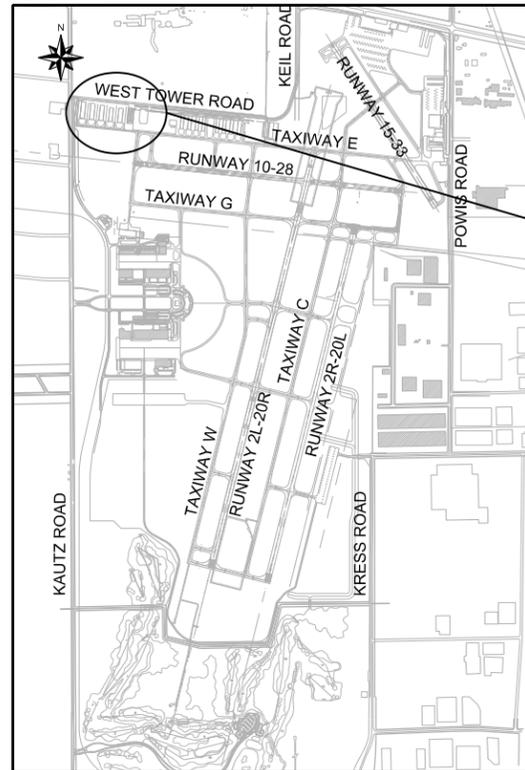


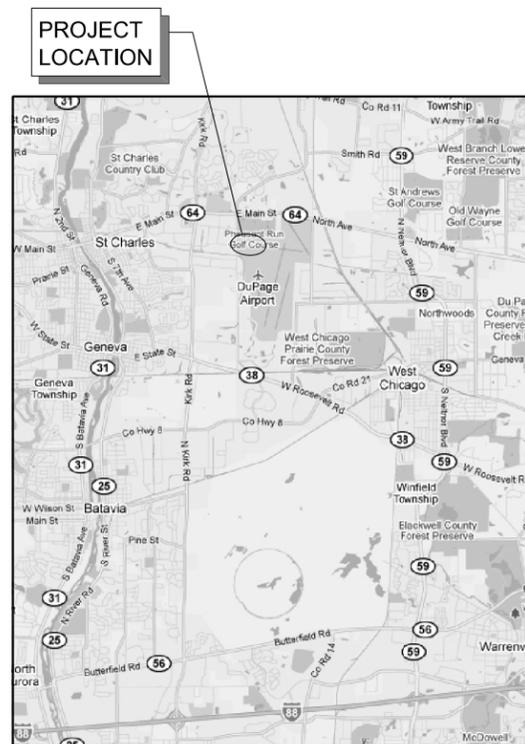
LETTING 12A

# DUPAGE AIRPORT AUTHORITY WEST CHICAGO, ILLINOIS

DU085



**SITE PLAN**



**LOCATION MAP**

## CONSTRUCTION PLANS FOR DUPAGE AIRPORT PHASE 2: ECHO T-HANGAR ASPHALT PAVEMENT REHABILITATION

ILLINOIS PROJECT: DPA-4651  
A.I.P. PROJECT: 3-17-SBGP-144  
IDOT LETTING JANUARY 18, 2019  
NOVEMBER 16, 2018



INDEX OF DRAWINGS:

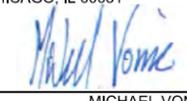
DRAWING NO.	SHEET NO	DESCRIPTION
G-100	1	COVER
G-101	2	SUMMARY OF QUANTITIES AND ABBREVIATIONS
G-102	3	GENERAL NOTES
G-103	4	CONTRACTOR STAGING AND ACCESS ROUTE PLAN
G-104	5	CONSTRUCTION NOTES AND DETAILS
G-105	6	EROSION CONTROL NOTES
G-106	7	EROSION CONTROL DETAILS
C-100	8	PROJECT LOCATION PLAN
C-101	9	PLAN DETAILS
C-102	10	PLAN DETAILS
C-103	11	PLAN DETAILS

DUPAGE AIRPORT AUTHORITY  
2700 INTERNATIONAL DRIVE  
SUITE 200  
WEST CHICAGO, IL 60185

APPROVED BY:   
EXECUTIVE DIRECTOR, DAVID BIRD

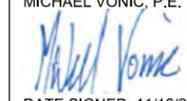
DATE: 11/14/18

ch2m  
8735 WEST HIGGINS RD  
SUITE 400  
CHICAGO, IL 60631

SUBMITTED BY:   
MICHAEL VONIC, P.E.

DATE: 11/16/2018

CH2M HILL  
MICHAEL VONIC, P.E.

  
DATE SIGNED: 11/16/2018

THIS SEAL APPLIES TO ALL SHEETS

EXPIRATION DATE: 11/30/2019



CALL J.U.L.I.E  
BEFORE EXCAVATING  
1-800-892-0123

**SUMMARY OF QUANTITIES**

ITEM NO.	PAY ITEM NO.	DESCRIPTION	UNIT	QUANTITY
1	AR150520	MOBILIZATION	LS	1
2	AR150530	TRAFFIC MAINTENANCE	LS	1
3	AR152511	SUBGRADE REPAIR	SY	400
4	AR156500	TEMPORARY EROSION CONTROL	LS	1
5	AR156520	INLET PROTECTION	EA	15
6	AR201661	CLEAN & SEAL BITUMINOUS CRACKS	LF	284
7	AR401610	BITUMINOUS SURFACE COURSE	TON	1,050
8	AR401650	BITUMINOUS PAVEMENT MILLING	SY	9,610
9	AR401910	REMOVE & REPLACE BIT. PAVEMENT	SY	193
10	AR603510	BITUMINOUS TACK COAT	GAL	1,450
11	AR620520	PAVEMENT MARKING - WATERBORNE	SF	1,481

**ABBREVIATIONS:**

AB	AGGREGATE BASE
ABV	ABOVE
A/C	ACCESS CONTROL
AC	ACRES
ADJ	ADJUST
AGG	AGGREGATE
AGS	AUXILIARY GAS VALVE
ALD	AIRFIELD LIGHTING DUCT
ALSF	APPROACH LIGHTING SEQUENCE FLASHERS
ARFF	AIRPORT RESCUE AND FIRE FIGHTING
AS	AERIAL SURVEYS
ASPH	ASPHALT
ASTM	AMERICAN SOCIETY FOR TESTING AND MATERIALS
AUX	AUXILIARY
AVE.	AVENUE
ATCT	AIR TRAFFIC CONTROL TOWER
AX	AXIS OF ROTATION
B-B	BACK TO BACK
BLVD	BOULEVARD
BM	BENCHMARK
BV	BALL VALVE
CL	CENTERLINE
CB	CATCH BASIN
CC	CENTER TO CENTER
CED	COMBINED / COMMON ELECTRICAL DUCTBANK
CI	CAST IRON
CMH	COMMUNICATION MANHOLE
CMP	CORRUGATED METAL PIPE
COMED	COMMONWEALTH EDISON
COMM	COMMUNICATIONS
CONC	CONCRETE
C.Y.	CUBIC YARD
CSPN	CARGO SITE PREP NORTH
DAA	DUPAGE AIRPORT AUTHORITY
DET	DETAIL
DIA.	DIAMETER
DIP	DUCTILE IRON PIPE
DME	DISTANCE MEASURING EQUIPMENT
DOA	DEPARTMENT OF AVIATION
E	EAST
ED or E.D.	EDGE DRAIN
EMH	ELECTRIC MANHOLE
EL or ELEV	ELEVATION
ESVCP	EXTRA STRONG VITRIFIED CLAY PIPE
EX	EXISTING

**ABBREVIATIONS:**

FAA	FEDERAL AVIATION ADMINISTRATION
FFM	FAR FIELD MONITOR
FH	FIRE HYDRANT
FT	FEET
G	GRADE
GRND	GROUND
GS	GLIDE SLOPE
HR	HANGAR ROAD
HV	HIGH VOLTAGE
IDOT	ILLINOIS DEPARTMENT OF TRANSPORTATION
ILS	INSTRUMENT LANDING SYSTEM
IE or I.E.	INVERT ELEVATION
ID	IDENTIFICATION
IH	INSPECTION HOLE
INV.	INVERT
JC	JUNCTION CHAMBER
JV	JOINT VALVE
KV	KILOVOLT
K=L/A	LENGTH OF VERTICAL CURVE/ALGEBRAIC DIFFERENCE IN GRADE
L	LENGTH
L.S.	LUMP SUM
LOC	LOCALIZER
MAX	MAXIMUM
MH	MANHOLE
MIN	MINIMUM
MJ	MECHANICAL JOINT
MPR	MOUNT PROSPECT ROAD
N	NORTH
NAVD	NORTH AMERICAN VERTICAL DATUM
N.C.	NORMALLY CLOSED
N.O.	NORMALLY OPEN
N.I.C.	NOT IN CONTRACT
NTS	NOT TO SCALE
NO	NUMBER
NPR	NORTH PERIMETER ROAD
O.D.	OUTSIDE DIAMETER
OFA	OBJECT FREE AREA
OZ	OUNCE

**ABBREVIATIONS:**

PAPI	PRECISION APPROACH PATH INDICATOR LIGHTS
PC	POINT OF CURVATURE
PCC	POINT OF COMPOUND CURVATURE
PCCP	PORTLAND CEMENT CONCRETE PAVEMENT
P.I.	POINT OF INTERSECTION
PL	PROPERTY LINE
PT	POINT OF TANGENCY
PVC	POINT OF VERTICAL CURVATURE
PVI	POINT OF VERTICAL INTERSECTION
PVT	POINT OF VERTICAL TANGENCY
PVC PIPE	POLYVINYL CHLORIDE PIPE
PSI	POUNDS PER SQUARE INCH
R	REMOVE
RAD	RADIUS
RC	REINFORCED CONCRETE
RCP	REINFORCED CONCRETE PIPE
RPP	REINFORCED POLYPROPYLENE
RPU	REMOTE PROCESSING UNIT
REV.	REVISION
RGS	RIGID STEEL
RD.	ROAD
RSA	RUNWAY SAFETY AREA
R/W	RUNWAY
RPZ	RUNWAY PROTECTION ZONE
SA	SANITARY
SCH	SCHEDULE
SD	STORM DRAIN
SHT	SHEET
S	SOUTH
SQR	SQUARE
S.F.	SQUARE FEET
S.Y.	SQUARE YARD
STA	STATION
ST.	STREET
ST	STORM SEWER
T	TANGENT
T.S.	TANGENT SPIRAL
TSA	TAXIWAY SAFETY AREA
T/W	TAXIWAY
TDZ	TOUCHDOWN ZONE
TYP	TYPICAL
UD or U.D.	UNDERDRAIN
UMH	UNDERDRAIN MANHOLE
U.N.O.	UNLESS NOTED OTHERWISE
VERT.	VERTICAL
VPI	VERTICAL POINT OF INTERSECTION
W	WEST
W/	WITH
WMG	WEST MASS GRADING
YD	YARD



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		MV

8735 W. HIGGINS ROAD  
SUITE 400  
CHICAGO, IL 60631

PHASE 2: ECHO T-HANGAR ASPHALT  
PAVEMENT REHABILITATION  
CONTRACT #DU085



**SUMMARY OF QUANTITIES  
AND ABBREVIATIONS**

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DWG G-101
SHEET 2 of 11

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**CONSTRUCTION NOTES**

- CONTRACTOR IS RESPONSIBLE FOR TRANSPORTING EMPLOYEES TO AND FROM THE JOB SITE.
- CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIR OF ANY DAMAGES CAUSED BY DELIVERIES OR CONTRACTOR VEHICLES. PAVEMENT REPAIR OF THE HAUL ROUTES SHALL BE AT THE SOLE DISCRETION OF THE RESIDENT ENGINEER. NO SEPARATE MEASUREMENT OR PAYMENT SHALL BE MADE FOR REPAIR OF DAMAGE CAUSED BY THE CONTRACTOR'S ACTIVITIES.
- HAUL ROUTES ON THIS PLAN SHALL BE STRICTLY ADHERED TO AND REMAIN FREE AND CLEAN OF FOREIGN OBJECT DEBRIS (FOD). CONSTRUCTION ACCESS ROUTES MAY BE ADJUSTED IN FIELD TO THE SATISFACTION OF THE RESIDENT ENGINEER IN ACCORDANCE WITH THE CONTRACT DOCUMENTS. ADJUSTMENTS TO THE CONSTRUCTION ACCESS/HAUL ROUTE SHALL BE AGREED UPON BY THE CONTRACTOR AND RESIDENT ENGINEER, AND SHALL BE MADE AT NO ADDITIONAL COST TO THE OWNER.
- CONTRACTOR ACCESS ROADS AND STAGING AREAS SHALL BE RESTORED TO THEIR ORIGINAL CONDITION AT THE COMPLETION OF THE PROJECT.
- REFER TO DRAWING G-102, GENERAL NOTE 6 FOR DAILY INSPECTION AND CLEANING REQUIREMENTS OF ON- AND OFF-AIRPORT HAUL ROADS.
- CONTRACTOR TO COORDINATE WORK WITH ADJACENT PROJECTS. SEE GENERAL NOTES, SHEET G-102.
- THE CONTRACTORS STAGING AREA IS NOT WITHIN THE AOA, AND SHALL BE SECURED AT ALL TIMES. ALL MATERIALS AND EQUIPMENT, WHEN NOT IN USE, SHALL BE PLACED IN APPROVED AREAS WHERE THEY WILL NOT CONSTITUTE A HAZARD TO AIRCRAFT OPERATIONS. ALL EQUIPMENT SHALL BE STORED IN A LOWERED CONFIGURATION WHEN NOT IN USE. EQUIPMENT AND STOCKPILED MATERIAL SHALL BE CONSTRAINED IN A MANNER TO PREVENT MOVEMENT RESULTING FROM AIRCRAFT JET BLASTS OR WIND CONDITIONS. THE CONTRACTOR'S STAGING AREA MUST BE LOCATED OUTSIDE OF THE TAXIWAY E OBJECT FREE AREA.

- THE CONTRACTOR IS RESPONSIBLE FOR SECURING OBJECTS OR WRAPPING DEBRIS THAT MAY BE BLOWN BY WIND OR JET BLAST FROM THE SITE. CONTRACTOR TO NOTIFY THE TOWER IMMEDIATELY IF FOD GETS OUTSIDE THEIR WORK LIMITS.
- WORK ASSOCIATED WITH THIS CONTRACT SHALL BE COMPLETED WITHIN THE DURATION OF 10 CALENDAR DAYS.
- MAXIMUM ANTICIPATED HEIGHT OF CONSTRUCTION EQUIPMENT WITHIN PROJECT LIMITS SHOWN IS 25', EQUIVALENT TO A FULL SIZE DUMP TRUCK WITH EXTENDED BED.
- CONTRACTOR IS REQUIRED TO SUBMIT A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) TO DAA AND THE RESIDENT ENGINEER FOR APPROVAL PRIOR TO THE ISSUANCE OF NOTICE TO PROCEED. SEE FAA AC 150/5370-2G FOR ADDITIONAL DETAILS.
- AIRCRAFT WITHIN HANGARS ADJACENT TO WORK TO BE RELOCATED AS ASSIGNED BY DAA DURING DURATION OF PROPOSED WORK.
- CONTRACTOR WILL NOT BE PERMITTED TO CLOSE THE AFFECTED TAXILANE PAVEMENT FOR LONGER THAN 72 HOURS.
- OVERNIGHT CLOSURES WILL NOT BE PERMITTED.
- CONTRACTOR TO COORDINATE WITH DAA REGARDING PHASING OF PAVEMENT CLOSURES. PAVEMENT INCLUDED FOR A GIVEN PHASE MAY BE SUBJECT TO CHANGE DUE TO RELOCATION OF TENANT AIRCRAFT.

**ACCESS NOTES**

- CONTRACTOR SHALL CONSIDER IDLE TIMES FOR TRANSPORTATION TIME OF EQUIPMENT AND MATERIALS TO THE SITE.
- CONSTRUCTION STAGING AREAS SHOWN ON PLANS MAY BE RELOCATED BY THE AIRPORT OR RESIDENT ENGINEER IN THE VICINITY OF THE PROJECT AREA.
- ACCESS TO SITE: THE AIRPORT WILL NOT PROVIDE AIRFIELD OPERATIONS AREA ESCORTS. CONTRACTOR'S ACCESS TO THE SITE SHALL BE AS SHOWN ON THE SAFETY PLAN OR AS DIRECTED BY THE RESIDENT ENGINEER. NO OTHER ACCESS POINT SHALL BE ALLOWED UNLESS APPROVED BY THE RESIDENT ENGINEER. IF GATE IS TO BE LEFT UNLOCKED, CONTRACTOR SHALL PROVIDE GATE GUARDS FOR THE DURATION OF THE UNLOCKED CONDITION. ALL CONTRACTOR TRAFFIC AUTHORIZED TO ENTER THE SITE SHALL BE EXPERIENCED IN THE ROUTE OR ESCORTED BY THE CONTRACTOR PERSONNEL. THE CONTRACTOR SHALL BE RESPONSIBLE FOR TRAFFIC CONTROL TO AND FROM THE ACCESS GATE TO THE SITE. A CONTRACTOR'S FLAGMAN OR TRAFFIC CONTROL PERSON SHALL MONITOR AND COORDINATE ALL CONTRACTOR TRAFFIC AT THE ACCESS GATE. THE CONTRACTOR SHALL NOT PERMIT ANY UNAUTHORIZED CONSTRUCTION PERSONNEL OR TRAFFIC ON THE AIRPORT OPERATIONS AREA (AOA). DIRECTIONAL SIGNING ALONG THE DELIVERY ROUTE TO THE STORAGE AREA OR WORK SITE SHALL BE AS DIRECTED BY THE RESIDENT ENGINEER.
- THE CONTRACTOR SHALL COORDINATION PROJECT CONSTRUCTION SCHEDULE WITH DAA AND IAA SO THAT NO IAA AIR TRAFFIC IS IMPACTED.

**SAFETY NOTES**

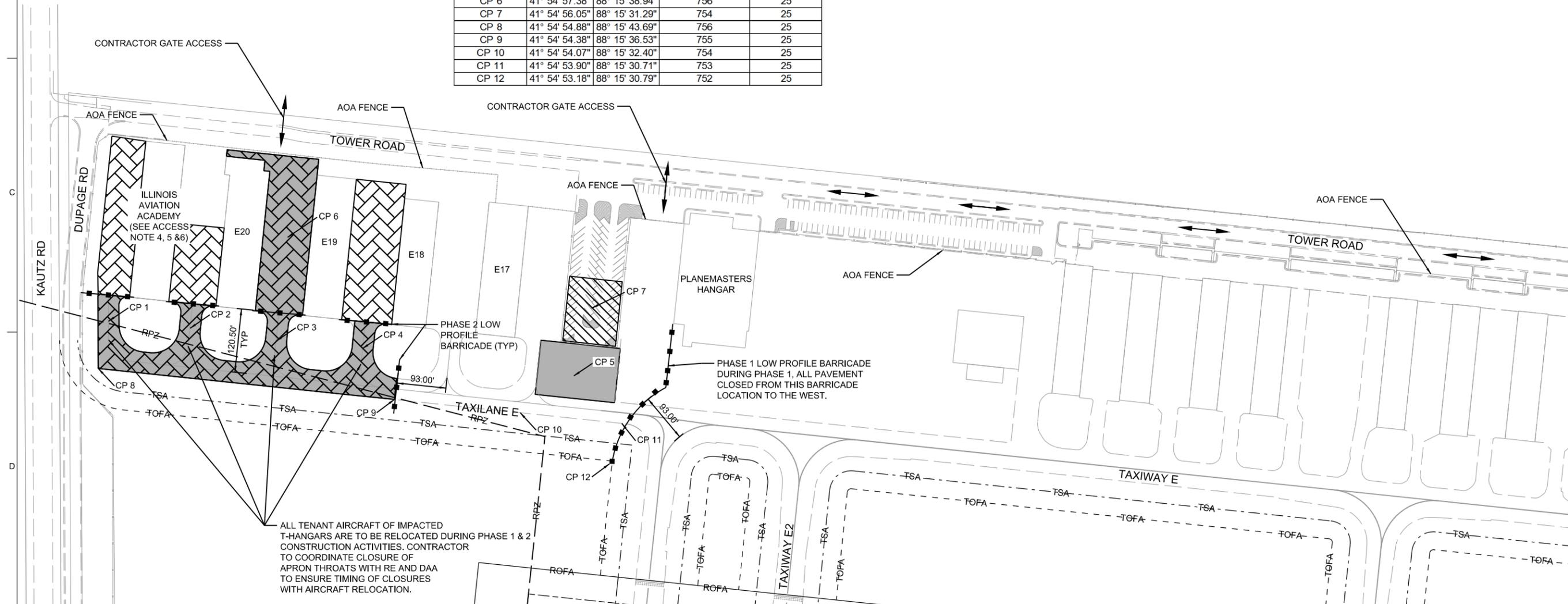
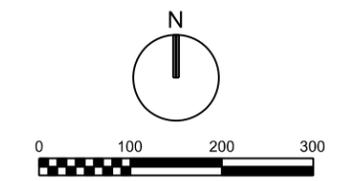
- PRIOR TO THE START OF WORK EACH DAY AS PART OF THE PRE-ACTIVITY MEETING, THE CONTRACTOR SHALL A HOLD SAFETY MEETING.
- CONTRACTOR VEHICLES SHALL GIVE AIRCRAFT THE RIGHT OF WAY AND OPERATE IN A SAFE MANNER TO AVOID ACCIDENTS.

**LEGEND:**

-  AREAS AVAILABLE FOR CONTRACTOR STAGING AREA
-  PROJECT WORK AREAS
-  CONTRACTOR HAUL ROUTE
-  LOW PROFILE BARRICADE
- 
-  TOFA TAXIWAY OBJECT FREE AREA
-  TSA TAXIWAY SAFETY AREA
-  ROFA RUNWAY OBJECT FREE AREA
-  PAVEMENT CLOSURE PHASE 2

**CRITICAL POINTS**

Point Number	Latitude	Longitude	Ground Elevation	Object Height
CP 1	41° 54' 55.81"	88° 15' 43.44"	757	25
CP 2	41° 54' 55.68"	88° 15' 41.39"	757	25
CP 3	41° 54' 55.43"	88° 15' 39.24"	756	25
CP 4	41° 54' 55.29"	88° 15' 37.06"	756	25
CP 5	41° 54' 54.78"	88° 15' 31.75"	755	25
CP 6	41° 54' 57.38"	88° 15' 38.94"	756	25
CP 7	41° 54' 56.05"	88° 15' 31.29"	754	25
CP 8	41° 54' 54.88"	88° 15' 43.69"	756	25
CP 9	41° 54' 54.38"	88° 15' 36.53"	755	25
CP 10	41° 54' 54.07"	88° 15' 32.40"	754	25
CP 11	41° 54' 53.90"	88° 15' 30.71"	753	25
CP 12	41° 54' 53.18"	88° 15' 30.79"	752	25



ALL TENANT AIRCRAFT OF IMPACTED T-HANGARS ARE TO BE RELOCATED DURING PHASE 1 & 2 CONSTRUCTION ACTIVITIES. CONTRACTOR TO COORDINATE CLOSURE OF APRON THROATS WITH RE AND DAA TO ENSURE TIMING OF CLOSURES WITH AIRCRAFT RELOCATION.



8735 W. HIGGINS ROAD  
SUITE 400  
CHICAGO, IL 60631

PHASE 2: ECHO T-HANGAR ASPHALT PAVEMENT REHABILITATION CONTRACT #DU085

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CONTRACTOR STAGING AND ACCESS ROUTE PLAN

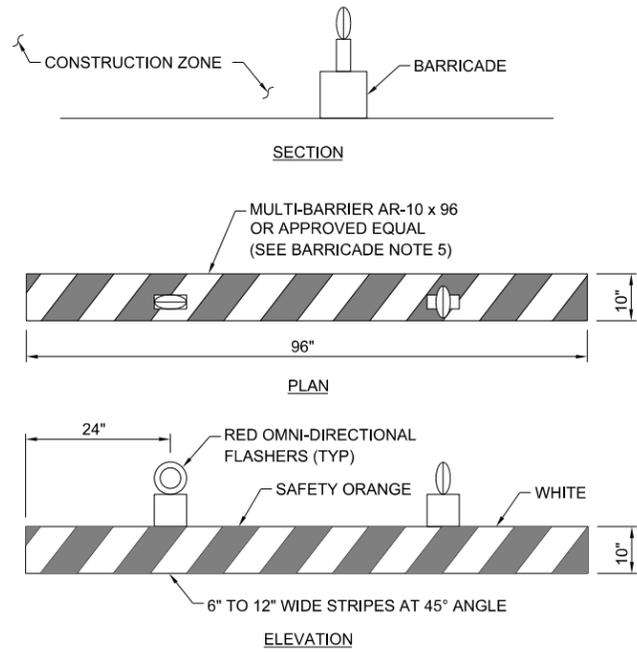
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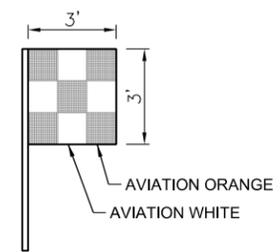
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**1** LOW-PROFILE BARRICADE DETAIL  
NTS



**2** VEHICLE FLAG  
NTS

**BARRICADE NOTES:**

1. ALL AIRFIELD PAVEMENT CLOSURES SHALL BE TEMPORARY CLOSURES AS DESCRIBED IN FAA AC 150/5370-2G, "OPERATIONAL SAFETY ON AIRPORTS DURING CONSTRUCTION." THE CONTRACTOR SHALL COMPLY WITH ALL THE REQUIREMENTS SET FORTH IN THE CONTRACT DOCUMENTS REGARDING AIRFIELD CLOSURES, SAFETY, AND SECURITY.
2. PRIOR TO REOPENING CLOSED AIRFIELD PAVEMENTS, THE ENTIRE SAFETY AREA MUST MEET FAA CRITERIA UNLESS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER.
3. FLASHERS TO BE BATTERY OPERATED AND/OR SOLAR POWERED. LENS TO BE RED AND BE ABLE TO ROTATE 90°.
4. FACING OF LOW-PROFILE BARRICADE TO BE COVERED WITH REFLECTIVE MATERIAL.
5. LOW-PROFILE BARRICADES TO BE PLACED END TO END ALONG OPERATIONAL PAVEMENT, ADJACENT TO CONSTRUCTION, AS DIRECTED BY THE RESIDENT ENGINEER.
6. FLASHERS SHALL BE SECURED TO THE BARRICADES, PER MANUFACTURER'S INSTRUCTIONS. ALTERNATE FLASHER LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
7. LOW-PROFILE BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF IT'S COMPONENTS, AND WEIGHTED OR STURDILY ATTACHED TO THE SURFACE. IF AFFIXED TO THE SURFACE, THE BARRICADE MUST BE FRANGIBLE AT GRADE LEVEL OR AS LOW AS POSSIBLE, NOT TO EXCEED 3 INCHES ABOVE THE GROUND.
8. THE CONTRACTOR SHALL PROVIDE BARRICADES WITH RED FLASHING BATTERY OPERATED LIGHTS AS SHOWN IN GOOD WORKING ORDER. CONTRACTOR SHALL MAINTAIN ALL BARRICADES AND ENSURE THEY ARE IN WORKING ORDER TWENTY-FOUR (24) HOURS A DAY, FOR THE DURATION OF THE PROJECT. THE BARRICADE LIGHTING SHALL BE CHECKED NIGHTLY BY THE CONTRACTOR.
9. THE LIGHTS SHALL BE OPERATIONAL AT EACH BARRICADE LOCATION AT ALL TIMES. ANY LIGHTS FOUND NON-OPERATIONAL SHALL BE REPAIRED IMMEDIATELY WITH EMERGENCY CONTACT NUMBER 24/7.
10. ALL BARRICADES SHALL BE CHECKED VISUALLY FOR SIGNS OF WEAR AND TEAR ON A DAILY BASIS AND SHALL BE REPAINTED OR REPLACED WHEN DEEMED APPROPRIATE BY THE RESIDENT ENGINEER. THE CONDITIONS OF LIGHTING UNITS SHALL BE CHECKED DAILY. ALL LIGHT FIXTURES SHALL BE VERIFIED IN OPERATING CONDITION AND GOOD WORKING ORDER BY THE CONTRACTOR ON A DAILY BASIS, BEFORE THE CONTRACTOR CEASES OPERATIONS FOR THE DAY.



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8735 W. HIGGINS ROAD  
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PHASE 2: ECHO T-HANGAR ASPHALT  
PAVEMENT REHABILITATION  
CONTRACT #DU085

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CONSTRUCTION NOTES  
AND DETAILS

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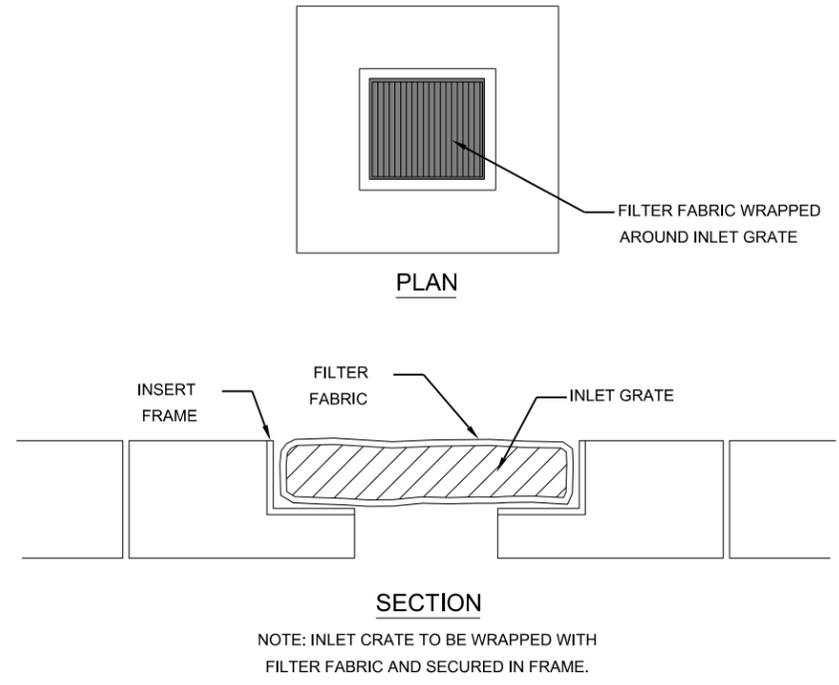
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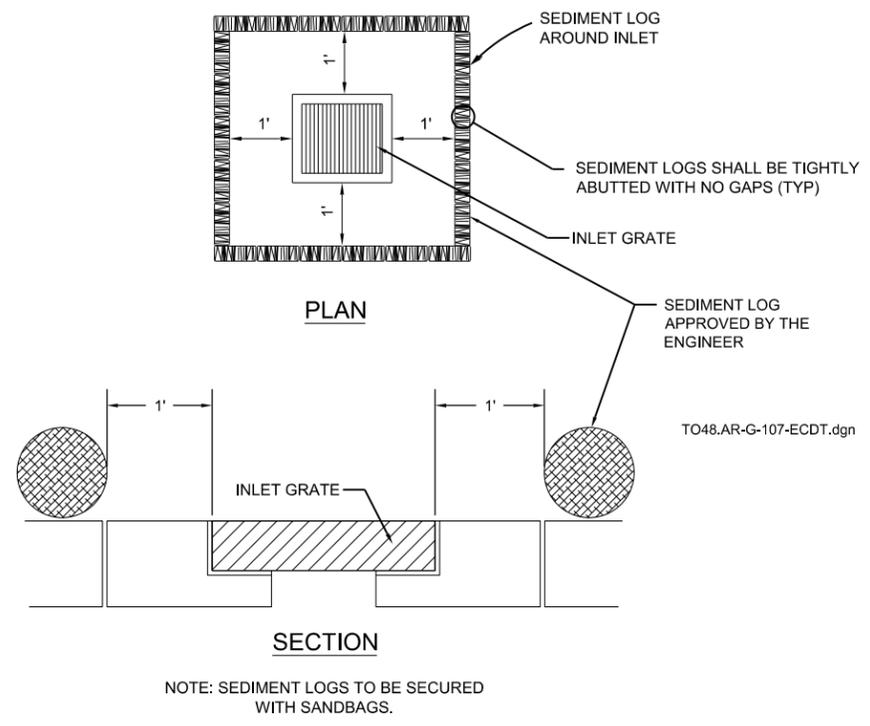


**LEGEND**

- ◆ 1 G-106
- 2 G-106



**1** INLET PROTECTION FILTER FABRIC DETAIL  
NTS



**2** INLET PROTECTION SEDIMENT LOG DETAIL  
NTS

**DUPAGE AIRPORT AUTHORITY**

8735 W. HIGGINS ROAD  
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PHASE 2: ECHO T-HANGAR ASPHALT  
PAVEMENT REHABILITATION  
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**CH2MHILL®**

EROSION CONTROL DETAILS

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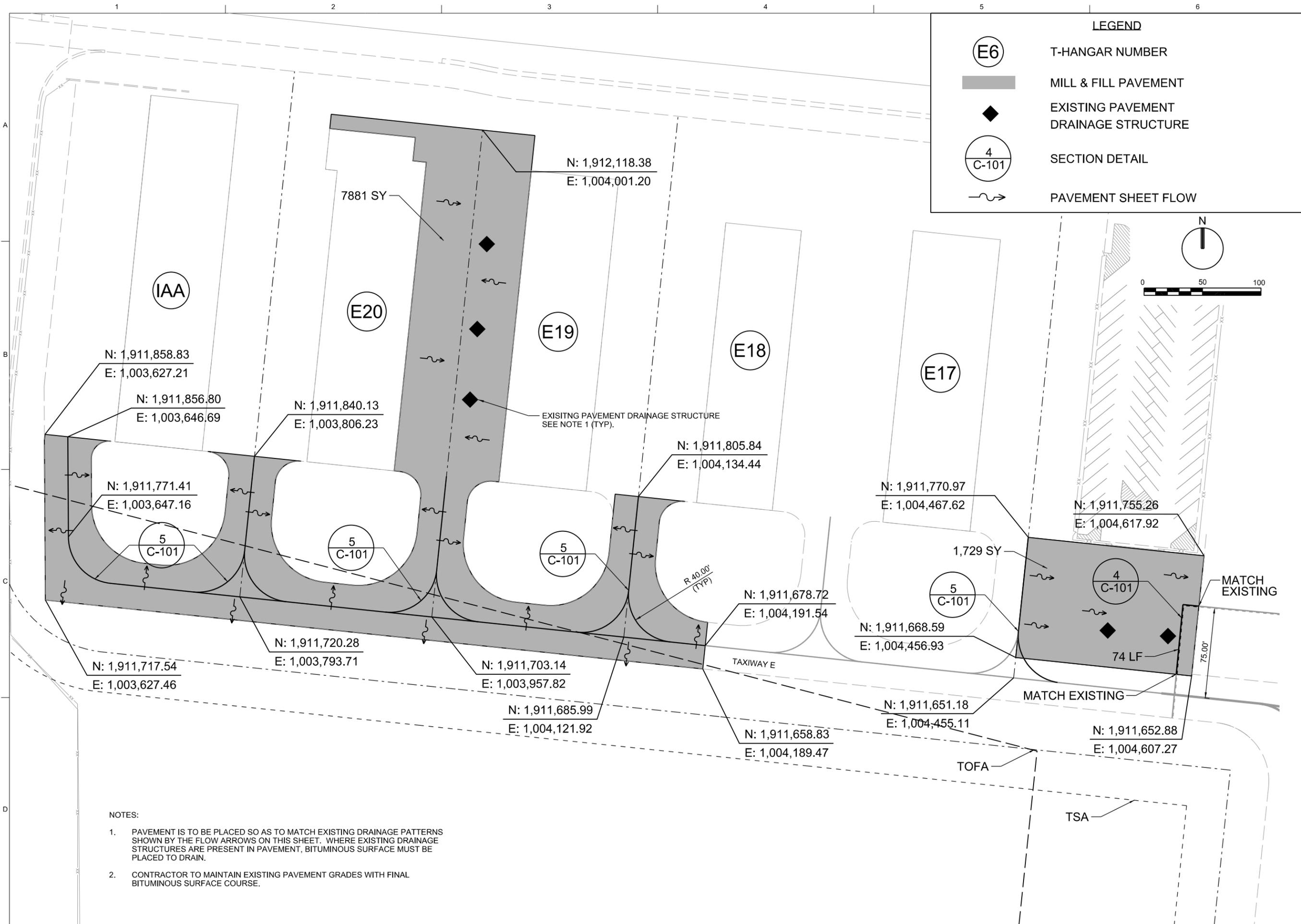
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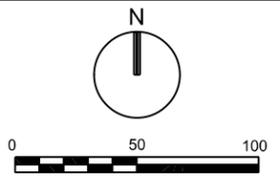
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**LEGEND**

- E6 T-HANGAR NUMBER
- MILL & FILL PAVEMENT
- EXISTING PAVEMENT DRAINAGE STRUCTURE
- 4  
C-101 SECTION DETAIL
- PAVEMENT SHEET FLOW



- NOTES:**
1. PAVEMENT IS TO BE PLACED SO AS TO MATCH EXISTING DRAINAGE PATTERNS SHOWN BY THE FLOW ARROWS ON THIS SHEET. WHERE EXISTING DRAINAGE STRUCTURES ARE PRESENT IN PAVEMENT, BITUMINOUS SURFACE MUST BE PLACED TO DRAIN.
  2. CONTRACTOR TO MAINTAIN EXISTING PAVEMENT GRADES WITH FINAL BITUMINOUS SURFACE COURSE.

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PHASE 2: ECHO T-HANGAR ASPHALT PAVEMENT REHABILITATION  
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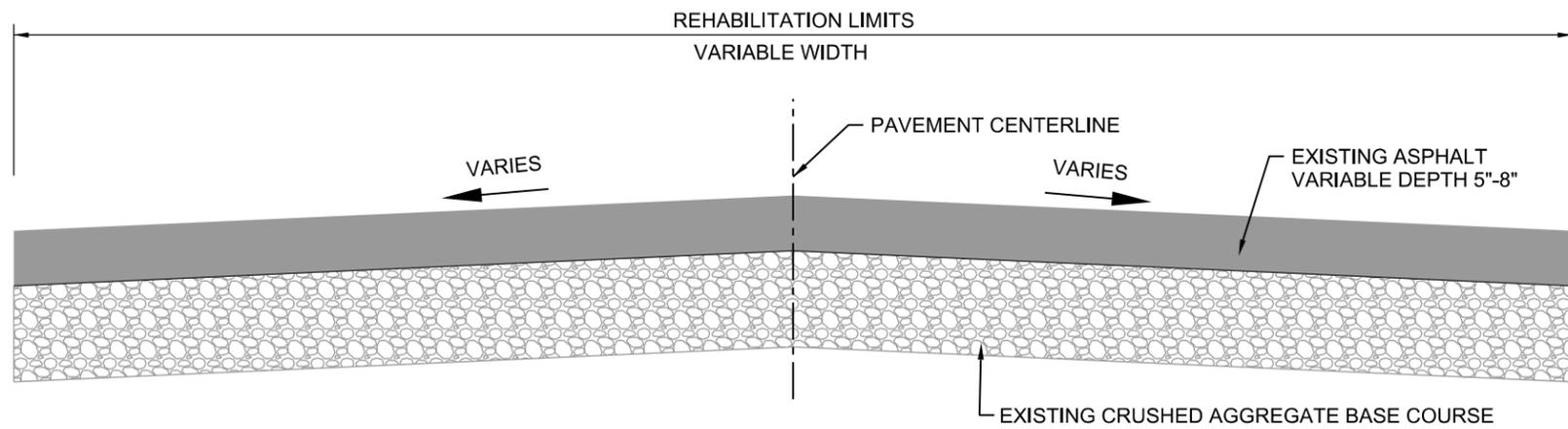
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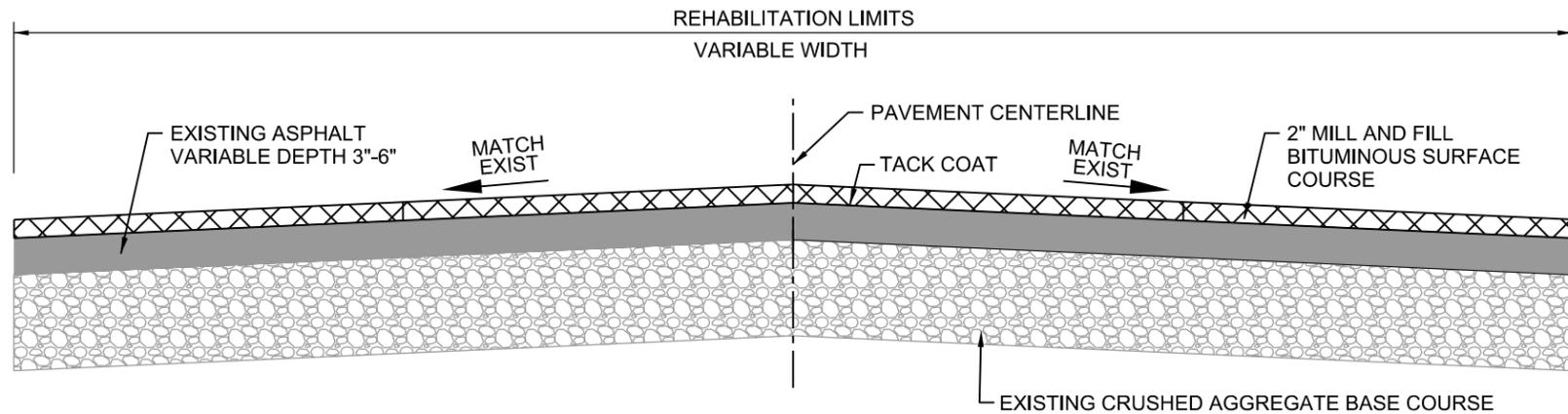
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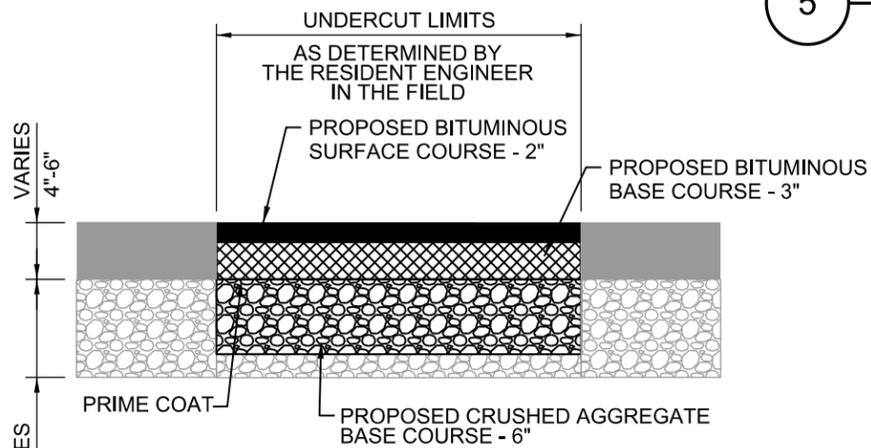
**1** EXISTING TYPICAL SECTION  
NTS



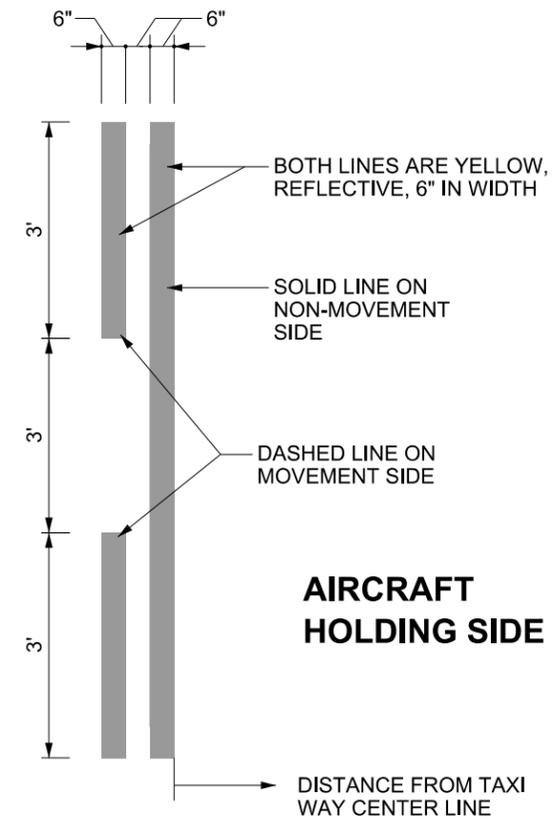
**2** MILL AND FILL SECTION  
NTS

**NOTES:**

1. BITUMINOUS PAVEMENT MILLING SHALL BE PAID FOR UNDER ITEM AR401650.
2. BITUMINOUS SURFACE COURSE SHALL BE PAID FOR UNDER ITEM AR401610.
3. BITUMINOUS TACK COAT SHALL BE PAID FOR UNDER ITEM AR603510.
4. RESIDENT ENGINEER DIRECTED UNDERCUT SHALL BE PAID FOR UNDER AR152511 SUBGRADE REPAIR.
5. ITEM AR152511 IS INTENDED TO COVER EXCAVATION AND REPLACEMENT OF PAVEMENT STRUCTURE IN AREAS OF INADEQUATE PAVEMENT BASE, LIMITS DETERMINED BY RESIDENT ENGINEER. THIS DIFFERS FROM ITEM AR401910 WHICH IS FOR CRACK REPAIR FOR CRACKS GREATER THAN 1" AFTER MILLING.
6. PRIME COAT AS SHOWN IN SECTION 3 IS INCIDENTAL TO THE COST OF THE CONTRACT.



**3** UNDERCUT SECTION  
NTS



**4** NON-MOVEMENT AREA MARKINGS  
NTS



**5** TAXIWAY CENTERLINE DETAIL  
NTS

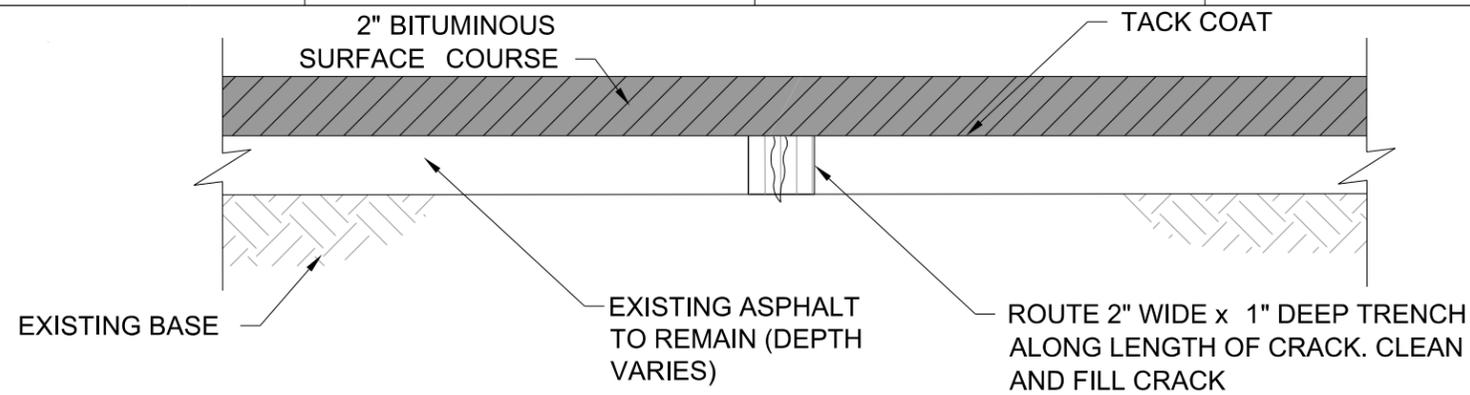


ISSUED FOR BID	BY	APVD
REVISION	CHK	APVD
NO.	DATE	MV
1	11/16/2018	DR
DSGN		MF/MV

8735 W. HIGGINS ROAD  
SUITE 400  
CHICAGO, IL 60631

PHASE 2: ECHO T-HANGAR ASPHALT  
PAVEMENT REHABILITATION  
CONTRACT #DU085

<b>CH2MHILL®</b>	
PLAN DETAILS	
AS SHOWN	
VERIFY SCALE	
BAR IS ONE INCH ON ORIGINAL DRAWING.	
DATE	11/16/2018
PROJ	DPA-4651
DWG	C-101
SHEET	9 of 11

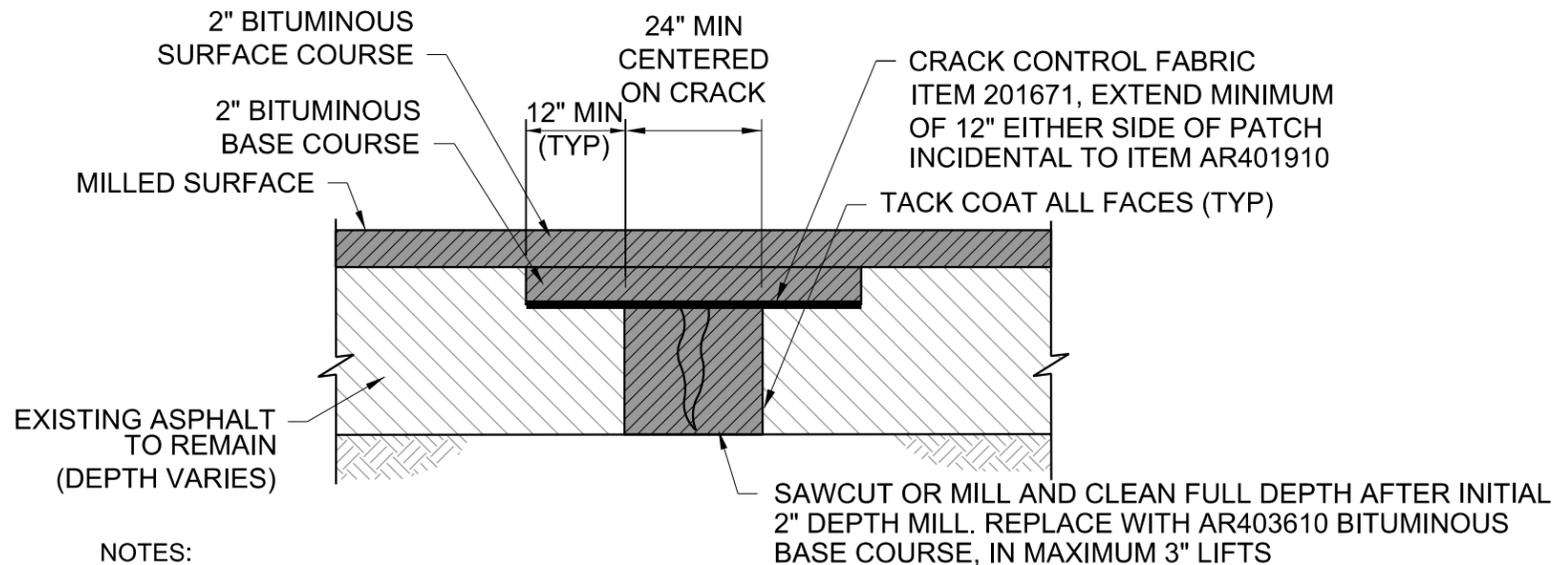


NOTES:

1. CRACKS SHALL BE FIELD LOCATED BY THE RESIDENT ENGINEER.
2. PAYMENT FOR CLEAN & SEAL BITUMINOUS CRACKS SHALL BE PER LINEAR FOOT UNDER ITEM AR201661, AND SHALL INCLUDE ALL WORK SHOWN IN THIS DETAIL BELOW THE LEVEL OF THE INITIAL 2" MILL. THIS INCLUDES SAW CUTTING, DETAIL MILLING, CLEANING, ROUTING, AND CRACK SEALANT. BITUMINOUS SURFACE COURSE WITH BITUMINOUS TACK COAT SHALL BE PAID FOR SEPARATELY UNDER AR401610 AND AR603510 RESPECTIVELY.

**1 CLEAN & SEAL BITUMINOUS CRACKS - CRACKS GREATER THAN 1/2" WIDE BUT LESS THAN 1" WIDE AFTER MILL**

NTS



NOTES:

1. CRACKS SHALL BE FIELD LOCATED BY THE RESIDENT ENGINEER.
2. PAYMENT FOR REMOVE & REPLACE BITUMINOUS PAVEMENT SHALL BE ONLY AT CRACKS GREATER THEN 1" AFTER MILL AND MEASURED PER SQUARE YARD UNDER ITEM AR401910, AND SHALL INCLUDE ALL WORK SHOWN IN THIS DETAIL BELOW THE LEVEL OF THE INITIAL 2" MILL EXCEPT FOR BITUMINOUS SURFACE COURSE. THIS INCLUDES SAW CUTTING, DETAIL MILLING, CLEANING AND CRACK CONTROL FABRIC. BITUMINOUS SURFACE COURSE WITH BITUMINOUS TACK COAT SHALL BE PAID FOR SEPARATELY UNDER AR401610 AND AR603510 RESPECTIVELY.
3. ITEM AR401910 IS INTENDED TO COVER CRACK REPAIR FOR CRACKS GREATER THAN 1" AFTER MILLING. THIS DIFFERS FROM ITEM AR152511 WHICH IS INTENDED TO COVER EXCAVATION AND REPLACEMENT OF PAVEMENT STRUCTURE IN AREAS OF INADEQUATE PAVEMENT BASE, LIMITS DETERMINED BY RESIDENT ENGINEER.

**2 REMOVE & REPLACE BITUMINOUS PAVEMENT - CRACKS GREATER THAN 1" AFTER MILL**

NTS



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		MV

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PHASE 2: ECHO T-HANGAR ASPHALT  
PAVEMENT REHABILITATION  
CONTRACT #DU085

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PROJ	DPA-4651
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# Existing Pavement Cores



 PERFORMED CORE LOCATION



**CORE 8**  
~7.5" ASPHALT



**CORE 9**  
~8.0" ASPHALT

**1** EXISTING PAVEMENT CORE DATA - FOR INFORMATION ONLY  
NTS



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