTH OF CAST IN PLACE CONCRETE, F'C 4000 PSI
 9. YIELD STRESS OF REINFORCEMENT STEEL, Fy 60,000 PSI
 10. ALLOWABLE SOIL BEARING PRESSURE BASED ON THE SOILS REPORT PREPARED BY WANG ENGINEERING WEI #1100-04-01.



D:\160W29-sht-Siphon-02.dgn	DESIGNED - JEH	REVISED - A 01/20/2014
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PLOT SCALE = 100.0000' / 1"	CHECKED - JWC	REVISED -
PLOT DATE = 1/21/2014	DATE - 10/30/2013	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

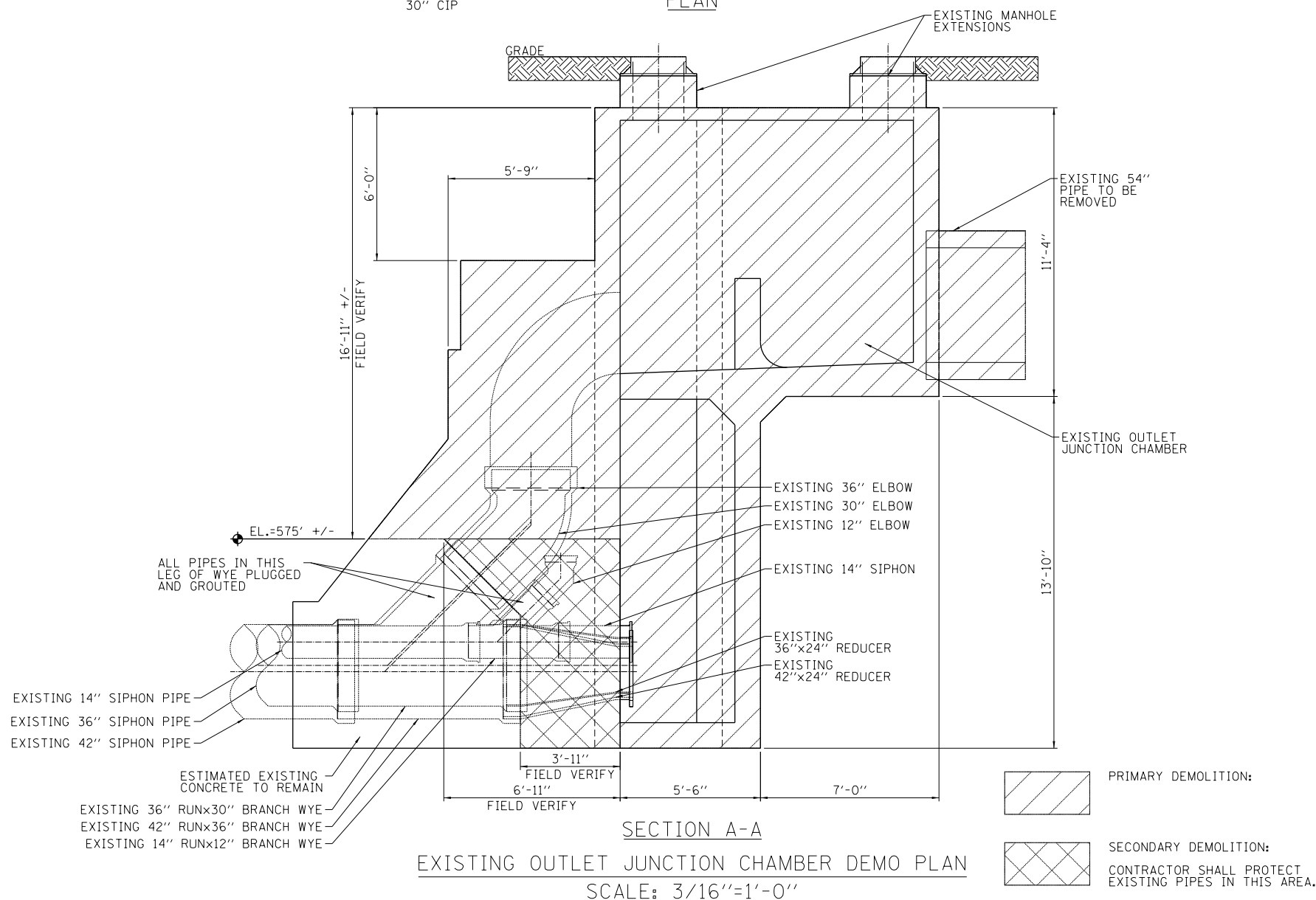
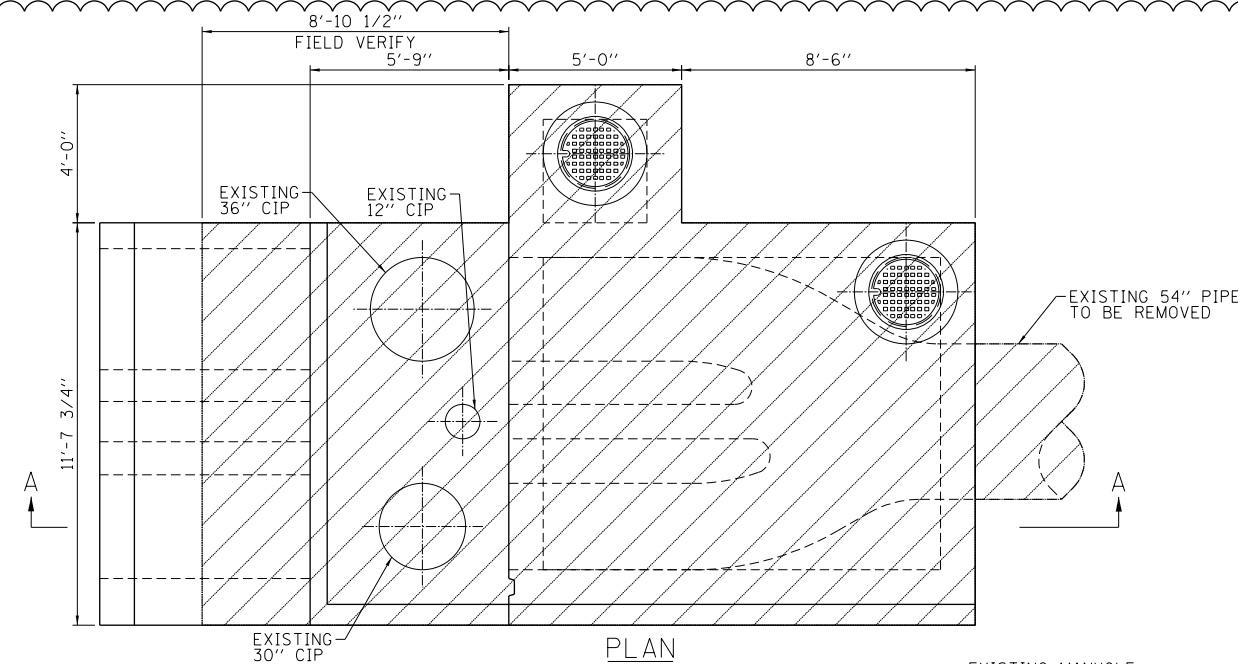
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SCALE:	SHEET 3	OF 11 SHEETS	STA. TO STA.

F.A.I. RTE. 90/94/290	SECTION 2013-011R	COUNTY COOK	TOTAL SHEETS 356	SHEET NO. 72
ILLINOIS FED. AID PROJECT				CONTRACT NO. 60W29

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NOTES:

- ALL DIMENSIONS, ELEVATIONS AND PIPE SIZES SHOWN HAVE BEEN TAKEN FROM RECORD DRAWINGS TITLED (INCLUDED IN THE PLANS);
WEST ROUTE SUPER HIGHWAY
PLANS FOR PEORIA STREET BRIDGE SECTION 2525.1B
F.A.ROUTE NO. 131 PROJECT UI261(29)
APPROVED JULY 1, 1949
- CONTRACTOR SHALL VERIFY ALL DIMENSIONS, ELEVATIONS AND SIZES PRIOR TO CONSTRUCTION OR DEMOLITION AND REPORT DISCREPANCIES TO THE ENGINEER BEFORE PROCEEDING
- CONDITION OF THE EXISTING 54" BRICK COMBINED SEWER PIPE IS EXPECTED TO BE GOOD AND THE EXTERIOR SURFACE SUITABLE FOR APPLICATION OF AN ADHESIVE WATER STOP SYSTEM.
- DEMOLITION OF THE EXISTING OUTLET JUNCTION CHAMBER SHALL BE CONDUCTED USING CAUTION SO THAT THE EXISTING SIPHON PIPES ARE NOT DAMAGED. IN PARTICULAR CARE SHALL BE TAKEN WHEN REMOVING CONCRETE FROM AROUND THE REDUCER FITTINGS AND THE CONNECTION TO THE LARGER PIPE.
- CAUTION SHALL BE TAKEN DURING DEMOLITION SO THE BYPASS PIPING IS NOT DAMAGED.
- GEOMETRY OF THE CONNECTION FLANGE ON THE EXISTING 42" X 24" REDUCER PIPE FLANGE HAS BEEN TAKEN FROM THE PROVIDED DRAWINGS AND SHALL BE VERIFIED.



SEQUENCE OF CONSTRUCTION:

THE SEQUENCE OF CONSTRUCTION MUST INSURE THAT SEWER FLOWS WILL BE UNINTERRUPTED DURING THE CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL BE FULLY RESPONSIBLE FOR DEVELOPING THE SEQUENCE OF CONSTRUCTION FOR THE WORK. THE FOLLOWING SEQUENCE OF CONSTRUCTION WAS DEVELOPED AS ONE OPTION AND IS THE BASIS FOR THE BYPASS PIPING AND STRUCTURES AS SHOWN IN THE PLANS. OTHER CONSTRUCTION SEQUENCES WILL BE CONSIDERED.

- EXCAVATE AND EXPOSE THE EXISTING 54" BRICK SEWER.
- REMOVE AND CAP CURB INLET PIPING.
- CONSTRUCT THE PROPOSED OUTLET JUNCTION BOX AROUND THE EXISTING 54" BRICK SEWER. THE BOX WOULD BE PERMANENTLY SEALED AGAINST THE OUTSIDE OF THE 54" BRICK SEWER ON THE NORTH END. THE BOX WILL HAVE NO TOP AND THE TEMPORARY WALL #1 IS SEALED AGAINST THE 54" BRICK SEWER. THE BASE OF THE BOX WILL BE CAST AGAINST THE UNDERSIDE OF THE BRICK SEWER.
- THE 24" HDPE BYPASS LINE IS CONNECTED TO THE PROPOSED OUTLET JUNCTION BOX AND RUN TO THE EXISTING OUTLET JUNCTION CHAMBER.
- THE 42" SIPHON LINE IS TEMPORARILY PLUGGED AT THE SOUTH END, DEWATERED AND CLEANED.
- THE 36" WYE BRANCH LINE ON THE 42" SIPHON LINE IS PLUGGED AND PERMANENTLY GROUTED TIGHT BELOW ELEVATION 575.00.
- THE EXTERIOR WALL OF THE EXISTING OUTLET JUNCTION CHAMBER IS REMOVED TO AN EXTENT SO THE 24" BYPASS LINE CAN BE CONNECTED TO THE 24" FLANGE CONNECTION ON THE 42" SIPHON LINE.
- THE EXISTING 54" BRICK SEWER WITHIN THE PROPOSED OUTLET JUNCTION BOX IS REMOVED.
- TEMPORARY WALL #2 IS CONSTRUCTED IN THE PROPOSED OUTLET JUNCTION BOX SEPARATING THE INLET 54" SEWER LINE FROM THE 24" BYPASS LINE AND OUTLET 54" SEWER LINE.
- SEWER FLOWS ARE DIVERTED TO THE 42" SIPHON LINE AND THE 24" BYPASS LINE.
- THE 36" AND 14" SIPHON LINES ARE TEMPORARILY PLUGGED AT THE SOUTH END, DEWATERED AND CLEANED.
- THE EXISTING OUTLET JUNCTION CHAMBER ABOVE ELEVATION 575.00, THE EXISTING 54" SEWER LINE FROM THE EXISTING OUTLET JUNCTION CHAMBER TO THE PROPOSED OUTLET JUNCTION BOX AND THE TEMPORARY WALL #1 OF THE PROPOSED OUTLET JUNCTION BOX ARE REMOVED.
- THE PERMANENT SOUTH WALL OF THE PROPOSED OUTLET JUNCTION BOX IS CONSTRUCTED WITH 42", 36" AND 14" WALL PIPES.
- THE 30" AND 12" WYE BRANCH LINES ON THE 36" AND 14" SIPHON LINES ARE PLUGGED AND PERMANENTLY GROUTED TIGHT.
- THE CONCRETE AROUND THE 42", 36" AND 14" SIPHON LINES IS CAREFULLY REMOVED EXPOSING THE 42" X 24" AND 36" X 24" REDUCERS AND THE 14" PIPE.
- THE 36" X 24" REDUCER IS REMOVED AND THE NEW 36" PIPE IS CONNECTED. THE 14" PIPE FLANGE SECTION IS REMOVED AND THE NEW 14" PIPE IS CONNECTED.
- THE 36" AND 14" SIPHON LINES ARE CONSTRUCTED FROM THE EXISTING OUTLET JUNCTION CHAMBER TO THE PROPOSED OUTLET JUNCTION BOX.
- THE 42" SIPHON LINE IS CONSTRUCTED FROM THE PROPOSED OUTLET JUNCTION BOX TO THE EXISTING OUTLET JUNCTION CHAMBER.
- THE THREE SIPHON LINES ARE ENCASED IN CONCRETE.
- INTERIOR BAFFLE WALLS AND FORMED INVERTS ARE CONSTRUCTED ON THE DRY (SOUTH) SIDE OF THE TEMPORARY WALL #2.
- THE TEMPORARY WALL #2 IS REMOVED FROM THE PROPOSED OUTLET JUNCTION BOX.
- SEWER FLOWS ARE DIVERTED TO THE 36" AND 14" SIPHON LINES.
- THE INVERT IN THE PROPOSED OUTLET JUNCTION BOX IS COMPLETED ON THE WET (NORTH) SIDE OF THE PROPOSED OUTLET JUNCTION BOX.
- THE 42" SIPHON AND 24" BYPASS LINES ARE PLUGGED AND DEWATERED.
- THE 24" BYPASS LINE IS REMOVED. AND THE 24" OPENING IN THE PROPOSED OUTLET JUNCTION BOX IS PERMANENTLY SEALED.
- THE 42" X 24" REDUCER IS REMOVED AND THE NEW 42" LINE IS CONNECTED TO THE EXISTING 42" SIPHON LINE.
- AREA AROUND THE SIPHON PIPE CONNECTIONS IS FILLED WITH ENCASEMENT CONCRETE.
- THE TOP OF THE PROPOSED OUTLET JUNCTION BOX IS CONSTRUCTED ALONG WITH THE ACCESS MANHOLE AND CONNECTIONS TO THE ADJACENT CURB INLET STRUCTURES.
- FLOW ESTABLISHED IN THE 3 SIPHON LINES.

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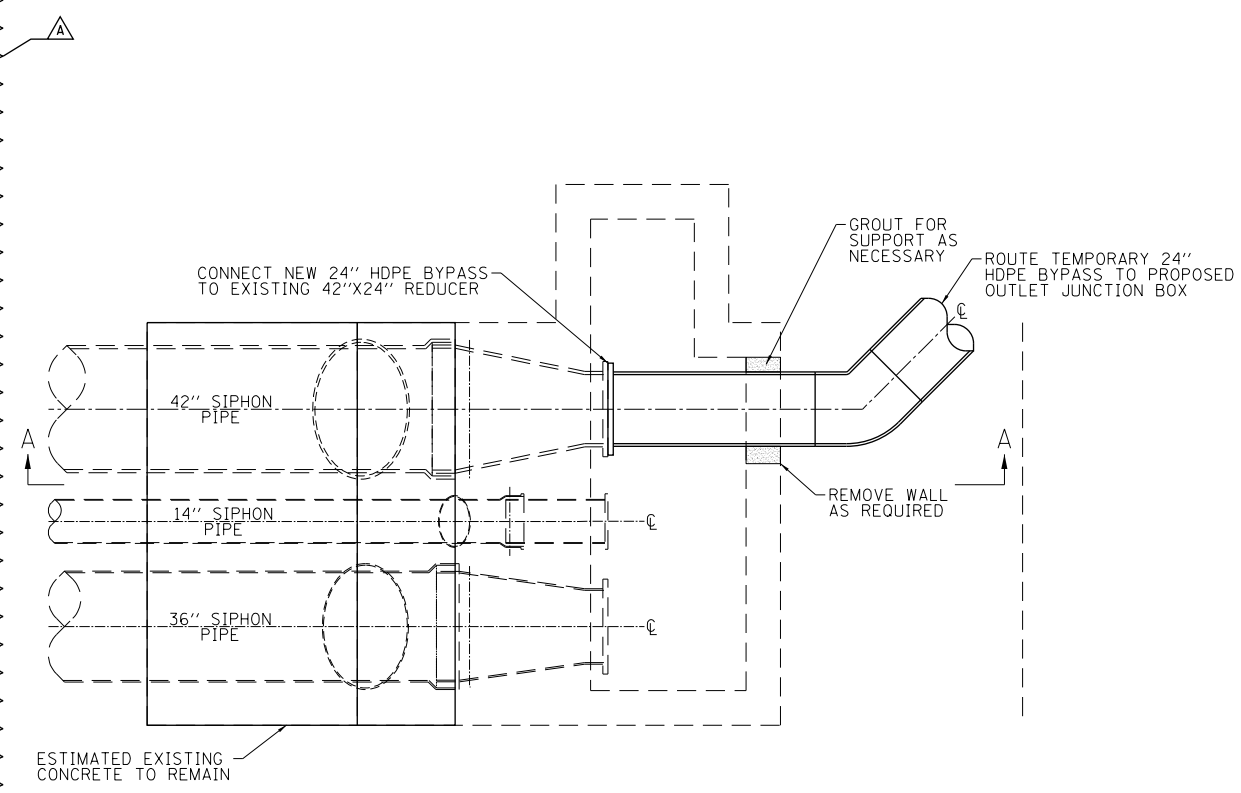
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SIPHON DEMOLITION DETAILS
PEORIA STREET**

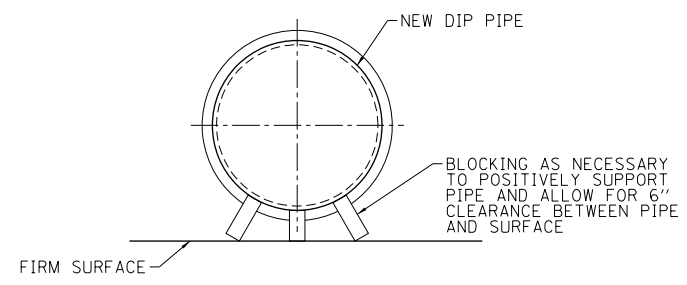
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F.A.I. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	73
CONTRACT NO. 60W29				
ILLINOIS FED. AID PROJECT				

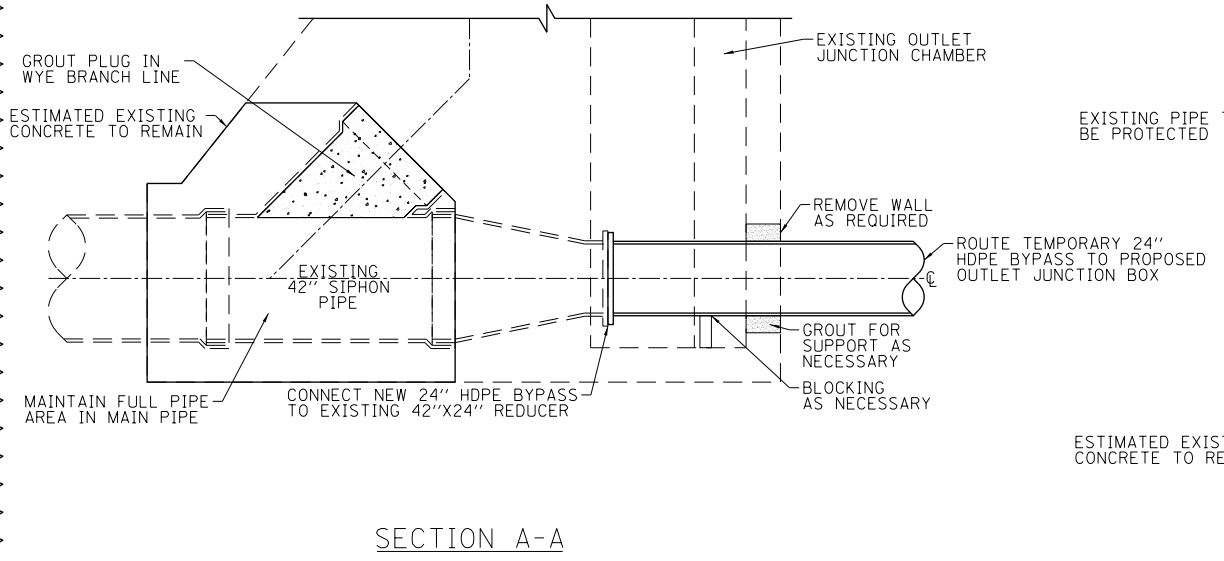
- NOTES:
- ALL DIMENSIONS, ELEVATIONS AND PIPE SIZES SHOWN HAVE BEEN TAKEN FROM RECORD DRAWINGS TITLED (INCLUDED IN THE PLANS);
WEST ROUTE SUPER HIGHWAY
PLANS FOR PEORIA STREET BRIDGE SECTION 2525.1B
F.A.ROUTE NO. 131 PROJECT UI261(29)
APPROVED JULY 1, 1949
 - CONTRACTOR SHALL VERIFY ALL DIMENSIONS, ELEVATIONS AND SIZES PRIOR TO CONSTRUCTION OR DEMOLITION AND REPORT DISCREPANCIES TO THE ENGINEER BEFORE PROCEEDING
 - CONDITION OF THE EXISTING 54" BRICK COMBINED SEWER PIPE IS EXPECTED TO BE GOOD AND THE EXTERIOR SURFACE SUITABLE FOR APPLICATION OF AN ADHESIVE WATER STOP SYSTEM.
 - DUCTILE IRON PIPES CONNECTING TO THE EXISTING CAST IRON PIPES SHALL BE CLOSELY MATED AND THE JOINT SEALED WITH RAN-NEK SEALANT OR EQUAL. THE JOINT SHALL BE SEALED FLUSH WITH AN EPOXY GROUT OR EQUAL.
 - PIPE SHALL BE FULLY SUPPORTED WITH A MINIMUM SPACE OF 6" FOR PIPE ENCASEMENT CONCRETE.
 - NEW DIP PIPES CONNECTING TO THE EXISTING SIPHON PIPES SHALL BE FULLY SUPPORTED SO THAT NO WEIGHT IS PLACED ON THE EXISTING PIPES.
 - ADHESIVE WATERSTOP SHALL BE APPLIED TO ALL FORMED SURFACES REQUIRED TO BE WATER TIGHT.
 - CENTER BULB WATER STOP SHALL BE CAST INTO ALL CAST JOINTS REQUIRED TO BE WATER TIGHT.
 - THREADED REINFORCEMENT CASTINGS SHALL BE LENTON OR APPROVED EQUAL. PLUGS SHALL BE USED ON ALL COUPLERS.
 - WALL PIPE ASSEMBLIES SHALL BE DUCTILE IRON WITH WATERSTOP RINGS.
 - THE SURFACE UNDER THE SIPHON PIPE ENCASEMENT SHALL BE EXAMINED FOR ANY SOFT OR YIELDING AREAS. ANY AREAS FOUND SHALL BE EXCAVATED AND RECOMPACTED TO PROVIDE SUITABLE SUPPORT FOR THE SIPHON PIPES.
 - THE NEW DUCTILE IRON SIPHON PIPE AND FITTINGS SHALL BE SECURELY SUPPORTED TO INSURE NO MOVEMENT OCCURS PRIOR TO OR DURING PLACEMENT OF THE SIPHON PIPE CONCRETE ENCASEMENT. THE SIPHON PIPE SHALL BE SUPPORTED SO THAT A MINIMUM OF SIX (6) INCHES OF CONCRETE ENCASEMENT CAN BE PLACED AROUND THE PIPE. MEASURES SHALL BE TAKEN TO INSURE FULL CONSOLIDATION OF THE CONCRETE ENCASEMENT MATERIAL OCCURS AROUND AND UNDER THE SIPHON PIPES.
 - THE NEW DUCTILE IRON SIPHON PIPE SHALL BE SECURELY SUPPORTED AT THE CONNECTION TO THE EXISTING SIPHON PIPES TO MINIMIZE ANY LOAD ON THE EXISTING PIPE.
 - DRILL AND EPOXY (a7) 12" INTO THE EXISTING CONCRETE STRUCTURE. EPOXY CONCRETE MATERIALS SHALL COMPLY WITH SECTION 1025 OF THE IDOT STANDARD SPECIFICATIONS.



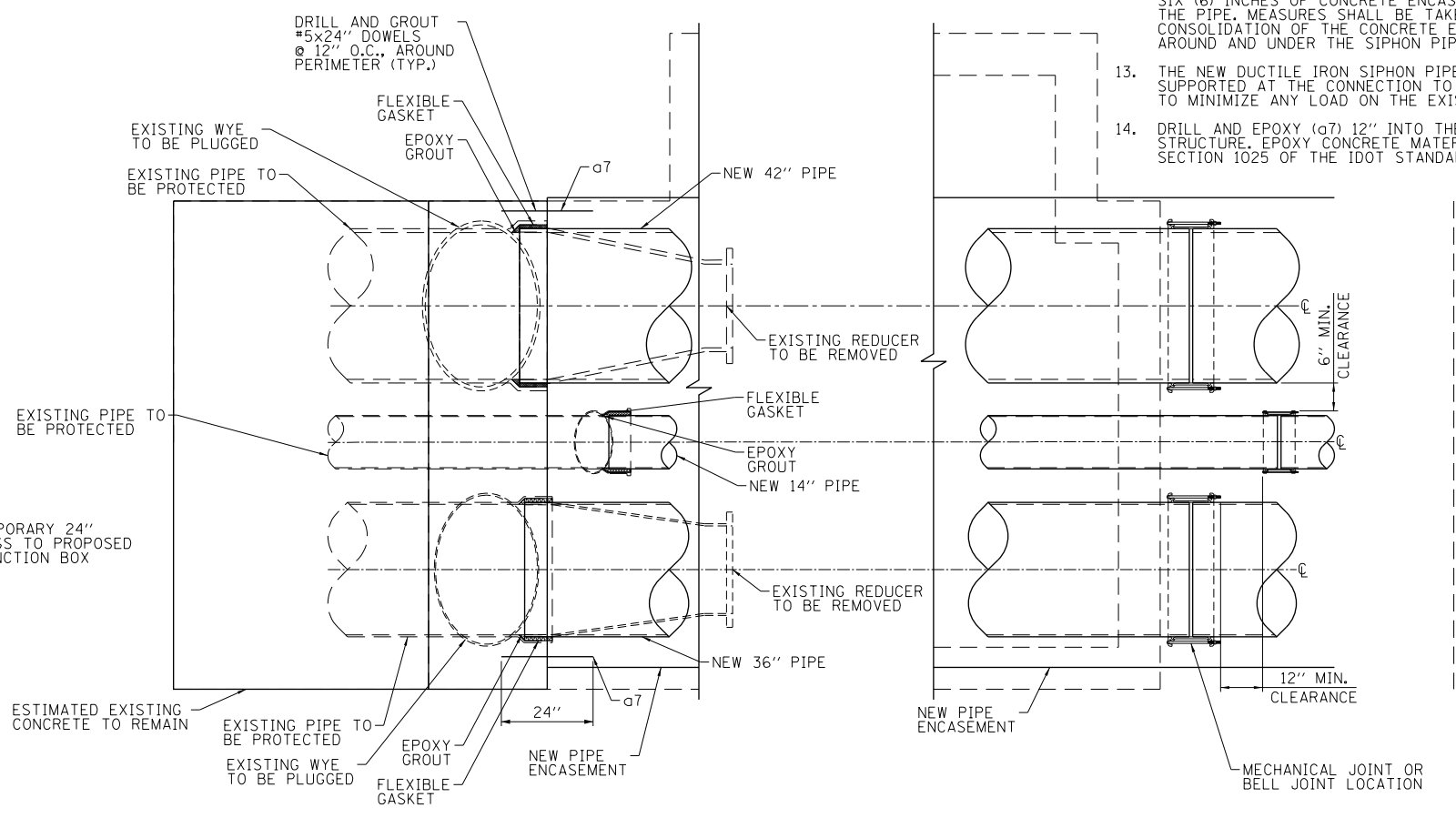
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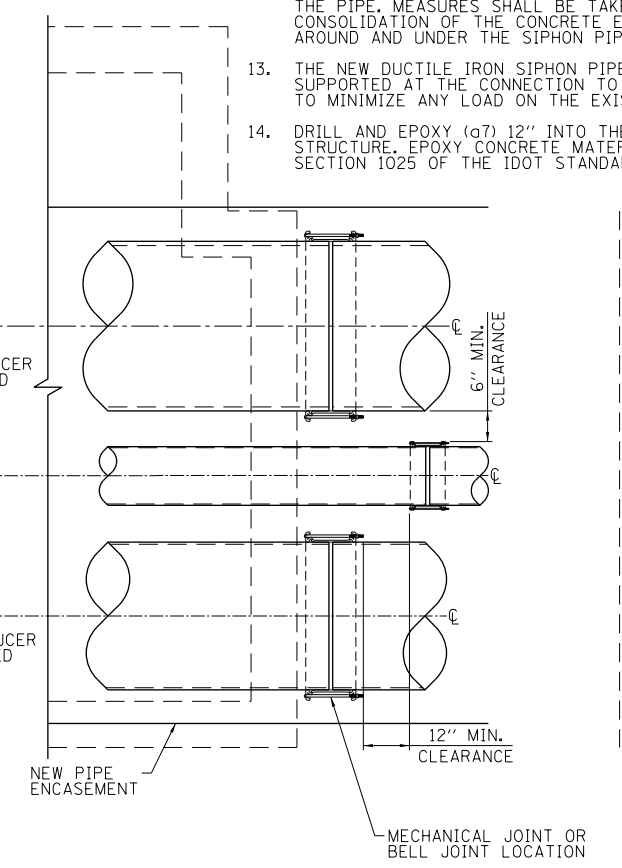
PIPE SUPPORT DETAIL
NOT TO SCALE



SECTION A-A
TEMPORARY BYPASS CONNECTION DETAIL
SCALE: 3/16"=1'-0"



PIPE CONNECTION DETAIL
SCALE: 1/4"=1'-0"



PIPE JOINT SPACING DETAIL
SCALE: 1/4"=1'-0"

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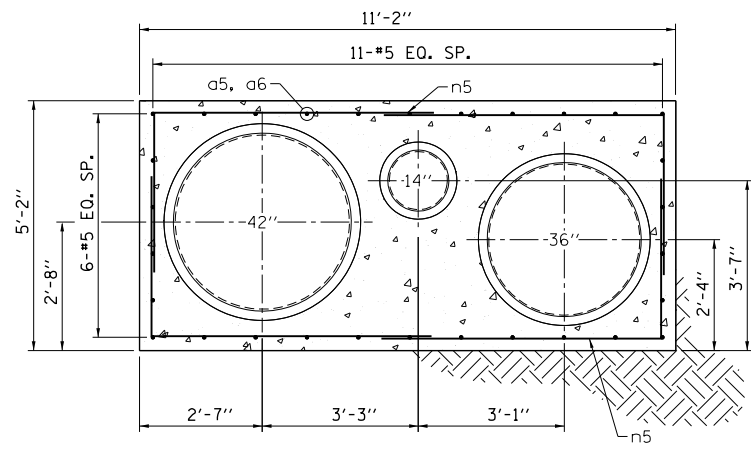
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIPHON DETAILS
PEORIA STREET

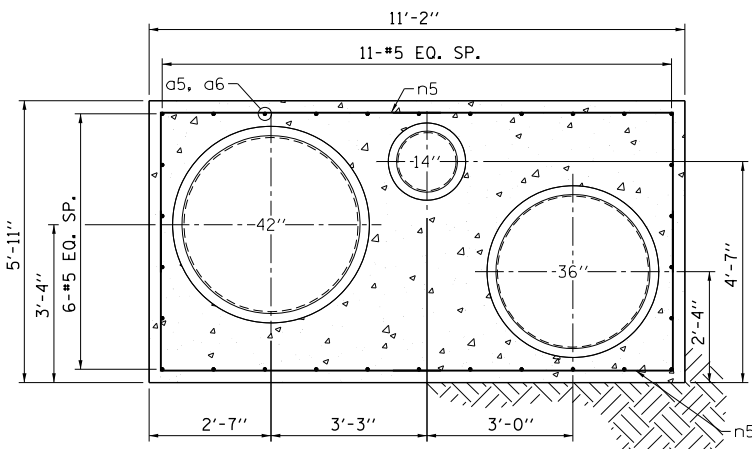
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 60W29			ILLINOIS FED. AID PROJECT	

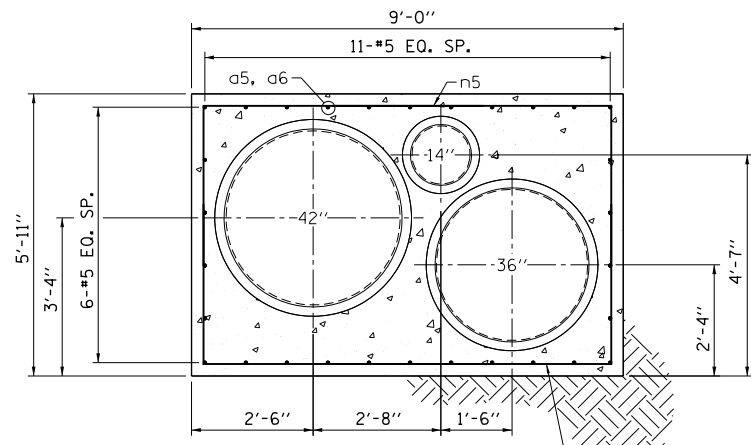
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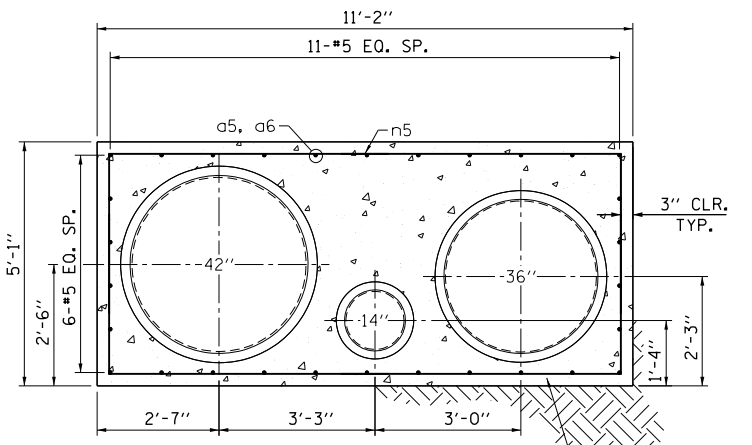
SECTION A-A



SECTION C-C



SECTION B-B



SECTION D-D

CONCRETE ENCASEMENT DETAIL
SCALE: 1/4"=1'-0"

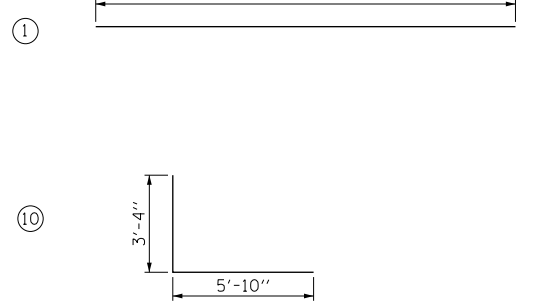
NOTES:

1. THE SURFACE UNDER THE SIPHON PIPE ENCASEMENT SHALL BE EXAMINED FOR ANY SOFT OR YIELDING AREAS. ANY AREAS FOUND SHALL BE EXCAVATED AND RECOMPACTED TO PROVIDE SUITABLE SUPPORT FOR THE SIPHON PIPES.
2. THE NEW DUCTILE IRON SIPHON PIPE AND FITTINGS SHALL BE SECURELY SUPPORTED TO INSURE NO MOVEMENT OCCURS PRIOR TO OR DURING PLACEMENT OF THE SIPHON PIPE CONCRETE ENCASEMENT. THE SIPHON PIPE SHALL BE SUPPORTED SO THAT A MINIMUM OF SIX (6) INCHES OF CONCRETE ENCASEMENT CAN BE PLACED AROUND THE PIPE. MEASURES SHALL BE TAKEN TO INSURE FULL CONSOLIDATION OF THE CONCRETE ENCASEMENT MATERIAL OCCURS AROUND AND UNDER THE SIPHON PIPES.
3. THE NEW DUCTILE IRON SIPHON PIPE SHALL BE SECURELY SUPPORTED AT THE CONNECTION TO THE EXISTING SIPHON PIPES TO MINIMIZE ANY LOAD ON THE EXISTING PIPE.

BILL OF MATERIAL (FOR INFORMATION ONLY)					
BAR MARK	NO.	SIZE	LENGTH	SHAPE	NOTES
a5	60	#5	40'-0"	1	LAP 2'-0" MIN.
a6	30	#5	23'-0"	1	LAP 2'-0" MIN.
a7	30	#5	2'-0"	1	DOWLES
n5	384	#5	9'-2"	10	LAP 1'-0" MIN, 3'-2" MAX
CONCRETE STRUCTURES			CU YD	131.7	
REINFORCEMENT BARS			POUND	6954	

1. COST OF CONCRETE STRUCTURES AND REINFORCEMENT BARS IS INCLUDED WITH SIPHON RECONSTRUCTION.

BAR SHAPE



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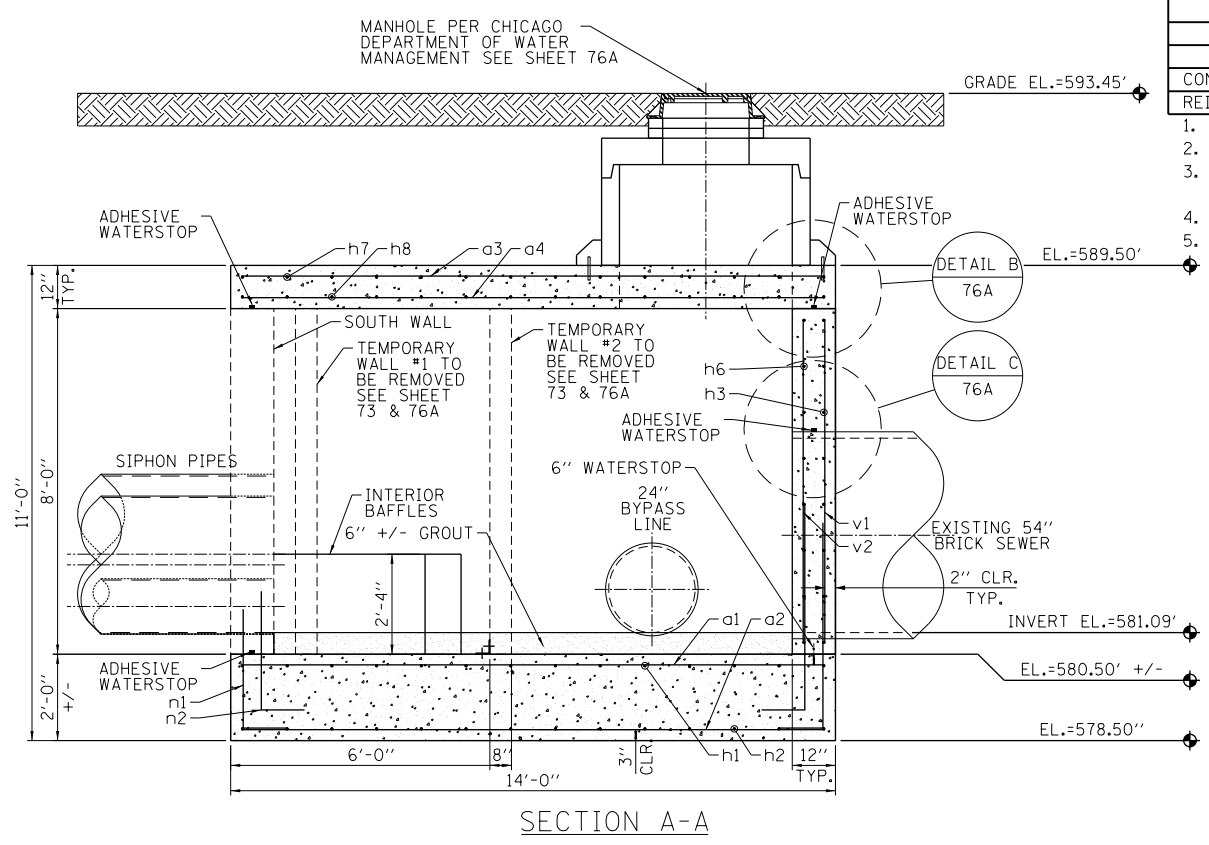
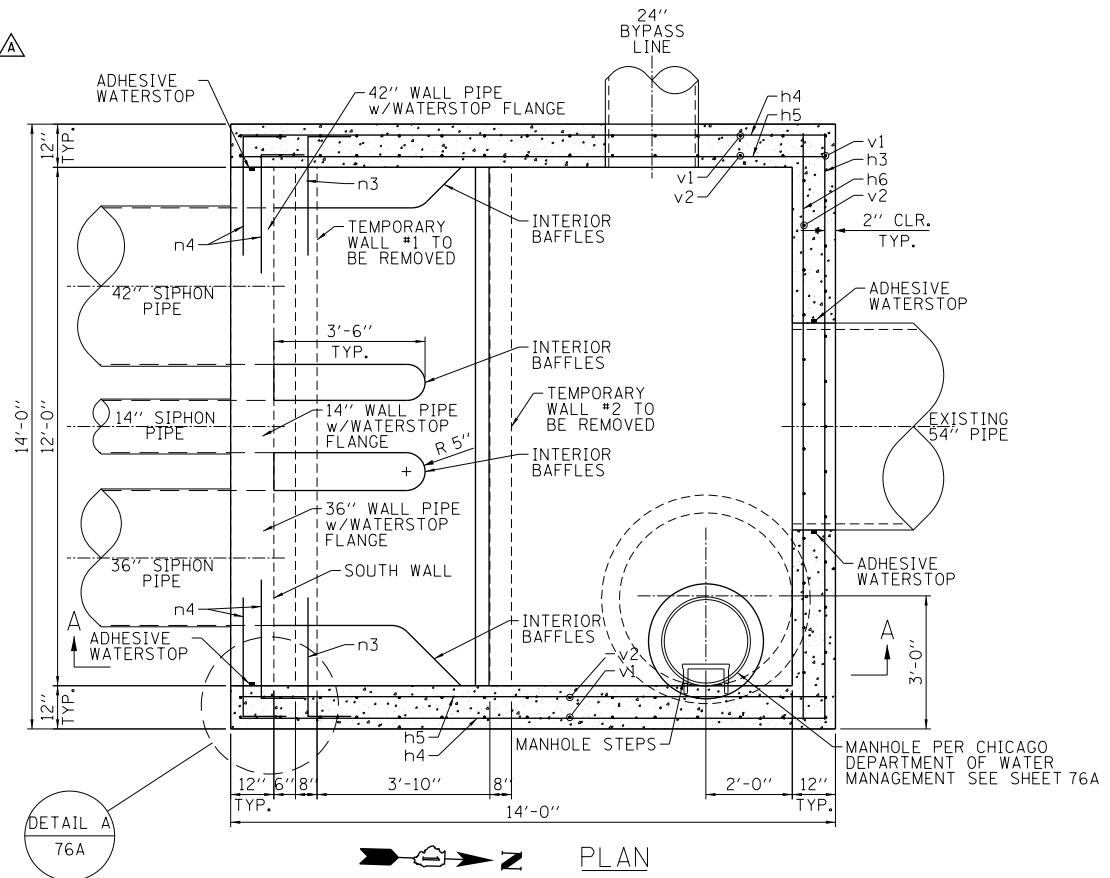
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIPHON DETAILS
PEORIA STREET

SCALE: NONE SHEET 6 OF 11 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	75
CONTRACT NO. 60W29				
ILLINOIS FED. AID PROJECT				

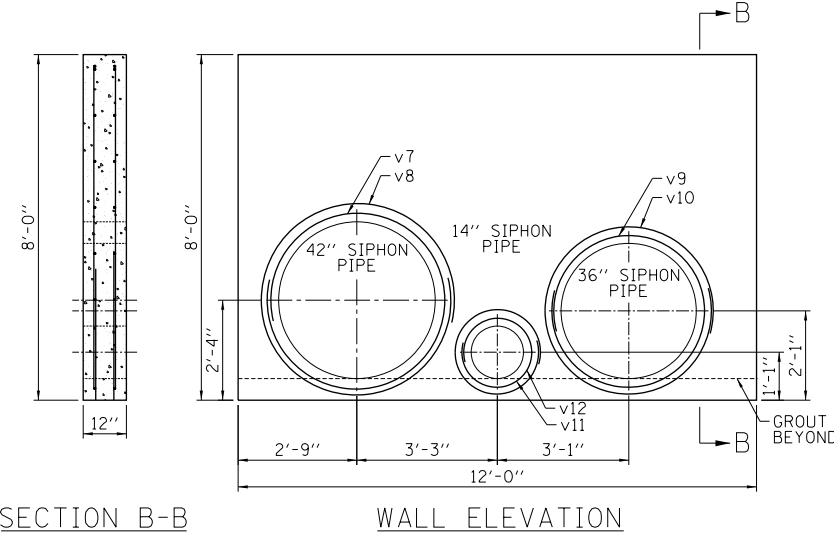
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PROPOSED OUTLET JUNCTION BOX
SCALE: 3/16"=1'-0"

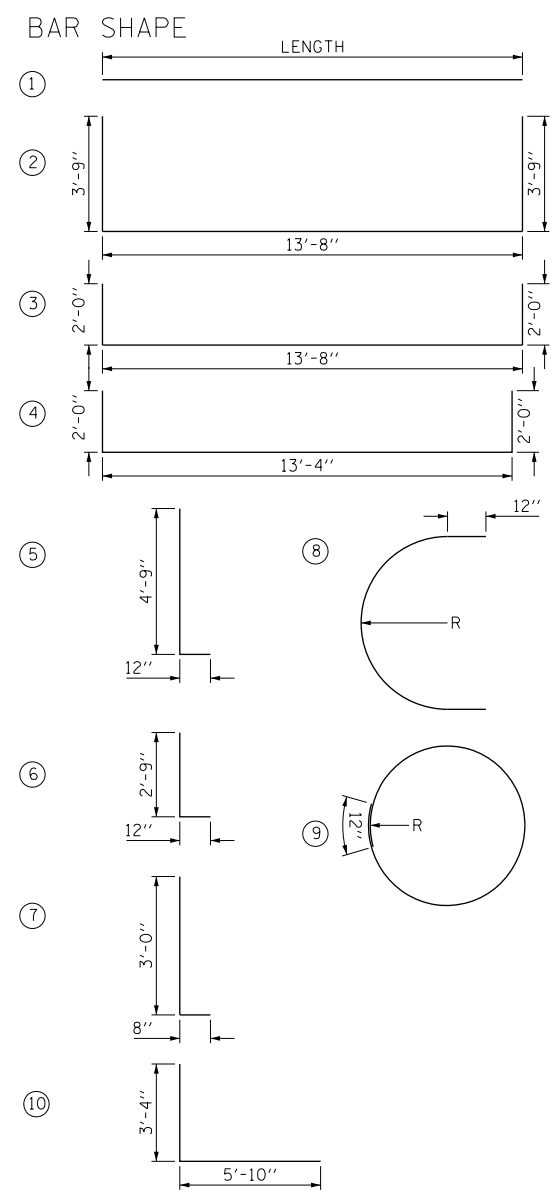
BILL OF MATERIAL (FOR INFORMATION ONLY)					
BAR MARK	NO.	SIZE	LENGTH	SHAPE	NOTES
a1	15	#5	13'-8"	1	SLAB LONG TOP
a2	15	#5	21'-2"	2	SLAB LONG BOTTOM
a3	15	#5	13'-8"	1	LID LONG TOP
a4	45	#5	13'-8"	1	LID LONG BOTTOM
h1	15	#5	13'-8"	1	SLAB TRANS TOP
h2	15	#5	21'-2"	2	SLAB TRANS BOTTOM
h3	9	#5	17'-8"	3	N WALL HOR OUTSIDE
h4	18	#5	13'-8"	1	E & W WALL HOR OUTSIDE
h5	18	#5	15'-0"	4	E & W WALL HOR INSIDE
h6	9	#5	15'-0"	1	N WALL HOR INSIDE
h7	15	#5	13'-8"	1	LID TRANS TOP
h8	45	#5	13'-8"	1	LID TRANS BOTTOM
h9	9	#5	11'-8"	1	DIVIDER WALL #1 HOR
h10	9	#5	11'-8"	1	DIVIDER WALL #2 HOR
h11	18	#5	11'-8"	1	S WALL HOR INSIDE & OUTSIDE
n1	56	#5	5'-9"	5	SLAB DOWELL OUTSIDE
n2	96	#5	5'-9"	5	SLAB DOWELL INSIDE
n3	18	#5	3'-8"	6	DIVIDER WALL #1 DOWELL
n4	36	#5	3'-8"	7	S WALL DOWELL BARS
v1	56	#5	7'-8"	1	WALL VERT OUTSIDE
v2	48	#5	7'-8"	1	WALL VERT INSIDE
v3	13	#5	7'-4"	1	DIVIDER WALL #1 VERT
v4	13	#5	7'-4"	1	DIVIDER WALL #2 VERT
v5	4	#5	10'-8"	8	54" PIPE OPENING REINF.
v6	4	#5	19'-4"	8	54" PIPE OPENING REINF.
v7	2	#5	13'-10"	9	42" PIPE OPENING REINF.
v8	2	#5	14'-11"	9	42" PIPE OPENING REINF.
v9	2	#5	12'-3"	9	36" PIPE OPENING REINF.
v10	2	#5	13'-4"	9	36" PIPE OPENING REINF.
v11	2	#5	6'-3"	9	14" PIPE OPENING REINF.
v12	2	#5	7'-3"	9	14" PIPE OPENING REINF.
v13	2	#5	8'-10"	9	24" BYPASS OPENING REINF.
v14	2	#5	9'-11"	9	24" BYPASS OPENING REINF.
v15	2	#5	15'-2"	9	48" MANHOLE RISER REINF.
v16	2	#5	16'-9"	9	48" MANHOLE RISER REINF.
CONCRETE STRUCTURES			CU YD	41.9	
REINFORCEMENT BARS			POUND	6605	

- COST OF CONCRETE STRUCTURES AND REINFORCEMENT BARS IS INCLUDED WITH SIPHON RECONSTRUCTION.
- REINFORCEMENT BARS IN PIPE OPENINGS HAVE NOT BEEN SUBTRACTED FROM BILL OF MATERIALS.
- MANHOLE RISER PIPE 48" RCP PIPE SECTION PER CHICAGO DEPARTMENT OF WATER MANAGEMENT (CDWM), INCLUDED IN PLANS.
- MANHOLE RISER AND MANHOLE LID PER CHICAGO DEPARTMENT OF WATER MANAGEMENT, INCLUDED IN PLANS.
- MANHOLE STEPS PER CHICAGO DEPARTMENT OF WATER MANAGEMENT, INCLUDED IN PLANS.



SECTION B-B
WALL ELEVATION
SCALE: 3/16"=1'-0"

- NOTES:
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WEST ROUTE SUPER HIGHWAY
PLANS FOR PEORIA STREET BRIDGE SECTION 2525.1B
F.A.ROUTE NO. 131 PROJECT UI261(29)
APPROVED JULY 1, 1949
 - CONTRACTOR SHALL VERIFY ALL DIMENSIONS, ELEVATIONS AND SIZES PRIOR TO CONSTRUCTION OR DEMOLITION AND REPORT DISCREPANCIES TO THE ENGINEER BEFORE PROCEEDING
 - CONDITION OF THE EXISTING 54" BRICK COMBINED SEWER PIPE IS EXPECTED TO BE GOOD AND THE EXTERIOR SURFACE SUITABLE FOR APPLICATION OF AN ADHESIVE WATER STOP SYSTEM.
 - DUCTILE IRON PIPES CONNECTING TO THE EXISTING CAST IRON PIPES SHALL BE CLOSELY MATED AND THE JOINT SEALED WITH RAN-NEK SEALANT OR EQUAL. THE JOINT SHALL BE SEALED FLUSH WITH AN EPOXY GROUT OR EQUAL.
 - PIPE SHALL BE FULLY SUPPORTED WITH A MINIMUM SPACE OF 6" FOR PIPE ENCASEMENT CONCRETE.
 - NEW DIP PIPES CONNECTING TO THE EXISTING SIPHON PIPES SHALL BE FULLY SUPPORTED SO THAT NO WEIGHT IS PLACED ON THE EXISTING PIPES.
 - ADHESIVE WATERSTOP SHALL BE APPLIED TO ALL FORMED SURFACES REQUIRED TO BE WATER TIGHT.
 - CENTER BULB WATER STOP SHALL BE CAST INTO ALL CAST JOINTS REQUIRED TO BE WATER TIGHT.
 - THREADED REINFORCEMENT CASTINGS SHALL BE LENTON OR APPROVED EQUAL. PLUGS SHALL BE USED ON ALL COUPLERS.
 - WALL PIPE ASSEMBLIES SHALL BE DUCTILE IRON WITH WATERSTOP RINGS.

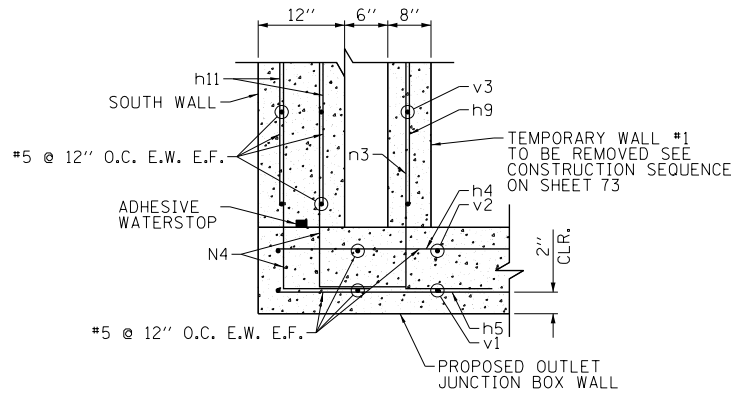


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PLOT DATE = 12/19/2013	DATE - 10/30/2013	REVISED -

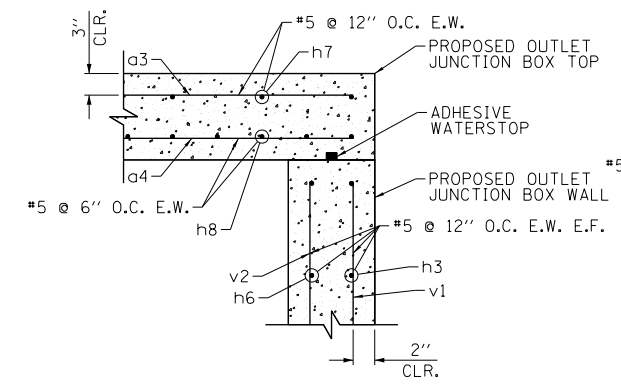
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIPHON OUTLET BOX DETAILS			
PEORIA STREET			
SCALE: NONE	SHEET 7	OF 11 SHEETS	STA. TO STA.

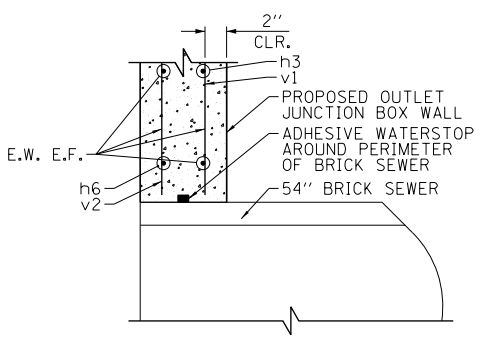
F.A.I. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	76
CONTRACT NO. 60W29				
ILLINOIS FED. AID PROJECT				



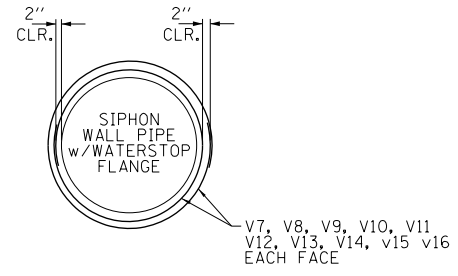
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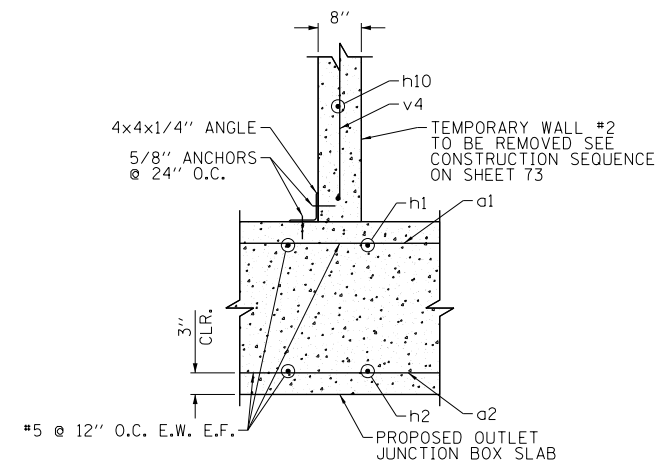
DETAIL "B"
SIPHON OUTLET BOX LID DETAIL
SCALE: 3/8"=1'-0"



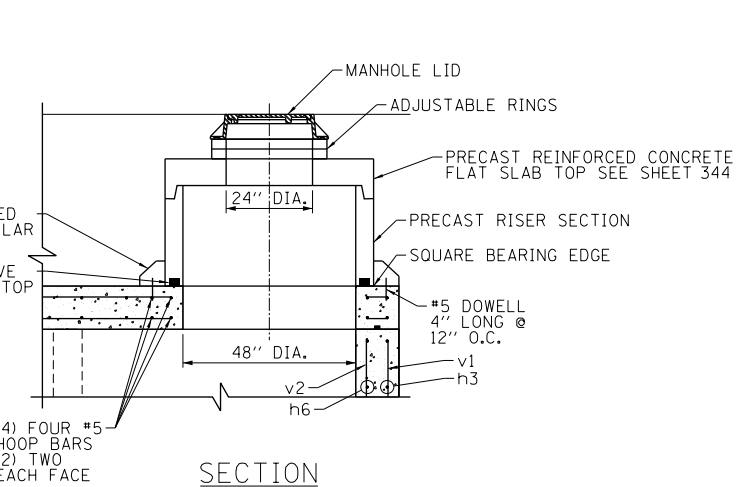
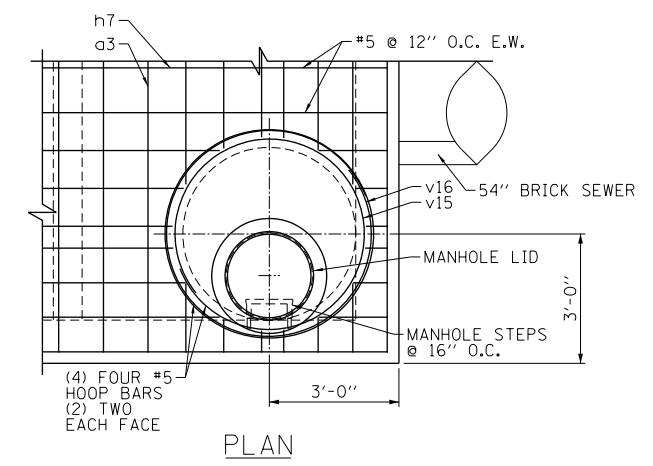
DETAIL "C"
BRICK SEWER CONNECTION DETAIL
SCALE: 3/8"=1'-0"



WALL PIPE REINFORCEMENT DETAIL
SCALE: 3/16"=1'-0"

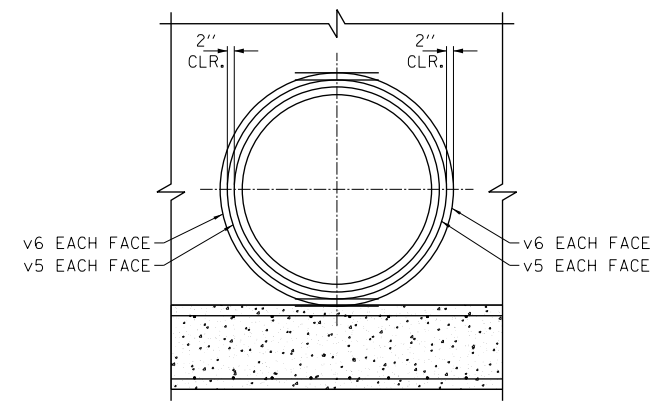


TEMPORARY WALL #2 DETAIL
SCALE: 3/8"=1'-0"



NOTE:
REINFORCING BAR TO BE FIELD CUT
IN AREA AROUND MANHOLE.

MANHOLE DETAIL
SCALE: 3/16"=1'-0"



BRICK SEWER
WALL PIPE REINFORCEMENT DETAIL
SCALE: 3/16"=1'-0"

- NOTES:
- ALL DIMENSIONS, ELEVATIONS AND PIPE SIZES SHOWN HAVE BEEN TAKEN FROM RECORD DRAWINGS TITLED (INCLUDED IN THE PLANS);
WEST ROUTE SUPER HIGHWAY
PLANS FOR PEORIA STREET BRIDGE SECTION 2525.1B
F.A.ROUTE NO. 131 PROJECT UI261(29)
APPROVED JULY 1, 1949
 - CONTRACTOR SHALL VERIFY ALL DIMENSIONS, ELEVATIONS AND SIZES PRIOR TO CONSTRUCTION OR DEMOLITION AND REPORT DISCREPANCIES TO THE ENGINEER BEFORE PROCEEDING
 - CONDITION OF THE EXISTING 54" BRICK COMBINED SEWER PIPE IS EXPECTED TO BE GOOD AND THE EXTERIOR SURFACE SUITABLE FOR APPLICATION OF AN ADHESIVE WATER STOP SYSTEM.
 - DUCTILE IRON PIPES CONNECTING TO THE EXISTING CAST IRON PIPES SHALL BE CLOSELY MATED AND THE JOINT SEALED WITH RAN-NEK SEALANT OR EQUAL. THE JOINT SHALL BE SEALED FLUSH WITH AN EPOXY GROUT OR EQUAL.
 - PIPE SHALL BE FULLY SUPPORTED WITH A MINIMUM SPACE OF 6" FOR PIPE ENCASEMENT CONCRETE.
 - NEW DIP PIPES CONNECTING TO THE EXISTING SIPHON PIPES SHALL BE FULLY SUPPORTED SO THAT NO WEIGHT IS PLACED ON THE EXISTING PIPES.
 - ADHESIVE WATERSTOP SHALL BE APPLIED TO ALL FORMED SURFACES REQUIRED TO BE WATER TIGHT.
 - CENTER BULB WATER STOP SHALL BE CAST INTO ALL CAST JOINTS REQUIRED TO BE WATER TIGHT.
 - THREADED REINFORCEMENT CASTINGS SHALL BE LENTON OR APPROVED EQUAL. PLUGS SHALL BE USED ON ALL COUPLERS.
 - WALL PIPE ASSEMBLIES SHALL BE DUCTILE IRON WITH WATERSTOP RINGS.

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PLOT SCALE = 100.0000' / in.	CHECKED - JWC	REVISED -
PLOT DATE = 12/19/2013	DATE - 10/30/2013	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SIPHON OUTLET BOX DETAILS		F.A.I. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PEORIA STREET		90/94/290	2013-011R	COOK	356	76A
SCALE: NONE		SHEET 7A OF 11 SHEETS		STA.	TO STA.	
ILLINOIS FED. AID PROJECT						

CONTRACT NO. 60W29	
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INDEX OF SHEETS

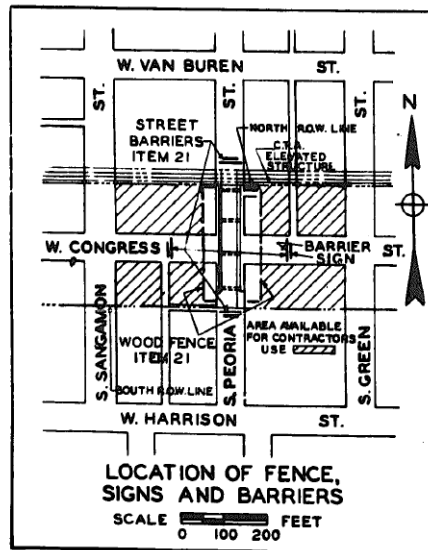
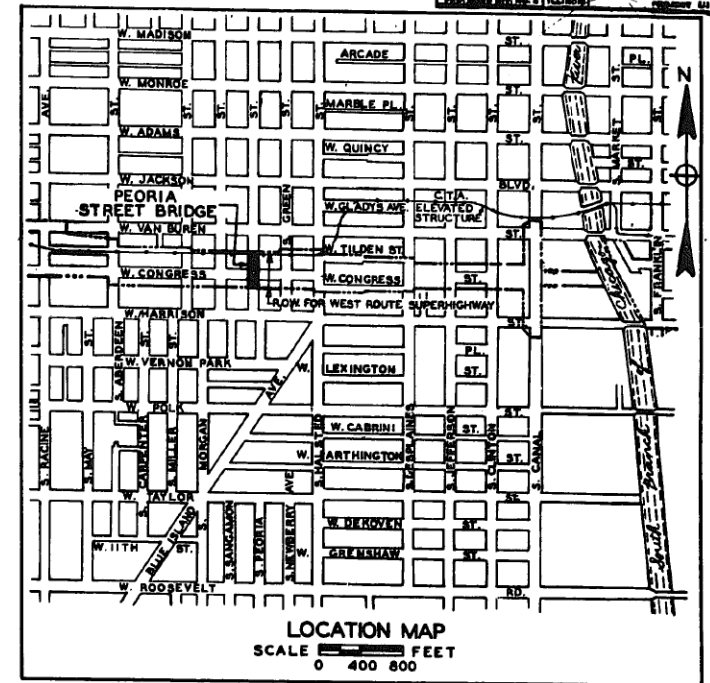
SHEET NO	DESCRIPTION
1	TITLE SHEET
2	LOCATION PLAN AND PROFILE
3	EXCAVATION AND BACKFILL PAY LIMITS
4	DESIGN DATA
5	STEEL FRAMING PLAN
6	GIRDER DETAILS
7	STRUCTURAL STEEL DETAILS
8	GIRDER BEARINGS
9	EXPANSION JOINTS
10	PILING PLAN
11	PIERS
12	SOUTH ABUTMENT
13	NORTH ABUTMENT
14	SOUTH WING WALLS
15	CONCRETE DECK DETAILS
16	DECK REINFORCEMENT
17	BAR LIST
18	ARCHITECTURAL FINISH
19	RAILING DETAILS
20	SEWER SIPHON GENERAL PLAN AND PROFILE
21	INLET JUNCTION CHAMBER SPECIAL PIPE FITTINGS
22	OUTLET JUNCTION CHAMBER
23	SEWER DETAILS
24	PAVEMENT DETAILS
25	MISCELLANEOUS TIMBER DETAILS
26	MANHOLE FRAMES AND COVERS MISCELLANEOUS STEEL
27	SEWER AND SIPHON CONCRETE AND REINFORCEMENT DETAILS
28	INLET JUNCTION CHAMBER REINFORCEMENT DETAILS
29	OUTLET JUNCTION CHAMBER REINFORCEMENT DETAILS
30	BAR LIST FOR SIPHON

STATE OF ILLINOIS
CITY OF CHICAGO
DEPARTMENT OF SUBWAYS AND SUPERHIGHWAYS

WEST ROUTE SUPERHIGHWAY
PLANS FOR
PEORIA STREET BRIDGE
SECTION 2525.1-1B
F.A.ROUTE NO.131 PROJECT UI 261(29)

SUMMARY OF QUANTITIES

ITEM NO.	UNIT	QUANTITY	DESCRIPTION
1	CU.YDS.	15,500	SPECIAL EXCAVATION
2	CU.YDS.	1,550	BACKFILL
3	CU.YDS.	2,100	CLASS X CONCRETE
4	LBS.	200,000	REINFORCEMENT BARS
5	LBS.	600,000	STRUCTURAL STEEL
6	LIN.FT.	12,636	FURNISHING TREATED PILES, 20.1 TO 38 FEET LONG
7	LIN.FT.	12,636	DRIVING TIMBER PILES, 36 FEET LONG
8	EACH	4	TEST PILES
9	LBS.	340,000	STEEL SHEETING AND BRACING
10	LBS.	9,300	ALUMINUM RAILING
11	TONS	80	BITUMINOUS CONCRETE BINDER COURSE
12	TONS	80	SHEET ASPHALT SURFACE COURSE, SUBCLASS J-1
13	SQ.YDS.	850	BITUMINOUS MACADAM PAVEMENT
14	LBS.	24,000	CAST IRON PIPE, 12 FEET AND OVER IN LENGTH
15	LBS.	2,500	CAST IRON PIPE, OVER 3 FEET AND LESS THAN 12 FEET IN LENGTH
16	LBS.	38,000	CAST IRON PIPE, 3 FEET OR LESS IN LENGTH, AND FITTINGS
17	LIN.FT.	330	36-INCH REINFORCED CONCRETE PIPE
18	LIN.FT.	300	42-INCH REINFORCED CONCRETE PIPE
19	LBS.	5,200	MISCELLANEOUS IRON CASTINGS
20	LBS.	800	MISCELLANEOUS STEEL
21	M.FT.B.M.	11	MISCELLANEOUS TIMBER
22	EACH	2	SHORING OF ELEVATED COLUMNS
23	LIN.FT.	5,600	3/2-INCH CONDUIT
24	LIN.FT.	2,100	4-INCH CONDUIT
25	MONTHS	12	PUMPING



CONVENTIONAL SIGNS

- COMMONWEALTH EDISON CO. _____
- WESTERN UNION CO. _____
- WATER _____
- GAS _____
- SEWER _____
- SEWER MANHOLE (Symbol) _____
- CATCH BASIN (Symbol) _____
- VALVE VAULT (Symbol) _____
- LIGHT STANDARD (Symbol) _____
- FIRE HYDRANT (Symbol) _____
- R.O.W. LINE _____

RECOMMENDED FOR APPROVAL DATE _____

DISTRICT ENGINEER
PUBLIC ROADS ADMINISTRATION
FEDERAL WORKS AGENCY

APPROVED _____ DATE _____

DIVISION ENGINEER
PUBLIC ROADS ADMINISTRATION
FEDERAL WORKS AGENCY

APPROVED JULY 1, 1949
J. B. Gumbach
COMMISSIONER OF SUBWAYS AND SUPERHIGHWAYS

THE DEPARTMENT OF
PUBLIC WORKS AND BUILDINGS
DIVISION OF HIGHWAYS

PASSED _____ 19____

APPROVED _____ 19____
BRIDGE ENGINEER

APPROVED _____ 19____
ENGINEER OF DESIGN

APPROVED _____ 19____
CHIEF HIGHWAY ENGINEER

APPROVED _____ 19____
DIRECTOR

PLANS APPROVED
BY STATE DIVISION OF HIGHWAYS

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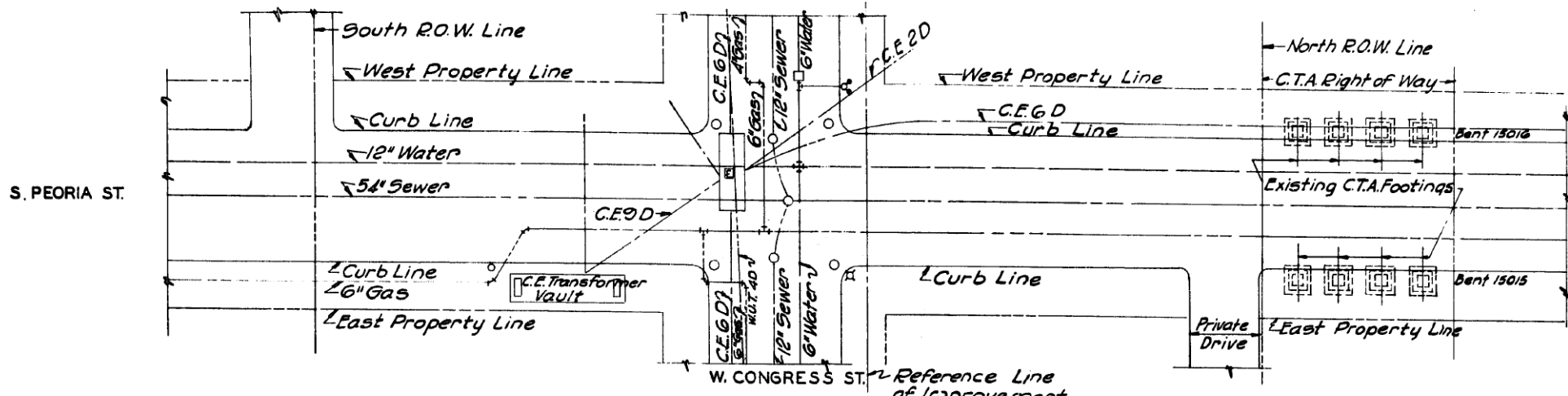
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

AS BUILT SIPHON PLANS
PEORIA STREET

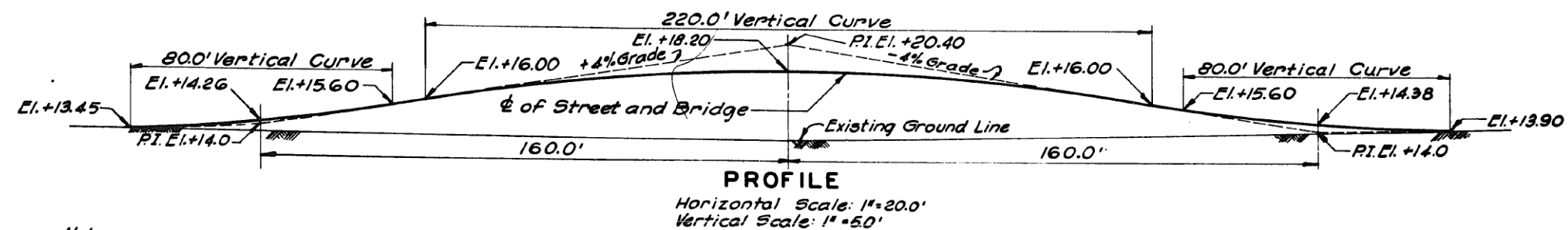
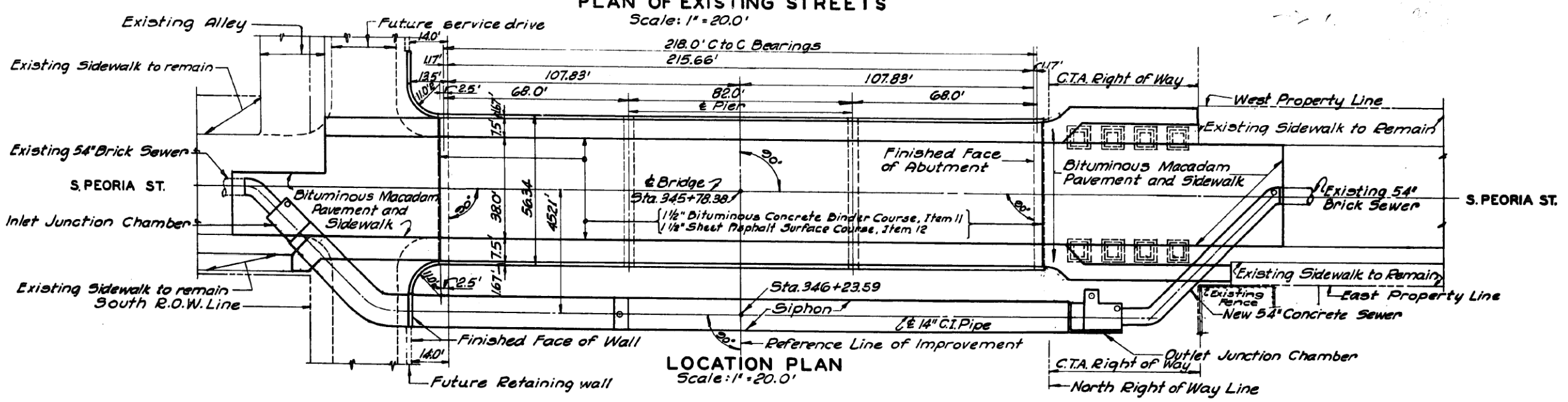
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F.A.I. R.T.E. 90/94/290	SECTION 2013-011R	COUNTY COOK	TOTAL SHEETS 356	SHEET NO. 76B
ILLINOIS FED. AID PROJECT				CONTRACT NO. 60W29

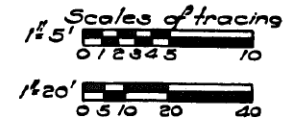
PROJECT NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
P.A. 181	2525.1-1B	COOK	30	2



Notes:
 For General Plan of Sewer Siphon see Sheet 20
 For Plan of Bituminous Macadam Pavement and Sidewalk see Sheet 24
 For Excavation Pay Limits see Sheet 3



Notes:
 The location and sizes of the conduits, pipes, manholes, and other sub-surface utility equipment shown on this drawing are based on the best information obtainable and are believed to be accurate but cannot be guaranteed. Additional abandoned utilities may be encountered. For procedure in handling these abandoned utilities see Section 18 of Special Provisions (City of Chicago)



Drawn By E. Zullo
 Traced By E. Zullo
 Checked By Jan J. [unclear]
 Approved [unclear]

CITY OF CHICAGO
 DEPARTMENT OF SUBWAYS AND SUPERHIGHWAYS
 WEST ROUTE SUPERHIGHWAY
 SECTION 2525.1-1B
 PEORIA STREET BRIDGE

LOCATION PLAN AND PROFILE
 SCALE: AS NOTED
 SEPTEMBER, 1949
 SHEET NO. 2 OF 30 SHEETS

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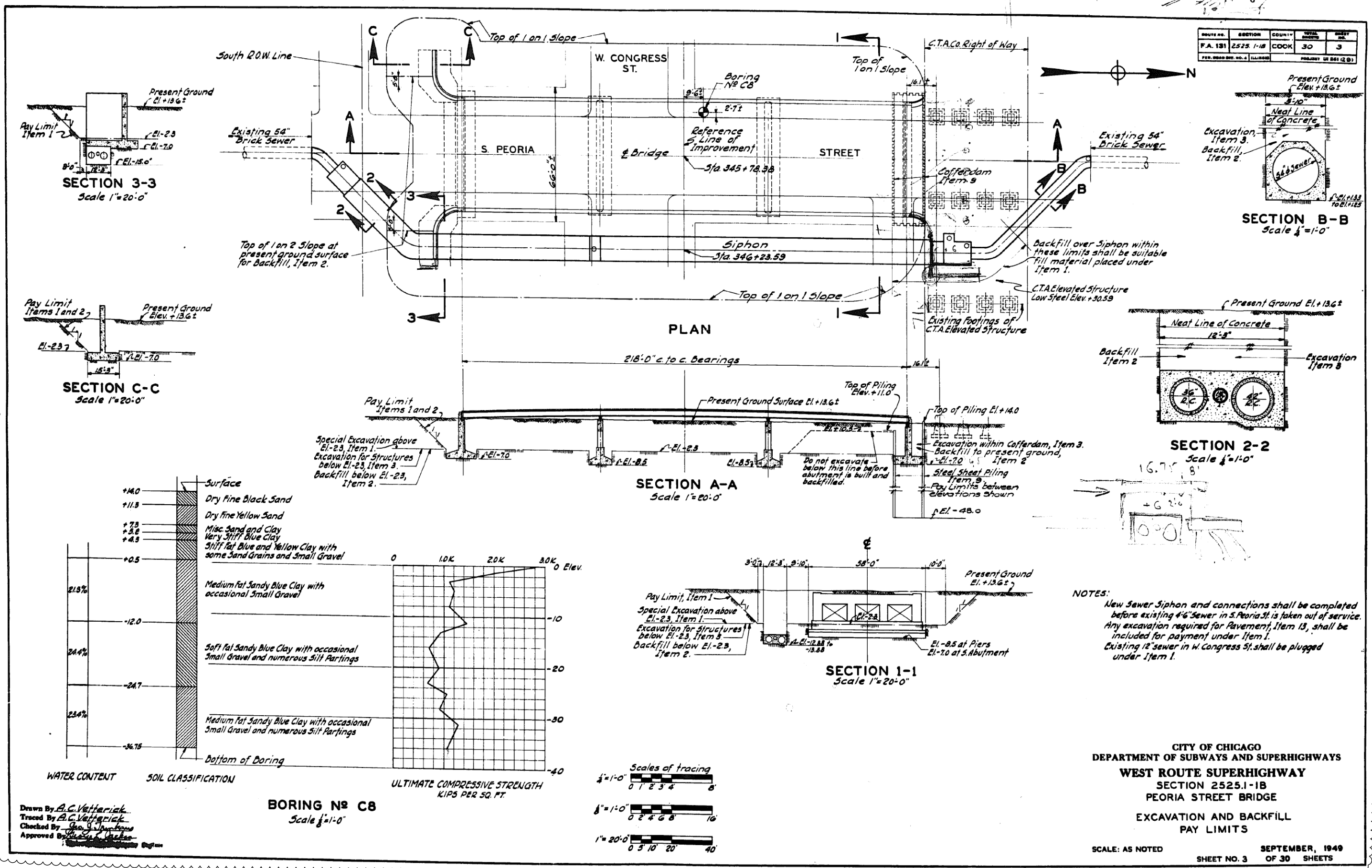
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PLOT DATE = 12/19/2013	DATE - 10/30/2013	REVISED -	

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

AS BUILT SIPHON PLANS PEORIA STREET			
SCALE: NONE	SHEET 2	OF 14 SHEETS	STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	76C
CONTRACT NO. 60W29				
ILLINOIS FED. AID PROJECT				

ROUTE NO.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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FED. ROAD DIST. NO. 6 ILLINOIS			PROJECT U.S. 661 (20)	

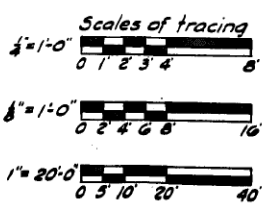


NOTES:
 New Sewer Siphon and connections shall be completed before existing 4'-6" sewer in S. Peoria St. is taken out of service. Any excavation required for Pavement, Item 13, shall be included for payment under Item 1.
 Existing 12" sewer in W. Congress St. shall be plugged under Item 1.

CITY OF CHICAGO
 DEPARTMENT OF SUBWAYS AND SUPERHIGHWAYS
WEST ROUTE SUPERHIGHWAY
 SECTION 2525.1-1B
 PEORIA STREET BRIDGE
 EXCAVATION AND BACKFILL
 PAY LIMITS
 SCALE: AS NOTED
 SEPTEMBER, 1949
 SHEET NO. 3 OF 30 SHEETS

Drawn By *A.C. Vetterick*
 Traced By *A.C. Vetterick*
 Checked By *Geo. J. Johnson*
 Approved By *Charles E. DeLo*

BORING NO. C8
 Scale 1/4"=1'-0"



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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

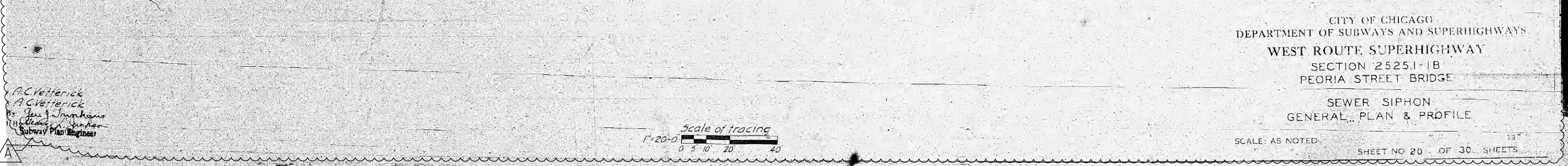
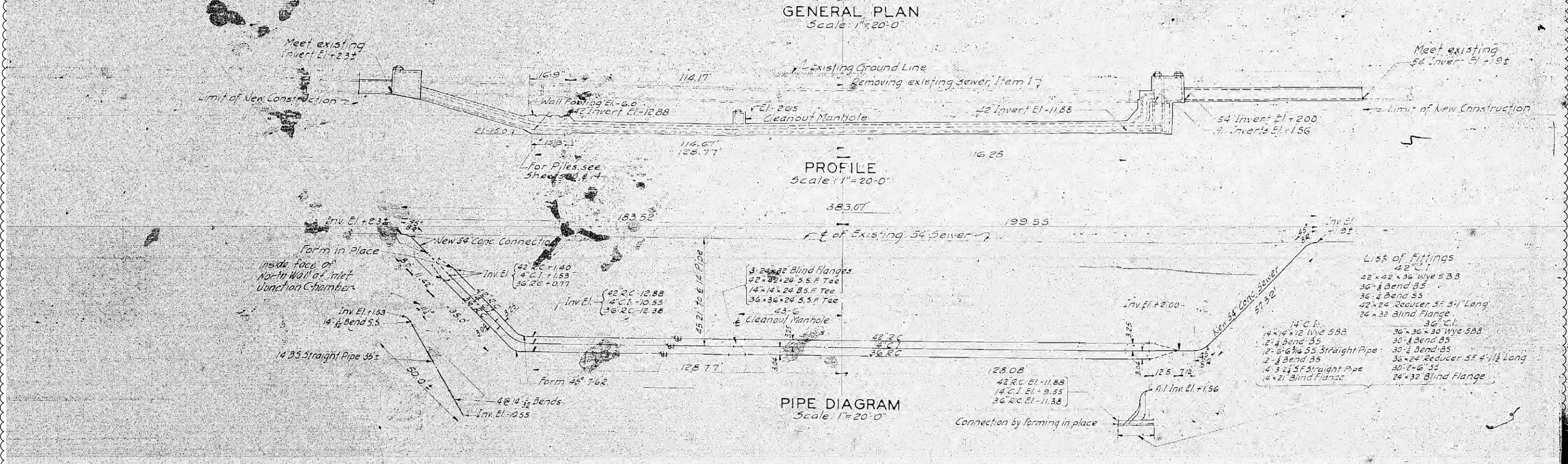
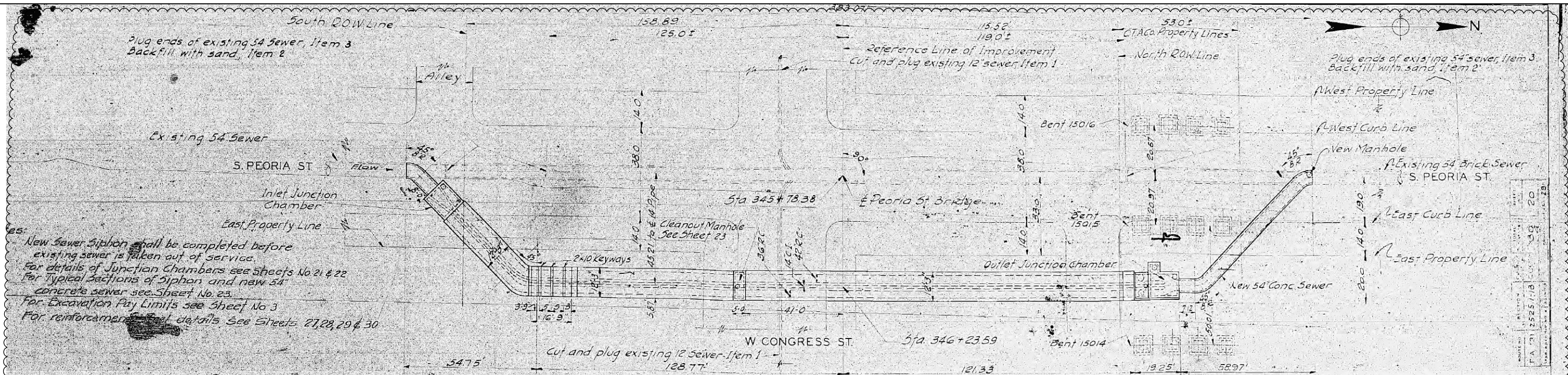
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 PEORIA STREET
 SCALE: NONE SHEET 3 OF 14 SHEETS STA. TO STA.

F.A.I. R.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-01R	COOK	356	76D
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60W29	

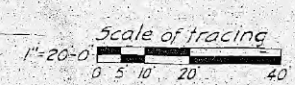
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A.C. Vetterick
A.C. Vetterick
By: Jan J. Junkins
IBS, Inc.
Subway Plan Engineer



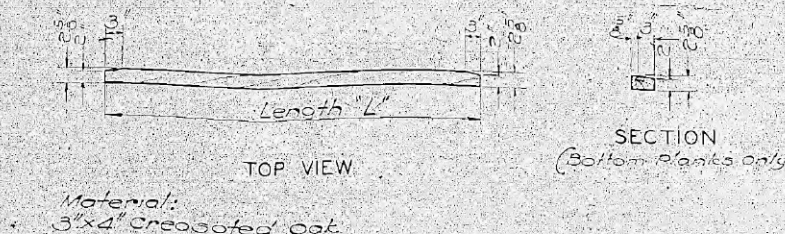
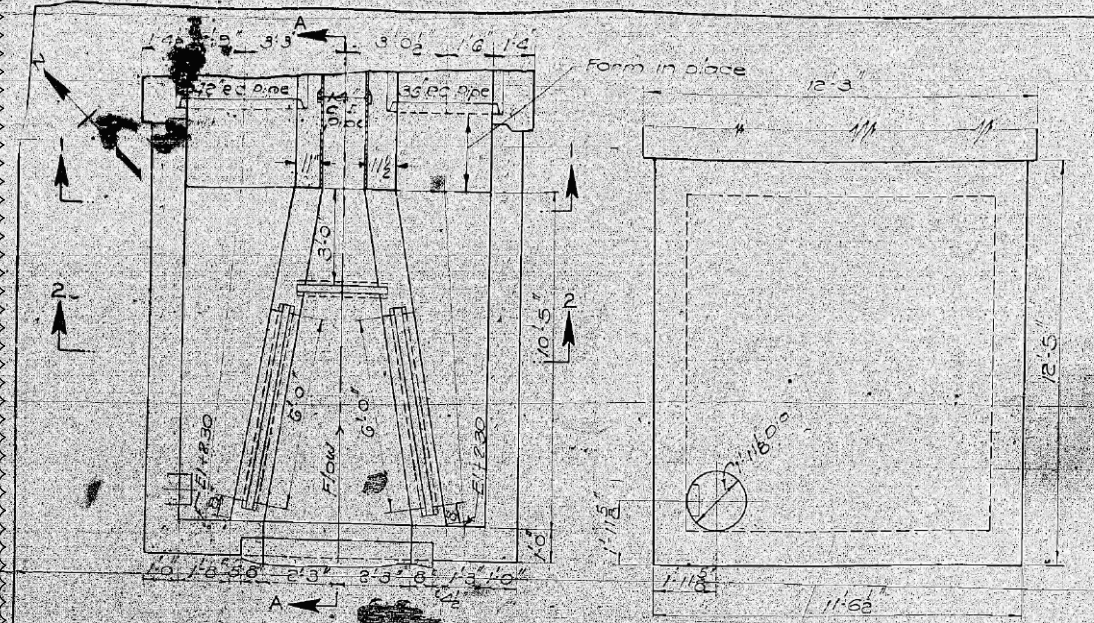
CITY OF CHICAGO
DEPARTMENT OF SUBWAYS AND SUPERHIGHWAYS
WEST ROUTE SUPERHIGHWAY
SECTION 2525.1-1B
PEORIA STREET BRIDGE

SEWER SIPHON
GENERAL PLAN & PROFILE

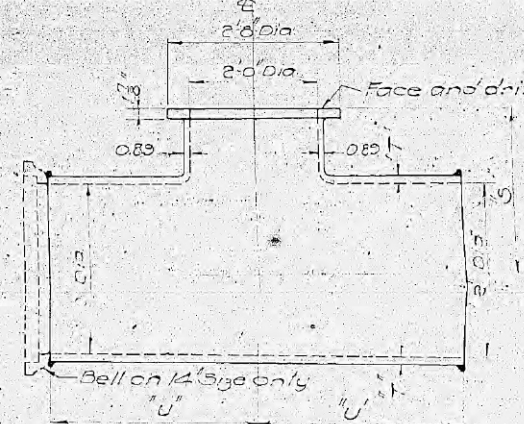
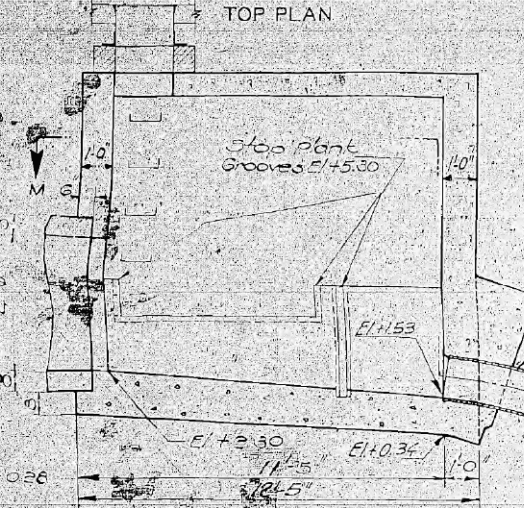
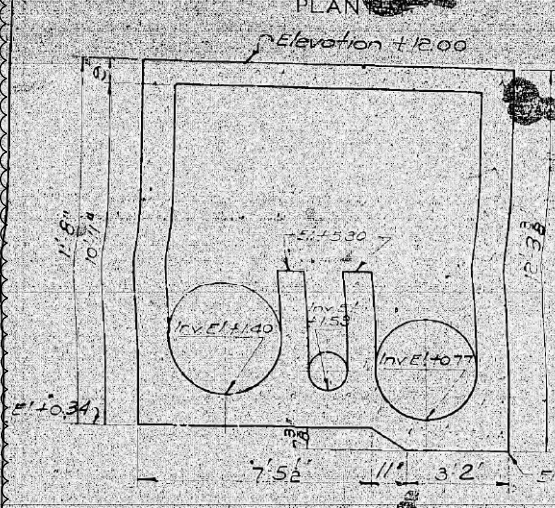
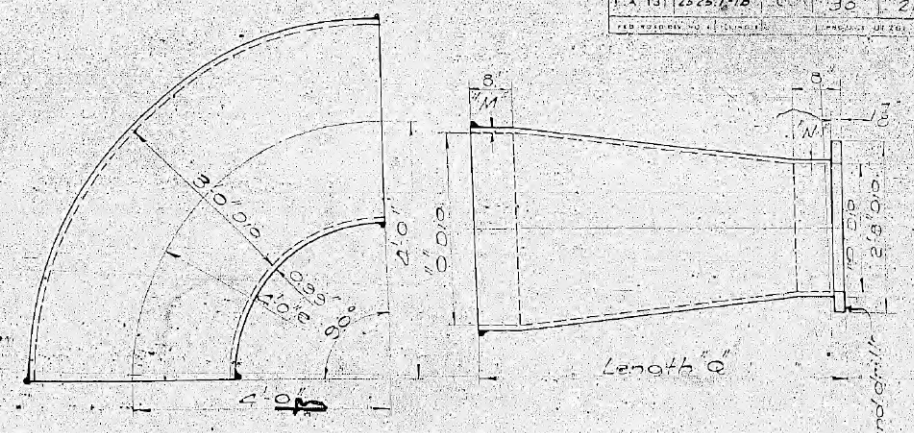
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SHEET NO 20 OF 30 SHEETS

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	PLOT DATE = 12/19/2013	DATE - 10/30/2013	REVISED -											ILLINOIS FED. AID PROJECT

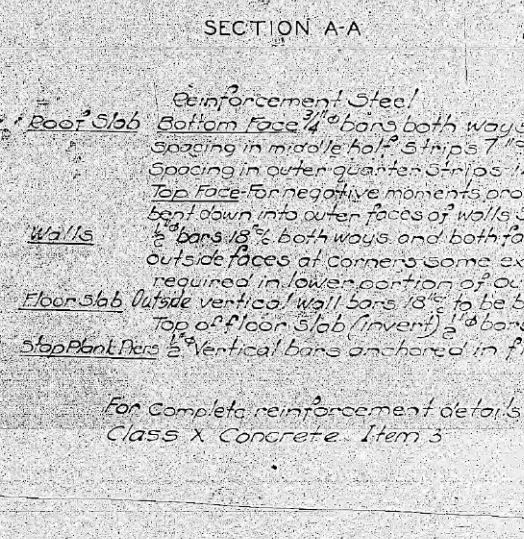
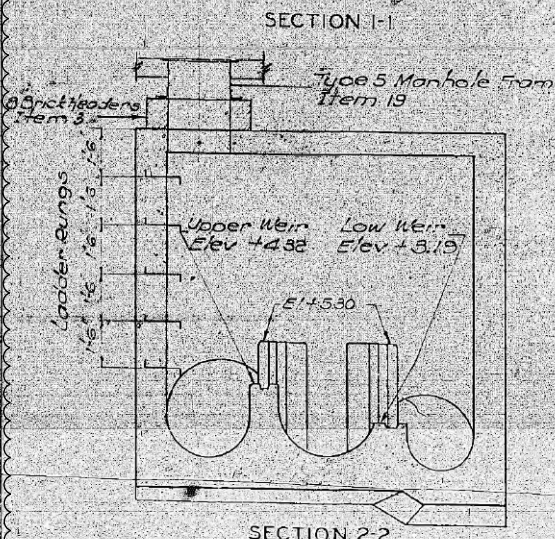
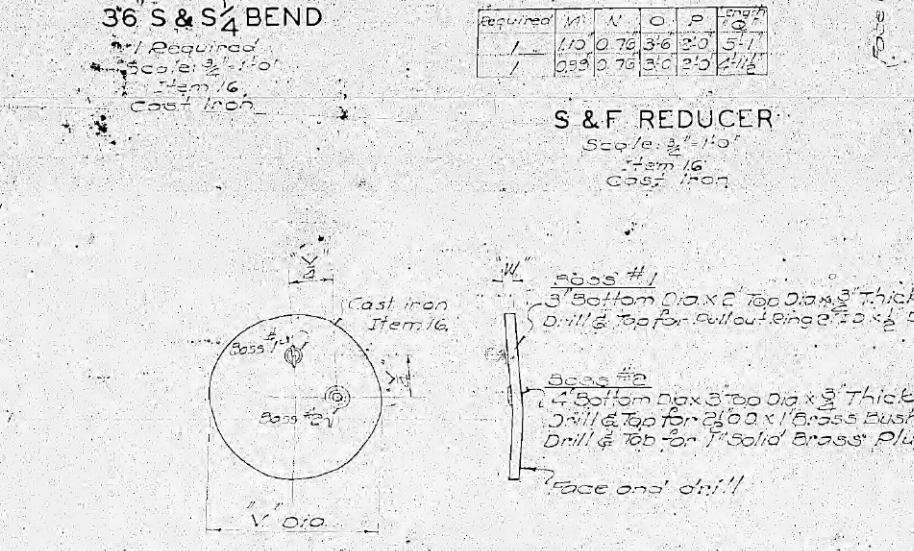
PROJECT NO.	SECTION	SHEET	TOTAL SHEETS
F.A. 131 2525.1-1B	2013-011R	30	21



Required	Length	Junction Chamber	Bottom Planks
12	1'3"	Outlet	1'3"
12	2'9"	Inlet	2'9"
12	2'9"	Outlet	2'9"
12	3'3"	Outlet	3'3"
10	6'3"	Inlet Weirs	2'6'3"

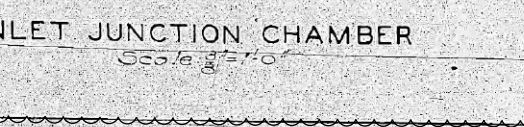
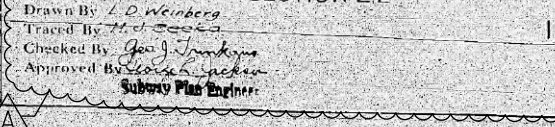


Required	V	W	U
1	14	2'10"	2'3"
1	36	3'0"	3'2"
1	22	3'3"	3'2"

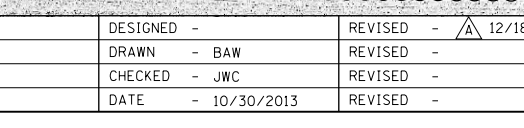
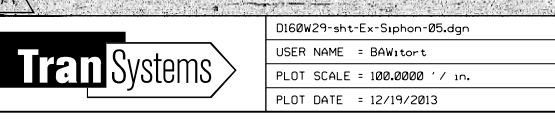


Required	V	W	U
1	14	2'10"	2'3"
1	36	3'0"	3'2"
1	22	3'3"	3'2"

Required	V	W	U	Q	P	R
1	14	0.75	3.6	3.0	5.1	9
1	36	0.75	3.0	3.5	4.1	16



Required	V	W	U
1	14	2'10"	2'3"
1	36	3'0"	3'2"
1	22	3'3"	3'2"



Required	V	W	U
1	14	2'10"	2'3"
1	36	3'0"	3'2"
1	22	3'3"	3'2"



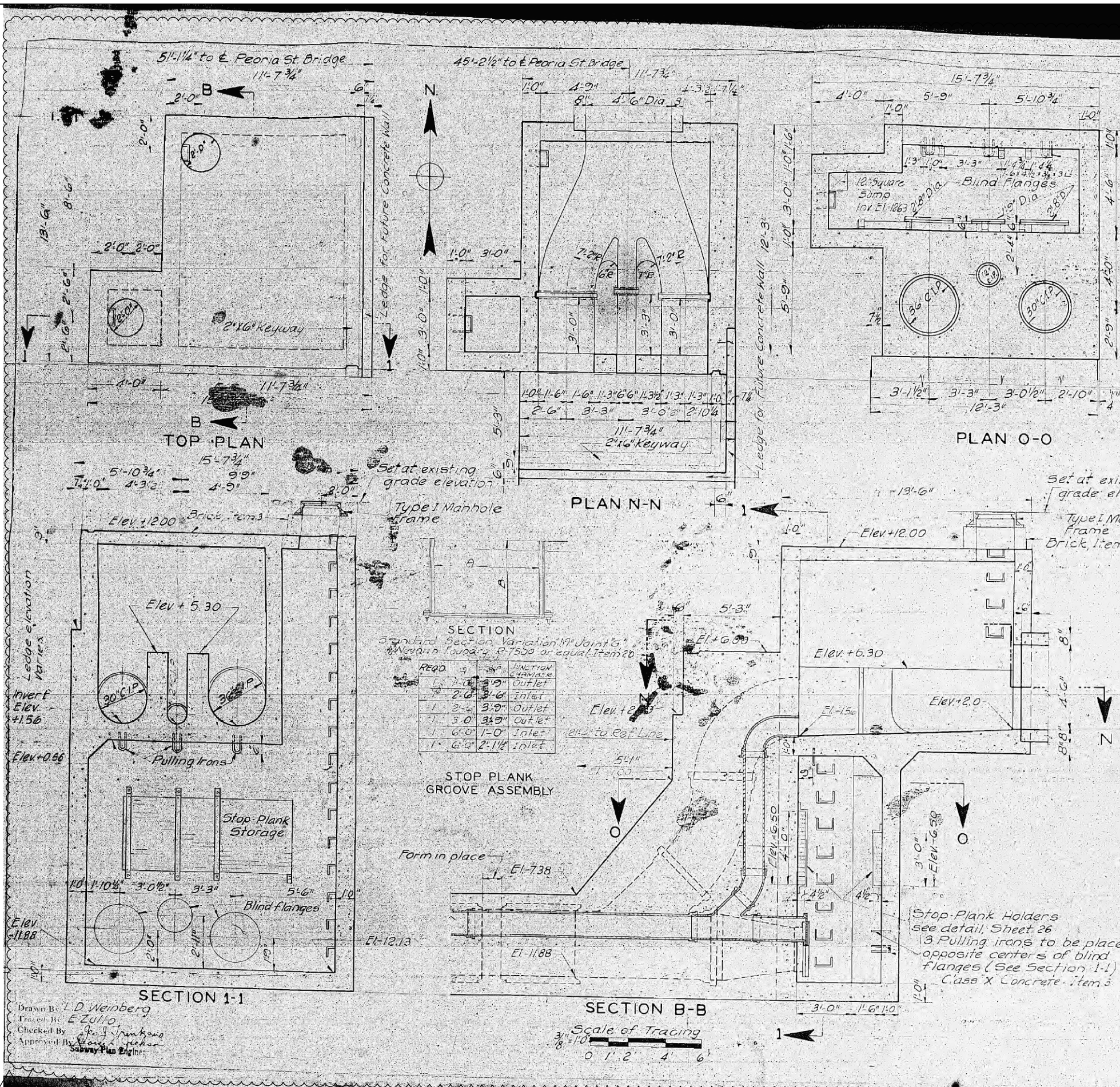
Drawn By: L.D. Weinberg
 Traced By: M.C. Sosa
 Checked By: Geo. J. Tankoski
 Approved By: [Signature]
 Subcity Plan Engineer

Reinforcement Steel
 Roof Slab Bottom Face 1/4" bars both ways
 Spacing in middle half strips 7" @
 Spacing in outer quarter strips 14" @
 Top Face For negative moments provide short bars
 bent down into outer faces of walls spacing same as bottom bars
 Walls 1/2" bars 18" @ both ways and both faces with continuity in
 outside faces at corners some extra vertical bars
 required in lower portion of outer faces
 Floor Slab Outside vertical wall bars 18" @ to be bent into floor slab bottom
 Top of floor slab (invert) 1/2" bars 18" @ both ways
 Stop Plank Face 1/2" vertical bars anchored in floor 1/2" ties
 For complete reinforcement details see Sheet 28
 Class X Concrete Item 3

INLET JUNCTION CHAMBER
 Scale 3/8" = 1'-0"

CITY OF CHICAGO
 DEPARTMENT OF SUBWAYS AND SUPERHIGHWAYS
 WEST ROUTE SUPERHIGHWAY
 SECTION 2525.1-1B
 PEORIA STREET BRIDGE
 INLET JUNCTION CHAMBER
 SPECIAL PIPE FITTINGS
 SCALE: AS NOTED
 SHEET NO. 21 OF 30 SHEETS

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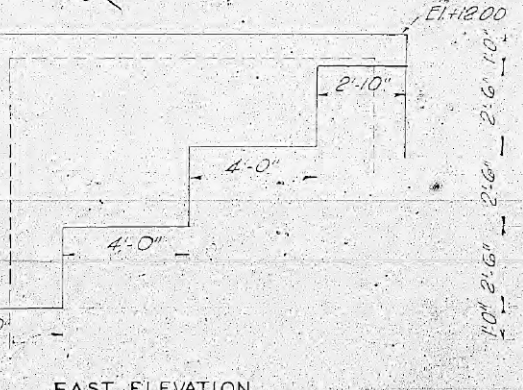
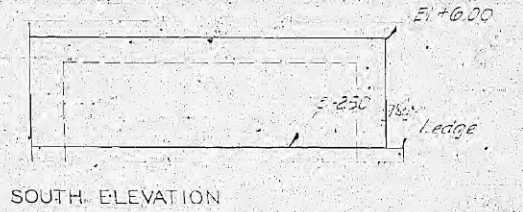


Reinforcement Steel
 Roof Slab - Bottom face $\frac{3}{4}$ " bars both ways, spacing in middle half strips 6×9 " spacing in outer quarter strips 15×6 "
 Top face For negative moments provide short $\frac{3}{4}$ " bars bent down into outer faces of walls. Spacing same as bottom bars
 Walls - $\frac{1}{2}$ " bars 18×6 " both ways and both faces with continuity in outside faces at corners. Additional reinforcement required in lower portion of walls including encasement of pipe risers.
 Floor Slab - Vertical wall bars 18×6 " in outside of Wall to be bent into floor slab bottom. Top of floor slab (invert) $\frac{1}{2}$ " bars 18×6 " both ways
 Stop Plank Piers - $\frac{1}{2}$ " vertical bars anchored in floor and $\frac{1}{2}$ " ties
 For complete reinforcement details see Sheet 29

SECTION
 Standard Section Variation M-Joint 5"
 Minimum Foundation R-7500 or equal Item 20

REQD	QTY	DESCRIPTION
1	2'-0"	3'-0" Outlet
1	2'-0"	3'-0" Inlet
1	2'-0"	3'-0" Outlet
1	3'-0"	3'-0" Outlet
1	6'-0"	1'-0" Inlet
1	6'-0"	2'-11/2" Inlet

STOP PLANK GROOVE ASSEMBLY



LEDGE LOCATION FOR FUTURE CONCRETE WALL

CITY OF CHICAGO
 DEPARTMENT OF SUBWAYS AND SUPERHIGHWAYS
 WEST ROUTE SUPERHIGHWAY
 SECTION 2525.1-1B
 PEORIA STREET BRIDGE
 OUTLET JUNCTION CHAMBER

SCALE AS NOTED
 SHEET NO. 22 OF 30 SHEETS

Drawn By: L.D. Weinberg
 Traced By: EZullo
 Checked By: [Signature]
 Approved By: [Signature]
 Subway Plan Engineer

SECTION B-B
 Scale of Tracing
 3/8" = 1'-0"
 0 1 2 4 6



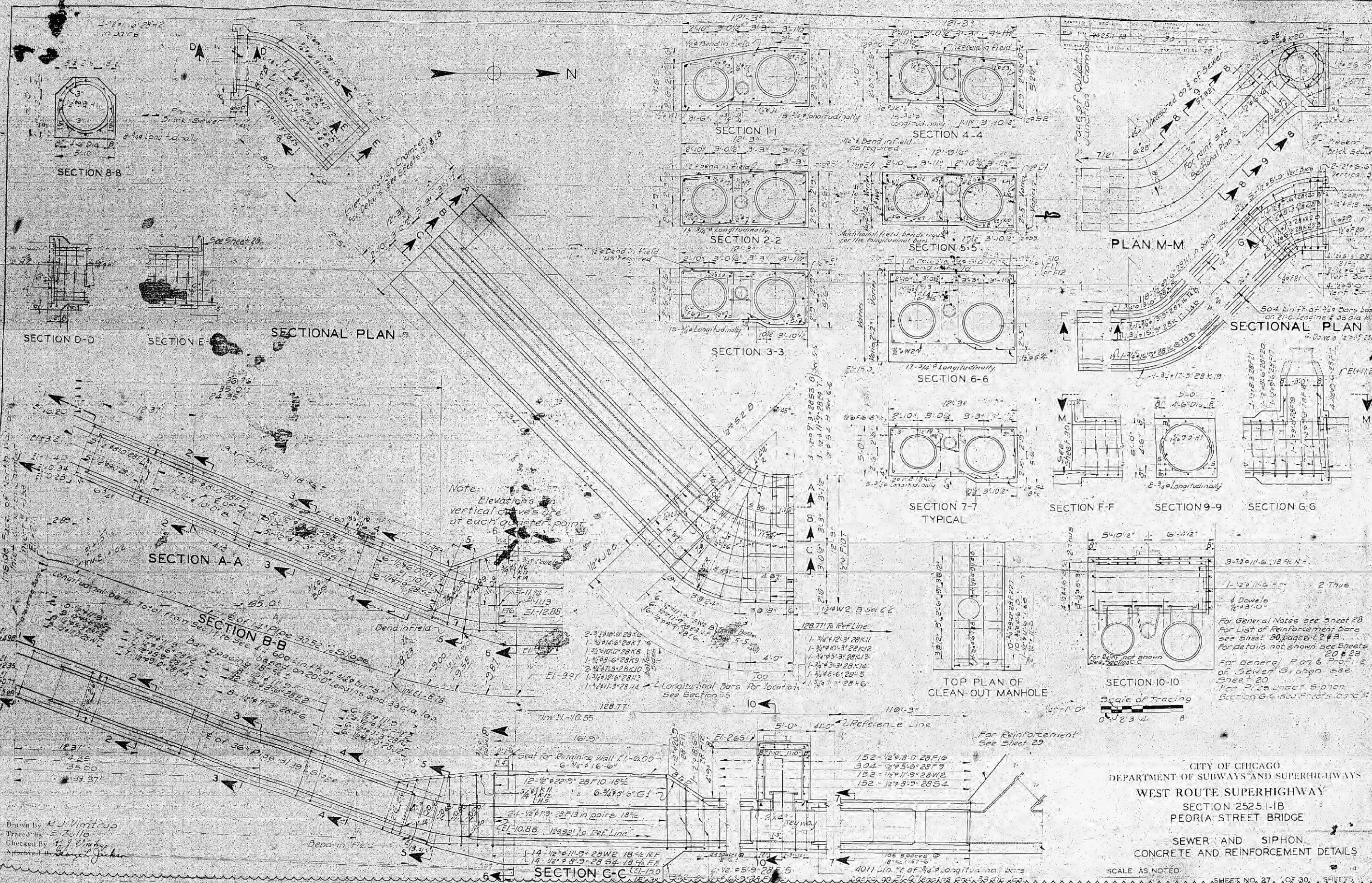
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PLOT DATE = 12/19/2013	DATE - 10/30/2013	REVISED -	

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

AS BUILT SIPHON PLANS PEORIA STREET			
SCALE: NONE	SHEET 6	OF 14 SHEETS	STA. TO STA.

F.A.I. RTE. 90/94/290	SECTION 2013-011R	COUNTY COOK	TOTAL SHEETS 356	SHEET NO. 76G
CONTRACT NO. 60W29				ILLINOIS FED. AID PROJECT

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Drawn by: R.J. Vintrop
 Traced by: E. Zullo
 Checked by: P. J. Cimber
 Approved by: George J. Jucker

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

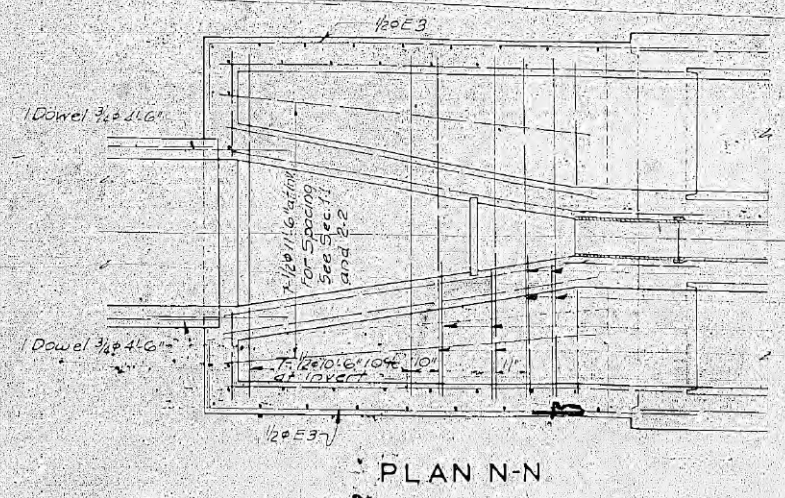
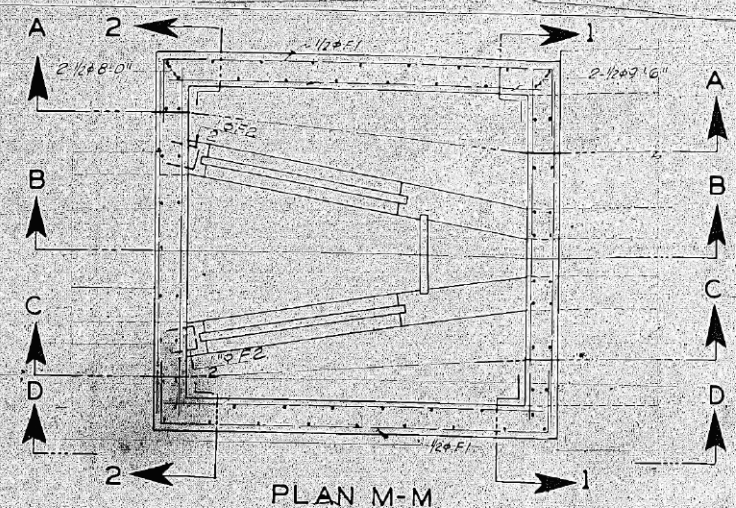
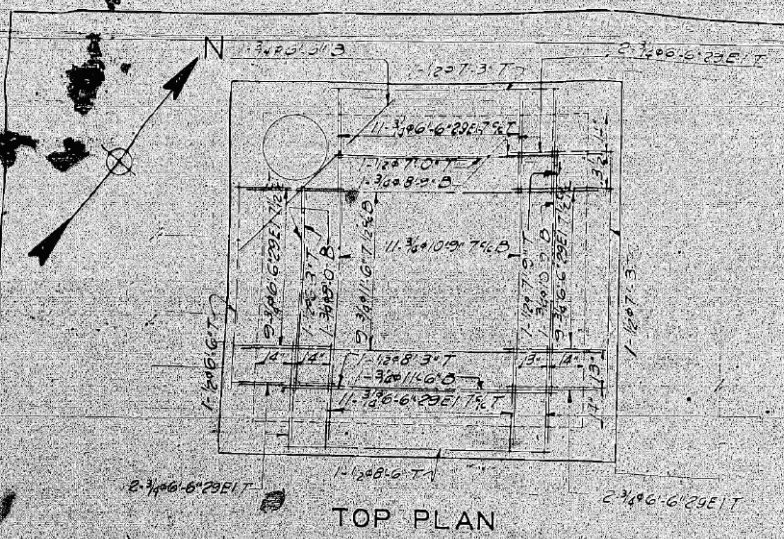
AS BUILT SIPHON PLANS
 PEORIA STREET

CITY OF CHICAGO
 DEPARTMENT OF SUBWAYS AND SUPERHIGHWAYS
 WEST ROUTE SUPERHIGHWAY
 SECTION 2525.1-B
 PEORIA STREET BRIDGE
 SEWER AND SIPHON
 CONCRETE AND REINFORCEMENT DETAILS

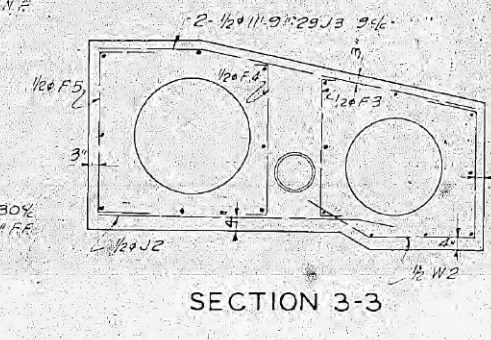
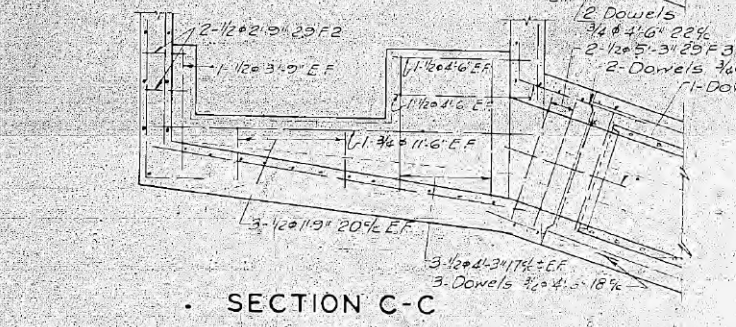
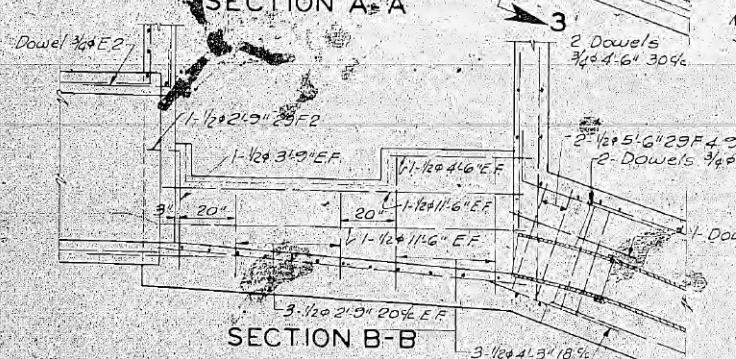
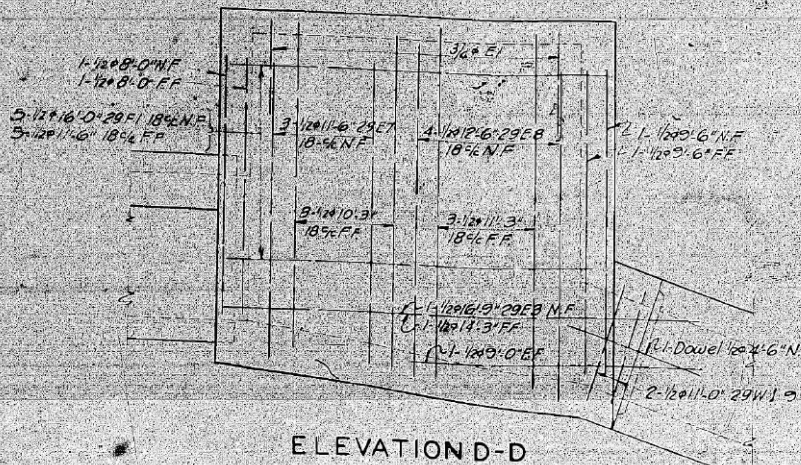
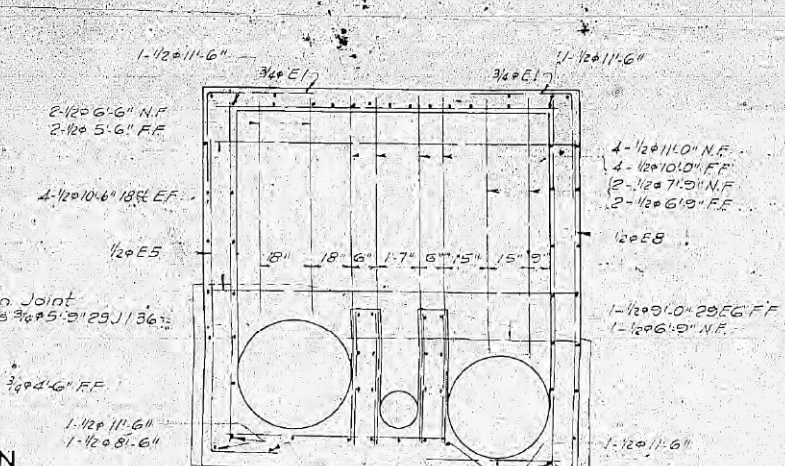
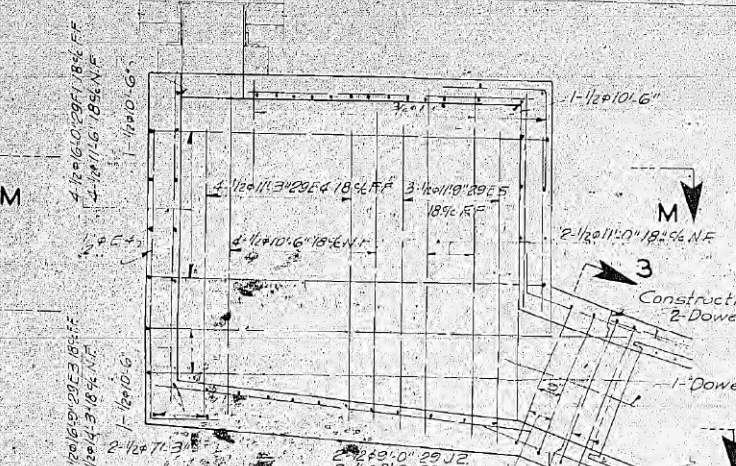
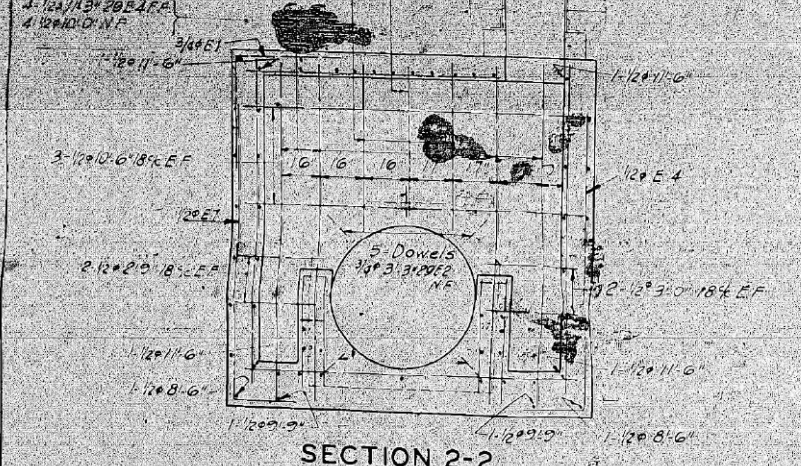
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PLOT DATE = 12/19/2013	DATE - 10/30/2013	REVISED -	

SCALE: NONE	SHEET 11	OF 14 SHEETS	STA. TO STA.
F.A.I. RTE. 90/94/290	SECTION 2013-011R	COUNTY COOK	TOTAL SHEETS 356
			SHEET NO. 76L
			CONTRACT NO. 60W29
			ILLINOIS FED. AID PROJECT





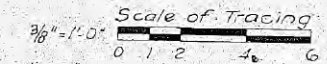
- 1-1203/6" FF
- 1-1204/0" NF
- 2-1204/0" EF
- 2-1205/3" NF
- 1-1204/13" 20E4 FF
- 1-1204/0" NF



- 2-1204-3" 11% at Invert
- 2-1201/0" 20E2 11% at Invert
- 3-1204/0" 10% at Invert
- 3-1201/0" 20E1 10% at Invert

General Notes
 Unless otherwise noted on drawing, reinforcement bars shall have the following minimum covering of concrete:
 in bottom of floor slabs 3"
 in top of floor slabs or inverts 3"
 in top of roof slabs 2"
 in bottom of roof slabs 1 1/2"
 in walls each face 2"
 Bars shall be lapped a minimum of 33 diameters at splices.
 The following abbreviations are used in locating bars:
 B denotes Bottom
 T denotes Top
 NF denotes Near face
 FF denotes Far face
 EF denotes Each face

For concrete details see sheet 21
 For list of reinforcement bars see sheet 30, pages 4 & 5
 For other details of inlet junction chamber see sheet 21



CITY OF CHICAGO
 DEPARTMENT OF SUBWAYS AND SUPERHIGHWAYS
 WEST ROUTE SUPERHIGHWAY
 SECTION 2525.1-B
 PEORIA STREET BRIDGE
 INLET JUNCTION CHAMBER
 REINFORCEMENT DETAILS

SCALE: AS NOTED SHEET NO. 28 OF 30 SHEETS

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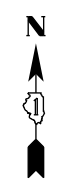


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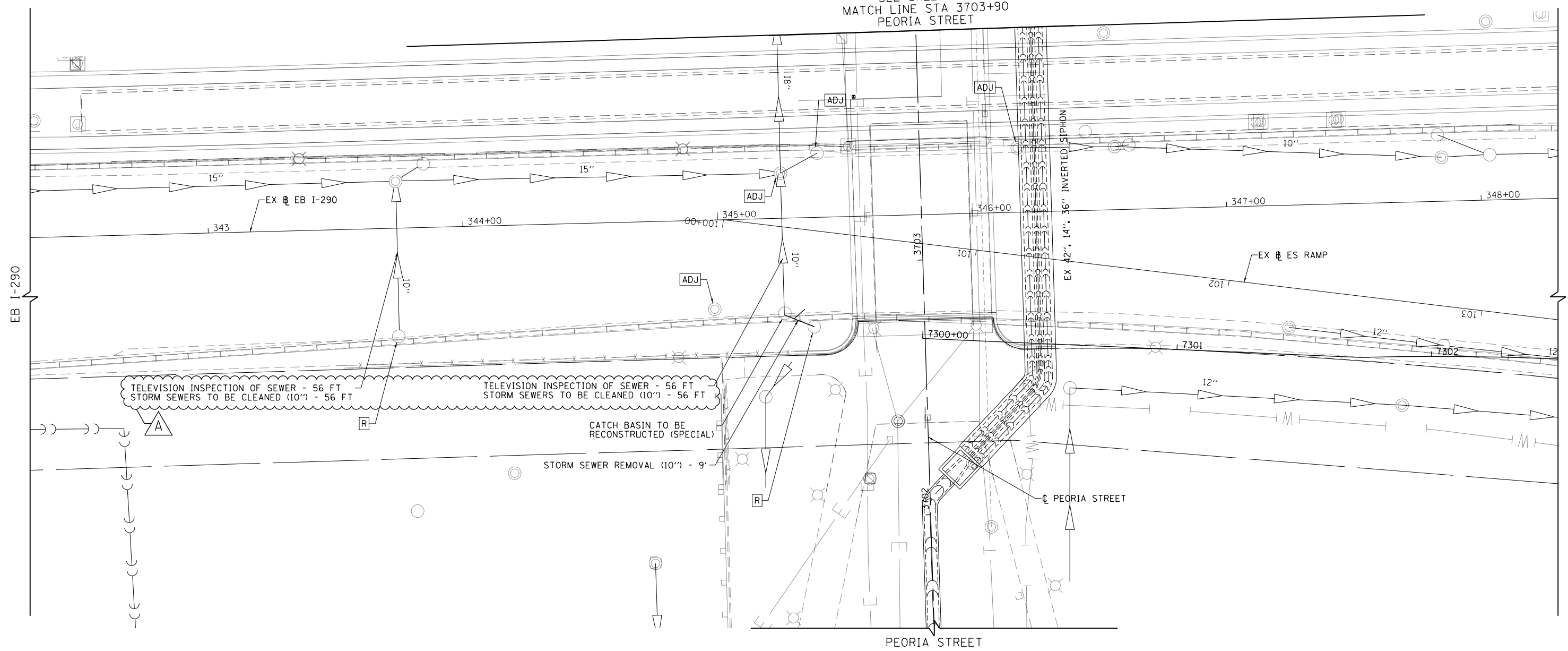
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

AS BUILT SIPHON PLANS PEORIA STREET			
SCALE: NONE	SHEET 12	OF 14 SHEETS	STA. TO STA.

F.A.I. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-01R	COOK	356	76M
CONTRACT NO. 60W29			ILLINOIS FED. AID PROJECT	



SEE SHEET NO. 77
MATCH LINE STA 3703+90
PEORIA STREET

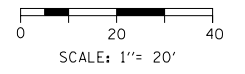


ADJUSTMENT AND REMOVAL LEGEND

- ADJ ADJUST BY CONTRACTOR
- R REMOVAL BY CONTRACTOR

NOTES:

1. ALL STATIONS AND OFFSETS ARE MEASURED FROM EX EB I-290 & RAMP ALIGNMENTS UNLESS OTHERWISE NOTED.
2. SEE SHEET 70 FOR PEORIA STREET DRAINAGE AND REMOVALS.
3. REFER TO SHEETS 28 TO 29 FOR DRAINAGE STRUCTURE AND PIPE SCHEDULES.



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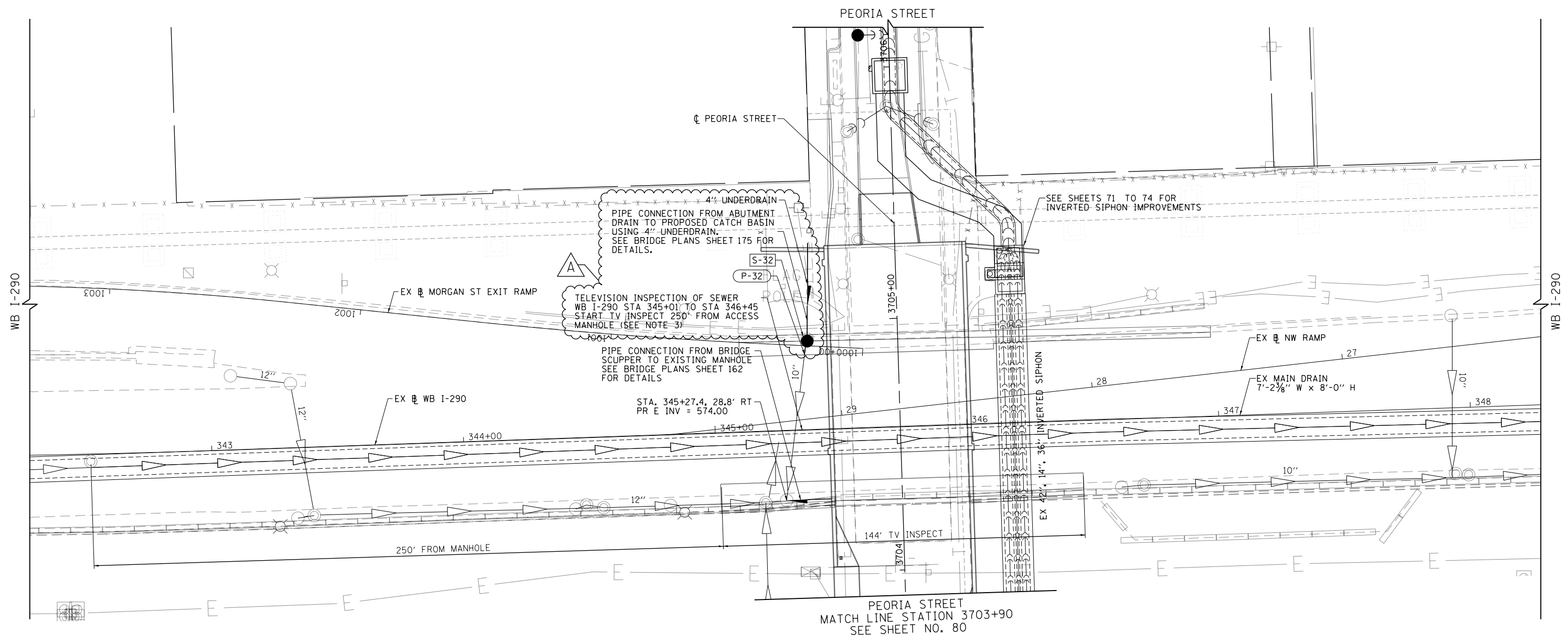
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PLOT DATE = 12/19/2013	DATE - 10/30/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DRAINAGE REMOVAL PLAN
I-290 EASTBOUND**

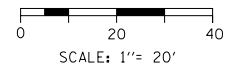
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F.A.I. R.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	78
CONTRACT NO. 60W29				
ILLINOIS FED. AID PROJECT				



NOTES:

1. REFER TO SHEETS 28 TO 29 FOR DRAINAGE STRUCTURE AND PIPE SCHEDULES.
2. SEE SHEET 70 FOR PEORIA STREET DRAINAGE AND REMOVALS.
3. ACCESS FOR TELEVISION IS WB I-290 STA. 342+52, 3.0' RT (1,898,033.2, 1,170,163.1)



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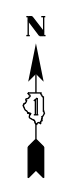


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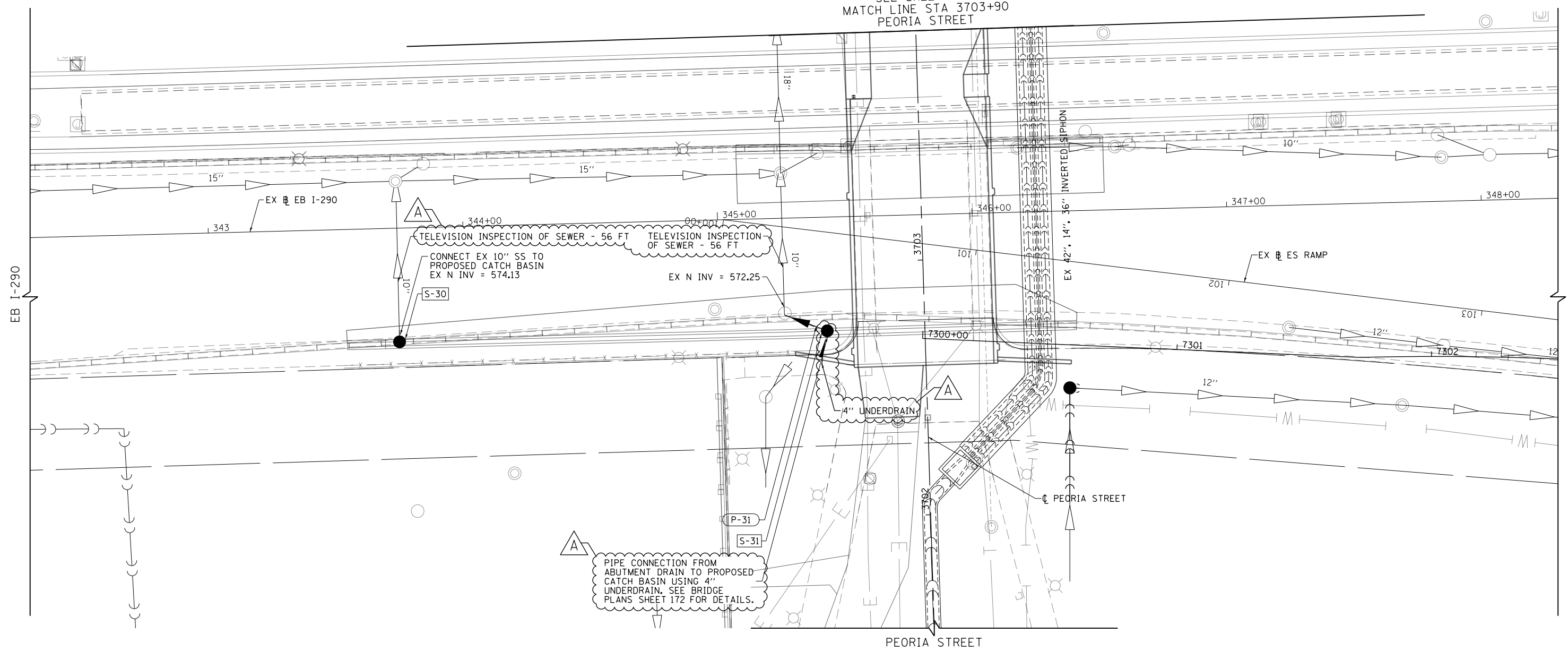
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DRAINAGE PLAN I-290 WESTBOUND			
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F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	79
CONTRACT NO. 60W29				
ILLINOIS FED. AID PROJECT				

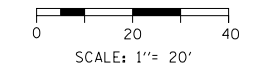


SEE SHEET NO. 79
MATCH LINE STA 3703+90
PEORIA STREET



NOTES:

1. REFER TO SHEETS 28 TO 29 FOR DRAINAGE STRUCTURE AND PIPE SCHEDULES.
2. SEE SHEET 28 FOR PEORIA STREET DRAINAGE AND REMOVALS.
3. EXISTING INVERTS SHOWN AT STRUCTURES ARE BASED ON AS-BUILT DRAWINGS AND SHALL BE CONFIRMED BY THE CONTRACTOR PRIOR TO FABRICATING PROPOSED STRUCTURES.



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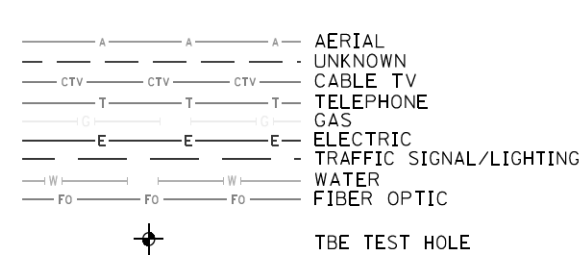
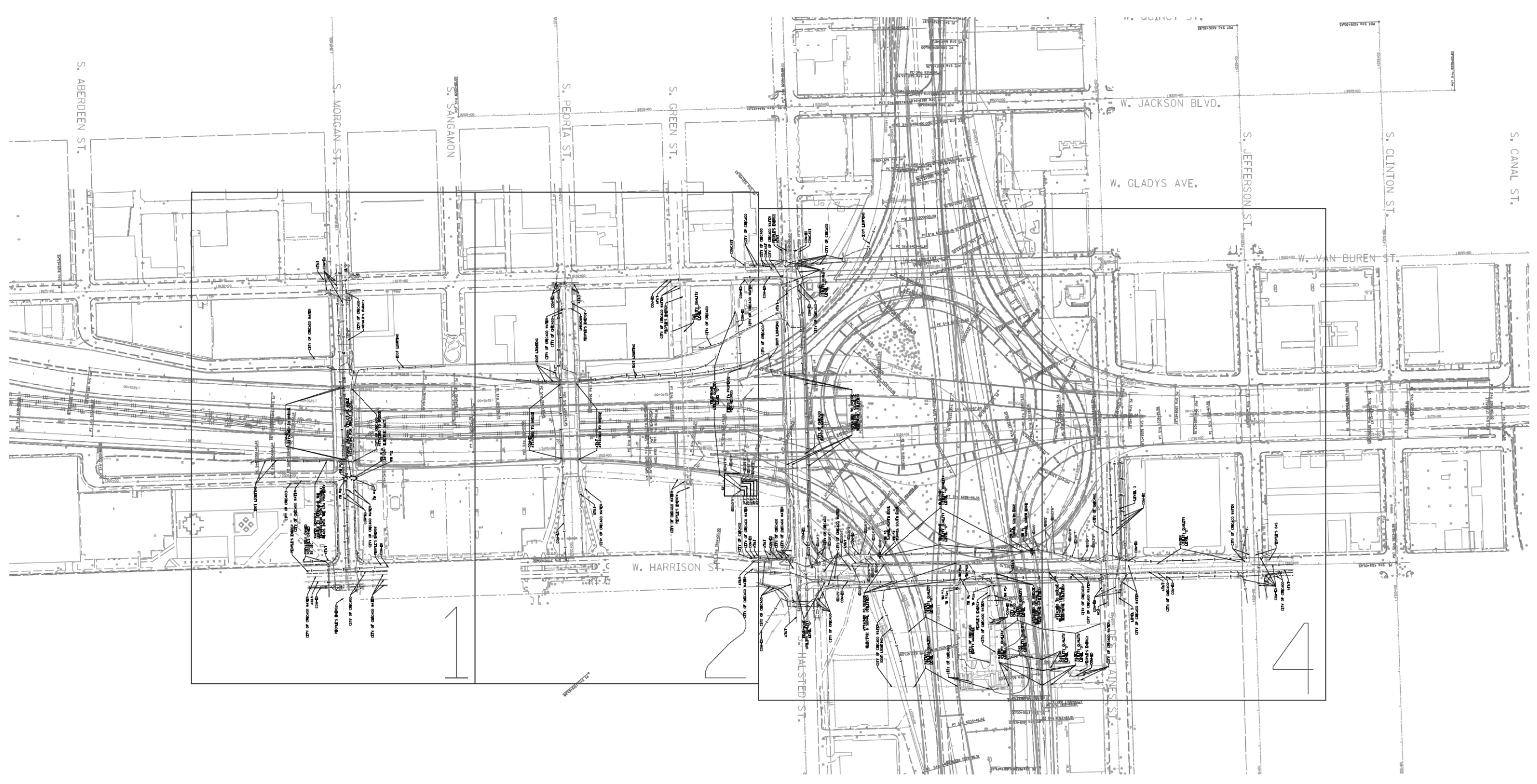
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PLOT DATE = 12/19/2013	DATE - 10/30/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DRAINAGE PLAN
I-290 EASTBOUND**

SCALE: 1"=20' SHEET 11 OF 11 SHEETS STA. 342+30 TO STA. 348+30

F.A.I. R.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	80
CONTRACT NO. 60W29				
ILLINOIS FED. AID PROJECT				



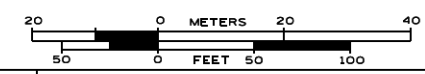
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CITY OF CHICAGO = ELECTRIC	CITY OF CHICAGO WATER = WATER
IDOT LIGHTING = ELECTRIC	LEVEL 3 = FIBER OPTIC
MCI = FIBER OPTIC	PEOPLE'S ENERGY = GAS

NOTE: ALL ELECTRONIC DEPTHS MARKED AS (ED) WERE OBTAINED UTILIZING A SONDE DEVICE. DEPTHS ON THE AT&T DUCT WERE RECORDED FROM AN AT&T CONTRACTOR. DEPTHS ON THE COMED DUCT WERE RECORDED BY A CARDNO CREW. CARDNO CAN NOT VERIFY THE ACCURACY OF THESE DEPTHS AND SHOULD BE USED AS APPROXIMATE AND AS INFORMATION ONLY.

UTILITIES SHOWN ON THESE PLANS AS DEPICTED IN THE LEGEND HAVE BEEN INVESTIGATED BY CARDNO IN ACCORDANCE WITH SUE INDUSTRY STANDARDS. ALL OTHER INFORMATION SHOWN HAS BEEN PROVIDED TO CARDNO BY OTHERS. CARDNO SUE FIELD INVESTIGATION WAS PERFORMED 3/14/13 THROUGH 9/13/13. CHANGES TO UTILITIES MAY HAVE BEEN MADE AND THEREFORE MAY RESULT IN VARIANCES FROM THIS PLAN. CONSIDERATION SHOULD BE GIVEN TO UPDATING THIS PLAN IF DEEMED ADVISABLE PRIOR TO FINAL DESIGN AND CONSTRUCTION.

PRELIMINARY DATA FOR DELIVERY 9/16/2013

ALL UTILITIES SHOWN QUALITY LEVEL "B" UNLESS NOTED OTHERWISE.



TBE Job No. IL09510531
SUE Plan Pages Cover

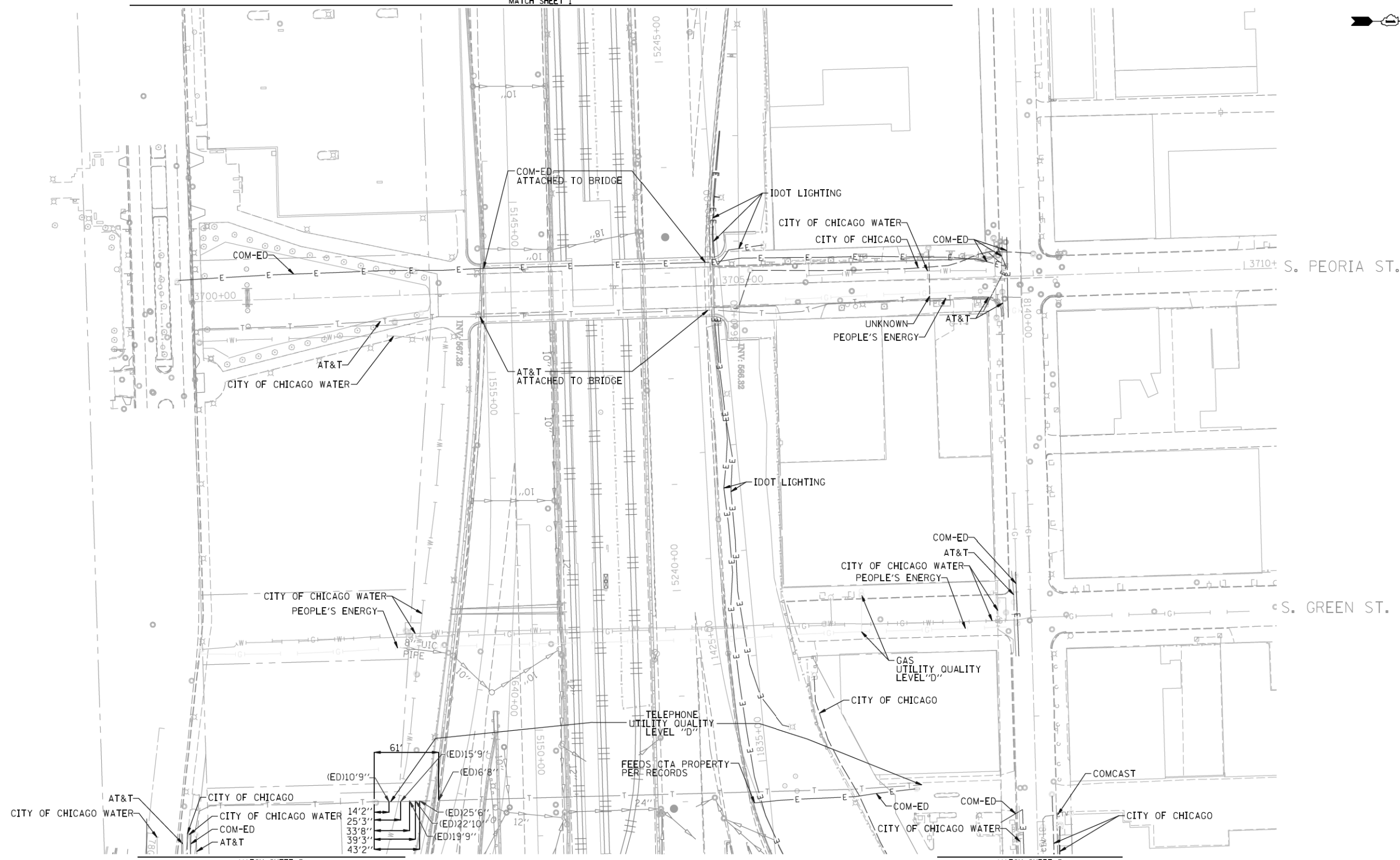
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Utility Quality Level "D" : Records Research

DESIGNED	EG	REVISED	
DRAWN	SRK	REVISED	
CHECKED	KFS	REVISED	
DATE	9/16/13	REVISED	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

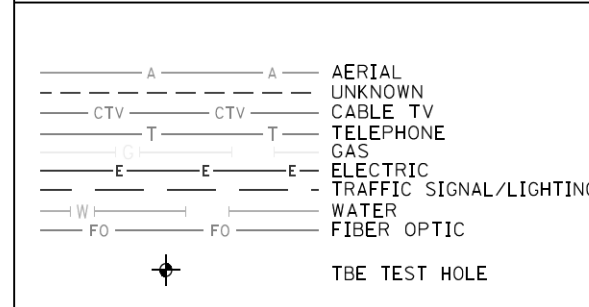
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Chicago, Illinois

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	N/A	Cook	356	81
Contract No. N/A				
FED. ROAD DIST. NO. [ILLINOIS] IDOT Project No. P-91-259-12				



MATCH SHEET 3

MATCH SHEET 3



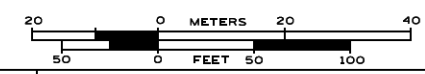
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COM-ED	= ELECTRIC
CITY OF CHICAGO	= ELECTRIC
CITY OF CHICAGO WATER	= WATER
IDOT LIGHTING	= ELECTRIC
PEOPLE'S ENERGY	= GAS

NOTE: ALL ELECTRONIC DEPTHS MARKED AS (ED) WERE OBTAINED UTILIZING A SONDE DEVICE. DEPTHS ON THE AT&T DUCT WERE RECORDED FROM AN AT&T CONTRACTOR. DEPTHS ON THE COMED DUCT WERE RECORDED BY A CARDNO CREW. CARDNO CAN NOT VERIFY THE ACCURACY OF THESE DEPTHS AND SHOULD BE USED AS APPROXIMATE AND AS INFORMATION ONLY.

UTILITIES SHOWN ON THESE PLANS AS DEPICTED IN THE LEGEND HAVE BEEN INVESTIGATED BY CARDNO IN ACCORDANCE WITH SUE INDUSTRY STANDARDS. ALL OTHER INFORMATION SHOWN HAS BEEN PROVIDED TO CARDNO BY OTHERS. CARDNO SUE FIELD INVESTIGATION WAS PERFORMED 3/14/13 THROUGH 9/13/13. CHANGES TO UTILITIES MAY HAVE BEEN MADE AND THEREFORE MAY RESULT IN VARIANCES FROM THIS PLAN. CONSIDERATION SHOULD BE GIVEN TO UPDATING THIS PLAN IF DEEMED ADVISABLE PRIOR TO FINAL DESIGN AND CONSTRUCTION.

PRELIMINARY DATA FOR DELIVERY 9/16/2013

ALL UTILITIES SHOWN QUALITY LEVEL "B" UNLESS NOTED OTHERWISE.



TBE Job No. IL09510531
SUE Plan Page 2 of 4

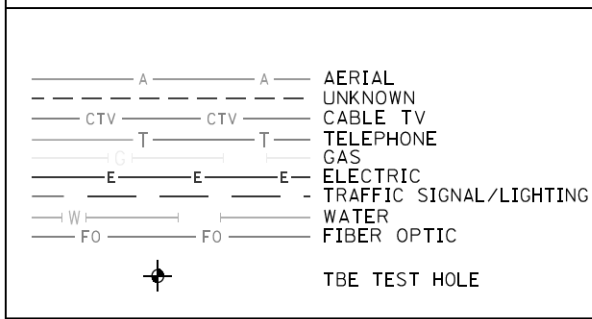
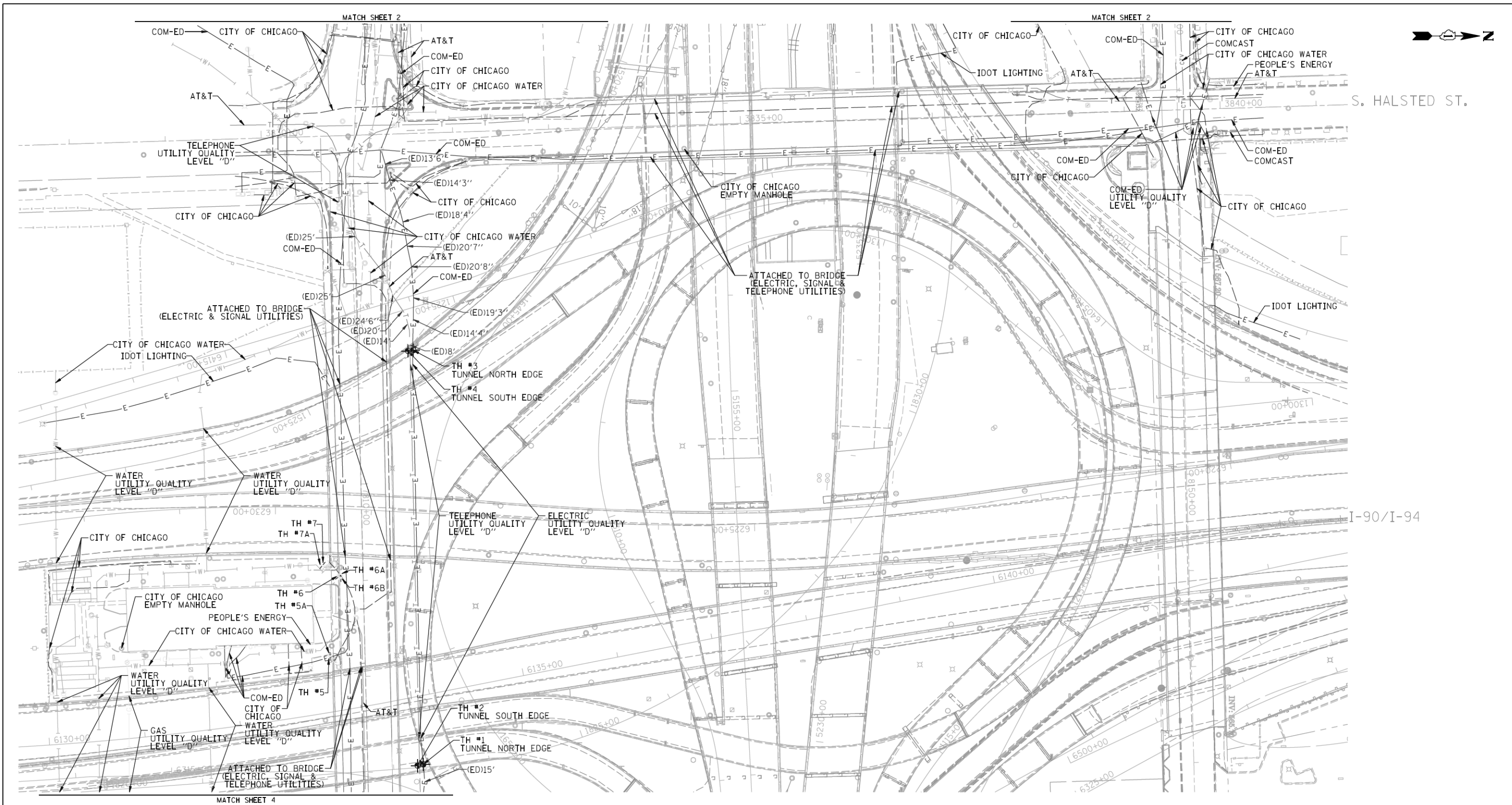
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Utility Quality Level "D": Records Research	DATE	9/16/13	REVISED	

DESIGNED	EG	REVISED	
DRAWN	SRK	REVISED	
CHECKED	KFS	REVISED	
DATE	9/16/13	REVISED	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-941-901-290 (Circle Interchange)
Chicago, Illinois

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	N/A	Cook	356	82
Contract No. N/A				
FED. ROAD DIST. NO. [ILLINOIS] IDOT Project No. P-91-259-12				



DESIGNED	EG	REVISED
DRAWN	SRK	REVISED
CHECKED	KFS	REVISED
DATE	9/16/13	REVISED

NOTE: ALL ELECTRONIC DEPTHS MARKED AS (ED) WERE OBTAINED UTILIZING A SONDE DEVICE. DEPTHS ON THE AT&T DUCT WERE RECORDED FROM AN AT&T CONTRACTOR. DEPTHS ON THE COMED DUCT WERE RECORDED BY A CARNO CREW. CARNO CAN NOT VERIFY THE ACCURACY OF THESE DEPTHS AND SHOULD BE USED AS APPROXIMATE AND AS INFORMATION ONLY.

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PRELIMINARY DATA FOR DELIVERY 9/16/2013

ALL UTILITIES SHOWN QUALITY LEVEL "B" UNLESS NOTED OTHERWISE.

Cardno TBE

Dynasty Group
Engineers & Surveyors

TBE Job No. IL09510531
SUE Plan Pages 3 of 4

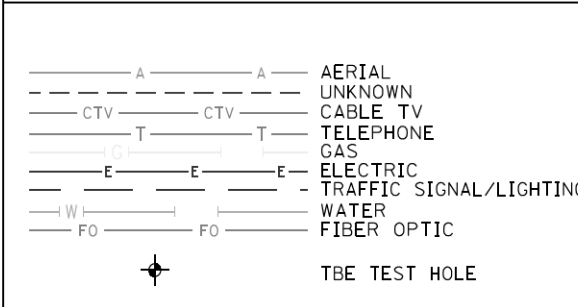
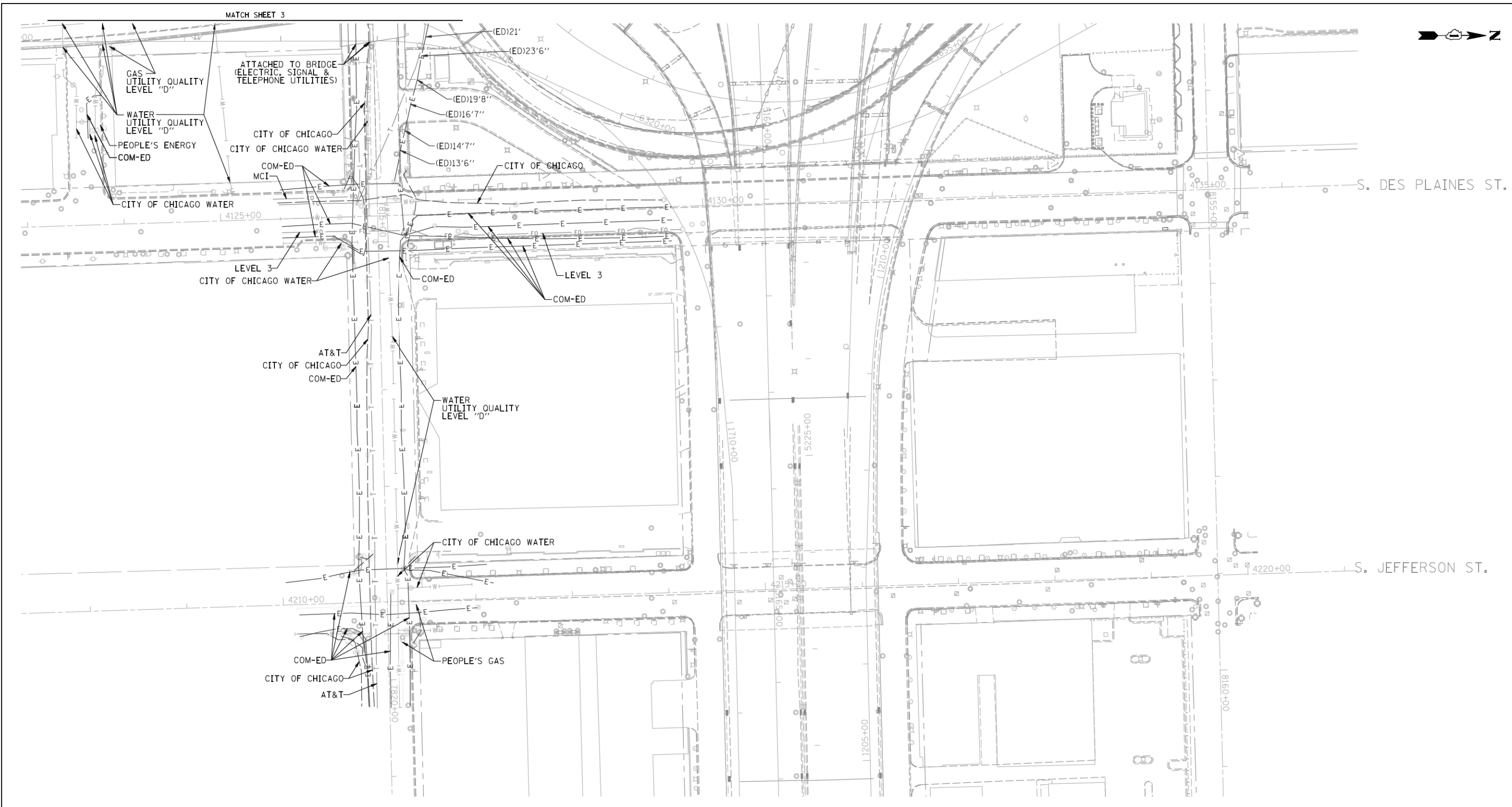
UTILITY OWNERS

DESIGNED	EG	REVISED
DRAWN	SRK	REVISED
CHECKED	KFS	REVISED
DATE	9/16/13	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-941-901-290 (Circle Interchange)
Chicago, Illinois

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	N/A	Cook	356	83
Contract No. N/A				
FED. ROAD DIST. NO. ILLINOIS IDOT Project No. P-91-259-12				



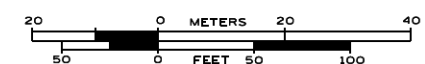
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COM-ED	= ELECTRIC
CITY OF CHICAGO	= ELECTRIC
CITY OF CHICAGO WATER	= WATER
IDOT LIGHTING	= ELECTRIC
LEVEL 3	= FIBER OPTIC
MCI	= FIBER OPTIC
PEOPLE'S ENERGY	= GAS

NOTE: ALL ELECTRONIC DEPTHS MARKED AS (ED) WERE OBTAINED UTILIZING A SONDE DEVICE. DEPTHS ON THE AT&T DUCT WERE RECORDED FROM AN AT&T CONTRACTOR. DEPTHS ON THE COMED DUCT WERE RECORDED BY A CARDNO CREW. CARDNO CAN NOT VERIFY THE ACCURACY OF THESE DEPTHS AND SHOULD BE USED AS APPROXIMATE AND AS INFORMATION ONLY.

UTILITIES SHOWN ON THESE PLANS AS DEPICTED IN THE LEGEND HAVE BEEN INVESTIGATED BY CARDNO IN ACCORDANCE WITH SUE INDUSTRY STANDARDS. ALL OTHER INFORMATION SHOWN HAS BEEN PROVIDED TO CARDNO BY OTHERS. CARDNO SUE FIELD INVESTIGATION WAS PERFORMED 3/14/13 THROUGH 7/30/13. CHANGES TO UTILITIES MAY HAVE BEEN MADE AND THEREFORE MAY RESULT IN VARIANCES FROM THIS PLAN. CONSIDERATION SHOULD BE GIVEN TO UPDATING THIS PLAN IF DEEMED ADVISABLE PRIOR TO FINAL DESIGN AND CONSTRUCTION.

PRELIMINARY DATA FOR DELIVERY 8/14/2013

ALL UTILITIES SHOWN QUALITY LEVEL "B" UNLESS NOTED OTHERWISE.



TBE Job No. IL09510531
SUE Plan Page: 4 of 4

Utility Quality Level "A": Visually Verified Test Hole
Utility Quality Level "B": Designating/non Visually Verified Test Hole
Utility Quality Level "C": Research with Survey
Utility Quality Level "D": Records Research

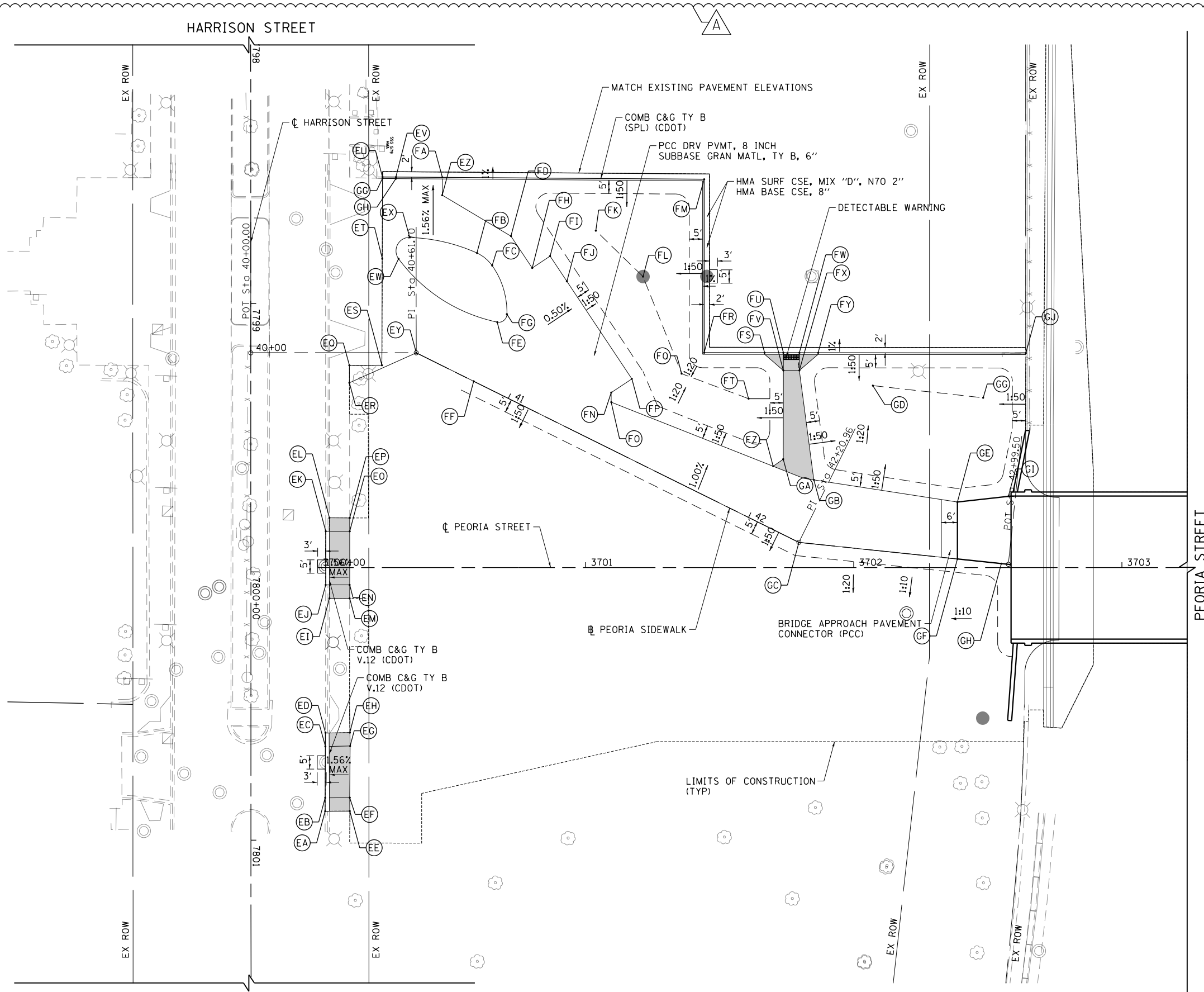
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DRAWN	SRK	REVISED	
CHECKED	KFS	REVISED	
DATE	8/14/13	REVISED	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

I-941-901-290 (Circle Interchange)
Chicago, Illinois

F.A. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	N/A	Cook	356	84
Contract No. N/A				
FED. ROAD DIST. NO. ILLINOIS IDOT Project No. P-91-259-12				

FILE PATH = p:\388935-par\1\ascom\1\me\local\p\ae\cd\00\Documents\01_Americas\Transportation\66269938_Circle\Phase\1\000_CD\006_Roadway\Sheets\66W29_Sht-Intersec-01.dgn



LEGEND
 HARRISON STREET
 PCC SIDEWALK, 5 INCH SAND CUSHION, 4 INCH, (CDOT)

	STATION	OFFSET	LT/RT	ELEVATION	TOP OF CURB / GROUND ELEVATION BEHIND WALL (IF APPLICABLE)
EA	7800+89.23	27.67'	LT	592.69 *	593.13 *
EB	7800+84.23	27.69'	LT	592.68 *	593.18
EC	7800+65.07	27.78'	LT	592.62 *	593.12
ED	7800+59.96	27.80'	LT	592.60 *	593.02 *
EE	7800+89.17	36.88'	LT	593.48 *	N/A
EF	7800+84.17	36.85'	LT	593.29	N/A
EG	7800+64.93	36.99'	LT	593.24	N/A
EH	7800+59.93	36.91'	LT	593.33 *	N/A
EI	7800+09.85	29.32'	LT	593.18 *	N/A
EJ	7800+04.85	27.89'	LT	592.91 *	593.18 *
EK	7799+84.85	27.93'	LT	593.03 *	593.19 *
EL	7799+79.85	29.31'	LT	593.19 *	N/A
EM	7800+09.86	36.83'	LT	593.28 *	N/A
EN	7800+04.86	36.84'	LT	593.29	N/A
EO	7799+84.86	36.87'	LT	593.30	N/A
EP	7799+79.86	36.87'	LT	593.53	N/A
EQ	40+36.78	4.65'	RT	593.35 *	N/A
ER	40+36.77	11.17'	RT	593.38 *	N/A
ES	40+48.90	4.64'	RT	593.50 *	N/A
ET	40+49.04	35.08'	LT	593.37 *	N/A
EU	40+49.15	65.68'	LT	593.16	593.75
EV	40+54.15	65.54'	LT	593.22	593.72
EW	40+55.13	35.06'	LT	593.46	N/A
EX	40+59.32	43.02'	LT	593.49	N/A
EY	40+61.70	0.00'	RT	593.71	N/A
EZ	40+61.70	59.55'	LT	593.97	N/A
FA	40+61.70	66.13'	LT	593.46	593.96
FB	40+65.49	43.39'	LT	593.90	N/A
FC	40+72.66	41.71'	LT	593.83	N/A
FD	40+74.05	54.67'	LT	593.80	N/A
FE	40+83.68	23.67'	LT	593.69	N/A
FF	40+85.61	0.00'	RT	593.95	N/A
FG	40+85.61	27.92'	LT	593.69	N/A
FH	40+86.43	47.53'	LT	593.72	N/A
FI	40+90.42	54.46'	LT	593.68	N/A
FJ	41+00.21	48.82'	LT	593.57	N/A
FK	41+01.52	70.53'	LT	592.50	N/A
FL	41+24.89	63.00'	LT	592.42	N/A
FM	41+29.21	105.64'	LT	593.55	594.05
FN	41+33.45	18.98'	LT	594.24	N/A
FO	41+35.03	15.79'	LT	594.29	N/A
FP	41+38.07	26.99'	LT	594.19	N/A
FQ	41+53.86	36.96'	LT	593.64	N/A
FR	41+57.91	47.69'	LT	593.49	593.99
FS	41+78.11	57.67'	LT	593.44	593.94
FT	41+80.37	39.68'	LT	593.77	N/A
FU	41+84.37	60.77'	LT	593.43	593.46
FV	41+87.30	54.87'	LT	593.55	N/A
FW	41+89.77	63.43'	LT	593.46	593.49
FX	41+92.68	57.52'	LT	593.58	N/A
FY	41+96.04	66.53'	LT	593.49	594.07
FZ	41+99.75	21.20'	LT	594.88	N/A
GA	42+01.96	25.16'	LT	594.74	N/A
GB	42+15.60	23.37'	LT	595.13	N/A
GC	42+20.96	0.00'	LT	595.32	N/A
GD	42+42.30	60.99'	LT	593.83	N/A
GE	42+78.19	21.10'	LT	596.53	N/A
GF	42+80.40	0.00'	RT	596.77	N/A
GG	42+83.69	60.78'	LT	593.34	N/A
GH	42+96.86	0.00'	LT	596.98	N/A
GI	42+96.86	25.21'	LT	596.63	N/A
GJ	42+97.81	79.03'	LT	593.30	593.88
GG	40+49.15	65.08'	LT	593.27	593.75
GH	40+54.15	64.96L	LT	593.43	593.72

• MATCH EXISTING

CURB TRANSITION			
FROM POINT	CURB HEIGHT	TO POINT	CURB HEIGHT
FS	6"	FV	DEPRESSED CURB
FW	DEPRESSED CURB	FY	7"

• MATCH EXISTING



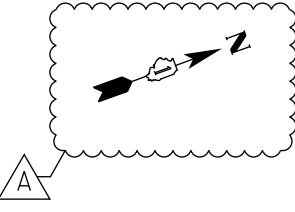
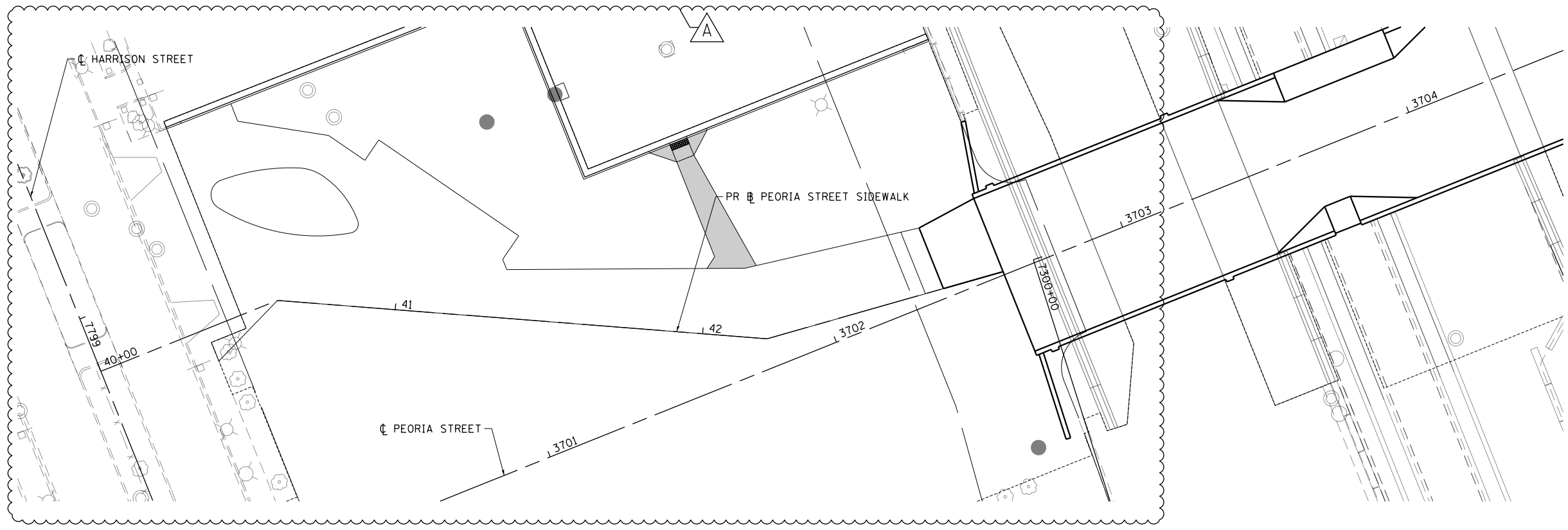
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PLOT DATE = 12/19/2013	DATE - 10/30/2013	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

ROADWAY AND GRADING PLAN DETAILS			
PEORIA STREET			
SCALE: 1"=20'	SHEET 1	OF 5 SHEETS	STA. 3700+00 TO STA. 3704+25

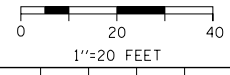
F.A.I. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	86
CONTRACT NO. 60W29				
ILLINOIS FED. AID PROJECT				

PLAN	SURVEYED	DATE
	PLOTTED	BY
	NOTE BOOK	
	CHECKED	
	NO. _____	
	FILE NAME	

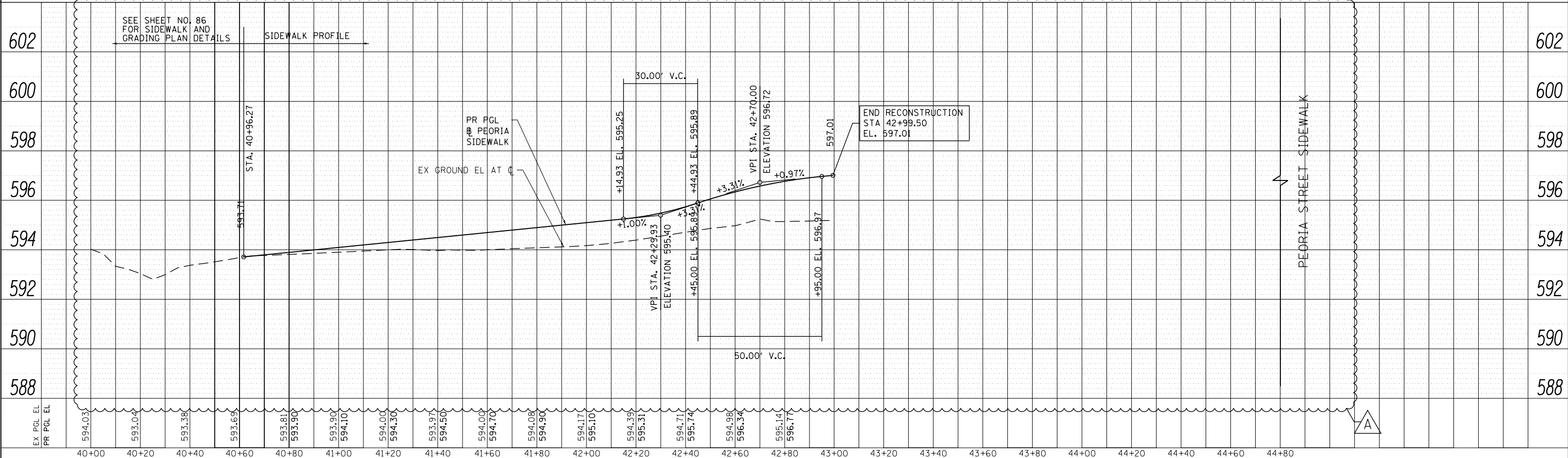


SEE SHEET 86 FOR GRADING PLAN AND DETAILS

FOR INFORMATION ONLY



PROFILE	SURVEYED	DATE
	PLOTTED	BY
	NOTE BOOK	
	CHECKED	
	NO. _____	
	FILE NAME	



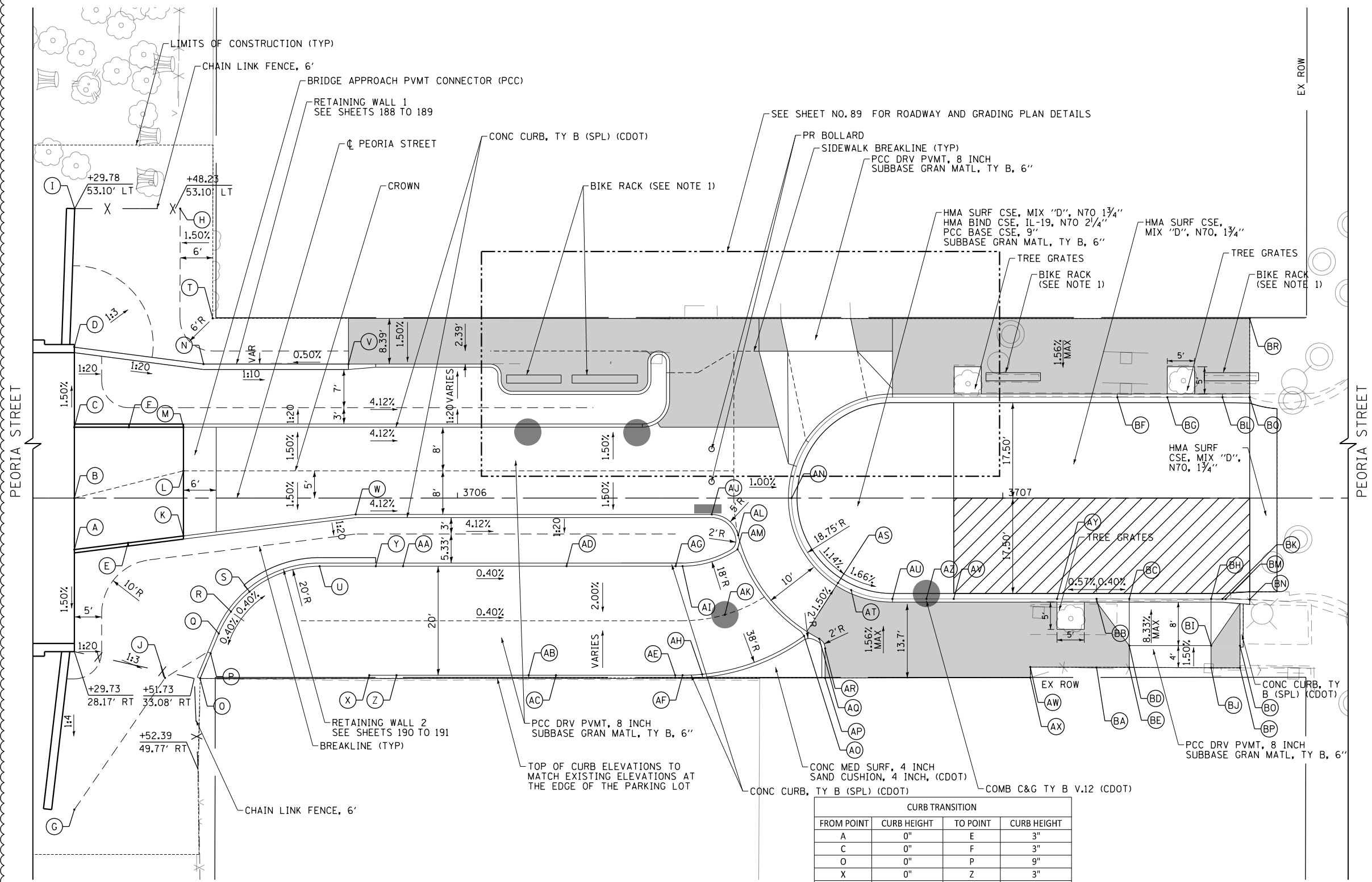
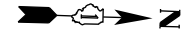
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PLOT DATE = 12/19/2013	DATE - 10/30/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY PLAN AND PROFILE
PEORIA STREET SIDEWALK**

SCALE: 1"=20' SHEET 2 OF 5 SHEETS STA. 40+00 TO STA. 44+80

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	87
CONTRACT NO. 60W29			ILLINOIS FED. AID PROJECT	



	STATION	OFFSET	LT/RT	ELEVATION	TOP OF CURB / GROUND ELEVATION BEHIND WALL (IF APPLICABLE)
A	3705+29.73	9.46'	RT	598.02	598.02
B	3705+29.73	0.00'	RT	598.16	N/A
C	3705+29.73	13.00'	LT	597.96	597.96
D	3705+29.73	27.72'	LT	593.45	597.74
E	3705+39.58	8.22'	RT	597.64	597.89
F	3705+39.73	13.00'	LT	597.64	597.89
G	3705+29.73	57.15'	RT	591.78	N/A
H	3705+49.13	53.10'	LT	594.27 *	N/A
I	3705+29.78	53.10'	LT	598.79 *	N/A
J	3705+46.36	32.98'	RT	593.48	N/A
K	3705+49.73	6.95'	RT	597.25	597.50
L	3705+49.73	5.00'	LT	597.43	N/A
M	3705+49.73	13.00'	LT	597.31	597.56
N	3705+53.43	24.58'	LT	593.06	596.57
O	3705+52.81	33.08'	RT	593.73 *	593.73
P	3705+54.72	28.46'	RT	593.76	594.51
Q	3705+56.22	24.85'	RT	593.78	594.53
R	3705+58.45	20.83'	RT	593.76	594.51
S	3705+61.85	17.17'	RT	593.74	594.49
T	3705+55.10	32.98'	LT	593.28	N/A
U	3705+74.70	12.50'	RT	593.69	594.44
V	3705+80.00	24.58'	LT	593.19	593.94
W	3705+81.31	3.00'	RT	596.01	596.26
X	3705+83.80	32.50'	RT	593.71 *	593.71 *
Y	3705+85.00	12.50'	RT	593.65	594.40
Z	3705+88.80	32.50'	RT	593.47	593.72 *
AA	3705+90.00	12.50'	RT	593.63	594.38
AB	3706+13.05	32.50'	RT	593.45	593.70 *
AC	3706+18.05	32.50'	RT	593.50	593.67 *
AD	3706+20.00	12.50'	RT	593.51	594.26
AE	3706+39.92	32.50'	RT	593.59	593.76 *
AF	3706+41.26	32.50'	RT	593.75	593.75
AG	3706+40.00	12.50'	RT	593.43	593.68
AH	3706+44.87	32.33'	RT	593.57	593.82
AI	3706+41.26	12.50'	RT	593.42	593.67
AJ	3706+46.62	3.00'	RT	593.66	593.91
AK	3706+49.03	21.40'	RT	593.19	N/A
AL	3706+51.48	6.83'	RT	593.57	593.82
AM	3706+51.35	9.41'	RT	593.38	593.55
AN	3706+61.25	0.00'	RT	593.35	593.52
AO	3706+63.56	25.27'	RT	593.29	593.46
AP	3706+67.41	33.00'	RT	593.58	593.75
AQ	3706+66.34	25.80'	RT	593.48	593.65
AR	3706+67.41	27.58'	RT	593.50	593.67
AS	3706+69.77	15.58'	RT	593.14	593.31
AT	3706+72.20	16.89'	RT	593.09	593.34
AU	3706+79.75	18.50'	RT	592.96	593.46
AV	3706+90.99	18.50'	RT	592.89	593.39
AW	3707+05.04	31.00'	RT	593.58 *	N/A
AX	3707+05.04	32.95'	RT	593.67 *	N/A
AY	3707+09.94	18.50'	RT	593.00	593.50
AZ	3706+85.99	18.50'	RT	592.86	593.36
BA	3707+17.20	31.07'	RT	593.66 *	N/A
BB	3707+17.20	18.50'	RT	593.04	593.54
BC	3707+23.20	18.50'	RT	593.02	593.09
BD	3707+23.20	27.07'	RT	594.03	N/A
BE	3707+23.20	31.07'	RT	593.68	N/A
BF	3707+21.00	18.50'	LT	592.82 *	593.15
BG	3707+30.17	18.50'	LT	592.79 *	593.14
BH	3707+38.20	18.50'	RT	592.96	593.03
BI	3707+38.20	27.07'	RT	593.67	N/A
BJ	3707+38.20	31.13'	RT	593.73 *	N/A
BK	3707+40.28	18.50'	RT	592.87	593.15
BL	3707+40.28	18.50'	LT	592.75 *	593.13
BM	3707+40.85	18.51'	RT	592.84	593.17
BN	3707+45.28	18.59'	RT	592.64 *	593.11 *
BO	3707+43.50	27.07'	RT	593.62 *	593.87 *
BP	3707+43.50	31.15'	RT	593.75 *	N/A
BQ	3707+45.28	18.51'	LT	592.73 *	593.08 *
BR	3707+45.28	32.95'	LT	593.34 *	N/A

* MATCH EXISTING

LEGEND

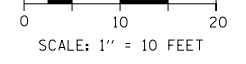
- PCC SIDEWALK, 5 INCH SAND CUSHION, 4 INCH, (CDOT)
- HMA SURF CSE, MIX "D", N70 1 3/4" HMA BIND CSE, IL-19, N70 VARIES 2 1/4" TO 5 1/4"

NOTES

1. SEE NOTE 35 ON THE GENERAL NOTES SHEET FOR INFORMATION REGARDING REMOVAL, STORAGE, AND RELOCATION OF BIKE RACKS.
2. STATIONS, OFFSETS, AND ELEVATIONS ON PEORIA STREET ARE GIVEN ALONG THE FLOW LINE.

CURB TRANSITION			
FROM POINT	CURB HEIGHT	TO POINT	CURB HEIGHT
A	0"	E	3"
C	0"	F	3"
O	0"	P	9"
X	0"	Z	3"
AB	3"	AC	2"
AD	9"	AG	3"
AE	2"	AH	3"
AM	2"	AO	2"
AS	2"	AU	6"
BB	6"	BC	DEPRESSED CURB
BF	4"	BL	4.5"
BH	DEPRESSED CURB	BN	5.6" *
BL	4.5"	BQ	4.2"

* MATCH EXISTING



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PLOT DATE = 12/19/2013	DATE - 10/30/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND GRADING PLAN DETAILS
PEORIA STREET**

SCALE: 1"=10' SHEET 3 OF 5 SHEETS STA. 3705+22 TO STA. 3707+40

F.A.I. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-01R	COOK	356	88
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60W29	

	STATION	OFFSET	LT/RT	ELEVATION	TOP OF CURB / GROUND ELEVATION BEHIND WALL (IF APPLICABLE)
CA	3706+20.00	5.00'	LT	594.53	N/A
CB	3706+20.00	13.00'	LT	594.41	594.66
CC	3706+20.00	19.58'	LT	593.37	594.12
CD	3706+20.00	32.97'	LT	593.36 *	N/A
CE	3706+28.90	5.00'	LT	594.21	N/A
CF	3706+28.90	13.00'	LT	594.09	594.34
CG	3706+28.90	19.58'	LT	593.39	594.16
CH	3706+33.90	13.00'	LT	593.84	594.09
CI	3706+32.90	19.58'	LT	593.39	594.14
CJ	3706+34.90	21.58'	LT	593.40	594.15
CK	3706+34.90	24.96'	LT	593.35	594.1
CL	3706+38.90	5.00'	LT	593.93	N/A
CM	3706+38.90	18.00'	LT	593.46	594.02
CN	3706+38.90	23.00'	LT	593.28	594.03
CO	3706+38.90	24.96'	LT	593.29	594.04
CP	3706+38.90	32.96'	LT	593.34 *	N/A
CQ	3706+44.06	23.00'	LT	593.28 *	N/A
CR	3706+06.00	24.58'	LT	593.23	593.98
CS	3706+08.00	22.58'	LT	593.28	594.03
CT	3706+50.39	32.96'	LT	593.33 *	N/A
CU	3706+50.67	5.00'	LT	593.67	N/A
CV	3706+50.67	26.96'	LT	593.30	N/A
CW	3706+55.30	26.96'	LT	593.28	N/A
CX	3706+55.30	32.96'	LT	593.31	N/A
CY	3706+58.94	13.00'	LT	593.45	N/A
CZ	3706+60.68	6.31'	LT	593.55	N/A
DA	3706+59.30	32.96'	LT	593.37 *	N/A
DB	3706+60.53	26.96'	LT	593.27	N/A
DC	3706+08.00	21.58'	LT	593.30	594.05
DD	3706+62.18	5.84'	LT	593.28	593.53
DE	3706+64.36	10.27'	LT	593.23	593.40
DF	3706+72.10	32.96'	LT	593.45 *	N/A
DG	3706+73.32	26.96'	LT	593.21	N/A
DH	3706+75.20	17.93'	LT	593.08	593.24
DI	3706+79.75	18.50'	LT	593.03	593.28
DJ	3706+79.75	20.08'	LT	593.28	N/A
DK	3706+79.75	26.96'	LT	593.28	N/A
DL	3706+79.75	32.96'	LT	593.32 *	N/A
DM	3706+10.00	19.58'	LT	593.35	594.10
DN	3706+90.99	18.50'	LT	592.90	593.23
DO	3706+90.99	32.96'	LT	593.28 *	N/A

• MATCH EXISTING

CURB TRANSITION			
FROM POINT	CURB HEIGHT	TO POINT	CURB HEIGHT
CH	3"	CN	9"
AN	2"	DD	3"
DD	3"	DE	2"
DH	2"	DI	3"
DI	3"	DM	4"

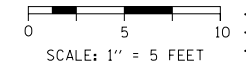
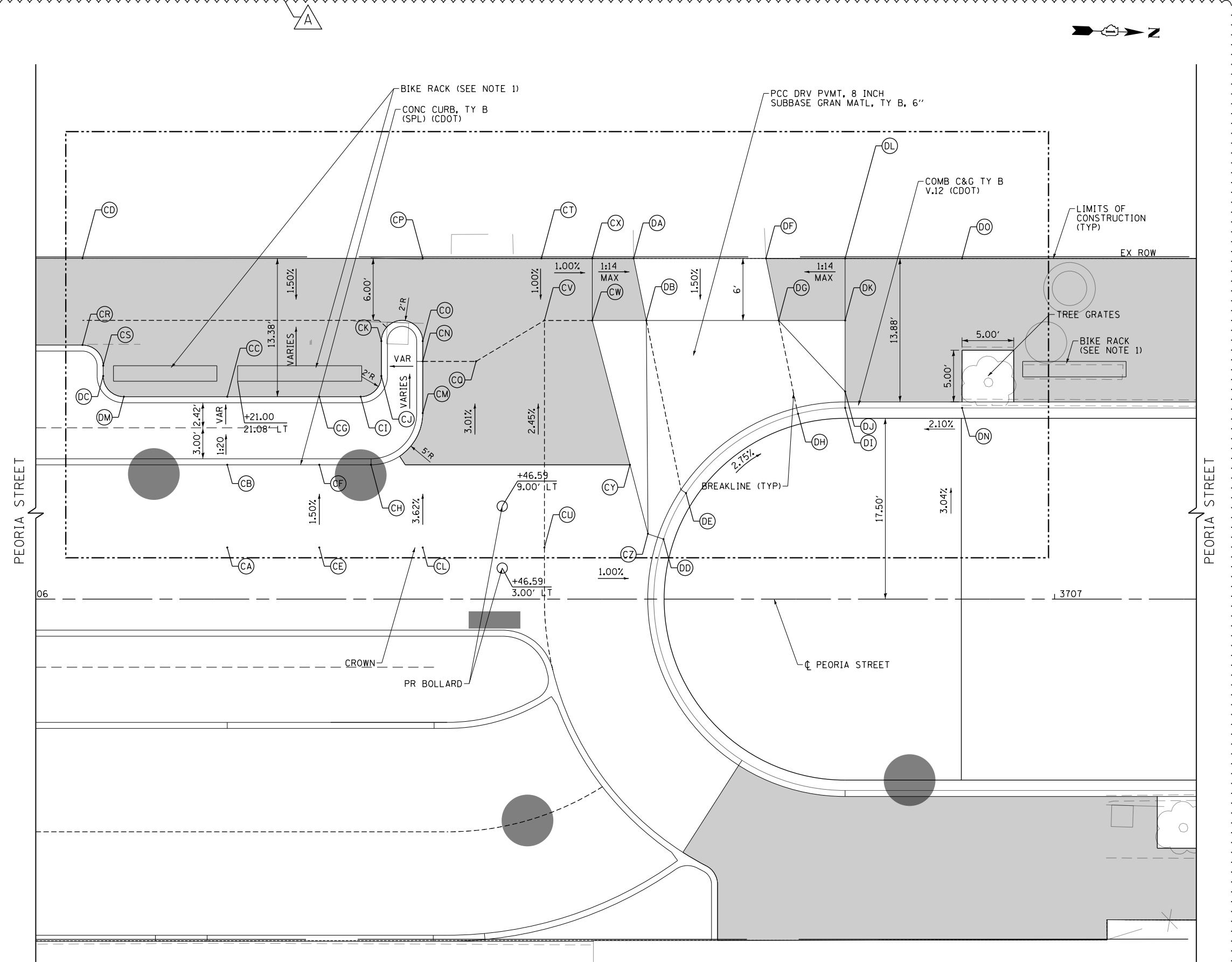
• MATCH EXISTING

NOTES

- SEE NOTE 35 ON THE GENERAL NOTES SHEET FOR INFORMATION REGARDING REMOVAL, STORAGE, AND RELOCATION OF BIKE RACKS.
- STATIONS, OFFSETS, AND ELEVATIONS ON PEORIA STREET ARE GIVEN ALONG THE FLOW LINE.

LEGEND

■ PCC SIDEWALK, 5 INCH SAND CUSHION, 4 INCH, (CDOT)



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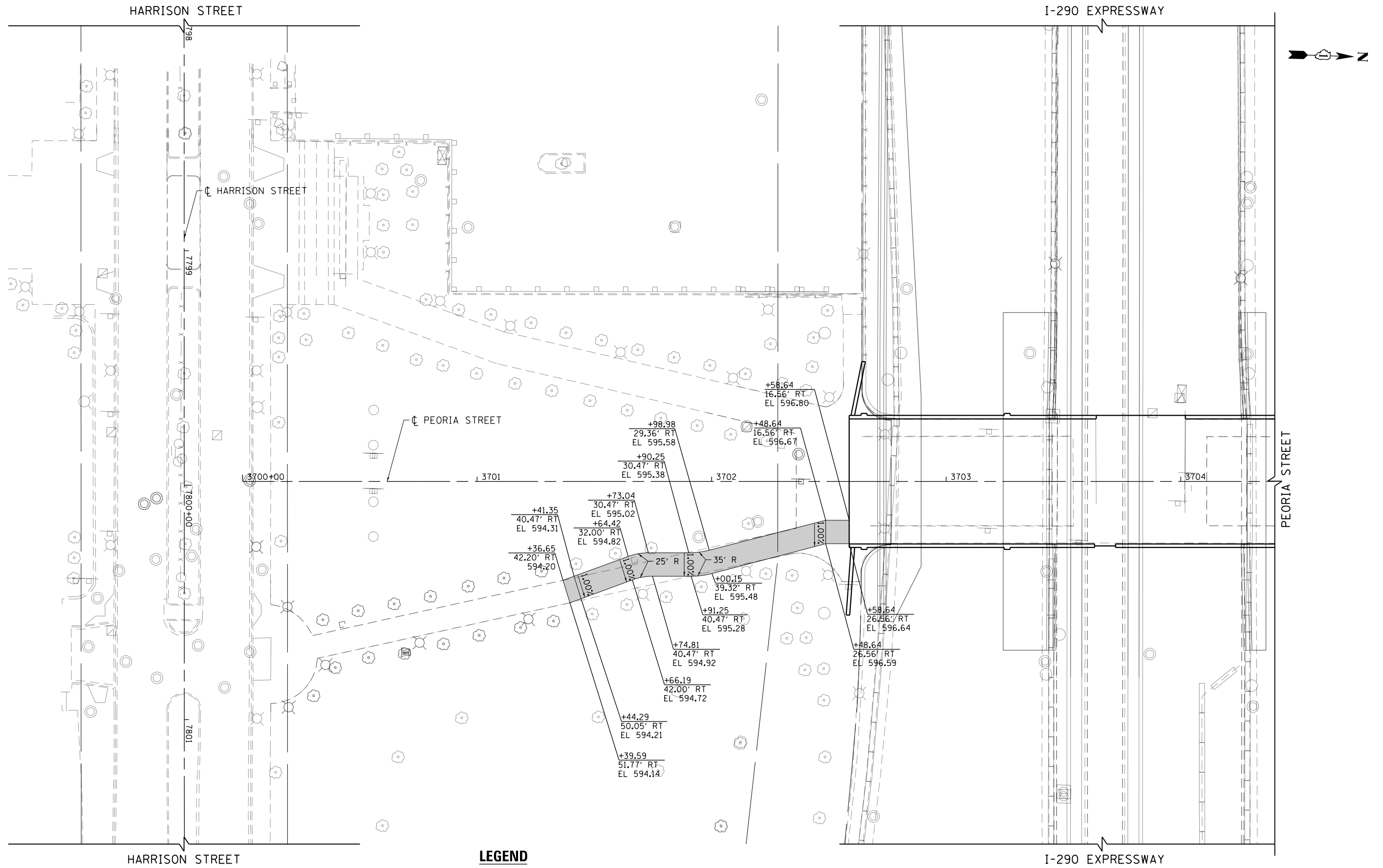
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND GRADING PLAN DETAILS
PEORIA STREET**

SCALE: 1"=5' SHEET 4 OF 5 SHEETS STA. 3705+93 TO STA. 3707+28

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	89
CONTRACT NO. 60W29				
ILLINOIS FED. AID PROJECT				

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0 20 40
SCALE: 1" = 20 FEET



D160W29-Sht-Intersec-04A.dgn
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 PLOT SCALE = 40.0000' / in.
 PLOT DATE = 10/28/2013

DESIGNED - JLV
 DRAWN - KAL
 CHECKED - JMG
 DATE - 10/30/2013

REVISED -
 REVISED -
 REVISED -
 REVISED -

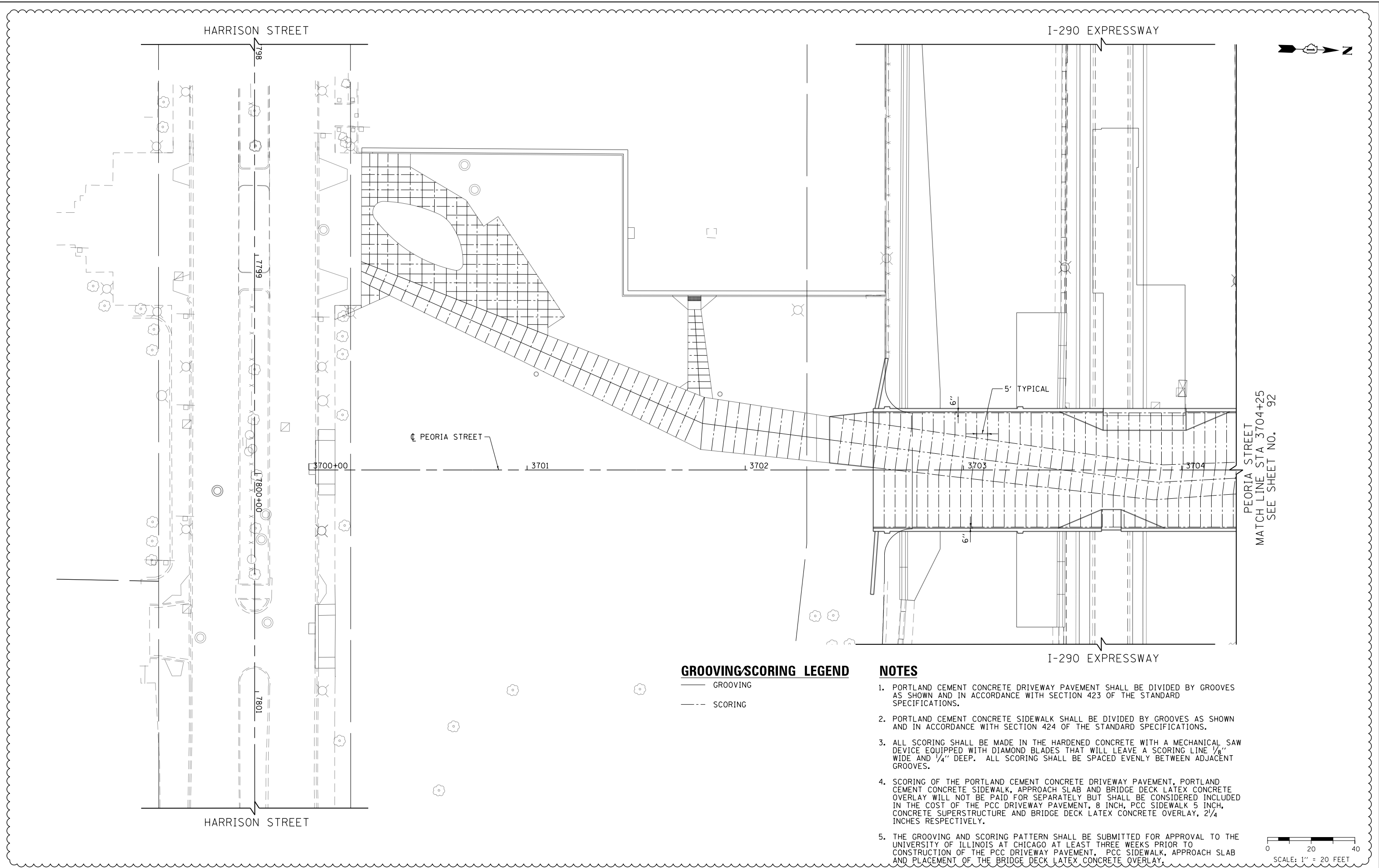
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND GRADING PLAN DETAILS
 TEMPORARY CONNECTION**

SCALE: 1"=20' SHEET 5 OF 5 SHEETS STA. 3700+00 TO STA. 3704+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	90
CONTRACT NO. 60W29				
ILLINOIS FED. AID PROJECT				

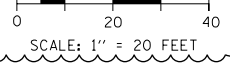
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GROOVING/SCORING LEGEND
 — GROOVING
 - - - SCORING

NOTES

1. PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT SHALL BE DIVIDED BY GROOVES AS SHOWN AND IN ACCORDANCE WITH SECTION 423 OF THE STANDARD SPECIFICATIONS.
2. PORTLAND CEMENT CONCRETE SIDEWALK SHALL BE DIVIDED BY GROOVES AS SHOWN AND IN ACCORDANCE WITH SECTION 424 OF THE STANDARD SPECIFICATIONS.
3. ALL SCORING SHALL BE MADE IN THE HARDENED CONCRETE WITH A MECHANICAL SAW DEVICE EQUIPPED WITH DIAMOND BLADES THAT WILL LEAVE A SCORING LINE 1/8" WIDE AND 1/4" DEEP. ALL SCORING SHALL BE SPACED EVENLY BETWEEN ADJACENT GROOVES.
4. SCORING OF THE PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, PORTLAND CEMENT CONCRETE SIDEWALK, APPROACH SLAB AND BRIDGE DECK LATEX CONCRETE OVERLAY WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE PCC DRIVEWAY PAVEMENT, 8 INCH, PCC SIDEWALK 5 INCH, CONCRETE SUPERSTRUCTURE AND BRIDGE DECK LATEX CONCRETE OVERLAY, 2 1/4 INCHES RESPECTIVELY.
5. THE GROOVING AND SCORING PATTERN SHALL BE SUBMITTED FOR APPROVAL TO THE UNIVERSITY OF ILLINOIS AT CHICAGO AT LEAST THREE WEEKS PRIOR TO CONSTRUCTION OF THE PCC DRIVEWAY PAVEMENT, PCC SIDEWALK, APPROACH SLAB AND PLACEMENT OF THE BRIDGE DECK LATEX CONCRETE OVERLAY.



PEORIA STREET
 MATCH LINE STA 3704+25
 SEE SHEET NO. 92



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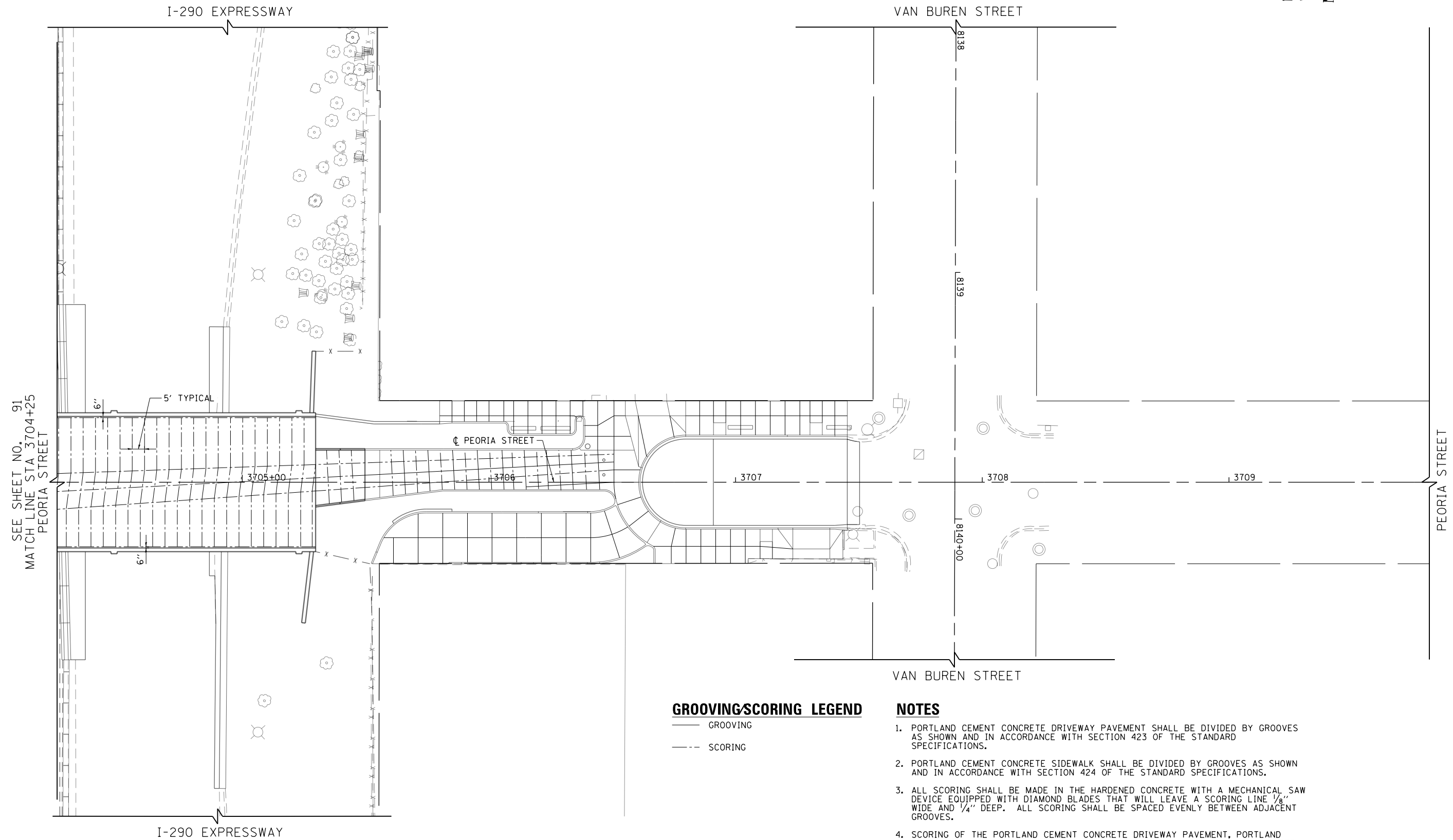
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY DETAILS
 PEORIA STREET SCORING PLAN**

SCALE: 1"=20' SHEET 1 OF 2 SHEETS STA. 3700+00 TO STA. 3704+25

F.A.I. RTE. 90/94/290	SECTION 2013-011R	COUNTY COOK	TOTAL SHEETS 356	SHEET NO. 91
CONTRACT NO. 60W29			ILLINOIS FED. AID PROJECT	

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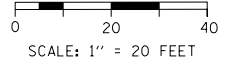
SEE SHEET NO. 91
MATCH LINE STA 3704+25
PEORIA STREET

GROOVING/SCORING LEGEND

- GROOVING
- - - SCORING

NOTES

1. PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT SHALL BE DIVIDED BY GROOVES AS SHOWN AND IN ACCORDANCE WITH SECTION 423 OF THE STANDARD SPECIFICATIONS.
2. PORTLAND CEMENT CONCRETE SIDEWALK SHALL BE DIVIDED BY GROOVES AS SHOWN AND IN ACCORDANCE WITH SECTION 424 OF THE STANDARD SPECIFICATIONS.
3. ALL SCORING SHALL BE MADE IN THE HARDENED CONCRETE WITH A MECHANICAL SAW DEVICE EQUIPPED WITH DIAMOND BLADES THAT WILL LEAVE A SCORING LINE 1/8" WIDE AND 1/4" DEEP. ALL SCORING SHALL BE SPACED EVENLY BETWEEN ADJACENT GROOVES.
4. SCORING OF THE PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, PORTLAND CEMENT CONCRETE SIDEWALK, APPROACH SLAB AND BRIDGE DECK LATEX CONCRETE OVERLAY WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCLUDED IN THE COST OF THE PCC DRIVEWAY PAVEMENT, 8 INCH, PCC SIDEWALK 5 INCH, CONCRETE SUPERSTRUCTURE AND BRIDGE DECK LATEX CONCRETE OVERLAY, 2 1/4 INCHES RESPECTIVELY.
5. THE GROOVING AND SCORING PATTERN SHALL BE SUBMITTED FOR APPROVAL TO THE UNIVERSITY OF ILLINOIS AT CHICAGO AT LEAST THREE WEEKS PRIOR TO CONSTRUCTION OF THE PCC DRIVEWAY PAVEMENT, PCC SIDEWALK, APPROACH SLAB AND PLACEMENT OF THE BRIDGE DECK LATEX CONCRETE OVERLAY.



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PLOT SCALE = 40.0000' / in.	CHECKED - JMG	REVISED -
PLOT DATE = 12/19/2013	DATE - 10/30/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

ROADWAY DETAILS			
PEORIA STREET SCORING PLAN			
SCALE: 1"=20'	SHEET 2	OF 2 SHEETS	STA. 3705+22 TO STA. 3707+40

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	92
CONTRACT NO. 60W29				
ILLINOIS FED. AID PROJECT				

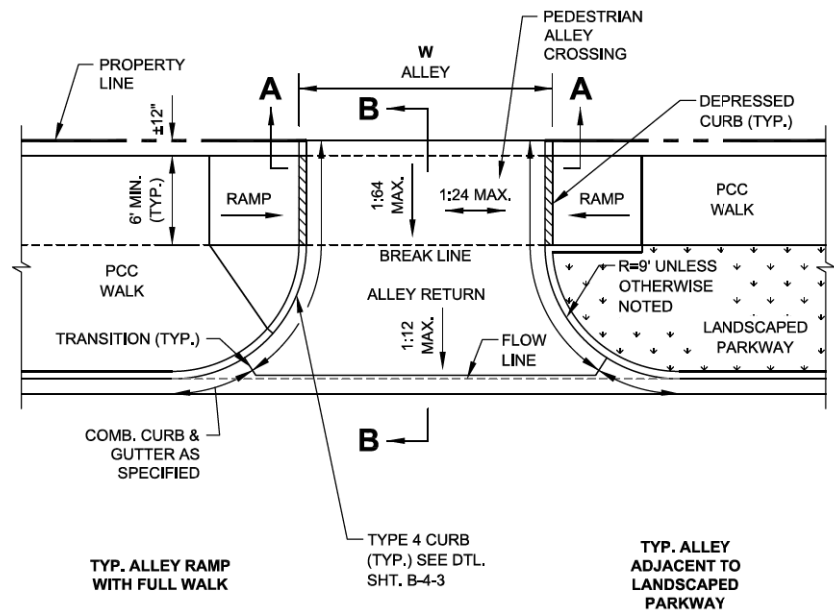
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NOTES:

A. DETECTABLE WARNING IS NOT REQUIRED AT ALLEY RETURNS.

B. RAMP DETAILS ARE DEPENDENT UPON SITE SPECIFIC CONDITIONS (WHERE RAMPS ARE NECESSARY). SEE APPROPRIATE RAMP SHEET AS REQUIRED (TYP.).

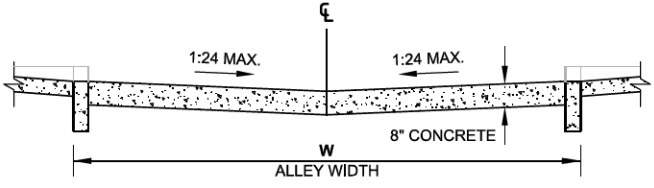
C. SEE DETAIL B-2-5 FOR REDUCED WIDTH (4' MIN.) PEDESTRIAN ACCESSIBLE ROUTE ACROSS ALLEY / DRIVEWAY (TO ALLOW FOR ADDITIONAL SLOPE AT ALLEY / DRIVEWAY).



PLAN VIEW

NOTE:
WORK THIS SHEET WITH SHEET B-2-2.

 City of Chicago Rahm Emanuel, Mayor Department of Transportation Division of Engineering www.cityofchicago.org	DATE	REVISION	CITY OF CHICAGO ALLEY RETURN PLAN VIEW SHEET B-2-1 SCALE: NOT TO SCALE DATE: 10/23/2006 DRAWN BY: CDOT CHECKED BY: LCM
	02/20/07	REVISION 1	
	11/15/07	REVISION 2	
	11/14/08	REVISION 3	
	11/02/09	REVISION 4	
	08/10/12	REVISION 5	



SECTION A-A: CROSS SECTION AT PROPERTY LINE

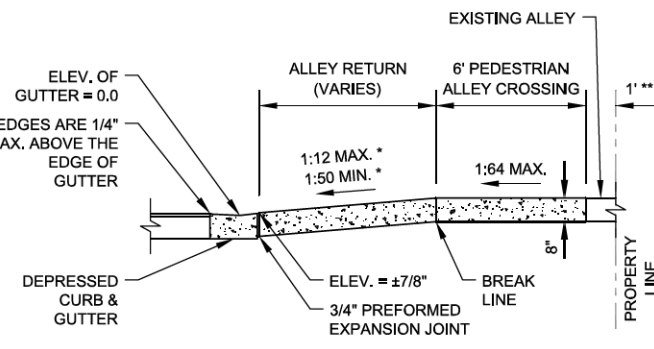
ALLEY RETURN NOTES:

A. WHEN A PORTION OF AN EXISTING PAVED ALLEY IS REMOVED TO ADJUST THE ALLEY PAVEMENT TO THE NEW GRADE, 3/4\"/>

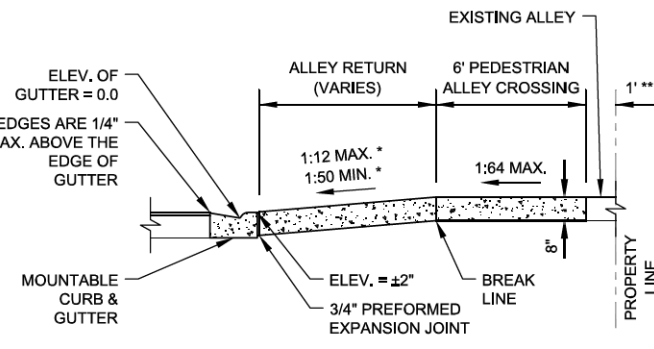
B. SAWED JOINTS SHALL BE SEALED WITH A POURED MATERIAL MEETING THE REQUIREMENTS OF SECTION 1050 OF THE SSRBC.

* SLOPE VARIES AND IS NOT GOVERNED BY ADA

** THE SLOPE OF THE ALLEY IN THE 1' SPACE ADJACENT TO THE PROPERTY LINE MAY BE ALTERED UP TO 10% TO MEET EXISTING CONDITIONS



SECTION B-B: LONGITUDINAL SECTION SHOWING DEPRESSED CURB & GUTTER



SECTION B-B: LONGITUDINAL SECTION SHOWING MOUNTABLE CURB & GUTTER

NOTE:
WORK THIS SHEET WITH SHEET B-2-1.

 City of Chicago Rahm Emanuel, Mayor Department of Transportation Division of Engineering www.cityofchicago.org	DATE	REVISION	CITY OF CHICAGO ALLEY RETURN SECTIONS SHEET B-2-2 SCALE: NOT TO SCALE DATE: 10/23/2006 DRAWN BY: CDOT CHECKED BY: LCM
	02/20/07	REVISION 1	
	11/15/07	REVISION 2	
	11/14/08	REVISION 3	
	11/02/09	REVISION 4	
	08/10/12	REVISION 5	



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PLOT DATE = 10/28/2013	DATE - 10/30/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CITY OF CHICAGO
ADA STANDARDS**


SCALE: NONE SHEET 1 OF 3 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	93
CONTRACT NO. 60W29				
ILLINOIS FED. AID PROJECT				

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GENERAL NOTES:

1. THE DETECTABLE WARNING USED SHALL BE CHOSEN FROM THE CHICAGO DEPARTMENT OF TRANSPORTATION LIST OF APPROVED DETECTABLE WARNING PRODUCTS (AVAILABLE ON THE CITY OF CHICAGO WEBSITE). IT IS NOT ACCEPTABLE TO INSTALL TWO DIFFERENT DETECTABLE WARNING PRODUCTS ADJACENT TO ONE ANOTHER AT ANY LOCATION. IN THE CENTRAL BUSINESS DISTRICT, GRANITE OR OTHER SPECIALTY PAVING MATERIALS MAY BE SUBMITTED TO THE COMMISSIONER FOR APPROVAL.
2. THE DETECTABLE WARNING MUST BE INSTALLED A MAXIMUM OF 8" OR LESS FROM FACE OF CURB (SEE DETAIL SHEET B-4-2).
3. THE DETECTABLE WARNING MUST COVER FULL WIDTH OF RAMP EXCLUDING SIDE FLARES FOR A MINIMUM UNOBSTRUCTED DEPTH OF 24". THE DETECTABLE WARNING LOCATED ON THE SURFACES OF RAMPS IS TYPICALLY ORIENTED PERPENDICULAR TO THE RUN OF THE RAMP UNLESS SPECIAL CIRCUMSTANCES OCCUR (SEE DETAIL SHEET B-1-5). THE DETECTABLE WARNING MUST BE PROVIDED FOR A MINIMUM DEPTH OF 24" FOR THE ENTIRE LENGTH OF THE SIDEWALK WHERE THE SIDEWALK IS FLUSH WITH THE STREET (DEPRESSED CURB OR FLUSH TRANSITION). IF IT IS NECESSARY TO CUT A UNIT(S) IN THE PROVISION OF A COMPLIANT RAMP OR SIDEWALK WITH 24" MINIMUM DEPTH OF DETECTABLE WARNING, THE UNITS SHALL BE CUT IN A NEAT AND WORKMAN LIKE MANNER PER MANUFACTURER'S REQUIREMENTS WITH A MINIMUM OF THREE PINS OR ANCHOR POINTS (WHERE APPLICABLE). THE UNITS SHALL BE ARRANGED SO THAT THE CUT UNITS ARE LARGE ENOUGH TO BE PROPERLY AND ADEQUATELY SECURED. CUT UNITS SHALL NOT BE USED UNLESS ALL OTHER DESIGN OPTIONS HAVE BEEN EXHAUSTED. THE USE OF SALVAGE PIECES FROM UNITS THAT ARE CUT WILL NOT BE PERMITTED WITHOUT WRITTEN APPROVAL OF THE COMMISSIONER. CUT UNIT SALVAGE PIECES NOT APPROVED FOR USE MUST BE REMOVED FROM THE SITE AND DISPOSED OF PROPERLY.
4. WHERE APPLICABLE, A COMBINATION OF STRAIGHT AND RADIAL DETECTABLE WARNING UNITS MAY BE USED ON COMPOUND AND LARGE RADII. CONTRACTOR MUST MAKE THIS DETERMINATION AND VERIFY IN FIELD.
5. THE DETECTABLE WARNING MUST CONTRAST WITH ADJACENT PAVEMENT. IF LIGHT COLORED PAVEMENT IS USED THE DETECTABLE WARNING COLOR SHALL BE RED. IF A DARK COLORED PAVEMENT IS USED THE DETECTABLE WARNING COLOR SHALL BE YELLOW. CONTRACTOR TO VERIFY THAT PROPER CONTRAST IS OBTAINED.
6. PRIOR TO PLACING CONCRETE FOR DEPRESSED CURBS, RAMPS, OR SIDEWALKS THE CONTRACTOR SHALL VERIFY THAT LAYOUT OR DESIGN COMPLIES WITH THE REQUIREMENTS OF THE CDOT ADA STANDARDS.
7. RAMP WIDTH MUST BE A MINIMUM OF 6'-0" AND IN INCREMENTS OF 1'-0", EXCEPT WHEN USING THE PERPENDICULAR RAMP AT CORNER (OR OTHER SPECIAL CDOT APPROVED CONDITIONS), WHICH HAS A MINIMUM WIDTH OF 4'-0".
8. THE MAXIMUM ALLOWABLE RAMP RUNNING SLOPE IS 1:14, MEASURED AT ANY PORTION OF THE RAMP. IF POSSIBLE, A MORE GRADUAL SLOPE SHALL BE USED. GRADE BREAKS AT THE TOP AND BOTTOM OF RAMPS SHALL BE PERPENDICULAR TO THE DIRECTION OF RAMP RUN.
9. THE MAXIMUM ALLOWABLE RAMP CROSS SLOPE IS 1:64, MEASURED AT ANY PORTION OF THE RAMP. IF POSSIBLE, A MORE GRADUAL SLOPE SHALL BE USED.
10. THE MAXIMUM ALLOWABLE RAMP LANDING SLOPE IS 1:64, MEASURED AT ANY LOCATION AND IN ANY DIRECTION ON THE LANDING. THE RAMP LANDING WIDTH SHALL MATCH THE FULL WIDTH OF THE RAMP FOR A MINIMUM UNOBSTRUCTED DEPTH OF 4'-0". RAMP LANDINGS SHALL BE PROVIDED AT THE TOP AND/OR BOTTOM OF RAMPS WHERE TURNING IS REQUIRED.
11. RAMP SIDE FLARES SHALL BE INSTALLED AT ANY LOCATION WHERE THE SURFACE ADJACENT TO THE RAMP SURFACE IS INTENDED FOR PEDESTRIAN USE. TRIPPING HAZARDS, INCLUDING STEPS, DROP-OFFS, OR CURBS SHALL NOT BE LOCATED WITHIN THE LIMITS OF THE SIDEWALK. RAMP SIDE FLARES ARE NOT REQUIRED WHERE THE SURFACE ADJACENT TO THE RAMP SURFACE IS LANDSCAPED OR IS OCCUPIED BY A BARRIER THAT BLOCKS PEDESTRIAN ACCESS. EXCEPTIONS TO THIS RULE MAY BE SUBMITTED TO THE COMMISSIONER FOR APPROVAL.

	DATE	REVISION	CITY OF CHICAGO GENERAL NOTES SHEET B-3-2	SCALE: NOT TO SCALE DATE: 10/23/2006 DRAWN BY: CDOT CHECKED BY: LCM
	02/20/07	REVISION 1		
	11/15/07	REVISION 2		
	11/14/08	REVISION 3		
	11/02/09	REVISION 4		
08/10/12	REVISION 5			

GENERAL NOTES (CONTINUED):

12. UTILITIES, SUCH AS LIGHT POLES, TRAFFIC POLES AND HYDRANTS, MAY BE LOCATED IN THE FLARE OF THE RAMP BUT ARE NOT ALLOWED ON THE RAMP SURFACE OR LANDING AREAS. EXISTING UTILITY STRUCTURE LIDS MAY REMAIN WITHIN THE FLARE OR ON THE SURFACE OF THE RAMP IF THE REQUIREMENTS OF GENERAL NOTE #19 ARE MET.
13. ALL LOCATIONS WITH TYPE 4 OR TYPE B CURB (EXCEPT ALLEY APRONS) SHALL BE CONSTRUCTED AS CURB AND GUTTER TYPE BV.12 THROUGH THE LIMITS OF THE CORNER AND THE CURB RAMPS.
14. ALTERATIONS SHALL NOT DECREASE THE ACCESSIBILITY TO EXISTING FACILITIES, SIDEWALKS LEADING TO EXISTING FACILITIES, OR DOOR OR GATE ACCESS POINTS TO FACILITIES. THE ELEVATION AT THE EXISTING PROPERTY LINE OR FACILITY ACCESS POINT SHALL BE MAINTAINED AT A MINIMUM. ANY ALTERATIONS ADJACENT TO OR AFFECTING A FACILITY ACCESS POINT SHALL RESULT IN IMPROVED ACCESS OR AT A MINIMUM A REPLICATION OF EXISTING CONDITIONS, INCLUDING SIDEWALK SLOPES AND SURFACE CONDITIONS. FACILITIES INCLUDE, BUT ARE NOT LIMITED TO PRIVATE BUSINESSES, PUBLIC BUILDINGS, RESIDENCES, BUS STOPS, PUBLIC BENCHES, PAY PHONES, AND PARKING METERS.
15. THE MINIMUM CROSSWALK WIDTH IS 6'-0". CROSSWALKS SHALL BE LOCATED AS SHOWN IN THE PLAN SHEETS DEPENDING ON THE TYPE OF CURB RAMP USED. BEYOND THE CURB FACE AT THE BASE OF CURB RAMPS, A CLEAR SPACE OF 4'-0" BY 4'-0" MINIMUM SHALL BE PROVIDED WITHIN THE STRIPES OF THE CROSSWALK (WHERE PROVIDED).
16. IF SIDEWALK AND ALLEY ARE AT THE SAME GRADE, A RAMP IS NOT REQUIRED. IF SIDEWALK AND DRIVEWAY ARE AT THE SAME GRADE, A RAMP IS NOT REQUIRED BUT DETECTABLE WARNING UNITS ARE STILL REQUIRED IF THE DRIVEWAY HAS TRAFFIC CONTROL DEVICES (I.E. TRAFFIC SIGNALS).
17. MAIN LINE SIDEWALK SHALL HAVE A MAXIMUM CROSS SLOPE NOT TO EXCEED 1:64 FOR THE FULL WIDTH OF WALK UNLESS OTHERWISE APPROVED BY THE COMMISSIONER. WHERE TURNING IS REQUIRED AND WHERE SIDEWALKS INTERSECT, THE SLOPE OF THE SIDEWALK SHALL NOT EXCEED 1:64 IN ANY DIRECTION.
18. MAIN LINE SIDEWALK RUNNING SLOPES SHALL NOT EXCEED 1:24 OR THE GENERAL GRADE ESTABLISHED FOR THE ADJACENT STREET, WHICH EVER IS HIGHER.
19. THERE SHALL BE NO VERTICAL LEVEL DIFFERENCES BETWEEN SURFACES GREATER THAN 1/4" ON THE MAIN LINE SIDEWALK. THERE SHALL BE NO HORIZONTAL GAPS OR OPENINGS GREATER THAN 1/2" ON THE MAIN LINE SIDEWALK.
20. WHERE OBSTRUCTIONS EXIST ON THE MAINLINE SIDEWALK, THE CLEAR WIDTH OF USEABLE SIDEWALK SHALL NOT BE LESS THAN 4'-0". OBSTRUCTIONS INCLUDE, BUT ARE NOT LIMITED TO SIDEWALK BENCHES, FIRE HYDRANTS, SIGNAL OR LIGHT POLES, NEWSPAPER DISPENSERS, TRASH RECEPTACLES, AND UTILITY PEDESTALS.
21. CURB RAMPS AND LANDING (KEYSTONE) TO BE CONSTRUCTED WITH 8" THICK CONCRETE AT ALL TRAFFIC SIGNALIZED INTERSECTIONS AND INDUSTRIAL STREET INTERSECTIONS. AT ALL OTHER LOCATIONS, 5" THICK CONCRETE TO BE USED.
22. NO DEVIATIONS FROM THESE STANDARDS ARE ALLOWED WITHOUT WRITTEN APPROVAL FROM THE COMMISSIONER.
23. DEPRESSED CURB, RAMP, OR SIDEWALK DESIGNS OR LAYOUTS SHALL MAINTAIN OR IMPROVE EXISTING DRAINAGE AND THE EXISTING INTERSECTION GEOMETRY SHALL NOT BE MODIFIED WITHOUT CDOT APPROVAL.
24. ALL CONSTRUCTION DOCUMENTS MUST BE STAMPED BY A LICENSED ARCHITECT/LANDSCAPE ARCHITECT/ENGINEER TO CERTIFY THAT THEY ARE IN ACCORDANCE WITH THE AMERICANS WITH DISABILITIES ACT (ADA) AND ALL CODES AND BUILDING ORDINANCES OF THE CITY OF CHICAGO AND STATE OF ILLINOIS.

	DATE	REVISION	CITY OF CHICAGO GENERAL NOTES (CONTINUED) SHEET B-3-3	SCALE: NOT TO SCALE DATE: 10/23/2006 DRAWN BY: CDOT CHECKED BY: LCM
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	11/15/07	REVISION 2		
	11/14/08	REVISION 3		
	11/02/09	REVISION 4		
08/10/12	REVISION 5			

ADA COMPLIANCE AND TRANSITION GUIDELINES

POLICY STATEMENT: ANY ALTERATION OF THE PUBLIC WAY MUST BE RESTORED IN AN ADA COMPLIANT MANNER

I. STREET RESURFACING / RECONSTRUCTION *

FOR ANY RESURFACING/RECONSTRUCTION PROJECT WHERE, WITHIN THE PROJECT LIMITS, A CROSSWALK IS ENCOUNTERED OR WHERE THE PROJECT LIMITS TERMINATE WITHIN 4' OR LESS OF A CROSSWALK, THOSE CROSSWALKS AND THE ASSOCIATED CURB RAMPS MUST BE IMPROVED TO CURRENT ADA STANDARDS. WHERE RESURFACING/RECONSTRUCTION ENCOUNTERS LESS THAN 1/4 POINT OF THE STREET SURFACE (LONGITUDINAL CUT), IT IS ACCEPTABLE PRACTICE TO EXCLUDE IMPROVEMENTS TO THE ASSOCIATED CROSSWALKS AND CURB RAMPS.

WHEN A PROJECT SCOPE OF WORK CALLS FOR ONLY AN INTERSECTION TO BE REPAVED, THE INTERSECTION LIMITS AS DEFINED BY THE AREA OUTLINED BY OUTERMOST CROSSWALK LINES AND ADJACENT CURB FACES AND ALL ADJOINING CROSSWALKS AND CURB RAMPS MUST BE IMPROVED TO CURRENT ADA STANDARDS.

WHEN A PROJECT SCOPE OF WORK IS LIMITED TO A SINGLE CORNER OF AN INTERSECTION, THE CURB RAMP MUST BE IMPROVED TO CURRENT ADA STANDARDS AND THE ADJACENT PAVEMENT MUST BE RESURFACED, AS NECESSARY TO PROVIDE FOR A FLUSH TRANSITION.

FOR ANY RESURFACING / RECONSTRUCTION PROJECT WHERE, WITHIN THE PROJECT LIMITS, AN ALLEY APRON IS ENCOUNTERED, THE ASSOCIATED CURB RAMPS, ALLEY APRON, AND SIDEWALKS MUST BE IMPROVED TO CURRENT ADA STANDARDS.

II. SIDEWALK INSTALLATION / REPAIRS / RECONSTRUCTION

THE LIMITS OF ANY MAINLINE SIDEWALK REPLACEMENT, GREATER THAN TEN FEET (10') IN LENGTH, THAT ABUT AN EXISTING RAMP LOCATION (INCLUDING THE FOUR FOOT (4') LANDING AREA AND/OR THE "KEYSTONE"), SHALL BE EXTENDED TO INCLUDE THE AFFECTED RAMPS AND THESE RAMPS SHALL BE RECONSTRUCTED TO CURRENT ADA STANDARDS. IN ADDITION, ALL NEWLY PLACED SIDEWALK TEN FEET (10') OR MORE IN LENGTH SHALL BE CONSTRUCTED IN ACCORDANCE WITH ALL CURRENT APPLICABLE STANDARDS WHICH INCLUDE PROVIDING A MINIMUM FOUR FEET (4') WIDTH ACCESSIBLE PATHWAY WITH A CROSS SLOPE NOT TO EXCEED 1:64.

III. GUIDELINES FOR TRANSITIONING TO EXISTING NON-COMPLIANT CONDITION


NEW SIDEWALK PLACEMENTS GREATER THAN TEN FEET IN CONTIGUOUS LENGTH:
 THE LIMITS OF ANY MAINLINE SIDEWALK REPLACEMENT, GREATER THAN TEN FEET (10') IN LENGTH, MUST BE EXTENDED FOR A MINIMUM FIVE ADDITIONAL FEET (5') EITHER SIDE IN ORDER TO PROVIDE A REASONABLE TRANSITION TO MATCH THE EXISTING SIDEWALK. THE LENGTH OF TRANSITION SHALL BE LENGTHENED AS NECESSARY TO ENSURE THAT THE RUNNING SLOPE OF THE TRANSITION DOES NOT EXCEED A SLOPE OF 1:24 (PREFERRED) OR 1:14 (MAXIMUM) AT ANY POINT.

NEW SIDEWALK REPLACEMENTS TEN FEET OR LESS IN CONTIGUOUS LENGTH (REPAIRS):
 IT IS ACCEPTABLE PRACTICE TO MATCH ADJACENT SIDEWALKS AT THE EXISTING SLOPE.

CURB RAMP REPLACEMENTS:
 SIDEWALK REPLACEMENT BEYOND THE LIMITS OF THE LANDING AREA AND/OR THE "KEYSTONE" MUST BE EXTENDED A MINIMUM OF AN AN ADDITIONAL FIVE FEET (5') EITHER SIDE IN ORDER TO PROVIDE A REASONABLE TRANSITION TO MATCH THE EXISTING SIDEWALK. THE TRANSITION SHALL BE LENGTHENED AS NECESSARY TO ENSURE THAT THE RUNNING SLOPE OF THE TRANSITION DOES NOT EXCEED A SLOPE OF 1:24 (PREFERRED) OR 1:14 (MAXIMUM) AT ANY POINT.

NO EXCEPTIONS TO THE ABOVE WILL BE ALLOWED WITHOUT WRITTEN APPROVAL FROM THE COMMISSIONER.

* THE REQUIREMENTS OF SECTION I. SHALL APPLY ONLY TO CITY AGENCIES

	DATE	REVISION	CITY OF CHICAGO ADA COMPLIANCE AND TRANSITION GUIDELINES SHEET B-3-4	SCALE: NOT TO SCALE DATE: 10/23/2006 DRAWN BY: CDOT CHECKED BY: LCM
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	11/15/07	REVISION 2		
	11/14/08	REVISION 3		
	11/02/09	REVISION 4		
08/10/12	REVISION 5			



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USER NAME = BAW1tor t	DRAWN - CDOT	REVISED -
PLOT SCALE = 2.0000' / in.	CHECKED - CDOT	REVISED -
PLOT DATE = 10/28/2013	DATE - 10/30/2013	REVISED -

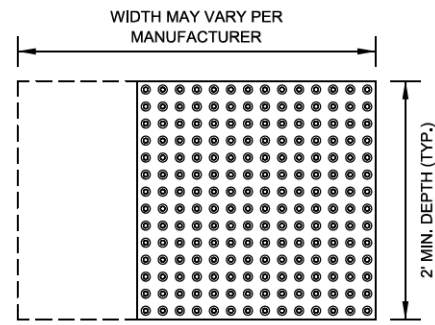
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO
ADA STANDARDS

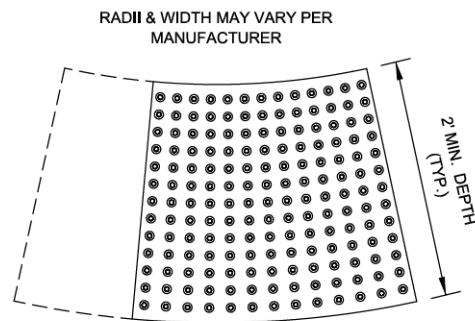
SCALE: NONE SHEET 2 OF 3 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	94
CONTRACT NO. 60W29			ILLINOIS FED. AID PROJECT	

STRAIGHT DETECTABLE WARNING UNITS



RADIAL DETECTABLE WARNING UNITS



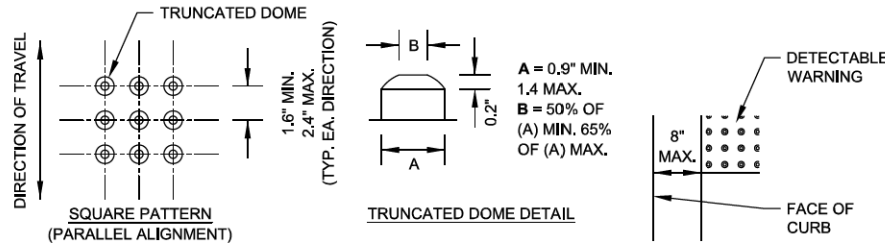
DETECTABLE WARNING UNIT SIZES

- VERIFY ALL DIMENSIONS WITH THE PRODUCT MANUFACTURER.
- IF USING RADIAL UNITS, VERIFY THAT THE CURB RADIUS MATCHES AVAILABLE UNIT RADIUS WITH THE PRODUCT MANUFACTURER.
- APPROVED LIST OF DETECTABLE WARNING PRODUCTS CAN BE FOUND ON CDOT'S WEBSITE (www.cityofchicago.org).

	DATE	REVISION	CITY OF CHICAGO DETECTABLE WARNING UNIT SIZES SHEET B-4-1	SCALE: NOT TO SCALE DATE: 10/23/2006	DRAWN BY: CDOT CHECKED BY: LCM
	02/20/07	REVISION 1			
	11/15/07	REVISION 2			
	11/14/08	REVISION 3			
	11/02/09	REVISION 4			
08/10/12	REVISION 5				

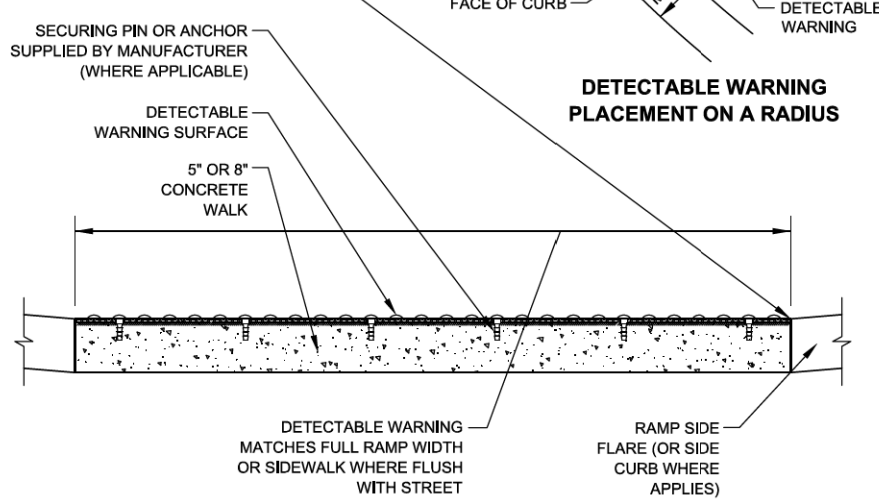
GENERAL NOTE:

THE ROWS OF DOMES IN THE DETECTABLE WARNING MATERIAL MUST BE ALIGNED WITH THE PATH OF WHEELCHAIR TRAVEL WHICH IS REQUIRED TO BE PERPENDICULAR TO THE GRADE BREAK AT THE BOTTOM OF THE RAMP TO PERMIT TRACKING BETWEEN DOME ROWS. ON BLENDED TRANSITIONS OR FLUSH TRANSITIONS, WHERE RADIAL UNITS ARE SITUATED ABOUT THE CURB RADIUS, DOME ORIENTATION IS NOT SIGNIFICANT.



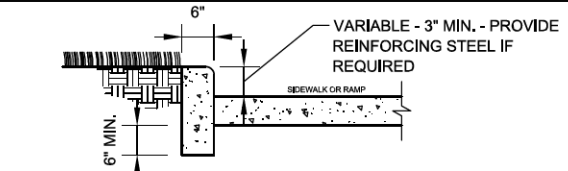
UNIT PATTERN & DOME DETAIL

TYPICAL DETECTABLE WARNING PLACEMENT

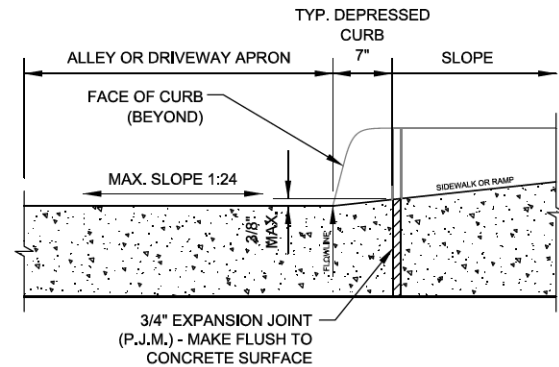


DETECTABLE WARNING UNIT SECTION

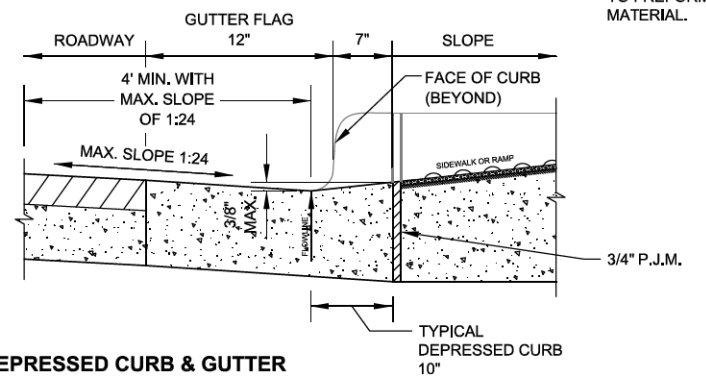
	DATE	REVISION	CITY OF CHICAGO DETECTABLE WARNING UNIT DETAILS SHEET B-4-2	SCALE: NOT TO SCALE DATE: 10/23/2006	DRAWN BY: CDOT CHECKED BY: LCM
	02/20/07	REVISION 1			
	11/15/07	REVISION 2			
	11/14/08	REVISION 3			
	11/02/09	REVISION 4			
08/10/12	REVISION 5				



SIDE CURB - SECTION



DEPRESSED CURB & GUTTER AT ALLEY/DRIVEWAY APRON (TYPE 4 OR B CURB)



DEPRESSED CURB & GUTTER AT BOTTOM OF TYPICAL CURB RAMP

	DATE	REVISION	CITY OF CHICAGO CURB & GUTTER DETAILS SHEET B-4-3	SCALE: NOT TO SCALE DATE: 10/23/2006	DRAWN BY: CDOT CHECKED BY: LCM
	02/20/07	REVISION 1			
	11/15/07	REVISION 2			
	11/14/08	REVISION 3			
	11/02/09	REVISION 4			
08/10/12	REVISION 5				

NOTES FOR CURB & GUTTER DETAILS THIS SHEET:

- A. CROSS SLOPE AT DEPRESSED CURB & GUTTER NOT TO EXCEED 1:64.
- B. DETECTABLE WARNING SURFACE AT DRIVEWAYS REQUIRED ONLY FOR COMMERCIAL DRIVEWAYS WITH TRAFFIC CONTROL DEVICES, I.E. SIGNALS.
- C. REFER TO REGULATIONS FOR OPENINGS, CONSTRUCTION AND REPAIR IN THE PUBLIC WAY (CDOT) FOR ADDITIONAL REQUIREMENTS FOR CURB AND GUTTER INSTALLATION.
- D. RAMP SIDE FLARES SHALL BE INSTALLED AT ANY LOCATION WHERE THE SURFACE ADJACENT TO THE RAMP SURFACE IS INTENDED FOR PEDESTRIAN USE. TRIPPING HAZARDS, INCLUDING STEPS, DROP-OFFS, OR SIDE CURBS SHALL NOT BE LOCATED WITHIN THE LIMITS OF THE SIDEWALK.
- E. 'P.J.M.' THIS SHEET REFERS TO PREFORMED JOINT MATERIAL.

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PLOT DATE = 10/28/2013	DATE - 10/30/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**CITY OF CHICAGO
ADA STANDARDS**

SCALE: NONE SHEET 3 OF 3 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	95
CONTRACT NO. 60W29			ILLINOIS FED. AID PROJECT	

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PAVEMENT MARKING SCHEDULE

LOCATION	STATION	TO	STATION	EPOXY PAVEMENT MARKING - LINE 4"	EPOXY PAVEMENT MARKING - LINE 5"	EPOXY PAVEMENT MARKING - LINE 8"	EPOXY PAVEMENT MARKING - LINE 12"	RAISED REFLECTIVE PAVEMENT MARKER	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	PAVEMENT MARKING REMOVAL
				FOOT	FOOT	FOOT	FOOT	EACH	EACH	SQ FT
PEORIA STREET										
I-290 WB				1,034	584	1,017	53	8	8	1,313
I-290 EB				1,880	1,341	3,165	369	95	90	3,374
TOTALS				2,914	1,925	4,182	421	103	98	4,687



DI60W29-sht-Pmk-Schedule.dgn	DESIGNED - JDT	REVISED - 12/18/2013
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PLOT SCALE = 20.0000' / in.	CHECKED - JMG	REVISED -
PLOT DATE = 12/19/2013	DATE - 10/30/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING PLAN
SCHEDULE**

SCALE: NONE SHEET 1 OF 3 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	96
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60W29	

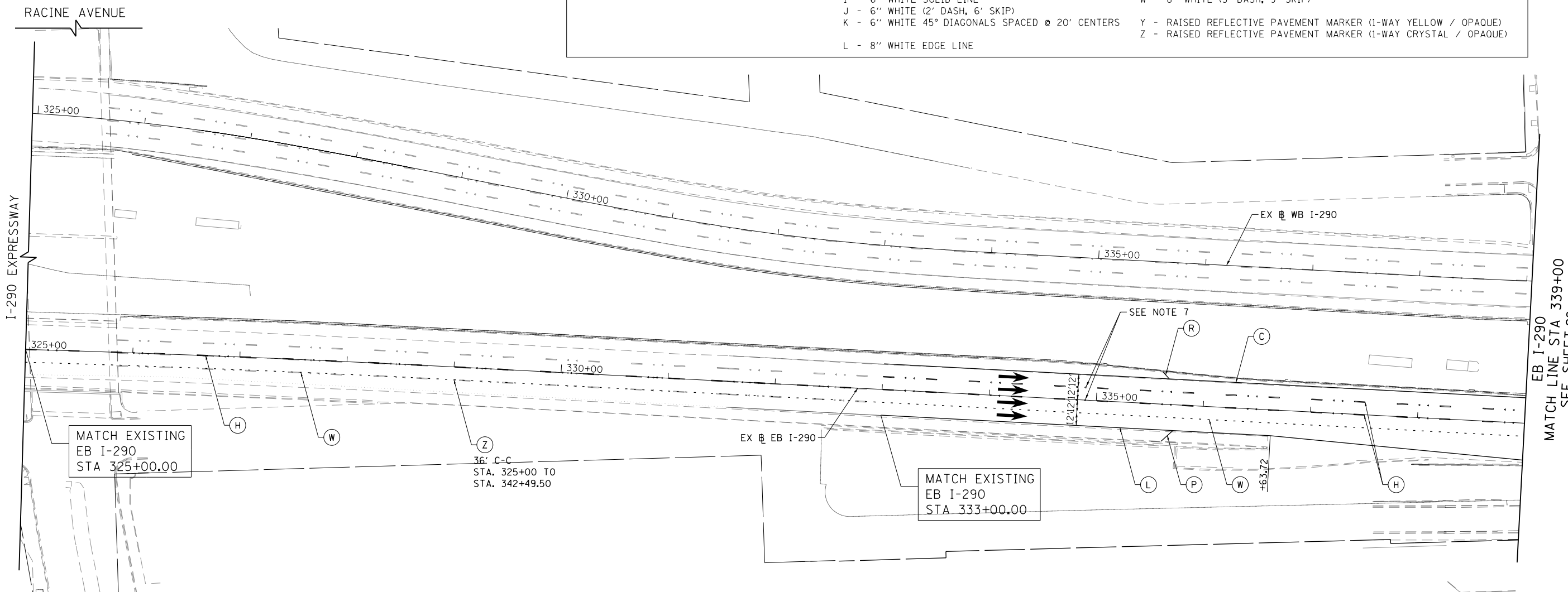
LEGEND

PAVEMENT MARKING MATERIAL

- EPOXY PAVEMENT MARKING
- ◇ TEMPORARY PAVEMENT MARKING
- THERMOPLASTIC PAVEMENT MARKING
- △ POLYUREA PAVEMENT MARKING TYPE I

PAVEMENT MARKING TYPE

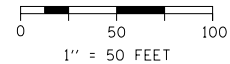
- A - LETTERS AND SYMBOLS (WHITE)
- B - 4" WHITE EDGE LINE
- C - 4" YELLOW EDGE LINE
- D - 4" DOUBLE YELLOW CENTERLINE
- E - 4" WHITE (6' DASH, 18' SKIP)
- F - 4" YELLOW (6' DASH, 18' SKIP)
- G - 4" WHITE (2' DASH, 6' SKIP)
- H - 5" WHITE (10' DASH, 30' SKIP)
- I - 6" WHITE SOLID LINE
- J - 6" WHITE (2' DASH, 6' SKIP)
- K - 6" WHITE 45° DIAGONALS SPACED @ 20' CENTERS
- L - 8" WHITE EDGE LINE
- M - 12" WHITE (2' DASH, 2' SKIP)
- N - 12" WHITE 45° DIAGONALS SPACED @ 20' CENTERS
- O - 12" WHITE 45° DIAGONALS SPACED @ 30' CENTERS
- P - 12" WHITE 45° DIAGONALS SPACED @ 500' CENTERS
- Q - 12" YELLOW 45° DIAGONALS SPACED @ 50' CENTERS
- R - 12" YELLOW 45° DIAGONALS SPACED @ 500' CENTERS
- S - 12" WHITE CHEVRONS SPACED @ 30' CENTERS
- T - 24" WHITE STOP BAR
- U - 24" WHITE CROSSWALK, 6' WIDTH, SPACED @ 4' CENTERS
- W - 8" WHITE (3' DASH, 9' SKIP)
- Y - RAISED REFLECTIVE PAVEMENT MARKER (1-WAY YELLOW / OPAQUE)
- Z - RAISED REFLECTIVE PAVEMENT MARKER (1-WAY CRYSTAL / OPAQUE)



PAVEMENT MARKING NOTES

1. ALL FINAL PAVEMENT MARKING MATERIALS PLACED ON PCC PAVEMENT SHALL BE POLYUREA TYPE I EXCEPT WHERE NOTED IN THE PLANS.
2. ALL FINAL PAVEMENT MARKING MATERIALS PLACED ON BITUMINOUS PAVEMENT SHALL BE THERMOPLASTIC EXCEPT WHERE NOTED IN THE PLANS.
3. ALL FINAL PAVEMENT MARKING SYMBOLS SHALL BE OF SMALL SIZE.
4. ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE OF THE IMPROVEMENT LIMITS SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
5. BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RECORD AND RETAIN ALL EXISTING PAVEMENT MARKINGS AND MARKERS FOR FUTURE REFERENCE. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE DIRECTED BY THE ENGINEER.
6. THE RESIDENT ENGINEER SHALL CONTACT JERNARD PERKINS, AREA TRAFFIC FIELD ENGINEER AT 708-524-2145 A MINIMUM OF 2 WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

7. PROPOSED MARKINGS SHALL BE LAID OUT IN ORDER TO UTILIZE EXISTING RAISED REFLECTIVE MARKERS LOCATIONS. PROPOSED RAISED REFLECTIVE MARKER LOCATIONS ARE CALLED OUT IN THE PLANS ACCORDINGLY.
8. ALL REMOVED, DAMAGED, OR BROKEN RAISED REFLECTIVE MARKER REFLECTORS SHALL BE REPLACED AT THE DIRECTION OF THE RESIDENT ENGINEER AND PAID FOR AS REPLACEMENT REFLECTOR.



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PLOT DATE = 12/19/2013	DATE - 10/30/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKING PLAN			
I-290			
SCALE: 1"=50'	SHEET 2	OF 3 SHEETS	STA. 325+00 TO STA. 339+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	97
CONTRACT NO. 60W29				
ILLINOIS FED. AID PROJECT				

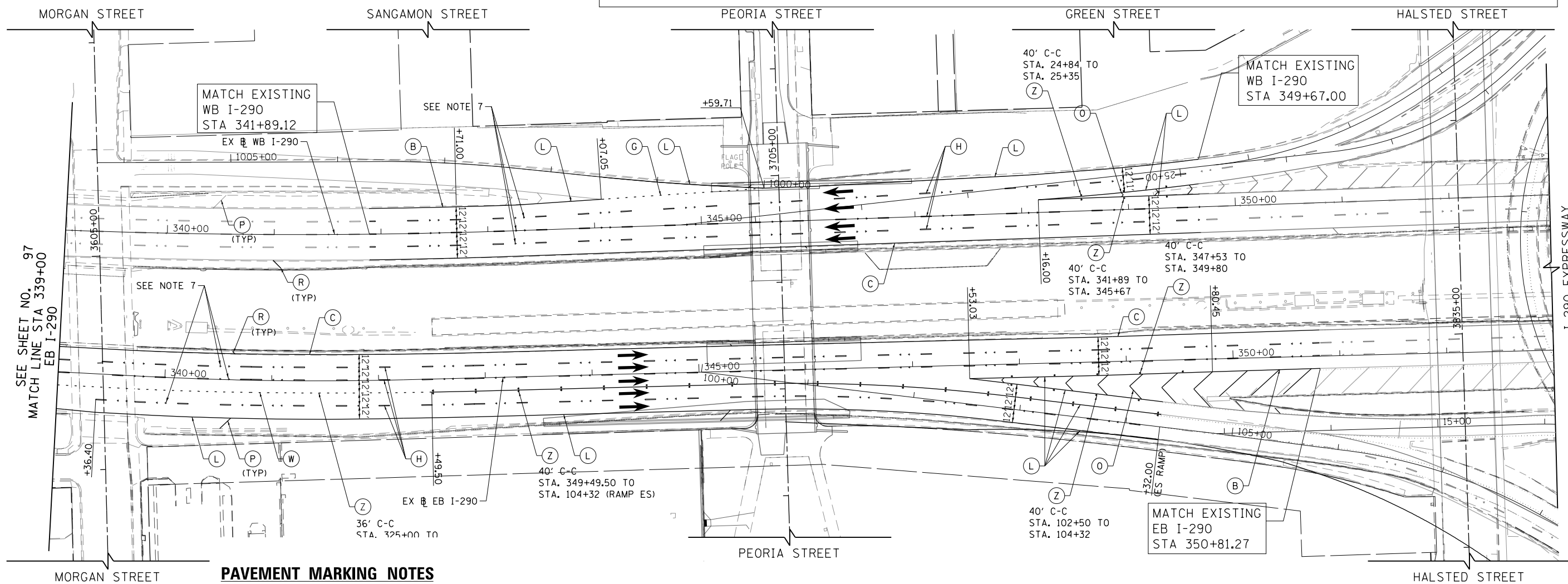
LEGEND

PAVEMENT MARKING MATERIAL

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- ◇ TEMPORARY PAVEMENT MARKING
- THERMOPLASTIC PAVEMENT MARKING
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- K - 6" WHITE 45° DIAGONALS SPACED @ 20' CENTERS
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- M - 12" WHITE (2' DASH, 2' SKIP)
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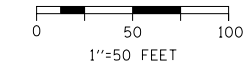


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PLOT DATE = 12/19/2013	DATE - 10/30/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

PAVEMENT MARKING PLAN			
I-290			
SCALE: 1"=50'	SHEET 3	OF 3 SHEETS	STA. 339+00 TO STA. 353+00

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-011R	COOK	356	98
CONTRACT NO. 60W29				
ILLINOIS FED. AID PROJECT				



SIGNING SCHEDULE

LOCATION	SIGN NO.	LEGEND / DESCRIPTION	CODE	ACTION	LOCATION (STATION / OFFSET)				EXISTING PANEL DIMENSIONS		PROPOSED PANEL DIMENSIONS		NUMBER OF POSTS	REMOVE SIGN PANEL - TYPE 1 (SQ FT)	REMOVE SIGN PANEL - TYPE 2 (SQ FT)	REMOVE OVERHEAD SIGN STRUCTURE - WALKWAY (FOOT)	REMOVE OVERHEAD SIGN STRUCTURE - BRIDGE MOUNTED (EACH)	REMOVE SIGN PANEL ASSEMBLY - TYPE A (EACH)	REMOVE SIGN PANEL ASSEMBLY - TYPE B (EACH)	SIGN PANEL - TYPE 1 (SQ FT)	SIGN PANEL - TYPE 2 (SQ FT)	SIGN PANEL - TYPE 3 (SQ FT)	STEEL POSTS, SPECIAL (EACH)	OVERHEAD SIGN STRUCTURE - BRIDGE MOUNTED (FT)	TELESCOPING STEEL SIGN SUPPORT (FT)
					EXISTING		PROPOSED		WIDTH (FT)	HEIGHT (FT)	WIDTH (FT)	HEIGHT (FT)													
PEORIA STREET	PEO-01-ST	CTA BUS STOP	CUSTOM	EXISTING SIGN TO REMAIN	3700+06	69.03' LT	-	-	1.50	2.00	-	-	1												
	PEO-02-LP	PEDESTRIAN CROSSING	W11-2	EXISTING SIGN TO REMAIN	3700+06	47.33' LT	-	-	2.50	2.50	-	-													
	PEO-03-LP	NO PARKING / TOW ZONE	R7-201-4	EXISTING SIGN TO REMAIN	3700+06	27.86' RT	-	-	1.50	1.50	-	-													
	PEO-04-CP	UIC	CUSTOM	REMOVE AND REPLACE SIGN (SEE NOTE 1)	3700+56	3.39' LT	(BY OTHERS)		3.33	9.00	3.33	9.00													
	PEO-05-ST	UIC MAP	CUSTOM	REMOVE AND REPLACE SIGN (SEE NOTE 1)	3702+38	0.16' RT	(BY OTHERS)		2.92	2.92	2.92	2.92	1												
	PEO-06-ST	NO PETS ALLOWED	CUSTOM	REMOVE AND REPLACE SIGN (SEE NOTE 1)	3702+58	45.03' RT	(BY OTHERS)		1.50	1.00	1.50	1.00	1												
	PEO-07-ST	NO PARKING-ANY TIME	R7-4	REMOVE	3703+18	21.20' LT	-	-	1.50	1.00	-	-	1				1								
	PEO-08-ST	NO PARKING-ANY TIME	R7-4	REMOVE	3703+18	21.02' RT	-	-	1.00	1.50	-	-	1				1								
	PEO-09-ST	WELCOME TO UIC	CUSTOM	REMOVE AND REPLACE SIGN (SEE NOTE 1)	3703+56	26.14' RT	(BY OTHERS)		3.92	4.58	3.92	4.58	2												
	PEO-10-ST	HANDICAP ACCESS	CUSTOM	REMOVE AND REPLACE SIGN (SEE NOTE 2)	3703+99	9.38' RT	(BY OTHERS)		1.00	1.50	1.00	1.50	2												
	PEO-11-ST	CTA BLUE LINE UIC-HALSTED	CUSTOM	REMOVE AND REPLACE SIGN (SEE NOTE 2)			(BY OTHERS)	0.33	1.42	0.33	1.42														
	PEO-12-ST	NO PARKING / TOW ZONE	R7-201-4	REMOVE	3704+03	2.59' LT	-	-	1.50	1.00	-	-	1				1								
	PEO-13-LP	NO PARKING / TOW ZONE	R7-201-1/3	REMOVE	3705+14	20.99' LT	-	-	1.50	1.50	-	-		2.25											
	PEO-14-LP	NO PARKING / TOW ZONE	R7-201-1/3	REMOVE	3705+15	21.12' RT	-	-	1.50	1.00	-	-		1.50											
	PEO-15-ST	NO PARKING / TOW ZONE	R7-201-1/3	REMOVE	3705+52	20.68' LT	-	-	1.50	1.50	-	-	1				1								
	PEO-16-CU	UIC SIGN	CUSTOM	REMOVE AND RELOCATE SIGN (SEE NOTE 1 & 3)	3706+37	22.89' LT	(SEE NOTE 3)		4.83	3.75	4.83	3.75	2												
	PEO-17-LP	NO PARKING/LOADING ZONE	R7-601-93	REMOVE	3706+38	20.89' RT	-	-	1.50	1.00	-	-	1	1.50											
	PEO-17A-LP	AUTHORIZED VEHICLES ONLY	R5-11	PROPOSED SIGN	-	-	3706+39	8.50' RT	-	-	2.50	2.00								5.00					
	PEO-18-ST	NO PARKING / TOW ZONE	R7-201-4	REMOVE	3706+52	20.85' LT	-	-	1.50	1.50	-	-	1				1								
	PEO-19-ST	NO PARKING / TOW ZONE	R7-201-4	REMOVE AND REPLACE SIGN	3706+79	20.17' LT	3706+82	21.50' LT	1.50	1.50	1.50	1.50	1				1			2.25		1			
	PEO-20-ST	NO PARKING / TOW ZONE / LEFT ARROW	R7-201-3	REMOVE AND REPLACE SIGN	3707+07	20.98' RT	3706+87	20.75' RT	1.50	1.50	1.50	1.50	1				1			2.25		1			
	PEO-21-ST	NO PARKING/LOADING ZONE	R7-601-91	REMOVE	3707+07	20.98' RT	-	-	1.50	1.00	-	-	1				1								
PEO-22-CU	UIC SIGN	CUSTOM	REMOVE AND RELOCATE SIGN (SEE NOTE 1 & 3)	3707+23	22.84' LT	(SEE NOTE 3)		4.75	2.75	4.75	2.75	2													
PEO-23-LP	STOP	R1-1-2	EXISTING SIGN TO REMAIN	3707+48	21.15' RT	-	-	3.00	3.00	-	-	1													
PEO-24-LP	STOP	R1-1-2	EXISTING SIGN TO REMAIN	3707+48	21.15' RT	-	-	3.00	3.00	-	-	1													
PEO-25-ST	UIC SIGN	CUSTOM	EXISTING SIGN TO REMAIN	3707+53	32.15' RT	-	-	4.75	2.75	-	-	2													
PEO-26-ST	NO OUTLET	W14-2	PROPOSED SIGN	-	-	3707+48	21.15' RT	-	-	3.00	3.00	1							9.00		1				
PEO-27-LP	NO OUTLET	W14-2	PROPOSED SIGN	-	-	3707+48	22.12' LT	-	-	3.00	3.00	1							9.00						
EB I-290	EB-01-BM	CONGRESS PARKWAY - 2 LEFT LANES	CUSTOM	PROPOSED SIGN	-	-	339+10	NOTE 4	-	-	16.00	5.50													
	EB-02-BM	I-90/94 WEST KENNEDY EXPY - WISCONSIN	CUSTOM	PROPOSED SIGN	-	-	339+12	NOTE 4	-	-	18.50	13.00													
	EB-03-BM	I-90/94 EAST RYAN EXPY - INDIANA	CUSTOM	PROPOSED SIGN	-	-	339+13	NOTE 4	-	-	16.50	13.00													
	EB-04-BM	I-90/94 EAST RYAN EXPY - INDIANA	CUSTOM	PROPOSED SIGN	-	-	339+13	NOTE 4	-	-	18.50	13.00													
	EB-05-BM	NO PANEL	-	-	345+53	11.70' LT	-	-	-	-	-	-													
	EB-06-BM	I-90/94 WEST KENNEDY EXPY - WISCONSIN	CUSTOM	REMOVE	345+53	5.60' RT	-	-	16.50	10.50	-	-				17									
	EB-07-BM	I-90/94 EAST RYAN EXPY - INDIANA	CUSTOM	REMOVE	345+53	24.00' RT	-	-	17.00	10.50	-	-				17									
	EB-08-BM	PEORIA STREET 900 W	CUSTOM	REMOVE AND REPLACE SIGN	345+53	39.46' RT	345+50	52.00' RT	6.00	2.00	7.00	2.50		12.00					17.50						
WB I-290	WB-01-BM	EXIT 28B - ASHLAND AVE/PAULINA ST 3/4 MILE	CUSTOM	REMOVE & RELOCATE	346+02	0.00' RT	-	-	19.00	9.50	-	-													
	WB-02A-BM	EXIT 28B	CUSTOM	PROPOSED SIGN	-	-	346+02	0.00' RT	-	-	11.50	2.50													
	WB-02B-BM	ASHLAND AVE 3/4 MILE	CUSTOM	PROPOSED SIGN	-	-	346+02	0.00' RT	-	-	17.00	6.00													
	WB-03A-BM	EXIT 29B	CUSTOM	REMOVE AND REPLACE SIGN	346+00	26.86' LT	346+02	55.26' LT	11.50	2.50	11.50	2.50													
	WB-03B-BM	MORGAN ST	CUSTOM	REMOVE AND REPLACE SIGN	346+00	26.86' LT	346+02	55.26' LT	17.50	5.00	17.50	5.00													
	WB-04-TS	PEORIA STREET 900 W	CUSTOM	REMOVE AND REPLACE SIGN	346+08	45.66' LT	346+02	66.16' LT	6.00	2.00	7.00	2.50							1		17.50			35	
	WB-05-LP	AMERICAN BYWAYS	D6-4	EXISTING SIGN TO REMAIN	346+75	49.11' LT	-	-	2.00	2.00	-	-													
TOTAL													6	12	88	4	7	1	28	35	1,031	3	35	35	

NOTES:

- SIGN PANEL TO BE REMOVED. THE CONTRACTOR SHALL NOTIFY THE UNIVERSITY OF ILLINOIS AT CHICAGO AT LEAST THREE WEEKS PRIOR TO REMOVAL OF THE SIGN PANEL. THE CONTRACTOR SHALL MOVE THE SIGN PANEL AND POSTS TO EITHER ONE OF THE DESIGNATED STORAGE LOCATIONS INDICATED IN THE GENERAL NOTES. THE REMOVAL OR TRANSPORTATION OF THESE ITEMS WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT BID PRICE. ANY ITEM THAT IS DAMAGED DURING REMOVAL OR TRANSPORT SHALL BE REPLACED AT THE CONTRACTOR'S OWN EXPENSE.
- SIGN PANEL TO BE REMOVED AND REPLACED BY THE CTA. CONTRACTOR SHALL NOTIFY THE CTA AT LEAST THREE WEEKS PRIOR TO BEGINNING CONSTRUCTION ACTIVITIES WHICH WOULD IMPACT THE SIGN PANEL.
- SIGN PANEL TO BE RELOCATED. THE SIGN PANEL SHALL BE RETRIEVED FROM THE DESIGNATED UNIVERSITY OF ILLINOIS AT CHICAGO STORAGE LOCATION AND PLACED IN THE FINAL LOCATION AS APPROVED BY THE RESIDENT ENGINEER. IF NO FINAL LOCATION HAS BEEN APPROVED, THE SIGN SHALL REMAIN IN STORAGE. THE TRANSPORTATION OR PLACEMENT OF THESE ITEMS WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE CONTRACT BID PRICE. ANY ITEM THAT IS DAMAGED DURING TRANSPORT OR PLACEMENT SHALL BE REPLACED AT THE CONTRACTOR'S OWN EXPENSE.
- THE OVERHEAD SIGN STRUCTURE - BRIDGE MOUNTED WILL BE PLACED IN CONTRACT 60W25. THE SIGN STRUCTURES WERE DESIGNED FOR THE OVERHEAD SIGNS THAT ARE REQUIRED AFTER I-290 MAINLINE IS EXPANDED TO SIX LANES. SIGNS PROPOSED IN THIS CONTRACT SHALL BE PLACED ON THE MORGAN STREET BRIDGE MOUNTS AS SHOWN IN THE OVERHEAD SIGN STRUCTURES SIGN LAYOUT DETAILS. SEE SHEETS 106 - 106A

FILE PATH = p:\388935-par\1\aeconom\line\local\PEORIA\PEORIA\Schedule-01.dgn



D:\60W29-sht-Sign-Schedule-01.dgn	DESIGNED - JDT	REVISED - 12/18/2013
USER NAME = BAW1tor1	DRAWN - JTR	REVISED -
PLOT SCALE = 20.0000' / in.	CHECKED - JMG	REVISED -
PLOT DATE = 12/19/2013	DATE - 10/30/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SIGNING PLAN SCHEDULE			
SCALE: NONE	SHEET 1	OF 4 SHEETS	STA. TO STA.

F.A.I. RTE. 90/94/290	SECTION 2013-011R	COUNTY COOK	TOTAL SHEETS 356	SHEET NO. 99
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60W29	

