

	STATION	OFFSET	LT/RT	ELEVATION	TOP OF CURB / GROUND ELEVATION BEHIND WALL (IF APPLICABLE)
A	3705+29.73	9.46'	RT	598.02	598.02
B	3705+29.73	0.00'	RT	598.16	N/A
C	3705+29.73	13.00'	LT	597.96	597.96
D	3705+29.73	27.72'	LT	593.45	597.74
E	3705+39.58	8.22'	RT	597.64	597.89
F	3705+39.73	13.00'	LT	597.64	597.89
G	3705+29.73	57.15'	RT	591.78	N/A
H	3705+49.13	53.10'	LT	594.27 *	N/A
I	3705+29.78	53.10'	LT	598.79 *	N/A
J	3705+46.36	32.98'	RT	593.48	N/A
K	3705+49.73	6.95'	RT	597.25	597.50
L	3705+49.73	5.00'	LT	597.43	N/A
M	3705+49.73	13.00'	LT	597.31	597.56
N	3705+53.43	24.58'	LT	593.06	596.57
O	3705+52.81	33.08'	RT	593.73 *	593.73
P	3705+54.72	28.46'	RT	593.76	594.51
Q	3705+56.22	24.85'	RT	593.78	594.53
R	3705+58.45	20.83'	RT	593.76	594.51
S	3705+61.85	17.17'	RT	593.74	594.49
T	3705+55.10	32.98'	LT	593.28	N/A
U	3705+74.70	12.50'	RT	593.69	594.44
V	3705+80.00	24.58'	LT	593.19	593.94
W	3705+81.31	3.00'	RT	596.01	596.26
X	3705+83.80	32.50'	RT	593.71 *	593.71 *
Y	3705+85.00	12.50'	RT	593.65	594.40
Z	3705+88.80	32.50'	RT	593.47	593.72 *
AA	3705+90.00	12.50'	RT	593.63	594.38
AB	3706+13.05	32.50'	RT	593.45	593.70 *
AC	3706+18.05	32.50'	RT	593.50	593.67 *
AD	3706+20.00	12.50'	RT	593.51	594.26
AE	3706+39.92	32.50'	RT	593.59	593.76 *
AF	3706+41.26	32.50'	RT	593.75	593.75
AG	3706+40.00	12.50'	RT	593.43	593.68
AH	3706+44.87	32.33'	RT	593.57	593.82
AI	3706+41.26	12.50'	RT	593.42	593.67
AJ	3706+46.62	3.00'	RT	593.66	593.91
AK	3706+49.03	21.40'	RT	593.19	N/A
AL	3706+51.48	6.83'	RT	593.57	593.82
AM	3706+51.35	9.41'	RT	593.38	593.55
AN	3706+61.25	0.00'	RT	593.35	593.52
AO	3706+63.56	25.27'	RT	593.29	593.46
AP	3706+67.41	33.00'	RT	593.58	593.75
AQ	3706+66.34	25.80'	RT	593.48	593.65
AR	3706+67.41	27.58'	RT	593.50	593.67
AS	3706+69.77	15.58'	RT	593.14	593.31
AT	3706+72.20	16.89'	RT	593.09	593.34
AU	3706+79.75	18.50'	RT	592.96	593.46
AV	3706+90.99	18.50'	RT	592.89	593.39
AW	3707+05.04	31.00'	RT	593.58 *	N/A
AX	3707+05.04	32.95'	RT	593.67 *	N/A
AY	3707+09.94	18.50'	RT	593.00	593.50
AZ	3706+85.99	18.50'	RT	592.86	593.36
BA	3707+17.20	31.07'	RT	593.66 *	N/A
BB	3707+17.20	18.50'	RT	593.04	593.54
BC	3707+23.20	18.50'	RT	593.02	593.09
BD	3707+23.20	27.07'	RT	594.03	N/A
BE	3707+23.20	31.07'	RT	593.68	N/A
BF	3707+21.00	18.50'	LT	592.82 *	593.15
BG	3707+30.17	18.50'	LT	592.79 *	593.14
BH	3707+38.20	18.50'	RT	592.96	593.03
BI	3707+38.20	27.07'	RT	593.67	N/A
BJ	3707+38.20	31.13'	RT	593.73 *	N/A
BK	3707+40.28	18.50'	RT	592.87	593.15
BL	3707+40.28	18.50'	LT	592.75 *	593.13
BM	3707+40.85	18.51'	RT	592.84	593.17
BN	3707+45.28	18.59'	RT	592.64 *	593.11 *
BO	3707+43.50	27.07'	RT	593.62 *	593.87 *
BP	3707+43.50	31.15'	RT	593.75 *	N/A
BQ	3707+45.28	18.51'	LT	592.73 *	593.08 *
BR	3707+45.28	32.95'	LT	593.34 *	N/A

* MATCH EXISTING

LEGEND

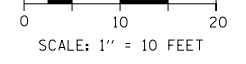
- PCC SIDEWALK, 5 INCH SAND CUSHION, 4 INCH, (CDOT)
- HMA SURF CSE, MIX "D", N70 1 3/4" HMA BIND CSE, IL-19, N70 VARIES 2 1/4" TO 5 1/4"

NOTES

1. SEE NOTE 35 ON THE GENERAL NOTES SHEET FOR INFORMATION REGARDING REMOVAL, STORAGE, AND RELOCATION OF BIKE RACKS.
2. STATIONS, OFFSETS, AND ELEVATIONS ON PEORIA STREET ARE GIVEN ALONG THE FLOW LINE.

FROM POINT	CURB HEIGHT	TO POINT	CURB HEIGHT
A	0"	E	3"
C	0"	F	3"
O	0"	P	9"
X	0"	Z	3"
AB	3"	AC	2"
AD	9"	AG	3"
AE	2"	AH	3"
AM	2"	AO	2"
AS	2"	AU	6"
BB	6"	BC	DEPRESSED CURB
BF	4"	BL	4.5"
BH	DEPRESSED CURB	BN	5.6" *
BL	4.5"	BQ	4.2"

* MATCH EXISTING



D:\60W29-Sht-Intersec-03.dgn	DESIGNED - JLV	REVISED - 12/18/2013
USER NAME = BAWtor t	DRAWN - KAL	REVISED -
PLOT SCALE = 20.0000' / in.	CHECKED - JMG	REVISED -
PLOT DATE = 12/19/2013	DATE - 10/30/2013	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND GRADING PLAN DETAILS
PEORIA STREET**

SCALE: 1"=10' SHEET 3 OF 5 SHEETS STA. 3705+22 TO STA. 3707+40

F.A.I. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
90/94/290	2013-01R	COOK	356	88
ILLINOIS FED. AID PROJECT			CONTRACT NO. 60W29	