



Illinois Department of Transportation

Office of Intermodal Project Implementation / Division of Aeronautics
1 Langhorne Bond Drive / Springfield, Illinois 62707-8415

January 14, 2022

SUBJECT: Aurora Municipal Airport
Aurora, Illinois
Kane County
Illinois Project Number: ARR-4785
SBG Project Number: 3-17-SBGP-162/171
Contract No. AU071
Item No. 04A, January 21, 2022
Addendum B

NOTICE TO PROSPECTIVE BIDDERS

Attached is an addendum to the plans or proposal. This addendum involves revised and/or added material.

Reason for Addendum:

Construction plan and special provision revisions issued as Addendum B.

To All Plan Holders:

Plan Changes:

1. Replace Existing Pavement Jointing Details Sheet 14 with Revised Plan Sheet 14:
 - Revised detail Contraction Joint – Detail A material callout to remove references of jet-fuel resistant sealant and included material meeting ASTM D 6690.
 - Revised detail Construction Joint – Detail A material callout to remove references of jet-fuel resistant sealant and included material meeting ASTM D 6690.
 - Revised New Type A Isolation Joint material callout to remove references of jet-fuel resistant sealant and included material meeting ASTM D 6690.

Special Provisions Changes:

1. Remove and Replace the Third paragraph of Section 605-1.1 of the Special Provisions with the following:

This section shall apply to all concrete/concrete interface joint sealing.
The Contractor shall be limited to either hot applied sealant or silicone joint sealant only.

2. Remove and Replace Section 605-2.1 Joint Sealants of the Special Provisions with the following:

DELETE: The paragraphs b and d.

ADD: To paragraph c: The silicone joint sealing material shall be single-component, low-modulus, neutral-curing, non-sag silicone sealant complying with ASTM D5893 for Type NS.

Schedule of Prices Changes:

No changes.

Prime contractors must utilize the enclosed material when preparing their bid and must include any changes to the Schedule of Prices in their bid.

Questions on this addendum may be directed to Antonio Marin of Crawford, Murphy & Tilly, Inc. at 630-907-7041.

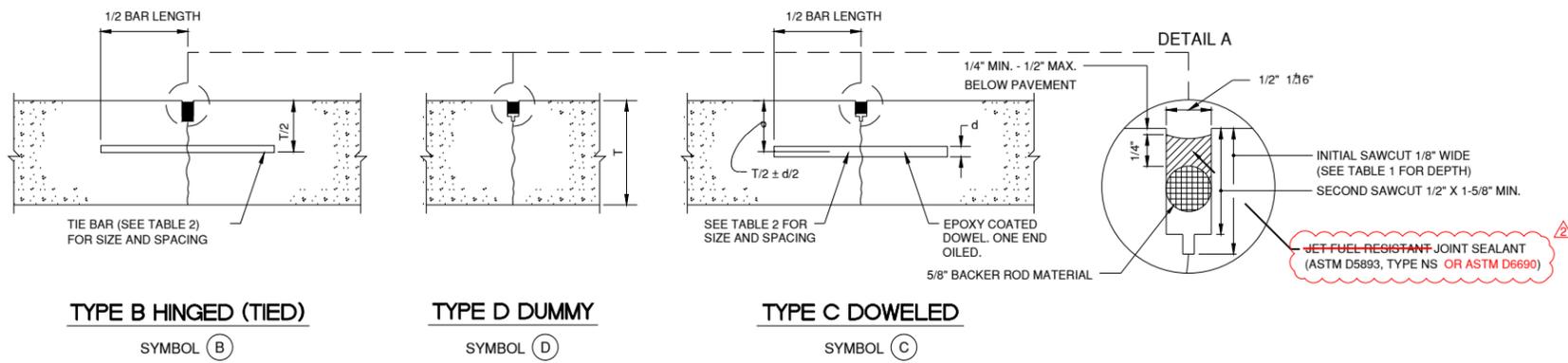
MARK	DATE	DESCRIPTION
▲	1/10/22	ADDENDUM B

AIP PROJ. NO.	3-17-SBGP-162/TBD
IL PROJ. NO.	ARR-4785
CMT PROJECT NO.	200285-02
CAD DWG FILE:	20028502 - JOINTING DETAILS.DWG
DESIGNED BY:	ARM
DRAWN BY:	JRO
CHECKED BY:	DKP
APPROVED BY:	ARM
COPYRIGHT:	

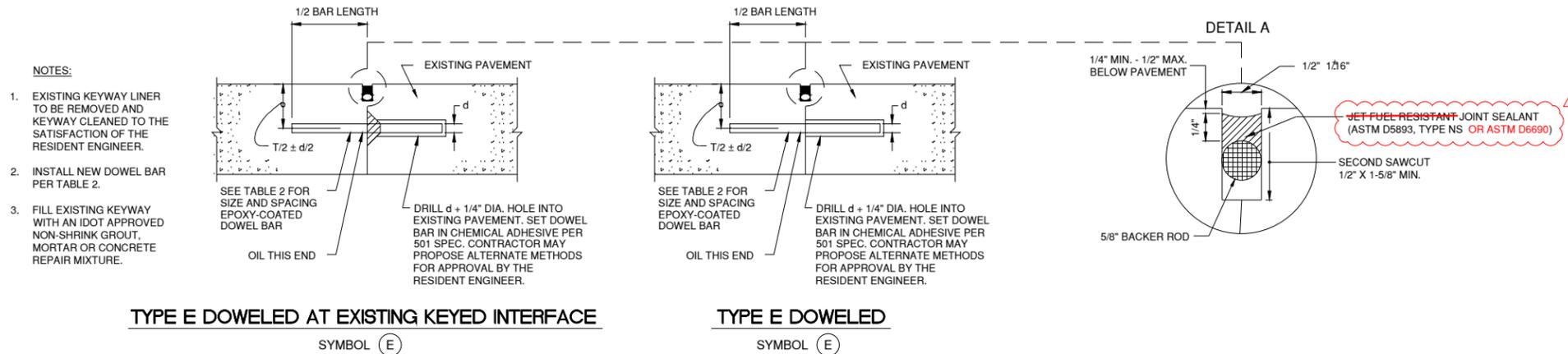
SHEET TITLE
**PAVEMENT JOINTING
DETAILS**

JOINT NOTES

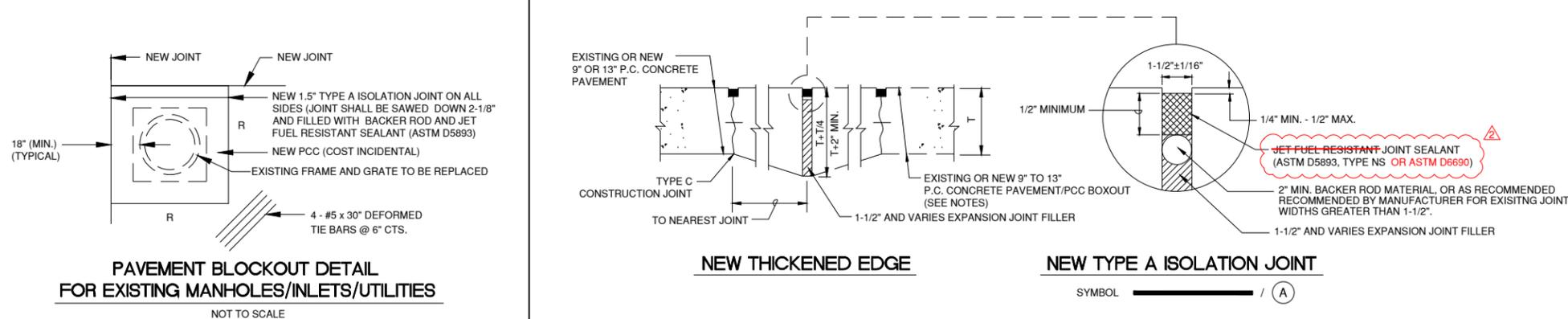
- ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
- THE INITIAL SAWCUT FOR ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS SHALL BE SAWS AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT. SAWING OF LONGITUDINAL CONTRACTION JOINTS ADJACENT TO THE THICKENED EDGES SHALL BE GIVEN PRIORITY OVER OTHER LONGITUDINAL JOINT SAWING.
- ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY, WHICH WILL INSURE THAT THEY WILL REMAIN PARALLEL TO THE PAVEMENT LANES. THE DOWEL BAR ASSEMBLIES SHALL BE APPROVED BY THE ENGINEER PRIOR TO INSTALLATION.
- ALL TIE BARS AND MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR PLACED BY OTHER APPROVED METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT.
- TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH ASTM A615 OR ASTM A616, EXCEPT THAT RAIL STEEL BARS, GRADE 50 OR 60 SHALL NOT BE USED FOR THE BARS THAT ARE TO BE BENT OR RESTRAIGHTEND DURING CONSTRUCTION. TIE BARS DESIGNATED AS GRADE 40 IN ASTM A615 CAN BE USED FOR CONSTRUCTION REGARDING BENT BARS.
- THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSION OF THE SECOND SAWCUT WILL NOT BE ALLOWED.
- JOINTS SHALL BE DRY AND CLEAN BEFORE SEALING OPERATIONS BEGIN.
- COST OF ALL JOINT SAWING, CLEANING AND SEALING OF NEW CONCRETE PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ASSOCIATED PAY ITEM AND NO SEPARATE PAYMENT SHALL BE MADE.
- SHOULD THE POURING OPERATIONS REQUIRE THE INSERTION OF AN INTERMEDIATE HEADER, A DOWEL BASKET ASSEMBLY OR OTHER APPROVED METHOD OF DOWEL BAR PLACEMENT SHALL BE REQUIRED.
- DOWEL BASKET ASSEMBLIES MEETING IDOT APPROVAL MAY BE PROPOSED BY THE CONTRACTOR TO BE APPROVED BY THE ENGINEER. DOWELS IN THE APPROVED BASKET ASSEMBLIES SHALL CONFORM TO TABLE 2.



CONTRACTION JOINTS



CONSTRUCTION JOINTS



- ABOVE BLOCKOUT SHALL BE CONSTRUCTED FOR EXISTING MANHOLE/ INLET/UTILITIES CONSTRUCTION. DIMENSION OF BOXOUT MAY VARY TO FIT FIELD CONDITIONS.
- R - DENOTES ODD SHAPED REINFORCED PANELS TO BE REINFORCED WITH WIRE FABRIC AS SHOWN ON THIS SHEET. ALL NON RECTANGULAR SHAPED PANELS SHALL BE REINFORCED. (REINFORCEMENT NOT SHOWN).
- ALL CONCRETE AND OTHER MATERIALS SHALL BE IN CONFORMANCE WITH THE SPECIFICATION FOR ITEMS 501 AND 605.

NOTES

- THE CONTRACTOR SHALL VISIT THE PROJECT SITE AND ACQUAINT HIMSELF WITH THE REQUIRED WORK. CONTRACTOR SHALL BE FAMILIAR TO THE SITE AND REQUIRED WORK PRIOR TO BID. SITE VISITS SHALL BE COORDINATED WITH THE AIRPORT MANAGER AT (630) 256-3120.
- PRIOR TO ORDERING NEW MATERIAL, CONTRACTOR SHALL FIELD MEASURE FOR JOINTING MATERIAL SIZES AND AMOUNT. MEASUREMENTS SHALL ALSO DETERMINE BACKER ROD SIZE AND QUANTITIES. ALSO PER NEW JOINTS DETAILS, DEPTH OF NEW JOINT SEALANT SHALL BE AS RECOMMENDED BY MANUFACTURER. NO EXTRA COMPENSATION WILL BE MADE FOR EXCESS MATERIALS NOT USED.
- FOR VARYING JOINT WIDTHS AND DEPTHS, THERE SHALL BE NO ADDITIONAL COST FOR DEEPER SAWCUTS, LARGER BACKER RODS AND SEALANT MATERIAL USED.

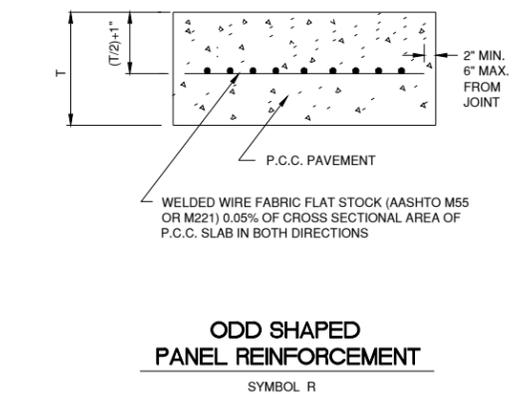
TABLE 1

PAVEMENT THICKNESS T - INCHES	DEPTH OF CONTRACTION JOINT INITIAL SAW CUT T, INCHES T=(T/4) ±1/4"
9	2.25"
10	2.50"
11	2.75"
12	3"
13	3.25"

TABLE 2

PAVEMENT THICKNESS T - INCHES	DOWEL BAR DETAILS			TIE BAR DETAILS		
	DIA. (d)	LENGTH	SPACING	BAR SIZE	LENGTH	SPACING
8' - 12"	1"	19"	12"	#5	30"	30"

DIMENSION TABLES



NOTE: REINFORCEMENT SHALL NOT CROSS ANY JOINT

