FED. ROAD DIST. NO. T		TILLINGE	DES FED. AID PRELIFCY		
STA.		1	TO STA.		
90/94	2004-075RS		COOK	65	48
F. A. RTE.	SECTION		COUNTY	TOTAL. SHEETS	SHEET NO

62814

NOTES :

VARIES 3.6 m (12'-0") TO 4.8 m (15'-0") (TYP.)

-75 (3) TYP.

0.6 m (2'-0") MIN. TYP.

SAW CUT OF

CURB & GUTTER

CIRCULAR JOINT.

SUITABLE QUARTER-CIRCULAR FORM

16 No. 20 (6) DEFORMED BARS EQUALLY SPACED DRILLED AND ——
GROUTED IN PLACE 450 (18) LONG

2 No. 15 (4) 2.2 m (7'-0") TYP.

T. MATOUSEK 08-28-00
T. MATOUSEK 10-02-00
T. MATOUSEK 04-25-02
P. LAFLEUR 08-27-02

DETAIL B

FRAME & GRATE EXTENDING

±300 (12) INTO PAVEMENT

5 No. 20 (6) DEFORMED BARS 450 (18) LONG

DETAIL C

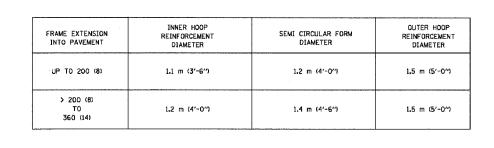
(USE WHEN SEMI-CIRCLULAR FORM LESS THAN

600 (24) TO CONTRACTION JOINT)

1. THE ROUNDOUT AND ADDED REINFORCEMENT WILL NOT BE PAID SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE PAVEMENT.

- TRANSVERSE JOINTS MAY BE MOVED TO ACCOMMODATE ROUNDOUT, EDGE OF CIRCULAR JOINT SHALL BE MINIMUM 300 (12) FROM TRANSVERSE JOINT. RELOCATED TRANSVERSE JOINT SHALL BE CONTINUOUS FROM EDGE OF PAVEMENT TO EDGE OF PAVEMENT.
- 3. SEMI-CIRCULAR FORM SHALL BE REMOVED PRIOR TO DRILL AND GROUT OF TIE BARS.
- 4. ALL REINFORCED BARS SHALL BE EPOXY COATED.
- 5. DRILL AND GROUT IS PREFERRED, HOWEVER TIE BARS CAN BE POURED IN PLACE IF CLEARANCE IS PROVIDED TO OUTER EDGE OF FRAME. MINIMUM 50 (2) CLEARANCE.
- 6. WOOD SHIMS SHALL BE USED TO ADJUST ALL FRAMES. AFTER ADJUSTING MORTAR HAS CURED, THE WOOD SHIMS SHALL BE REMOVED AND THE VOIDS UNDER THE FRAMES FILLED WITH NON SHRINK GROUT.
- 7. HOOP REINFORCEMENT SHALL BE ONE PIECE CONSTRUCTION.
- 8. CIRCULAR FRAMES AND GRATES MAY BE SUBSTITUTED.
- 9. CURB DOWELS MUST BE PLACED LEVEL & TRUE TO ALLOW CONTRACTION MOVEMENT.

3.3



THIS DETAIL IS TO BE USED WHEN THE GUTTER FLAG IS VARIES 3.6 m (12'-0") TO 4.8 m (15'-0") (TYP.) 3 NO. 20 (6) DEFORMED INNER HOOP BAR TO EXTEND 75 (3) TYP. BARS 450 (18) LONG INTO GUTTER (TYP.) 75 (3) TYP. CONTRACTION DOWEL BAR No. 25 (B) (ON ONE SIDE) 450 (18) LONG SUITABLE SEMI-CIRCULAR FORM OUTER HOOP DETAIL D (USE WHEN FRAME AND GRATE AT CONSTRUCTION JOINT) SEE SAWED CONTRACTION JOINT (TYP.) - SEMI-CIRCULAR NO. 20 (NO. 6) BAR PLACED @ PAVEMENT MIDPOINT NO. 15 (4) 300 (12") LONG BARS TO BE POUNDED INTO SUBGRADE AS CHAIRS MIN, 5 FOR OUTER HOOP INNER HOOP

TRANSITION TAPER (3:1 TYP.)

ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE NOTED

ILLINOIS DEPARTMENT OF TRANSPORTATION

PCC PAVEMENT ROUNDOUTS AT **CURB AND GUTTER**

SCALE: NONE DATE 10/18/2002

DRAWN BY: TOM MATOUSEK CHECKED BY: A. ABBAS

BD-48

REVISION DATE: 10/02/00

LEGEND:

DESIGNER NOTE:

- - - - - - CASTING

TRANSITION CURB & AND GUTTER INTO PARKWAY (PREFERRED BECAUSE PAVING OF MAINLINE NOT AFFECTED.)

0.6 m RAD. (2'-0") TYP.

INNER HOOP MAY REST ON DOWEL BAR (TIE BAR AT LONGITUDINAL JOINT) BUT SHALL NOT INTERFERE IN THE ALIGNMENT.

SAW CUT OR EXPANSION JOINT

2 No. 15 1200 (48) LONG

WITH 2 DOWEL BARS (TYP.)

75 (3) CLEARANCE (TYP.)

| INNER PAVEMENT MAY BE POURED MONOLITHICALLY

WITH CURB & GUTTER

DETAIL A

FRAME & GRATE EXTENDING

150 (6) INTO PAVEMENT