

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

F.A.L. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(57-24 & 57-24-1)RS-1	MCLEAN	48	1
		ILLINOIS	CONTRACT NO. 70A60	

FOR INDEX OF SHEETS, SEE SHEET NO. 2  
FOR SUMMARY OF QUANTITIES, SEE SHEET NO. 5 THROUGH 9

# PROPOSED HIGHWAY PLANS

FAI ROUTE 74 (I-74)  
SECTION (57-24 & 57-24-1)RS-1  
PROJECT ACNHPP-0074(323)  
RESURFACING (INT-2ND) RAMP REPAIR  
MCLEAN COUNTY

C-95-041-16

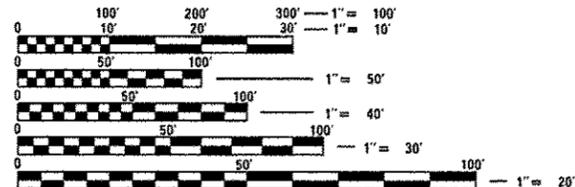
E OF LEROY TO DEWITT CO LINE

**CURRENT TRAFFIC DATA**

	LEG "A"	LEG "B"	LEG "C"	LEG "D"	LEG "E"	LEG "F"
2016 ADT	21,100	20,700	450	450	375	375
P.U. %	67.1	66.1	75.4	77.4	69.7	65.3
S.U. %	6.6	7.1	9.0	9.7	13.6	9.2
M.U. %	26.3	26.3	15.6	12.9	16.7	25.5

LEG "A" IS I-74 FROM LEROY TO US 136.  
LEG "B" IS I-74 FROM US 136 TO THE MCLEAN/DEWITT COUNTY LINE.  
LEG "C" IS I-74 EB TO US 136 RAMP.  
LEG "D" IS US 136 TO I-74 WB RAMP.  
LEG "E" IS US 136 TO I-74 EB RAMP.  
LEG "F" IS I-74 WB TO US 136 RAMP.

DESIGN DESIGNATION  
N/A



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
1-800-892-0123  
OR 811  
EMPIRE & WEST TOWNSHIPS

PROJECT ENGINEER: NANCY FASIG  
SQUAD LEADER: BRIAN J. HOGAN  
DESIGNER: BILLY J. MURPHY  
PHONE NUMBER: (217)-465-4181  
CONTRACT NO. 70A60

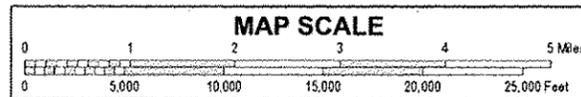
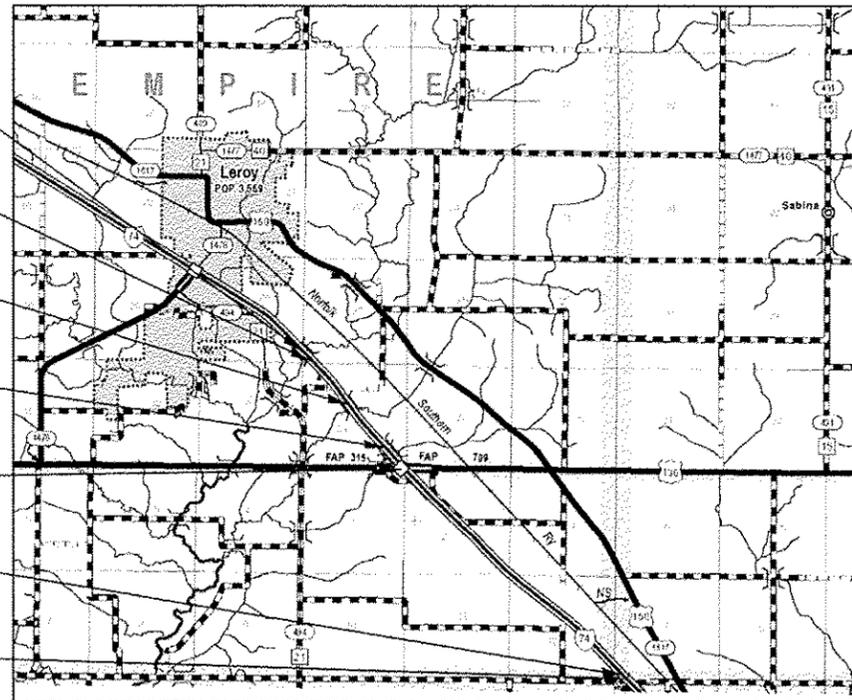
S.N. 057-0133 I-74 EB  
S.N. 057-0134 I-74 WB  
STA. 376+87.50  
  
PROJECT BEGINS  
STA. 377+89.05 I-74 EB  
STA. 377+84.95 I-74 WB

S.N. 057-2029  
STA. 417+50.00  
  
S.N. 057-2030  
STA. 446+67.00

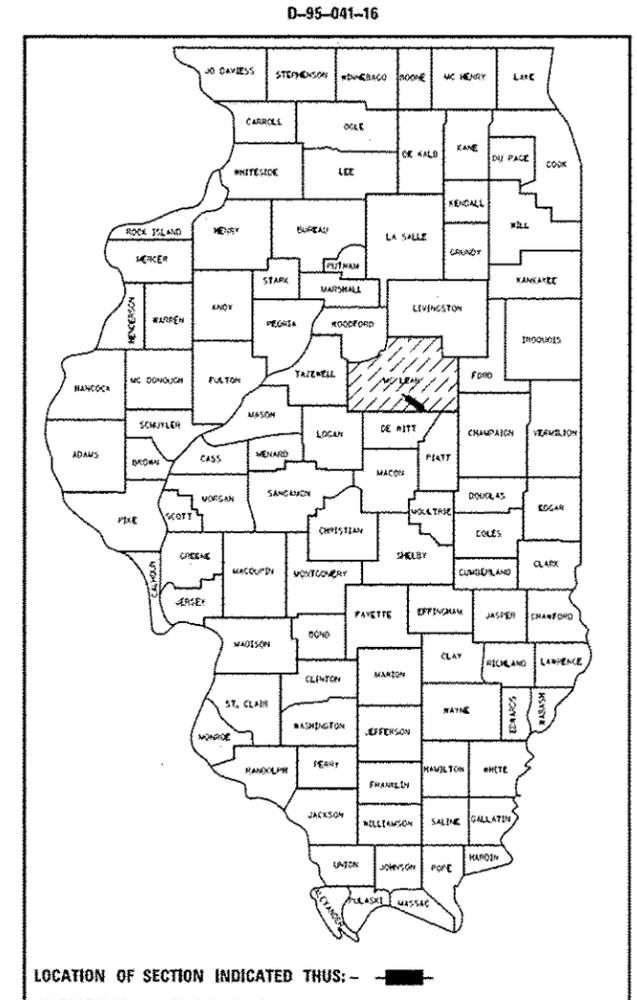
OVERHEAD  
S.N. 057-0135  
STA. 455+63.96

STATION EQUATION  
STA. 610+00.91 BK =  
STA. 633+68.77 AH

PROJECT ENDS  
STA. 634+40.00 I-74 EB  
STA. 609+20.00 I-74 WB



GROSS LENGTH = 23,283.09 FT. = 4.410 MILES  
NET LENGTH = 23,283.09 FT. = 4.410 MILES



STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION  
  
SUBMITTED OCTOBER 17 20 16  
Kensil A. Barnett  
REGIONAL ENGINEER  
  
Dec 9 20 16  
Maureen M. Addis  
ENGINEER OF DESIGN AND ENVIRONMENT  
  
Dec 9 20 16  
Maureen M. Addis  
DIRECTOR OF PROGRAM DEVELOPMENT

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OF THE STATE OF ILLINOIS



# GENERAL NOTES

G.N.-100  
ENGLISH UNITS OF MEASUREMENT SHALL GOVERN OVER AND SUPERSEDE ANY METRIC UNITS SHOWN IN THIS CONTRACT. WHERE INCLUDED, METRIC UNITS ARE FOR INFORMATION ONLY.

G.N.-100A  
ELECTRONIC FILES AND/OR ELECTRONIC SURVEY INFORMATION INCLUDING CADD FILES WILL NOT BE AVAILABLE TO THE CONTRACTOR.

G.N.-105.09A  
ALL ELEVATIONS SHOWN IN THE PLANS ARE BASED ON NORTH AMERICAN VERTICAL DATUM OF 1988. (NAVD 88)

G.N.-406  
THE QUANTITIES INCLUDED IN THE PLANS FOR HOT-MIX ASPHALT RESURFACING ARE INTENDED TO GIVE THE COVERAGE SHOWN ON THE TYPICAL CROSS SECTIONS. IT IS NOT INTENDED TO INCREASE THE THICKNESS OF THE HOT-MIX ASPHALT MIXTURE IN ORDER TO USE ALL OF THE QUANTITIES INCLUDED IN THE CONTRACT.

GN 406H Mixture Requirements                      Contract:                      70A60

Location	I-74 and Ramp	I-74	I-74 and Ramp	I-74 and Ramp
Mixture Use	Polymer Surface	Polymer Binder	Shoulder and Incidental	Class D Patch
AC/PG	SBS PG 70-22	SBS PG 70-22	PG 64-22	PG 64-22
Design Air Voids	4.0% @ Ndes=90	4.0% @ Ndes=90	4.0% @ Ndes=30	4.0% @ Ndes=90
Mix Comp(Gradation)	IL 9.5	IL 19.0	IL 9.5L	IL 19.0
Friction Aggregate	Mix D	N.A.	Mix C	N.A.
Mixture Weight	112	112	112	112
Quality Management Program	PFP	PFP	QC/QA	QC/QA
Sublot Size	1000	1000	N.A.	N.A.

G.N.-440B  
THE EXISTING TIE BARS BETWEEN THE EXISTING PAVEMENT AND EXISTING MEDIANS, GUTTERS AND/OR COMBINATION CURB AND GUTTERS THAT ARE FOUND SUITABLE FOR REUSE SHALL BE CLEANED, STRAIGHTENED AND INCORPORATED INTO THE NEW CONSTRUCTION. ANY EXISTING TIE BARS THAT ARE FOUND UNSUITABLE TO BE INCORPORATED INTO THE PROPOSED CONSTRUCTION DUE TO EXCESSIVE RUSTING OR DISTRESS SHALL BE REMOVED FLUSH WITH THE FACE OF THE EXISTING CONCRETE AND DISPOSED OF OUTSIDE THE LIMITS OF THE RIGHT-OF-WAY IN ACCORDANCE WITH ARTICLE 202.03 OF THE STANDARD SPECIFICATIONS.

THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCLUDED IN THE VARIOUS REMOVAL PAY ITEMS AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

G.N.-442B -- PATCHING SCHEDULES  
THE PATCHING SCHEDULES INCLUDED IN THE PLANS REPRESENT THE BEST INFORMATION AVAILABLE AT THE TIME OF COMPLETION OF THE PLANS FOR LETTING. VARIATIONS IN LOCATION AND SIZES OF BOTH FULL-DEPTH AND PARTIAL-DEPTH PATCHES MAY OCCUR.

G.N.-482  
ALL LOW ESAL MIXTURE PLACED AS HOT-MIX ASPHALT SHOULDERS SHALL BE COMPACTED TO 94.0 – 98.4 PERCENT OF THE MAXIMUM THEORETICAL DENSITY. THIS REQUIREMENT SHALL APPLY TO IL 9.5L GRADATION MIXES. THIS MAXIMUM DENSITY SHALL BE DETERMINED FROM THE MOVING AVERAGE OF FOUR TESTS AS IN OTHER QC/QA TESTING. A NUCLEAR GAUGE DENSITY/CORE CORRELATION SHALL BE PERFORMED FOR THE IL 9.5L USING STANDARD CORRELATION PROCEDURES WHEN MORE THAN 3,000 TONS ARE TO BE PLACED.

G.N.-667  
THE RESIDENT ENGINEER SHALL CONTACT THE PROGRAM DEVELOPMENT CHIEF OF SURVEYS PRIOR TO THE PRE-CONSTRUCTION CONFERENCE FOR INSTRUCTION AS TO SETTING OF TEMPORARY OR PERMANENT TIES FOR CENTERLINE ALIGNMENT CONTROL SURVEY MARKERS (PC'S, PT'S, AND PI'S). PROJECT IMPLEMENTATION PERSONNEL WILL BE RESPONSIBLE FOR LAYOUT OF THESE MARKERS.

G.N.-703A  
SHORT TERM PAVEMENT MARKING SHALL BE APPLIED TO THE PAVEMENT AFTER ANY OF THE FOLLOWING: COLD MILLING AND/OR PLACING BITUMINOUS MATERIALS (PRIME COAT), LEVELING BINDER (MACHINE METHOD), BINDER AND SURFACE COURSES. SHORT TERM PAVEMENT MARKING PLACED ON THE SURFACE, SHALL COINCIDE WITH THE FINAL PAVEMENT STRIPING. SHORT TERM PAVEMENT MARKING PLACED PRIOR TO THE SURFACE SHALL COINCIDE WITH THE EXISTING PAVEMENT MARKINGS. USE 4 FEET PER 40 FEET (OR 10% PER STATION).

G.N.-781 - SPL  
RAISED REFLECTIVE PAVEMENT MARKERS SHALL BE PLACED IN ACCORDANCE WITH STANDARD 781001, AND THE DETAILS SHOWN IN THE PLANS. IF THERE IS ANY DISCREPANCY BETWEEN THE STANDARD AND THE DETAILS IN THE PLANS, THE DETAILS IN THE PLANS SHALL GOVERN. THE FINAL PAVEMENT MARKINGS SHALL BE IN PLACE PRIOR TO PLACING THE RAISED REFLECTIVE PAVEMENT MARKERS.

PAVEMENT MARKERS ON INTERSTATE 74 SHALL BE PLACED TWO (EACH) AT EVEN SPACING IN EVERY OTHER 30 FOOT (9 m) SPACE BETWEEN THE DASHED CENTERLINE STRIPES.

G.N.-1004.01  
COARSE AGGREGATE GRADATION CA-10 MAY BE USED WHENEVER COARSE AGGREGATE CA-6 IS SPECIFIED IN THE STANDARD SPECIFICATIONS

NO COMMITMENTS

# STRUCTURE INFORMATION

## STRUCTURE INFORMATION

### HMA SURFACE REMOVAL 5" & HMA RESURFACING 5" (MAINLINE)

S.N. 057-2029 - DOUBLE BARREL 11'-0" X 8'-0" RCBC STA.417+50  
 S.N. 057-2030 - DOUBLE BARREL 10'-0" X 5'-0" RCBC STA.446+67  
 S.N. 057-8569 - SINGLE BARREL 8'-0" X 3'-0" RCBC STA.554+00  
 S.N. 057-8570 - DOUBLE 38" X 24" RCCP STA.596+00

### MATERIAL TRANSFER DEVICE (MAINLINE)

#### UNLOADED ONLY

S.N. 057-0133; I-74 EB OVER N FORK SALT CREEK STA.376+87.50  
 S.N. 057-0134; I-74 WB OVER N FORK SALT CREEK STA. 376+87.50  
 S.N. 057-2029 - DOUBLE BARREL 11'-0" X 8'-0" RCBC STA.417+50  
 S.N. 057-2030 - DOUBLE BARREL 10'-0" X 5'-0" RCBC STA.446+67  
 S.N. 057-8569 - SINGLE BARREL 8'-0" X 3'-0" RCBC STA.554+00  
 S.N. 057-8570 - DOUBLE 38" X 24" RCCP STA.596+00

### MATERIAL TRANSFER DEVICE (U.S. 136)

#### UNLOADED ONLY

S.N. 057-0135; U.S. 136 OVER I-74; U.S. 136 STA.50+00.00, I-74 STA.455+53.96

FILE NAME =	USER NAME = piersonbr	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STRUCTURE INFORMATION</b>			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 5\Projects\0570A60\Drawings\Design\0570A60-sh-structure.dwg	DESIGNED -	REVISED -	REVISED -					74	(57-24&57-24-DRS-1	MCLEAN	48	4
*MODELNAME*	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -		SCALE: N/A    SHEET 1 OF 1 SHEETS    STA.    TO STA.			CONTRACT NO. 70A60				
	PLOT DATE = 10/6/2016	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							







# SUMMARY OF QUANTITIES

LOCATION OF WORK:

I-74

RURAL

INTERSTATE

STA. 377+84.95 TO STA. 634+40.00

MCLEAN COUNTY

90% FEDERAL / 10% STATE

FUNDING BREAKOUT:

CONSTRUCTION TYPE CODE:

0005

CODE NO.	ITEM	UNIT	TOTAL QUANTITY
X4402020	CONCRETE MEDIAN SURFACE REMOVAL	SQ FT	250.0
X6350120	DELINEATOR REMOVAL	EACH	90.0
X7010216	TRAFFIC CONTROL AND PROTECTION, (SPECIAL)	LSUM	1.0
<del>X7010410</del>	<del>SPEED DISPLAY TRAILER</del>	<del>CAL MO</del>	<del>8.0</del>
X7015005	CHANGEABLE MESSAGE SIGN	CAL DA	28.0
X7200201	WIDTH RESTRICTION SIGNING	LSUM	1.0
* X7830060	GROOVING FOR RECESSED PAVEMENT MARKING, LETTERS AND SYMBOLS	SQ FT	111.0
* X7830070	GROOVING FOR RECESSED PAVEMENT MARKING 5"	FOOT	108,750.0
* X7830074	GROOVING FOR RECESSED PAVEMENT MARKING 7"	FOOT	360.0
* X7830076	GROOVING FOR RECESSED PAVEMENT MARKING 9"	FOOT	3,260.0
* X7830078	GROOVING FOR RECESSED PAVEMENT MARKING 13"	FOOT	290.0
* X7830090	GROOVING FOR RECESSED PAVEMENT MARKING 25"	FOOT	75.0
XZ193300	SURVEY MARKER, TYPE 1 (SPECIAL)	EACH	1.0
XZ193400	SURVEY MARKER, TYPE 2 (SPECIAL)	EACH	2.0

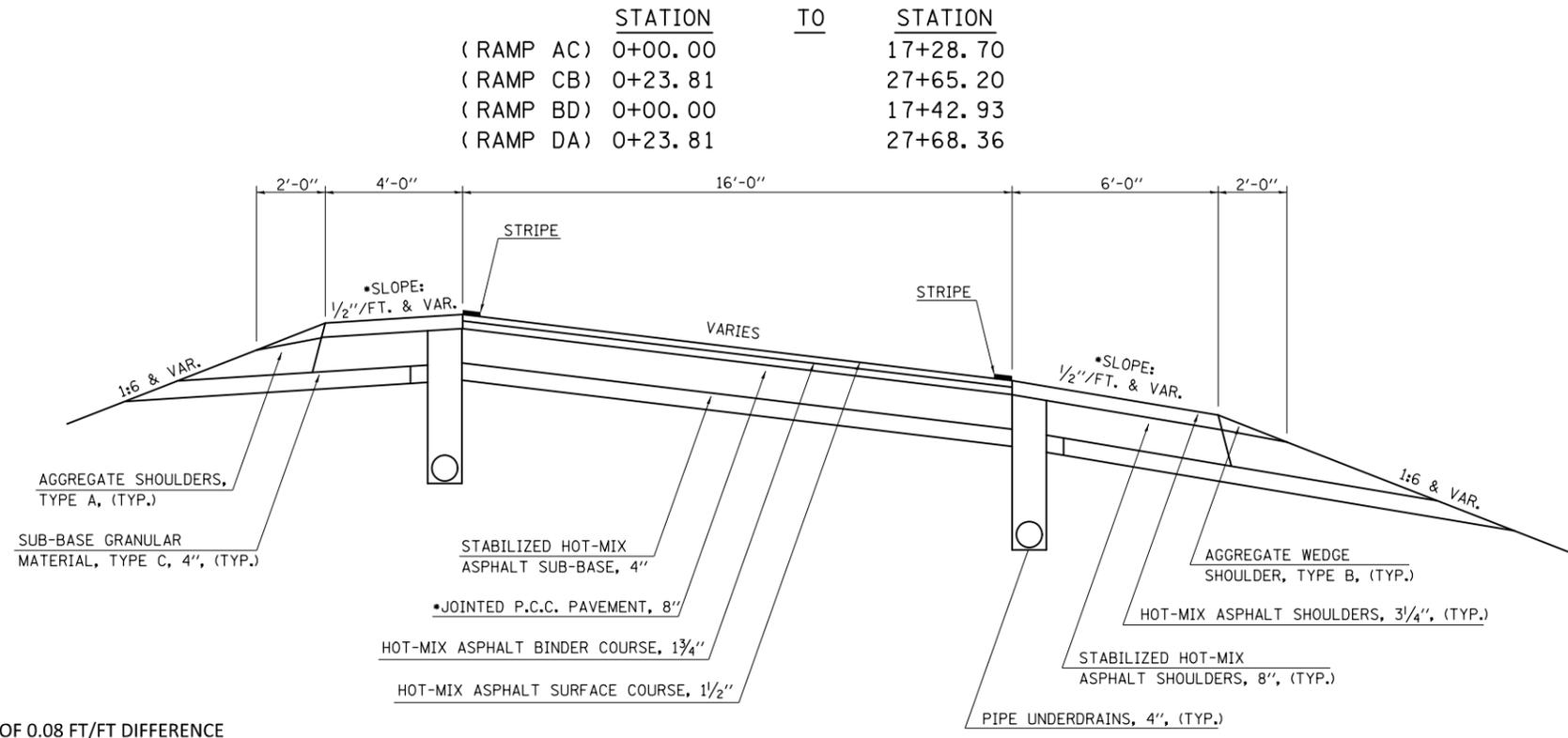
\* DENOTES SPECIALTY ITEM

14

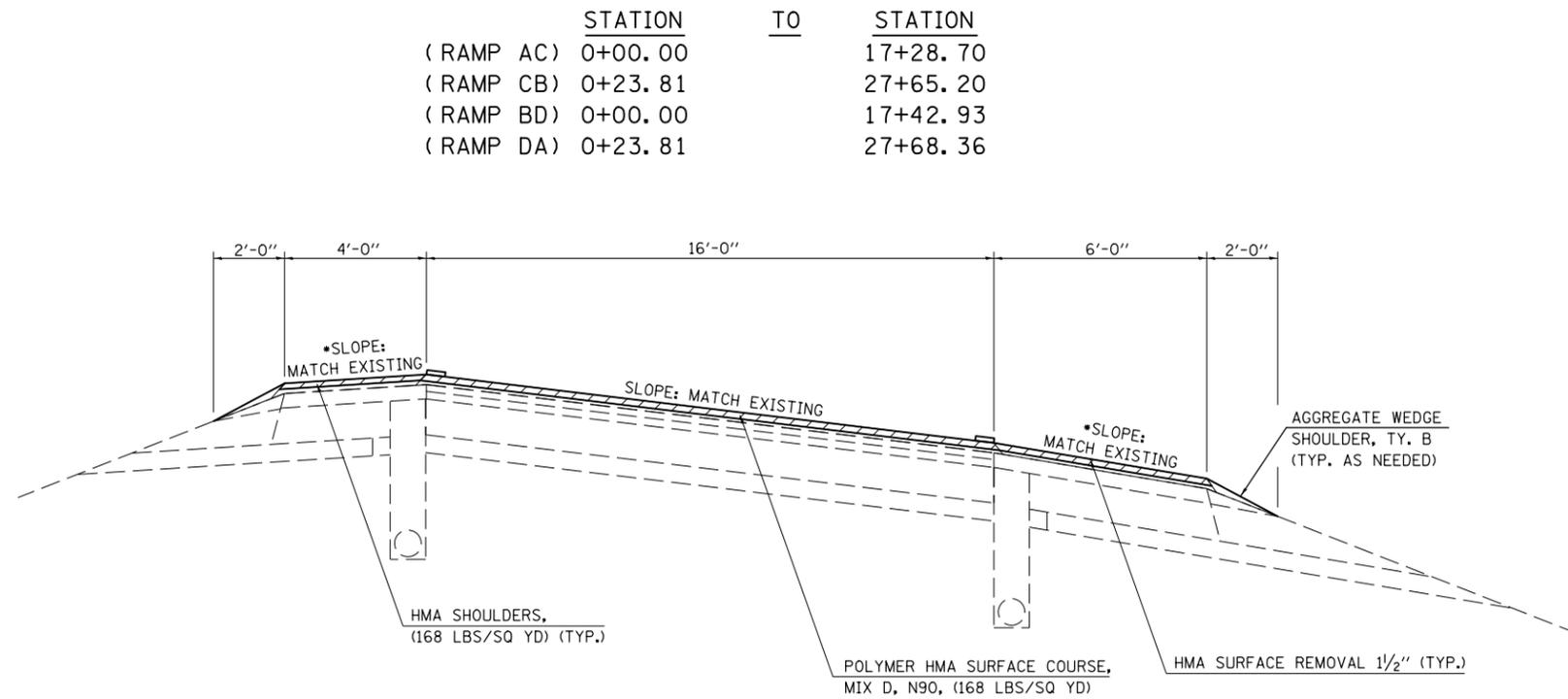




## ② EXISTING TYPICAL RAMP CROSS SECTION



## ② PROPOSED TYPICAL RAMP CROSS SECTION



FILE NAME =	USER NAME = hoganbj	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TYPICAL CROSS SECTION</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
<p style="font-size: small;">p:\11\084EBID\INTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 5\Projects\057\BROWND\Design\0570A60-Sht-Typical.dwg</p>						74	(57-24 & 57-24-1)RS-1	MCLEAN	48	11
<p style="font-size: small;">PLOT SCALE = 40.0000' / in.</p>						CONTRACT NO. 70A60				
<p style="font-size: small;">#MODELNAME#</p>						ILLINOIS FED. AID PROJECT				
<p style="font-size: small;">PLOT DATE = 10/17/2016</p>						SCALE: SHEET 2 OF 2 SHEETS STA. TO STA.				



# STAGE III RESURFACING F.A.I. 74

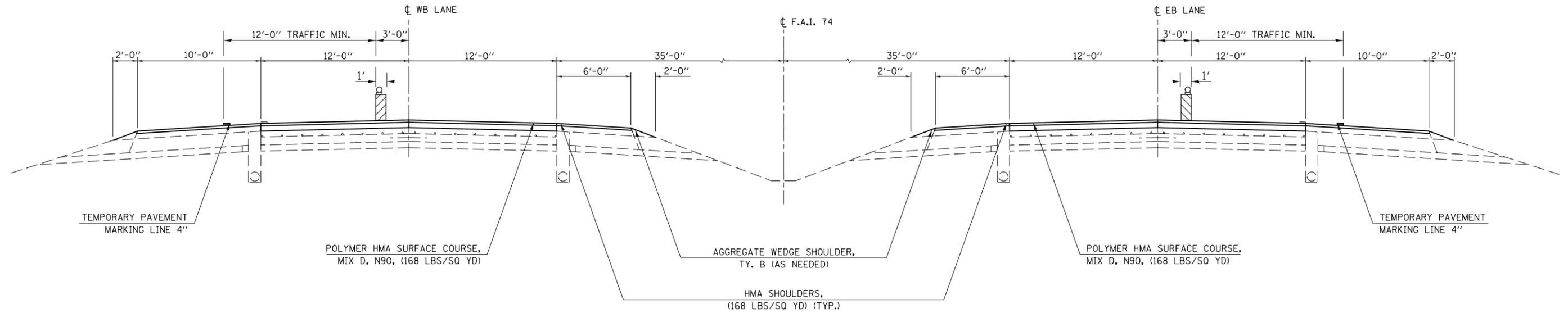
## PASSING LANE SURFACE COURSE CONSTRUCTION

### WESTBOUND LANES

STATION TO STATION  
377+84.95 TO 609+20.00

### EASTBOUND LANES

STATION TO STATION STATION EQUATION:  
377+89.05 TO 610+00.91 BK. STA. 610+00.91 BK.  
633+68.77 AH. 634+40.00 = STA. 633+68.77 AH



FILE NAME =	USER NAME = hoganbj	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PROPOSED STAGING PLAN</b>			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG\illinois.gov\PI\DOT\Documents\DOT Offices\District 5\Projects\057\DRAWING\Design\0570A60-Shr-Typical.dwg	DRAWN BY =	CHECKED -	REVISED -					74	(57-24 & 57-24-1)RS-1	MCLEAN	48	13
#MODELNAME#	PLOT SCALE = 40.0000' / in.	DATE -	REVISED -		CONTRACT NO. 70A60			ILLINOIS FED. AID PROJECT				
	PLOT DATE = 10/17/2016				SCALE:	SHEET 2	OF 2 SHEETS	STA.	TO STA.			

# SCHEDULE OF QUANTITIES

## AGGREGATE WEDGE SHOULDER AND RUMBLE STRIP

LOCATION	STA	TO	STA	LENGTH(')	AREA*(SQ FT)	VOLUME(CU YD)	AGG WEDGE SHLDS TYPE B 48102100 (TON)	SHOULDER RUM STRIP 16 64200116 (FOOT)	
EB OFF RAMP 'AC'	0+00		17+29.51	1729.51	0.25	16.0	28.8		
EB ON RAMP 'CB'	0+23.00		27+65.20	2742.2	0.25	25.4	45.7		
WB ON RAMP 'DA'	0+23.00		27+68.36	2745.36	0.25	25.4	45.8		
WB OFF RAMP 'BD'	0+00		17+43.74	1743.74	0.25	16.1	29.1		
EB DRIVING SHLDR	377+89.05		433+92.92	5603.9				5603.9	
EB DRIVING SHLDR	440+55.66		463+63.30	2307.6				2307.6	
EB DRIVING SHLDR	475+45.45		610+00.91Bk	13455.5				13455.5	
EB DRIVING SHLDR	633+68.77Ah		634+40.00	71.2				71.2	
EB PASSING SHLDR	377+89.05		610+00.91Bk	23211.9				23211.9	
EB PASSING SHLDR	633+68.77Ah		634+40.00	71.2				71.2	
WB DRIVING SHLDR	377+84.95		435+77.50	5792.6				5792.6	
WB DRIVING SHLDR	447+60.63		470+90.57	2329.9				2329.9	
WB DRIVING SHLDR	477+54.98		609+20.00	13165.0				13165.0	
WB PASSING SHLDR	377+84.95		609+20.00	23135.1				23135.1	
**I-74 SHLDS	UNSPECIFIED LOCATIONS							100.0	
TOTAL =							249.3	89143.9	
ROUND TO							250.0	89,150.0	

\*ASSUME RAMP AGGREGATE SHOULDERS ARE 2" LOW AT THE EDGE OF PAVEMENT TO 1" LOW 2' OUTSIDE  
 AVERAGE THICKNESS = 1.5" IN 2' WEDGE  
 AREA (SQ FT) = 1.5" X 2' / 12" / (") = 0.25 SQ FT  
 \*\*THIS QUANTITY IS INTENDED TO BRING UP LOW SHOULDERS ON THE MAIN LINE AND MAY BE DELETED AS NECESSARY.

## CHANGEABLE MESSAGE SIGN

LOCATION*	CHANGEABLE MESSAGE SIGN X7015005 (CAL DA)
WB I-74	7.0
EB I-74	7.0
WB ON RAMP 'DA'	7.0
EB ON RAMP 'CB'	7.0
TOTAL =	28.0

\*LOCATIONS TO BE DETERMINED BY THE ENGINEER.

## COMBINATION CONCRETE CURB & GUTTER / CONCRETE MEDIAN SURFACE

LOCATION	LENGTH(')*	AREA(SQ FT)*	COMB CC & G TM 6.06 60608600 (FOOT)	CONC MED SURF, 4" 60618300 (SQ FT)
EB OFF RAMP 'AC'	54.2	115.5	54.2	115.5
WB OFF RAMP 'BD'	57.0	129.9	57.0	129.9
TOTAL =			111.2	245.4
ROUND TO			115.0	250.0

\*MEASURED IN CADD

# SCHEDULE OF QUANTITIES

## DELINEATOR & RAISED REFLECTIVE PAVEMENT MARKERS

LOCATION	STA	TO	STA	DELINEATORS 63500105 (EACH)					RAISED REFL PAVT MKR 78100100 (EACH)
				SINGLE WHITE	SINGLE AMBER	DOUBLE WHITE	DOUBLE AMBER	TRIPLE AMBER	
EB OFF RAMP 'AC'	0+00 RT					1			
EB OFF RAMP 'AC'	1+00 RT					1			
EB OFF RAMP 'AC'	2+00 RT					1			
EB OFF RAMP 'AC'	3+00 RT					1			
EB OFF RAMP 'AC'	4+00 RT					1			
EB OFF RAMP 'AC'	5+00 RT					1			
EB OFF RAMP 'AC'	6+00 RT					1			
EB OFF RAMP 'AC'	6+80 RT			1					
EB OFF RAMP 'AC'	7+60 RT & LT			1	1				
EB OFF RAMP 'AC'	8+40 RT & LT			1	1				
EB OFF RAMP 'AC'	9+20 RT & LT			1	1				
EB OFF RAMP 'AC'	10+00 RT & LT			1	1				
EB ON RAMP 'CB'	2+00 RT			1					
EB ON RAMP 'CB'	2+55 RT			1					
EB ON RAMP 'CB'	3+10 RT			1					
EB ON RAMP 'CB'	3+65 RT			1					
EB ON RAMP 'CB'	4+20 RT			1					
EB ON RAMP 'CB'	4+75 RT			1					
EB ON RAMP 'CB'	5+30 RT			1					
EB ON RAMP 'CB'	5+85 RT			1					
EB ON RAMP 'CB'	6+40 RT			1					
EB ON RAMP 'CB'	6+95 RT			1					
EB ON RAMP 'CB'	7+50 RT			1					
EB ON RAMP 'CB'	8+05 RT			1					
EB ON RAMP 'CB'	10+50 RT & LT			1	1				
EB ON RAMP 'CB'	11+30 RT & LT			1	1				
EB ON RAMP 'CB'	12+10 RT & LT			1	1				
EB ON RAMP 'CB'	12+90 RT & LT			1	1				
EB ON RAMP 'CB'	13+70 RT & LT			1	1				
EB ON RAMP 'CB'	14+50 RT & LT			1	1				
EB ON RAMP 'CB'	15+30 RT & LT			1	1				
EB ON RAMP 'CB'	16+10 RT			1					
EB ON RAMP 'CB'	16+90 RT			1					
EB ON RAMP 'CB'	17+90 RT					1			
EB ON RAMP 'CB'	18+90 RT					1			
EB ON RAMP 'CB'	19+90 RT					1			
EB ON RAMP 'CB'	20+90 RT					1			
EB ON RAMP 'CB'	21+90 RT					1			
EB ON RAMP 'CB'	22+90 RT					1			
EB ON RAMP 'CB'	23+90 RT					1			
EB ON RAMP 'CB'	24+90 RT					1			
EB ON RAMP 'CB'	25+90 RT					1			
EB ON RAMP 'CB'	26+90 RT					1			
WB ON RAMP 'DA'	2+00 RT			1					
WB ON RAMP 'DA'	2+55 RT			1					
WB ON RAMP 'DA'	3+10 RT			1					
WB ON RAMP 'DA'	3+65 RT			1					
WB ON RAMP 'DA'	4+20 RT			1					
WB ON RAMP 'DA'	4+75 RT			1					
WB ON RAMP 'DA'	5+30 RT			1					
WB ON RAMP 'DA'	5+85 RT			1					
WB ON RAMP 'DA'	6+40 RT			1					
WB ON RAMP 'DA'	6+95 RT			1					
SUBTOTAL 1 =				36	11	17	0	0	0

# SCHEDULE OF QUANTITIES

## DELINEATOR & RAISED REFLECTIVE PAVEMENT MARKERS

LOCATION	STA	TO	STA	DELINEATORS 63500105 (EACH)					RAISED REFL PAVT MKR 78100100 (EACH)
				SINGLE WHITE	SINGLE AMBER	DOUBLE WHITE	DOUBLE AMBER	TRIPLE AMBER	
WB ON RAMP 'DA'	7+50 RT			1					
WB ON RAMP 'DA'	8+05 RT			1					
WB ON RAMP 'DA'	10+50 RT & LT			1	1				
WB ON RAMP 'DA'	11+30 RT & LT			1	1				
WB ON RAMP 'DA'	12+10 RT & LT			1	1				
WB ON RAMP 'DA'	12+90 RT & LT			1	1				
WB ON RAMP 'DA'	13+70 RT & LT			1	1				
WB ON RAMP 'DA'	14+50 RT & LT			1	1				
WB ON RAMP 'DA'	15+30 RT & LT			1	1				
WB ON RAMP 'DA'	16+10 RT			1					
WB ON RAMP 'DA'	16+90 RT			1					
WB ON RAMP 'DA'	17+90 RT					1			
WB ON RAMP 'DA'	18+90 RT					1			
WB ON RAMP 'DA'	19+90 RT					1			
WB ON RAMP 'DA'	20+90 RT					1			
WB ON RAMP 'DA'	21+90 RT					1			
WB ON RAMP 'DA'	22+90 RT					1			
WB ON RAMP 'DA'	23+90 RT					1			
WB ON RAMP 'DA'	24+90 RT					1			
WB ON RAMP 'DA'	25+90 RT					1			
WB ON RAMP 'DA'	26+90 RT					1			
WB OFF RAMP 'BD'	0+00 RT					1			
WB OFF RAMP 'BD'	1+00 RT					1			
WB OFF RAMP 'BD'	2+00 RT					1			
WB OFF RAMP 'BD'	3+00 RT					1			
WB OFF RAMP 'BD'	4+00 RT					1			
WB OFF RAMP 'BD'	5+00 RT					1			
WB OFF RAMP 'BD'	6+00 RT					1			
WB OFF RAMP 'BD'	6+80 RT			1					
WB OFF RAMP 'BD'	7+60 RT & LT			1	1				
WB OFF RAMP 'BD'	8+40 RT & LT			1	1				
WB OFF RAMP 'BD'	9+20 RT & LT			1	1				
WB OFF RAMP 'BD'	10+00 RT & LT			1	1				
XOVER STA 510+69.91	510+94.91 WB						1		
XOVER STA 510+69.91	518+94.91 WB							1	
XOVER STA 510+69.91	510+44.91 EB						1		
XOVER STA 510+69.91	502+44.91 EB							1	
XOVER STA 596+05.81	596+30.81 WB						1		
XOVER STA 596+05.81	604+30.81 WB							1	
XOVER STA 596+05.81	595+80.81 EB						1		
XOVER STA 596+05.81	587+80.81 EB							1	
I-74 WB 2 each 80'	377+84.95		609+20.00						579
I-74 EB 2 each 80'	377+89.05		610+00.91Bk						581
I-74 EB 2 each 80'	633+68.77Ah		634+40.00						2
EB OFF RAMP 'AC' 40' Spacing both sides	436+96.02		440+55.66						18
EB OFF RAMP 'AC' 20' Spacing	0+00		6+62.40						34
EB OFF RAMP 'AC' 40' Spacing in. curve	6+62.40		10+07.33						9
WB OFF RAMP 'BD' 40' Spacing both sides	470+90.57		474+92.63						22
WB OFF RAMP 'BD' 20' Spacing	0+00		6+62.39						34
WB OFF RAMP 'BD' 40' Spacing in. curve	6+62.39		10+34.84						10
SUBTOTAL 2 =				16	11	17	4	4	1289
TOTAL = 1 + 2 =				52	22	34	4	4	1289
GRAND TOTAL =				116.0					1,289.0
ROUND TO				120.0					1,290.0

# SCHEDULE OF QUANTITIES

## HMA - MAINLINE

DIRECTION	STATION	TO	STATION	LENGTH (FOOT)	AVG. WIDTH (FOOT)	AREA* (SQ YD)	HOT MIX ASPHALT BINDER COURSE THICKNESS (INCH)	HOT MIX ASPHALT SURFACE COURSE THICKNESS (INCH)	BITUMINOUS MATERIALS (TACK COAT)** 40600290 (POUND)	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 40603240 (TON)	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90 40603545 (TON)	MATERIAL TRANSFER DEVICE. Z0034105 (TON)
I-74 RURAL MAINLINE												
I 74 EB	377+89.05		610+00.91	23,211.86	24.00	61,898.3	3.5	1.5	41,781.3	12,132.1	5,199.5	17,331.5
I 74 EB	633+68.77		634+40.00	71.23	24.00	189.9	3.5	1.5	128.2	37.2	16.0	53.2
I 74 WB	377+84.95		609+20.00	23,135.05	24.00	61,693.5	3.5	1.5	41,643.1	12,091.9	5,182.3	17,274.2
RAMP AC EB OFF	+0.0		17+28.95	1,728.95	16' & VAR	3,157.3	0.0	1.5	1,420.8		265.2	265.2
RAMP CB EB ON	+24.38		26+90.81	2,666.43	16' & VAR	4,350.0	0.0	1.5	1,957.5		365.4	365.4
RAMP DA WB ON	+23.72		26+14.03	2,590.31	16' & VAR	4,417.9	0.0	1.5	1,988.1		371.1	371.1
RAMP BD WB OFF	+4.71		17+32.27	1,727.56	16' & VAR	3,282.4	0.0	1.5	1,477.1		275.7	275.7
I-74 RURAL MAINLINE TOTAL =									90,396.1	24,261.2	11,675.1	35,936.3

\*RAMP AREAS MEASURED FROM CADD

\*\*PRIME APPLICATION RATE = 0.05 LB/SQ FT FOR MILLED SURFACE & 0.025 LB/SQ FT FOR HMA LIFTS

## HMA - SHOULDERS

DIRECTION	DL/PL	STATION	TO	STATION	AVG. WIDTH (FOOT)	LENGTH (FOOT)	AREA (SQ YD)	HOT-MIX ASPHALT SHOULDERS THICKNESS (INCH)	BITUMINOUS MATERIALS (TACK COAT)* 40600290 (POUND)	HOT-MIX ASPHALT SHOULDERS 48203100 (TON)
I-74 RURAL SHOULDERS										
I-74 EB	DL	377+89.05		440+43.17	10.00	6,254.12	6,949.0	1.5	3,127.1	583.7
I-74 EB	DL	437+04.65		467+06.40	10.00	3,001.75	3,335.3	1.5	1,500.9	280.2
I-74 EB	DL	465+95.62		610+00.91	10.00	14,405.29	16,005.9	1.5	7,202.6	1,344.5
I-74 EB	DL	633+68.77		634+40.00	10.00	71.23	79.1	1.5	35.6	6.6
I-74 EB	PL	377+89.05		610+00.91	6.00	23,211.86	15,474.6	1.5	6,963.6	1,299.9
I-74 EB	PL	633+68.77		634+40.00	6.00	71.23	47.5	1.5	21.4	4.0
I-74 WB	DL	377+84.95		447+44.83	10.00	6,959.88	7,733.2	1.5	3,479.9	649.6
I-74 WB	DL	444+45.55		474+77.05	10.00	3,031.50	3,368.3	1.5	1,515.8	282.9
I-74 WB	DL	471+40.86		609+20.00	10.00	13,779.14	15,310.2	1.5	6,889.6	1,286.1
I-74 WB	PL	377+84.95		609+20.00	6.00	23,135.05	15,423.4	1.5	6,940.5	1,295.6
RAMP AC EB OFF	LT	3+11.23		17+33.43	4.00	1,422.20	632.1	1.5	284.4	53.1
RAMP AC EB OFF	RT	6+51.63		17+85.10	6.00	1,133.47	755.6	1.5	340.0	63.5
RAMP CB EB ON	LT	+0.0		19+23.78	4.00	1,923.78	855.0	1.5	384.8	71.8
RAMP CB EB ON	RT	+0.0		18+12.12	6.00	1,812.12	1,208.1	1.5	543.6	101.5
RAMP DA WB ON	LT	+0.0		19+00.00	4.00	1,900.00	844.4	1.5	380.0	70.9
RAMP DA WB ON	RT	+0.0		16+00.00	6.00	1,600.00	1,066.7	1.5	480.0	89.6
RAMP BD WB OFF	LT	2+76.72		17+39.57	4.00	1,462.85	650.2	1.5	292.6	54.6
RAMP BD WB OFF	RT	6+13.28		18+06.80	6.00	1,193.52	795.7	1.5	358.1	66.8
I-74 RURAL SHOULDERS TOTAL =									40,740.4	7,604.9

\*PRIME APPLICATION RATE = 0.05 LB/SQ FT FOR MILLED SURFACE

# SCHEDULE OF QUANTITIES

**HMA - INCIDENTAL**

LOCATION	STATION	AREA* (SQ YD)	INCIDENTAL HOT-MIX ASPHALT THICKNESS (INCH)	BITUMINOUS MATERIALS (TACK COAT)** 40800029 (POUND)	INCIDENTAL HOT-MIX ASPHALT SURFACING 40800050 (TON)
I-74 RURAL INCIDENTAL					
* MEDIAN X-OVER	510+69.91	269.6	1.5	121.3	22.6
* MEDIAN X-OVER	596+05.81	285.5	1.5	128.5	24.0
I-74 RURAL SHOULDERS TOTAL =				249.8	46.6

\*AREAS MEASURED FROM CADD

\*\*TACK APPLICATION RATE = 0.05 LB/SQ FT FOR MILLED SURFACE

**HMA -SUMMARY**

LOCATION	BITUMINOUS MATERIALS (TACK COAT) 40600290 (POUND)	POLYMERIZED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N90 40603240 (TON)	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N90 40603545 (TON)	BITUMINOUS MATERIALS (TACK COAT) 40800029 (POUND)	INCIDENTAL HOT-MIX ASPHALT SURFACING 40800050 (TON)	HOT-MIX ASPHALT SHOULDERS 48203100 (TON)	MATERIAL TRANSFER DEVICE Z0034105 (TON)
I 74 EB	60,760.7	12,169.3	5,215.4			3,518.9	17,384.7
I 74 WB	60,468.9	12,091.9	5,182.3			3,514.1	17,274.2
US 136 RAMP AC EB OFF	2,045.3		265.2			116.6	265.2
US 136 RAMP CB EB ON	2,885.9		365.4			173.3	365.4
US 136 RAMP DA WB ON	2,848.1		371.1			160.5	371.1
US 136 RAMP BD WB OFF	2,127.7		275.7			121.5	275.7
MEDIAN CROSS OVERS				249.8	46.6		
GRAND TOTAL I-74 RURAL =	131,136.5	24,261.2	11,675.1	249.8	46.6	7,604.9	35,936.3
ROUND TO:	131,200.0	24,270.0	11,680.0	250.0	50.0	7,610.0	35,950.0



# SCHEDULE OF QUANTITIES

## PATCHING SCHEDULE

STA	DIRECTION	LANE	LENGTH(')	WIDTH(')	CL A PATCH T1 8" 44200525 (SQ YD)	CL A PATCH T2 8" 44200529 (SQ YD)	CL A PATCH T3 8" 44200533 (SQ YD)	CL D PATCH T2 10" 44201765 (SQ YD)	CL D PATCH T1 12" 44201785 (SQ YD)	CL D PATCH T2 12" 44201789 (SQ YD)	PATCH REINFORCEMENT 44213000 (SQ YD)	SAW CUTS 44213200 (FOOT)
377+90	I-74 EB	DL	6	12				8.0				
377+90	I-74 EB	PL	6	12				8.0				
462+56	I-74 EB	DL	6	12		8.0					8.0	60.0
464+45	I-74 EB	DL	6	6	4.0						4.0	36.0
465+46	I-74 EB	DL	6	12		8.0					8.0	60.0
472+70	I-74 EB	DL	6	12		8.0					8.0	60.0
478+67	I-74 EB	DL	6	12		8.0					8.0	60.0
546+50	I-74 EB	DL	6	6	4.0						4.0	36.0
565+35	I-74 EB	PL	6	12		8.0					8.0	60.0
599+10	I-74 EB	PL	6	12		8.0					8.0	60.0
601+06	I-74 EB	PL	6	12		8.0					8.0	60.0
604+99	I-74 EB	DL	6	12		8.0					8.0	60.0
604+99	I-74 EB	PL	12	12			16.0				16.0	72.0
610+00	I-74 EB	DL	6	6	4.0						4.0	36.0
634+10	I-74 EB	DL	6	12		8.0					8.0	60.0
377+85	I-74 WB	DL	6	12				8.0				
377+85	I-74 WB	PL	6	12				8.0				
378+95	I-74 WB	DL	6	12		8.0					8.0	60.0
379+10	I-74 WB	DL	6	12		8.0					8.0	60.0
520+50	I-74 WB	DL	10	12		13.3					13.3	68.0
"AC" 1+54	EB	OFF RAMP	4	8					3.6			
"AC" 6+70	EB	OFF RAMP	4	16						7.1		
"AC" 11+08	EB	OFF RAMP	4	16						7.1		
"AC" 13+45	EB	OFF RAMP	4	16						7.1		
"BD" 6+10	WB	OFF RAMP	4	16						7.1		
"BD" 6+90	WB	OFF RAMP	4	16						7.1		
"BD" 12+40	WB	OFF RAMP	4	16						7.1		
"BD" 15+06	WB	OFF RAMP	6	16						10.7		
"DA" 8+00	WB	ON RAMP	4	16						7.1		
"DA" 16+25	WB	ON RAMP	4	16						7.1		
UNSPECIFIED	INSIDE SHLDS	PL							20.0			
UNSPECIFIED	INSIDE SHLDS	PL								15.0		
TOTAL =					12.0	101.3	16.0	32.0	23.6	82.6	129.3	908.0
ROUND TO					15.0	105.0	20.0	35.0	25.0	85.0	130.0	910.0

# SCHEDULE OF QUANTITIES

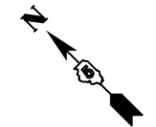
## PERMANENT PAVEMENT MARKINGS

LOCATION	SYMBOL	STA	TO	STA	THPL PVT MK LTR & SYM 78000100 (SQ FT)	THPL PVT MK LINE 4" 78000200 (FOOT)		THPL PVT MK LINE 6" 78000400 (FOOT)*	THPL PVT MK LINE 8" 78000500 (FOOT)	THPL PVT MK LINE 12" 78000600 (FOOT)*	THPL PVT MK LINE 24" 78000650 (FOOT)*	PREF PL PM TB INL L 4" 78004210 (FOOT)	GRV RCSD PM LTR & SYM X7830060 (SQ FT)	GRV RCSD PVT MRKG 5" X7830070 (FOOT)	GRV RCSD PVT MRKG 7" X7830074 (FOOT)*	GRV RCSD PVT MRKG 9" X7830076 (FOOT)	GRV RCSD PVT MRKG 13" X7830078 (FOOT)*	GRV RCSD PVT MRKG 25" X7830090 (FOOT)*	
						WHITE	YELLOW												
WB OFF RAMP 'BD'	LT ARROW/ISLAND				15.6			177.8		138.5	35.1		15.6		177.8		138.5	35.1	
WB OFF RAMP 'BD'	RT ARROW				15.6								15.6						
WB OFF RAMP 'BD'	WRONG WAY ARROW				24.3								24.3						
EB OFF RAMP 'AC'	LT ARROW/ISLAND				15.6			180.2		148.9	35.5		15.6		180.2		148.9	35.5	
EB OFF RAMP 'AC'	RT ARROW				15.6								15.6						
EB OFF RAMP 'AC'	WRONG WAY ARROW				24.3								24.3						
EB OFF RAMP 'AC'	EDGE/WHITE	0+00		17+29.51		1,729.5								1,729.5					
EB OFF RAMP 'AC'	EDGE/YELLOW	6+50.00		17+29.51			1,079.5							1,079.5					
WB OFF RAMP 'BD'	EDGE/WHITE	0+00		17+43.74		1,743.7								1,743.7					
WB OFF RAMP 'BD'	EDGE/YELLOW	6+60.00		17+43.74			1,083.7							1,083.7					
EB ON RAMP 'CB'	EDGE/WHITE	0+23.00		27+65.20		2,742.2								2,742.2					
EB ON RAMP 'CB'	EDGE/YELLOW	0+23.00		16+00.00			1,577.0							1,577.0					
WB ON RAMP 'DA'	EDGE/WHITE	0+23.00		27+68.36		2,745.4								2,745.4					
WB ON RAMP 'DA'	EDGE/YELLOW	0+23.00		16+00.00			1,577.0							1,577.0					
I-74 EB DL	EDGE/WHITE	371+24.05		433+92.92		6,268.9								6,268.9					
I-74 EB DL	EDGE/WHITE	440+55.66		463+63.30		2,307.6								2,307.6					
I-74 EB DL	EDGE/WHITE	475+45.45		610+00.91 (BK)		13,455.5								13,455.5					
I-74 EB DL	EDGE/WHITE	633+68.77 (AH)		641+05.00		736.2								736.2					
I-74 EB PL	EDGE/YELLOW	371+24.05		610+00.91 (BK)			23,876.9							23,876.9					
I-74 EB PL	EDGE/YELLOW	633+68.77 (AH)		641+05.00			736.2							736.2					
I-74 WB DL	EDGE/WHITE	371+19.95		435+77.50		6,457.6								6,457.6					
I-74 WB DL	EDGE/WHITE	447+60.63		470+90.57		2,329.9								2,329.9					
I-74 WB DL	EDGE/WHITE	477+54.98		610+00.91 (BK)		13,245.9								13,245.9					
I-74 WB DL	EDGE/WHITE	633+68.77 (AH)		639+53.77		585.0								585.0					
I-74 WB PL	EDGE/YELLOW	371+19.95		610+00.91 (BK)			23,881.0							23,881.0					
I-74 WB PL	EDGE/YELLOW	633+68.77 (AH)		639+53.77		585.0								585.0					
WB OFF RAMP 'BD'	GORE	2+57.94		6+60.00					402.06								402.06		
I-74 WB	GORE	470+90.57		474+92.63					402.06								402.06		
EB OFF RAMP 'AC'	GORE	2+90.36		6+50.00					359.64								359.64		
I-74 EB	GORE	436+96.02		440+55.66					359.64								359.64		
WB ON RAMP 'DA'	GORE	16+00.00		20+33.06					433.06								433.06		
I-74 WB	GORE	443+27.57		447+60.63					433.06								433.06		
EB ON RAMP 'CB'	GORE	16+00.00		20+34.52					434.52								434.52		
I-74 EB	GORE	463+63.30		467+97.82					434.52								434.52		
I-74 WB	CL	377+84.95		609+20.00								5783.8							
I-74 EB	CL	377+89.05		610+00.91Bk								5803.0							
I-74 EB	CL	633+68.77Ah		634+40.00								17.8							
SUBTOTAL =							54,347.4	54,396.3											
TOTAL =						111.0	108,743.7	358.0	3258.6	287.4	70.6	11586.7	111.0	108,743.7	358.0	3258.6	287.4	70.6	
ROUND TO						111.0	108,750.0	360.0	3,260.0	290.0	75.0	11,590.0	111.0	108,750.0	360.0	3,260.0	290.0	75.0	

\*LENGTHS WERE MEASURED USING CADD

SEC. 27, T 22 N, R 3 E, 3rd P.M.

SEC. 34, T 22 N, R 4 E, 3rd P.M.



SECTION (57-24 & 57-24-1)RS-1  
BEGINS STA. 377+84.95 (WB)

S.N. 057-0134

375+00 376  $\text{C}$  I-74 377 378 379 380+00 381 382 383 384 385+00 386 387 388 389

S.N. 057-0133

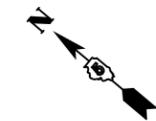
SECTION (57-24 & 57-24-1)RS-1  
BEGINS STA. 377+89.05 (EB)

MATCHLINE STATION 390+00.00

- HMA SURFACE REMOVAL, 5"
- HMA SURFACE REMOVAL, 1 1/2"

SEC. 34, T 22 N, R 4 E, 3rd P.M.

L = 1,359.31'  
E = 30.34'  
e = ----  
T.R. = ----  
S.E. RUN = ----  
P.C. STA. = 391+42.44  
P.T. STA. = 405+01.75



MATCHLINE STATION 390+00.00

390+00 391 P.C. STA. 391+42.44 392  $\text{C}$  I-74 393 394 395+00 396 397 398 399 400+00 401 402 403 404

MATCHLINE STATION 405+00.00

- HMA SURFACE REMOVAL, 5"
- HMA SURFACE REMOVAL, 1 1/2"

FILE NAME =	USER NAME = piersonbr	DESIGNED -	REVISED -
p:\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 5\Projects\0570A60\Drawings\Design\0570A60-shr-plan.dgn		DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
*MODELNAME#	PLOT DATE = 10/6/2016	DATE -	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

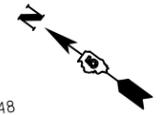
PLAN SHEETS F.A.I. - 74 (I-74)

SCALE: 1" = 50' SHEET 1 OF 8 SHEETS STA. 375+00.00 TO STA. 405+00.00

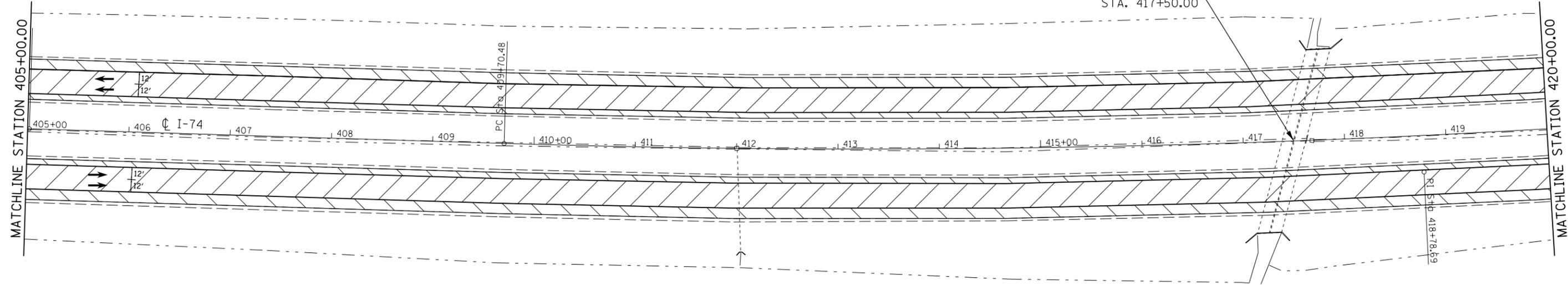
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(57-24&57-24-1)RS-1	MCLEAN	48	22
CONTRACT NO. 70A60			ILLINOIS FED. AID PROJECT	

SEC. 34, T 22 N, R 4 E, 3rd P.M.

$\Delta = 90^\circ$   
 $D = 0^\circ 30' 05''$   
 $R = 11,428.31'$   
 $T = 908.20'$   
 $L = 1,812.59'$   
 $E = 36.03'$   
 $e =$   
 $T.R. =$   
 $S.E. RUN = 409+70.48$   
 $P.C. STA. = 427+83.08$   
 $P.T. STA. = 427+83.08$

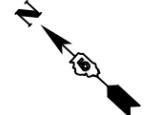


S.N. 057-2029  
STA. 417+50.00

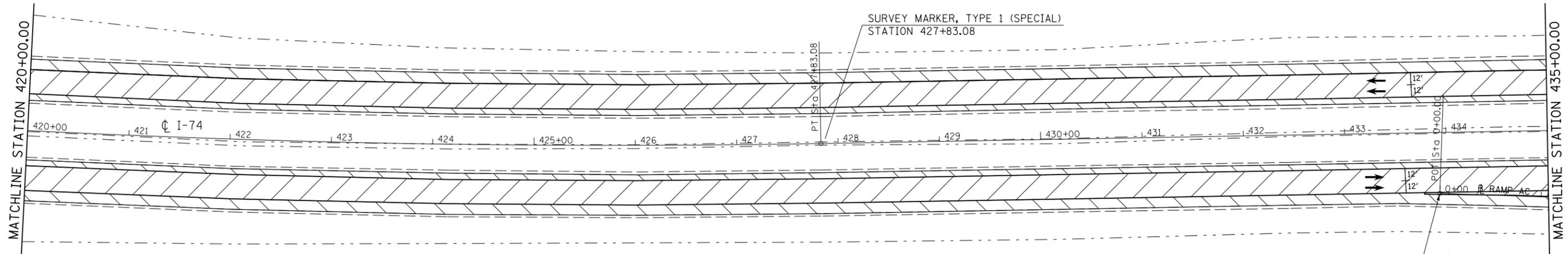


- HMA SURFACE REMOVAL, 5"
- HMA SURFACE REMOVAL, 1/2"

SEC. 34, T 22 N, R 4 E, 3rd P.M.



SURVEY MARKER, TYPE 1 (SPECIAL)  
STATION 427+83.08



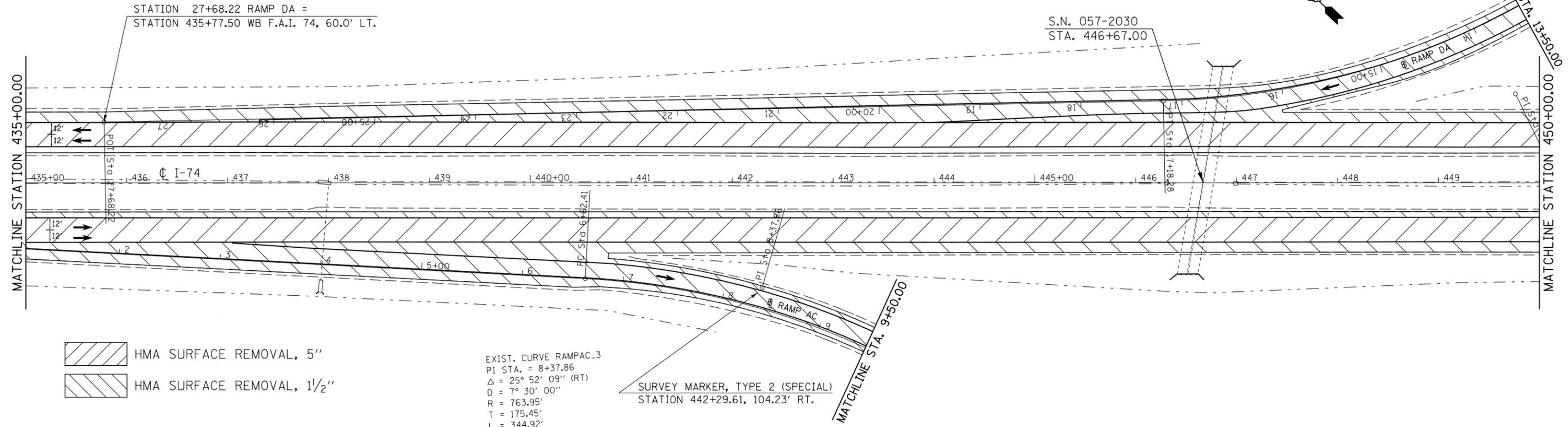
- HMA SURFACE REMOVAL, 5"
- HMA SURFACE REMOVAL, 1/2"

STATION 0+00.00 RAMP AC =  
STATION 433+92.92 EB F.A.I. 74, 60.0' RT.

FILE NAME =	USER NAME = ceerlockjd	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PLAN SHEETS F.A.I. - 74 (I-74)</b>			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
DRAWN								SCALE: 1" = 50'			74	(57-24&57-24-1)RS-1
PLOT SCALE = 100.0000' / in.					SHEET 2 OF 8 SHEETS			CONTRACT NO. 70A60				
PLOT DATE = 10/17/2016					STA. 405+00.00 TO STA. 435+00.00			ILLINOIS FED. AID PROJECT				

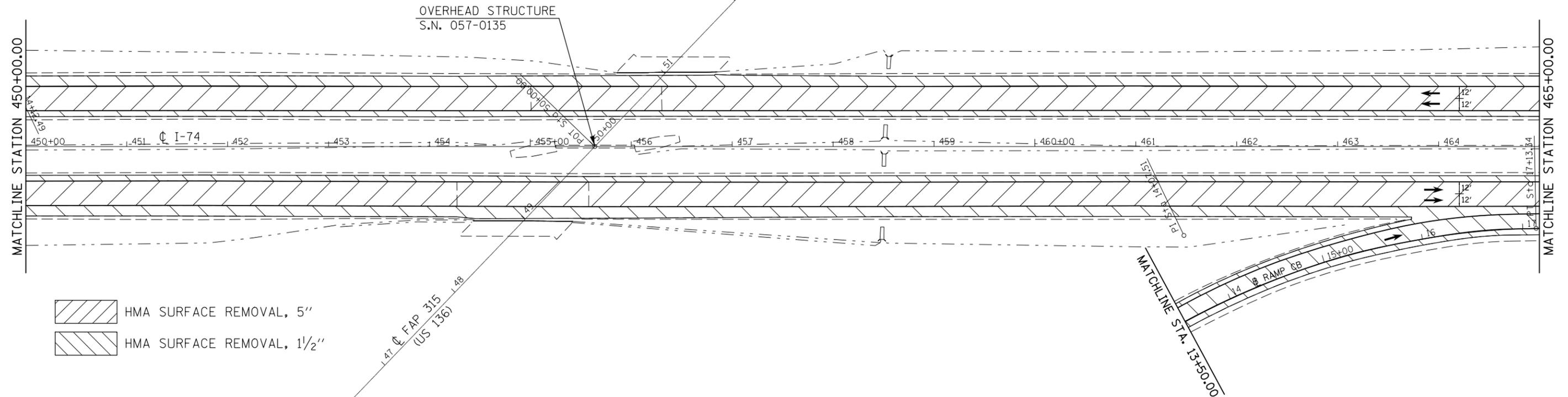
SEC. 34, T 22 N, R 4 E, 3rd P.M.

e = -----  
 T.R. = -----  
 S.E. RUN = -----  
 P.C. STA. = 10+63.39  
 P.T. STA. = 17+18.28



SEC. 34, T 22 N, R 4 E, 3rd P.M.

SEC. 2, T 21 N, R 4 E, 3rd P.M.



FILE NAME =	USER NAME = piersonbr	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PLAN SHEETS F.A.I. - 74 (I-74)</b>			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\1\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\IDOT Offices\District 5\Projects\0570A60\Design\0570A60-shr-plan.dgn	DRAWN	REVISED -	REVISED -					74	(57-24&57-24-1)RS-1	MCLEAN	48	24
PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -	REVISED -		CONTRACT NO. 70A60							
*MODELNAME#	DATE -	REVISED -	REVISED -		ILLINOIS FED. AID PROJECT							

SCALE: 1" = 50' SHEET 3 OF 8 SHEETS STA. 435+00.00 TO STA. 465+00.00

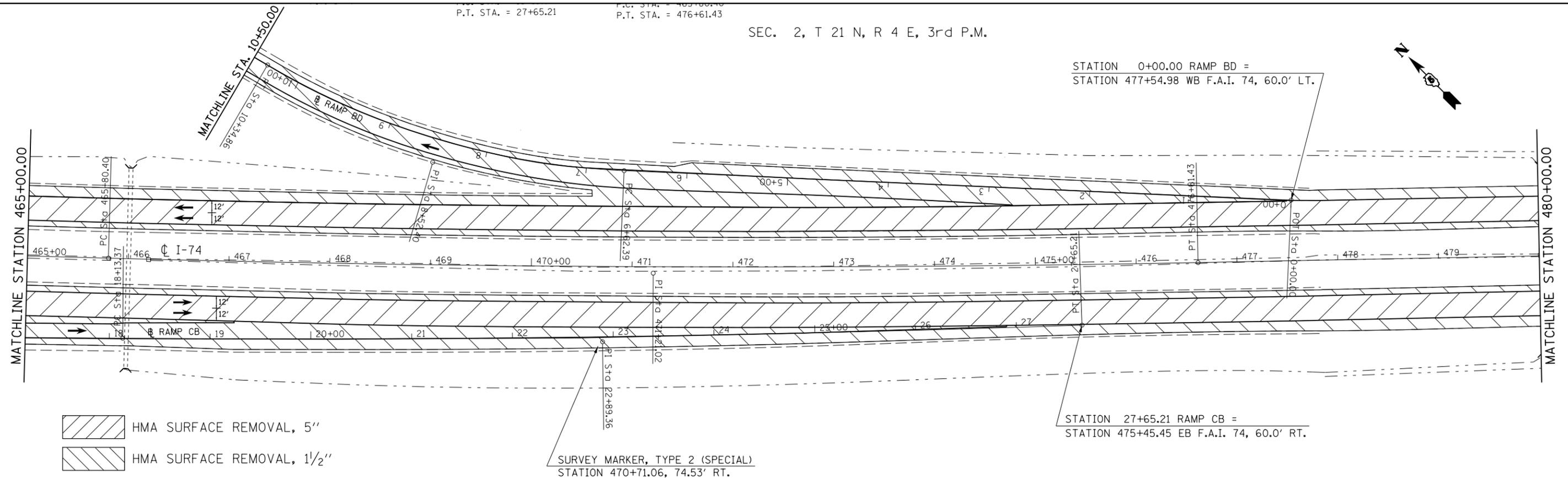
P.T. STA. = 27+65.21

P.I. STA. = 405+66.10  
P.T. STA. = 476+61.43

SEC. 2, T 21 N, R 4 E, 3rd P.M.

STATION 0+00.00 RAMP BD =  
STATION 477+54.98 WB F.A.I. 74, 60.0' LT.

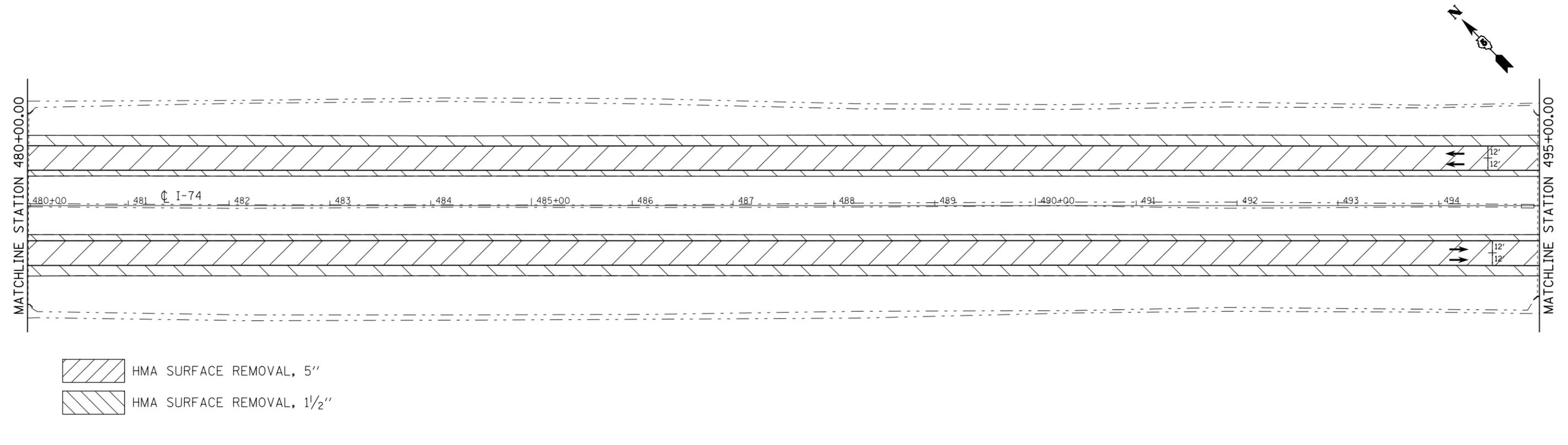
STATION 27+65.21 RAMP CB =  
STATION 475+45.45 EB F.A.I. 74, 60.0' RT.



- HMA SURFACE REMOVAL, 5"
- HMA SURFACE REMOVAL, 1 1/2"

SURVEY MARKER, TYPE 2 (SPECIAL)  
STATION 470+71.06, 74.53' RT.

SEC. 2, T 21 N, R 4 E, 3rd P.M.



- HMA SURFACE REMOVAL, 5"
- HMA SURFACE REMOVAL, 1 1/2"

FILE NAME =	USER NAME = piersonbr	DESIGNED -	REVISED -
p:\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 5\Projects\0579\DRAWING\Design\0570A60-shr-plan.dgn		DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / 1"	CHECKED -	REVISED -
#MODELNAME#	PLOT DATE = 10/6/2016	DATE -	REVISED -

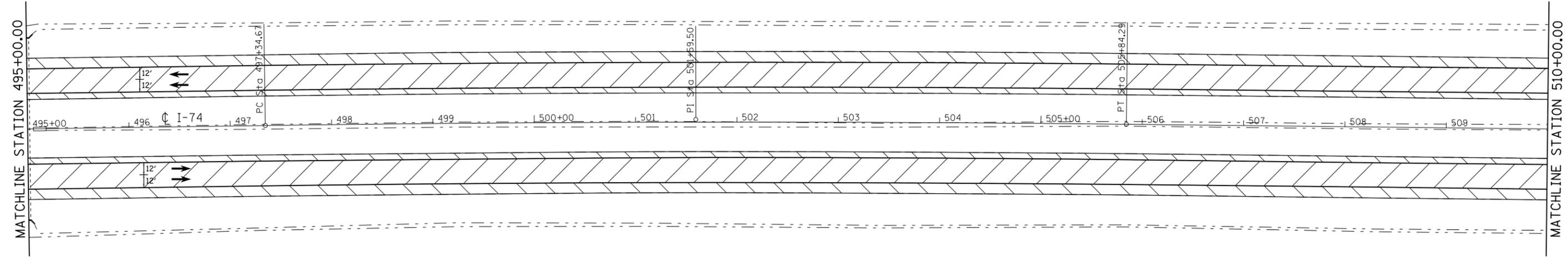
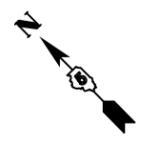
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

PLAN SHEETS F.A.I. - 74 (I-74)

SCALE: 1" = 50' SHEET 4 OF 8 SHEETS STA. 465+00.00 TO STA. 495+00.00

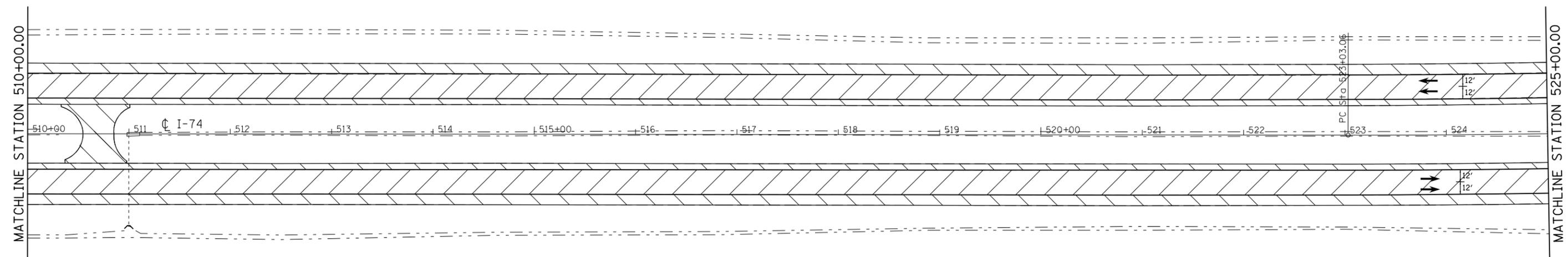
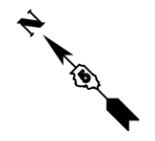
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(57-24&57-24-1)RS-1	MCLEAN	48	25
CONTRACT NO. 70A60			ILLINOIS FED. AID PROJECT	

L = 849.62'  
 E = 2.65'  
 e = -----  
 T.R. = -----  
 S.E. RUN = -----  
 P.C. STA. = 497+34.67    SEC. 2, T 21 N, R 4 E, 3rd P.M.  
 P.T. STA. = 505+84.29



HMA SURFACE REMOVAL, 5"  
 HMA SURFACE REMOVAL, 1 1/2"

SEC. 2, T 21 N, R 3 E, 3rd P.M.



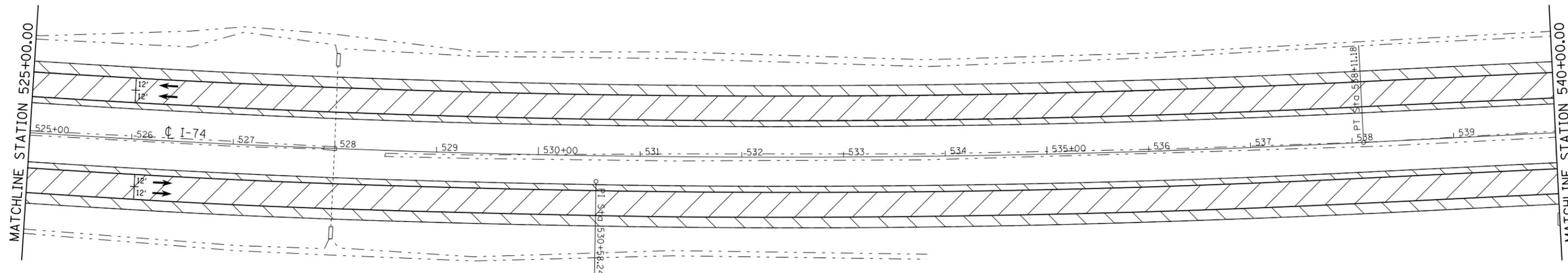
HMA SURFACE REMOVAL, 5"  
 HMA SURFACE REMOVAL, 1 1/2"

FILE NAME =	USER NAME = piersonbr	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PLAN SHEETS F.A.I. - 74 (I-74)</b>			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
DRAWN								SCALE: 1" = 50'			74	(57-24&57-24-1)RS-1	MCLEAN
PLOT SCALE = 100.0000' / 1in.					SHEET 5 OF 8 SHEETS			CONTRACT NO. 70A60			ILLINOIS FED. AID PROJECT		
DATE					STA. 495+00.00 TO STA. 525+00.00								

SEC. 2, T 21 N, R 4 E, 3rd P.M.

R = 11,326.81'  
 T = 755.18'  
 L = 1,508.12'  
 E = 25.15'  
 e = -----  
 T.R. = -----  
 S.E. RUN = -----  
 P.C. STA. = 523+03.06  
 P.T. STA. = 538+11.18

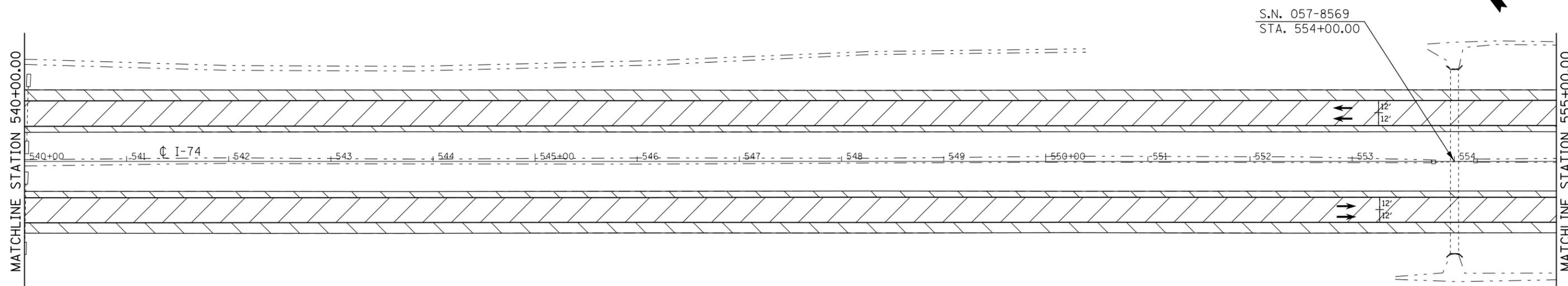
SEC. 1, T 21 N, R 4 E, 3rd P.M.



- HMA SURFACE REMOVAL, 5"
- HMA SURFACE REMOVAL, 1 1/2"

SEC. 1, T 21 N, R 4 E, 3rd P.M.

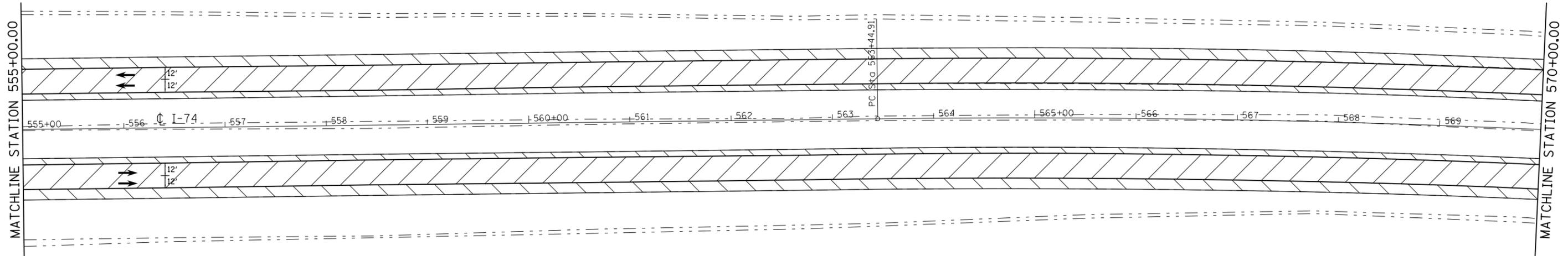
SEC. 12, T 21 N, R 4 E, 3rd P.M.



- HMA SURFACE REMOVAL, 5"
- HMA SURFACE REMOVAL, 1 1/2"

FILE NAME =	USER NAME = piersonbr	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PLAN SHEETS F.A.I. - 74 (I-74)</b>			F.A.I. RE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG\Illinois.gov\PWIDOT\Documents\IDOT Offices\District 5\Projects\057\0570A60\Design\0570A60-shr-plan.dgn	DRAWN	REVISED -	REVISED -					74	(57-24&57-24-1)RS-1	MCLEAN	48	27
*MODELNAME#	PLOT SCALE = 100.0000' / 1".	CHECKED -	REVISED -		SCALE: 1" = 50'			CONTRACT NO. 70A60				
	PLOT DATE = 10/6/2016	DATE -	REVISED -		SHEET 6 OF 8 SHEETS STA. 525+00.00 TO STA. 555+00.00			ILLINOIS FED. AID PROJECT				

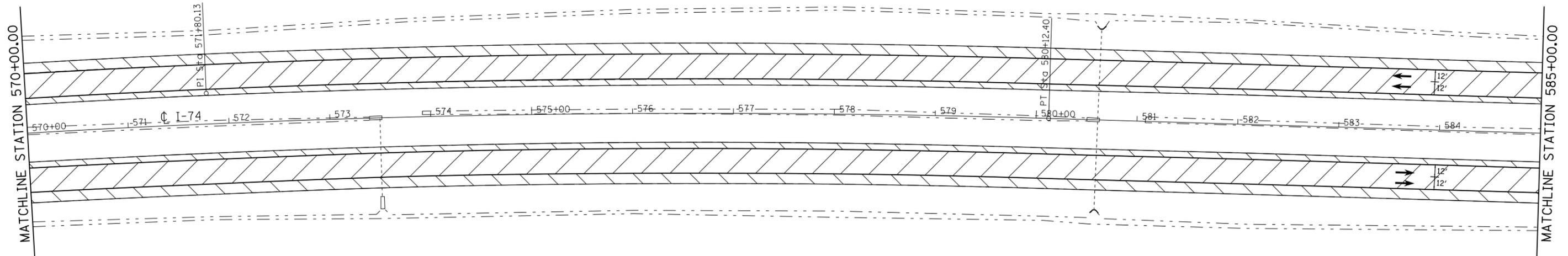
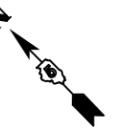
SEC. 12, T 21 N, R 4 E, 3rd P.M.



- HMA SURFACE REMOVAL, 5"
- HMA SURFACE REMOVAL, 1 1/2"

E = 30.34'  
 e = -----  
 T.R. = -----  
 S.E. RUN = -----  
 P.C. STA. = 563+44.91  
 P.T. STA. = 580+12.40

SEC. 12, T 21 N, R 4 E, 3rd P.M.



- HMA SURFACE REMOVAL, 5"
- HMA SURFACE REMOVAL, 1 1/2"

FILE NAME =	USER NAME = piersonbr	DESIGNED -	REVISED -
p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 5\Projects\0570A60\Drawings\Design\0570A60-shr-plan.dgn		DRAWN -	REVISED -
	PLOT SCALE = 100.0000' / 1" =	CHECKED -	REVISED -
#MODELNAME#	PLOT DATE = 10/6/2016	DATE -	REVISED -

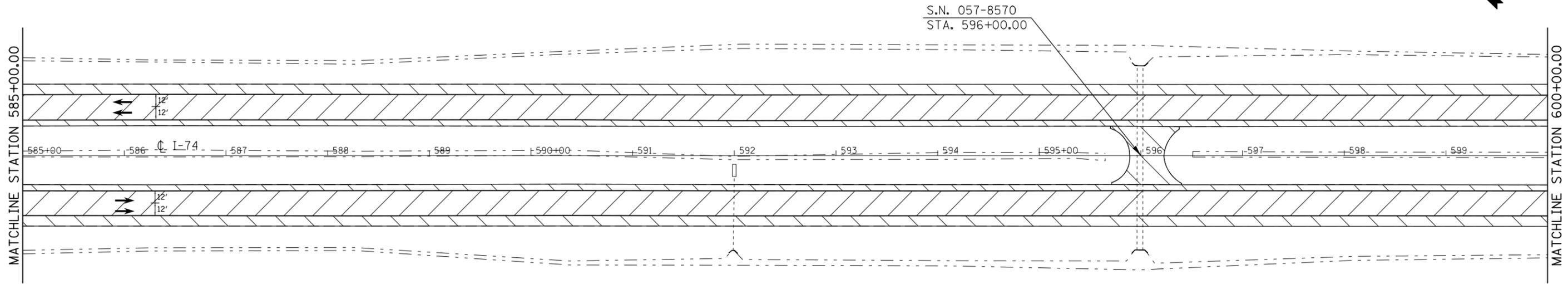
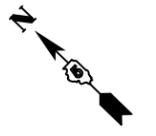
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**PLAN SHEETS F.A.I. - 74 (I-74)**

SCALE: 1" = 50' SHEET 7 OF 8 SHEETS STA. 555+00.00 TO STA. 585+00.00

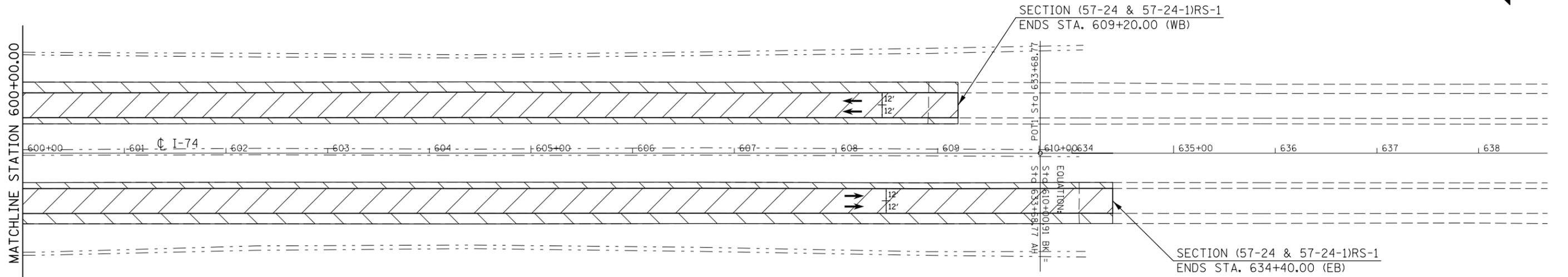
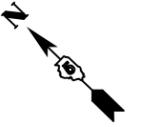
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(57-24&57-24-1)RS-1	MCLEAN	48	28
CONTRACT NO. 70A60			ILLINOIS FED. AID PROJECT	

SEC. 12, T 21 N, R 4 E, 3rd P.M.



- HMA SURFACE REMOVAL, 5"
- HMA SURFACE REMOVAL, 1 1/2"

SEC. 7, T 21 N, R 5 E, 3rd P.M.



- HMA SURFACE REMOVAL, 5"
- HMA SURFACE REMOVAL, 1 1/2"

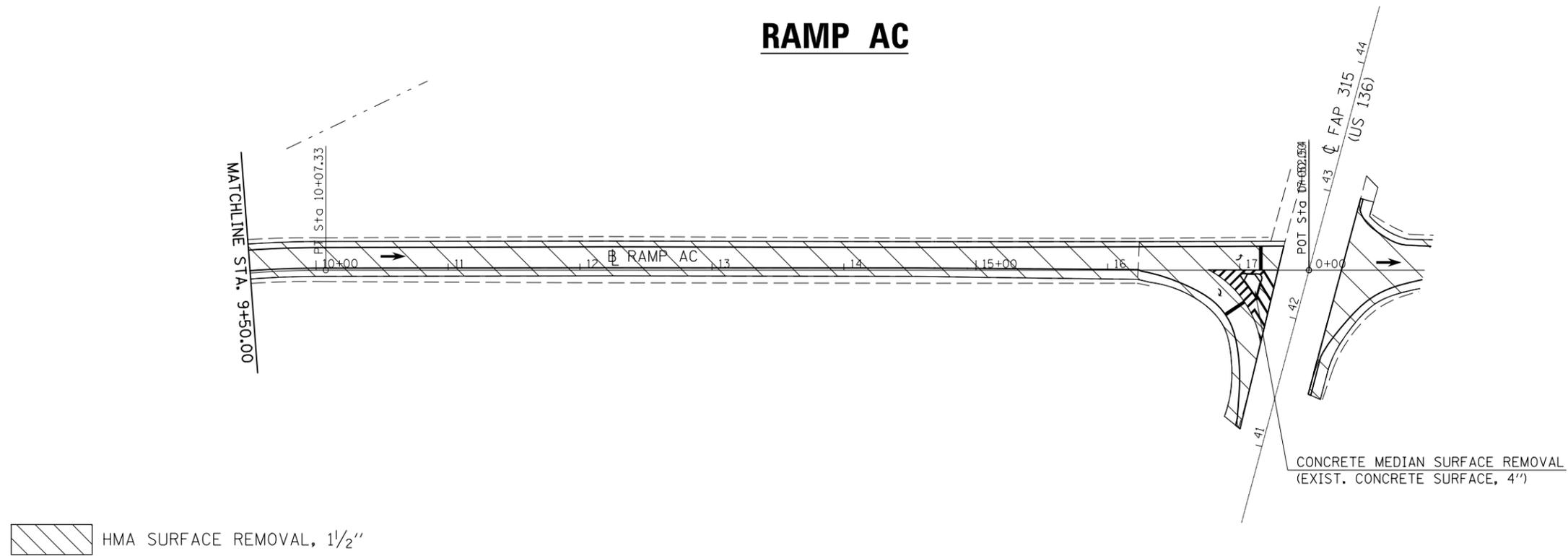
FILE NAME =	USER NAME = piersonbr	DESIGNED -	REVISED -
p:\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 5\Projects\0570A60\Drawings\0570A60-shr-plan.dgn		DRAWN -	REVISED -
		CHECKED -	REVISED -
#MODELNAME#	PLOT DATE = 10/6/2016	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>PLAN SHEETS F.A.I. - 74 (I-74)</b>			
SCALE: 1" = 50'	SHEET 8	OF 8 SHEETS	STA. 585+00.00 TO STA. 635+00.00

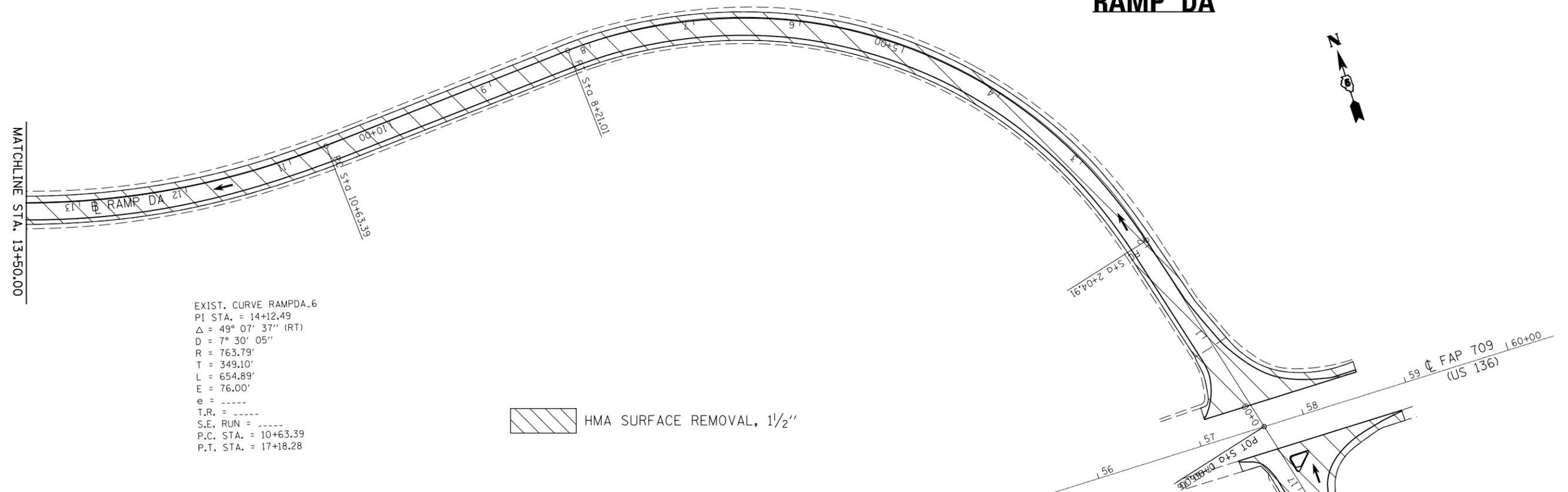
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(57-24&57-24-1)RS-1	MCLEAN	48	29
CONTRACT NO. 70A60			ILLINOIS FED. AID PROJECT	

# RAMP AC



HMA SURFACE REMOVAL, 1 1/2"

# RAMP DA

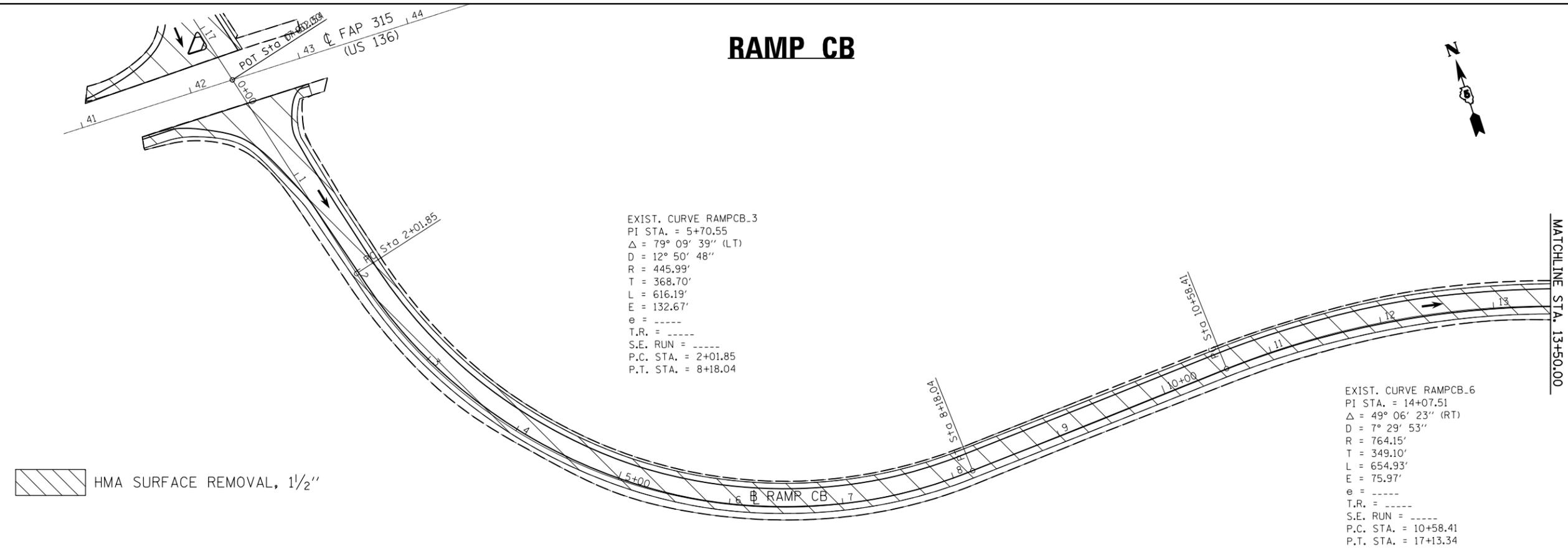


EXIST. CURVE RAMPDA-6  
 PI STA. = 14+12.49  
 Δ = 49° 07' 37" (RT)  
 D = 7° 30' 05"  
 R = 763.79'  
 T = 349.10'  
 L = 654.89'  
 E = 76.00'  
 e = -----  
 T.R. = -----  
 S.E. RUN = -----  
 P.C. STA. = 10+63.39  
 P.T. STA. = 17+18.28

HMA SURFACE REMOVAL, 1 1/2"

FILE NAME =	USER NAME = piersonbr	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PLAN SHEETS F.A.I.-74 (I-74) RAMPS AC &amp; DA</b>			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 5\Projects\0570A60\Design\0570A60-shr-plan.dgn		CHECKED -	REVISED -		74	(57-24&57-24-1)RS-1	MCLEAN	48	30			
*MODELNAME#	PLOT SCALE = 100.0000' / in.	DATE -	REVISED -		CONTRACT NO. 70A60			ILLINOIS FED. AID PROJECT				
	PLOT DATE = 10/6/2016				SCALE: 1" = 50'	SHEET 1	OF 1 SHEETS	STA.	TO STA.			

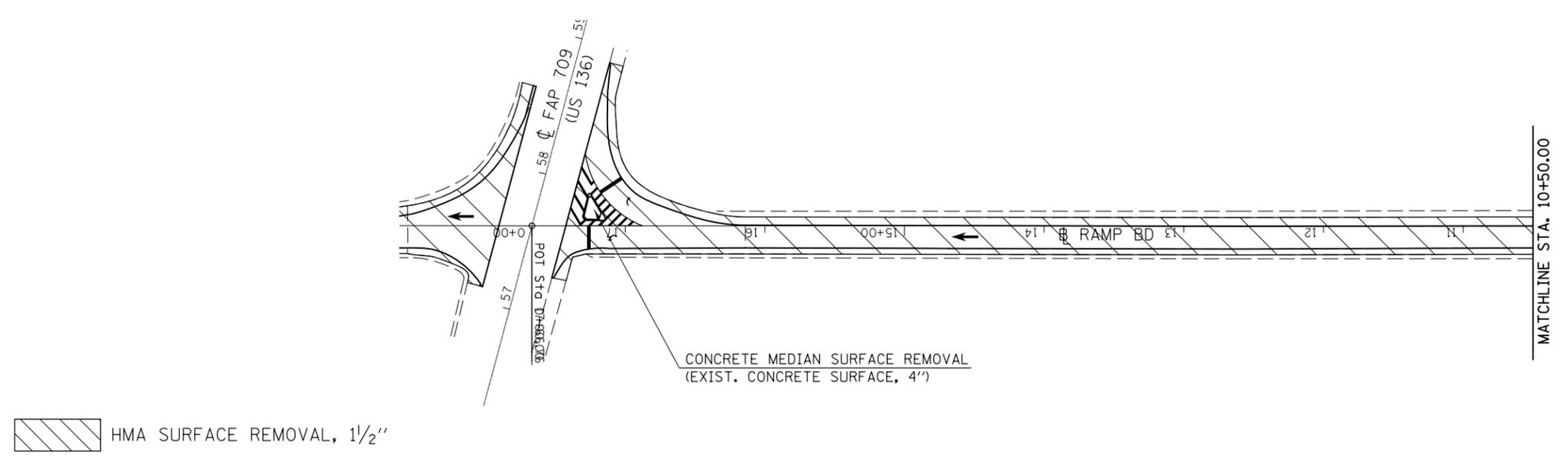
# RAMP CB



EXIST. CURVE RAMP CB.3  
 PI STA. = 5+70.55  
 $\Delta = 79^\circ 09' 39''$  (LT)  
 $D = 12^\circ 50' 48''$   
 $R = 445.99'$   
 $T = 368.70'$   
 $L = 616.19'$   
 $E = 132.67'$   
 $e = \text{-----}$   
 $T.R. = \text{-----}$   
 $S.E. RUN = \text{-----}$   
 $P.C. STA. = 2+01.85$   
 $P.T. STA. = 8+18.04$

EXIST. CURVE RAMP CB.6  
 PI STA. = 14+07.51  
 $\Delta = 49^\circ 06' 23''$  (RT)  
 $D = 7^\circ 29' 53''$   
 $R = 764.15'$   
 $T = 349.10'$   
 $L = 654.93'$   
 $E = 75.97'$   
 $e = \text{-----}$   
 $T.R. = \text{-----}$   
 $S.E. RUN = \text{-----}$   
 $P.C. STA. = 10+58.41$   
 $P.T. STA. = 17+13.34$

# RAMP BD

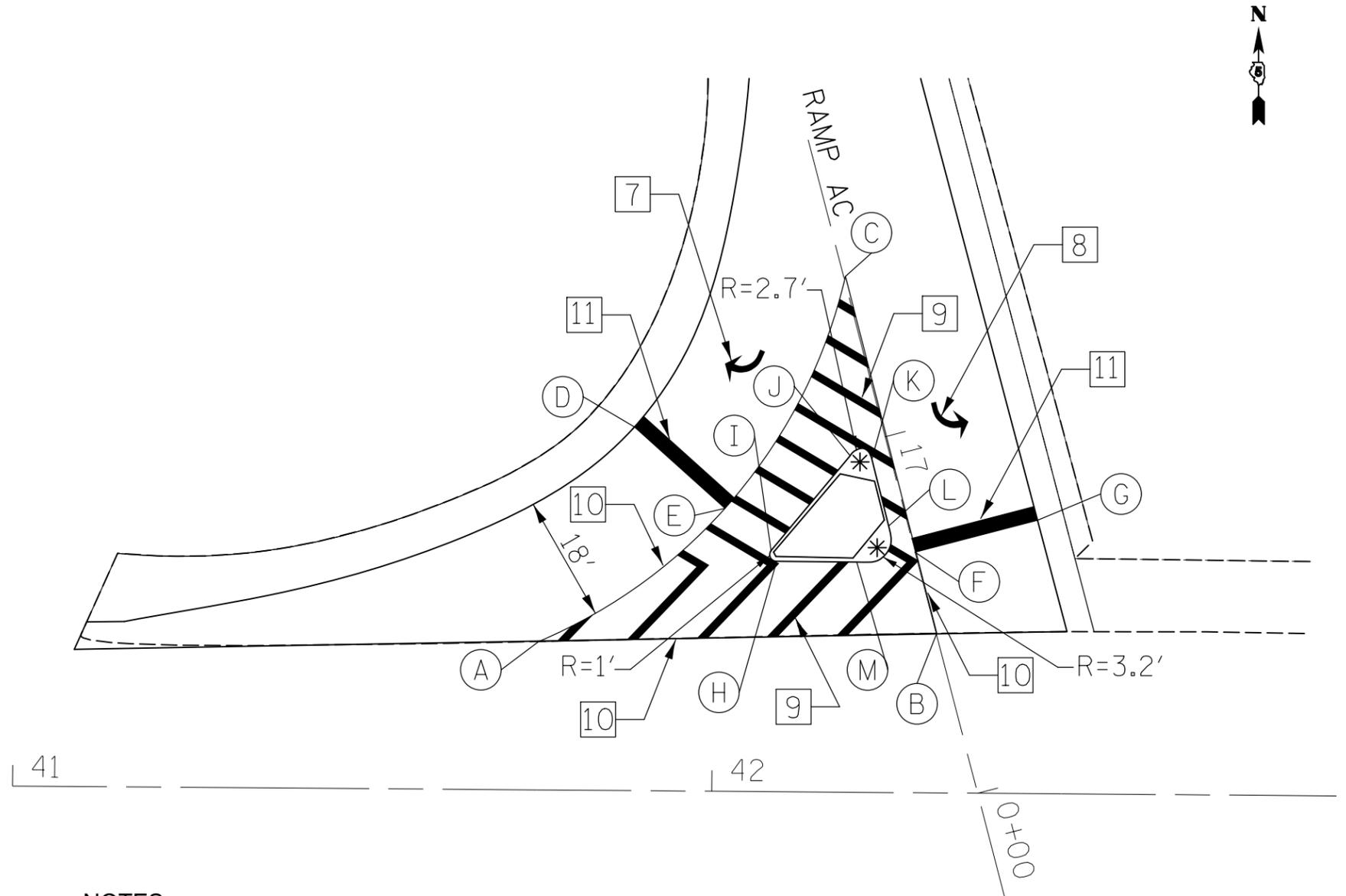


CONCRETE MEDIAN SURFACE REMOVAL  
 (EXIST. CONCRETE SURFACE, 4")

FILE NAME =	USER NAME = piersonbr	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PLAN SHEETS F.A.I.-74 (I-74) RAMPS CB &amp; BD</b>			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 5\Projects\0579\BROWND\Design\0579A60-shr-plan.dgn		CHECKED -	REVISED -		74	(57-24&57-24-1)R5-1	MCLEAN	48	31			
#MODELNAME#	PLOT DATE = 10/6/2016	DATE -	REVISED -		SCALE: 1" = 50' SHEET 1 OF 1 SHEETS STA. TO STA.			CONTRACT NO. 70A60				
ILLINOIS FED. AID PROJECT												

ISLAND & PAVT. MKING. LOCATIONS - RAMP 'AC'			
POINT	STA. RAMP 'AC'	OFFSET (')	
		LT.	RT.
A	17+15.81		53.30
B	17+28.97	0.00	0.00
C	16+76.41	0.00	0.00
D	16+89.62		34.36
E	17+04.13		24.75
F	17+17.00	0.00	0.00
G	17+17.00	18.10	
H	17+13.28		19.49
I	17+11.61		19.89
J	17+01.35		5.54
K	17+02.62		2.65
L	17+12.43		2.71
M	17+16.28		8.44

## PAVEMENT MARKING PLANS – RAMP AC



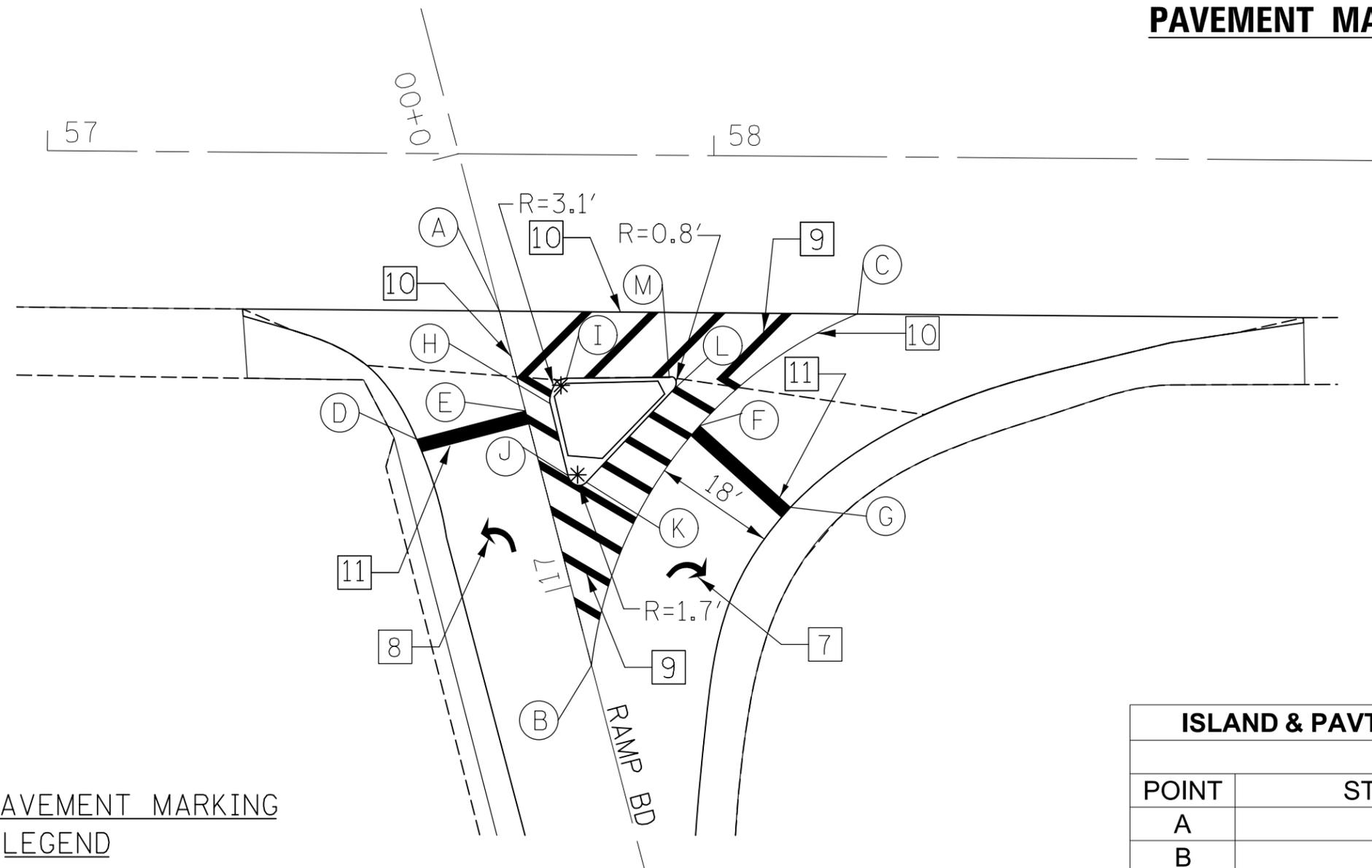
### TYPICAL PAVEMENT MARKING LEGEND

- 7 RIGHT ARROW
- 8 LEFT ARROW
- 9 12" DIAGONAL WHITE
- 10 6" SOLID WHITE
- 11 24" STOP BAR (WHITE)

### NOTES:

- ① \*RAMPED NOSES (SEE HIGHWAY STANDARD 606301-04).
- ② ADDITIONAL COARSE AGGREGATE REQUIRED TO BRING THE CONCRETE MEDIAN SURFACE TO PROPER GRADE WILL BE CONSIDERED INCLUDED IN THE PAY ITEM "CONCRETE MEDIAN SURFACE, 4 INCH" AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

# PAVEMENT MARKING PLANS – RAMP BD



## TYPICAL PAVEMENT MARKING LEGEND

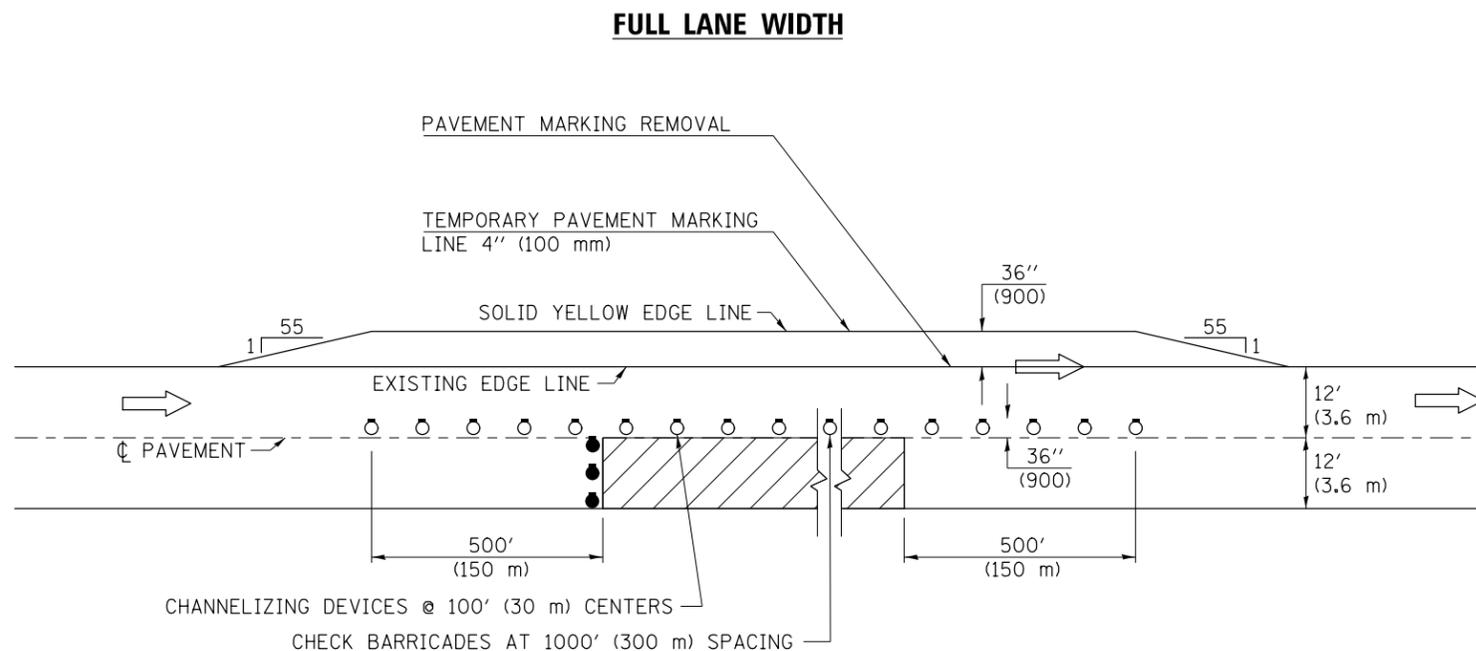
- 7 RIGHT ARROW
- 8 LEFT ARROW
- 9 12" DIAGONAL WHITE
- 10 6" SOLID WHITE
- 11 24" STOP BAR (WHITE)

### NOTES:

- ① \*RAMPED NOSES (SEE HIGHWAY STANDARD 606301-04).
- ② ADDITIONAL COARSE AGGREGATE REQUIRED TO BRING THE CONCRETE MEDIAN SURFACE TO PROPER GRADE WILL BE CONSIDERED INCLUDED IN THE PAY ITEM "CONCRETE MEDIAN SURFACE, 4 INCH" AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.

## ISLAND & PAVT. MKING. LOCATIONS - RAMP 'BD'

POINT	STA. RAMP 'BD'	OFFSET (')	
		LT.	RT.
A	17+42.44	0.00	0.00
B	16+87.62	0.00	0.00
C	17+28.48		51.87
D	17+27.00	16.87	
E	17+27.00	0.00	0.00
F	17+18.27		24.76
G	17+03.01		34.84
H	17+27.12		3.86
I	17+30.19		7.20
J	17+16.03		3.75
K	17+14.37		6.16
L	17+24.51		22.34
M	17+26.40		22.15



TRAFFIC CONTROL DETAILS NOT SHOWN SHALL BE ACCORDING TO TRAFFIC CONTROL STANDARD 701401.

AT RAMPS, THE OVERLAY WORK AREA SHALL BE STAGED TO COORDINATE WITH STANDARD 701411.

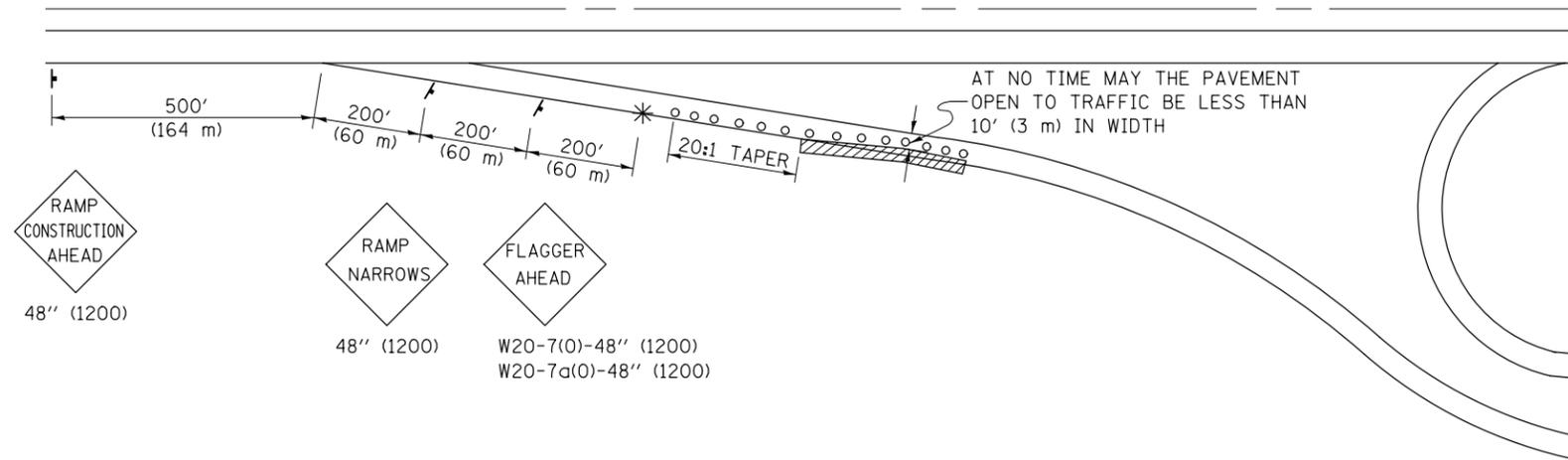
PASSING LANE CLOSURES SHALL BE SIMILAR. THE TEMPORARY PAVEMENT MARKING LINE 4" (100 mm) SHALL BE A SOLID WHITE LINE.

SYMBOLS	
	DIRECTION OF TRAVEL
	WORK AREA
	DRUM WITH STEADY BURN MONODIRECTIONAL LIGHT
	HIGH INTENSITY REFLECTORIZED VERTICAL BARRICADE WITH STEADY BURNING MONODIRECTIONAL LIGHT

**Note:** All dimensions are in INCHES (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = piersonbr	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL AND PROTECTION, STANDARD 701401 (SPECIAL)</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 5\Projects\0579\Drawings\Design\0570A60-shd-Details.dwg	DESIGNED -	REVISED -	74			(57-24 & 57-24-1)RS-1	MCLEAN	48	34	
PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -	CONTRACT NO. 70A60							
*MODELNAME*	DATE -	REVISED -	ILLINOIS FED. AID PROJECT							
					SCALE:	SHEET 1 OF 1 SHEETS		STA.	TO STA.	

**DAY OPERATION ONLY  
PARTIAL RAMP CLOSURE**



SYMBOLS

- TYPE I OR II BARRICADES, VERTICAL BARRICADES OR DRUMS @ 50' (15 m) CTS.
- \* FLAGGER PLACED AS DIRECTED BY THE ENGINEER
- † SIGN ON PORTABLE OR PERMANENT SUPPORT
- ▨ WORK AREA

TYPICAL APPLICATIONS

-

GENERAL NOTES

CONSTRUCTION OPERATIONS SHALL BE CONFINED TO AN AREA NARROW ENOUGH THAT A MINIMUM OF 10' (3 m) OF PAVEMENT SHALL BE OPEN TO TRAFFIC AT ALL TIMES.

FULL WIDTH PAVEMENT ON THE RAMPS SHALL BE OPEN TO TRAFFIC AT NIGHT.

WHEN NO WORK IS BEING PERFORMED, THE FLAGGER WILL NOT BE REQUIRED. IF THE FLAGGER IS NOT PRESENT, THE FLAGGER SIGNS SHALL BE REMOVED OR COVERED.

ALL SIGNS SHALL BE POST MOUNTED IF WORK IN THE AREA EXCEEDS FOUR DAYS OF DAYTIME OPERATIONS.

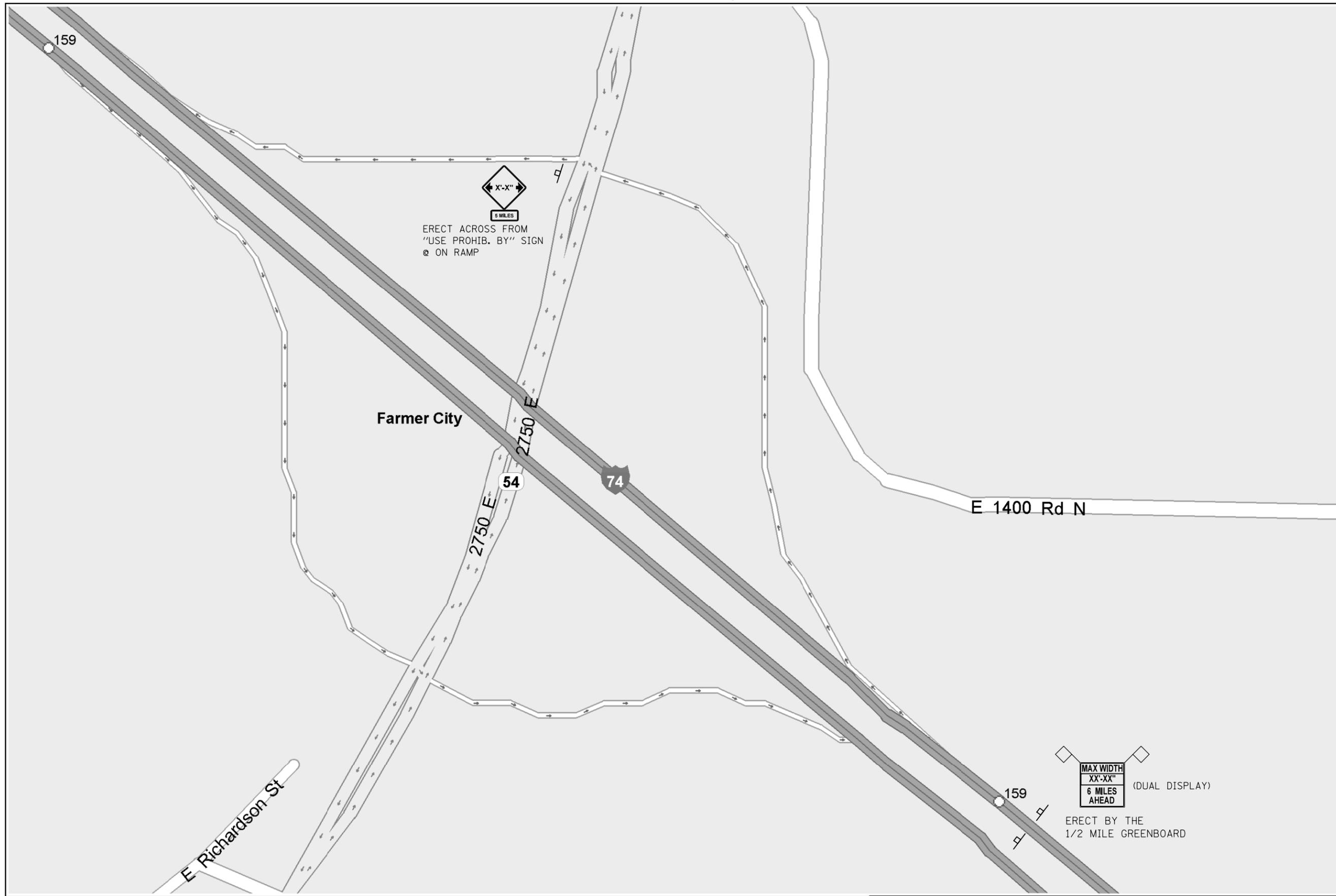
LONGITUDINAL DIMENSIONS MAY BE ADJUSTED SLIGHTLY TO FIT FIELD CONDITIONS.

ALL VEHICLES, EQUIPMENT, WORKERS (EXCEPT FLAGGER) AND THEIR ACTIVITIES ARE RESTRICTED AT ALL TIMES TO ONE SIDE OF THE PAVEMENT UNLESS OTHERWISE AUTHORIZED BY THE DISTRICT ENGINEER.

Traffic Control for all ramps shall be in accordance with the appropriate application of plan detail **TRAFFIC CONTROL FOR RAMPS** and will not be paid for separately, but shall be included in the contract lump sum price for Traffic Control and Protection, (Special).

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = piersonbr	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL FOR RAMPS</b>			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 5\Projects\0579\BROWND\Design\0570A60-sh-t-Details.d	DRAWN	REVISION	REVISION					74	(57-24 & 57-24-1)RS-1	MCLEAN	48	35
\$MODELNAME\$	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 70A60							
	PLOT DATE = 10/6/2016	DATE -	REVISED -		SCALE:	SHEET 1 OF 1 SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT				



FILE NAME =	USER NAME = ppersonbr	DESIGNED -	REVISED -
p:\11\084EBID\INTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 5\Projects\0570A60\0570A60-shr-wrd.dgn	DRAWN	DATE -	REVISED -
*MODELNAME*	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 10/6/2016	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**WIDTH RESTRICTION DETAIL**

SCALE: 1" = 20' SHEET 1 OF 3 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(57-24&57-24-1)RS-1	MCLEAN	48	36
ILLINOIS FED. AID PROJECT			CONTRACT NO. 70A60	



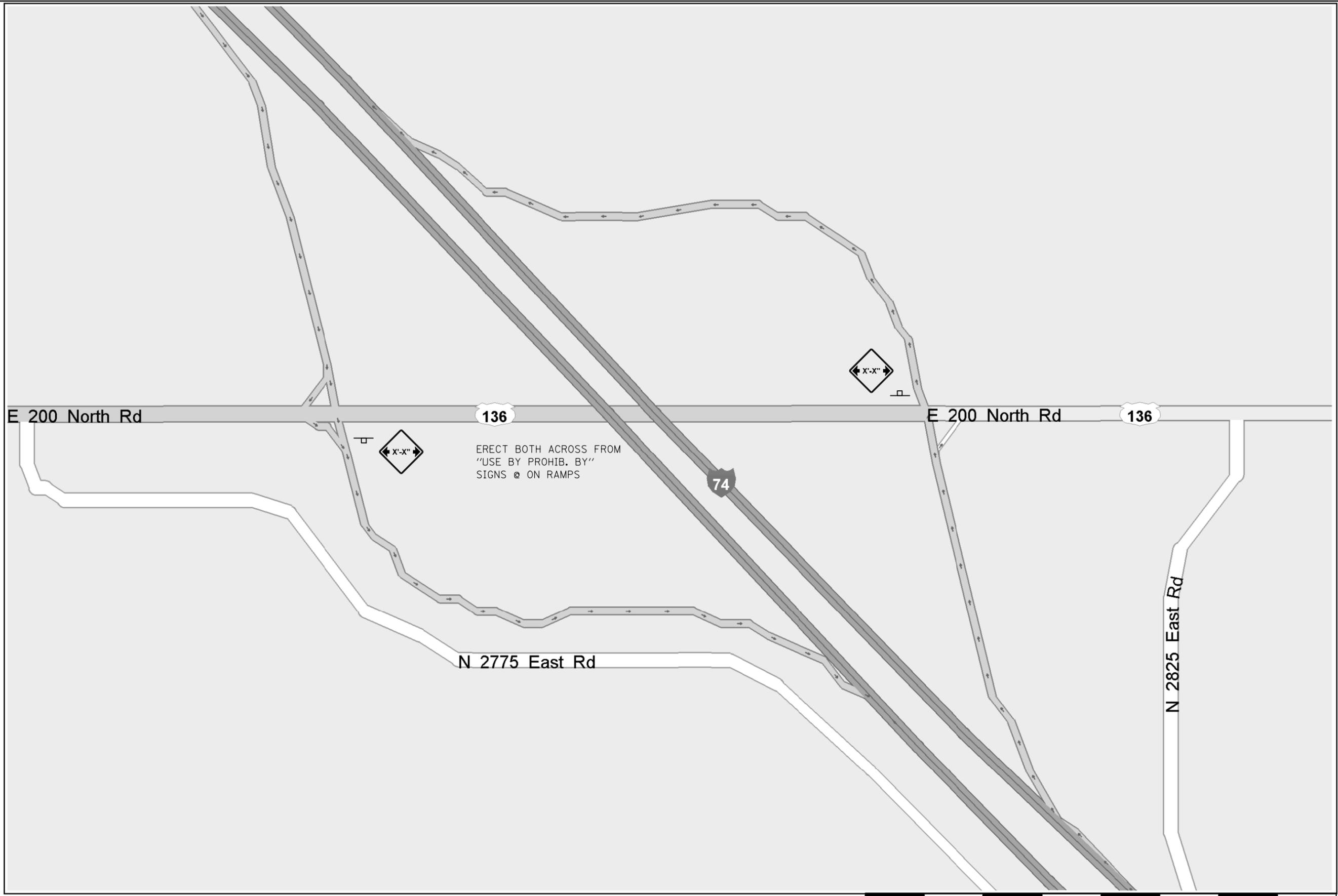
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*MODELNAME*	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 10/6/2016	DATE	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**WIDTH RESTRICTION DETAIL**

SCALE: 1" = 20'    SHEET 2 OF 3 SHEETS    STA.    TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(57-24&57-24-1)RS-1	MCLEAN	48	37
CONTRACT NO. 70A60			ILLINOIS FED. AID PROJECT	



136

136

74

E 200 North Rd

E 200 North Rd

N 2775 East Rd

N 2825 East Rd



ERECT BOTH ACROSS FROM  
"USE BY PROHIB. BY"  
SIGNS @ ON RAMPS



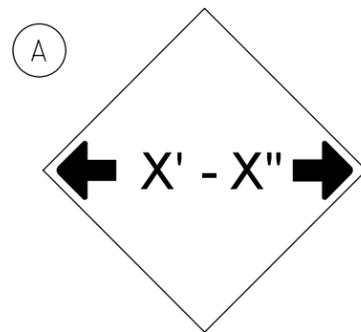
FILE NAME =	USER NAME = piersonbr	DESIGNED -	REVISED -
p:\11\084EBID\INTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 5\Projects\0579\DRAWING\0579A60-shr-WRD.dgn		REVISION	REVISION
*MODELNAME*	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 10/6/2016	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**WIDTH RESTRICTION DETAIL**

SCALE: 1" = 20' SHEET 3 OF 3 SHEETS STA. TO STA.

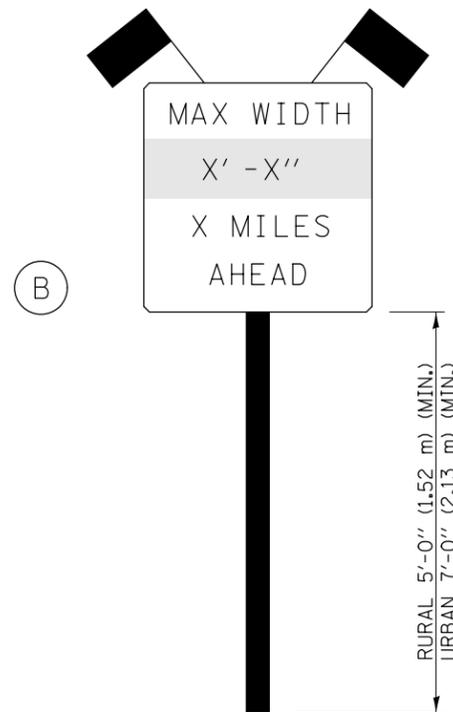
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(57-24&57-24-1)RS-1	MCLEAN	48	38
ILLINOIS FED. AID PROJECT			CONTRACT NO. 70A60	



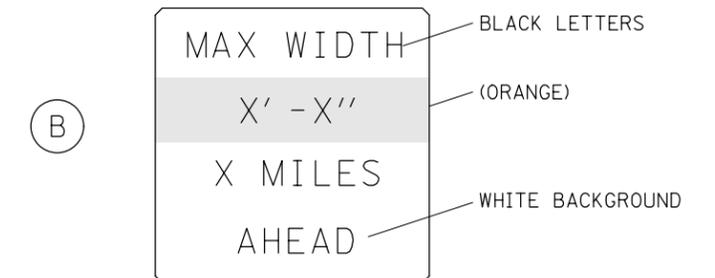
W12-2(0)-48"x48"(1200x1200)

SIGN (A) 2 SIGNS - W12-2(0)-48"x48"(1200x1200) ARE TO BE PLACED AS SHOWN IN THE PLANS OR AS DIRECTED BY THE ENGINEER.

SIGN (B) 2 SIGNS - (SIGN PANEL, TYPE II) AS SHOWN ARE TO BE PLACED AS SHOWN IN THE PLANS OR AS DIRECTED BY THE ENGINEER.



SIGN PANEL, TYPE II



W12-I103(0)-48"x48"(1200x1200)  
"D" LETTERS/NUMBERS

GENERAL NOTES

1. ALL TRAFFIC CONTROL DEVICES SHALL BE FURNISHED, ERECTED AND MAINTAINED BY THE CONTRACTOR.
2. ALL (B) SIGNS SHALL HAVE FLAGS INSTALLED UNLESS OTHERWISE DIRECTED.
3. LOCATIONS OF TRAFFIC CONTROL DEVICES MAY BE ADJUSTED BY THE ENGINEER.
4. ALL TRAFFIC CONTROL SHOWN ON THIS SHEET SHALL BE PAID FOR AT THE CONTRACT LUMP SUM PRICE FOR WIDTH RESTRICTION SIGNING.
5. ALL SIGNS SHALL BE POST MOUNTED UNLESS OTHERWISE DIRECTED.
6. ALL SIGNS SHOWN ORANGE (O) SHALL BE FLUORESCENT ORANGE.
7. ALL SIGNS SHOWN SHALL CONSIST OF THE CURRENT RETROREFLECTIVE SHEETING REQUIREMENTS AS OUTLINED IN SECTION 1106.01 OF THE STANDARD SPECIFICATIONS BOOK.

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = ppersonbr	DESIGNED -	REVISED - 03/11 - KJT
pw:\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 5\Projects\0579\DRAWING\Design\0579A60-sh-t-Details.d		DRAWN -	REVISED - 05/08
*MODELNAME*	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED - 10/08 - KJT
	PLOT DATE = 10/6/2016	DATE -	REVISED - 07/09 - KJT

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

WIDTH RESTRICTION SIGNING

SCALE: SHEET 1 OF 1 SHEETS STA. TO STA.

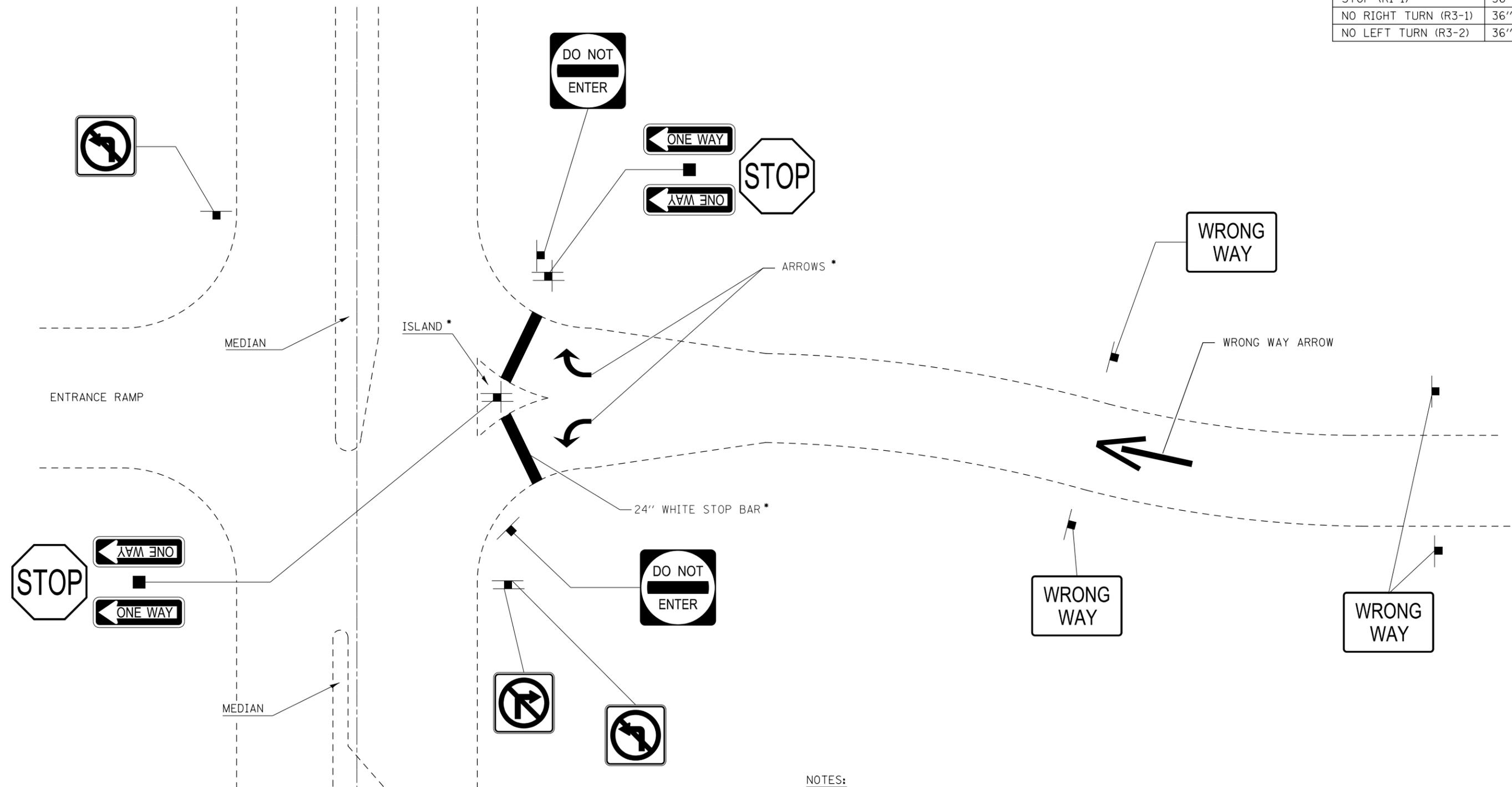
DISTRICT 5 DETAIL NO. X7200201

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(57-24&57-24-1)RS-1	MCLEAN	48	39
CONTRACT NO. 70A60			ILLINOIS FED. AID PROJECT	

# WRONG WAY ARROW PAVEMENT MARKING PLACEMENT DETAIL FOR DIAMOND INTERCHANGE EXIT RAMP

(57-24&57-24-1)RS-1

EXISTING SIGN TYPE	SIZE
DO NOT ENTER (R5-1)	48" X 48"
WRONG WAY (R5-1A)	42" X 30"
ONE WAY (R6-1L/R)	54" X 18"
STOP (R1-1)	36" X 36"
NO RIGHT TURN (R3-1)	36" X 36"
NO LEFT TURN (R3-2)	36" X 36"



\* SEE STRIPING PLAN SHT. XX FOR ALL OTHER PAVEMENT MARKING PLACEMENT

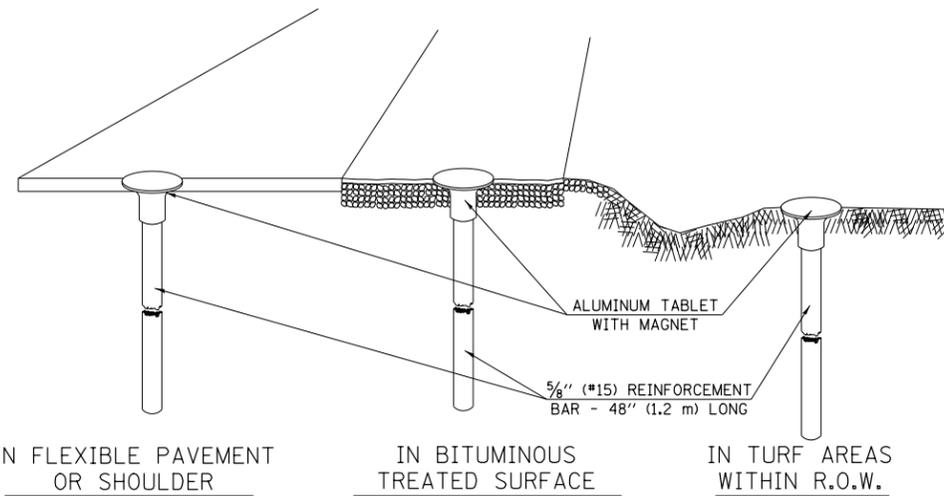
**NOTES:**

- ① ALL SIGNAGE IS EXISTING.
- ② WRONG WAY ARROW PAVEMENT MARKING SHALL BE ALIGNED WITH WRONG WAY (R5-1A) SIGNS NEAREST THE INTERSECTION. THE TIP OF THE ARROWHEAD TO BE INLINE WITH THE WRONG WAY SIGN POSTS.

FILE NAME =	USER NAME = ppersonbr	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>WRONG WAY ARROW PAVEMENT MARKING PLACEMENT DETAIL FOR DIAMOND INTERCHANGE EXIT RAMP</b>	F.A.I. RTE. =	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
p:\11\084EBIDINTEG.illinois.gov\PI\DOT\Documents\DOT Offices\District 5\Projects\05724\Drawings\Design\570A60-shr-Detail.dwg						DRAWN	REVISED	74	(57-24 & 57-24-1)RS-1	MCLEAN	48	40
PLOT SCALE = 40.0000' / in.						CHECKED -	REVISED -	CONTRACT NO. 70A60				
*MODELNAME*						DATE -	REVISED -	ILLINOIS FED. AID PROJECT				
SCALE:						SHEET 1 OF 1 SHEETS		STA.		TO STA.		

## XZ193300 – SURVEY MARKER, TYPE 1 (SPECIAL)

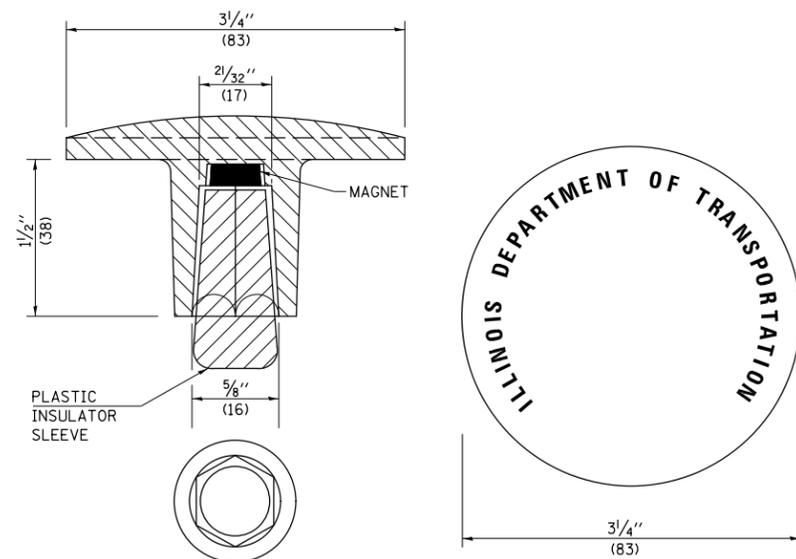
TO BE INSTALLED IN FLEXIBLE PAVEMENT OR SHOULDER, BITUMINOUS TREATED SURFACE AND TURF AREAS WITHIN THE RIGHT-OF-WAY FOR PRESERVING PERMANENT SURVEY MARKERS (PI'S, PT'S, PC'S, POC'S, & POT'S)



IN FLEXIBLE PAVEMENT  
OR SHOULDER

IN BITUMINOUS  
TREATED SURFACE

IN TURF AREAS  
WITHIN R.O.W.



THE DIMENSIONS SHOWN SHALL BE EXACT, OTHERS MAY VARY, BUT SHALL BE SHOWN ON SHOP DRAWINGS.

### GENERAL NOTES

1. THE CONTRACT UNIT PRICE, EACH, FOR SURVEY MARKER, TYPE 1 (SPECIAL) SHALL BE PAYMENT IN FULL FOR FURNISHING THE REINFORCEMENT BAR AND ALUMINUM TABLET AND FOR ALL LABOR AND MATERIAL REQUIRED TO SET THE MARKER IN PLACE.
2. ALL SURVEY MARKERS, TYPE 1 (SPECIAL) SHALL BE PLACED  $\pm 1/4$ " (6 mm) BELOW THE FINAL SURFACE. ALSO, THE CONTRACT UNIT PRICE SHALL INCLUDE REMOVAL AND DISPOSAL OF THE EXISTING MARKER AT STA. 427+83.08, 0.00' OFFSET IN ACCORDANCE WITH ARTICLE 202.03 OF THE STANDARD SPECIFICATIONS.
3. WHEN THE TABLET AND REBAR ARE PLACED AS PART OF A SURVEY MARKER VAULT, THEY SHALL BE CONSIDERED AS INCLUDED IN THAT PAY ITEM AND THERE WILL BE NO PAYMENT FOR THE SURVEY MARKER, TYPE 1 (SPECIAL).

### SPECIFICATIONS FOR ALUMINUM TABLET

SURVEY CAP FOR REBAR.  $3/4$ " (83 mm) CONVEX SURVEY CAP FOR  $5/8$ " (15 mm) REBAR WITH ILLINOIS DEPARTMENT OF TRANSPORTATION LOGO. THIS LOGO SHALL PROVIDE LETTERS RECESSED INTO THE SURFACE A MINIMUM OF  $1/32$ " (0.8 mm) FOR EASY AND LONG-TERM LEGIBILITY. THE ALUMINUM CAP FOR REBAR SHALL BE PRODUCED BY THE PROCESS OF ORBITAL FORGING TO PRODUCE A HIGH-STRENGTH AND DURABLE MARKER CAP WHICH WILL NOT CHIP OR BREAK AND PROVIDE A SMOOTH FINISH FOR STAMPING OF DATA IN THE FIELD. THE ALUMINUM CAP FOR REBAR SHALL BE TAPERED FOR A PERFECT COMPRESSION FIT. A SPECIAL PLASTIC INSULATOR SHALL BE INSTALLED TO PREVENT DISSIMILAR METAL CONTACT AND CORROSION. THE PLASTIC INSULATOR SHALL FORM READILY TO THE OUTER SHAPE OF THE REBAR AND TO THE INNER SHAPE OF THE ALUMINUM CAP SOCKET. THE PLASTIC INSULATOR SHALL BE LOW DENSITY POLYETHYLENE, A MINIMUM  $1/2$ " (38 mm) LONG AND CONFORM TO FEDERAL SPECIFICATION L-P 390.

COMPOSITION: ALUMINUM 98.3-98.7%; OTHER 1.3-1.7%; STRENGTH: YIELD 28 KSI (193 MPa), ULTIMATE 32 KSI (221 MPa). ELONGATION 15% [IN 2" (50 mm)]. SPECIFICATIONS: ALUMINUM ALLOY 6101-0; ASTM B317-83 (EXCEPT TEMPER) AS FORGED. NO EXCEPTIONS.

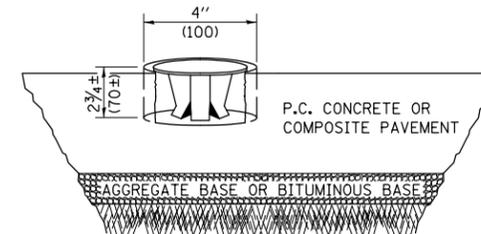
### SPECIFICATIONS FOR REBAR

REBAR FOR ALUMINUM TABLET. REINFORCEMENT BAR SHALL BE  $5/8$ " (#15) X 48" (1.2 m) (DEFORMED).

INSPECTION OF REINFORCEMENT BAR  $5/8$ " (#15) SHALL BE DONE BY DISTRICT PERSONNEL OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION, DIVISION OF HIGHWAYS.

## XZ193400 – SURVEY MARKER, TYPE 2 (SPECIAL)

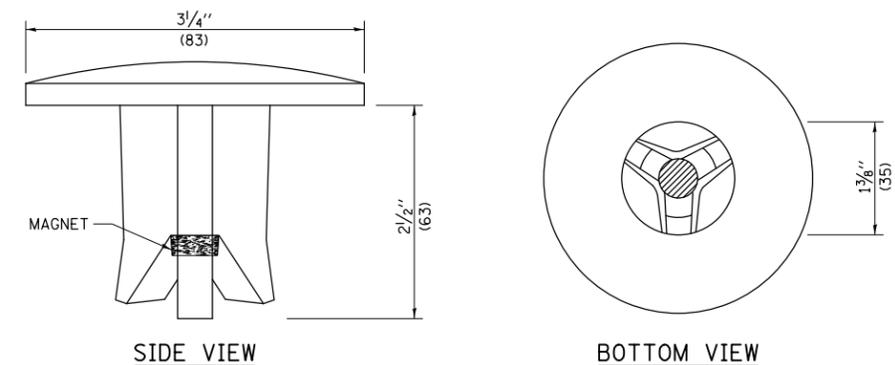
TO BE INSTALLED IN RIGID OR COMPOSITE PAVEMENT FOR PRESERVING PERMANENT SURVEY MARKERS (PI'S, PT'S, PC'S, POC'S, & POT'S)



### SPECIFICATIONS FOR ALUMINUM TABLET (FORKED)

ALUMINUM TABLET (FORKED) FOR USE WITH "SURVEY MARKER, TYPE 2, (SPECIAL)" SHALL BE AS SHOWN ON THE DETAIL FOR THE  $3/4$ " (83 mm) CONVEX SURVEY TABLET WITH ILLINOIS DEPARTMENT OF TRANSPORTATION LOGO. THIS LOGO SHALL PROVIDE FOR LETTERS RECESSED INTO THE SURFACE A MINIMUM OF  $1/32$ " (0.8 mm) FOR EASY AND LONG-TERM LEGIBILITY. THE ALUMINUM TABLET SHALL BE PRODUCED BY THE PROCESS OF ORBITAL FORGING TO PRODUCE A HIGH-STRENGTH AND DURABLE MARKER CAP WHICH WILL NOT CHIP OR BREAK AND PROVIDE A SMOOTH FINISH FOR STAMPING OF DATA IN THE FIELD. THE ALUMINUM TABLET SHALL BE DESIGNED NOT TO TURN OR ROTATE. THREE PRONGS ON A  $2/2$ " (63 mm) STEM SHALL BE SUCH THAT THE ALUMINUM TABLET CANNOT BE EASILY REMOVED.

COMPOSITION: ALUMINUM 92-93%; MAGNESIUM 6.5-7.5%. STRENGTH: YIELD 19,000-21,000 PSI (131-145 MPa); TENSILE 38,000-44,000 PSI (262-303 MPa); ELONGATION 10-15% [IN 2" (50 mm)]. SPECIFICATIONS: ALLOY 535.0; QQ-A-601ES. NO EXCEPTIONS.



SIDE VIEW

BOTTOM VIEW

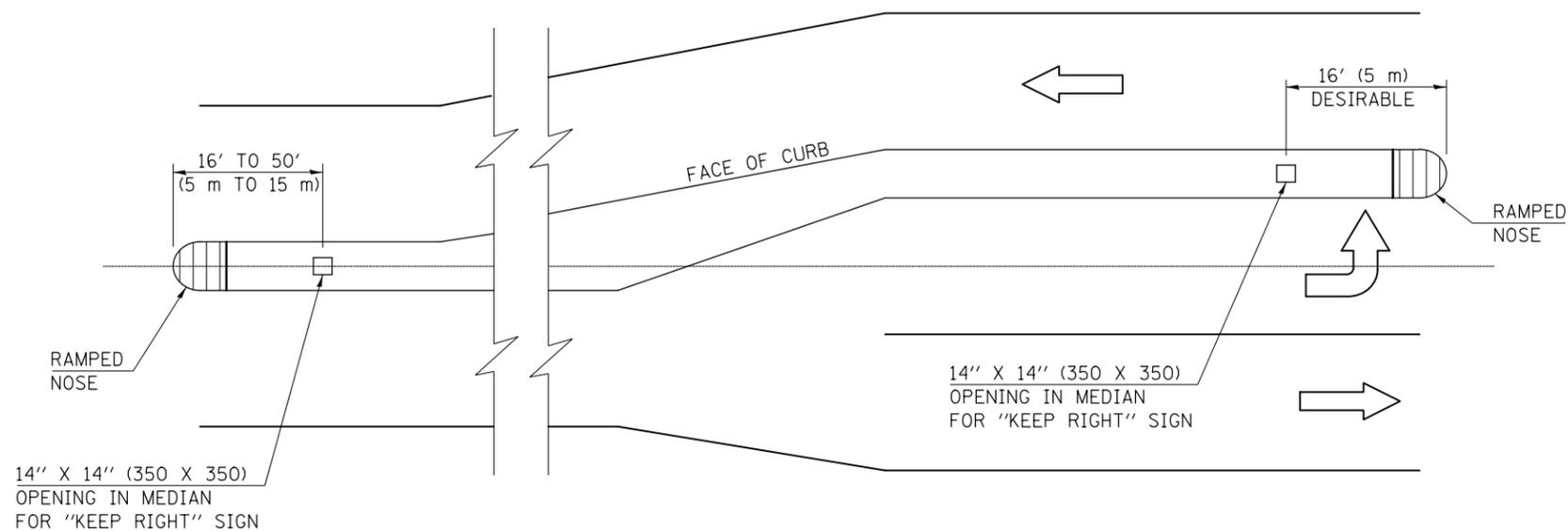
THE DIMENSIONS SHOWN SHALL BE EXACT, OTHERS MAY VARY, BUT SHALL BE SHOWN ON SHOP DRAWINGS.

### GENERAL NOTES

1. WORK ON THIS ITEM SHALL NOT START UNTIL THE FINAL SURFACE IS COMPLETED.
2. THE ALUMINUM TABLET (FORKED) SHALL REST UPON THE BOTTOM OF THE 4" (100 mm) CORE HOLE. IF THE HOLE IS TOO DEEP, EPOXY GROUT MUST BE USED TO DECREASE THE DEPTH AND ALLOWED TO HARDEN BEFORE PROCEEDING.
3. THE ALUMINUM TABLET SHALL BE ANCHORED IN THE 4" (100 mm) DIAMETER HOLE IN THE NEW PAVEMENT WITH TWO-COMPONENT EPOXY CONFORMING TO APPLICABLE PORTIONS OF ARTICLE 1025.01 OF THE STANDARD SPECIFICATIONS.
4. THE 4" (100 mm) CORE HOLE SHALL BE SUBJECT TO THE APPROVAL OF THE ENGINEER.
5. THE CONTRACT PRICE, EACH, FOR SURVEY MARKER, TYPE 2 (SPECIAL) SHALL BE PAYMENT IN FULL FOR FURNISHING THE ALUMINUM TABLET AND FOR ALL LABOR AND MATERIAL REQUIRED TO SET THE MARKER IN PLACE, AS SPECIFIED, INCLUDING CORING THE NEW PAVEMENT.
6. ALL SURVEY MARKERS, TYPE 2 (SPECIAL) SHALL BE PLACED  $\pm 1/4$ " (6 mm) BELOW THE FINAL SURFACE.

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

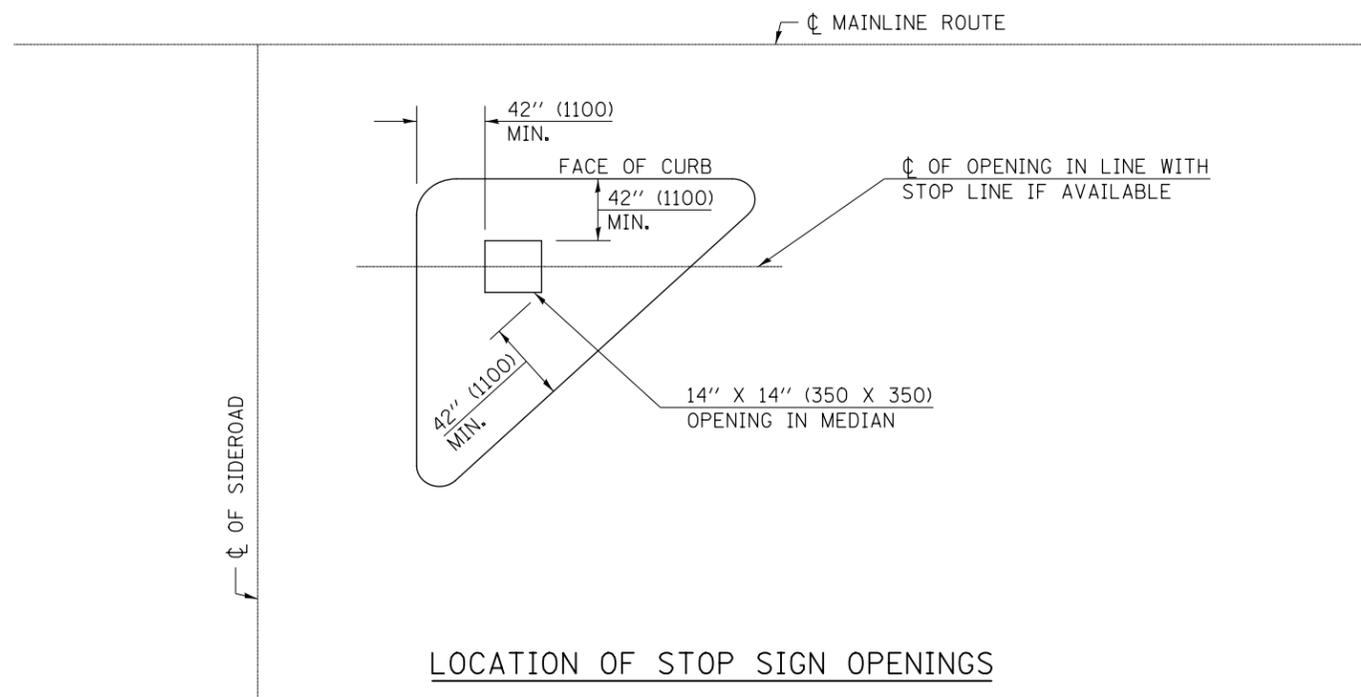
FILE NAME =	USER NAME = piersonbr	DESIGNED -	REVISED - 11/06	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SURVEY MARKERS TYPE 1 &amp; 2 (SPECIAL)</b>			F.A.I. RT.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\11\084EBIDINTEG\Illinois.gov\PIWIDOT\Documents\DOT Offices\District 5\Projects\0579\Drawings\Design\0579A60-shd-Details.dwg	DRAWN	REVISED - 11/10	74					(57-24&57-24-JRS-1	MCLEAN	48	41	
*MODELNAME*	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -		CONTRACT NO. 70A60							
	PLOT DATE = 10/6/2016	DATE -	REVISED -		SCALE:	SHEET 1 OF 1 SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT				



LOCATION OF OPENINGS FOR "KEEP RIGHT" SIGNS

GENERAL NOTES

1. ALL SMALL ISLANDS SHALL BE CONSTRUCTED WITH THE STOP SIGN ISLANDS AS SHOWN, UNLESS OTHERWISE SPECIFIED.
2. OPENINGS FOR SIGNS IN MEDIANS SHALL BE AS SHOWN OR AS DIRECTED BY THE ENGINEER.
3. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR THE TYPE OF MEDIAN SPECIFIED IN THE PLANS AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.



LOCATION OF STOP SIGN OPENINGS

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

**DISTRICT 5 DETAIL NO. 60623711**

FILE NAME =	USER NAME = piersonbr	DESIGNED -	REVISED -
p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 5\Projects\0579\BROWND\Design\570A60-shit-Details.d		DRAWN -	REVISED -
	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED -
\$MODELNAME\$	PLOT DATE = 10/6/2016	DATE -	REVISED -

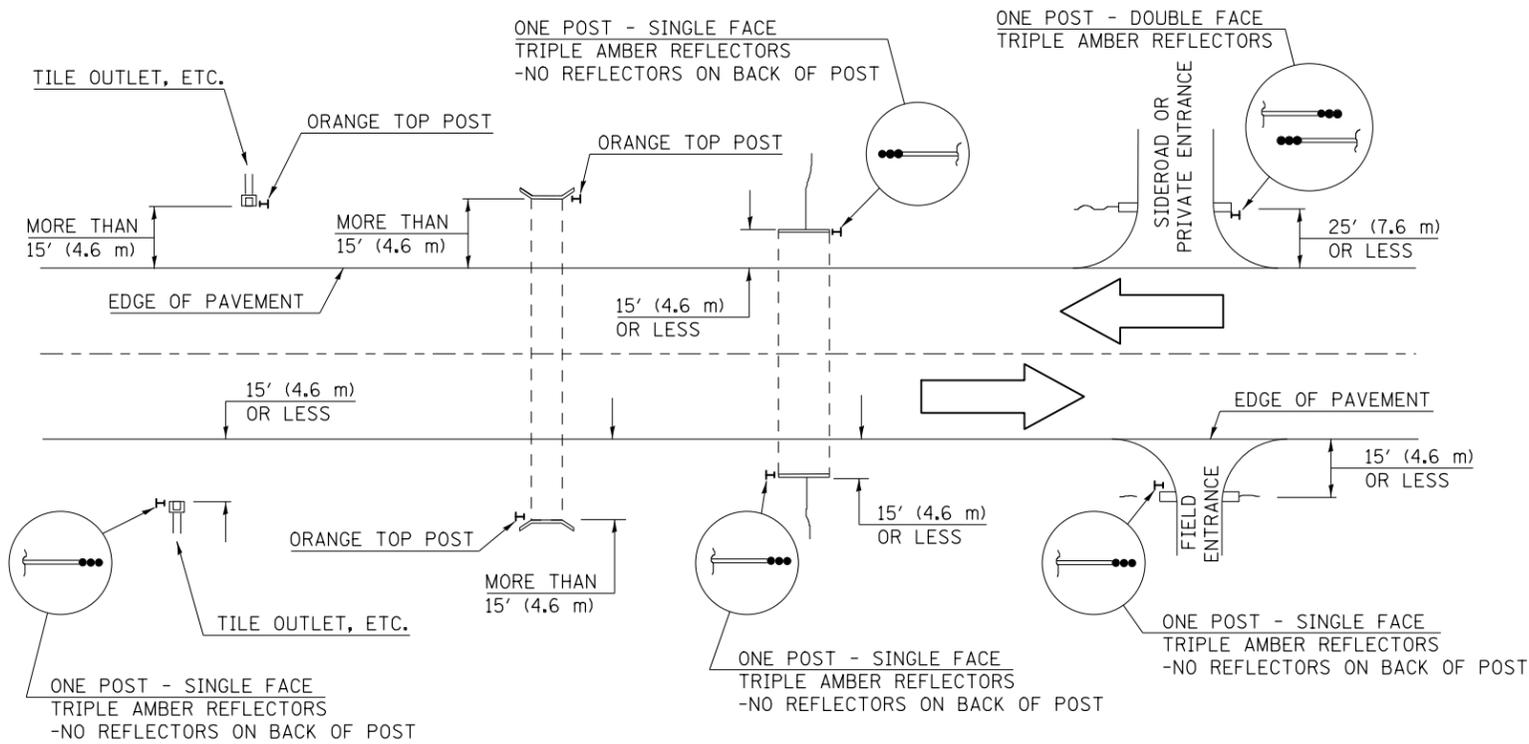
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SIGN POST BLOCKOUTS**

SCALE: SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(57-24&57-24-1)RS-1	MCLEAN	48	42
CONTRACT NO. 70A60			ILLINOIS FED. AID PROJECT	

IDENTIFICATION OF ROADSIDE HAZARDS FOR TWO-LANE ROADWAYS



BILL OF MATERIALS

DELINEATOR TYPE	SINGLE FACE	DOUBLE FACE	NO REFLECTOR	TOTAL DELINEATORS
SINGLE CRYSTAL		N/A	N/A	
DOUBLE CRYSTAL			N/A	
SINGLE AMBER			N/A	
DOUBLE AMBER		N/A	N/A	
TRIPLE AMBER			N/A	
ORANGE TOP	N/A	N/A		
			TOTAL	

NOTES

DELINEATORS FOR ROADSIDE HAZARDS SHALL ONLY BE PLACED AT LOCATIONS WHERE THERE IS NO GUARDRAIL, OR OTHER PERMANENT BARRIER, ON THE SAME SIDE OF ROAD AS THE HAZARD.

DELINEATORS FOR ROADSIDE HAZARDS SHALL ONLY BE PLACED AT LOCATIONS WHERE DELINEATORS ARE NOT IN PLACE ALONG THE EDGE OF SHOULDER.

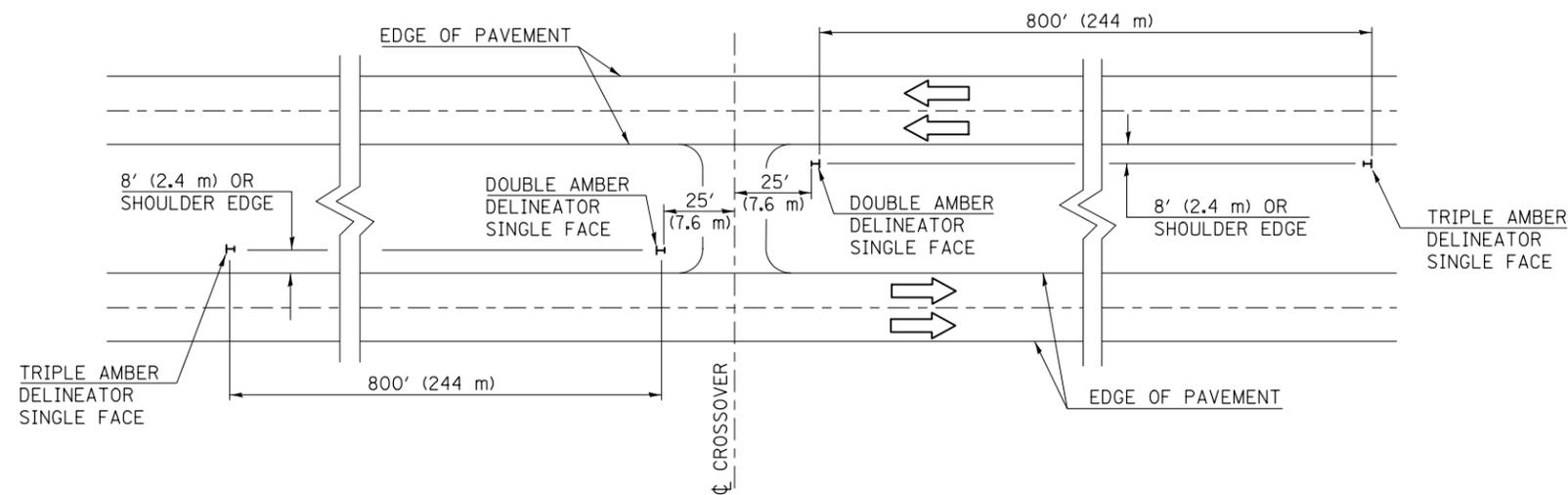
EACH POST SHALL BE CONSIDERED AS ONE DELINEATOR FOR PAYMENT, REGARDLESS OF THE NUMBER OF DELINEATORS ATTACHED TO IT.

POSTS INDICATED AS "ORANGE TOP" SHALL HAVE NO REFLECTORS. THEY SHALL HAVE THE TOP 12" (300 mm) (MINIMUM) OF THE POST PAINTED A BRIGHT ORANGE COLOR SIMILAR TO CONSTRUCTION SIGNS, AND SHALL MEET THE APPROVAL OF THE ENGINEER. FLUORESCENT PAINT OR OTHER SPECIAL RETROREFLECTIVE COATINGS WILL NOT BE REQUIRED.

FOR ONE-WAY ROADWAYS THE APPLICATION SHALL BE SIMILAR WITH DELINEATORS PLACED ON THE TRAFFIC APPROACH SIDE OF HAZARDS AND OBJECTS. ONLY SINGLE FACE DELINEATORS WILL BE REQUIRED ON ONE-WAY ROADWAYS.

FOR OTHER DELINEATOR APPLICATIONS, REFER TO HIGHWAY STANDARD 635001.

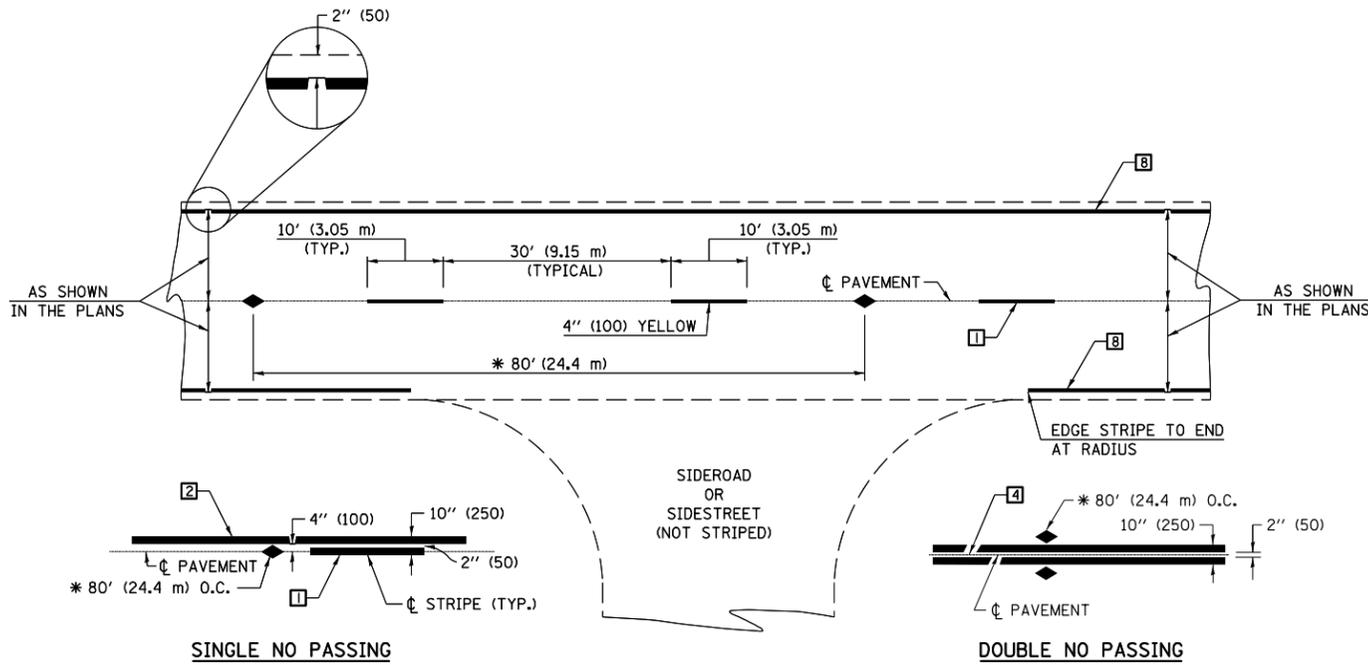
MEDIAN DELINEATORS AT CROSSOVER  
(FOR INTERSTATES, EXPRESSWAYS, DUAL HIGHWAYS)



Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

DISTRICT 5 DETAIL NO. 63500105

FILE NAME =	USER NAME = piersonbr	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DELINEATORS</b>			F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\IL\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 5\Projects\0579\Drawings\Design\0579A60-shd-Detail.dwg		CHECKED -	REVISED -		74	(57-24&57-24-1)RS-1	MCLEAN	48	43			
\$MODELNAME\$	PLOT SCALE = 40.0000' / in.	DATE -	REVISED -		CONTRACT NO. 70A60							
	PLOT DATE = 10/6/2016				ILLINOIS FED. AID PROJECT							



\* REDUCE TO 40' (12.2 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEEDS OF 45 mph (70 km/h) OR LESS.

**TWO LANE/TWO WAY**

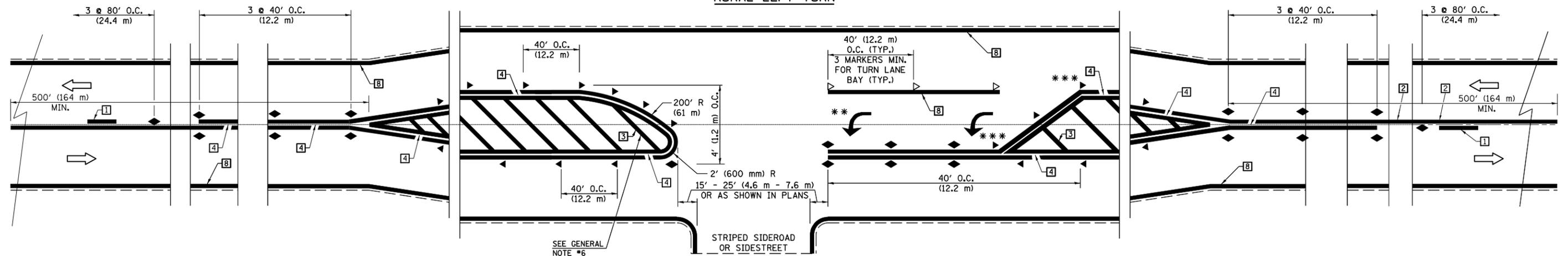
**TYPICAL PAVEMENT MARKING LEGEND**

- 1 4" (100) SKIP-DASH (YELLOW)
- 2 4" (100) SOLID (YELLOW)
- 3 12" (300) DIAGONAL (YELLOW)
- 4 4" (100) DOUBLE YELLOW (NARROW)
- 5 RESERVED
- 6 RESERVED
- 7 4" (100) SKIP-DASH (WHITE)
- 8 4" (100) SOLID (WHITE)
- 9 12" (300) DIAGONAL (WHITE)
- 10 6" (150) SOLID (WHITE)
- 11 24" (600) STOP BAR (WHITE)
- 12 8" (200) SOLID (WHITE)
- 13 4" (100) LANE LINE EXTENSIONS (WHITE)
- 14 4" (100) PARKING WHITE

**TYPICAL PAVEMENT MARKERS LEGEND**

- ◆ TWO-WAY AMBER MARKER
- ▶ ONE-WAY AMBER MARKER
- ▷ ONE-WAY CRYSTAL MARKER

**RURAL LEFT TURN**



\*\*\* REDUCE SPACING IF NECESSARY TO ASSURE MARKERS AT CORNER POINTS.

\*\* TURN ARROWS SHALL BE PLACED AS SHOWN ON SHEET #2.

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

**DISTRICT 5 DETAIL NO. 7800AAA**

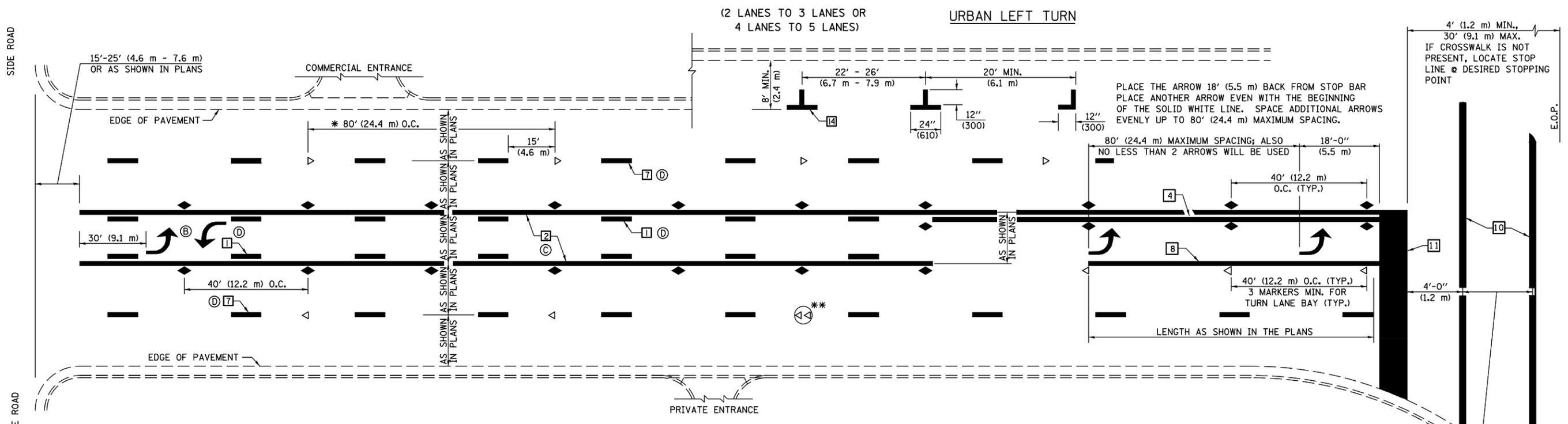
FILE NAME =	USER NAME = piersonbr	DESIGNED -	REVISED - 11/06
p:\1\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\DOT Offices\District 5\Projects\0579\Drawings\Design\0579A60-shd-Detail.dwg		DRAWN -	REVISED - 09/2009 - KJT
	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED - 04/14 - JLA
*MODELNAME*	PLOT DATE = 10/6/2016	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING AND MARKERS  
(RURAL & URBAN APPLICATIONS)**

SCALE: SHEET 1 OF 4 SHEETS STA. TO STA.

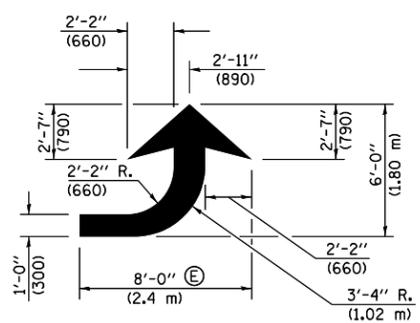
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(57-24&57-24-1)RS-1	MCLEAN	48	44
CONTRACT NO. 70A60			ILLINOIS FED. AID PROJECT	



\* REDUCE TO 40 FEET (12.2 METERS) ON CENTER ON CURVES WHERE ADVISORY SPEEDS ARE 10 MPH (15 km/h) LOWER THAN POSTED SPEEDS.

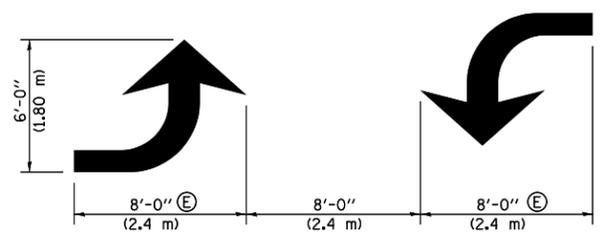
\*\* DOUBLE LANE LINE MARKERS SHALL BE SPECIFIED AND SPACED AS SHOWN IN HIGHWAY STANDARD 781001 FOR MULTI-LANE DIVIDED AND UNDIVIDED HIGHWAYS.

- GENERAL NOTES:**
- ⓑ TURN ARROW PAIRS SHALL BE PLACED AT 250' (75 m) INTERVALS AND SHALL BE EVENLY SPACED BETWEEN BOTH ENDS OF THE BIDIRECTIONAL LEFT TURN LANE.
  - ⓒ THE SOLID YELLOW PAVEMENT MARKINGS [2] SHOULD GENERALLY START OR END NEAR THE RADIUS POINT OF EACH STREET RETURN EXCEPT WHERE ONE OR BOTH ENDS WOULD INCLUDE STOP BARS.
  - ⓓ THE SKIP-DASH PAVEMENT MARKINGS [1] OR [7] SHOULD BE CENTERED BETWEEN BOTH ENDS OF EACH CITY BLOCK AND SHALL BE PLACED SO THEY LINE UP ACROSS FROM EACH OTHER. SEE EXAMPLE ON SHEET 2 OF 3.
  - ⓔ USE LARGE ARROW SIZE FOR BOTH RURAL AND URBAN LOCATIONS. (SEE LAST PAGE OF SECTION 780x FOR SYMBOLS TABLE)



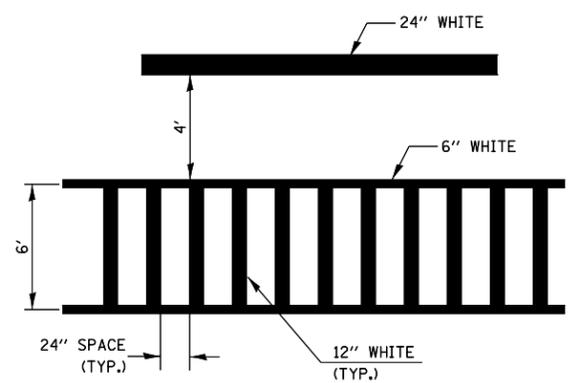
**LEFT ARROW**

REVERSE FOR RIGHT ARROW  
 AREA = 15.6 SQ. FT. (1.47 m<sup>2</sup>)  
 (WHITE)

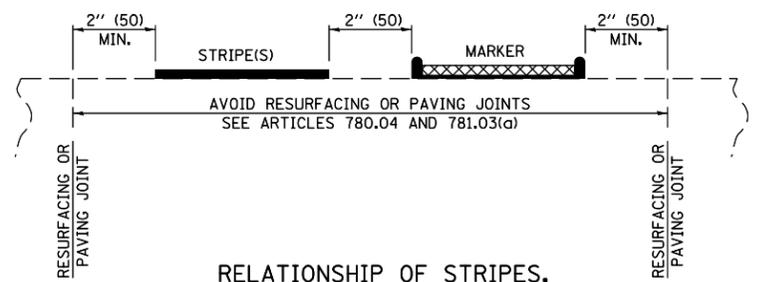


**TYPICAL DOUBLE TURN ARROWS (WHITE)**

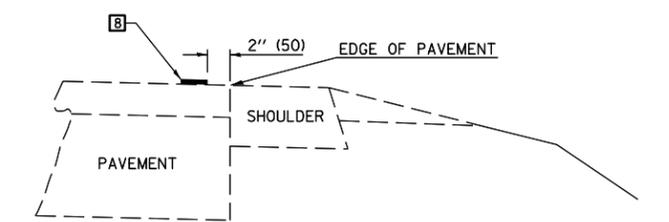
**BLOOMINGTON-NORMAL CITY LIMITS ONLY**



**TYPICAL SPACING FOR CROSSWALKS & STOP BARS**



**RELATIONSHIP OF STRIPES, MARKERS AND JOINTS**

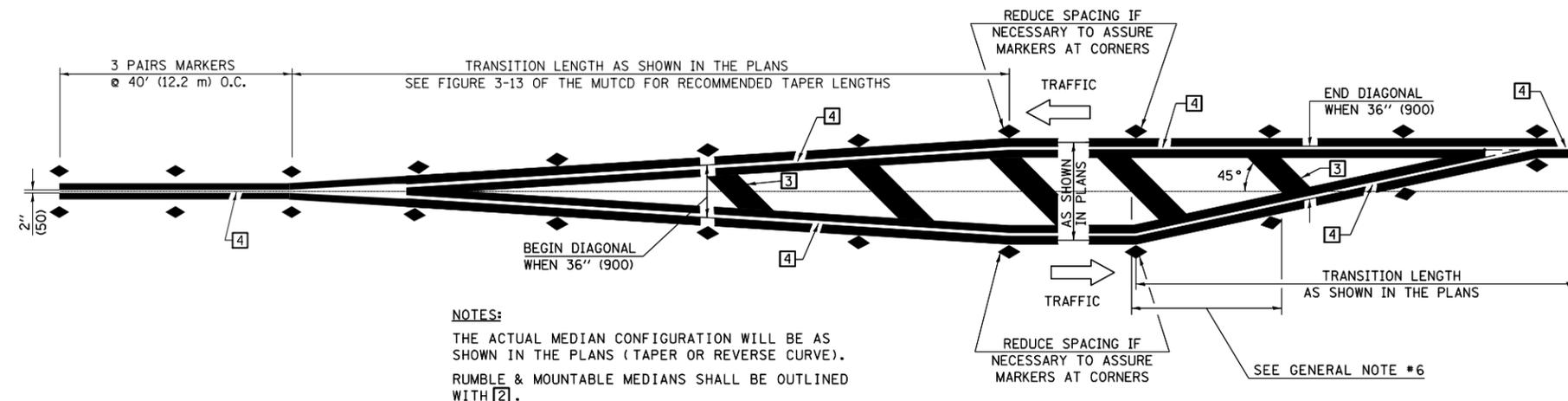


**RELATIONSHIP OF EDGE LINE TO EDGE OF PAVEMENT (SAFETY SHOULDER OR PAVED SURFACE) SEE ARTICLE 780.04**

CROSSWALK WIDTH 6'-0" (1.8 m) OR AS SHOWN IN THE PLANS

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = piersonbr	DESIGNED -	REVISED - 11/06	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT MARKING AND MARKERS (RURAL &amp; URBAN APPLICATIONS)</b>	F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw:\11\084EBIDINTEG\illinois.gov\PIWIDOT\Documents\DOT Offices\District 5\Projects\0579\Drawings\Design\570A60-shd-Detail.dwg	DESIGNED -	REVISED - 09/2009 - KJT	74			(57-24&57-24-1)RS-1	MCLEAN	48	45	
*MODELNAME*	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED - 04/14 - JLA			CONTRACT NO. 70A60				
	PLOT DATE = 10/6/2016	DATE -	REVISED -			ILLINOIS FED. AID PROJECT				

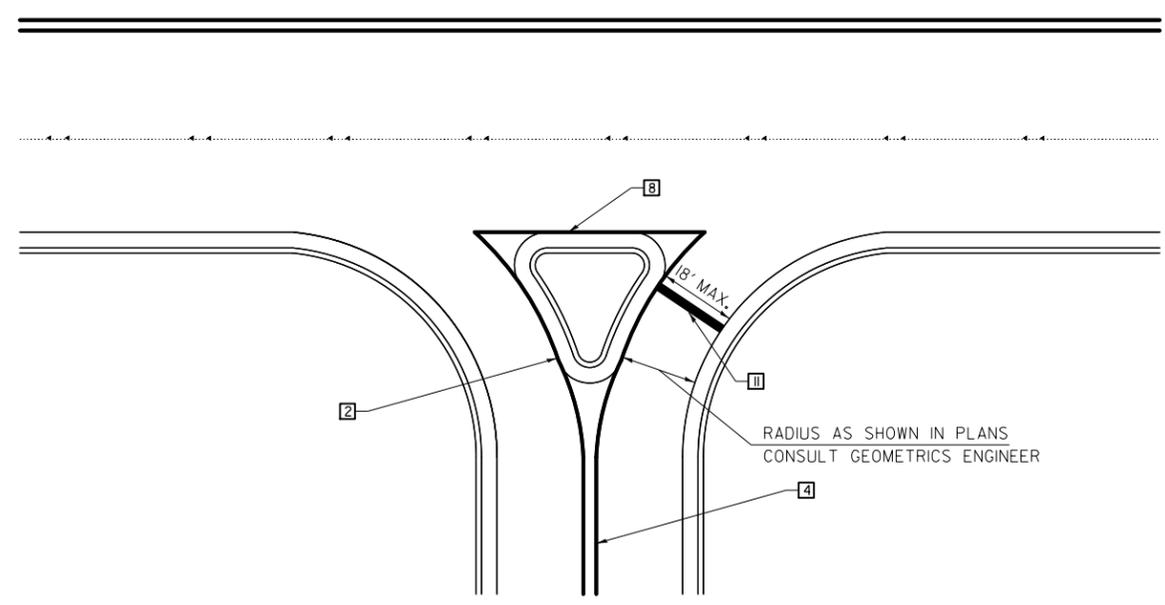


**NOTES:**  
 THE ACTUAL MEDIAN CONFIGURATION WILL BE AS SHOWN IN THE PLANS (TAPER OR REVERSE CURVE).  
 RUMBLE & MOUNTABLE MEDIANS SHALL BE OUTLINED WITH [2].

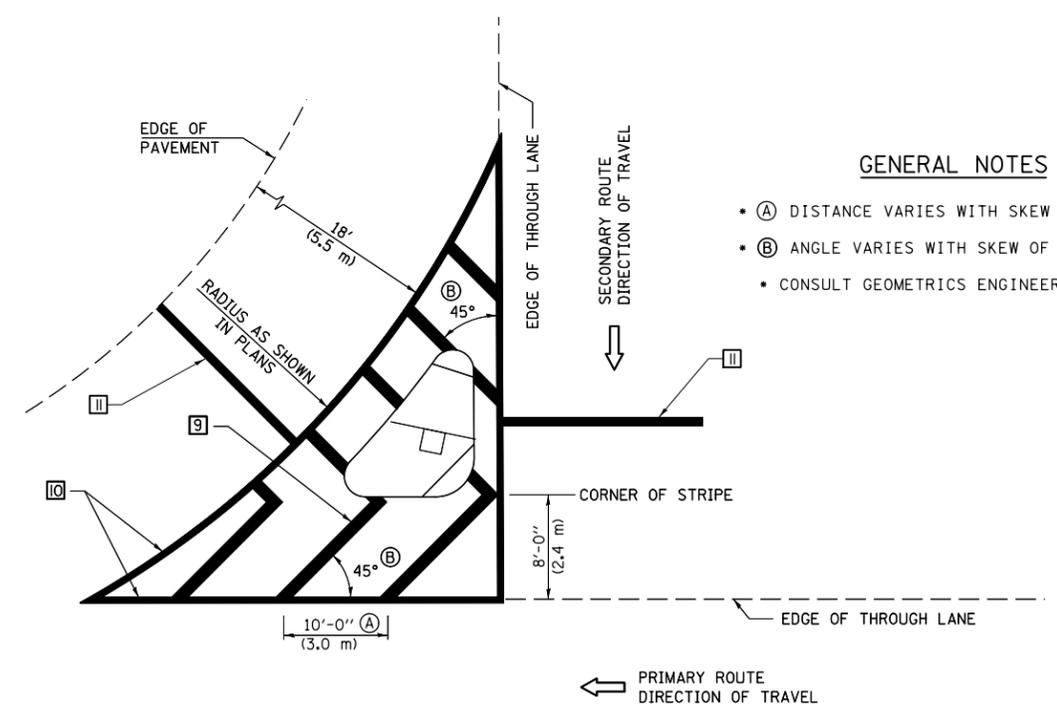
**TYPICAL MEDIAN TRANSITIONS**

**GENERAL NOTES**

1. WHEN MEDIANS ARE PRESENT, PAVEMENT MARKINGS ARE TO BE PLACED ADJACENT TO MEDIANS.
2. SOME OF THE INFORMATION INCLUDED WITH THIS DETAIL MAY NOT BE APPLICABLE TO THIS IMPROVEMENT.
3. PAVEMENT MARKINGS ARE TO BE EXTENDED THROUGH OMISSIONS WHEN APPLICABLE.
4. A STRIPING KEY IS AVAILABLE ELSEWHERE AND SHALL BE SHOWN WHERE THE QUANTITIES ARE LISTED.
5. FINAL PAVEMENT MARKINGS SHALL BE IN PLACE PRIOR TO PLACING ANY RAISED REFLECTIVE PAVEMENT MARKERS.
6. THE FOLLOWING CRITERIA SHALL BE USED FOR SELECTING THE DIAGONAL PAVEMENT MARKING SPACING,  
 < 30 MPH USE 15' (< 50 km/h USE 4.5 m)  
 30-45 MPH USE 20' (50-75 km/h USE 6.0 m)  
 > 45 MPH USE 30' (> 75 km/h USE 9.0 m)



**RIGHT IN - RIGHT OUT ACCESS**



**GENERAL NOTES**

- (A) DISTANCE VARIES WITH SKEW OF INTERSECTION.
- (B) ANGLE VARIES WITH SKEW OF INTERSECTION.
- CONSULT GEOMETRICS ENGINEER

**ISLAND**

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = piersonbr	DESIGNED -	REVISED - 11/06
p:\11\084EBIDINTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 5\Projects\0579\Drawings\Design\0579A60-shd-Detail.dwg		DRAWN -	REVISED - 09/2009 - KJT
	PLOT SCALE = 40.0000' / in.	CHECKED -	REVISED - 04/14 - JLA
\$MODELNAME\$	PLOT DATE = 10/6/2016	DATE -	REVISED -

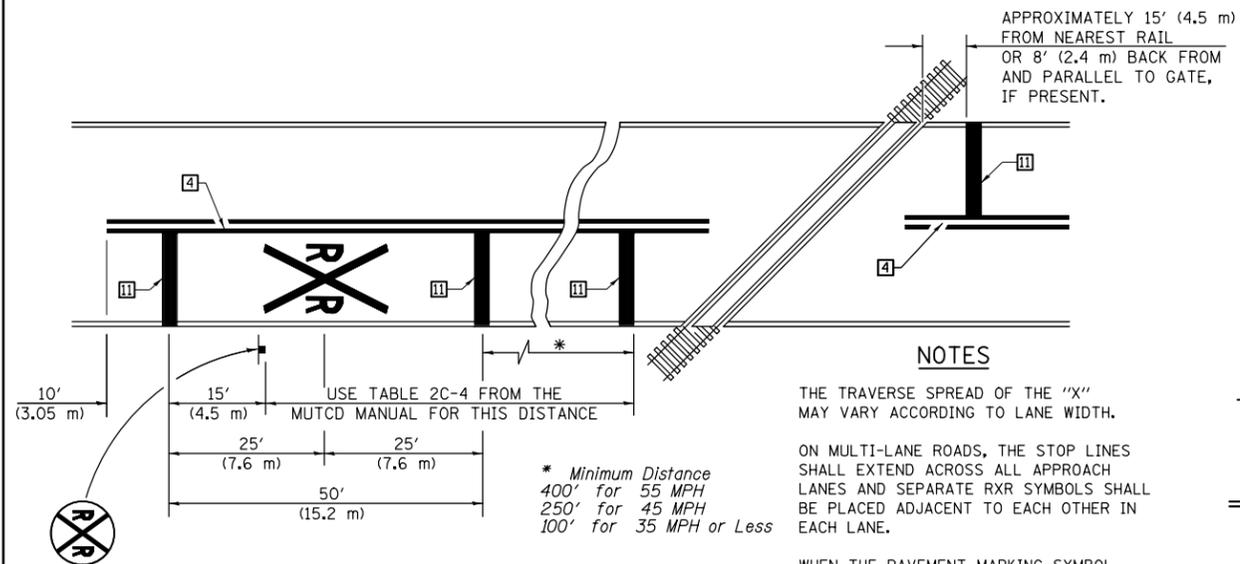
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**PAVEMENT MARKING AND MARKERS  
 (RURAL & URBAN APPLICATIONS)**

SCALE: SHEET 3 OF 4 SHEETS STA. TO STA.

**DISTRICT 5 DETAIL NO. 7800AAA**

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
74	(57-24&57-24-1)RS-1	MCLEAN	48	46
CONTRACT NO. 70A60			ILLINOIS FED. AID PROJECT	



**PAVEMENT MARKINGS AT RAILROAD-HIGHWAY GRADE CROSSING**

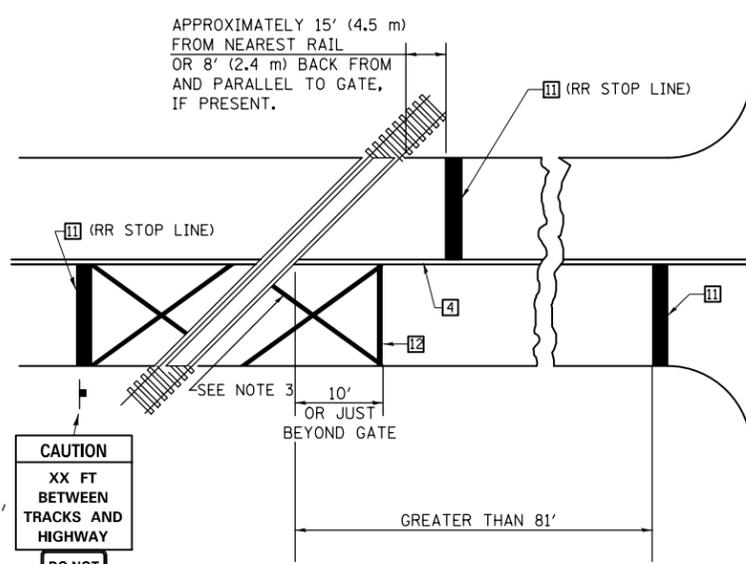
**NOTES**

THE TRAVERSE SPREAD OF THE "X" MAY VARY ACCORDING TO LANE WIDTH.

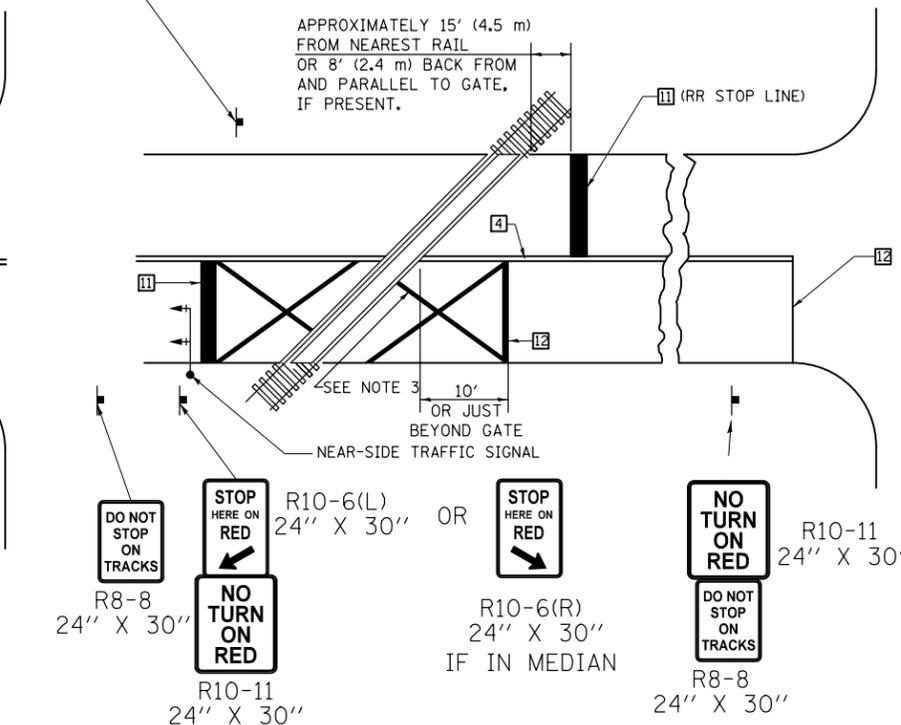
ON MULTI-LANE ROADS, THE STOP LINES SHALL EXTEND ACROSS ALL APPROACH LANES AND SEPARATE RXR SYMBOLS SHALL BE PLACED ADJACENT TO EACH OTHER IN EACH LANE.

WHEN THE PAVEMENT MARKING SYMBOL IS USED, A PORTION OF THE SYMBOL SHOULD BE LOCATED DIRECTLY ADJACENT TO THE ADVANCE WARNING SIGN (W10-1) AS PLACED BY TABLE II-1, CONDITION B OF THE MUTCD.

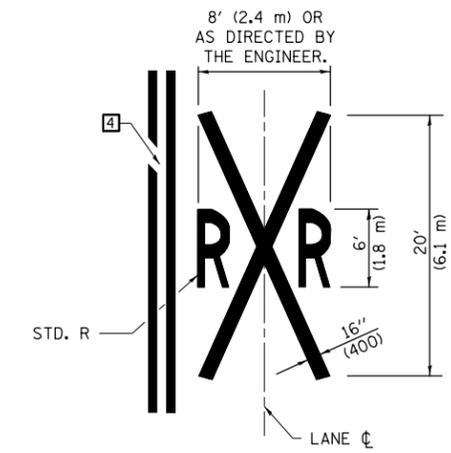
**RAILROAD CROSSING WITH INTERCONNECT ONLY**



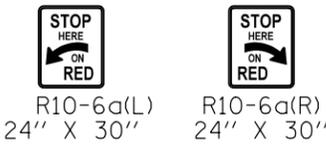
**RAILROAD CROSSING WITH INTERCONNECT AND PRE-SIGNALS**



**SUPPLEMENTAL PAVEMENT MARKING TREATMENT FOR RAILROAD-HIGHWAY GRADE CROSSING**



**ALTERNATE SIGNS**



**GENERAL NOTES**

- SUPPLEMENTAL PAVEMENT MARKINGS TO BE INSTALLED ONLY ON APPROACHES TO INTERSECTIONS CONTROLLED BY TRAFFIC SIGNALS WHICH ARE INTERCONNECTED WITH THE RAILROAD WARNING SIGNALS.
- EXTEND PAVEMENT MARKINGS TO THE INTERSECTION ONLY WHERE NEAR-SIDE TRAFFIC SIGNALS ARE USED.
- 6" WHITE PAVEMENT MARKINGS AT 45° TO PAVEMENT, 8' CENTER TO CENTER.
- XX DISTANCE TO BE SHOWN ON SIGN MEASURED FROM A POINT 6 FEET FROM THE RAIL CLOSEST TO THE INTERSECTION OR FROM THE CLOSEST POINT ALONG THE EXIT GATE IF PRESENT OVER THE ROADWAY WHEN IN THE LOWERED POSITION TO THE STOP BAR OR CROSSWALK, WHICH EVER IS CLOSEST, ROUNDED DOWN TO NEAREST 5 FEET. WHERE THERE IS NO STOP LINE, MEASURE TO POINT WHERE DRIVER HAS A VIEW OF APPROACHING TRAFFIC.
- THE CLEARANCE SIGN IS ALSO TO BE USED AS AN INTERIM MEASURE AT LOCATIONS WITH INTERCONNECTED INTERSECTION TRAFFIC SIGNALS WHERE IT IS PLANNED TO CHANGE THEM TO NEAR-SIDE SIGNALS AT A FUTURE TIME. IN THIS CASE, THE DISTANCE TO BE SHOWN ON THE SIGN IS MEASURED FROM THE EDGE OF THE STRIPED-OUT AREA INSTEAD OF 6 FEET FROM THE RAIL. THE SIGN IS TO BE REMOVED WHEN THE NEAR-SIDE SIGNALS ARE INSTALLED AND THE PAVEMENT MARKINGS EXTENDED TO THE INTERSECTION.

Note: All dimensions are in INCHES (millimeters) unless otherwise shown.

