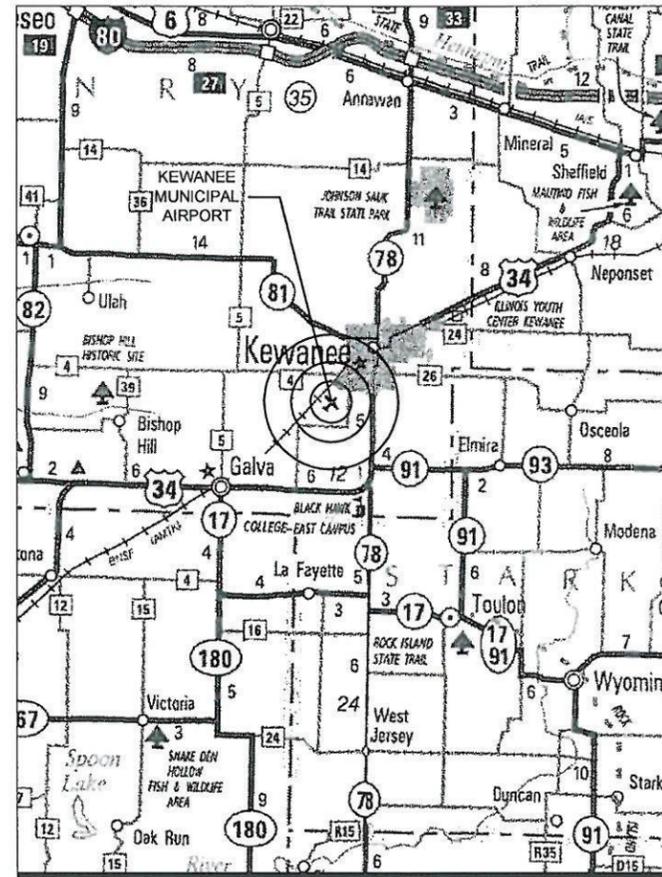


CONSTRUCTION PLANS

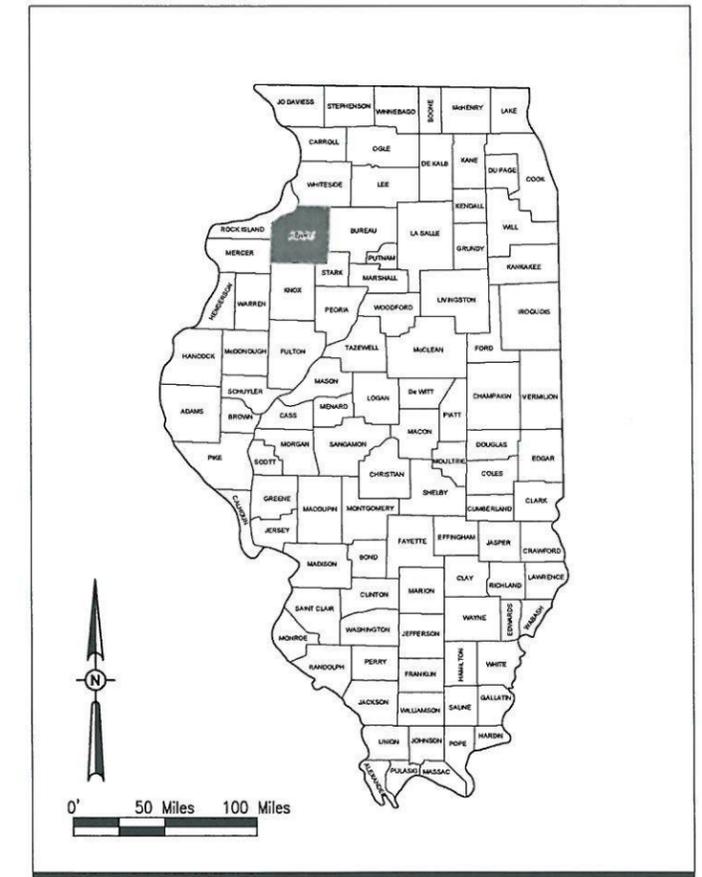
PERFORM LARGE CRACK REPAIR AND ASSOCIATED PAVEMENT RE-MARKING ON RUNWAY 9/27

KEWANEE MUNICIPAL AIRPORT (EZI) KEWANEE, HENRY COUNTY, ILLINOIS

SBG PROJECT NO. 3-17-SBGP-XX IDA PROJECT NO. EZI-4316



VICINITY MAP



LOCATION MAP

NOTICE TO CONTRACTORS AND BIDDERS

THESE CONSTRUCTION PLANS RELY UPON THE SPECIAL PROVISIONS AND THE SPECIFICATIONS TO PROVIDE FOR A COMPLETE DESCRIPTION OF THE WORK AND CONSTRUCTION REQUIREMENTS. THE PLANS SHALL ONLY BE USED IN COMBINATION WITH ALL CONTRACT DOCUMENTS.



SEPTEMBER 8, 2014



HANSON PROFESSIONAL SERVICES INC.
1525 S. Sixth St.
Springfield, Illinois 62703
Telephone: 217.788.2450
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Robert A. Waller 12/5/14
 ROBERT A. WALLER, P.E. Date
 Project Manager

KEWANEE MUNICIPAL AIRPORT
3761 Midland Road
Kewanee, Illinois 61443
Telephone: 309.583.8871
Fax:

Dale Lindstrom 12/5/14
 Dale Lindstrom Date
 Airport Manager

| No. | Issue/Description | Sheets Changed | Date | By |
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FILENAME:LONG

SUMMARY OF QUANTITIES

| ITEM NO. | DESCRIPTION | UNIT | TOTAL QUANTITIES | AS BUILT QUANTITIES |
|----------|---------------------------------|------|------------------|---------------------|
| AR201661 | CLEAN & SEAL BITUMINOUS CRACKS | L.F. | 4,950 | |
| AR401910 | REMOVE & REPLACE BIT. PAVEMENT | S.Y. | 796 | |
| AR620520 | PAVEMENT MARKING - WATERBORNE | S.F. | 302 | |
| AR620525 | PAVEMENT MARKING - BLACK BORDER | S.F. | 362 | |

INDEX TO SHEETS

| SHEET NO. | DESCRIPTION |
|-----------|--|
| 1 | COVER SHEET |
| 2 | SUMMARY OF QUANTITIES AND INDEX TO SHEETS |
| 3 | PROPOSED SAFETY PLAN |
| 4 | PROPOSED STAGING PLAN |
| 5 | TYPICAL SECTIONS, NOTES & DETAILS |
| 6 | PAVEMENT REPAIR & REMARKING STA. 218+00 TO STA. 227+00 |
| 7 | PAVEMENT REPAIR & REMARKING STA. 227+00 TO STA. 241+00 |
| 8 | PAVEMENT REPAIR & REMARKING STA. 241+00 TO STA. 252+00 |
| 9 | PAVEMENT REPAIR & REMARKING STA. 252+00 TO STA. 263+00 |

PAVEMENT
IMPROVEMENTS TO
RUNWAY 9/27
IDA No: EZI-4316

Contract No. KE013

| NO. | DATE | DESCRIPTION | | |
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| | | DES | DWN | REV |
| 1 | 12/23/14 | IDA REVIEW CAH CAH | | |

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DRAWN BY: BAK 06/02/2014
REVIEWED BY: CAH 06/16/2014

SHEET TITLE

SUMMARY OF
QUANTITIES AND
INDEX TO SHEETS

| NO. | DATE | DESCRIPTION | | |
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SHEET TITLE

PROPOSED SAFETY PLAN

J.U.L.I.E. INFORMATION

COUNTY: HENRY
CITY: KEWANEE
TOWNSHIP: WETHERFIELD
SECTION NO: 17
ADDRESS: KEWANEE MUNICIPAL AIRPORT
3769 MIDLAND ROAD
KEWANEE, ILLINOIS 61443

CRITICAL POINT DATA

- | | |
|---|---|
| C.P. 1 LATITUDE: 41° 12' 02.84" LONGITUDE: 89° 57' 32.34" ELEVATION: 846.8' M.S.L. | C.P. 3 LATITUDE: 41° 12' 14.18" LONGITUDE: 89° 57' 45.52" ELEVATION: 856.6' M.S.L. |
| C.P. 2 LATITUDE: 41° 12' 14.13" LONGITUDE: 89° 57' 42.20" ELEVATION: 857.6' M.S.L. | C.P. 4 LATITUDE: 41° 12' 10.24" LONGITUDE: 89° 57' 46.57" ELEVATION: 854.4' M.S.L. |
| C.P. 5 LATITUDE: 41° 12' 10.04" LONGITUDE: 89° 57' 32.98" ELEVATION: 851.8' M.S.L. | |

PROPOSED SAFETY PLAN

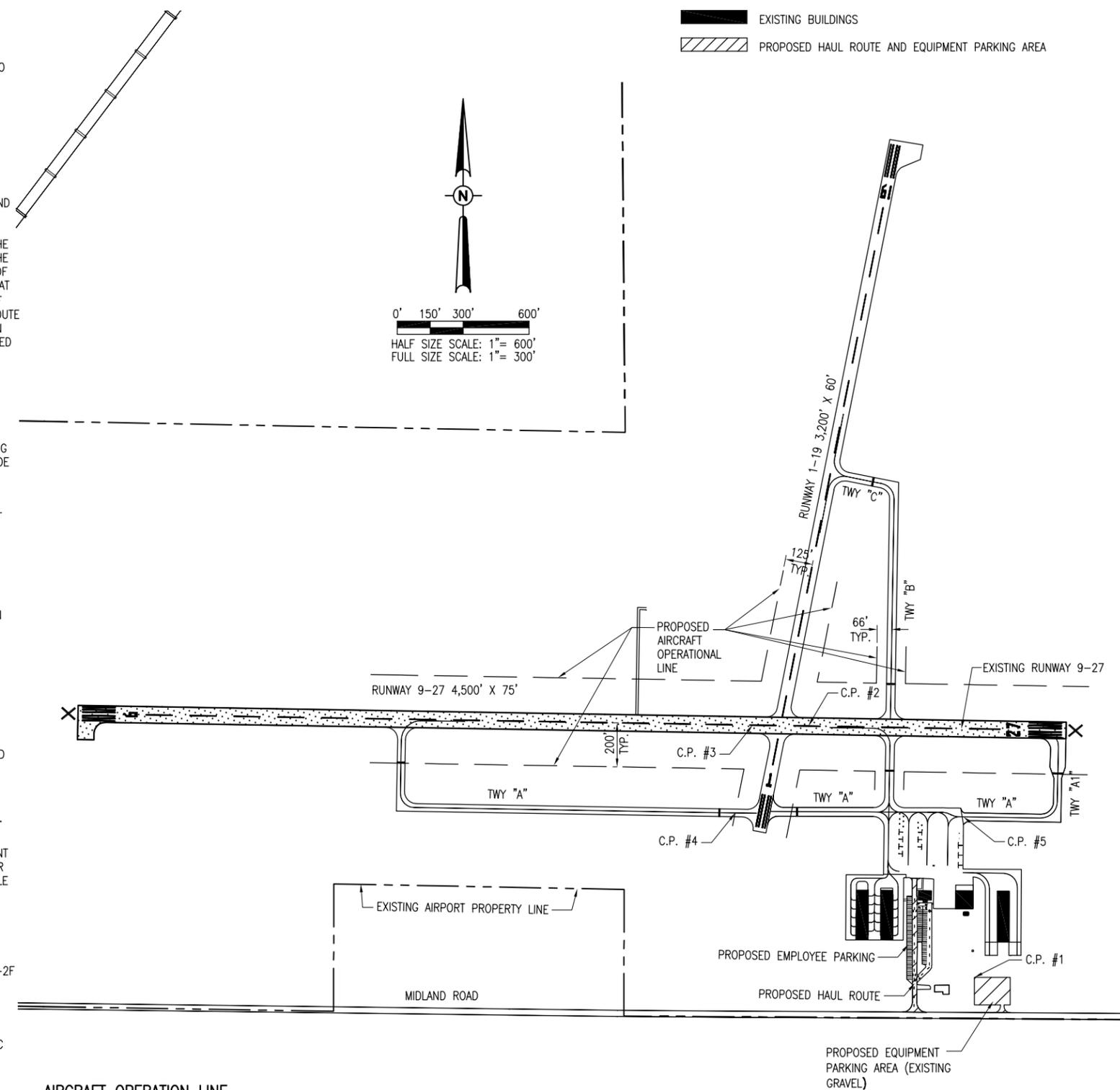
- GENERAL - THE KEWANEE MUNICIPAL AIRPORT IS COMPRISED OF TWO RUNWAYS. THE PROPOSED CONSTRUCTION WILL NECESSITATE CLOSING BOTH RUNWAY 9-27 AND 1-19 AND PORTIONS OF THE TAXIWAYS. RUNWAY 9-27 WILL BE CLOSED AT THE START OF THIS PROJECT AND WILL REMAIN CLOSED UNTIL THE COMPLETION OF THIS PROJECT. RUNWAY 1-19 WILL BE CLOSED WHEN CONSTRUCTION ACTIVITIES ARE WITHIN 125 FEET OF ITS CENTERLINE. TAXIWAY "B" WILL BE CLOSED WHEN CONSTRUCTION ACTIVITIES ARE WITHIN 66 FEET OF ITS CENTERLINE. THE CLOSURE OF RUNWAY 1-19 WILL CLOSE THE AIRPORT; THEREFORE, THE CONTRACTOR WILL BE ALLOWED ONE CALENDAR DAY TO COMPLETE THE REQUIRED WORK WITHIN THE STAGE 2 WORK AREA. ALL WORK INCLUDED IN OPENING AND CLOSING THE RUNWAY WILL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- IDENTIFICATION - WHEN THE CONTRACTOR'S VEHICLES AND EQUIPMENT ARE ON THE AIRPORT THEY SHALL BE PROPERLY MARKED WITH THREE (3') FOOT SQUARE CHECKERED FLAGS (INTERNATIONAL ORANGE AND WHITE).
- RADIO CONTROL - THE CONTRACTOR WILL BE REQUIRED TO BE IN TWO-WAY RADIO CONTACT (122.80 MHz.) WITH THE AIRPORT UNICOM. THIS WILL KEEP THE CONTRACTOR IN CONSTANT CONTACT WITH THE KEWANEE MUNICIPAL AIRPORT AND ENABLE THE AIRPORT TO IMMEDIATELY CONTACT THE CONTRACTOR IN CASE OF AN AERONAUTIC EMERGENCY THAT WOULD REQUIRE ACTION BY THE CONTRACTOR AND/OR HIS PERSONNEL.

RUNWAY AND TAXIWAY CLOSURE PROCEDURES

- THE AIRPORT MANAGER OR REPRESENTATIVE WILL ISSUE ALL APPLICABLE NOTAM'S TO OPEN AND CLOSE THE RUNWAYS AND TAXIWAYS.
- THE CONTRACTOR WILL OBTAIN APPROVAL FROM THE AIRPORT MANAGER OR REPRESENTATIVE PRIOR TO ENTERING ONTO ANY ACTIVE RUNWAY OR TAXIWAY.
- THE CONTRACTOR WILL CONTACT THE AIRPORT MANAGER OR REPRESENTATIVE FOR VERIFICATION OF RUNWAY OR TAXIWAY CLOSURE PRIOR TO ENTERING ONTO A RUNWAY OR TAXIWAY TO PLACE EQUIPMENT SUCH AS BARRICADES AND RUNWAY DESIGNATION CROSSES. THE AIRPORT MANAGER OR REPRESENTATIVE WILL DIRECT THE LOCATION OF SUCH EQUIPMENT.
- THE CONTRACTOR WILL CONTACT THE AIRPORT MANAGER OR REPRESENTATIVE OF WORK COMPLETION ON THE RUNWAY OR TAXIWAY PRIOR TO REMOVING ANY EQUIPMENT SUCH AS BARRICADES AND RUNWAY DESIGNATION CROSSES.
- THE AIRPORT MANAGER OR REPRESENTATIVE WILL INSPECT THE AREA PRIOR TO REMOVING EQUIPMENT AND CANCELLING NOTAM'S.
- THE AIRPORT SPONSOR IS ULTIMATELY RESPONSIBLE FOR ACTIVELY PREVENTING RUNWAY INCURSIONS.

LEGEND

- EXISTING IMPROVEMENTS
- PROPOSED IMPROVEMENTS
- EXISTING BUILDINGS
- PROPOSED HAUL ROUTE AND EQUIPMENT PARKING AREA



0' 150' 300' 600'

HALF SIZE SCALE: 1" = 600'
FULL SIZE SCALE: 1" = 300'

SCOPE OF WORK

- THIS PROJECT CONSISTS OF PAVEMENT IMPROVEMENTS TO RUNWAY 9-27. ASSOCIATED WORK ITEMS INCLUDE BITUMINOUS PAVEMENT REPAIRS, CRACK CLEANING AND SEALING AND PAVEMENT MARKING.

AIRPORT SECURITY NOTE

- AIRPORT SECURITY WILL BE MAINTAINED AT ALL TIMES. THE CONTRACTOR WILL CLOSE THE EXISTING GATE IN THE HAUL ROUTE WHENEVER HE IS NOT IN A CONTINUOUS HAULING OPERATION. DURING CONTINUOUS HAULING OPERATIONS HE WILL PROVIDE A PERSON TO WATCH THE GATE TO INSURE NO ONE UNAUTHORIZED USES THE OPEN GATE.

HEIGHT OF CONSTRUCTION EQUIPMENT

- THE MAXIMUM ANTICIPATED HEIGHT OF THE CONSTRUCTION EQUIPMENT WILL BE 25 FEET. THE TALLEST EQUIPMENT IS EXPECTED TO BE A DUMP TRUCK.

HAUL ROUTE AND VEHICLE PARKING

- THE CONTRACTOR WILL USE THE DESIGNATED HAUL ROUTE AND PARKING AREA AS SHOWN ON THIS SHEET. THE PROPOSED PARKING AREA WILL BE THE LIMITS OF THE EXISTING GRAVEL AREA. THE CONTRACTOR WILL BE REQUIRED TO MAINTAIN THE PROPOSED HAUL ROUTE AND PARKING AREA THROUGHOUT THE COURSE OF THE PROJECT. ANY AREAS DAMAGED OUTSIDE OF THESE AREAS WILL BE REPAIRED BY THE CONTRACTOR AND AT THE CONTRACTOR'S OWN EXPENSE. AT THE CONCLUSION OF THE PROJECT THE CONTRACTOR WILL RESTORE THE HAUL ROUTE AND PARKING AREA TO THEIR ORIGINAL STATE. RESTORATION OF THE HAUL ROUTE AND PARKING AREA WILL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

CONTRACTOR RESPONSIBILITIES

- THE CONTRACTOR'S EQUIPMENT PARKING AND STORAGE AREA WILL BE AS SHOWN ON THIS SHEET. THE CONTRACTOR'S EMPLOYEES WILL PARK THEIR VEHICLES IN THE AUTO PARKING LOT. ONLY CONTRACTOR VEHICLES WILL BE ALLOWED OUTSIDE THIS AREA.
- THE CONTRACTOR AND HIS EMPLOYEES WILL BE RESTRICTED TO THE WORK AREA AND ALL OTHER AREAS OF THE AIRPORT ARE "OFF LIMITS" TO THEM.
- THE CONTRACTOR SHALL KEEP ONE RUNWAY OPEN AT ALL TIMES AND MAINTAIN CONTINUOUS TAXIWAY ACCESS TO ALL HANGARS AND ADMINISTRATIVE AREAS. THE ONLY EXCEPTION WILL BE AS DESCRIBED IN THE PROPOSED SEQUENCE OF WORK STAGE 2 NOTES ON SHEET 4 OF THESE PLANS, WHEN BOTH RUNWAYS ARE TO BE CLOSED AT THE SAME TIME.
- ALL WORK PERFORMED SHALL BE DONE IN A ORDERLY AND EFFECTIVE MANNER TO MINIMIZE RUNWAY CLOSURE.

CERTIFIED PAYROLLS

- THE RESIDENT ENGINEER CANNOT FORWARD CONSTRUCTION REPORTS TO THE ILLINOIS DIVISION OF AERONAUTICS FOR PROCESSING UNTIL ALL CERTIFIED PAYROLLS FOR THE PERIOD HAVE BEEN RECEIVED.

MATERIAL CERTIFICATION

- MATERIALS TO BE INCORPORATED INTO THE PROJECT CANNOT BE USED WITHOUT PRIOR APPROVAL. ALL MATERIALS TO BE USED IN THE PROJECT MUST BE SUBMITTED TO THE RESIDENT ENGINEER FOR APPROVAL. USE OF MATERIALS WITHOUT PRIOR APPROVAL AND ULTIMATELY DETERMINED TO BE UNACCEPTABLE BY THE ILLINOIS DIVISION OF AERONAUTICS ARE SUBJECT TO REMOVAL AND/OR NON-PAYMENT.

NOTE

- ALL CONSTRUCTION/OPERATIONS ARE TO BE PERFORMED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR (AC) 150/5370-2F "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION" AND AC 150/5300-13A "AIRPORT DESIGN".
- ALL CONSTRUCTION EQUIPMENT ON THE AIRPORT SHALL BE MARKED, LIGHTED AND/OR FLAGGED IN ACCORDANCE WITH AC 150/5210-5D AND 70/7460-1.

EROSION CONTROL

- THIS PROJECT WILL DISTURB LESS THAN 1 ACRE OF LAND, THEREFORE NO N.P.D.E.S. PERMIT WILL BE REQUIRED.

AIRCRAFT OPERATION LINE

- THE AIRCRAFT OPERATION LINE WILL DESIGNATE HOW CLOSE TO A RUNWAY/TAXIWAY ANY CONTRACTOR PERSON/VEHICLE MAY GET WITHOUT CLOSING THE RUNWAY/TAXIWAY. AS THE CONTRACTOR ENTERS EACH PROPOSED STAGE OF CONSTRUCTION AS DESCRIBED ON THE FOLLOWING SHEET, HE WILL PLACE BARRICADES ACROSS THE INACTIVE (CLOSED) PAVEMENT WHERE HIS PERSONNEL WILL BE CROSSING THE ACTIVE RUNWAY/TAXIWAY. THE BARRICADES WILL BE PLACED 10' APART FOR A DISTANCE OF 30' (ON TAXIWAYS) AND 75' (ON RUNWAYS). THE CONTRACTOR WILL MAINTAIN THE AIRCRAFT OPERATION LINES FOR THE DURATION OF THIS PROJECT.

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PROPOSED SEQUENCE OF WORK

STAGE 1

- STAGE 1 CONSTRUCTION SHALL CONSIST OF ALL WORK LOCATED OUTSIDE OF 125' FROM RUNWAY 1-19 CENTERLINE AND 66' FROM TAXIWAY "B" CENTERLINE.
- AT THE START OF CONSTRUCTION ACTIVITIES THE CONTRACTOR SHALL CLOSE RUNWAY 9-27 IN ACCORDANCE WITH THE PROCEDURE SET FORTH ON THE PROPOSED SAFETY PLAN. THE ASSOCIATED RUNWAY LIGHTING CIRCUIT AND NAVAIDS WILL BE TURNED OFF.
- RUNWAY 9-27 WILL BE CLOSED FOR THE DURATION OF THIS PROJECT.
- ONLY AFTER THE CROSSES AND BARRICADES HAVE BEEN PLACED WILL THE CONTRACTOR BE ALLOWED TO MOVE ONTO RUNWAY 9-27 AND BEGIN WORKING.
- ONCE ALL THE CRACK CLEANING AND SEALING, AND PAVEMENT REPAIRS WITHIN THE STAGE 1 WORK AREA HAVE BEEN COMPLETED, THE CONTRACTOR WILL BE ALLOWED TO START WORK WITHIN THE STAGE 2 AREA.

STAGE 2

- STAGE 2 CONSTRUCTION SHALL CONSIST OF CLEANING AND SEALING CRACKS, REMOVE AND REPLACE BITUMINOUS PAVEMENT (114 S. Y.) AND PAVEMENT MARKING THAT ARE LOCATED WITHIN 125 FEET OF RUNWAY 1-19 CENTERLINE AND 66 FEET OF TAXIWAY "B" CENTERLINE.
- THE CONTRACTOR WILL PLACE CROSSES ON RUNWAY ENDS 1 AND 19 AND PLACE BARRICADES AS SHOWN ON THIS SHEET.
- SINCE THIS STAGE WILL CLOSE THE AIRPORT TO ALL AIRCRAFT ACTIVITY THE CONTRACTOR WILL HAVE ONE CALENDAR DAY TO COMPLETE ALL THE WORK WITHIN STAGE 2.
- THE CONTRACTOR MAY DELAY THE STAGE 1 MARKING UNTIL THE STAGE 2 CONSTRUCTION. HOWEVER, DUE TO THE ONE CALENDAR DAY CLOSURE LIMITATION, THE STAGE 2 MARKING MUST BE COMPLETED BY THE END OF THE DAY.
- UPON COMPLETION OF STAGE 2 CONSTRUCTION ACTIVITIES THE CONTRACTOR WILL RE-OPEN RUNWAY 1-19 FOLLOWING THE PROCEDURE SET FORTH ON SHEET NO. 3 AND THIS SHEET.
- IF UPON COMPLETION OF THE STAGE 2 CONSTRUCTION ALL THE PROPOSED PAVEMENT MARKING HAS BEEN COMPLETED, THEN THE CONTRACTOR WILL RE-OPEN RUNWAY 9-27. RE-OPENING RUNWAY 9-27 WILL FOLLOW THE SAME PROCEDURES SET FORTH ON SHEET NO. 3 AND THIS SHEET.

BARRICADES

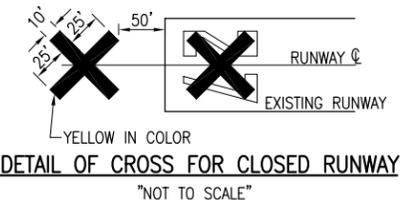
- IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO PLACE AND MAINTAIN BARRICADES AS SHOWN ON THIS SHEET AND AS DIRECTED BY THE AIRPORT MANAGER. THE BARRICADES WILL BE EQUIPPED WITH RED FLASHING OR RED STEADY-BURN LIGHTS AND 20" SQUARE ORANGE FLAGS. THE BARRICADES, THEIR MAINTENANCE, PLACEMENT AND REMOVAL WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

RUNWAY CLOSURES:

- WHENEVER A CONTRACTOR IS WORKING WITHIN 200' OF THE RUNWAY 9-27 CENTERLINE, WITHIN 125' OF THE RUNWAY 1-19 CENTERLINE, OR WITHIN 66' OF THE CENTERLINE OF AN ACTIVE TAXIWAY, THE RESPECTIVE RUNWAY/TAXIWAY WILL BE CLOSED. THE CONTRACTOR WILL PLACE CROSSES ON THE RUNWAY AS DETAILED ON THIS SHEET.

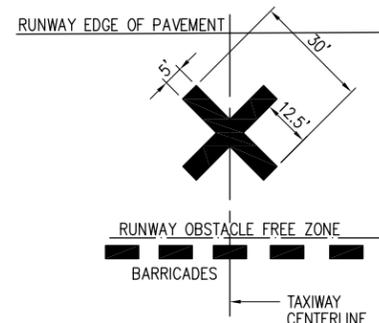
TAXIWAY LIGHTING CLOSURE NOTES

- WHEN STAGE 1 CONSTRUCTION ACTIVITIES BEGIN, SECTIONS OF TAXIWAY "A" WILL BE CLOSED (BARRICADED) TO AIRCRAFT MOVEMENT. AT THE SAME TIME TAXIWAY "B" WILL BE OPEN TO AIRCRAFT MOVEMENT. THOSE SECTIONS OF TAXIWAY "A" THAT ARE CLOSED TO AIRCRAFT MOVEMENT WILL NEED TO HAVE THEIR LIGHTING CIRCUIT DEACTIVATED.
- BOTH TAXIWAY "A" AND "B" ARE ON THE SAME LIGHTING CIRCUIT. THE CONTRACTOR WILL BE ALLOWED TO DEACTIVATE SECTIONS OF THE TAXIWAY LIGHTING CIRCUIT BY EITHER/OR OF THE FOLLOWING METHODS:
 - THE CONTRACTOR WILL BE ALLOWED TO PLACE JUMPER CABLES ACROSS THE CLOSED TAXIWAY AT THE LOCATION OF THE PROPOSED BARRICADES. THE JUMPER CABLES WILL BE PROTECTED IN ACCORDANCE WITH AC 150/5370-2F "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION".
 - THE CONTRACTOR WILL "BAG" THE EXISTING TAXIWAY LIGHTS THAT ARE LOCATED ON THE SECTION OF TAXIWAY "A" THAT IS BEING CLOSED. THE "BAG" WILL NOT ALLOW ANY LIGHT TO EMIT FROM THE TAXIWAY LIGHT. ANY TAXI GUIDANCE SIGNS LOCATED WITHIN THE CLOSED AREA WILL ALSO BE "BAGGED".
- DURING STAGE 2 CONSTRUCTION ACTIVITIES, THE TAXIWAY REGULATOR WILL BE TURNED OFF DURING THE CONSTRUCTION DAY AND RE-ACTIVATED AT THE END OF THE CONSTRUCTION DAY.
- ONCE THE PROJECT HAS BEEN COMPLETED WHICHEVER METHOD WAS USED WILL BE REMOVED AND THE LIGHTING CIRCUIT PLACED BACK INTO OPERATION.
- THE JUMPERING OF THE LIGHTING CIRCUIT OR THE "BAGGING" OF TAXIWAY LIGHTS AND SIGNS WILL BE CONSIDERED AS AN INCIDENTAL ITEM TO THE PROPOSED PROJECT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.



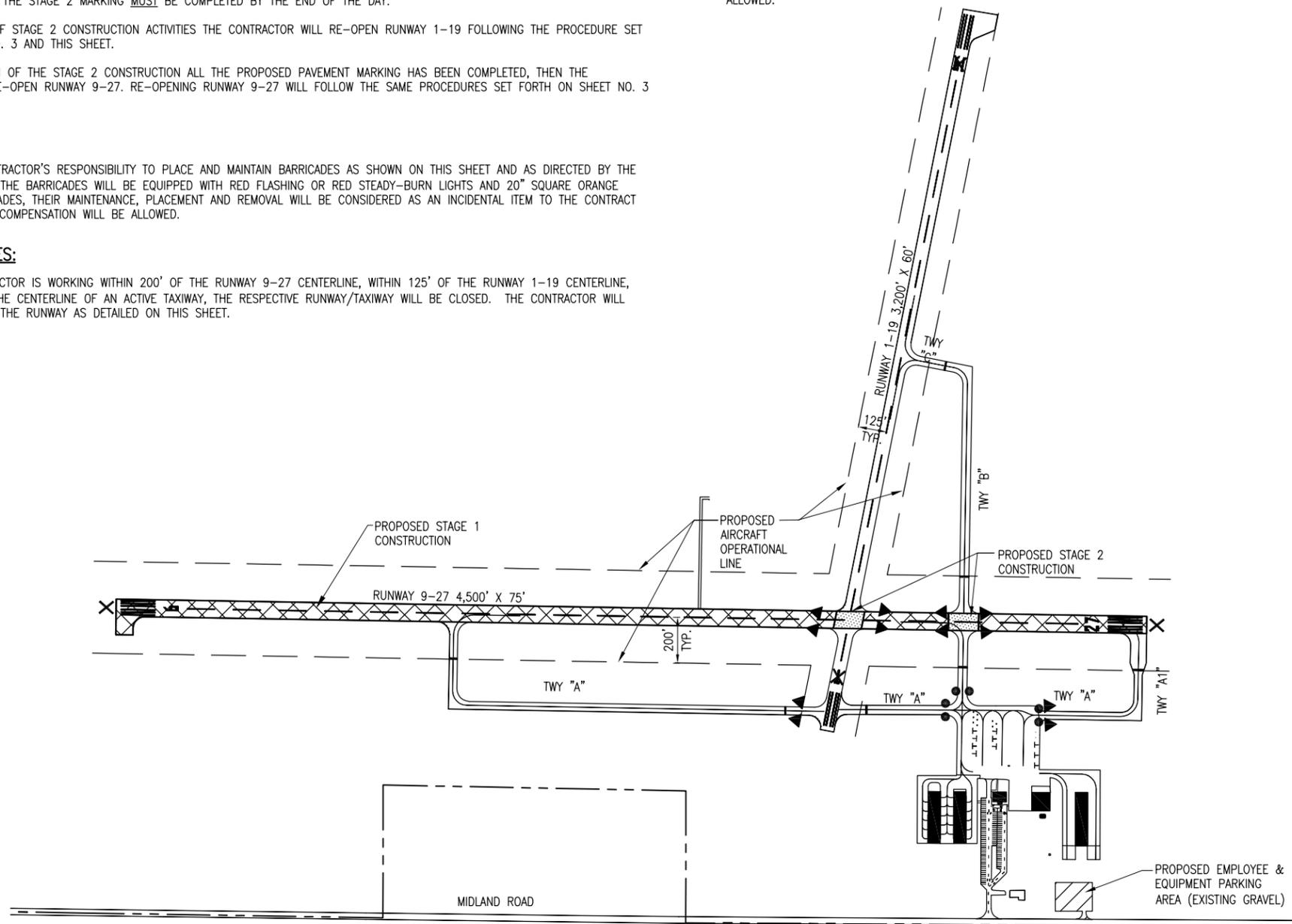
NOTE:

- COST OF CONSTRUCTING, PLACING, MAINTAINING AND REMOVING CROSSES WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED. THE CROSSES WILL BE YELLOW IN COLOR AND SHALL BE MADE OF A SUITABLE MATERIAL AS APPROVED BY THE AIRPORT MANAGER. THE CROSSES WILL BE PLACED AS SHOWN IN THE ABOVE DETAIL AND SECURED IN A MANNER APPROVED BY THE AIRPORT MANAGER. THE PROPOSED CROSSES WILL BE PLACED EACH DAY THE RUNWAY IS CLOSED AND REMOVED WHEN THE RUNWAY IS RE-OPENED. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE PLACEMENT AND REMOVAL OF THE CROSSES. NO ADDITIONAL COMPENSATION WILL BE ALLOWED.



- TEMPORARY "CLOSED TAXIWAY" MARKINGS SHALL BE "AVIATION YELLOW"
- TEMPORARY "CLOSED TAXIWAY" MARKINGS SHALL BE CONSTRUCTED USING TEMPORARY PAINT SUCH AS SEYMOUR TEMPORARY MARKER, PART NO. 20-636, OR AN EQUIVALENT PAINT EASILY REMOVED WITH WATER WITHOUT DEFACING THE PAVEMENT.
- TEMPORARY "CLOSED TAXIWAY" MARKINGS SHALL BE PLACED WHEN THE RUNWAY IS OPEN AND THE TAXIWAY IS CLOSED MORE THAN 72 HOURS. THE "CLOSED TAXIWAY" MARKINGS SHALL BE PLACED AS SHOWN ON THIS SHEET. IT IS ANTICIPATED THAT ONLY TAXIWAY "A" AT THE WEST SIDE OF THE INTERSECTION WITH RUNWAY END 1 WILL REQUIRE A TEMPORARY "CLOSED TAXIWAY" MARKING.
- THE PROPOSED CROSS WILL BE REMAIN IN PLACE EACH DAY THE TAXIWAY IS CLOSED AND REMOVED WHEN THE TAXIWAY IS RE-OPENED. THE CONTRACTOR WILL BE RESPONSIBLE FOR THE PLACEMENT AND REMOVAL OF THE CROSSES. "CLOSED TAXIWAY" MARKINGS SHALL NOT BE A PAY ITEM AND SHALL BE INCIDENTAL TO OTHER CONTRACT BID ITEMS.

TEMPORARY TAXIWAY CLOSURE CROSS DETAIL
NOT TO SCALE



LEGEND

- EXISTING IMPROVEMENTS
- EXISTING BUILDINGS
- PROPOSED EQUIPMENT PARKING AREA
- PROPOSED BARRICADES STAGE 1
- PROPOSED BARRICADES STAGE 2
- PROPOSED AREA STAGE 1
- PROPOSED AREA STAGE 2

PAVEMENT IMPROVEMENTS TO RUNWAY 9/27
IDA No: EZI-4316

Contract No. KE013

| NO. | DATE | DESCRIPTION | | |
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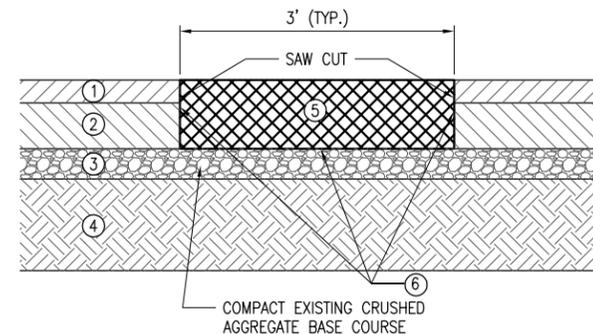
PROPOSED STAGING PLAN

REMOVE & REPLACE BITUMINOUS PAVEMENT

1. THE AREA DESIGNATED AS [REDACTED] ON THE CONSTRUCTION SHEETS WITHIN THESE PLANS WILL HAVE THE EXISTING PAVEMENT REMOVED (FULL DEPTH) AS SHOWN ON SHEETS 6 THROUGH 9.
2. WHERE THE PROPOSED REMOVAL AREA ABUTS THE EXISTING PAVEMENT, THE PAVEMENT WILL BE SAWED. THE SAWING WILL BE CONSIDERED AS PART OF THE PROPOSED PAVEMENT REMOVAL AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
3. THE BITUMINOUS SURFACE COURSE SHALL BE AN APPROVED ITEM 401 MIX, APPROVAL WILL BE BY THE DIVISION OF AERONAUTICS (IDA). AN AVERAGE OF TWO (2) NUCLEAR DENSITY TESTS PER 100 S.Y. PER LIFT WILL BE REQUIRED FOR ACCEPTANCE TESTING. TO BE ACCEPTABLE THE AVERAGE OF THESE TWO (2) TESTS MUST BE 90% OR BETTER FOR THE FIRST LIFT AND 93% OR BETTER FOR THE EACH OF NEXT TWO LIFTS. THE FINAL LIFT OF SURFACE COURSE SHALL BE INSTALLED FLUSH WITH THE EXISTING ADJACENT PAVEMENT.
4. EACH REMOVAL AND REPLACEMENT AREA WILL BE TESTED WITH A STRAIGHT EDGE TO DETERMINE IF THE AREA IS FLUSH WITH THE ADJACENT PAVEMENT. ALL IRREGULARITIES WILL BE CORRECTED BY THE CONTRACTOR AS AN INCIDENTAL ITEM TO THIS WORK.
5. THE BITUMINOUS SURFACE COURSE WILL BE INSTALLED AS PER THE SPECIFICATIONS EXCEPT AS STATED IN THESE PLANS. THE REMOVAL AND DISPOSAL OF THE EXISTING PAVEMENT; PROVIDING AND INSTALLING THESE MATERIALS TO GRADE IS CONSIDERED PART OF THIS PAY ITEM AND NO OTHER COMPENSATION WILL BE ALLOWED.
5. THE PAVEMENT CONDITIONS AND CRACKS WERE IDENTIFIED DURING A SURVEY IN THE SPRING 2014.
6. THE REMOVAL AND REPLACEMENT OF BITUMINOUS PAVEMENT WILL BE PAID FOR UNDER ITEM:
AR401910 "REMOVE & REPLACE BIT. PAVEMENT"---- PER SQ. YDS.

CLEAN & SEAL BITUMINOUS CRACKS

1. ALL CRACKS DESIGNATED BY THE RESIDENT ENGINEER FOR REPAIR WILL BE COMPLETED AS STATED IN THE SPECIAL PROVISIONS.
2. THE EXACT AMOUNT OF CRACKS TO BE CLEANED AND SEALED WILL BE THE NUMBER OF LINEAR FEET OF CRACKS MARKED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.
3. THE PAVEMENT CONDITIONS AND CRACKS WERE IDENTIFIED AND LOCATED AS SHOWN DURING A SURVEY IN THE SPRING OF 2014.
4. THE BITUMINOUS CRACK CLEANING AND SEALING WILL BE PAID FOR UNDER ITEM:
AR201661 - CLEAN & SEAL BITUMINOUS CRACKS - PER L.F.



REMOVE & REPLACE BIT. PAVEMENT

TYPICAL SECTION STA. 257+00 TO STA. 263+00 (END)

- ① EXISTING BIT. SURFACE COURSE (3" AVG.)
- ② EXISTING BIT. PAVEMENT (6" AVG.)
- ③ EXISTING CRUSHED AGGREGATE BASE COURSE (4" AVG.)
- ④ EXISTING LIME MODIFIED SUBGRADE (12" AVG.)
- ⑤ PROPOSED BITUMINOUS SURFACE COURSE (9" DEPTH); PLACE IN 3" MAXIMUM LIFT DEPTH
- ⑥ PROPOSED BITUMINOUS TACK COAT (603)

PAVEMENT
IMPROVEMENTS TO
RUNWAY 9/27
IDA No: EZI-4316

Contract No. KE013

| 1 | 12/23/14 | IDA REVIEW CAH CAH | | |
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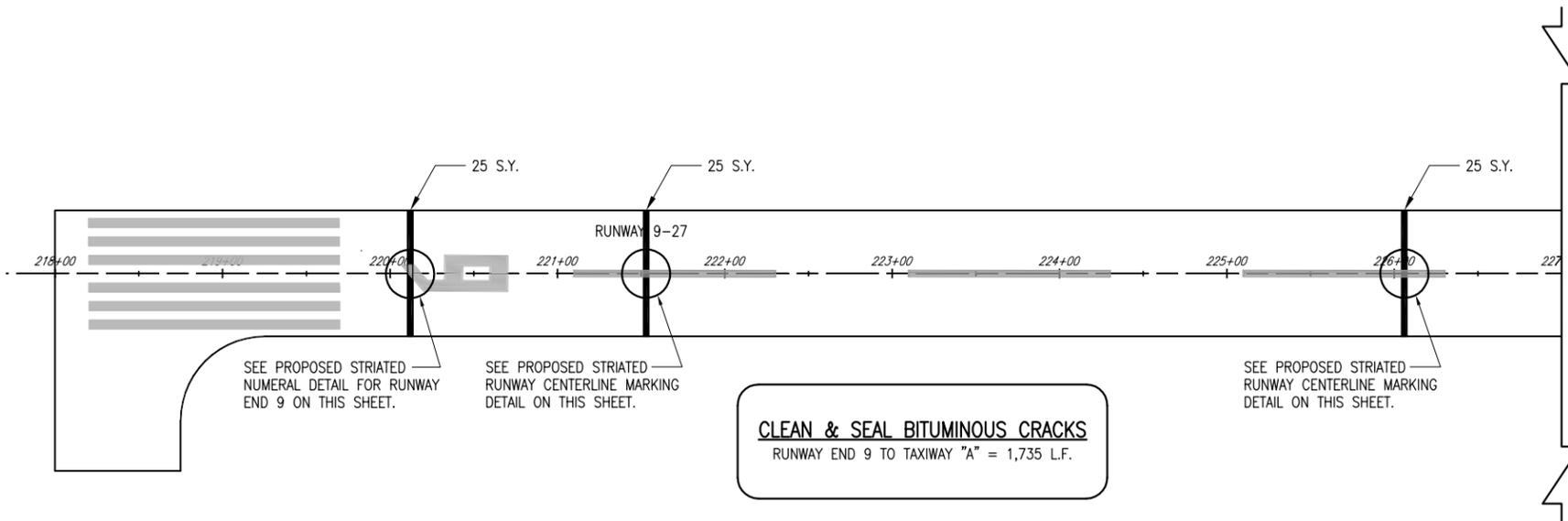
SHEET TITLE

TYPICAL SECTIONS,
NOTES & DETAILS

620-PAVEMENT MARKING-WATERBORNE NOTES

1. THE PAVEMENT MARKING-WATERBORNE (620) SHALL BE PLACED IN ACCORDANCE WITH ITEM 620 "PAVEMENT MARKING" AS STATED IN THE STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS, ADOPTED APRIL 1, 2012.
2. THIS ITEM SHALL CONSIST OF RUNWAY MARKING & TAXIWAY CENTERLINE IN ACCORDANCE WITH THESE SPECIFICATIONS AND AT THE LOCATIONS SHOWN ON THE CONSTRUCTION PLANS. ALL RUNWAY MARKING WILL BE STRIATED AND WHITE IN COLOR. ALL STRIATIONS WILL BE 6 INCHES IN WIDTH, SPACED 6 INCHES FROM EDGE TO EDGE, AS SHOWN IN THE STRIATED MARKING DETAIL ON THIS SHEET. THE EXCEPTION TO THIS IS CENTERLINE MARKINGS, WHICH SHALL BE CONSTRUCTED AS DETAILED ON THIS SHEET.
3. THE TAXIWAY CENTERLINE STRIPE WILL BE SOLID AND YELLOW IN COLOR.
3. THE PROPOSED PAVEMENT MARKING WILL BE APPLIED IN TWO APPLICATIONS.
4. ALL PROPOSED WHITE AND YELLOW MARKING WILL BE APPLIED WITH A REFLECTIVE MEDIA IN ACCORDANCE WITH THE SPECIFICATIONS. THE REFLECTIVE MEDIA SHALL BE REQUIRED ONLY ON THE SECOND APPLICATION OF WHITE AND YELLOW MARKINGS.
5. REFLECTIVE MEDIA IS NOT REQUIRED IN THE BLACK PAINT.
6. ANY MATERIAL DELIVERED THAT FAILS TO MEET THE SPECIFICATIONS SHALL BE DISPOSED OF BY THE VENDOR AND IMMEDIATELY REPLACED WITH ACCEPTABLE MATERIAL ENTIRELY AT THE VENDOR'S EXPENSE, INCLUDING HANDLING AND TRANSPORTATION CHARGES.
7. ALL PROPOSED MARKING WILL BE COMPLETED IN ACCORDANCE WITH THE DETAILS SHOWN ON THE CONSTRUCTION PLANS.
8. CUT-OFF SHEETS WILL BE REQUIRED TO INSURE STRAIGHT EDGES.
9. THE PROPOSED MARKING WILL BE PAID FOR UNDER ITEM:
WHITE AND YELLOW
AR620520 PAVEMENT MARKING-WATERBORNE ____ PER S.F.

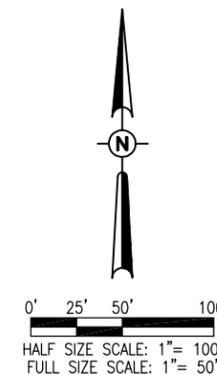
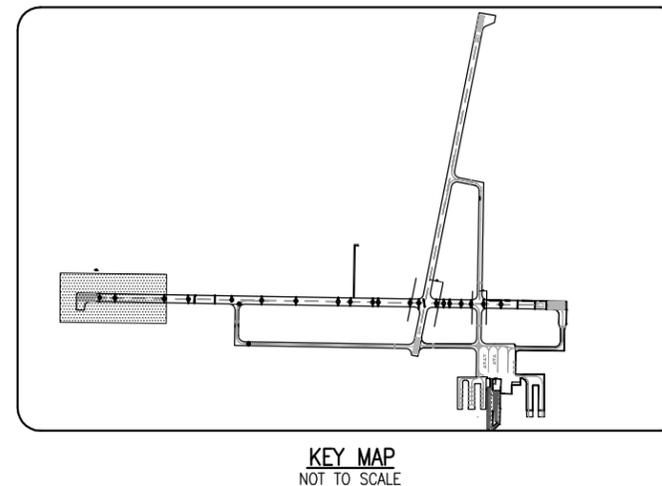
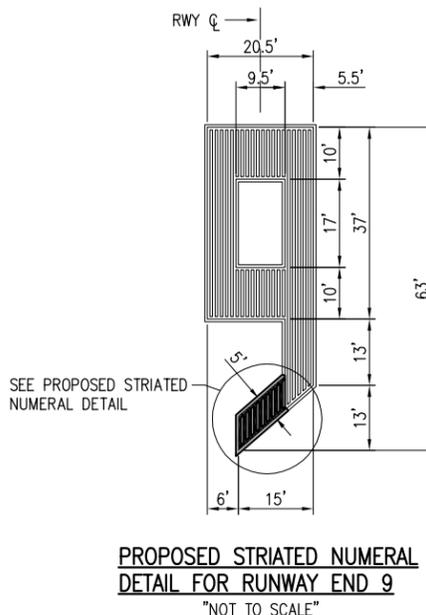
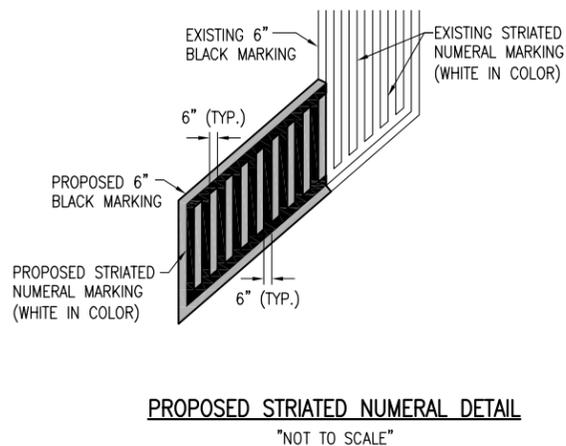
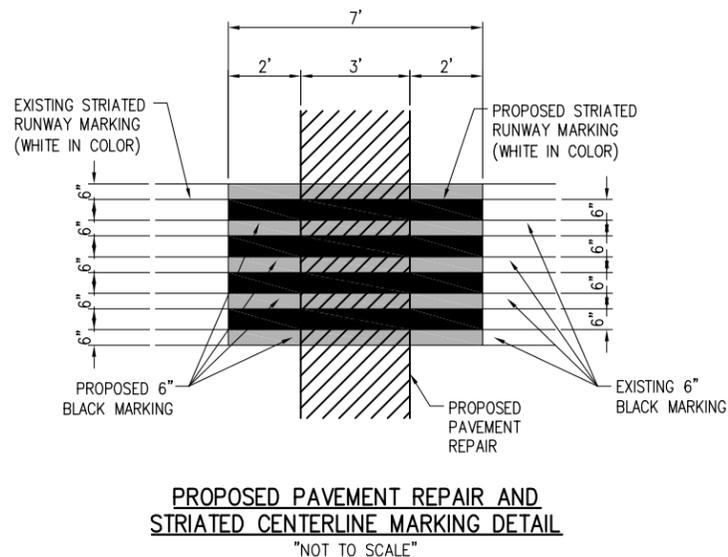
BLACK
AR620525 PAVEMENT MARKING-BLACK BORDER ____ PER S.F.



| MARKING QUANTITIES | | | |
|--|-----------|--------------|-------------------|
| DESCRIPTION | UNIT AREA | NO. REQUIRED | TOTAL AREA (S.F.) |
| RUNWAY 9-27 CENTERLINE STRIPE (STRIATED) | 14 | 16 | 224 |
| NUMERAL 9 (STRIATED) | 36 | 1 | 36 |
| TOTAL WHITE: | | | 260 |
| TAXIWAY CENTERLINE | 7 | 6 | 42 |
| TOTAL YELLOW: | | | 42 |
| RUNWAY 9-27 CENTERLINE STRIPE (STRIATED) | 17.5 | 16 | 280 |
| NUMERAL 9 (STRIATED) | 40 | 1 | 40 |
| TAXIWAY CENTERLINE STRIPES | 7 | 6 | 42 |
| TOTAL BLACK: | | | 362 |
| TOTAL MARKING: | | | 664 |

NOTE:
THE QUANTITY PROVIDED IS BASED ON THE ACTUAL AREA OF MARKING TO BE VISIBLE UPON COMPLETION.
TWO COATS OF PAINT ARE REQUIRED FOR ALL MARKING.

MARKING LEGEND:



PAVEMENT IMPROVEMENTS TO RUNWAY 9/27
IDA No: EZI-4316

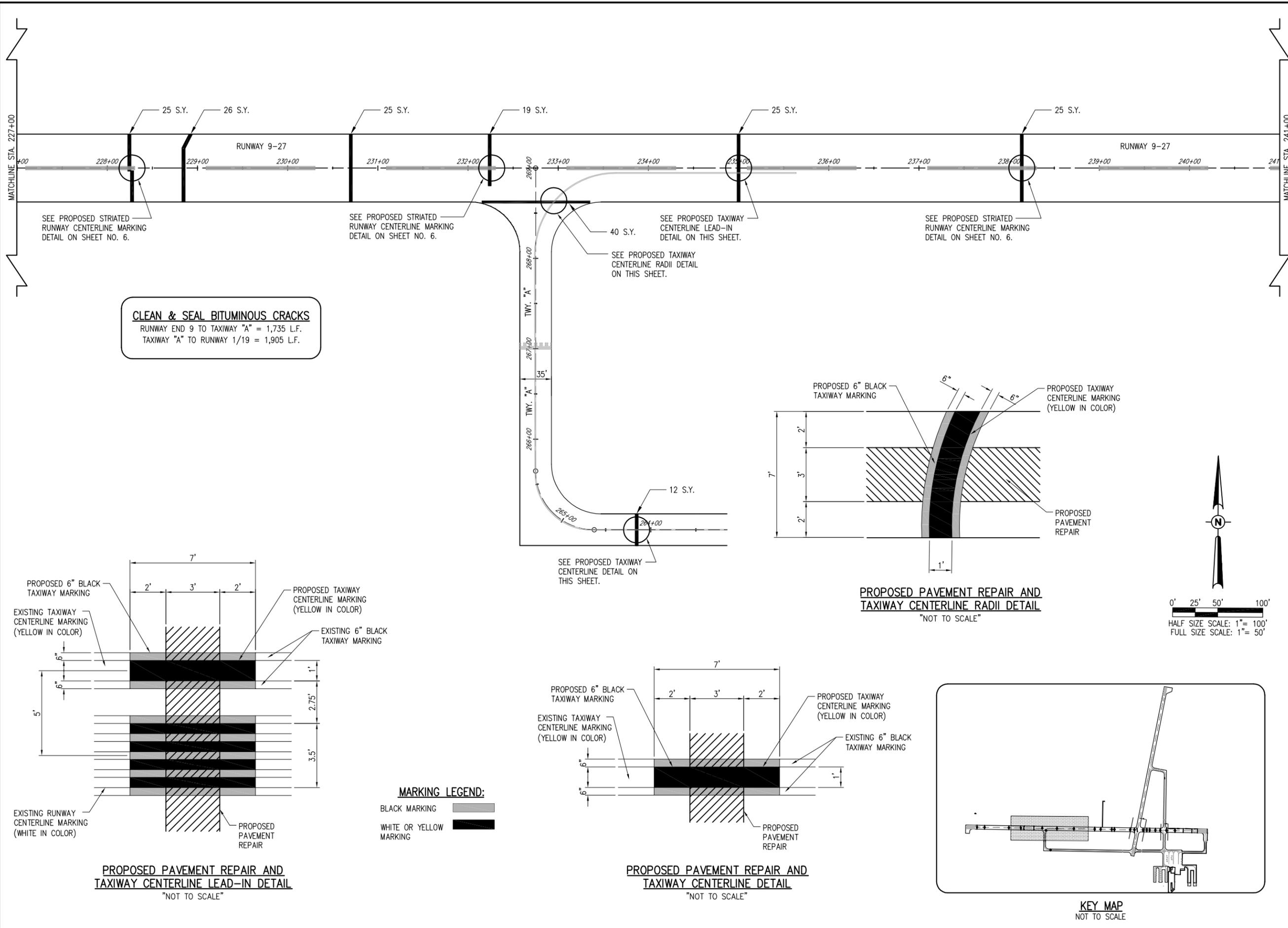
Contract No. KE013

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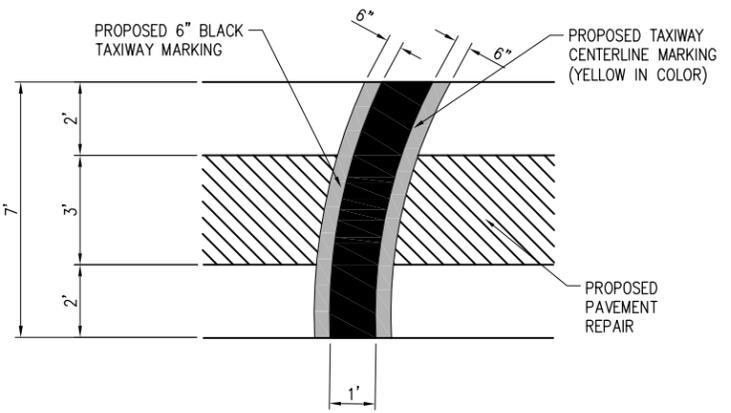
ISSUE: September 8, 2014
PROJECT NO: 14A0030D
CAD FILE: C-121-CON.DWG
DESIGN BY: BKB 06/02/2014
DRAWN BY: BAK 06/02/2014
REVIEWED BY: CAH 06/16/2014

SHEET TITLE

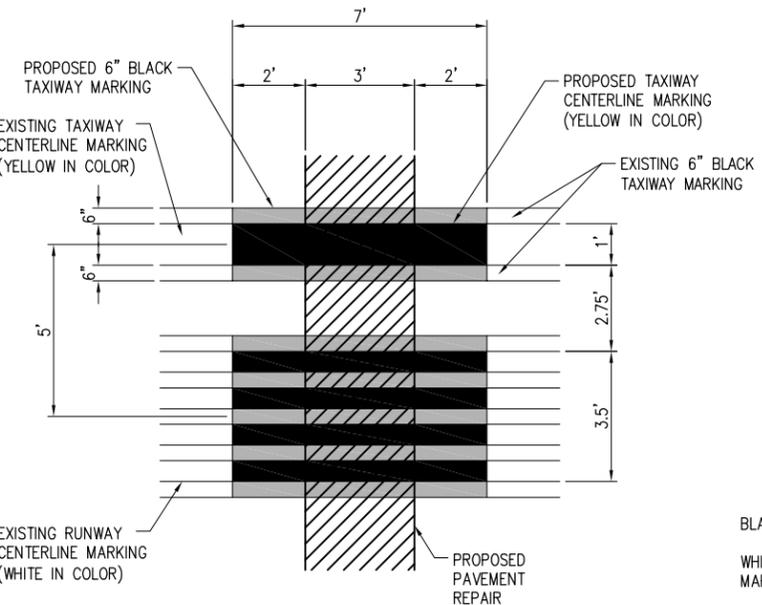
PAVEMENT REPAIR & REMARKING STA. 218+00 TO STA. 227+00



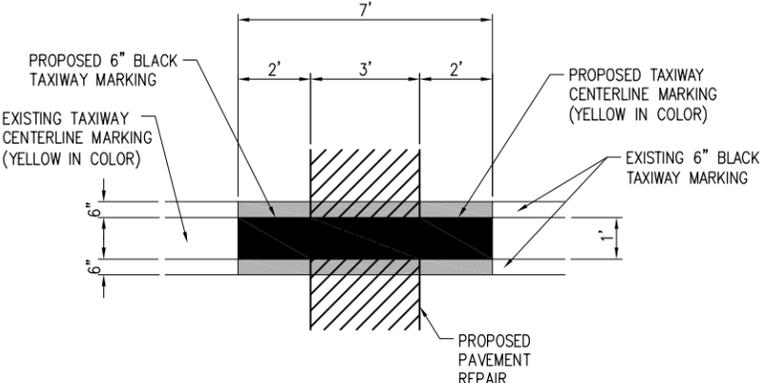
CLEAN & SEAL BITUMINOUS CRACKS
RUNWAY END 9 TO TAXIWAY "A" = 1,735 L.F.
TAXIWAY "A" TO RUNWAY 1/19 = 1,905 L.F.



PROPOSED PAVEMENT REPAIR AND TAXIWAY CENTERLINE RADII DETAIL
"NOT TO SCALE"

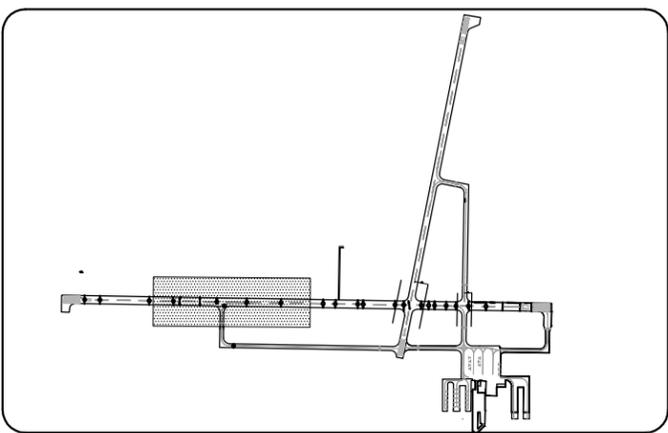


PROPOSED PAVEMENT REPAIR AND TAXIWAY CENTERLINE LEAD-IN DETAIL
"NOT TO SCALE"



PROPOSED PAVEMENT REPAIR AND TAXIWAY CENTERLINE DETAIL
"NOT TO SCALE"

MARKING LEGEND:
BLACK MARKING [Symbol]
WHITE OR YELLOW MARKING [Symbol]



KEY MAP
NOT TO SCALE

PAVEMENT IMPROVEMENTS TO RUNWAY 9/27
IDA No: EZI-4316

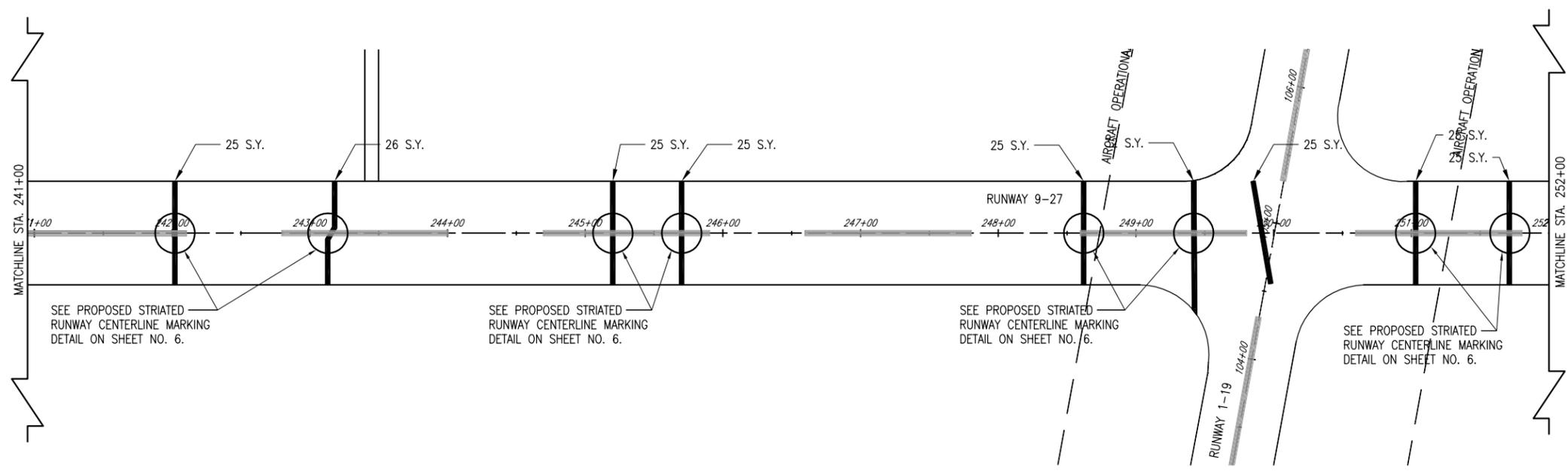
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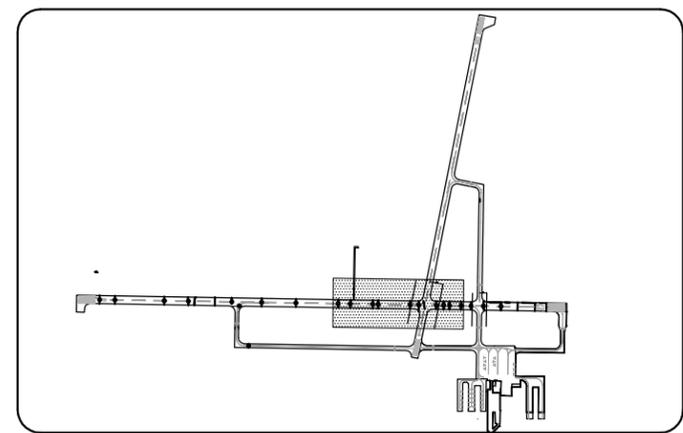
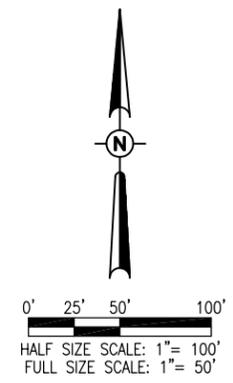
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SHEET TITLE

PAVEMENT REPAIR & REMARKING STA. 227+00 TO STA. 241+00



CLEAN & SEAL BITUMINOUS CRACKS
TAXIWAY "A" TO RUNWAY 1/19 = 1,905 L.F.
RUNWAY 1/19 TO TAXIWAY "B" = 744 L.F.



KEY MAP
NOT TO SCALE

PAVEMENT IMPROVEMENTS TO RUNWAY 9/27
IDA No: EZI-4316

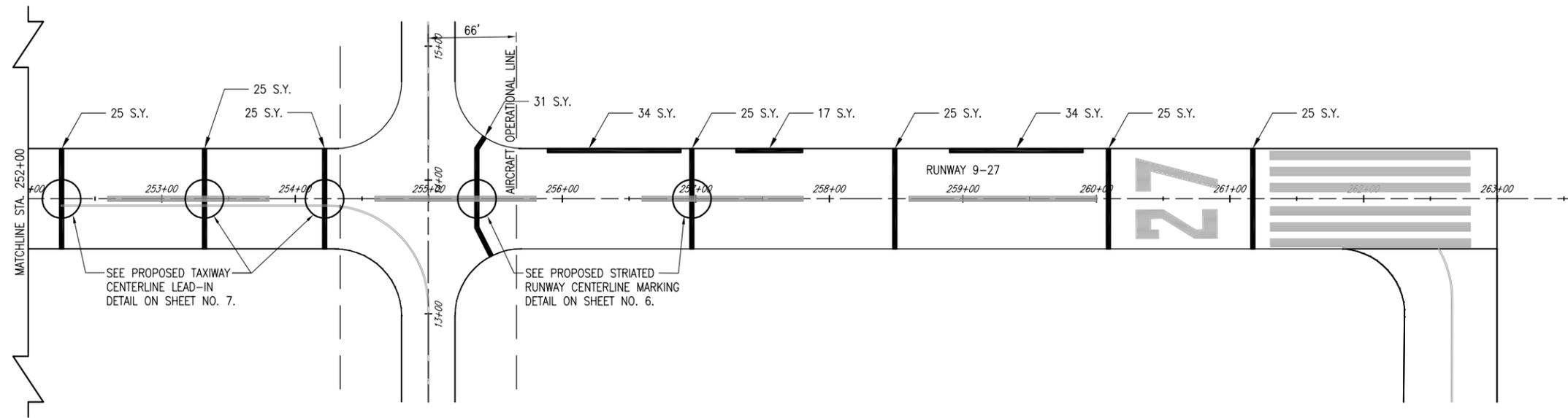
Contract No. KE013

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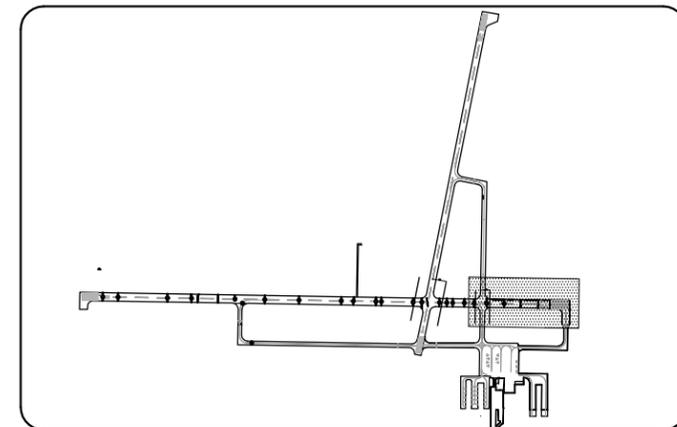
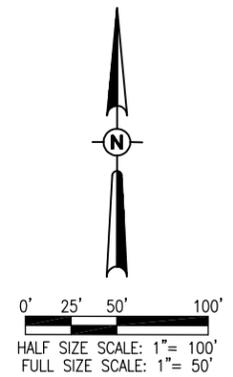
ISSUE: September 8, 2014
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DRAWN BY: BAK 06/02/2014
REVIEWED BY: CAH 06/16/2014

SHEET TITLE

PAVEMENT REPAIR & REMARKING
STA. 241+00 TO
STA. 252+00



CLEAN & SEAL BITUMINOUS CRACKS
 RUNWAY 1/19 TO TAXIWAY "B" = 744 L.F.
 TAXIWAY "B" TO RUNWAY END 27 = 566 L.F.



PAVEMENT
IMPROVEMENTS TO
RUNWAY 9/27
IDA No: EZI-4316

Contract No. KE013

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SHEET TITLE

PAVEMENT REPAIR &
REMARKING
STA. 252+00 TO
STA. 263+00