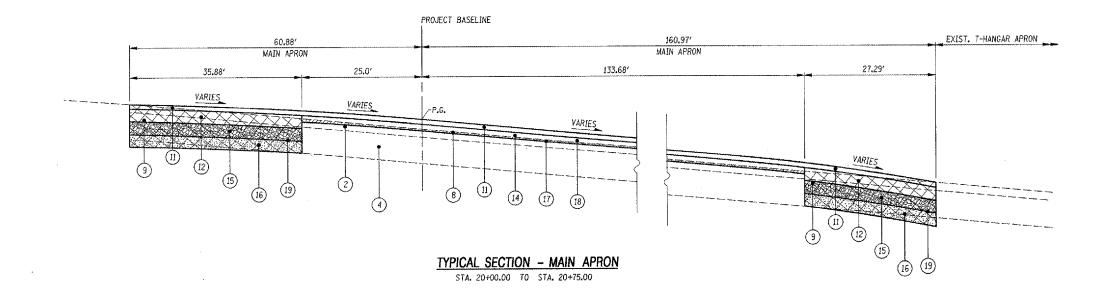


TYPICAL SECTION - TAXIWAY A1 STA. 13+74.00 TO STA. 17+50.73

(1) TRANSITION CROSS-SLOPE FROM -1.50% TO +1.00% FROM STA. 17+28.85 TO STA. 18+25.00

TYPICAL SECTION - TAXIWAY A1 (2) TRANSITION CROSS-SLOPE FROM -1.50% TO -1.00% FROM STA, 19+25.00 TO STA, 19+50.00

STA. 17+50.73 TO STA. 20+00.00



<u>LEGEND</u>

- 1 EXISTING BITUMINOUS CONC. PAVEMENT (±6")
- 2) EXISTING BITUMINOUS CONC. PAVEMENT (±21/2")
- (3) EXISTING AGGREGATE BASE (±10")
- 4 EXISTING AGGREGATE BASE (±81/2")
- EXISTING EARTH SHOULDER
- 6 EXISTING RUNWAY/TAXIWAY LIGHT
- (1) BITUMINOUS PAVEMNT MILLING, VARIABLE DEPTH, (11/2" MIN.)
- 8 BITUMINOUS PAVEMNT MILLING, VARIABLE DEPTH, (1/2" MIN.)
- PAVEMENT REMOVAL AND RECONSTRUCTION
- 10) 51/2" PCC PAVEMENT
- 1 1/2" BITUMINOUS SURFACE COURSE
- (12) 4" BITUMINOUS BASE COURSE
- (13) BITUMINOUS BASE COURSE, LEVELING, VARIABLE DEPTH, (1" MIN.)
- (14) BITUMINOUS BASE COURSE, LEVELING, VARIABLE DEPTH, (2" MIN.)
- 4" CRUSHED AGGREGATE BASE COURSE
- (6) 6" RECYCLED SUBBASE MATERIAL FROM (7), (8) AND (9)
- SAND MIX CRACK REPAIR
- (18) CRACK CONTROL MATERIAL
- (19) SOIL STABILIZATION FABRIC
- (20) SHOULDER ADJUSTMENT (GRADING & SEEDING)

- 1. SEE SHEETS 13 THRU 15 FOR PAVEMENT GRADES AND ELEVATIONS
- 2. SEE SHEETS 26 THRU 30 FOR CROSS SECTIONS IN THESE AREAS
- 3. SEE SHEET 16 FOR PCC AND MISCELLANEOUS AC DETAILS
- 4. PAVEMENT BETWEEN TYPICAL SECTIONS ARE EITHER IN TRANSITION IN GRADES, CROSS-SLOPES OR PAVEMENT WIDTHS
- 5. ALTHOUGH NOT SHOWN, A BITUMINOUS PRIME COAT SHALL BE APPLIED TO ANY EXPOSED AGGREGATE BASE AT A RATE OF 0.30 GALLONS/SQUARE YARD
- 6. ALTHOUGH NOT SHOWN, AT THE DISCRETION OF THE ENGINEER, A BITUMINOUS TACK COAT MAY BE APPLIED BETWEEN SUCCESSIVE COURSES OF BITUMINOUS PAVEMENTS AT A RATE OF 0.08 GALLONS/SQUARE YARD

ENGINEERS CARLYLE, ILLINOIS 62231

TYPICAL SECTIONS

GRE-3215 J.P. PROJ. NO. 3-17-0049-B4 BE NAME GA-TYPSEC-01 01-19-2007