

VILLAGE OF SCHAUMBURG SCHAUMBURG, ILLINOIS

CONSTRUCTION PLANS FOR SCHAUMBURG REGIONAL AIRPORT

REHABILITATE TERMINAL ENTRANCE ROAD AND AUTO PARKING LOT

ILLINOIS PROJECT: 06C-4649

SUMMARY OF QUANTITIES				
ITEM NO.	DESCRIPTION	UNIT	ESTIMATED QUANTITY	RECORD QUANTITY
AR150520	MOBILIZATION	LS	1	
AR152540	SOIL STABILIZATION FABRIC	SY	1,884	
AR208515	POROUS GRANULAR EMBANKMENT	CY	628	
AR208910	REMOVE & REPLACE AGGREGATE BASE	SY	1,884	
AR401610	BITUMINOUS SURFACE COURSE	TON	1,116	
AR401650	BITUMINOUS PAVEMENT MILLING	SY	12,811	
AR401916	REM & REP BIT PAVEMENT - TYPE B	SY	1,884	
AR501605	5" PCC SIDEWALK	SF	763	
AR501690	PCC SIDEWALK REMOVAL	SF	763	
AR602510	BITUMINOUS PRIME COAT	GAL	565	
AR603510	BITUMINOUS TACK COAT	GAL	1,884	
AR620520	PAVEMENT MARKING - WATERBORNE	SF	2,354	
AR620900	PAVEMENT MARKING REMOVAL	SF	100	
AR754984	RECONSTRUCT COMB CURB & GUTTER	LF	950	
AR800009	PCC SPALL REPAIR	SF	5	
AR910200	ROADWAY SIGN	EA	2	
AR910915	REMOVE ROADWAY SIGN	EA	8	

INDEX TO SHEETS

- COVER SHEET
- SITE PLAN AND PROJECT CONTROL PLAN
- CONSTRUCTION SAFETY AND PHASING PLAN
- CONSTRUCTION SAFETY AND PHASING PLAN NOTES
- TYPICAL SECTIONS
- EXISTING CONDITIONS AND PROPOSED REMOVALS
- PROPOSED IMPROVEMENTS
- MISCELLANEOUS DETAILS

NOVEMBER 16, 2018

CMT 18256-02
CRAWFORD, MURPHY & TILLY, INC.
CONSULTING ENGINEERS
License No. 184-000613

DOUGLAS J. KLONOWSKI
062-050464
STATE OF ILLINOIS
LICENSED PROFESSIONAL ENGINEER
expires: 11-30-19

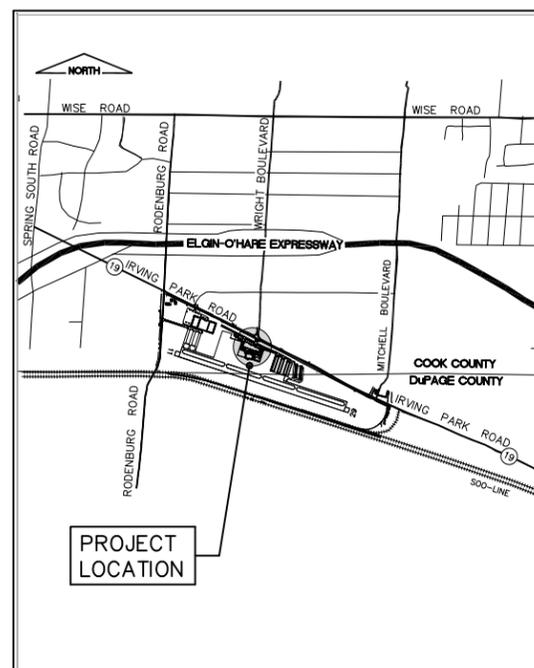
SUBMITTED BY *Douglas J. Klonowski PE*
DOUGLAS J. KLONOWSKI, PE

DATE NOVEMBER 16, 2018

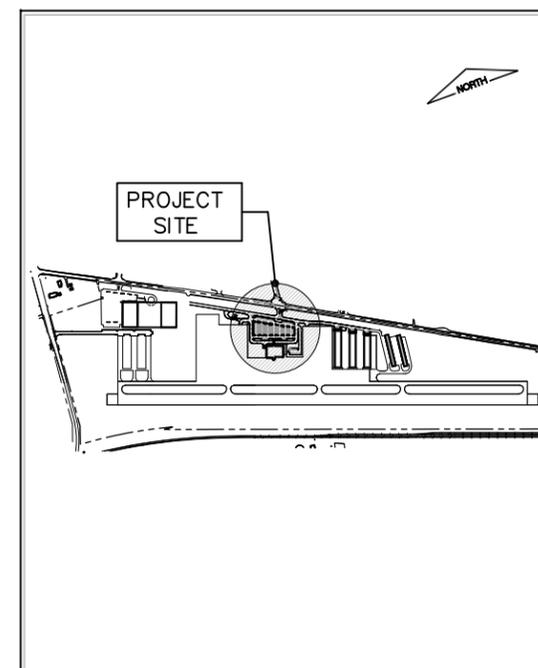
VILLAGE OF SCHAUMBURG
SCHAUMBURG REGIONAL AIRPORT

APPROVED BY *Maryn Robles*
MARYN ROBLES, DIRECTOR OF TRANSPORTATION

DATE NOVEMBER 16, 2018



LOCATION MAP



SITE PLAN

811 Know what's below.
Call before you dig.

J.U.L.I.E.
JOINT UTILITY LOCATING
INFORMATION FOR EXCAVATORS
www.illinois1call.com

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

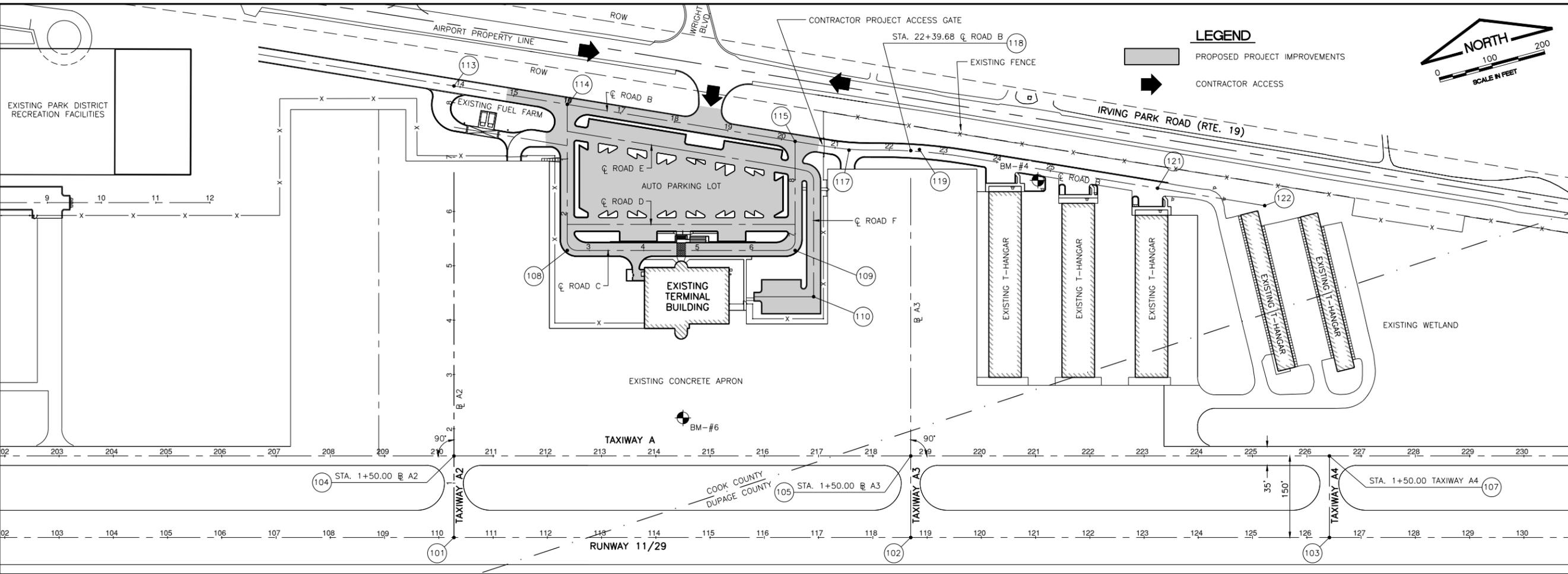
DESIGN AIRCRAFT APPROACH CATEGORY: B
DESIGN AIRCRAFT GROUP: I

SCHAUMBURG REGIONAL AIRPORT

TOWNSHIP: 40 NORTH TOWNSHIP: 41 NORTH
RANGE: 10 EAST RANGE: 10 EAST
DUPAGE COUNTY COOK COUNTY
BLOOMINGDALE TOWNSHIP (SECTION: 4) SCHAUMBURG TOWNSHIP (SECTION: 33)

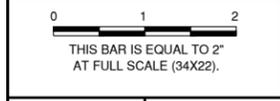
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 UPDATE BY: Neil Finke
 LAYOUT: Layout1
 REF: DWG: 18256.dwg
 base: parking lot.dwg
 IMAGE FILES: Schaumburg_Village_solid.rvt
 2950_001.rvt



IL CONTRACT: **SH029**
 IL LETTING ITEM: **21A**
 IL PROJECT: **06C-4649**
 S.B.G. PROJECT: **N/A**

REVISIONS		
NUMBER	BY	DATE



**SCHAUMBURG REGIONAL AIRPORT
 SCHAUMBURG, ILLINOIS
 REHABILITATE TERMINAL ENTRANCE ROAD
 AND AUTO PARKING LOT**

SITE PLAN AND PROJECT CONTROL PLAN

HORIZONTAL CONTROL (NAD 83)					
POINT NUMBER	DESCRIPTION	NORTHING	EASTING	STATION	OFFSET
101	RUNWAY 11/29 AND TAXIWAY A2	1,939,016.834	1,046,503.336	110+27.88 @ RUNWAY 11/29	0.00'
102	RUNWAY 11/29 AND TAXIWAY A3	1,938,799.016	1,047,316.674	118+69.88 @ RUNWAY 11/29	0.00'
103	RUNWAY 11/29 AND TAXIWAY A4	1,938,599.394	1,048,062.067	126+41.54 @ RUNWAY 11/29	0.00'
104	TAXIWAY A AND TAXIWAY A2	1,939,161.728	1,046,542.139	210+27.88 @ TAXIWAY A	0.00'
105	TAXIWAY A AND TAXIWAY A3	1,938,943.909	1,047,355.478	218+69.88 @ TAXIWAY A	0.00'
107	TAXIWAY A AND TAXIWAY A4	1,938,744.288	1,048,100.871	226+41.54 @ TAXIWAY A	0.00'
108	P.I. CURVE #3, ROAD C	1,939,472.984	1,046,841.744	2+68.84 @ ROAD C	7.46' RT.
109	P.I. CURVE #4, ROAD C	1,939,364.334	1,047,247.448	6+81.12 @ ROAD C	7.46' RT.
110	ROAD F AND CENTERLINE PARKING LOT	1,939,273.242	1,047,258.368	5+88.22 @ ROAD F	0.00'
113	PERIMETER ROAD B AND EXTENDED BASELINE A2	1,939,819.458	1,046,718.284	13+88.38 @ ROAD B	0.00'
114	PERIMETER ROAD B AND ROAD C (WEST)	1,939,732.670	1,046,911.290	16+00.00 @ ROAD B	0.00'
115	PERIMETER ROAD B AND ROAD C (EAST)	1,939,558.169	1,047,299.358	20+25.50 @ ROAD B	0.00'
117	P.I. CURVE #1, PERIMETER ROAD B	1,939,519.987	1,047,384.272	21+18.60 @ ROAD B	0.00'
118	@ PERIMETER ROAD AND BASELINE A3	1,939,486.617	1,047,500.819	7+11.83 @ A3	0.00'
119	P.I. CURVE #2, PERIMETER ROAD B	1,939,484.464	1,047,516.914	22+55.74 @ ROAD B	0.00'
121	PERIMETER ROAD B	1,939,302.127	1,047,922.410	27+00.00 @ ROAD B	0.00'
122	EXTENDED CENTERLINE ROAD B	1,939,220.105	1,048,104.818	29+00.00 @ ROAD B	0.00'

VERTICAL CONTROL		
BENCHMARK	DESCRIPTION	ELEVATION
BM - #1	GPS SURVEY MONUMENT MARKED "1034". 2" ALUMINUM CAP GROUDED IN CONCRETE PAD ON SOUTHWEST CORNER OF RODENBURG ROAD AND SOO LINE RAILROAD INTERSECTION. BENCHMARK "1034" IS A MONUMENT WHICH IS PART OF THE VILLAGE OF SCHAUMBURG "GEONEX" BENCHMARK SYSTEM. (NOT SHOWN ON THIS SHEET)	796.19
BM - #2	NGS FAA 06C POINT "A" IRON ROD STA. 211+36, 298' RT @ TXY. A (NOT SHOWN ON THIS SHEET)	793.81
BM - #4	CHISELED "□" NORTH SIDE OF CONCRETE BASE FOR BEACON LIGHT STA. 24+80.09, 20.84' RT @ ROAD B	804.41
BM - #6	NORTH RIM INLET AT EXISTING TERMINAL APRON STA. 214+50, 70' LT @ TXY. A	800.17

CURVE DATA			
PERIMETER ROAD B		PERIMETER ROAD C	
CURVE 1 DATA	CURVE 2 DATA	CURVE 3 DATA	CURVE 4 DATA
P.I. STA. = 21+18.60	P.I. STA. = 22+55.74	P.I. STA. = 2+68.84	P.I. STA. = 6+81.12
Δ = 09°13'06.4"	Δ = 09°13'10.5"	Δ = 90°00'00"	Δ = 90°00'00"
D = 11°27'33"	D = 05°43'46.5"	D = 318°18'35.6"	D = 318°18'35.6"
R = 500.00'	R = 1000.00'	R = 18.00'	R = 18.00'
L = 80.45'	L = 160.91'	L = 28.27'	L = 28.27'
T = 40.31'	T = 80.63'	T = 18.00'	T = 18.00'
P.C. STA. = 20+78.29	P.C. STA. = 21+75.11	P.C. STA. = 2+50.84	P.C. STA. = 6+63.12
P.T. STA. = 21+58.74	P.T. STA. = 23+36.02	P.T. STA. = 2+79.11	P.T. STA. = 6+91.39

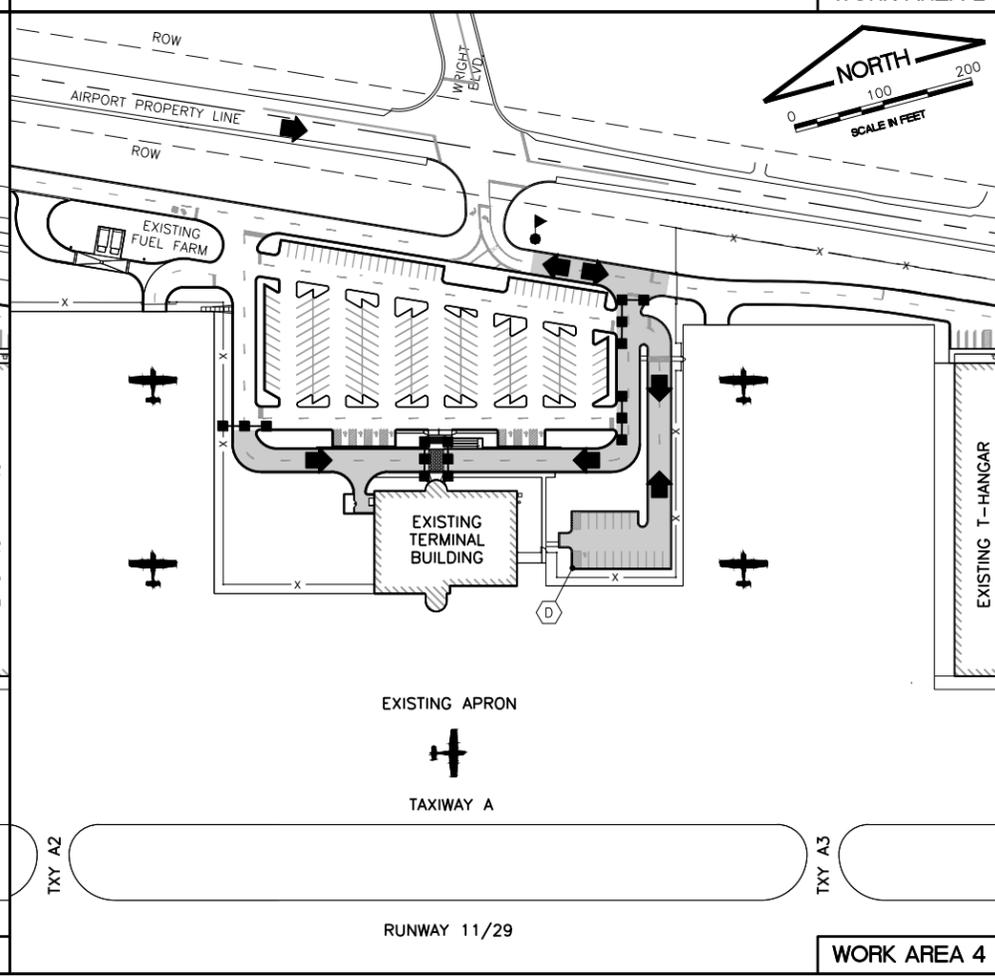
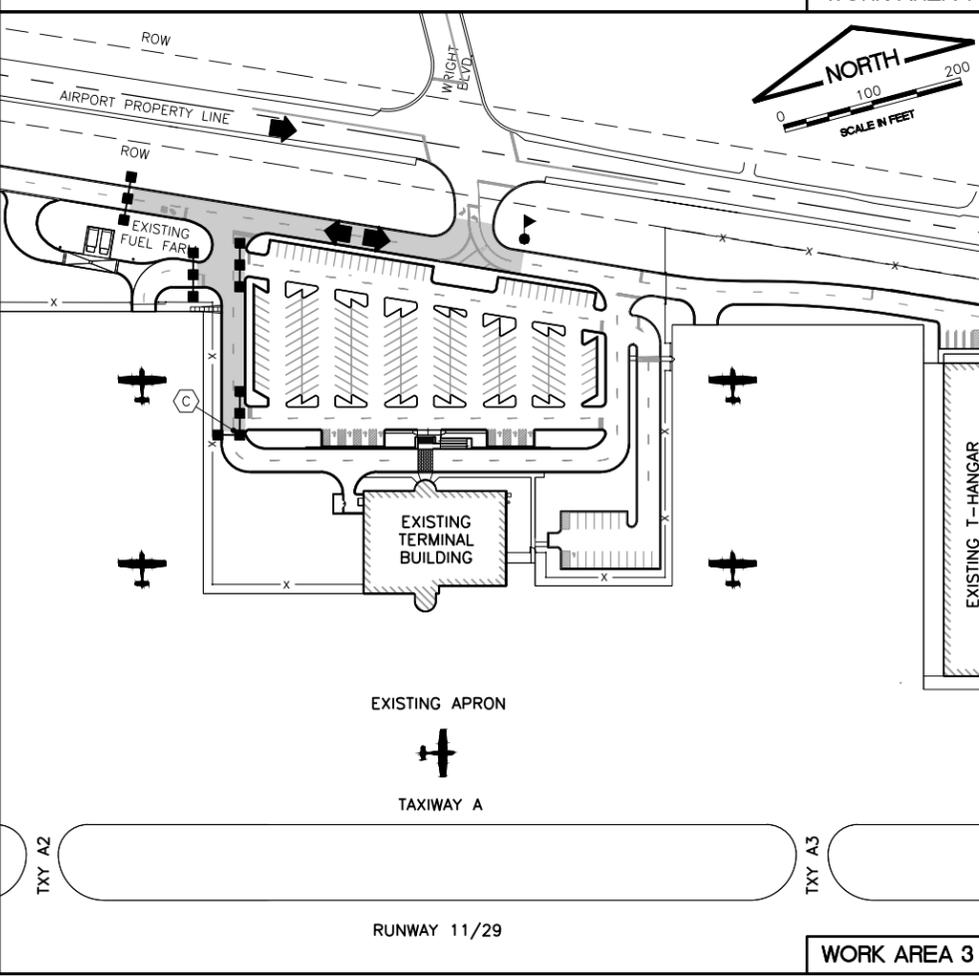
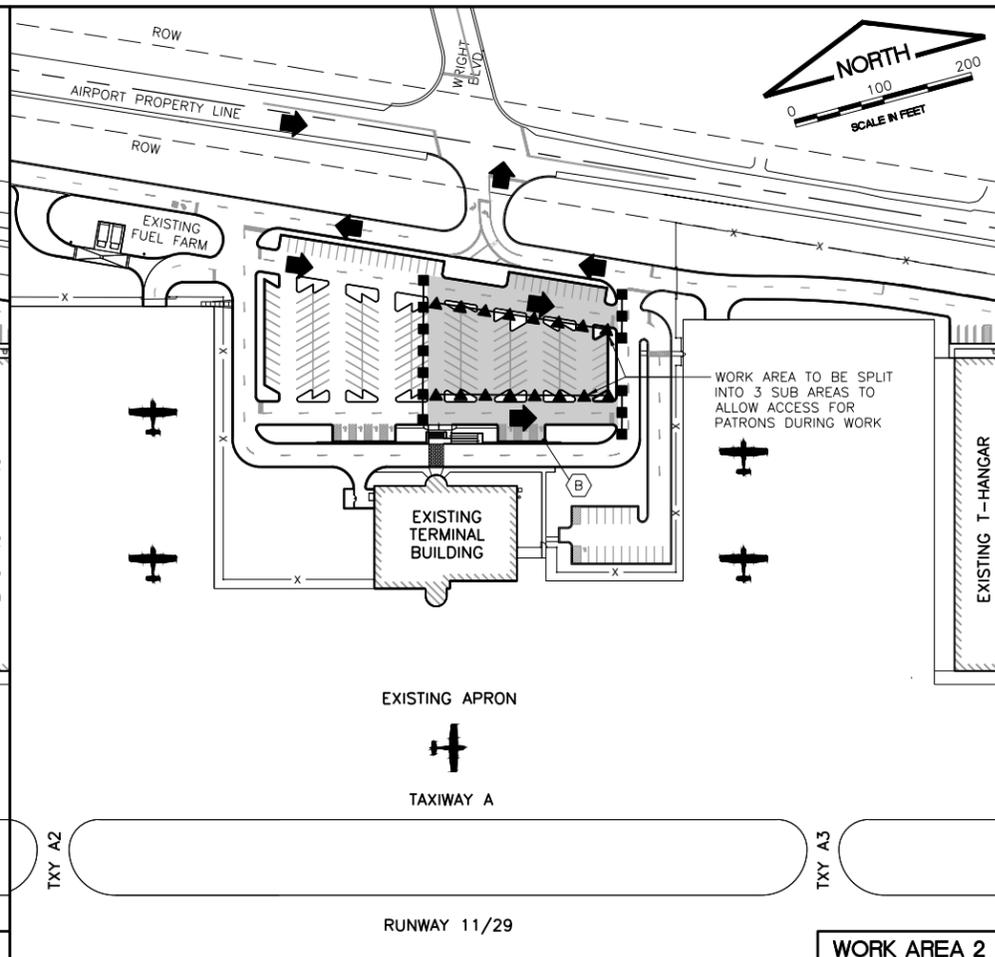
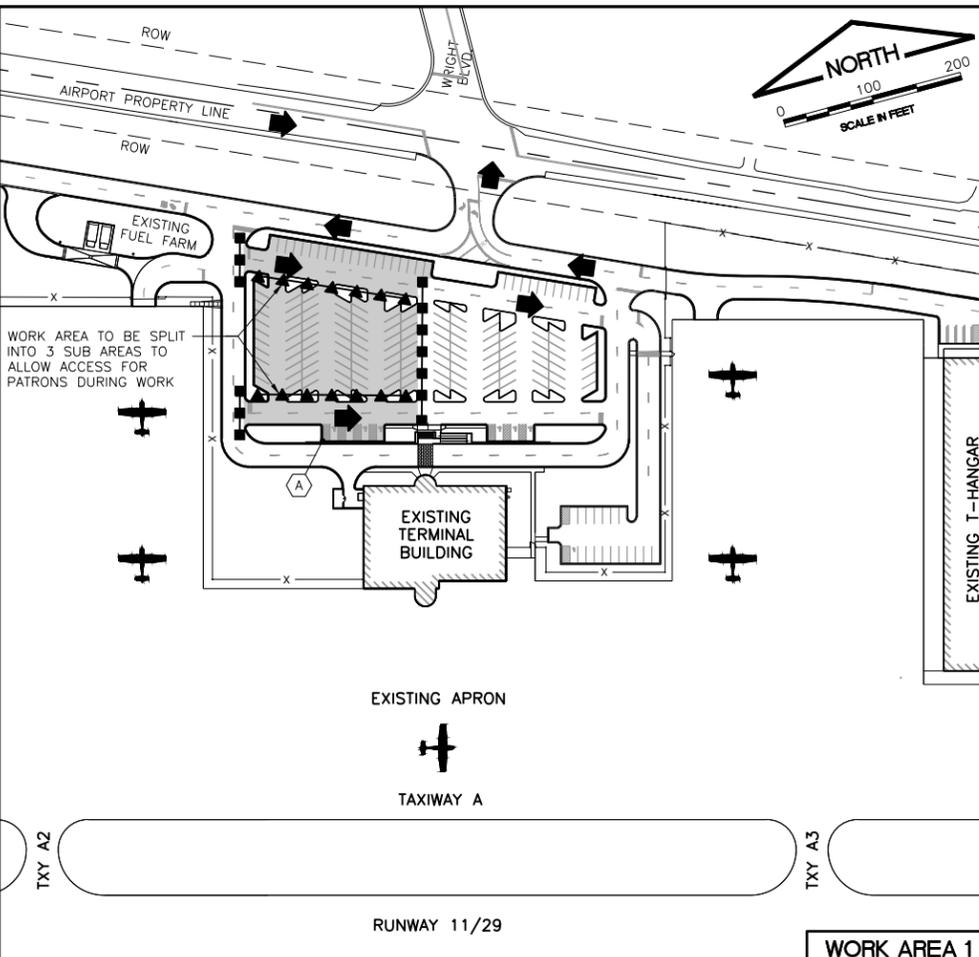
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DESIGN BY: MND
 DRAWN BY: JRO
 CHECKED BY: MND
 APPROVED BY: MND
 DATE: 11/16/2018
 JOB No: 18256-02

SHEET 2 OF 8 SHEETS

DATE: Friday, November 16, 2018, 2:58:24 PM
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 UPDATE BY: Neil Finke
 LAYOUT: Layout1
 REF DWG: 18256-02_Base_Parking_Layout.dwg
 IMAGE FILES: Schaumburg_Village_seal.tif



- LEGEND**
- WORK AREA LIMITS
 - BARRICADE PLACEMENT
 - SUB WORK AREA BARRICADE PLACEMENT
 - CONTRACTOR'S HAUL ROUTE
 - AIRCRAFT MOVEMENT AREA
 - CRITICAL POINT ON CONSTRUCTION SITE TO RUNWAY 11/29
 - FLAGMAN

MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT: 25' FEET

DESIGN AIRCRAFT APPROACH CATEGORY: B
DESIGN AIRCRAFT GROUP: I

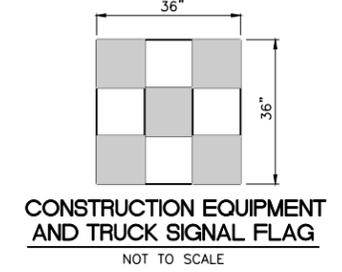
CRITICAL POINTS	
POINT "A"	POINT "B"
LATITUDE: 41°59'28.29"	LATITUDE: 41°59'27.70"
LONGITUDE: 88°06'09.82"	LONGITUDE: 88°06'06.87"
GROUND ELEVATION: 808.5'	GROUND ELEVATION: 806.9'
ABOVE GROUND ELEVATION: 833.5'	ABOVE GROUND ELEVATION: 931.9'
POINT "C"	POINT "D"
LATITUDE: 41°59'28.70"	LATITUDE: 41°59'26.35"
LONGITUDE: 88°06'11.00"	LONGITUDE: 88°06'06.93"
GROUND ELEVATION: 808.3'	GROUND ELEVATION: 800.8'
ABOVE GROUND ELEVATION: 833.3'	ABOVE GROUND ELEVATION: 825.8'

SUGGESTED SEQUENCE OF CONSTRUCTION (EACH WORK AREA)

- NOTIFY RESIDENT ENGINEER AND VILLAGE OF SCHAUMBURG 5 BUSINESS DAYS PRIOR TO INITIAL START OF CONSTRUCTION.
- PLACE TRAFFIC CONTROL.
- LOCATE ALL UTILITIES.
- COMPLETE REMOVAL ITEMS AND MILLING.
- COMPLETE PAVEMENT IMPROVEMENTS, MARKING AND SIGNAGE.
- NOTIFY RESIDENT ENGINEER AND VILLAGE OF SCHAUMBURG 3 BUSINESS DAYS PRIOR TO CHANGING TO NEXT PHASE/WORK AREA.
- CLEAN PAVEMENT.
- REMOVE BARRICADES AND OPEN PAVEMENT TO VEHICLES.

NOTES

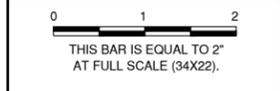
1. CONTRACTOR STAGING/EMPLOYEE PARKING AREA WILL BE LOCATED WITHIN THE LIMITS OF EACH WORK AREA. CONTRACTOR SHALL COORDINATE TEMPORARY RELOCATION OF EMPLOYEE PARKING DURING PAVING OPERATIONS.
2. CONTRACTOR MUST ALLOW ACCESS FOR ALL VEHICLES, INCLUDING EMERGENCY AND FUEL TRUCKS, TO AND FROM ALL APRON AREAS, THE FUEL FARM AND THE PERIMETER ROAD EAST/WEST OF THE PARKING LOT AT ALL TIMES.



IL. CONTRACT: SH029
IL. LETTING ITEM: 21A
IL. PROJECT: 06C-4649
S.B.G. PROJECT: N/A

REVISIONS

NUMBER	BY	DATE



SCHAUMBURG REGIONAL AIRPORT
 SCHAUMBURG, ILLINOIS
 REHABILITATE TERMINAL ENTRANCE ROAD
 AND AUTO PARKING LOT
 CONSTRUCTION SAFETY AND PHASING PLAN

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DRAWN BY:	JRO
CHECKED BY:	MND
APPROVED BY:	MND
DATE:	11/16/2018
JOB No:	18256-02

DATE: Friday, November 16, 2018 2:58:28 PM
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UPDATE BY: Neil Finkle
LAYOUT: Layout1
IMAGE FILES: Schaumburg_Village.sait
VREF DWG: Rebuilding

GENERAL

1. THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2, AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.
2. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2. NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.
3. THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING SAFETY REQUIREMENTS.
4. A MINIMUM OF 10 DAYS PRIOR TO THE PRECONSTRUCTION MEETING THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.
5. A MINIMUM OF 10 DAYS PRIOR TO THE NOTICE TO PROCEED THE CONTRACTOR SHALL SUBMIT THE SPCD FOR APPROVAL.
6. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL SIGN THE SWPPP CERTIFICATION STATEMENT.
7. THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE NEW IMPROVEMENTS WHILE MAINTAINING AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED WITH THE APPROVAL OF THE RESIDENT ENGINEER. HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT.
8. ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS ACCEPTABLE TO THE RESIDENT ENGINEER AND AIRPORT FOR ALL PHASES. ALL TEMPORARY CABLING AND SPLICING NECESSARY TO KEEP THE CIRCUITS IN OPERATION SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
9. ALL EXISTING AND PROPOSED FENCE LINES, EXCEPT AS OTHERWISE NOTED, SHALL BE MAINTAINED AND SHALL SERVE AS CONSTRUCTION AROUND THE PERIMETER OF THE PROJECT. ALL EXISTING GATES SHALL BE MAINTAINED, CLOSED AND LOCKED AS DIRECTED BY THE AIRPORT OWNER'S REPRESENTATIVE. SHOULD THE CONTRACTOR CHOOSE TO KEEP A GATE OPEN FOR CONSTRUCTION OPERATIONS, A COMPETENT SECURITY GUARD SHALL MONITOR THE OPEN GATE. ANY COST SHALL NOT BE PAID FOR SEPARATELY, BUT WILL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
10. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE DUST CONTROL AT ALL TIMES DURING THE PROJECT DURATION. A WATER TRUCK SHALL BE REQUIRED TO BE ONSITE DURING ALL CONSTRUCTION OPERATION WORKING HOURS, UNLESS WAIVED BY THE AIRPORT. PAYMENT FOR DUST CONTROL SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
11. PAYMENT FOR ALL AIRSIDE AND ROADWAY TRAFFIC CONTROL INCLUDING BUT NOT LIMITED TO, TEMPORARY CONSTRUCTION FENCING, BARRICADES, SIGNING, FLAGGER, AIR OPERATIONS AREA (A.O.A) LATH AND RIBBON, ETC. SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
12. ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS A SPECIFIC PAY ITEM IS PROVIDED.

1. COORDINATION

1. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRECONSTRUCTION CONFERENCE WITH THE AIRPORT, RESIDENT ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRECONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.
2. ON OR BEFORE THE PRECONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL SUBMIT A PROPOSED SCHEDULE FOR THE PROJECT. THE SCHEDULE SHALL INCLUDE A START AND COMPLETION DATE FOR EACH ITEM OF WORK. THE SCHEDULE SHALL BE UPDATED ON A WEEKLY BASIS. ALL COSTS ASSOCIATED WITH THE SCHEDULE SHALL BE INCIDENTAL TO THE CONTRACT.
3. THE CONTRACTOR SHALL BE REQUIRED TO ESTABLISH A COORDINATION PLAN WITH THE AIRPORT OR HIS/HER DESIGNATED REPRESENTATIVE, REGARDING DE-ENERGIZING AND ENERGIZING OF THE AIRFIELD CIRCUITS IMPACTED BY CONSTRUCTION ACTIVITY.

2. PHASING

1. TOTAL BASE BID CONTRACT TIME SHALL BE 40 CALENDAR DAYS.
2. PHASING SHALL BE AS SHOWN ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET.

3. AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY

1. ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET.
2. WHEN CONFLICTS ARISE BETWEEN CONSTRUCTION ACTIVITIES AND AIRCRAFT OPERATIONS AND SAFETY, AIRCRAFT OPERATIONS AND SAFETY SHALL TAKE PRECEDENCE AND SHALL GOVERN. FINAL AUTHORITY IN THE APPROVAL OF CONSTRUCTION SEQUENCING LIES WITH THE AIRPORT.
3. AIRCRAFT OPERATIONS HAVE THE RIGHT-OF-WAY ON THE AIRFIELD. ALL CONSTRUCTION TRAFFIC SHALL IMMEDIATELY YIELD TO ONCOMING AIRCRAFT AT ALL TIMES.
4. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARY RELOCATE EQUIPMENT AT ANY TIME TO ALLOW AN AIRCRAFT TO PASS, THE CONTRACTOR SHALL DO SO IMMEDIATELY AT NO EXTRA COST TO THE OWNER.

4. PROTECTION OF NAVIGATION AIDS (NAVAIDS)

1. THE CONTRACTOR SHALL REMAIN CLEAR OF THE ILS CRITICAL AREAS AND OTHER NAVAIDS FACILITIES AT ALL TIMES.

5. CONTRACTOR ACCESS

1. CONTRACTOR ACCESS SHALL BE AS NOTED BELOW AND AS SHOWN ON THE SITE PLAN AND CONSTRUCTION SAFETY AND PHASING PLAN SHEETS.
2. THE CONTRACTOR IS TO ACCESS THE SITE USING THE EXISTING GATE SHOWN. THE ENTRANCE SHALL BE SIGNED ACCORDINGLY AS TO ALLOW ONLY CONSTRUCTION VEHICLES ACCESS IF APPLICABLE AND WILL ONLY BE ACCESSIBLE DURING THE CONTRACTOR'S SCHEDULED WORK DAY. ALL SIGNAGE SHALL CONFORM TO IDOT CONSTRUCTION STANDARDS FOR VEHICLES ENTERING AND LEAVING THE SITE.
3. SUPERVISORY PERSONNEL SHALL DEMONSTRATE IN THE PRESENCE OF THE AIRPORT MANAGER THAT THEY ARE FAMILIAR WITH AIRPORT RADIO AND AIRPORT DRIVING PROCEDURES IN ORDER TO PERFORM WORK. OTHER CONSTRUCTION PERSONNEL CAN BE WITHIN THE AIRFIELD LIMITS PROVIDED THAT THEY ARE UNDER ESCORT AND IN THE PRESENCE OF AN AUTHORIZED SUPERVISOR. KNOWLEDGE OF THE AIRPORTS PROCEDURES BY THE SUPERVISORY PERSONNEL MUST BE DEMONSTRATED PRIOR TO THE START OF CONSTRUCTION.
4. DRIVERS OF TRUCKS CONTAINING MATERIAL DELIVERIES (AGGREGATE, CONCRETE, ETC.) NEED NOT OBTAIN AN AIRPORT ID BADGE BUT SHALL BE REQUIRED TO SUBMIT THEIR NAME, DRIVER'S LICENSE NUMBER, TRUCK LICENSE PLATE NUMBER AND NAME OF TRUCKING COMPANY TO THE PRIME CONTRACTOR PRIOR TO ENTERING THE JOBSITE.
5. THE CONTRACTOR'S STORAGE AND STAGING AREA WILL BE AS SHOWN IN THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET.
6. THE CONTRACTOR SHALL KEEP A RECORD OF THE NAMES OF ALL EMPLOYEES ENTERING THE JOB SITE ON A DAILY BASIS. A RECORD OF EACH SUBCONTRACTOR ENTERING THE JOB SITE SHALL ALSO BE KEPT BY THE CONTRACTOR.
7. THE CONTRACTOR'S MATERIAL AND EQUIPMENT, WHEN NOT IN USE, SHALL BE STORED IN THE CONTRACTOR'S STAGING AREA. ALL DELIVERIES, EQUIPMENT REFUELING, EQUIPMENT MAINTENANCE AND EQUIPMENT TRANSFER SHALL TAKE PLACE WITHIN THE CONTRACTOR'S STAGING AREA.
8. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE.
9. ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT IN RADIO CONTACT WITH THE ATCT. THE CONTRACTOR SHALL PROVIDE HIS OWN FLAGMEN.
10. ALL PAVEMENTS, DRIVES OR ANY OTHER AREAS UTILIZED BY THE CONTRACTOR FOR HAUL ROADS, STORAGE AREAS AND/OR STAGING AREAS SHALL BE MAINTAINED AND REPAIRED TO THE SAME CONDITION OR BETTER THAN THEY WERE PRIOR TO BEGINNING CONSTRUCTION. NO ADDITIONAL COMPENSATION WILL BE MADE TO THE CONTRACTOR FOR THIS WORK.
11. ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.

6. WILDLIFE MANAGEMENT

1. THE CONTRACTOR SHALL NOTIFY AIRPORT OPERATIONS OR THE RESIDENT ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.
2. CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED WHEN THE CONTRACTOR IS NOT WORKING.
3. THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.

7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

1. THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS.
2. THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS.

8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

1. THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

1. THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER.
2. THE CONTRACTOR SHALL GIVE A MINIMUM OF 72 HOURS NOTICE TO AIRPORT OPERATIONS PRIOR TO CLOSING ANY PAVEMENTS SO THAT PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT.
3. FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.
4. IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.
5. CONTACTS FOR THIS PROJECT WILL BE DETERMINED AT THE PRECONSTRUCTION MEETING PRIOR TO THE PROJECT START.

10. INSPECTION REQUIREMENTS

1. THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2 MAY BE USED TO AID IN THE INSPECTIONS.
2. THE CONTRACTOR SHALL ATTEND AN INSPECTION OF EACH PHASE WORK AREA PRIOR TO OPENING THE AREA TO AIRPORT OPERATIONS.

11. UNDERGROUND UTILITIES

1. IT WILL BE NECESSARY FOR THE CONTRACTOR TO MAKE HIS OWN FIELD INVESTIGATION TO DETERMINE THE EXACT LOCATION OF THE UNDERGROUND UTILITIES AT CRITICAL POINTS. SEE SECTION 70-17 OF THE STANDARD SPECIFICATIONS AND SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY IN RESPECT TO THE ACCURACY, COMPLETENESS OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY/OWNER OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER AND THE ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT HIS EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.
2. BEFORE INITIATING ANY DIGGING, DRILLING OR EXCAVATING ON THE AIRPORT PROPERTY, THE CONTRACTOR SHALL CALL J.U.L.I.E. AND CONTACT THE LOCAL FAA OFFICE TO ARRANGE FOR UTILITY LOCATES. SEE SECTION 70-17 OF THE SPECIAL PROVISIONS FOR UTILITY CONTACT INFORMATION.
3. SHOULD A UTILITY COMPANY OR GOVERNMENT AGENCY BE UNABLE TO LOCATE FACILITIES, THE CONTRACTOR SHALL LOCATE THESE FACILITIES. PAYMENT FOR THIS LOCATION SHALL BE INCIDENTAL TO THE IMPROVEMENTS REQUIRING THE LOCATE.
4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING ALL AIRPORT OWNED UTILITIES AND SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

12. PENALTIES

1. NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.

13. SPECIAL CONDITIONS

1. ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR.

14. RUNWAY AND TAXIWAY VISUAL AIDS

1. RUNWAY OR TAXIWAY CLOSURES (IF REQUIRED) ARE AS DETAILED IN THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET FOR THIS PROJECT. IF ANY RUNWAY OR TAXIWAY CLOSURES ARE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOW THE REQUIREMENTS OF FAA AC 150/5370-2.

15. MARKING AND SIGNS FOR ACCESS ROUTES

1. MARKING AND SIGNAGE FOR THE ACCESS ROUTE SHALL BE AS SHOWN ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET OR AS DIRECTED BY THE RESIDENT ENGINEER.

16. HAZARD MARKING AND LIGHTING

1. THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
2. ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2 AND 150/5210-5 AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
3. BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET OR AS DIRECTED BY THE RESIDENT ENGINEER. THE CONTRACTOR SHALL PLACE ALL BARRICADES AND CONSTRUCTION SETBACK LINES ITEMS AS SHOWN PRIOR TO INITIATING WORK IN EACH PHASE. ALL COSTS TO FURNISH, INSTALL, REPOSITION, AND MAINTAIN THESE ITEMS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
4. THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.
5. ACCESS TO ACTIVE RUNWAY AND TAXIWAY PAVEMENTS SHALL BE SIGNED WITH STOP SIGNS MOUNTED ON TYPE II BARRICADES (2 EACH, RIGHT AND LEFT). IN ADDITION TO THE STOP SIGNS, WARNING SIGNS (2 EACH, RIGHT AND LEFT) SHALL BE MOUNTED. WARNING SIGNS SHALL STATE "UNAUTHORIZED ACCESS NOT ALLOWED".

17. PROTECTION

1. CONTRACTOR PERSONNEL, VEHICLES, EQUIPMENT AND BARRICADES SHALL NOT BE ALLOWED WITHIN THE TAXIWAY OBJECT FREE AREA (TOFA) OF ACTIVE TAXIWAYS AND THE RUNWAY SAFETY AREA (RSA) OF ACTIVE RUNWAYS.

18. OTHER LIMITATIONS ON CONSTRUCTION

1. IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
2. THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING RUNWAYS AND TAXIWAYS OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE RESIDENT ENGINEER. SHOULD THE CONTRACTOR TRACK ANY DEBRIS ONTO EXISTING PAVEMENTS, THIS DEBRIS SHALL BE REMOVED IMMEDIATELY WITH A PICK UP SWEEPER. A PICK UP SWEEPER SHALL BE REQUIRED TO BE ON SITE AND OPERATE DURING ALL CONSTRUCTION OPERATION WORKING HOURS.
3. THE CONTRACTOR SHALL PROVIDE WASTE RECEPTACLES THROUGHOUT THE WORK ZONE AND MAINTAIN SANITARY FACILITIES FOR EMPLOYEES TO USE. FACILITIES WITHIN THE HANGARS/AIRPORT BUILDINGS SHALL NOT BE USED.
4. WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL AREA LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTIONS. LIGHT SHALL CONSIST OF MOVEABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL BE APPROVED BY THE ENGINEER AND SHALL NOT BE USED IF THEY AFFECT FLIGHT SAFETY.
5. THE CONTRACTOR SHALL SUPPLY AND HAVE IN THEIR POSSESSION AT ALL TIMES AT LEAST ONE AIRPORT RADIO. IN THE EVENT THAT THE AIRPORT MANAGER NEEDS TO CONTACT THE CONTRACTOR DIRECTLY, THE OPERATOR OF SAID RADIO SHALL BE FAMILIAR WITH AIRPORT RADIO PROCEDURES AND TUNED INTO THE GROUND CONTROL FREQUENCY.
6. BROKEN CONCRETE, BROKEN ASPHALT, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.

IL CONTRACT: **SH029**

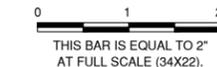
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IL PROJECT: **06C-4649**

S.B.G. PROJECT: **N/A**

REVISIONS

NUMBER	BY	DATE

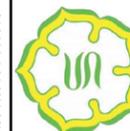


**SCHAUMBURG REGIONAL AIRPORT
SCHAUMBURG, ILLINOIS
REHABILITATE TERMINAL ENTRANCE ROAD
AND AUTO PARKING LOT**

**CONSTRUCTION SAFETY AND
PHASING PLAN NOTES**

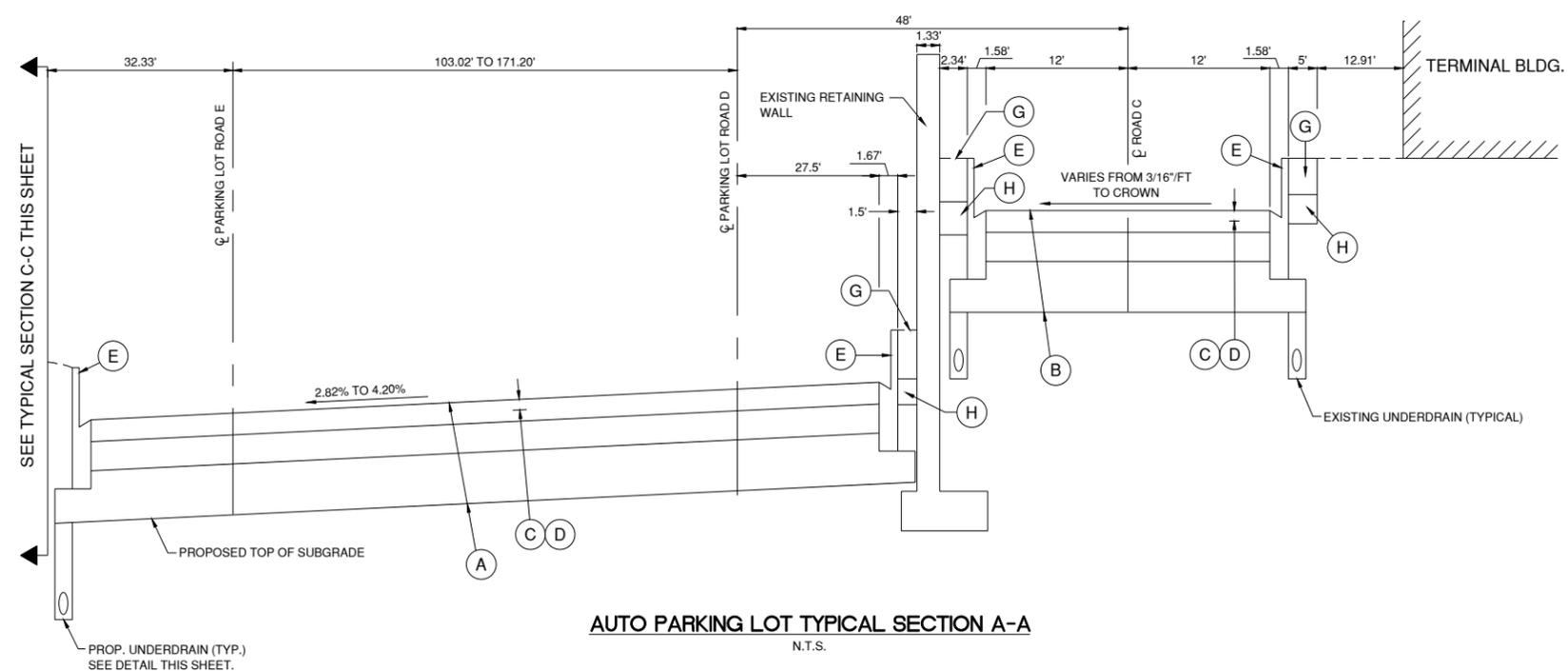
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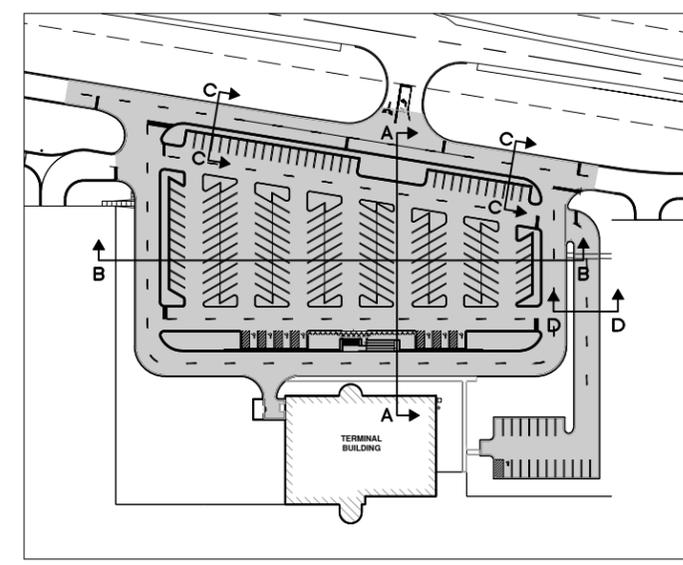


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CHECKED BY:	MND
APPROVED BY:	MND
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JOB No:	18256-02

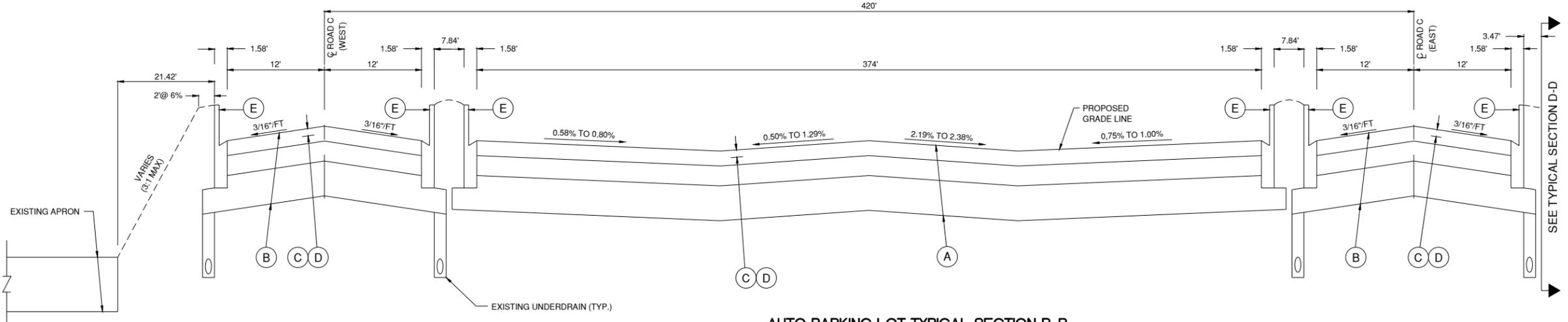
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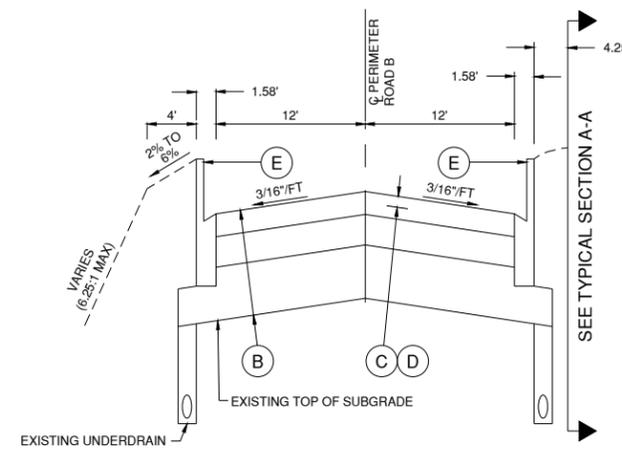
AUTO PARKING LOT TYPICAL SECTION A-A
N.T.S.



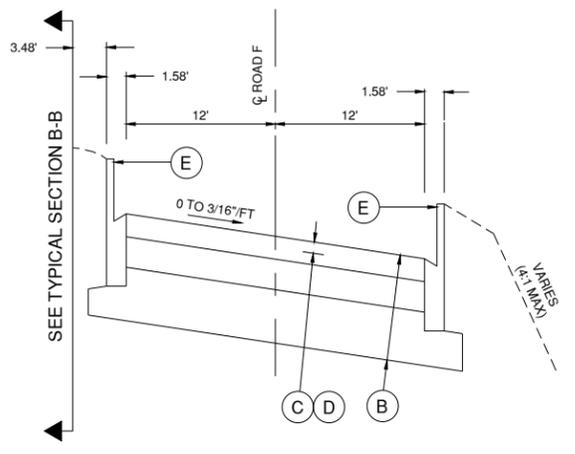
KEY MAP



AUTO PARKING LOT TYPICAL SECTION B-B
N.T.S.

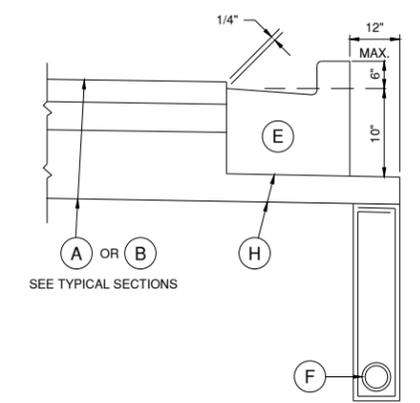


ROADWAY TYPICAL SECTION C-C
N.T.S.



ROADWAY TYPICAL SECTION D-D
N.T.S.

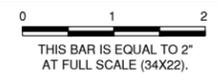
- LEGEND**
- (A) EXISTING PARKING LOT PAVEMENT STRUCTURE
3" BITUMINOUS SURFACE COURSE
3" BITUMINOUS CONCRETE BINDER COURSE
7" AGGREGATE BASE COURSE, TYPE B
 - (B) EXISTING ROADWAY PAVEMENT STRUCTURE
3" BITUMINOUS SURFACE COURSE
4" BITUMINOUS BINDER COURSE
7" AGGREGATE BASE COURSE
 - (C) PROPOSED PARKING LOT PAVEMENT STRUCTURE
1.5" BITUMINOUS MILLING
1.5" BITUMINOUS SURFACE COURSE
 - (D) BITUMINOUS MATERIALS (TACK COAT)
 - (E) EXISTING COMBINATION CONCRETE CURB AND GUTTER,
TYPE B-6.12 (GUTTER THICKNESS SHALL BE 10")
 - (F) EXISTING 6" PIPE UNDERDRAIN W/SOCK
 - (G) EXISTING 5" P.C. CONCRETE SIDEWALK (1/4" PER FOOT SLOPE)
 - (H) EXISTING 4" AGGREGATE BASE COURSE



**UNDERDRAIN/BACK OF CURB/
EDGE OF PAVEMENT DETAIL**
NTS

IL CONTRACT: **SH029**
 IL LETTING ITEM: **21A**
 IL PROJECT: **06C-4649**
 S.B.G. PROJECT: **N/A**

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NUMBER	BY	DATE



**SCHAUMBURG REGIONAL AIRPORT
 SCHAUMBURG, ILLINOIS
 REHABILITATE TERMINAL ENTRANCE ROAD
 AND AUTO PARKING LOT**

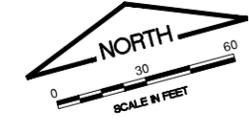
TYPICAL SECTIONS

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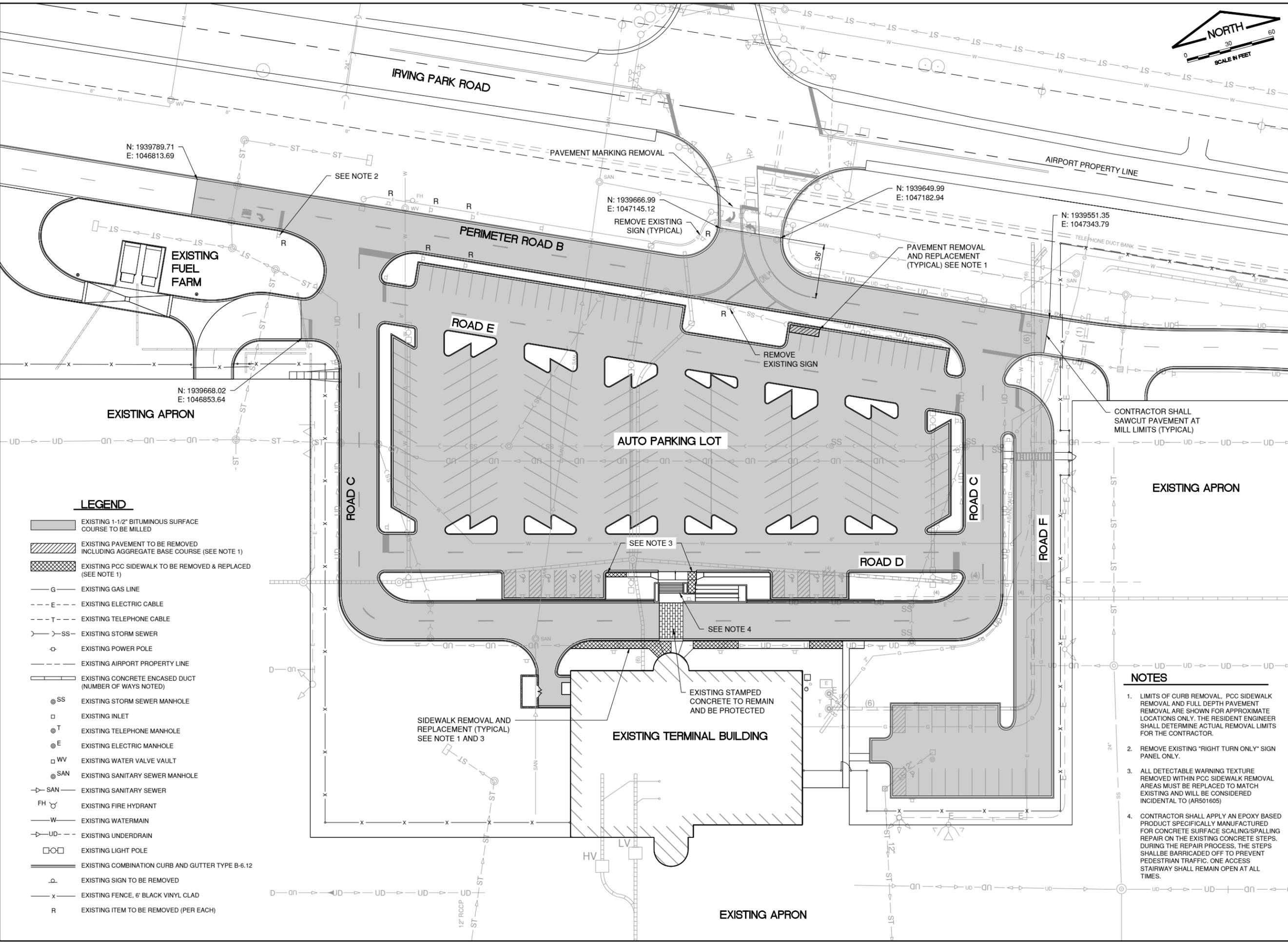
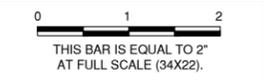
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 UPDATE BY: Neil Finkle
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 REF DWG: base-parking building



IL CONTRACT: **SH029**
 IL LETTING ITEM: **21A**
 IL PROJECT: **06C-4649**
 S.B.G. PROJECT: **N/A**

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LEGEND

- EXISTING 1-1/2" BITUMINOUS SURFACE COURSE TO BE MILLED
- EXISTING PAVEMENT TO BE REMOVED INCLUDING AGGREGATE BASE COURSE (SEE NOTE 1)
- EXISTING PCC SIDEWALK TO BE REMOVED & REPLACED (SEE NOTE 1)
- EXISTING GAS LINE
- EXISTING ELECTRIC CABLE
- EXISTING TELEPHONE CABLE
- EXISTING STORM SEWER
- EXISTING POWER POLE
- EXISTING AIRPORT PROPERTY LINE
- EXISTING CONCRETE ENCASED DUCT (NUMBER OF WAYS NOTED)
- EXISTING STORM SEWER MANHOLE
- EXISTING INLET
- EXISTING TELEPHONE MANHOLE
- EXISTING ELECTRIC MANHOLE
- EXISTING WATER VALVE VAULT
- EXISTING SANITARY SEWER MANHOLE
- EXISTING SANITARY SEWER
- EXISTING FIRE HYDRANT
- EXISTING WATERMAIN
- EXISTING UNDERDRAIN
- EXISTING LIGHT POLE
- EXISTING COMBINATION CURB AND GUTTER TYPE B-6.12
- EXISTING SIGN TO BE REMOVED
- EXISTING FENCE, 6" BLACK VINYL CLAD
- EXISTING ITEM TO BE REMOVED (PER EACH)

NOTES

1. LIMITS OF CURB REMOVAL, PCC SIDEWALK REMOVAL AND FULL DEPTH PAVEMENT REMOVAL ARE SHOWN FOR APPROXIMATE LOCATIONS ONLY. THE RESIDENT ENGINEER SHALL DETERMINE ACTUAL REMOVAL LIMITS FOR THE CONTRACTOR.
2. REMOVE EXISTING "RIGHT TURN ONLY" SIGN PANEL ONLY.
3. ALL DETECTABLE WARNING TEXTURE REMOVED WITHIN PCC SIDEWALK REMOVAL AREAS MUST BE REPLACED TO MATCH EXISTING AND WILL BE CONSIDERED INCIDENTAL TO (AR501605)
4. CONTRACTOR SHALL APPLY AN EPOXY BASED PRODUCT SPECIFICALLY MANUFACTURED FOR CONCRETE SURFACE SCALING/SPALLING REPAIR ON THE EXISTING CONCRETE STEPS. DURING THE REPAIR PROCESS, THE STEPS SHALL BE BARRICADED OFF TO PREVENT PEDESTRIAN TRAFFIC. ONE ACCESS STAIRWAY SHALL REMAIN OPEN AT ALL TIMES.

**SCHAUMBURG REGIONAL AIRPORT
 SCHAUMBURG, ILLINOIS
 REHABILITATE TERMINAL ENTRANCE ROAD
 AND AUTO PARKING LOT**

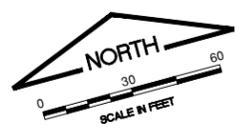
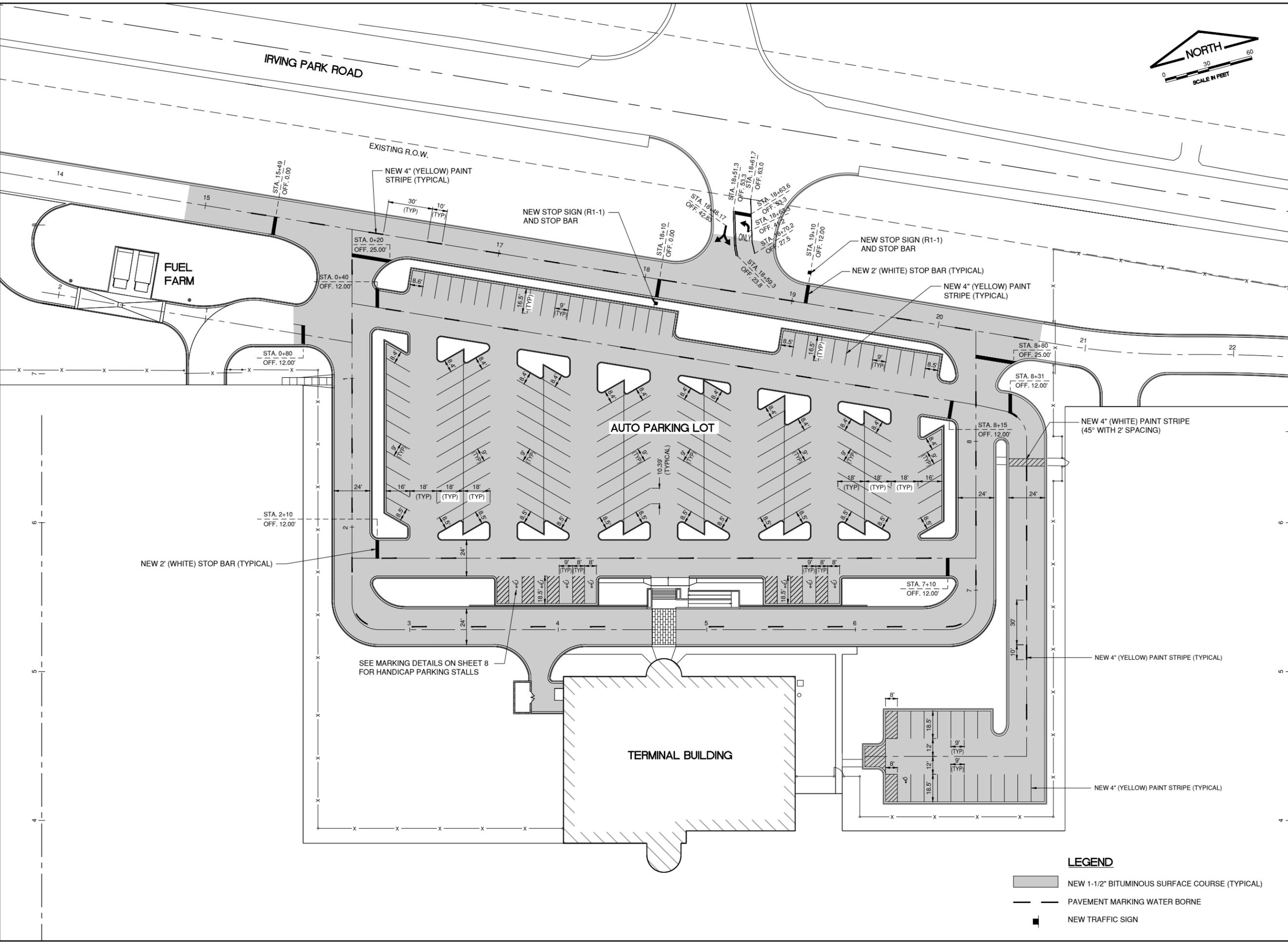
**EXISTING CONDITIONS AND
 PROPOSED REMOVALS**

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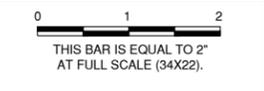
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 Plotting



IL CONTRACT: **SH029**
 IL LETTING ITEM: **21A**
 IL PROJECT: **06C-4649**
 S.B.G. PROJECT: **N/A**

REVISIONS		
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**SCHAUMBURG REGIONAL AIRPORT
 SCHAUMBURG, ILLINOIS
 REHABILITATE TERMINAL ENTRANCE ROAD
 AND AUTO PARKING LOT**
PROPOSED IMPROVEMENTS

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BARRIER CURB, DEPRESSED CURB AND GUTTER TYPE B-6.12 NOTES

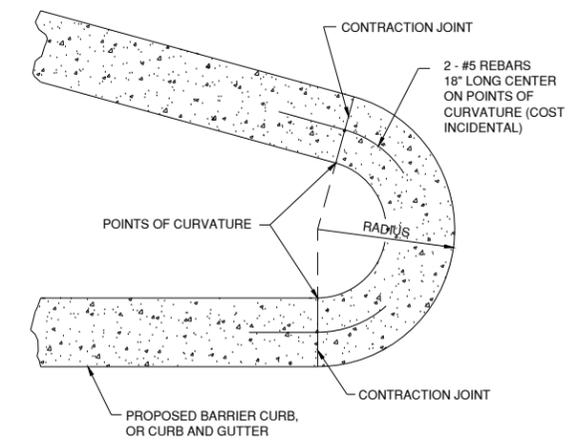
- CONTRACTION JOINTS SHALL BE PLACED AT TEN (10) FOOT MINIMUM ON CENTERS AND SHALL BE SAW CUT TO MINIMUM DEPTH OF TWO (2) INCHES FROM FRONT TO BACK AS SOON AS THE CONCRETE IS SUFFICIENTLY CURED TO ALLOW CUTTING. AS A MAXIMUM, THE CURB SHALL BE SAWED WITHIN TWENTY-FOUR (24) HOURS OF CONCRETE PLACEMENT. CONTRACTION JOINTS SHALL BE FILLED WITH GRAY NP1 OR EQUIVALENT.
- EXPANSION JOINTS SHALL BE CONSTRUCTED AT A 50' MAXIMUM SPACING.
- ALL CURBS SHALL BE CURED AND PROTECTED TO THE REQUIREMENTS OF ARTICLE 606 OF THE STATE OF ILLINOIS "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" LATEST EDITION. MEMBRANE CURING WITH W.R. MEADOWS CS 309, OR APPROVED EQUAL, WILL BE ALLOWED WITH A WHITE FUGITIVE DYE, AS PER TYPE II MEMBRANE CURING.
- CURB AND GUTTER SHALL HAVE A LIGHT BROOM FINISH.
- EXPANSION JOINTS SHALL ALSO BE PLACED AT ALL POINTS OF CURVATURE, AT 5' EACH SIDE OF ALL DRAINAGE STRUCTURES, AT THE END OF THE DAYS POUR, OR AS DIRECTED BY THE ENGINEER.
- TWO (2) NO. 5 REBARS (TEN) 18" LONG SHALL BE PLACED ON EITHER SIDE OF ALL PROPOSED UTILITY TRENCHES.
- THREE (3) NO. 5 REBARS SPACED 5" APART SHALL BE PLACED THE LENGTH OF ALL DEPRESSED CURB.

SIDEWALK RAMPS

- SIDEWALK RAMPS ACCESSIBLE TO THE DISABLED SHALL BE PROVIDED AT ALL LOCATIONS WHERE THE PROPOSED SIDEWALK MEETS THE PROPOSED DEPRESSED CURB AND GUTTER AND SHALL BE CONSTRUCTED IN ACCORDANCE WITH SECTION 424 OF THE STANDARD SPECIFICATIONS AND STANDARD 424001. THIS WORK SHALL BE PAID FOR AS PORTLAND CEMENT CONCRETE SIDEWALK.

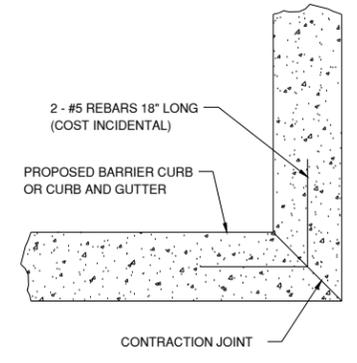
ISLAND RADII REINFORCEMENT DETAIL

N.T.S.
 INSTALL TIE BARS IN LIEU OF DOWEL BARS WHEN RADII ARE 7.5' AND LESS.



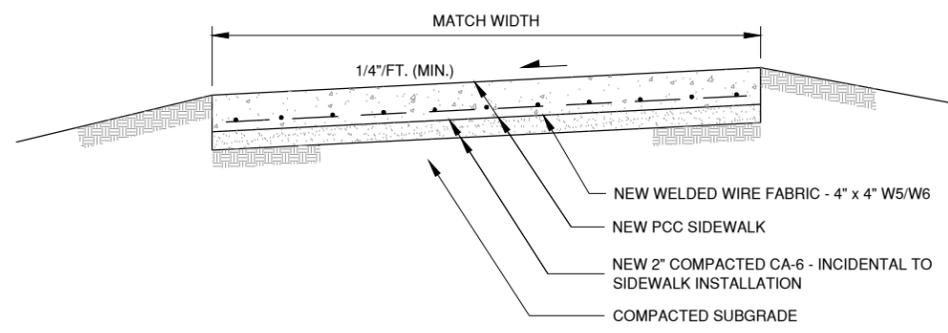
CORNER REINFORCEMENT DETAIL

N.T.S.
 REINFORCEMENTS SHALL BE INSTALLED AT ALL CORNERS.

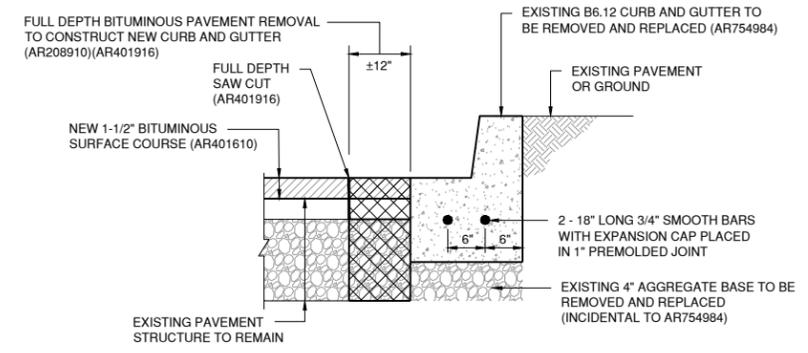


R1-1
 30" x 30"
SIGN DETAIL
 NO SCALE

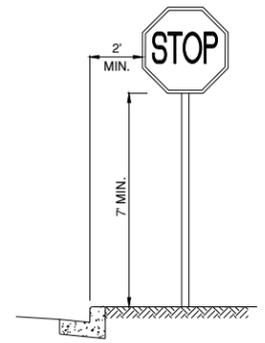
NOTE: 1. ALL SIGNS ARE STANDARD MATERIALS, COLORS AND LETTER STYLE AND SIZE AS SHOWN IN THE "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES", LATEST EDITION.



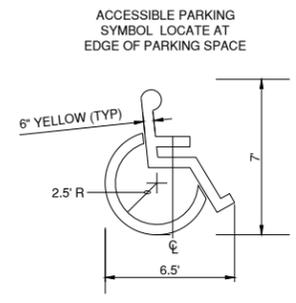
NEW SIDEWALK TYPICAL SECTION
 NO SCALE



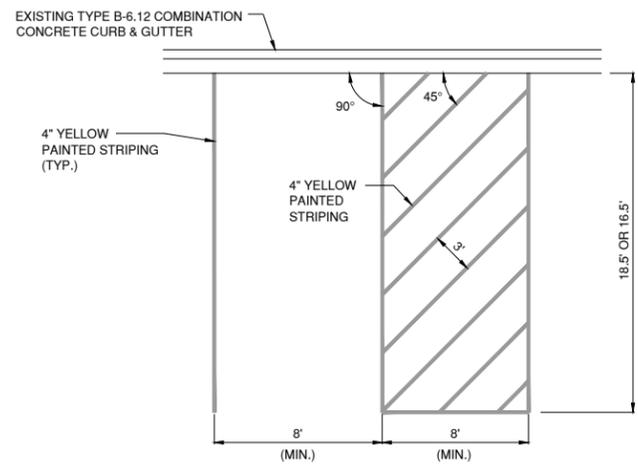
RECONSTRUCT COMBINATION CURB AND GUTTER DETAIL
 NO SCALE



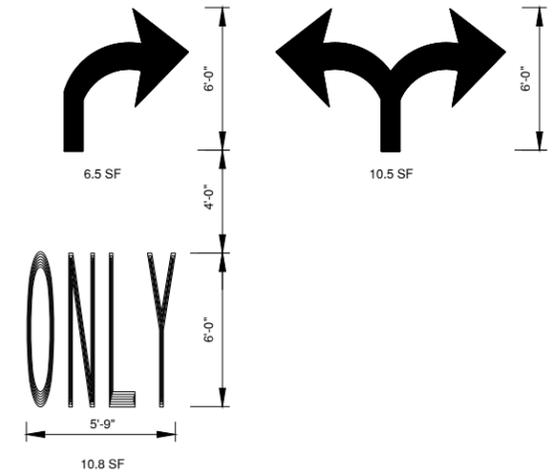
POST MOUNTED SIGN LOCATION DETAIL
 NO SCALE



ACCESSIBLE PARKING SYMBOL LOCATE AT EDGE OF PARKING SPACE



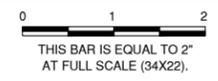
HANDICAP PARKING STALL STRIPING DETAIL
 NO SCALE



DIRECTION SYMBOL DETAIL
 SYMBOLS SHALL BE WHITE

IL CONTRACT: **SH029**
 IL LETTING ITEM: **21A**
 IL PROJECT: **06C-4649**
 S.B.G. PROJECT: **N/A**

REVISIONS		
NUMBER	BY	DATE



**SCHAUMBURG REGIONAL AIRPORT
 SCHAUMBURG, ILLINOIS
 REHABILITATE TERMINAL ENTRANCE ROAD
 AND AUTO PARKING LOT**

MISCELLANEOUS DETAILS

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