



CONSULTANTS

FINAL

NOVEMBER 19, 2021
AIRFIELD PAVEMENT
REHABILITATION - PHASE 4

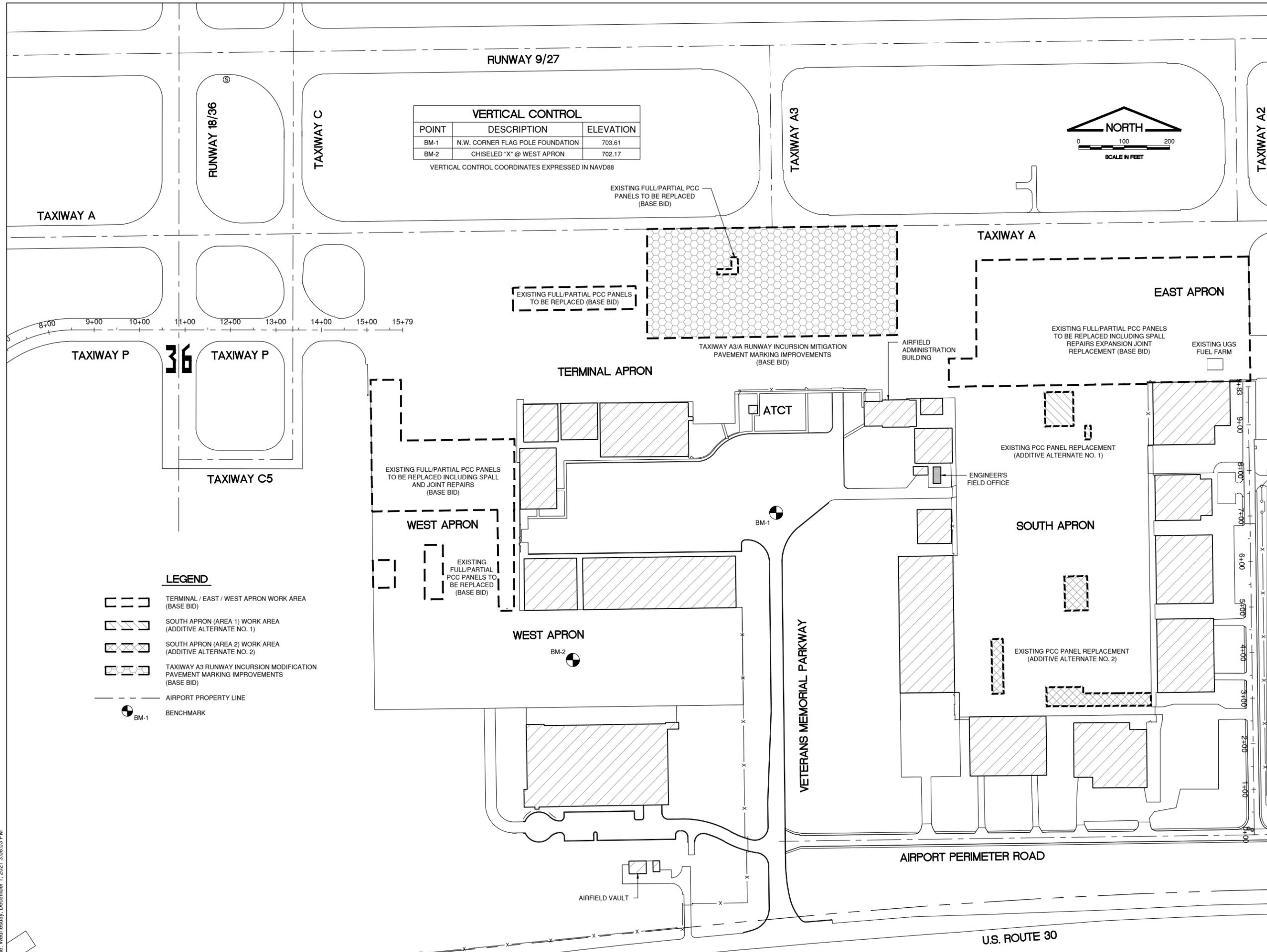


AURORA MUNICIPAL AIRPORT
AURORA, ILLINOIS

MARK | DATE | DESCRIPTION

| |
|--|
| AIP PROJ. NO. 3-17-SBGP-162/TBD |
| IL PROJ. NO. ARR-4785 |
| CMT PROJECT NO: 200285-02 |
| CAD DWG FILE: 20028502 - SITE PLAN.DWG |
| DESIGNED BY: ARM |
| DRAWN BY: JRO |
| CHECKED BY: DKP |
| APPROVED BY: ARM |
| COPYRIGHT: |

SHEET TITLE
**SITE PLAN AND
PROJECT CONTROL
PLAN**



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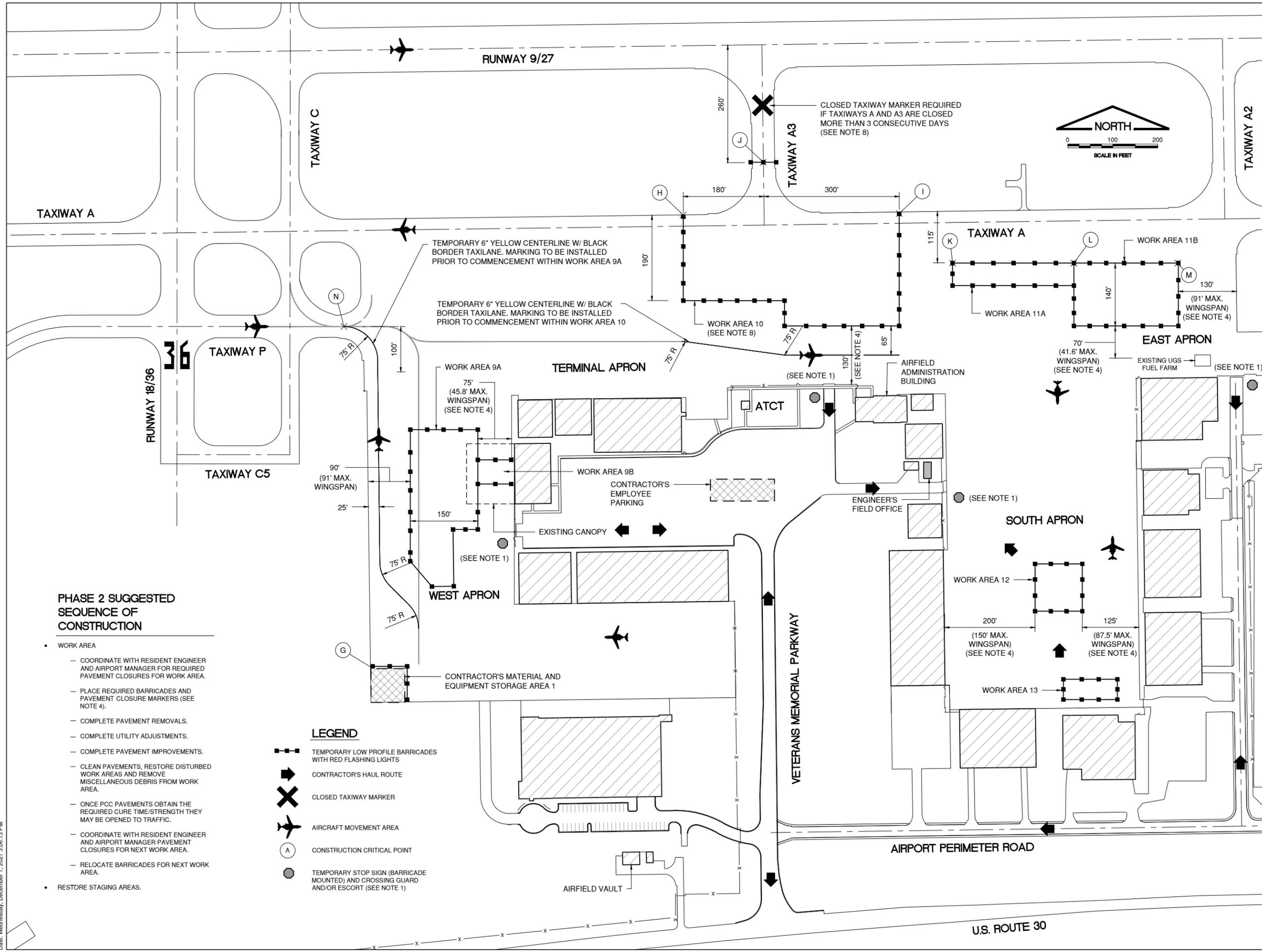
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AIRFIELD PAVEMENT
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AURORA, ILLINOIS

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| CMT PROJECT NO: 200285-02 | | |
| CAD DWG FILE: 20028502 - SEQUENCE - 2.DWG | | |
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| COPYRIGHT: | | |

SHEET TITLE
**SEQUENCE OF
CONSTRUCTION -
PHASE 2**



**PHASE 2 SUGGESTED
SEQUENCE OF
CONSTRUCTION**

- WORK AREA
 - COORDINATE WITH RESIDENT ENGINEER AND AIRPORT MANAGER FOR REQUIRED PAVEMENT CLOSURES FOR WORK AREA.
 - PLACE REQUIRED BARRICADES AND PAVEMENT CLOSURE MARKERS (SEE NOTE 4).
 - COMPLETE PAVEMENT REMOVALS.
 - COMPLETE UTILITY ADJUSTMENTS.
 - COMPLETE PAVEMENT IMPROVEMENTS.
 - CLEAN PAVEMENTS, RESTORE DISTURBED WORK AREAS AND REMOVE MISCELLANEOUS DEBRIS FROM WORK AREA.
 - ONCE PCC PAVEMENTS OBTAIN THE REQUIRED CURE TIME/STRENGTH THEY MAY BE OPENED TO TRAFFIC.
 - COORDINATE WITH RESIDENT ENGINEER AND AIRPORT MANAGER PAVEMENT CLOSURES FOR NEXT WORK AREA.
 - RELOCATE BARRICADES FOR NEXT WORK AREA.
- RESTORE STAGING AREAS.

LEGEND

- TEMPORARY LOW PROFILE BARRICADES WITH RED FLASHING LIGHTS
- CONTRACTOR'S HAUL ROUTE
- CLOSED TAXIWAY MARKER
- AIRCRAFT MOVEMENT AREA
- CONSTRUCTION CRITICAL POINT
- TEMPORARY STOP SIGN (BARRICADE MOUNTED) AND CROSSING GUARD AND/OR ESCORT (SEE NOTE 1)

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AIRFIELD PAVEMENT
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AURORA MUNICIPAL AIRPORT
AURORA, ILLINOIS

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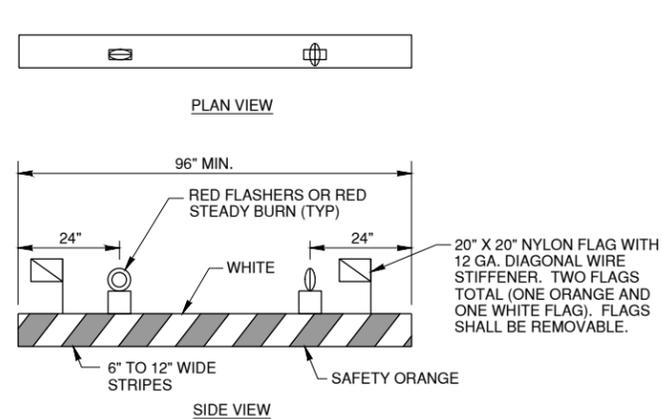
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| COPYRIGHT: |

SHEET TITLE
**SEQUENCE OF
CONSTRUCTION
GENERAL DETAILS**

| CONSTRUCTION POINT TABLE | | | | |
|--------------------------|-----------------------|----------------|----------------|-----------|
| POINT | NEAREST ACTIVE RUNWAY | LATITUDE | LONGITUDE | ELEVATION |
| A | RUNWAY 18/36 | 41° 46' 03.15" | 88° 28' 12.17" | 701.0 |
| B | RUNWAY 18/36 | 41° 46' 06.11" | 88° 28' 12.31" | 702.0 |
| C | RUNWAY 9/27 | 41° 46' 08.08" | 88° 28' 07.51" | 703.0 |
| D | RUNWAY 9/27 | 41° 46' 08.08" | 88° 28' 04.74" | 704.0 |
| E | RUNWAY 9/27 | 41° 46' 08.78" | 88° 27' 48.39" | 705.0 |
| F | RUNWAY 9/27 | 41° 46' 08.79" | 88° 27' 46.74" | 705.0 |
| G | RUNWAY 18/36 | 41° 45' 59.65" | 88° 28' 12.10" | 700.0 |
| H | RUNWAY 9/27 | 41° 46' 09.57" | 88° 28' 02.96" | 704.0 |
| I | RUNWAY 9/27 | 41° 46' 09.60" | 88° 27' 56.62" | 705.0 |
| J | RUNWAY 9/27 | 41° 46' 10.72" | 88° 28' 00.61" | 705.0 |
| K | RUNWAY 9/27 | 41° 46' 08.52" | 88° 27' 55.05" | 704.0 |
| L | RUNWAY 9/27 | 41° 46' 08.52" | 88° 27' 51.50" | 703.0 |
| M | RUNWAY 9/27 | 41° 46' 08.53" | 88° 27' 48.45" | 704.0 |
| N | RUNWAY 18/36 | 41° 46' 07.11" | 88° 28' 12.90" | 702.0 |

CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE STANDARD SPECIFICATIONS SECTION 50-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS. IT IS ANTICIPATED THE FOLLOWING PROJECTS MAY BE UNDER CONSTRUCTION CONCURRENTLY WITH THIS PROJECT. NO ADDITIONAL COMPENSATION SHALL BE CONSIDERED FOR ANY EFFORTS TO COORDINATE AND ACCESS TO THE SITE DUE TO ADJACENT CONSTRUCTION.

MAXIMUM ANTICIPATED EQUIPMENT HEIGHT: 25'

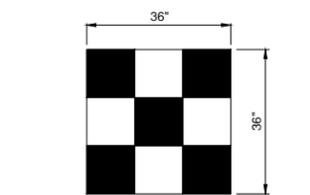


AIRSIDE LOW PROFILE LIGHTED BARRICADE

NOT TO SCALE

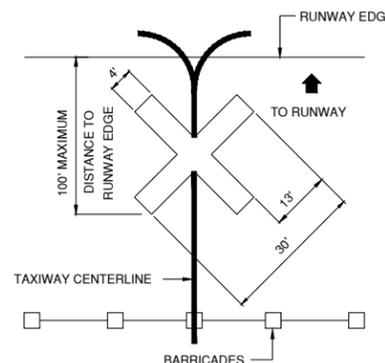
BARRICADE NOTES

1. FLASHER OR STEADY BURN LIGHTS SHALL BE BATTERY OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
2. FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
3. BARRICADES TO BE PLACED WITH A MAXIMUM OF 4' SPACING END TO END UP TO THE EDGE OF PAVEMENT ALONG OPERATIONAL PAVEMENT ADJACENT TO CONSTRUCTION AS DIRECTED BY THE RESIDENT ENGINEER. ALTERNATE FLASHER OR STEADY BURN LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
4. FLASHER OR STEADY BURN LIGHTS SHALL BE SECURED TO THE BARRICADES, AS APPROVED BY THE RESIDENT ENGINEER.
5. BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF IT COMPONENTS, AND WEIGHTED TO AVOID BEING BLOWN OVER.
6. BARRICADES SHALL BE OF A COMMERCIAL DESIGN AND SHALL MEET CURRENT FAA REQUIREMENTS.
7. PLACE ALL BARRICADES OUTSIDE RUNWAY SAFETY AREAS AND OUTSIDE TAXIWAY OBJECT FREE AREAS.
8. ALL COST ASSOCIATED WITH THE LOW PROFILE BARRICADES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.



CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG

NOT TO SCALE



CLOSED TAXIWAY MARKER DETAIL

NOT TO SCALE

CLOSED TAXIWAY MARKER DETAIL NOTES

1. CLOSED TAXIWAY MARKERS SHALL BE PAINTED WITH TEMPORARY MARKING CAPABLE OF BEING REMOVED WITH LOW PRESSURE WATER BLASTING OR OTHER MATERIAL THAT DOES NOT VIOLATE THE OBJECT FREE AREA CRITERIA AND RUNWAY SAFETY AREA CRITERIA PER ADVISORY CIRCULAR 150/5300-13A (LATEST EDITION) AND ARE APPROVED BY THE AIRPORT.
2. CONTRACTOR SHALL MAINTAIN AND RELOCATE MARKERS AS SHOWN ON THE PLANS OR AS NEEDED TO FACILITATE CONSTRUCTION
3. COST OF FURNISHING, INSTALLING, MAINTAINING AND REMOVING MARKERS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
4. PLACE MARKERS OVER TAXIWAY CENTERLINE.
5. MARKERS SHALL BE ADEQUATELY SECURED TO PREVENT MOVEMENT BY PROPELLER WASH, JET BLAST OR OTHER WIND CURRENTS.
6. MARKERS ARE ONLY REQUIRED FOR TAXIWAYS CLOSED THREE (3) CONSECUTIVE DAYS OR MORE.



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| | | CMT PROJECT NO: 200285-02 |
| | | CAD DWG FILE: 20028502 - PROPOSED IMPROVEMENTS - |
| | | DESIGNED BY: JRW |
| | | DRAWN BY: JRO |
| | | CHECKED BY: CHK |
| | | APPROVED BY: APR |
| | | COPYRIGHT: |

SHEET TITLE
**PROPOSED
IMPROVEMENTS - 1**

THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.

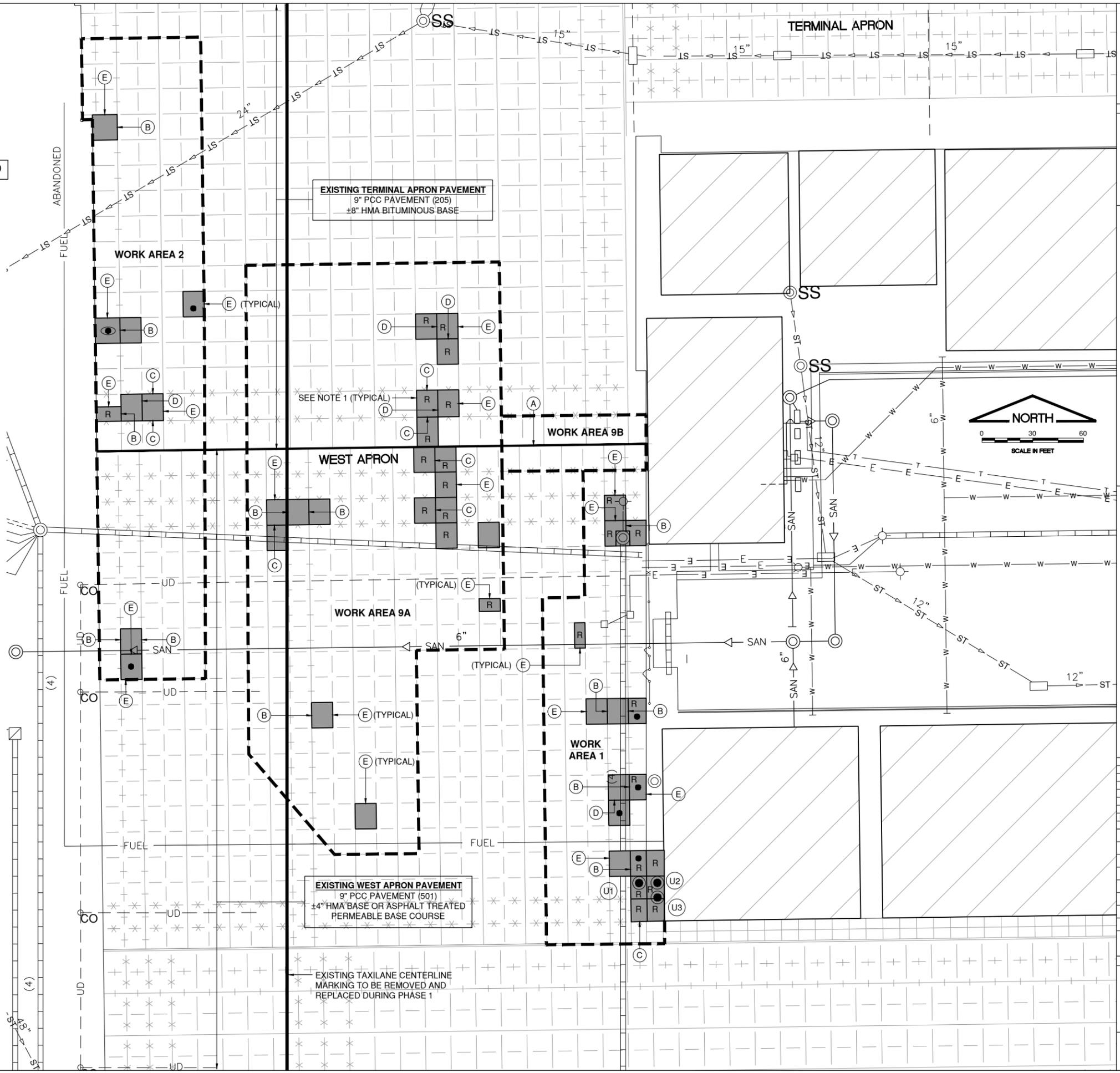
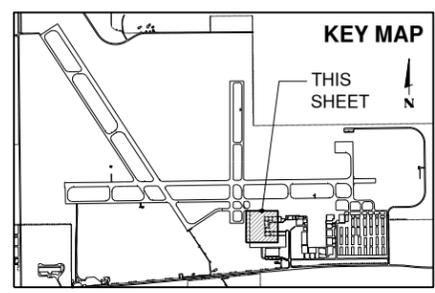
SEE SHEET 2 FOR EXISTING CONDITIONS LEGEND

LEGEND

- UTILITY TO BE RECONSTRUCTED
- NEW TIE DOWN (AR/AS/ATS10510)
- NEW TYPE B - HINGED CONTRACTION JOINT
- NEW TYPE C - DOWELED CONTRACTION JOINT
- NEW TYPE D DUMMY CONTRACTION JOINT
- NEW TYPE E - DOWELED CONTRACTION JOINT
- R REINFORCED PANEL
- (A) WIDEN, CLEAN AND SEAL ISOLATION JOINT
- EXISTING PANEL TO BE REMOVED AND REPLACED (BASE BID)
- ▨ EXISTING PANEL TO BE REMOVED AND REPLACED (ADDITIVE ALTERNATE NO. 1)
- ▩ EXISTING PANEL TO BE REMOVED AND REPLACED (ADDITIVE ALTERNATE NO. 2)
- ▧ EXISTING PANEL TO BE REMOVED AND REPLACED HIGH EARLY MIX (ADDITIVE ALTERNATE NO. 2)
- - - - - WORK AREA LIMITS
- GROUND ROD

NOTES

1. INSTALL ISOLATION JOINT AT CANOPY COLUMNS.
2. PRIOR TO REMOVAL OF PCC PANELS, CONTRACTOR SHALL RECON THE LOCATIONS AND NUMBER OF TIE DOWNS AND GROUND RODS TO BE REPLACED.



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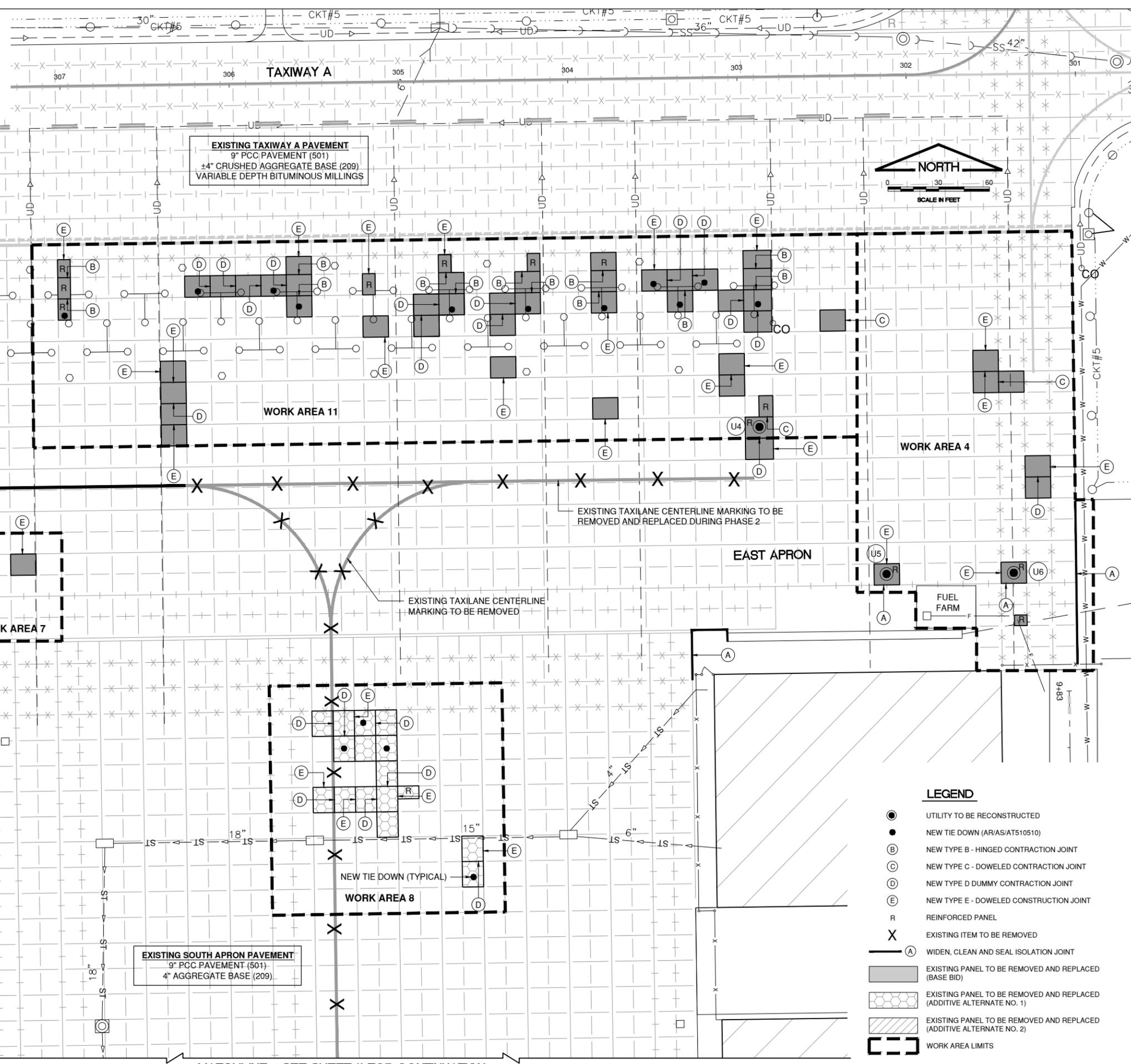
FINAL

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AIRFIELD PAVEMENT
REHABILITATION - PHASE 4



AURORA MUNICIPAL AIRPORT
AURORA, ILLINOIS

THE INFORMATION SHOWN ON THESE PLANS HAS BEEN OBTAINED FROM AVAILABLE RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY OR SUFFICIENCY OF THE INFORMATION AND THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED THAT THE CONDITIONS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE FIELD. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VISIT THE SITE AND ACQUAINT HIMSELF WITH THE EXISTING CONDITIONS.



MATCHLINE - SEE SHEET 9 FOR CONTINUATION

MATCHLINE - SEE SHEET 11 FOR CONTINUATION

LEGEND

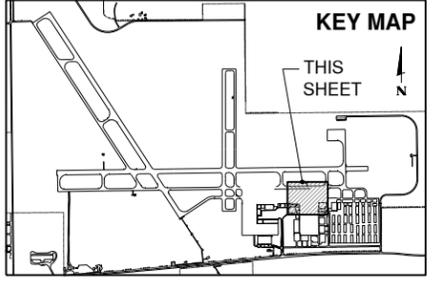
- UTILITY TO BE RECONSTRUCTED
- NEW TIE DOWN (AR/AS/ATS10510)
- (B) NEW TYPE B - HINGED CONTRACTION JOINT
- (C) NEW TYPE C - DOWELED CONTRACTION JOINT
- (D) NEW TYPE D DUMMY CONTRACTION JOINT
- (E) NEW TYPE E - DOWELED CONTRACTION JOINT
- R REINFORCED PANEL
- X EXISTING ITEM TO BE REMOVED
- (A) WIDEN, CLEAN AND SEAL ISOLATION JOINT
- EXISTING PANEL TO BE REMOVED AND REPLACED (BASE BID)
- EXISTING PANEL TO BE REMOVED AND REPLACED (ADDITIVE ALTERNATE NO. 1)
- EXISTING PANEL TO BE REMOVED AND REPLACED (ADDITIVE ALTERNATE NO. 2)
- WORK AREA LIMITS

EXISTING EAST APRON PAVEMENT
9" PCC PAVEMENT (501)
±8" AND VARIABLE HMA BITUMINOUS BASE

EXISTING TAXIWAY A PAVEMENT
9" PCC PAVEMENT (501)
±4" CRUSHED AGGREGATE BASE (209)
VARIABLE DEPTH BITUMINOUS MILLINGS

EXISTING SOUTH APRON PAVEMENT
9" PCC PAVEMENT (501)
4" AGGREGATE BASE (209)

KEY MAP



Path: K:\Aurora\20228502-00_RehabApronPh4\Drawn\Sheets\20028502 - proposed improvements - 3.dwg
Date: Wednesday, December 1, 2021 3:05:39 PM

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| | | DESIGNED BY: JRM |
| | | DRAWN BY: JRO |
| | | CHECKED BY: DKP |
| | | APPROVED BY: ARM |
| | | COPYRIGHT: |

PROPOSED IMPROVEMENTS - 3

STORM WATER POLLUTION PREVENTION NOTES

THE FOLLOWING IS TO ESTABLISHED BEST MANAGEMENT PRACTICES TO BE INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

SITE DESCRIPTION

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF RECONSTRUCTING EXISTING HMA PAVEMENTS, PCC PAVEMENT REPLACEMENT, TOPSOIL PLACEMENT, SEEDING AND MULCHING AT AURORA MUNICIPAL AIRPORT.

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS EXCAVATION AND GRADING:

1. INSTALL TEMPORARY EROSION CONTROL MEASURES.
2. INSTALL CONCRETE WASHOUT AT CONTRACTOR STAGING AREA.
3. REMOVE EXISTING PAVEMENTS.
4. CONSTRUCT NEW PAVEMENTS.
5. FINAL GRADING AND OTHER MISCELLANEOUS ITEMS.
6. PLACEMENT OF PERMANENT EROSION CONTROL, SUCH AS SEEDING AND MULCH IF REQUIRED.

AREA OF CONSTRUCTION SITE

THE TOTAL AREA OF THE CONSTRUCTION SITE THAT WILL BE DISTURBED BY EXCAVATION, GRADING AND OTHER ACTIVITIES IS LESS THAN ONE (1) ACRE.

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION SITE DRAINS INTO STORM SEWERS THAT OUTLET INTO THE WELCH CREEK.

SEDIMENTATION AND EROSION CONTROL NOTES

THE RESIDENT ENGINEER WILL PERFORM PERIODIC INSPECTION OF THE SITE TO IDENTIFY POTENTIAL SEDIMENT AND EROSION ISSUES.

ALL SOIL EROSION AND SEDIMENT CONTROL PRACTICES ARE REFERENCED FROM THE ILLINOIS URBAN MANUAL, UNLESS OTHERWISE STATED.

THE DRAWINGS, SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE SEEDING AND MULCHING AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED WITHIN SEVEN (7) DAYS OF DISTURBANCE.

DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

1. WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.
2. EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTORS EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN FOURTEEN DAYS.
3. AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:
 - A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.
 - B. EXCAVATED AREAS AND EMBANKMENT AREAS SHALL BE PERMANENTLY SEEDED IMMEDIATELY AFTER FINAL GRADING. IF NOT, THEY SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S COST, IF NO CONSTRUCTION ACTIVITY IN THE AREA IS PLANNED FOR SEVEN DAYS.
4. CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.
5. SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF OFF SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCIDENTAL TO THE COST OF THE PROJECT.
6. THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCIDENTAL TO THE CONTRACT.

DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

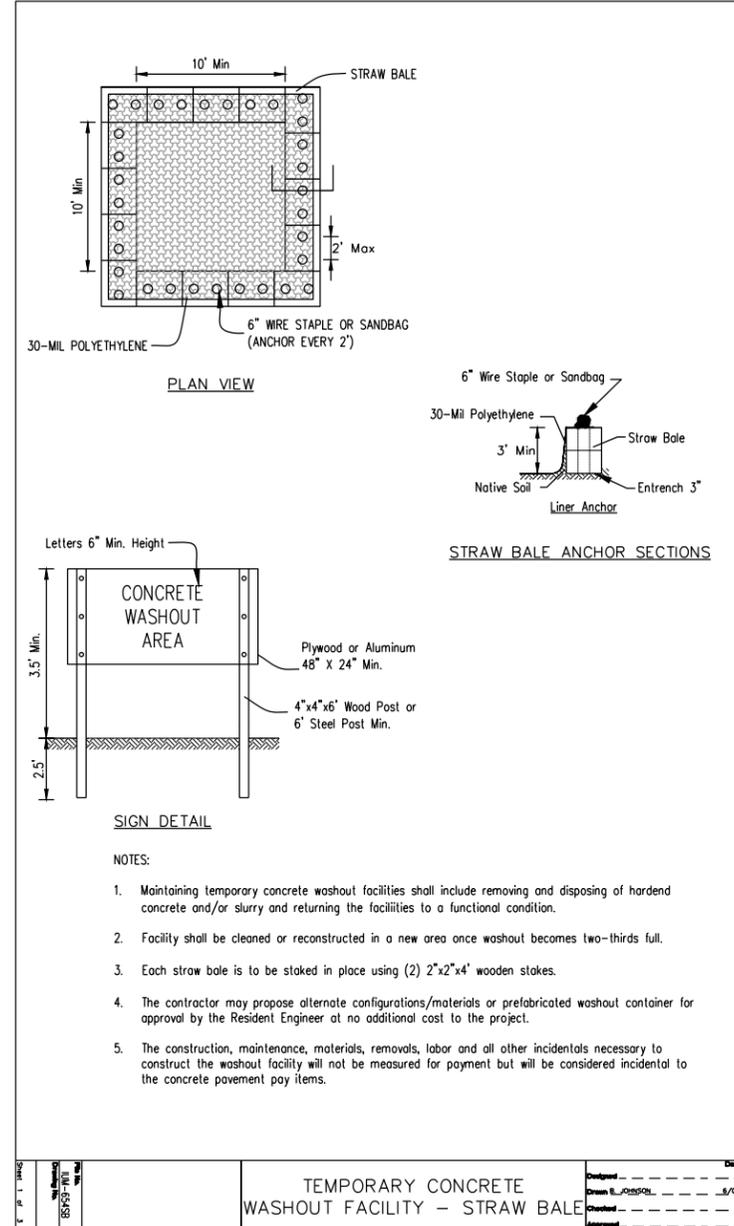
TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED.

ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RESEEDED.

MAINTENANCE AFTER CONSTRUCTION

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

AUTOCAD2006



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CHECKED BY: DKP

APPROVED BY: ARM

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SHEET TITLE

STORM WATER
POLLUTION PREVENTION
NOTES AND DETAILS

