

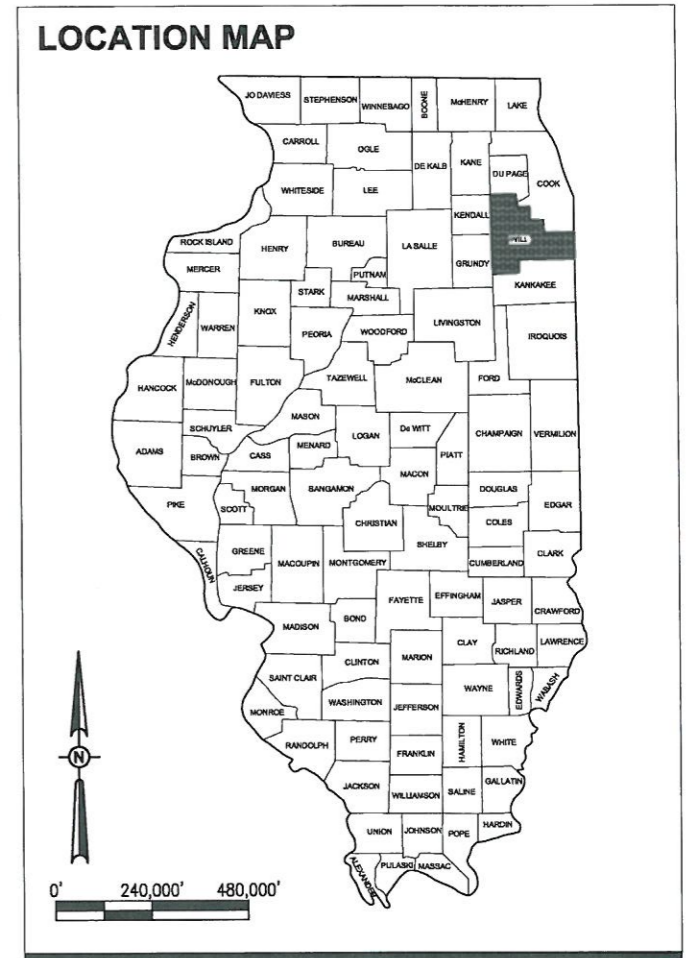
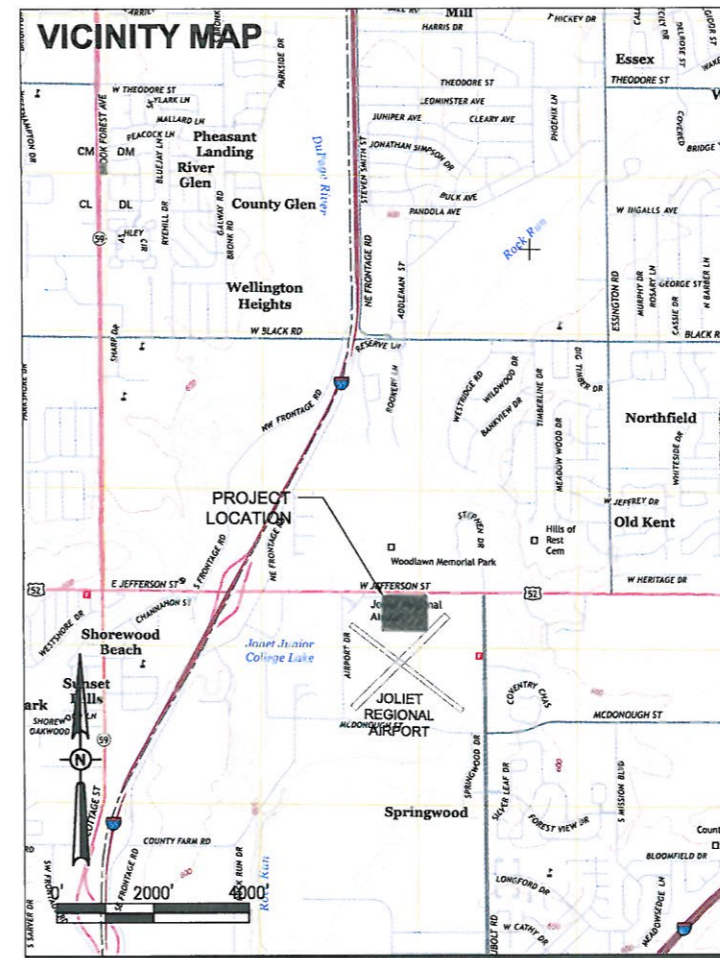
CONSTRUCTION PLANS

REHABILITATE TAXILANES DESIGNATED TH/7, TH/7-1 & TH/7-2

JOLIET PARK DISTRICT  
JOLIET REGIONAL AIRPORT (JOT)  
JOLIET, WILL COUNTY, ILLINOIS

IDA PROJECT NO. JOT-4671  
SBGP PROJECT NO. 3-17-SBGP-144

NOVEMBER 16, 2018



NOTICE TO CONTRACTORS AND BIDDERS

THESE CONSTRUCTION PLANS RELY UPON THE SPECIAL PROVISIONS AND THE SPECIFICATIONS TO PROVIDE FOR A COMPLETE DESCRIPTION OF THE WORK AND CONSTRUCTION REQUIREMENTS. THE PLANS SHALL ONLY BE USED IN COMBINATION WITH ALL CONTRACT DOCUMENTS.

No.	Issue/Description	Sheets Changed	Date	By

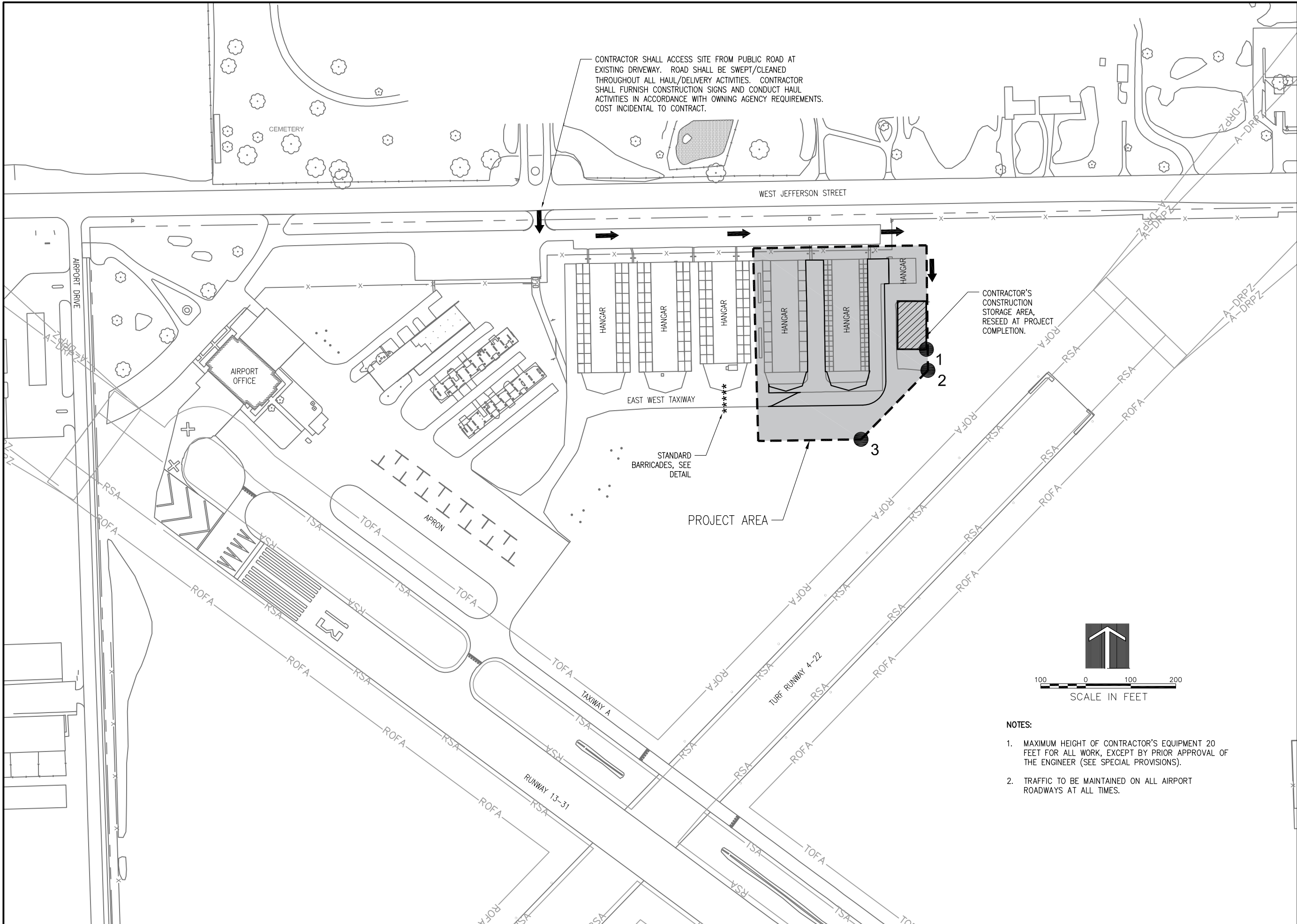
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 062-059464  
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*Lindsay D. Hausman*  
 Lindsay D. Hausman, P.E.,  
 Project Engineer  
 NOVEMBER 16, 2018  
 Date

COVERING ELECTRICAL DESIGN  
  
 KEVIN N. LIGHTFOOT  
 062-047643  
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*Kevin N. Lightfoot*  
 Kevin N. Lightfoot, P.E.,  
 Project Engineer  
 NOVEMBER 16, 2018  
 Date

JOLIET REGIONAL AIRPORT  
 JOLIET PARK DISTRICT  
 4000 W. Jefferson Street  
 Joliet, Illinois, 60431  
 Telephone: 815.741.7267  
  
 Tom Carstern  
 NOVEMBER 16, 2018  
 Date







**NOTES:**

1. MAXIMUM HEIGHT OF CONTRACTOR'S EQUIPMENT 20 FEET FOR ALL WORK, EXCEPT BY PRIOR APPROVAL OF THE ENGINEER (SEE SPECIAL PROVISIONS).
2. TRAFFIC TO BE MAINTAINED ON ALL AIRPORT ROADWAYS AT ALL TIMES.

**REHABILITATE TAXILANES TH/7, TH/7-1 & TH/7-2**

IDA No: JOT-4671  
SBGP No: 3-17-SBGP-144

JO024


NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: November 16, 2018

PROJECT NO: 18A0010  
CAD FILE: 03-SITEPLAN.DWG

DESIGN BY: LDH 9/21/18  
DRAWN BY: LDH 9/21/18

REVIEWED BY: KMS 10/15/18

SHEET TITLE

**SITE AND SAFETY PLAN**

# GENERAL NOTES

## PROJECT DESCRIPTION

THIS PROJECT IS TO REHABILITATE THE T-HANGAR AND ACCESS TAXIWAY PAVEMENTS AT JOLIET REGIONAL AIRPORT INCLUDING, AMONG OTHER INCIDENTAL WORK, THE FOLLOWING ITEMS:

- PLACEMENT OF TEMPORARY SOIL EROSION CONTROL MEASURES.
- REMOVAL OF PAVEMENTS.
- PROVISION OF REQUIRED UNCLASSIFIED EXCAVATION.
- REMOVAL AND ADJUSTMENT OF EXISTING DRAINAGE STRUCTURES, AND INSTALLATION OF NEW DRAINAGE STRUCTURES.
- INSTALLATION OF NEW DRAINAGE LAYER, AGGREGATES AND BITUMINOUS (HMA) PAVEMENTS.
- INSTALLATION OF UNDERDRAINS.
- PLACEMENT OF PAVEMENT MARKINGS.
- TOPSOILING, SEEDING AND MULCHING IN ALL DISTURBED AREAS, INCLUDING ALONG NEW PAVEMENT EDGES.

## PROTECTION OF EXISTING AIRPORT FACILITIES

THE CONTRACTOR IS TO BE RESPONSIBLE FOR THE PROTECTION OF EXISTING UNDERGROUND AND OVERHEAD UTILITIES AND LIGHTING EQUIPMENT; DRIVEWAY AND ROAD PAVEMENT AND SHOULDERS; RUNWAY, TAXIWAY AND APRON PAVEMENTS AND SHOULDERS; RUNWAY, TAXIWAY AND AIRPORT LIGHTING EQUIPMENT; AND SEEDED AND TURFED AREAS THAT ARE UTILIZED IN OR AFFECTED BY THE CONTRACTOR'S ACTIVITIES. ITEMS DAMAGED BY THE CONTRACTOR ARE TO BE REPAIRED AT CONTRACTOR'S EXPENSE AND TO THE SATISFACTION OF THE AIRPORT MANAGER AND THE OWNER'S REPRESENTATIVE.

IN ADDITION, WHEN CONDITIONS DICTATE OR AS DETERMINED BY THE AIRPORT MANAGER OR THE OWNER'S REPRESENTATIVE, THE CONTRACTOR SHALL BE REQUIRED TO USE A PICK-UP TYPE SWEEPER IN ALL ACTIVE CONSTRUCTION AIRFIELD PAVEMENT AREAS. THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES. THE COST OF SWEEPING SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

## CONTRACTOR'S ACCESS AND TEMPORARY FACILITIES

CONTRACTOR'S ACCESS TO THE PROJECT WHEN ON AIRPORT PROPERTY IS SHOWN ON SHEET 3. CONTRACTOR'S ACCESS TO THE AIRPORT ITSELF IS TO BE PROVIDED BY PUBLIC RIGHTS-OF-WAY. THE CONTRACTOR IS TO SECURE ALL NECESSARY PERMITS FOR THE USE OF ANY PUBLIC RIGHTS-OF-WAY AND IS TO MAINTAIN TRAFFIC ON THESE PUBLIC ROADS AT ALL TIMES, WITH THE COSTS OF PERMITTING, CLEANING AND REPAIRING OF PAVEMENT DAMAGED BY CONTRACTOR'S ACTIVITIES INCIDENTAL TO THE CONTRACT. USE OF AND REPAIRS TO ANY PUBLIC FACILITIES ARE TO BE COMPLETED TO THE SATISFACTION OF THE FACILITY'S OWNER.

THE CONTRACTOR IS TO PROVIDE TEMPORARY CONSTRUCTION ROADS WITHIN THE CONSTRUCTION LIMIT LINES AS MAY BE REQUIRED BY HIS ACTIVITIES. HEAVY VEHICLES SHALL NOT CROSS EXISTING PAVEMENT SURFACES EXCEPT AS APPROVED BY THE AIRPORT MANAGER AND THE OWNER'S REPRESENTATIVE. ANY DAMAGE TO PAVEMENTS THAT MAY OCCUR BY THE CONTRACTOR'S ACTIVITIES SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE AND TO THE SATISFACTION OF THE AIRPORT MANAGER AND THE OWNER'S REPRESENTATIVE. FOR HAUL ROUTES MADE BY CONTRACTOR THROUGH GRASSED AREAS, CONTRACTOR SHALL GRADE, LEVEL, TOPSOIL, SEED AND MULCH AT THE END OF THE PROJECT, COST INCIDENTAL TO THE CONTRACT.

THE CONTRACTOR IS TO PROVIDE AN EQUIPMENT STORAGE AND PARKING AREA AT THE LOCATIONS SHOWN ON SHEET 3. IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN THE ACCESS ROADS AND THE STORAGE AREA DURING CONSTRUCTION AND TO RESTORE THE AREAS AT PROJECT COMPLETION TO CONDITIONS SUITABLE TO THE AIRPORT MANAGER AND THE OWNER'S REPRESENTATIVE. AT THE AIRPORT MANAGER'S DISCRETION, THE TEMPORARY FACILITIES MAY REMAIN, BUT THEY MUST BE LEFT IN CONDITIONS SUITABLE TO THE AIRPORT MANAGER. THE COST OF PROVIDING, MAINTAINING AND RESTORING THE TEMPORARY FACILITIES IS INCIDENTAL TO THE CONTRACT.

## RESPONSIBILITY FOR EXISTING UTILITIES

THE LOCATION, SIZE AND/OR TYPE OF MATERIAL OF EXISTING UNDERGROUND OR OVERHEAD UTILITIES AS MAY BE INDICATED ON THESE CONSTRUCTION PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE PROJECT ENGINEER HAVE INDEPENDENTLY VERIFIED THIS INFORMATION AND NEITHER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, SUFFICIENCY OR COMPLETENESS OF THE INFORMATION AND GIVE NO EXPRESSED OR IMPLIED GUARANTEE THAT ANY CONDITIONS INDICATED ARE REPRESENTATIVE OF ACTUAL CONDITIONS TO BE ENCOUNTERED.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY ALL UTILITY COMPANIES AND AGENCIES OF HIS CONSTRUCTION PLANS AND SHALL OBTAIN FROM EACH PARTY DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF ALL UTILITIES AND THE WORKING SCHEDULE OF ANY REMOVALS OR ADJUSTMENTS REQUIRED OF THE UTILITY. THE CONTRACTOR SHALL CONTACT J.U.L.I.E. (PHONE 800-892-0123) TO ASSIST IN THE ABOVE.

THE CONTRACTOR SHALL PROTECT ANY FACILITIES TO THE SATISFACTION OF THE UTILITY OR OWNING-AGENCY WITH THE COST OF ANY REQUIRED PROTECTION TO BE INCIDENTAL TO THE CONTRACT. IN THE EVENT A UTILITY LINE OR SERVICE IS UNEXPECTEDLY ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE OWNER'S REPRESENTATIVE AND THE UTILITY COMPANY OR AGENCY OF JURISDICTION. ANY SUCH UTILITIES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED TO SERVICE AT ONCE.

## EXISTING BENCHMARKS

PROJECT BENCHMARKS ARE AS FOLLOWS:

MF1784  
N 1,767,814.886  
E 1,026,441.694  
ELEV. 579.893

MF0081  
N 1,767,965.5  
E 1,026,578.7  
ELEV. 583.15



PROJECT IS LOCATED IN NORTHWEST 1/4 OF SECTION 14, TROY TOWNSHIP, WILL COUNTY

## NOTES

1. COORDINATES ARE IN NAD 83 FOR HORIZONTAL AND NAVD 88 FOR VERTICAL.
2. STATIONS, OFFSETS AND ELEVATIONS SHOWN ARE IN FEET.
3. THE AIRPORT REFERENCE CODE FOR RUNWAY 13-31 IS B-1. RUNWAY 13 HAS A NON-PRECISION APPROACH WITH VISIBILITY 1 MILE. RUNWAY 31 HAS A VISUAL APPROACH.
4. THE AIRPORT REFERENCE CODE FOR RUNWAY 4-22 IS A-1 WITH VISUAL APPROACHES ON BOTH RUNWAYS 4 AND 22.

## RUNWAY END COORDINATES

DESCRIPTION	LATITUDE	LONGITUDE	RUNWAY ELEVATION
RUNWAY 13 END	41°31'11.79" N	88°10'46.85" W	580.2
RUNWAY 31 END	41°30'54.48" N	88°10'15.86" W	577.3
RUNWAY 4 END	41°30'54.66" N	88°10'45.64" W	574.6
RUNWAY 22 END	41°31'14.09" N	88°10'20.47" W	578.4

OBJECT INFORMATION												
ITEM NO.	DESCRIPTION	MOBILITY	GROUND ELEVATION	OBJECT ELEVATION	LATITUDE	LONGITUDE	RUNWAY 13-31 STATION	RUNWAY 13-31 OFFSET	RUNWAY 13-31 EXIST EL.	RUNWAY 4-22 STATION	RUNWAY 4-22 OFFSET	RUNWAY 4-22 EXIST EL.
1	CONTRACTOR'S CONSTRUCTION STORAGE	STATIONARY	578.1	598.1	41° 31' 15.0858" N	88° 10' 24.7201" W	31+78.45	1,269.7	580.0	245+91.77	300.0	578.0
2	CONSTRUCTION EQUIPMENT	MOVING	578.1	598.1	41° 31' 14.6240" N	88° 10' 24.6759" W	32+08.98	1,234.1	580.0	245+60.58	265.0	578.0
3	CONSTRUCTION EQUIPMENT	MOVING	581.0	601.0	41° 31' 13.1097" N	88° 10' 26.6331" W	31+80.58	1,022.3	580.0	243+46.91	265.0	578.0



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Illinois Licensed  
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REHABILITATE TAXILANES TH/7, TH/7-1 & TH/7-2

IDA No: JOT-4671  
SBGP No: 3-17-SBGP-144

JO024

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: November 16, 2018

PROJECT NO: 18A0010

CAD FILE: 04-GENERALNOTES.DWG

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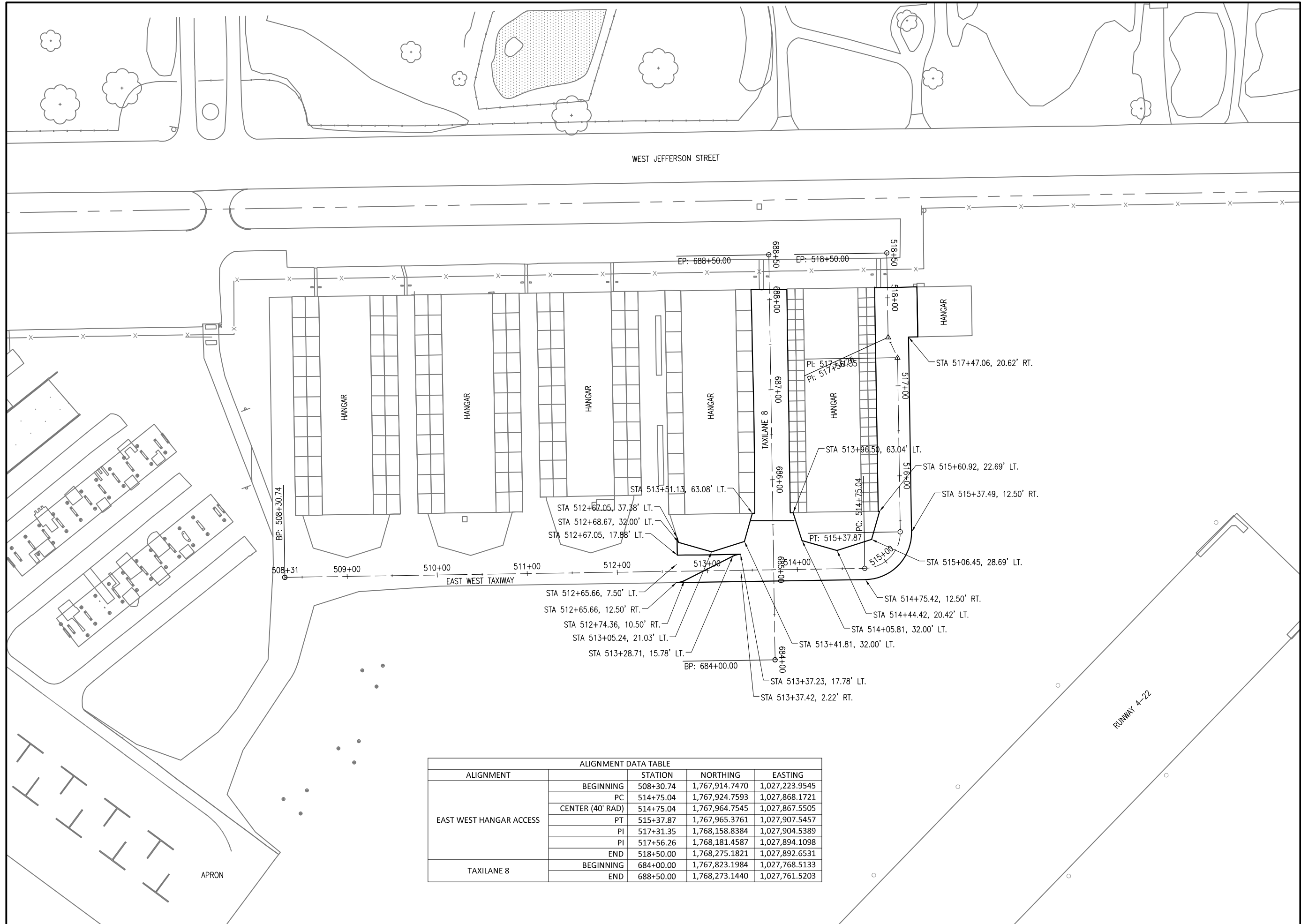
REVIEWED BY: KMS 10/15/18

SHEET TITLE

SITE AND SAFETY PLAN NOTES







**REHABILITATE  
TAXILANES TH/7, TH/7-1  
& TH/7-2**

IDA No: JOT-4671  
SBGP No: 3-17-SBGP-144

JO024

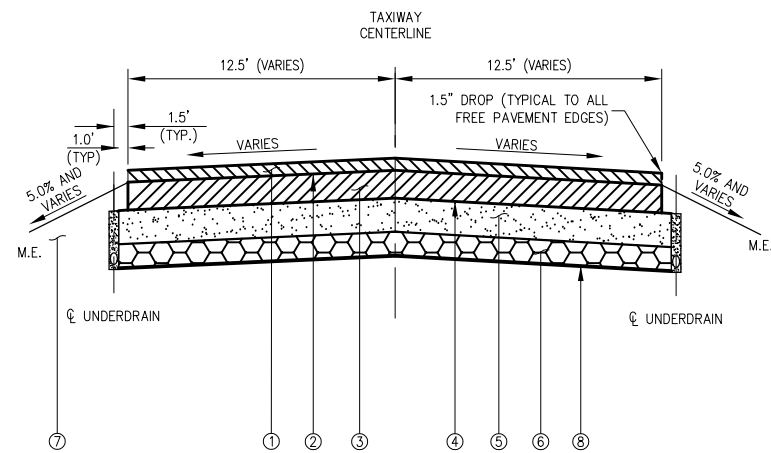

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SHEET TITLE

**ALIGNMENT TABLE  
AND PAVEMENT  
LAYOUT**

ALIGNMENT DATA TABLE				
ALIGNMENT	STATION	NORTHING	EASTING	
EAST WEST HANGAR ACCESS	BEGINNING	508+30.74	1,767,914.7470	1,027,223.9545
	PC	514+75.04	1,767,924.7593	1,027,868.1721
	CENTER (40' RAD)	514+75.04	1,767,964.7545	1,027,867.5505
	PT	515+37.87	1,767,965.3761	1,027,907.5457
	PI	517+31.35	1,768,158.8384	1,027,904.5389
	PI	517+56.26	1,768,181.4587	1,027,894.1098
	END	518+50.00	1,768,275.1821	1,027,892.6531
TAXILANE 8	BEGINNING	684+00.00	1,767,823.1984	1,027,768.5133
	END	688+50.00	1,768,273.1440	1,027,761.5203

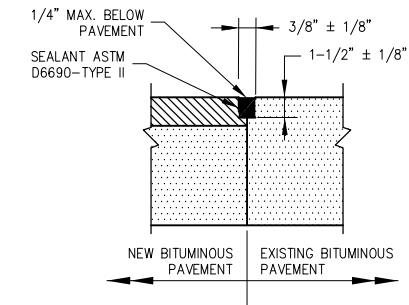


**TYPICAL SECTION - TAXIWAY/TAXILANE PAVEMENTS**

- ① PROPOSED 2.0 INCH BITUMINOUS SURFACE COURSE, ITEM AR401613
- ② PROPOSED BITUMINOUS TACK COAT, ITEM AR603510 \*
- ③ PROPOSED 2.0 INCH BITUMINOUS BASE COURSE, ITEM AR403613
- ④ PROPOSED BITUMINOUS PRIME COAT, ITEM AR602510
- ⑤ PROPOSED 6 INCH CRUSHED AGGREGATE BASE COURSE, ITEM AR209606
- ⑥ PROPOSED 6 INCH GRANULAR DRAINAGE SUBBASE, ITEM AR154606
- ⑦ PROPOSED 4 INCH TOPSOIL, ITEM AR905510
- ⑧ PROPOSED SOIL STABILIZATION FABRIC, ITEM AR152540

\* BITUMINOUS TACK COAT SHALL BE APPLIED BETWEEN EACH LIFT OF BITUMINOUS BASE COURSE AND BETWEEN THE BITUMINOUS BASE COURSE TOP LIFT AND THE BITUMINOUS SURFACE COURSE - NO EXCEPTIONS.

BITUMINOUS PRIME AND BITUMINOUS TACK COAT SHALL BE REQUIRED AS SPECIFIED. SEE SPECIAL PROVISIONS AND STANDARD PROVISIONS.



**NOTE:**  
ALL BITUMINOUS/BITUMINOUS JOINT SEALING TO BE PAID UNDER SAW AND SEAL BITUMINOUS JOINTS, ITEM AR401660.

**BITUMINOUS/BITUMINOUS SEAL**

REHABILITATE  
TAXILANES TH/7, TH/7-1  
& TH/7-2

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SBGP No: 3-17-SBGP-144

JO024

NO.	DATE	DESCRIPTION		
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SHEET TITLE

TYPICAL SECTION  
AND PAVEMENT  
DETAILS





NO.	DATE	DESCRIPTION		
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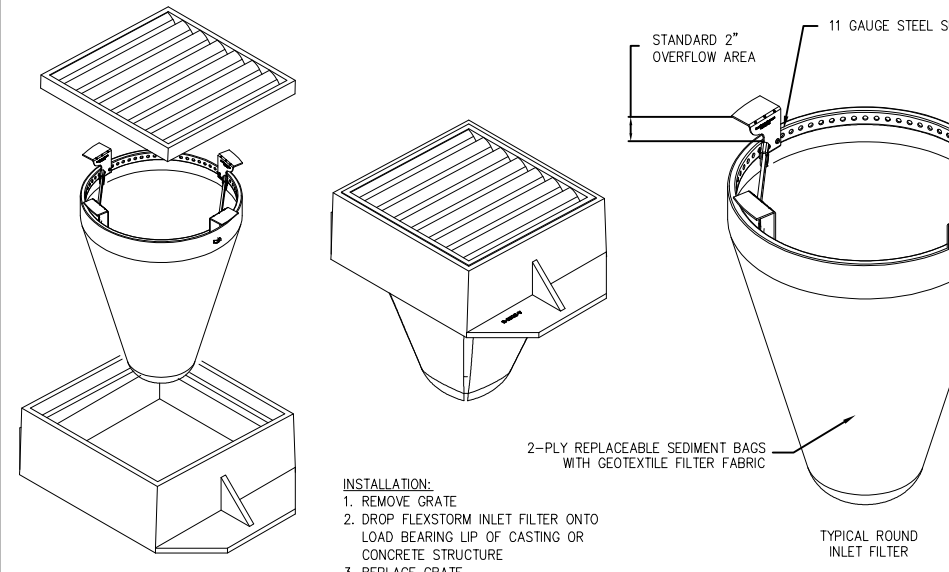
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SHEET TITLE

SWPPP DETAILS

**SEDIMENTATION AND EROSION CONTROL NOTES:**

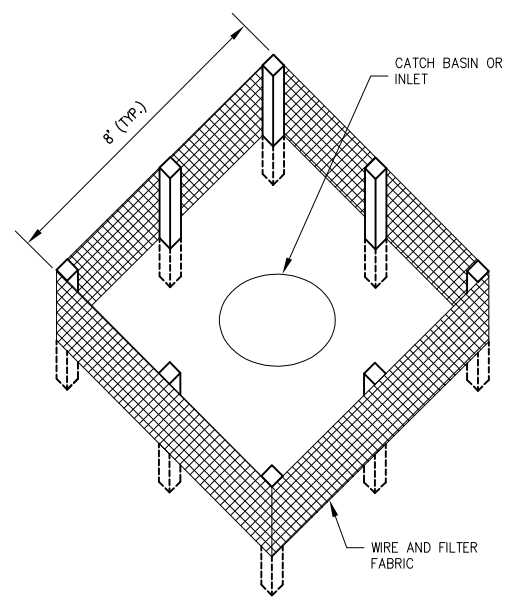
- SEDIMENT CONTROL MEASURES SHALL BE INSTALLED PRIOR TO THE COMMENCEMENT OF HYDROLOGIC DISTURBANCE OF UPLAND AREAS.
- FOR THOSE DEVELOPMENTS THAT REQUIRE A DESIGNATED EROSION CONTROL INSPECTOR (DECI), INSPECTIONS AND DOCUMENTATION SHALL BE PERFORMED AT A MINIMUM:
  - UPON COMPLETION OF SEDIMENT AND RUNOFF CONTROL MEASURES (INCLUDING PERIMETER CONTROLS AND DIVERSIONS), PRIOR TO PROCEEDING WITH ANY OTHER EARTH DISTURBANCE OR GRADING.
  - AFTER EVERY SEVEN (7) CALENDAR DAYS OR STORM EVENT WITH GREATER THAN 0.5 INCH OF RAINFALL OR LIQUID EQUIVALENT PRECIPITATION.
- SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. IF STRIPPING, CLEARING, GRADING, OR LANDSCAPING ARE TO BE DONE IN PHASES, THE PERMITTEE SHALL PLAN FOR APPROPRIATE SOIL EROSION AND SEDIMENT CONTROL MEASURES.
- A STABILIZED MAT OF CRUSHED STONE MEETING IDOT GRADATION CA-01 UNDERLAIN WITH FILTER FABRIC AND IN ACCORDANCE WITH THE ILLINOIS URBAN MANUAL, OR OTHER APPROPRIATE MEASURE(S) AS APPROVED BY THE ENFORCEMENT OFFICER, SHALL BE INSTALLED AT ANY POINT WHERE TRAFFIC WILL BE ENTERING OR LEAVING A CONSTRUCTION SITE. SEDIMENT OR SOIL REACHING AN IMPROVED PUBLIC RIGHT OF WAY, STREET, ALLEY OR PARKING AREA SHALL BE REMOVED BY SCRAPING OR STREET CLEANING AS ACCUMULATIONS WARRANT AND TRANSPORTED TO A CONTROLLED SEDIMENT DISPOSAL AREA.
- TEMPORARY DIVERSIONS SHALL BE CONSTRUCTED AS NECESSARY TO DIRECT ALL RUNOFF FROM HYDROLOGICALLY DISTURBED AREAS TO AN APPROPRIATE SEDIMENT TRAP OR BASIN.
- DISTURBED AREAS SHALL BE STABILIZED WITH TEMPORARY OR PERMANENT MEASURES WITHIN SEVEN (7) CALENDAR DAYS FOLLOWING THE END OF ACTIVE HYDROLOGIC DISTURBANCE OR REDISTURBANCE.
- ALL STOCKPILES SHALL HAVE APPROPRIATE MEASURES TO PREVENT EROSION. STOCKPILES SHALL NOT BE PLACED IN FLOOD PRONE AREAS OR WETLANDS AND DESIGNATED BUFFERS.
- SLOPES STEEPER THAN 3H:1V SHALL BE STABILIZED WITH APPROPRIATE MEASURES AS APPROVED BY THE ENFORCEMENT OFFICER.
- APPROPRIATE EROSION CONTROL BLANKET SHALL BE INSTALLED ON ALL INTERIOR DETENTION BASIN SIDE SLOPES BETWEEN THE NORMAL WATER LEVEL AND HIGH WATER LEVEL.
- STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY AN APPROPRIATE SEDIMENT CONTROL MEASURE.
- IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION AND SEDIMENTATION. DISCHARGES SHALL BE ROUTED THROUGH AN APPROVED ANIONIC POLYMER DEWATERING SYSTEM OR A SIMILAR MEASURE AS APPROVED BY THE ENFORCEMENT OFFICER. DEWATERING SYSTEMS SHOULD BE INSPECTED DAILY DURING OPERATIONAL PERIODS. THE ENFORCEMENT OFFICER, OR APPROVED REPRESENTATIVE, MUST BE PRESENT AT THE COMMENCEMENT OF DEWATERING ACTIVITIES.
- IF INSTALLED SOIL EROSION AND SEDIMENT CONTROL MEASURES DO NOT MINIMIZE SEDIMENT LEAVING THE DEVELOPMENT SITE, ADDITIONAL MEASURES SUCH AS ANIONIC POLYMERS OR FILTRATION SYSTEMS MAY BE REQUIRED BY THE ENFORCEMENT OFFICER.
- ALL TEMPORARY AND PERMANENT EROSION CONTROL MEASURES MUST BE MAINTAINED AND REPAIRED AS NEEDED. THE PROPERTY OWNER SHALL BE ULTIMATELY RESPONSIBLE FOR MAINTENANCE AND REPAIR.
- ALL TEMPORARY SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED.
- THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER, ENFORCEMENT OFFICER, OR OTHER GOVERNING AGENCY.



**INSTALLATION:**  
1. REMOVE GRATE  
2. DROP FLEXSTORM INLET FILTER ONTO LOAD BEARING LIP OF CASTING OR CONCRETE STRUCTURE  
3. REPLACE GRATE

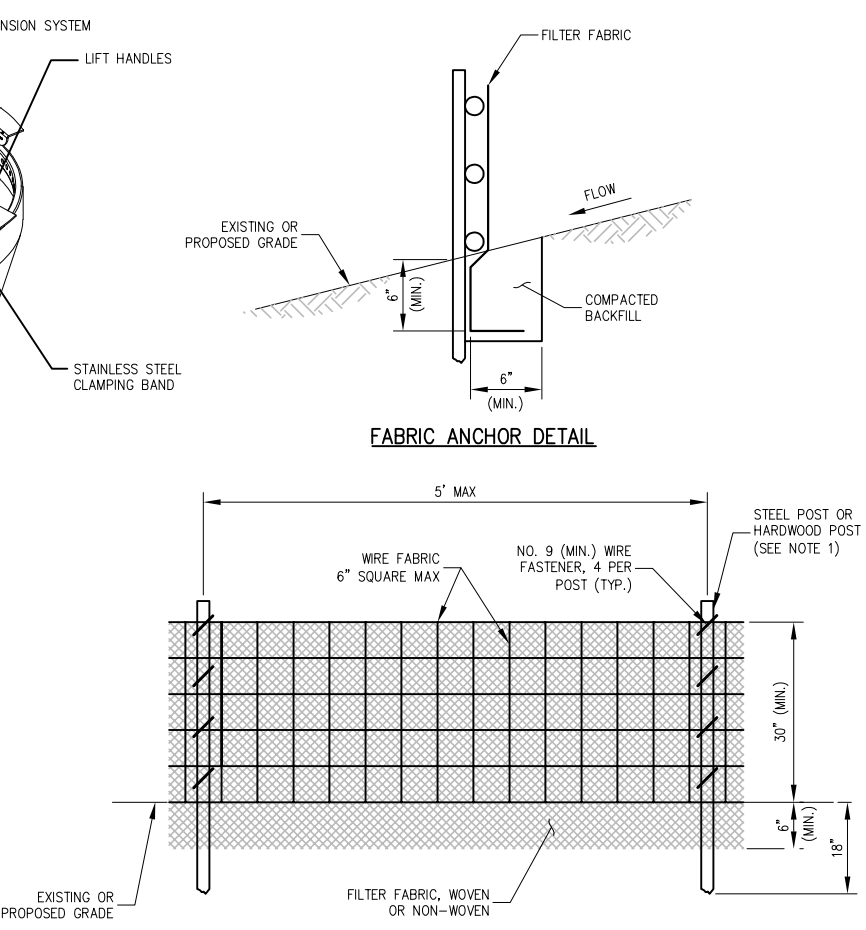
- NOTES:**
- FILTER FABRIC INLET PROTECTION SHALL CONSIST OF INLET BASKET AND FABRIC INSERT, FLEXSTORM CATCH-IT BY ADVANCED DRAINAGE SYSTEMS, FLOGARD TEMPORARY INLET FILTER BY OLDCASTLE, OR APPROVED EQUAL.
  - DEVICE SHALL BE EQUIPPED WITH AN OVERFLOW FEATURE SO DRAINAGE TO INLET IS NOT COMPLETELY BLOCKED IF DEVICE IS FULL OF SILT.
  - INLET BASKET IS AVAILABLE TO FIT ROUND, RECTANGULAR, BEEHIVE OR CURB INLET CASTINGS.
  - FILTER FABRIC SHALL HAVE AN APPARENT OPENING SIZE (AOS) OF AT LEAST 70 SIEVE FOR NONWOVEN.
  - FILTER FABRIC SHALL HAVE A GRAB TENSILE STRENGTH OF A LEAST 100 LBS FOR NON WOVEN.
  - POLYESTER OUTER REINFORCEMENT BAG SHALL HAVE FABRIC WITH A WEIGHT OF 4.55 OZ/SQYD +/- 15 PERCENT.
  - FRAME CONSTRUCTION SHALL HAVE A TENSILE STRENGTH OF AT LEAST 58,000 PSI AND A YIELD STRENGTH OF AT LEAST 36,000 PSI.
  - MAINTENANCE SHALL BE PERFORMED AS NEEDED. REMOVE SILT FROM FABRIC INSERT WHEN 50% OF CAPACITY IS REACHED. REMOVE SILT FROM INTERIOR AND EXTERIOR OF INLET DAM WHEN 50% OF DAM HEIGHT IS REACHED.
  - PAYMENT FOR INLET PROTECTION MAINTENANCE SHALL BE INCIDENTAL TO INLET PROTECTION.

**INLET PROTECTION AT MANHOLES IN IMPERVIOUS AREAS**



- NOTES:**
- FILTER FABRIC SHALL BE EMBEDDED 8" INTO THE SOIL.
  - INSPECTION SHALL BE FREQUENT AND REPAIR/REPLACEMENT SHALL BE MADE PROMPTLY AS NEEDED.
  - SILT FENCE SHALL BE REMOVED WHEN IT HAS SERVED ITS USEFULNESS AT THE DIRECTION OF THE AIRPORT REPRESENTATIVE OR OWNER SO AS NOT TO BLOCK OR IMPEDE STORM FLOW OR DRAINAGE. CONTRACTOR SHALL PLACE SEED AND MULCH AROUND STRUCTURES PER LANDSCAPING PLAN. COST OF REMOVAL SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE FOR SILT FENCE.
  - AREAS DISTURBED OUTSIDE OF CONSTRUCTION LIMITS DURING PLACEMENT OF INLET PROTECTION TO BE RE-GRADED, SEEDED AND MULCHED, COST INCIDENTAL TO SILT FENCE.
  - FENCE AND POSTS SHALL BE REMOVED WHEN DIRECTED AT PROJECT END.
  - PAID UNDER AR156510 SILT FENCE

**SILT FENCE AT MANHOLES IN PERVIOUS AREAS**



- NOTES:**
- FENCE POST SHALL BE EITHER STEEL "1" LINE POST OR HARDWOOD POST WITH A MINIMUM SECTIONAL AREA OF 2.0 SQUARE INCHES. A CARPENTER'S (NOMINAL) 2"x2" POST WILL MEET SPECIFICATIONS.
  - TOP AND BOTTOM WIRE OF WIRE FABRIC SHALL BE MINIMUM GAGE NO. 9. INTERMEDIATE WIRES OF THE WIRE FABRIC SHALL BE MINIMUM GAGE NO. 11.
  - WIRE FABRIC SHALL BE SECURELY FASTENED TO FENCE POSTS WITH NO. 9 GAGE WIRE MINIMUM. FOUR (4) FASTENERS PER POST REQUIRED.
  - FILTER FABRIC SHALL BE SECURELY FASTENED TO WIRE FABRIC AND POSTS WITH TIES OR STAPLES SPACED AT 12" APART AT THE TOP, MIDDLE AND BOTTOM.
  - WHEN TWO SECTIONS OF FILTER FABRIC MEET, THEY SHALL BE OVERLAPPED BY 6" AND FOLDED AND ATTACHED TO THE WIRE FABRIC AT A POST.
  - FILTER FABRIC SHALL BE IN ACCORDANCE WITH SPECIAL PROVISIONS WITH APPARENT OPENING SIZE (AOS) OF AT LEAST 40 FOR NONWOVEN AND WOVEN. **THE FABRIC MUST MEET THE APPLICABLE STANDARDS OF AASHTO 288-00 (Article IV, Section B.1.1.i, AS AMENDED), OR EQUIVALENT.**
  - A MAXIMUM OF 5 FEET IS USED FOR POST-TO-POST SPACING.
  - SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. SOIL STABILIZATION MEASURES SHALL CONSIDER THE TIME OF YEAR, SITE CONDITIONS AND THE USE OF TEMPORARY OR PERMANENT MEASURES.
  - ALL STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY AN APPROPRIATE SEDIMENT CONTROL MEASURE.
  - SILT FENCE SHALL BE INSTALLED PRIOR TO ANY GRADING WORK IN THE AREA TO BE PROTECTED. PERIODIC INSPECTION SHALL BE PERFORMED AND REQUIRED MAINTENANCE SHALL BE PROVIDED AFTER EACH RAIN EVENT.
  - MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED AND REPLACED WHEN BULGES DEVELOP IN THE SILT FENCE.
  - IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION. DISCHARGES SHALL BE ROUTED THROUGH AN EFFECTIVE SEDIMENT CONTROL MEASURE (E.G. SEDIMENT TRAP, SEDIMENT BASIN, OR OTHER APPROPRIATE MEASURE).
  - FENCE POSTS SHALL BE REMOVED WHEN DIRECTED AT PROJECT END.
  - THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER OR GOVERNING AGENCY.

**ELEVATION**

**STORM WATER POLLUTION PREVENTION NOTES**

**GENERAL**  
THE CONTRACTOR SHALL IMPLEMENT ALL PROVISIONS OF THE CONTRACT DOCUMENTS TO ASSURE THAT STORM WATER POLLUTION PREVENTION ITEMS ARE CONSTRUCTED AND MAINTAINED IN A TIMELY MANNER. SEDIMENTATION MUST NOT BE TRANSPORTED OFF THE CONSTRUCTION SITE. PERMANENT DRAINAGE FEATURES AND VEGETATIVE MEASURES SHALL BE PROVIDED AS SOON AS POSSIBLE.

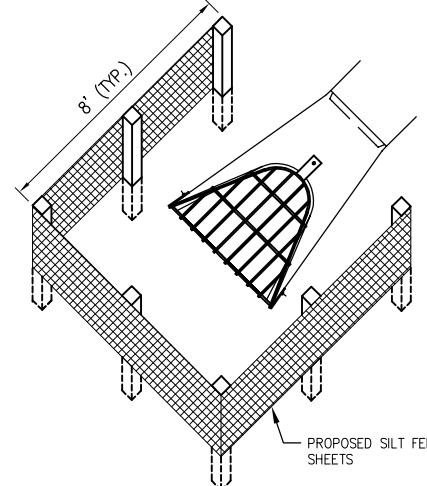
**THE MAINTENANCE OF ALL STORM WATER POLLUTION PREVENTION MEASURES IS INCIDENTAL TO THE ASSOCIATED ITEM.**

**POLLUTION PREVENTION MEASURES**  
THE CONTRACTOR SHALL BE REQUIRED TO IMPLEMENT AND MAINTAIN STORM WATER POLLUTION PREVENTION PRACTICES AND MEASURES PRIOR TO THE STRIPPING OF EXISTING VEGETATION WHEREVER POSSIBLE AND AS SOON AS CONSTRUCTION PERMITS IN OTHER AREAS. POLLUTION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE CONTRACT DOCUMENTS, INCLUDING THESE CONSTRUCTION PLANS, AND WITH STANDARDS AND SPECIFICATIONS FOR SOIL EROSION AND SEDIMENT CONTROL, ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, CURRENT ISSUE. THE CONTRACTOR SHALL ADJUST HIS OPERATIONS AND IMPLEMENT POLLUTION CONTROL MEASURES SO THAT NO RUNOFF FROM STRIPPED AREAS WILL LEAVE THE CONSTRUCTION SITE OTHER THAN THROUGH SEDIMENT TRAPS OR OTHER SUITABLE CONTROL MEASURES.

POLLUTION CONTROL ITEMS SHALL BE PROVIDED AS NOTED ON THE STORM WATER POLLUTION PREVENTION PLAN AND IN THE STORM WATER POLLUTION PREVENTION DETAILS AND AS DIRECTED BY THE ENGINEER. THE LIMITS OF SUCH MEASURES SHALL BE STAKED BY THE CONTRACTOR PRIOR TO THE COMMENCEMENT OF CONSTRUCTION. SUCH LIMITS MAY BE ADJUSTED BY THE ENGINEER TO ACCOUNT FOR ACTUAL SITE CONDITIONS EXPERIENCED DURING CONSTRUCTION. ADDITIONAL COMPENSATION FOR MEASURES EXCEEDING THE PLAN QUANTITIES WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR EACH ITEM.

THE CONTRACTOR IS TO MAINTAIN AND ADJUST, REPAIR OR REPLACE ALL POLLUTION PREVENTION MEASURES AS REQUIRED OR AS DIRECTED BY THE ENGINEER UNTIL PERMANENT VEGETATION HAS BEEN ESTABLISHED. MAINTENANCE OF POLLUTION CONTROL MEASURES IS TO BE PROVIDED AT NO ADDITIONAL COST TO THE CONTRACT.

ADDITIONAL STORMWATER POLLUTION PREVENTION MEASURES ARE EXISTING ON SITE LOCATED AT DRAINAGE FACILITIES AND ALONG THE PROPERTY LINE.

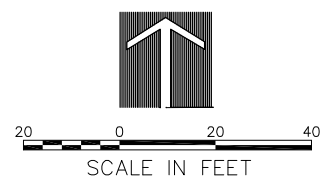
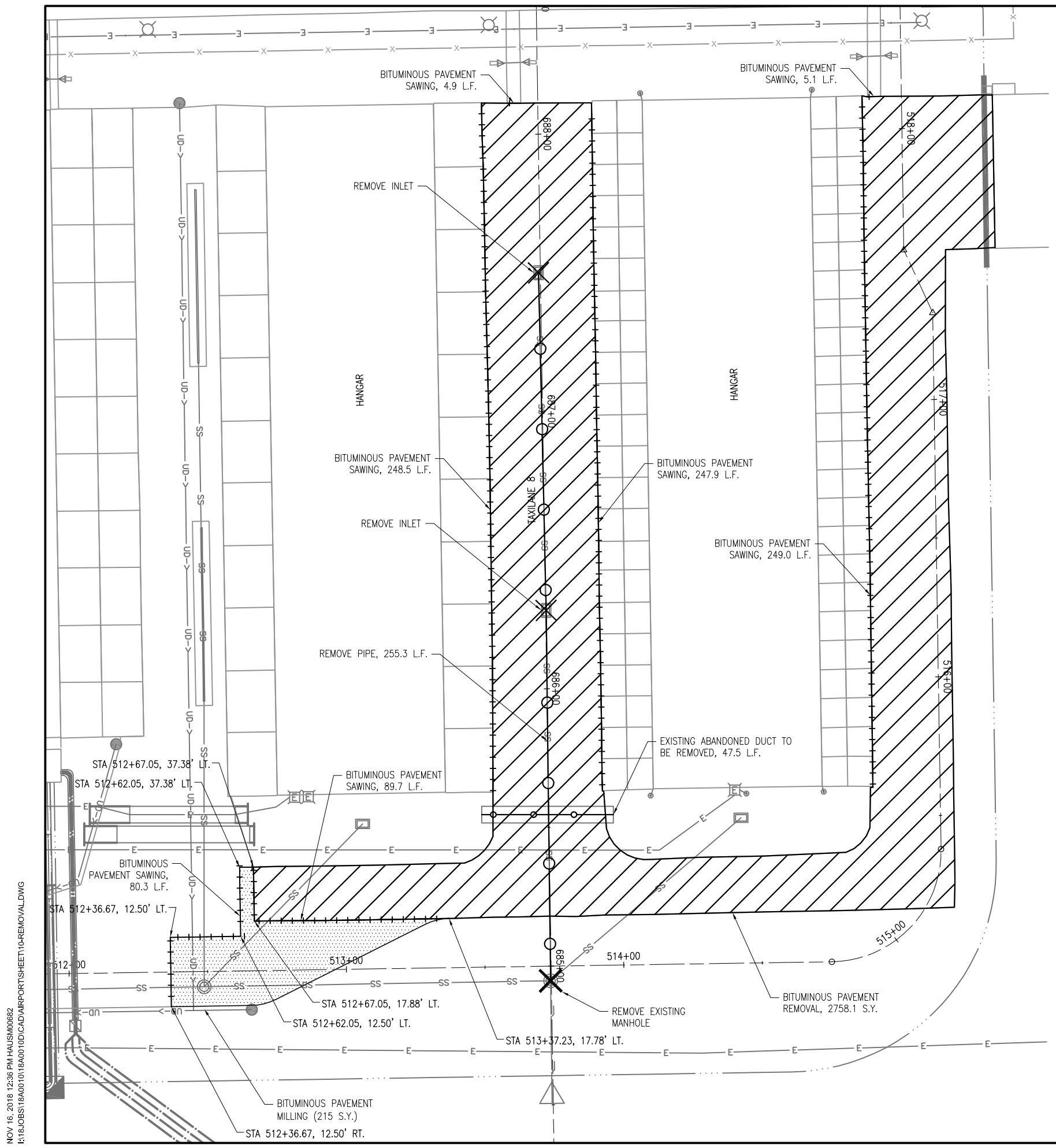


**SILT FENCE PLACEMENT AT FLARED END SECTIONS (FES)**

**REHABILITATE  
TAXILANES TH/7, TH/7-1  
& TH/7-2**

IDA No: JOT-4671  
SBGP No: 3-17-SBGP-144

JO024



- LEGEND:**
- PROPOSED BITUMINOUS PAVEMENT MILLING
  - PROPOSED BITUMINOUS PAVEMENT REMOVAL
  - PROPOSED BITUMINOUS PAVEMENT SAWING
  - PROPOSED PIPE REMOVAL

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND/OR THE RESIDENT ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY. CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123. CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. LOCATION OF FAA POWER CONTROL, AND COMMUNICATION CABLES SHALL BE COORDINATED WITH AND/OR LOCATED BY THE FAA. CONTACT AIRPORT DIRECTOR/MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVE GROUND UTILITIES.

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: November 16, 2018  
PROJECT NO: 18A0010  
CAD FILE: 10-REMOVAL.DWG  
DESIGN BY: LFH 9/22/18  
DRAWN BY: LDH 9/22/18  
REVIEWED BY: KMS 10/15/18

SHEET TITLE

**REMOVAL PLAN**

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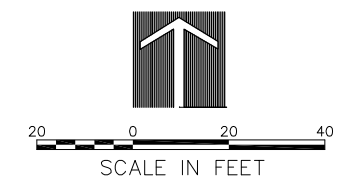
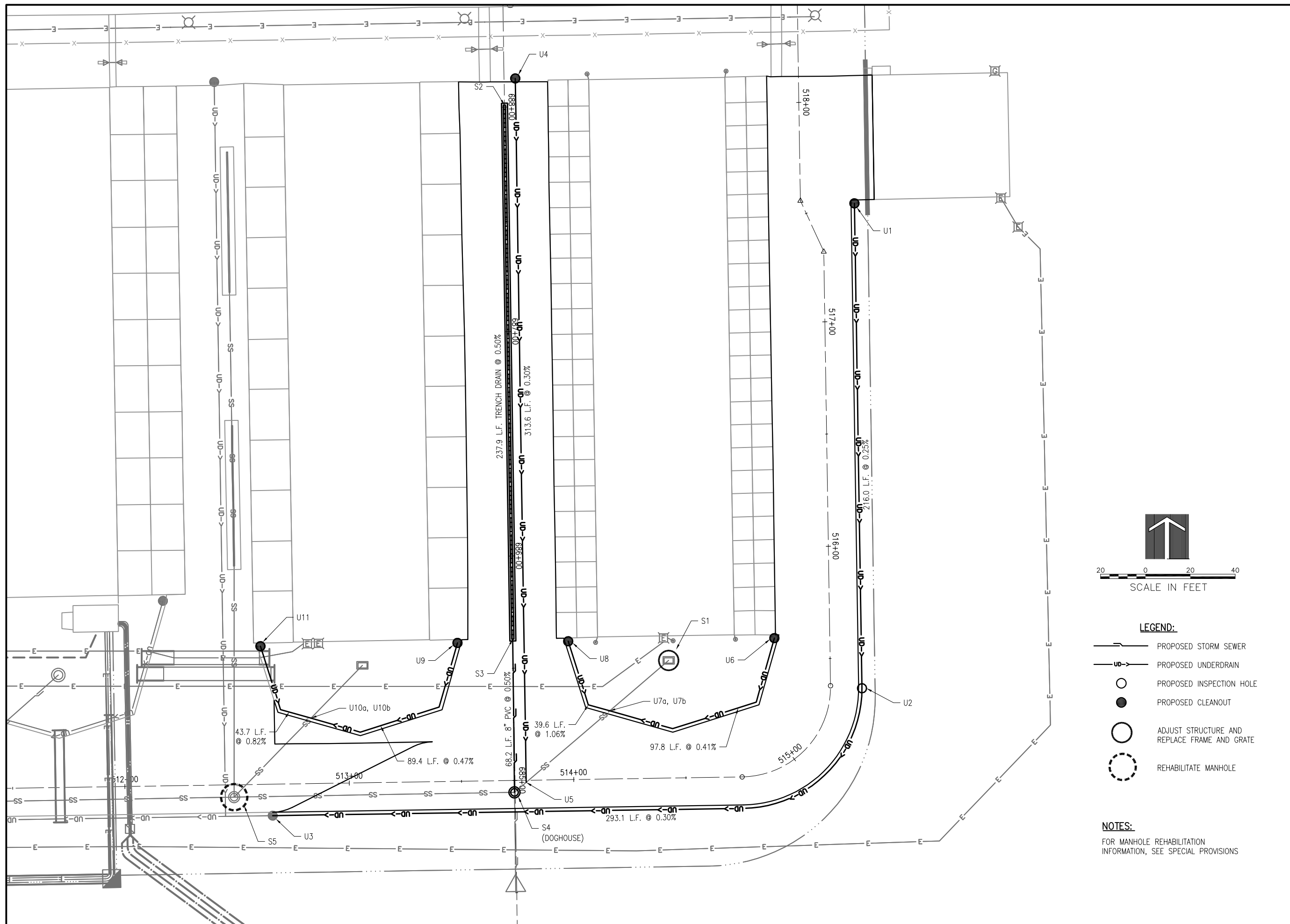


**REHABILITATE  
TAXILANES TH/7, TH/7-1  
& TH/7-2**

IDA No: JOT-4671  
SBGP No: 3-17-SBGP-144

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- LEGEND:**
- PROPOSED STORM SEWER
  - PROPOSED UNDERDRAIN
  - PROPOSED INSPECTION HOLE
  - PROPOSED CLEANOUT
  - ADJUST STRUCTURE AND REPLACE FRAME AND GRATE
  - REHABILITATE MANHOLE

**NOTES:**  
FOR MANHOLE REHABILITATION  
INFORMATION, SEE SPECIAL PROVISIONS

NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: November 16, 2018  
PROJECT NO: 18A0010  
CAD FILE: 11-DRAINAGE.DWG  
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REVIEWED BY: KMS 10/15/18

SHEET TITLE

**DRAINAGE PLAN**

### STORM SEWER SCHEDULE

Structure	Station	Offset	Type	Rim El.	Invert El.	Pipe Pay Length	Size	Type	Slope %
S2	688+00.89	0.0	Trench Drain	580.22	578.97				
						237.9	8.0	Trench Drain	0.50
S3	685+62.95	0.0	Trench Drain	580.22	577.78				
						66.2	8.0	PVC	0.50
S4	684+94.74	0.3	LT 4' Manhole	580.13	577.44				

### MANHOLE/INLET ADJUSTMENT SCHEDULE

Existing Structure	New Grate Type (Neenah/East Jordan) or Approved Equal	Station	Offset	Existing Rim Elevation	Proposed Rim Elevation
S1	R-2390-C/1040 Grate	514+43.10	52.4 LT	579.54	579.12

### STRUCTURE SCHEDULE

Structure	Structure	Diameter	Frame Height	Grate Diameter	Frame Type	Cover/Grate
Number	Type	D (in.)	(in.)	(in.)	(Neenah/East Jordan) or Approved Equal	(Neenah/East Jordan) or Approved Equal
S4	Manhole (Doghouse)	60	9	32-3/8	R-3492-A/1895	Closed/Cover

### UNDERDRAIN SCHEDULE

Structure	Station	Offset	Type	Rim El.	Invert El.	Pay Length	Slope %
U1	517+45.08	21.38	RT Cleanout	580.06	578.48		
						216.0	0.25
U2	515+36.88	14.00	RT Inspection Hole	579.76	577.94		
						293.1	0.25
U3	512+65.66	14.00	RT Meet Existing at Cleanout	--	577.21		
U4	688+12.69	5.00	RT Cleanout	580.18	578.18		
						313.6	0.30
U5	684+99.10	5.00	RT RCP Connection	--	577.24		
U6	515+59.47	24.68	LT Cleanout	580.23	577.73		
						97.8	0.41
U7a	514+16.51	30.35	LT RCP Connection	--	577.33		
U8	513+98.49	61.63	LT Cleanout	580.25	577.75		
						39.6	1.06
U7b	514+16.51	30.35	LT RCP Connection	--	577.33		
U9	513+49.14	61.65	LT Cleanout	580.17	577.67		
						89.4	0.47
U10a	512+83.38	29.15	LT RCP Connection	--	577.25		
U11	512+61.36	61.57	LT Cleanout	580.11	577.61		
						43.7	0.82
U10b	512+83.38	29.15	LT RCP Connection	--	577.25		

REHABILITATE  
TAXILANES TH/7, TH/7-1  
& TH/7-2

IDA No: JOT-4671  
SBGP No: 3-17-SBGP-144

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NO.	DATE	DESCRIPTION		
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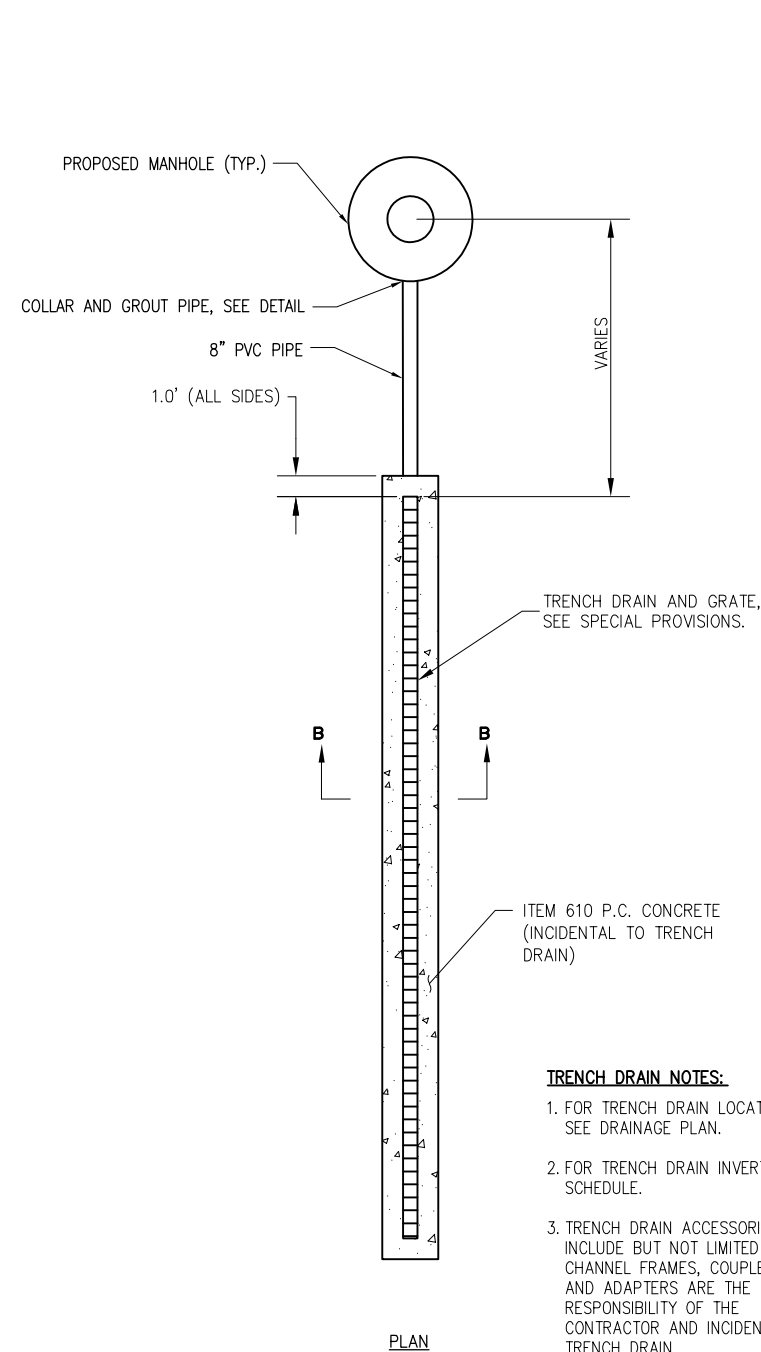
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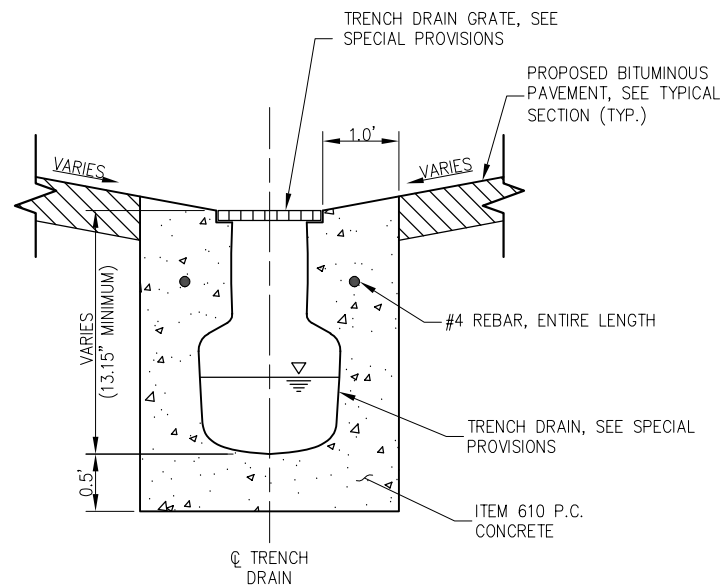
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SHEET TITLE

STORM SEWER AND  
UNDERDRAIN  
SCHEDULE

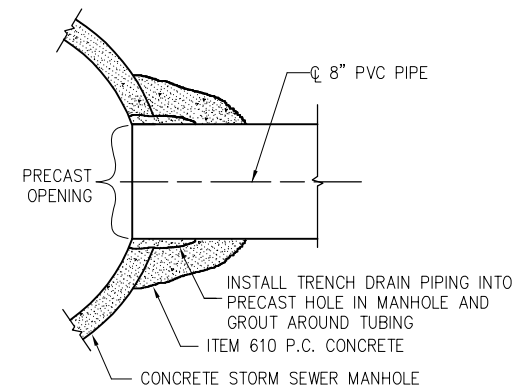


- TRENCH DRAIN NOTES:**
1. FOR TRENCH DRAIN LOCATION, SEE DRAINAGE PLAN.
  2. FOR TRENCH DRAIN INVERTS SEE SCHEDULE.
  3. TRENCH DRAIN ACCESSORIES, TO INCLUDE BUT NOT LIMITED TO, CHANNEL FRAMES, COUPLERS, AND ADAPTERS ARE THE RESPONSIBILITY OF THE CONTRACTOR AND INCIDENTAL TO TRENCH DRAIN.



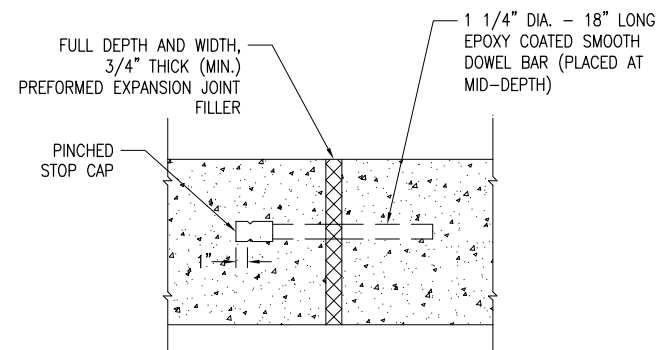
- NOTE:**  
12 INCHES P.C. CONCRETE AROUND ALL SIDES OF TRENCH DRAIN, SEE PLAN.

SECTION B-B  
**TRENCH DRAIN**



- NOTES**
1. HOLE FOR TRENCH DRAIN PIPING TO BE PRECAST INTO MANHOLE AT ELEVATION SPECIFIED IN TRENCH DRAIN SCHEDULE.
  2. CONNECTIONS INCIDENTAL TO UNDERDRAIN.

**TRENCH DRAIN CONCRETE COLLAR AND GROUT CONNECTION**



- NOTES:**
1. CONTRACTION JOINTS TO BE LOCATED AT NO GREATER THAN 12.5' ON CENTER. CONTRACTION JOINTS ARE TO BE EITHER 3/4" THICK PREFORMED JOINT FILLER, SAWED 2" DEEP AT 4 TO 24 HOURS OR FORMED WITH A 1/8" THICK STEEL TEMPLATE 2" DEEP.
  2. EXPANSION JOINTS TO BE LOCATED AT 50' MAX SPACING.
  3. ALL EXPANSION AND SAWED CONTRACTION JOINTS SHALL BE SEALED WITH HOT-POURED SEALANT, ASTM D6690, TYPE II, COST INCIDENTAL TO TRENCH DRAIN.

**EXPANSION JOINT DETAIL**

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**REHABILITATE  
TAXILANES TH/7, TH/7-1  
& TH/7-2**

IDA No: JOT-4671  
SBGP No: 3-17-SBGP-144

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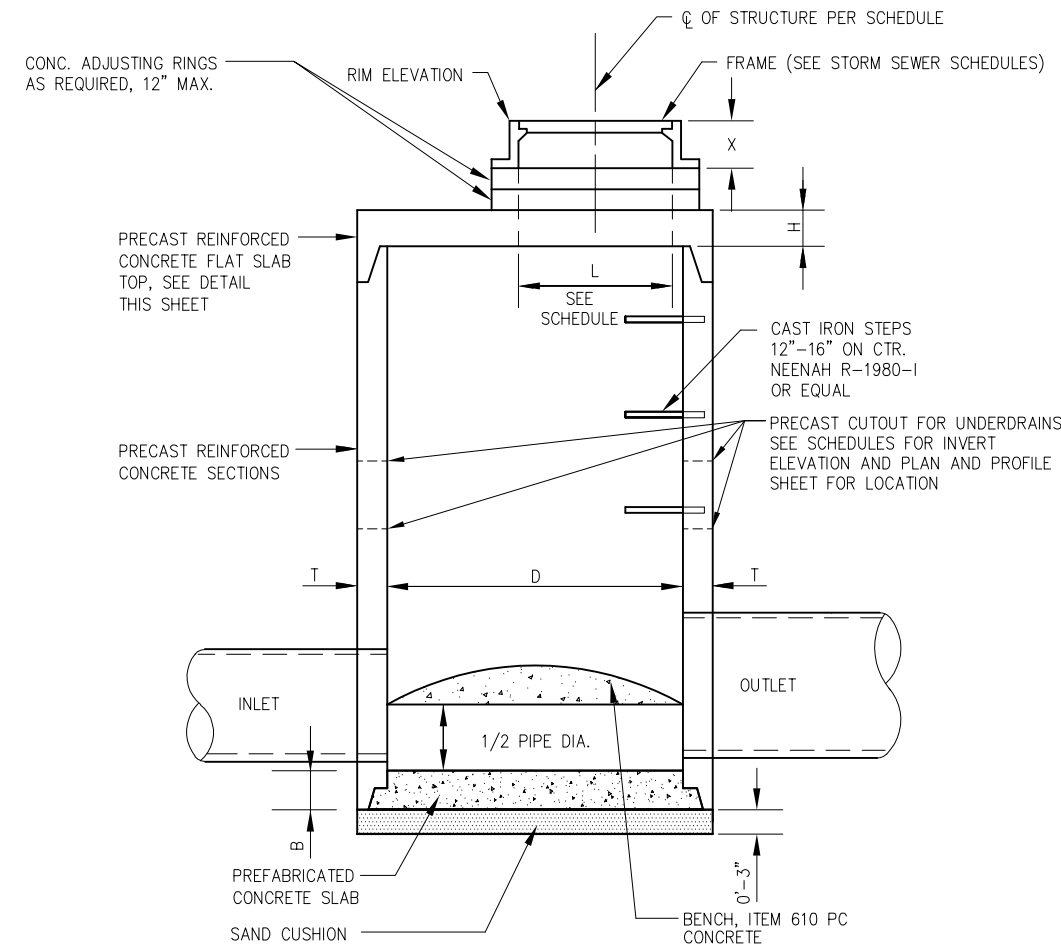
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CAD FILE: 13-DRAINAGE\DET.DWG  
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SHEET TITLE

**DRAINAGE DETAILS  
01**





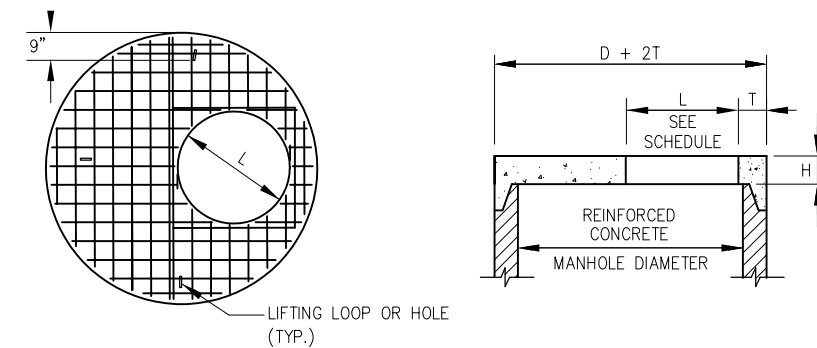
**MANHOLE DATA**

**NOTES**

- FOR "L" DIMENSION AND FRAME AND LID INFORMATION SEE STORM SEWER SCHEDULES.
- CENTER OF FRAME TO BE USED FOR LOCATING STRUCTURE. FOR STRUCTURE LOCATIONS AND ADDITIONAL INFORMATION SEE SCHEDULE.
- ALL STRUCTURES TO BE PRECAST REINFORCED CONCRETE SECTIONS; BENCHES MAY BE CAST IN PLACE.
- BLOCKOUTS FOR UNDERDRAIN CONNECTIONS AND FUTURE PIPES SHALL BE PRECAST INTO THE STRUCTURE.

INSIDE DIA. "D" (IN.)	WALL THICKNESS "T" (IN.)	TOP THICKNESS "H" (IN.)	BOTTOM THICKNESS "B" (IN.)
48	5	6	6
60	5	8	8
72	7	8	8
108	9	9	8

**MANHOLE WITH FLAT SLAB TOP**  
(IDOT STANDARD 602401-MODIFIED)



**NOTES**

- ADDITIONAL TOP AND BOTTOM BARS PLACED ADJACENT TO ACCESS HOLE.
- MINIMUM 1" COVER ON STEEL BARS.
- THREE LIFTING LOOPS OR HOLES.
- MINIMUM STEEL REINFORCEMENT IN EACH DIRECTION TO BE WWF 1.06 SQ. IN./FT. IN ACCORDANCE WITH AASHTO M199 AND IDOT STANDARDS.
- FOR "L" DIMENSION SEE STORM SEWER SCHEDULES.

**PRECAST REINFORCED CONCRETE FLAT SLAB TOP**  
(IDOT STANDARD 602601)

REHABILITATE  
TAXILANES TH/7, TH/7-1  
& TH/7-2

IDA No: JOT-4671  
SBGP No: 3-17-SBGP-144

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NO.	DATE	DESCRIPTION		
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REVIEWED BY: KMS 10/15/18

SHEET TITLE

DRAINAGE DEATILS  
02

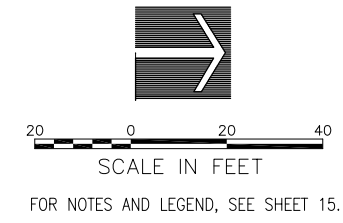
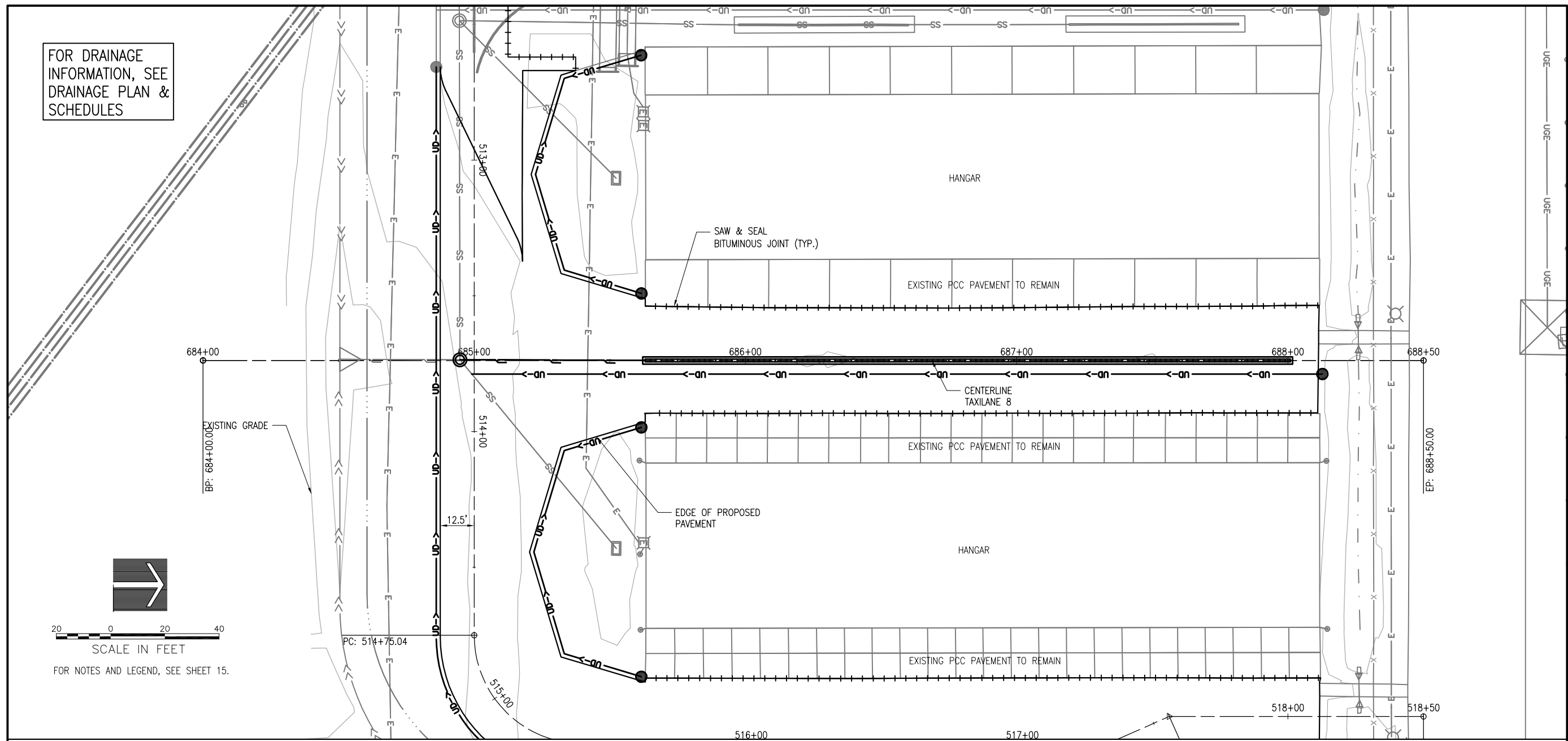








FOR DRAINAGE  
INFORMATION, SEE  
DRAINAGE PLAN &  
SCHEDULES



**REHABILITATE  
TAXILANES TH/7, TH/7-1  
& TH/7-2**

IDA No: JOT-4671  
SBGP No: 3-17-SBGP-144

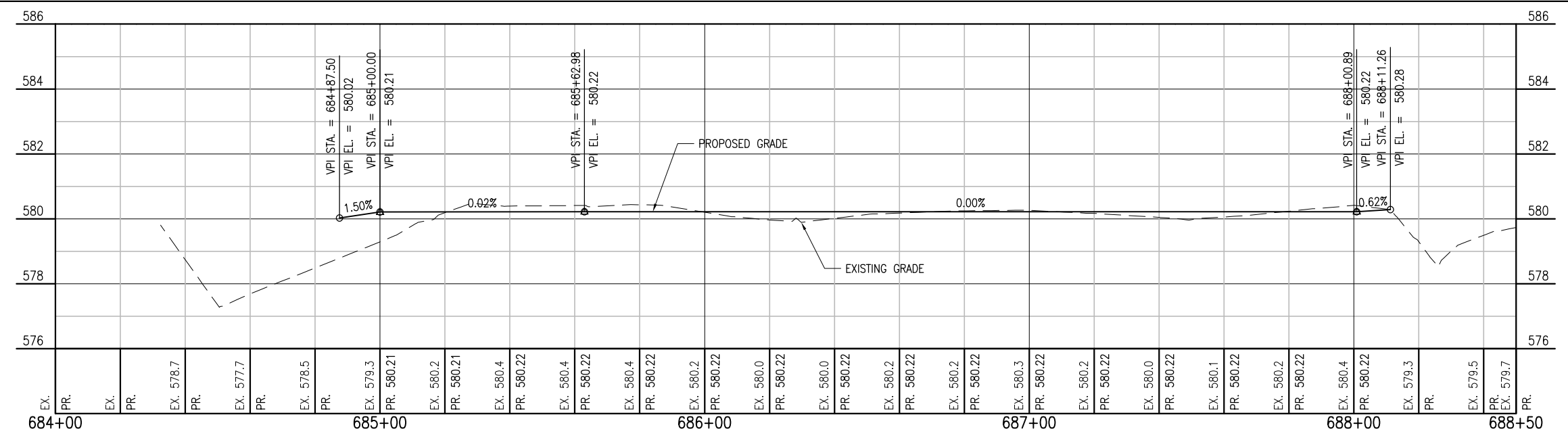
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CAD FILE: 17-P&P.DWG  
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SHEET TITLE

**PLAN & PROFILE -  
TAXILANE 8**



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REHABILITATE  
TAXILANES TH/7, TH/7-1  
& TH/7-2

IDA No: JOT-4671  
SBGP No: 3-17-SBGP-144

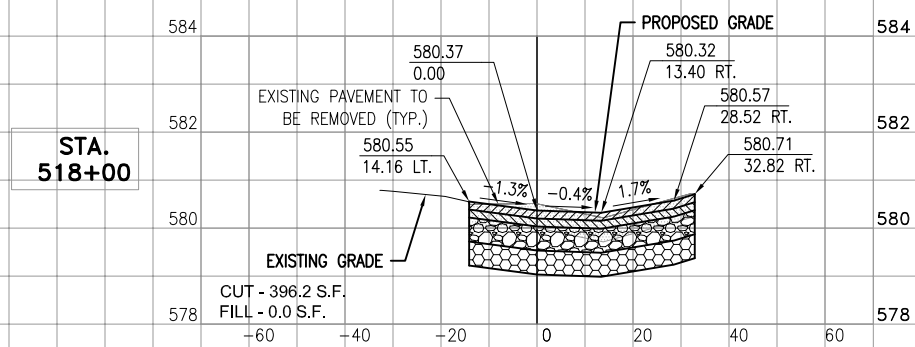
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NO.	DATE	DESCRIPTION		
		DES	DWN	REV

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PROJECT NO: 18A0010  
CAD FILE: 20-SECTIONS.DWG  
DESIGN BY: LDH 10/1/18  
DRAWN BY: LDH 10/1/18  
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SHEET TITLE

CROSS SECTIONS -  
EAST WEST TAXIWAY







**REHABILITATE  
TAXILANES TH/7, TH/7-1  
& TH/7-2**

IDA No: JOT-4671  
SBGP No: 3-17-SBGP-144

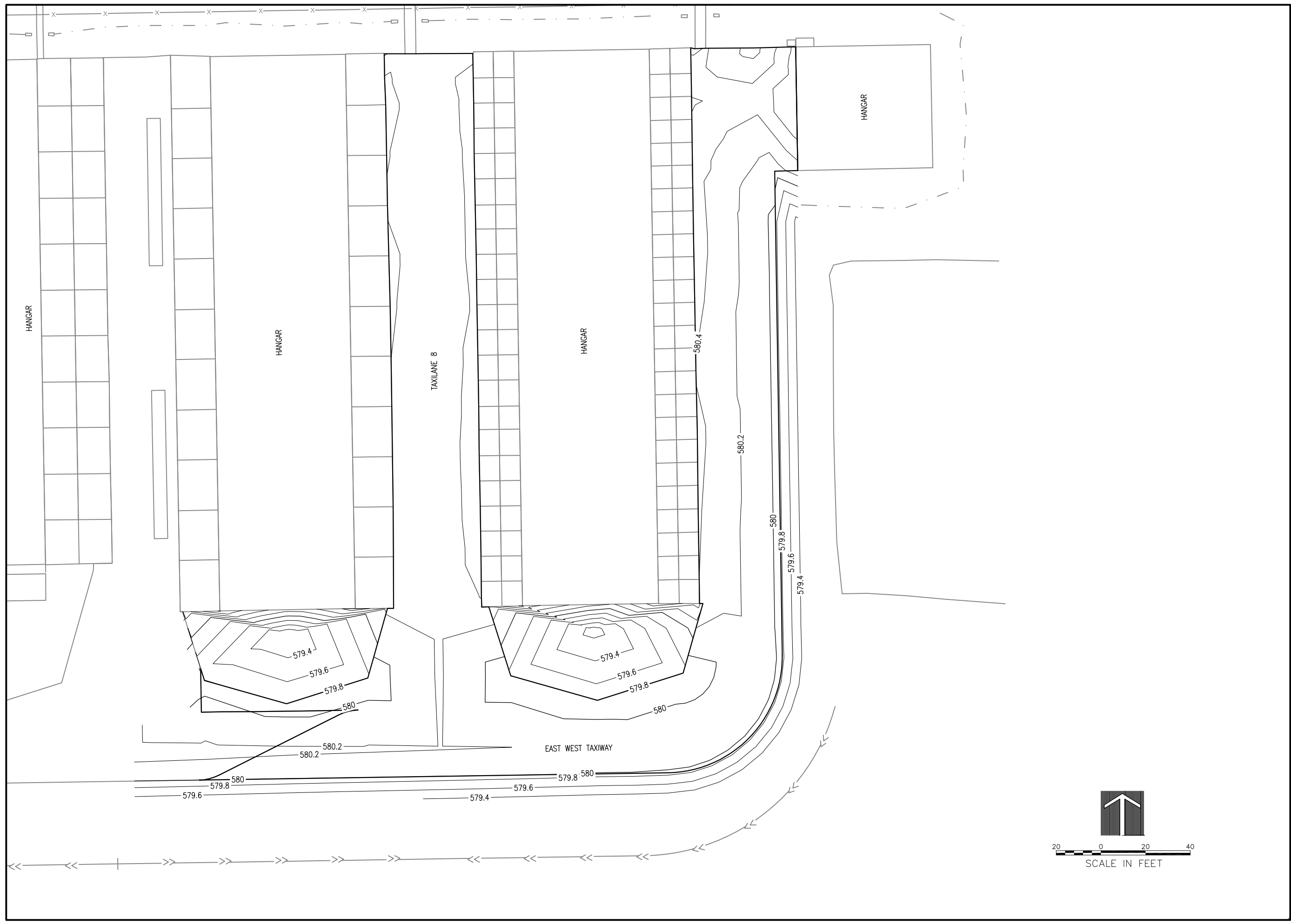
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NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: November 16, 2018  
PROJECT NO: 18A0010  
CAD FILE: 22-GRADING.DWG  
DESIGN BY: LDH 10/1/18  
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SHEET TITLE

**GRADING PLAN**



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**REHABILITATE  
TAXILANES TH/7, TH/7-1  
& TH/7-2**

IDA No: JOT-4671  
SBGP No: 3-17-SBGP-144

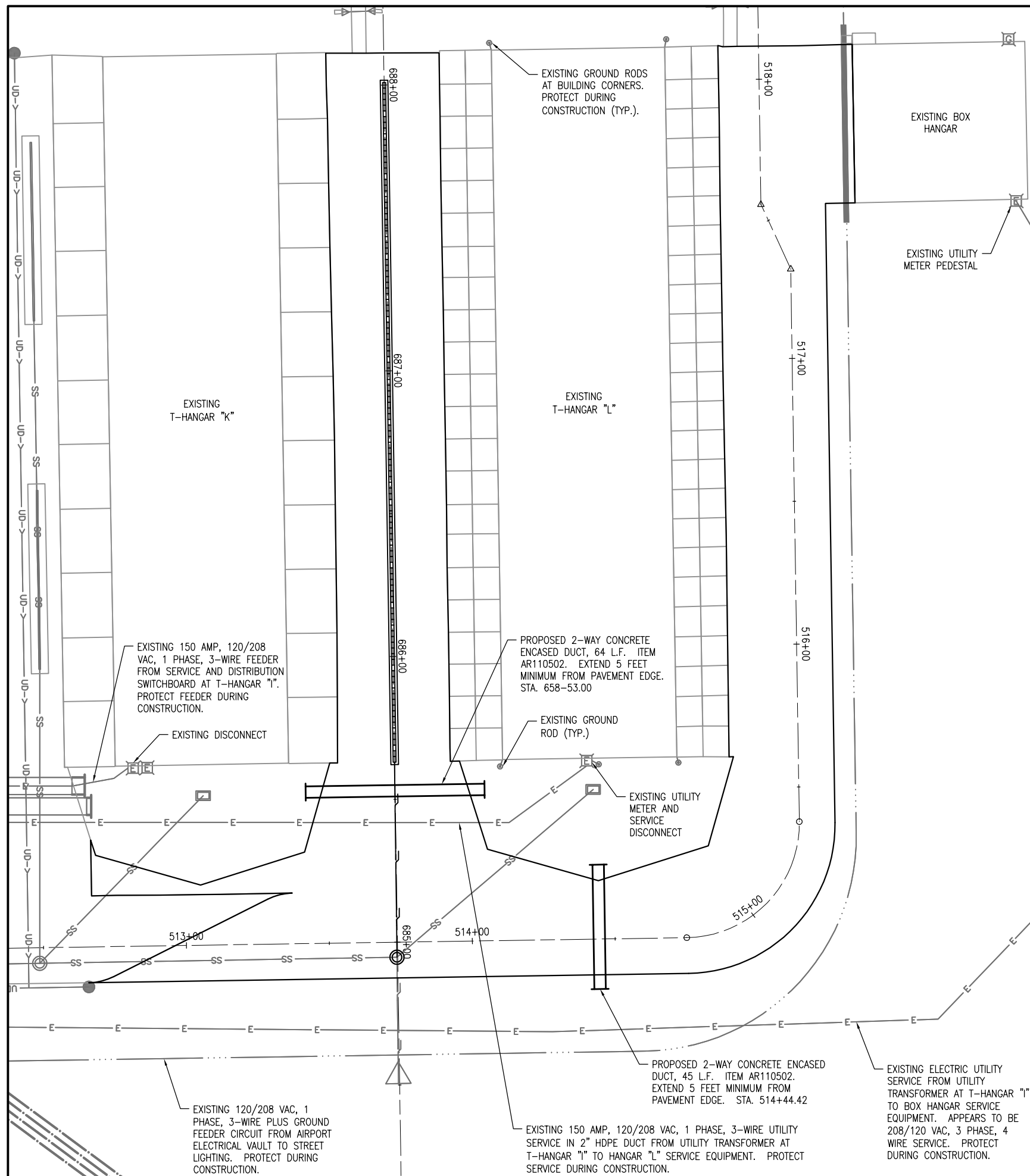
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NO.	DATE	DESCRIPTION		
		DES	DWN	REV

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PROJECT NO: 18A0010  
CAD FILE: 23-ELECPLAN.DWG  
DESIGN BY: LDH 9/22/18  
DRAWN BY: LDH 9/22/18  
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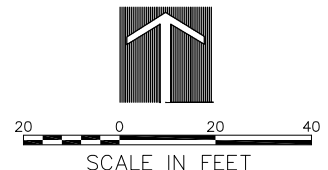
SHEET TITLE

**PROPOSED  
ELECTRICAL PLAN**



**NOTES**

1. KEEP ALL WORK, POWER OUTAGES, AND/OR SHUT DOWN OF EXISTING SYSTEMS COORDINATED WITH THE AIRPORT MANAGER OR DESIGNATED REPRESENTATIVE. ONCE SHUT DOWN, THE CIRCUITS SHALL BE LABELED AS SUCH TO PREVENT ACCIDENTAL ENERGIZING OF THE RESPECTIVE CIRCUITS. ALL PERSONNEL SHALL FOLLOW U.S. DEPARTMENT OF LABOR OCCUPATIONAL SAFETY & HEALTH ADMINISTRATION (OSHA) 29 CFR PART 1910 OCCUPATIONAL SAFETY & HEALTH STANDARDS FOR ELECTRICAL SAFETY AND LOCKOUT/TAGOUT PROCEDURES INCLUDING, BUT NOT LIMITED TO 29 CFR SECTION 1910.147 THE CONTROL OF HAZARDOUS ENERGY (LOCKOUT/TAGOUT).
2. VERIFY EXISTING SITE CONDITIONS. CONTRACTOR SHALL FIELD VERIFY RESPECTIVE CIRCUITS AND POWER SOURCES PRIOR TO REMOVING, DISCONNECTING, ADJUSTING, CONNECTING, OR WORKING ON THE RESPECTIVE FACILITY, EQUIPMENT, OR OTHER DEVICE. CONTRACTOR SHALL LOCATE AND PROTECT EXISTING SERVICES, FEEDERS, LIGHTING CIRCUITS, ELECTRIC LINES, GROUNDING AND OTHER UTILITY LINES AND FACILITIES.
3. IN AREAS WHERE THERE IS A CONGESTION OF CABLES OR WHERE THE PROPOSED DUCT OR CABLE CROSSES AN EXISTING CABLE, DUCT, LINE, OR OTHER UTILITY THE CONTRACTOR IS REQUIRED TO HAND DIG THE TRENCH NECESSARY FOR THE PROPOSED INSTALLATION. HAND DIGGING, TRENCHING AND/OR PLOWING WILL BE CONSIDERED INCIDENTAL TO THE PROPOSED WORK AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
4. THE CONTRACTOR IS REQUIRED TO FILL IN ALL HOLES AND DEPRESSIONS RESULTING FROM THE NEW WORK, WITH EARTH MATERIAL. THE AREAS SHALL BE COMPACTED TO PREVENT FUTURE SETTLEMENT AND TOPSOILED, SEEDED AND MULCHED IN ACCORDANCE WITH ITEMS 901, 905 AND 908.
5. NO CONNECTION TO AN ACTIVE POWER CIRCUIT WILL BE BROKEN UNTIL THE CIRCUIT HAS BEEN TURNED OFF IN ACCORDANCE WITH NOTE 1.



**PROPOSED LEGEND**

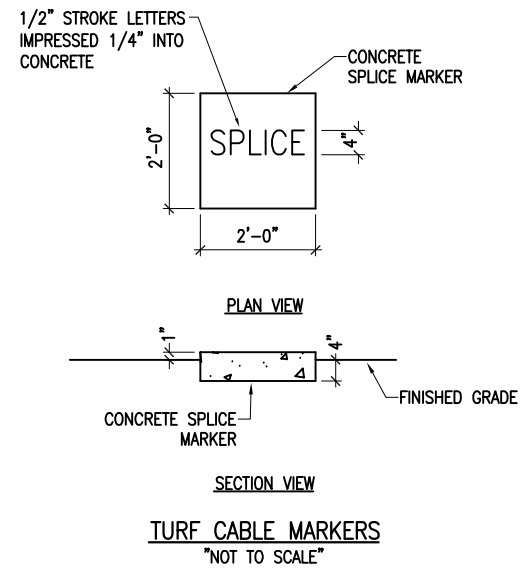
- EXISTING ELECTRIC CABLES IN DUCT OR UNIT DUCT
- ▬ PROPOSED CONCRETE ENCASED DUCT
- EXISTING DIRECTIONAL BORE OR DIRECT BURY DUCT
- E— EXISTING ELECTRICAL

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE AIRPORT OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE AIRPORT MANAGER OR DESIGNATED REPRESENTATIVE AND/OR THE RESIDENT ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

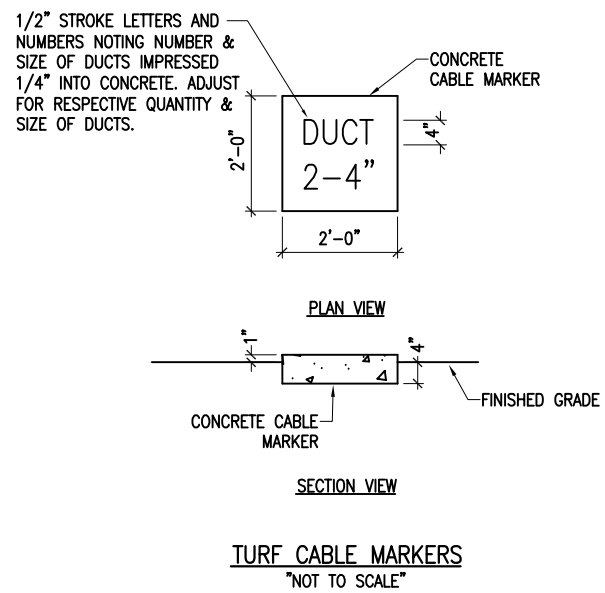
ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY. CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123. CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. LOCATION OF FAA POWER CONTROL, AND COMMUNICATION CABLES SHALL BE COORDINATED WITH AND/OR LOCATED BY THE FAA. CONTACT AIRPORT MANAGER OR DESIGNATED REPRESENTATIVE FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVE GROUND UTILITIES.

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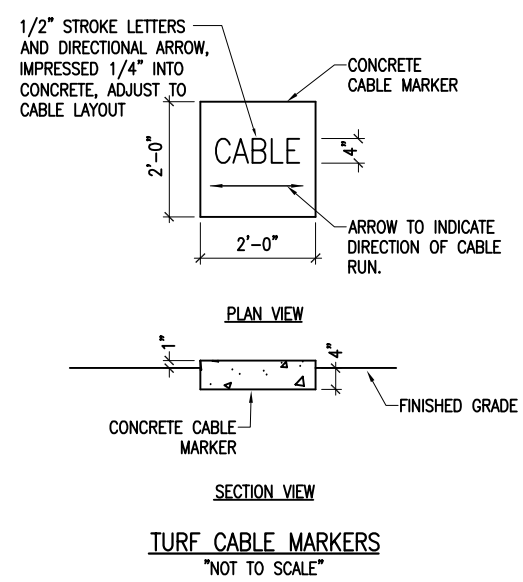




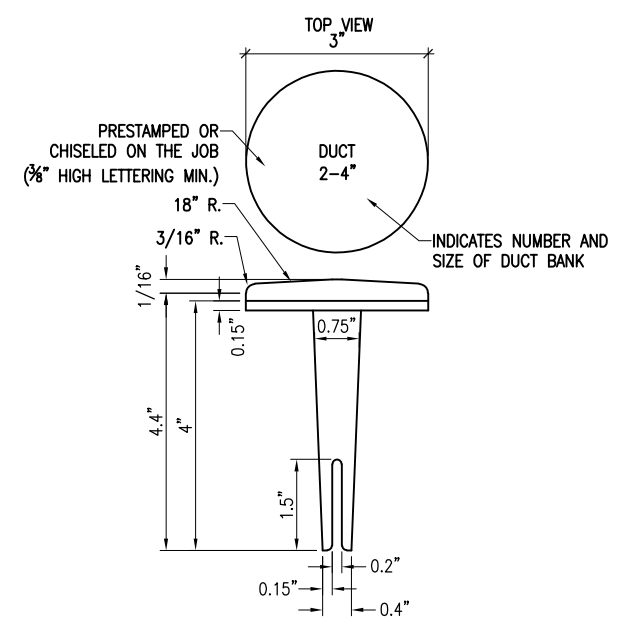
**TURF CABLE MARKERS**  
"NOT TO SCALE"



**TURF CABLE MARKERS**  
"NOT TO SCALE"

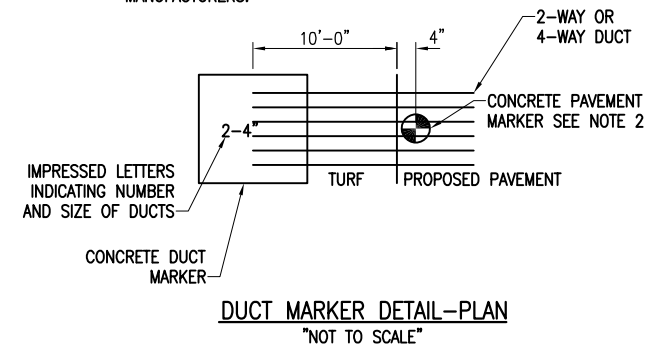


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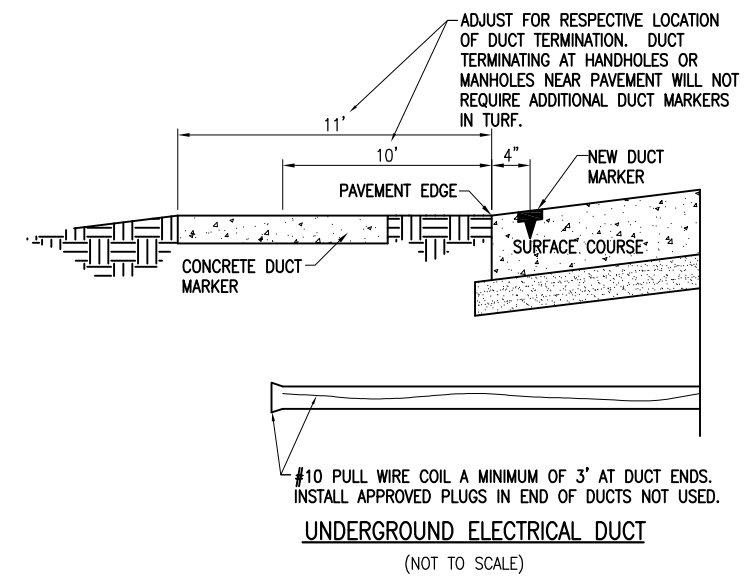


**BITUMINOUS PAVEMENT DUCT MARKERS**  
"NOT TO SCALE"

- NOTE:**
1. TOP OF MARKER SHALL BE FLUSH WITH FINISHED PAVEMENT SURFACE. MARKER MAY BE INSTALLED IN A DRILLED HOLE AND SECURED WITH EPOXY GLUE.
  2. BRASS DUCT MARKERS ARE AVAILABLE FROM G&S FOUNDRY & MANUFACTURING CO. INC., 210 KASKASKIA DRIVE, RED BUD, IL 62278, PHONE: (618)-282-4114, SURV-KAP, 3225 E. 47TH ST., TUCSON, AZ 85713, PHONE: (502)-622-6011, OR OTHER EQUIVALENT MANUFACTURERS.



**DUCT MARKER DETAIL-PLAN**  
"NOT TO SCALE"



**UNDERGROUND ELECTRICAL DUCT**  
(NOT TO SCALE)

**CABLE & DUCT MARKER NOTES:**

1. THE COST OF ALL TURF AND PAVEMENT DUCT MARKERS SHALL BE INCIDENTAL TO THE DUCT. THE COST OF ALL CABLE MARKERS SHALL BE INCIDENTAL TO THE CABLE.
2. BITUMINOUS PAVEMENT DUCT MARKER AND CONCRETE DUCT MARKER TO BE PROVIDED AT EACH END OF EACH DUCT AS SHOWN ON THE LOCATION PLAN. FOR CONCRETE PAVEMENT, THE LETTER "D" SHALL BE IMPRESSED IN THE PAVEMENT INSTEAD OF THE MARKER. THE LETTER SHALL BE INFORMED AS DESCRIBED IN NOTE 4.
3. UNDERGROUND CABLE RUNS MUST BE IDENTIFIED BY CABLE MARKERS AT 200 FEET (61 M) MAXIMUM SPACING WITH AN ADDITIONAL MARKER AT EACH CHANGE OF DIRECTION OF THE CABLE RUN. CABLE MARKERS MUST BE INSTALLED ABOVE THE CABLE. CABLE MARKERS ARE NOT REQUIRED FOR CABLE RUNS BETWEEN RUNWAY/TAXIWAY EDGE LIGHTS.
4. CONCRETE CABLE MARKERS AND DUCT MARKERS SHALL HAVE LETTERS 4" HIGH, 3" WIDE WITH WIDTH OF STROKE 1/2" AND 1/4" DEEP. ALL LETTERS, NUMBERS AND ARROWS TO BE IMPRESSED.
5. EMPLOY THE FOLLOWING METHODS WHERE ADDITIONAL SPACE TO FIT THE LEGEND IS REQUIRED:
  - A. REDUCE LETTER SIZE TO 3" HIGH, 2" WIDE.
  - B. INCREASE THE MARKER SIZE TO 30" X 30".
  - C. PROVIDE ADDITIONAL MARKERS PLACED SIDE BY SIDE.
6. TURF DUCT MARKERS ARE NOT REQUIRED AT PAVEMENT CROSSINGS WHERE DUCTS TERMINATE IN HANDHOLES, OR JUNCTION STRUCTURES.
7. LOCATION OF ALL DIRECT EARTH BURIAL UNDERGROUND CABLE SPLICE/CONNECTIONS, EXCEPT THOSE AT ISOLATION TRANSFORMERS, MUST BE IDENTIFIED BY SPLICE MARKERS. SPLICE MARKERS MUST BE PLACED ABOVE THE SPLICE/CONNECTIONS. DIRECT EARTH BURIAL UNDERGROUND CABLE SPLICES SHALL BE AVOIDED WHERE POSSIBLE. CABLE SPLICES SHALL BE LOCATED IN SPLICE CANS, LIGHT BASES, HANDHOLES, MANHOLES, OR OTHER JUNCTION STRUCTURES UNLESS OTHERWISE APPROVED BY THE PROJECT ENGINEER.
8. THE CABLE AND SPLICE MARKERS MUST IDENTIFY THE CIRCUITS TO WHICH THE CABLES BELONG. FOR EXAMPLE: RWY 13-31, TAXIWAY, T-HGR K, T-HGR L.
9. LOCATIONS OF ENDS OF ALL UNDERGROUND DUCTS MUST BE IDENTIFIED BY DUCT MARKERS.

**REHABILITATE TAXILANES TH/7, TH/7-1 & TH/7-2**

IDA No: JOT-4671  
SBGP No: 3-17-SBGP-144

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NO.	DATE	DESCRIPTION		
		DES	DWN	REV

ISSUE: November 16, 2018  
PROJECT NO: 18A0010  
CAD FILE: 25-E-502-ELEC.DWG  
DESIGN BY: KNL 9/27/18  
DRAWN BY: LDH 9/28/18  
REVIEWED BY: KMS 10/15/18

SHEET TITLE

**CABLE AND DUCT MARKER DETAILS**

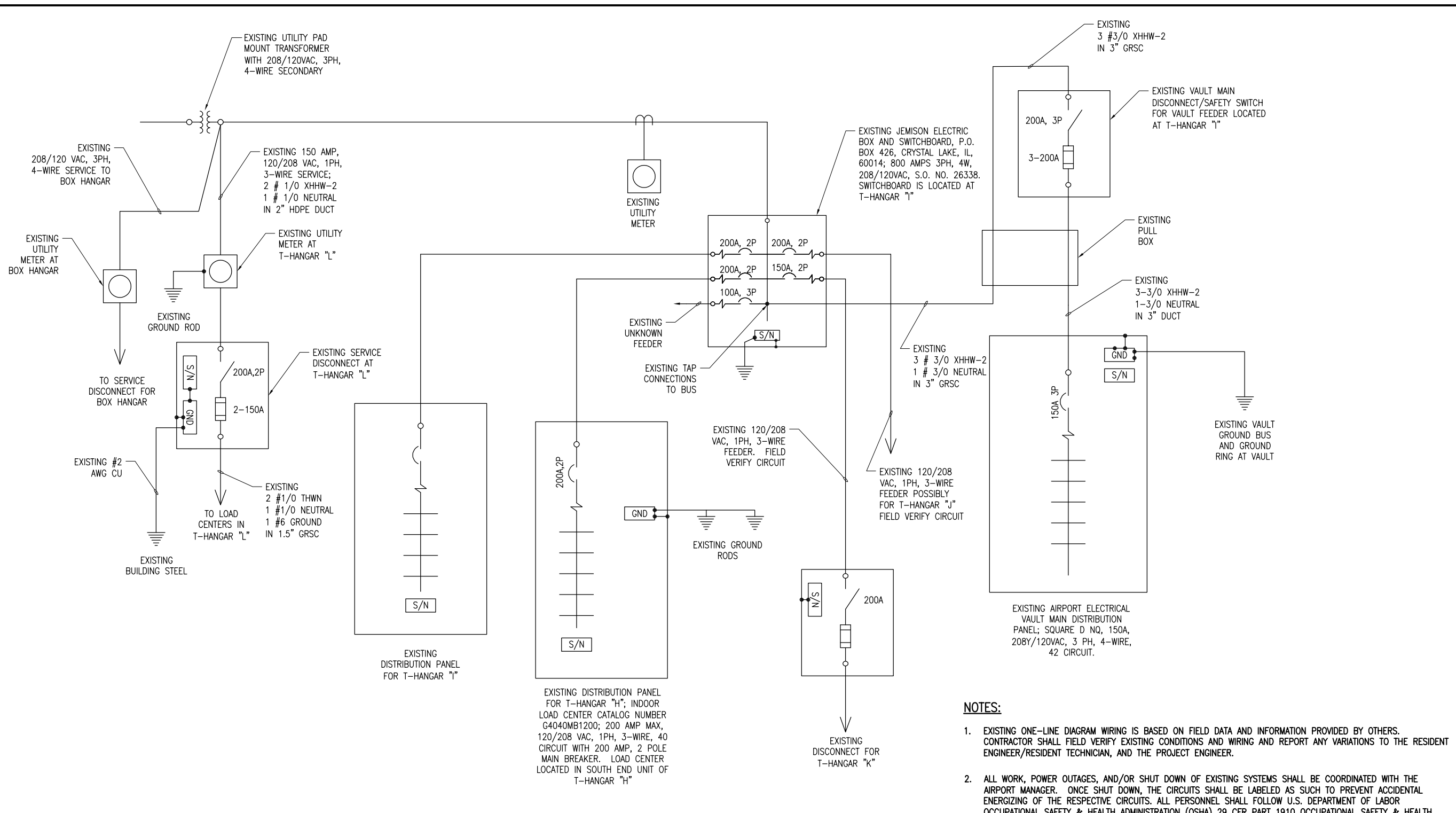


NO.	DATE	DESCRIPTION		
		DES	DWN	REV

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DESIGN BY: KNL 9/27/18  
DRAWN BY: LDH 9/28/18  
REVIEWED BY: KMS 10/15/18

SHEET TITLE

EXISTING  
ELECTRICAL  
ONE-LINE DIAGRAM  
FOR T-HANGARS AND  
VAULT



EXISTING ELECTRICAL ONE-LINE DIAGRAM FOR T-HANGARS AND VAULT

**NOTES:**

- EXISTING ONE-LINE DIAGRAM WIRING IS BASED ON FIELD DATA AND INFORMATION PROVIDED BY OTHERS. CONTRACTOR SHALL FIELD VERIFY EXISTING CONDITIONS AND WIRING AND REPORT ANY VARIATIONS TO THE RESIDENT ENGINEER/RESIDENT TECHNICIAN, AND THE PROJECT ENGINEER.
- ALL WORK, POWER OUTAGES, AND/OR SHUT DOWN OF EXISTING SYSTEMS SHALL BE COORDINATED WITH THE AIRPORT MANAGER. ONCE SHUT DOWN, THE CIRCUITS SHALL BE LABELED AS SUCH TO PREVENT ACCIDENTAL ENERGIZING OF THE RESPECTIVE CIRCUITS. ALL PERSONNEL SHALL FOLLOW U.S. DEPARTMENT OF LABOR OCCUPATIONAL SAFETY & HEALTH ADMINISTRATION (OSHA) 29 CFR PART 1910 OCCUPATIONAL SAFETY & HEALTH STANDARDS FOR ELECTRICAL SAFETY AND LOCKOUT/TAGOUT PROCEDURES INCLUDING, BUT NOT LIMITED TO, 29 CFR SECTION 1910.147 THE CONTROL OF HAZARDOUS ENERGY (LOCKOUT/TAGOUT).
- CONTRACTOR SHALL EXAMINE THE SITE TO DETERMINE THE EXTENT OF THE WORK. CONTRACTOR SHALL FIELD VERIFY EXISTING SITE CONDITIONS. CONTRACTOR SHALL FIELD VERIFY RESPECTIVE CIRCUITS AND POWER SOURCES PRIOR TO ADJUSTING, REMOVING, RELOCATING, DISCONNECTING, CONNECTING, WORKING ON OR WORKING AROUND THE RESPECTIVE FACILITY, EQUIPMENT, OR OTHER DEVICE. EXISTING CIRCUITS SHALL BE LOCATED AND PROTECTED FOR THE DURATION OF THE PROJECT.
- CONTRACTOR SHALL COMPLY WITH THE REQUIREMENTS OF FAA AC NO. 150/5370-2G (OR MOST CURRENT ISSUE) "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION".
- CONTRACTOR SHALL COMPLY WITH THE APPLICABLE REQUIREMENTS OF NFPA 70E - STANDARD FOR ELECTRICAL SAFETY IN THE WORKPLACE.
- NOTE SOME OF THE T-HANGAR BUILDINGS HAVE APPARENT (NATIONAL ELECTRICAL CODE) VIOLATIONS WHICH MIGHT CAUSE UNSAFE WORKING CONDITIONS. APPARENT NEC VIOLATIONS INCLUDE, BUT ARE NOT LIMITED TO, SERVICE BREAKERS NOT IDENTIFIED, MAIN DISCONNECTS FOR RESPECTIVE HANGAR NOT IDENTIFIED, POWER SOURCES NOT IDENTIFIED, VOLTAGE SYSTEMS NOT IDENTIFIED, MAX AVAILABLE FAULT CURRENT NOT IDENTIFIED, AND SOME FACILITIES MIGHT NOT HAVE GROUNDING ELECTRODE CONNECTIONS TO THE BUILDING DISCONNECTING MEANS. CONTRACTOR WILL NEED TO FIELD VERIFY EXISTING CONDITIONS AND EXERCISE CAUTION AT THIS FACILITY.



**REHABILITATE  
TAXILANES TH/7, TH/7-1  
& TH/7-2**

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SBGP No: 3-17-SBGP-144

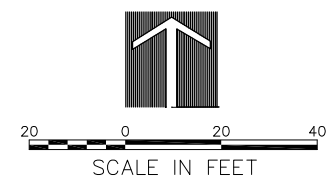
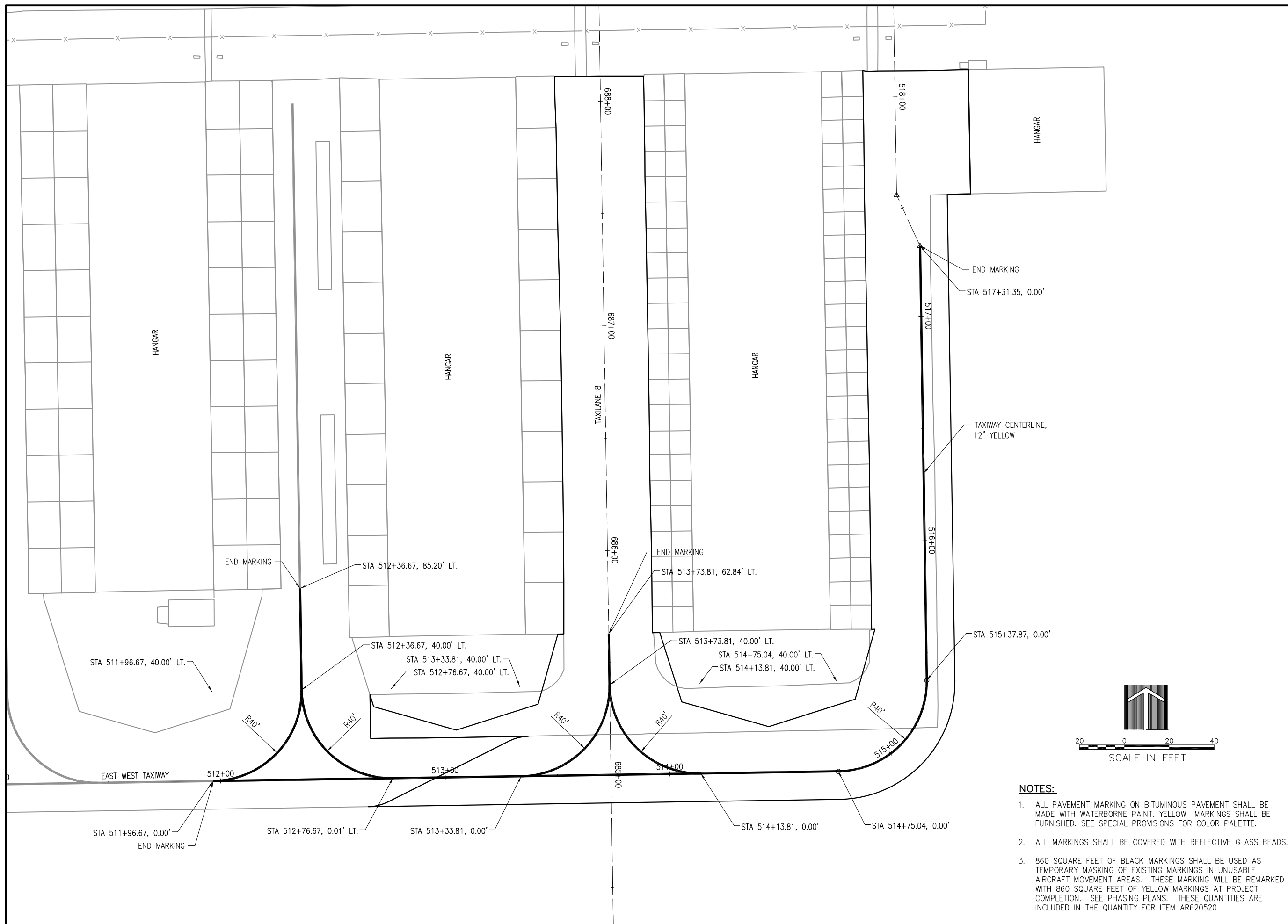
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SHEET TITLE

**MARKING PLAN**



- NOTES:**
1. ALL PAVEMENT MARKING ON BITUMINOUS PAVEMENT SHALL BE MADE WITH WATERBORNE PAINT. YELLOW MARKINGS SHALL BE FURNISHED. SEE SPECIAL PROVISIONS FOR COLOR PALETTE.
  2. ALL MARKINGS SHALL BE COVERED WITH REFLECTIVE GLASS BEADS.
  3. 860 SQUARE FEET OF BLACK MARKINGS SHALL BE USED AS TEMPORARY MASKING OF EXISTING MARKINGS IN UNUSABLE AIRCRAFT MOVEMENT AREAS. THESE MARKING WILL BE REMARKED WITH 860 SQUARE FEET OF YELLOW MARKINGS AT PROJECT COMPLETION. SEE PHASING PLANS. THESE QUANTITIES ARE INCLUDED IN THE QUANTITY FOR ITEM AR620520.

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**REHABILITATE  
TAXILANES TH/7, TH/7-1  
& TH/7-2**

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SHEET TITLE

**LANDSCAPING PLAN**

