CONSTRUCTION PLANS

REHABILITATE TAXILANES DESIGNATED TH/7, TH/7-1 & TH/7-2

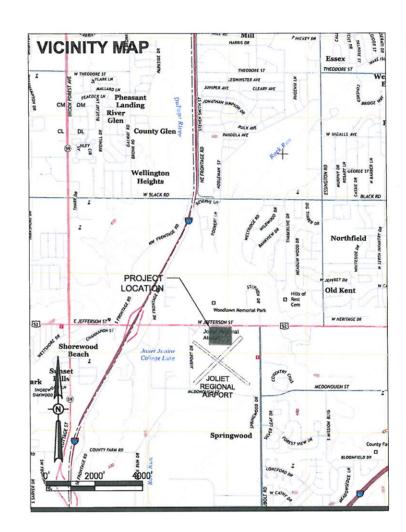
JOLIET PARK DISTRICT
JOLIET REGIONAL AIRPORT (JOT)
JOLIET, WILL COUNTY, ILLINOIS

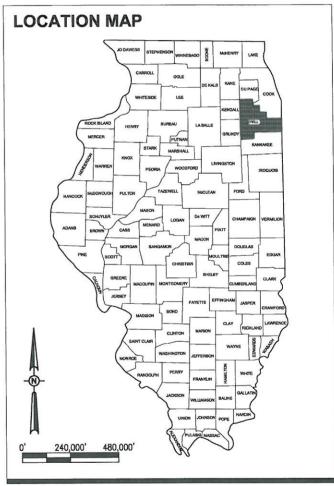
IDA PROJECT NO. JOT-4671 SBGP PROJECT NO. 3-17-SBGP-144

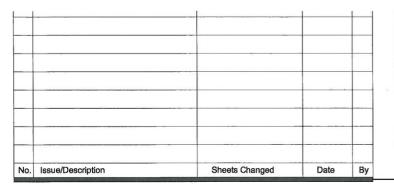
NOVEMBER 16, 2018

NOTICE TO CONTRACTORS AND BIDDERS

THESE CONSTRUCTION PLANS RELY UPON THE SPECIAL PROVISIONS AND THE SPECIFICATIONS TO PROVIDE FOR A COMPLETE DESCRIPTION OF THE WORK AND CONSTRUCTION REQUIREMENTS. THE PLANS SHALL ONLY BE USED IN COMBINATION WITH ALL CONTRACT DOCUMENTS.











JOLIET REGIONAL AIRPORT
JOLIET PARK DISTRICT
4000 W. Jefferson Street
Joliet, Illinois, 60431
Telephone: 815.741.7267

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| ITEM NO. | DESCRIPTION | F QUANTITIES UNIT | AS-BID QUANTITY | RECORD PAID |
|----------|-----------------------------------|----------------------|--------------------|-------------|
| AR110502 | 2-WAY CONCRETE ENCASED DUCT | LINEAR FOOT | | RECORD PAIL |
| | | | 109.00 | |
| AR110900 | REMOVE DUCT | LINEAR FOOT | 48.00 | |
| AR150510 | ENGINEER'S FIELD OFFICE | LUMP SUM | 1.00 | |
| AR150520 | MOBILIZATION | LUMP SUM | 1.00 | |
| AR150530 | TRAFFIC MAINTENANCE | LUMP SUM | 1.00 | |
| AR152410 | UNCLASSIFIED EXCAVATION | CUBIC YARD | 1,342.00 | |
| AR152540 | SOIL STABILIZATION FABRIC | SQUARE YARD | 3,510.00 | |
| AR154606 | GRANULAR DRAINAGE SUBBASE - 6" | SQUARE YARD | 3,510.00 | |
| AR156510 | SILT FENCE | LINEAR FOOT | 540.00 | |
| AR156520 | INLET PROTECTION | EACH | 2.00 | |
| AR209606 | CRUSHED AGG. BASE COURSE - 6" | SQUARE YARD | 3,510.00 | |
| AR401613 | BIT. SURF. CSEMETHOD I, SUPERPAVE | TON | 448.00 | |
| AR401650 | BITUMINOUS PAVEMENT MILLING | SQUARE YARD | 215.00 | |
| AR401660 | SAW & SEAL BIT. JOINTS | LINEAR FOOT | 826.00 | |
| AR401665 | BITUMINOUS PAVEMENT SAWING | LINEAR FOOT | 926.00 | |
| AR401900 | REMOVE BITUMINOUS PAVEMENT | SQUARE YARD | 2,759.00 | |
| AR403613 | BIT. BASE CSEMETHOD I, SUPERPAVE | TON | 421.00 | |
| AR602510 | BITUMINOUS PRIME COAT | GALLONS | 1,028.00 | |
| AR603510 | BITUMINOUS TACK COAT | GALLONS | 514.00 | |
| AR620520 | PAVEMENT MARKING-WATERBORNE | SQUARE FOOT | 855.00 | |
| AR620525 | PAVEMENT MARKING-BLACK BORDER | SQAURE FOOT | 855.00 | |
| AR701008 | 8" PVC STORM SEWER | LINEAR FOOT | 67.00 | |
| AR701900 | REMOVE PIPE | LINEAR FOOT | 256.00 | |
| AR705506 | 6" PERFORATED UNDERDRAIN | LINEAR FOOT | 1,094.00 | |
| AR705630 | UNDERDRAIN INSPECTION HOLE | EACH | 1.00 | |
| AR705640 | UNDERDRAIN CLEANOUT | EACH | 6.00 | |
| AR751001 | TRENCH DRAIN | LINEAR FOOT | 238.00 | |
| AR751550 | MANHOLE 5' | EACH | 1.00 | |
| AR751900 | REMOVE INLET | EACH | 2.00 | |
| AR751903 | REMOVE MANHOLE | EACH | 1.00 | |
| AR751927 | REPLACE FRAME & GRATE | EACH | 1.00 | |
| AR751940 | ADJUST INLET | EACH | 1.00 | |
| AR751993 | REHABILITATE MANHOLE | EACH | 1.00 | |
| AR901510 | SEEDING | ACRE | 0.44 | |
| AR905510 | TOPSOILING (FROM ON SITE) | CUBIC YARD | 236.00 | |
| AR908510 | MULCHING | ACRE | 0.44 | |

PAYMENT WILL BE MADE UNDER THE ITEM NUMBERS, DESCRIPTIONS AND UNITS NOTED IN THE ABOVE TABLE IN ACCORDANCE WITH THE BASIS OF PAYMENT FOR EACH RESPECTIVE WORK ITEM NOTED IN THE SPECIAL PROVISIONS, COMPLETED AND ACCEPTED BY THE ENGINEER.

HANSON Engineering | Planning | Allied Services

Offices Nationwide www.hanson-inc.com

Hanson Professional Services Inc. 1525 S. Sixth Street Springfield, IL 62703 phone: 217.788.2450 fax: 217.788.2503

Illinois Licensed Professional Service Corporation #184-001084

JOLIET REGIONAL AIRPORT JOLIET PARK DISTRICT 4000 W. Jefferson Street Joliet, Illinois 60431 phone: 815.741.7267

REHABILITATE TAXILANES TH/7, TH/7-1 & TH/7-2

IDA No: JOT-4671 SBGP No: 3-17-SBGP-144

JO024

| NO. | DATE | DES | CRIPT | ION |
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PROJECT NO: 18A0010
CAD FILE: 02-SOQ.DWG

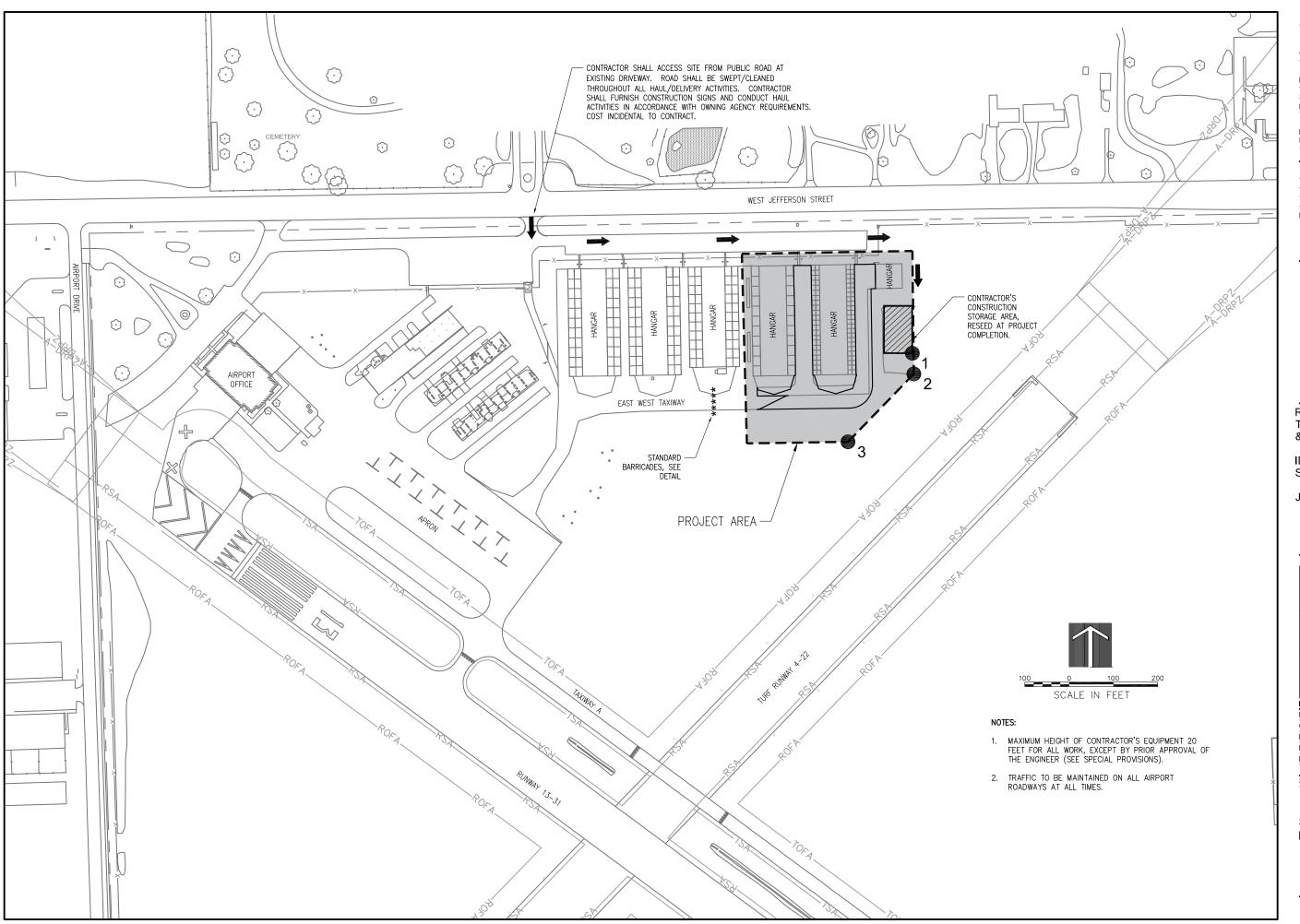
DESIGN BY: LDH 10/11/18

DRAWN BY: LDH 10/11/18

REVIEWED BY: KMS 10/15/18

SHEET TITLE

SHEET INDEX AND SUMMARY OF QUANTITIES



HANSON Englineering | Planning | Ailled Service

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| SSUE: | Novemb | er 16. | 2018 | |

ISSUE: November 16, 201 PROJECT NO: 18A0010

CAD FILE: 03-SITEPLAN.DWG
DESIGN BY: LDH 9/21/18
DRAWN BY: LDH 9/21/18

DRAWN BY: LDH 9/21/18
REVIEWED BY: KMS 10/15/18

SHEET TITLE

SITE AND SAFETY PLAN

GENERAL NOTES

PROJECT DESCRIPTION

THIS PROJECT IS TO REHABILITATE THE T-HANGAR AND ACCESS TAXIWAY PAVEMENTS AT JOLIET REGIONAL AIRPORT INCLUDING, AMONG OTHER INCIDENTAL WORK, THE FOLLOWING ITEMS:

- PLACEMENT OF TEMPORARY SOIL FROSION CONTROL MEASURES.
- REMOVAL OF PAVEMENTS.
- PROVISION OF REQUIRED UNCLASSIFIED EXCAVATION.
- . REMOVAL AND ADJUSTMENT OF EXISTING DRAINAGE STRUCTURES, AND INSTALLATION OF NEW DRAINAGE
- INSTALLATION OF NEW DRAINAGE LAYER, AGGREGATES AND BITUMINOUS (HMA) PAVEMENTS.
- INSTALLATION OF UNDERDRAINS.
- PLACEMENT OF PAVEMENT MARKINGS.
- TOPSOILING, SEEDING AND MULCHING IN ALL DISTURBED AREAS, INCLUDING ALONG NEW PAVEMENT

PROTECTION OF EXISTING AIRPORT FACILITIES

THE CONTRACTOR IS TO BE RESPONSIBLE FOR THE PROTECTION OF EXISTING UNDERGROUND AND OVERHEAD UTILITIES AND LIGHTING EQUIPMENT; DRIVEWAY AND ROAD PAVEMENT AND SHOULDERS; RUNWAY, TAXIWAY AND APRON PAVEMENTS AND SHOULDERS: RUNWAY, TAXIWAY AND AIRPORT LIGHTING EQUIPMENT: AND SEEDED AND TURFED AREAS THAT ARE UTILIZED IN OR AFFECTED BY THE CONTRACTOR'S ACTIVITIES. ITEMS DAMAGED BY THE CONTRACTOR ARE TO BE REPAIRED AT CONTRACTOR'S EXPENSE AND TO THE SATISFACTION OF THE AIRPORT MANAGER AND THE OWNER'S REPRESENTATIVE.

IN ADDITION, WHEN CONDITIONS DICTATE OR AS DETERMINED BY THE AIRPORT MANAGER OR THE OWNER'S REPRESENTATIVE, THE CONTRACTOR SHALL BE REQUIRED TO USE A PICK-UP TYPE SWEEPER IN ALL ACTIVE CONSTRUCTION AIRFIELD PAVEMENT AREAS. THE CONTRACTOR WILL BE REQUIRED TO HAVE A SWEEPER AVAILABLE FOR USE AT ALL TIMES. THE COST OF SWEEPING SHALL BE CONSIDERED INCIDENTAL TO THE

CONTRACTOR'S ACCESS AND TEMPORARY FACILITIES

CONTRACTOR'S ACCESS TO THE PROJECT WHEN ON AIRPORT PROPERTY IS SHOWN ON SHEET 3. CONTRACTOR'S ACCESS TO THE AIRPORT ITSELF IS TO BE PROVIDED BY PUBLIC RIGHTS-OF-WAY. THE CONTRACTOR IS TO SECURE ALL NECESSARY PERMITS FOR THE USE OF ANY PUBLIC RIGHTS-OF-WAY AND IS TO MAINTAIN TRAFFIC ON THESE PUBLIC ROADS AT ALL TIMES, WITH THE COSTS OF PERMITTING, CLEANING AND REPAIRING OF PAVEMENT DAMAGED BY CONTRACTOR'S ACTIVITIES INCIDENTAL TO THE CONTRACT. USE OF AND REPAIRS TO ANY PUBLIC FACILITIES ARE TO BE COMPLETED TO THE SATISFACTION OF THE FACILITY'S

THE CONTRACTOR IS TO PROVIDE TEMPORARY CONSTRUCTION ROADS WITHIN THE CONSTRUCTION LIMIT LINES AS MAY BE REQUIRED BY HIS ACTIVITIES. HEAVY VEHICLES SHALL NOT CROSS EXISTING PAVEMENT SURFACES EXCEPT AS APPROVED BY THE AIRPORT MANAGER AND THE OWNER'S REPRESENTATIVE. ANY DAMAGE TO PAVEMENTS THAT MAY OCCUR BY THE CONTRACTOR'S ACTIVITIES SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE AND TO THE SATISFACTION OF THE AIRPORT MANAGER AND THE OWNER'S REPRESENTATIVE. FOR HAUL ROUTES MADE BY CONTRACTOR THROUGH GRASSED AREAS, CONTRACTOR SHALL GRADE, LEVEL, TOPSOIL, SEED AND MULCH AT THE END OF THE PROJECT, COST INCIDENTAL TO THE CONTRACT.

THE CONTRACTOR IS TO PROVIDE AN EQUIPMENT STORAGE AND PARKING AREA AT THE LOCATIONS SHOWN ON SHEET 3. IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN THE ACCESS ROADS AND THE STORAGE AREA DURING CONSTRUCTION AND TO RESTORE THE AREAS AT PROJECT COMPLETION TO CONDITIONS SUITABLE TO THE AIRPORT MANAGER AND THE OWNER'S REPRESENTATIVE. AT THE AIRPORT MANAGER'S DISCRETION, THE TEMPORARY FACILITIES MAY REMAIN, BUT THEY MUST BE LEFT IN CONDITIONS SUITABLE TO THE AIRPORT MANAGER. THE COST OF PROVIDING, MAINTAINING AND RESTORING THE TEMPORARY FACILITIES IS INCIDENTAL TO THE CONTRACT

RESPONSIBILITY FOR EXISTING UTILITIES

THE LOCATION, SIZE AND/OR TYPE OF MATERIAL OF EXISTING UNDERGROUND OR OVERHEAD UTILITIES AS MAY BE INDICATED ON THESE CONSTRUCTION PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE PROJECT ENGINEER HAVE INDEPENDENTLY VERIFIED THIS INFORMATION AND NEITHER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, SUFFICIENCY OR COMPLETENESS OF THE INFORMATION AND GIVE NO EXPRESSED OR IMPLIED GUARANTEE THAT ANY CONDITIONS INDICATED ARE REPRESENTATIVE OF ACTUAL CONDITIONS TO BE ENCOUNTERED

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY ALL UTILITY COMPANIES AND AGENCIES OF HIS CONSTRUCTION PLANS AND SHALL OBTAIN FROM EACH PARTY DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF ALL UTILITIES AND THE WORKING SCHEDULE OF ANY REMOVALS OR ADJUSTMENTS REQUIRED OF THE UTILITY. THE CONTRACTOR SHALL CONTACT J.U.L.I.E. (PHONE 800-892-0123) TO ASSIST IN THE ABOVE.

THE CONTRACTOR SHALL PROTECT ANY FACILITIES TO THE SATISFACTION OF THE UTILITY OR OWNING-AGENCY WITH THE COST OF ANY REQUIRED PROTECTION TO BE INCIDENTAL TO THE CONTRACT. IN THE EVENT A UTILITY LINE OR SERVICE IS UNEXPECTEDLY ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE OWNER'S REPRESENTATIVE AND THE UTILITY COMPANY OR AGENCY OF JURISDICTION. ANY SUCH UTILITIES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED TO SERVICE AT ONCE.

EXISTING BENCHMARKS

PROJECT BENCHMARKS ARE AS FOLLOWS:

N 1,767,814.886 E 1,026,441.694 ELEV. 579.893

MF0081 N 1,767,965.5 E 1.026.578.7 ELEV. 583.15



PROJECT IS LOCATED IN NORTHWEST 1/4 OF **SECTION 14, TROY** TOWNSHIP, WILL COUNTY

800.892.0123

NOTES

- 1. COORDINATES ARE IN NAD 83 FOR HORIZONTAL AND NAVD 88 FOR VERTICAL.
- 2. STATIONS, OFFSETS AND ELEVATIONS SHOWN ARE IN FEET.
- 3. THE AIRPORT REFERENCE CODE FOR RUNWAY 13-31 IS B-I. RUNWAY 13 HAS A NON-PRECISION APPROACH WITH VISIBILITY 1 MILE. RUNWAY 31 HAS A VISUAL APPROACH.
- 4. THE AIRPORT REFERENCE CODE FOR RUNWAY 4-22 IS A-I WITH VISUAL APPROACHES ON BOTH RUNWAYS 4 AND 22.

RUNWAY END COORDINATES

| DESCRIPTION | LATITUDE | LONGITUDE | RUNWAY ELEVATION |
|---------------|----------------|----------------|---------------------|
| RUNWAY 13 END | 41°31'11.79" N | 8810'46.85" W | 580.2 |
| RUNWAY 31 END | 41°30'54.48" N | 8810'15.86" W | 577.3 |
| RUNWAY 4 END | 41°30'54.66" N | 88*10'45.64" W | 574.6 |
| RUNWAY 22 END | 41°31'14.09" N | 88*10'20.47" W | 578.4 |

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REHABILITATE TAXILANES TH/7. TH/7-1 & TH/7-2

IDA No: JOT-4671 SBGP No: 3-17-SBGP-144

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SHEET TITLE

SITE AND SAFETY PLAN NOTES

| | OBJECT INFORMATION | | | | | | | | | | | |
|-------------|-----------------------------------|------------|---------------------|---------------------|--------------------|--------------------|-------------------------|------------------------|---------------------------|------------------------|-----------------------|--------------------------|
| ITEM NO. | DESCRIPTION | MOBILITY | GROUND ELEVATION | OBJECT ELEVATION | LATITUDE | LONGITUDE | RUNWAY 13-31 STATION | RUNWAY 13-31 OFFSET | RUNWAY 13-31 EXIST EL. | RUNWAY 4-22 STATION | RUNWAY 4-22 OFFSET | RUNWAY 4-22 EXIST EL. |
| 1 | CONTRACTOR'S CONSTRUCTION STORAGE | STATIONARY | 578.1 | 598.1 | 41° 31′ 15.0858" N | 88° 10' 24.7201" W | 31+78.45 | 1,269.7 | 580.0 | 245+91.77 | 300.0 | 578.0 |
| 2 | CONSTRUCTION EQUIPMENT | MOVING | 578.1 | 598.1 | 41° 31' 14.6240" N | 88° 10' 24.6759" W | 32+08.98 | 1,234.1 | 580.0 | 245+60.58 | 265.0 | 578.0 |
| 3 | CONSTRUCTION EQUIPMENT | MOVING | 581.0 | 601.0 | 41° 31′ 13.1097" N | 88° 10' 26.6331" W | 31+80.58 | 1,022.3 | 580.0 | 243+46.91 | 265.0 | 578.0 |

SAFETY IS REQUIRED

CONSTRUCTION OF THE PROJECT SHALL BE PERFORMED BY THE CONTRACTOR IN ACCORDANCE WITH THE GUIDELINES SPECIFIED IN FAA ADVISORY CIRCULAR 150/5370-2 (CURRENT ISSUE). ANY CONTRACTOR ACTIVITIES REQUIRED FOR PROJECT SAFETY SHALL BE PROVIDED BY THE CONTRACTOR AND INCIDENTAL TO THE CONTRACT.

PRIOR TO THE ISSUANCE OF A CONSTRUCTION NOTICE—TO—PROCEED (NTP) BY THE ILLINOIS DIVISION OF AERONAUTICS. THE CONTRACTOR SHALL PREPARE AND SUBMIT A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2 (CURRENT ISSUE), PARAGRAPH 2.4.2, OR EQUIVALENT SECTION IN SUBSEQUENT/CURRENT ISSUE. THE SPCD SHALL BE REVIEWED AND APPROVED BY THE AIRPORT MANAGER/DIRECTOR, WHO WILL THEN SUBMIT THE DOCUMENT TO THE ILLINOIS DIVISION OF AERONAUTICS FOR THEIR APPROVAL PRIOR TO NOTICE TO PROCEED.

TO MINIMIZE DISRUPTIONS AT AIRPORT OPERATIONS, CONSTRUCTION OPERATIONS MUST BE CONTROLLED THROUGHOUT THE PROJECT'S DURATION, AND WORK MUST BE COMPLETED EXPEDITIOUSLY. A CONSTRUCTION PHASING PLAN DETAILING THE SEQUENCING OF THE CONTRACTOR'S WORK THROUGHOUT THE PROJECT IS INCLUDED IN THE PLANS. THE CONTRACTOR SHALL PROVIDE HIS WRITTEN ACCEPTANCE OF THE PROJECT CONSTRUCTION PHASING PLAN AT THE PRE-CONSTRUCTION CONFERENCE. ANY AND ALL CHANGES TO THE CONSTRUCTION PHASING PLAN THAT MAY BE REQUESTED BY THE CONTRACTOR MUST BE APPROVED BY THE PROJECT ENGINEER AND THE AIRPORT OWNER. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE SUFFICIENT ADVANCE NOTICE OF ANY PROPOSED PHASING CHANGE TO PERMIT CONSIDERATION AND APPROVAL BY THE PROJECT ENGINEER AND THE AIRPORT OWNER. THE CONTRACTOR SHALL NOT BE ENTITLED TO ANY EXTRA COMPENSATION, NOR EXTENSION TO THE CONTRACT TIME, BECAUSE OF A PHASING CHANGE REQUEST NOR FOR ANY TIME NECESSARY IN RECEIVING THE REQUIRED APPROVALS. THE CONTRACTOR SHALL EXPEDITE WORK AT THOSE STAGES WHERE ACTIVE TAXIWAYS, HANGAR ACCESS, APRONS, ROADWAYS OR PARKING LOTS MUST BE CLOSED, TO MINIMIZE THE LENGTH OF TIME THAT AIRPORT OPERATIONS ARE RESTRICTED

THE PRE-CONSTRUCTION CONFERENCE, THE CONTRACTOR SHALL PROVIDE A CONTRACTOR COORDINATION PLAN THAT COORDINATES HIS WORK WITH THE WORK OF HIS SUBCONTRACTORS AND THE WORK OF OTHER CONTRACTORS OF OTHER ON-GOING AIRPORT PROJECTS.

RUNWAY CLOSURE

NO RUNWAY CLOSURES WILL BE PERMITTED AS PART OF THIS PROJECT.

THE CONTRACTOR SHALL FURNISH BARRICADES FOR ANY AIRFIELD OR ROADWAY PAVEMENT TO BE CLOSED BY HIS WORK, IT WILL BE THE CONTRACTOR'S RESPONSIBILITY TO FURNISH, PLACE AND MAINTAIN BARRICADES AS SHOWN IN DETAILS A AND B, THIS SHEET, AND AS DIRECTED BY THE RESIDENT ENGINEER AND AIRPORT MANAGER/DIRECTOR THE COST OF THESE ITEMS, AND THEIR MAINTENANCE, IS TO BE PAID UNDER AR150530 TRAFFIC MAINTENANCE. ANY WORK THAT REQUIRES PORTIONS OF AN ACTIVE RUNWAY, TAXIWAY OR APRON TO BE CLOSED MUST BE COMPLETED EXPEDITIOUSLY TO MINIMIZE DISRUPTION TO AIRCRAFT OPERATIONS.

VEHICULAR TRAFFIC CONTROL

THE CONTRACTOR SHALL ERECT AND MAINTAIN, AT NO COST TO THE CONTRACT, DIRECTIONAL AND INFORMATIONAL SIGNS FOR THE CONTRACTOR'S ACCESS ROUTES AT THE EXISTING CONSTRUCTION ENTRANCES AND FOR THE CONTRACTOR'S ROUTE WITHIN THE AIRPORT OPERATIONS AREA, AS NOTED ON THE PLANS OR AS DIRECTED BY THE RESIDENT ENGINEER. WHERE CONTRACTOR EQUIPMENT IS OPERATING WITHIN ACTIVE AIRCRAFT OPERATIONS AREAS, RADIO-EQUIPED FLAGGERS SHALL BE FURNISHED BY THE CONTRACTOR CONTINUOUS PAVEMENT SWEEPING SHALL BE FURNISHED TO REMOVE DEBRIS FROM ACTIVE AIRCRAFT MOVEMENT PATHS. THE COST OF TRAFFIC CONTROL/FLAGGERS AND PAVEMENT SWEEPING SHALL BE PAID UNDER AR150530 TRAFFIC MAINTENANCE.

AIRFIELD OPERATIONAL SAFETY DURING CONSTRUCTION

THE CONTRACTOR SHALL NOT HAVE ACCESS TO ANY PART OF THE ACTIVE AIRFIELD (RUNWAYS, TAXIWAYS OR APRONS) FOR ANY EQUIPMENT OR PERSONNEL WITHOUT THE APPROVAL OF THE RESIDENT ENGINEER AND THE AIRPORT OWNER. ACTIVITIES WITHIN THE AIRPORT OPERATIONS AREA (AOA) ARE SUBJECT TO FEDERAL ACCESS CONTROL. BECAUSE OF THE HIGH REQUIREMENTS FOR AIRPORT SECURITY AND SAFETY, THE FOLLOWING REQUIREMENTS MUST BE ADHERED TO:

- ALL EMPLOYEES OF THE CONTRACTOR SHALL PARK THEIR PERSONAL VEHICLES IN THE DESIGNATED EQUIPMENT PARKING AND STORAGE AREA. EACH PERSON OR VEHICLE ENTERING THE CONTRACTOR AREA SHALL DO SO IN ACCORDANCE WITH THE POLICIES AND PROCEDURES OF THE AIRPORT OWNER. THE CONTRACTOR WILL TRANSPORT THE WORKERS FROM THE PARKING AREAS TO THE WORK AREA. ONLY CONTRACTOR VEHICLES WILL BE ALLOWED OUTSIDE OF THE PROPOSED EQUIPMENT STORAGE AND PARKING
- SHOULD ANY CONTRACTOR PERSONNEL BE IDENTIFIED AS NONCOMPLIANT WITH ANY VEHICLE DRIVING SAFETY REQUIREMENTS IN THIS PROJECT SAFETY PLAN OR IN THE AIRPORT VEHICLE OPERATIONS REGULATIONS, SUCH DRIVERS SHALL BE PENALIZED BY RESCISSION OF THEIR ON-AIRPORT DRIVING PRIVILEGES, AND THEIR ACCESS TO THE CONSTRUCTION LIMIT AREA WHEN OPERATING VEHICLES SHALL BE REVOKED.
- THE CONTRACTOR WILL BE REQUIRED TO BE IN CONTACT WITH AIRPORT OPERATIONS. THIS WILL KEEP THE CONTRACTOR IN CONTACT WITH AIRPORT PERSONNEL AND ENABLE THE AIRPORT PERSONNEL TO IMMEDIATELY CONTACT THE CONTRACTOR IN CASE OF AN AERONAUTICAL EMERGENCY THAT WOULD REQUIRE ACTION BY THE CONTRACTOR AND/OR HIS PERSONNEL

THE CONTRACTOR SHALL REMAIN WITHIN THE CONSTRUCTION LIMITS LINE SHOWN IN THE PLANS. WHEN OUTSIDE THESE LIMITS, ALL CONTRACTOR ACTIVITIES SHALL REMAIN MORE THAN 125 FEET FROM THE CENTERLINE AND 240 FEET FROM THE END OF ACTIVE RUNWAY 13-31 AND 4-22. FOR WORK NEAR TAXIWAYS AND APRONS, THE CONTRACTOR'S PERSONNEL AND EQUIPMENT MUST REMAIN AT LEAST 44.5 FEET FROM CENTERLINE OF ACTIVE CATEGORY I TAXIWAYS, 65.5 FEET FROM ACTIVE CATEGORY II TAXIWAYS, AND TEN (10) FEET FROM ACTIVE APRONS. WHEN CONSTRUCTION OPERATIONS MUST BE CONDUCTED WITHIN THESE SEPARATIONS. THE PAVEMENT MUST BE CLOSED TO AIRCRAFT ACTIVITY THE CONTRACTOR BY PROVIDING TEMPORARY BARRICADES AS SHOWN IN THE PLANS, AND IN THE CASE RUNWAY PAVEMENTS, CLOSED RUNWAY MARKERS

THE CONTRACTOR SHALL KEEP ALL OF HIS EQUIPMENT AND PERSONNEL AT LEAST 15 FEET FROM THE EDGE OF ANY ACTIVE ROADWAY OR AUTO PARKING PAVEMENT. WHEN HIS ACTIVITIES REQUIRE WORKING WITHIN 15 FEET OF THE ROAD/PAVEMENT EDGE, THE CONTRACTOR SHALL PROVIDE FOR TRAFFIC CONTROL IN ACCORDANCE WITH IDOT

OPEN TRENCHES, EXCAVATIONS AND STOCKPILED MATERIAL AT THE CONSTRUCTION SITE SHALL BE DELINEATED WITH THE USE OF BARRICADES DURING HOURS OF RESTRICTED VISIBILITY AND/OR DARKNESS. NO OPEN TRENCHES SHALL BE ALLOWED WITHIN THE RUNWAY SAFETY AREA (RSA) OR THE TAXIWAY SAFETY AREA (TSA) WHEN THE RUNWAY OR TAXIWAY IS OPEN TO AIR TRAFFIC (INCLUDING OVERNIGHT). THE RSA IS DEFINED AS 60 FEET FROM THE CENTERLINE AND 240 FEET FROM THE END OF RUNWAY 13-31 AND 4-22. THE TSA IS MEASURED AT 24.5 FEET FROM THE CATEGORY I TAXIWAY CENTERLINE AND 39.5 FEET FROM THE CATEGORY II TAXIWAY CENTERLINE. NO VERTICAL DROP OF GREATER THAN 3-INCHES IN HEIGHT FROM PAVEMENT EDGE TO EARTH GRADE OR EARTH GRADE TO EARTH GRADE WITHIN THE RSA OR TSA WILL BE PERMITTED WHEN THE RUNWAY OR TAXIWAY IS OPEN TO AIR TRAFFIC. THE CONTRACTOR WILL HAVE STEEL PLATES ON-SITE TO ALLOW FOR THE RAPID COVERING OF TRENCHES OR EARTH DROPS IN THE EVENT OF UNEXPECTED WORK STOPPAGES FOR WEATHER OR AIRPORT EMERGENCIES.

WHEN NOT IN USE AND DURING NONWORKING HOURS, CONTRACTOR'S EQUIPMENT SHALL BE PARKED WITHIN THE CONTRACTOR'S EQUIPMENT STORAGE AND PARKING AREAS. THE EQUIPMENT STORAGE AND PARKING AREAS ARE TO BE LOCATED AS SHOWN ON THE PHASING PLAN. THE CONTRACTOR WILL BE RESPONSIBLE FOR MAINTAINING THE CONSTRUCTION ENTRANCES IN GOOD CONDITION. THE COST OF MAINTAINING THE CONSTRUCTION ENTRANCE AND CONTRACTOR AREAS IS TO BE INCIDENTAL TO THE CONTRACT. THE CONTRACTOR SHALL PROTECT ALL EXISTING PAVEMENT EDGES FROM DAMAGE FROM CONSTRUCTION EQUIPMENT AND HAUL VEHICLES.

AT NO TIME SHALL THE CONTRACTOR CONDUCT ANY ACTIVITIES OR OPERATE OR PARK EQUIPMENT SO AS TO OBSTRUCT ACTIVE PART 77 AIRPORT IMAGINARY SURFACES OR THE RUNWAY PROTECTION ZONES (RPZ) AS DELINEATED IN THE PLANS. CONTRACTOR'S EQUIPMENT SHALL EXTEND NO HIGHER THAN 20 FEET. CRANES SHALL NOT BE USED DURING INSTRUMENT WEATHER CONDITIONS OR AT NIGHT. CRANES SHALL BE LOWERED WHEN NOT IN USE

BEFORE REOPENING TEMPORARILY CLOSED PAVEMENTS, THE CONTRACTOR SHALL INSPECT AND CLEAN, AS NECESSARY, THE PAVEMENT TO ASSURE THAT NO MATERIALS OR OBJECTS THAT MAY DAMAGE AIRCRAFT OR VEHICLES REMAIN. ANY REQUIRED CLEANING SHALL BE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND AIRPORT OWNER AND

ALL WORK SHALL BE COMPLETED IN ACCORDANCE WITH THE APPROVED PROJECT SAFETY PLAN, ISSUED BY THE ILLINOIS DIVISION OF AERONAUTICS

FAILURE TO USE THESE PRESCRIBED PROCEDURES OR ADHERE TO THE SAFETY REQUIREMENTS WILL RESULT IN THE SUSPENSION OF WORK

NOTIFICATIONS BY CONTRACTOR

THE CONTRACTOR MUST NOTIFY THE RESIDENT ENGINEER AND THE AIRPORT OWNER 3 DAYS IN ADVANCE OF ANY REQUIRED PARTIAL OR COMPLETE CLOSING OF ANY RUNWAY, TAXIWAY OR APRON. THE DATE, TIME AND SCHEDULED DURATION OF THE CLOSING MUST BE APPROVED BY THE RESIDENT ENGINEER AND THE AIRPORT OWNER. CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT OWNER 3 DAYS IN ADVANCE OF THE CONTRACTOR'S CLOSING OF OTHER ACTIVE ROADWAYS, AIRFIELD OR ROADWAY LIGHTING CIRCUITS. OR OTHER AIRPORT FACILITIES.

CONTRACTOR'S USE OF SITE

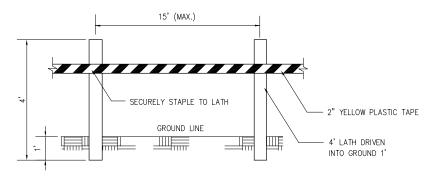
CONTRACTOR'S ACCESS TO THE PROJECT WHEN ON AIRPORT PROPERTY IS SHOWN IN THE PLANS. CONTRACTOR'S ACCESS TO THE AIRPORT ITSELF IS TO BE PROVIDED BY PUBLIC RIGHTS-OF-WAY. THE CONTRACTOR IS TO SECURE ALL NECESSARY PERMITS FOR THE USE OF ANY PUBLIC RIGHTS-OF-WAY AND IS TO MAINTAIN TRAFFIC ON THESE PUBLIC ROADS AT ALL TIMES, WITH THE COSTS OF PERMITTING, CLEANING AND REPAIRING OF PAVEMENT DAMAGED BY CONTRACTOR'S ACTIVITIES INCIDENTAL TO THE CONTRACT. USE OF AND REPAIRS TO ANY PUBLIC FACILITIES ARE TO BE COMPLETED TO THE SATISFACTION OF THE FACILITY'S OWNER.

THE CONTRACTOR IS TO PROVIDE TEMPORARY CONSTRUCTION ROADS WITHIN THE CONSTRUCTION LIMIT LINES AS MAY BE REQUIRED BY HIS ACTIVITIES. HEAVY VEHICLES SHALL NOT CROSS EXISTING PAVEMENT SURFACES EXCEPT AS APPROVED BY THE AIRPORT OWNER AND THE RESIDENT ENGINEER. ANY DAMAGE TO PAVEMENTS THAT MAY OCCUR BY THE CONTRACTOR'S ACTIVITIES SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE AND TO THE SATISFACTION OF THE AIRPORT OWNER AND THE RESIDENT ENGINEER. FOR HAUL ROUTES MADE BY CONTRACTOR THROUGH GRASSED AREAS, CONTRACTOR SHALL GRADE, LEVEL, TOPSOIL, SEED AND MULCH AT THE END OF THE PROJECT, COST INCIDENTAL TO THE CONTRACT.

THE CONTRACTOR IS TO PROVIDE AN EQUIPMENT STORAGE AND PARKING AREA AT THE LOCATIONS SHOWN IN THE PLANS. IT IS THE CONTRACTOR'S RESPONSIBILITY TO MAINTAIN THE ACCESS ROADS AND THE STORAGE AREA DURING CONSTRUCTION AND TO RESTORE THE AREAS AT PROJECT COMPLETION TO CONDITIONS SUITABLE TO THE AIRPORT OWNER AND THE RESIDENT ENGINEER. AT THE AIRPORT OWNER'S DISCRETION, THE TEMPORARY FACILITIES MAY REMAIN, BUT THEY MUST BE LEFT IN CONDITIONS SUITABLE TO THE AIRPORT OWNER. THE COST OF PROVIDING, MAINTAINING AND RESTORING THE TEMPORARY FACILITIES IS INCIDENTAL TO THE CONTRACT.

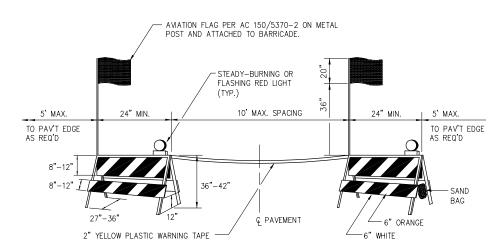
UTILITY OUTAGES AND SHUTDOWNS

THE CONTRACTOR SHALL PROVIDE 3 DAYS PRIOR NOTICE OF ANY OUTAGES OR SHUTDOWNS TO THE OWNER AND THE AGENCY OWNING THE AFFECTED UTILITY. THE CONTRACTOR SHALL PROVIDE ANY TEMPORARY CONNECTIONS OR OTHER MEASURES AS MAY BE REQUIRED TO MAINTAIN SERVICE AS MAY BE REQUIRED BY THE OWNING AGENCY AT NO COST



MATERIALS ARE TO BE APPROVED BY ENGINEER PRIOR TO INSTALLATION. COST OF MATERIALS, INSTALLATION, RELOCATION AND MAINTENANCE OF LATHING AND WARNING TAPE IS TO BE INCIDENTAL TO THE CONTRACT.

DETAIL A LATHING AND WARNING TAPE



BARRICADES ARE TO BE OF IDOT TYPE II. A STEADY-BURNING OR FLASHING RED LIGHT FACING PASSING TRAFFIC IS TO BE MOUNTED ABOVE THE TOP OF EACH BARRICADE FRAME. THE BARRICADE IS TO BE STABILIZED FROM WIND BY SANDBAGS PLACED ON THE FRAME OR OTHER METHODS APPROVED BY THE RESIDENT ENGINEER. NO PART OF THE REFLECTORIZED PORTION OF THE BARRICADE IS TO BE OBSTRUCTED IN ANY MANNER. COST OF FURNISHING, INSTALLING, RELOCATING, MAINTAINING AND REMOVING BARRICADES IS TO BE PAID UNDER AR150530 TRAFFIC MAINTENANCE.

DETAIL B STANDARD PAVEMENT BARRICADES



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RECORD DRAWING

These Record Drawings have been prepared based in part, on information provided by others. Hansa Professional Services Inc. has not verified the Professional Services Inc. has not verified the accuracy and/or completeness of the informati provided and shall not be responsible for any error or omissions that may be incorporated in these drawings as a result of erroneous or incomplete information provided by others.

Unless noted otherwise, work originally shown as "Proposed" was constructed in substantial conformance to the drawings and general notes.

T-HANGAR TAXILANE REHABILITATION AND WIDENING: TH/5, TH/6, TH/6-1, TH/6-2, TH/6-3 AND TH/6-4

IDA No: JOT-4313 SBGP Nos: 3-17-SBGP-111 3-17-SBGP-120 3-17-SBGP-133

JO023

DESCRIPTION NO. DATE DES DWN REV ISSUE: April 21, 2017 PROJECT NO: 15A0062

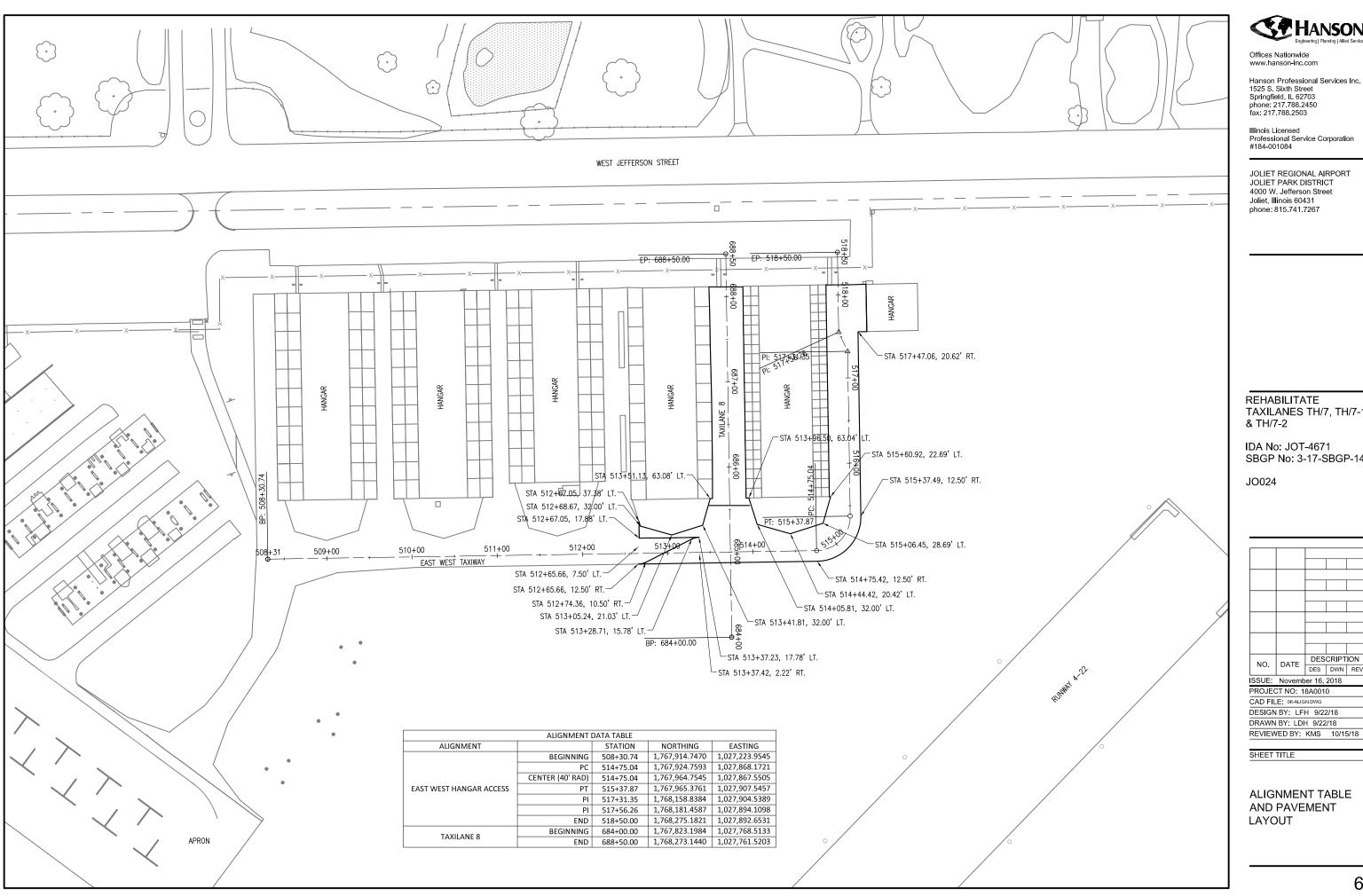
CAD FILE: 05-SAFETYNOTES.DWG DESIGN BY: 1 DH 9/21/18

DRAWN BY: LDH 9/21/18

REVIEWED BY: SJM 12/2/16

SHEET TITLE

CONSTRUCTION SAFETY NOTES AND **DETAILS**





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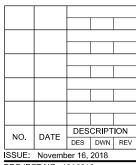
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REHABILITATE TAXILANES TH/7, TH/7-1 & TH/7-2

IDA No: JOT-4671 SBGP No: 3-17-SBGP-144

JO024

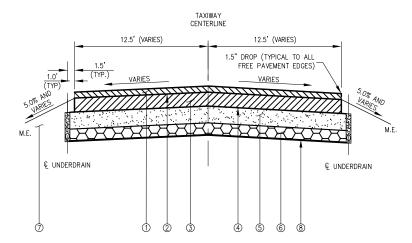


PROJECT NO: 18A0010 CAD FILE: 06-ALIGN.DWG

DESIGN BY: LFH 9/22/18 DRAWN BY: LDH 9/22/18

SHEET TITLE

ALIGNMENT TABLE AND PAVEMENT LAYOUT

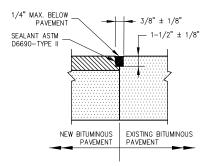


TYPICAL SECTION - TAXIWAY/TAXILANE PAVEMENTS

- ① PROPOSED 2.0 INCH BITUMINOUS SURFACE COURSE, ITEM AR401613
- PROPOSED BITUMINOUS TACK COAT, ITEM AR603510 *
- ③ PROPOSED 2.0 INCH BITUMINOUS BASE COURSE, ITEM AR403613
- 4 PROPOSED BITUMINOUS PRIME COAT, ITEM AR602510
- ⑤ PROPOSED 6 INCH CRUSHED AGGREGATE BASE COURSE, ITEM AR209606
- 6 PROPOSED 6 INCH GRANULAR DRAINAGE SUBBASE, ITEM AR154606
- PROPOSED 4 INCH TOPSOIL, ITEM AR905510
- 8 PROPOSED SOIL STABILIZATION FABRIC, ITEM AR152540

* BITUMINOUS TACK COAT SHALL BE APPLIED BETWEEN EACH LIFT OF BITUMINOUS BASE COURSE AND BETWEEN THE BITUMINOUS BASE COURSE TOP LIFT AND THE BITUMINOUS SURFACE COURSE — NO EXCEPTIONS.

BITUMINOUS PRIME AND BITUMINOUS TACK COAT SHALL BE REQUIRED AS SPECIFIED. SEE SPECIAL PROVISIONS AND STANDARD PROVISIONS.



NOTE:

ALL BITUMINOUS/BITUMINOUS JOINT SEALING TO BE PAID UNDER SAW AND SEAL BITUMINOUS JOINTS, ITEM AR401660.

BITUMINOUS/BITUMINOUS SEAL

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REHABILITATE TAXILANES TH/7, TH/7-1 & TH/7-2

IDA No: JOT-4671 SBGP No: 3-17-SBGP-144

JO024

NO. DATE DESCRIPTION
DES DWN REV

ISSUE: November 16, 2018
PROJECT NO: 18A0010

SHEET TITLE

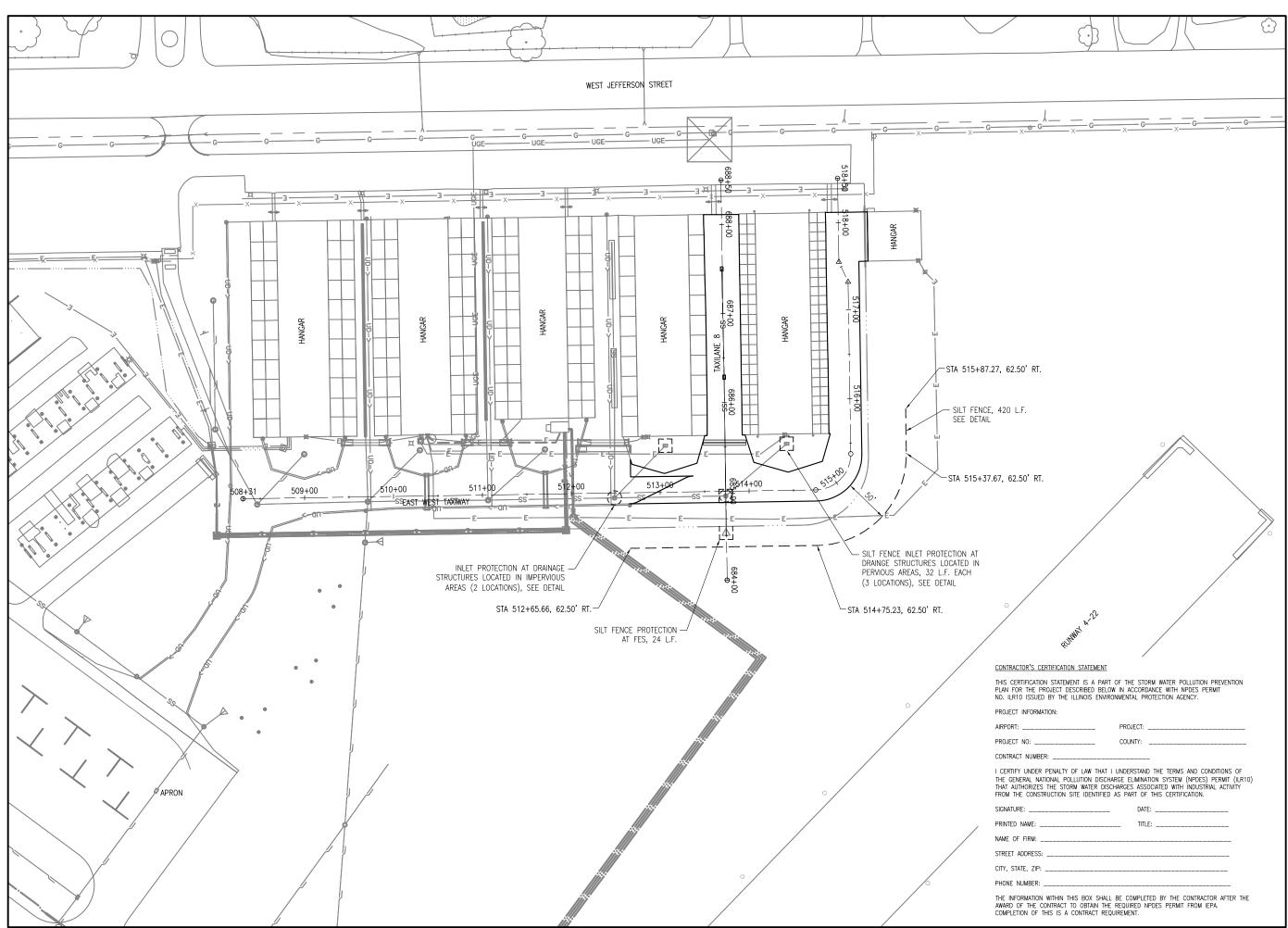
CAD FILE: 07-TYPICAL.DWG

DESIGN BY: LDH 9/22/18

DRAWN BY: LDH 9/22/18

REVIEWED BY: KMS 10/15/18

TYPICAL SECTION AND PAVEMENT DETAILS





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REHABILITATE TAXILANES TH/7, TH/7-1 & TH/7-2

IDA No: JOT-4671 SBGP No: 3-17-SBGP-144

JO024

| NO. | DATE | DES | CRIPT | ION |
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| NO. | DAIL | DES | DWN | REV |
| SSUE: | Novemb | er 16, | 2018 | |

PROJECT NO: 18A0010
CAD FILE: 08-SWPPP.DWG

DESIGN BY: LFH 9/22/18

DRAWN BY: LDH 9/22/18

REVIEWED BY: KMS 10/15/18

SHEET TITLE

STORM WATER
POLLUTION
PREVENTION PLAN

TYPICAL ROUND

INLET FILTER

FILTER FABRIC INLET PROTECTION SHALL CONSIST OF INLET BASKET AND FABRIC INSERT, FLEXSTORM CATCH—IT BY ADVANCED DRAINAGE SYSTEMS, FLOGARD TEMPORARY INLET FILTER BY OLDCASTLE, OR APPROVED EQUAL.

2. DROP FLEXSTORM INLET FILTER ONTO

LOAD BEARING LIP OF CASTING OR

CONCRETE STRUCTURE

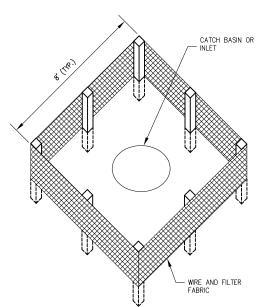
- 2. DEVICE SHALL BE EQUIPPED WITH AN OVERFLOW FEATURE SO DRAINAGE TO INLET IS NOT COMPLETELY BLOCKED IF DEVICE IS FULL OF SILT.
- 3. INLET BASKET IS AVAILABLE TO FIT ROUND, RECTANGULAR, BEEHIVE OR CURB INLET CASTINGS.

. REMOVE GRATE

3. REPLACE GRATE

- 4. FILTER FABRIC SHALL HAVE AN APPARENT OPENING SIZE (AOS) OF AT LEAST 70 SIEVE FOR NONWOVEN
- 5. FILTER FABRIC SHALL HAVE A GRAB TENSILE STRENGTH OF A LEAST 100 LBS FOR NON WOVEN
- 6. POLYESTER OUTER REINFORCEMENT BAG SHALL HAVE FABRIC WITH A WEIGHT OF 4.55 OZ/SQYD +/- 15 PERCENT
- 7. FRAME CONSTRUCTION SHALL HAVE A TENSILE STRENGTH OF AT LEAST 58,000 PSI AND A YIELD STRENGTH OF AT LEAST 36,000
- 8. MAINTENANCE SHALL BE PERFORMED AS NEEDED. REMOVE SILT FROM FABRIC INSERT WHEN 50% OF CAPACITY IS REACHED. REMOVE SILT FROM INTERIOR AND EXTERIOR OF INLET DAM WHEN 50% OF DAM HEIGHT IS REACHED.
- 9. PAYMENT FOR INLET PROTECTION MAINTENANCE SHALL BE INCIDENTAL TO INLET PROTECTION.

INLET PROTECTION AT MANHOLES IN IMPERVIOUS AREAS



- FILTER FABRIC SHALL BE EMBEDDED 8" INTO THE SOIL.
- 2. INSPECTION SHALL BE FREQUENT AND REPAIR/REPLACEMENT SHALL BE MADE PROMPTLY AS NEEDED.
- SILT FENCE SHALL BE REMOVED WHEN IT HAS SERVED ITS USEFULNESS AT THE DIRECTION OF THE AIRPORT REPRESENTATIVE OR OWNER SO AS NOT TO BLOCK OR IMPEDE STORM FLOW OR DRAINAGE. CONTRACTOR SHALL PLACE SEED AND MULCH AROUND STRUCTURES PER LANDSCAPING PLAN COST OF REMOVAL SHALL B INCLUDED IN THE CONTRACT UNIT PRICE FOR SILT FENCE
- AREAS DISTURBED OUTSIDE OF CONSTRUCTION LIMITS DURING PLACEMENT OF INLET PROTECTION TO BE RE-GRADED, SEEDED AND MULCHED, COST INCIDENTAL TO
- 5. FENCE AND POSTS SHALL BE REMOVED WHEN DIRECTED AT PROJECT END.
- 6. PAID UNDER AR156510 SILT FENCE

SILT FENCE AT MANHOLES IN PERVIOUS AREAS

EXISTING OF

FENCE POST SHALL BE EITHER STEEL "T" LINE POST OR HARDWOOD POST WITH A MINIMUM SECTIONAL AREA OF 2.0 SQUARE INCHES. A CARPENTER'S (NOMINAL) 2"x2" POST WILL MEET

WIRE EABRIC

FILTER FABRIC, WOVEN

OR NON-WOVEN

ELEVATION

6" SQUARE MAX

- 2. TOP AND BOTTOM WIRE OF WIRE FABRIC SHALL BE MINIMUM GAGE NO. 9. INTERMEDIATE WIRES OF THE WIRE FABRIC SHALL BE MINIMUM GAGE NO. 11.
- 3. WIRE FABRIC SHALL BE SECURELY FASTENED TO FENCE POSTS WITH NO. 9 GAGE WIRE MINIMUM. FOUR (4) FASTENERS PER POST REQUIRED.
- FILTER FABRIC SHALL BE SECURELY FASTENED TO WIRE FABRIC AND POSTS WITH TIES OR STAPLES SPACED AT 12" APART AT THE TOP, MIDDLE AND BOTTOM.
- WHEN TWO SECTIONS OF FILTER FABRIC MEET, THEY SHALL BE OVERLAPPED BY 6" AND FOLDED AND ATTACHED TO THE WIRE FABRIC AT A POST.
- 6. FILTER FABRIC SHALL BE IN ACCORDANCE WITH SPECIAL PROVISIONS WITH APPARENT OPENING SIZE (AOS) OF AT LEAST 40 FOR NONWOVEN AND WOVEN. THE FABRIC MUST MEET THE APPLICABLE STANDARDS OF AASHTO 288-00 (Article IV, Section B.1.j.1.f.i, AS AMENDED), OR COLUMN AND WOVEN.
- 7. A MAXIMUM OF 5 FEET IS USED FOR POST-TO-POST SPACING.
- 8. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. SOIL STABILIZATION MEASURES SHALL CONSIDER THE TIME OF YEAR, SITE CONDITIONS AND THE USE
- ALL STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY AN APPROPRIATE SEDIMENT CONTROL MEASURE.
- 10. SILT FENCE SHALL BE INSTALLED PRIOR TO ANY GRADING WORK IN THE AREA TO BE PROTECTED. PERIODIC INSPECTION SHALL BE PERFORMED AND REQUIRED MAINTENANCE SHALL BE PROVIDED AFTER EACH RAIN EVENT.
- 11. MAINTENANCE SHALL BE PERFORMED AS NEEDED AND MATERIAL REMOVED AND REPLACED WHEN BULGES DEVELOP IN THE SILT FENCE.
- 12. IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION. DISCHARGES SHALL BE ROUTED THROUGH AN EFFECTIVE SEDIMENT CONTROL MEASURE (E.G. SEDIMENT TRAP, SEDIMENT BASIN, OR OTHER APPROPRIATE MEASURE.
- 13. FENCE POSTS SHALL BE REMOVED WHEN DIRECTED AT PROJECT END.
- 14. THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER OR GOVERNING

SEDIMENTATION AND EROSION CONTROL NOTES:

- A. SEDIMENT CONTROL MEASURES SHALL BE INSTALLED PRIOR TO THE COMMENCEMENT OF HYDROLOGIC DISTURBANCE OF UPLAND
- B. FOR THOSE DEVELOPMENTS THAT REQUIRE A DESIGNATED EROSION CONTROL INSPECTOR (DECI), INSPECTIONS AND DOCUMENTATION SHALL BE PERFORMED, AT A MINIMUM:

 UPON COMPLETION OF SEDIMENT AND RUNOFF CONTROL MEASURES (INCLUDING PERIMETER CONTROLS AND DIVERSIONS), PRIOR TO PROCEEDING WITH ANY OTHER EARTH DISTURBANCE OR GRADING.
 - AFTER EVERY SEVEN (7) CALENDAR DAYS OR STORM EVENT WITH GREATER THAN 0.5 INCH OF RAINFALL OR LIQUID
- C. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. IF STRIPPING, CLEARING, GRADING, OR LANDSCAPING ARE TO BE DONE IN PHASES, THE PERMITTEE SHALL PLAN FOR APPROPRIATE SOIL EROSION AND SEDIMENT
- D. A STABILIZED MAT OF CRUSHED STONE MEETING IDOT GRADATION CA-01 UNDERLAIN WITH FILTER FABRIC AND IN ACCORDANCE WITH THE ILLINOIS URBAN MANUAL, OR OTHER APPROPRIATE MEASURE(S) AS APPROVED BY THE ENFORCEMENT OFFICER, SHALL BE INSTALLED AT ANY POINT WHERE TRAFFIC WILL BE ENTERING OR LEAVING A CONSTRUCTION SITE. SEDIMENT OR SOIL REACHIN AN IMPROVED PUBLIC RIGHT OF WAY, STREET, ALLEY OR PARKING AREA SHALL BE REMOVED BY SCRAPING OR STREET CLEANING AS ACCUMULATIONS WARRANT AND TRANSPORTED TO A CONTROLLED SEDIMENT DISPOSAL AREA.
- E. TEMPORARY DIVERSIONS SHALL BE CONSTRUCTED AS NECESSARY TO DIRECT ALL RUNOFF FROM HYDROLOGICALLY DISTURBED AREAS TO AN APPROPRIATE SEDIMENT TRAP OR BASIN.
- F. DISTURBED AREAS SHALL BE STABILIZED WITH TEMPORARY OR PERMANENT MEASURES WITHIN SEVEN (7) CALENDAR DAYS FOLLOWING THE END OF ACTIVE HYDROLOGIC DISTURBANCE OR REDISTURBANCE.
- G. ALL STOCKPILES SHALL HAVE APPROPRIATE MEASURES TO PREVENT EROSION. STOCKPILES SHALL NOT BE PLACED IN FLOOD PRONE AREAS OR WETLANDS AND DESIGNATED BUFFERS.
- H. SLOPES STEEPER THAN 3H:1V SHALL BE STABILIZED WITH APPROPRIATE MEASURES AS APPROVED BY THE ENFORCEMENT OFFICER.
- APPROPRIATE EROSION CONTROL BLANKET SHALL BE INSTALLED ON ALL INTERIOR DETENTION BASIN SIDE SLOPES BETWEEN THE NORMAL WATER LEVEL AND HIGH WATER LEVEL.
- J. STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY AN APPROPRIATE SEDIMENT CONTROL MEASURE.
- K. IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION AND SEDIMENTATION. DISCHARGES SHALL BE ROUTED THROUGH AN APPROVED ANIONIC POLYMER DEWATERING SYSTEM OR A SIMILAR MEASURE AS APPROVED BY THE ENFORCEMENT OFFICER. DEWATERING SYSTEMS SHOULD BE INSPECTED DAILY DURING OPERATIONAL PERIODS. THE ENFORCEMENT OFFICER, OR APPROVED REPRESENTATIVE, MUST BE PRESENT AT THE COMMENCEMENT
- L. IF INSTALLED SOIL EROSION AND SEDIMENT CONTROL MEASURES DO NOT MINIMIZE SEDIMENT LEAVING THE DEVELOPMENT SITE, ADDITIONAL MEASURES SUCH AS ANIONIC POLYMERS OR FILTRATION SYSTEMS MAY BE REQUIRED BY THE ENFORCEMENT OFFICER.
- M. ALL TEMPORARY AND PERMANENT EROSION CONTROL MEASURES MUST BE MAINTAINED AND REPAIRED AS NEEDED. THE PROPERTY OWNER SHALL BE ULTIMATELY RESPONSIBLE FOR MAINTENANCE AND REPAIR.
- N. ALL TEMPORARY SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED.
- O. THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER, ENFORCEMENT OFFICER, OR OTHER GOVERNING AGENCY.

STORM WATER POLLUTION PREVENTION NOTES

STEEL POST OR

(SEE NOTE 1)

NO 9 (MIN) WIRE

FASTENER, 4 PER -

POST (TYP.)

HARDWOOD POST

THE CONTRACTOR SHALL IMPLEMENT ALL PROVISIONS OF THE CONTRACT DOCUMENTS TO ASSURE THAT STORM WATER POLLUTION PREVENTION ITEMS ARE CONSTRUCTED AND MAINTAINED IN A TIMELY MANNER. SEDIMENTATION MUST NOT BE TRANSPORTED OFF THE CONSTRUCTION SITE. PERMANENT DRAINAGE FEATURES AND VEGETATIVE MEASURES SHALL BE PROVIDED AS SOON AS POSSIBLE.

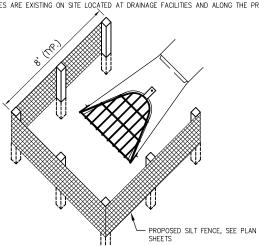
THE MAINTENANCE OF ALL STORM WATER POLLUTION PREVENTION MEASURES IS INCIDENTAL TO THE ASSOCIATED ITEM.

THE CONTRACTOR SHALL BE REQUIRED TO IMPLEMENT AND MAINTAIN STORM WATER POLLUTION PREVENTION PRACTICES AND MEASURES PRIOR TO THE THE CONTRACTOR SHALL BE REQUIRED TO IMPLEMENT AND MAINTAIN STORM WATER POLLUTION PERMITS IN OTHER AREAS. POLLUTION CONTROL MEASURES STRIPPING OF EXISTING VEGETATION WHEREVER POSSIBLE AND AS SOON AS CONSTRUCTION PERMITS IN OTHER AREAS. POLLUTION CONTROL MEASURES SHALL BE IN ACCORDANCE WITH THE CONTRACT DOCUMENTS, INCLUDING THESE CONSTRUCTION PLANS, AND WITH STANDARDS AND SPECIFICATIONS FOR SOIL EROSION. AND SEDIMENT CONTROL, ILLINOIS ENVIRONMENTAL PROTECTION AGENCY, CHERRENT ISSUE. THE CONTRACTOR SHALL ADJUST HIS OPERATIONS AND IMPLEMENT POLLUTION CONTROL MEASURES SO THAT NO RUNOFF FROM STRIPPED AREAS WILL LEAVE THE CONSTRUCTION SITE OTHER

POLLUTION CONTROL ITEMS SHALL BE PROVIDED AS NOTED ON THE STORM WATER POLLUTION PREVENTION PLAN AND IN THE STORM WATER POLLUTION PREVENTION DETAILS AND AS DIRECTED BY THE ENGINEER. THE LIMITS OF SUCH MEASURES SHALL BE STAKED BY THE CONTRACTOR PRIOR TO THE COMMENCEMENT OF CONSTRUCTION. SUCH LIMITS MAY BE ADJUSTED BY THE ENGINEER TO ACCOUNT FOR ACTUAL SITE CONDITIONS EXPERIENCED DURING CONSTRUCTION. ADDITIONAL COMPENSATION FOR MEASURES EXCEEDING THE PLAN QUANTITIES WILL BE PAID FOR AT THE CONTRACT UNIT PRICE FOR

THE CONTRACTOR IS TO MAINTAIN AND ADJUST, REPAIR OR REPLACE ALL POLLUTION PREVENTION MEASURES AS REQUIRED OR AS DIRECTED BY THE ENGINEER UNTIL PERMANENT VEGETATION HAS BEEN ESTABLISHED. MAINTENANCE OF POLLUTION CONTROL MEASURES IS TO BE PROVIDED AT NO ADDITIONAL COST TO THE CONTRACT.

ADDITIONAL STORMWATER POLLUTION PREVENTION MEASURES ARE EXISTING ON SITE LOCATED AT DRAINAGE FACILITIES AND ALONG THE PROPERTY LINE.



SILT FENCE PLACEMENT AT FLARED END SECTIONS (FES)



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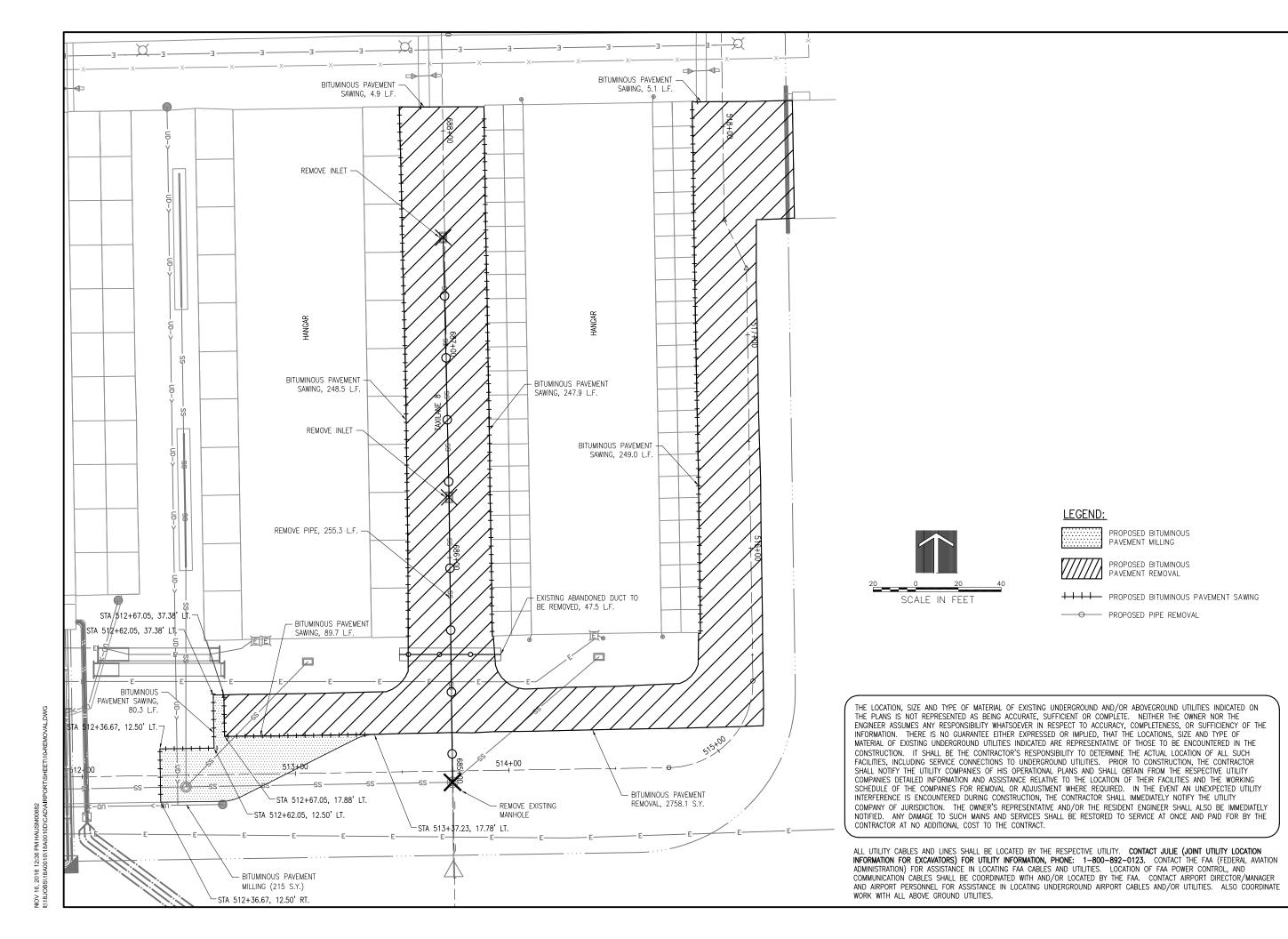
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| SSUE: | Novemb | er 16, | 2018 | |
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CAD FILE: 09-SWPPP DET.DWG DESIGN BY: LDH 9/22/18 DRAWN BY: LDH 9/22/18

REVIEWED BY: KMS 10/15/18

SHEET TITLE

SWPPP DETAILS



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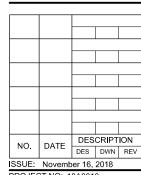
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REHABILITATE TAXILANES TH/7, TH/7-1 & TH/7-2

IDA No: JOT-4671 SBGP No: 3-17-SBGP-144

JO024



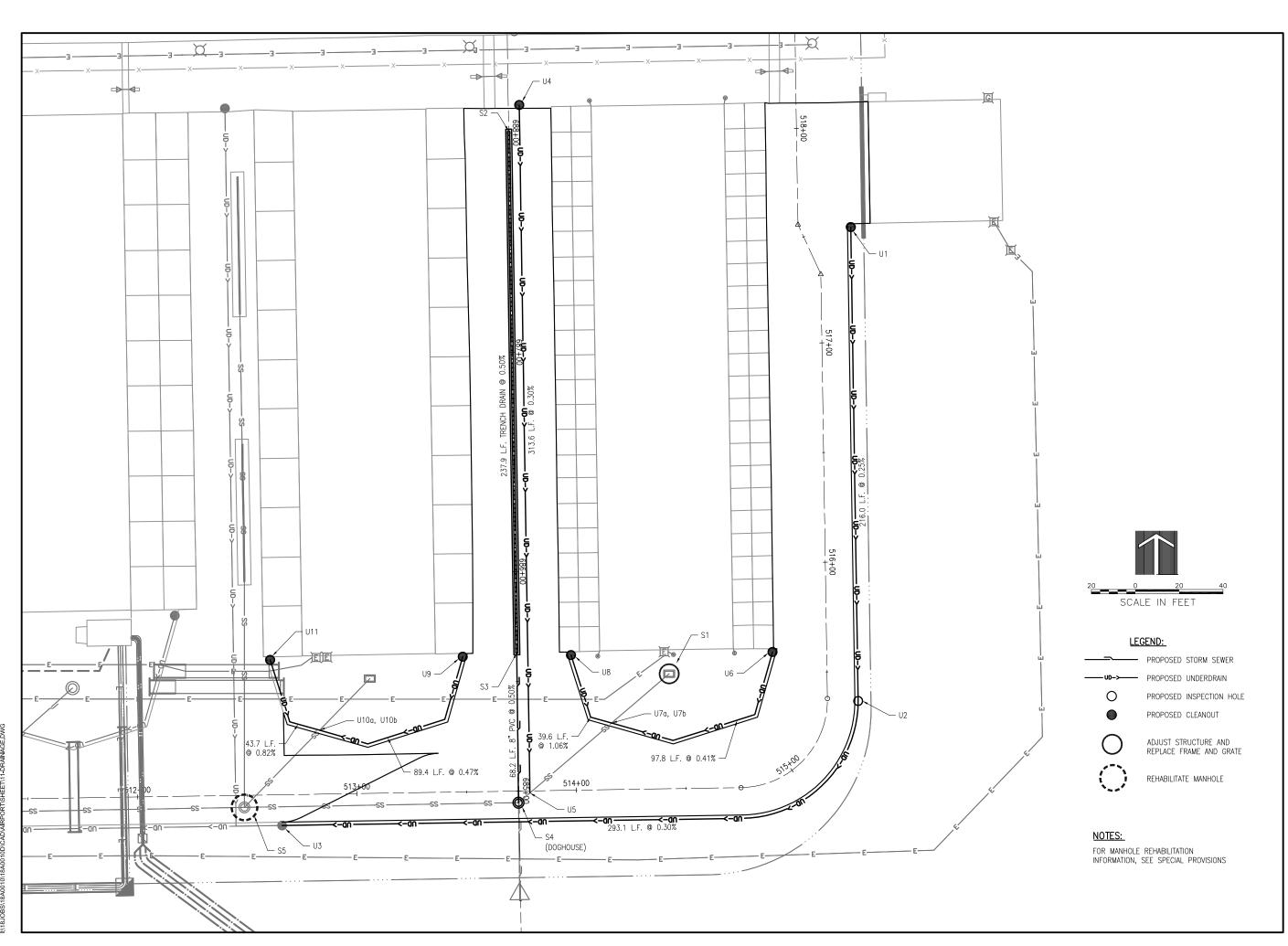
PROJECT NO: 18A0010

CAD FILE: 10-REMOVAL.DWG DESIGN BY: LEH 9/22/18 DRAWN BY: LDH 9/22/18

REVIEWED BY: KMS 10/15/18

SHEET TITLE

REMOVAL PLAN





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REHABILITATE TAXILANES TH/7, TH/7-1 & TH/7-2

IDA No: JOT-4671 SBGP No: 3-17-SBGP-144

JO024

| NO. | DATE | DES | CRIPT | ION |
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| NO. | DAIL | DES | DWN | REV |
| ISSUE: | Novemb | er 16, | 2018 | |
| PROJEC | CT NO: 1 | 8A001 | 0 | |
| CAD FIL | E: 11-DRAI | NAGE.DV | ٧G | |
| DESIGN | IBY: LFI | H 9/22 | 2/18 | |
| DRAWN | BY: LDI | H 9/22 | 2/18 | |

REVIEWED BY: KMS 10/15/18

SHEET TITLE

DRAINAGE PLAN

STORM SEWER SCHEDULE

| Structure | Station | Offse | t | Туре | Rim El. | lnv | ert El. | Pipe Pay Length | Size | Туре | Slope % |
|-----------|-----------|-------|----|--------------|---------|-----|---------|-----------------|------|--------------|---------|
| | | | | | | | | | | | |
| S2 | 688+00.89 | 0.0 | | Trench Drain | 580.22 | | 578.97 | | | | |
| | | | | | | | | 237.9 | 8.0 | Trench Drain | 0.50 |
| S3 | 685+62.95 | 0.0 | | Trench Drain | 580.22 | | 577.78 | | | | |
| | | | | | | | | 66.2 | 8.0 | PVC | 0.50 |
| S4 | 684+94.74 | 0.3 | LT | 4' Manhole | 580.13 | | 577.44 | | | | |
| | | | | | | | | | | | |

MANHOLE/INLET ADJUSTMENT SCHEDULE

| Existing Structure | New Grate Type (Neenah/East Jordan) or Approved Equal | Station | Offse | t | Existing Rim Elevation | Proposed RIm Elevation |
|--------------------|-------------------------------------------------------------|-----------|-------|----|------------------------|------------------------|
| S1 | R-2390-C/1040 Grate | 514+43.10 | 52.4 | LT | 579.54 | 579.12 |

STRUCTURE SCHEDULE

| Structure | Structure | Diameter | Frame Helght | Grate Dlameter | Frame Type | Cover/Grate |
|-----------|--------------------|----------|--------------|----------------|-------------------------------------------|-------------------------------------------|
| Number | Туре | D (in.) | (in.) | (in.) | (Neenah/East Jordan) or Approved Equal | (Neenah/East Jordan) or Approved Equal |
| | | | | | | |
| S4 | Manhole (Doghouse) | 60 | 9 | 32-3/8 | R-3492-A/1895 | Closed/Cover |

UNDERDRAIN SCHEDULE

| Structure | Station | Offse | t | Туре | Rim El. | Invert El. | Pay Length | Slope 9 |
|-----------|-----------|-------|------------|---------------------------|---------|------------|------------|---------|
| | | | | | | | | |
| U1 | 517+45.08 | 21.38 | RT | Cleanout | 580.06 | 578.48 | | |
| | | | | | | | 216.0 | 0.25 |
| U2 | 515+36.88 | 14.00 | RT | Inspection Hole | 579.76 | 577.94 | | |
| | | | | | | | 293.1 | 0.25 |
| U3 | 512+65.66 | 14.00 | RT | Meet Existing at Cleanout | | 577.21 | | |
| | | | | | | | | |
| | | | | | | | | |
| U4 | 688+12.69 | 5.00 | RT | Cleanout | 580.18 | 578.18 | | |
| | | | | | | | 313.6 | 0.30 |
| U5 | 684+99.10 | 5.00 | RT | RCP Connection | | 577.24 | | |
| | | | | | | | | |
| U6 | 515+59.47 | 24.68 | LT | Cleanout | 580,23 | 577.73 | | |
| | 010100.47 | 24,00 | | Gleanout | 000.20 | 077.70 | 97.8 | 0.41 |
| U7a | 514+16.51 | 30.35 | LT | RCP Connection | | 577.33 | 37.0 | 0.41 |
| O/a | 314.10.31 | 30.33 | | Kor connection | | 377.33 | | |
| | | | | | | | | |
| U8 | 513+98.49 | 61.63 | LT | Cleanout | 580.25 | 577.75 | | |
| | | | | | | | 39.6 | 1.06 |
| U7b | 514+16.51 | 30.35 | LT | RCP Connection | | 577.33 | | |
| | | | | | | | | |
| U9 | 513+49.14 | 61.65 | LT | Cleanout | 580.17 | 577.67 | | |
| | | | <u>-</u> - | | | | 89.4 | 0.47 |
| U10a | 512+83.38 | 29.15 | LT | RCP Connection | | 577.25 | | |
| | | | | | | | | |
| | | | | | | | | |
| U11 | 512+61.36 | 61.57 | LT | Cleanout | 580.11 | 577.61 | | |
| | | | | | | | 43.7 | 0.82 |
| U10b | 512+83.38 | 29.15 | LT | RCP Connection | | 577.25 | | |

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IDA No: JOT-4671 SBGP No: 3-17-SBGP-144

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| NO. | DATE | DES | CRIPT | |
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| 1,0. | DATE | DES | DWN | REV |
| SSUE: | Novemb | er 16, | 2018 | |

PROJECT NO: 18A0010
CAD FILE: 12-DRNSCH.DWG
DESIGN BY: LDH 9/22/18

DRAWN BY: LDH 9/22/18
REVIEWED BY: KMS 10/15/18

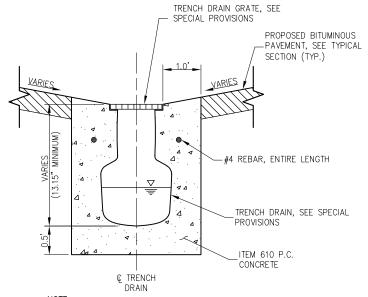
SHEET TITLE

STORM SEWER AND UNDERDRAIN SCHEDULE

<u>PLAN</u>

CONTRACTOR AND INCIDENTAL TO

TRENCH DRAIN.

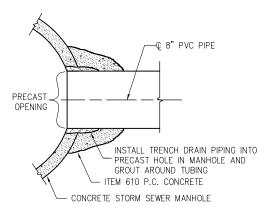


NOTE:

12 INCHES P.C. CONCRETE AROUND ALL SIDES OF TRENCH DRAIN, SEE PLAN.

SECTION B-B

TRENCH DRAIN

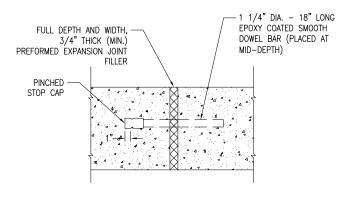


<u>NOTES</u>

1.HOLE FOR TRENCH DRAIN PIPING TO BE PRECAST INTO MANHOLE AT ELEVATION SPECIFIED IN TRENCH DRAIN SCHEDULE.

2.CONNECTIONS INCIDENTAL TO UNDERDRAIN.

TRENCH DRAIN CONCRETE COLLAR AND GROUT CONNECTION



NOTES:

- CONTRACTION JOINTS TO BE LOCATED AT NO GREATER THAN 12.5' ON CENTER. CONTRACTION JOINTS ARE TO BE EITHER 3/4" THICK PREFORMED JOINT FILLER, SAWED 2" DEEP AT 4 TO 24 HOURS OR FORMED WITH A 1/8" THICK STEEL TEMPLATE 2" DEEP.
- 2. EXPANSION JOINTS TO BE LOCATED AT 50' MAX SPACING.
- ALL EXPANSION AND SAWED CONTRACTION JOINTS SHALL BE SEALED WITH HOT-POURED SEALANT, ASTM D6690, TYPE II, COST INCIDENTAL TO TRENCH DRAIN.

EXPANSION JOINT DETAIL



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ISSUE: November 16, 2018

PROJECT NO: 18A0010

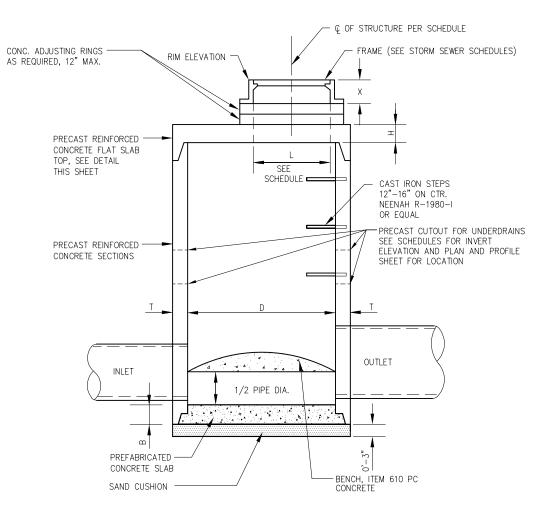
CAD FILE: 13-DRAINAGEDET.DWG

DESIGN BY: LDH 9/22/18
DRAWN BY: LDH 9/22/18

REVIEWED BY: KMS 10/15/18

SHEET TITLE

DRAINAGE DETAILS 01



MANHOLE DATA

NOTES

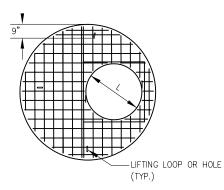
1. FOR "L" DIMENSION AND FRAME AND LID INFORMATION SEE STORM SEWER SCHEDULES.

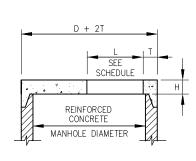
- 2. CENTER OF FRAME TO BE USED FOR LOCATING STRUCTURE. FOR STRUCTURE LOCATIONS AND ADDITIONAL INFORMATION SEE SCHEDULE.
- 3. ALL STRUCTURES TO BE PRECAST REINFORCED CONCRETE SECTIONS; BENCHES MAY BE CAST IN PLACE.
- 4. BLOCKOUTS FOR UNDERDRAIN CONNECTIONS AND FUTURE PIPES SHALL BE PRECAST INTO THE STRUCTURE.

| INSIDE DIA. "D" (IN.) | WALL THICKNESS "T" (IN.) | TOP THICKNESS "H" (IN.) | BOTTOM THICKNESS "B" (IN.) |
|--------------------------|-----------------------------|----------------------------|-------------------------------|
| 48 | 5 | 6 | 6 |
| 60 | 5 | 8 | 8 |
| 72 | 7 | 8 | 8 |
| 108 | 9 | 9 | 8 |

MANHOLE WITH FLAT SLAB TOP

(IDOT STANDARD 602401-MODIFIED)





NOTES

- 1. ADDITIONAL TOP AND BOTTOM BARS PLACED ADJACENT TO ACCESS HOLE.
- 2. MINIMUM 1" COVER ON STEEL BARS.
- 3. THREE LIFTING LOOPS OR HOLES.
- 4. MINIMUM STEEL REINFORCEMENT IN EACH DIRECTION TO BE WWF 1.06 SQ. IN./FT. IN ACCORDANCE WITH AASHTO M199 AND IDOT STANDARDS.
- 5. FOR "L" DIMENSION SEE STORM SEWER SCHEDULES.

PRECAST REINFORCED CONCRETE FLAT SLAB TOP

(IDOT STANDARD 602601)

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NO. DATE DESCRIPTION
DES DWN REV
ISSUE: November 16, 2018

PROJECT NO: 18A0010

CAD FILE: 13A-DRAINAGE DET.DWG

DESIGN BY: LDH 9/22/18

DRAWN BY: LDH 9/22/18

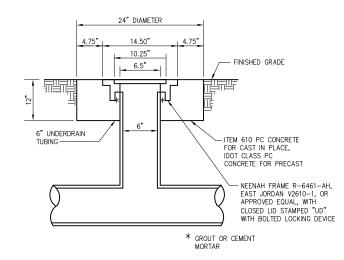
REVIEWED BY: KMS 10/15/18

SHEET TITLE

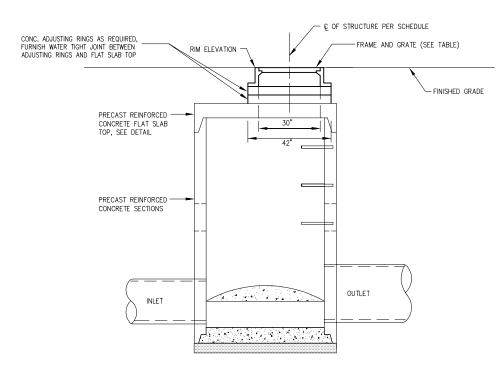
DRAINAGE DEATILS

- UNSUITABLE MATERIAL ENCOUNTERED DURING PLACEMENT OF BEDDING SHALL BE REMOVED AND REPLACED.
- WITHIN 3 FEET OF FUTURE PAVED AREA, GRANULAR BACKFILL IS TO BE USED INSTEAD OF EARTH BACKFILL.
- 3. AT CONTRACTOR'S OPTION IDOT CONTROLLED LOW STRENGTH MATERIAL WITH A HIGH EARLY STRENGTH, "FLASH FILL", MAY BE USED INSTEAD OF GRANULAR TRENCH BACKFILL UNDER PAVEMENTS.
- CA-7 BEDDING MAY BE PERMITTED IN CERTAIN CONDITIONS AS SPECIFIED IN STANDARD SPECIFICATIONS.

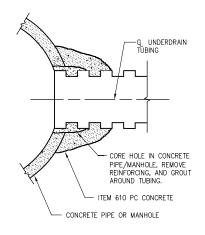
PIPE TRENCH



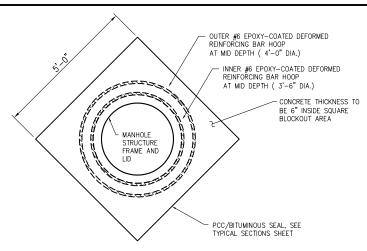
UNDERDRAIN INSPECTION HOLE



EXISTING MANHOLE ADJUSTMENT



STORM SEWER CONCRETE COLLAR AND GROUT CONNECTION



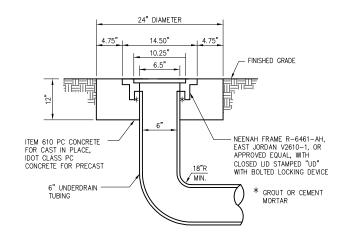
HOOP REINFORCEMENT REQUIRED AND SHALL BE ONE PIECE CONSTRUCTION HAVING A MINIMUM LAP LENGTH OF 2'-0".

CONCRETE NOTES

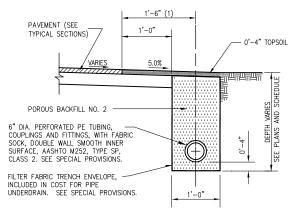
1. CONCRETE SHALL BE IDOT CLASS BS (4,000 PSI AT 14 DAYS), AIR ENTRAINED, SLUMP LIMITED TO 3 INCHES, COARSE AGGREGATE GRADATION CA-11.

ALL MATERIALS AND WORK TO BE PAID UNDER ITEM AR751943 ADJUST MANHOLE.

REINFORCING AT IN PAVEMENT MANHOLES



UNDERDRAIN CLEANOUT



UNDERDRAIN ALONG PAVEMENT EDGE



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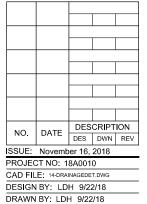
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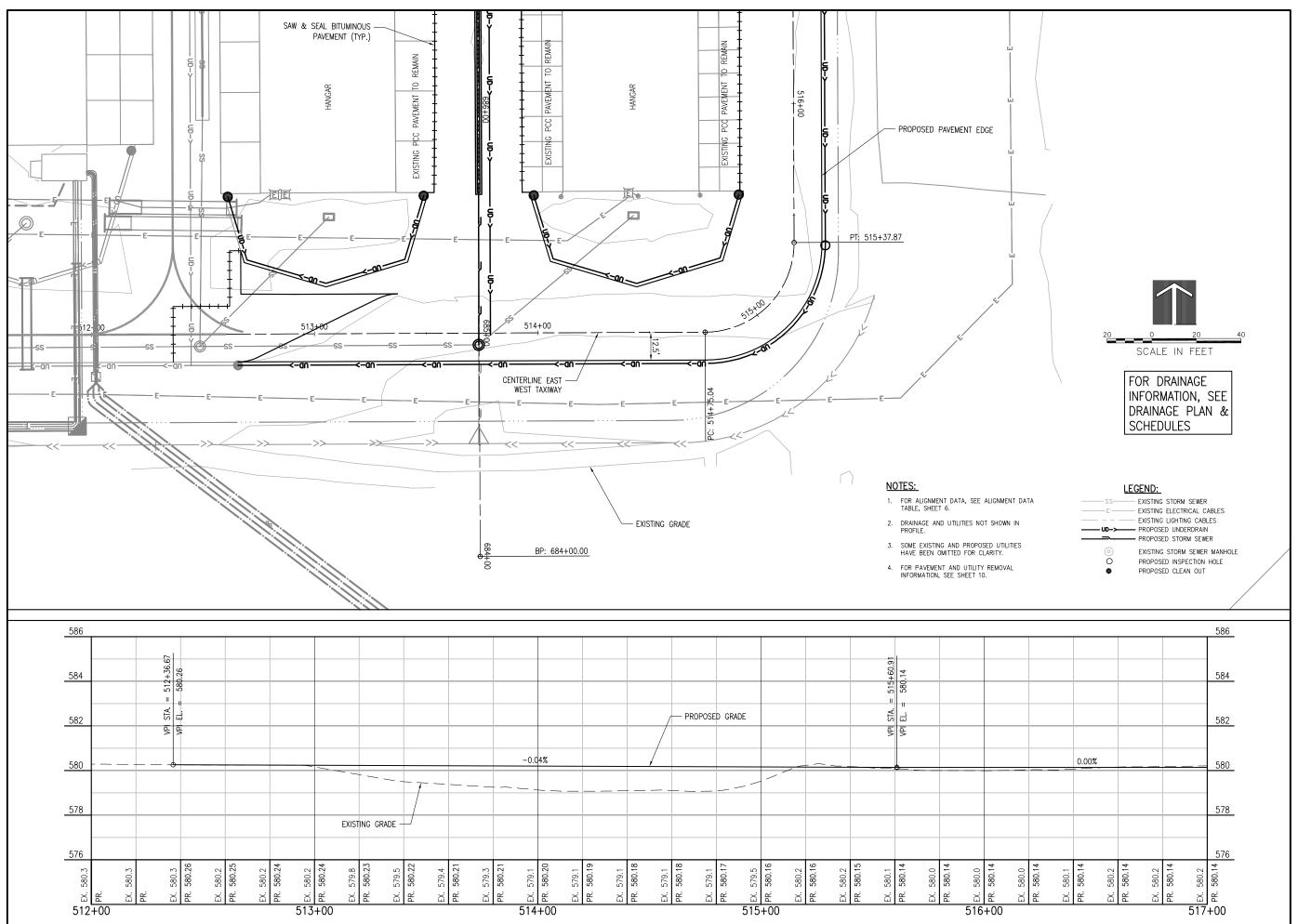
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DRAINAGE DETAILS

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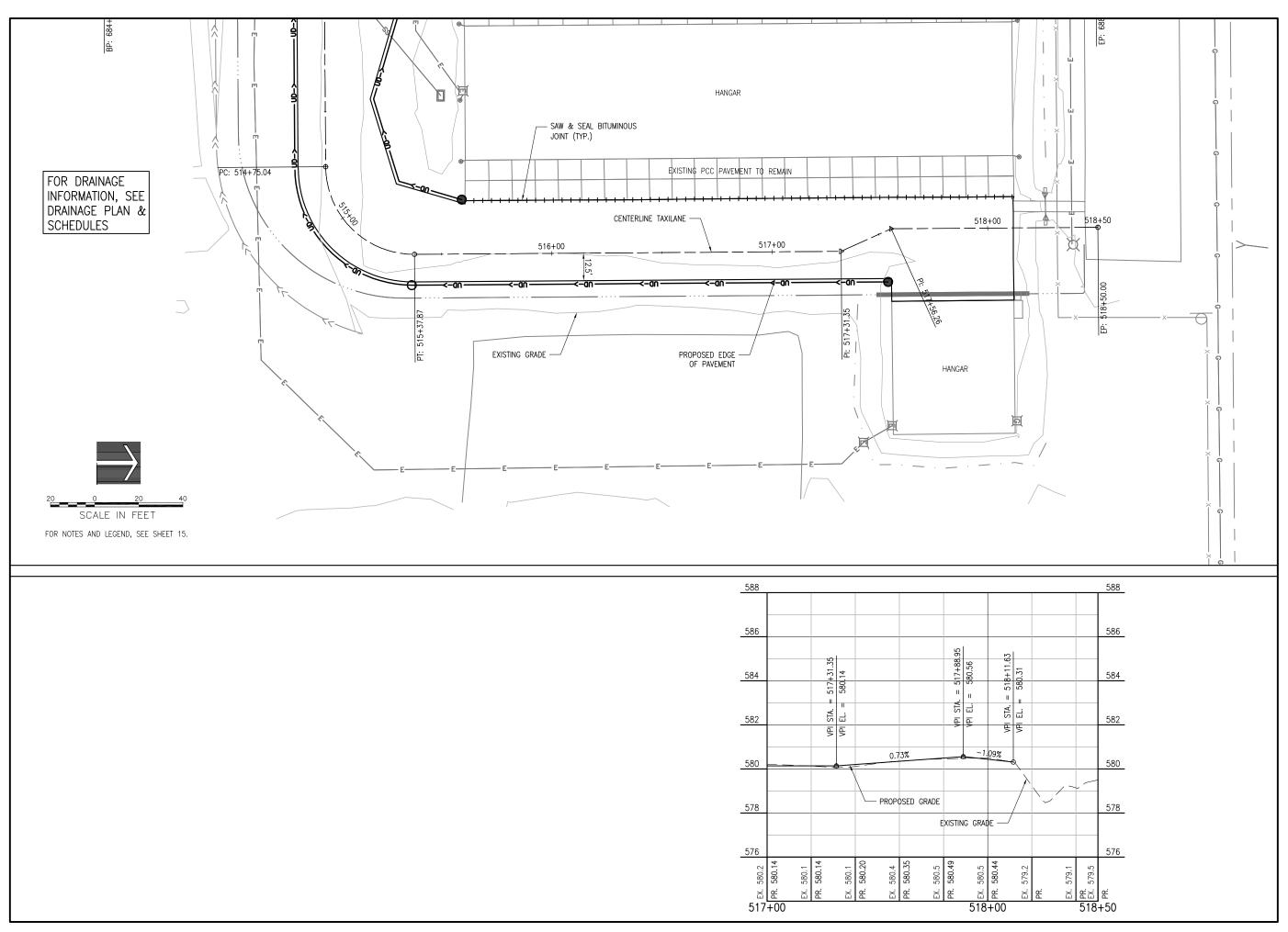
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ISSUE: November 16, 2018
PROJECT NO: 18A0010
CAD FILE: 15-PAP.DWG
DESIGN BY: LDH 9/28/18
DRAWN BY: LDH 9/28/18

PLAN & PROFILE -EAST WEST TAXIWAY

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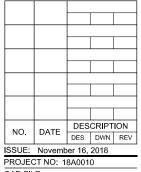
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IDA No: JOT-4671 SBGP No: 3-17-SBGP-144

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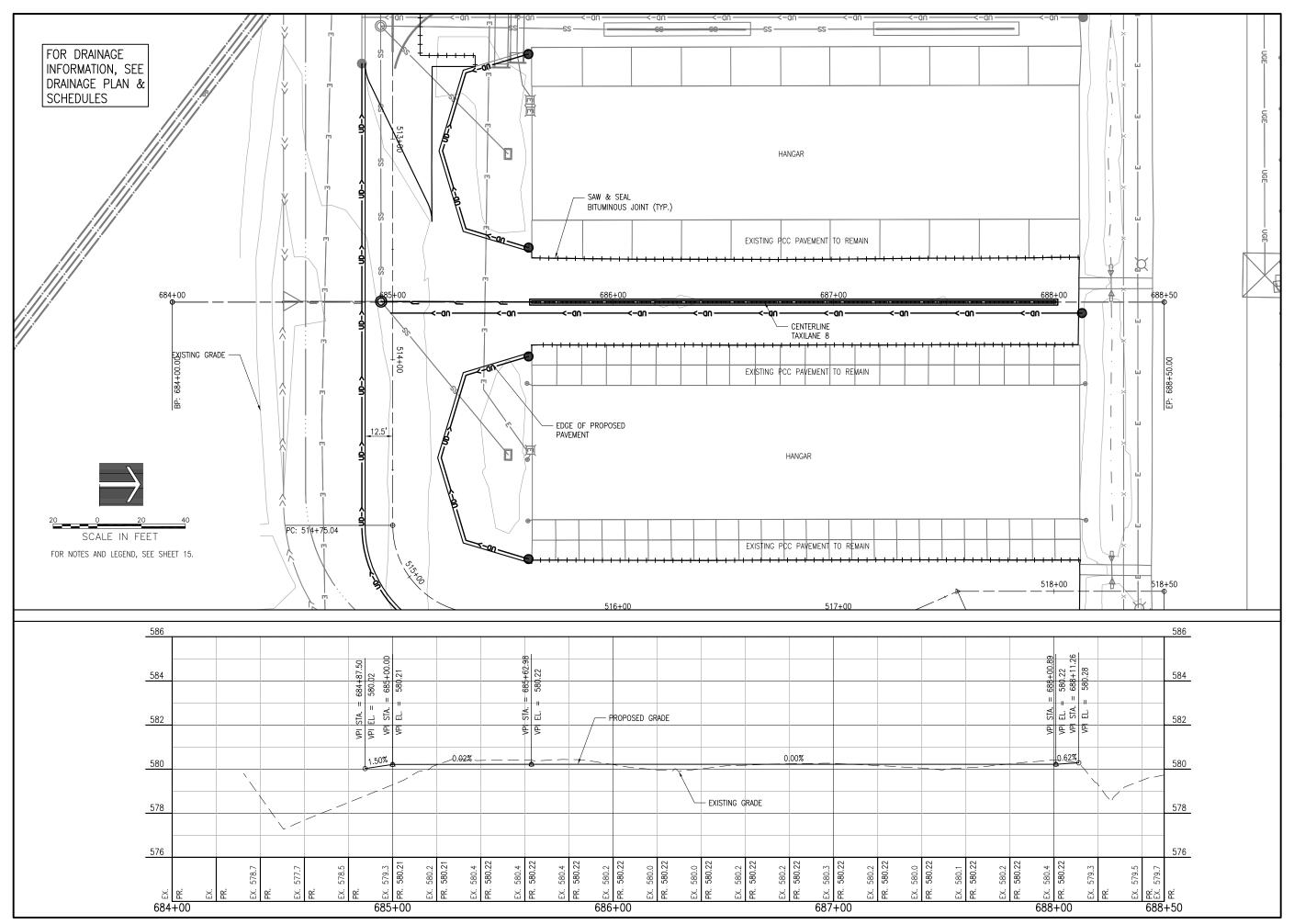


CAD FILE: 16-P&P.DWG
DESIGN BY: LDH 9/28/18
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SHEET TITLE

PLAN & PROFILE -EAST WEST TAXIWAY



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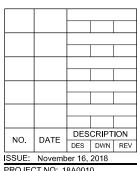
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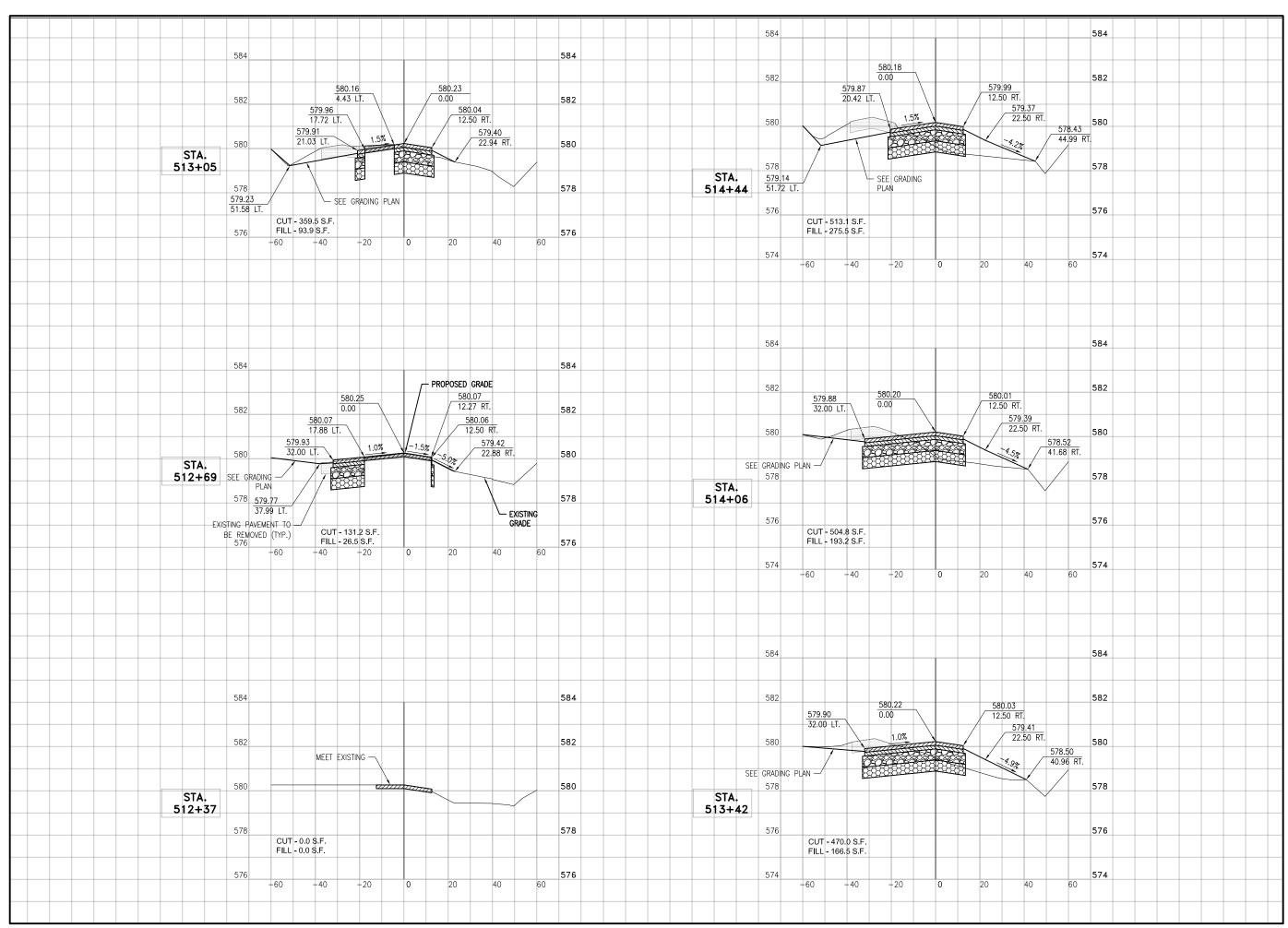


PROJECT NO: 18A0010
CAD FILE: 17-P8P.DWG
DESIGN BY: LDH 9/28/18

DRAWN BY: LDH 9/28/18
REVIEWED BY: KMS 10/15/18

SHEET TITLE

PLAN & PROFILE - TAXILANE 8





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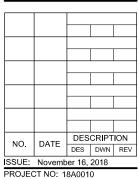
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REHABILITATE TAXILANES TH/7, TH/7-1 & TH/7-2

IDA No: JOT-4671 SBGP No: 3-17-SBGP-144

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PROJECT NO: 18A0010

CAD FILE: 18-SECTIONS.DWG

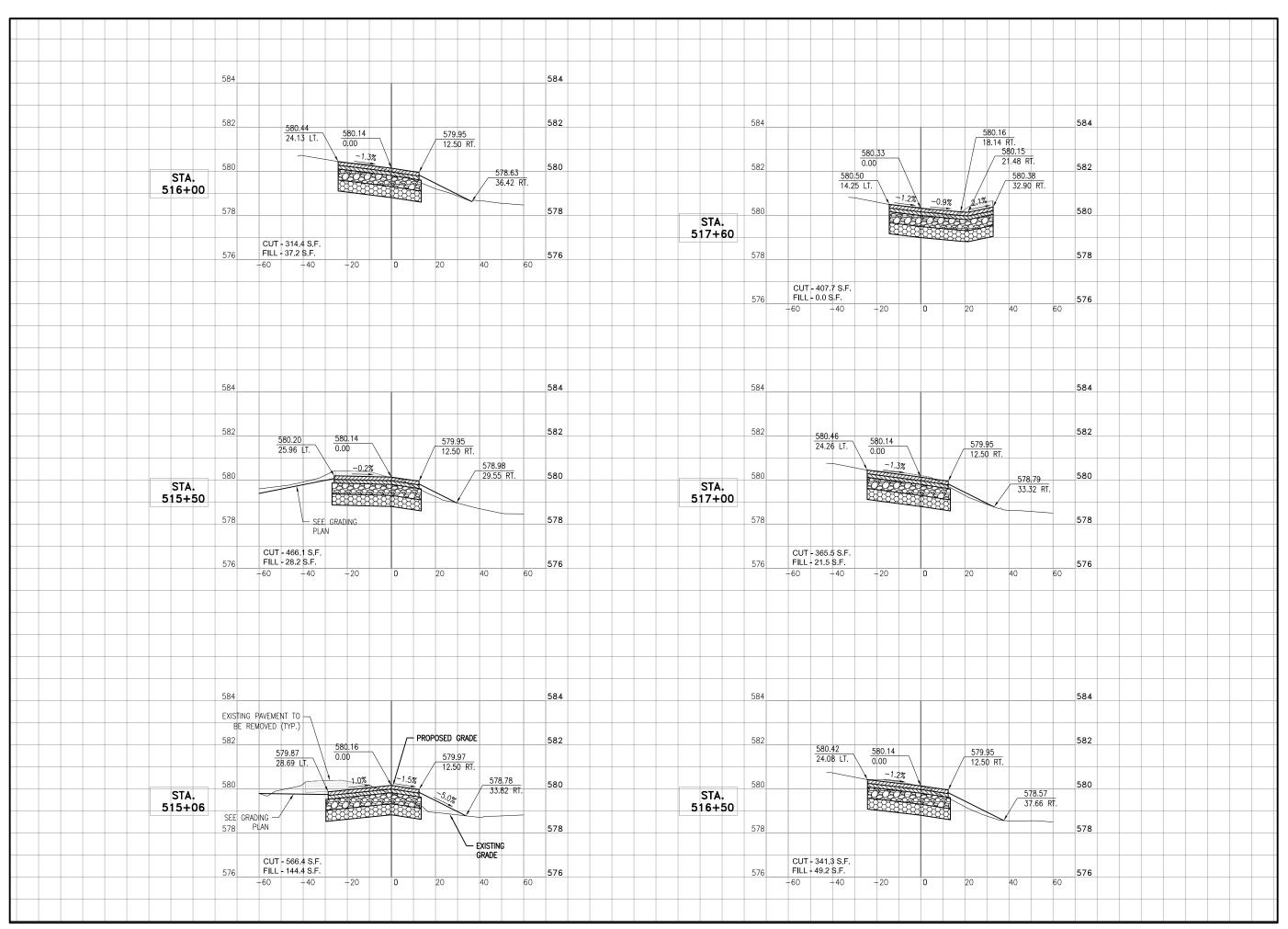
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DRAWN BY: LDH 10/1/18

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CROSS SECTIONS -EAST WEST TAXIWAY





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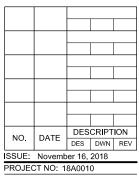
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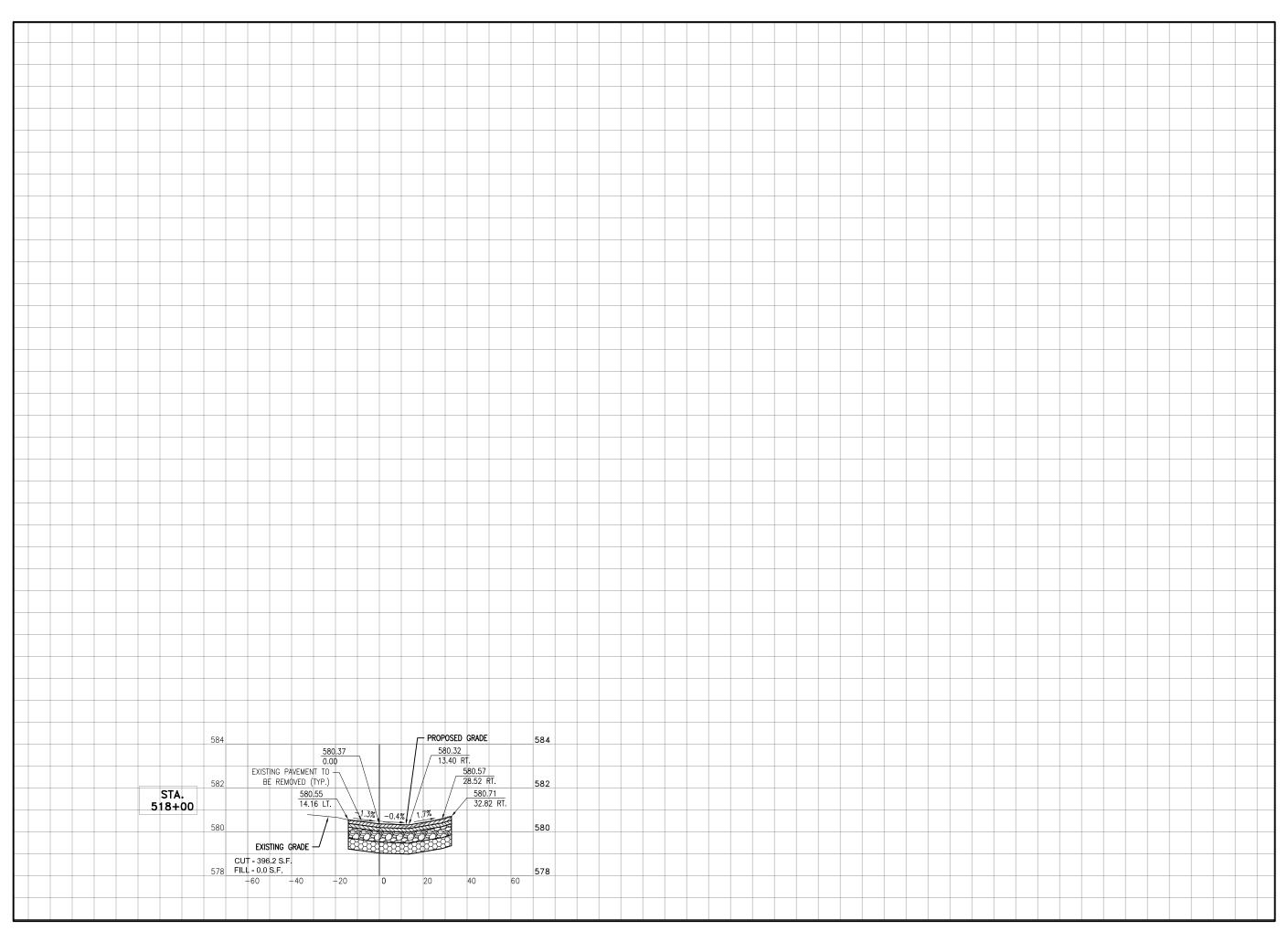


PROJECT NO: 18A0010
CAD FILE: 19-SECTIONS.DWG
DESIGN BY: LDH 10/1/18
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CROSS SECTIONS -EAST WEST TAXIWAY





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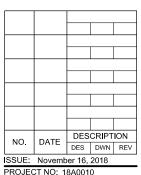
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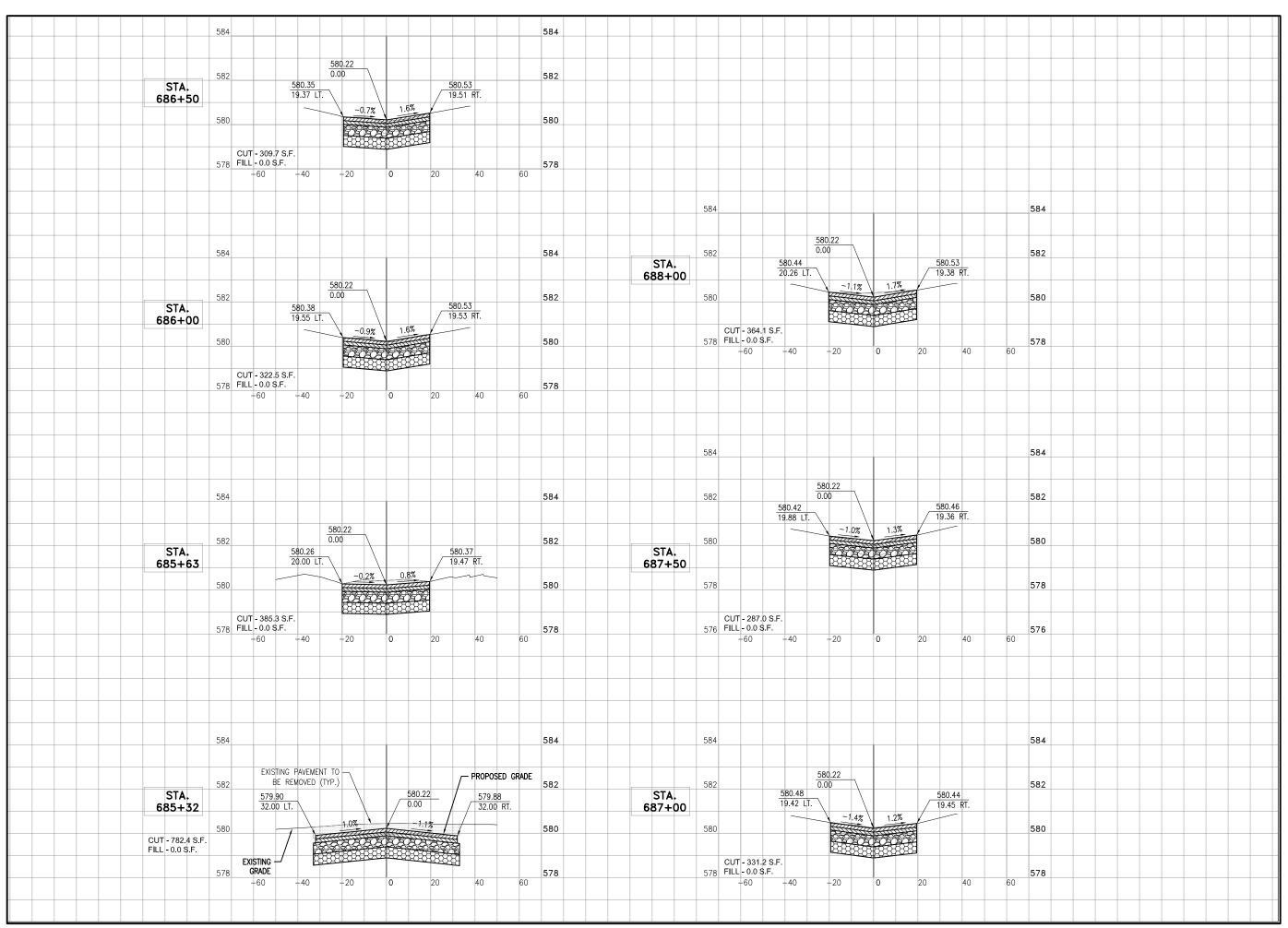
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DESIGN BY: LDH 10/1/18
DRAWN BY: LDH 10/1/18

DRAWN BY: LDH 10/1/18

REVIEWED BY: KMS 10/15/18

SHEET TITLE

CROSS SECTIONS -EAST WEST TAXIWAY





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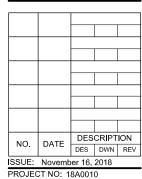
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IDA No: JOT-4671 SBGP No: 3-17-SBGP-144

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PROJECT NO: 18A0010

CAD FILE: 21-SECTIONS.DWG

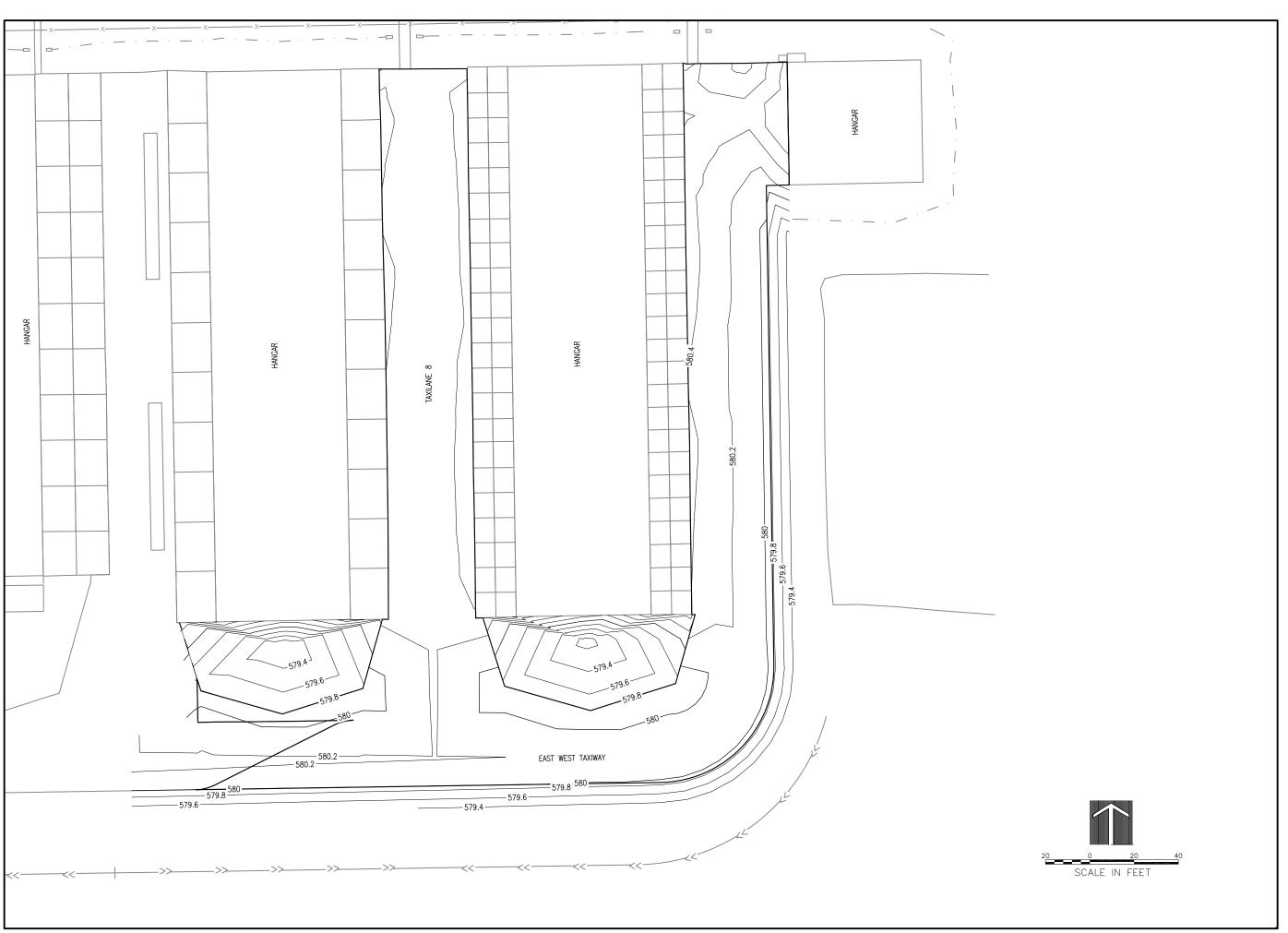
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DRAWN BY: LDH 10/1/18

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SHEET TITLE

CROSS SECTIONS - TAXILANE 8





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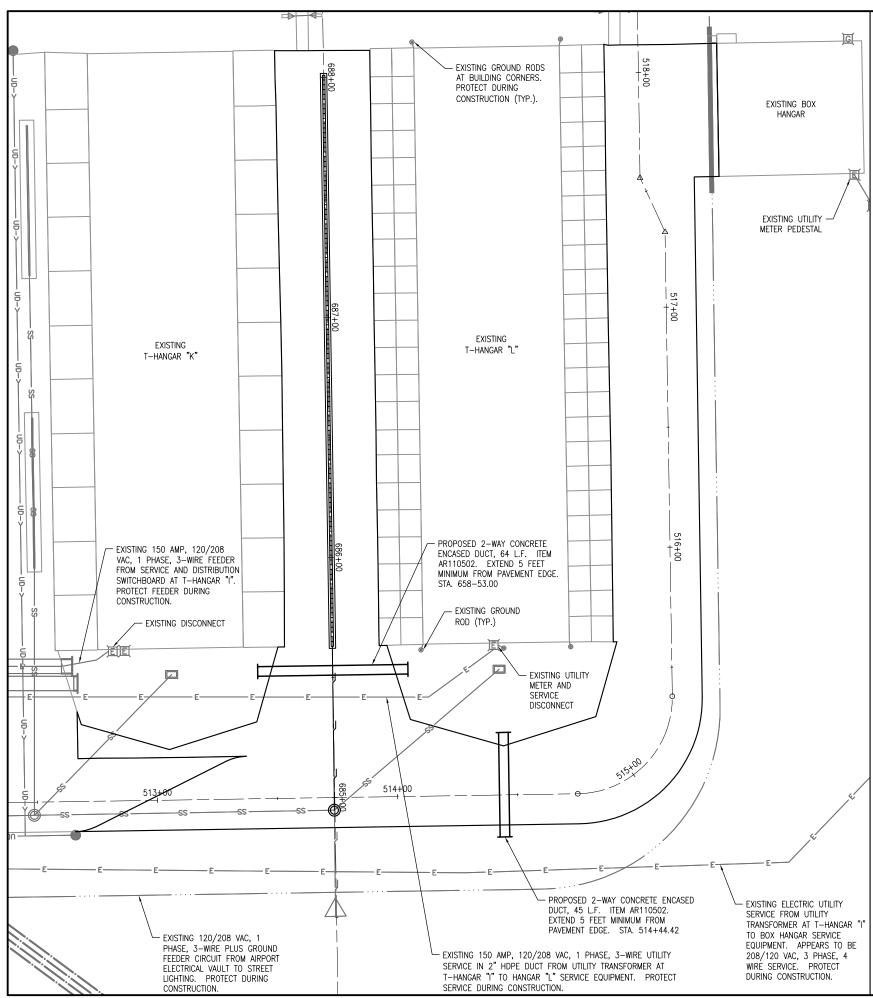
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DESIGN BY: LDH 10/1/18 DRAWN BY: LDH 10/1/18

REVIEWED BY: KMS 10/15/18

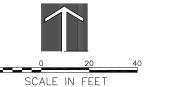
SHEET TITLE

GRADING PLAN



NOTES

- KEEP ALL WORK, POWER OUTAGES, AND/OR SHUT DOWN OF EXISTING SYSTEMS COORDINATED WITH THE AIRPORT MANAGER OR DESIGNATED REPRESENTATIVE. ONCE SHUT DOWN, THE CIRCUITS SHALL BE LABELED AS SUCH TO PREVENT ACCIDENTAL ENERGIZING OF THE RESPECTIVE CIRCUITS. ALL PERSONNEL SHALL FOLLOW U.S. DEPARTMENT OF LABOR OCCUPATIONAL SAFETY & HEALTH ADMINISTRATION (OSHA) 29 CFR PART 1910 OCCUPATIONAL SAFETY & HEALTH STANDARDS FOR ELECTRICAL SAFETY AND LOCKOUT/TAGOUT PROCEDURES INCLUDING, BUT NOT LIMITED TO 29 CFR SECTION 1910.147 THE CONTROL OF HAZARDOUS ENERGY (LOCKOUT/TAGOUT).
- 2. VERIFY EXISTING SITE CONDITIONS. CONTRACTOR SHALL FIELD VERIFY RESPECTIVE CIRCUITS AND POWER SOURCES PRIOR TO REMOVING, DISCONNECTING, ADJUSTING, CONNECTING, OR WORKING ON THE RESPECTIVE FACILITY, EQUIPMENT, OR OTHER DEVICE. CONTRACTOR SHALL LOCATE AND PROTECT EXISTING SERVICES, FEEDERS, LIGHTING CIRCUITS, ELECTRIC LINES, GROUNDING AND OTHER UTILITY LINES AND FACILITIES.
- IN AREAS WHERE THERE IS A CONGESTION OF CABLES OR WHERE THE PROPOSED DUCT OR CABLE CROSSES AN EXISTING CABLE, DUCT, LINE, OR OTHER UTILITY THE CONTRACTOR IS REQUIRED TO HAND DIG THE TRENCH NECESSARY FOR THE PROPOSED INSTALLATION. HAND DIGGING, TRENCHING AND/OR PLOWING WILL BE CONSIDERED INCIDENTAL TO THE PROPOSED WORK AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- THE CONTRACTOR IS REQUIRED TO FILL IN ALL HOLES AND DEPRESSIONS RESULTING FROM THE NEW WORK, WITH EARTH MATERIAL. THE AREAS SHALL BE COMPACTED TO PREVENT FUTURE SETTLEMENT AND TOPSOILED, SEEDED AND MULCHED IN ACCORDANCE WITH ITEMS 901, 905 AND 908.
- 5. NO CONNECTION TO AN ACTIVE POWER CIRCUIT WILL BE BROKEN UNTIL THE CIRCUIT HAS BEEN TURNED OFF IN ACCORDANCE



PROPOSED LEGEND

EXISTING ELECTRIC CABLES IN DUCT OR

PROPOSED CONCRETE ENCASED DUCT

EXISTING DIRECTIONAL BORE OR DIRECT BURY DUCT

EXISTING ELECTRICAL

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE AIRPORT OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE AIRPORT MANAGER OR DESIGNATED REPRESENTATIVE AND/OR THE RESIDENT ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY. CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123. CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. LOCATION OF FAA POWER CONTROL, AND COMMUNICATION CABLES SHALL BE COORDINATED WITH AND/OR LOCATED BY THE FAA. CONTACT AIRPORT MANAGER OR DESIGNATED REPRESENTATIVE FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVE GROUND UTILITIES



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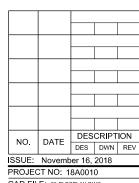
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REHABILITATE TAXILANES TH/7, TH/7-1 & TH/7-2

IDA No: JOT-4671 SBGP No: 3-17-SBGP-144

JO024

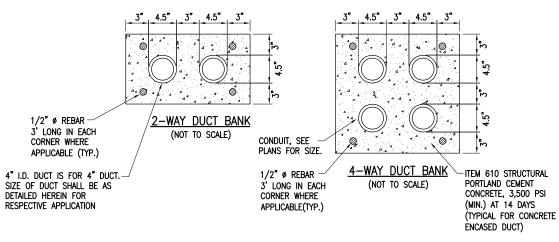


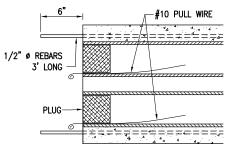
CAD FILE: 23-ELECPLAN.DWG DESIGN BY: LDH 9/22/18

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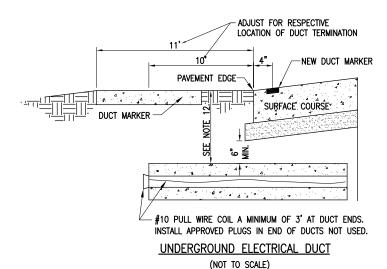
SHEET TITLE

PROPOSED ELECTRICAL PLAN





TYPICAL SECTION (NOT TO SCALE)



DUCT BANK NOTES:

- 1. DIMENSIONS FOR CONCRETE COVERAGE AND SEPARATION BETWEEN DUCTS ARE MINIMUM
- 2. INCLUDE DUCT SPACERS AS MANUFACTURED BY UNDERGROUND DEVICES INC., CARLON, OR APPROVED EQUAL TO MAINTAIN PROPER SEPARATION OF CONDUITS
- 3. PROVIDE REBAR WHERE APPLICABLE TO ACCOMMODATE INTERFACE OF CONCRETE ENCASED DUCT BANKS TERMINATING IN HANDHOLE OR MANHOLE. PROVIDE REBAR WHERE APPLICABLE TO EXTEND AN EXISTING CONCRETE DUCT BANK OR WHERE NECESSARY TO CONSTRUCT A DUCT BANK WITH MULTIPLE CONCRETE POURS AT DIFFERENT STAGES. PROVIDE REBAR REINFORCEMENT WHERE DUCT BANK IS LOCATED BELOW PAVEMENT. REBAR SHALL CONFORM TO THE REQUIREMENTS OF ASTM A 706, GRADE 60, OR ASTM A615, GRADE 60.

DUCT INSTALLATION NOTES

- ALL ELECTRICAL EQUIPMENT AND MATERIALS SHALL BE INSTALLED IN CONFORMANCE WITH NFPA 70 - NATIONAL ELECTRICAL CODE (NEC) MOST CURRENT ISSUE IN FORCE, THE RESPECTIVE EQUIPMENT MANUFACTURER'S DIRECTIONS AND ALL OTHER APPLICABLE LOCAL CODES, LAWS, ORDINANCES, AND REQUIREMENTS IN FORCE. ANY INSTALLATIONS WHICH VOID THE U.L. LISTING, INTERTEK TESTING SERVICES VERIFICATION/ETL LISTING (OR OTHER THIRD PARTY LISTING) AND/OR THE MANUFACTURER'S WARRANTY OF A DEVICE WILL NOT BE
- CONTRACTOR SHALL KEEP A COPY OF THE LATEST NEC IN FORCE ON SITE AT 2. ALL TIMES DURING CONSTRUCTION FOR USE AS A REFERENCE.
- CONTRACTOR SHALL COORDINATE WORK AND ANY POWER OUTAGES AND/OR SHUT DOWN OF SYSTEMS WITH THE RESPECTIVE FACILITY OWNER PERSONNEL AND THE AIRPORT MANAGER/DIRECTOR, ONCE SHUT DOWN, THE CIRCUITS SHALL BE LABELED AS SUCH TO PREVENT ACCIDENTAL ENERGIZING OF THE RESPECTIVE CIRCUITS. ALL PERSONNEL SHALL FOLLOW U.S. DEPARTMENT OF LABOR OCCUPATIONAL SAFETY & HEALTH ADMINISTRATION (OSHA) 29 CFR PART 1910 OCCUPATIONAL SAFETY & HEALTH STANDARDS FOR ELECTRICAL SAFETY AND LOCKOUT/TAGOUT PROCEDURES INCLUDING. BUT NOT LIMITED TO. 29 CFR SECTION 1910.147 THE CONTROL OF HAZARDOUS ENERGY (LOCKOUT/TAGOUT).
- THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND AND/OR ABOVEGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE OWNER'S REPRESENTATIVE AND/OR THE RESIDENT ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY DAMAGE TO SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT. ALL UTILITY CABLES AND LINES SHALL BE LOCATED BY THE RESPECTIVE UTILITY. CONTACT JULIE (JOINT UTILITY LOCATION INFORMATION FOR EXCAVATORS) FOR UTILITY INFORMATION, PHONE: 1-800-892-0123. CONTACT THE FAA (FEDERAL AVIATION ADMINISTRATION) FOR ASSISTANCE IN LOCATING FAA CABLES AND UTILITIES. ALSO CONTACT AIRPORT DIRECTOR/MANAGER AND AIRPORT PERSONNEL FOR ASSISTANCE IN LOCATING UNDERGROUND AIRPORT CABLES AND/OR UTILITIES. ALSO COORDINATE WORK WITH ALL ABOVEGROUND LITHLITIES
- ADJUSTMENTS TO DUCT BANK ROUTES MIGHT BE REQUIRED TO ACCOMMODATE EXISTING SITE CONDITIONS AND UNDERGROUND LINES AND UTILITIES. CONTRACTOR SHALL FIELD VERIFY EXISTING SITE CONDITIONS. CONTRACTOR SHALL COORDINATE DUCT ROUTE ADJUSTMENTS WITH THE RESIDENT ENGINEER/ RESIDENT TECHNICIAN AND THE AIRPORT MANAGER.
- CONTRACTOR SHALL LOCATE AND MARK ALL EXISTING CABLES, LINES, OR UTILITIES WITHIN 10 FT OF PROPOSED EXCAVATING/TRENCHING AREA. ANY CABLES, LINES, AND UTILITIES FOUND INTERFERING WITH PROPOSED EXCAVATION OR CABLE/TRENCHING SHALL BE HAND DUG AND EXPOSED. ANY DAMAGED CABLES OR OTHER UTILITIES SHALL BE IMMEDIATELY REPAIRED TO THE SATISFACTION OF THE RESPECTIVE OWNER'S REPRESENTATIVE AT THE CONTRACTOR'S EXPENSE. THE RESIDENT ENGINEER/RESIDENT TECHNICIAN AND OWNER SHALL BE NOTIFIED IMMEDIATELY IF ANY CABLES OR OTHER UTILITIES ARE
- PAYMENT FOR LOCATING AND MARKING UNDERGROUND UTILITIES AND CABLES WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED INCIDENTAL TO THE RESPECTIVE DUCT INSTALLATION.
- THE CONTRACTOR WILL DETERMINE IF THERE IS A CONFLICT BETWEEN THE INSTALLATION OF THE PROPOSED ELECTRICAL DUCTS AND ANY EXISTING UTILITIES. HE WILL MAKE ALL NECESSARY ADJUSTMENTS IN DEPTH OF INSTALLATION TO AVOID ANY AND ALL PROPOSED UNDERGROUND IMPROVEMENTS
- CONDUITS FOR DIRECT BURIAL OR CONCRETE ENCASED DUCT BANK SHALL BE SCHEDULE 40 PVC CONDUIT, UL-LISTED, RATED FOR 90°C CABLE-CONFORMING TO NEMA STANDARD TC-2 AND UL 651. LISTED SUITABLE FOR UNDERGROUND USE EITHER DIRECT-BURIED OR ENCASED IN CONCRETE, OR SCHEDULE 40 (MINIMUM) HDPE CONDUIT, UL LISTED, CONFORMING TO NEMA STANDARD TC-7 AND UL 651B AND LISTED SUITABLE FOR UNDERGROUND USE; EITHER DIRECT BURY OR ENCASED IN CONCRETE.

- 10. CONDUITS FOR DIRECTIONAL BORING SHALL BE SCHEDULE 40 PVC CONDUIT OR SCHEDULE 80 PVC CONDUIT, UL-LISTED, RATED FOR 90°C CABLE-CONFORMING TO NEMA STANDARD TC-2 AND UL 651 AND SUITABLE FOR DIRECTIONAL BORING INSTALLATION, SCHEDULE 80 HDPE CONDUIT, UL-LISTED, CONFORMING TO NEMA STANDARD TC-7 AND UL 651B AND SUITABLE FOR DIRECTIONAL BORING INSTALLATION, OR WALL TYPE SDR 13.5 OR SDR 11 HDPE CONDUIT MANUFACTURED IN ACCORDANCE WITH ASTM D-3350 (SPECIFICATION OF POLYETHYLENE PLASTICS PIPE AND FITTINGS MATERIALS) AND ASTM F2160 (STANDARD SPECIFICATION FOR SOLID WALL, HIGH-DENSITY POLYETHYLENE CONDUIT BASED ON CONTROLLED OUTSIDE DIAMETER), AND SUITABLE FOR DIRECTIONAL BORING INSTALLATION. PER NEC 300.5 (K), RACEWAYS INSTALLED USING DIRECTIONAL BORING EQUIPMENT SHALL BE APPROVED FOR THE PURPOSE.
- 11 INSTALLATION OF CONDUIT AND DUCTS SHALL CONFORM TO ITEM 110 AIRPORT UNDERGROUND ELECTRICAL DUCT BANKS AND CONDUITS.
- 12. MINIMUM COVER REQUIREMENTS TO TOP OF ELECTRICAL DUCT ENCASEMENT SHALL BE AS FOLLOWS:
 - -18 IN. MIN. IN TURF AREAS AT AIRPORTS OR ADJACENT AREAS WHERE TRESPASSING IS PROHIBITED, AND NOT SUBJECT TO FARMING. -42 IN. MIN. IN AREAS SUBJECT TO FARMING.
 - -30 IN. MIN. WHERE LOCATED BELOW PAVEMENT OR ROADWAY.

WHERE DETAILED ON THE PLANS OR WHERE REQUIRED TO AVOID OBSTRUCTIONS, DUCTS SHALL BE BURIED DEEPER. COVER IS DEFINED AS THE SHORTEST DISTANCE IN INCHES MEASURED BETWEEN A POINT ON THE TOP SURFACE OF ANY DIRECT-BURIED CONDUCTOR, CABLE, CONDUIT, OR OTHER RACEWAY AND THE TOP SURFACE OF FINISHED GRADE, CONCRETE OR SIMILAR COVER.

- 13. WHERE CONCRETE-ENCASED DUCT INTERFACES TO AN ELECTRICAL HANDHOLE OR MANHOLE, THE CONCRETE ENCASEMENT SHALL BE INSTALLED UP TO THE RESPECTIVE HANDHOLE OR MANHOLE. PROVIDE BUSHINGS OR BELLS AT CONDUIT TERMINATIONS IN FLECTRICAL HANDHOLES OR MANHOLES
- 14. UNDERGROUND DUCTS INSTALLED BY DIRECTIONAL-BORING METHOD SHALL BE INSTALLED IN A MANNER THAT WILL NOT DAMAGE ANY EXISTING UNDERGROUND UTILITIES, AND SHALL NOT DISTURB OR DAMAGE THE RESPECTIVE PAVEMENT OR ROADWAY SURFACE. DUCTS SHALL BE DIRECTIONAL-BORED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION PLANS. THE DUCTS WILL BE BORED AT A MINIMUM DEPTH OF 42 IN. BELOW THE RESPECTIVE PAVEMENT IT IS BEING
- 15. A PULL WIRE SHALL BE INSTALLED IN EACH CONDUIT OR DUCT TO BE LEFT VACANT.
- 16. HIGH VOLTAGE CIRCUITS (AIRFIELD LIGHTING 5000 VOLT SERIES CIRCUITS AND/OR OTHER CIRCUITS RATED ABOVE 600 VOLTS) AND LOW VOLTAGE CIRCUITS (RATED 600 VOLTS AND BELOW) SHALL NOT BE INSTALLED IN THE SAME RACEWAY, CONDUIT, DUCT, HANDHOLE, OR MANHOLE.
- 17. CONTROL CABLES SHALL BE RUN IN SEPARATE DUCTS FROM POWER CABLES. COMMUNICATION CABLES SHALL BE RUN IN SEPARATE DUCTS FROM POWER
- 18. HOMERUN CABLES FOR A RESPECTIVE CIRCUIT SHALL BE INSTALLED IN THE SAME RACEWAY OR DUCT
- 19. COORDINATE DUCT INTERFACE TO MANHOLES AND HANDHOLES. FIELD CUT OPENINGS FOR CONDUITS AND DUCTS TO INTERFACE TO MANHOLES AND/OR HANDHOLES. CUT WALL OF RESPECTIVE HANDHOLE OR MANHOLE WITH A TOOL DESIGNED FOR MATERIAL TO BE CUT. SIZE HOLES FOR RESPECTIVE DUCTS. CONDUITS, AND TERMINATION FITTINGS AND SEAL AROUND PENETRATIONS. ALL CORING, INTERFACE, CUTTING, AND SEALING WILL BE CONSIDERED INCIDENTAL TO THE RESPECTIVE DUCT INSTALLATION AND/OR RESPECTIVE HANDHOLE/MANHOLE
- 20. CONTRACTOR SHALL COORDINATE DUCT MARKING WITH AIRPORT.
- 21. ALL POWER AND CONTROL CABLES IN HANDHOLES, MANHOLES, AND JUNCTION BOXES SHALL BE TAGGED TO IDENTIFY THE RESPECTIVE CABLE. A MINIMUM OF TWO TAGS SHALL BE PROVIDED ON EACH CABLE IN A MANHOLE; ONE AT THE CABLE ENTRANCE AND ONE AT THE CABLE EXIT. CABLE TAGS SHALL BE STAMPED BRASS TAGS OR OTHER WEATHERPROOF/WATERPROOF CORROSION RESISTANT MATERIAL



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IDA No: JOT-4671 SBGP No: 3-17-SBGP-144

JO024

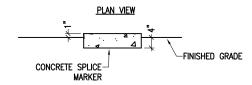
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CAD FILE: 24-E-501-ELEC.DWG DESIGN BY: KNI 9/27/18 DRAWN BY: LDH 9/28/18

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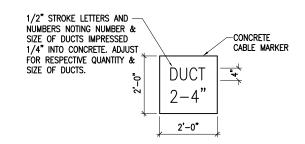
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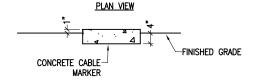
DUCT BANK DETAILS AND NOTES



SECTION VIEW

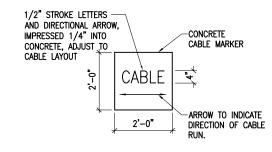
TURF CABLE MARKERS "NOT TO SCALE"

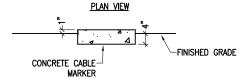




SECTION VIEW

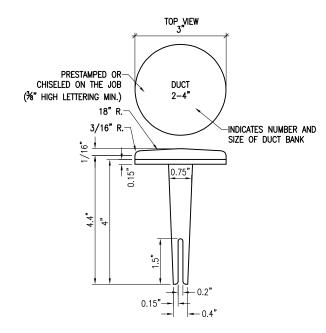
TURF CABLE MARKERS "NOT TO SCALE"





SECTION VIEW

TURF CABLE MARKERS "NOT TO SCALE"



BITUMINOUS PAVEMENT DUCT MARKERS
"NOT TO SCALE"

NOTE:

- TOP OF MARKER SHALL BE FLUSH WITH FINISHED PAVEMENT SURFACE.
 MARKER MAY BE INSTALLED IN A DRILLED HOLE AND SECURED WITH
 FROMY CLUF.

 TOP OF MARKER SHALL BE FLUSH WITH FINISHED PAVEMENT SURFACE.

 MARKER MAY BE INSTALLED IN A DRILLED HOLE AND SECURED WITH

 FROMY CLUF.

 TOP OF MARKER SHALL BE FLUSH WITH FINISHED PAVEMENT SURFACE.

 MARKER MAY BE INSTALLED IN A DRILLED HOLE AND SECURED WITH

 FROM YOUR PARENT SHALL BE FLUSH WITH FINISHED PAVEMENT SURFACE.

 MARKER MAY BE INSTALLED IN A DRILLED HOLE AND SECURED WITH

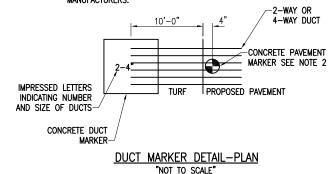
 FROM YOUR PARENT SHALL BE FLUSH WITH FINISHED PAVEMENT SURFACE.

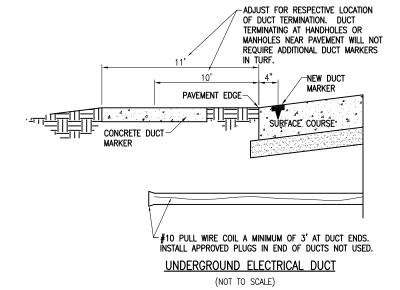
 MARKER MAY BE INSTALLED IN A DRILLED HOLE AND SECURED WITH

 FROM YOUR PARENT SHALL BE FLUSH WITH FINISHED PAVEMENT SURFACE.

 MARKER MAY BE INSTALLED IN A DRILLED HOLE AND SECURED WITH

 FROM YOUR PARENT SHALL BE FLUSH WITH FINISHED PAVEMENT SHALL BE FROM YOUR PARENT SHALL BE FROM YOUR PARENT
- BRASS DUCT MARKERS ARE AVAILABLE FROM G&S FOUNDRY & MANUFACTURING CO. INC., 210 KASKASKIA DRIVE, RED BUD, IL 62278, PHONE: (618)–282–4114, SURV-KAP, 3225 E. 47TH ST., TUCSON, AZ 85713, PHONE: (502)–622–6011, OR OTHER EQUIVALENT





CABLE & DUCT MARKER NOTES:

- THE COST OF ALL TURF AND PAVEMENT DUCT MARKERS SHALL BE INCIDENTAL TO THE DUCT. THE COST OF ALL CABLE MARKERS SHALL BE INCIDENTAL TO THE CABLE.
- BITUMINOUS PAVEMENT DUCT MARKER AND CONCRETE DUCT MARKER TO BE PROVIDED AT EACH END OF EACH DUCT AS SHOWN ON THE LOCATION PLAN. FOR CONCRETE PAVEMENT, THE LETTER "D" SHALL BE IMPRESSED IN THE PAVEMENT INSTEAD OF THE MARKER. THE LETTER SHALL BE INFORMED AS DESCRIBED IN NOTE 4.
- 3. UNDERGROUND CABLE RUNS MUST BE IDENTIFIED BY CABLE MARKERS AT 200 FEET (61 M) MAXIMUM SPACING WITH AN ADDITIONAL MARKER AT EACH CHANGE OF DIRECTION OF THE CABLE RUN. CABLE MARKERS MUST BE INSTALLED ABOVE THE CABLE. CABLE MARKERS ARE NOT REQUIRED FOR CABLE RUNS BETWEEN RUNWAY/TAXIWAY EDGE LIGHTS.
- 4. CONCRETE CABLE MARKERS AND DUCT MARKERS SHALL HAVE LETTERS 4" HIGH, 3" WIDE WITH WIDTH OF STROKE ½" AND ¾" DEEP. ALL LETTERS, NUMBERS AND ARROWS TO BE IMPRESSED.
- EMPLOY THE FOLLOWING METHODS WHERE ADDITIONAL SPACE TO FIT THE LEGEND IS REQUIRED:
 - A. REDUCE LETTER SIZE TO 3" HIGH, 2" WIDE.
 - B. INCREASE THE MARKER SIZE TO 30" X 30".
 - C. PROVIDE ADDITIONAL MARKERS PLACED SIDE BY SIDE.
- 6. TURF DUCT MARKERS ARE NOT REQUIRED AT PAVEMENT CROSSINGS WHERE DUCTS TERMINATE IN HANDHOLES, OR JUNCTION STRUCTURES.
- 7. LOCATION OF ALL DIRECT EARTH BURIAL UNDERGROUND CABLE SPLICE/CONNECTIONS, EXCEPT THOSE AT ISOLATION TRANSFORMERS, MUST BE IDENTIFIED BY SPLICE MARKERS. SPLICE MARKERS MUST BE PLACED ABOVE THE SPLICE/CONNECTIONS. DIRECT EARTH BURIAL UNDERGROUND CABLE SPLICES SHALL BE AVOIDED WHETER POSSIBLE. CABLE SPLICES SHALL BE LOCATED IN SPLICE CANS, LIGHT BASES, HANDHOLES, MANHOLES, OR OTHER JUNCTION STRUCTURES UNLESS OTHERWISE APPROVED BY THE PROJECT ENGINEER.
- 8. THE CABLE AND SPLICE MARKERS MUST IDENTIFY THE CIRCUITS TO WHICH THE CABLES BELONG. FOR EXAMPLE: RWY 13-31, TAXIWAY, T-HGR K, T-HGR L.
- LOCATIONS OF ENDS OF ALL UNDERGROUND DUCTS MUST BE IDENTIFIED BY DUCT MARKERS.

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CABLE AND DUCT MARKER DETAILS

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DESIGN BY KNI 9/27/18

DRAWN BY: LDH 9/28/18

REVIEWED BY: KMS 10/15/18

COMPOUND FILLER

UNDERGROUND CABLE

SPEC. L-824, TYPICAL

HEAT SHRINKABLE TUBING WITH INTERNAL

TYPE B

FOR SPLICES AT JUNCTION OF HOMERUN
WITH LOOP CIRCUIT AND FOR SPLICES IN
HOMERUNS TO EXISTING CABLES

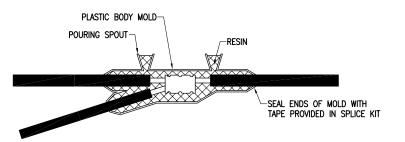
-L-823 RECEPTACLE END -L-823 PLUG END

AFTER SHRINKING

ADHESIVE. HEAT SHRINKABLE TUBING SHALL EXTEND FROM THE CABLE TO AT LEAST AN INCH PAST THE COMPLETED CONNECTION. -ADDITIONAL ADHESIVE L-823 RECEPTACLE END-COMPOUND FILLER 2" (TYP.) AFTÈR SHRINKING -823 PLUG END -WRAP WITH AT LEAST ONE LAYER OF RUBBER OR FACTORY MOLDED SYNTHETIC RUBBER TAPE AND ONE LAYER OF TRANSFORMER LEADS-PLASTIC TAPE, ONE-HALF LAPPED, EXTENDED AT LEAST 1-1/2 INCHES ON EACH SIDE OF JOINT HEAT SHRINKABLE TUBING WITH INTERNAL ADHESIVE. HEAT SHRINKABLE TUBING SHALL EXTEND FROM THE CABLE TO AT LEAST AN INCH PAST THE COMPLETED CONNECTION. AFTÈR SHRINKING TYPE C L-823 PLUG END--ADDITIONAL ADHESIVE FOR SPLICES AT RUNWAY COMPOUND FILLER AND TAXIWAY LIGHTS AND TAXI SIGNS L-823 RECEPTACLE END INSIDE DIAMETER OF CONNECTOR SHALL PROPERLY

CABLE SPLICES
(NOT TO SCALE)

MATCH THE OUTSIDE DIAMETER OF CABLE.

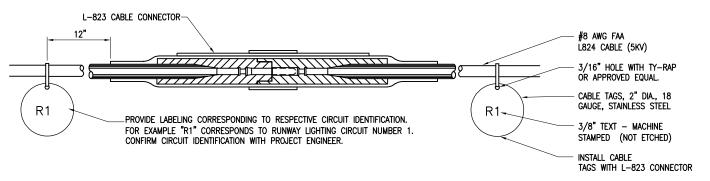


LOW VOLTAGE UNDERGROUND TAP SPLICE

FOR TAP SPLICES IN LOW VOLTAGE (600V) CABLE. SPLICES SHALL BE RATED AND LISTED SUITABLE FOR DIRECT BURIAL LOCATIONS. FOR SPLICES UP TO #2 AWG CONDUCTOR, SPLICES SHALL BE WYE RESIN TYPE POWER CABLE TAP SPLICE KIT SUITABLE FOR THE RESPECTIVE CABLES AND RESPECTIVE APPLICATION.

NOTE

- SPLICE DETAILS ARE PROVIDED FOR NEW WORK AND TO ASSIST IN REPAIRS OF ACCIDENTAL OR UNEXPECTED INTERRUPTIONS AND/OR CUTS TO AIRFIELD LIGHTING CABLES.
- CONTRACTOR SHALL KEEP ON HAND A MINIMUM OF 10 SETS OF SPLICE KITS FOR L-823 CONNECTORS AND A MINIMUM OF 10 SETS OF TYPE A LOW VOLTAGE SPLICE KITS TO ACCOMMODATE REPAIRS.
- EVERY AIRFIELD LIGHTING CABLE SPLICER SHALL BE QUALIFIED IN MAKING CABLE SPLICES AND TERMINATIONS ON CABLES RATED AT AND/OR ABOVE 5,000 VOLTS AC TO COMPLY WITH THE REQUIREMENTS OF FAA AC 150/5370-10G ITEM L-108.
- 4. WHEN PREPARING CABLE FOR SPLICES, THE CONTRACTOR SHALL USE A CABLE STRIPPER/PENCILLER WHENEVER CABLE CONNECTIONS ARE MADE.
- INSIDE DIAMETER OF RESPECTIVE CABLE CONNECTOR SHALL PROPERLY MATCH OUTSIDE DIAMETER OF CABLE.
- 6. WRAP ALL PRIMARY AND SECONDARY POWER CONNECTIONS WITH SUFFICIENT LAYERS OF HIGH VOLTAGE ELECTRICAL INSULATING TAPE (RUBBER SPLICING TAPE SUITABLE FOR PRIMARY ELECTRICAL INSULATION FOR SPLICING CABLE FROM 600 VOLTS TO 69,000 VOLTS) AND COVER WITH VINYL ELECTRICAL TAPE (ALL—WEATHER VINYL INSULATING TAPE SUITABLE FOR PROTECTIVE JACKETING FOR HIGH—VOLTAGE CABLE SPLICES AND REPAIRS) FOR FULL VALUE OF CABLE INSULATION VOLTAGE. PER ILLINOIS STANDARD SPECIFICATIONS FOR CONSTRUCTION OF AIRPORTS ITEM 108, ITEM 125, AND FAA AC 150/5370-10G ITEM L-108, HIGH VOLTAGE ELECTRICAL INSULATING TAPE SHALL BE 3M SCOTCH 23, 3M SCOTCH 130C OR APPROVED EQUIVALENT, AND VINYL ELECTRICAL TAPE SHALL BE 3M SCOTCH 88 OR APPROVED EQUIVALENT. TAPES MUST BE RATED SUITABLE FOR THE APPLICATION.
- PROVIDE CABLE TAGS TO IDENTIFY THE RESPECTIVE CIRCUITS ALL POINTS OF ACCESS INCLUDING L-867 BASES, L-868 BASES, HANDHOLES, MANHOLES, JUNCTION BOXES, AND WIREWAYS.
- 8. CONNECTION OF CONDUCTORS MUST BE MADE BY USING CRIMP CONNECTORS AND A CRIMPING TOOL APPROVED BY THE CONNECTOR/LUG MANUFACTURER. THE TOOL MUST PRODUCE A COMPLETE CRIMP BEFORE IT CAN BE REMOVED. FOR THE L-823 CONNECTORS, THE CRIMPING TOOL USED MUST BE LISTED BY THE L-823 KIT MANUFACTURER. MAKE THE NUMBER AND TYPE OF CRIMPS PER THE KIT MANUFACTURER'S INSTRUCTIONS.



- 1. CONTRACTOR SHALL PROVIDE CABLE CIRCUIT IDENTIFICATION MARKERS ATTACHED TO BOTH SIDES OF EACH CABLE CONNECTION.
- 2. CABLE IDENTIFICATION TAGS SHALL BE STAINLESS STEEL OR BRASS.
- 3. THE CABLE SHALL THOROUGHLY BE CLEANED PRIOR TO THE INSTALLATION OF THE L-823 CONNECTOR KIT.
- 4. ATTACH EACH CABLE TIE ENOUGH TO HOLD IN PLACE WITHOUT COMPRESSING EDGE OF CABLE TAG INTO CONDUCTOR. TRIM OFF EXCESS CABLE TIE.
- 5. CABLE TAGS SHALL BE PROVIDED AT ALL POINTS OF ACCESS INCLUDING L-867 BASES, L-868 BASES, HANDHOLES, MANHOLES, JUNCTION BOXES, AND WIREWAYS.

"NOT TO SCALE"

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REHABILITATE TAXILANES TH/7, TH/7-1 & TH/7-2

IDA No: JOT-4671 SBGP No: 3-17-SBGP-144

JO024

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ISSUE: November 16, 2018

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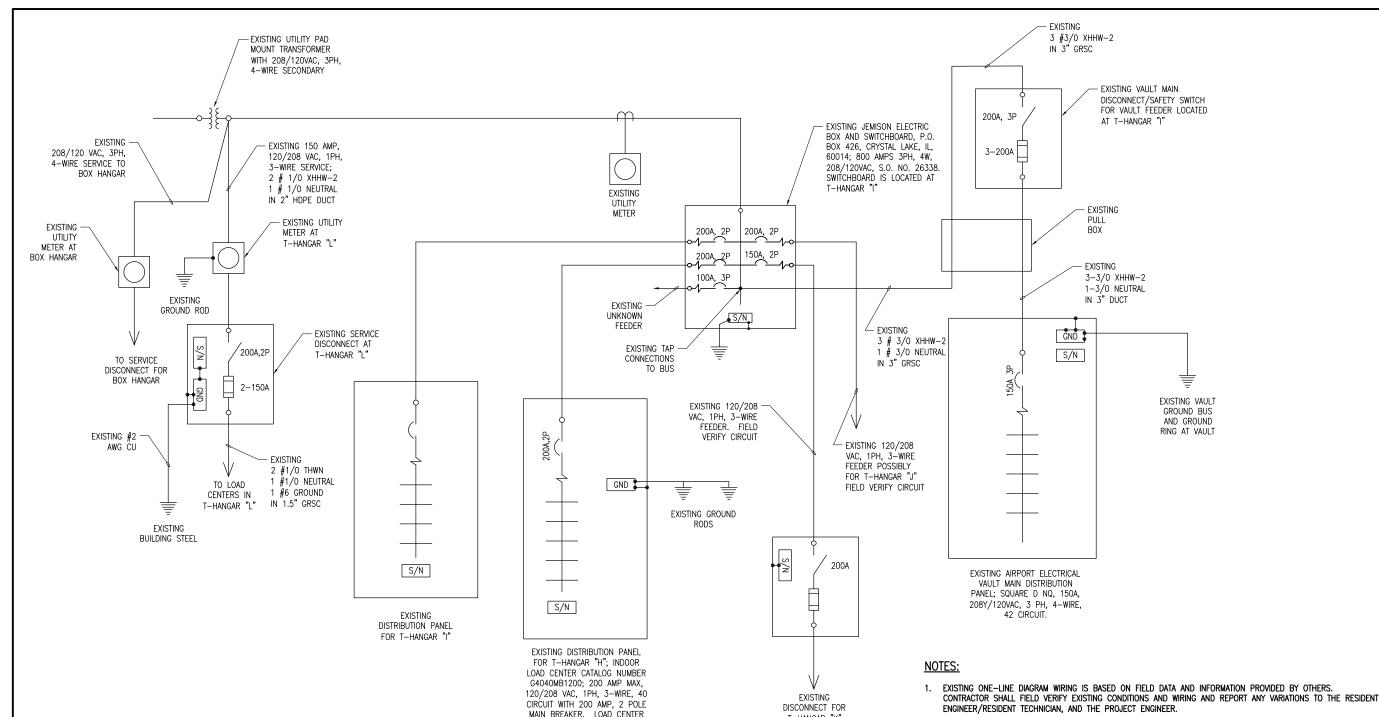
DESIGN BY: KNI 9/27/18

SHEET TITLE

AIRFIELD LIGHTING CABLE SPLICE DETAILS

DRAWN BY: LDH 9/28/18

REVIEWED BY: KMS 10/15/18



EXISTING ELECTRICAL ONE-LINE DIAGRAM FOR T-HANGARS AND VAULT

LOCATED IN SOUTH END UNIT OF T-HANGAR "H"



T-HANGAR "K"



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DRAWN BY: LDH 9/28/18

REVIEWED BY: KMS 10/15/18

SHEET TITLE

EXISTING ELECTRICAL ONE-LINE DIAGRAM FOR T-HANGARS AND VAULT

CONTRACTOR SHALL COMPLY WITH THE APPLICABLE REQUIREMENTS OF NFPA 70E - STANDARD FOR ELECTRICAL

2. ALL WORK, POWER OUTAGES, AND/OR SHUT DOWN OF EXISTING SYSTEMS SHALL BE COORDINATED WITH THE AIRPORT MANAGER. ONCE SHUT DOWN, THE CIRCUITS SHALL BE LABELED AS SUCH TO PREVENT ACCIDENTAL ENERGIZING OF THE RESPECTIVE CIRCUITS. ALL PERSONNEL SHALL FOLLOW U.S. DEPARTMENT OF LABOR OCCUPATIONAL SAFETY & HEALTH ADMINISTRATION (OSHA) 29 CFR PART 1910 OCCUPATIONAL SAFETY & HEALTH STANDARDS FOR ELECTRICAL SAFETY AND LOCKOUT/TAGOUT PROCEDURES INCLUDING, BUT NOT LIMITED TO, 29 CFR

3. CONTRACTOR SHALL EXAMINE THE SITE TO DETERMINE THE EXTENT OF THE WORK. CONTRACTOR SHALL FIELD VERIFY

4. CONTRACTOR SHALL COMPLY WITH THE REQUIREMENTS OF FAA AC NO. 150/5370-2G (OR MOST CURRENT ISSUE)

EXISTING SITE CONDITIONS. CONTRACTOR SHALL FIELD VERIFY RESPECTIVE CIRCUITS AND POWER SOURCES PRIOR TO ADJUSTING, REMOVING, RELOCATING, DISCONNECTING, CONNECTING, WORKING ON OR WORKING AROUND THE

RESPECTIVE FACILITY, EQUIPMENT, OR OTHER DEVICE. EXISTING CIRCUITS SHALL BE LOCATED AND PROTECTED FOR

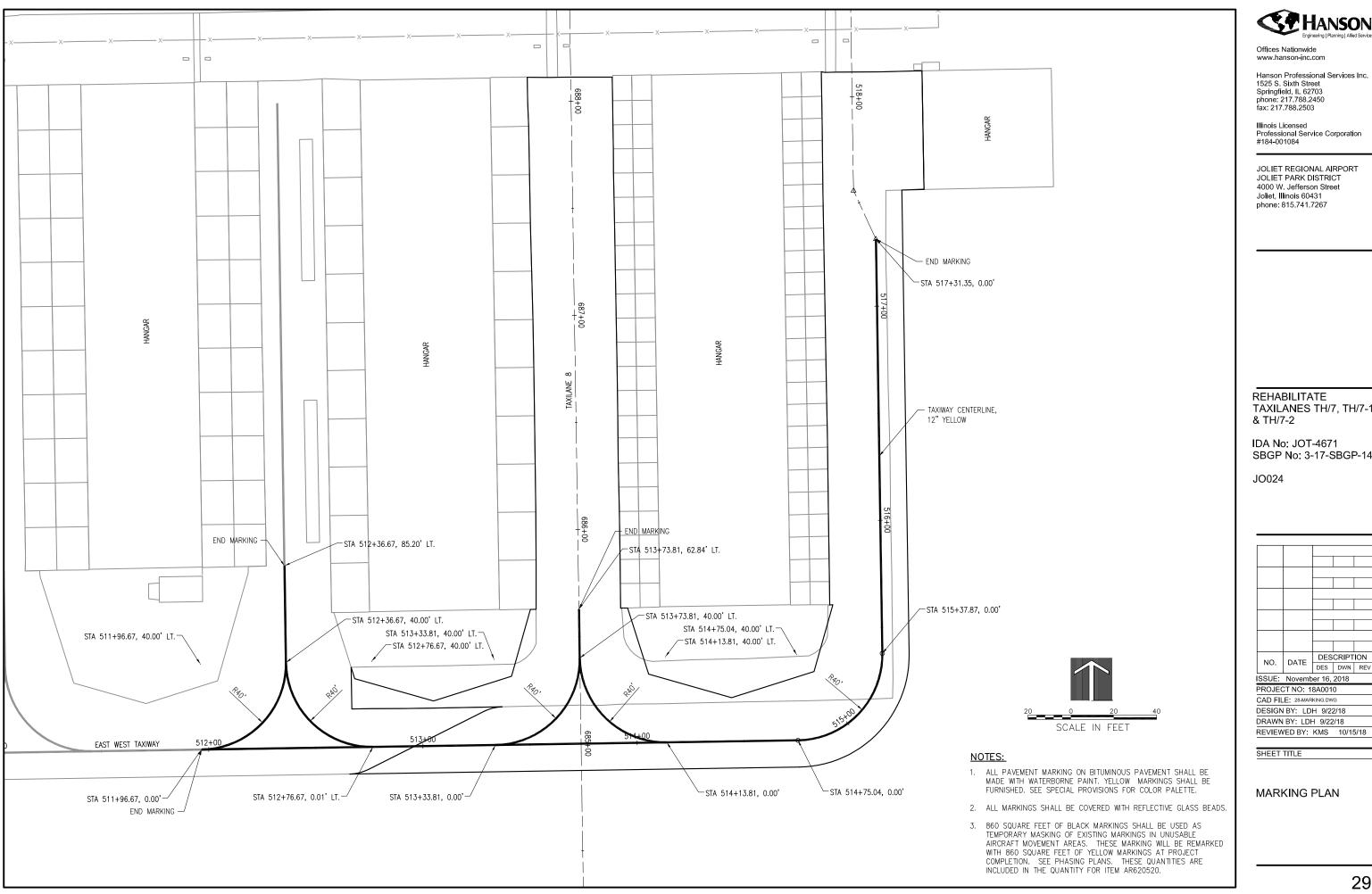
SECTION 1910.147 THE CONTROL OF HAZARDOUS ENERGY (LOCKOUT/TAGOUT).

"OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION".

THE DURATION OF THE PROJECT

SAFETY IN THE WORKPLACE.

6. NOTE SOME OF THE T-HANGAR BUILDINGS HAVE APPARENT (NATIONAL ELECTRICAL CODE) VIOLATIONS WHICH MIGHT CAUSE UNSAFE WORKING CONDITIONS. APPARENT NEC VIOLATIONS INCLUDE, BUT ARE NOT LIMITED TO, SERVICE BREAKERS NOT IDENTIFIED, MAIN DISCONNECTS FOR RESPECTIVE HANGAR NOT IDENTIFIED, POWER SOURCES NOT IDENTIFIED, VOLTAGE SYSTEMS NOT IDENTIFIED, MAX AVAILABLE FAULT CURRENT NOT IDENTIFIED, AND SOME FACILITIES MIGHT NOT HAVE GROUNDING ELECTRODE CONNECTIONS TO THE BUILDING DISCONNECTING MEANS. CONTRACTOR WILL NEED TO FIELD VERIFY EXISTING CONDITIONS AND EXERCISE CAUTION AT THIS FACILITY.





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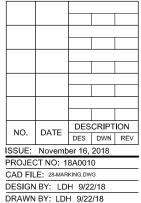
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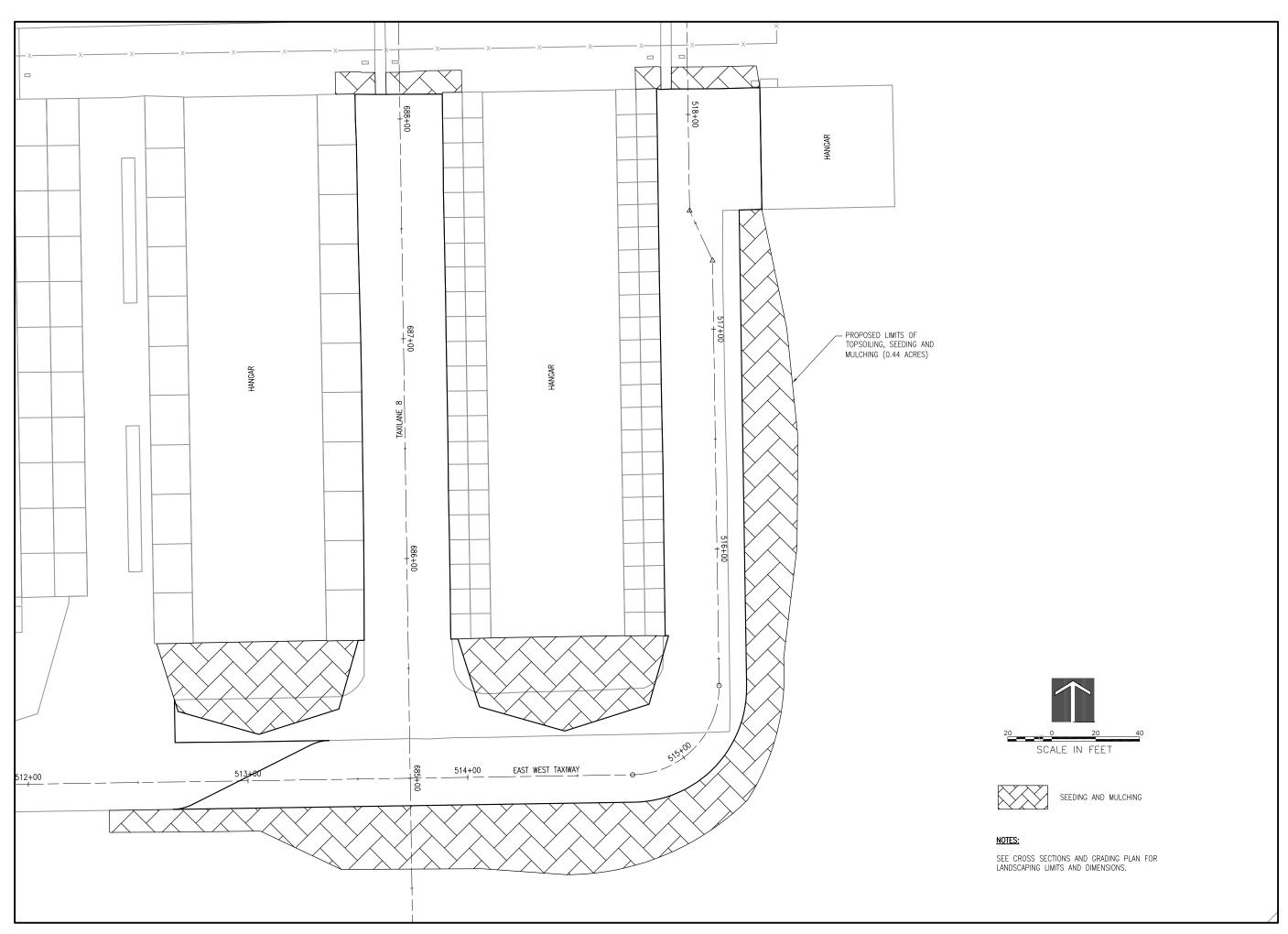
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MARKING PLAN

SHEET TITLE





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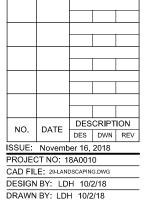
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LANDSCAPING PLAN

REVIEWED BY: KMS 10/15/18

SHEET TITLE