

CONSTRUCTION PLANS FOR JACKSONVILLE MUNICIPAL AIRPORT

JACKSONVILLE, MORGAN COUNTY, ILLINOIS

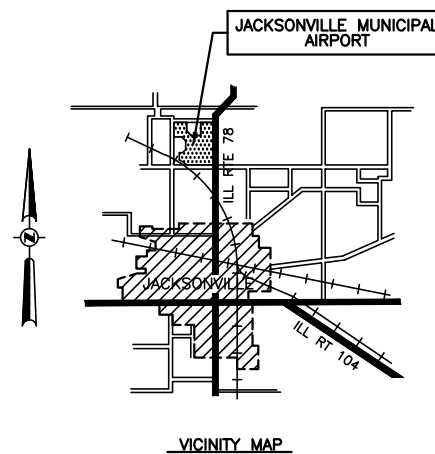
REHABILITATE APRON AREA PAVEMENT

ILLINOIS PROJECT NO. IJX-4375
SBG PROJECT NO. 3-17-SBGP-XX

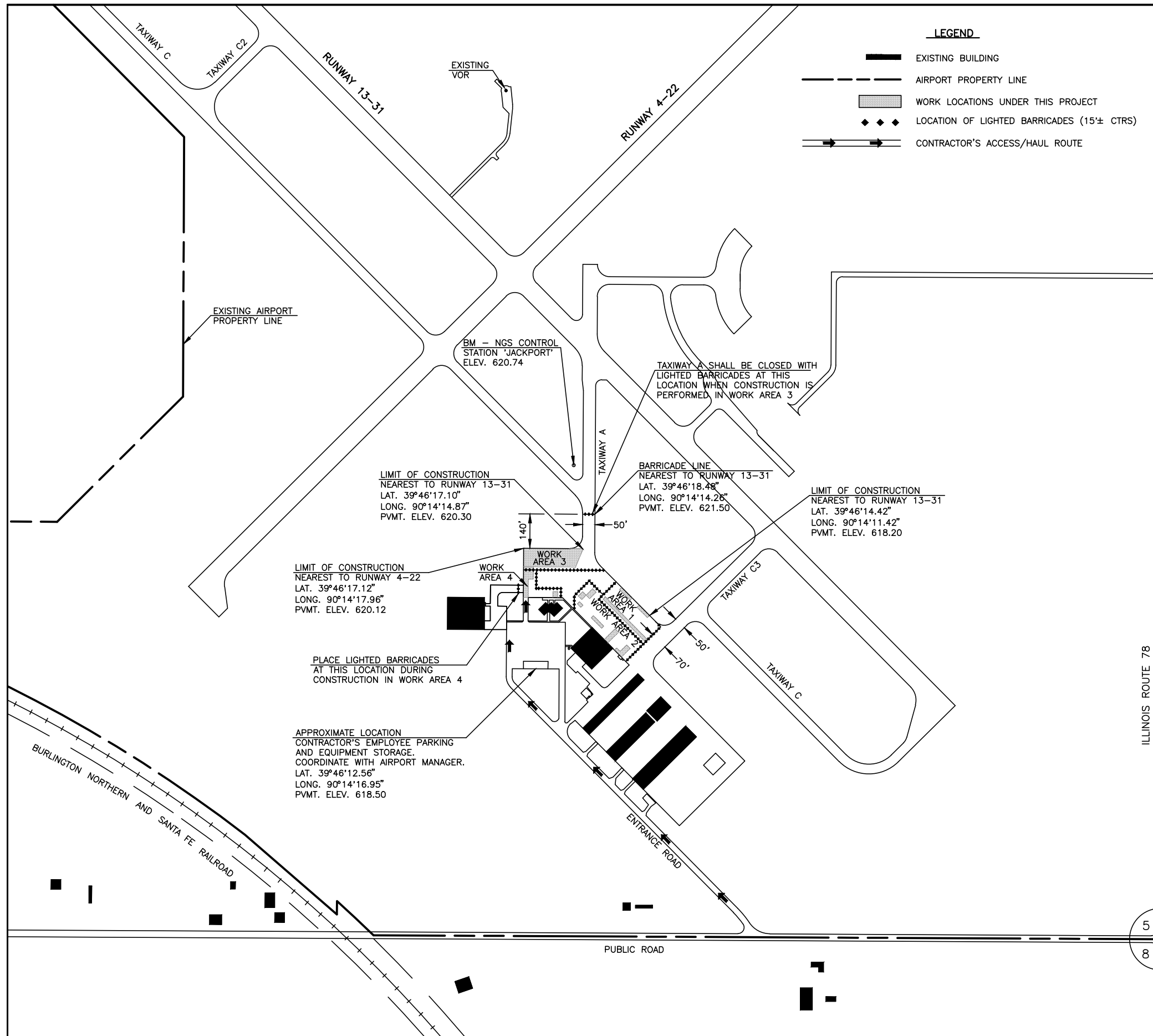
DECEMBER 5, 2014

SUMMARY OF QUANTITIES								
ITEM NO.	DESCRIPTION	UNIT	AS BID QUANTITIES			AS BUILT QUANTITIES		
			TOTAL QUANTITY	FEDERAL STATE LOCAL QUANTITY	FEDERAL LOCAL ONLY QUANTITY	TOTAL QUANTITY	FEDERAL STATE LOCAL QUANTITY	FEDERAL LOCAL ONLY QUANTITY
AR208530	AGGREGATE SUBBASE COURSE	TON	384	384				
AR208650	AGGREGATE SUBBASE PREPARATION	SQ. YD.	3,368	3,368				
AR501530	PCC TEST BATCH	EACH	1	1				
AR501910	REMOVE & REPLACE PCC PAVEMENT	SQ. YD.	1,893	1,893				
AR501925	REMOVE & REPLACE PCC PAVEMENT - REINFORCED	SQ. YD.	1,475	1,475				
AR510510	TIE DOWN	EACH	27	27				
AR605539	CLEAN & SEAL JOINTS AND CRACKS	LIN. FT	1,675	0	1,675			
AR620520	PAVEMENT MARKING - WATERBORNE	SQ. FT.	667	0	667			

INDEX OF SHEETS	
NO.	DESCRIPTION
1	COVER SHEET
2	IMPROVEMENT AND SAFETY PLAN
3	PLAN LAYOUT
4-5	APRON CONSTRUCTION DETAILS
6	SPECIAL DETAILS
7-8	PAVEMENT MARKING



	PLANS PREPARED BY : HUTCHISON ENGINEERING, INC. JACKSONVILLE, ILLINOIS	JACKSONVILLE AIRPORT AUTHORITY
	SUBMITTED <i>[Signature]</i> DATE <u>November 18, 2014</u>	APPROVED <i>[Signature]</i> Vice CHAIRMAN DATE <u>November 18, 2014</u>



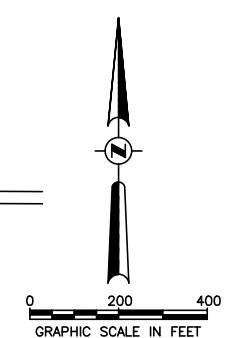
GENERAL NOTE

- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING UTILITY PROPERTY FROM CONSTRUCTION OPERATIONS. PRIOR TO STARTING WORK, THE CONTRACTOR SHALL CONTACT J.U.L.I.E. (JOINT UTILITY LOCATING INFORMATION FOR EXCAVATORS) PHONE: 1-800-892-0123 OR 811 TO ARRANGE FOR LOCATION OF UTILITIES IN THE WORK AREA. A MINIMUM OF FORTY-EIGHT HOURS ADVANCE NOTICE IS REQUIRED FOR NON-EMERGENCY WORK.

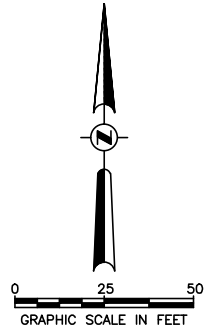
COUNTY - MORGAN
 CITY - JACKSONVILLE
 TOWNSHIP - T. 15 N., R. 10 W.
 SECTION NO. - 5
 ADDRESS - JACKSONVILLE MUNICIPAL AIRPORT
 1956 BALDWIN ROAD
 JACKSONVILLE, ILLINOIS 62650

CONSTRUCTION PROCEDURE NOTES

- ALL CONTRACTOR EMPLOYEES WILL PARK THEIR PERSONAL VEHICLES IN THE AREA DESIGNATED BY THE AIRPORT MANAGER FOR USE AS A VEHICLE PARKING AREA. THE CONTRACTOR WILL BE RESPONSIBLE FOR TRANSPORTING EMPLOYEES TO THE PROPOSED CONSTRUCTION SITE. ONLY AUTHORIZED CONTRACTOR VEHICLES WILL BE ALLOWED ON THE AIR OPERATIONS AREA OF THE AIRPORT AND ON THE CONSTRUCTION SITE.
- FLAGS WILL BE REQUIRED ON ALL CONTRACTOR'S VEHICLES AND EQUIPMENT USED ON THE AIRPORT OPERATIONS AREA AND CONSTRUCTION SITE. THE FLAGS SHALL BE THREE (3) FOOT SQUARE CHECKERED FLAGS (INTERNATIONAL ORANGE AND WHITE) DISPLAYED IN FULL VIEW ABOVE THE VEHICLE.
- THE CONTRACTOR WILL USE THE DESIGNATED HAUL ROUTE AND EQUIPMENT STORAGE AREA SHOWN ON THIS SHEET. IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN THE DESIGNATED HAUL ROUTE AND EQUIPMENT STORAGE AREA FOR THE DURATION OF THE PROJECT. ANY DAMAGE TO EXISTING PAVEMENTS USED TO HAUL MATERIAL TO THE CONSTRUCTION SITE WILL BE REPAIRED BY THE CONTRACTOR AT HIS OWN EXPENSE TO THE SATISFACTION OF THE AIRPORT MANAGER AND THE RESIDENT ENGINEER.
- THE CONTRACTOR SHALL FURNISH, MAINTAIN, AND REMOVE ALL BARRICADES REQUIRED TO DELINEATE THE WORK AREA AND KEEP AIRCRAFT FROM ENCROACHING INTO SAID WORK AREA. THIS REQUIREMENT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- CONSTRUCTION ACTIVITY CONTROL AND OPERATION AREA PROTECTION SHALL BE ACCOMPLISHED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR NO. 150/5370-2F.
- THE MAXIMUM CONSTRUCTION EQUIPMENT HEIGHT UTILIZED ON THE AIRPORT FOR THIS PROJECT SHALL BE 25'.
- THIS PROJECT CONSISTS OF WORK TO REHABILITATE APRON AREA PAVEMENT.
- THIS PROJECT WILL NOT DISTURB TURFED LAND.
- THE EXISTING AIRPORT REFERENCE CODE IS C-1. THE TAXILANE OBJECT FREE AREA FOR AIRPLANE DESIGN GROUP I IS 39.5' FROM CENTERLINE. NO WORK SHALL BE PERFORMED WITHIN ANY TAXILANE OR TAXIWAY OBJECT FREE AREA.
- WORK ON THE APRON AREA SHALL BE ACCOMPLISHED BY BARRICADING THE LOCATIONS WHERE WORK WILL BE PERFORMED TO SEPARATE TAXIING AIRCRAFT FROM THE CONTRACTOR'S WORKMEN AND CONSTRUCTION EQUIPMENT. THESE LOCATIONS ARE DESIGNATED AS WORK AREAS 1 THROUGH 4 ON THIS PLAN SHEET.
- IN ORDER TO ALLOW AIRCRAFT TO OPERATE AROUND THE WORK AREAS, AND MAINTAIN PILOT/PASSENGER ACCESS TO THE TERMINAL BUILDING, THE CONSTRUCTION SHALL BE PERFORMED IN EACH WORK AREA ONE WORK AREA AT A TIME. IT IS RECOMMENDED THAT THE SEQUENCE FOR PERFORMING CONSTRUCTION FOLLOW THE NUMBERING OF EACH WORK AREA (I.E. WORK AREA 1 BE COMPLETED FIRST, THEN WORK AREA 2, ETC.).



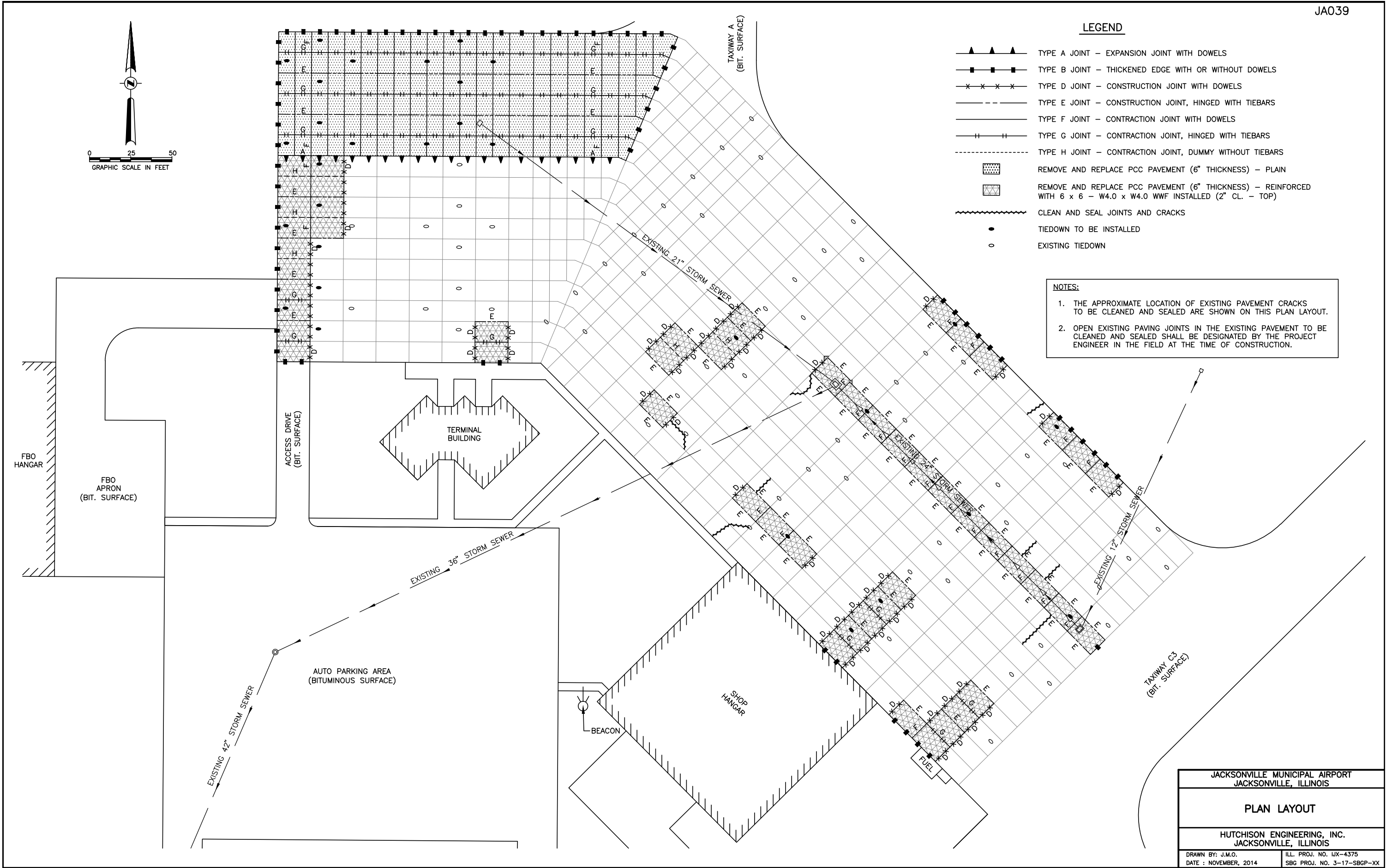
JACKSONVILLE MUNICIPAL AIRPORT JACKSONVILLE, ILLINOIS	
IMPROVEMENT AND SAFETY PLAN	
HUTCHISON ENGINEERING, INC. JACKSONVILLE, ILLINOIS	
DRAWN BY: J.M.O. DATE: NOVEMBER, 2014	ILL. PROJ. NO. IJX-4375 SBG PROJ. NO. 3-17-SBGP-XX



LEGEND

- ▲ ▲ ▲ TYPE A JOINT - EXPANSION JOINT WITH DOWELS
- ■ ■ TYPE B JOINT - THICKENED EDGE WITH OR WITHOUT DOWELS
- × × × TYPE D JOINT - CONSTRUCTION JOINT WITH DOWELS
- TYPE E JOINT - CONSTRUCTION JOINT, HINGED WITH TIEBARS
- — — TYPE F JOINT - CONTRACTION JOINT WITH DOWELS
- H — TYPE G JOINT - CONTRACTION JOINT, HINGED WITH TIEBARS
- - - - - TYPE H JOINT - CONTRACTION JOINT, DUMMY WITHOUT TIEBARS
- ▨ REMOVE AND REPLACE PCC PAVEMENT (6" THICKNESS) - PLAIN
- ▩ REMOVE AND REPLACE PCC PAVEMENT (6" THICKNESS) - REINFORCED WITH 6 x 6 - W4.0 x W4.0 WWF INSTALLED (2' CL. - TOP)
- ~~~~~ CLEAN AND SEAL JOINTS AND CRACKS
- TIEDOWN TO BE INSTALLED
- EXISTING TIEDOWN

- NOTES:**
1. THE APPROXIMATE LOCATION OF EXISTING PAVEMENT CRACKS TO BE CLEANED AND SEALED ARE SHOWN ON THIS PLAN LAYOUT.
 2. OPEN EXISTING PAVING JOINTS IN THE EXISTING PAVEMENT TO BE CLEANED AND SEALED SHALL BE DESIGNATED BY THE PROJECT ENGINEER IN THE FIELD AT THE TIME OF CONSTRUCTION.



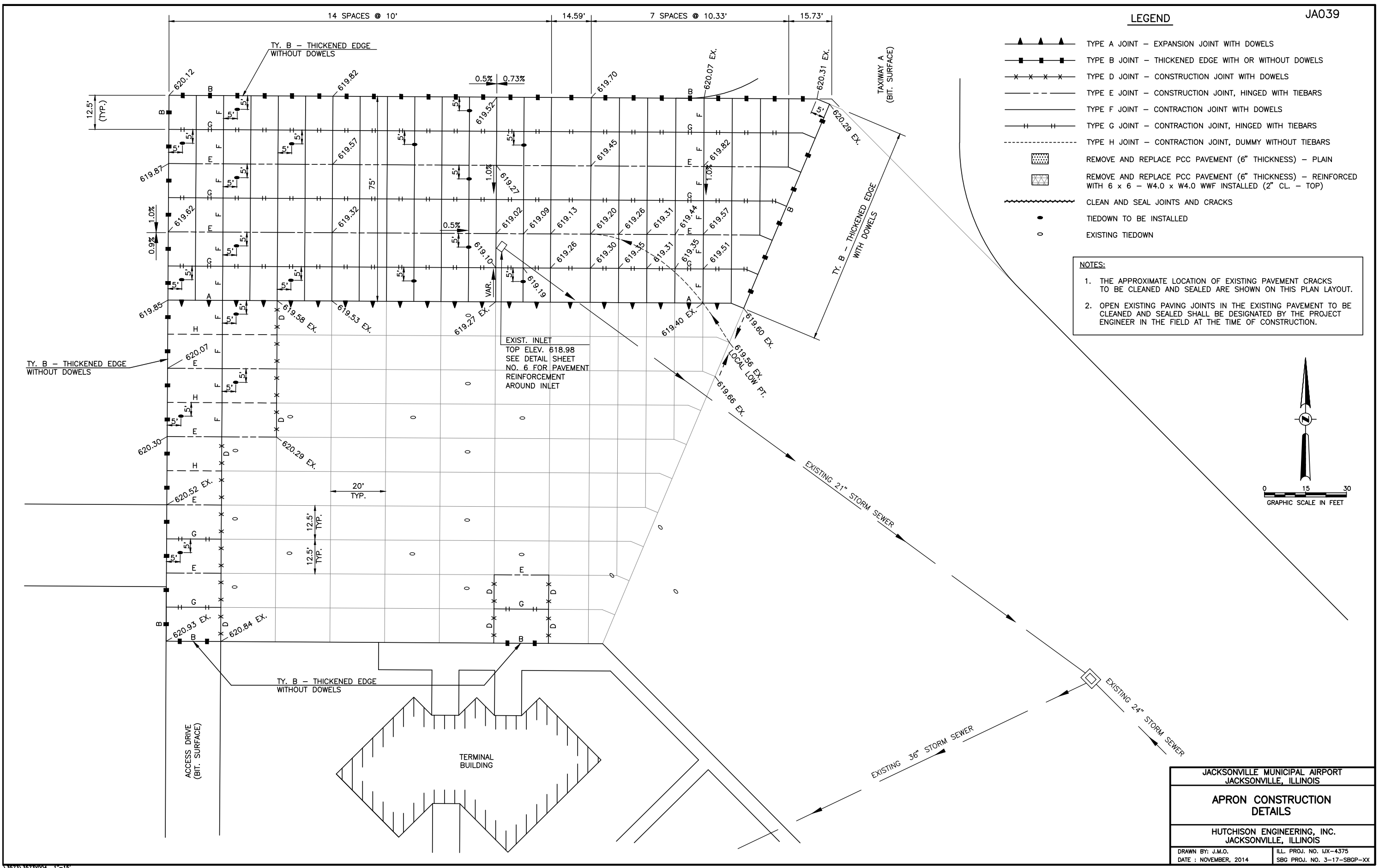
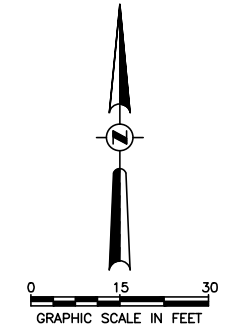
JACKSONVILLE MUNICIPAL AIRPORT JACKSONVILLE, ILLINOIS	
PLAN LAYOUT	
HUTCHISON ENGINEERING, INC. JACKSONVILLE, ILLINOIS	
DRAWN BY: J.M.O. DATE: NOVEMBER, 2014	ILL. PROJ. NO. IJX-4375 SBG PROJ. NO. 3-17-SBGP-XX

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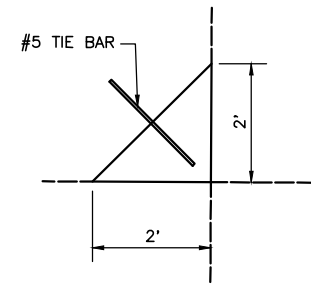
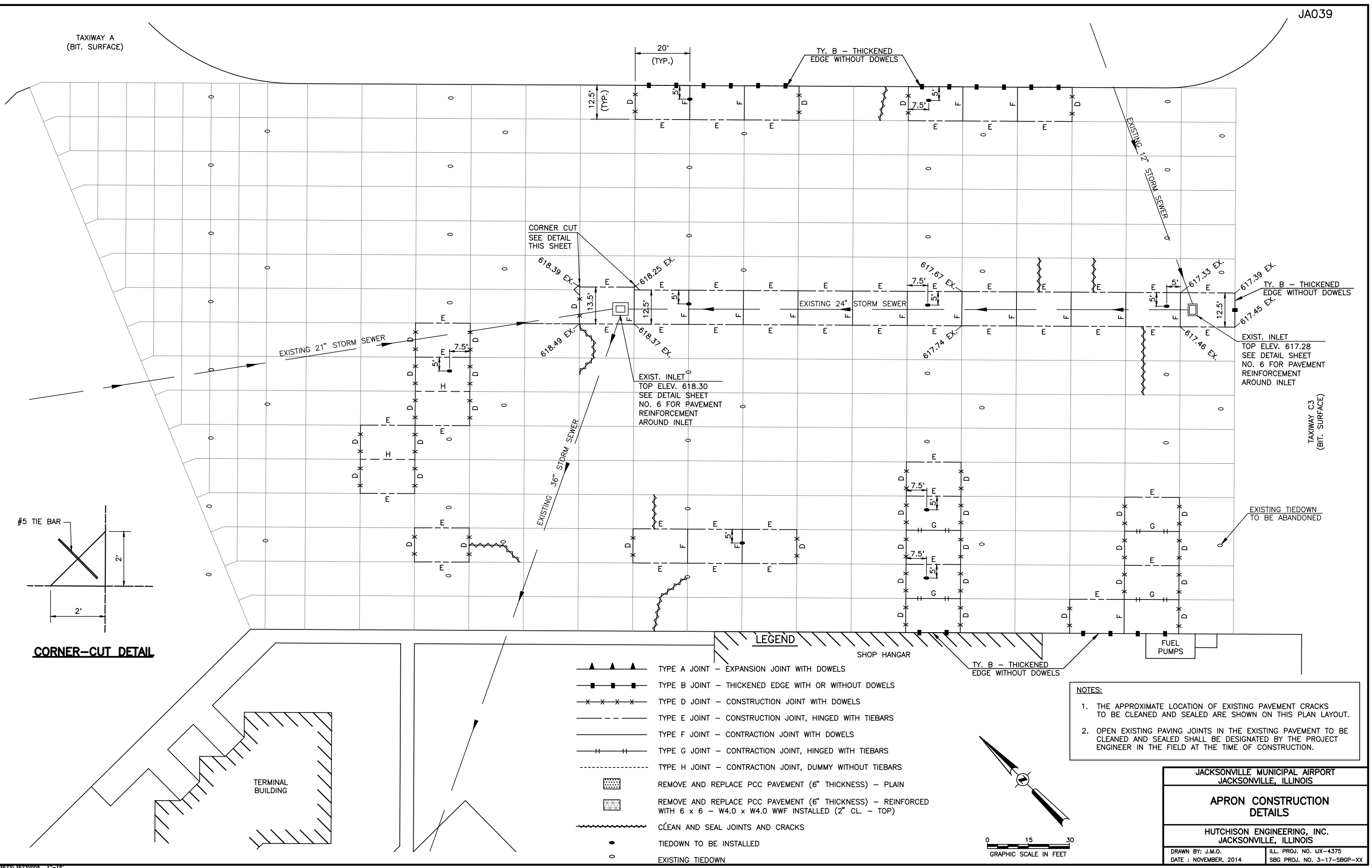
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JACKSONVILLE MUNICIPAL AIRPORT JACKSONVILLE, ILLINOIS	
APRON CONSTRUCTION DETAILS	
HUTCHISON ENGINEERING, INC. JACKSONVILLE, ILLINOIS	
DRAWN BY: J.M.O. DATE: NOVEMBER, 2014	ILL. PROJ. NO. IJX-4375 SBG PROJ. NO. 3-17-SBGP-XX

TAXIWAY A
(BIT. SURFACE)

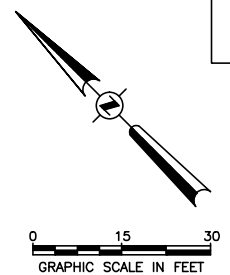


LEGEND

	TYPE A JOINT - EXPANSION JOINT WITH DOWELS
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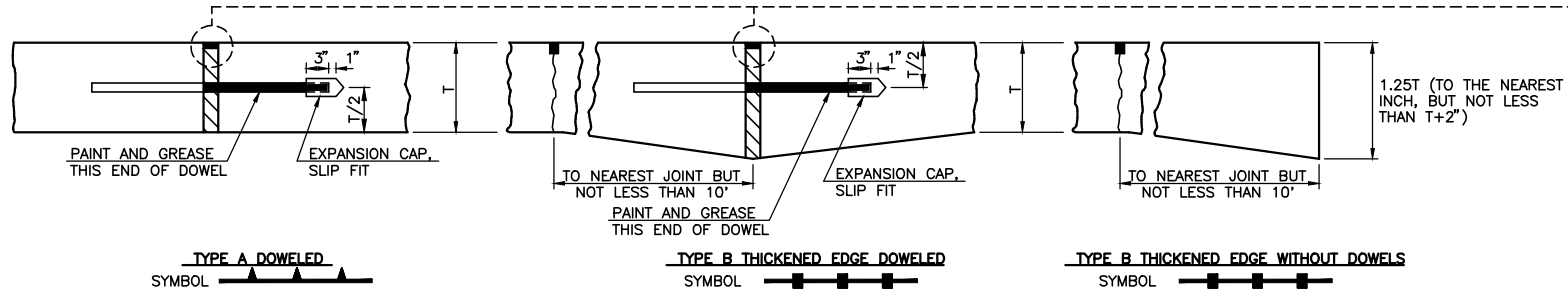


JACKSONVILLE MUNICIPAL AIRPORT
JACKSONVILLE, ILLINOIS

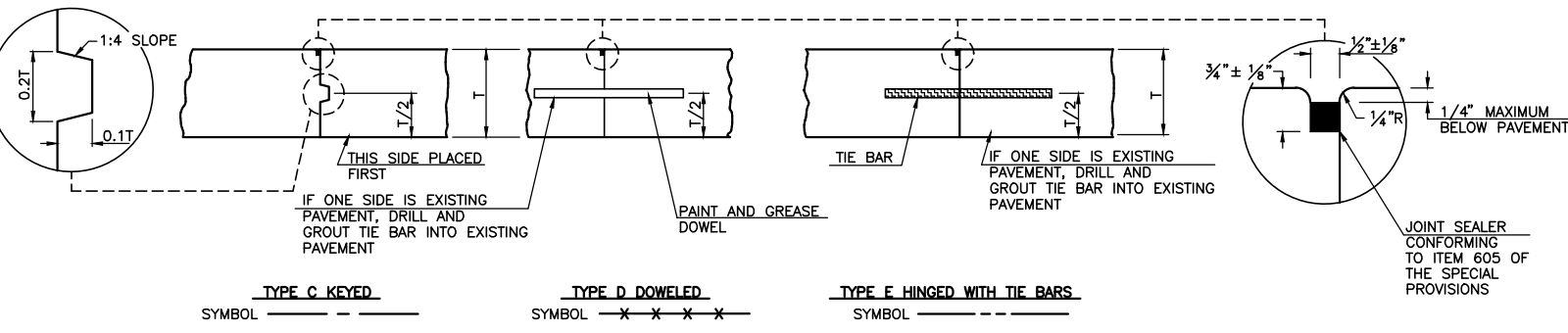
APRON CONSTRUCTION DETAILS

HUTCHISON ENGINEERING, INC.
JACKSONVILLE, ILLINOIS

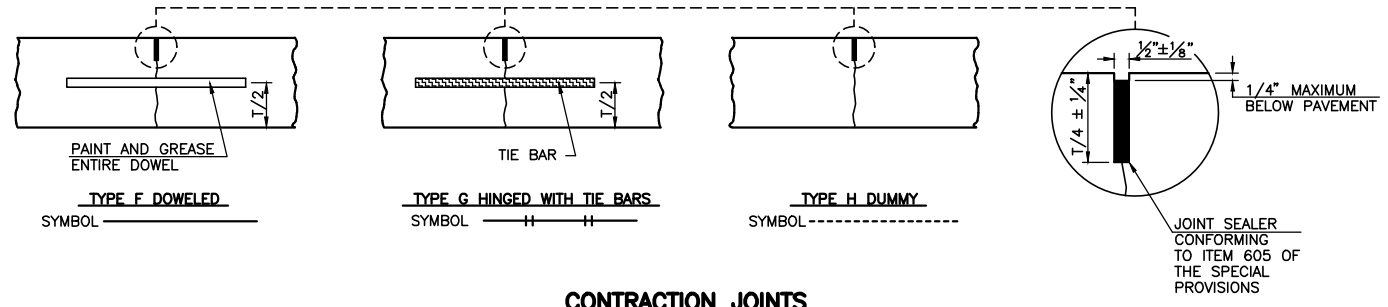
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DATE: NOVEMBER, 2014 SBG PROJ. NO. 3-17-SBGP-XX



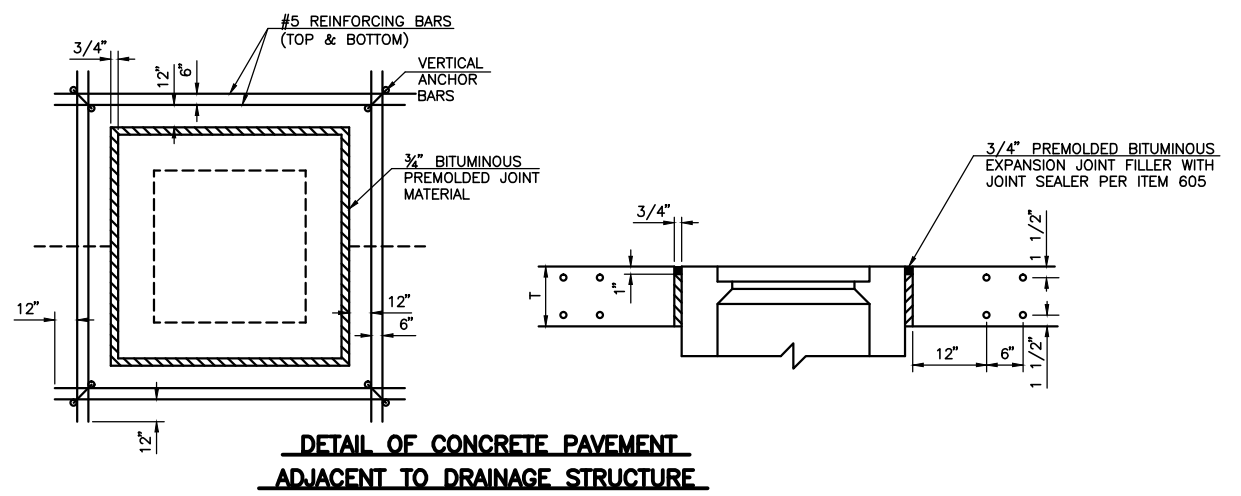
EXPANSION JOINTS



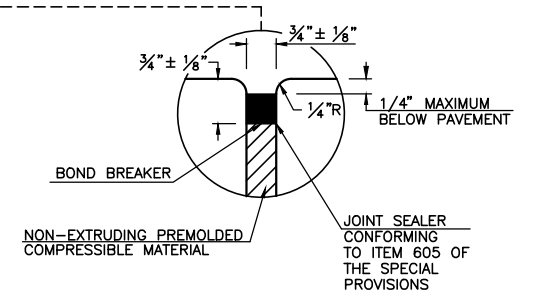
CONSTRUCTION JOINTS



CONTRACTION JOINTS



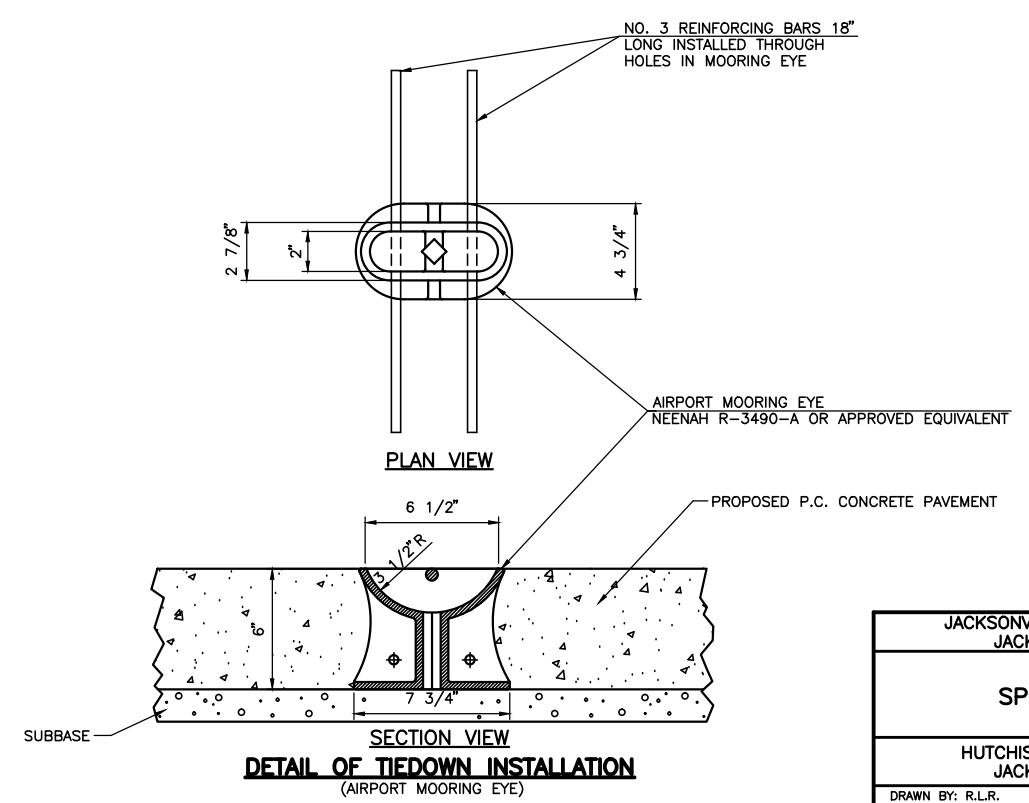
DETAIL OF CONCRETE PAVEMENT ADJACENT TO DRAINAGE STRUCTURE



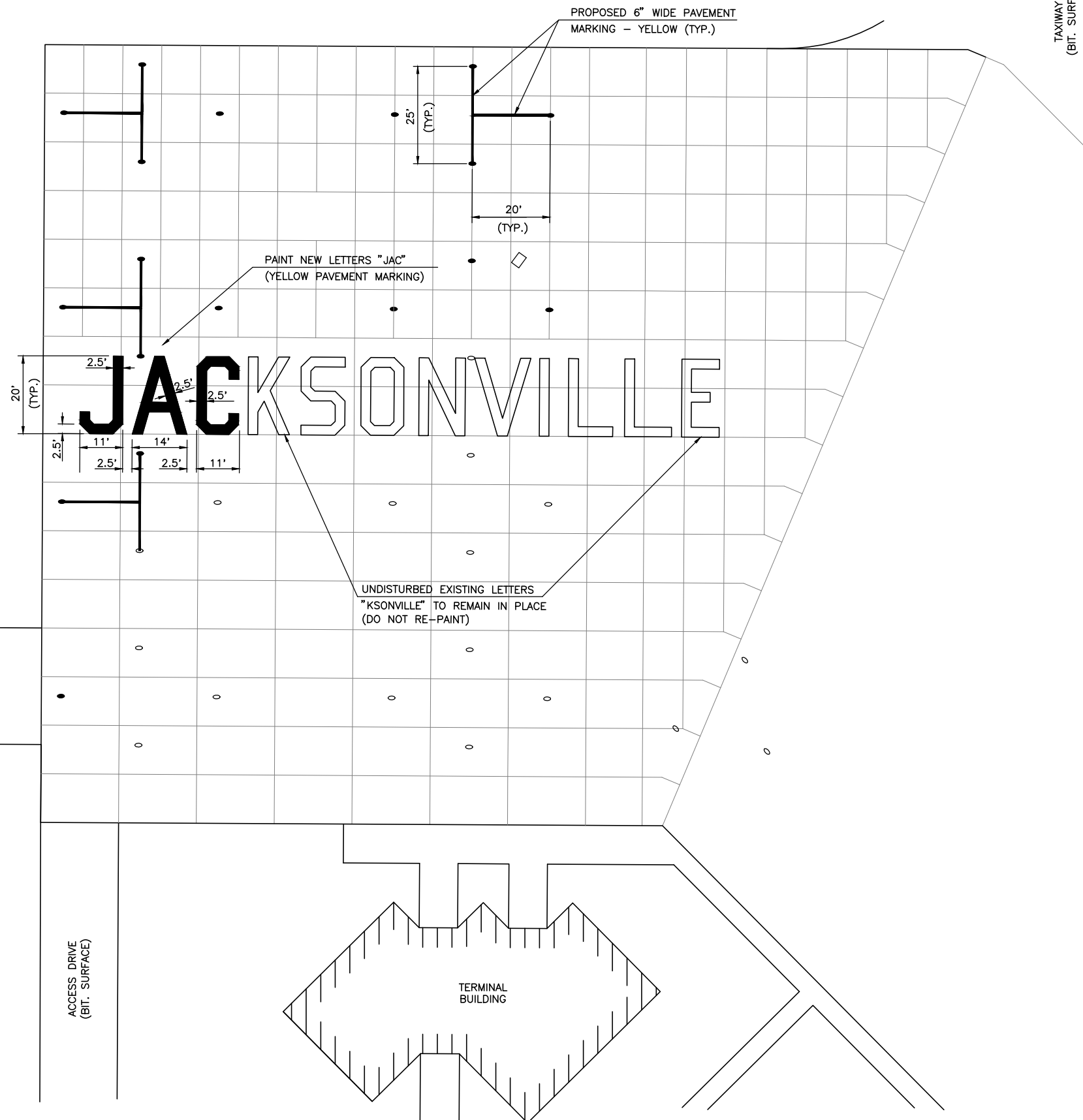
DOWEL BAR DETAILS				TIE BAR DETAILS		
T	DIA.	LENGTH	SPACING	BAR SIZE	LENGTH	SPACING
6"	3/4"	18"	12"	#5	30"	30"
7"	3/4"	19"	12"	#5	30"	30"
8"	1"	19"	12"	#5	30"	30"
9"	1"	19"	12"	#5	30"	30"
10"	1"	19"	12"	#5	30"	30"
11"	1"	19"	12"	#5	30"	30"
12"	1"	19"	12"	#5	30"	30"
13"	1 1/4"	20"	15"	#5	30"	24"
14"	1 1/4"	20"	15"	#5	30"	24"
15"	1 1/4"	20"	15"	#5	30"	24"
16"	1 1/4"	20"	15"	#5	30"	24"

NOTES:

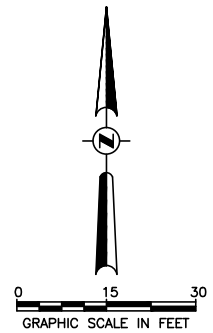
- "T" REFERS TO PAVEMENT THICKNESS.
- ALL EXPOSED JOINT EDGES SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/4".
- ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINT GROOVES SHALL BE SAWS OR FORMED. THESE JOINTS SHALL BE CUT AS SOON AFTER CONCRETE PLACEMENT AS POSSIBLE WITHOUT CAUSING THE CONCRETE TO CHIP, SPALL OR TEAR. ALL JOINTS SHALL BE CUT BEFORE UNCONTROLLED SHRINKAGE CRACKING TAKES PLACE USUALLY WITHIN 4 TO 24 HOURS OF CONCRETE PLACEMENT.
- ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY WHICH WILL INSURE THAT THEY WILL REMAIN PARALLEL TO THE SURFACE OF THE PAVEMENT AND TO THE CENTERLINES OF THE PAVEMENT LANES. THE DOWEL BAR ASSEMBLIES SHALL BE APPROVED BY THE ENGINEER.
- ALL TIE BARS SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR OTHER APPROVED METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT. SUPPORT PINS SHALL BE OF SUFFICIENT LENGTH TO PENETRATE AT LEAST 6" INTO THE SUBGRADE.
- ALL TIE BARS SHALL BE PLACED AT A POINT NOT EXCEEDING 15" FROM A TRANSVERSE CONTRACTION, EXPANSION, OR CONSTRUCTION JOINT AND SPACED 30" ON CENTERS AND SHALL BE SIZED ACCORDING TO THE ABOVE TIE BAR SCHEDULE.
- WHEN THE TIE BARS ARE REQUIRED FOR LONGITUDINAL CONSTRUCTION JOINTS, ONE END OF THE TIE BAR SHALL BE BENT AND INSERTED INTO THE MIDPOINT OF THE JOINT AND THE OTHER END RIGIDLY SUPPORTED BY PINS PRIOR TO PLACING CONCRETE ON THE ADJACENT LANE. THE BENT BARS SHALL BE STRAIGHTENED TO RIGHT ANGLES WITH THE LONGITUDINAL JOINT AND EXTEND INTO THE ADJACENT LANE PARALLEL TO THE PAVEMENT SURFACE.
- DOWELS IN TRANSVERSE CONTRACTION AND CONSTRUCTION JOINTS AND IN LONGITUDINAL BUTT CONSTRUCTION JOINTS SHALL BE COATED WITH A RUST-PROOFING COMPOUND AND THE ENTIRE LENGTH GREASED WITH A HEAVY GREASE, EXCEPT AS NOTED IN THE TYPE "A" EXPANSION JOINT.
- METAL FORMS FOR KEYED JOINTS MAY BE LEFT IN PLACE.
- GROOVE DEPTH FOR ALL DUMMY GROOVE JOINTS SHALL BE T/4 ± 1/4", BUT IN NO CASE LESS THAN 1 1/2".
- COST OF 3/4" PREMOLDED BITUMINOUS JOINT FILLER AROUND DRAINAGE STRUCTURES SHALL BE INCLUDED IN THE COST TO REMOVE AND REPLACE P.C.C. PAVEMENT



JACKSONVILLE MUNICIPAL AIRPORT JACKSONVILLE, ILLINOIS	
SPECIAL DETAILS	
HUTCHISON ENGINEERING, INC. JACKSONVILLE, ILLINOIS	
DRAWN BY: R.L.R. DATE: NOVEMBER, 2014	ILL. PROJ. NO. IJX-4375 AIP PROJ. NO. 3-17-0054-B19



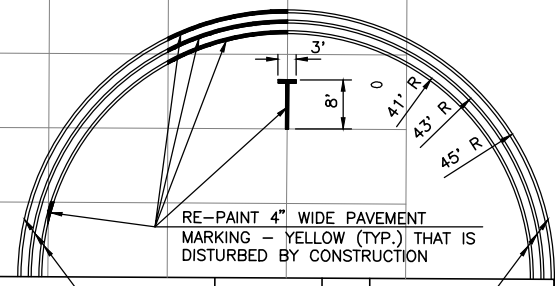
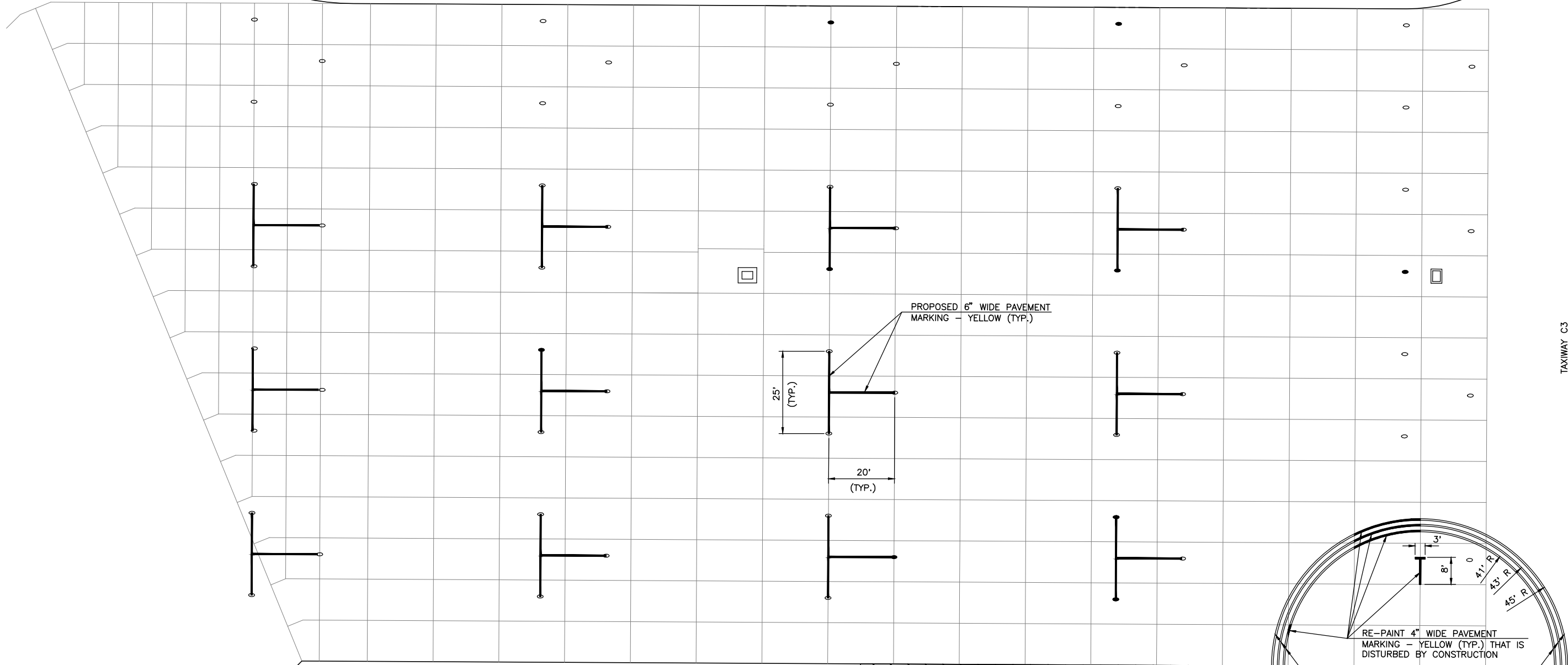
- NOTES:
1. ALL PAVEMENT MARKING SHALL CONSIST OF TWO APPLICATIONS OF PAINT. THE APPLICATION OF GLASS BEADS (REFLECTIVE MEDIA) WILL NOT BE REQUIRED ON THE PAVEMENT MARKING.
 2. PAINT SHALL BE APPLIED AT THE RATE OF 115 SQ. FT. PER GALLON PER APPLICATION.



JACKSONVILLE MUNICIPAL AIRPORT JACKSONVILLE, ILLINOIS	
PAVEMENT MARKING LAYOUT	
HUTCHISON ENGINEERING, INC. JACKSONVILLE, ILLINOIS	
DRAWN BY: J.M.O. DATE : NOVEMBER, 2014	ILL. PROJ. NO. IJX-4375 SBG PROJ. NO. 3-17-SBGP-XX

TAXIWAY A
(BIT. SURFACE)

TAXIWAY C3
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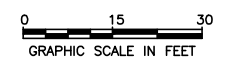
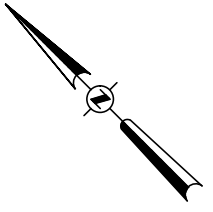


FUEL PUMPS

SHOP HANGAR

TERMINAL BUILDING

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