

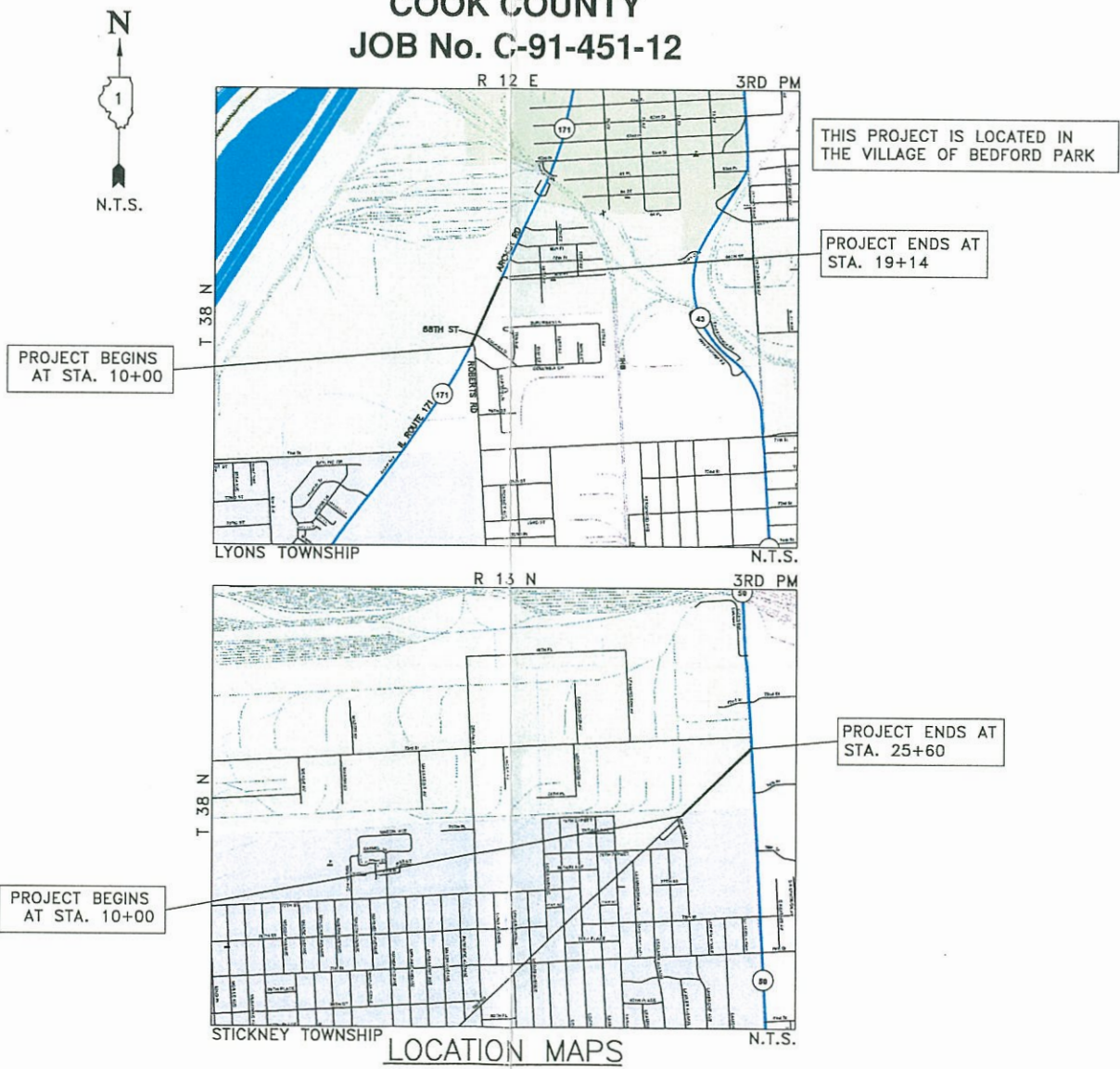
STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

PLANS FOR PROPOSED  
FEDERAL AID HIGHWAY

FAU RTE 3565 (IL ROUTE 171)(ARCHER RD) - 68TH STREET TO 66TH PLACE  
FAU RTE 3577 (STATE RD) - LAVERGNE AVE TO CICERO AVE (IL ROUTE 50)  
SIDEWALK CONSTRUCTION/SIGNAL MODIFICATION  
SECTION No. 12-00035-00-SW  
PROJECT No. KUUW (642)  
VILLAGE OF BEDFORD PARK  
COOK COUNTY  
JOB No. C-91-451-12

FAU RTES	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3565, 3577	12-00035-00-SW	COOK	30	1
STA. 10+00 TO STA. 19+14 (ARCHER ROAD)				
STA. 10+00 TO STA. 25+60 (STATE ROAD)				
FED. ROAD DIST. NO. ILLINOIS FED. AID PROJECT				
CONTRACT NO. 61F35				

INDEX OF SHEETS: SEE SHEET 2  
HIGHWAY STANDARDS: SEE SHEET 2

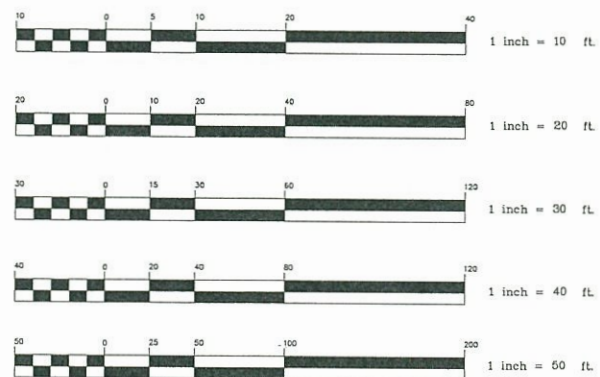


ADT = 27,900 (ARCHER RD, NORTH OF ROBERTS RD);  
16,900 (ARCHER RD, SOUTH OF ROBERTS RD)  
21,300 (STATE RD)

SPEED LIMIT = 35 MPH (ARCHER RD)  
35 MPH (STATE RD)

DESIGN SPEED = 40 MPH (ARCHER RD)  
40 MPH (STATE RD)

DESIGN DESIGNATION - MINOR ARTERIAL URBAN (ARCHER RD)  
MAJOR COLLECTOR (STATE RD)



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES, REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES, IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED

J.U.L.I.E.  
JOINT UTILITY LOCATION FOR EXCAVATION  
1-800-892-0123 -OR- 811

CONTRACT NO. 61F35

GROSS LENGTH OF PROJECT: ARCHER RD - 914 FEET (0.17 MILES)  
STATE ROAD - 1,560 FEET (0.30 MILES)  
NET LENGTH OF PROJECT: ARCHER RD - 914 FEET (0.17 MILES)  
STATE ROAD - 1,560 FEET (0.30 MILES)  
TOTAL LENGTH OF PROJECT: 2,414 FEET (0.47 MILES)

**hbe** Hoeffler-Butler Engineering, Inc.  
Consulting Civil Engineers - Land Surveyors  
PROFESSIONAL DESIGN FIRM LICENSE NO. 094-000819  
874 S. ROBERTS ROAD HICKORY HILLS, ILLINOIS 60437  
7080 599-8988 FAX 7080 599-8790

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

APPROVED *October 11, 2018*  
*Paul DRBO*  
VILLAGE OF BEDFORD PARK PRESIDENT

PASSED *NOVEMBER 1, 2018*  
*Chad Christopoulos*  
DISTRICT 1 ENGINEER OF LOCAL ROADS & STREETS

RELEASING FOR BID  
BASED ON LIMITED  
REVIEW *November 2, 2018*  
*Anthony J. Quinlan/AS*  
REGIONAL ENGINEER

EXPIRES 11-30-19

**STEPHEN A. LEIBER**  
38352  
REGISTERED PROFESSIONAL ENGINEER  
OF ILLINOIS

*Stephen A. Leiber*  
SIGNATURE  
*10/11/18*  
DATE

FEDERAL AID PROGRAM ENGINEER: CARMEN E. RAMOS, P.E. SCHAUMBURG, IL  
CONSULTANT: HOFFERLE BUTLER ENGINEERING, INC. 1-708-599-8980

HBE JOB No. 11-517



# GENERAL NOTES

## INDEX TO SHEETS

SHEET NO.	DESCRIPTION
1.	COVER SHEET
2.	GENERAL NOTES
3.	SUMMARY OF QUANTITIES
4.	TYPICAL EXISTING SECTIONS - DRIVEWAY DETAIL
5.	TYPICAL PROPOSED SECTIONS & HOT-MIX ASPHALT MIXTURE CHART
6-7.	PLAN AND PROFILE AND PAVEMENT MARKING SHEETS - ARCHER AVENUE
8-10.	PLAN AND PROFILE AND PAVEMENT MARKING SHEETS - STATE ROAD
11.	EROSION AND SEDIMENT CONTROL PLANS - ARCHER AVENUE
12.	EROSION AND SEDIMENT CONTROL PLANS - STATE ROAD
13.	EROSION AND SEDIMENT CONTROL NOTES
14-16.	TRAFFIC SIGNAL MODIFICATION PLANS - IL171 AT ROBERTS ROAD
17.	DISTRICT 1 DETAIL-TC10-TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS
18.	DISTRICT 1 DETAIL-TC13-TYPICAL PAVEMENT MARKINGS
19-25.	DISTRICT 1 DETAIL-TS05-STANDARD TRAFFIC SIGNAL DESIGN
26-30.	CROSS-SECTIONS

## HIGHWAY STANDARDS

STANDARD NO.	DESCRIPTION
000001-07:	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
280001-07:	TEMPORARY EROSION CONTROL SYSTEMS
424001-11:	PERPENDICULAR CURB RAMPS FOR SIDEWALKS
606001-07:	CONCRETE CURB, TYPE B AND COMBINATION CURB AND GUTTER
701101-05:	OFF ROAD OPERATIONS, MULTILANE, 15'(4.5M) TO 24"(600MM) FROM PAVEMENT EDGE
701606-10:	URBAN LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10:	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06:	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-08:	TRAFFIC CONTROL DEVICES
780001-05:	TYPICAL PAVEMENT MARKINGS

COLUMN 1 = FROM CROSS-SECTION END AREAS  
 COLUMN 2 = [COLUMN 1]  
 COLUMN 3 = [COLUMN 2] X .85  
 COLUMN 4 = FROM CROSS-SECTION END AREAS  
 COLUMN 5 = [COLUMN 3] - [COLUMN 4]

EARTHWORK						
LOCATION		1	2	3	4	5
		EARTH EXCAVATION	TOTAL SUITABLE EXCAVATION	EXCAVATION TO BE USED IN EMBANKMENT (15% SHRINKAGE)	EMBANKMENT	EARTHWORK BALANCE WASTE (+) OR SHORTAGE (-)
STA TO STA		(CU YD)	(CU YD)	(CU YD)	(CU YD)	(CU YD)
<b>ARCHER ROAD</b>						
10+05	10+59	9.3	9.3	7.9	1.7	6.2
10+59	11+09	9.6	9.6	8.2	0.0	8.2
11+09	11+51	6.4	6.4	5.4	0.0	5.4
12+28	12+45	5.2	5.2	4.4	0.0	4.4
12+45	12+58	4.1	4.1	3.5	0.1	3.4
13+09	13+59	8.3	8.3	7.1	0.1	7.0
13+59	14+09	7.5	7.5	6.4	0.1	6.3
14+09	14+59	8.9	8.9	7.6	0.0	7.6
14+59	15+09	5.9	5.9	5.0	0.0	5.0
15+09	15+59	9.5	9.5	8.1	0.1	8.0
15+59	16+09	7.5	7.5	6.4	0.1	6.3
16+09	16+59	9.9	9.9	8.4	0.0	8.4
16+59	17+09	11.8	11.8	10.0	0.0	10.0
17+09	17+59	10.3	10.3	8.8	0.0	8.8
17+59	18+09	9.3	9.3	7.9	0.0	7.9
18+09	18+59	8.0	8.0	6.8	0.0	6.8
18+59	18+79	2.8	2.8	2.4	0.2	2.2
18+79	19+14	4.6	4.6	3.9	0.3	3.6
<b>STATE ROAD</b>						
10+10	10+50	5.0	5.0	4.3	3.8	0.5
10+50	11+00	4.6	4.6	3.9	4.8	-0.9
11+00	11+50	5.9	5.9	5.0	0.3	4.7
11+50	12+00	4.0	4.0	3.4	3.2	0.2
12+00	12+50	1.3	1.3	1.1	11.4	-10.3
12+50	13+00	3.5	3.5	3.0	9.4	-6.4
13+00	13+50	6.5	6.5	5.5	1.1	4.4
13+50	14+00	6.0	6.0	5.1	0.7	4.4
14+00	14+50	9.9	9.9	8.4	0.8	7.6
14+50	15+00	8.9	8.9	7.6	0.4	7.2
15+00	15+50	7.6	7.6	6.5	0.4	6.1
15+50	16+00	11.0	11.0	9.4	0.0	9.4
16+00	16+50	10.2	10.2	8.7	0.1	8.6
16+50	17+00	7.5	7.5	6.4	0.1	6.3
17+00	17+50	8.1	8.1	6.9	0.0	6.9
17+50	18+00	9.1	9.1	7.7	0.0	7.7
18+00	18+50	9.0	9.0	7.7	0.1	7.6
18+50	19+00	7.2	7.2	6.1	0.1	6.0
19+00	19+50	6.0	6.0	5.1	0.7	4.4
19+50	20+00	7.4	7.4	6.3	0.7	5.6
20+00	20+50	11.5	11.5	9.8	0.0	9.8
20+50	21+00	13.7	13.7	11.6	0.0	11.6
21+00	21+34	8.1	8.1	6.9	0.0	6.9
21+34	21+95	10.6	10.6	9.0	0.7	8.3
21+95	22+50	6.6	6.6	5.6	0.9	4.7
22+50	23+00	6.2	6.2	5.3	0.3	5.0
23+00	23+50	6.6	6.6	5.6	0.4	5.2
23+50	24+00	6.6	6.6	5.6	0.4	5.2
24+00	24+50	6.7	6.7	5.7	0.1	5.6
24+50	25+00	7.8	7.8	6.6	0.2	6.4
25+00	25+60.44	9.8	9.8	8.3	0.1	8.2
TOTALS		371.8	371.8	316.0	43.9	272.4

## CLEARING

ALL AREAS DESIGNATED IN PLANS AS TO BE CLEARED SHALL INVOLVE REMOVAL OF OBSTRUCTIONS AS DEFINED IN ARTICLE 201.01(a) OF THE STANDARD SPECIFICATIONS. ALL CLEARING SHALL NOT BE PAID FOR SEPARATELY.

## UTILITIES

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNER OF ALL EXISTING FACILITIES SO THAT THE UTILITIES AND THEIR APPURTENANCES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS. THE CONTRACTORS SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE STANDARD SPECIFICATIONS.

THE LOCATIONS OF EXISTING DRAINAGE STRUCTURES, STORM AND SANITARY SEWERS, WATER SERVICE LINES AND OTHER UTILITY LINES ARE APPROXIMATE, AND THE VILLAGE DOES NOT GUARANTEE THEIR ACCURACY, THEIR EXACT HORIZONTAL AND VERTICAL LOCATIONS ARE TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER.

COORDINATION OF ALL UTILITY WORK INVOLVED IN THE CONSTRUCTION AREA WILL BE DISCUSSED AT THE PRECONSTRUCTION CONFERENCE.

BEFORE STARTING EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, GAS AND CABLE TELEVISION FACILITIES. (48 HOURS NOTIFICATION IS REQUIRED.)

CARE IS TO BE TAKEN AS NOT TO DAMAGE ANY OF THE EXISTING TRAFFIC SIGNAL CONDUITS, DETECTORS AND EQUIPMENT. IF ANY OF THE TRAFFIC SIGNAL CONDUIT AND/OR EQUIPMENT IS DAMAGED, THE CONTRACTOR SHALL REPAIR AND/OR REPLACE THE CONDUIT AND/OR EQUIPMENT.

## STORM SEWER

ANY EXISTING STORM SEWER DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR.

## RESTORATION

CONTRACTOR IS TO RESTORE ALL UNPAVED AREAS DAMAGED DURING CONSTRUCTION OPERATIONS TO THEIR ORIGINAL CONDITION INCLUDING THE FINE GRADING AND SODDING OR SEEDING OF AREAS WITHIN EASEMENTS AND RIGHTS-OF-WAY.

EROSION CONTROL BLANKET SHALL BE INSTALLED ON ALL DISTURBED AREAS PRIOR TO FINAL LANDSCAPING

## TRAFFIC SIGNS

THE RESIDENT ENGINEER SHALL CONTACT THE TRAFFIC CONTROL SUPERVISOR, CORY JUCIUS, AT 847-705-4470 PRIOR TO THE PLACEMENT OF ANY TEMPORARY TRAFFIC CONTROL DEVICES.

THE COST OF REMOVAL AND RE-INSTALLATION OF TRAFFIC SIGNS THAT ARE IN CONFLICT WITH, BLOCK ACCESS TO THE PROPOSED WORK, OR INHIBIT CONSTRUCTION IN ANY WAY, SHALL BE PAID FOR IN ACCORDANCE WITH ARTICLE 107.25 OF STANDARD SPECIFICATIONS.

## MISCELLANEOUS

ACCESS: THE CONTRACTOR SHALL PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING CONSTRUCTION, EXCEPT DURING HTE INSTALLATION OF SIDEWALK SPANNING DRIVEWAYS, WHICH INCLUDES THE LENGTH OF TIME NECESSARY FOR EXCAVATION, FRAMING, POURING, AND CURING.

DIMENSIONS: IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.

ANY EXISTING PAVEMENT OUTSIDE THE AREAS DESIGNATED FOR REMOVAL IN THE PLANS DAMAGED BY THE CONTRACTOR DURING THE CONSTRUCTION SHALL BE REPLACED/REPAIRED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER.

THE THICKNESS OF THE HOT-MIX ASPHALT SHOWN ON THE PLANS IS NOMINAL AND MAY VARY BY AS MUCH AS ONE (1) INCH. EXTRA COMPENSATION WILL BE DUE THE CONTRACTOR WHEN EXISTING SURFACE DESIGNATED FOR REMOVAL EXCEEDS FOUR (4) INCHES.

## STAKING

THE CONTRACTOR SHALL TAKE CARE TO PROTECT AND PRESERVE ALL STAKING AND MARKING DURING CONSTRUCTION. CONTINUOUS DESTRUCTION OF STAKES AS A RESULT OF CARELESSNESS MAY RESULT IN THE CONTRACTOR BEING BILLED FOR THE COST OF REPLACEMENT.

THE ENGINEER SHALL BE NOTIFIED AT LEAST TWO (2) WORKING DAYS PRIOR TO THE COMMENCEMENT OF WORK. THEY SHALL BE NOTIFIED AT LEAST 72 HOURS IN ADVANCE IF CONSTRUCTION STAKING IS REQUIRED.

## COMMITMENTS

NONE

STA. 10+00 TO STA. 19+14 (ARCHER ROAD) STA. 10+00 TO STA. 25+60 (STATE ROAD)	DRAWN BY: ALS	REVISED: PER IDOT REVIEW	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>GENERAL NOTES</b>	FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	CHECKED BY: SAL	REVISED: _____			3565, 3577	12-00035-00-SW	COOK	30	2
DATE: 08/01/2018	REVISED: _____	CONTRACT No. 61F35							
	REVISED: _____	SCALE: N.T.S.		SHEET No. OF SHEETS	FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

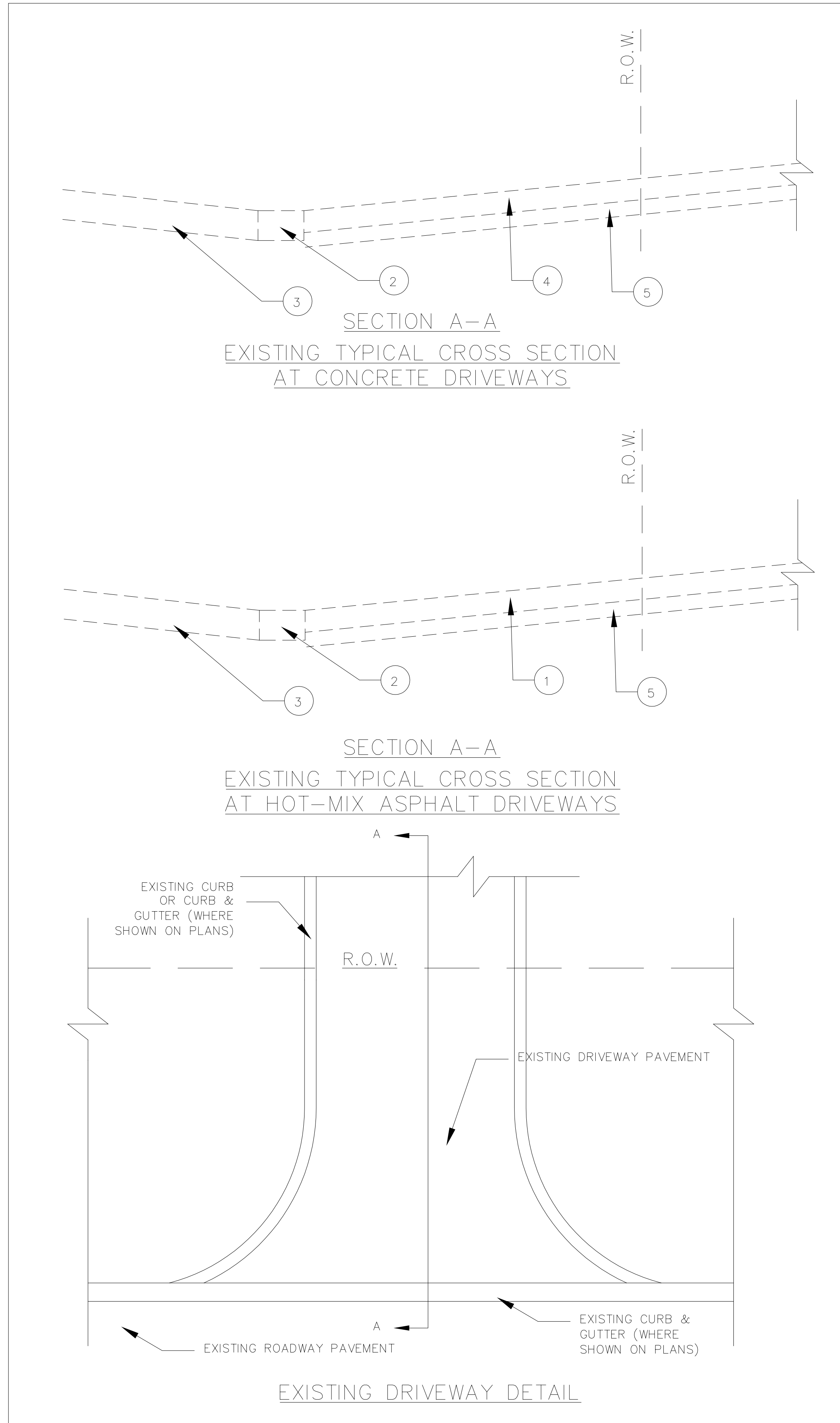
## SUMMARY OF QUANTITIES

CODE NUMBER	ITEM	UNIT	CONSTRUCTION CODE
			STP FUNDS
			70% FED, 30% LOCAL
			ROADWAY
			0021
URBAN			
			TOTAL QUANTITY
20100110	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNIT	80
20101100	TREE TRUNK PROTECTION	EACH	8
20101200	TREE ROOT PRUNING	EACH	8
20200100	EARTH EXCAVATION	CU YD	328
21101615	TOPSOIL FURNISH AND PLACE 4"	SQ YD	926
* 25000400	NITROGEN FERTILIZER NUTRIENT	POUND	11
* 25000500	PHOSPHOROUS FERTILIZER NUTRIENT	POUND	11
* 25000600	POTASIAM FERTILIZER NUTRIENT	POUND	11
* 25200110	SODDING, SALT TOLERANT	SQ YD	926
28000400	PERIMETER EROSION BARRIER	FOOT	1340
28000510	INLET FILTERS	EACH	2
35101600	AGGREGATE BASE COURSE, TYPE B 4"	SQ YD	1360
35102200	AGGREGATE BASE COURSE, TYPE B 10"	SQ YD	106
40201000	AGGREGATE FOR TEMPORARY ACCESS	TON	10
40603335	HOT MIX ASPHALT SURFACE COURSE, MIX "D", N50	TON	19
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	10873
42400410	PORTLAND CEMENT CONCRETE SIDEWALK 8 INCH	SQ FT	525
42400800	DETECTABLE WARNINGS	SQ FT	150
~ 44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	50
~ 44000300	CURB REMOVAL	FOOT	48
~ 44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	166
~ 60255500	MANHOLES TO BE ADJUSTED	EACH	2
60603800	COMBINATION CONCRETE CURB AND GUTTER, TYPE B6-12	FOOT	224
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	145
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	2

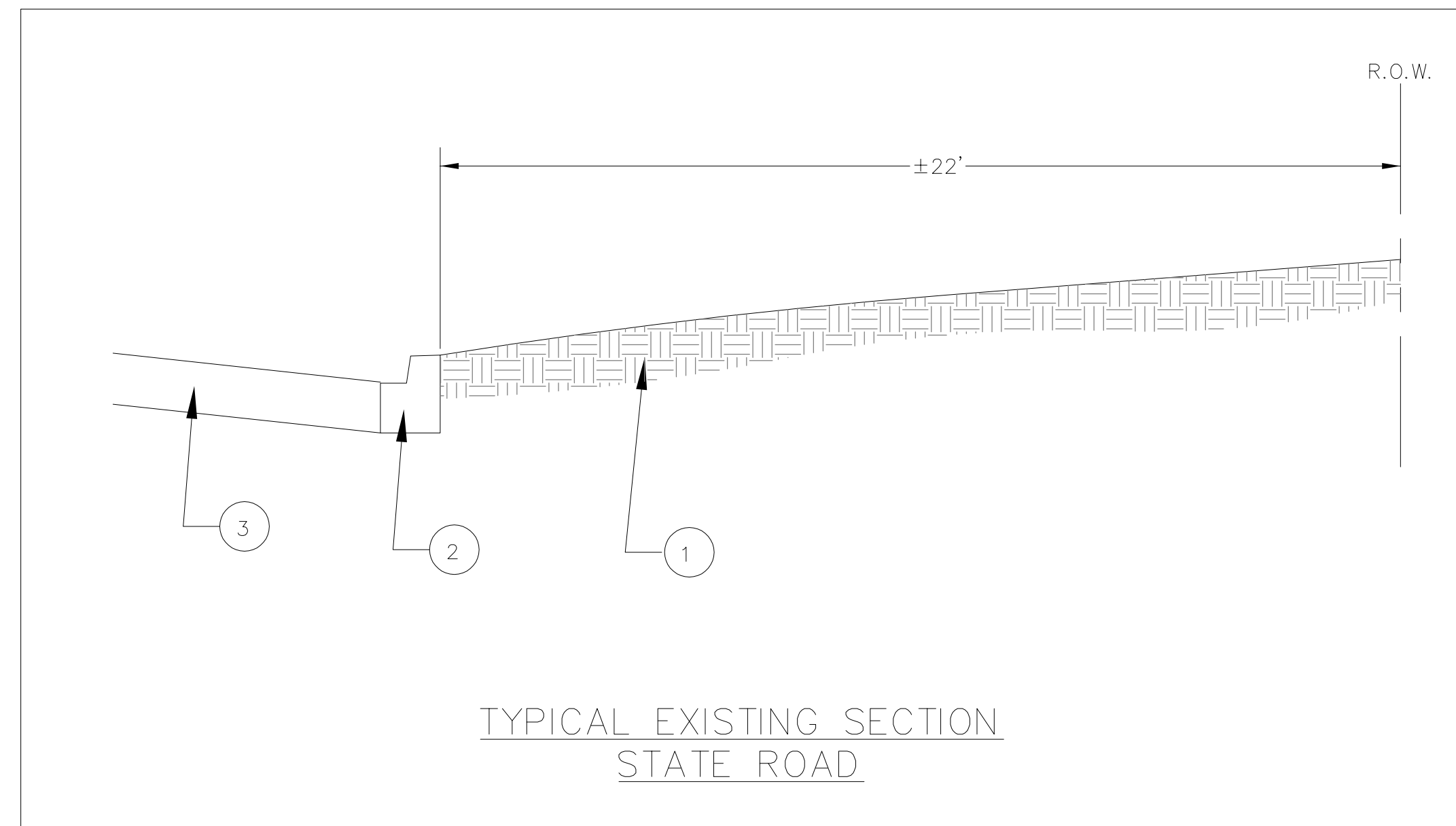
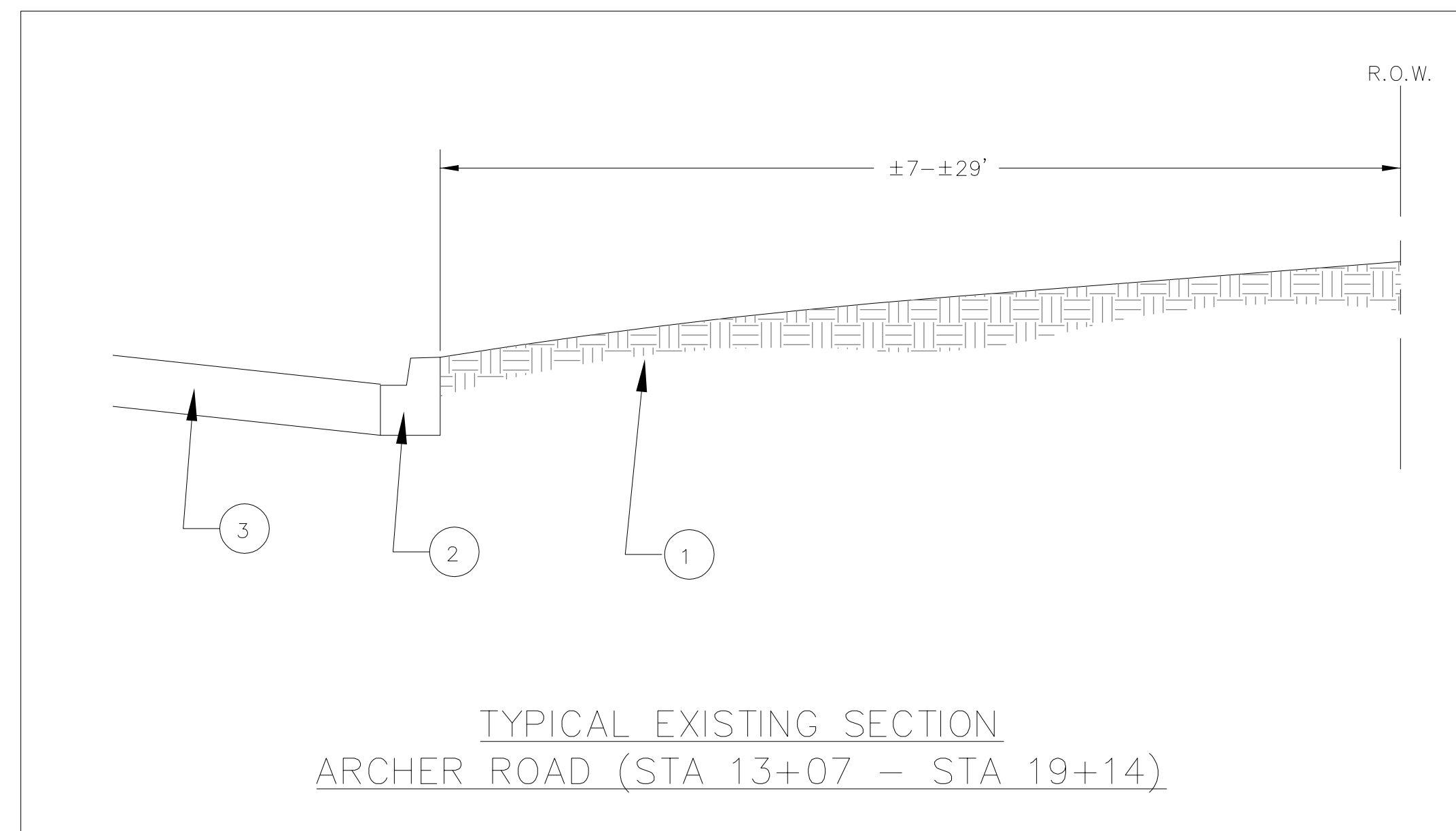
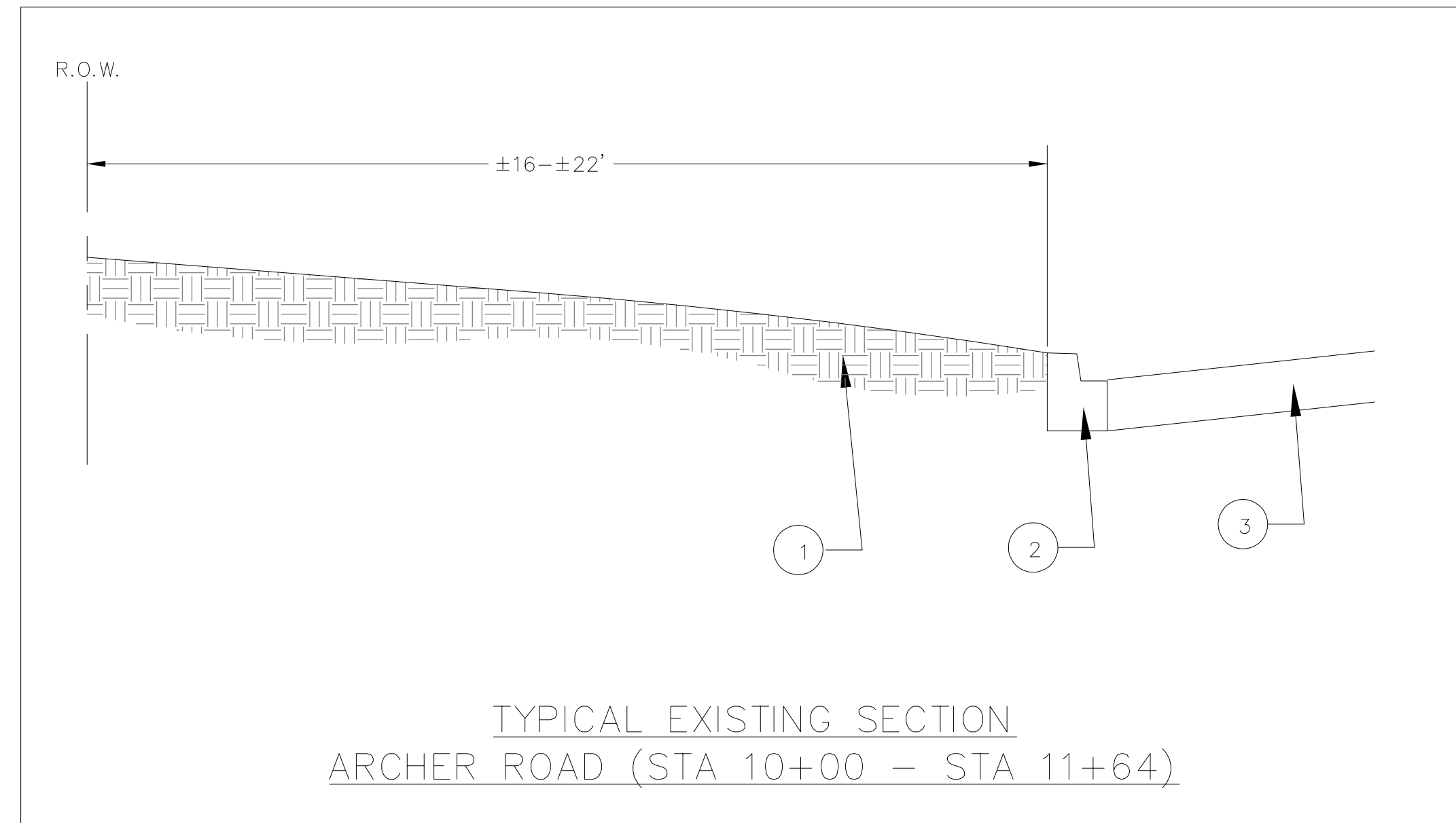
CODE NUMBER	ITEM	UNIT	CONSTRUCTION CODE
			STP FUNDS
			70% FED, 30% LOCAL
			ROADWAY
			0021
URBAN			
			TOTAL QUANTITY
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	L SUM	1
* 66901002	ON-SITE MONITORING OF REGULATED SUBSTANCES	CAL DA	4
* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	L SUM	1
67100100	MOBILIZATION	L SUM	1
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1
* 78000600	THERMOPLASTIC PAVEMENT MARKING-LINE 12"	FOOT	222
* 78000650	THERMOPLASTIC PAVEMENT MARKING-LINE 24"	FOOT	182
* 81028210	UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.	FOOT	35
~* 85000200	MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
~* 87301215	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	612
~* 87301225	ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	640
~* 87502440	TRAFFIC SIGNAL POST, GALVANIZED STEEL 10 FT.	EACH	2
~* 87800100	CONCRETE FOUNDATION, TYPE A	FOOT	8
* 87900200	DRILL EXISTING HANDHOLE	EACH	2
~* 88102717	PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	4
* 88800100	PEDESTRIAN PUSH-BUTTON	EACH	4
~* 89502200	MODIFY EXISTING CONTROLLER	EACH	1
~* 89502210	MODIFY EXISTING CONTROLLER CABINET	EACH	1
~* Z0033044	RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1
* A2004420	TREE, GINGKO BILBOA (GINGKO), 2-1/2" CALIPER, BALLED AND BURLAPPED	EACH	3
* A2004620	TREE, GLEDITSIA TRIACANTHOS INTERMIS (THORNLESS COMMON HONEYLOCUST) 2-1/2" CALIPER, BALLED AND BURLAPPED	EACH	5

\* INDICATES SPECIALTY ITEM  
~ INDICATES SPECIAL PROVISION ITEM

STA. 10+00 TO STA. 19+14 (ARCHER ROAD) STA. 10+00 TO STA. 25+60 (STATE ROAD)	DRAWN BY: ALS	REVISED: PER IDOT REVIEW	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>SUMMARY OF QUANTITIES</b>	FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	CHECKED BY: SAL	REVISED: _____			3565, 3577	12-00035-00-SW	COOK	30	3
DATE: 08/01/2018	REVISED: _____	CONTRACT No. 61F35							
	REVISED: _____	SCALE: N.T.S.		SHEET No. OF SHEETS	FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



- ① EXISTING HOT-MIX ASPHALT SURFACE, ±3"
- ② EXISTING CURB & GUTTER, B6.12 (DEPRESSED)
- ③ EXISTING ROADWAY PAVEMENT
- ④ EXISTING P.C.C. PAVEMENT, ±5"
- ⑤ EXISTING SUB-BASE GRANULAR MATERIAL



- ① EXISTING TURFED AREA OR AGGREGATE
- ② EXISTING CURB & GUTTER
- ③ EXISTING ROADWAY PAVEMENT

STA. 10+00 TO STA. 19+14 (ARCHER ROAD) STA. 10+00 TO STA. 25+60 (STATE ROAD)	DRAWN BY: ALS	REVISED: PER IDOT REVIEW
	CHECKED BY: SAL	REVISED: _____
	DATE: 08/01/2018	REVISED: _____
		REVISED: _____

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TYPICAL EXISTING SECTIONS  
DRIVEWAY DETAIL**

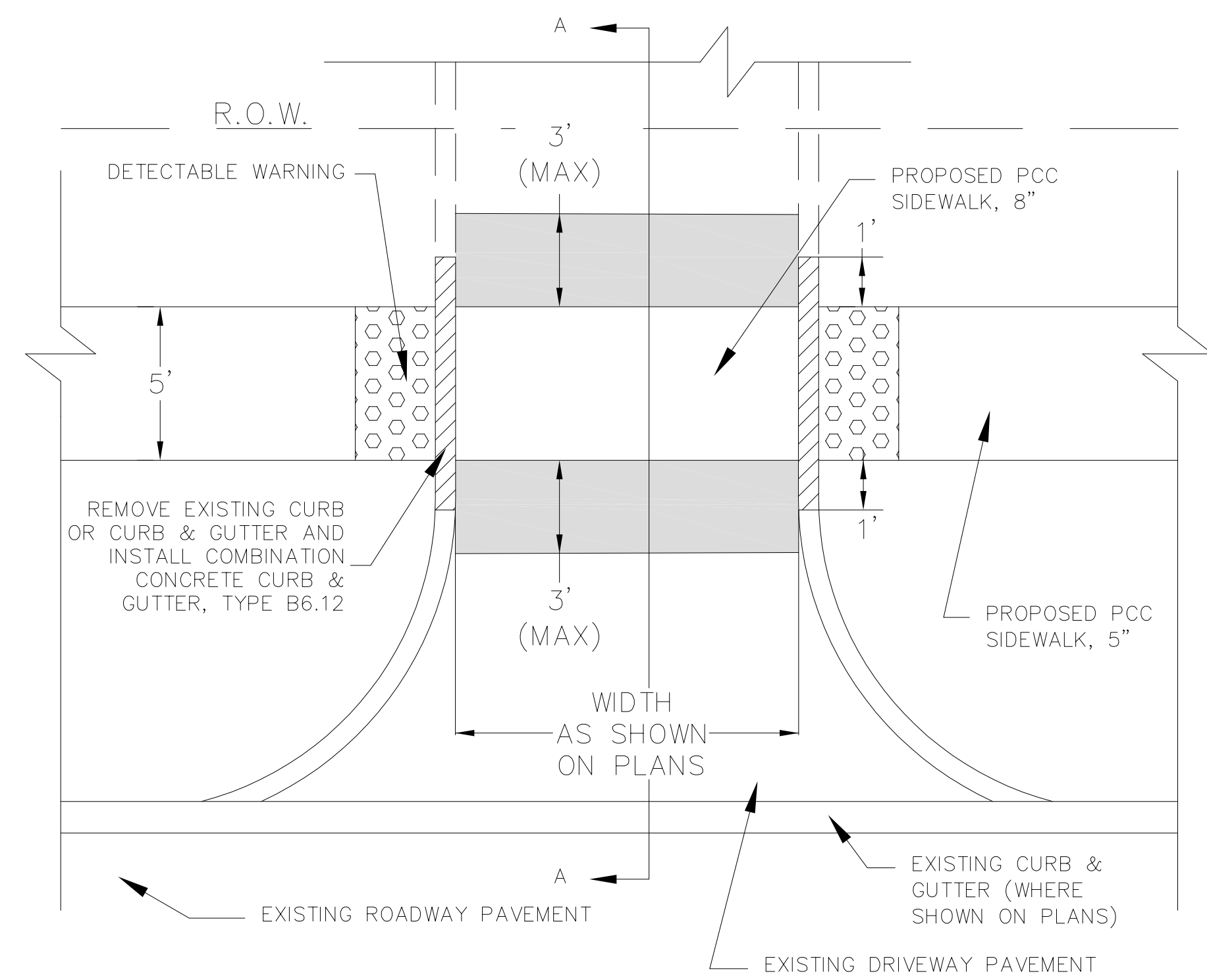
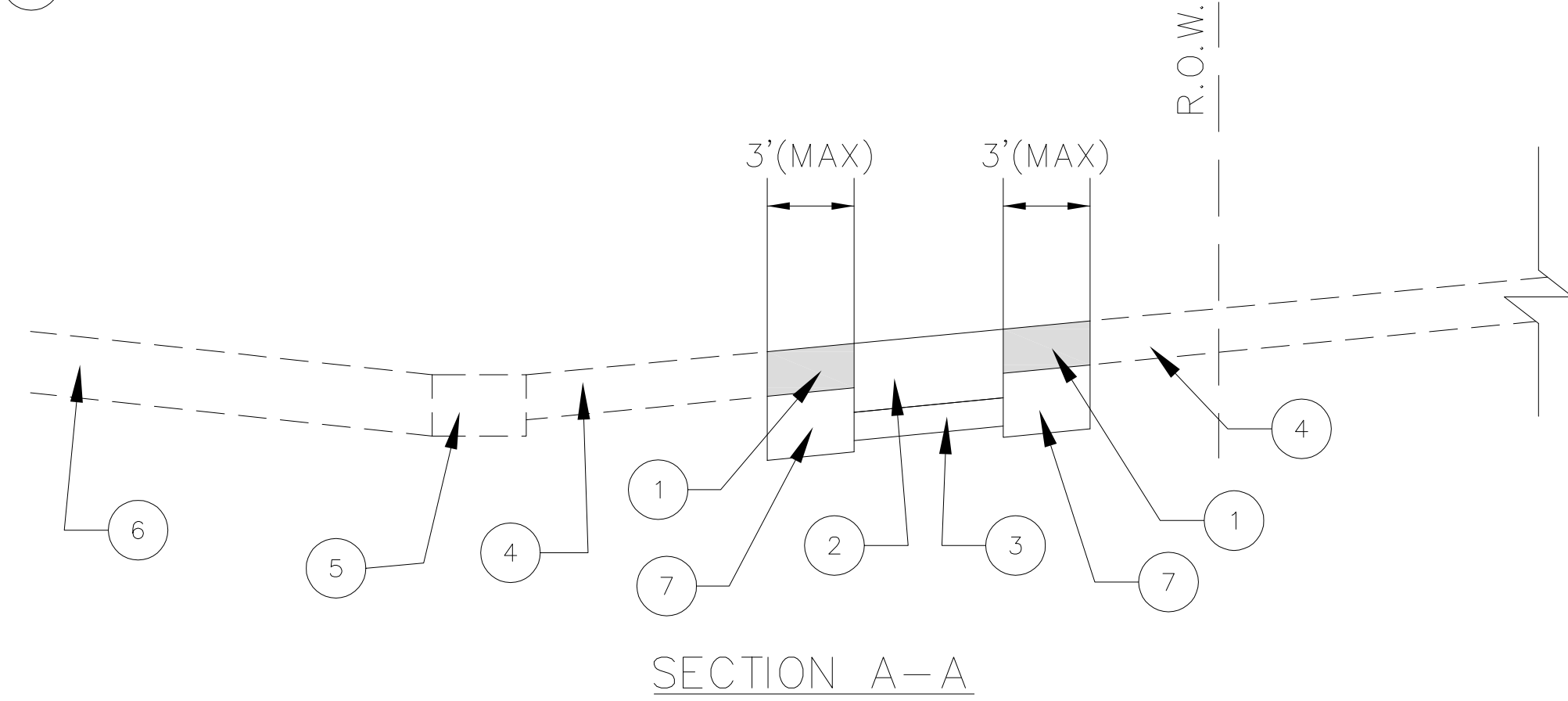
SCALE: N.T.S.

SHEET No. OF SHEETS

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3565, 3577	12-00035-00-SW	COOK	30	4
CONTRACT No. 61F35				
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



- 1 PROPOSED HOT-MIX ASPHALT SURFACE COURSE MIX "D", N50, 3"
- 2 PROPOSED PCC SIDEWALK, 8"
- 3 PROPOSED AGGREGATE BASE COURSE, TYPE B, 4"
- 4 EXISTING HOT-MIX ASPHALT DRIVEWAY
- 5 EXISTING CURB & GUTTER (WHERE SHOWN ON PLAN)
- 6 EXISTING ROADWAY PAVEMENT
- 7 PROPOSED AGGREGATE BASE COURSE, TYPE B, 10"



PROPOSED HOT-MIX ASPHALT DRIVEWAY DETAIL

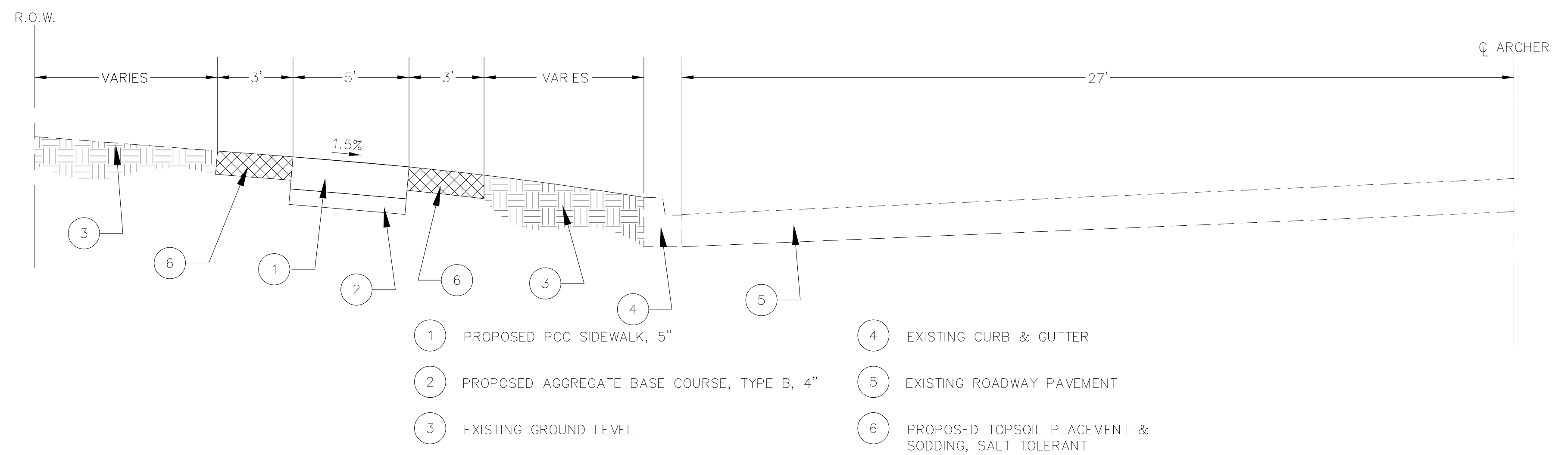
HOT-MIX ASPHALT MIXTURE REQUIREMENTS CHART

MIXTURE TYPE	AIR VOIDS AT Ndes
<b>DRIVEWAYS</b>	
HOT-MIX ASPHALT SURFACE COURSE, MIX 'D', N50 (IL 9.5mm), 3"	4% AT 50 GYR

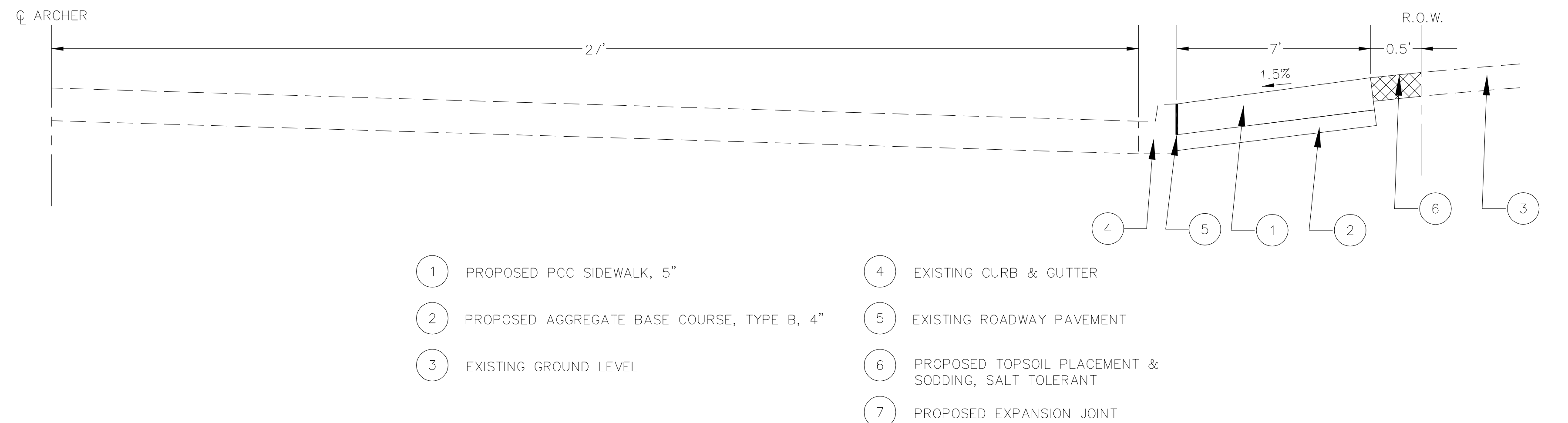
THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN

FOR THE NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

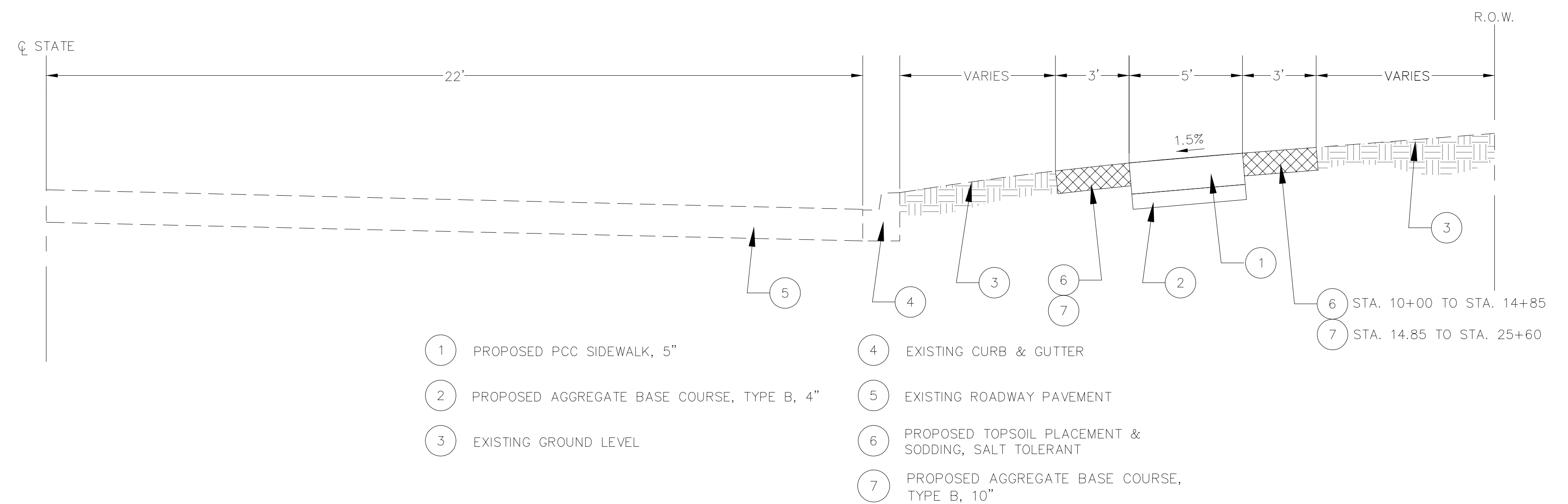
FOR USE OF RECYCLED MATERIALS SEE DISTRICT ONE SPECIAL PROVISIONS.



PROPOSED TYPICAL SECTION ARCHER ROAD (STA 10+00 - STA 11+63)



PROPOSED TYPICAL SECTION ARCHER ROAD (STA 12+25 - STA 16+25)



PROPOSED TYPICAL SECTION STATE ROAD

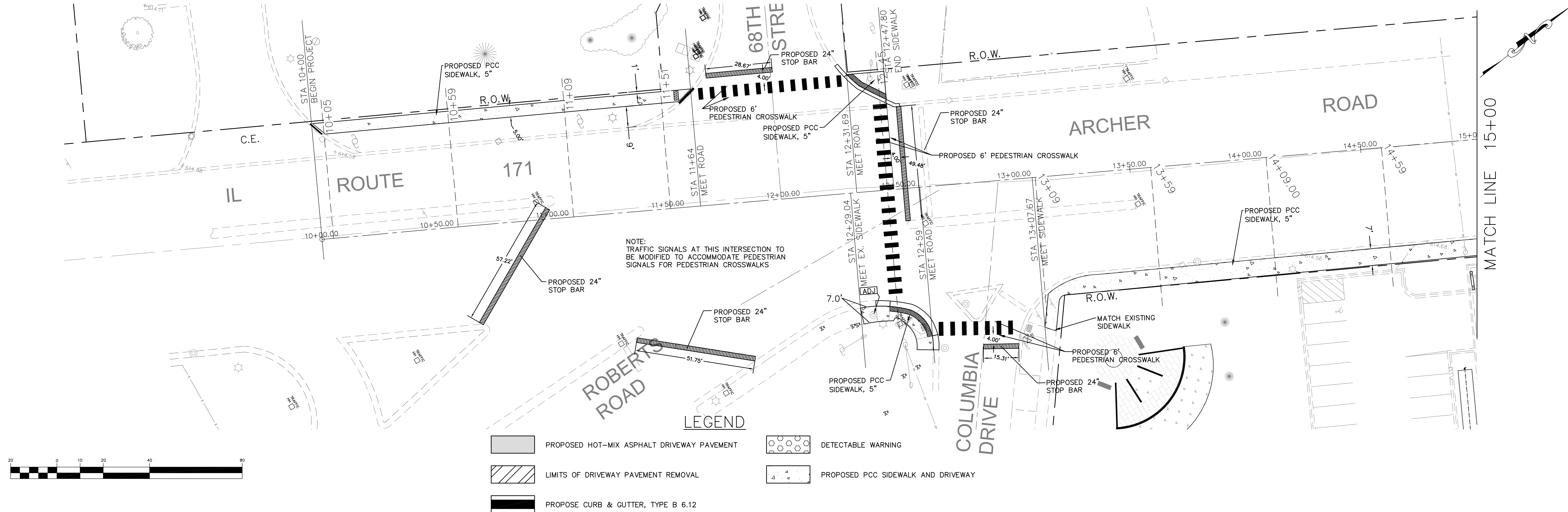
STA. 10+00 TO STA. 19+14 (ARCHER ROAD)	DRAWN BY: ALS	REVISED: PER IDOT REVIEW
STA. 10+00 TO STA. 25+60 (STATE ROAD)	CHECKED BY: SAL	REVISED: _____
	DATE: 08/01/2018	REVISED: _____
		REVISED: _____


**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>TYPICAL PROPOSED SECTIONS &amp; HOT-MIX ASPHALT MIXTURE CHART</b>	
SCALE: N.T.S.	SHEET No. OF SHEETS

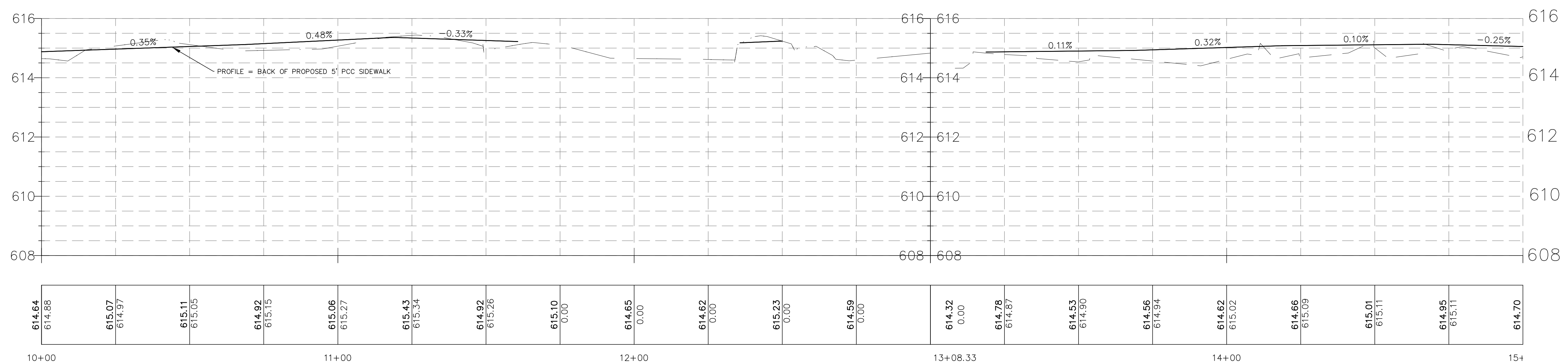
FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3565, 3577	12-00035-00-SW	COOK	30	5
CONTRACT No. 61F35				
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

HBE JOB No:11-517



- LEGEND**
- PROPOSED HOT-MIX ASPHALT DRIVEWAY PAVEMENT
  - LIMITS OF DRIVEWAY PAVEMENT REMOVAL
  - PROPOSE CURB & GUTTER, TYPE B 6.12
  - DETECTABLE WARNING
  - PROPOSED PCC SIDEWALK AND DRIVEWAY

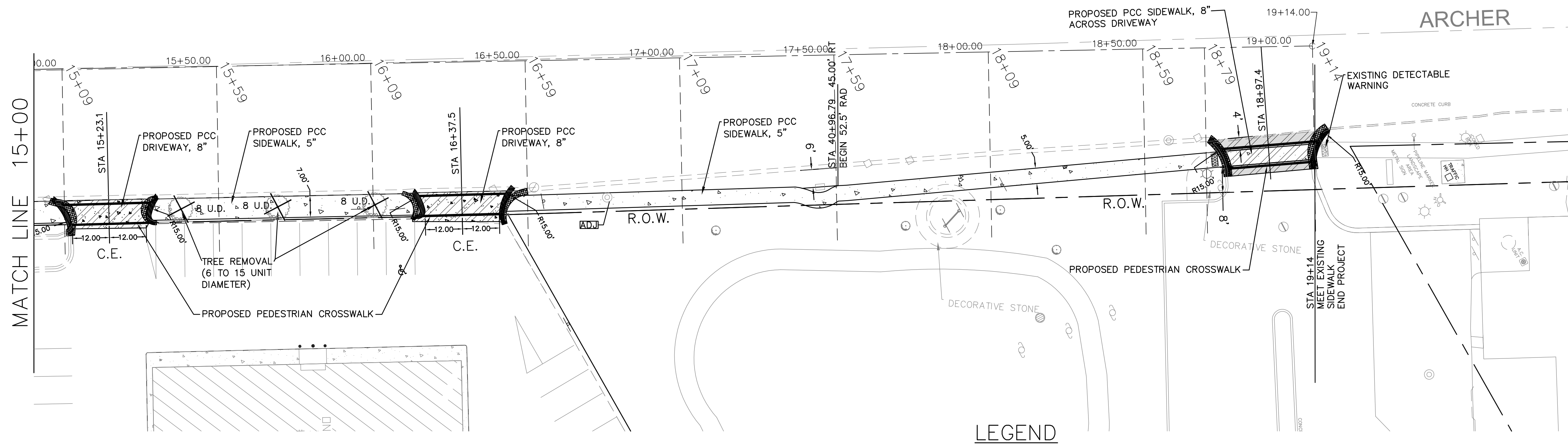
NOTE:  
TRAFFIC SIGNALS AT THIS INTERSECTION TO  
BE MODIFIED TO ACCOMMODATE PEDESTRIAN  
SIGNALS FOR PEDESTRIAN CROSSWALKS



STA. 10+00 TO STA. 19+14 (ARCHER ROAD) STA. 10+00 TO STA. 25+60 (STATE ROAD)	DRAWN BY: ALS	REVISED: PER IDOT REVIEW	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ARCHER ROAD PLAN AND PROFILE AND PAVEMENT MARKING SHEET STA 10+00 TO STA 15+00</b>	FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	CHECKED BY: SAL	REVISED: _____			3565, 3577	12-00035-00-SW	COOK	30	6
DATE: 08/01/2018	REVISED: _____	CONTRACT No. 61F35							
REVISED: _____	SCALE: H:1"=20' V:1"=2'	SHEET No. 1 OF 5 SHEETS		FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT			

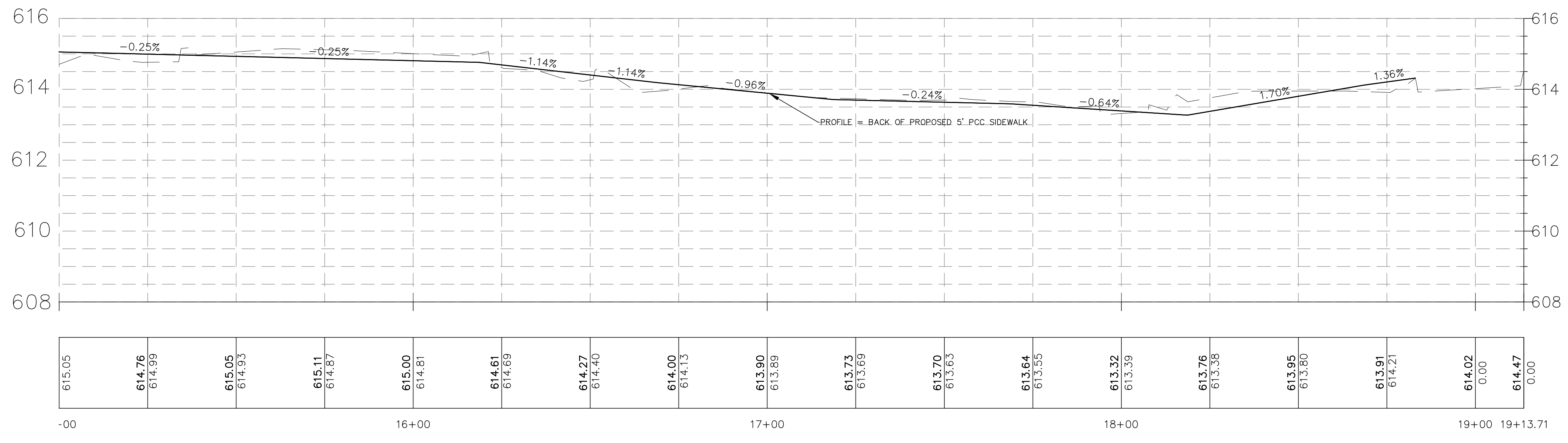
FILE JOB No. 11-517





**LEGEND**

- PROPOSED HOT-MIX ASPHALT DRIVEWAY PAVEMENT
- LIMITS OF DRIVEWAY PAVEMENT REMOVAL
- PROPOSED CURB & GUTTER, TYPE B 6.12
- DETECTABLE WARNING
- PROPOSED PCC SIDEWALK AND DRIVEWAY



STA. 10+00 TO STA. 19+14 (ARCHER ROAD)  
 STA. 10+00 TO STA. 25+60 (STATE ROAD)

DRAWN BY: ALS  
 CHECKED BY: SAL  
 DATE: 08/01/2018

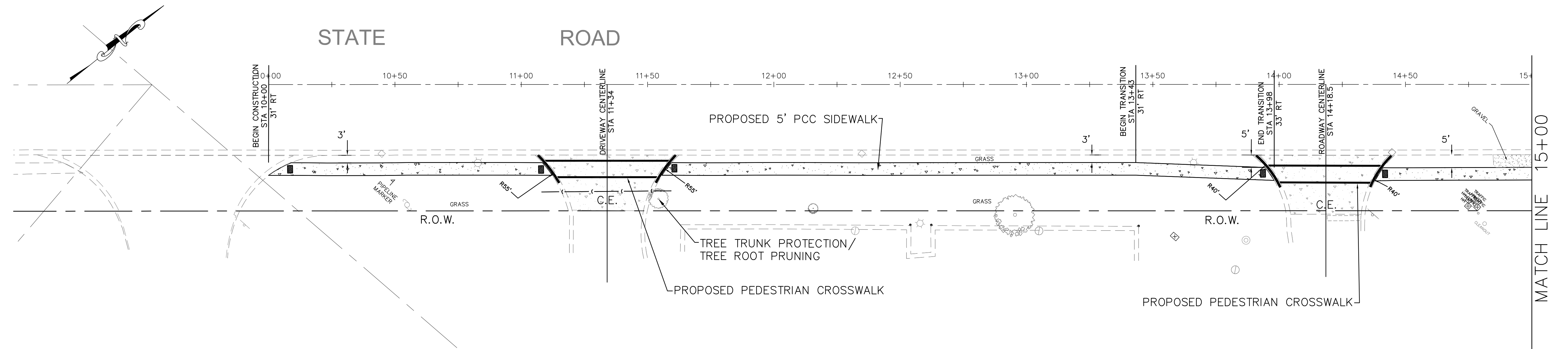
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**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**ARCHER ROAD PLAN AND PROFILE  
 AND PAVEMENT MARKING SHEET  
 STA 15+00 TO STA 19+14**

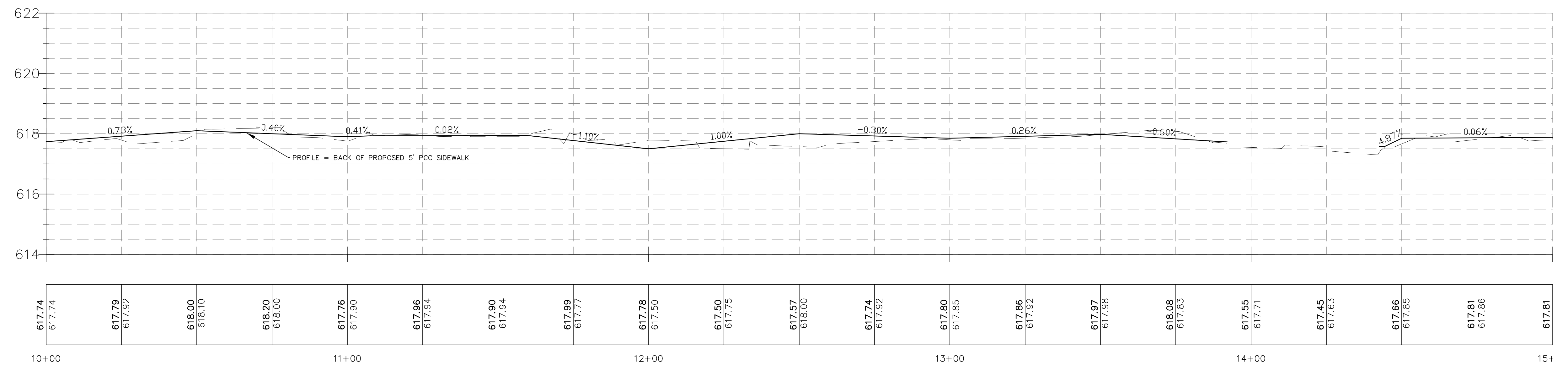
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FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT No. 61F35				
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	



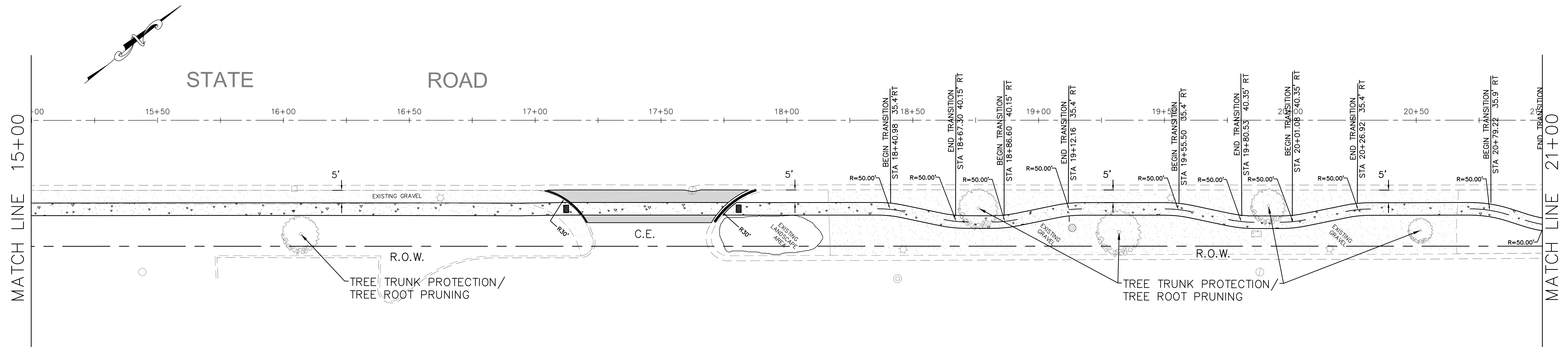
**LEGEND**

- PROPOSED HOT-MIX ASPHALT DRIVEWAY PAVEMENT
- LIMITS OF HOT-MIX ASPHALT SURFACE REMOVAL
- CURB & GUTTER REMOVAL & REPLACEMENT
- DETECTABLE WARNING
- PROPOSED PCC SIDEWALK



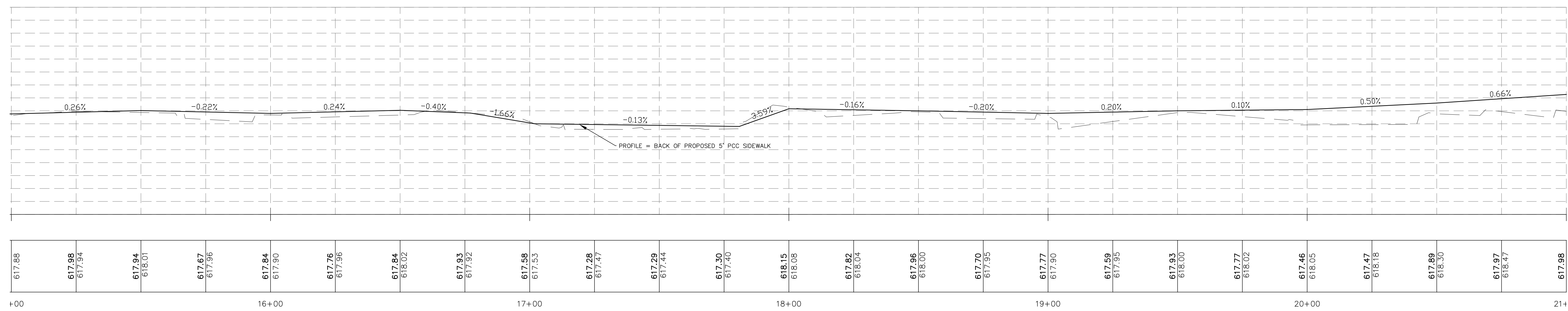
STA. 10+00 TO STA. 19+14 (ARCHER ROAD) STA. 10+00 TO STA. 25+60 (STATE ROAD)	DRAWN BY: ALS	REVISED: PER IDOT REVIEW	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STATE ROAD PLAN AND PROFILE AND PAVEMENT MARKING SHEET STA 10+00 TO STA 15+00</b>	FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	CHECKED BY: SAL	REVISED: _____			3565, 3577	12-00035-00-SW	COOK	30	8
DATE: 08/01/2018	REVISED: _____	CONTRACT No. 61F35							
REVISED: _____	SCALE: H: 1"=20' V: 1"=2'	SHEET No. 3 OF 5 SHEETS			FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		



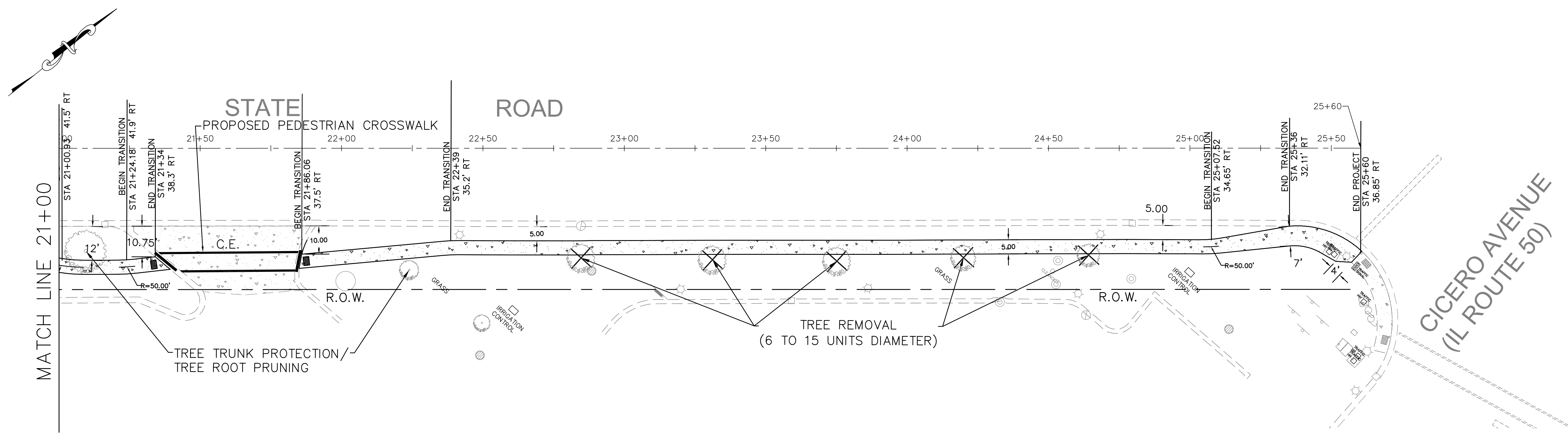


**LEGEND**

- PROPOSED HOT-MIX ASPHALT DRIVEWAY PAVEMENT
- LIMITS OF HOT-MIX ASPHALT DRIVEWAY REMOVAL
- CURB & GUTTER REMOVAL & REPLACEMENT
- DETECTABLE WARNING
- PROPOSED PCC SIDEWALK
- EXISTING GRAVEL
- EXISTING TREE

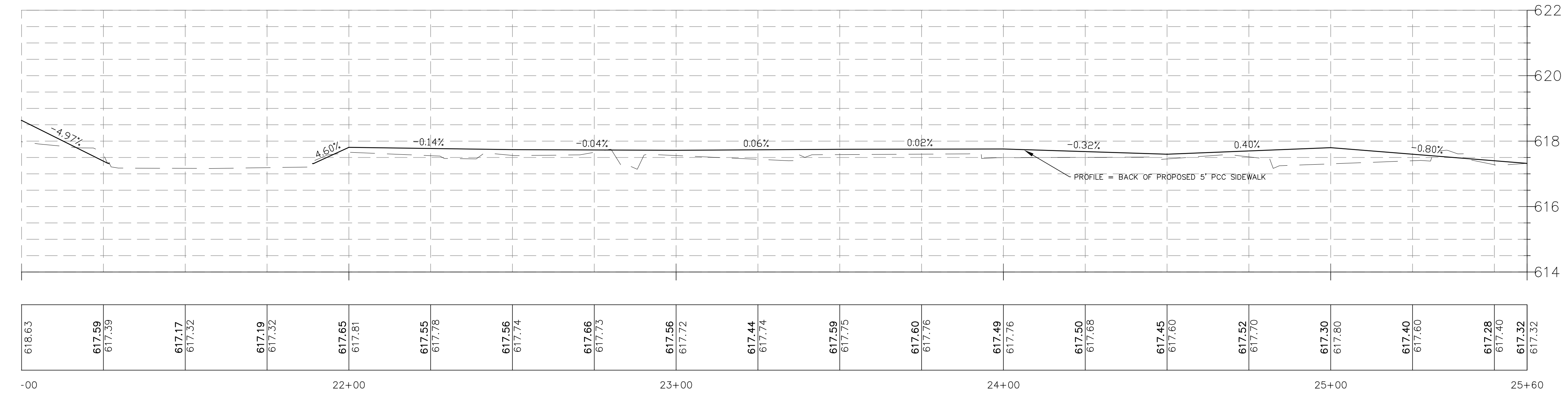


STA. 10+00 TO STA. 19+14 (ARCHER ROAD) STA. 10+00 TO STA. 25+60 (STATE ROAD)	DRAWN BY: ALS	REVISED: PER IDOT REVIEW	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>STATE ROAD PLAN AND PROFILE AND PAVEMENT MARKING SHEET STA 15+00 TO STA 21+00</b>	FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	CHECKED BY: SAL	REVISED: _____			3565, 3577	12-00035-00-SW	COOK	30	9
DATE: 08/01/2018	REVISED: _____	CONTRACT No. 61F35							
REVISED: _____	SCALE: H: 1"=20' V: 1"=2'				SHEET No. 4 OF 5 SHEETS	FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	



**LEGEND**

- PROPOSED HOT-MIX ASPHALT DRIVEWAY PAVEMENT
- LIMITS OF HOT-MIX ASPHALT SURFACE REMOVAL
- CURB & GUTTER REMOVAL & REPLACEMENT
- DETECTABLE WARNING
- PROPOSED PCC SIDEWALK



STA. 10+00 TO STA. 19+14 (ARCHER ROAD)  
 STA. 10+00 TO STA. 25+60 (STATE ROAD)

DRAWN BY: ALS  
 CHECKED BY: SAL  
 DATE: 08/01/2018

REVISED: PER IDOT REVIEW  
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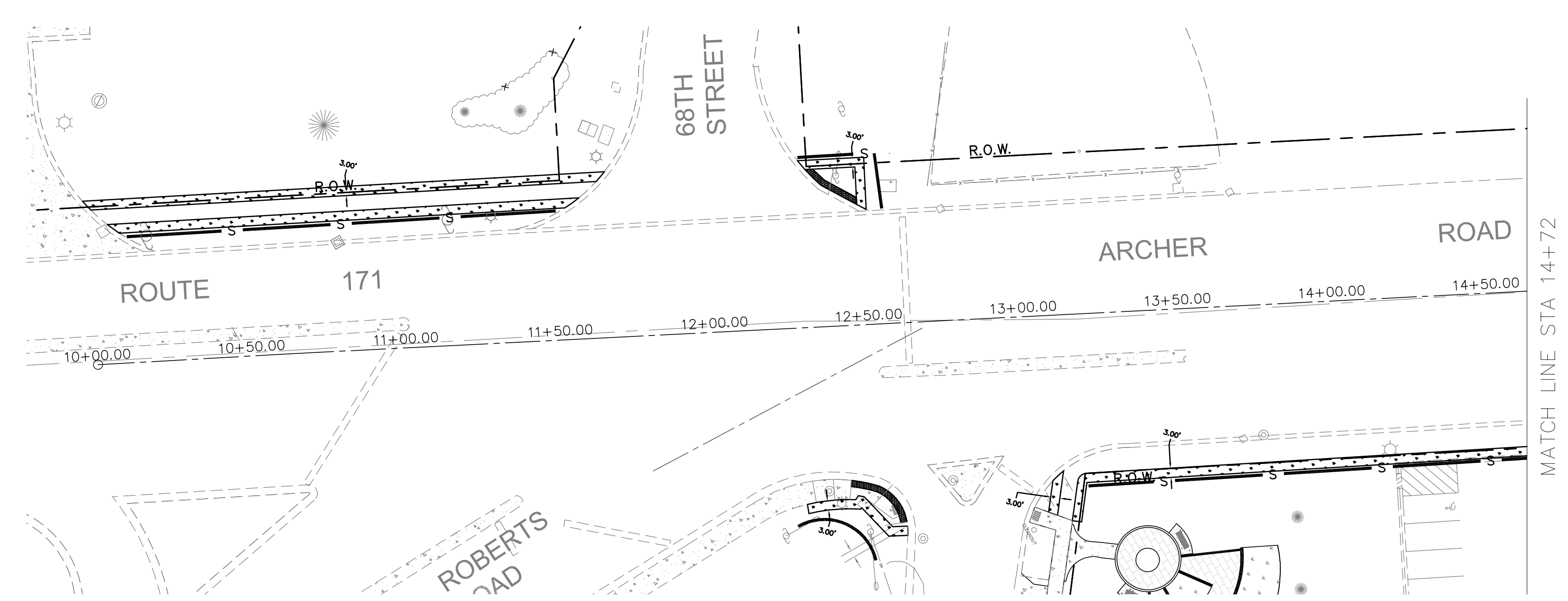
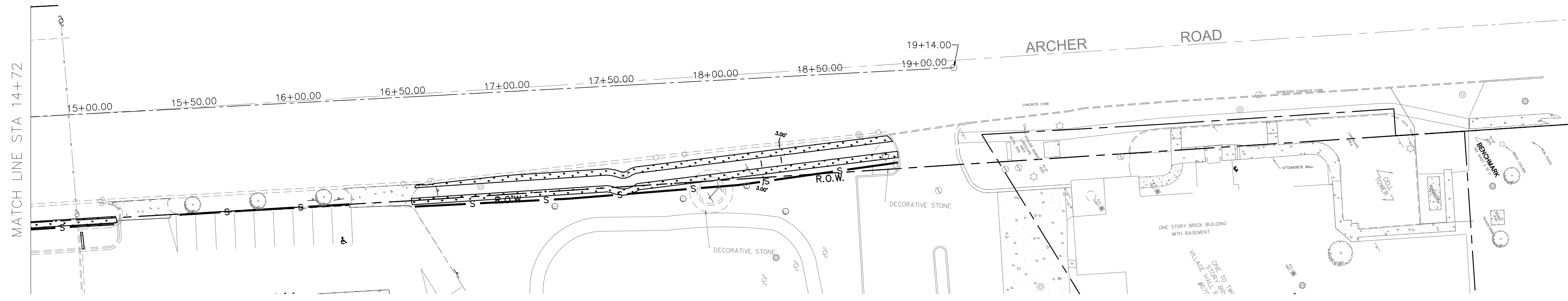
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**STATE ROAD PLAN AND PROFILE  
 AND PAVEMENT MARKING SHEET  
 STA 21+00 TO STA 25+60**

SCALE: H: 1"=20' V: 1"=2' SHEET No. 5 OF 5 SHEETS

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3565, 3577	12-00035-00-SW	COOK	30	10
CONTRACT No. 61F35				
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	





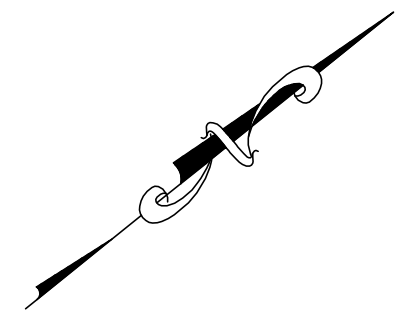
**LEGEND**

-  PERIMETER EROSION CONTROL
-  SODDING, SALT TOLERANT



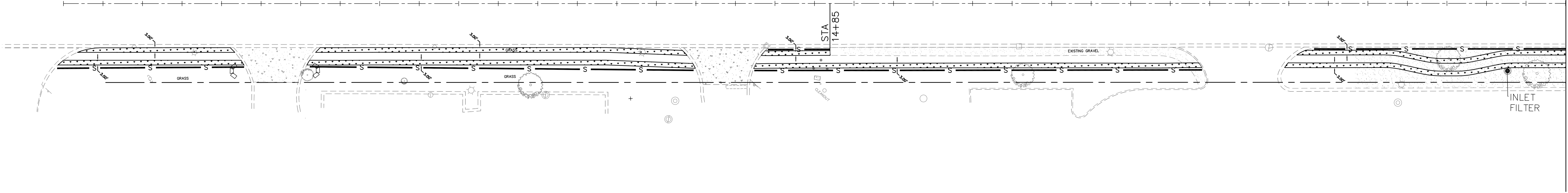
STA. 10+00 TO STA. 19+14 (ARCHER ROAD) STA. 10+00 TO STA. 25+60 (STATE ROAD)	DRAWN BY: ALS	REVISED: PER IDOT REVIEW	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ARCHER ROAD EROSION AND SEDIMENT CONTROL PLAN</b>		FAU RTE 3565, 3577	SECTION 12-00035-00-SW	COUNTY COOK	TOTAL SHEETS 30	SHEET NO. 11
	CHECKED BY: SAL	REVISED: _____		CONTRACT No. 61F35						
DATE: 08/01/2018	REVISED: _____	SCALE: H:1"=30'		SHEET No. 1 OF 3 SHEETS	FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT			
	REVISED: _____									

FILE JOB No. 11-517

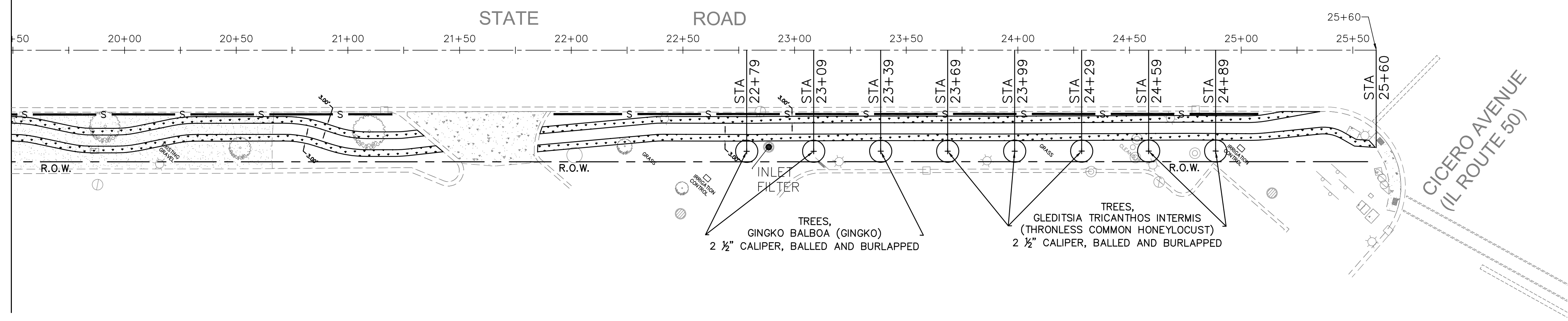


STATE ROAD STATE ROAD


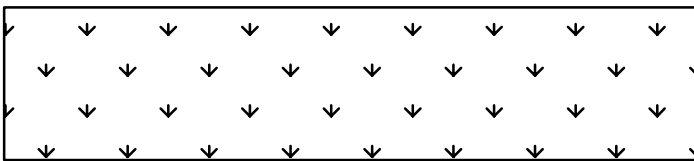
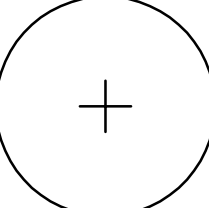
10+00 10+50 11+00 11+50 12+00 12+50 13+00 13+50 14+00 14+50 15+00 15+50 16+00 16+50 17+00 17+50 18+00 18+50 19+00



MATCH LINE 19+50



**LEGEND**

-  PERIMETER EROSION CONTROL
-  SODDING, SALT TOLERANT
-  TREES



STA. 10+00 TO STA. 19+14 (ARCHER ROAD) STA. 10+00 TO STA. 25+60 (STATE ROAD)
DRAWN BY: ALS
CHECKED BY: SAL
DATE: 08/01/2018

REVISED: PER IDOT REVIEW
REVISED: _____
REVISED: _____
REVISED: _____

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**STATE ROAD  
EROSION AND SEDIMENT CONTROL PLAN**

SCALE: H: 1"=30'

SHEET No. 2 OF 3 SHEETS

FAU RTE 3565, 3577	SECTION 12-00035-00-SW	COUNTY COOK	TOTAL SHEETS 30	SHEET NO. 12
CONTRACT No. 61F35				
FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT	

FILE JOB No. 11-517



# EROSION AND SEDIMENT CONTROL NOTES

THE FOLLOWING PLAN IS ESTABLISHED AND INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM SEWER WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE UNDER NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE CONSTRUCTION SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS IDENTIFIED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIME FRAME SPECIFIED HEREIN AND AS OUTLINED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, CAN BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS STIPULATED BY THE ENGINEER, LOCAL MUNICIPAL CODES, AND AS SHOWN IN IDOT STANDARD 280001 WHICH SUPPLEMENTS THIS PLAN. THE TEMPORARY EROSION CONTROL PROVISIONS OUTLINED IN SECTION 280 OF THE IDOT STANDARD SPECIFICATIONS ADDITIONALLY SUPPLEMENT THIS PLAN.

## DESCRIPTION OF SITE AND PLANNED CONSTRUCTION ACTIVITIES

1. THE PROJECT IS LOCATED IN THE VILLAGE OF BEDFORD PARK, ILLINOIS.
2. THE TOTAL AREA OF THE PROJECT SITE IS APPROXIMATELY 0.57 ACRES. THE AREA DISTURBED BY CONSTRUCTION ACTIVITIES IS APPROXIMATELY 0.90 ACRES.
3. THE PROJECT INVOLVES EXCAVATION OF EXISTING GROUND FOR INSTALLATION OF CONCRETE SIDEWALK, AND REPLACEMENT OF EXISTING DRIVEWAYS DUE TO AFOREMENTIONED SIDEWALKS.

## SEQUENCE FOR MAJOR CONSTRUCTION ACTIVITIES

THE FOLLOWING DESCRIBES THE PROJECTED SEQUENCE OF MAJOR CONSTRUCTION ACTIVITIES WHICH WILL DISTURB SOILS WITHIN THE PROJECT AREA.

1. INSTALLATION OF TEMPORARY EROSION CONTROL MEASURES REQUIRED TO LIMIT SOIL MIGRATION DURING SITE CLEARING, GRADING, FILLING, AND SUBSEQUENT DEVELOPMENT ACTIVITIES. MEASURES MAY INCLUDE PERIMETER EROSION CONTROL BARRIER, INLET AND PIPE PROTECTION, EROSION CONTROL BLANKETS, AND OTHER SIMILAR MEASURES. THE MEASURES ARE TO BE ADJUSTED IN KEEPING WITH SITE WORK PROGRESS.
2. CLEARING AND GRUBBING OF PROJECT SITE CONSISTENT WITH PHASING OF SIDEWALK CONSTRUCTION.
3. EXCAVATION AND GRADING TO ACHIEVE INTERIM WORK SITE ELEVATIONS.
4. FORMING AND INSTALLATION OF SIDEWALK.
5. FINISHED GRADING AND LANDSCAPE RESTORATION.
6. MAINTENANCE, REMOVAL, AND PROPER CLEAN-UP OF TEMPORARY EROSION CONTROL MEASURES.

## OTHER REPORTS, STUDIES, PLANS, SPECIFICATIONS WHICH ARE REFERENCED DOCUMENTS OF THE STORM WATER POLLUTION PREVENTION PLAN

1. INFORMATION CONCERNING THE SOILS CHARACTER AND TERRAIN WITHIN THE SITE WAS OBTAINED FROM TOPOGRAPHIC SURVEYS AND SOIL MAPS THAT WERE UTILIZED FOR THE DEVELOPMENT OF THE PROPOSED TEMPORARY EROSION CONTROL SYSTEMS.
2. PROJECT SPECIFICATIONS, SPECIAL PROVISIONS, AND DRAWINGS INDICATING DRAINAGE PATTERNS AND APPROXIMATE SLOPES ANTICIPATED AFTER GRADING ACTIVITIES, WERE UTILIZED FOR THE PROPOSED PLACEMENT OF THE TEMPORARY EROSION CONTROL SYSTEMS.
3. ILLINOIS URBAN MANUAL FOR SOIL EROSION CONTROL GUIDANCE AND CONTROL MEASURES.
4. IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION FOR SOIL EROSION CONTROL MATERIALS AND INSTALLATION GUIDANCE
5. LOCAL MUNICIPAL REQUIREMENTS FOR EROSION CONTROL.

## EROSION AND SEDIMENT CONTROL PRACTICES

THE DRAWINGS, SPECIFICATIONS AND SPECIAL PROVISIONS HAVE BEEN PLANNED TO ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE FEASIBLE, AND DISTURBED PORTIONS OF THE SITE ARE STABILIZED. STABILIZATION PRACTICES INCLUDE TEMPORARY SEEDING, PERMANENT SEEDING, MULCHING, PROTECTION OF TREES, PRESERVATION OF NATURAL VEGETATION, AND OTHER APPROPRIATE MEASURES AS IDENTIFIED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 7 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.

1. AREAS OF EXISTING VEGETATION, WOOD AND GRASSLANDS, OUTSIDE THE PROPOSED CONSTRUCTION LIMITS SHALL BE PROTECTED FROM CONSTRUCTION ACTIVITIES.
2. DEAD, DISEASED, UNSUITABLE, OR CONFLICTING VEGETATION WITHIN THE SITE SHALL BE REMOVED, ALONG WITH REQUIRED TREE REMOVAL.
3. TEMPORARY DITCH CHECKS, INLET AND PIPE PROTECTION, AND PERIMETER EROSION BARRIER SHALL BE INSTALLED PRIOR TO STRIPPING OF EXISTING VEGETATION.
4. BARE AND SPARSELY VEGETATED GROUND IN HIGH ERODABLE AREAS SHALL BE TEMPORARILY SEEDED AT THE BEGINNING OF CONSTRUCTION WHERE NO CONSTRUCTION ACTIVITIES ARE EXPECTED WITHIN SEVEN (7) DAYS.
5. IMMEDIATELY AFTER TREE REMOVAL IS COMPLETED, AREAS WHICH ARE HIGHLY ERODABLE AS DETERMINED BY THE ENGINEER, SHALL BE TEMPORARILY SEEDED WHEN NO CONSTRUCTION ACTIVITIES ARE EXPECTED WITHIN SEVEN (7) DAYS.
6. AT LOCATIONS WHERE A SIGNIFICANT AMOUNT OF WATER DRAINS INTO THE CONSTRUCTION ZONE FROM OUTSIDE AREAS ON ADJACENT LANDOWNERS, TEMPORARY DITCH CHECKS SHALL BE UTILIZED TO LOCALLY DIVERT WATER, REDUCE FLOW RATES, AND COLLECT OUTSIDE SILTATION INSIDE THE RIGHT-OF-WAY LINE.
7. DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS INDICATED ON THE PLANS SHALL BE PROTECTED FROM DAMAGE. THE CONTRACTOR SHALL NOT USE THESE AREAS FOR STAGING, PARKING OF VEHICLES OF CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS OR OTHER CONSTRUCTION RELATED ACTIVITIES EXCEPT AS DESCRIBED ON THE PLANS AND INDICATED BY THE ENGINEER.
8. WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION SHALL REMAIN UNDISTURBED UNTIL THE STAGE OF CONSTRUCTION IS INITIATED TO AVOID UNNECESSARY SOIL EROSION.
9. EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED IF THEY ARE TO REMAIN UNUSED FOR MORE THAN FOURTEEN (14) DAYS.
10. AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL PLACE TEMPORARY EROSION CONTROL FACILITIES AT NECESSARY LOCATIONS. THE ERODABLE EXPOSED EARTH SURFACES SHALL BE TEMPORARILY SEEDED ON A WEEKLY BASIS TO MINIMIZE THE AMOUNT OF EROSION THAT WILL OCCUR WITHIN THE PROJECT LIMITS. THE ROADSIDE DITCHES WITHIN THE PROJECT LIMITS SHALL BE MAINTAINED OR CONSTRUCTED AS REQUIRED TO LIMIT EROSION. TEMPORARILY DIVERT WATER AROUND PROPOSED CULVERT LOCATIONS UNTIL EROSION CONTROL MEASURES ARE IN PLACE.
11. CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR OTHER POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.
12. THE CONTRACTOR SHALL INSPECT THE PROJECT DAILY DURING CONSTRUCTION ACTIVITIES. INSPECTION SHALL ALSO BE DONE WEEKLY AND AFTER RAINS OF 1/2-INCH OR GREATER OR EQUIVALENT SNOWFALL AND DURING THE WINTER SHUTDOWN PERIOD. THE PROJECT SHALL ADDITIONALLY BE INSPECTED BY THE WORK SITE CONSTRUCTION MANAGER ON A BI-WEEKLY BASIS TO DETERMINE THAT EROSION CONTROL EFFORTS ARE IN PLACE AND EFFECTIVE AND IF OTHER EROSION CONTROL WORK IS NECESSARY.
13. SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON THE SITE ON A REGULAR BASIS. THE COST OF THIS MAINTENANCE SHALL BE INCLUDED IN THE UNIT BID PRICE FOR EACH EXCAVATION FOR EROSION CONTROL.
14. THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED 30 DAYS AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. AT THAT TIME, THE EROSION CONTROL SYSTEMS SHALL BE REMOVED, AND THE AREAS DAMAGED BY THE SYSTEMS' INSTALLATION SHALL BE RESTORED.
15. TEMPORARY DITCH CHECKS SHALL BE LOCATED AT EVERY 1 FOOT FALL OR RISE IN DITCH GRADE OR AS STIPULATED ON THE PLAN.
16. WHERE REQUIRED, THE TEMPORARY EROSION CONTROL SEEDING SHALL BE APPLIED AT A RATE OF 100 LBS PER ACRE OF AREA.
17. WATER PUMPED FROM EXCAVATIONS SHALL BE DIRECTED INTO THE SEDIMENT BASIN. THE SEDIMENT BASIN SHALL BE MAINTAINED AT A DEPTH BETWEEN 2 AND 3 FEET AND CLEARED OF SEDIMENT AS REQUIRED. SEDIMENT SHALL BE DISPOSED OF IN A MANNER NOT TO INCREASE SOIL MIGRATION.
18. ALL TEMPORARY AND PERMANENT EROSION CONTROL MEASURES MUST BE MAINTAINED AND REPAIRED AS NEEDED. THE CONTRACTOR SHALL BE ULTIMATELY RESPONSIBLE FOR MAINTENANCE AND REPAIR.
19. THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER OR GOVERNING AGENCY.

## MAINTENANCE AFTER CONSTRUCTION

1. THE CONSTRUCTION WILL BE CONSIDERED TO BE COMPLETE AFTER ACCEPTANCE BY THE MUNICIPAL AGENCY WITH JURISDICTION OVER THE IMPROVEMENTS. MAINTENANCE OF EROSION MEASURES AS WELL AS THE SITE IMPROVEMENTS WILL BE THE RESPONSIBILITY OF THE CONTRACTOR UNTIL THE DATE OF ACCEPTANCE.
2. TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED SEEDED OR SODDED AREAS HAVE BEEN ESTABLISHED.
3. ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED SURFACES RESTORED.

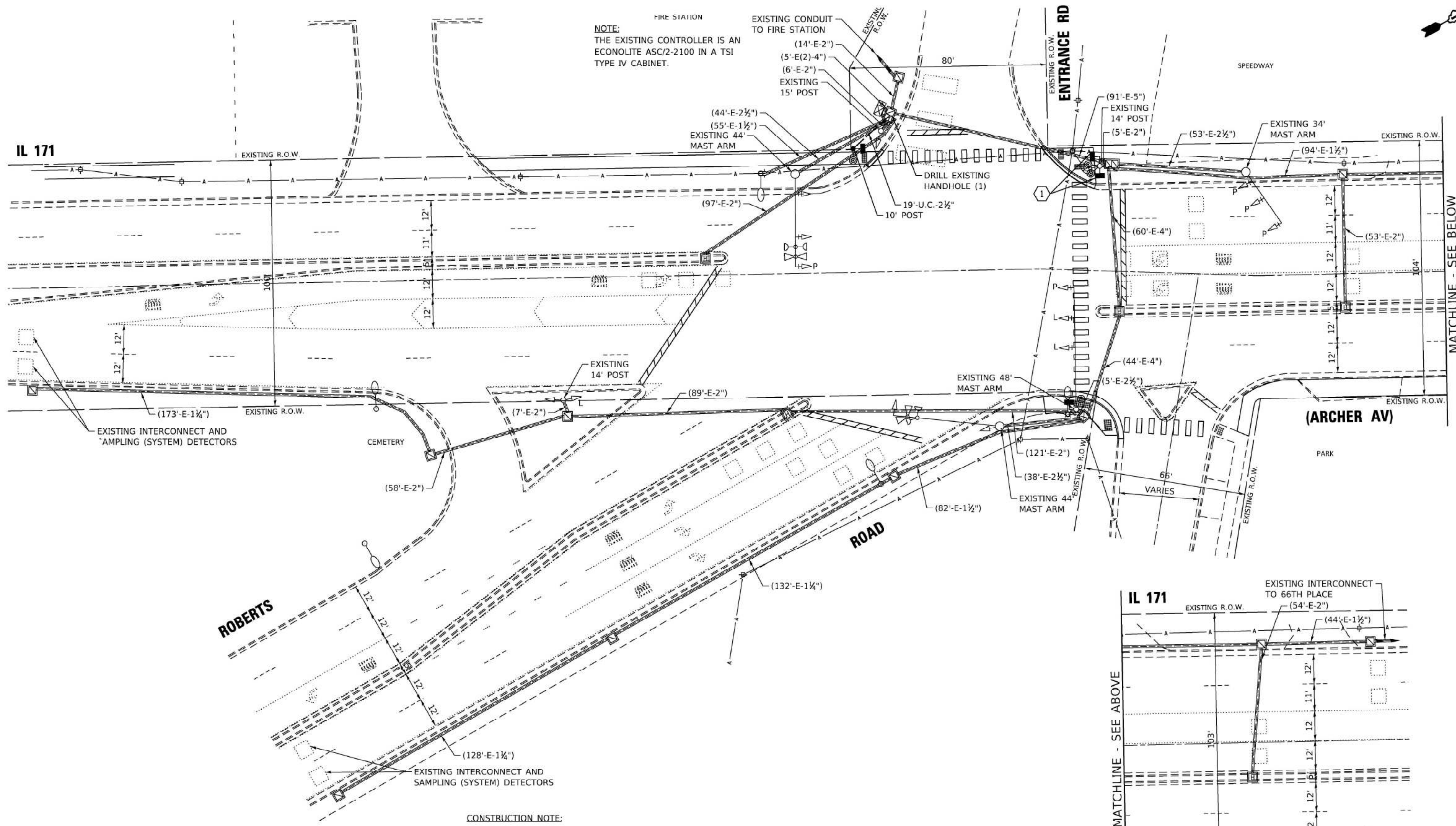
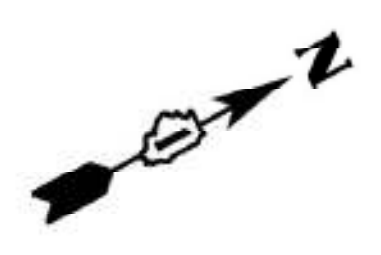
A COPY OF THE SWPPP (INCLUDING A COPY OF THE PERMIT), NOI, AND ACKNOWLEDGEMENT LETTER FROM EPA MUST BE RETAINED AT THE CONSTRUCTION SITE (OR OTHER LOCATION EASILY ACCESSIBLE DURING NORMAL BUSINESS HOURS TO EPA, A STATE, TRIBAL OR LOCAL AGENCY APPROVING SEDIMENT AND EROSION PLANS, GRADING PLANS, OR STORM WATER MANAGEMENT PLANS; LOCAL GOVERNMENT OFFICIALS; THE OPERATOR OF A MUNICIPAL SEPARATE STORM SEWER RECEIVING DISCHARGES FROM THE SITE; AND REPRESENTATIVES OF THE U.S. FISH AND WILDLIFE SERVICE OR THE NATIONAL MARINE FISHERIES SERVICE) FROM THE DATE OF COMMENCEMENT OF CONSTRUCTION ACTIVITIES TO THE DATE OF FINAL STABILIZATION. IF YOU HAVE DAY-TO-DAY OPERATIONAL CONTROL OVER SWPPP IMPLEMENTATION, YOU MUST HAVE A COPY OF THE SWPPP AVAILABLE AT A CENTRAL LOCATION ON-SITE FOR THE USE OF ALL THOSE IDENTIFIED AS HAVING RESPONSIBILITIES UNDER THE SWPPP WHENEVER THEY ARE ON THE CONSTRUCTION SITE. IF AN ON-SITE LOCATION IS UNAVAILABLE TO STORE THE SWPPP WHEN NO PERSONNEL ARE PRESENT, NOTICE OF THE PLAN'S LOCATION MUST BE POSTED NEAR THE MAIN ENTRANCE AT THE CONSTRUCTION SITE.

A SIGN OR OTHER NOTICE MUST BE POSTED CONSPICUOUSLY NEAR THE MAIN ENTRANCE OF THE CONSTRUCTION SITE. IF DISPLAYING NEAR THE MAIN ENTRANCE IS INFEASIBLE, THE NOTICE CAN BE POSTED IN A LOCAL PUBLIC BUILDING SUCH AS THE TOWN HALL OR PUBLIC LIBRARY. THE SIGN OR OTHER NOTICE MUST CONTAIN THE FOLLOWING INFORMATION:

1. A COPY OF THE COMPLETED NOTICE OF INTENT AS SUBMITTED TO THE EPA STORM WATER NOTICE PROCESSING CENTER; AND
2. IF THE LOCATION OF THE SWPPP OR THE NAME AND TELEPHONE NUMBER OF THE CONTACT PERSON FOR SCHEDULING SWPPP VIEWING TIMES HAS CHANGED (I.E., IS DIFFERENT THAN THAT SUBMITTED TO EPA IN THE NOI), THE CURRENT LOCATION OF THE SWPPP AND NAME AND TELEPHONE NUMBER OF A CONTACT PERSON FOR SCHEDULING VIEWING TIMES.

STA. 10+00 TO STA. 19+14 (ARCHER ROAD) STA. 10+00 TO STA. 25+60 (STATE ROAD)	DRAWN BY: ALS	REVISED: PER IDOT REVIEW	<b>STATE OF ILLINOIS</b> <b>DEPARTMENT OF TRANSPORTATION</b>	<b>ARCHER ROAD / STATE ROAD</b> <b>EROSION AND SEDIMENT CONTROL NOTES</b>	FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	CHECKED BY: SAL	REVISED: _____			3565, 3577	12-00035-00-SW	COOK	30	13
	DATE: 08/01/2018	REVISED: _____			CONTRACT No. 61F35				
		REVISED: _____		SCALE: N.T.S.	SHEET No. 3 OF 3 SHEETS	FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT	



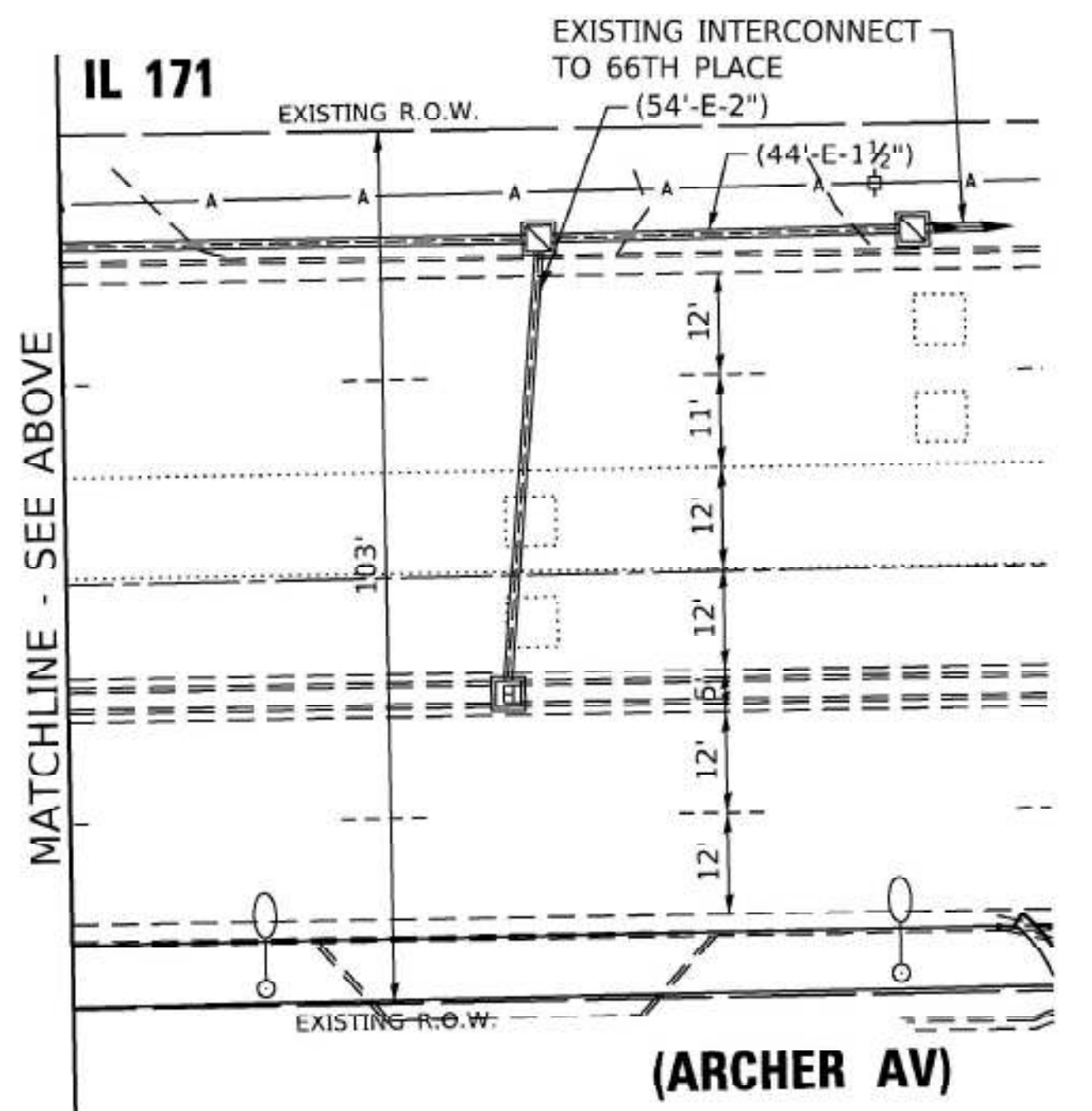


**NOTE:**  
 THE EXISTING CONTROLLER IS AN ECONOLITE ASC/2-2100 IN A TSI TYPE IV CABINET.

**CONSTRUCTION NOTE:**  
 ① THE CONTRACTOR SHALL MOUNT THE PEDESTRIAN PUSH-BUTTON STATION ON EXTENSION BRACKETS. THE COST OF THE EXTENSION BRACKETS SHALL BE INCLUDED IN THE COST OF "PEDESTRIAN PUSH-BUTTON" ITEM.

SEE GRADING PLANS FOR LIMITS OF SIDEWALK AND CURB AND GUTTER REMOVAL AND REPLACEMENT.

THE TRAFFIC SIGNAL CONTROL EQUIPMENT FOR THIS PROJECT SHALL BE "ECONOLITE" TO MATCH THE EXISTING ADJACENT SYSTEM.

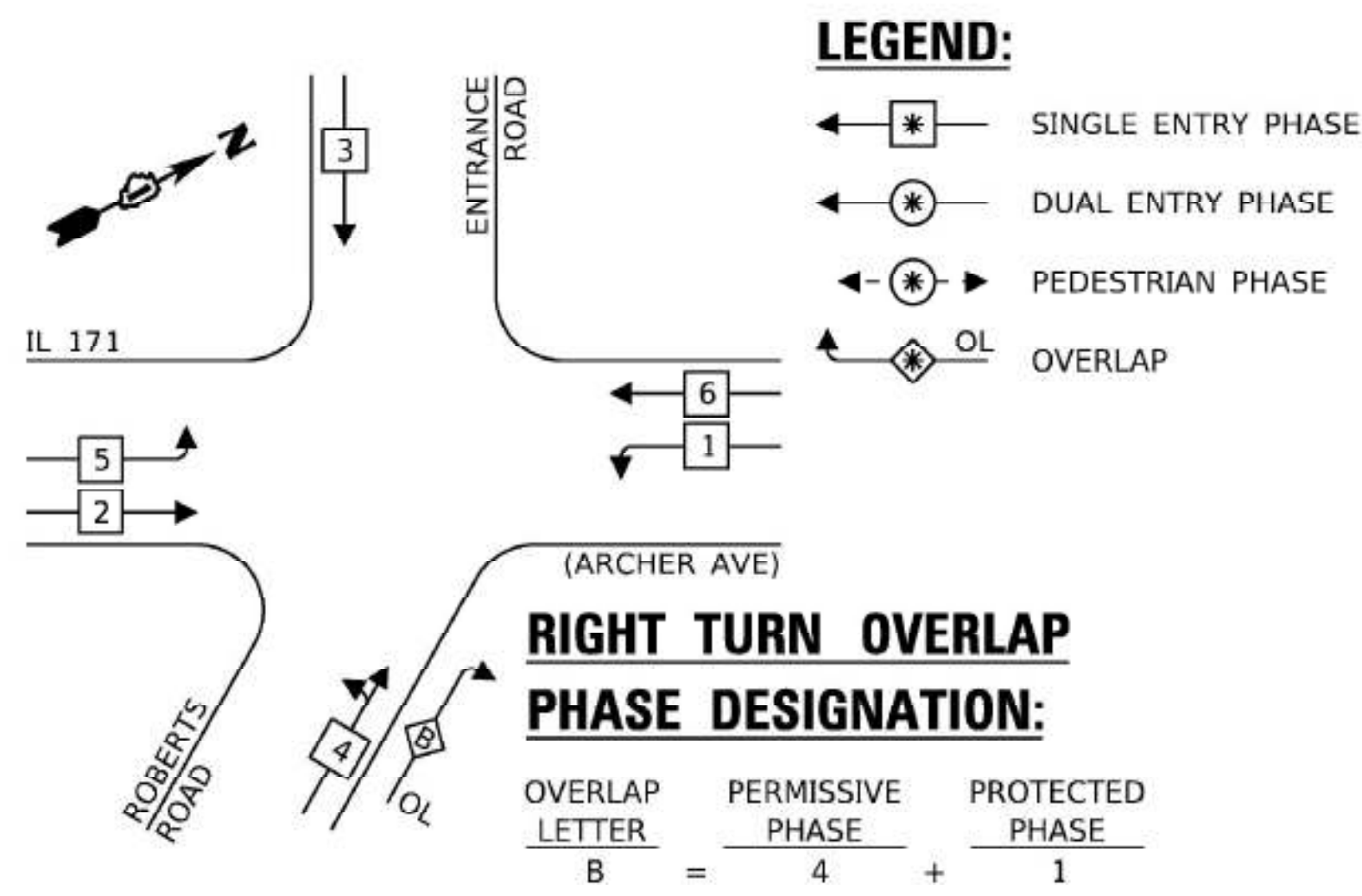


**GHA GEWALT HAMILTON ASSOCIATES, INC. TS SHT NO. 1**

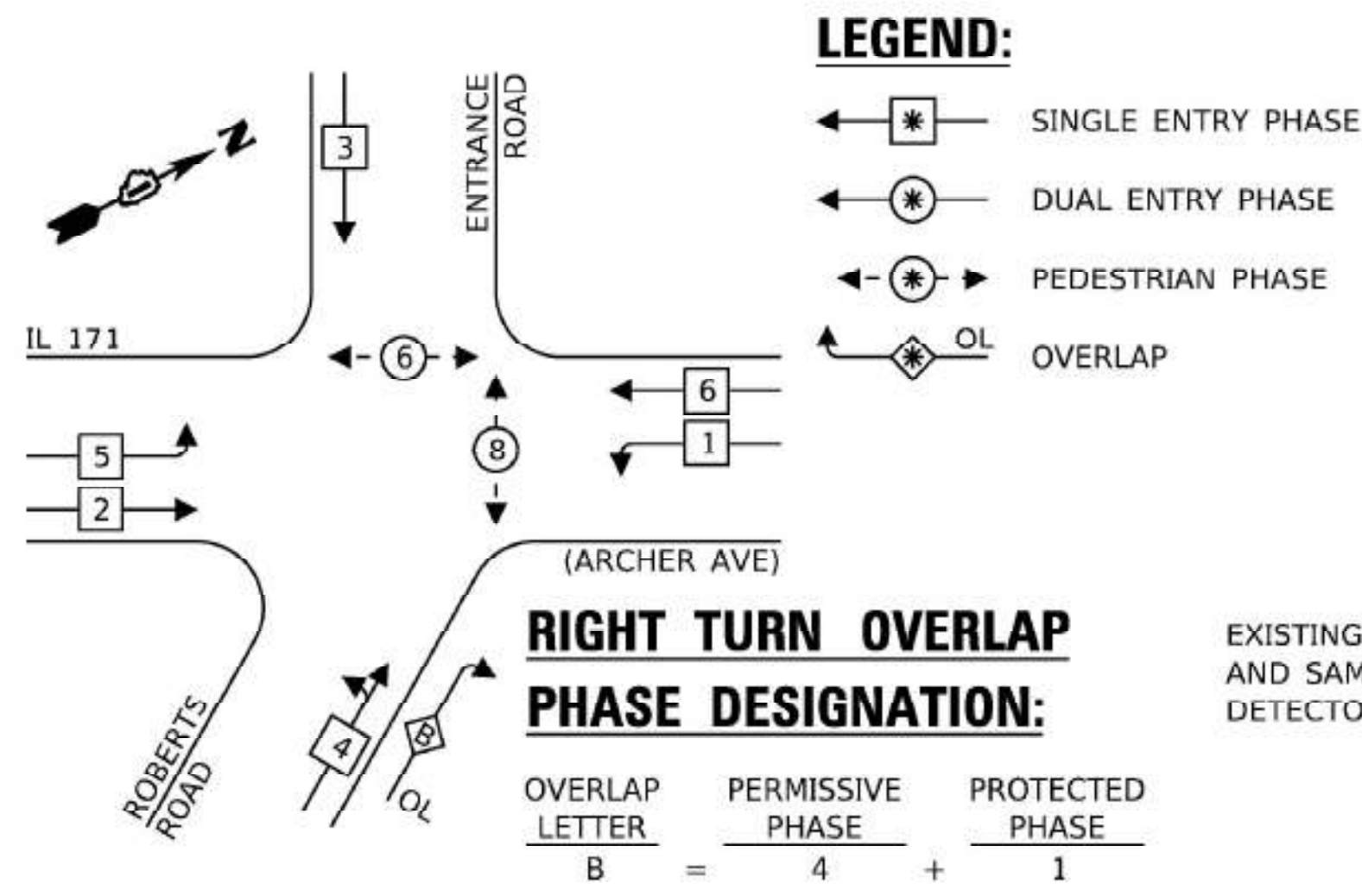
FILE NAME = Perm-Sig.dgn 4841.800 Default	USER NAME = zwoilsten	DESIGNED - JRD	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC SIGNAL MODERNIZATION PLAN IL 171 (ARCHER AVE) AT ROBERTS ROAD</b>	F.A.U. RTE. = 3577	SECTION = 12-00035-00-SW	COUNTY = COOK	TOTAL SHEETS = 30	SHEET NO. = 14	
	PLOT SCALE = 1:200	DRAWN - ZCW	REVISED -			CONTRACT NO. = 61F35					
	PLOT DATE = 7/30/2018	CHECKED - DPB	REVISED -			SCALE: 1"=20'	SHEET OF SHEETS STA. TO STA.	ILLINOIS FED. AID PROJECT			
		DATE -	REVISED -								



**EXISTING CONTROLLER SEQUENCE**



**PROPOSED CONTROLLER SEQUENCE**

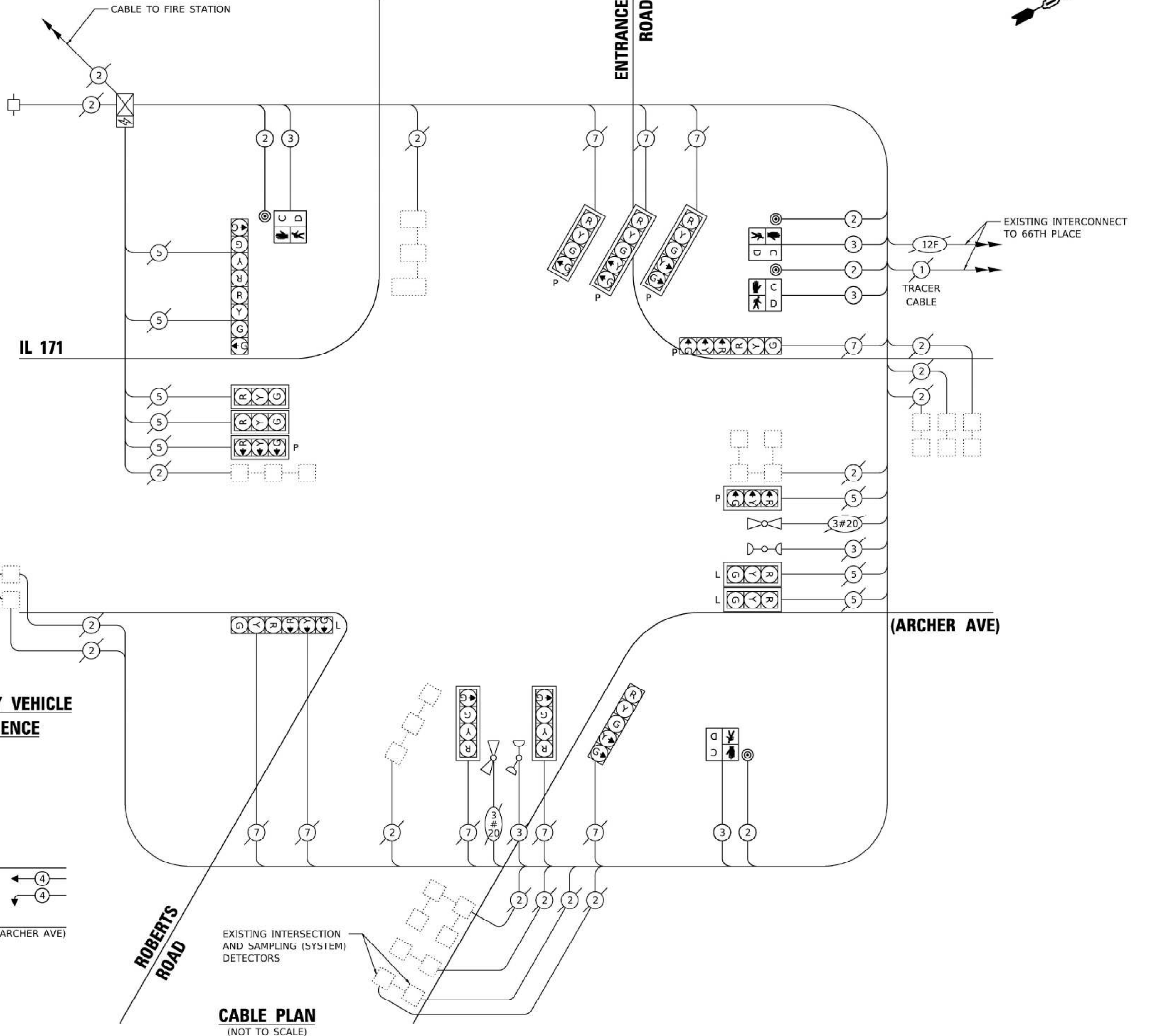
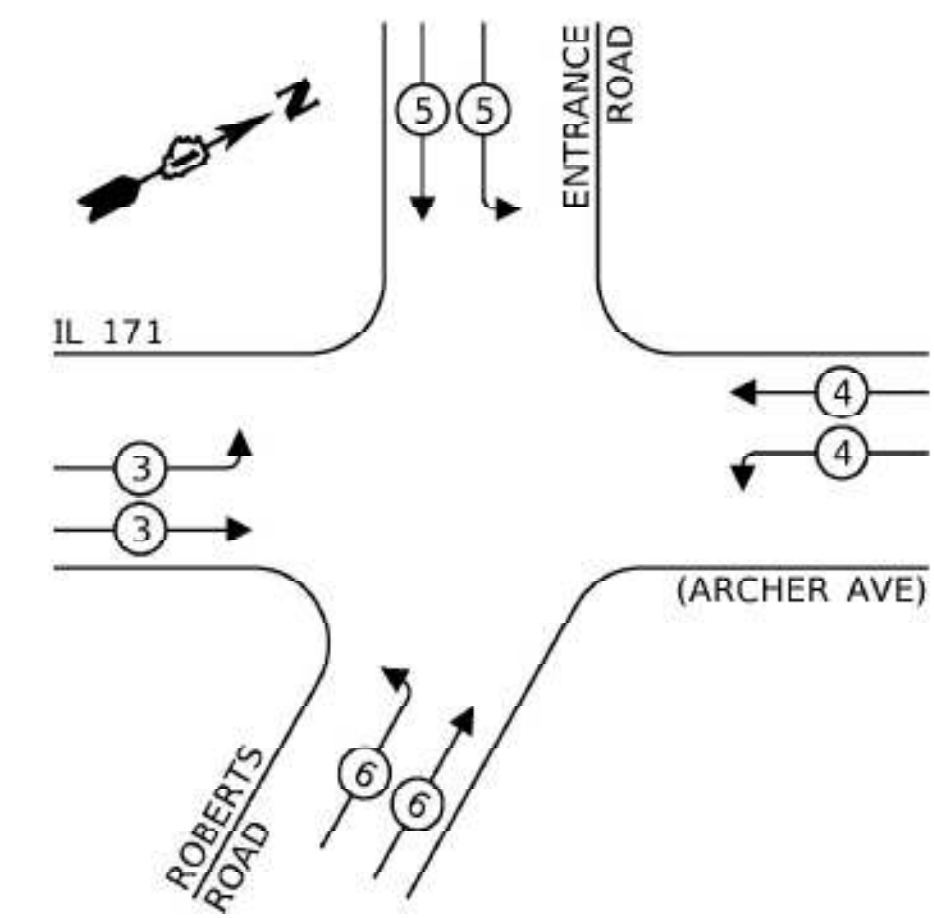


**TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS**

TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	18	11	50	-
(YELLOW)	18	20	5	-
(GREEN)	23	12	45	-
PERMISSIVE ARROW	6	10	10	-
PED. SIGNAL	4	20	100	-
CONTROLLER	1	100	100	-
UPS	1	25	100	-
VIDEO SYSTEM	-	150	100	-
BLANK-OUT SIGN	-	25	5	-
FLASHER	-	-	50	-
STREET NAME SIGN	-	120	50	-
LUMINAIRE	-	250	50	-
TOTAL =				-

ENERGY COSTS TO:  
 ILLINOIS DEPARTMENT OF TRANSPORTATION  
 201 W. CENTER COURT  
 SCHAUMBURG, IL 60196  
 ENERGY SUPPLY: CONTACT: \_\_\_\_\_  
 PHONE: (630) 431-2264  
 COMPANY: COM-ED UNIVERSITY PARK  
 ACCOUNT NUMBER: \_\_\_\_\_

**PROPOSED EMERGENCY VEHICLE PREEMPTION SEQUENCE**



**CABLE PLAN**  
(NOT TO SCALE)

**GEWALT HAMILTON ASSOCIATES, INC. TS SHT NO. 2**

FILE NAME =	USER NAME = zwa1sten	DESIGNED - JRD	REVISED -
Perm-Cable.dgn		DRAWN - ZCW	REVISED -
4841.800		CHECKED - DPB	REVISED -
Default		DATE -	REVISED -

**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**

**CABLE PLAN, PHASE DESIGNATION DIAGRAM, AND EMERGENCY VEHICLE PREEMPTION SEQUENCE  
 IL 171 (ARCHER AVE) AT ROBERTS ROAD**

F.A.U. RTE. = 3565	SECTION = 12-00035-00-SW	COUNTY = COOK	TOTAL SHEETS = 30	SHEET NO. = 15
3577		CONTRACT NO. =	61F35	
ILLINOIS FED. AID PROJECT				

SCALE: NONE SHEET OF SHEETS STA. TO STA.



**SCHEDULE OF QUANTITIES**

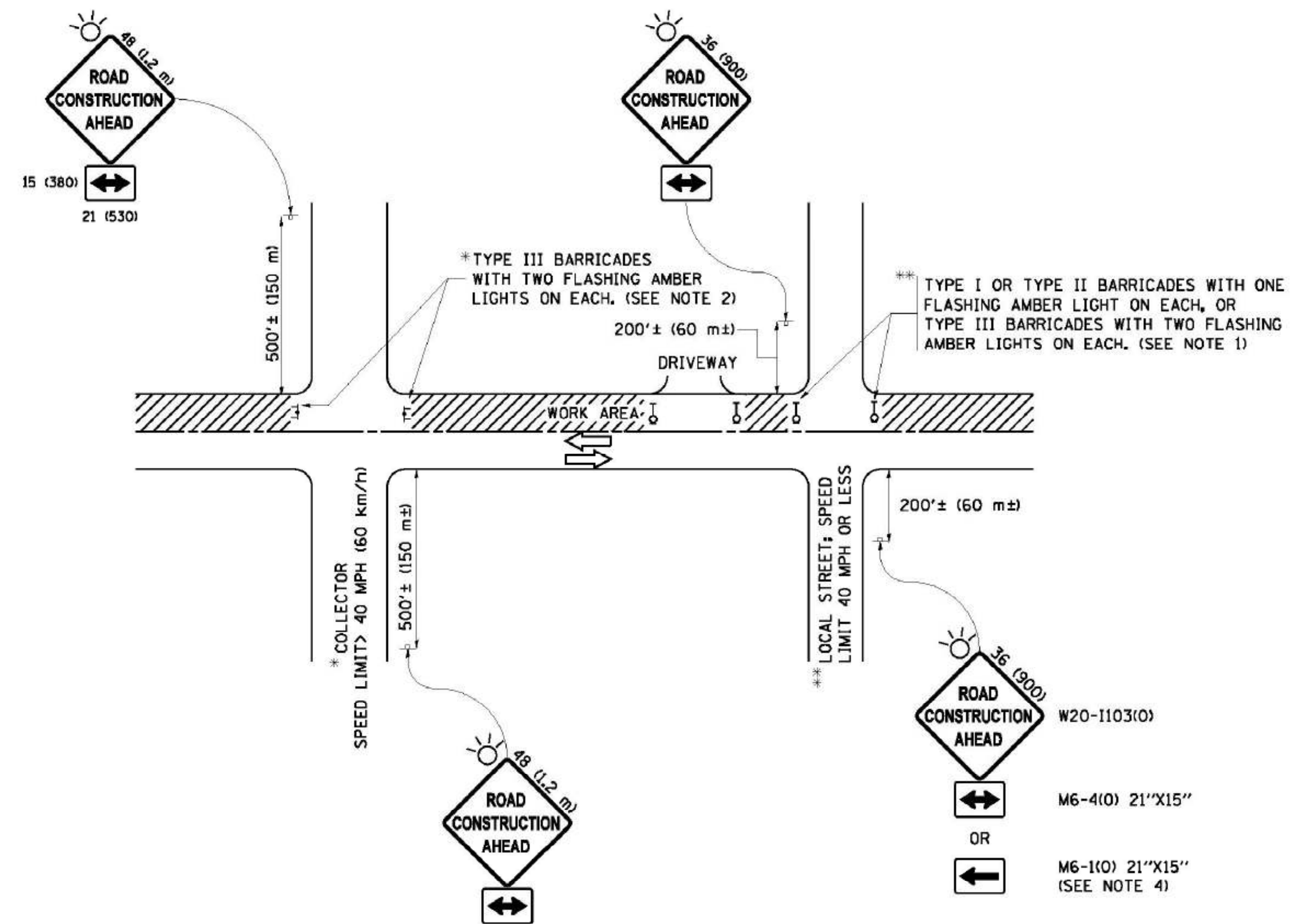
ITEM DESCRIPTION	UNITS	TOTAL QTY.
TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1.00
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.	FOOT	19
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	585
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	615
TRAFFIC SIGNAL POST, GALVANIZED STEEL 10 FT.	EACH	1
CONCRETE FOUNDATION, TYPE A	FOOT	4
DRILL EXISTING HANDHOLE	EACH	1
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	2
PEDESTRIAN SIGNAL HEAD, LED, 2-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	1
PEDESTRIAN PUSH-BUTTON	EACH	4
MODIFY EXISTING CONTROLLER CABINET	EACH	1
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1

FILE NAME = Sign-S00.dgn 4841.800 Default	USER NAME = zwallsten	DESIGNED - JRD	REVISED -
		DRAWN - ZCW	REVISED -
	PLOT SCALE = 1:20	CHECKED - DPB	REVISED -
	PLOT DATE = 7/30/2018	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**MAST ARM MOUNTED STREET NAME SIGNS**  
**AND SCHEDULE OF QUANTITIES**  
**IL 171 (ARCHER AVE) AT ROBERTS ROAD**

F.A.U. RTE. 3565	SECTION 12-00035-00-SW	COUNTY COOK	TOTAL SHEETS 30	SHEET NO. 16
SCALE: NONE	SHEET OF	SHEETS STA.	TO STA.	CONTRACT NO. 61F35
[ILLINOIS] FED. AID PROJECT				



**NOTES:**

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = fcolemj	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
pws\11084EBIDINTEG\Illinois.gov\PWIDOT\Documents\IDOT_Offices\District 1\Projects\Dist	DRAWN: CADData\CADsheets\tdl0.dgn	CHECKED -	REVISED - T. RAMMACHER 01-06-00
Default	PLOT SCALE = 50.000' / 1" =	DATE - 06-89	REVISED - A. SCHUETZE 07-01-13
	PLOT DATE = 7/15/2016		REVISED - A. SCHUETZE 09-15-16

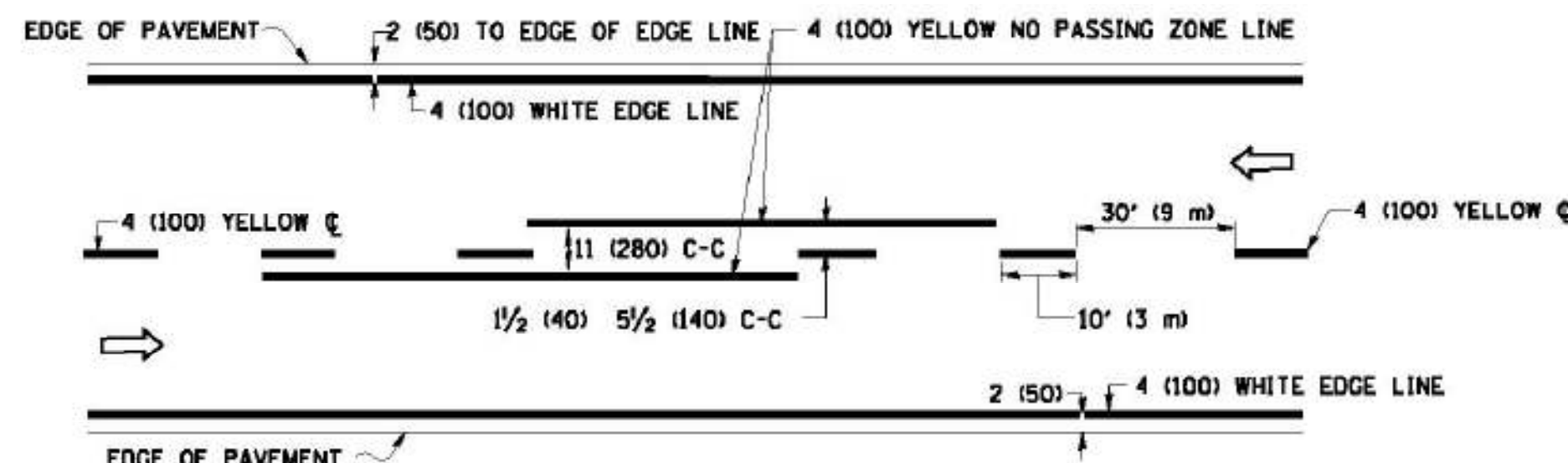
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

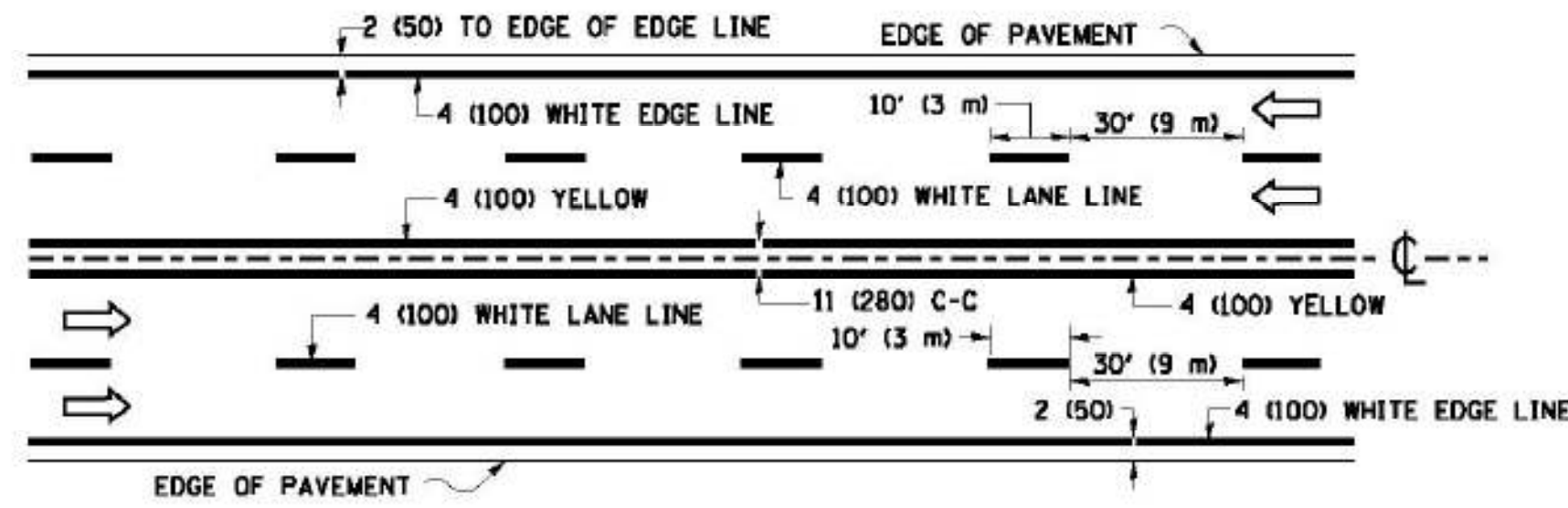
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A. RTE. 3565 3877	SECTION 12-00035-00-SW	COUNTY COOK	TOTAL SHEETS 30	SHEET NO. 17
TC-10		CONTRACT NO.	61F35	
ILLINOIS FED. AID PROJECT				

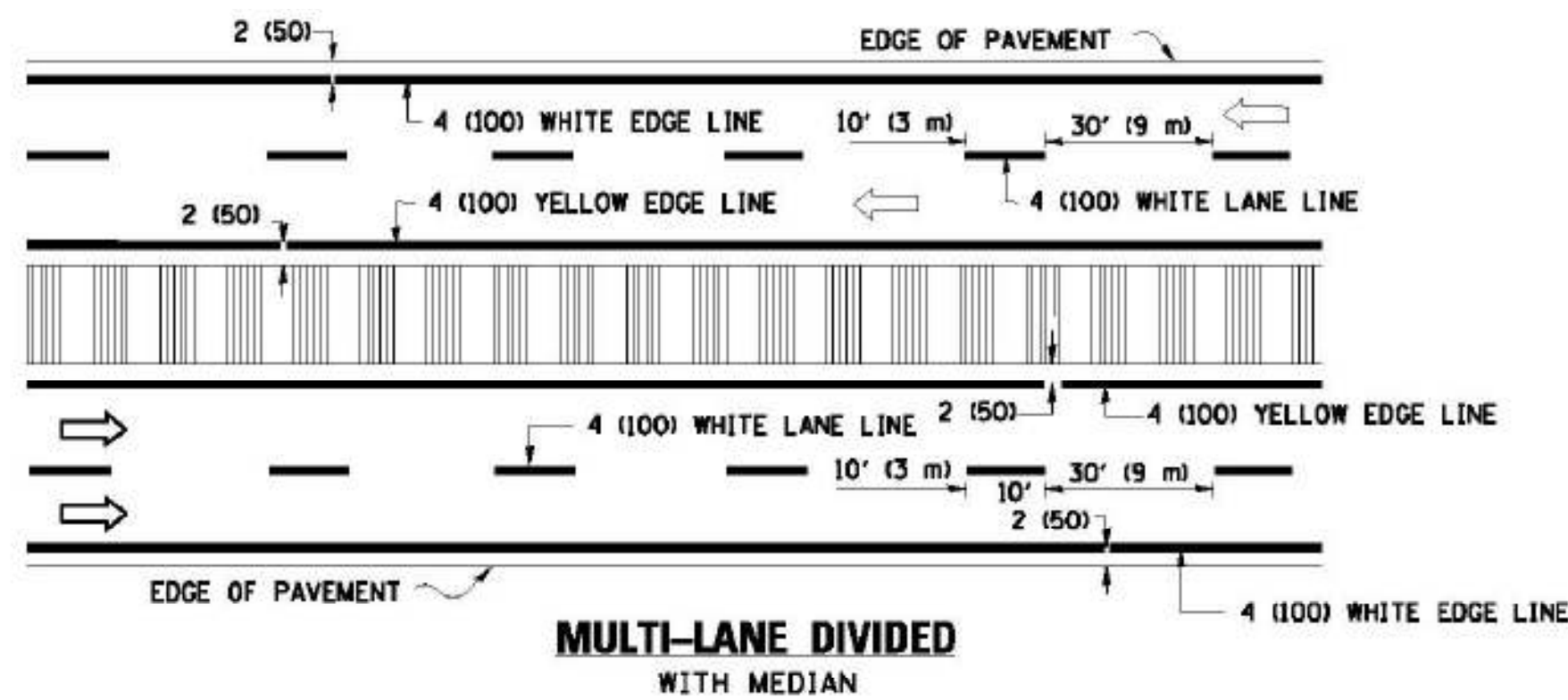




**2-LANE ROADWAY**

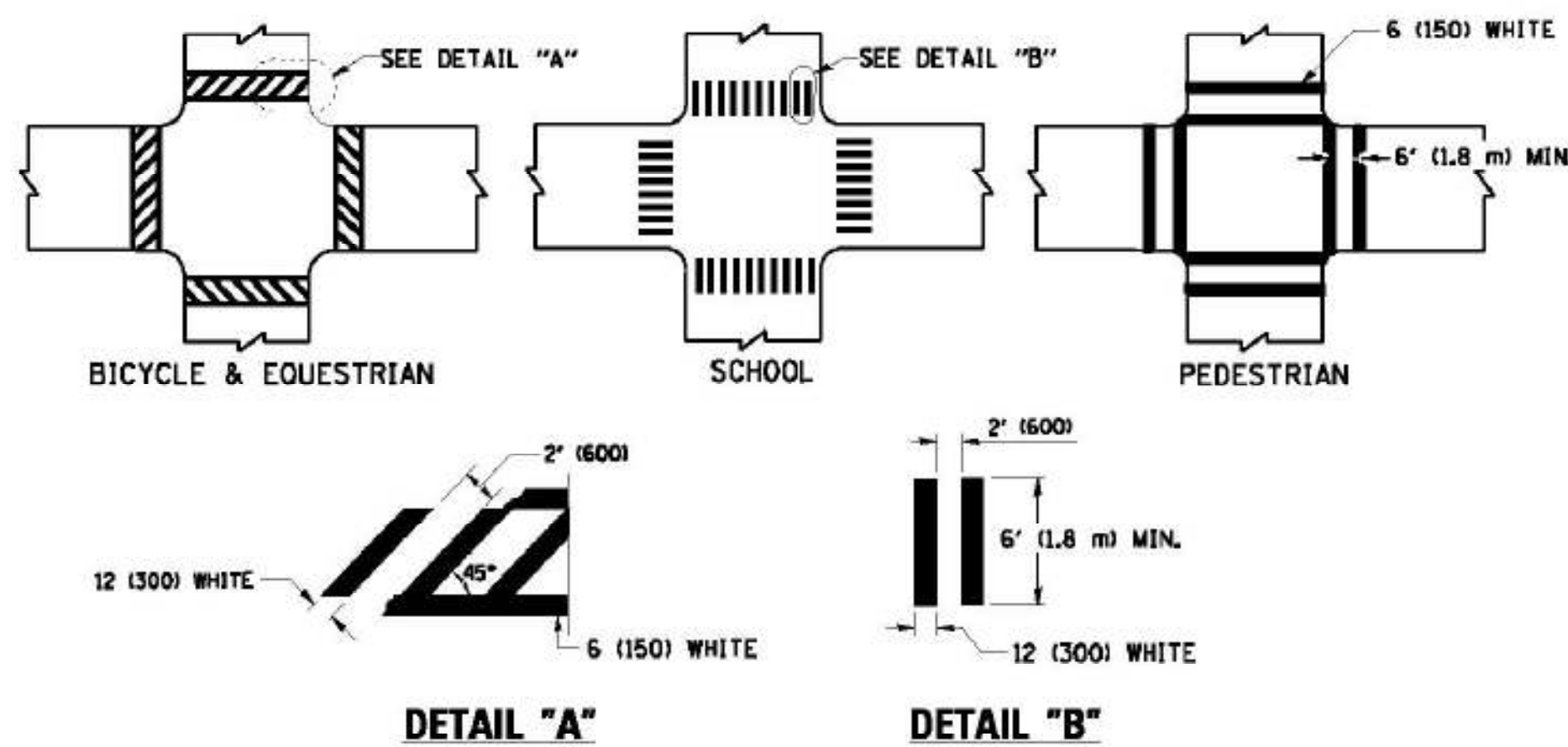


**MULTI-LANE UNDIVIDED**



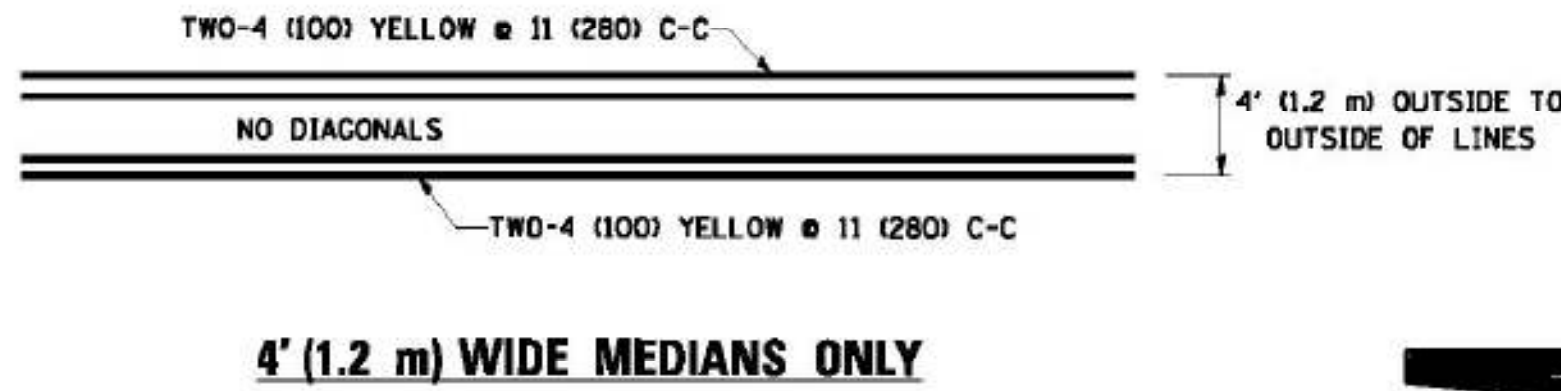
**MULTI-LANE DIVIDED WITH MEDIAN**

**TYPICAL LANE AND EDGE LINE MARKING**

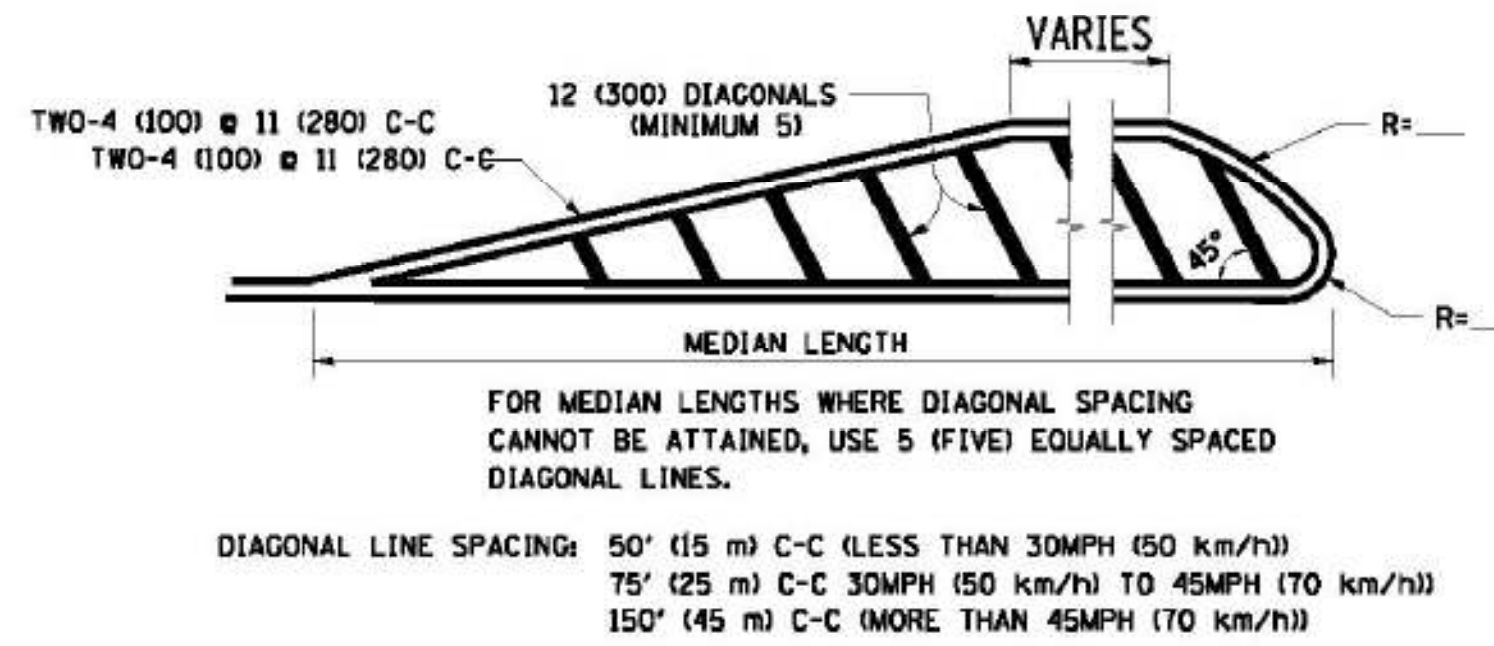


**DETAIL "A" TYPICAL CROSSWALK MARKING**

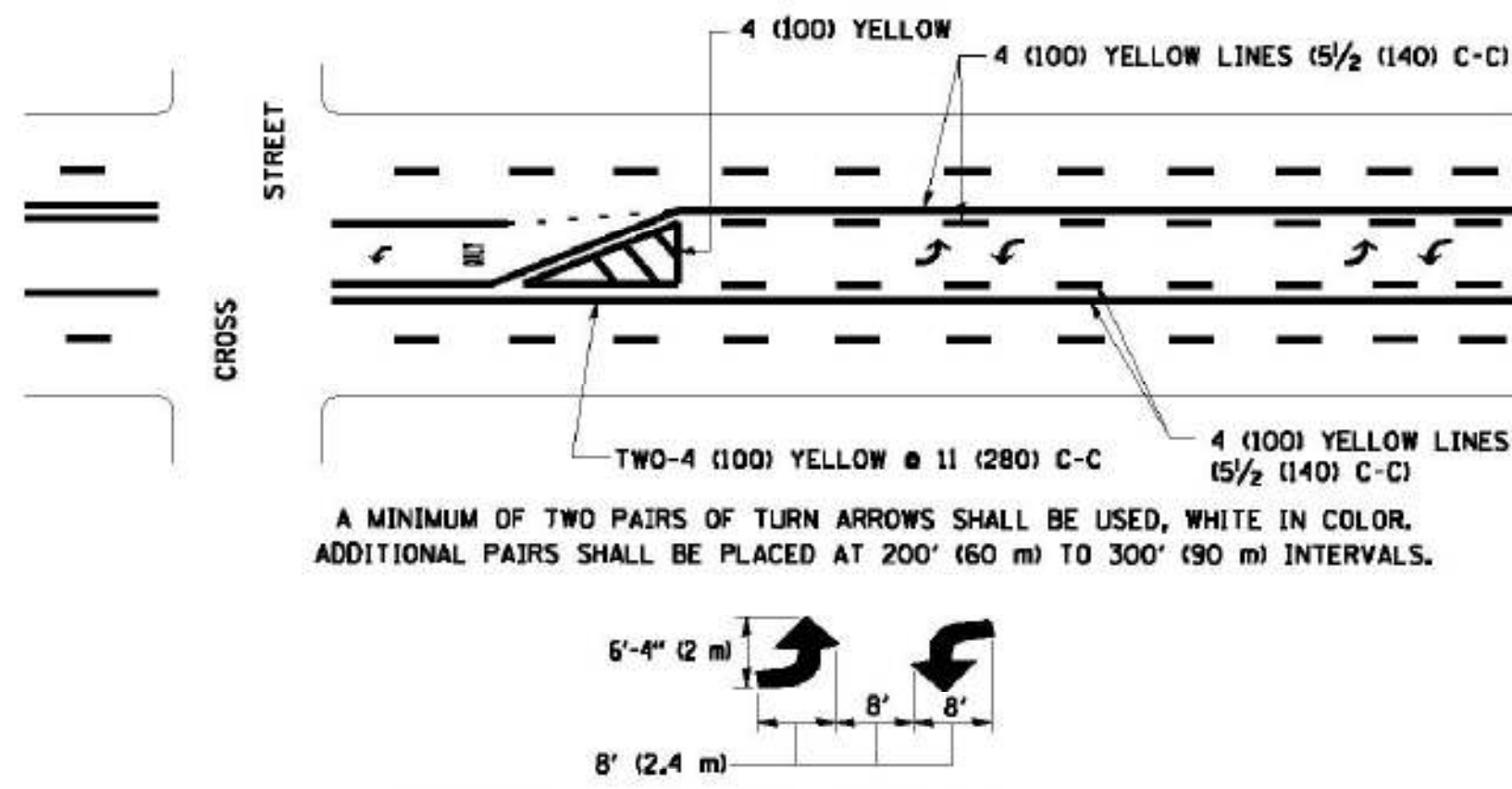
\* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



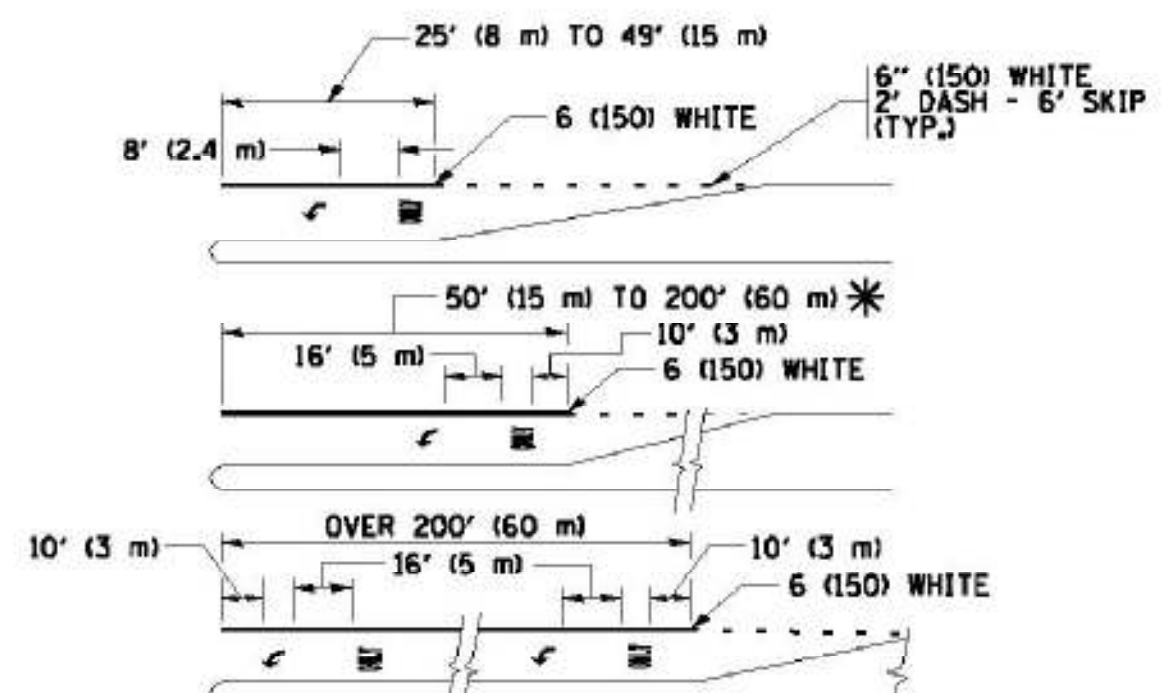
**4' (1.2 m) WIDE MEDIANS ONLY**



**MEDIANS OVER 4' (1.2 m) WIDE**

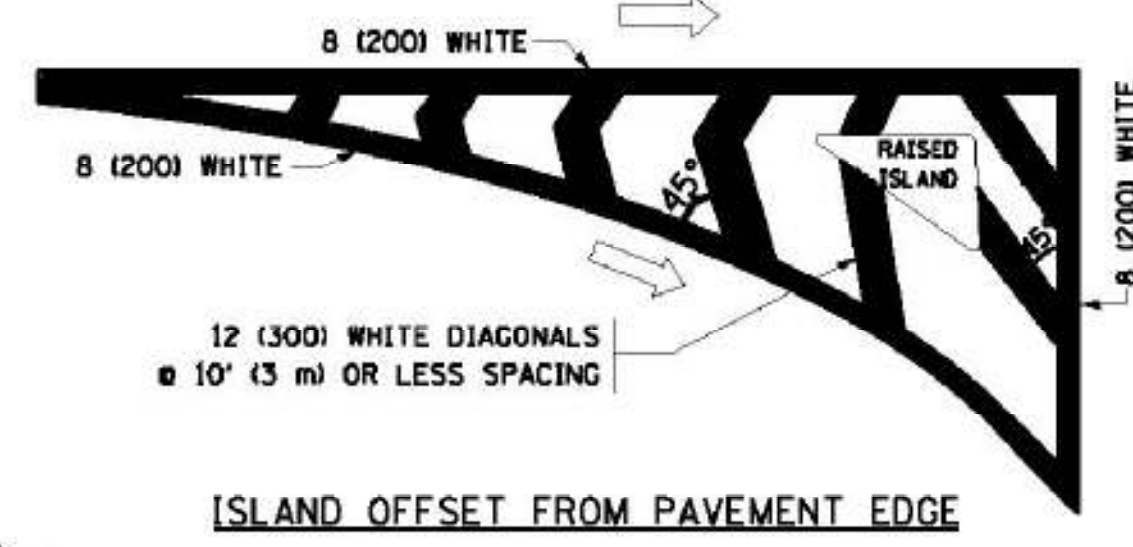


**MEDIAN WITH TWO-WAY LEFT TURN LANE TYPICAL PAINTED MEDIAN MARKING**

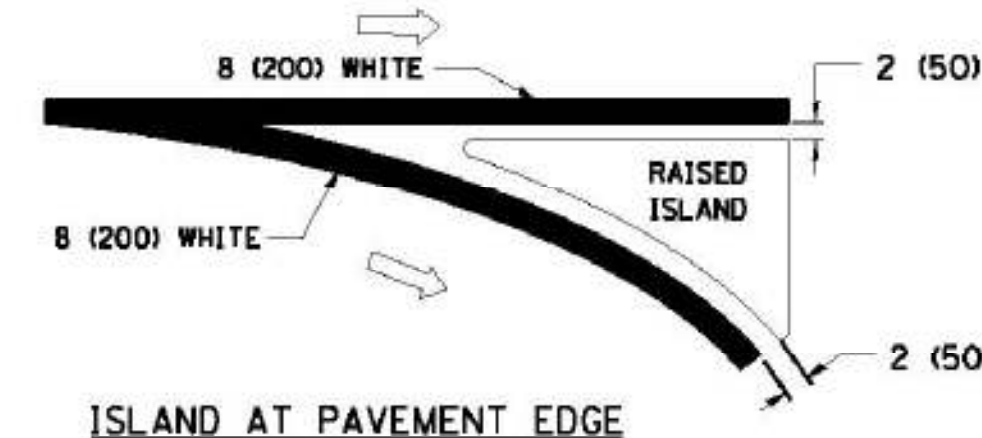


**TYPICAL LEFT (OR RIGHT) TURN LANE**

**TYPICAL TURN LANE MARKING**

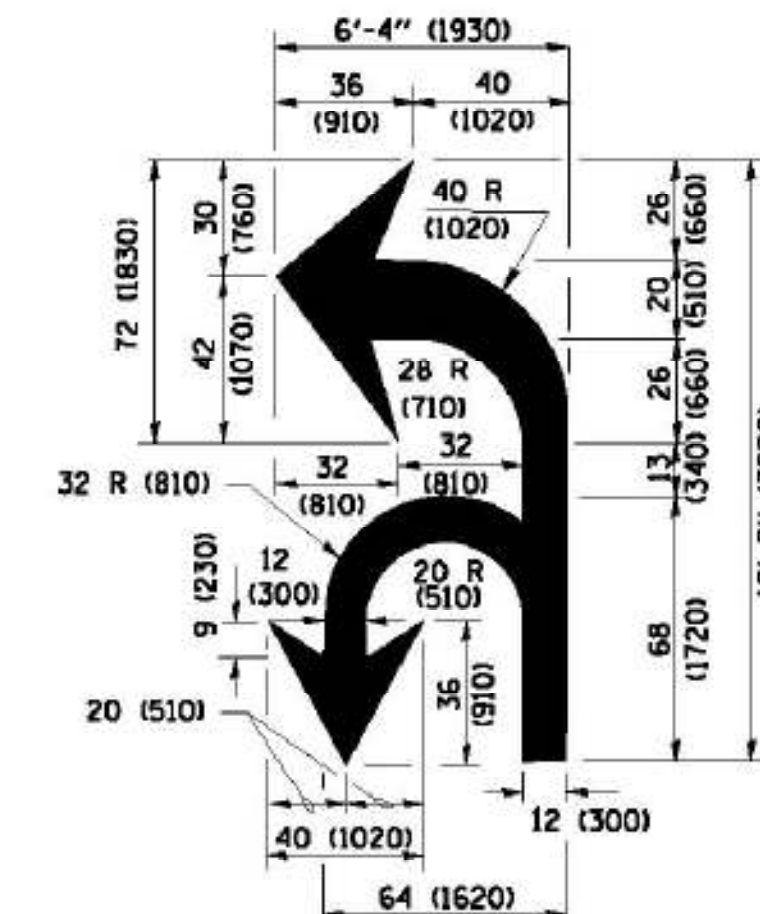


**ISLAND OFFSET FROM PAVEMENT EDGE**

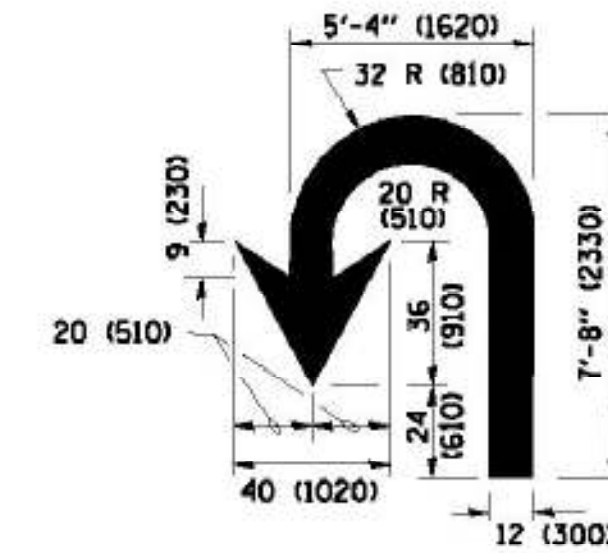


**ISLAND AT PAVEMENT EDGE**

**TYPICAL ISLAND MARKING**



**COMBINATION LEFT AND U-TURN**



**U-TURN**

**LANE REDUCTION TRANSITION**

\* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF "R" = 3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X" = 54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = W:\drafted\22x34\c13.dgn	USER NAME = lejoe	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09
Default	PLOT SCALE = 50.000 / 1 in.	DRAWN -	REVISED - C. JUCIUS 07-01-13
	PLOT DATE = 6/23/2017	CHECKED -	REVISED - C. JUCIUS 12-21-15
		DATE - 03-19-90	REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION**

<b>DISTRICT ONE TYPICAL PAVEMENT MARKINGS</b>			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

F.A. RTE. 3565 3577	SECTION 12-00035-00-SW	COUNTY COOK	TOTAL SHEETS 30	SHEET NO. 18
<b>TC-13</b>		CONTRACT NO.		61F35
ILLINOIS FED. AID PROJECT				



# TRAFFIC SIGNAL LEGEND

(NOT TO SCALE)

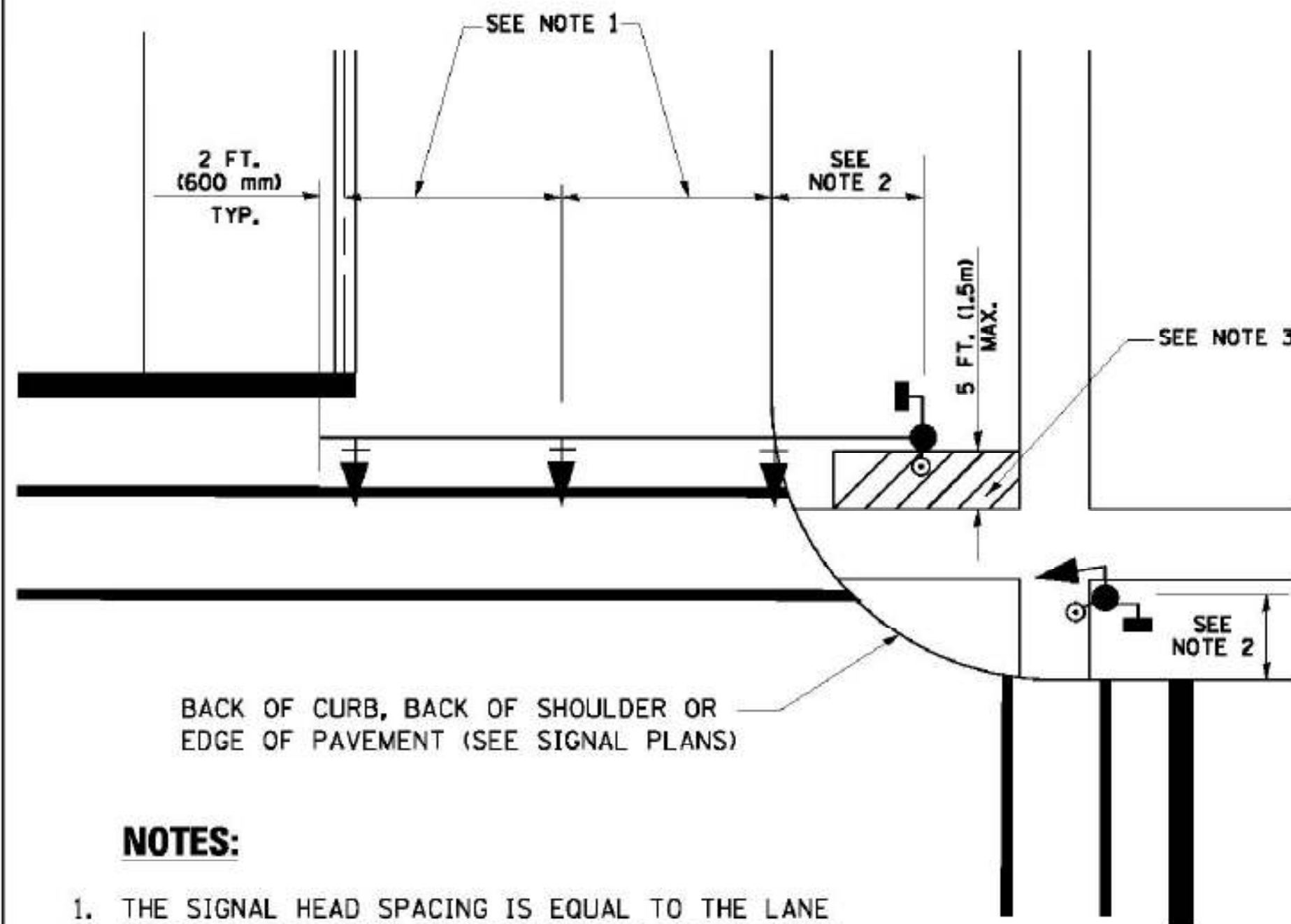
ITEM	EXISTING	PROPOSED	ITEM	EXISTING	PROPOSED	ITEM	EXISTING	PROPOSED
CONTROLLER CABINET			HANDHOLE -SQUARE -ROUND			SIGNAL HEAD -(P) PROGRAMMABLE SIGNAL HEAD		
COMMUNICATION CABINET			HEAVY DUTY HANDHOLE -SQUARE -ROUND			SIGNAL HEAD WITH BACKPLATE -(P) PROGRAMMABLE SIGNAL HEAD -(RB) RETROREFLECTIVE BACKPLATE		
MASTER CONTROLLER			DOUBLE HANDHOLE			PEDESTRIAN SIGNAL HEAD AT RAILROAD INTERSECTIONS		
MASTER MASTER CONTROLLER			JUNCTION BOX			PEDESTRIAN SIGNAL HEAD WITH COUNTDOWN TIMER		
UNINTERRUPTABLE POWER SUPPLY			RAILROAD CANTILEVER MAST ARM			ILLUMINATED SIGN "NO LEFT TURN"/"NO RIGHT TURN"		
SERVICE INSTALLATION -(P) POLE MOUNTED			RAILROAD FLASHING SIGNAL			NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE. ALL DETECTOR LOOP CABLE TO BE SHIELDED		
SERVICE INSTALLATION -(G) GROUND MOUNTED -(GM) GROUND MOUNTED METERED			RAILROAD CROSSING GATE			GROUND CABLE IN CONDUIT, NO. 6 SOLID COPPER (GREEN)		
TELEPHONE CONNECTION			RAILROAD CROSSBUCK			ELECTRIC CABLE IN CONDUIT, TRACER NO. 14 1/C		
STEEL MAST ARM ASSEMBLY AND POLE			RAILROAD CONTROLLER CABINET			COAXIAL CABLE		
ALUMINUM MAST ARM ASSEMBLY AND POLE			UNDERGROUND CONDUIT (UC), GALVANIZED STEEL			VENDOR CABLE		
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE			TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE			COPPER INTERCONNECT CABLE, NO. 18, 3 PAIR TWISTED, SHIELDED		
SIGNAL POST -(BM) BARREL MOUNTED - TEMPORARY			SYSTEM ITEM			FIBER OPTIC CABLE -NO. 62.5/125, MM12F -NO. 62.5/125, MM12F SM12F -NO. 62.5/125, MM12F SM24F		
WOOD POLE			INTERSECTION ITEM			GROUND ROD -(C) CONTROLLER -(M) MAST ARM -(P) POST -(S) SERVICE		
GUY WIRE			REMOVE ITEM					
SIGNAL HEAD			RELOCATE ITEM					
SIGNAL HEAD WITH BACKPLATE			ABANDON ITEM					
SIGNAL HEAD OPTICALLY PROGRAMMED			CONTROLLER CABINET AND FOUNDATION TO BE REMOVED					
FLASHER INSTALLATION -(FS) SOLAR POWERED			MAST ARM POLE AND FOUNDATION TO BE REMOVED					
PEDESTRIAN SIGNAL HEAD			SIGNAL POST AND FOUNDATION TO BE REMOVED					
PEDESTRIAN PUSH BUTTON -(APS) ACCESSIBLE PEDESTRIAN PUSH BUTTON			DETECTOR LOOP, TYPE I					
RADAR DETECTION SENSOR			PREFORMED DETECTOR LOOP					
VIDEO DETECTION CAMERA			SAMPLING (SYSTEM) DETECTOR					
RADAR/VIDEO DETECTION ZONE			INTERSECTION AND SAMPLING (SYSTEM) DETECTOR					
PAN, TILT, ZOOM (PTZ) CAMERA			QUEUE AND SAMPLING (SYSTEM) DETECTOR					
EMERGENCY VEHICLE LIGHT DETECTOR			WIRELESS DETECTOR SENSOR					
CONFIRMATION BEACON			WIRELESS ACCESS POINT					
WIRELESS INTERCONNECT								
WIRELESS INTERCONNECT RADIO REPEATER								







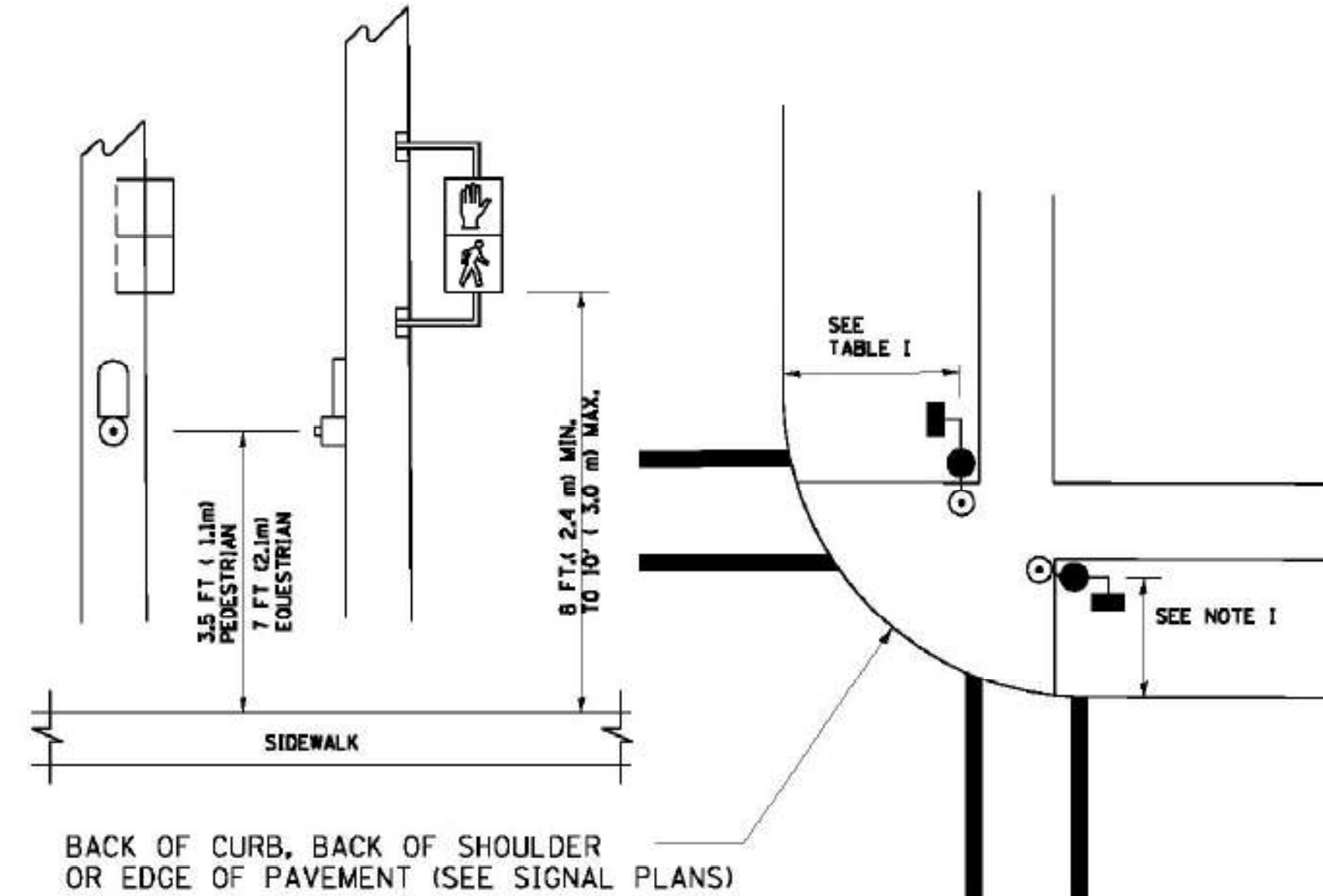
**TRAFFIC SIGNAL MAST ARM AND SIGNAL POST  
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR  
FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN  
WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.**



**NOTES:**

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

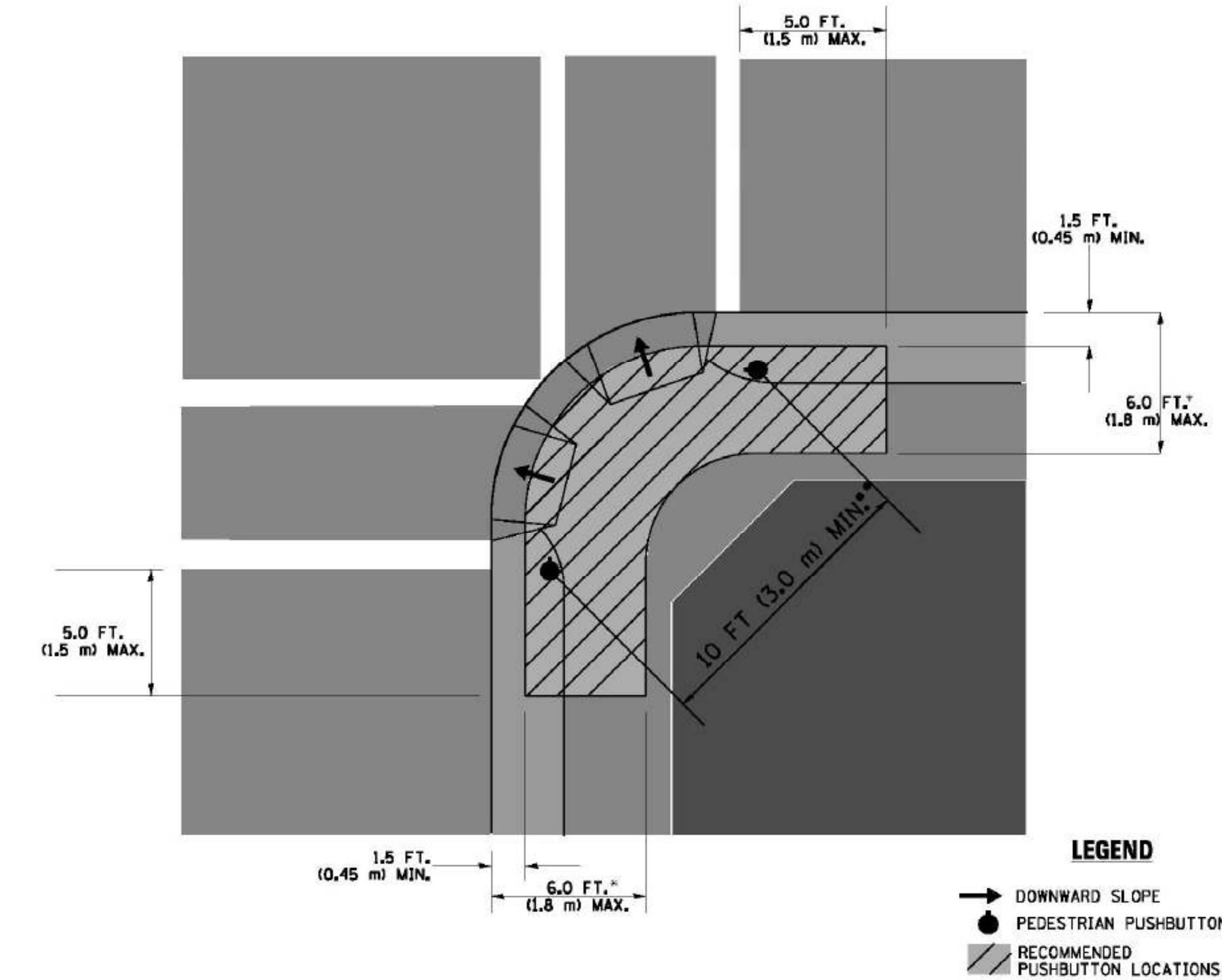
**PEDESTRIAN SIGNAL POST  
AND  
PEDESTRIAN PUSH BUTTON POST**



**NOTES:**

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

**RECOMMENDED PUSHBUTTON LOCATIONS**



- WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

**NOTES:**

1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

**TRAFFIC SIGNAL EQUIPMENT OFFSET**

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

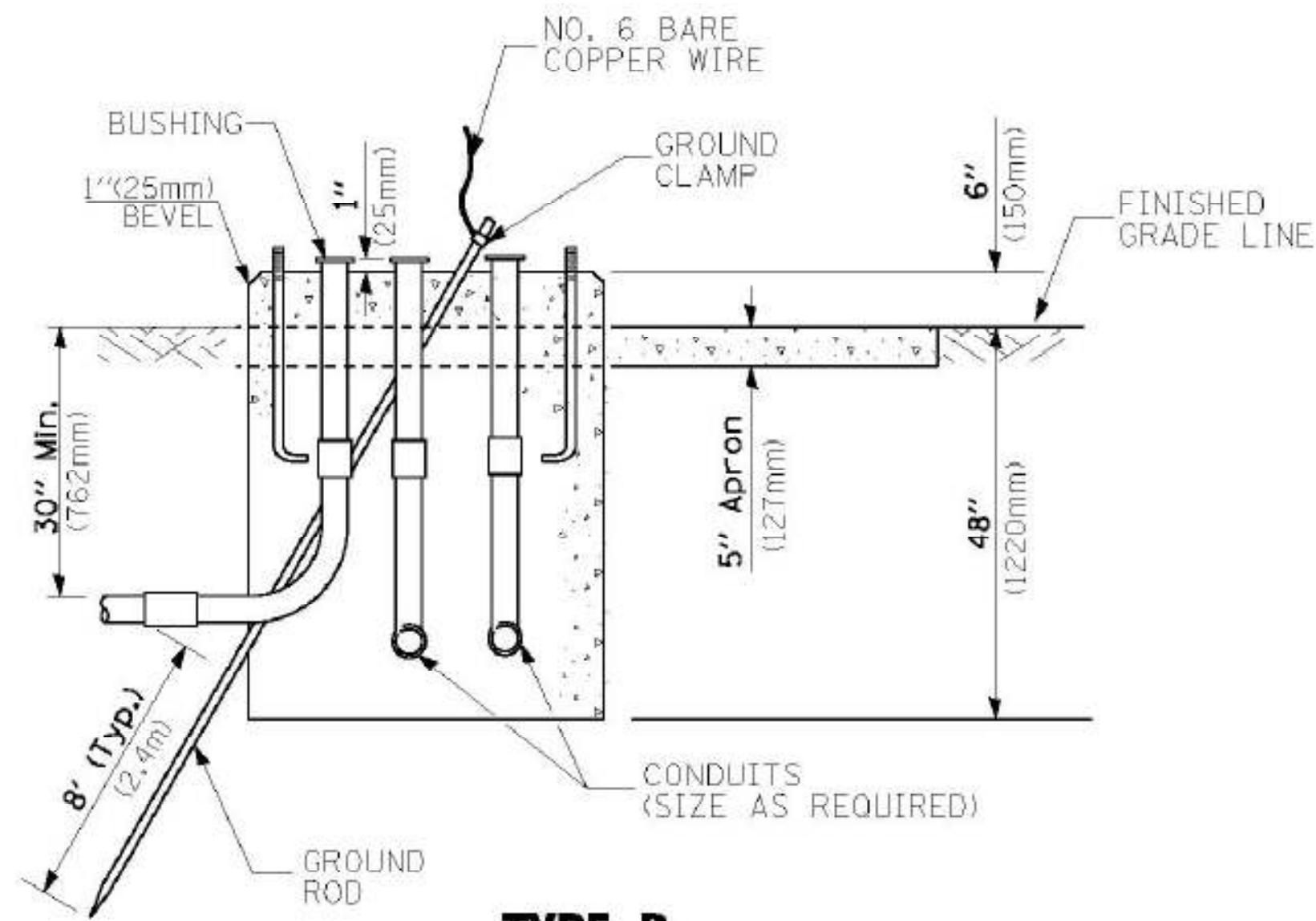
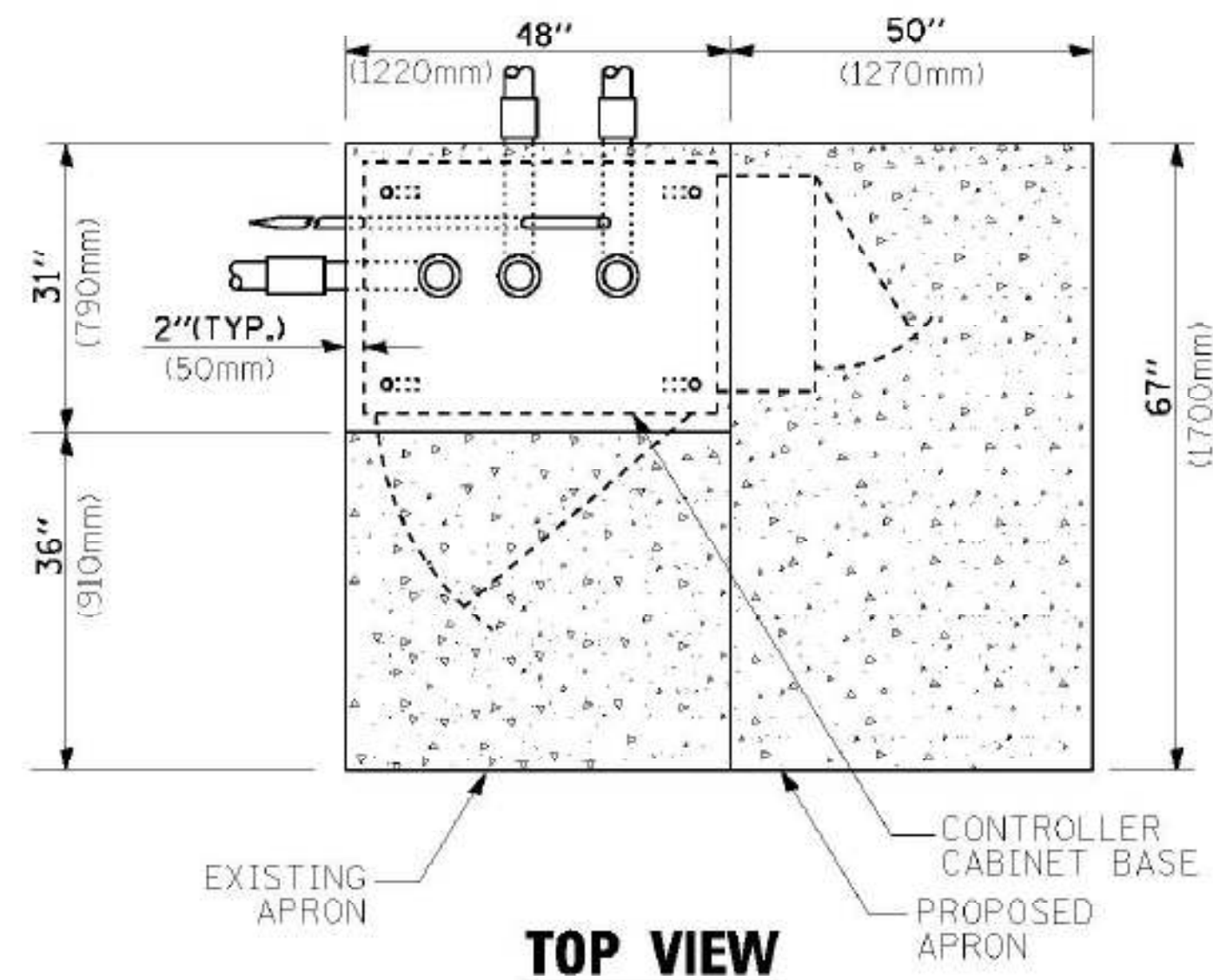
**NOTES:**

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD AFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

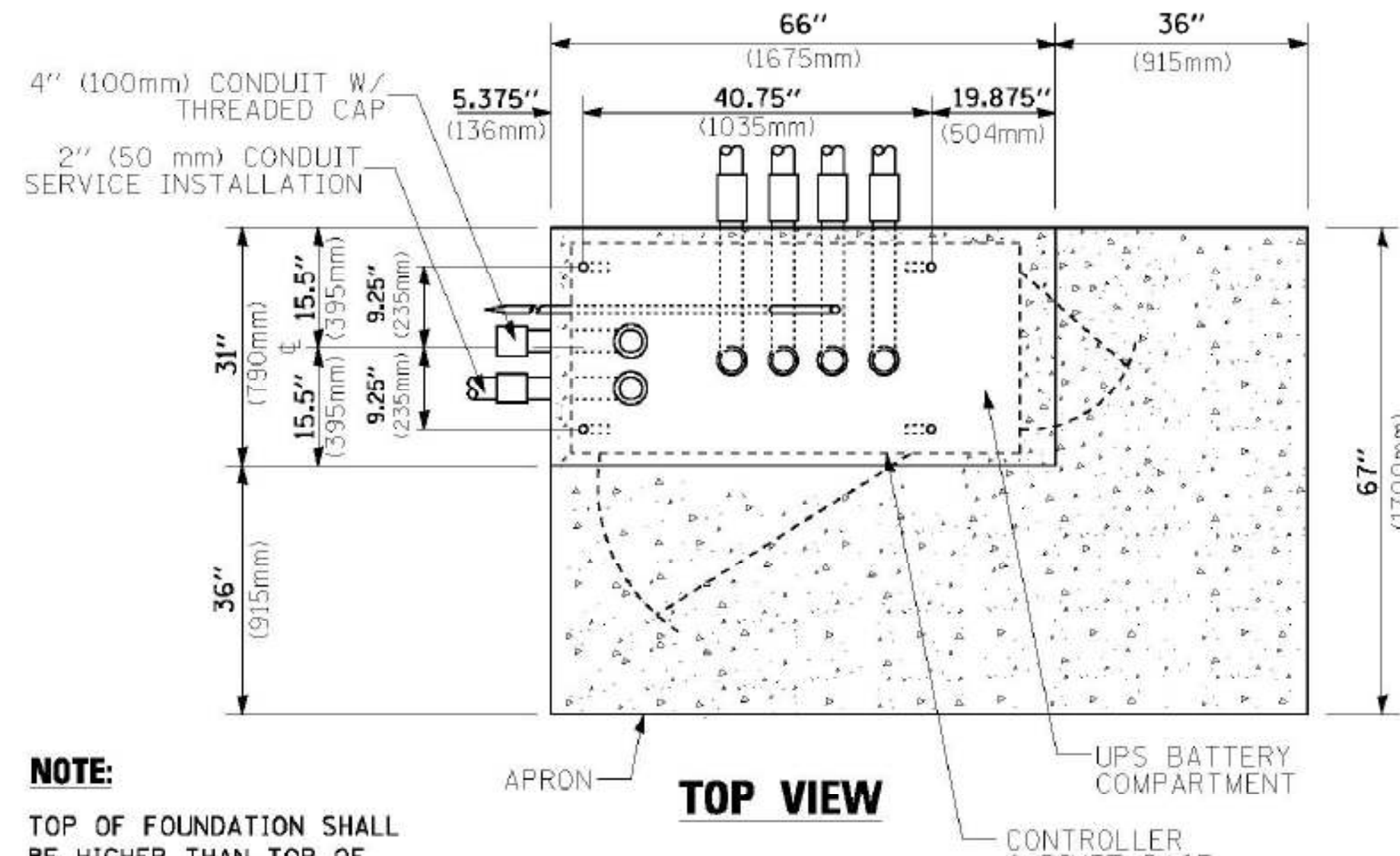




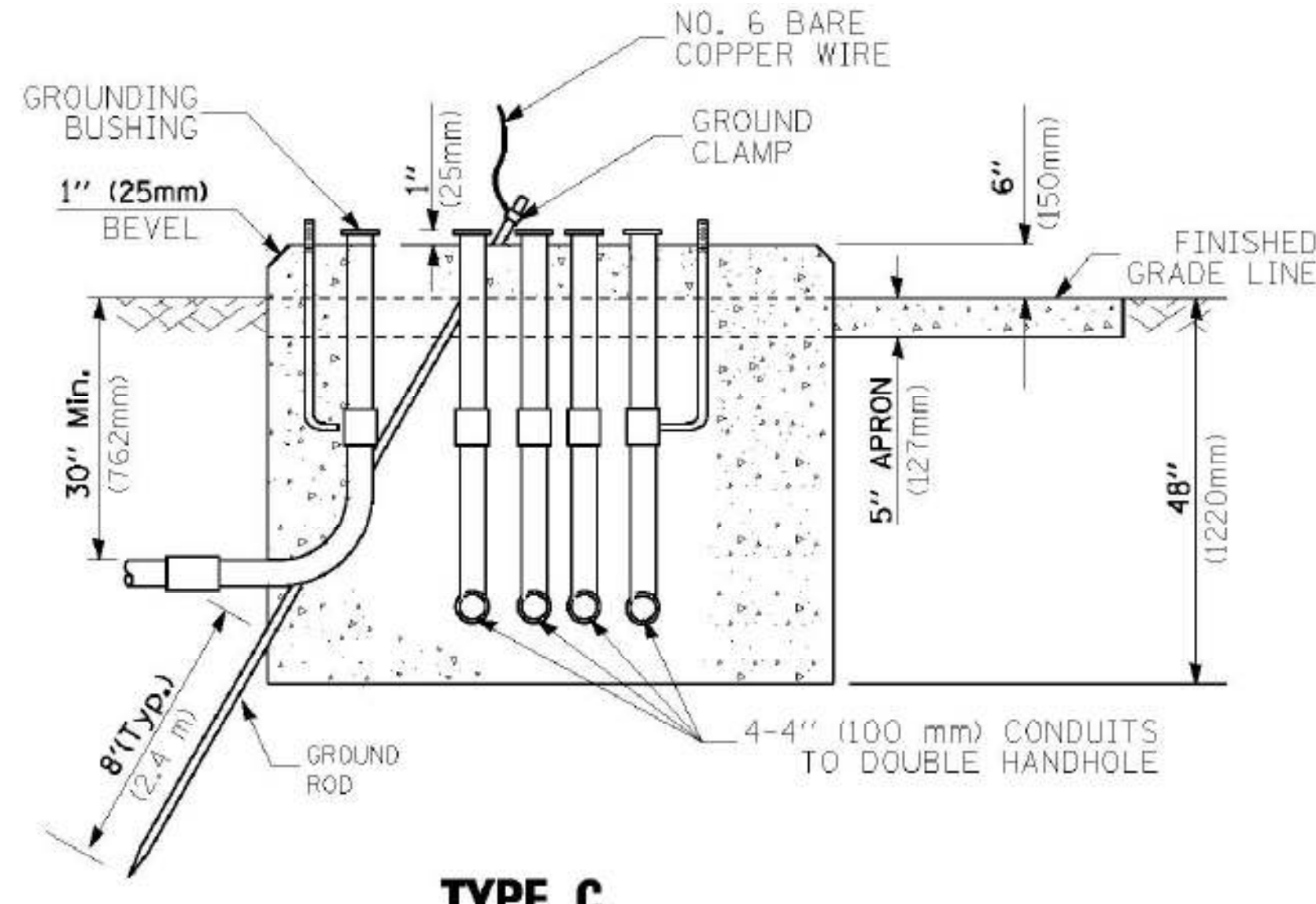




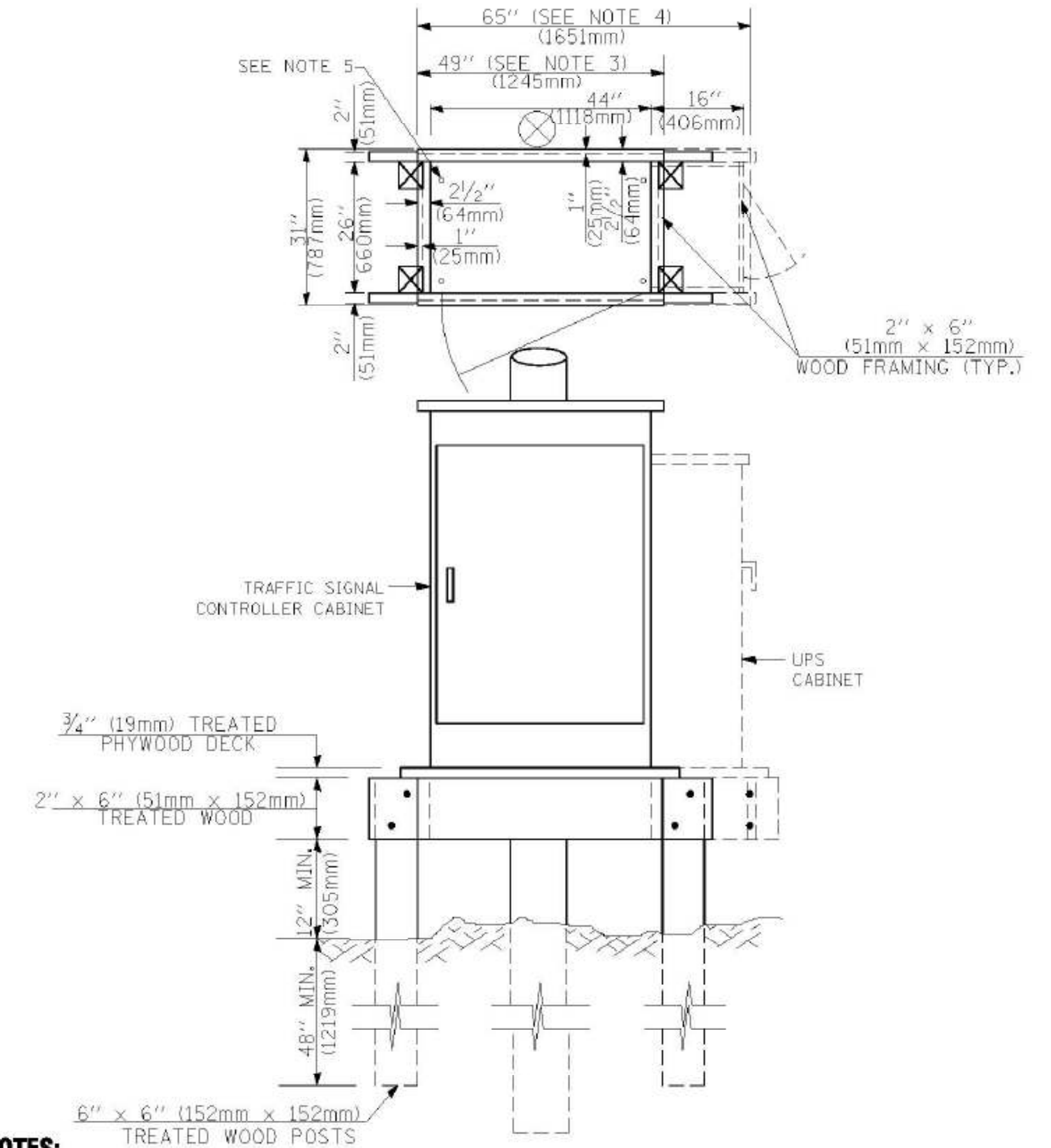
**TYPE D  
FOR GROUND MOUNTED  
CONTROLLER CABINET  
AND UPS BATTERY CABINET**



**NOTE:**  
TOP OF FOUNDATION SHALL  
BE HIGHER THAN TOP OF  
DOUBLE HANDHOLE



**TYPE C  
FOR GROUND MOUNTED  
SUPER P (TYPE IV) AND SUPER R (TYPE V)  
CONTROLLER CABINETS**



**NOTES:**

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION.

**TEMPORARY SIGNAL CONTROLLER  
WOOD SUPPORT PLATFORM**

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

**CABLE SLACK**

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

**VERTICAL CABLE LENGTH**

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

**DEPTH OF FOUNDATION**

MAST ARM LENGTH	① FOUNDATION DEPTH	FOUNDATION DIAMETER	SPIRAL DIAMETER	QUANTITY OF REBARS	SIZE OF REBARS
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 56' (16.8 m) and less than 65' (19.8 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

**NOTES:**

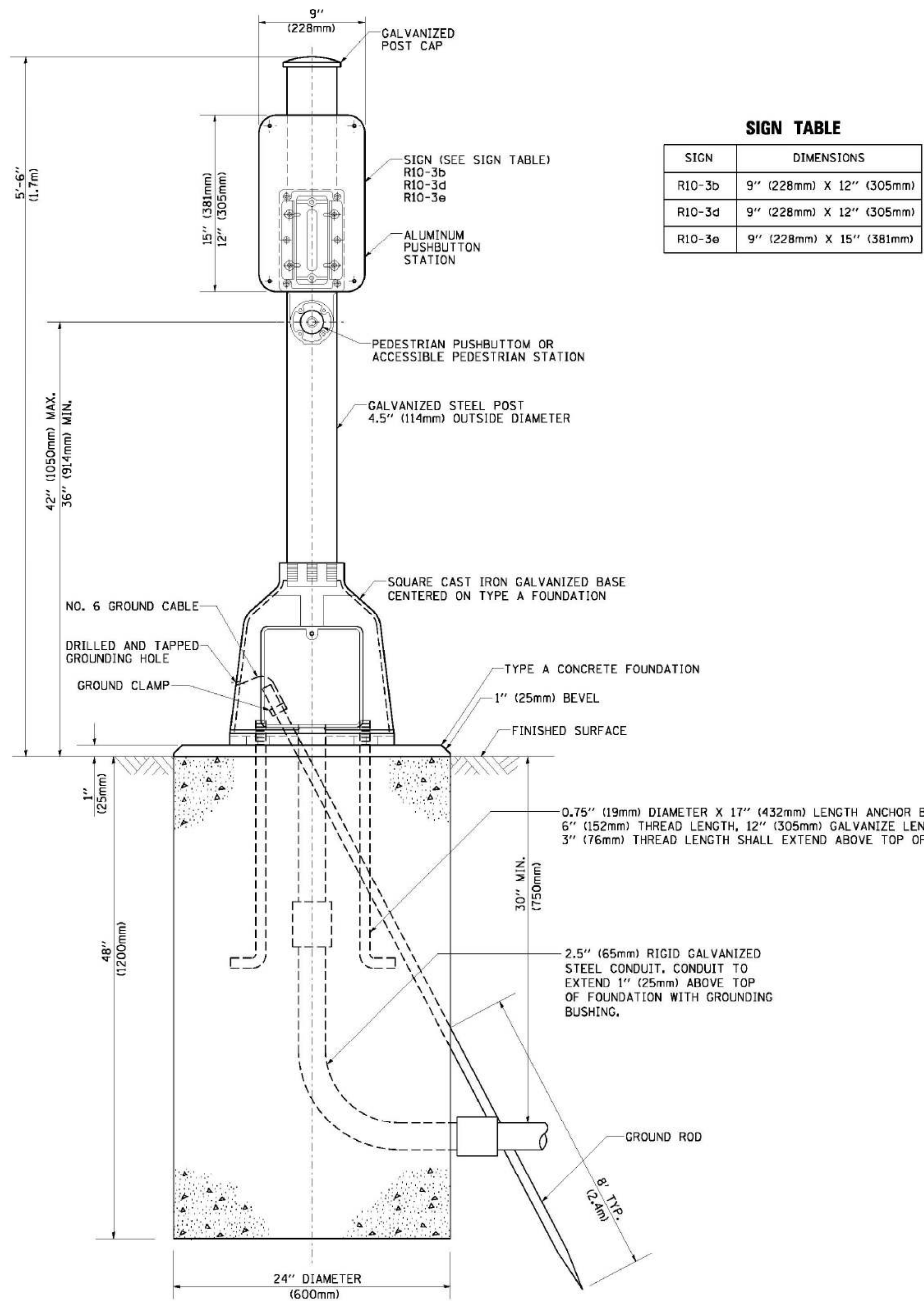
1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (Qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations
4. For mast arm assemblies with dual arms refer to state standard 878001.

**DEPTH OF MAST ARM FOUNDATIONS, TYPE E**



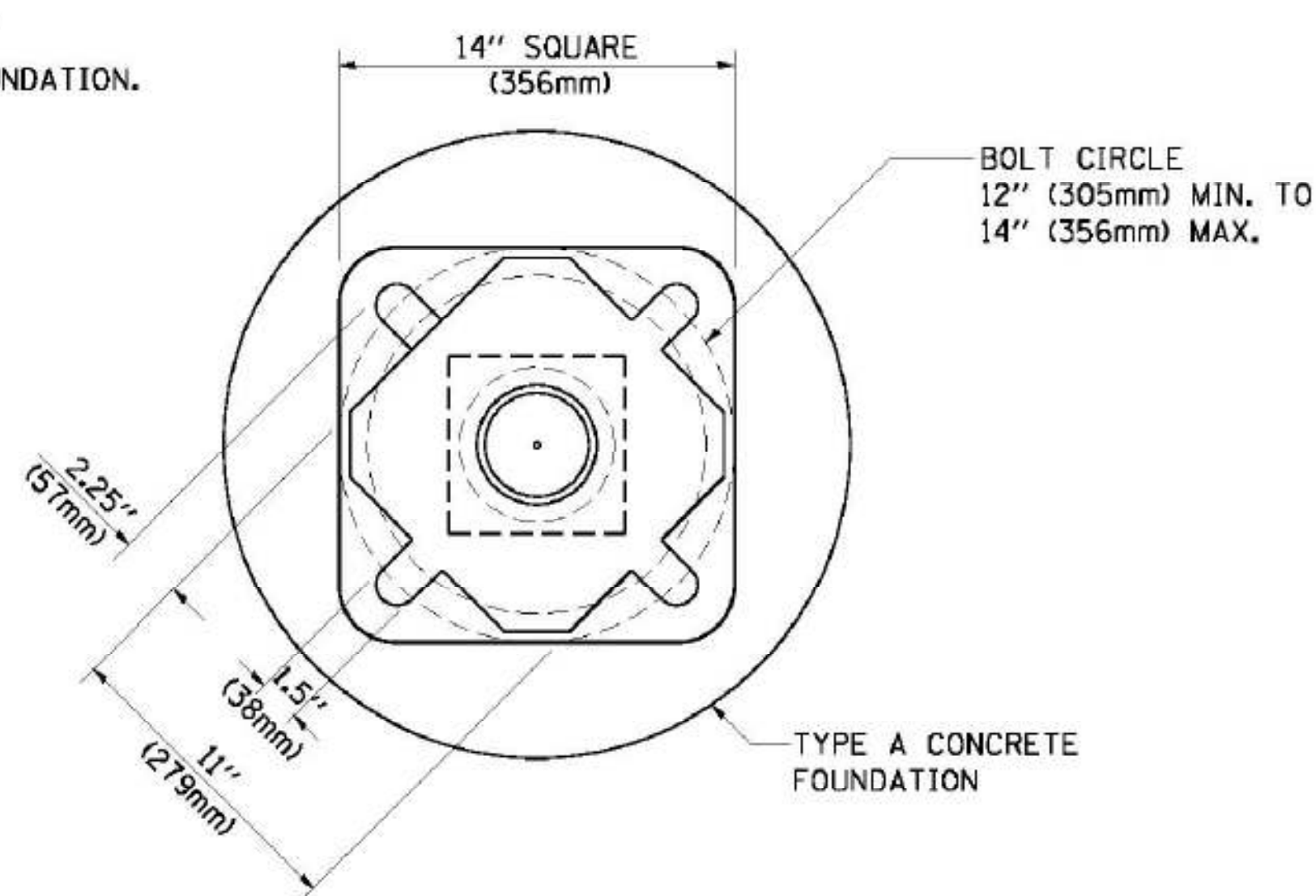






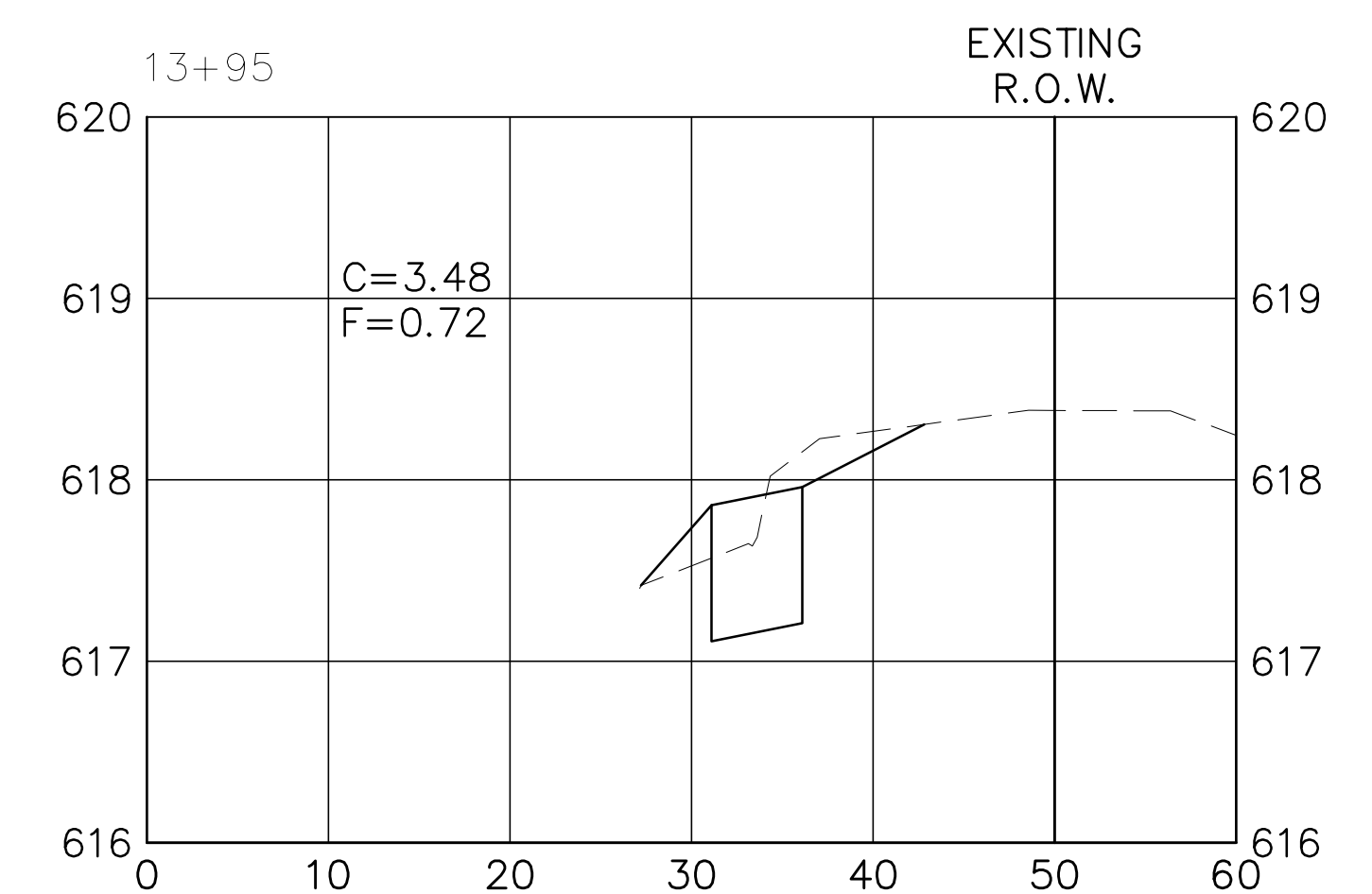
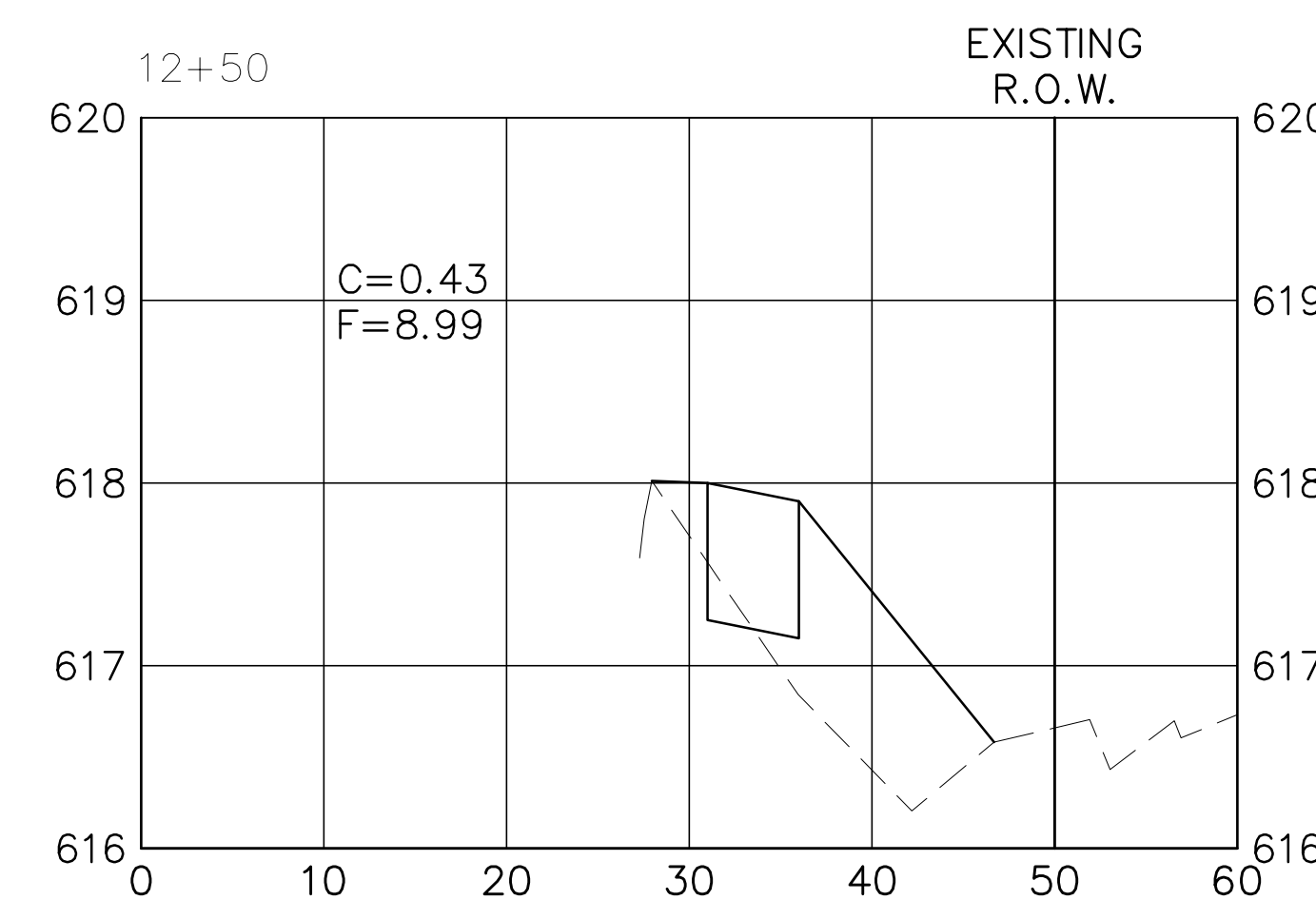
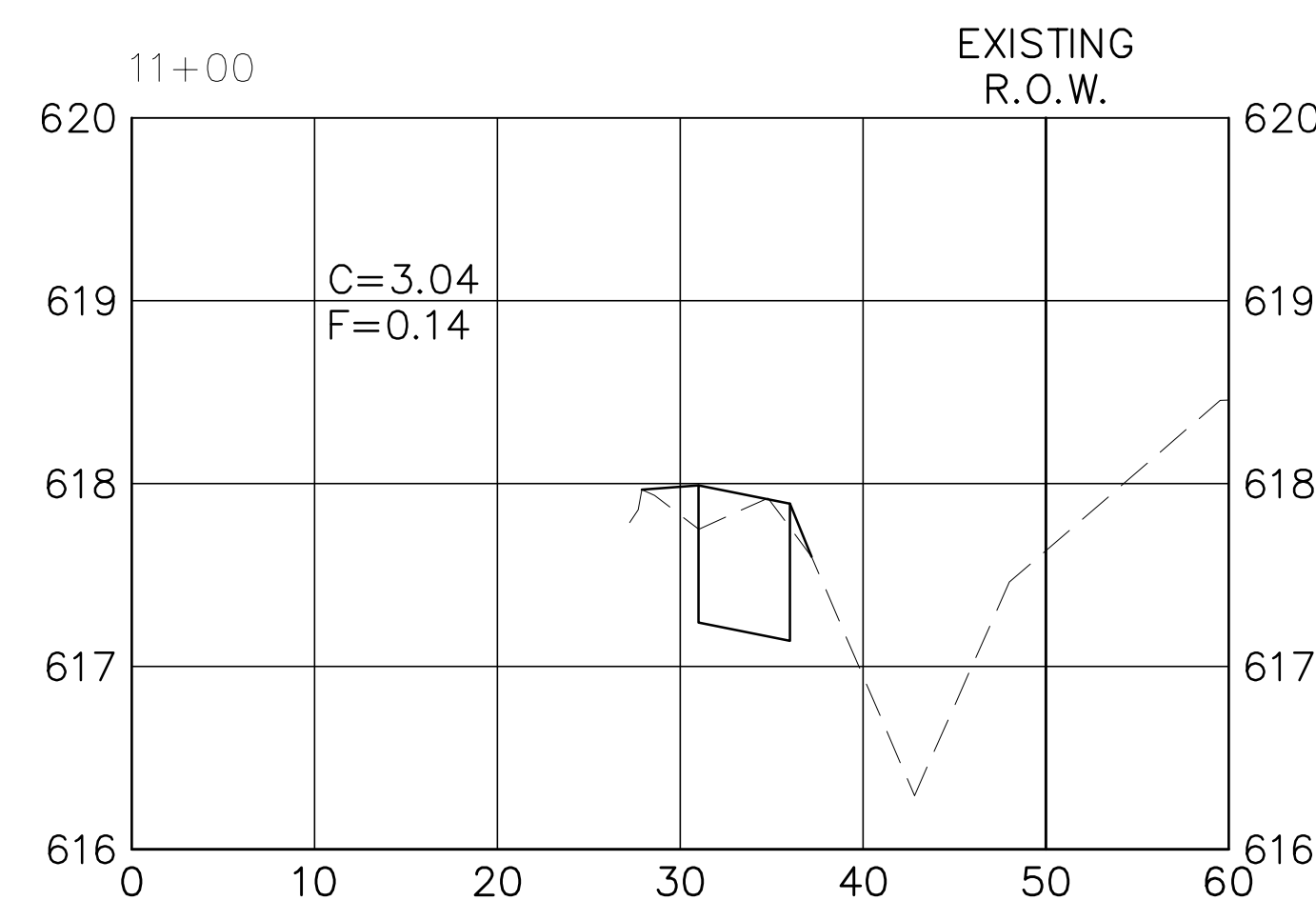
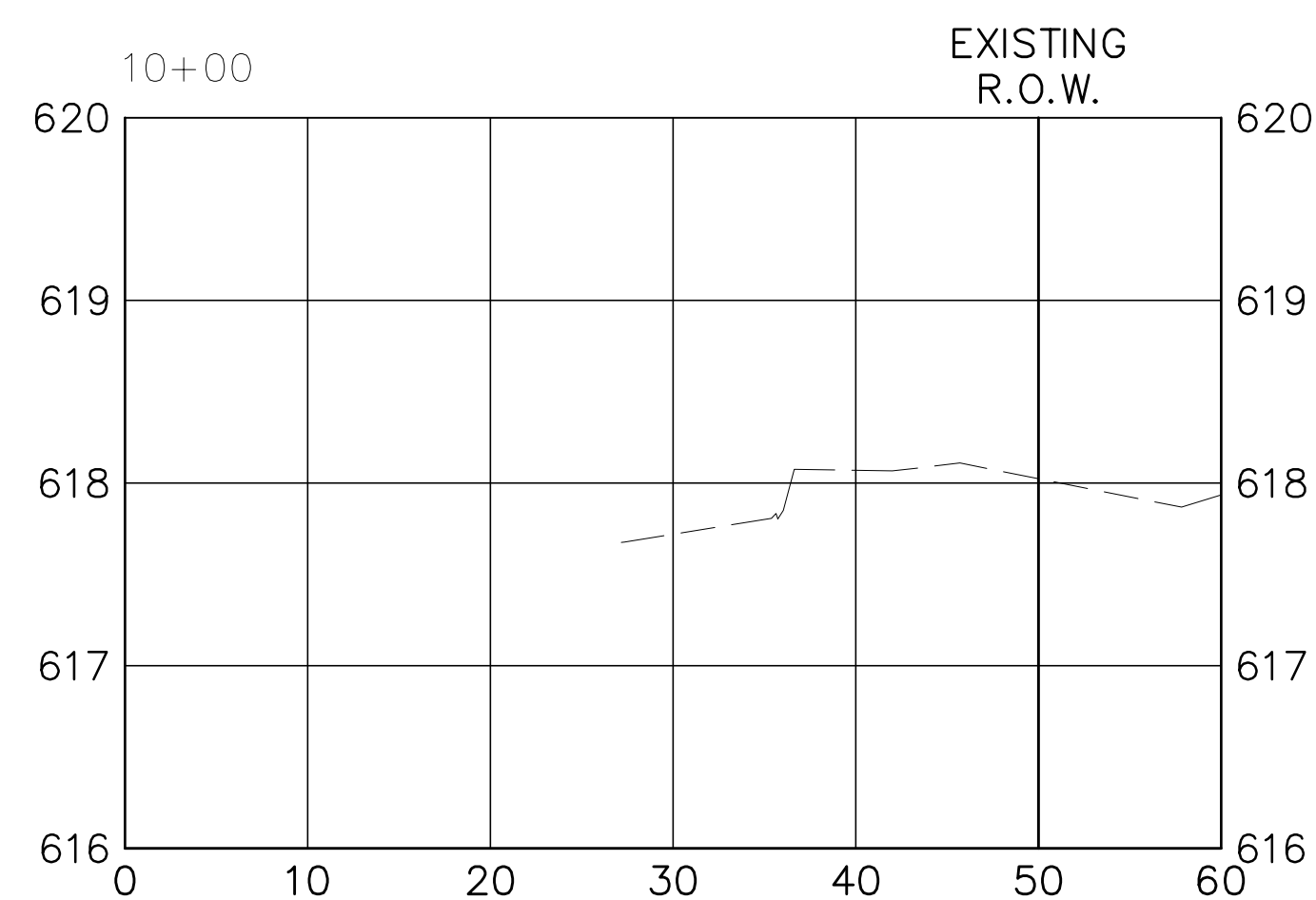
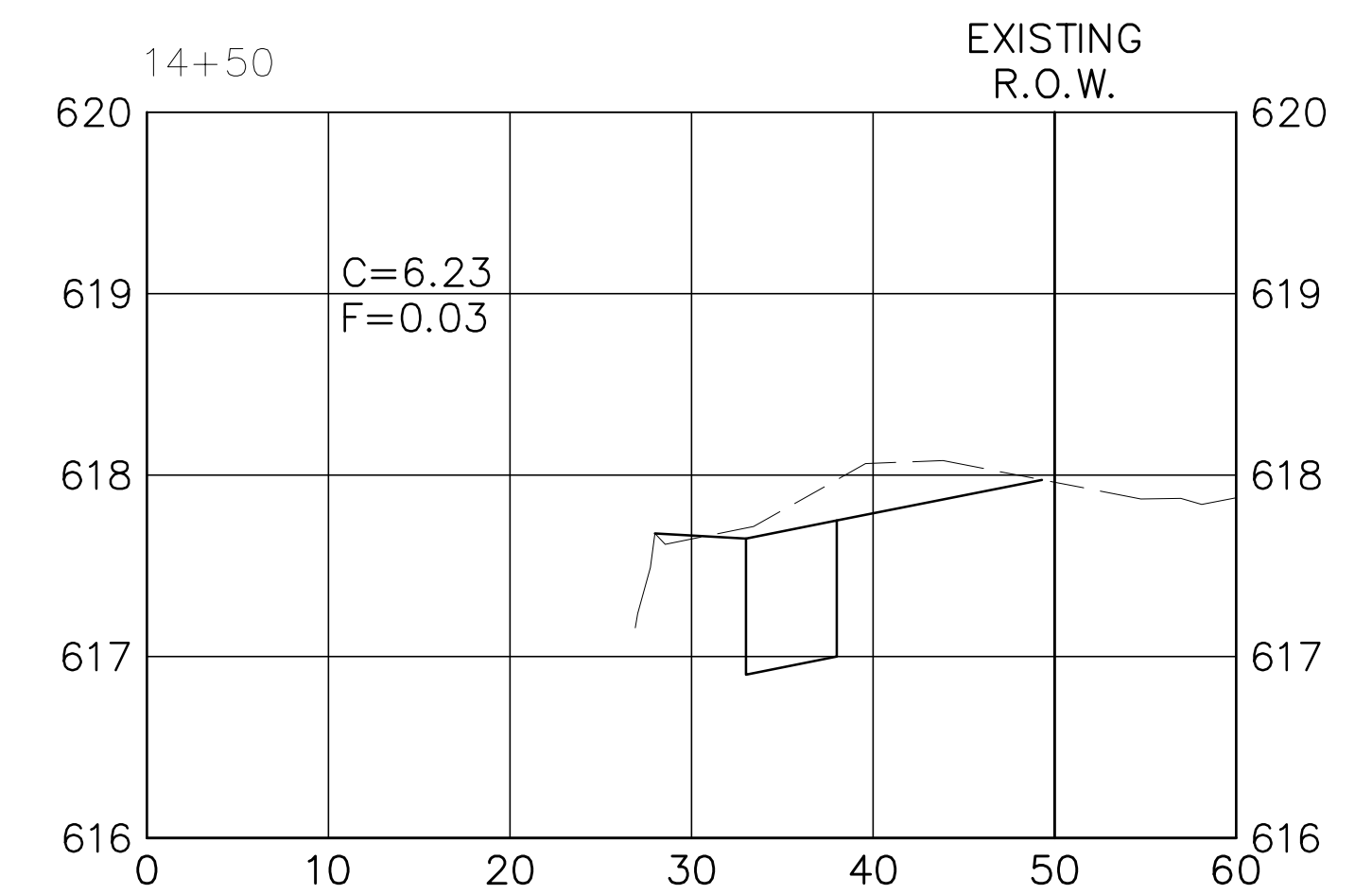
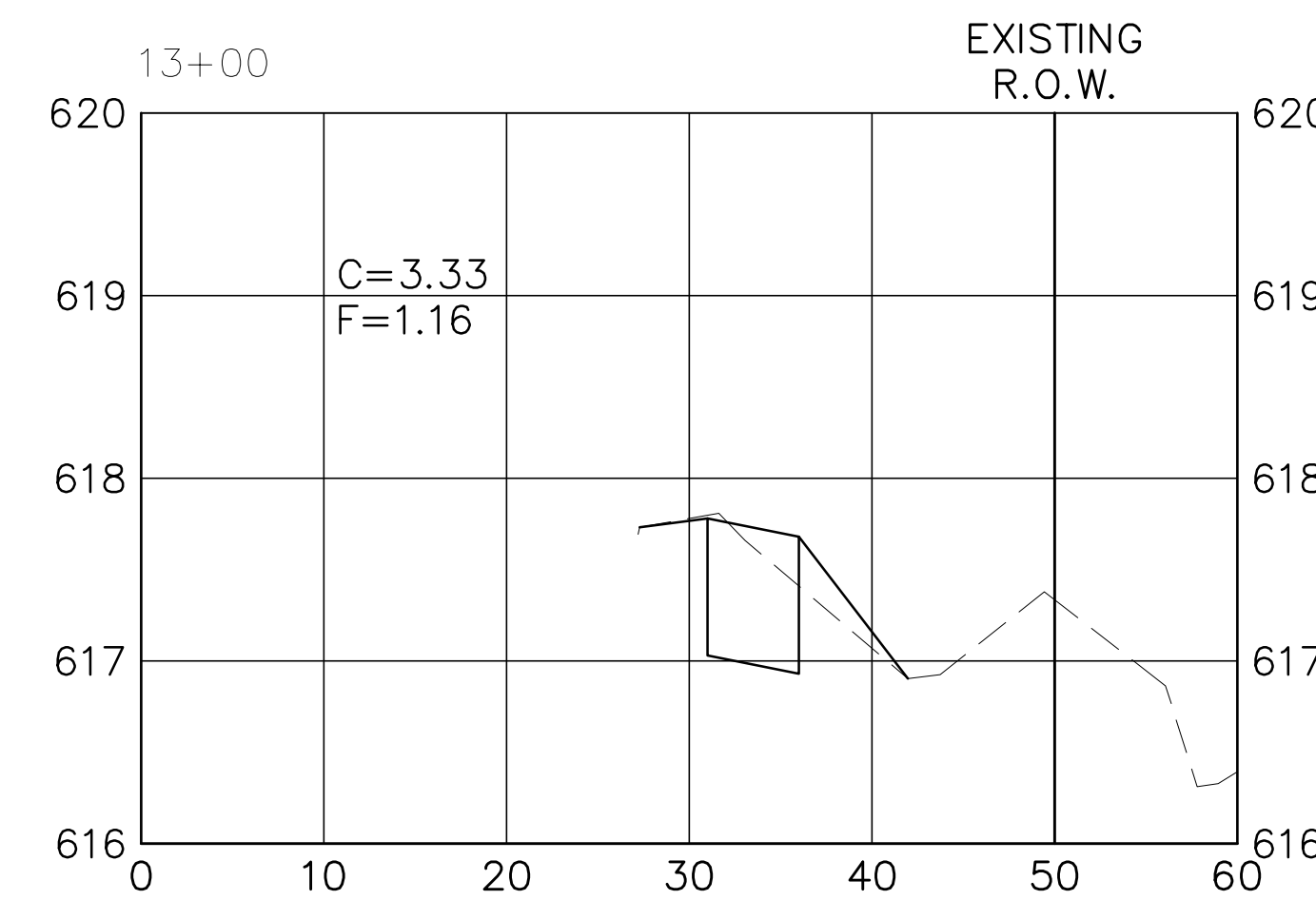
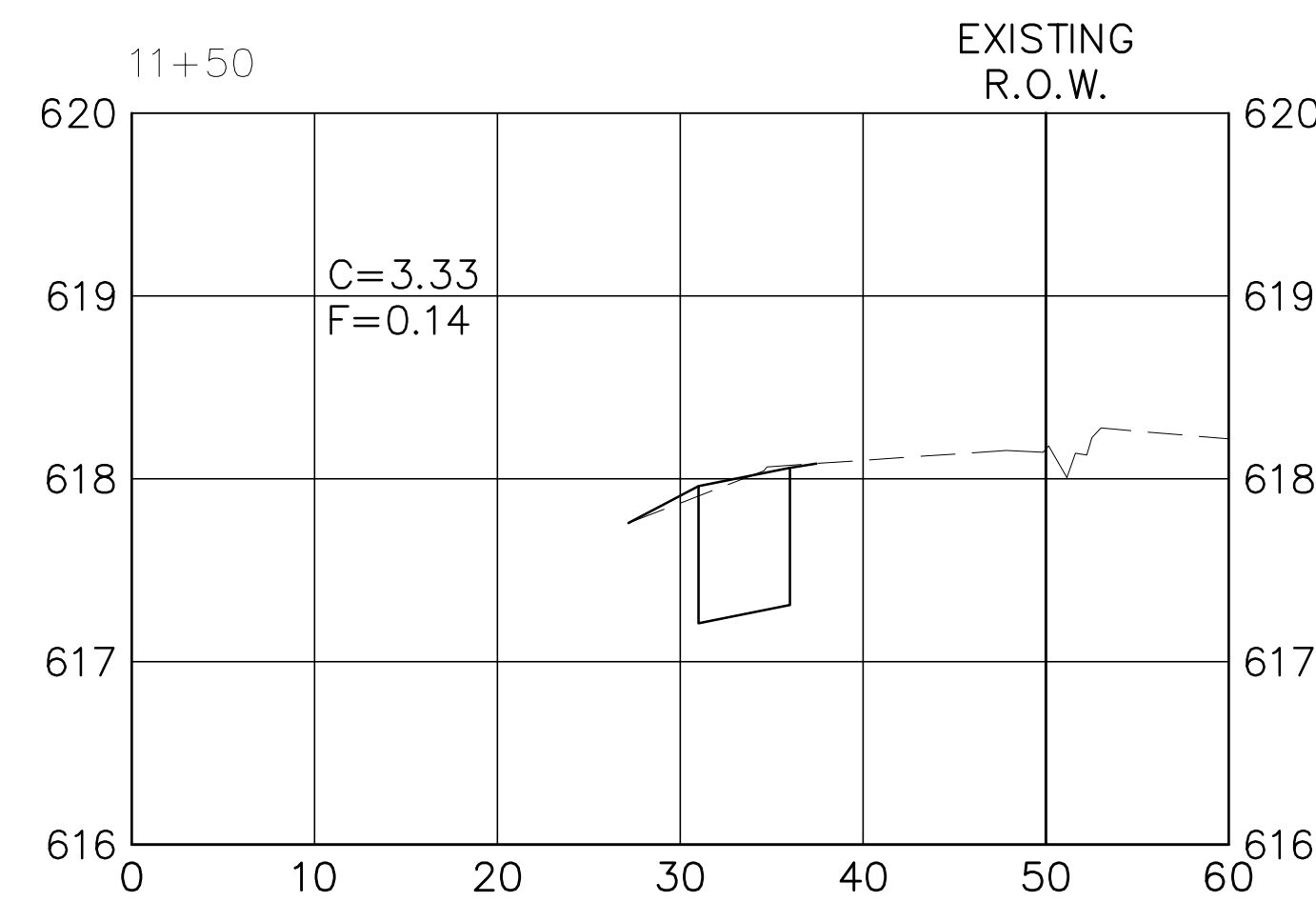
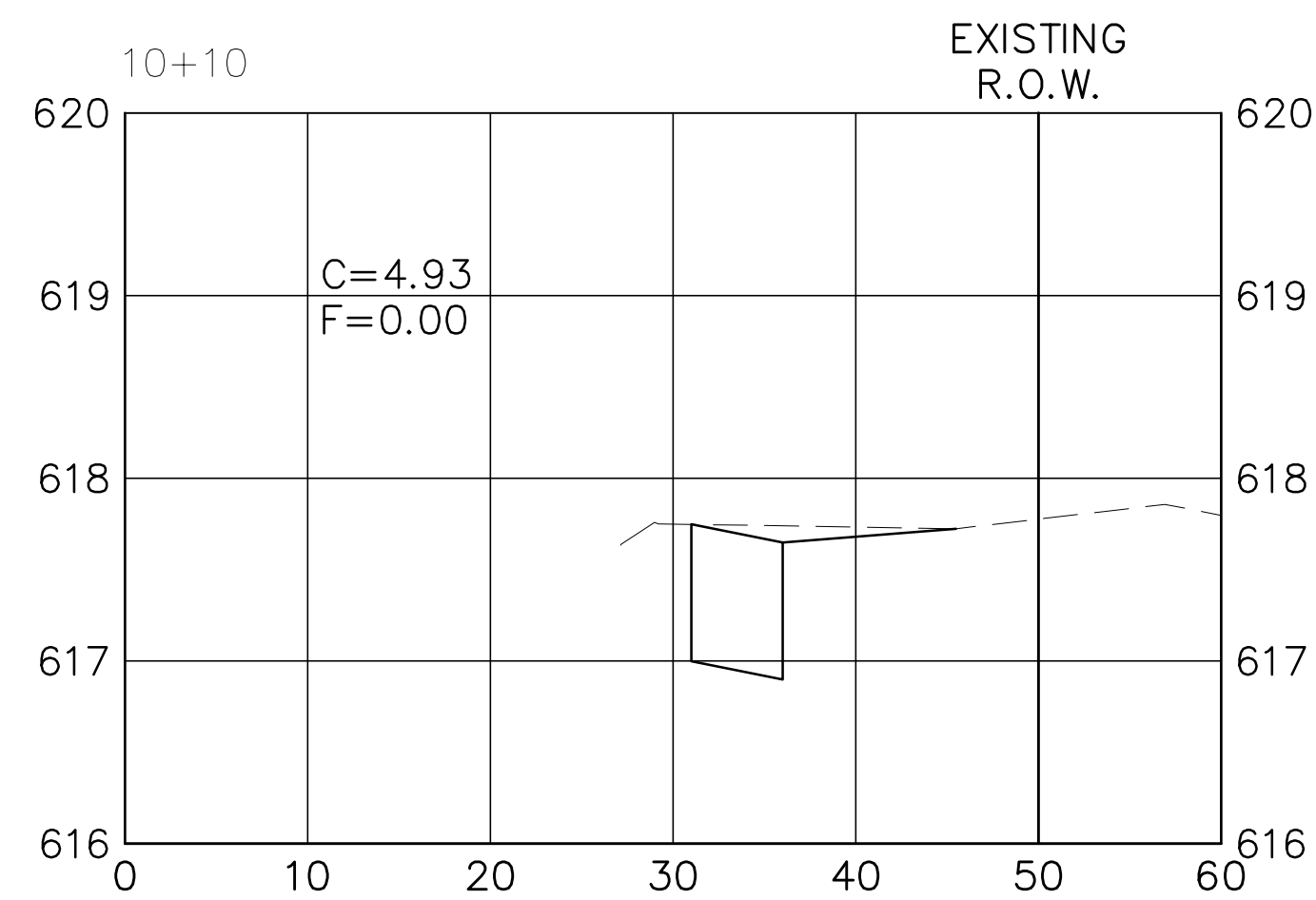
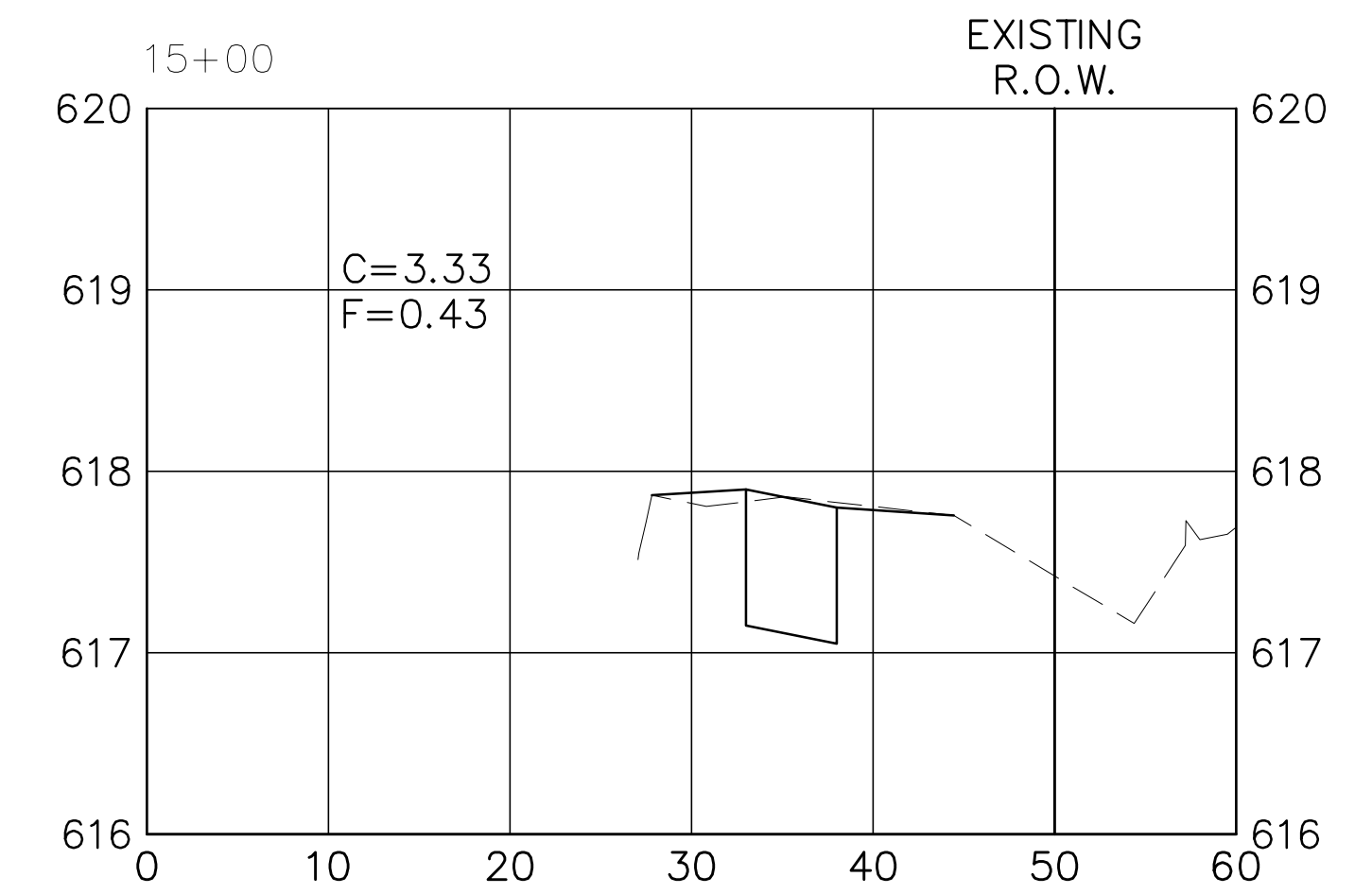
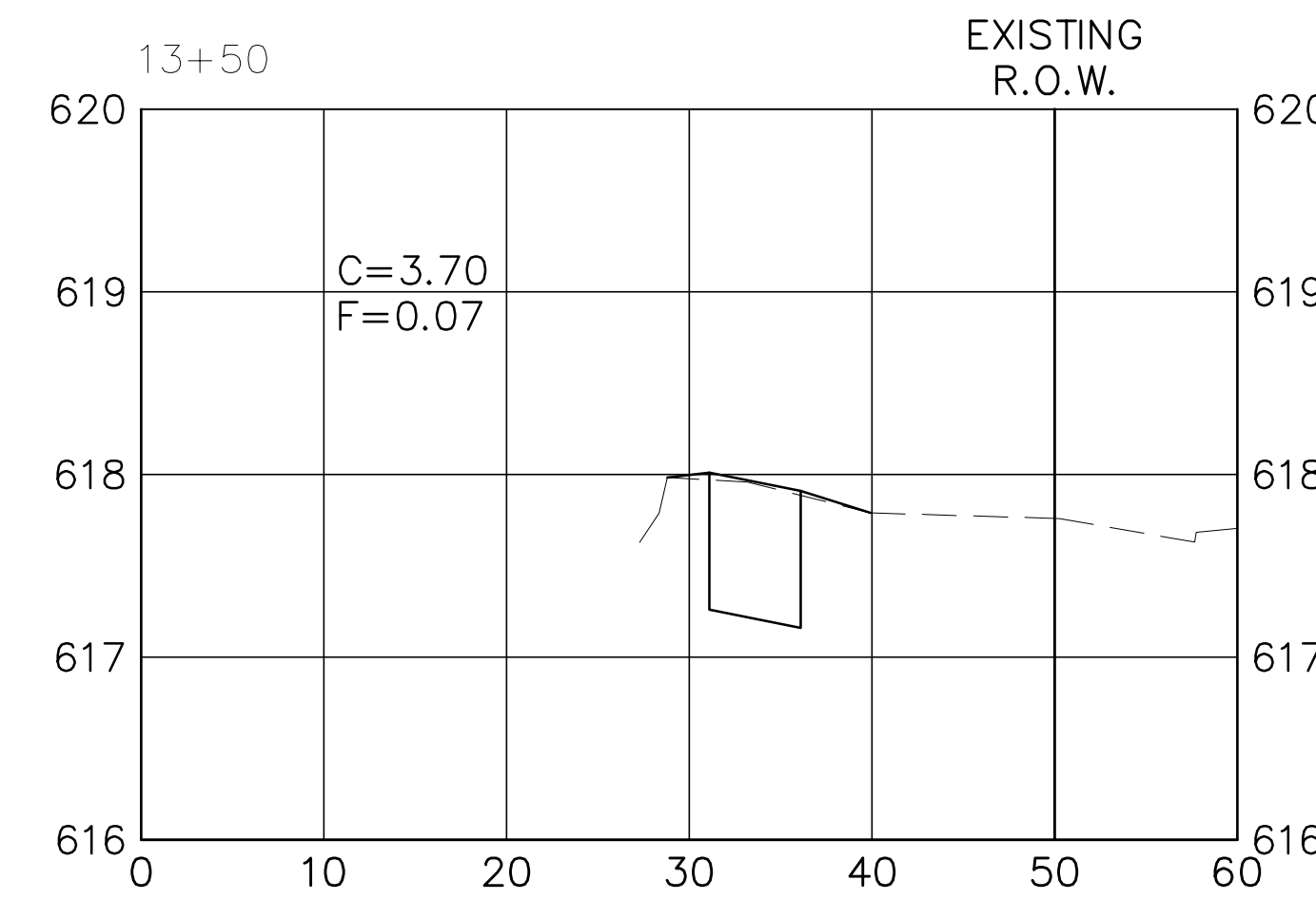
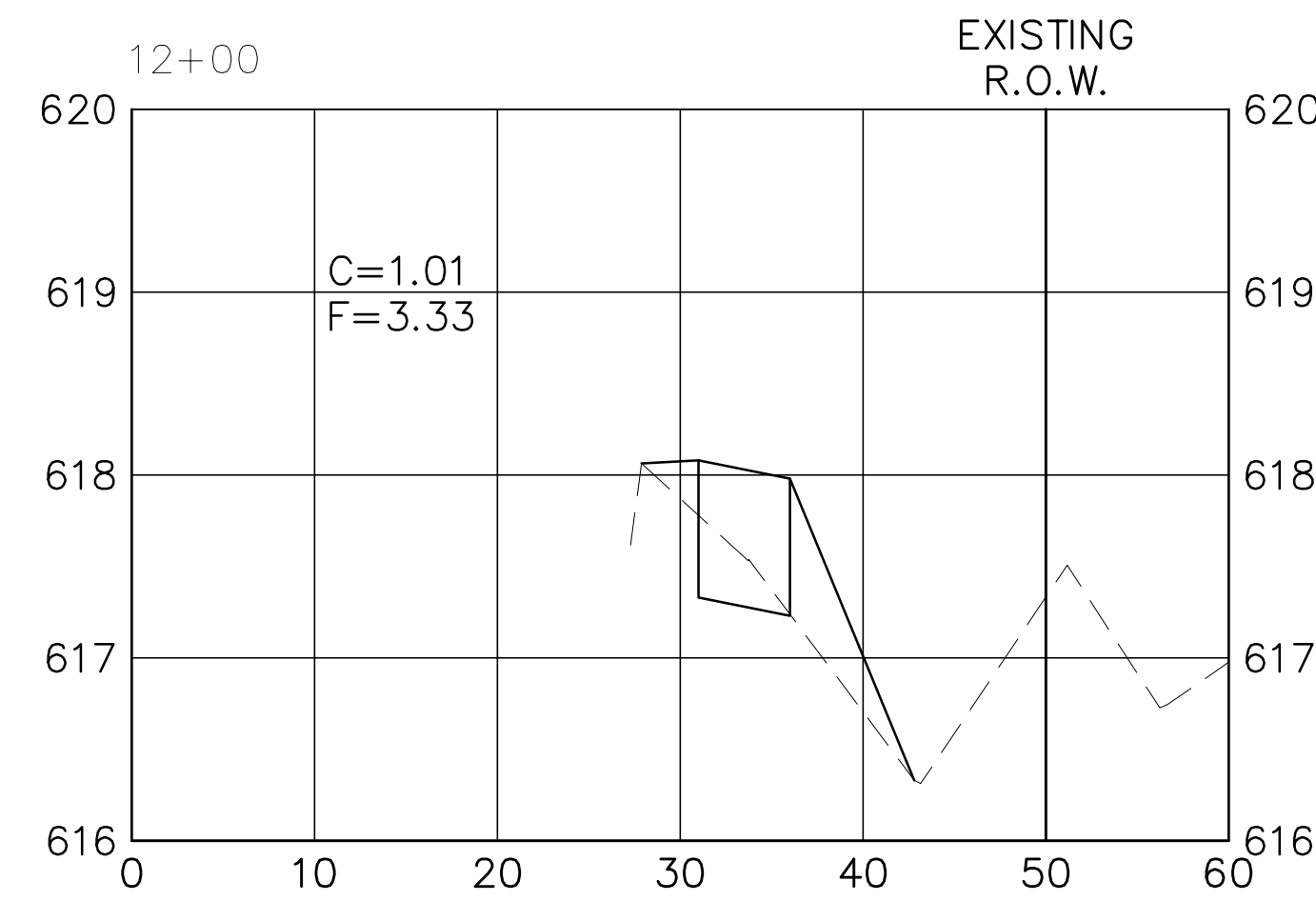
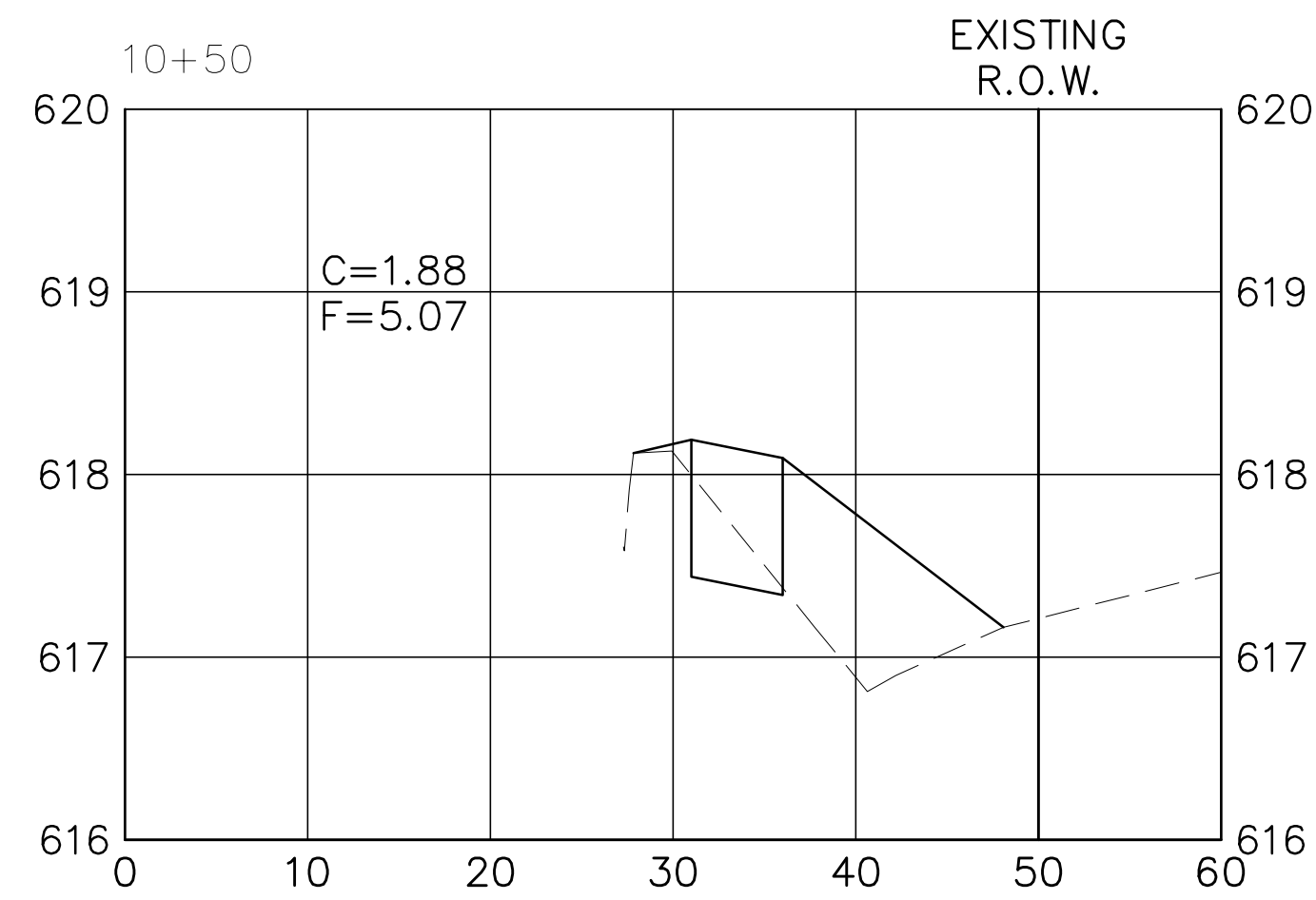
**SIGN TABLE**

SIGN	DIMENSIONS
R10-3b	9" (228mm) X 12" (305mm)
R10-3d	9" (228mm) X 12" (305mm)
R10-3e	9" (228mm) X 15" (381mm)



**BOLT PATTERN**

**PEDESTRIAN PUSH BUTTON POST, TYPE A**



STA. 10+00 TO STA. 19+14 (ARCHER ROAD)  
STA. 10+00 TO STA. 25+60 (STATE ROAD)

DRAWN BY: ALS

CHECKED BY: SAL

DATE: 08/01/2018

REVISED: \_\_\_\_\_

REVISED: PER IDOT REVIEW

REVISED: \_\_\_\_\_

REVISED: \_\_\_\_\_

REVISED: \_\_\_\_\_

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**STATE ROAD  
CROSS-SECTIONS  
STA 10+00 TO STA 15+00**

SCALE: 1"=10'H; 1"=1'V

SHEET No. 1 OF 3 SHEETS

FAU  
RTE  
3565, 3577

SECTION  
12-00035-00-SW

COUNTY  
COOK

TOTAL  
SHEETS  
30

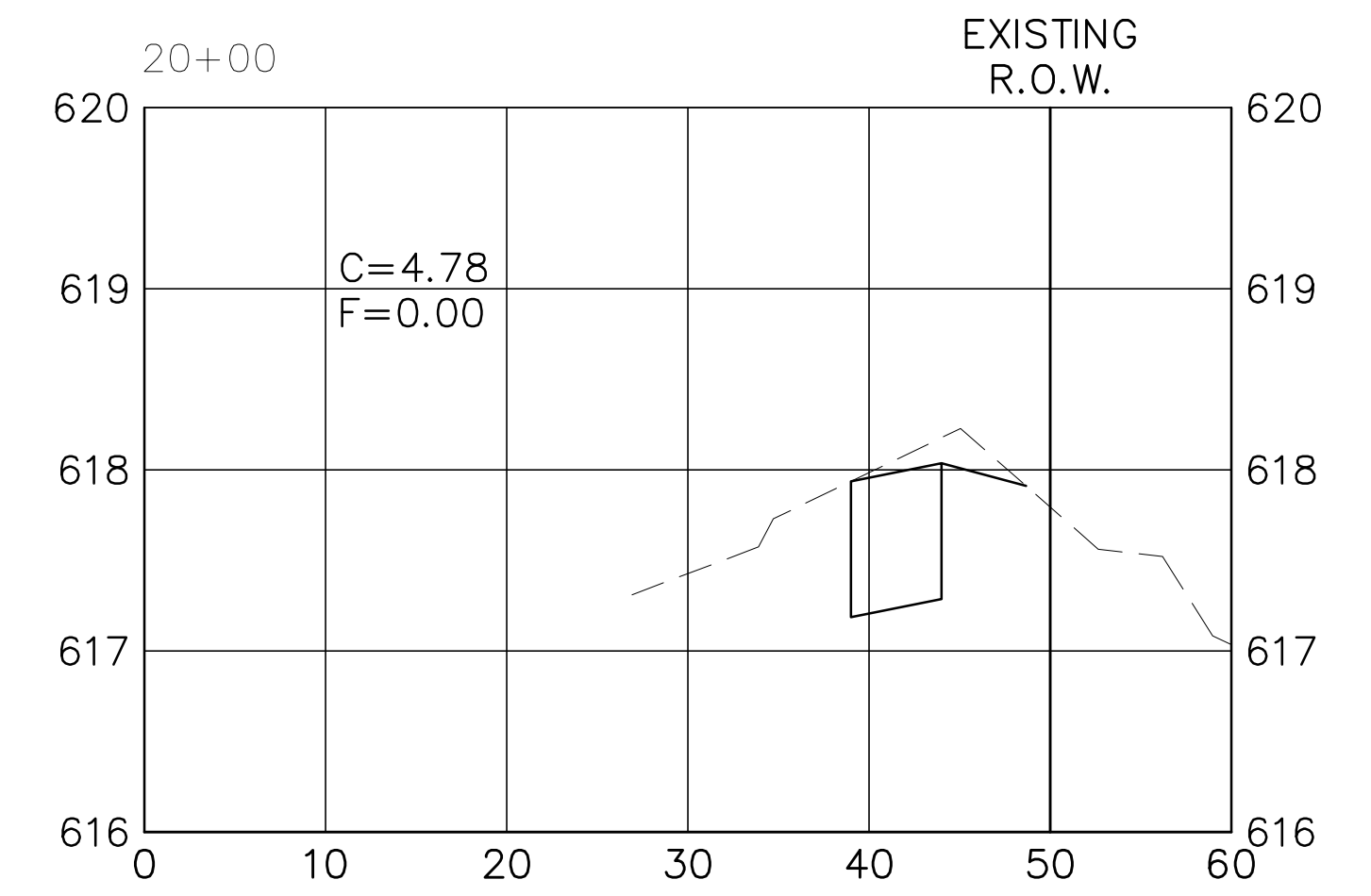
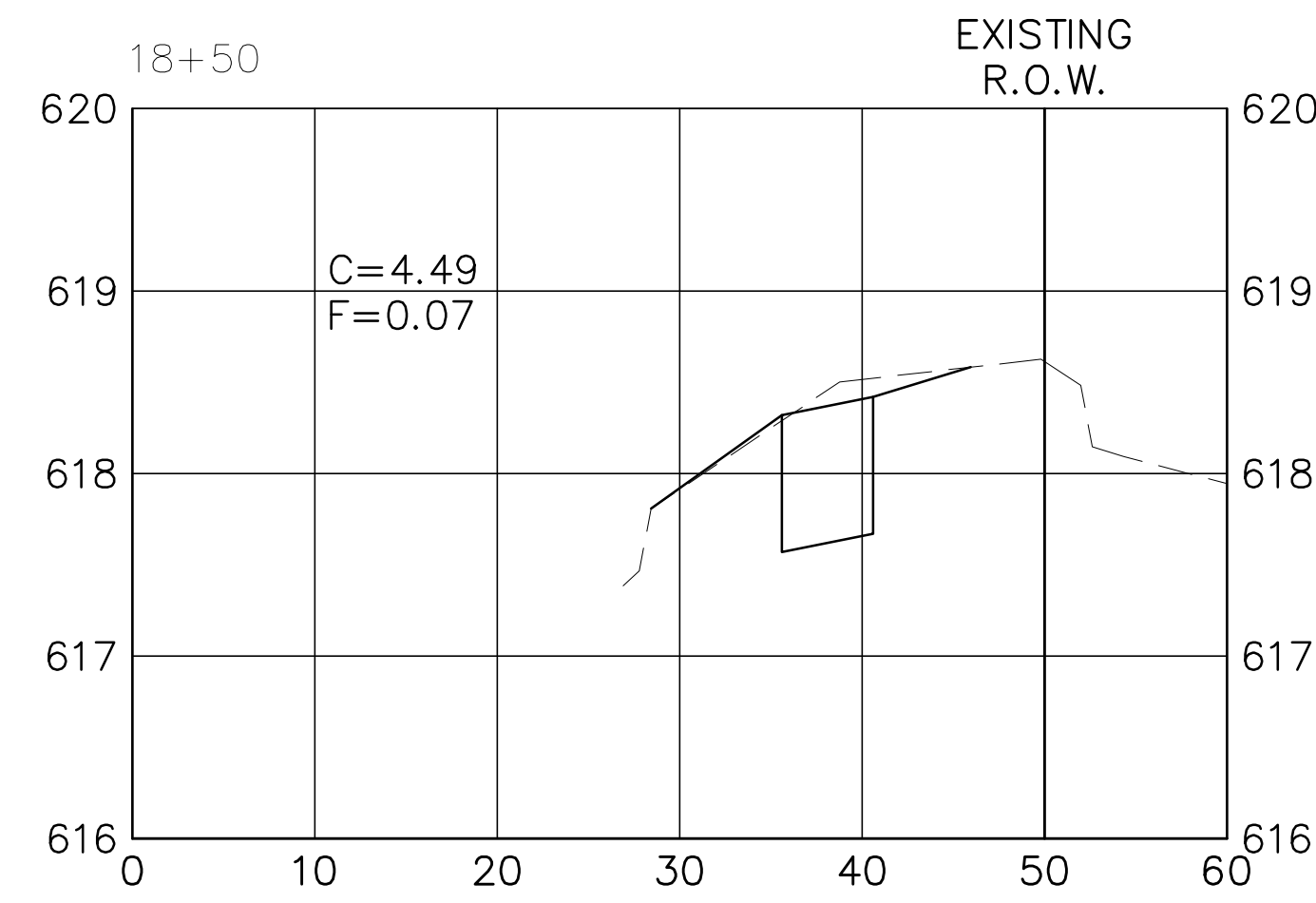
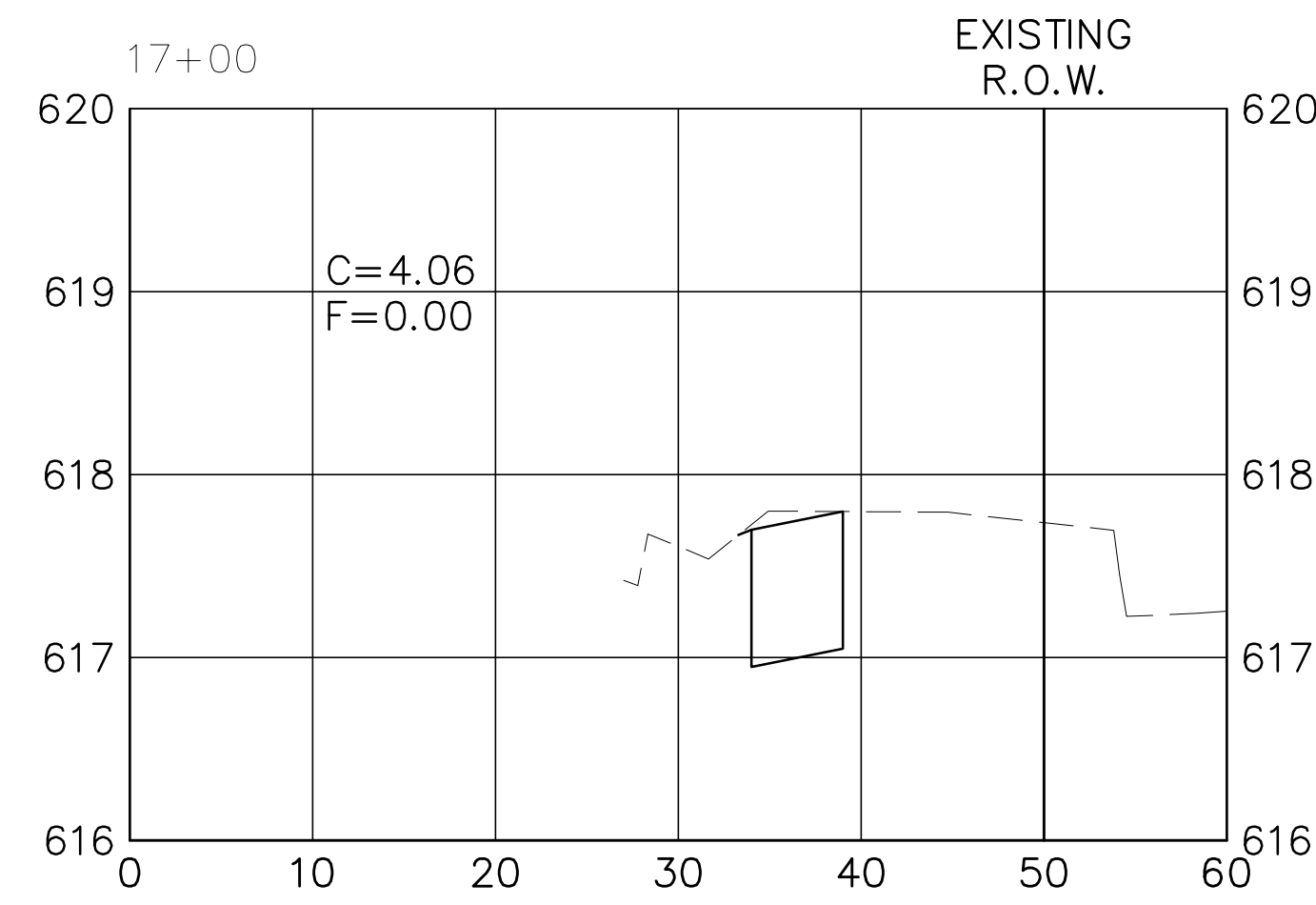
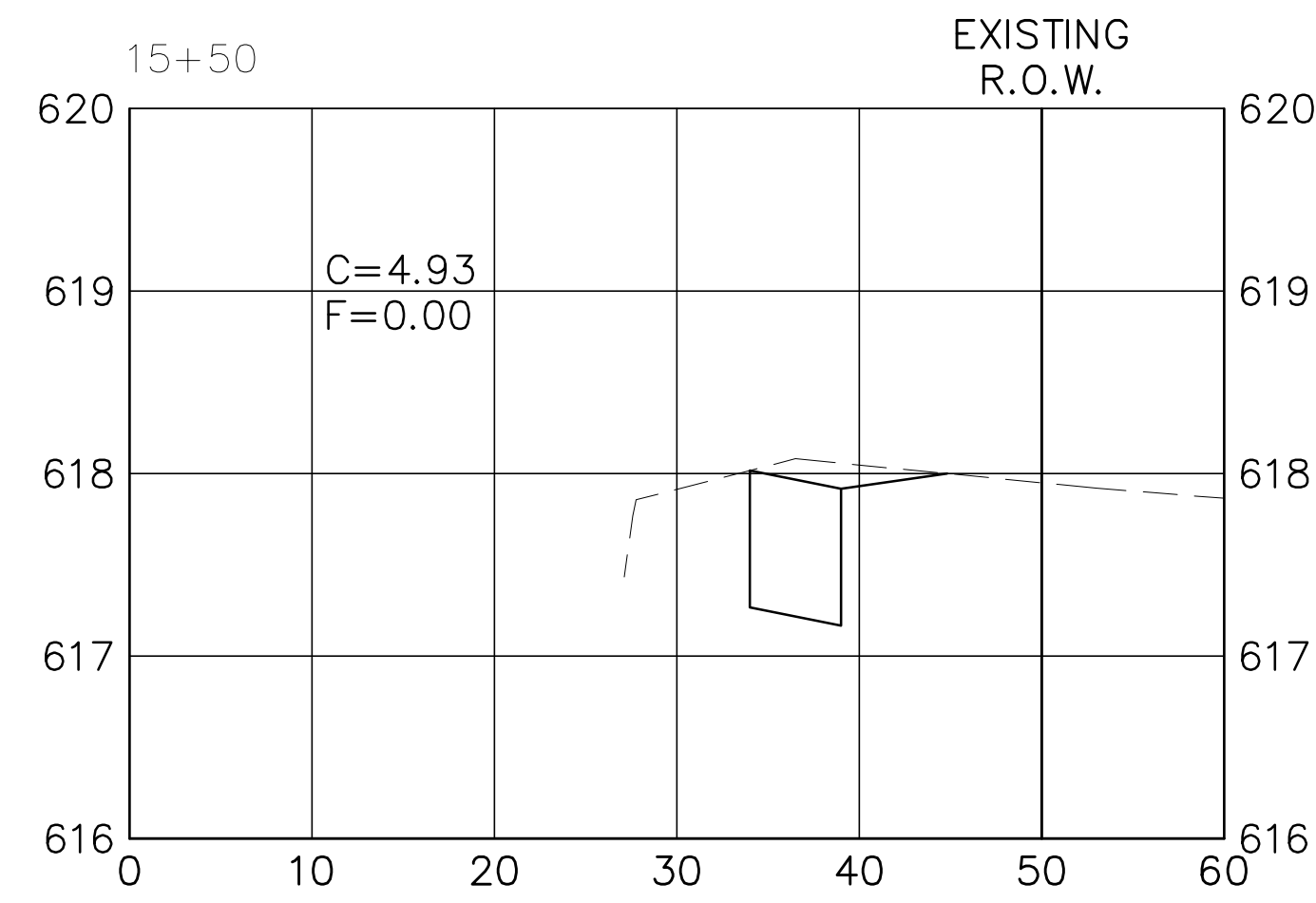
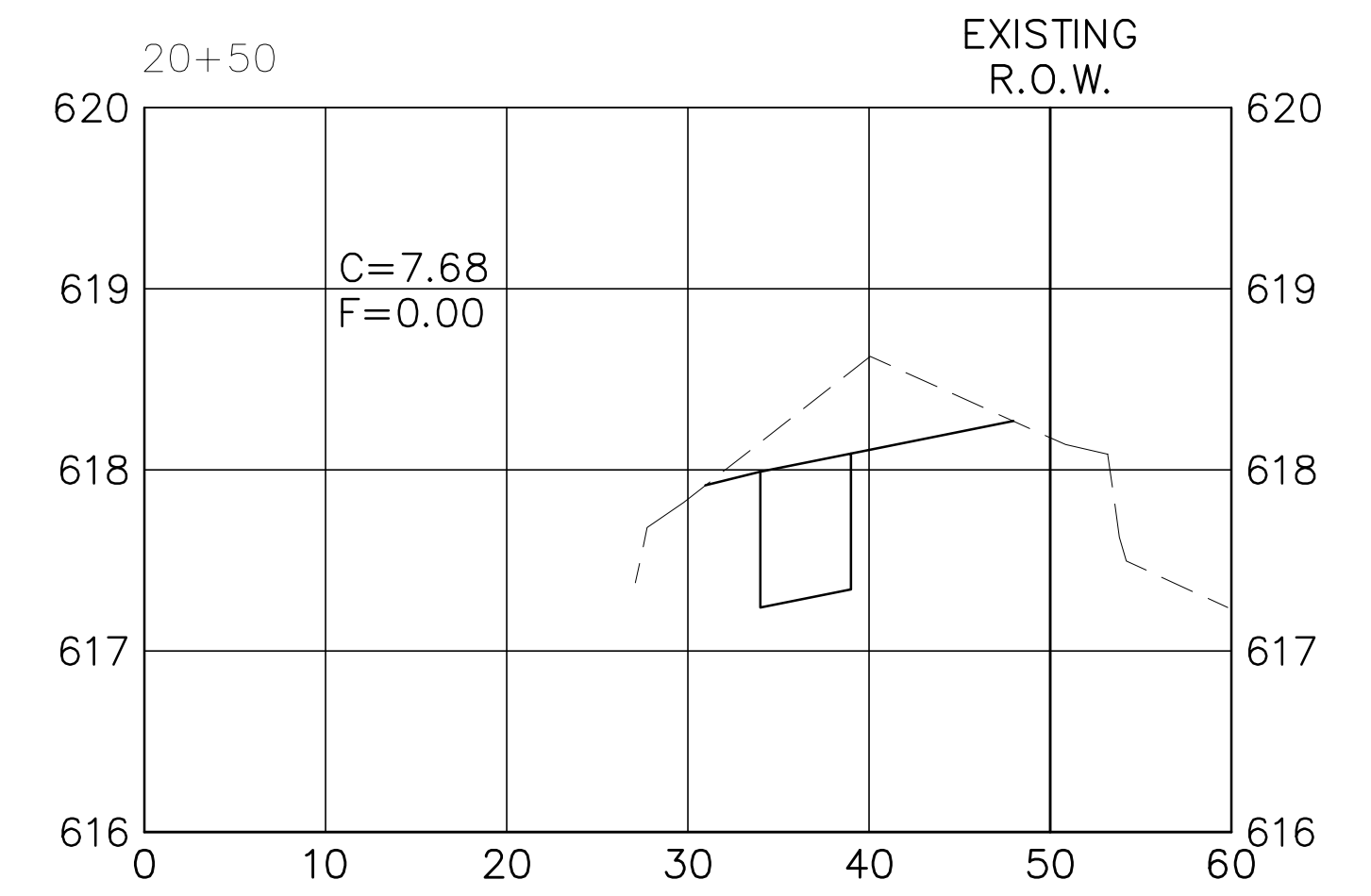
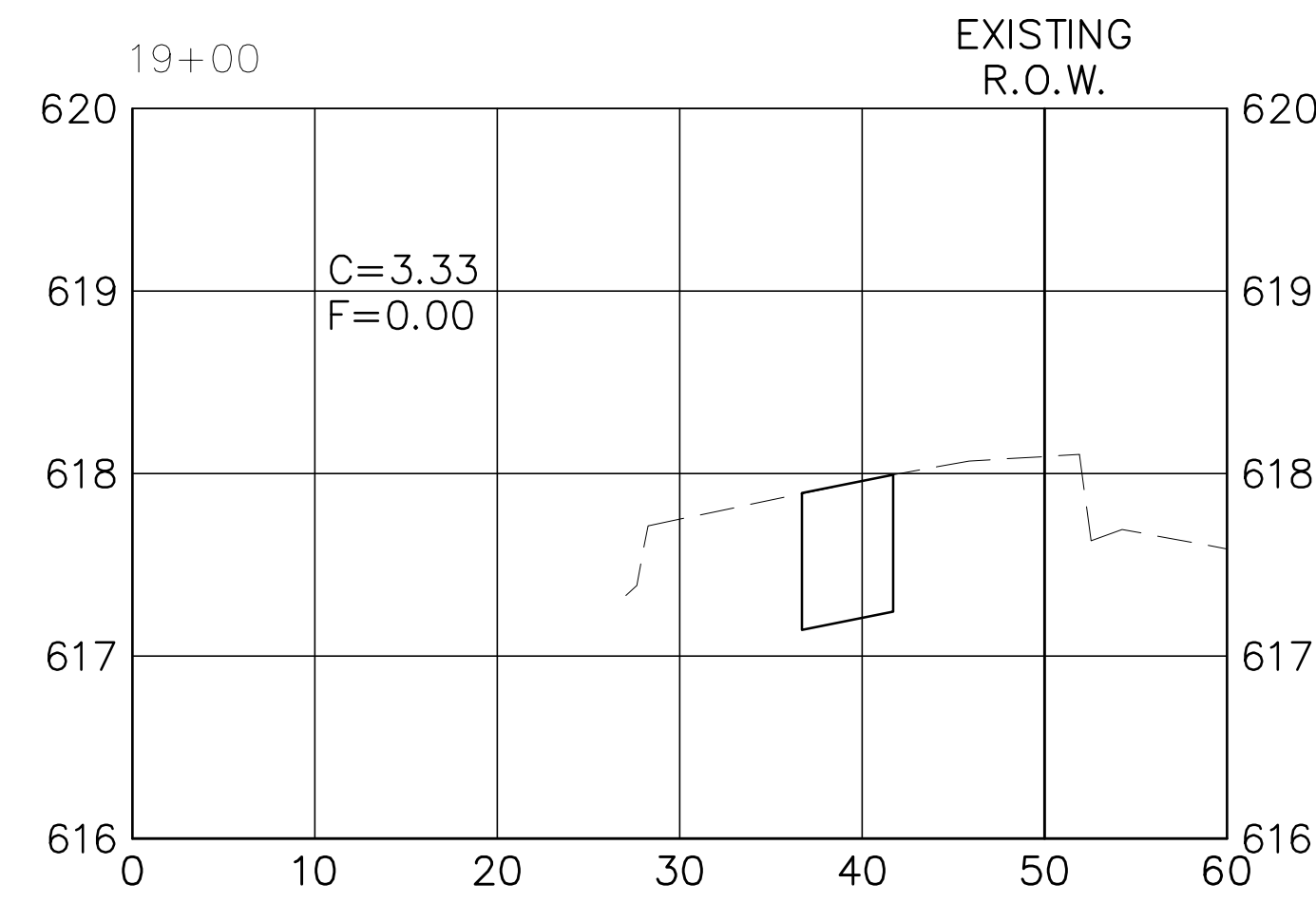
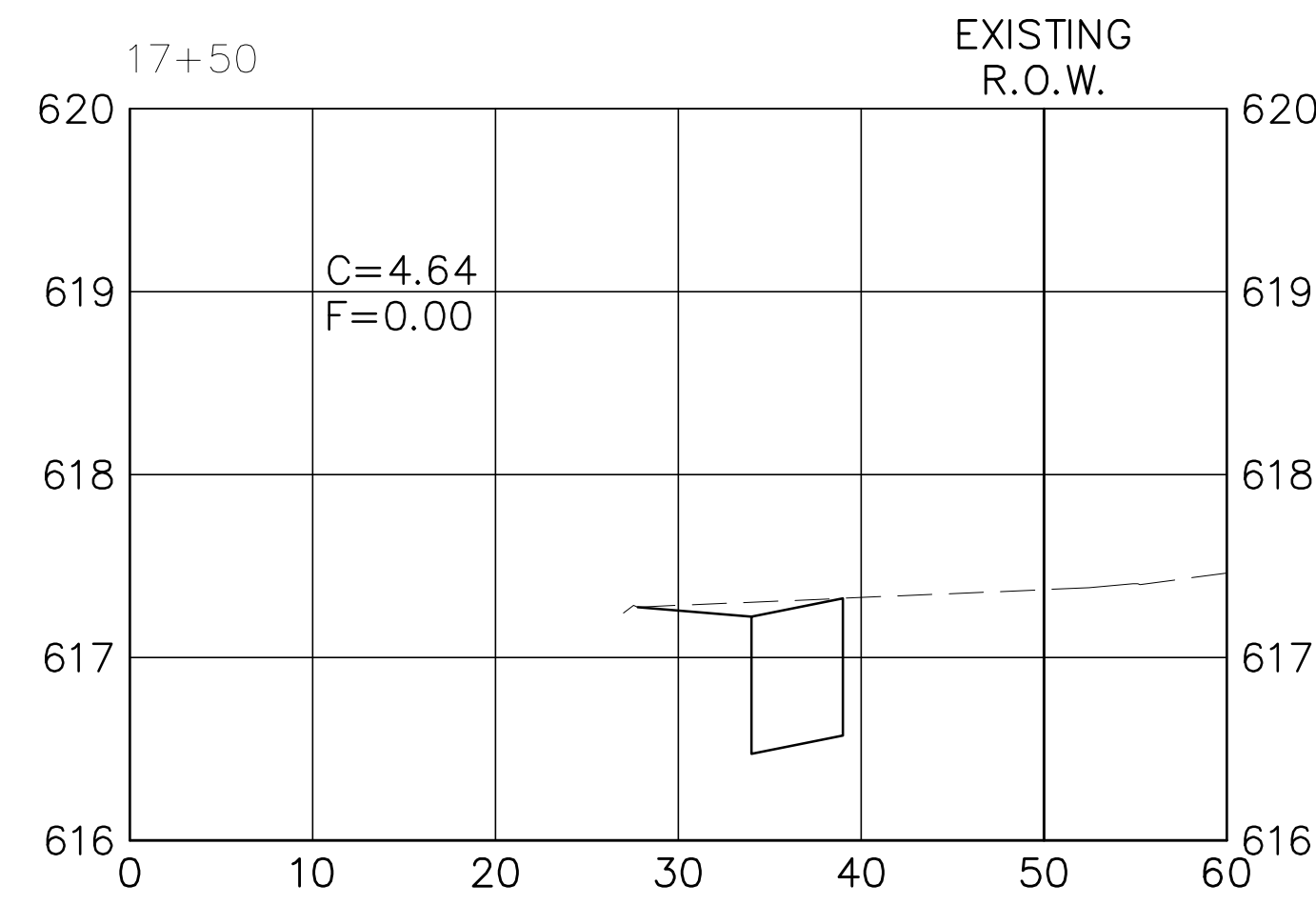
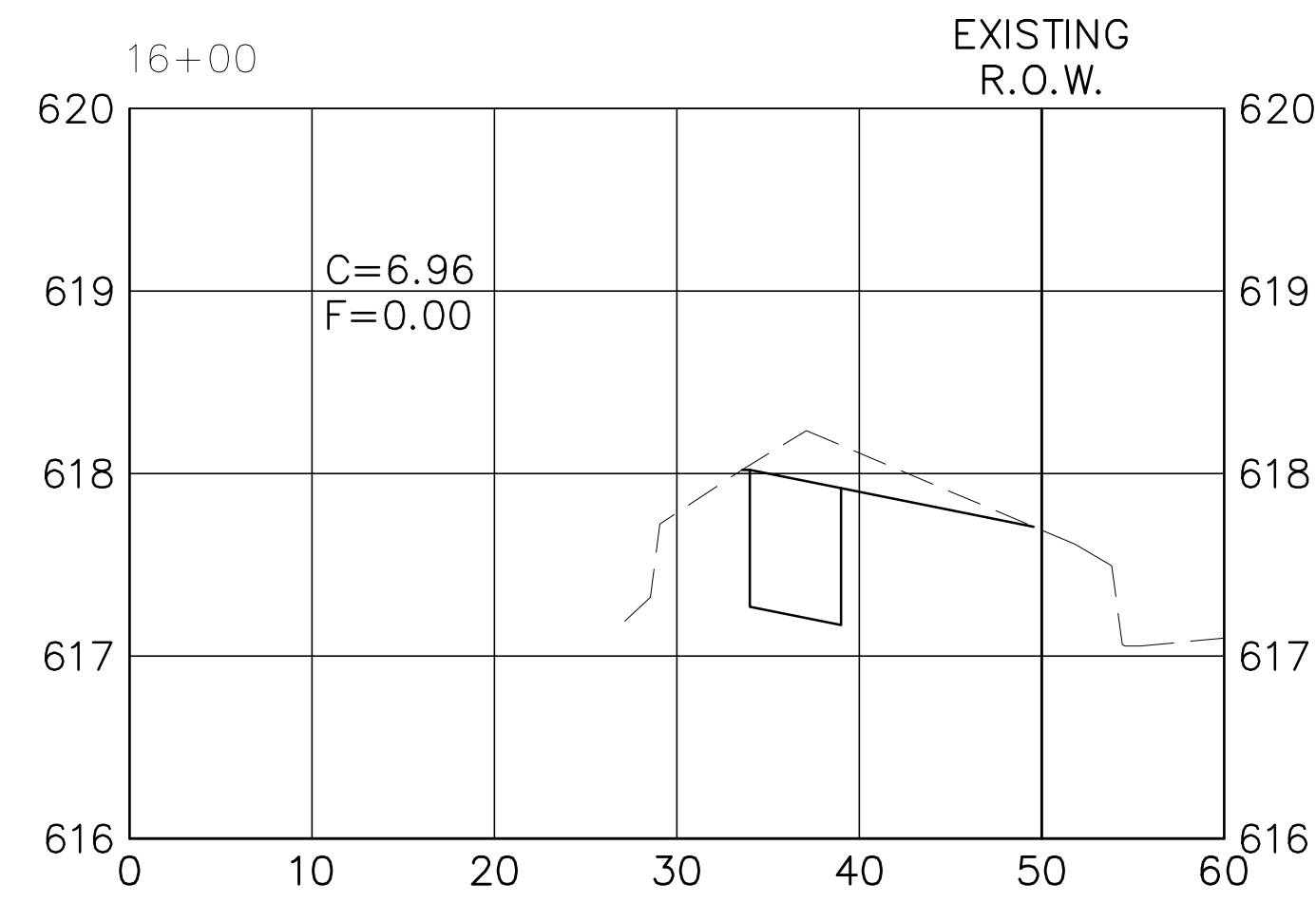
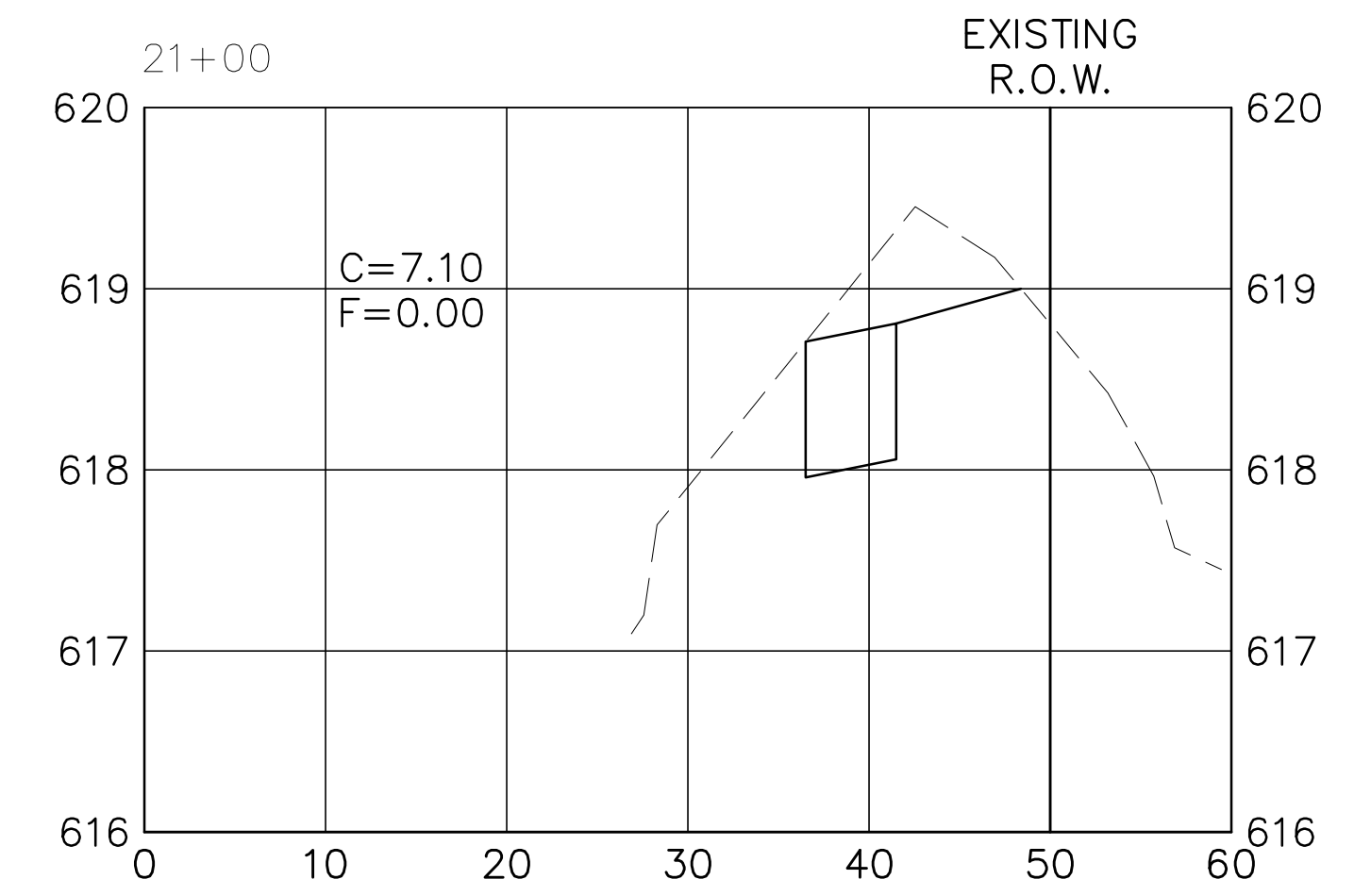
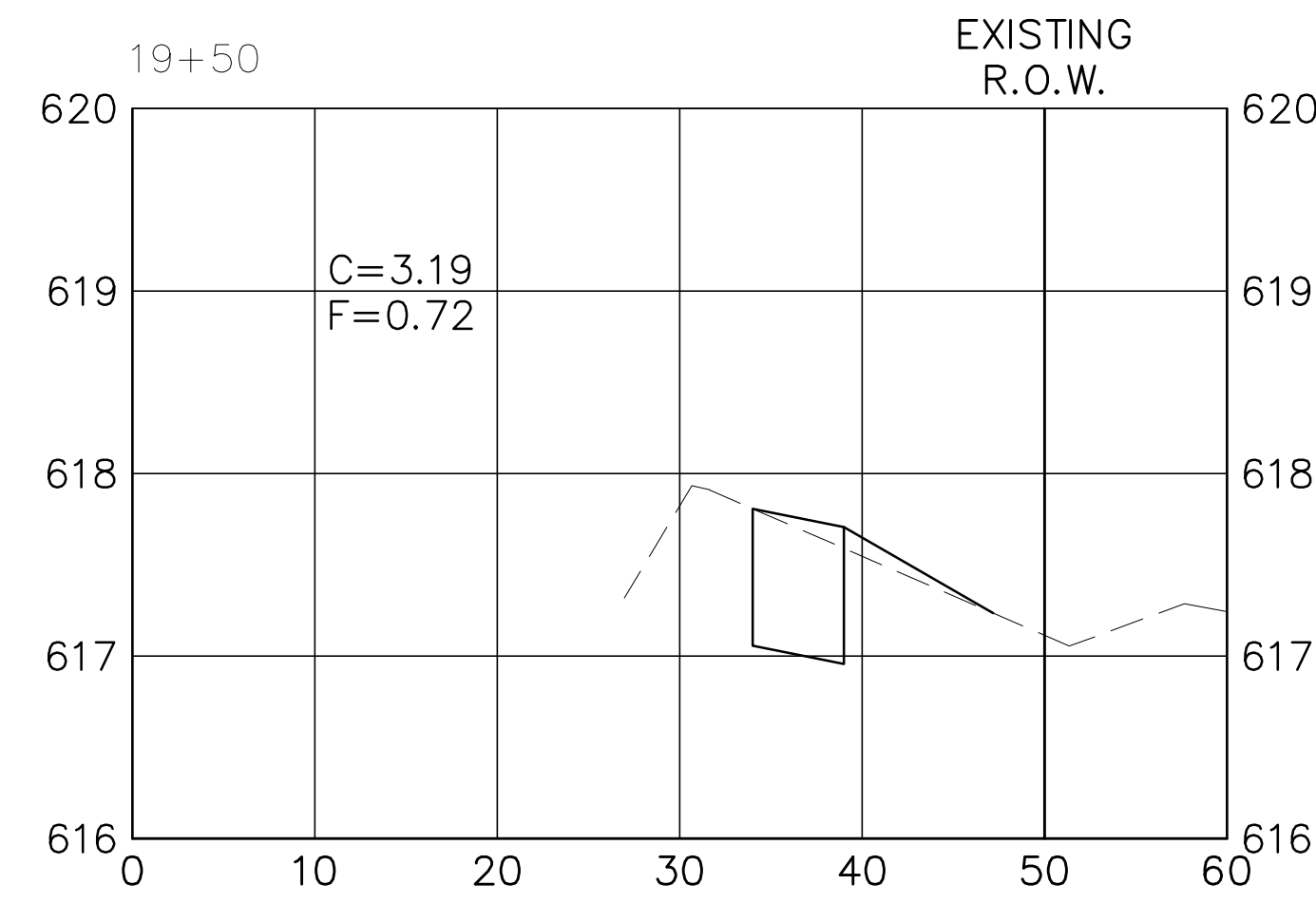
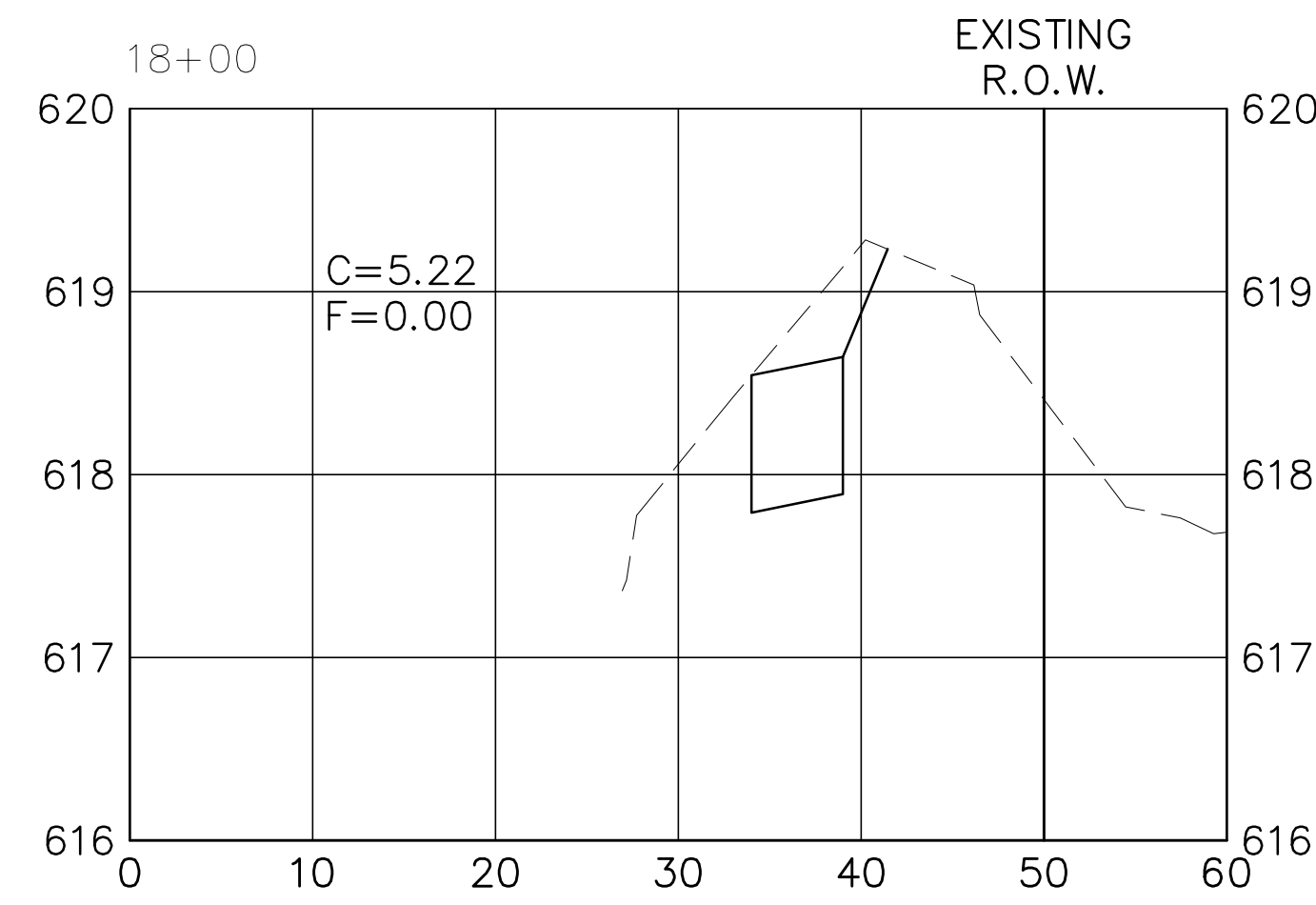
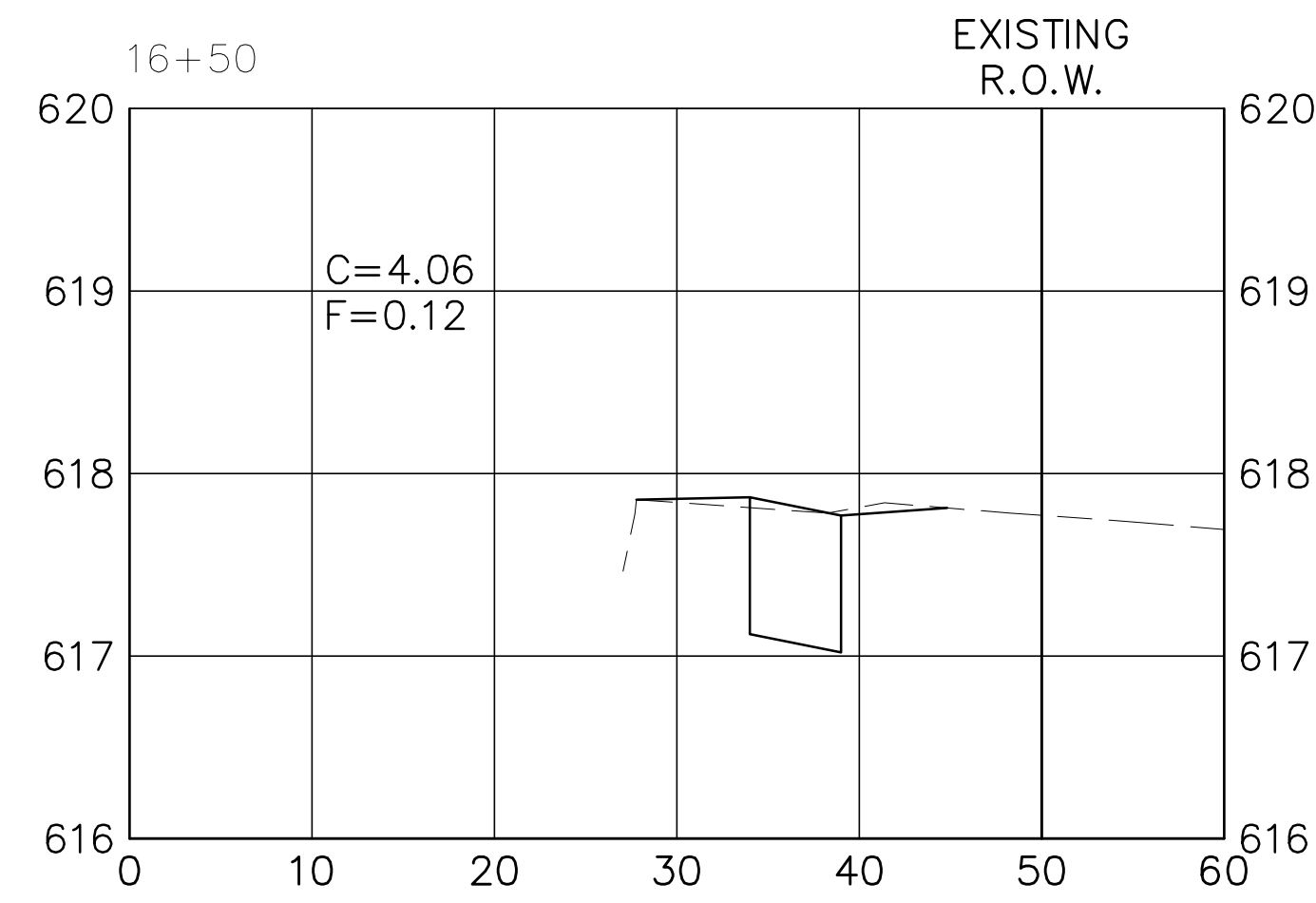
SHEET  
NO.  
26

CONTRACT No. 61F35

FED. ROAD DIST. NO.

ILLINOIS

FED. AID PROJECT



STA. 10+00 TO STA. 19+14 (ARCHER ROAD)  
STA. 10+00 TO STA. 25+60 (STATE ROAD)

DRAWN BY: ALS

CHECKED BY: SAL

DATE: 08/01/2018

REVISED: \_\_\_\_\_

REVISED: PER IDOT REVIEW

REVISED: \_\_\_\_\_

REVISED: \_\_\_\_\_

REVISED: \_\_\_\_\_

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**STATE ROAD  
CROSS-SECTIONS  
STA 15+50 TO STA 21+00**

SCALE: 1"=10'H; 1"=1'V

SHEET No. 2 OF 3 SHEETS

FAU  
RTE  
3565, 3577

SECTION  
12-00035-00-SW

COUNTY  
COOK

TOTAL  
SHEETS  
30

SHEET  
NO.  
27

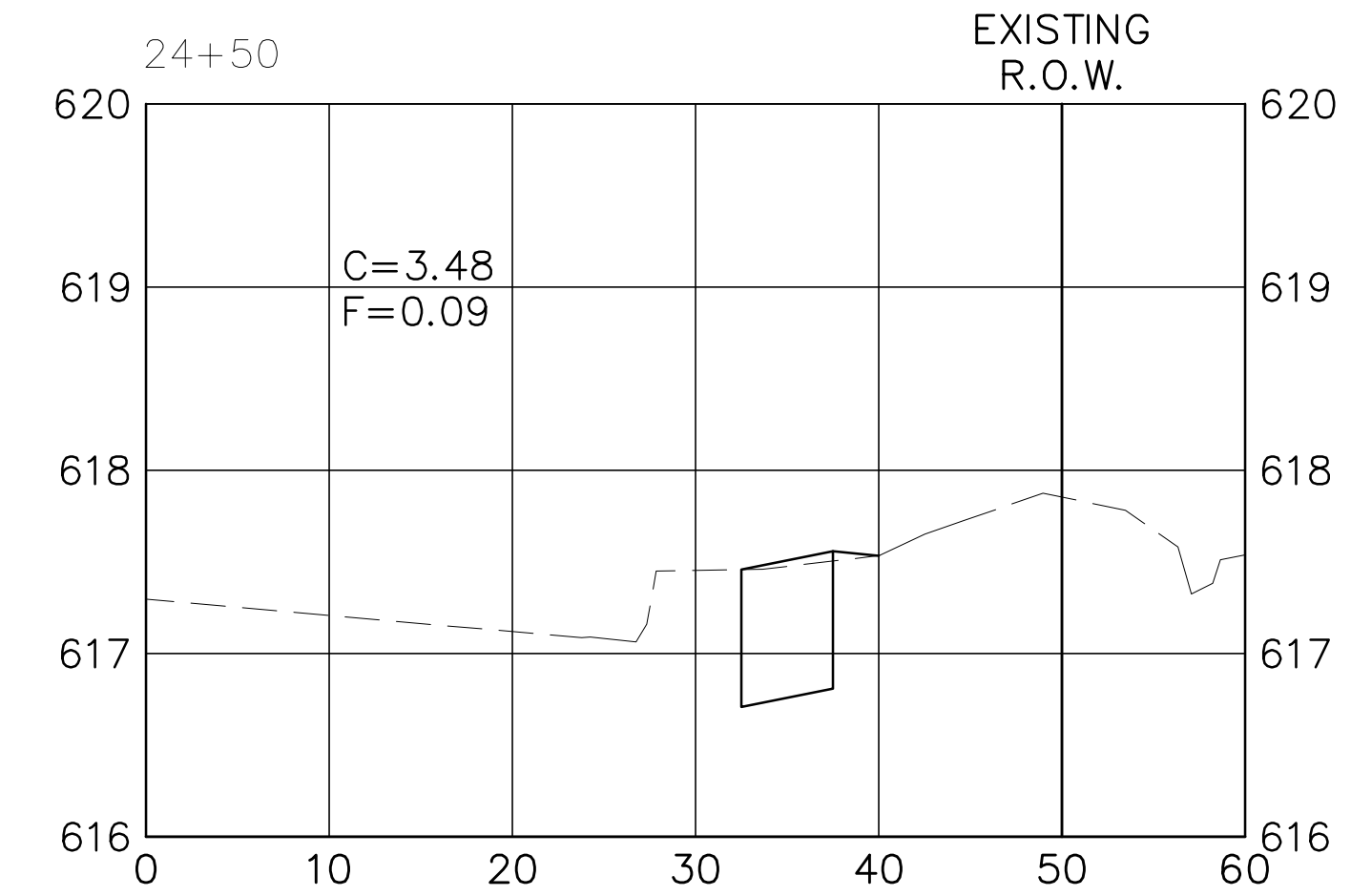
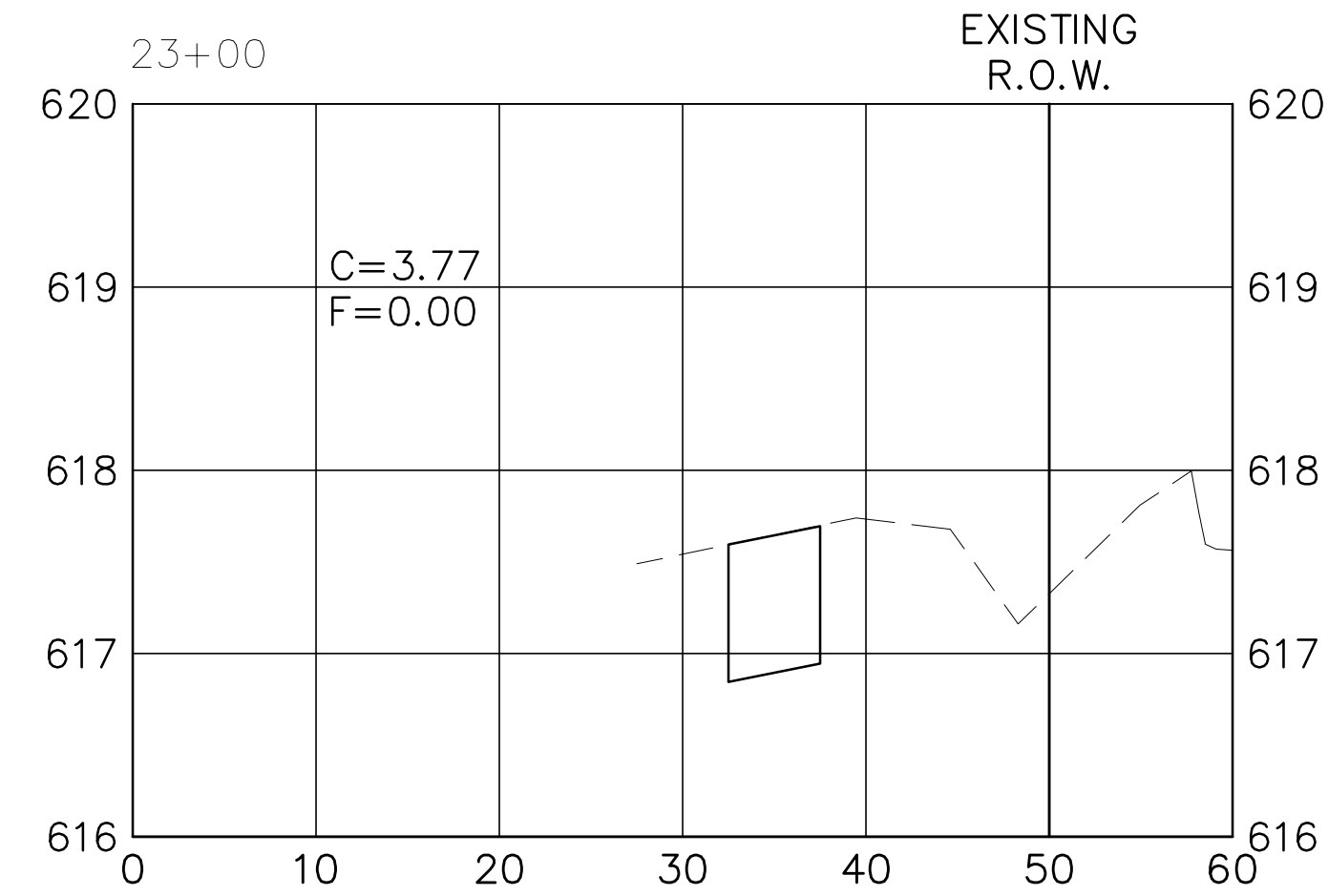
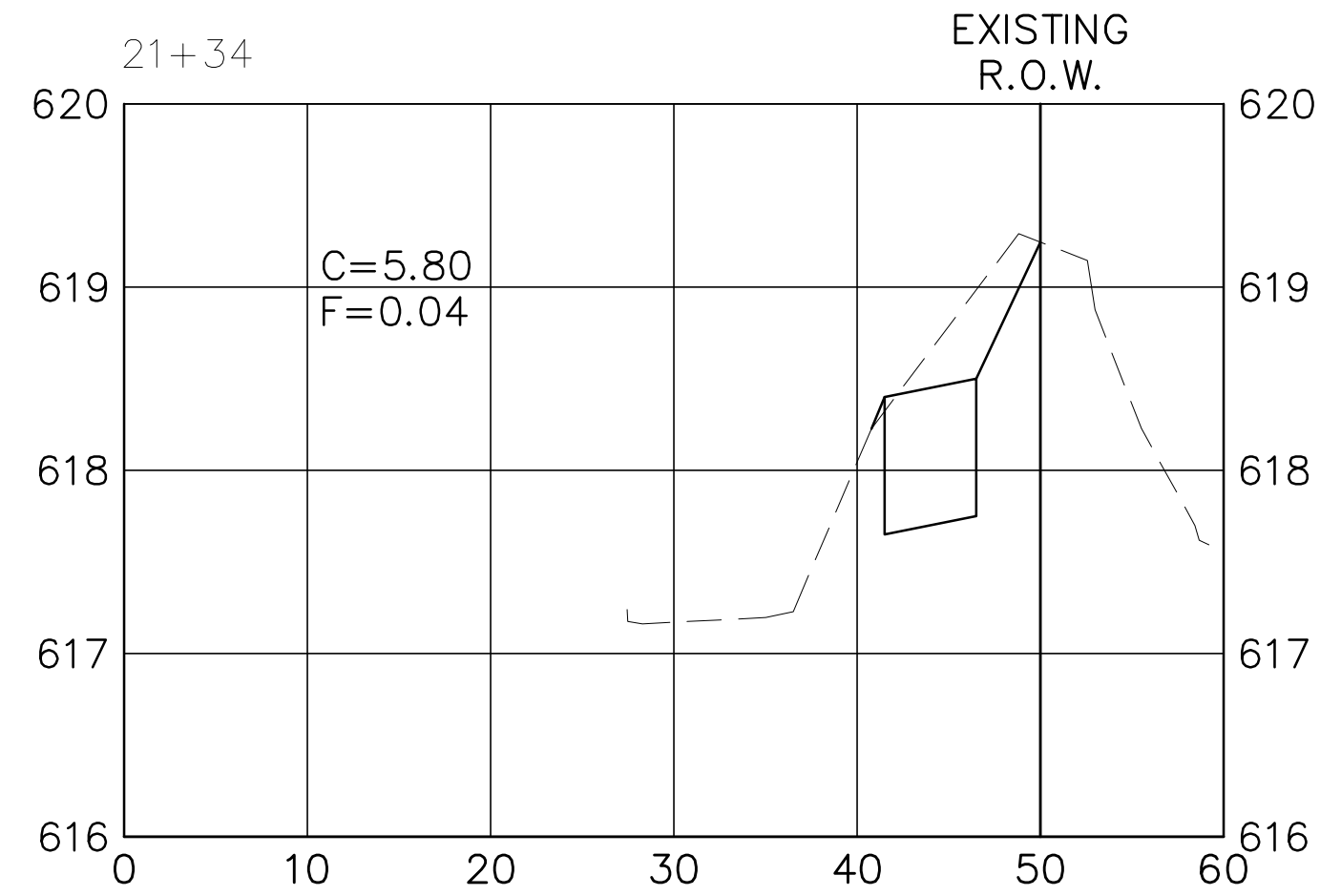
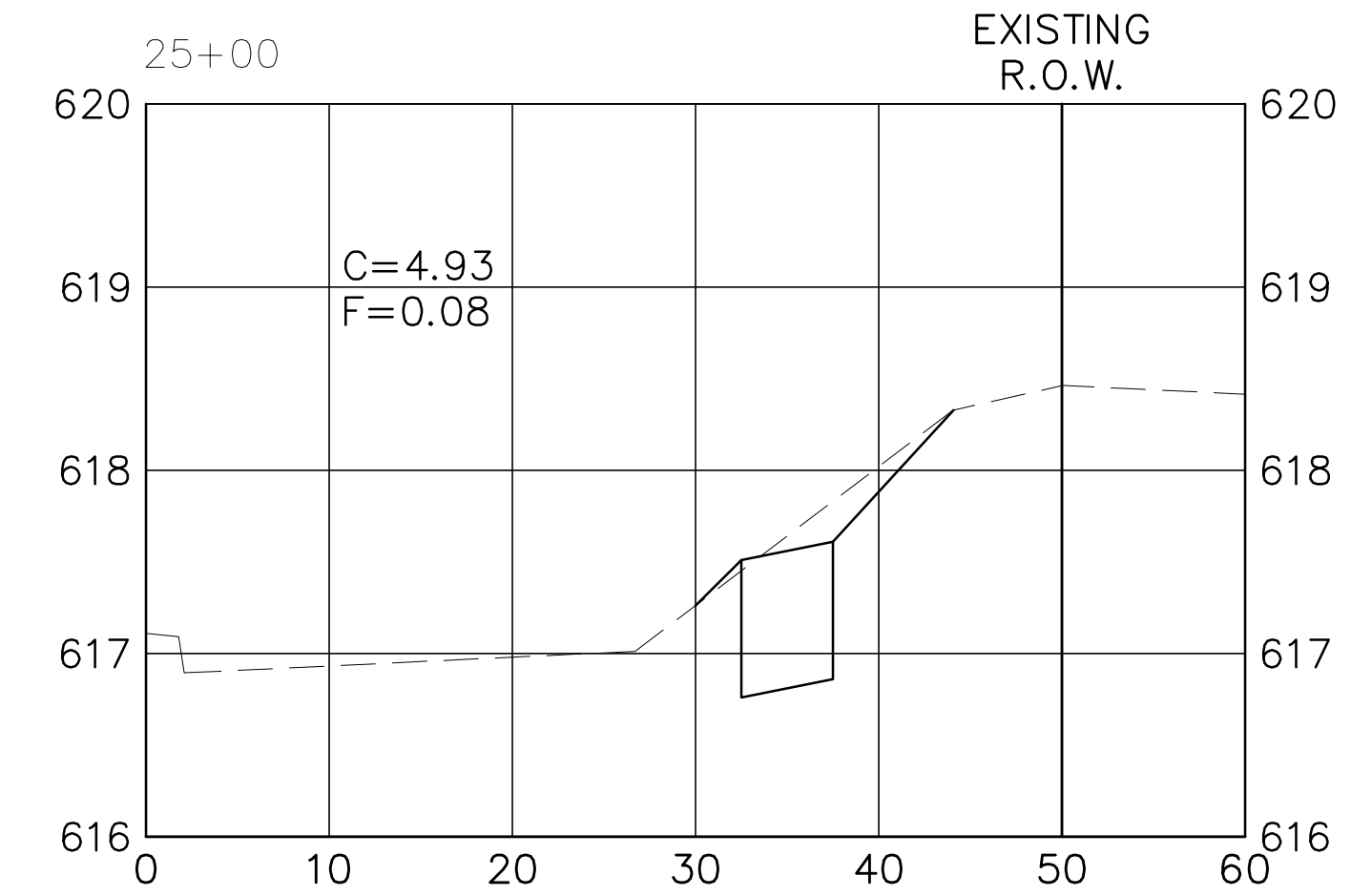
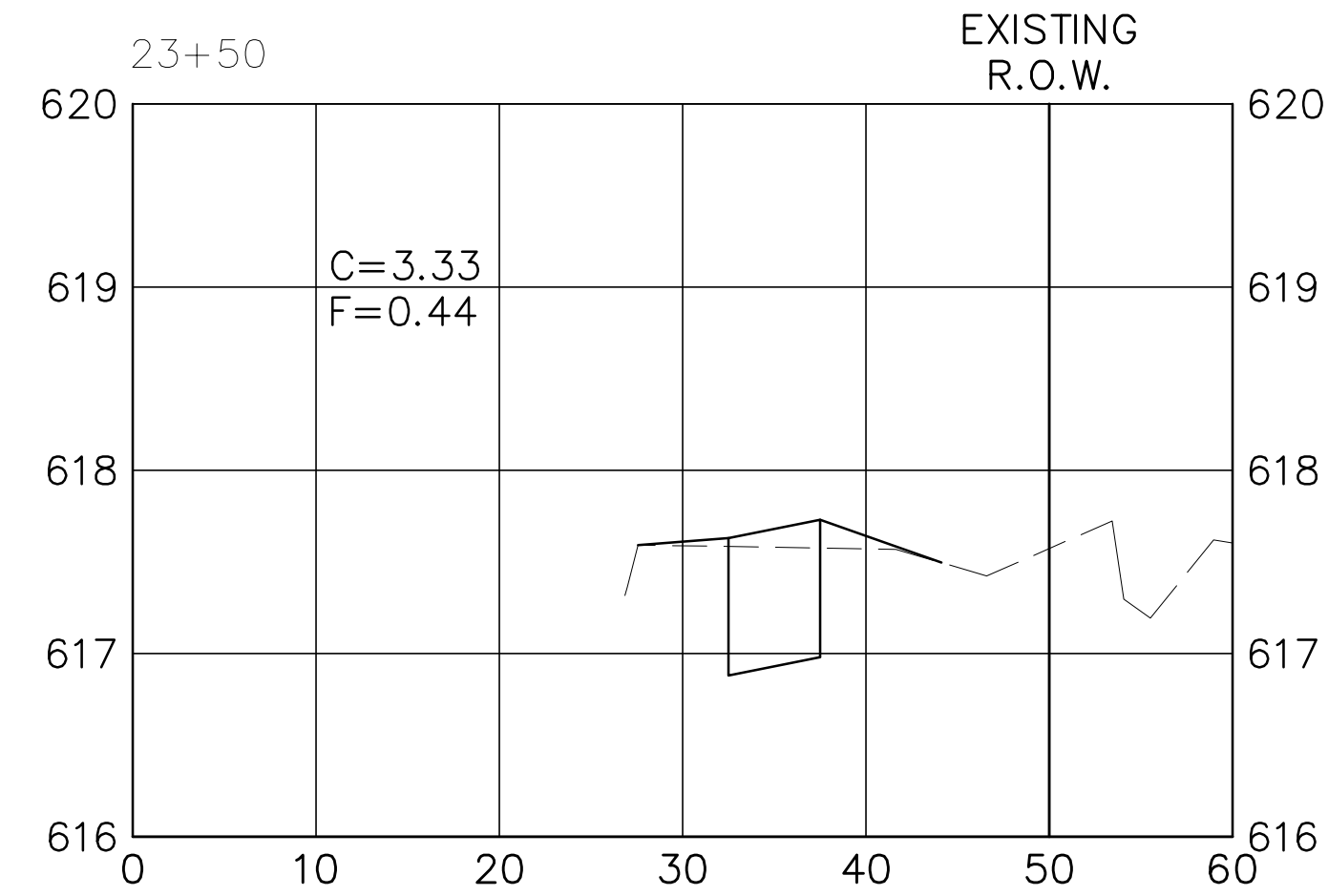
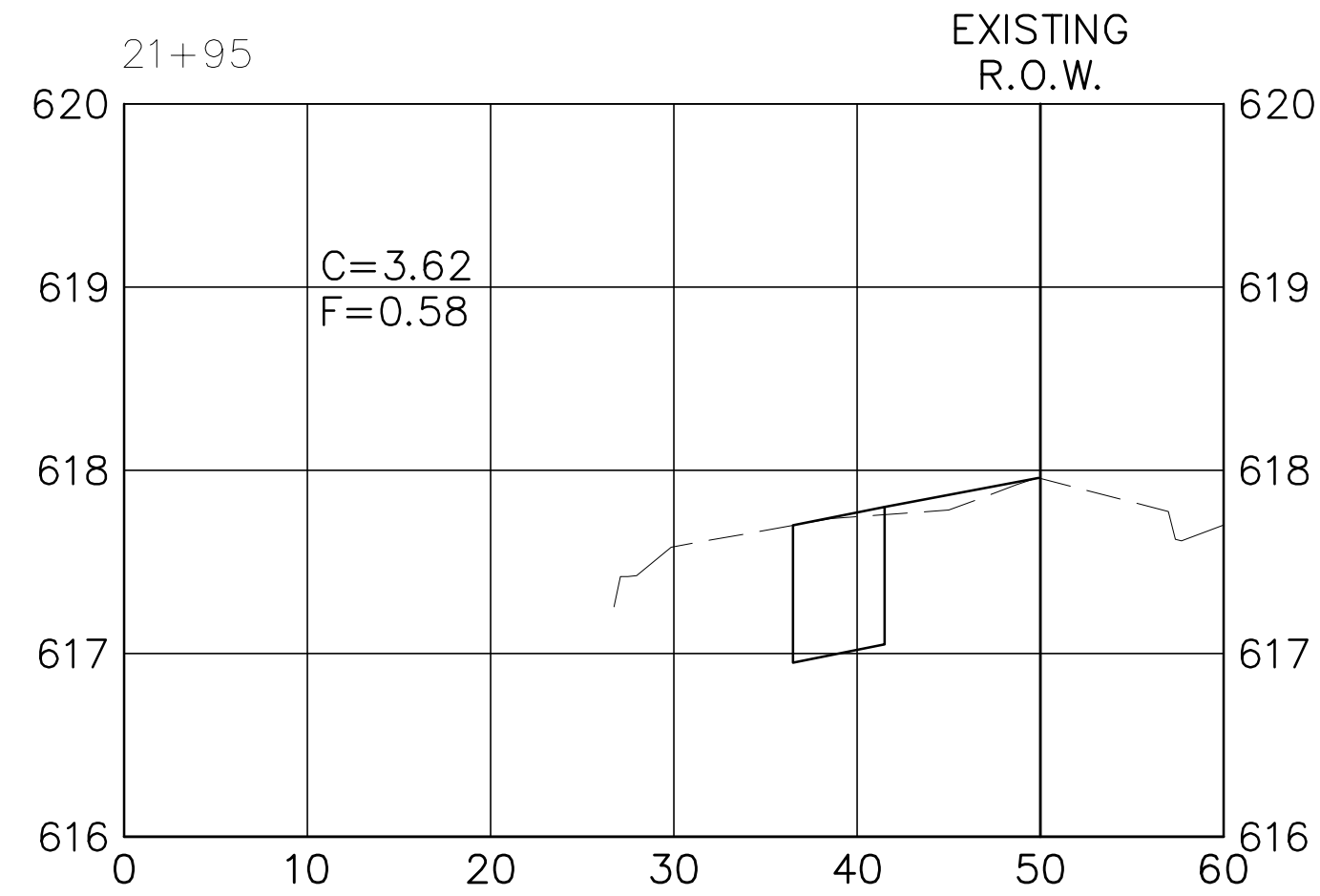
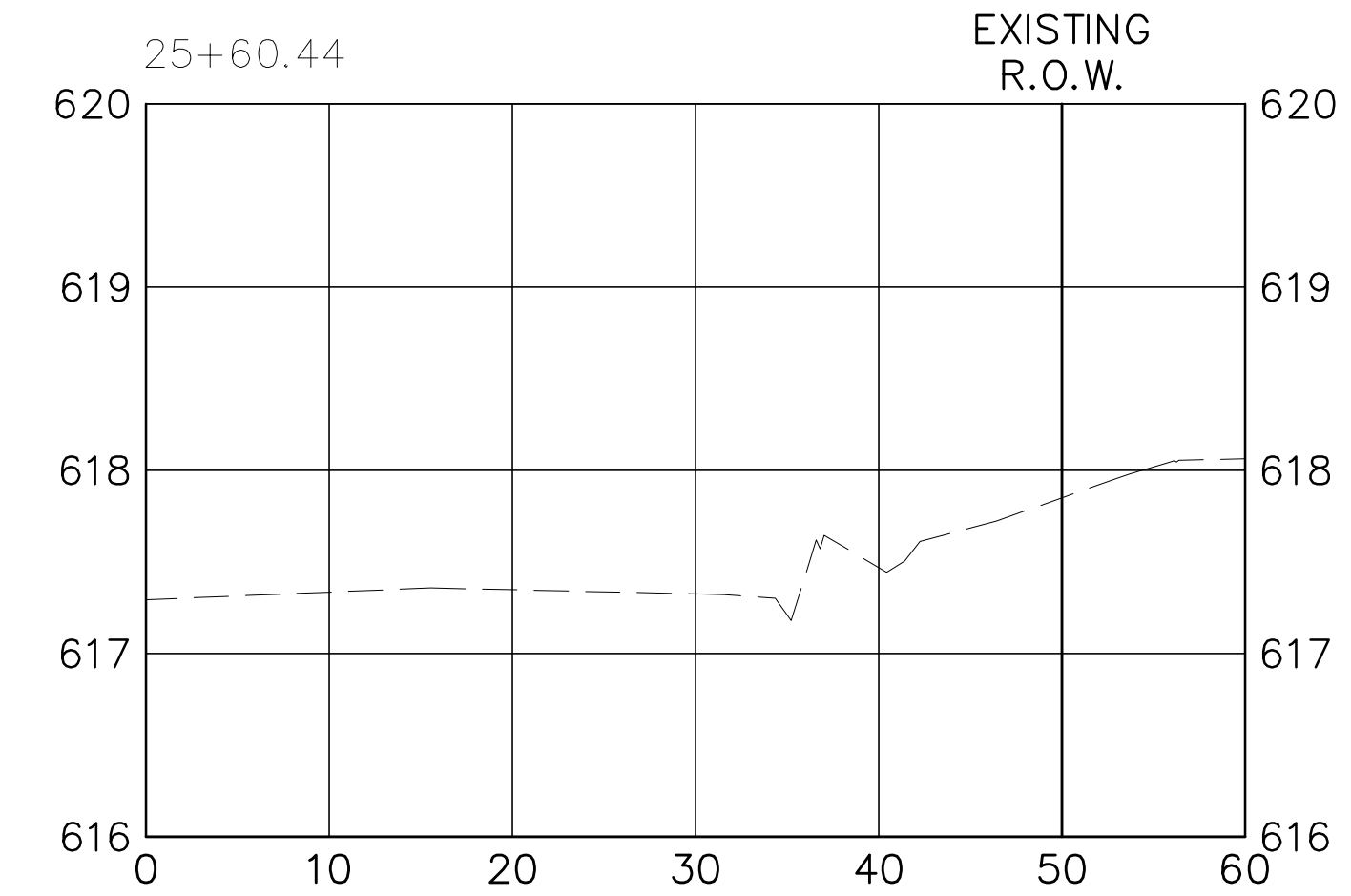
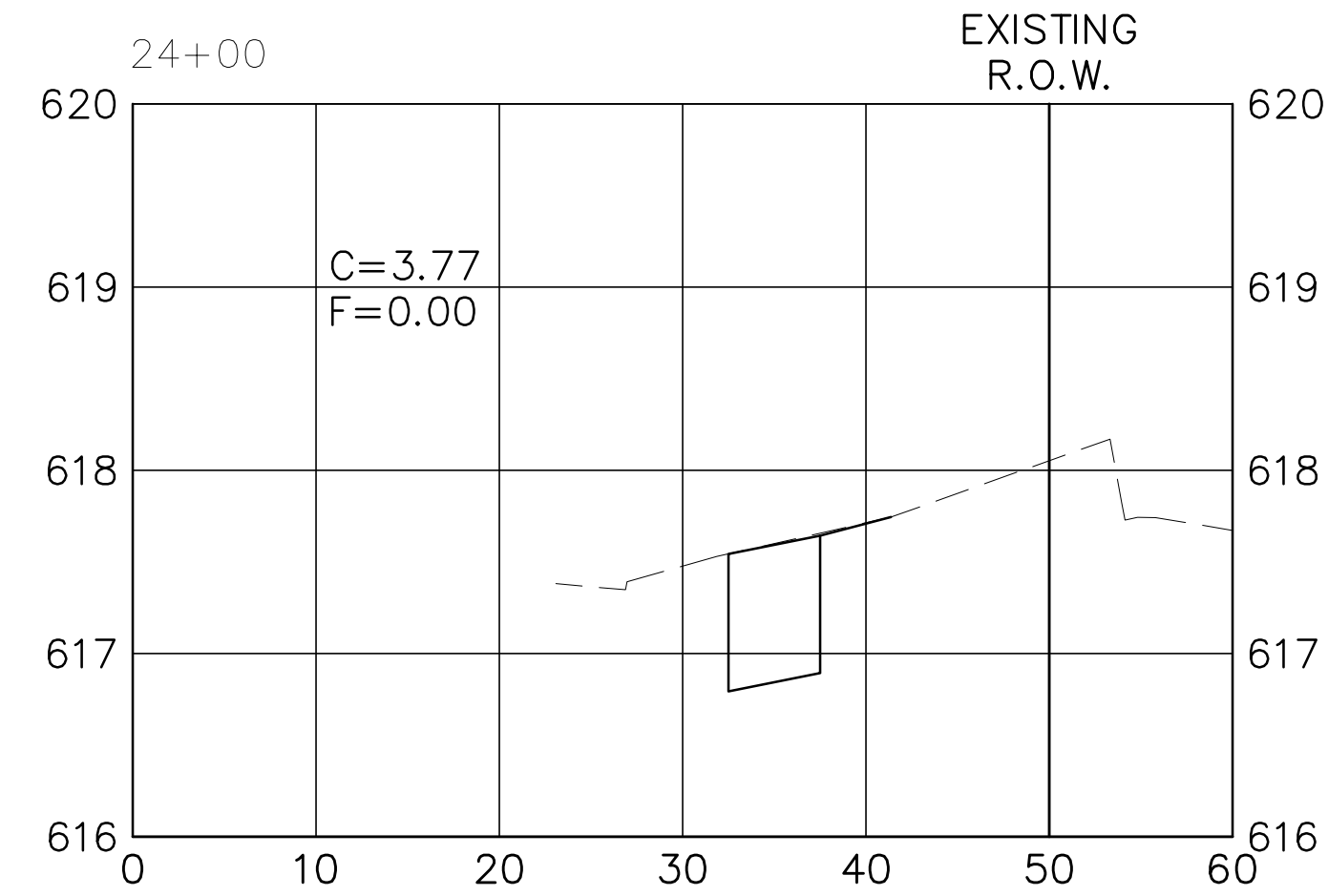
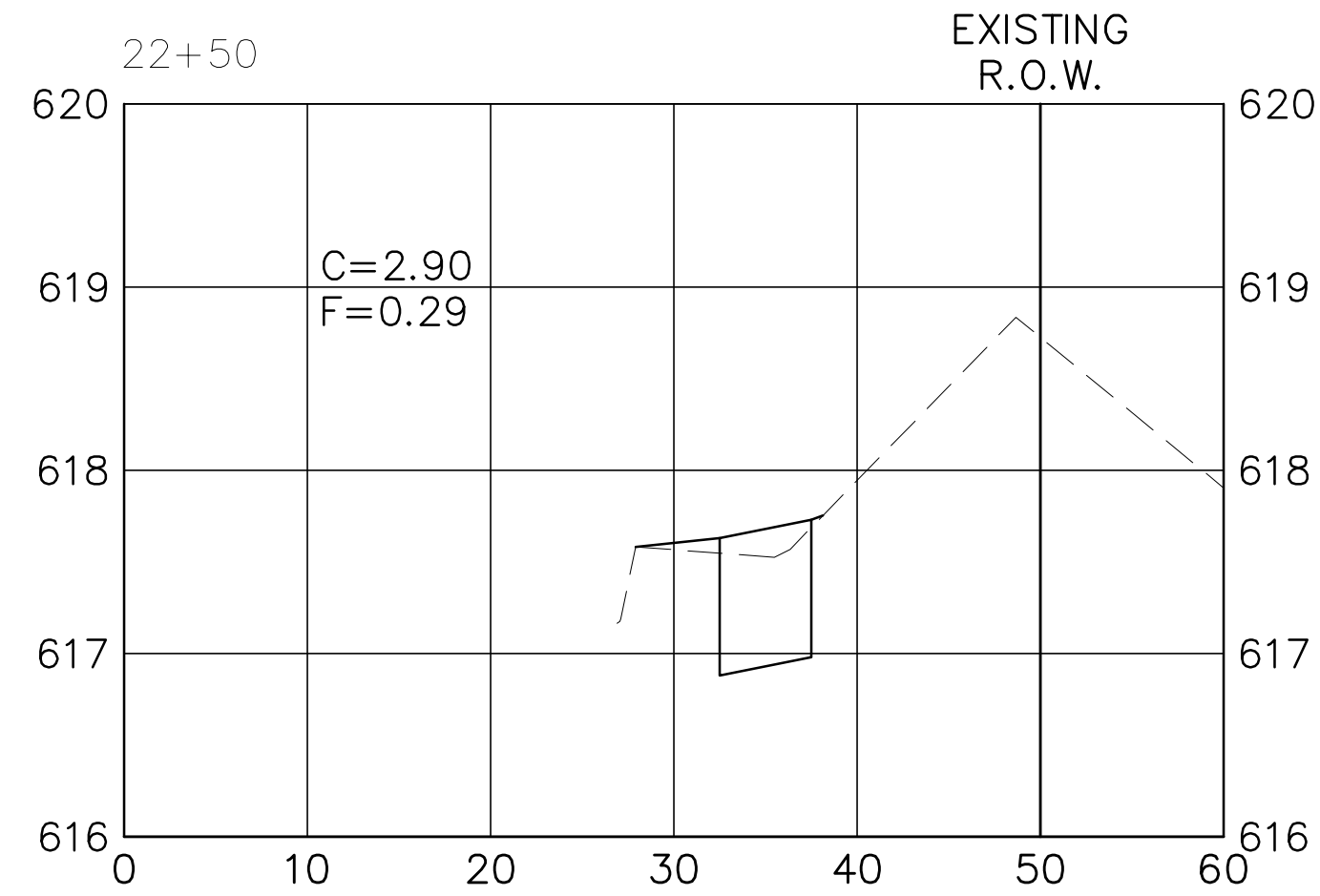
CONTRACT No. 61F35

FED. ROAD DIST. NO.

ILLINOIS

FED. AID PROJECT





STA. 10+00 TO STA. 19+14 (ARCHER ROAD)  
STA. 10+00 TO STA. 25+60 (STATE ROAD)

DRAWN BY: ALS	REVISED: PER IDOT REVIEW
CHECKED BY: SAL	REVISED: _____
DATE: 08/01/2018	REVISED: _____
REVISED: _____	REVISED: _____

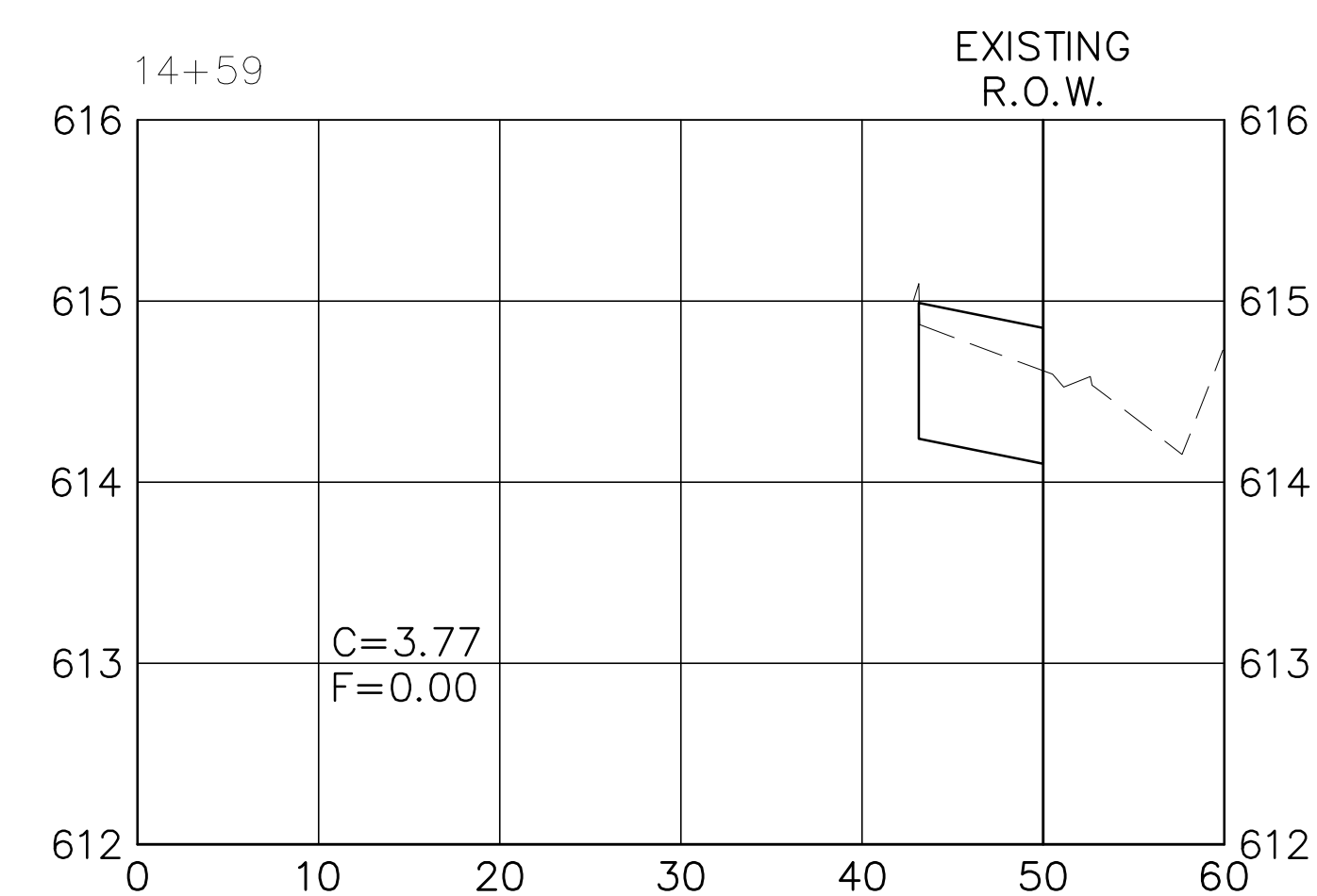
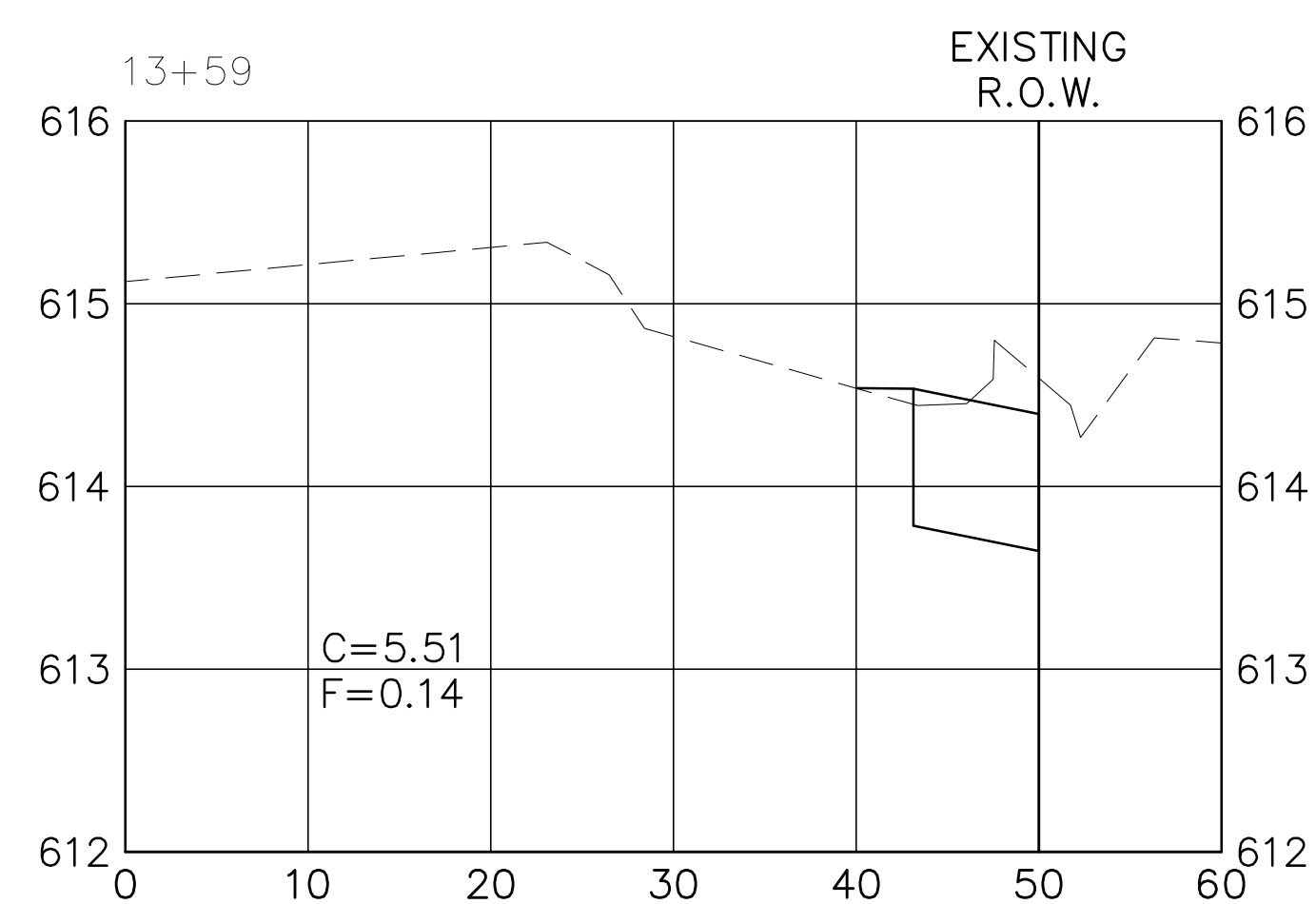
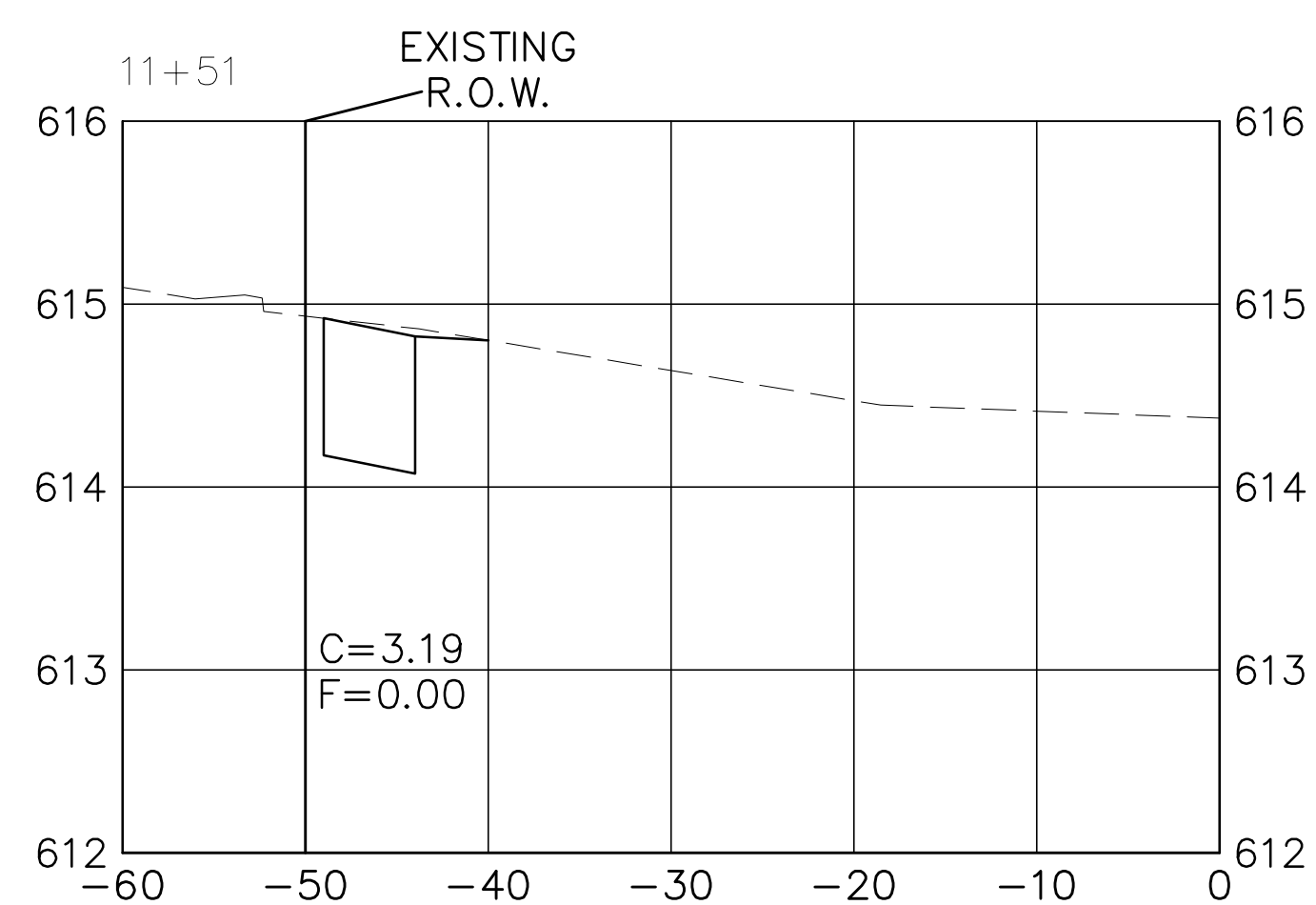
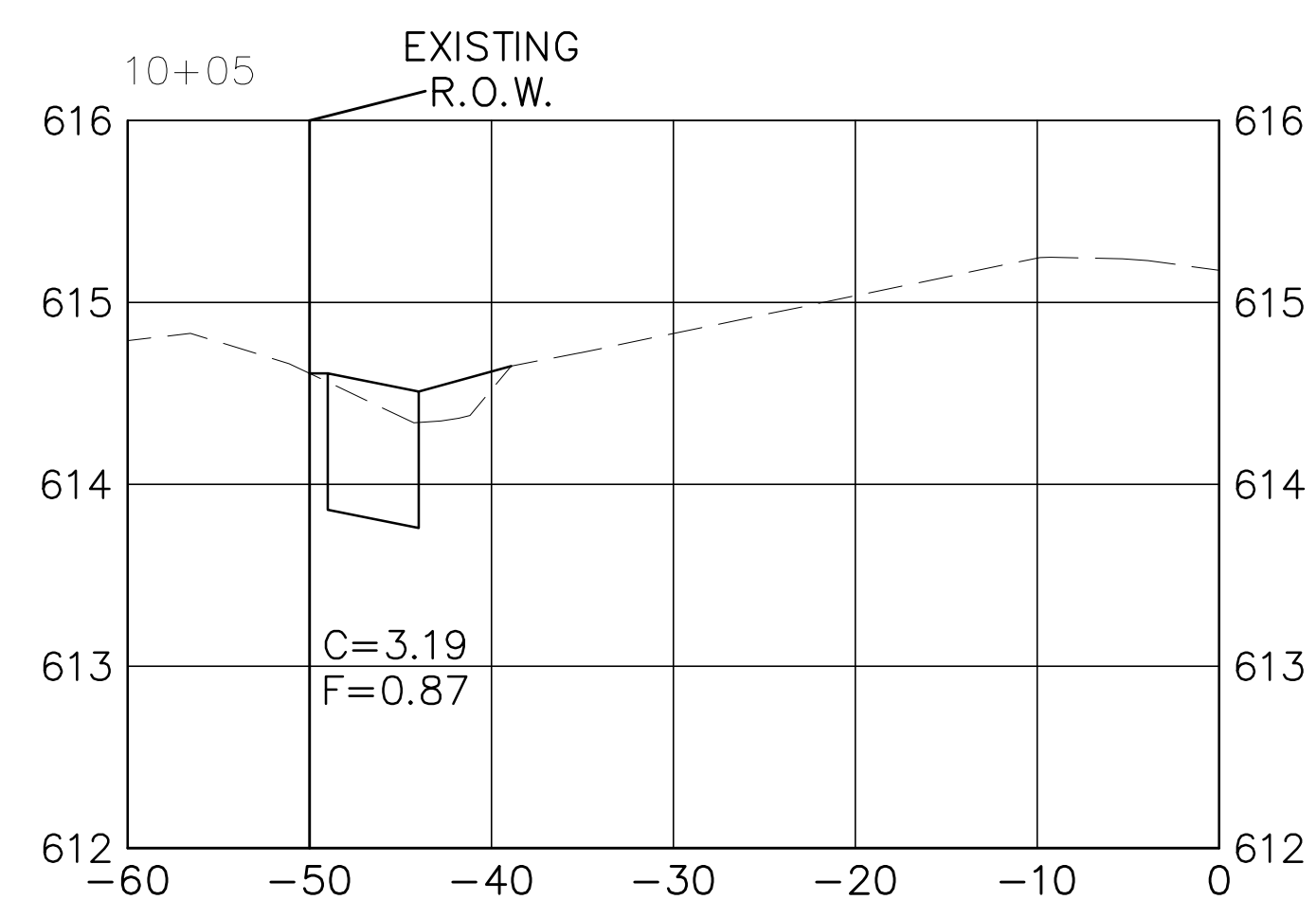
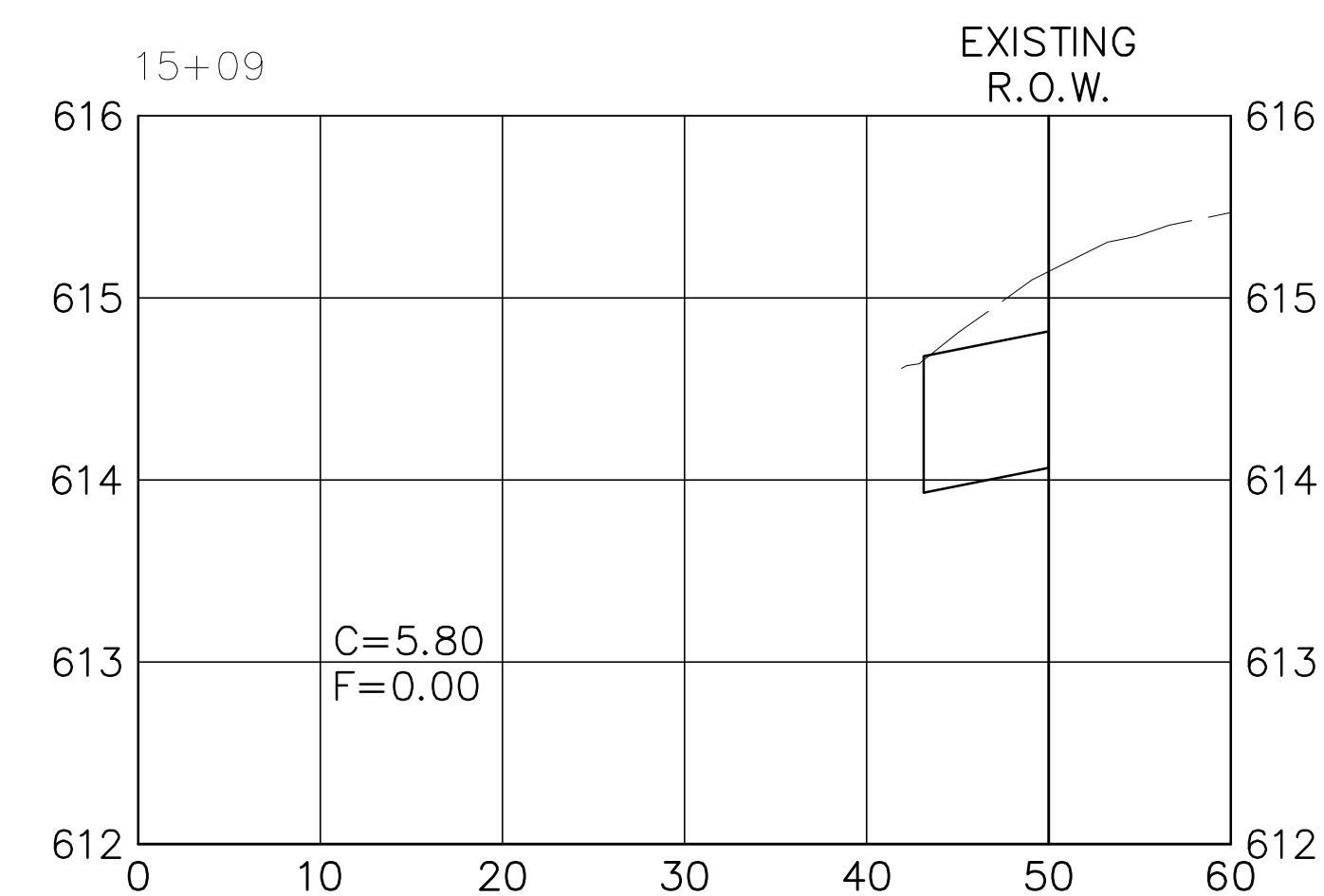
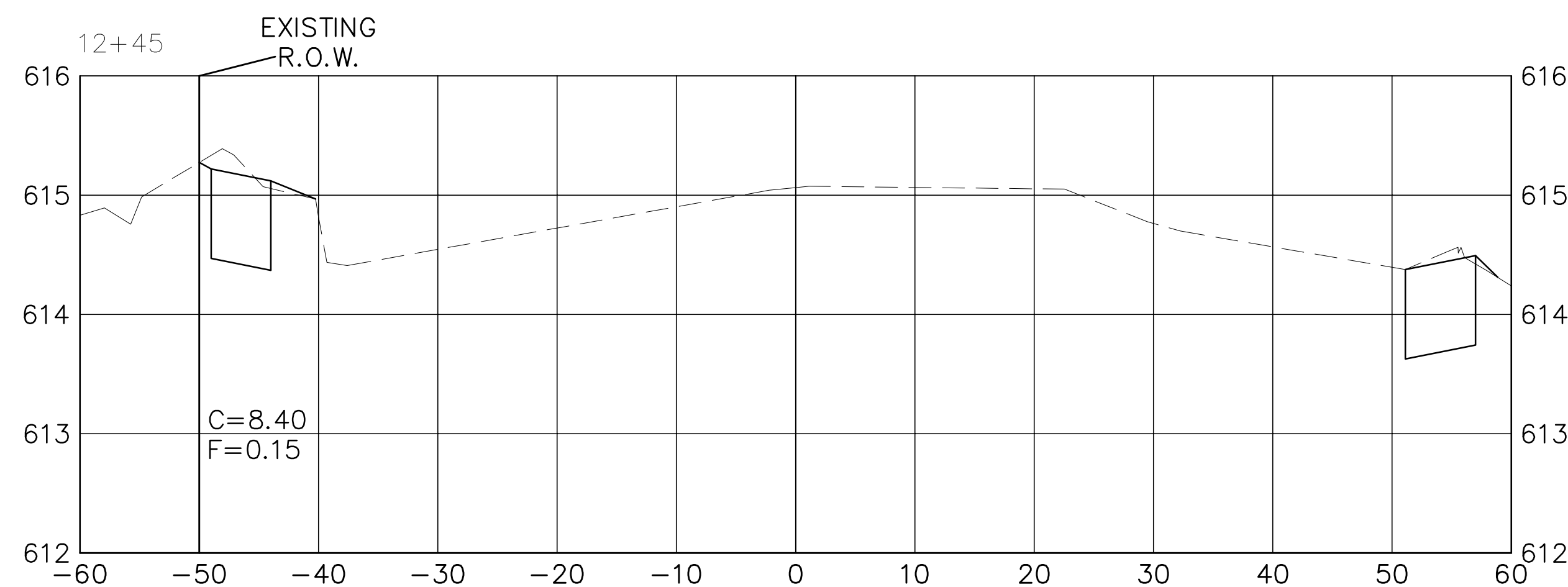
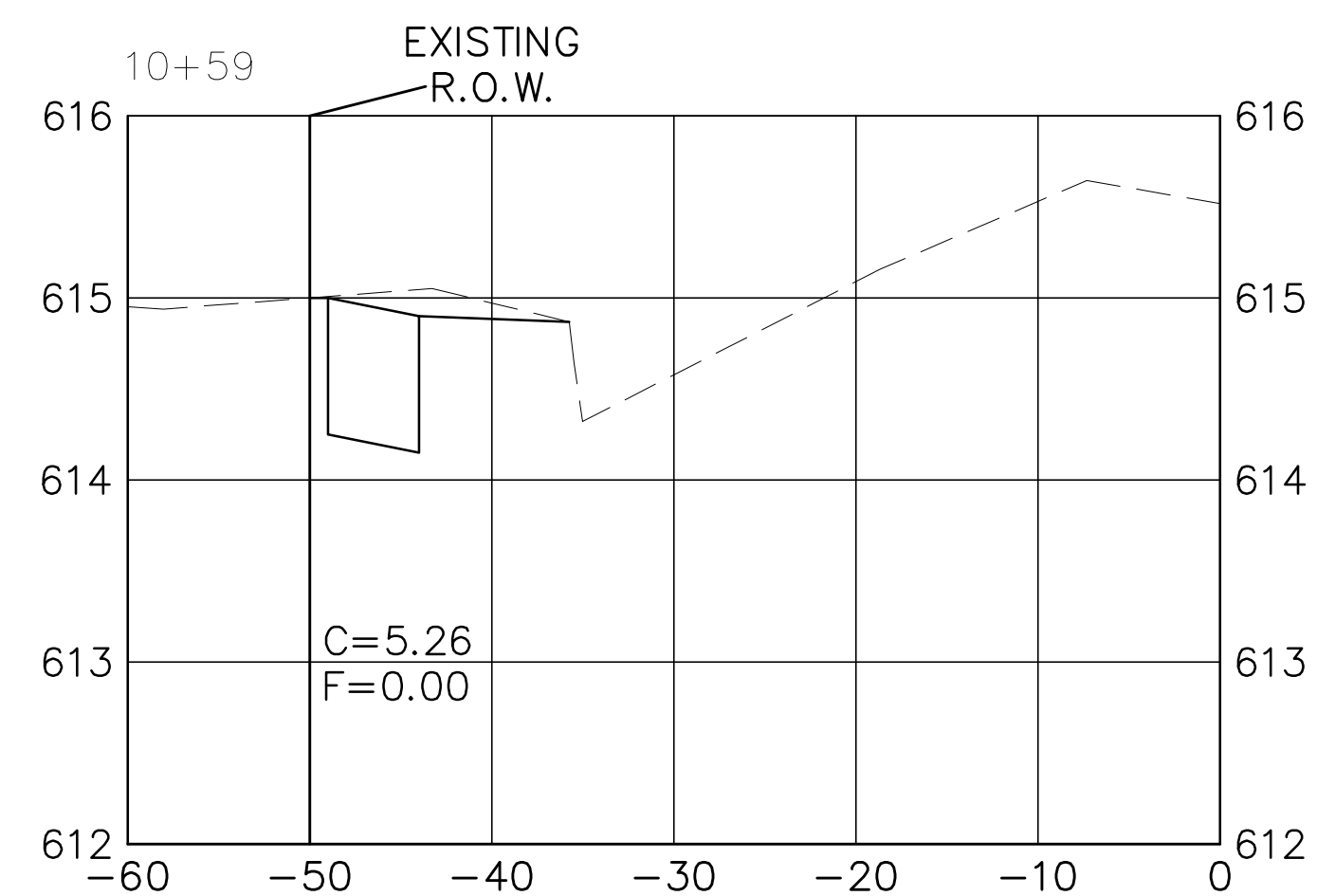
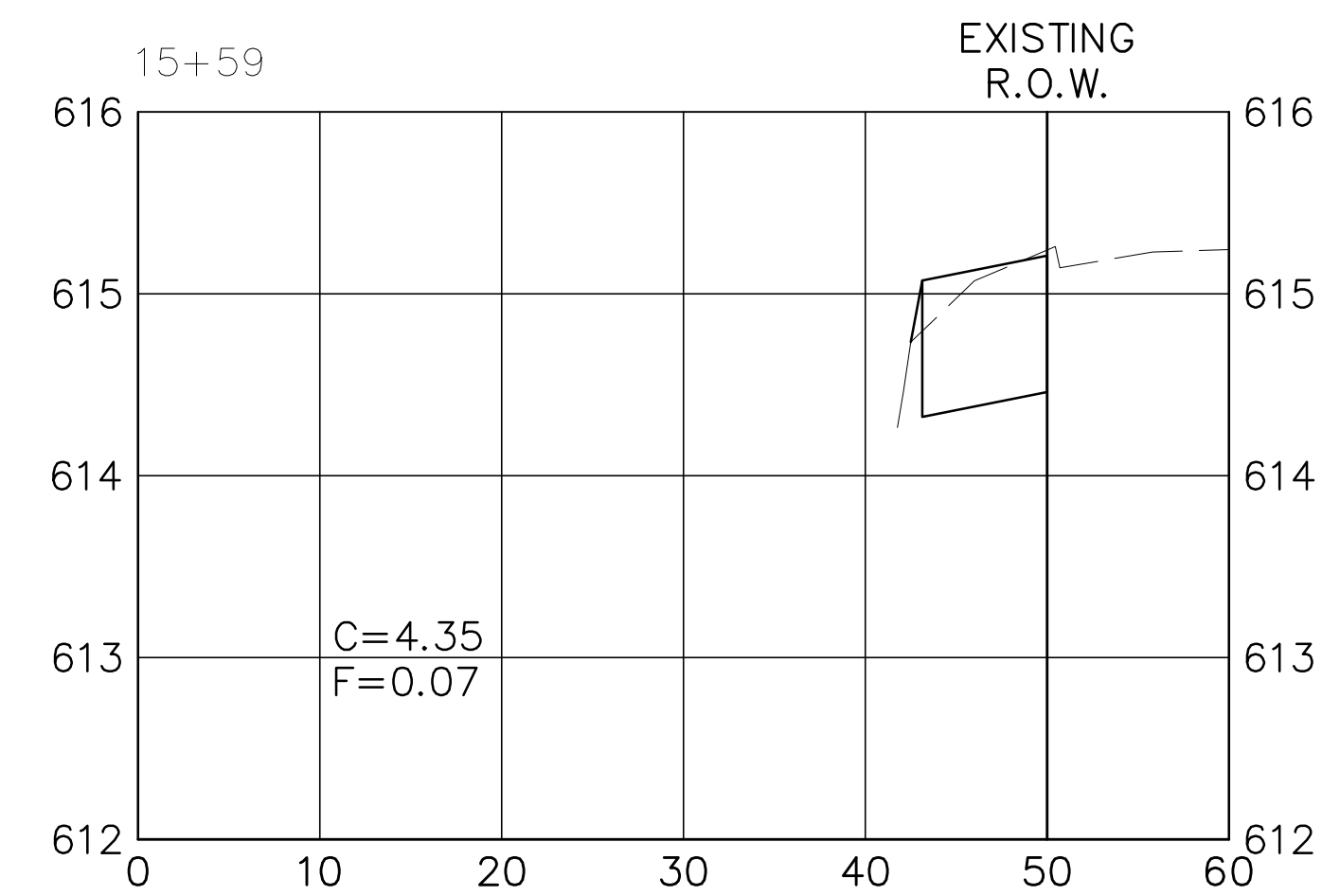
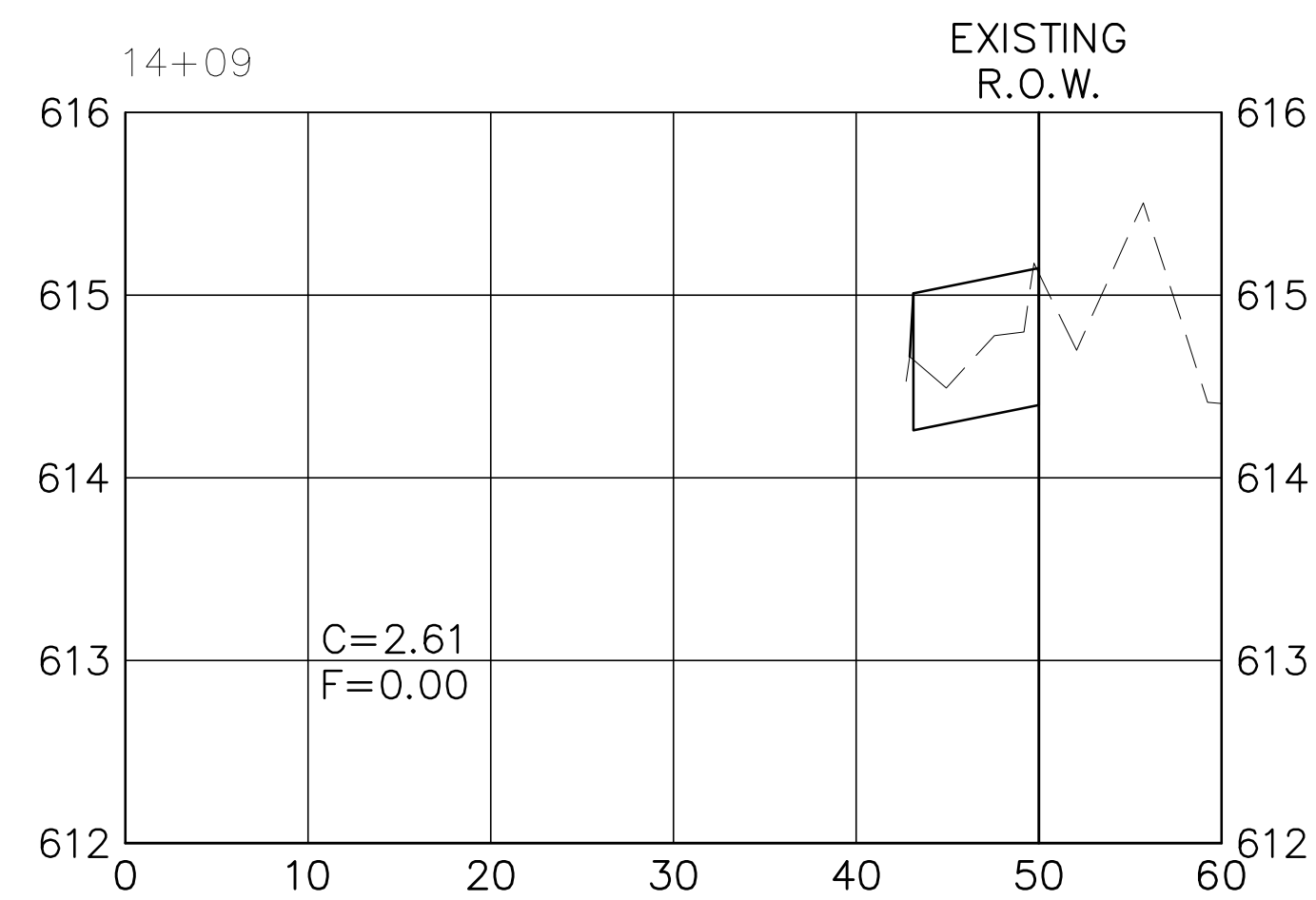
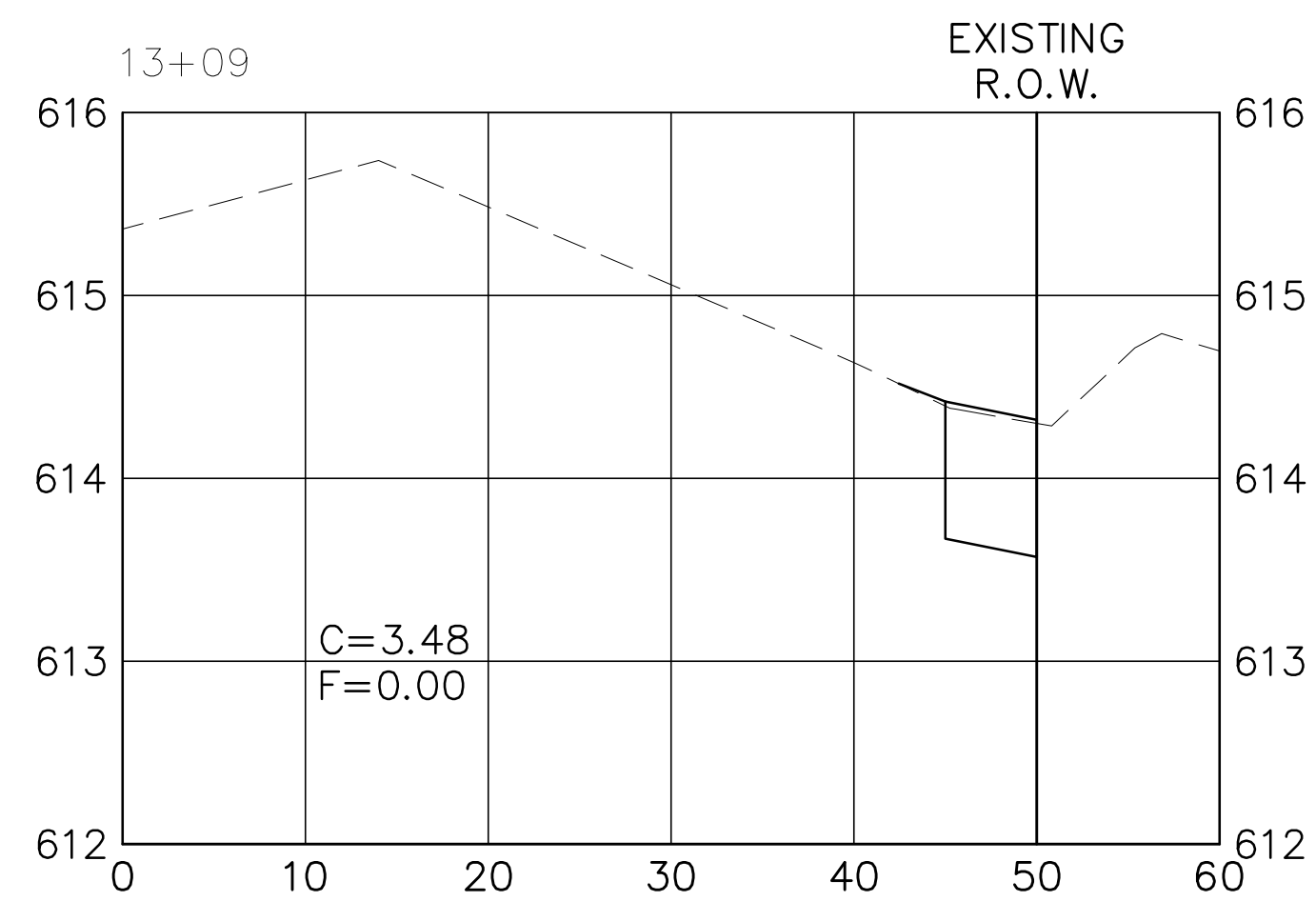
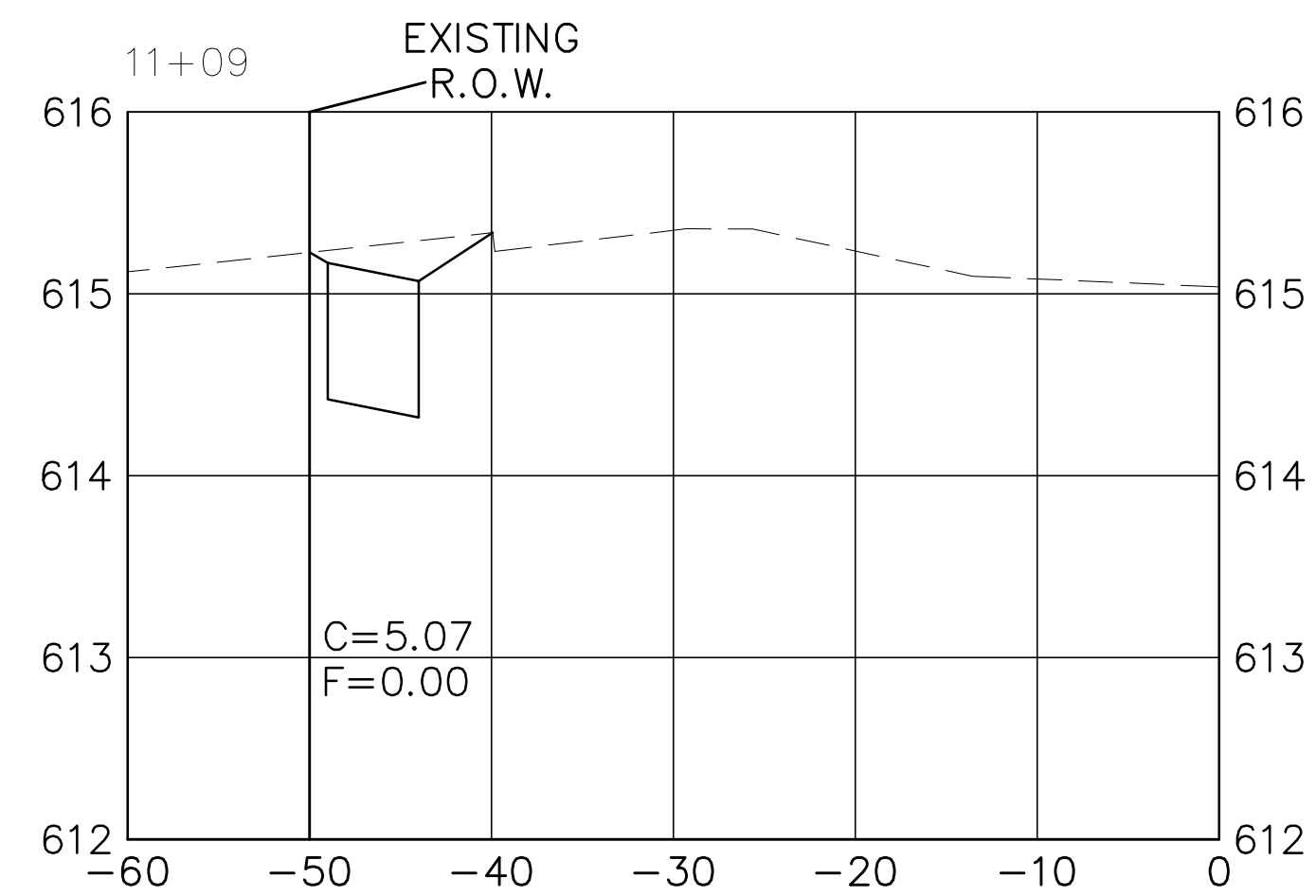
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**STATE ROAD  
CROSS-SECTIONS  
STA 21+34 TO STA 25+60**

SCALE: 1"=10'H; 1"=1'V      SHEET No. 3 OF 3 SHEETS

FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
3565, 3577	12-00035-00-SW	COOK	30	28
CONTRACT No. 61F35				
FED. ROAD DIST. NO.	ILLINOIS	FED. AID PROJECT		

FREE JOB No. 11-517



STA. 10+00 TO STA. 19+14 (ARCHER ROAD)  
STA. 10+00 TO STA. 25+60 (STATE ROAD)

DRAWN BY: ALS

CHECKED BY: SAL

DATE: 08/01/2018

REVISED: \_\_\_\_\_

REVISED: PER IDOT REVIEW

REVISED: \_\_\_\_\_

REVISED: \_\_\_\_\_

REVISED: \_\_\_\_\_

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ARCHER AVENUE  
CROSS-SECTIONS  
STA 10+00 - STA 14+59**

SCALE: 1"=10'H; 1"=1'V

SHEET No. 1 OF 2 SHEETS

FAU  
RTE  
3565, 3577

SECTION  
12-00035-00-SW

COUNTY  
COOK

TOTAL  
SHEETS  
30

SHEET  
NO.  
29

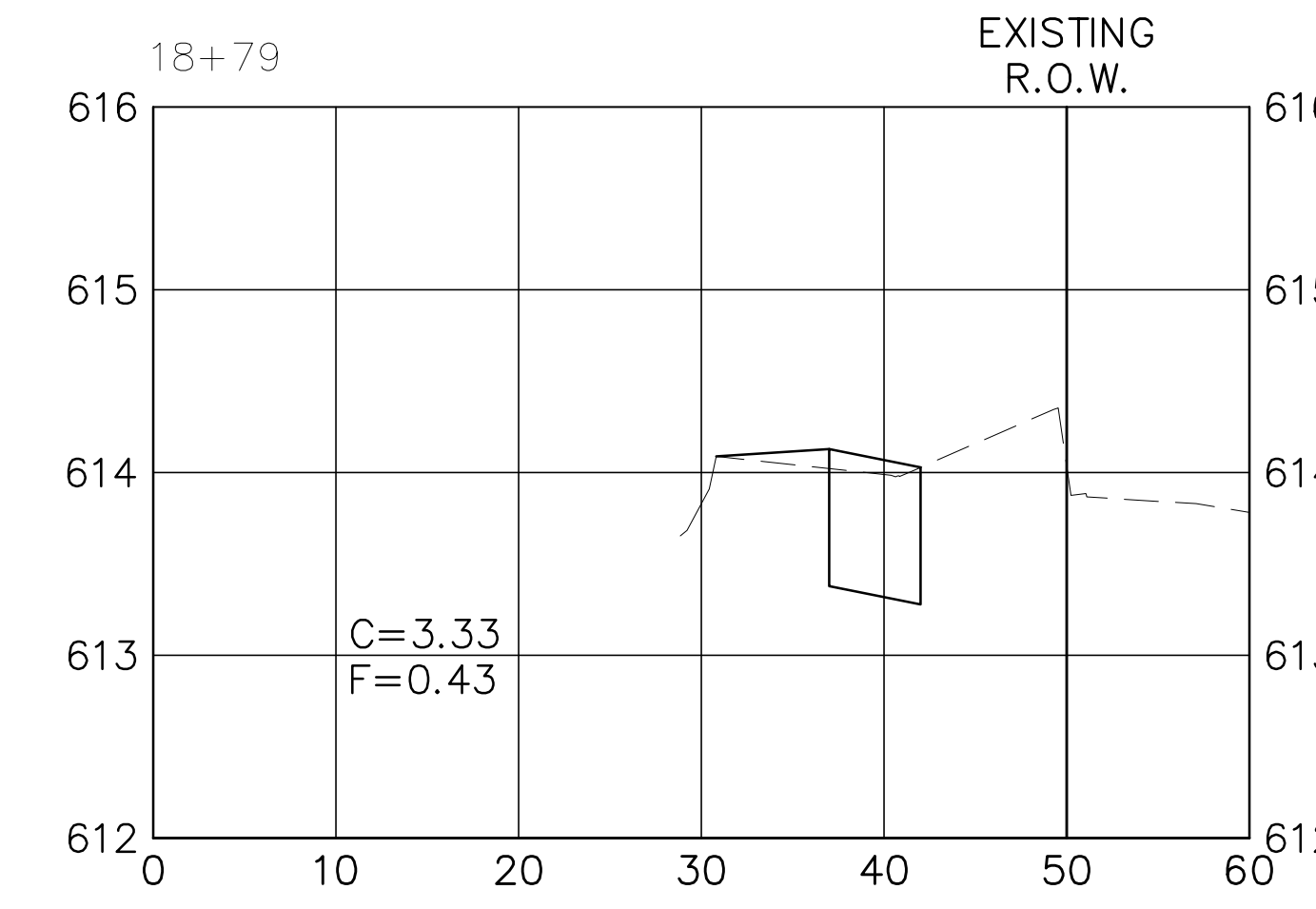
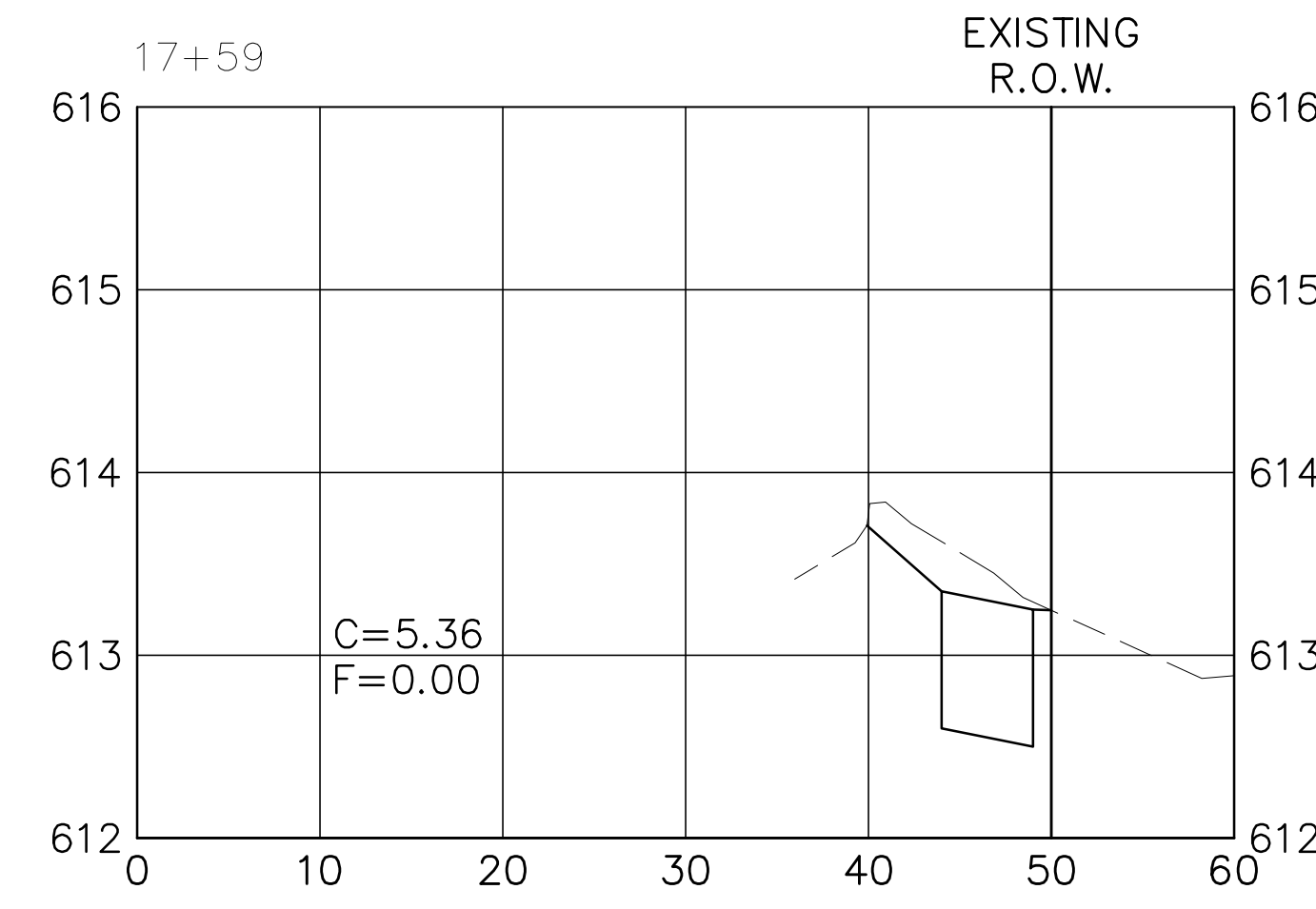
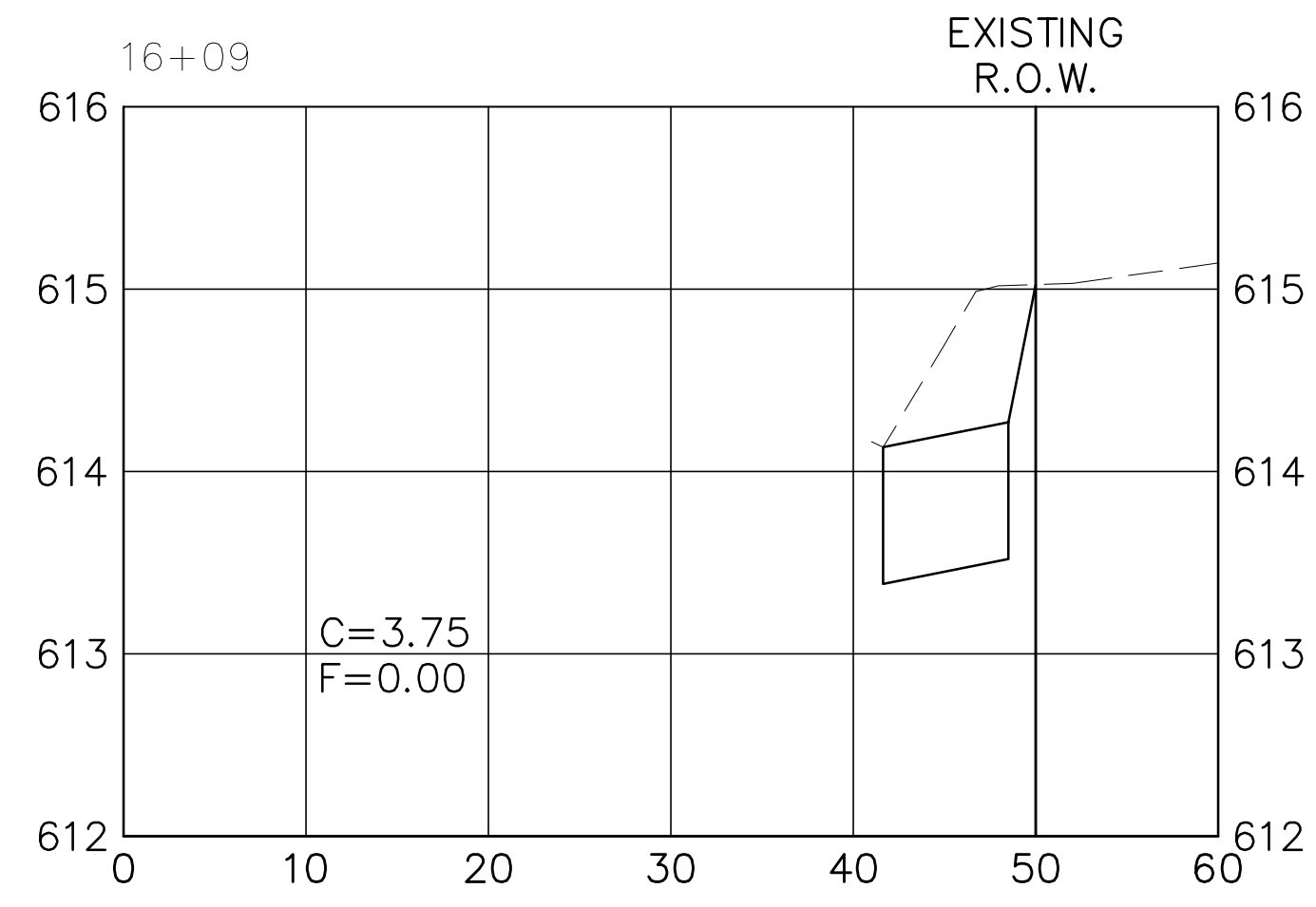
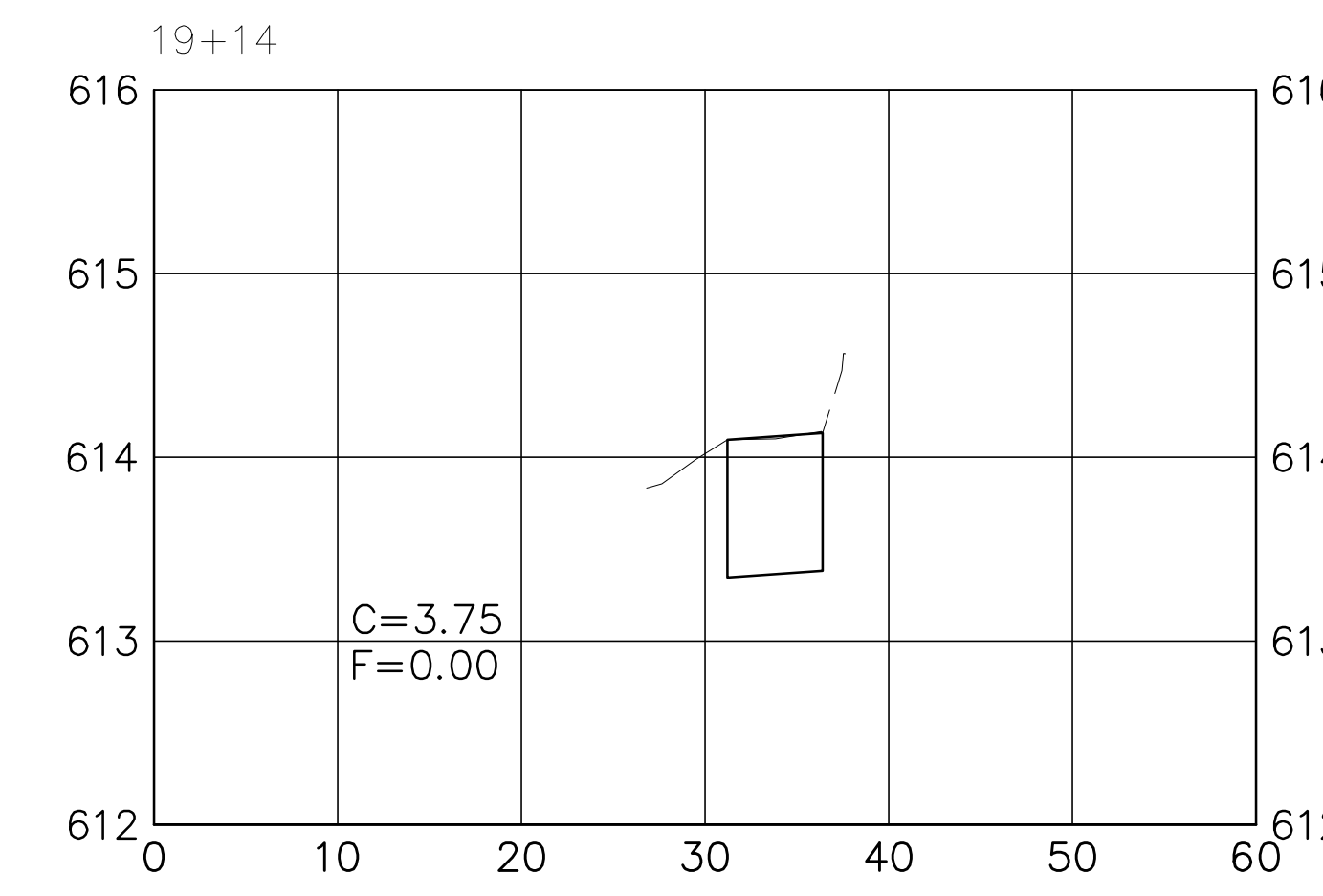
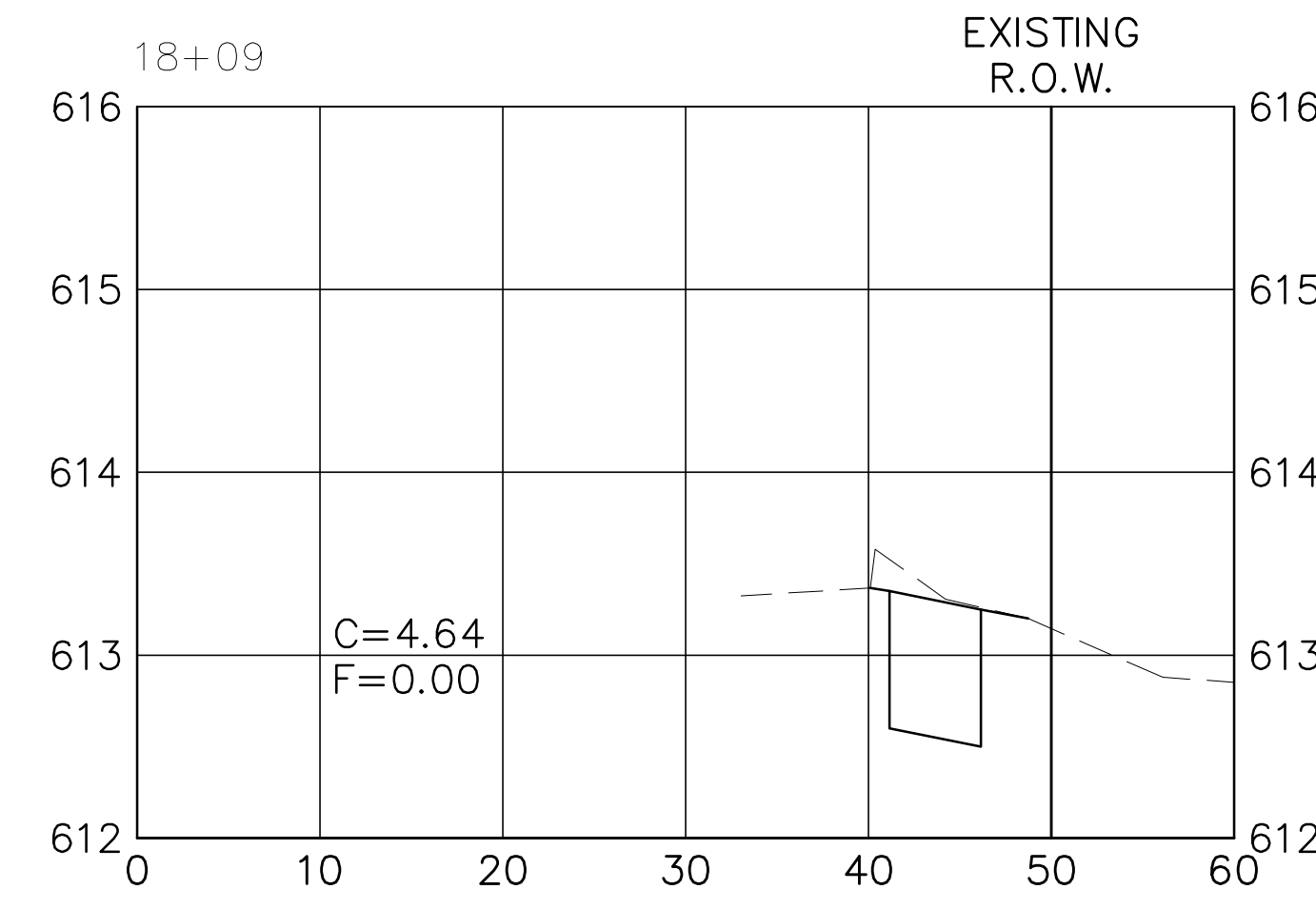
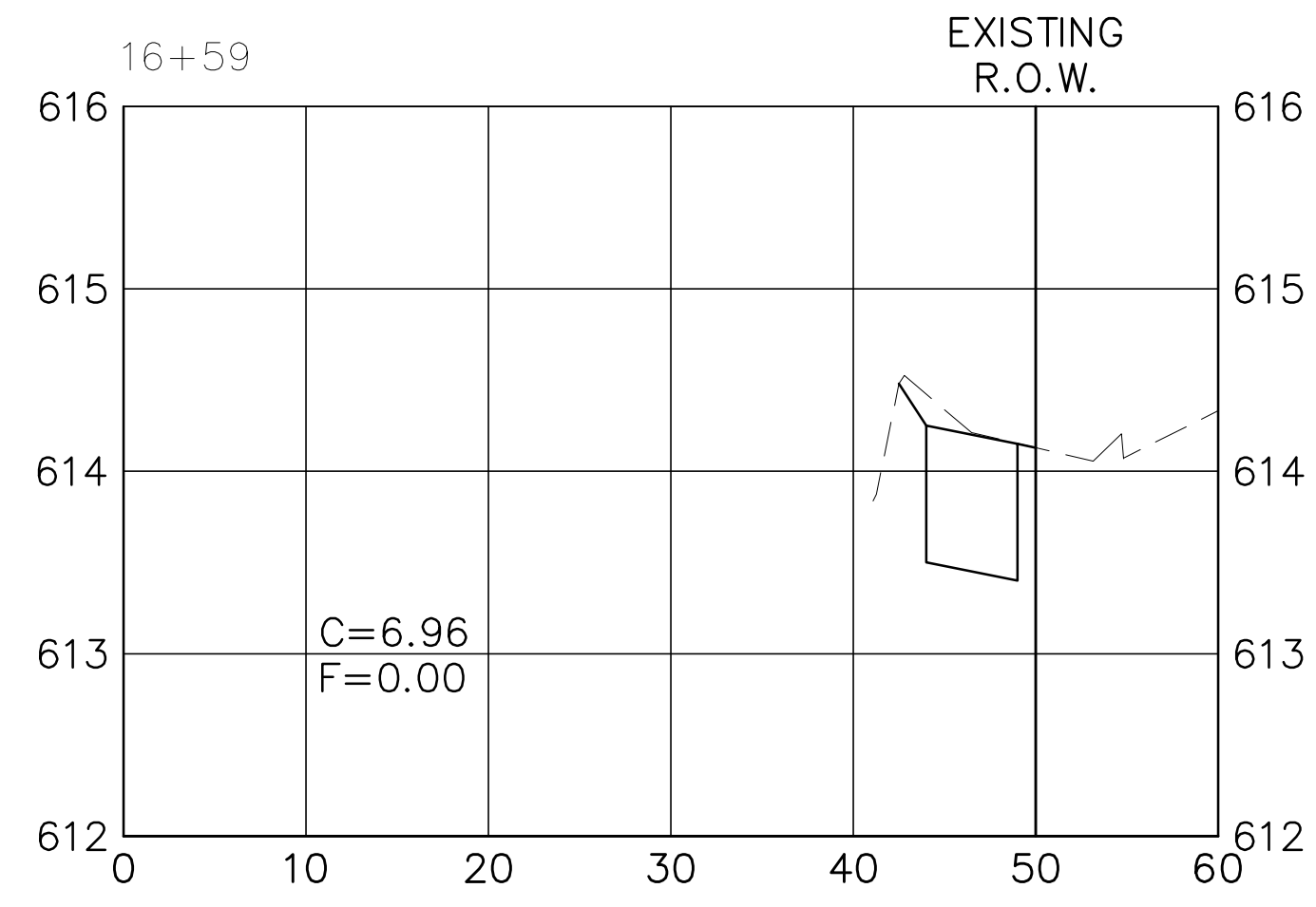
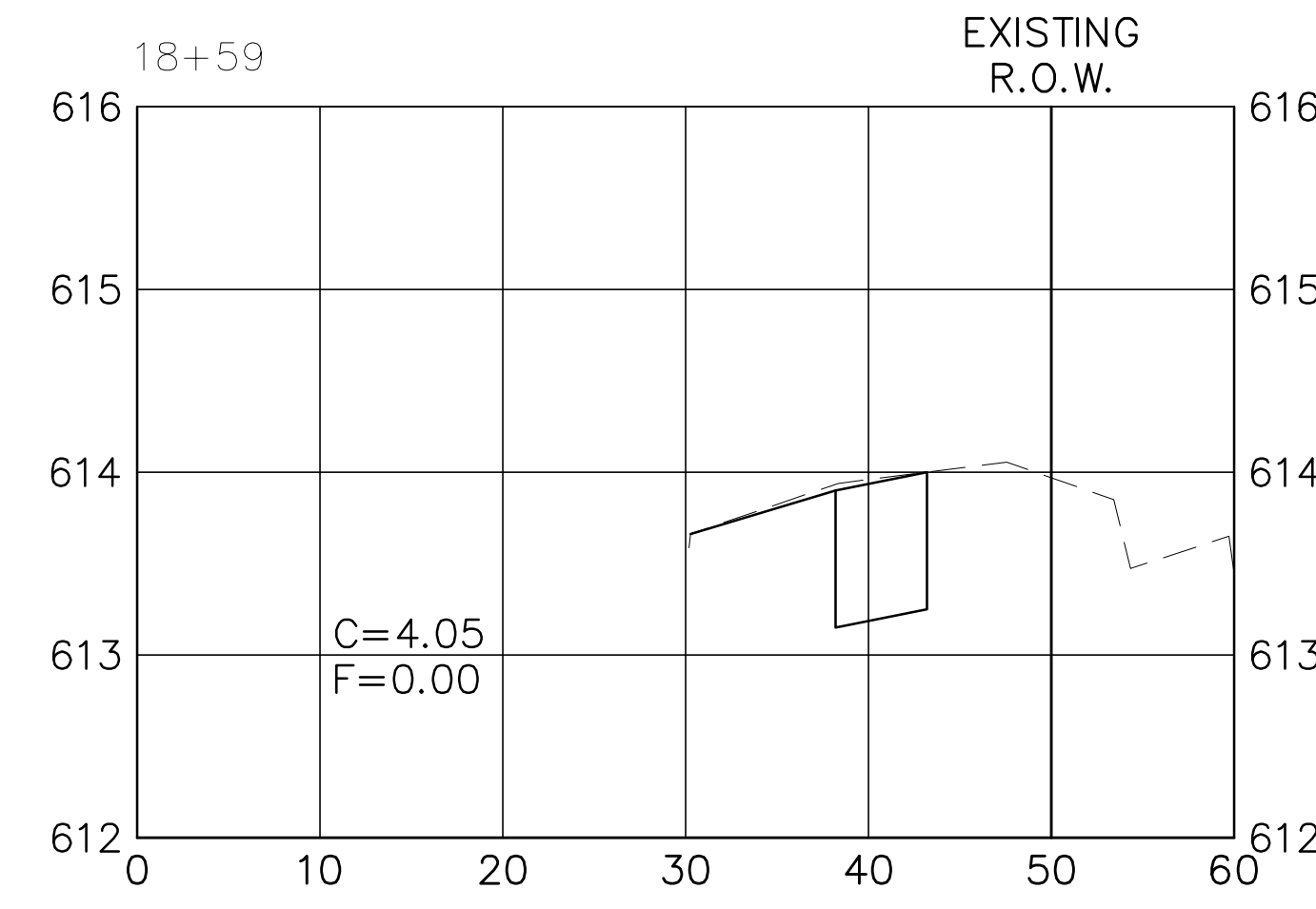
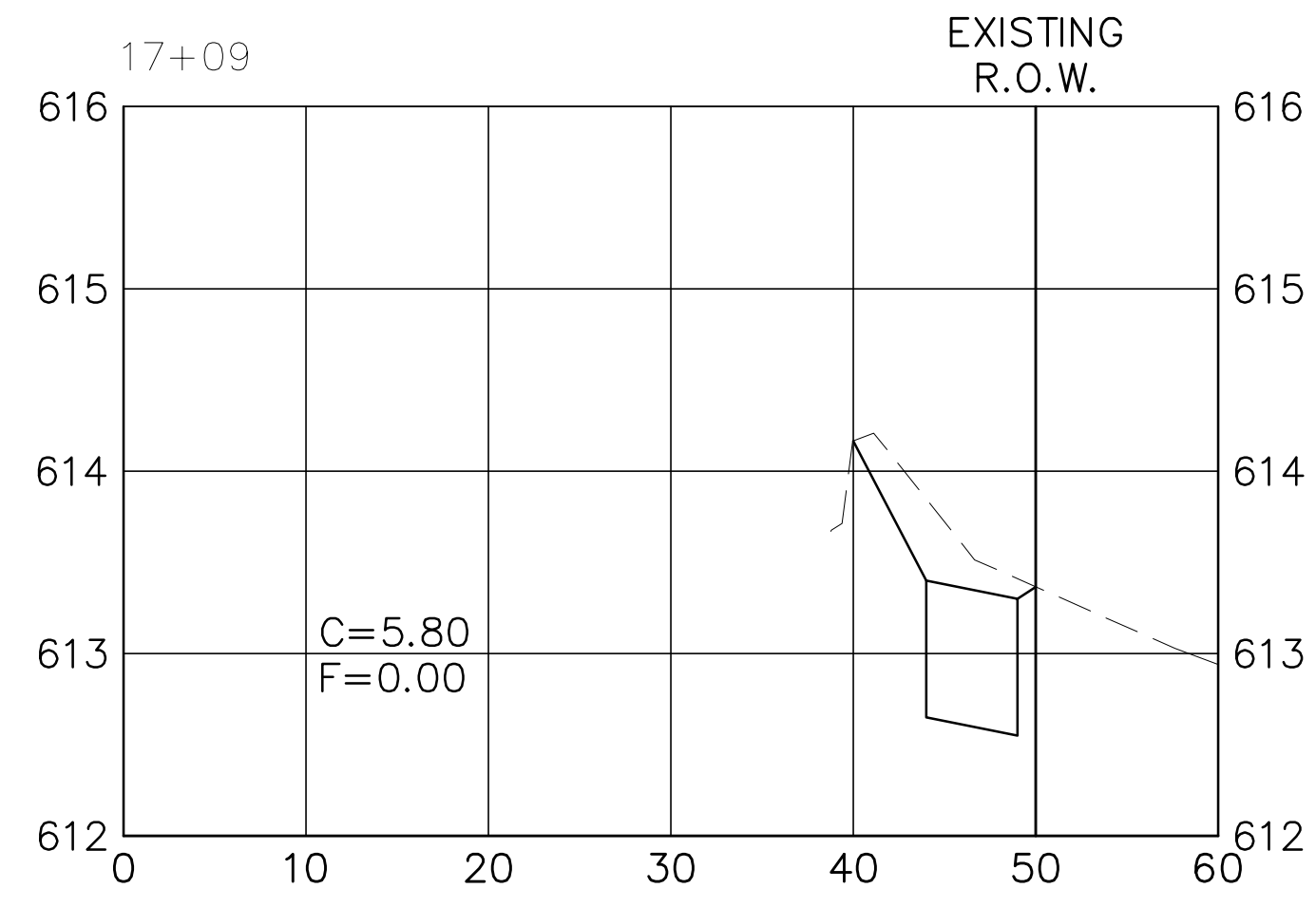
CONTRACT No. 61F35

FED. ROAD DIST. NO.

ILLINOIS

FED. AID PROJECT





STA. 10+00 TO STA. 19+14 (ARCHER ROAD) STA. 10+00 TO STA. 25+60 (STATE ROAD)	DRAWN BY: ALS	REVISED: PER IDOT REVIEW	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>ARCHER AVENUE CROSS-SECTIONS STA 16+59 - STA 18+79</b>	FAU RTE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	CHECKED BY: SAL	REVISED: _____			3565, 3577	12-00035-00-SW	COOK	30	30
DATE: 08/01/2018	REVISED: _____	CONTRACT No. 61F35							
REVISED: _____	REVISED: _____	FED. ROAD DIST. NO.		ILLINOIS	FED. AID PROJECT				
				SCALE: 1"=10'H; 1"=1'V	SHEET No. 2 OF 2 SHEETS				

FILE JOB No. 11-517