

FOR INDEX OF SHEETS, SEE SHEET 2
FOR LIST OF STATE STANDARDS, SEE SHEET 2

01-18-2019 LETTING ITEM 121

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION PLANS FOR PROPOSED FEDERAL-AID HIGHWAY

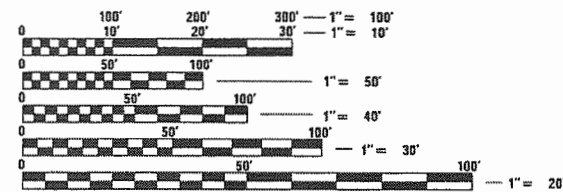
**FAU 2678A(YORK ROAD)
GRAND AVENUE TO GREEN STREET
RESURFACING
SECTION 18-00098-00-RS
PROJECT JZKB(780)
VILLAGE OF BENSENVILLE
DuPAGE COUNTY
C-91-204-18**

FAU R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2678	18-00098-00-RS	DUPAGE	45	1
		ILLINOIS	CONTRACT NO. 61F38	



TRAFFIC DATA

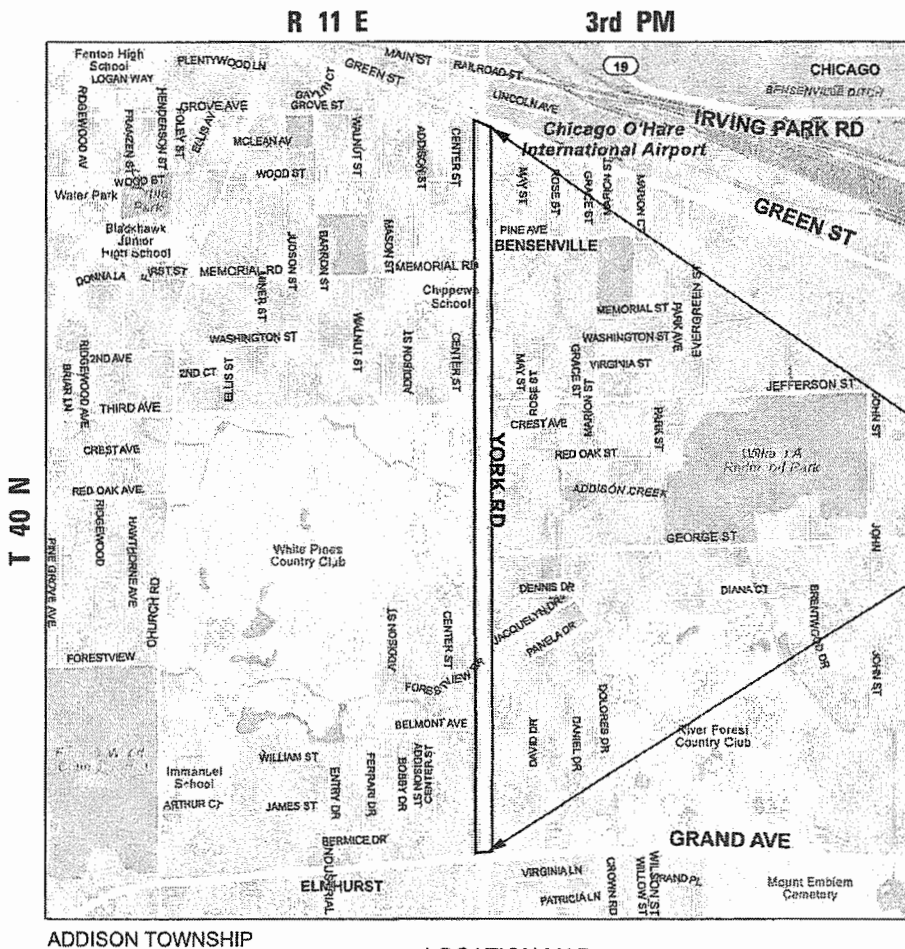
**YORK ROAD (FAU 2678)
MAJOR COLLECTOR
TRAFFIC = 12,600 ADT (2016)
POSTED SPEED = 40 MPH
DESIGN SPEED = 40 MPH**



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

CALL JULIE 811
WITH THE FOLLOWING:
COUNTY DUPAGE
CITY-TOWNSHIP BENSENVILLE/ADDISON
SECTION & RANGE T40N-R11E, Sec E23
48 HOURS BEFORE YOU DIG.
EXCLUDING SAT., SUN., & HOLIDAYS

MEADE ELECTRIC CO. DISTRICT ONE ELECTRICAL MAINTENANCE CONTRACTOR
LOCATES IDOT ELECTRICAL EQUIPMENT AND UNDERGROUND CABLES 773-287-7672.



**END IMPROVEMENTS
GREEN STREET
STA 106+00**

**BEGIN IMPROVEMENTS
GRAND AVE
STA 22+00**

MARTIN L. MICHALOWICZ
ILLINOIS REGISTRATION No. 052-059502
EXPIRATION DATE: 11/30/19

CHRISTOPHER B. BURKE ENGINEERING, LTD.
9575 W. Higgins Road, Suite 600
Rosemont, Illinois 60018
(847) 823-0500
PROFESSIONAL DESIGN FIRM NO. 184-001175
EXPIRATION DATE: 04/30/19

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

APPROVED
October 8, 2018
VILLAGE OF BENSENVILLE
VILLAGE ENGINEER

PASSED **NOVEMBER 8, 2018**

DISTRICT ENGINEER OF LOCAL ROADS AND STREETS

RELEASED FOR BID
BASED ON LIMITED
REVIEW **NOVEMBER 9, 2018**

REGIONAL ENGINEER

**PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS**

FEDERAL AID ENGINEER: CARMEN E. RAMOS, P.E., SCHAUMBURG, IL

CONTRACT NO. 61F38

LOCATION MAP
GROSS LENGTH = 8,443 FT. = 1.60 MILE
NET LENGTH = 8,443 FT. = 1.60 MILE

INDEX OF SHEETS

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13	RED OAK STREET DETAIL
14-16	SIDEWALK RAMP DETAILS
17-34	TRAFFIC SIGNAL PLANS
35-45	VILLAGE & DISTRICT 1 DETAILS

HIGHWAY STANDARDS

000001-07	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
280001-07	TEMPORARY EROSION CONTROL SYSTEMS
424001-11	PERPENDICULAR CURB RAMPS
424016-05	MID-BLOCK CURB RAMPS FOR SIDEWALKS
424021-05	DEPRESSED CORNER FOR SIDEWALKS
424026-03	ENTRANCE / ALLEY PEDESTRIAN CROSSINGS
604001-04	FRAME AND LIDS TYPE 1
604051-04	FRAME AND GRATE TYPE 11
606001-07	CONC. CURB TYPE B AND COMB. CONC CURB AND GUTTER
630001-12	STEEL PLATE BEAM GUARDRAIL
631011-10	STRONG POST GUARDRAIL ATTACHED TO CULVERT
701101-05	TRAFFIC BARRIER TERMINAL, TYPE 2
701101-05	OFF-RD OPERATIONS, MULTILANE, 15' TO 24" FROM PAVEMENT EDGE
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS <= 40 MPH
701602-10	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL TURN LANE
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701611-01	URBAN HALF ROAD CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES
720001-01	SIGN PANEL MOUNTING DETAILS
720006-04	SIGN PANEL ERECTION DETAILS
720011-01	METAL POSTS FOR SIGNS, MARKERS AND DELINEATORS
725001-01	OBJECT AND TERMINAL MARKERS
728001-01	TELESCOPING STEEL SIGN SUPPORT
729001-01	APPLICATIONS OF TYPES A AND B METAL POSTS (FOR SIGNS & MARKERS)
731001-01	BASE FOR TELESCOPING STEEL SIGN SUPPORT
780001-05	TYPICAL PAVEMENT MARKINGS
782006	GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS
814001-03	HANDHOLES
814006-02	DOUBLE HANDHOLES
857001-01	STANDARD PHASE DESIGNATION DIAGRAMS AND PHASE SEQUENCES
873001-02	TRAFFIC SIGNAL GROUNDING & BONDING
878001-10	CONCRETE FOUNDATION DETAILS
880006-01	TRAFFIC SIGNAL MOUNTING DETAILS
886001-01	DETECTOR LOOP INSTALLATIONS
886006-01	TYPICAL LAYOUTS FOR DETECTION LOOPS

GENERAL NOTES

SPECIFICATIONS, STANDARDS AND SPECIAL PROVISIONS

ALL CONSTRUCTION SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION", ADOPTED APRIL 1, 2016; THE "SUPPLEMENTAL SPECIFICATIONS AND RECURRING SPECIAL PROVISIONS", ADOPTED JANUARY 1, 2019; THE LATEST EDITION OF THE "ILLINOIS MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS" (IMUTCD); "THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS" JUNE 2014 SEVENTH EDITION; THE "ILLINOIS URBAN MANUAL " AND THE "ILLINOIS URBAN MANUAL FIELD MANUAL FOR INSPECTION OF EROSION AND SEDIMENT CONTROL BEST MANAGEMENT PRACTICES"; THE "AMERICANS WITH DISABILITIES ACT OF 1990 ACCESSIBILITY GUIDELINES"; THE "DRAFT" REHABILITATION ACT OF 1973 (SECTION 504); THE "DETAILS" IN THE PLANS; AND THE "SPECIAL PROVISIONS" INCLUDED IN THE CONTRACT DOCUMENTS.

ANY REFERENCE TO STANDARDS THROUGHOUT THE PLANS OR SPECIAL PROVISIONS SHALL BE INTERPRETED AS THE LATEST IDOT HIGHWAY STANDARD.

ALL TRAFFIC CONTROL AND OTHER ADVISORY SIGNS NEEDED FOR CONSTRUCTION ARE TO BE FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH SECTION 700 OF THE STANDARD SPECIFICATIONS.

UTILITIES

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE OWNERS OF ALL EXISTING FACILITIES SO THAT THE UTILITIES AND THEIR APPURTENANCES MAY BE LOCATED AND ADJUSTED OR MOVED, IF NECESSARY, PRIOR TO THE START OF CONSTRUCTION OPERATIONS. THE CONTRACTOR SHALL COOPERATE WITH ALL UTILITY OWNERS AS PROVIDED FOR IN THE STANDARD SPECIFICATIONS.

THE LOCATIONS OF EXISTING DRAINAGE STRUCTURES, STORM AND SANITARY SEWERS, WATER SERVICE LINES AND OTHER UTILITY LINES ARE APPROXIMATE, AND THE VILLAGE DOES NOT GUARANTEE THEIR ACCURACY. THEIR EXACT HORIZONTAL AND VERTICAL LOCATIONS ARE TO BE DETERMINED IN THE FIELD BY THE CONTRACTOR.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL UNDERGROUND OR SURFACE UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER.

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, GAS AND CABLE TELEVISION FACILITIES (48 HOURS NOTIFICATION IS REQUIRED.)

WHENEVER DURING CONSTRUCTION OPERATIONS ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DRAINAGE STRUCTURES SUCH THAT THE NATURAL FLOW OF WATER IS OBSTRUCTED, IT SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. AT THE CONCLUSION OF CONSTRUCTION OPERATIONS, ALL UTILITY STRUCTURES SHALL BE FREE FROM DIRT AND DEBRIS.

ANY EXISTING OR PROPOSED STORM SEWER DAMAGED BY THE CONTRACTOR DURING CONSTRUCTION SHALL BE REPLACED BY THE CONTRACTOR.

THE CONTRACTOR SHALL NOT OPEN OR SHUT ANY WATER VALVES OR FIRE HYDRANTS. CONTACT WATER DEPARTMENT FOR THEM TO TURN VALVES OR OPERATE HYDRANTS. UNAUTHORIZED USE SHALL SUBJECT THE OFFENDER TO ARREST AND PROSECUTION.

STAKING

THE CONTRACTOR SHALL PROTECT AND CAREFULLY PRESERVE ALL SECTION OR SUBSECTION MONUMENTS OR PROPERTY OR REFERENCE MARKERS UNTIL THE ENGINEER, HIS AGENT OR AN AUTHORIZED SURVEYOR HAS WITNESSED OR OTHERWISE REFERENCED THEIR LOCATIONS.

ALL RADII FOR PROPOSED CURB AND GUTTER ARE TO THE EDGE OF PAVEMENT.

MISCELLANEOUS

DIMENSIONS: IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.

AGGREGATE BASE REPAIR, CURB AND GUTTER REMOVAL AND REPLACEMENT, SIDEWALK REMOVAL AND REPLACEMENT, AND STRUCTURES TO BE ADJUSTED WILL BE DETERMINED BY THE ENGINEER IN THE FIELD.

RELOCATING EXISTING SIGNS: EXISTING SIGNS WHICH ARE IN CONFLICT WITH PROPOSED IMPROVEMENTS SHALL BE REMOVED AND REINSTALLED UPON COMPLETION OF CONFLICTING IMPROVEMENTS IN ACCORDANCE WITH THE ILLINOIS DEPARTMENT OF TRANSPORTATION "MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES" AND THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION". STOP SIGNS, SPEED LIMIT SIGNS, AND STREET NAME SIGNS SHALL BE UP AND VISIBLE AT ALL TIMES.

CURB AND GUTTER TO BE REMOVED AT DRIVEWAYS SHALL BE REPLACED WITH DEPRESSED CURB AND GUTTER.

QUANTITIES FOR NEW INLETS, CATCH BASINS, AND FRAMES AND LIDS HAVE BEEN INCLUDED AS CONTINGENCY ITEMS. THE ENGINEER SHALL VERIFY THE NEED FOR REPLACEMENT AS WELL AS THE TYPE NEEDED BEFORE THE CONTRACTOR ORDERS THESE ITEMS.

THE CONTRACTOR SHALL COORDINATE DETECTOR LOOP WORK WITH DuPAGE COUNTY DIVISION OF TRANSPORTATION AND CLOSELY FOLLOW THE SPECIAL PROVISIONS FOR DETECTOR LOOPS.

THE CONTRACTOR SHOULD BE AWARE THAT CONTAMINATED SOILS WERE FOUND AT THE INTERSECTIONS OF YORK RD AND GEORGE STREET AS WELL AS YORK RD AND GREEN ST. PRELIMINARY SITE INVESTIGATION REPORTS AVAILABLE UPON REQUEST.

SIGNS AND POSTS ALONG YORK ROAD SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. QUANTITIES FOR NEW SIGNS AND POSTS HAVE BEEN INCLUDED AS CONTINGENCY ITEMS. THE ENGINEER SHALL VERIFY THE TYPE NEEDED BEFORE THE CONTRACTOR ORDERS THESE ITEMS.

PAVING

HOT-MIX ASPHALT BINDER COURSE SHALL NOT BE PLACED ADJACENT TO CURB AND GUTTER UNTIL THE CURB AND GUTTER HAS BEEN PROPERLY CURED AND BACKFILLED TO THE SATISFACTION OF THE ENGINEER.

THE THICKNESSES OF HOT-MIX ASPHALT MIXTURES SHOWN ON THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACE, BINDER, OR BASE UPON WHICH THE HOT-MIX ASPHALT MATERIALS ARE PLACED. THE THICKNESSES SHOWN ON THE PLANS ARE THE MINIMUM ACCEPTABLE THICKNESSES.

LANDSCAPING

RESTORATION WORK WILL BE PAID FOR UP TO TWO FEET ADJACENT TO RECONSTRUCTED CURB AND GUTTER OR SIDEWALK. AREAS OUTSIDE THE TWO FOOT ZONE THAT ARE DAMAGED OR DISTURBED BY THE CONTRACTOR SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RESTORE.

CONTRACTOR SHALL BE CAUTIOUS NOT TO DISTURB ANY PARKWAY TREES DURING CONSTRUCTION. ANY TREES THAT ARE DAMAGED OR DISTURBED DURING CONSTRUCTION SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO REPLACE IN KIND.

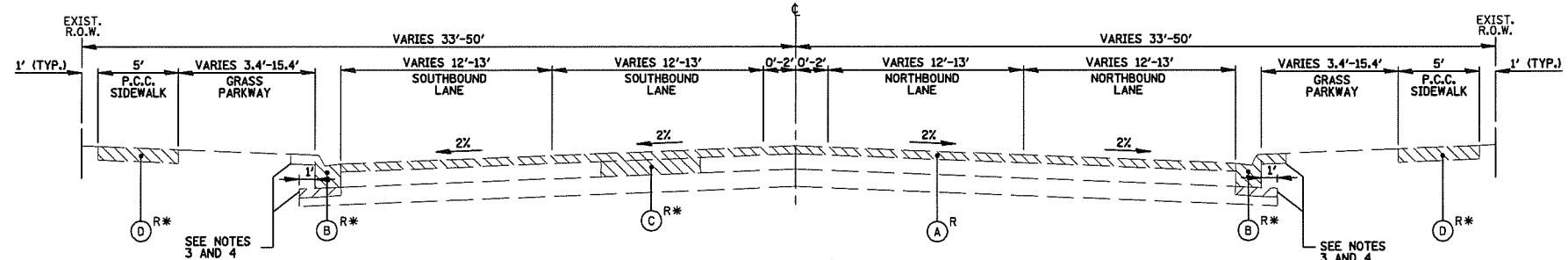
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N:\Bensenville\180145\Civil\GENEN.180145.SH	DRAWN MLM	REVISED -	2678			18-00098-00-RS	DUPAGE	45	2	
PLOT SCALE = 40'	CHECKED JGS	REVISED -	CONTRACT NO. 61F38							
Default	PLOT DATE = 11/14/2018	DATE -	ILLINOIS FED. AID PROJECT							
				SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.	

CODE NUMBER	ITEM	UNIT	TOTAL QUANTITY
20101200	TREE ROOT PRUNING	EACH	10
20101350	TREE PRUNING (OVER 10 INCH DIAMETER)	EACH	20
20200100	EARTH EXCAVATION	CU YD	25
21101600	TOPSOIL FURNISH AND PLACE, VARIABLE DEPTH	SQ YD	800
25200110	SODDING, SALT TOLERANT	SQ YD	800
25200200	SUPPLEMENTAL WATERING	UNIT	10
28000510	INLET FILTERS	EACH	83
31101100	SUBBASE GRANULAR MATERIAL, TYPE B	CU YD	388
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	34880
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	10
40600635	LEVELING BINDER (MACHINE METHOD), N70	TON	262
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	299
40603085	HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N70	TON	6511
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	5788
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FI	5972
42400800	DETECTABLE WARNINGS	SQ FT	417
44000166	HOT-MIX ASPHALT SURFACE REMOVAL, 4 1/4"	SQ YD	45420
44000171	HOT-MIX ASPHALT SURFACE REMOVAL, 5 1/2"	SQ YD	6250
44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	3952
44000600	SIDEWALK REMOVAL	SQ FT	6067
44200176	PAVEMENT PATCHING, TYPE I, 15 INCH	SQ YD	1000
44200180	PAVEMENT PATCHING, TYPE II, 15 INCH	SQ YD	1000
44200184	PAVEMENT PATCHING, TYPE III, 15 INCH	SQ YD	900

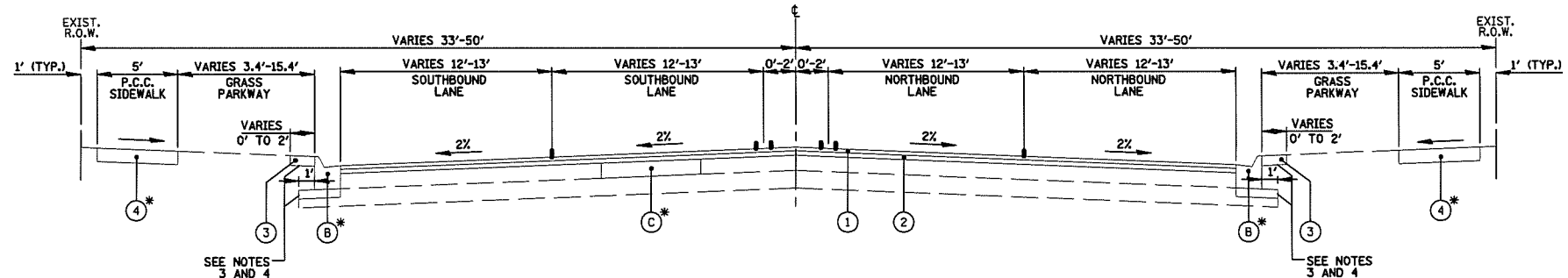
CODE NUMBER	ITEM	UNIT	TOTAL QUANTITY
44200186	PAVEMENT PATCHING, TYPE IV, 15 INCH	SQ YD	800
56500600	DOMESTIC WATER SERVICE BOXES TO BE ADJUSTED	EACH	5
60200105	CATCH BASINS, TYPE A, 4'-DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	10
60234200	INLETS, TYPE A, TYPE 1 FRAME, OPEN LID	EACH	10
60250200	CATCH BASINS TO BE ADJUSTED	EACH	20
60260100	INLETS TO BE ADJUSTED	EACH	10
60265700	VALVE VAULTS TO BE ADJUSTED	EACH	5
60266600	VALVE BOXES TO BE ADJUSTED	EACH	27
60404800	FRAMES AND GRATES, TYPE 11	EACH	4
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	5
Δ 63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	FOOT	125
Δ 63000030	STRONG POST GUARDRAIL ATTACHED TO CULVERT	FOOT	10
Δ 63100045	TRAFFIC BARRIER TERMINAL, TYPE 2	EACH	4
63200310	GUARDRAIL REMOVAL	FOOT	120
Δ 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	25
Δ 66900530	SOIL DISPOSAL ANALYSIS	EACH	2
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	2
67100100	MOBILIZATION	L SUM	1
Δ 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	LSUM	1
Δ 66901002	ON-SITE MONITORING OF REGULATED SUBSTANCES	CAL DA	5
Δ 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	LSUM	1
70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	L SUM	1
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1

Δ INDICATES SPECIALTY ITEM

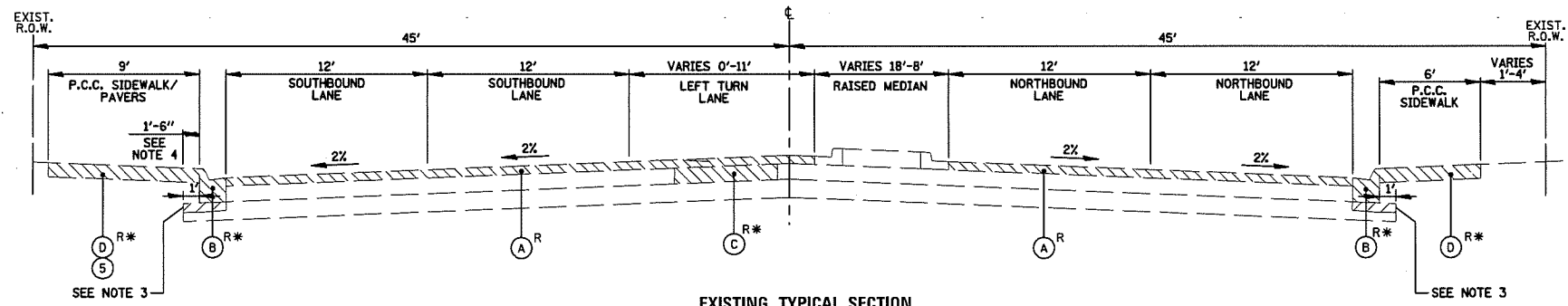
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Default	PLOT SCALE = 40'	CHECKED JGS	REVISED -			SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.
	PLOT DATE = 11/14/2018	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					
CONTRACT NO. 61F38											



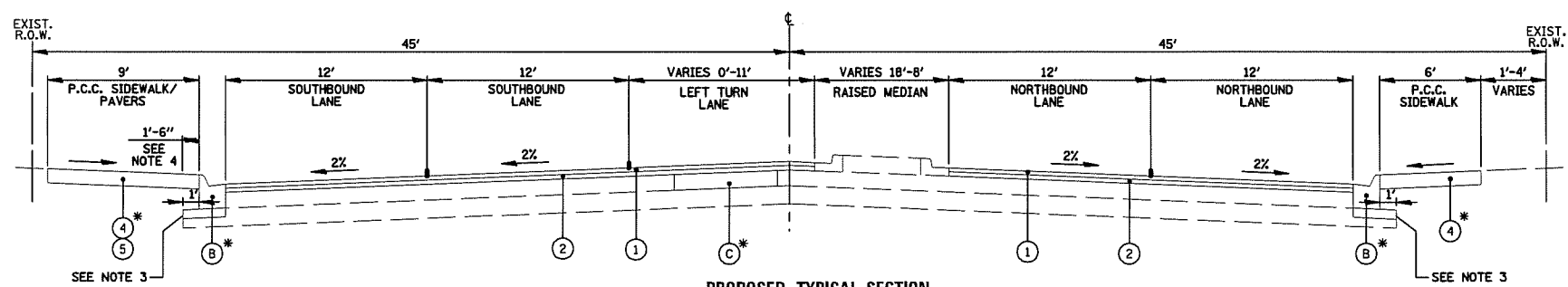
EXISTING TYPICAL SECTION
YORK ROAD
 25+00 TO 70+00 & 76+50 TO 104+00
 4-LANE SECTION



PROPOSED TYPICAL SECTION
YORK ROAD
 25+00 TO 70+00 & 76+50 TO 104+00
 4-LANE SECTION



EXISTING TYPICAL SECTION
YORK ROAD
 22+00 TO 25+00
 5-LANE SECTION AT GRAND AVENUE



PROPOSED TYPICAL SECTION
YORK ROAD
 22+00 TO 25+00
 5-LANE SECTION AT GRAND AVENUE

NOTE:
 FOR DRIVEWAY REPLACEMENT DETAIL SEE TYPICAL SECTIONS SHEET 2

- (A) HOT-MIX ASPHALT SURFACE REMOVAL, 4 1/4" (44000166)
- (B) COMBINATION CURB AND GUTTER REMOVAL (44000500)
 COMBINATION CONCRETE CURB AND GUTTER (TYPE TO MATCH EXISTING)
- (C) PAVEMENT PATCHING (TYPE AND THICKNESS WILL VARY)
- (D) SIDEWALK REMOVAL (44000600)
- (1) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (2") (40603340)
- (2) HOT-MIX ASPHALT BINDER COURSE, IL 19.0, N70 (2 1/4") (40603085)
- (3) SODDING, SALT TOLERANT (25200200)
 TOPSOIL FURNISH AND PLACE, VARIABLE DEPTH (21101600)
- (4) PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH (42400200)
- (5) REMOVE AND REINSTALL BRICK PAVER (X0327611)
- R REMOVAL
- * AT LOCATIONS AS DIRECTED BY THE ENGINEER

- NOTES:**
1. CONTRACTOR SHALL MILL PAVEMENT BEFORE PATCHING.
 2. THE EXISTING PAVEMENT SECTION CONSISTS OF VARIABLE DEPTH HMA. A CONCRETE BASE COURSE EXISTS IN SOME LOCATIONS. SEE PAVEMENT CORES FOR DETAILS OF EXISTING PAVEMENT STRUCTURE.
 3. CONTRACTOR SHALL REMOVE AND REPLACE ANY UNSUITABLE MATERIAL UNDER CURB AND GUTTER REPLACEMENT LOCATIONS AS DIRECTED BY THE ENGINEER. DISTURBED SOIL BEHIND REPLACED CURB AND GUTTER SHALL BE SODDED AS SHOWN IN THE PROPOSED TYPICAL SECTIONS.
 4. CONTRACTOR SHALL REMOVE AND REPLACE BRICK PAVERS BEHIND CURB AND GUTTER REPLACEMENT LOCATIONS AS DIRECTED BY THE ENGINEER.
 5. AGGREGATE BASE REPAIR (SUBBASE GRANULAR MATERIAL, TYPE B) UNDER SIDEWALKS AND CURB AND GUTTER WILL BE DETERMINED BY THE ENGINEER IN THE FIELD.
 6. BETWEEN STATION 92+00 AND STA 106+00 HMA REMOVAL SHALL BE TO THE CONCRETE BASE AND REPLACED WITH HMA RESURFACING (5 1/2").

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		AIR VOIDS @Ndes
MIXTURE TYPE		
HMA RESURFACING (4-1/4")		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5mm); 2"		4% @ 70 GYR.
HOT-MIX ASPHALT BINDER COURSE, IL 19.0, N70; 2 1/4"		4% @ 70 GYR.
HMA RESURFACING (5-1/2")		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (IL-9.5mm); 2"		4% @ 70 GYR.
HOT-MIX ASPHALT BINDER COURSE, IL 19.0, N70; 2 3/4"		4% @ 70 GYR.
LEVELING BINDER (MACHINE METHOD), N70; 3/4" (IL-9.5mm)		4% @ 70 GYR.
PAVEMENT PATCHING (CLASS D PATCHES)		
HOT-MIX ASPHALT BINDER, IL 19.0		4% @ 70 GYR.
DRIVEWAY REPLACEMENT (HMA)		
HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N50 (IL-9.5mm); 2"		4% @ 70 GYR.
HOT-MIX ASPHALT BASE COURSE (HMA BINDER IL-19.0), N50		4% @ 70 GYR.

- NOTES:**
1. THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES IS 112 LBS/SY/IN.
 2. THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIAL SEE SPECIAL PROVISIONS.

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PLT DATE = 11/14/2018	DATE -

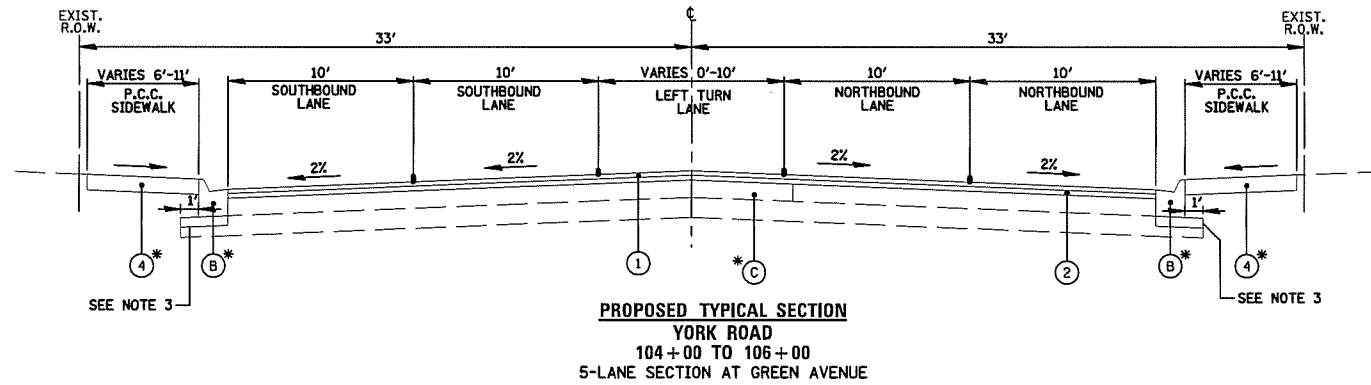
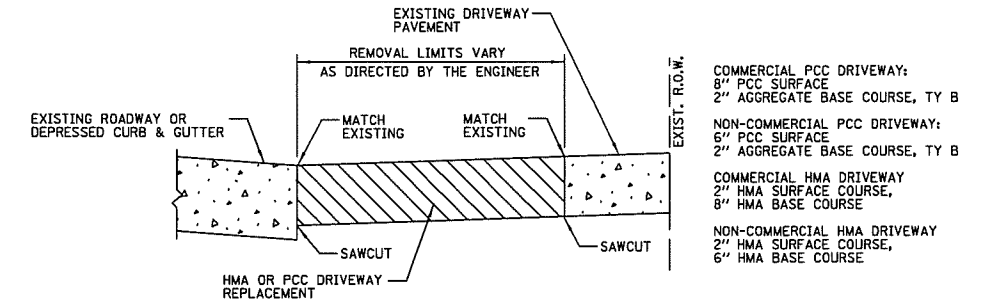
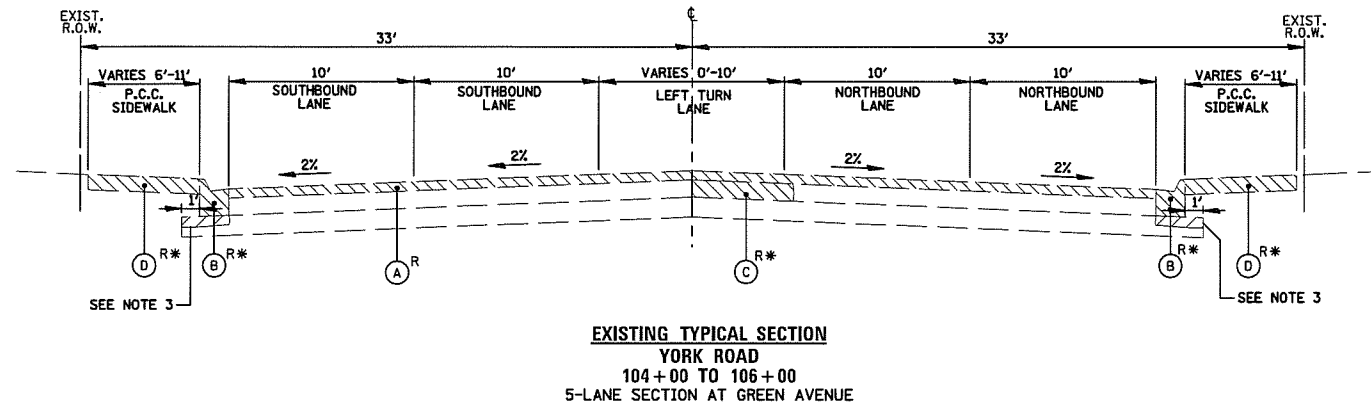
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CHECKED JCS	REVISED -
DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

VILLAGE OF BENSENVILLE
YORK ROAD
TYPICAL SECTIONS

SCALE: NONE SHEET 1 OF 2 SHEETS STA. TO STA.

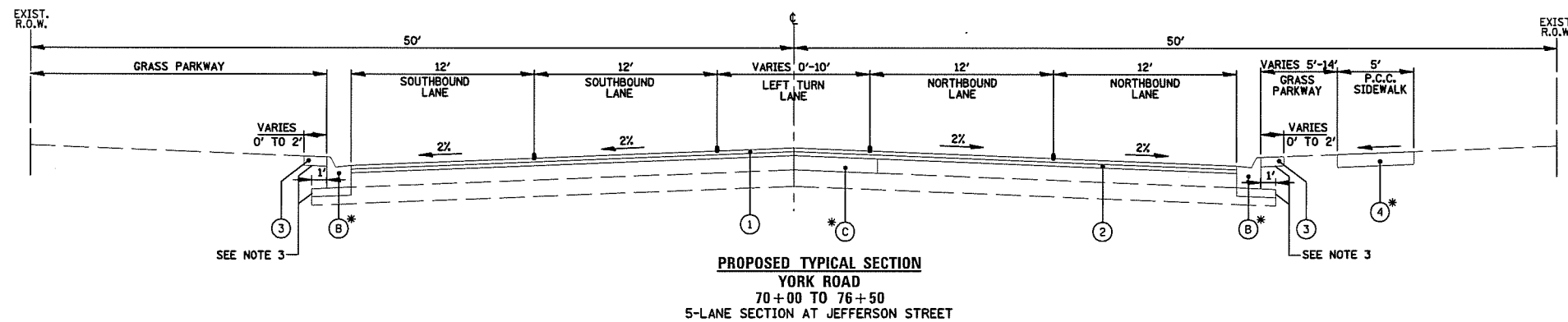
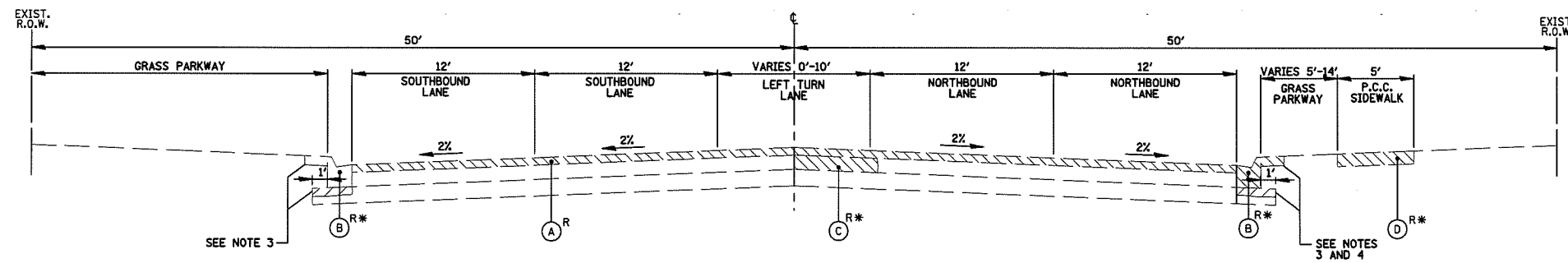
FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2678	18-00098-00-RS	DUPAGE	45	5
CONTRACT NO. 61F38			ILLINOIS FED. AID PROJECT	



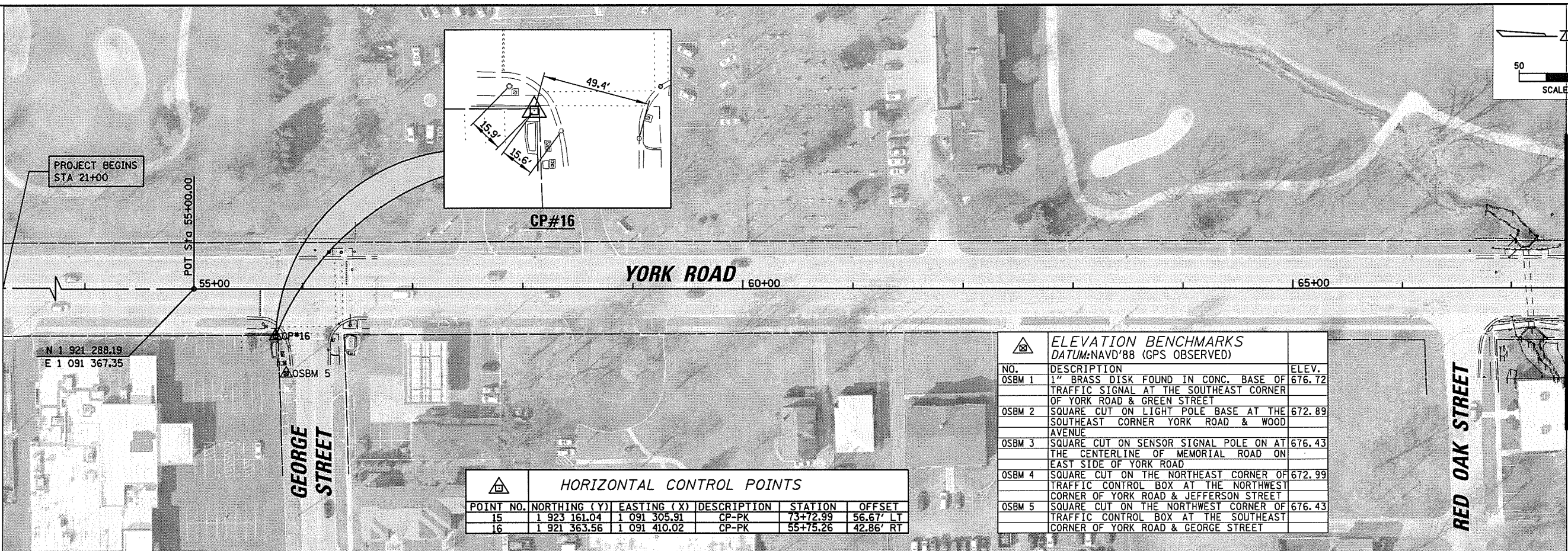
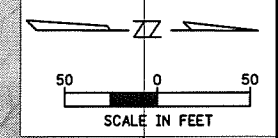
TYPICAL DRIVEWAY REPLACEMENT DETAIL
SEE DRIVEWAY REMOVAL AND REPLACEMENT SPECIAL PROVISION

- (A) HOT-MIX ASPHALT SURFACE REMOVAL, 4 1/4" (44000166)
- * (B) COMBINATION CURB AND GUTTER REMOVAL (44000500)
COMBINATION CONCRETE CURB AND GUTTER (TYPE TO MATCH EXISTING)
- (C) PAVEMENT PATCHING (TYPE AND THICKNESS WILL VARY)
- * (D) SIDEWALK REMOVAL (44000600)
- (1) HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 (2") (40603340)
- * (2) HOT-MIX ASPHALT BINDER COURSE, IL 19.0, N70 (2 1/4") (40603085)
- (3) SODDING, SALT TOLERANT (25200200)
TOPSOIL FURNISH AND PLACE, VARIABLE DEPTH (21101600)
- (4) PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH (42400200)
- (5) REMOVE AND REINSTALL BRICK PAVER (X0327611)
- R REMOVAL
- * AT LOCATIONS AS DIRECTED BY THE ENGINEER

- NOTES:**
1. CONTRACTOR SHALL MILL PAVEMENT BEFORE PATCHING.
 2. THE EXISTING PAVEMENT SECTION CONSISTS OF VARIABLE DEPTH HMA. A CONCRETE BASE COURSE EXISTS IN SOME LOCATIONS. SEE PAVEMENT CORES FOR DETAILS OF EXISTING PAVEMENT STRUCTURE.
 3. CONTRACTOR SHALL REMOVE AND REPLACE ANY UNSUITABLE MATERIAL UNDER CURB AND GUTTER REPLACEMENT LOCATIONS AS DIRECTED BY THE ENGINEER. DISTURBED SOIL BEHIND REPLACED CURB AND GUTTER SHALL BE SODDED AS SHOWN IN THE PROPOSED TYPICAL SECTIONS.
 4. CONTRACTOR SHALL REMOVE AND REPLACE BRICK PAVERS BEHIND CURB AND GUTTER REPLACEMENT LOCATIONS AS DIRECTED BY THE ENGINEER.
 5. AGGREGATE BASE REPAIR (SUBBASE GRANULAR MATERIAL, TYPE B) UNDER SIDEWALKS AND CURB AND GUTTER WILL BE DETERMINED BY THE ENGINEER IN THE FIELD.
 6. BETWEEN STATION 92+00 AND STA 106+00 HMA REMOVAL SHALL BE TO THE CONCRETE BASE AND REPLACED WITH HMA RESURFACING (5 1/2").



FILE NAME =	USER NAME = dcoconnell	DESIGNED MLM	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	VILLAGE OF BENSENVILLE YORK ROAD TYPICAL SECTIONS	FAU	SECTION	COUNTY	TOTAL	SHEET
N:\Bensenville\180145\Civil\TYP_180145-02.dwg	PLOT SCALE = 10"	DRAWN MLM	REVISED -			2678	18-00098-00-RS	DUPAGE	45	6
Default	PLOT DATE = 11/14/2018	CHECKED JCS	REVISED -			SCALE: SHEET 2 OF 2 SHEETS STA. TO STA.		CONTRACT NO. 61F38		
		DATE -	REVISED -			ILLINOIS FED. AID PROJECT				



PROJECT BEGINS
STA 21+00

POT. STA. 55+00.00

CP#16

YORK ROAD

60+00

65+00

MATCH LINE STA. 67 + 50

N 1 921 288.19
E 1 091 367.35

CP#16

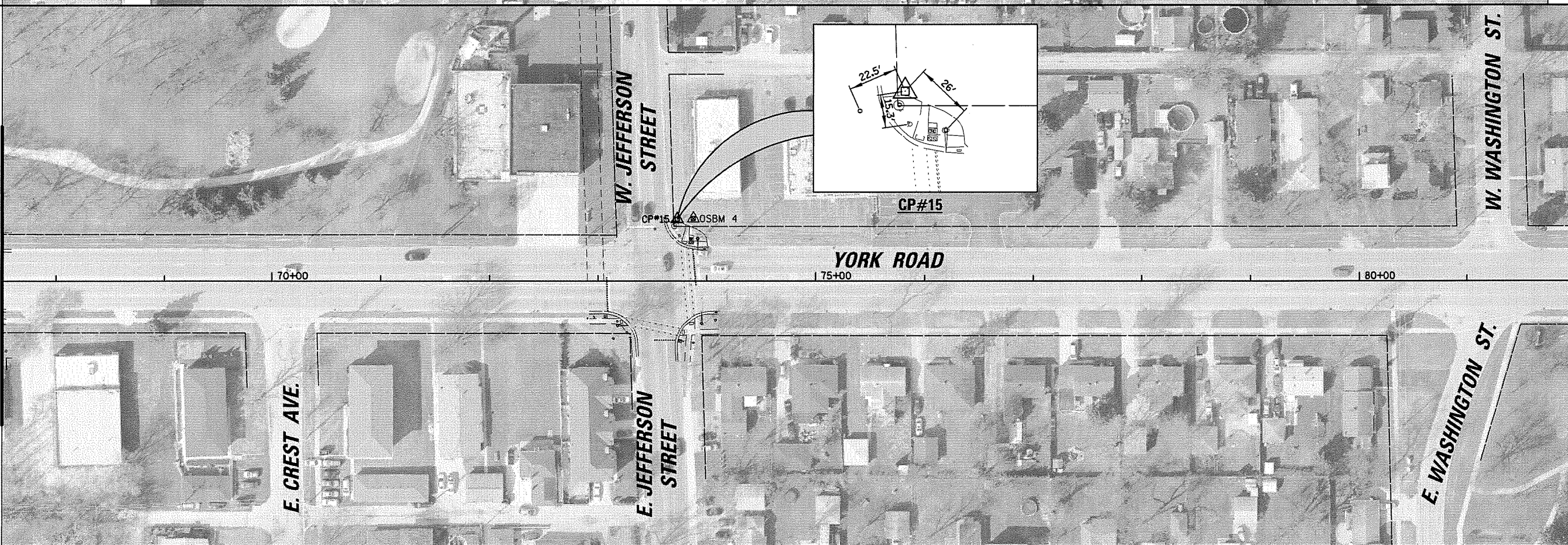
OSBM 5

GEORGE STREET

ELEVATION BENCHMARKS DATUM: NAVD'88 (GPS OBSERVED)		
NO.	DESCRIPTION	ELEV.
OSBM 1	1" BRASS DISK FOUND IN CONC. BASE OF TRAFFIC SIGNAL AT THE SOUTHEAST CORNER OF YORK ROAD & GREEN STREET	676.72
OSBM 2	SQUARE CUT ON LIGHT POLE BASE AT THE SOUTHEAST CORNER YORK ROAD & WOOD AVENUE	672.89
OSBM 3	SQUARE CUT ON SENSOR SIGNAL POLE ON AT THE CENTERLINE OF MEMORIAL ROAD ON EAST SIDE OF YORK ROAD	676.43
OSBM 4	SQUARE CUT ON THE NORTHEAST CORNER OF TRAFFIC CONTROL BOX AT THE NORTHWEST CORNER OF YORK ROAD & JEFFERSON STREET	672.99
OSBM 5	SQUARE CUT ON THE NORTHWEST CORNER OF TRAFFIC CONTROL BOX AT THE SOUTHEAST CORNER OF YORK ROAD & GEORGE STREET	676.43

HORIZONTAL CONTROL POINTS					
POINT NO.	NORTHING (Y)	EASTING (X)	DESCRIPTION	STATION	OFFSET
15	1 923 161.04	1 091 305.91	CP-PK	73+72.99	56.67' LT
16	1 921 363.56	1 091 410.02	CP-PK	55+75.26	42.86' RT

MATCH LINE STA. 67 + 50



YORK ROAD

75+00

80+00

MATCH LINE STA. 82 + 00

E. CREST AVE.

E. JEFFERSON STREET

W. JEFFERSON STREET

W. WASHINGTON ST.

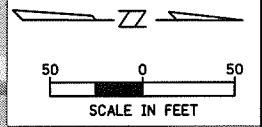
E. WASHINGTON ST.

CP#15

CP#15

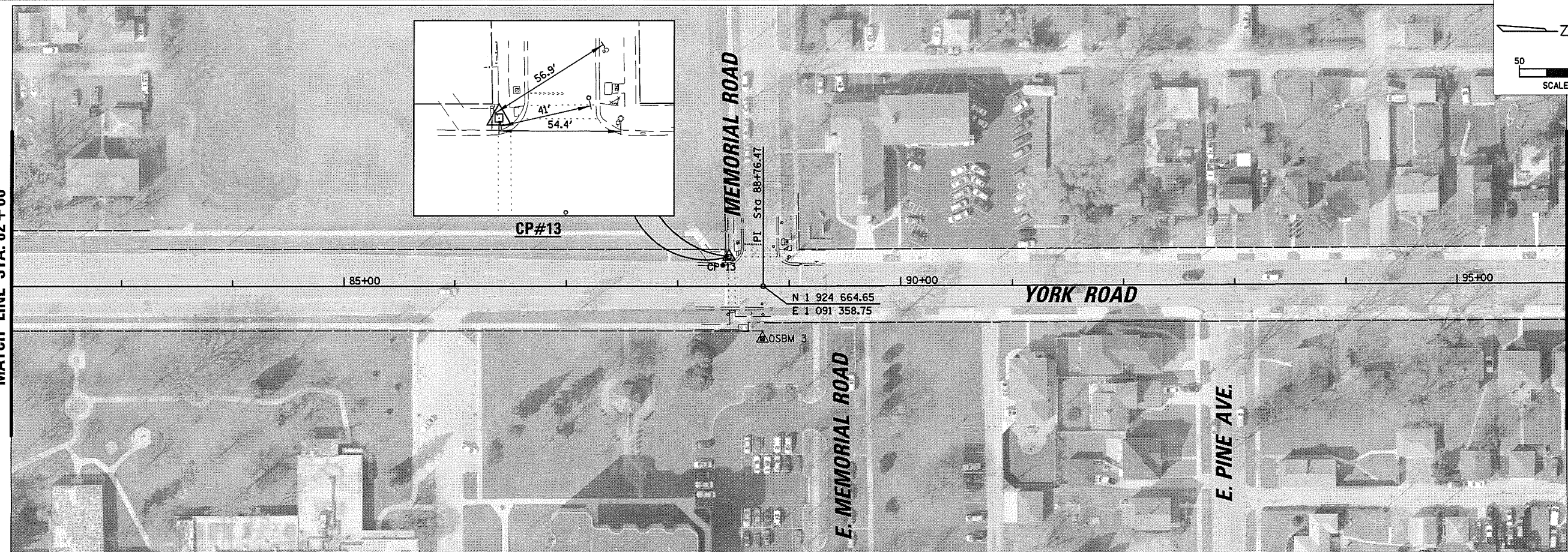
OSBM 4

FILE NAME = N:\Bensenville\180145\Civ\1\BHN\180145-01	USER NAME = doconne1	DESIGNED MLM	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	VILLAGE OF BENSENVILLE YORK ROAD ALIGNMENT, TIES AND BENCHMARK PLAN	FAU RTE. 2678	SECTION 18-00098-00-RS	COUNTY DuPAGE	TOTAL SHEETS 45	SHEET NO. 7		
Default	PLOT SCALE = 100'	DRAWN MLM	REVISED -			SCALE: 1" = 50'	SHEET 1 OF 2 SHEETS	STA. 53+25.00 TO STA. 82+00.00	CONTRACT NO. 61F38		ILLINOIS FED. AID PROJECT	
	PLOT DATE = 11/14/2018	CHECKED JCS	REVISED -									
		DATE -	REVISED -									

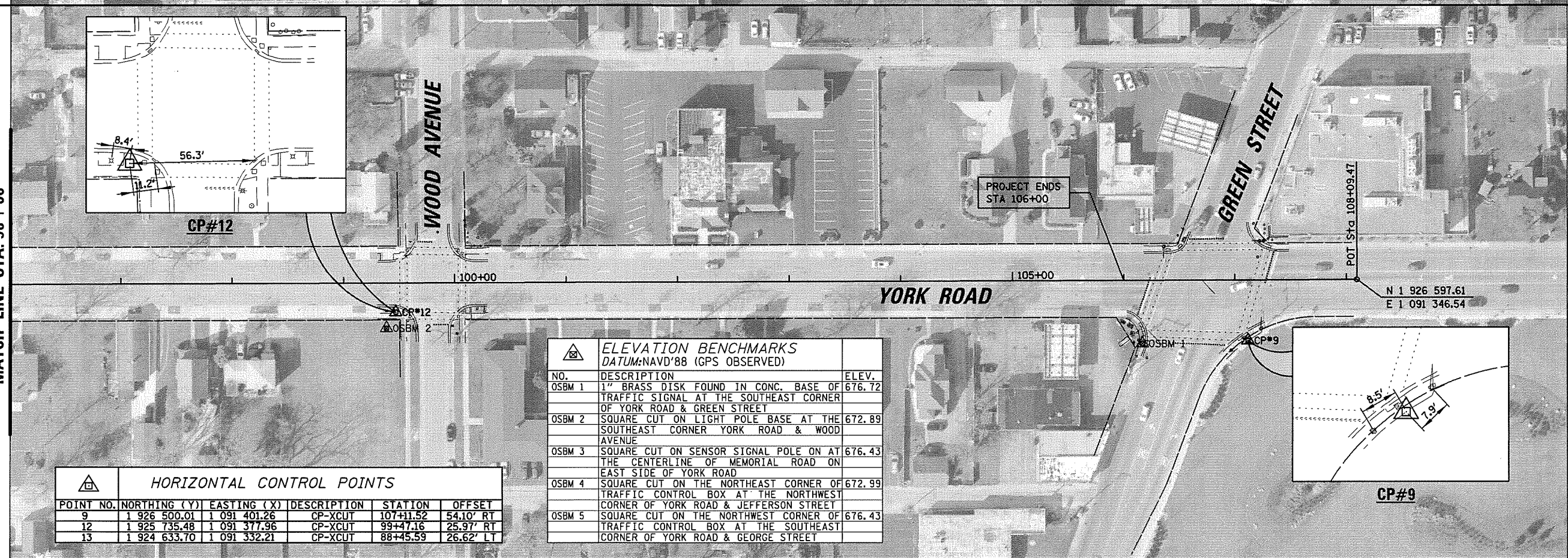


MATCH LINE STA. 82+00

MATCH LINE STA. 96+00



MATCH LINE STA. 96+00



ELEVATION BENCHMARKS DATUM: NAVD'88 (GPS OBSERVED)		
NO.	DESCRIPTION	ELEV.
OSBM 1	1" BRASS DISK FOUND IN CONC. BASE OF TRAFFIC SIGNAL AT THE SOUTHEAST CORNER OF YORK ROAD & GREEN STREET	676.72
OSBM 2	SQUARE CUT ON LIGHT POLE BASE AT THE SOUTHEAST CORNER YORK ROAD & WOOD AVENUE	672.89
OSBM 3	SQUARE CUT ON SENSOR SIGNAL POLE ON AT THE CENTERLINE OF MEMORIAL ROAD ON EAST SIDE OF YORK ROAD	676.43
OSBM 4	SQUARE CUT ON THE NORTHEAST CORNER OF TRAFFIC CONTROL BOX AT THE NORTHWEST CORNER OF YORK ROAD & JEFFERSON STREET	672.99
OSBM 5	SQUARE CUT ON THE NORTHWEST CORNER OF TRAFFIC CONTROL BOX AT THE SOUTHEAST CORNER OF YORK ROAD & GEORGE STREET	676.43

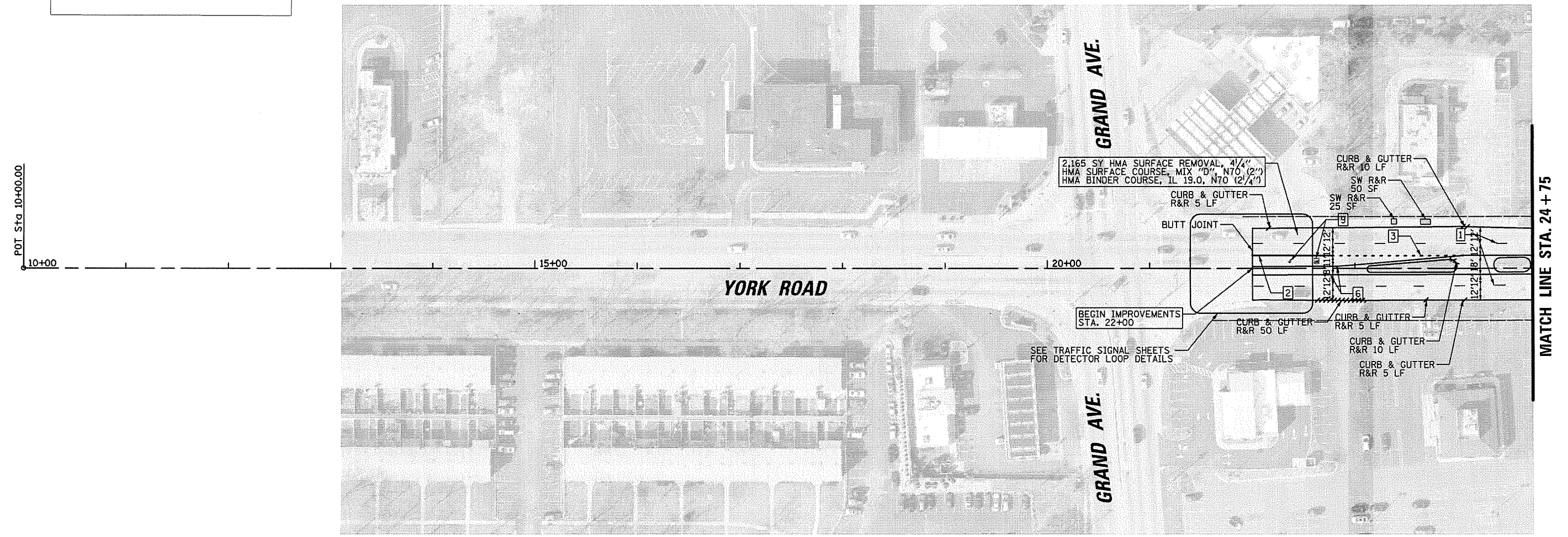
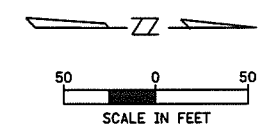
HORIZONTAL CONTROL POINTS					
POINT NO.	NORTHING (Y)	EASTING (X)	DESCRIPTION	STATION	OFFSET
9	1 926 500.01	1 091 401.26	CP-XCUT	107+11.52	54.10' RT
12	1 925 735.48	1 091 317.96	CP-XCUT	99+47.16	25.97' RT
13	1 924 633.70	1 091 332.21	CP-XCUT	88+45.59	26.62' LT

LEGEND

	SIDEWALK REMOVE AND REPLACE
	DETECTABLE WARNING (CURB RAMP)
	COMBINATION CONCRETE CURB AND GUTTER REMOVE AND REPLACE
	MANHOLES TO BE ADJUSTED
	EXISTING AERIAL LINE
	EXISTING WATER MAIN
	EXISTING STORM SEWER
	EXISTING COMBINED SEWER
	EXISTING FENCE

PAVEMENT MARKING LEGEND:

1	THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE LANE LINE - 10' DASH, 30' SKIP)
2	THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE SOLID LINE)
3	THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE LANE LINE - 2' DASH, 6' SKIP)
4	THERMOPLASTIC PAVEMENT MARKING - LINE 12" (WHITE LINE)
5	THERMOPLASTIC PAVEMENT MARKING - LINE 24" (WHITE STOP LINE)
6	THERMOPLASTIC PAVEMENT MARKING - LINE 4" (YELLOW SOLID LINE)
7	THERMOPLASTIC PAVEMENT MARKING - LINE 4" (DOUBLE YELLOW SOLID LINE 11" C-C)
8	THERMOPLASTIC PAVEMENT MARKING - LINE 12" (YELLOW DIAGONAL LINE)
9	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS

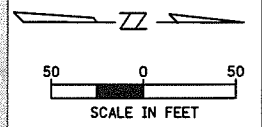


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	PLOT SCALE = 100'	CHECKED JGS	REVISED -
Default	PLOT DATE = 11/14/2018	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

VILLAGE OF BENSENVILLE YORK ROAD ROADWAY PLAN			
SCALE: 1" = 50'	SHEET 1 OF 4 SHEETS	STA. 10+00.00 TO STA. 24+75.00	

FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2678	18-00098-00-RS	DUPAGE	45	9
CONTRACT NO. 61F38			ILLINOIS FED. AID PROJECT	



MATCH LINE STA. 24 + 75

MATCH LINE STA. 39 + 00

LEGEND

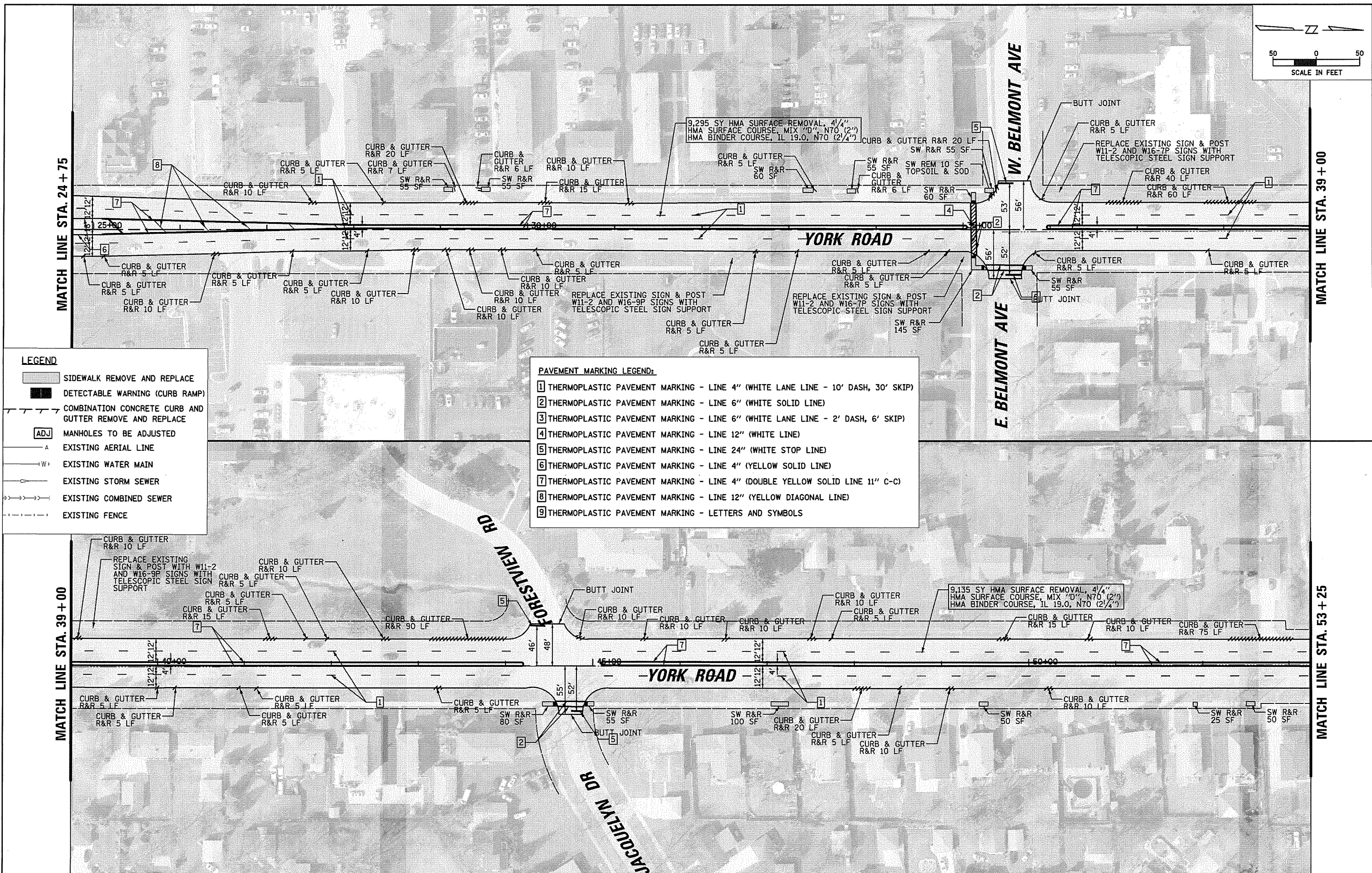
- SIDEWALK REMOVE AND REPLACE
- DETECTABLE WARNING (CURB RAMP)
- COMBINATION CONCRETE CURB AND GUTTER REMOVE AND REPLACE
- MANHOLES TO BE ADJUSTED
- EXISTING AERIAL LINE
- EXISTING WATER MAIN
- EXISTING STORM SEWER
- EXISTING COMBINED SEWER
- EXISTING FENCE

PAVEMENT MARKING LEGEND:

- 1 THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE LANE LINE - 10' DASH, 30' SKIP)
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- 8 THERMOPLASTIC PAVEMENT MARKING - LINE 12" (YELLOW DIAGONAL LINE)
- 9 THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS

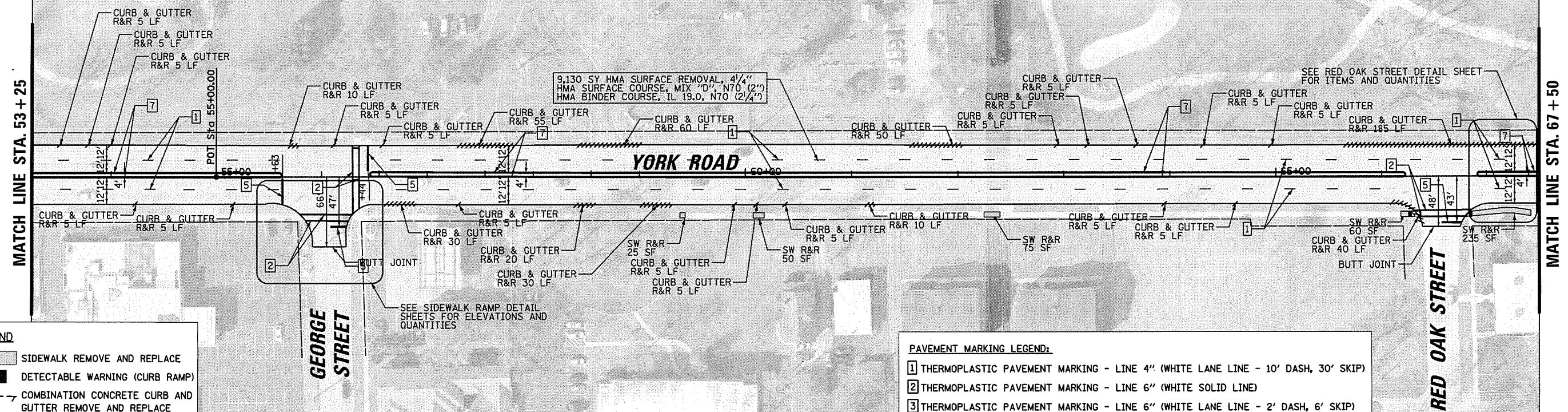
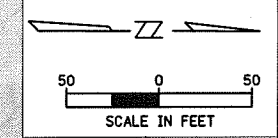
MATCH LINE STA. 39 + 00

MATCH LINE STA. 53 + 25



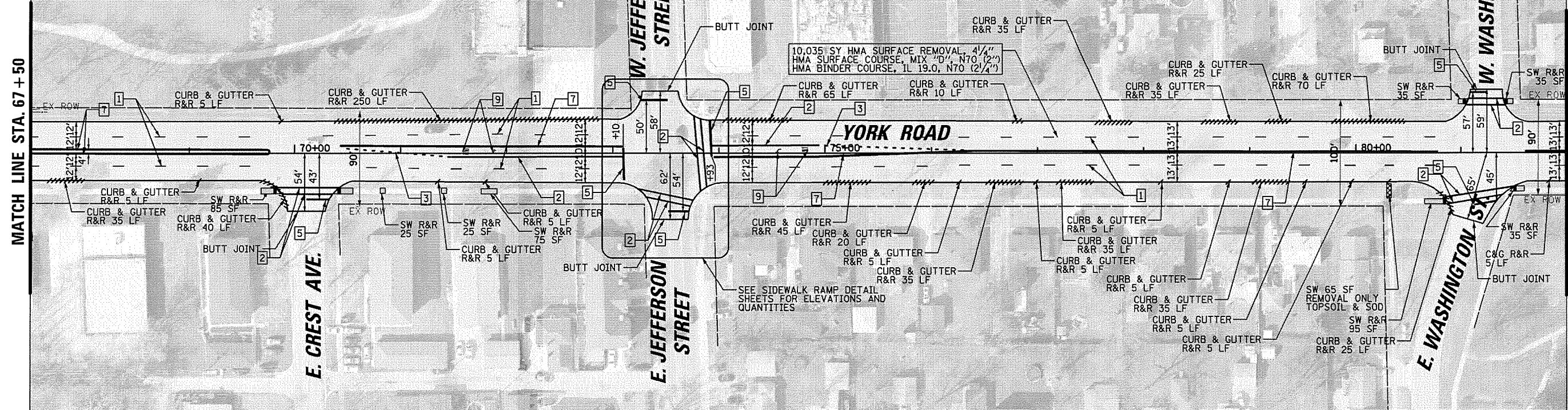
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PLT SCALE = 100'	DRAWN MLM	REVISED -	2678			18-00098-00-RS	DUPAGE	45	10	
PLT DATE = 11/14/2018	CHECKED JGS	REVISED -	CONTRACT NO. 61F38							
DATE -	DATE -	REVISED -	ILLINOIS FED. AID PROJECT							

SCALE: 1" = 50' | SHEET 2 OF 4 SHEETS | STA. 24+75.00 TO STA. 53+25.00

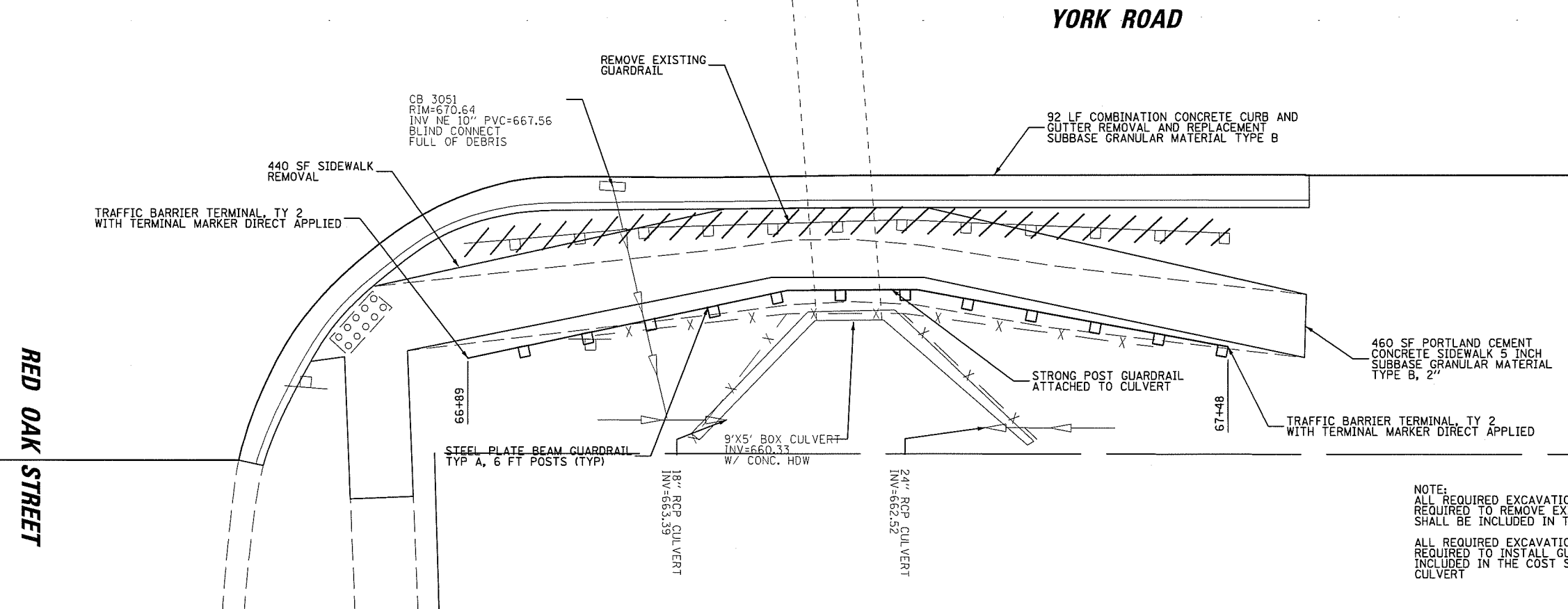
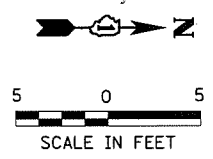
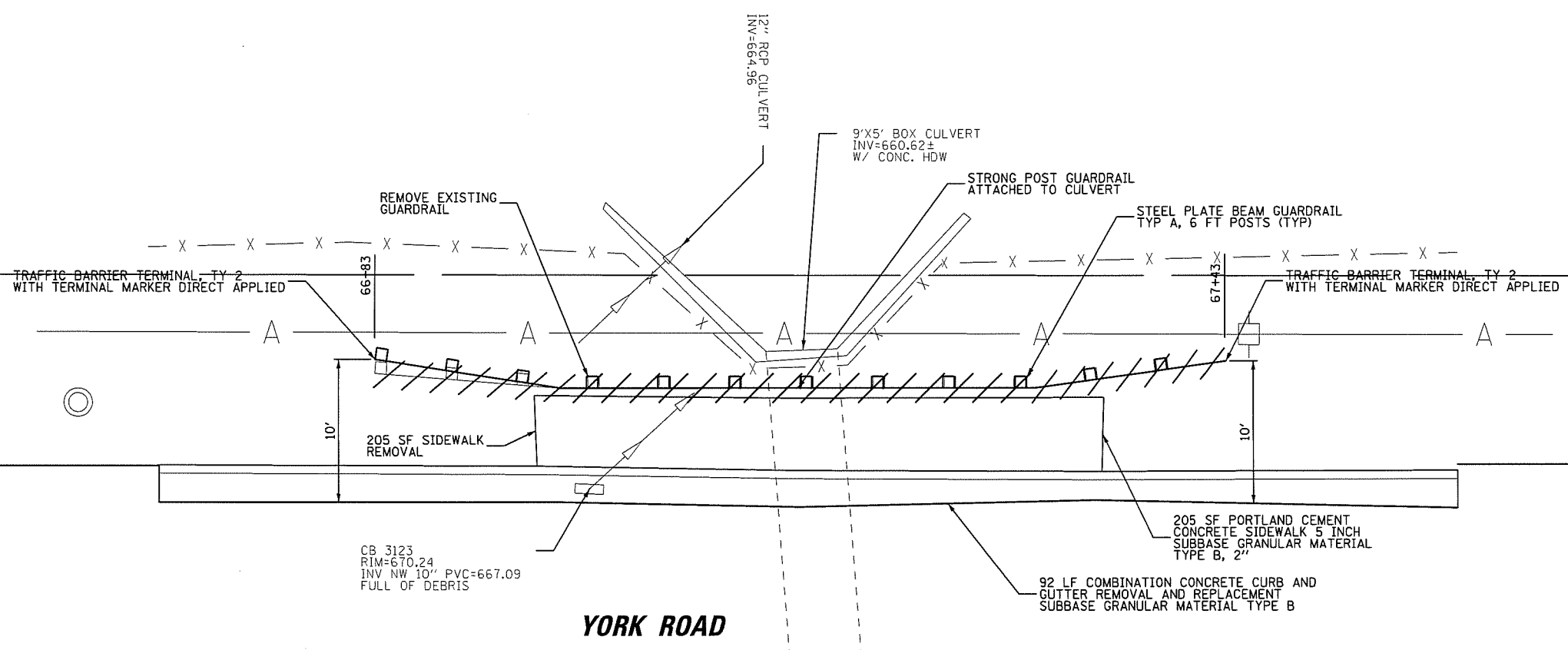
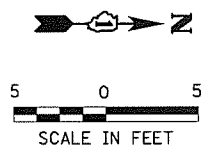


- LEGEND**
- SIDEWALK REMOVE AND REPLACE
 - DETECTABLE WARNING (CURB RAMP)
 - COMBINATION CONCRETE CURB AND GUTTER REMOVE AND REPLACE
 - MANHOLES TO BE ADJUSTED
 - EXISTING AERIAL LINE
 - EXISTING WATER MAIN
 - EXISTING STORM SEWER
 - EXISTING COMBINED SEWER
 - EXISTING FENCE

- PAVEMENT MARKING LEGEND:**
- 1 THERMOPLASTIC PAVEMENT MARKING - LINE 4" (WHITE LANE LINE - 10' DASH, 30' SKIP)
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 - 3 THERMOPLASTIC PAVEMENT MARKING - LINE 6" (WHITE LANE LINE - 2' DASH, 6' SKIP)
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 - 8 THERMOPLASTIC PAVEMENT MARKING - LINE 12" (YELLOW DIAGONAL LINE)
 - 9 THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS



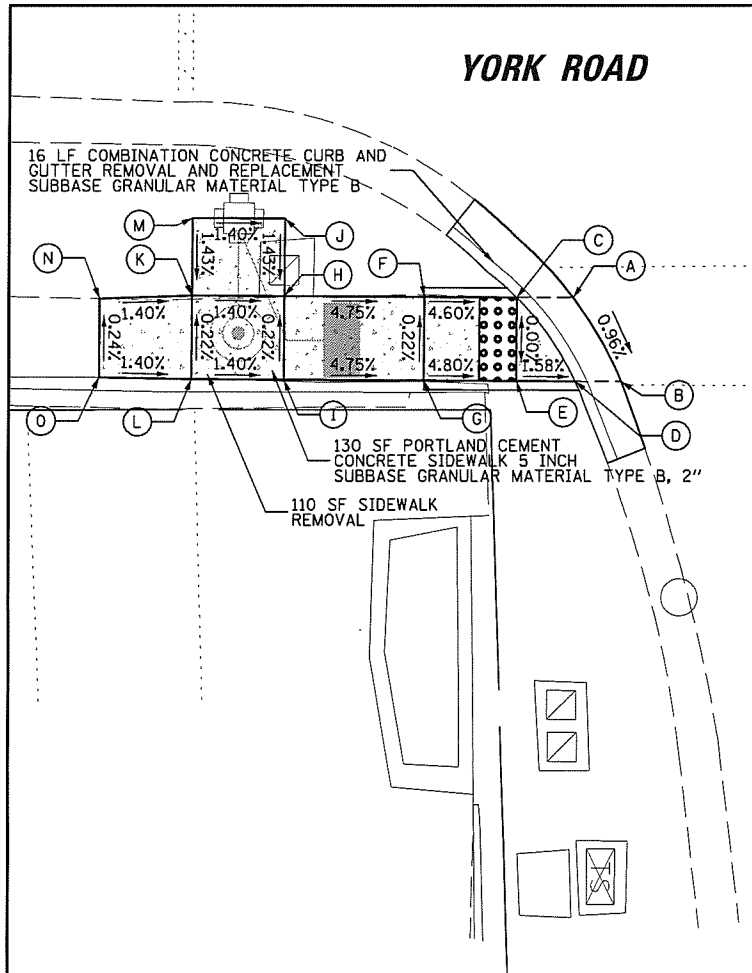
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PLOT SCALE = 100'	CHECKED JGS	REVISED -	SCALE: 1" = 50'			SHEET 3 OF 4 SHEETS	STA. 53+25.00 TO STA. 82+00.00	CONTRACT NO. 61F38		ILLINOIS FED. AID PROJECT	
Default	PLOT DATE = 11/14/2018	DATE -	REVISED -								



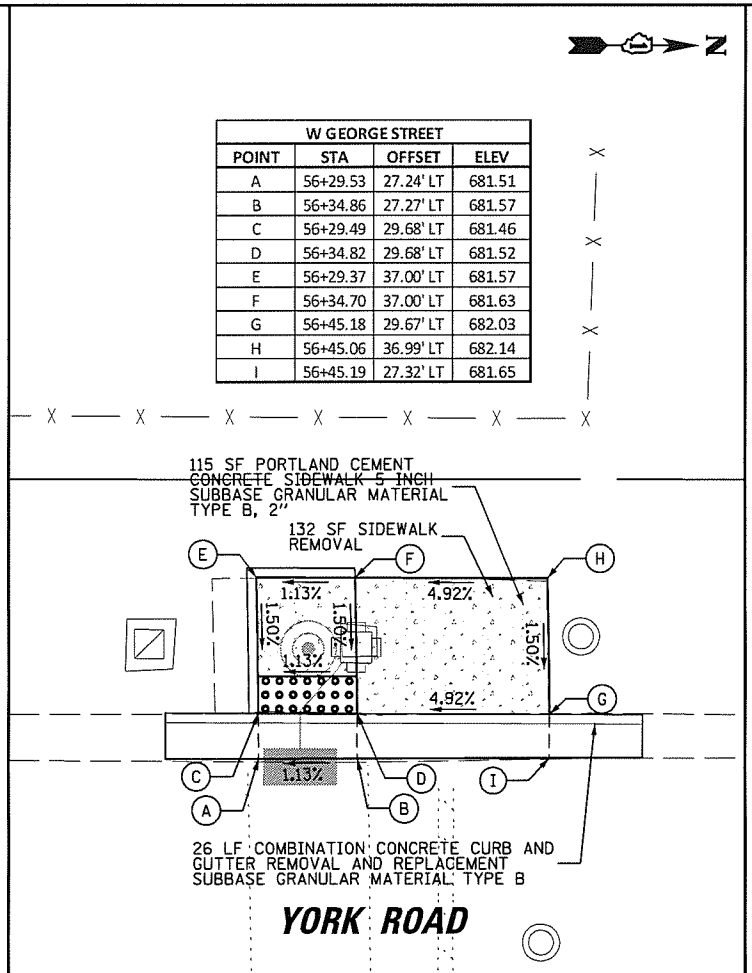
NOTE:
ALL REQUIRED EXCAVATION, MATERIALS, LABOR AND EQUIPMENT REQUIRED TO REMOVE EXISTING GUARDRAILS OVER CULVERT SHALL BE INCLUDED IN THE COST OF GUARDRAIL REMOVAL

ALL REQUIRED EXCAVATION, MATERIALS, LABOR AND EQUIPMENT REQUIRED TO INSTALL GUARDRAILS OVER CULVERT SHALL BE INCLUDED IN THE COST STRONG POST GUARDRAIL ATTACHED TO CULVERT

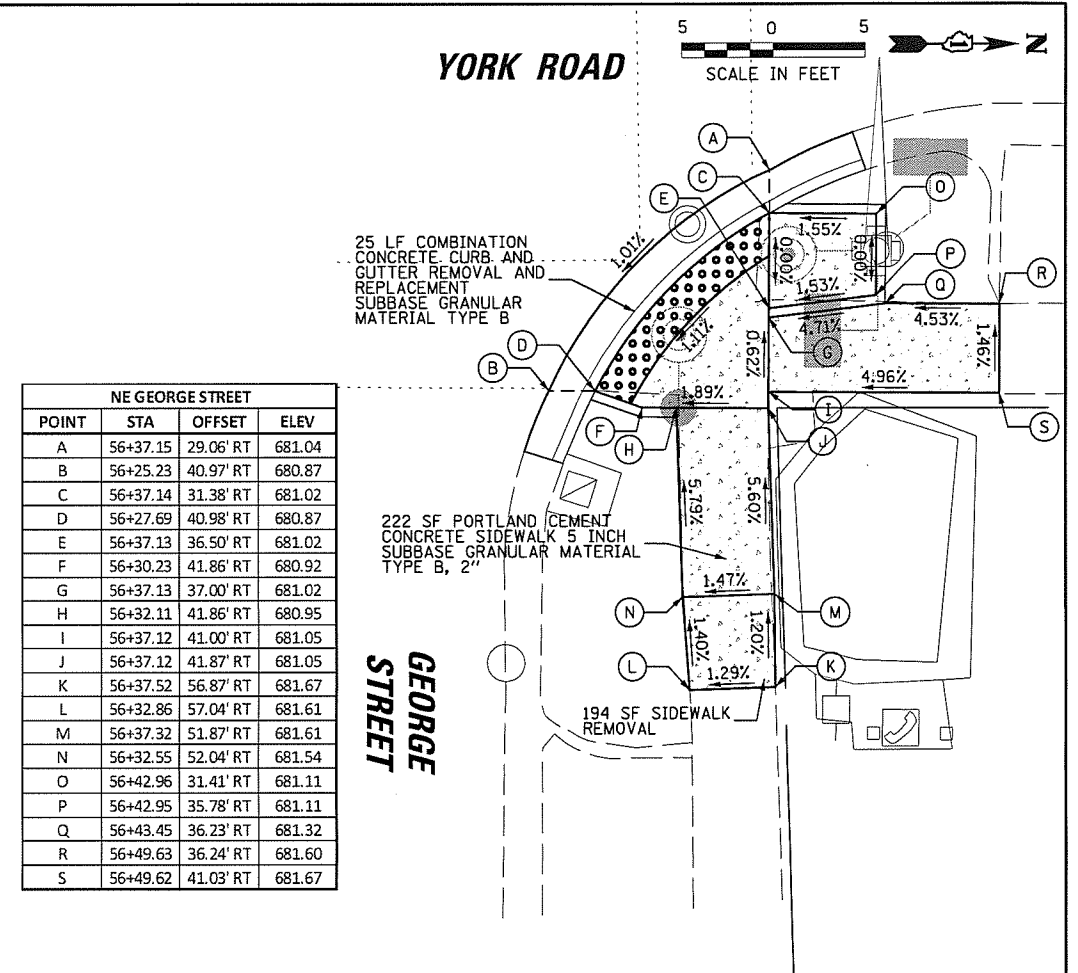
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PLOT SCALE = 10'	DRAWN MLM	REVISED -	CONTRACT NO. 61F38							
Default	CHECKED JGS	REVISED -	ILLINOIS FED. AID PROJECT							
PLOT DATE = 11/14/2018	DATE -	REVISED -	SCALE: 1" = 5' SHEET 1 OF 1 SHEETS STA. 66+50 TO STA. 68+00							



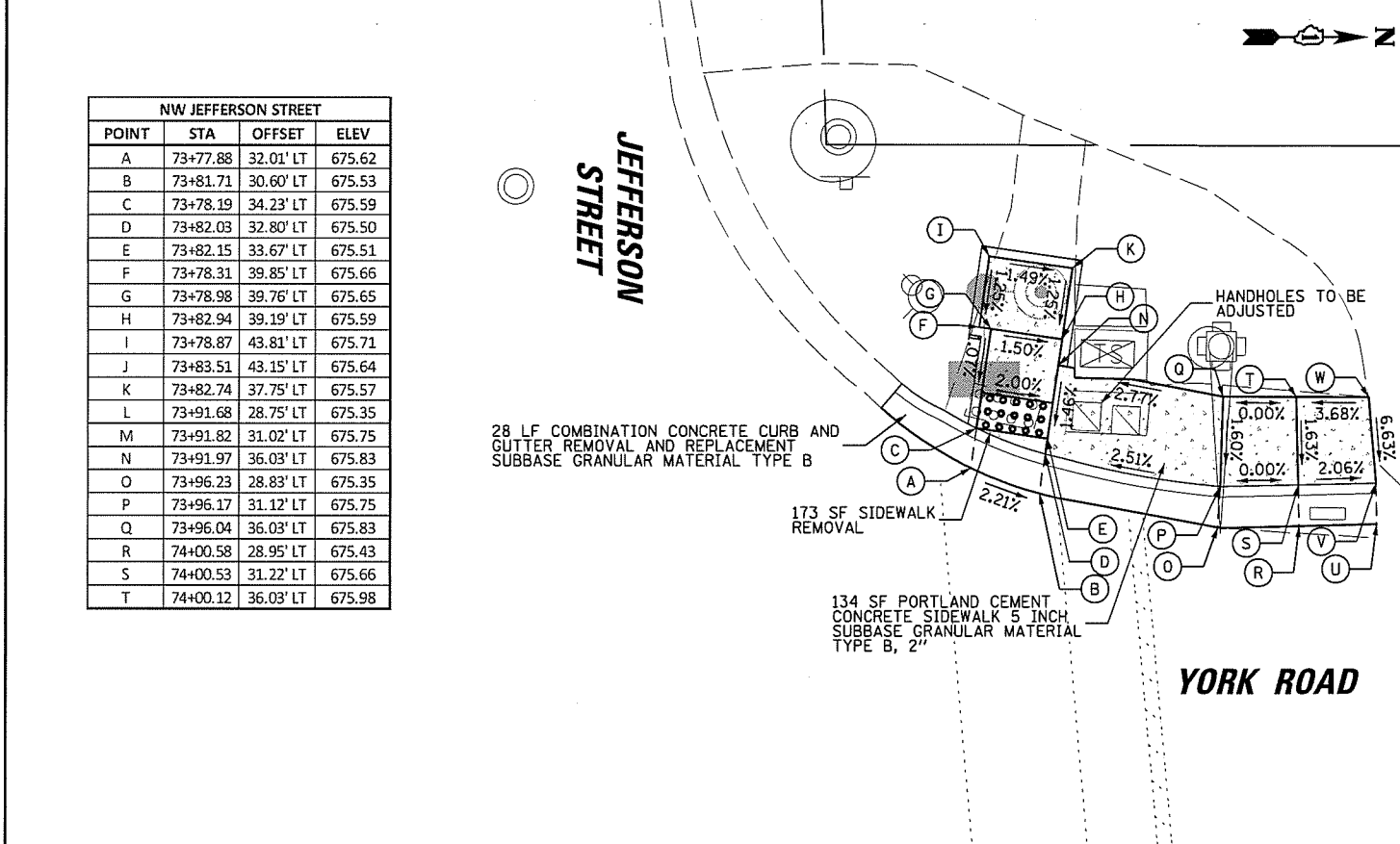
SE GEORGE ST			
POINT	STA	OFFSET	ELEV
A	55+81.96	36.02' RT	680.70
B	55+84.58	40.53' RT	680.65
C	55+78.93	36.02' RT	680.63
D	55+82.09	40.52' RT	680.58
E	55+78.92	40.52' RT	680.63
F	55+73.93	36.00' RT	680.86
G	55+73.92	40.50' RT	680.87
H	55+66.35	35.99' RT	681.22
I	55+66.34	40.49' RT	681.23
J	55+66.36	31.80' RT	681.28
K	55+61.35	35.98' RT	681.29
L	55+61.34	40.48' RT	681.30
M	55+61.36	31.79' RT	681.35
N	55+56.35	36.15' RT	681.36
O	55+56.34	40.39' RT	681.37



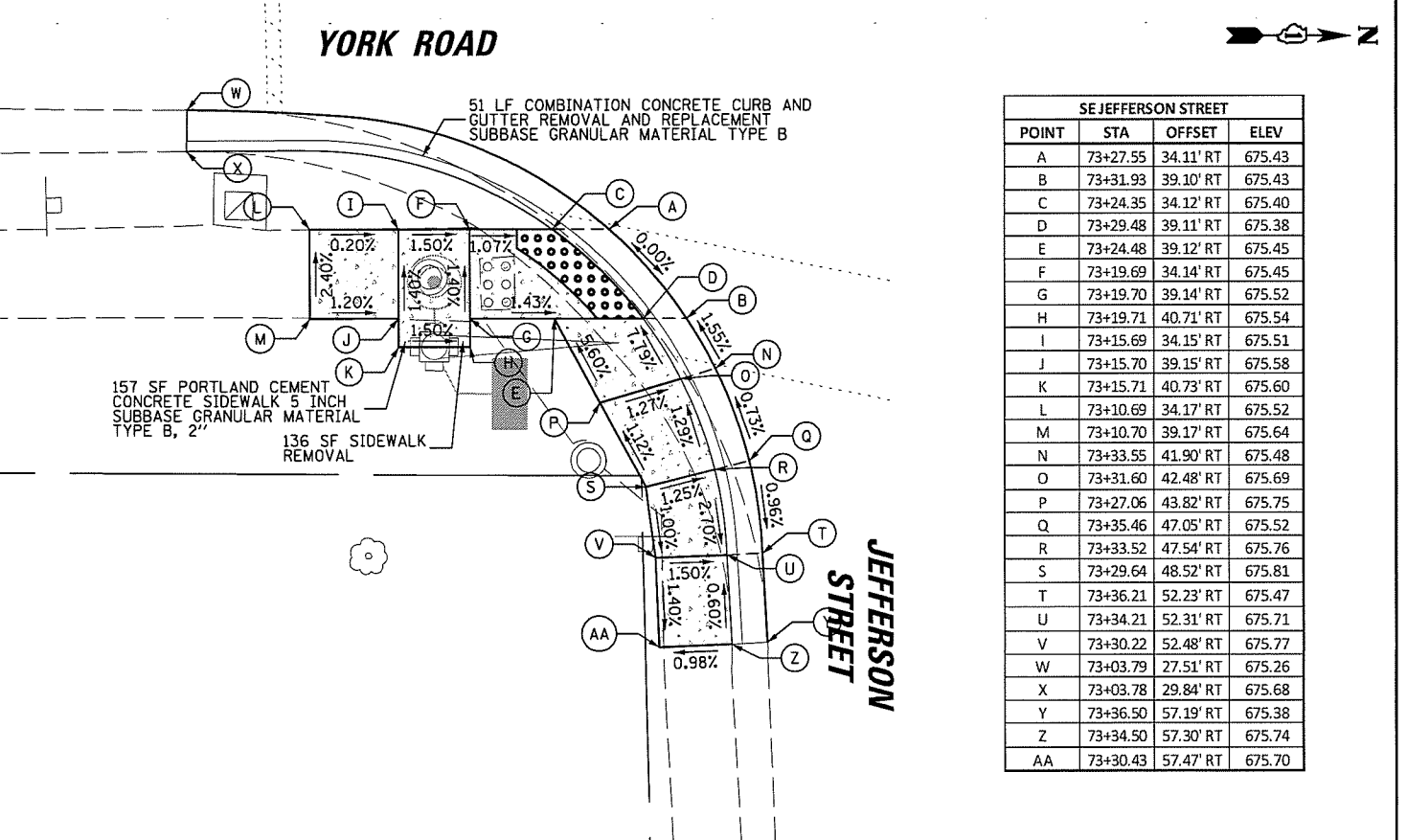
W GEORGE STREET			
POINT	STA	OFFSET	ELEV
A	56+29.53	27.24' LT	681.51
B	56+34.86	27.27' LT	681.57
C	56+29.49	29.68' LT	681.46
D	56+34.82	29.68' LT	681.52
E	56+29.37	37.00' LT	681.57
F	56+34.70	37.00' LT	681.63
G	56+45.18	29.67' LT	682.03
H	56+45.06	36.99' LT	682.14
I	56+45.19	27.32' LT	681.65



NE GEORGE STREET			
POINT	STA	OFFSET	ELEV
A	56+37.15	29.06' RT	681.04
B	56+25.23	40.97' RT	680.87
C	56+37.14	31.38' RT	681.02
D	56+27.69	40.98' RT	680.87
E	56+37.13	36.50' RT	681.02
F	56+30.23	41.86' RT	680.92
G	56+37.13	37.00' RT	681.02
H	56+32.11	41.86' RT	680.95
I	56+37.12	41.00' RT	681.05
J	56+37.12	41.87' RT	681.05
K	56+37.52	56.87' RT	681.67
L	56+32.86	57.04' RT	681.61
M	56+37.32	51.87' RT	681.61
N	56+32.55	52.04' RT	681.54
O	56+42.96	31.41' RT	681.11
P	56+42.95	35.78' RT	681.11
Q	56+43.45	36.23' RT	681.32
R	56+49.63	36.24' RT	681.60
S	56+49.62	41.03' RT	681.67

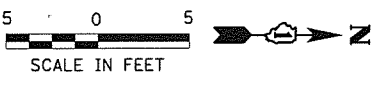
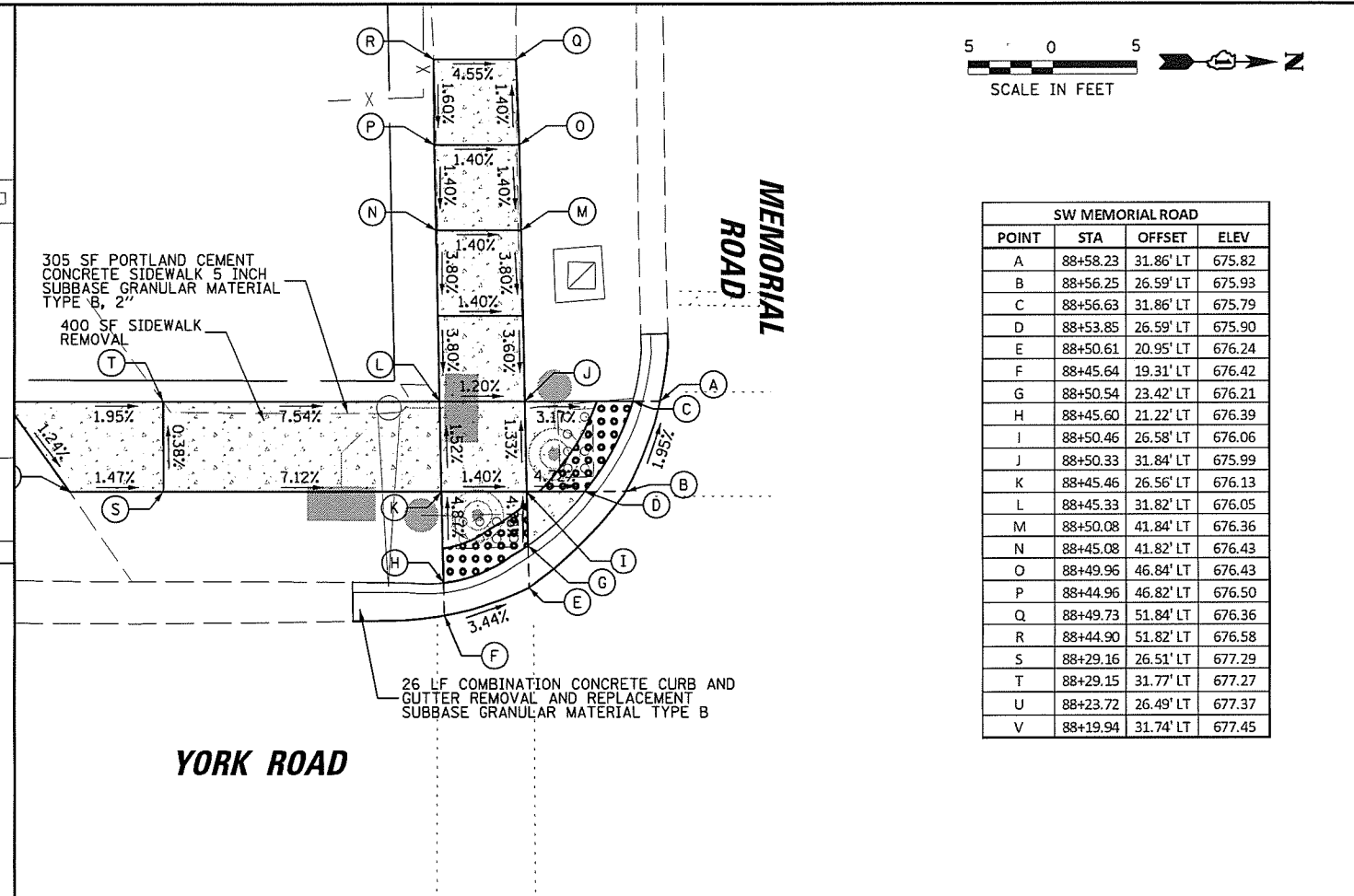
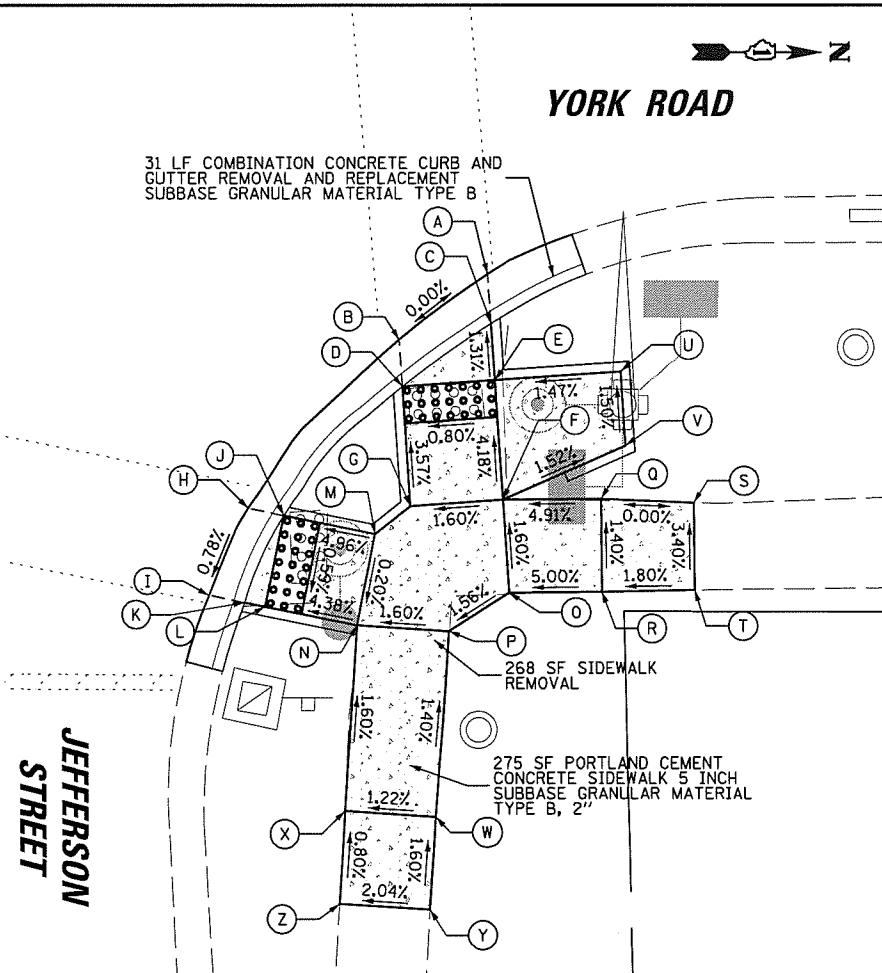


NW JEFFERSON STREET			
POINT	STA	OFFSET	ELEV
A	73+77.88	32.01' LT	675.62
B	73+81.71	30.60' LT	675.53
C	73+78.19	34.23' LT	675.59
D	73+82.03	32.80' LT	675.50
E	73+82.15	33.67' LT	675.51
F	73+78.31	39.85' LT	675.66
G	73+78.98	39.76' LT	675.65
H	73+82.94	39.19' LT	675.59
I	73+78.87	43.81' LT	675.71
J	73+83.51	43.15' LT	675.64
K	73+82.74	37.75' LT	675.57
L	73+91.68	28.75' LT	675.35
M	73+91.82	31.02' LT	675.75
N	73+91.97	36.03' LT	675.83
O	73+96.23	28.83' LT	675.35
P	73+96.17	31.12' LT	675.75
Q	73+96.04	36.03' LT	675.83
R	74+00.58	28.95' LT	675.43
S	74+00.53	31.22' LT	675.66
T	74+00.12	36.03' LT	675.98



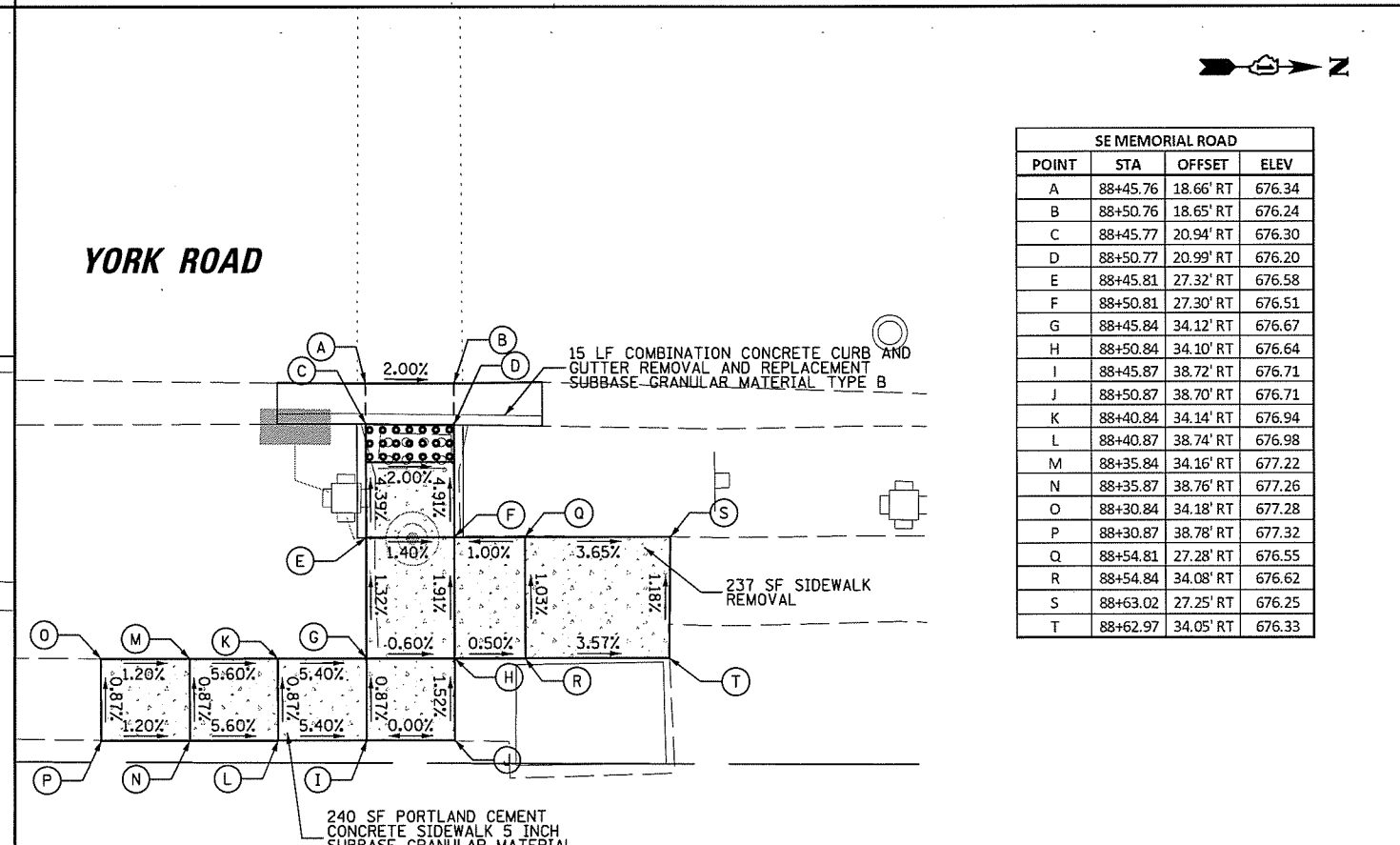
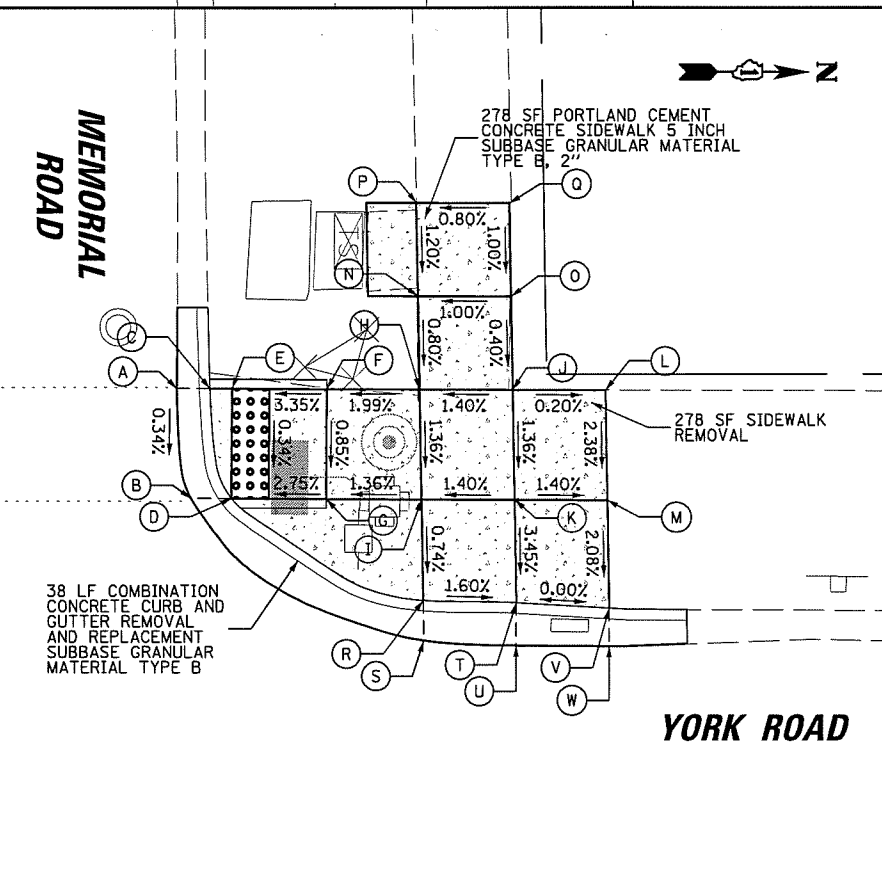
SE JEFFERSON STREET			
POINT	STA	OFFSET	ELEV
A	73+27.55	34.11' RT	675.43
B	73+31.93	39.10' RT	675.43
C	73+24.35	34.12' RT	675.40
D	73+29.48	39.11' RT	675.38
E	73+24.48	39.12' RT	675.45
F	73+19.69	34.14' RT	675.45
G	73+19.70	39.14' RT	675.52
H	73+19.71	40.71' RT	675.54
I	73+15.69	34.15' RT	675.51
J	73+15.70	39.15' RT	675.58
K	73+15.71	40.73' RT	675.60
L	73+10.69	34.17' RT	675.52
M	73+10.70	39.17' RT	675.64
N	73+33.55	41.90' RT	675.48
O	73+31.60	42.48' RT	675.69
P	73+27.06	43.82' RT	675.75
Q	73+35.46	47.05' RT	675.52
R	73+33.52	47.54' RT	675.76
S	73+29.64	48.52' RT	675.81
T	73+36.21	52.23' RT	675.47
U	73+34.21	52.31' RT	675.71
V	73+30.22	52.48' RT	675.77
W	73+03.79	27.51' RT	675.26
X	73+03.78	29.84' RT	675.68
Y	73+36.50	57.19' RT	675.38
Z	73+34.50	57.30' RT	675.74
AA	73+30.43	57.47' RT	675.70

NE JEFFERSON STREET			
POINT	STA	OFFSET	ELEV
A	73+88.05	31.88' RT	675.46
B	73+83.29	35.42' RT	675.46
C	73+88.24	34.53' RT	675.40
D	73+83.46	37.93' RT	675.40
E	73+88.45	37.57' RT	675.44
F	73+88.91	44.01' RT	675.71
G	73+83.92	44.36' RT	675.63
H	73+75.16	44.51' RT	675.45
I	73+73.08	49.19' RT	675.41
J	73+77.07	44.89' RT	675.41
K	73+74.99	49.57' RT	675.38
L	73+76.09	49.79' RT	675.40
M	73+82.01	45.88' RT	675.66
N	73+81.03	50.78' RT	675.65
O	73+89.26	49.00' RT	675.79
P	73+86.02	51.08' RT	675.73
Q	73+94.20	43.94' RT	675.97
R	73+94.26	48.94' RT	676.04
S	73+99.20	44.14' RT	675.97
T	73+99.26	48.84' RT	676.13
U	73+95.23	37.09' RT	675.54
V	73+95.52	41.08' RT	675.60
W	73+85.35	61.06' RT	675.87
X	73+80.43	60.76' RT	675.81
Y	73+85.06	66.05' RT	675.95
Z	73+80.16	65.76' RT	675.85

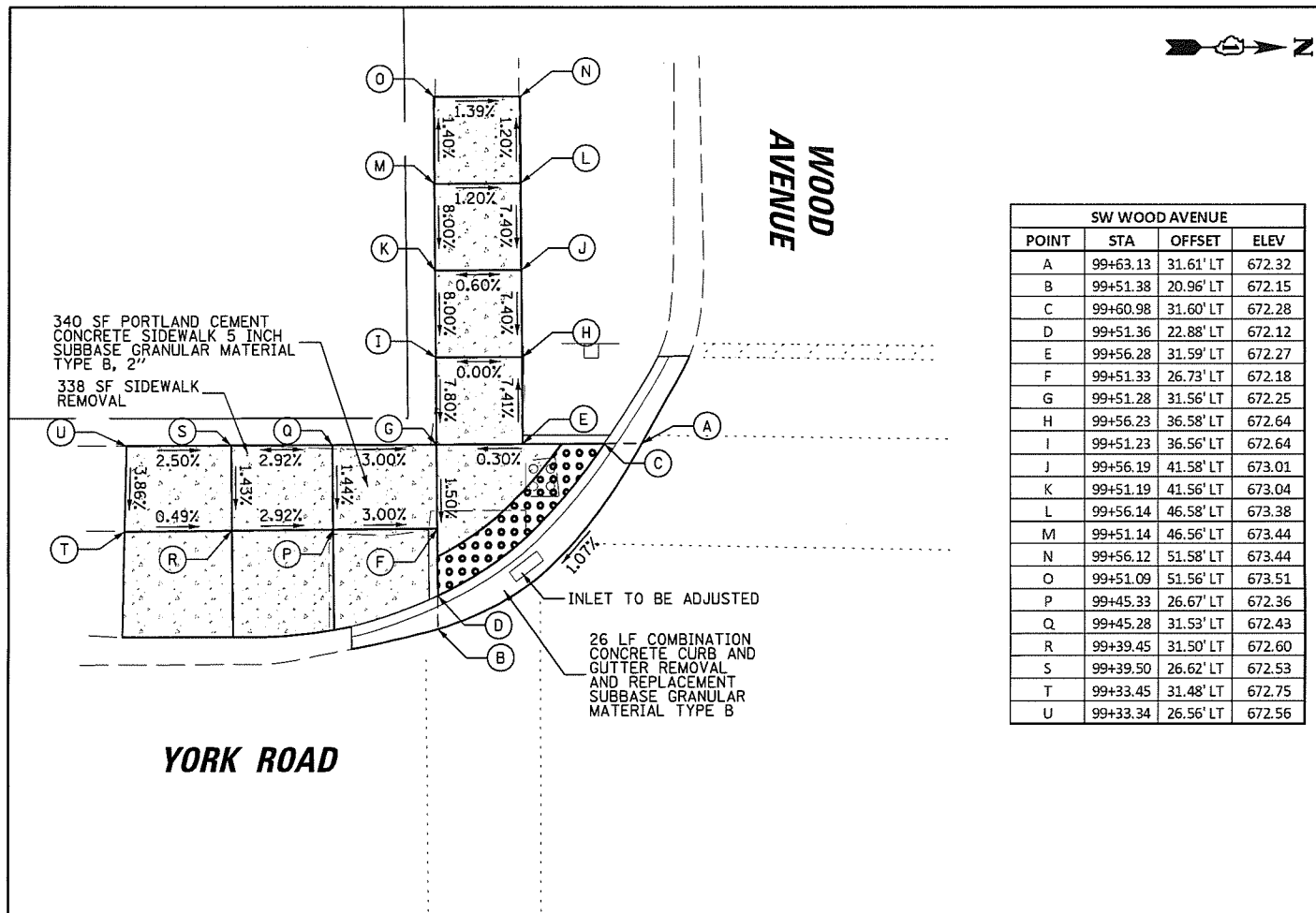


SW MEMORIAL ROAD			
POINT	STA	OFFSET	ELEV
A	88+58.23	31.86' LT	675.82
B	88+56.25	26.59' LT	675.93
C	88+56.63	31.86' LT	675.79
D	88+53.85	26.59' LT	675.90
E	88+50.61	20.95' LT	676.24
F	88+45.64	19.31' LT	676.42
G	88+50.54	23.42' LT	676.21
H	88+45.60	21.22' LT	676.39
I	88+50.46	26.58' LT	676.06
J	88+50.33	31.84' LT	675.99
K	88+45.46	26.56' LT	676.13
L	88+45.33	31.82' LT	676.05
M	88+50.08	41.84' LT	676.36
N	88+45.08	41.82' LT	676.43
O	88+49.96	46.84' LT	676.43
P	88+44.96	46.82' LT	676.50
Q	88+49.73	51.84' LT	676.36
R	88+44.90	51.82' LT	676.58
S	88+29.16	26.51' LT	677.29
T	88+29.15	31.77' LT	677.27
U	88+23.72	26.49' LT	677.37
V	88+19.94	31.74' LT	677.45

NW MEMORIAL ROAD			
POINT	STA	OFFSET	ELEV
A	88+88.91	32.41' LT	675.25
B	88+89.66	26.52' LT	675.23
C	88+90.68	32.40' LT	675.22
D	88+91.85	26.51' LT	675.20
E	88+91.89	32.40' LT	675.22
F	88+96.97	32.37' LT	675.39
G	88+96.94	26.48' LT	675.34
H	89+01.99	32.34' LT	675.49
I	89+02.09	26.46' LT	675.41
J	89+06.99	32.31' LT	675.56
K	89+07.09	26.43' LT	675.48
L	89+11.99	32.28' LT	675.55
M	89+12.09	26.40' LT	675.41
N	89+01.91	37.34' LT	675.53
O	89+06.91	37.31' LT	675.58
P	89+01.82	42.34' LT	675.59
Q	89+06.83	42.31' LT	675.63
R	89+02.18	21.04' LT	675.37
S	89+02.21	18.89' LT	674.96
T	89+07.18	20.92' LT	675.29
U	89+07.16	18.62' LT	674.88
V	89+12.18	20.62' LT	675.29
W	89+12.17	18.58' LT	674.83

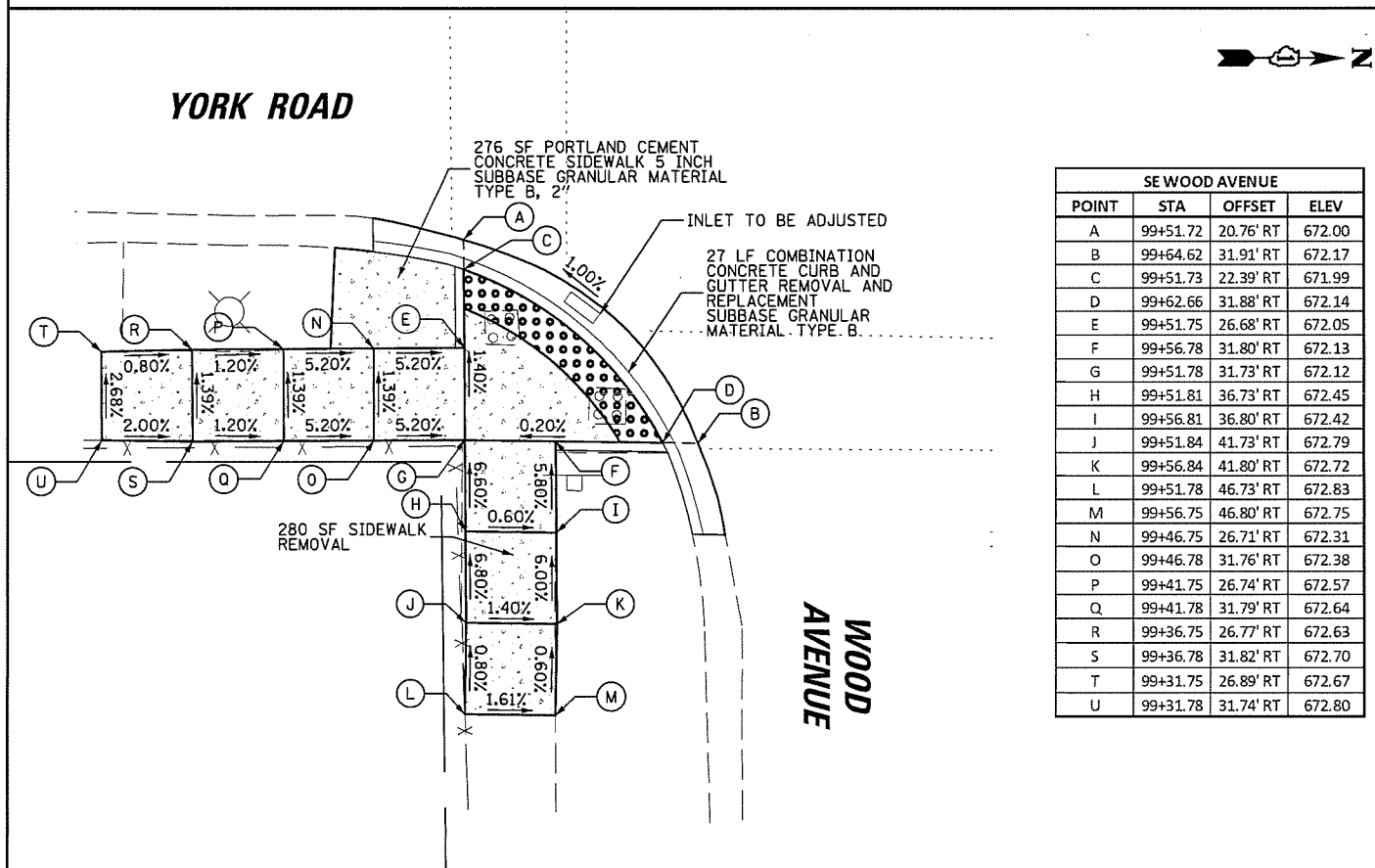
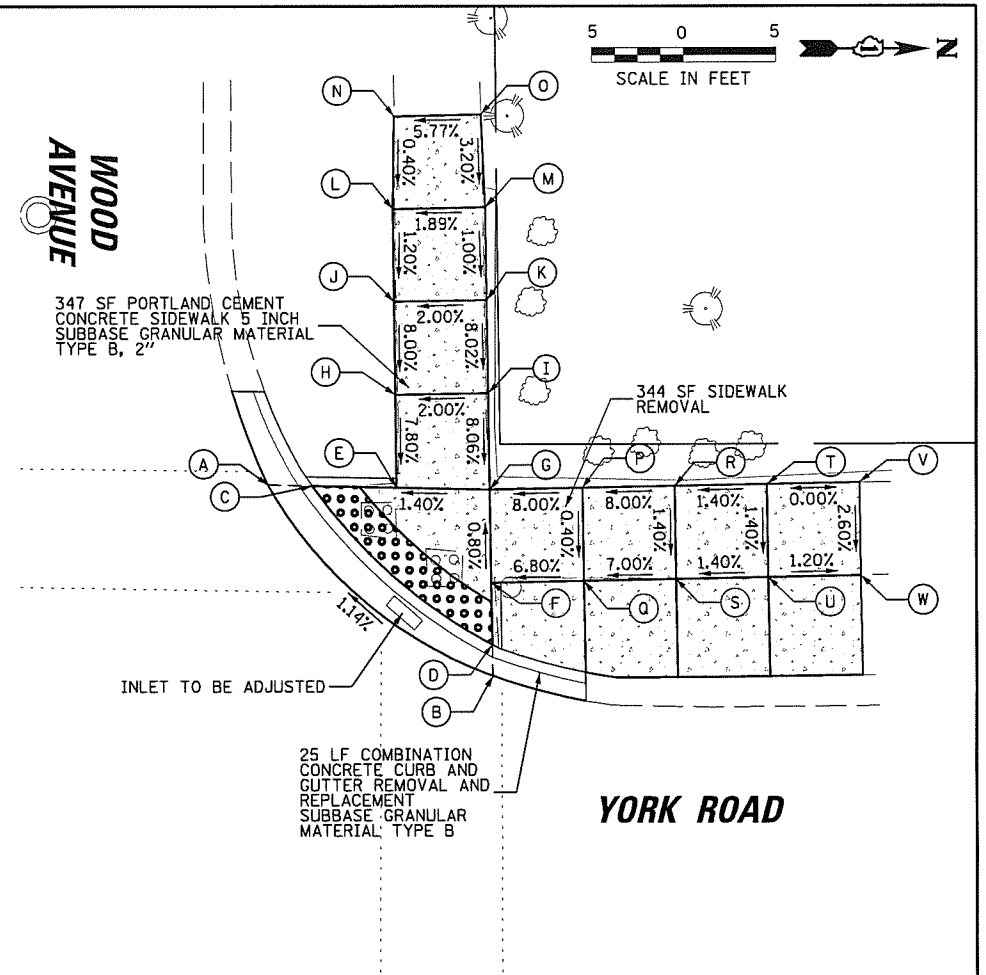


SE MEMORIAL ROAD			
POINT	STA	OFFSET	ELEV
A	88+45.76	18.66' RT	676.34
B	88+50.76	18.65' RT	676.24
C	88+45.77	20.94' RT	676.30
D	88+50.77	20.99' RT	676.20
E	88+45.81	27.32' RT	676.58
F	88+50.81	27.30' RT	676.51
G	88+45.84	34.12' RT	676.67
H	88+50.84	34.10' RT	676.64
I	88+45.87	38.72' RT	676.71
J	88+50.87	38.70' RT	676.71
K	88+40.84	34.14' RT	676.94
L	88+40.87	38.74' RT	676.98
M	88+35.84	34.16' RT	677.22
N	88+35.87	38.76' RT	677.26
O	88+30.84	34.18' RT	677.28
P	88+30.87	38.78' RT	677.32
Q	88+54.81	27.28' RT	676.55
R	88+54.84	34.08' RT	676.62
S	88+63.02	27.25' RT	676.25
T	88+62.97	34.05' RT	676.33

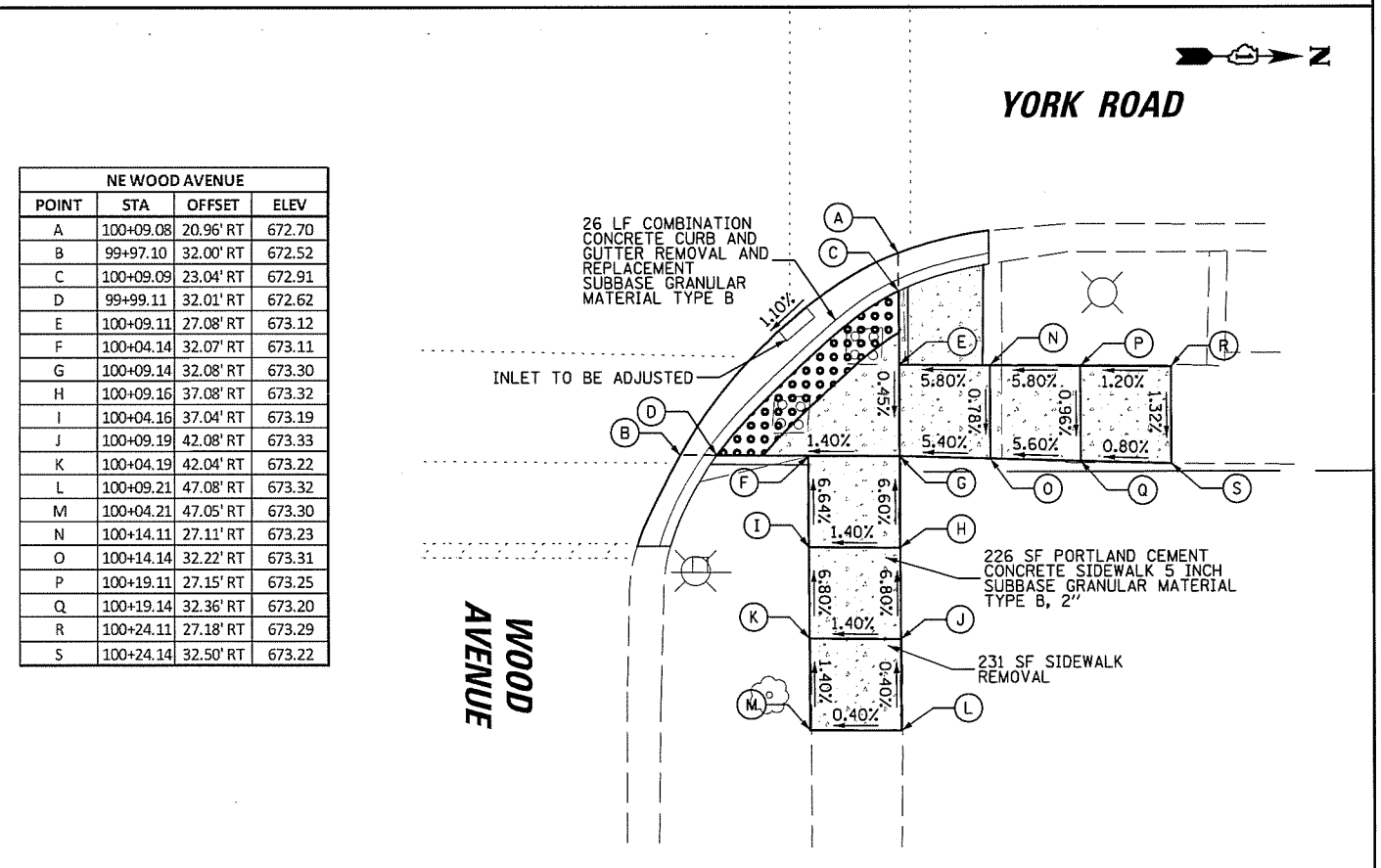


SW WOOD AVENUE			
POINT	STA	OFFSET	ELEV
A	99+63.13	31.61' LT	672.32
B	99+51.38	20.96' LT	672.15
C	99+60.98	31.60' LT	672.28
D	99+51.36	22.88' LT	672.12
E	99+56.28	31.59' LT	672.27
F	99+51.33	26.73' LT	672.18
G	99+51.28	31.56' LT	672.25
H	99+56.23	36.58' LT	672.64
I	99+51.23	36.56' LT	672.64
J	99+56.19	41.58' LT	673.01
K	99+51.19	41.56' LT	673.04
L	99+56.14	46.58' LT	673.38
M	99+51.14	46.56' LT	673.44
N	99+56.12	51.58' LT	673.44
O	99+51.09	51.56' LT	673.51
P	99+45.33	26.67' LT	672.36
Q	99+45.28	31.53' LT	672.43
R	99+39.45	31.50' LT	672.60
S	99+39.50	26.62' LT	672.53
T	99+33.45	31.48' LT	672.75
U	99+33.34	26.56' LT	672.56

NW WOOD AVENUE			
POINT	STA	OFFSET	ELEV
A	99+97.18	30.90' LT	672.52
B	100+09.25	20.62' LT	672.70
C	99+99.56	30.85' LT	672.47
D	100+09.23	22.30' LT	672.69
E	100+04.11	30.74' LT	672.53
F	100+09.18	25.63' LT	672.64
G	100+09.11	30.63' LT	672.60
H	100+04.04	35.74' LT	672.92
I	100+09.04	35.84' LT	673.02
J	100+03.97	40.74' LT	673.32
K	100+08.97	40.83' LT	673.42
L	100+04.01	45.74' LT	673.38
M	100+08.78	45.83' LT	673.47
N	100+03.97	50.74' LT	673.36
O	100+08.65	50.83' LT	673.63
P	100+14.11	30.72' LT	673.00
Q	100+14.18	25.72' LT	672.98
R	100+19.11	30.81' LT	673.40
S	100+19.18	25.82' LT	673.33
T	100+24.11	30.91' LT	673.47
U	100+24.18	25.91' LT	673.40
V	100+29.11	31.00' LT	673.47
W	100+29.18	26.00' LT	673.34



SE WOOD AVENUE			
POINT	STA	OFFSET	ELEV
A	99+51.72	20.76' RT	672.00
B	99+64.62	31.91' RT	672.17
C	99+51.73	22.39' RT	671.99
D	99+62.66	31.88' RT	672.14
E	99+51.75	26.68' RT	672.05
F	99+56.78	31.80' RT	672.13
G	99+51.78	31.73' RT	672.12
H	99+51.81	36.73' RT	672.45
I	99+56.81	36.80' RT	672.42
J	99+51.84	41.73' RT	672.79
K	99+56.84	41.80' RT	672.72
L	99+51.78	46.73' RT	672.83
M	99+56.75	46.80' RT	672.75
N	99+46.75	26.71' RT	672.31
O	99+46.78	31.76' RT	672.38
P	99+41.75	26.74' RT	672.57
Q	99+41.78	31.79' RT	672.64
R	99+36.75	26.77' RT	672.63
S	99+36.78	31.82' RT	672.70
T	99+31.75	26.89' RT	672.67
U	99+31.78	31.74' RT	672.80



NE WOOD AVENUE			
POINT	STA	OFFSET	ELEV
A	100+09.08	20.96' RT	672.70
B	99+97.10	32.00' RT	672.52
C	100+09.09	23.04' RT	672.91
D	99+99.11	32.01' RT	672.62
E	100+09.11	27.08' RT	673.12
F	100+04.14	32.07' RT	673.11
G	100+09.14	32.08' RT	673.30
H	100+09.16	37.08' RT	673.32
I	100+04.16	37.04' RT	673.19
J	100+09.19	42.08' RT	673.33
K	100+04.19	42.04' RT	673.22
L	100+09.21	47.08' RT	673.32
M	100+04.21	47.05' RT	673.30
N	100+14.11	27.11' RT	673.23
O	100+14.14	32.22' RT	673.31
P	100+19.11	27.15' RT	673.25
Q	100+19.14	32.36' RT	673.20
R	100+24.11	27.18' RT	673.29
S	100+24.14	32.50' RT	673.22

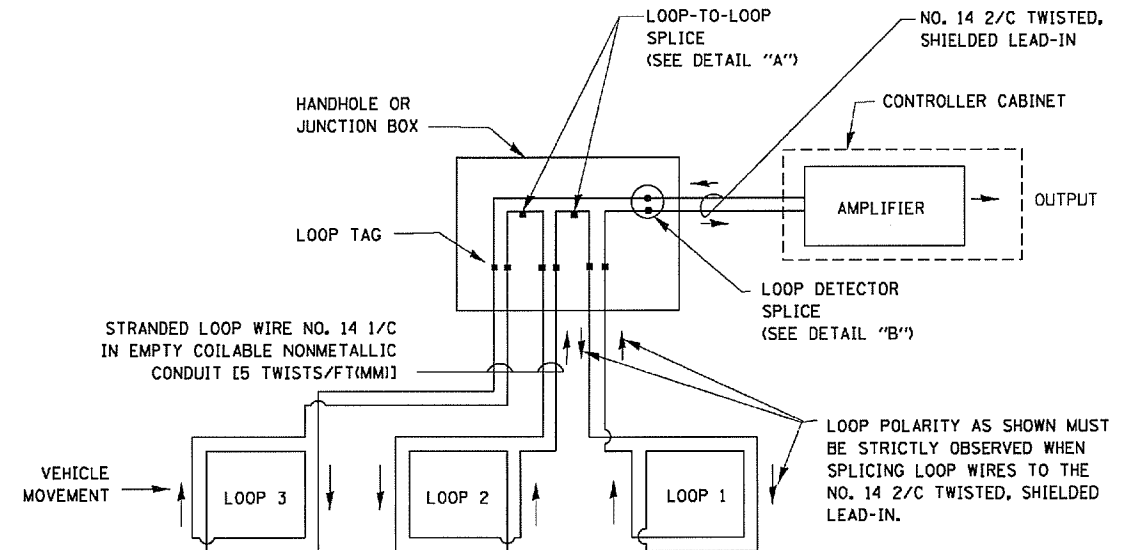
TRAFFIC SIGNAL LEGEND

(NOT TO SCALE)

ITEM	EXISTING	PROPOSED	ITEM	EXISTING	PROPOSED	ITEM	EXISTING	PROPOSED
CONTROLLER CABINET			HANDHOLE -SQUARE			SIGNAL HEAD		
COMMUNICATION CABINET			-ROUND			- (P) PROGRAMMABLE SIGNAL HEAD		
MASTER CONTROLLER			HEAVY DUTY HANDHOLE -SQUARE					
MASTER MASTER CONTROLLER			-ROUND			SIGNAL HEAD WITH BACKPLATE		
UNINTERRUPTABLE POWER SUPPLY			DOUBLE HANDHOLE			- (P) PROGRAMMABLE SIGNAL HEAD		
SERVICE INSTALLATION - (P) POLE MOUNTED			JUNCTION BOX			- (RB) RETROREFLECTIVE BACKPLATE		
SERVICE INSTALLATION - (G) GROUND MOUNTED			RAILROAD CANTILEVER MAST ARM					
- (GM) GROUND MOUNTED METERED			RAILROAD FLASHING SIGNAL			PEDESTRIAN SIGNAL HEAD AT RAILROAD INTERSECTIONS		
TELEPHONE CONNECTION			RAILROAD CROSSING GATE			PEDESTRIAN SIGNAL HEAD WITH COUNTDOWN TIMER		
STEEL MAST ARM ASSEMBLY AND POLE			RAILROAD CROSSBUCK			ILLUMINATED SIGN "NO LEFT TURN"/"NO RIGHT TURN"		
ALUMINUM MAST ARM ASSEMBLY AND POLE			RAILROAD CONTROLLER CABINET			NUMBER OF CONDUCTORS, ELECTRIC CABLE NO. 14, UNLESS NOTED OTHERWISE. ALL DETECTOR LOOP CABLE TO BE SHIELDED		
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE WITH LUMINAIRE			UNDERGROUND CONDUIT (UC), GALVANIZED STEEL			GROUND CABLE IN CONDUIT, NO. 6 SOLID COPPER (GREEN)		
SIGNAL POST - (BM) BARREL MOUNTED - TEMPORARY			TEMPORARY SPAN WIRE, TETHER WIRE, AND CABLE			ELECTRIC CABLE IN CONDUIT, TRACER NO. 14 1/C		
WOOD POLE			SYSTEM ITEM			COAXIAL CABLE		
GUY WIRE			INTERSECTION ITEM			VENDOR CABLE		
SIGNAL HEAD			REMOVE ITEM			COPPER INTERCONNECT CABLE, NO. 18, 3 PAIR TWISTED, SHIELDED		
SIGNAL HEAD WITH BACKPLATE			RELOCATE ITEM			FIBER OPTIC CABLE -NO. 62.5/125, MM12F		
SIGNAL HEAD OPTICALLY PROGRAMMED			ABANDON ITEM			-NO. 62.5/125, MM12F SM12F		
FLASHER INSTALLATION - (FS) SOLAR POWERED			CONTROLLER CABINET AND FOUNDATION TO BE REMOVED			-NO. 62.5/125, MM12F SM24F		
PEDESTRIAN SIGNAL HEAD			MAST ARM POLE AND FOUNDATION TO BE REMOVED			GROUND ROD		
PEDESTRIAN PUSH BUTTON - (APS) ACCESSIBLE PEDESTRIAN PUSH BUTTON			SIGNAL POST AND FOUNDATION TO BE REMOVED			- (C) CONTROLLER		
RADAR DETECTION SENSOR			DETECTOR LOOP, TYPE I			- (M) MAST ARM		
VIDEO DETECTION CAMERA			PREFORMED DETECTOR LOOP			- (P) POST		
RADAR/VIDEO DETECTION ZONE			SAMPLING (SYSTEM) DETECTOR			- (S) SERVICE		
PAN, TILT, ZOOM (PTZ) CAMERA			INTERSECTION AND SAMPLING (SYSTEM) DETECTOR					
EMERGENCY VEHICLE LIGHT DETECTOR			QUEUE AND SAMPLING (SYSTEM) DETECTOR					
CONFIRMATION BEACON			WIRELESS DETECTOR SENSOR					
WIRELESS INTERCONNECT			WIRELESS ACCESS POINT					
WIRELESS INTERCONNECT RADIO REPEATER								

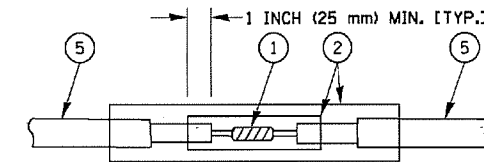
LOOP DETECTOR NOTES

1. EACH PAIR OF LOOP WIRES SHALL BE PLACED IN A SEPARATE EMPTY COILABLE NONMETALLIC CONDUIT FROM THE EDGE OF PAVEMENT TO THE HANDHOLE. SPACING BETWEEN THE HOLES DRILLED IN THE PAVEMENT SHALL NOT BE LESS THAN 6" (150 mm). EMPTY COILABLE NONMETALLIC CONDUIT SHALL BE INCLUDED IN THE COST OF THE LOOP WIRE.
2. THE NUMBER OF LOOP TURNS SHALL BE AS RECOMMENDED BY THE AMPLIFIER MANUFACTURER. ALL ADJACENT SIDES OF THE LOOPS SHALL BE INSTALLED IN SUCH A WAY THAT THE CURRENT FLOW IS IN THE SAME DIRECTION TO REINFORCE ITS MAGNETIC FIELDS FOR SMALL VEHICLE DETECTION.
3. EACH LOOP LEAD-IN SHALL BE IDENTIFIED AND PERMANENTLY TAGGED IN THE HANDHOLE. EACH LEAD-IN CABLE TAG SHALL INDICATE THE LOCATION OF THE LOOP, LOOP ROTATION (CLOCKWISE/COUNTERCLOCKWISE), LOOP LEAD-IN DIRECTION (IN OR OUT), LOOP CABLE NUMBER AND LOCATION IN CABINET, AND NUMBER OF TURNS IN THE DETECTOR LOOPS IN WATER PROOF INK AS INDICATED ON THE DISTRICT 1 STANDARD TRAFFIC SIGNAL DESIGN DETAIL. THE CONTRACTOR SHALL MARK LOOP LOCATIONS ON RECORD DRAWINGS AND PRESENT TO THE ENGINEER AFTER FINAL INSPECTION. LOOPS SHALL BE MARKED BY LANE AND LOOP NUMBER. SEE DETAIL BELOW.
4. ALL LOOP CABLE SHALL BE FASTENED WITH PLASTIC TIE WRAP TO THE HANDHOLE HOOKS.
5. IN ASPHALT PAVEMENT, LOOPS SHOULD BE PLACED IN THE BINDER AND DIVEHOLES MARKED AT THE CURB WITH A SAW-CUT. THE SAW-CUT SHALL BE CUT IN ACCORDANCE WITH LOCAL AND E.P.A. DUST CONTROL REQUIREMENTS. DETECTOR LOOP(S) SHALL NOT BE INSTALLED IN WET CONDITIONS AND THE SAW-CUTS MUST BE FREE OF DEBRIS AND RESIDUE SUCH AS DUST AND WATER WHICH IS TO BE ACHIEVED BY THE USE OF COMPRESSED AIR, WIRE BRUSHING AND HEAT DRYING ACCORDING TO SEALANT MANUFACTURER REQUIREMENTS. THE DETECTOR WIRE SHALL BE HELD IN PLACE BY THE USE OF FORM WEDGES. WEDGES SHALL BE SPACED NO MORE THAN 18" (450 mm) APART.
6. LOOP SPLICES SHALL BE SOLDERED USING A SOLDERING IRON. BLOW TORCHES OR OTHER DEVICES WHICH OXIDIZE COPPER CABLE SHALL NOT BE ALLOWED FOR SOLDERING OPERATIONS. SEE DETAIL BELOW RIGHT.
7. PREFORMED DETECTOR LOOPS SHALL BE USED, AS SHOWN ON THE PLANS, WHERE NEW CONCRETE PAVEMENT IS PROPOSED. THE INSTALLATION OF PREFORMED LOOPS SHALL BE IN ACCORDANCE WITH THE DISTRICT 1 SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER.

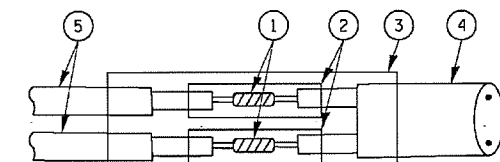


DETECTOR LOOP WIRING SCHEMATIC

- LOOPS SHALL BE SPLICED IN SERIES.
- SAW-CUTS SHALL BE A MINIMUM WIDTH OF 5/16" (8 mm).
- SAW-CUT DEPTHS SHALL BE 3" (75 mm). IF IN CONCRETE, THE SAW-CUT DEPTH SHALL BE TO THE TOP OF THE REINFORCEMENT.
- LOOP CORNERS SHALL BE DRILLED WITH A 2" (50 mm) DIAMETER CORE.

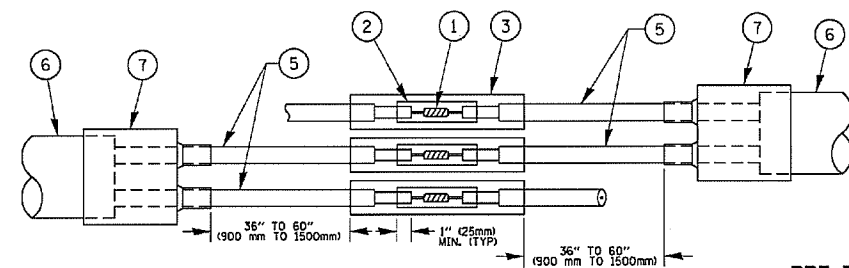


DETAIL "A"
LOOP-TO-LOOP SPLICE

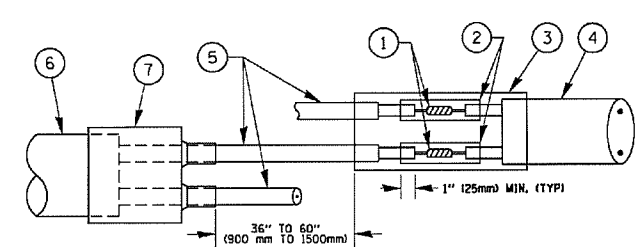


DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

TYPE I LOOP



DETAIL "A"
LOOP-TO-LOOP SPLICE



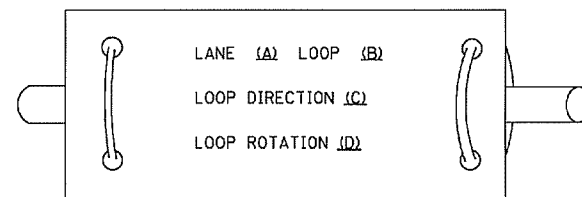
DETAIL "B"
LOOP-TO-CONTROLLER SPLICE

PRE-FORMED LOOP

LOOP DETECTOR SPLICE

- ① WESTERN UNION SPLICE SOLDERED WITH ROSIN CORE FLUX. ALL EXPOSED SURFACES OF THE SOLDER SHALL BE SMOOTH. THE WESTERN UNION SPLICES SHALL BE STAGGERED.
- ② WCSMW 30/100 HEAT SHRINK TUBE, MINIMUM LENGTH 3" (75 mm), UNDERWATER GRADE.
- ③ WCS 200/750 HEAT SHRINK TUBE, MINIMUM LENGTH 6" (150 mm), UNDERWATER GRADE.
- ④ NO. 14 2/C TWISTED, SHIELDED CABLE.
- ⑤ LOOP CONDUCTOR WITH FLEXIBLE PLASTIC TUBE.
- ⑥ PRE-FORMED LOOP
- ⑦ XL POLYOLEFIN 2 CONDUCTOR BREAKOUT SEALS. TYCO-CBR-2 OR APPROVED EQUAL

LOOP LEAD-IN CABLE TAG



- A. LANE 1 IS THE LANE CLOSEST TO THE CENTERLINE OF THE ROADWAY
- B. LOOP #1 IS THE LOOP IN THE LANE CLOSEST TO THE INTERSECTION.
- C. LABEL LOOP CABLE "IN" OR LOOP CABLE "OUT".
- D. LABEL LOOP CABLE CLOCKWISE OR LOOP CABLE COUNTERCLOCKWISE.

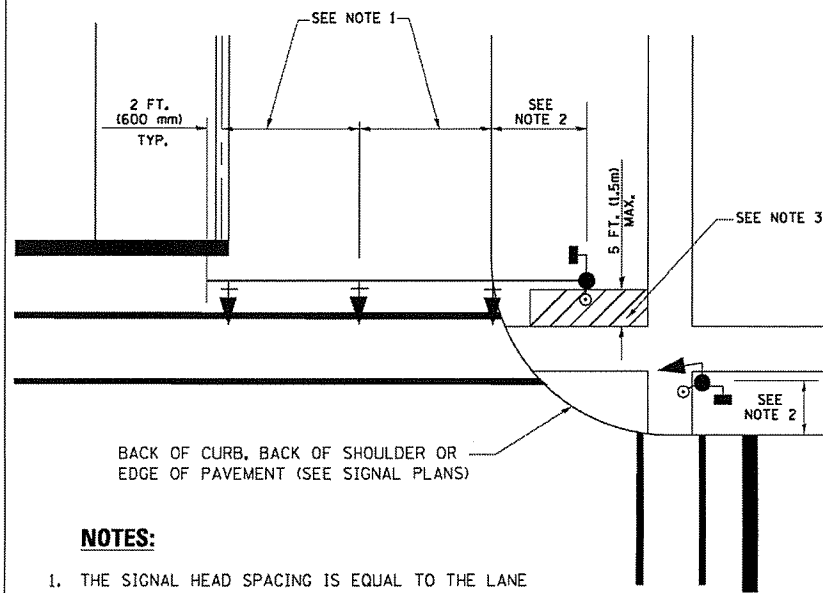
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PLOT SCALE = 58.0000' / in.		CHECKED - DAD	REVISED -
PLOT DATE = 1/13/2014		DATE - 10-28-09	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE	
STANDARD TRAFFIC SIGNAL DESIGN DETAILS	
SCALE: NONE	SHEET NO. 2 OF 7 SHEETS
STA.	TO STA.

F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2678	18-00098-00-RS	DUPAGE	45	18
TS-05			CONTRACT NO. 61F38	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

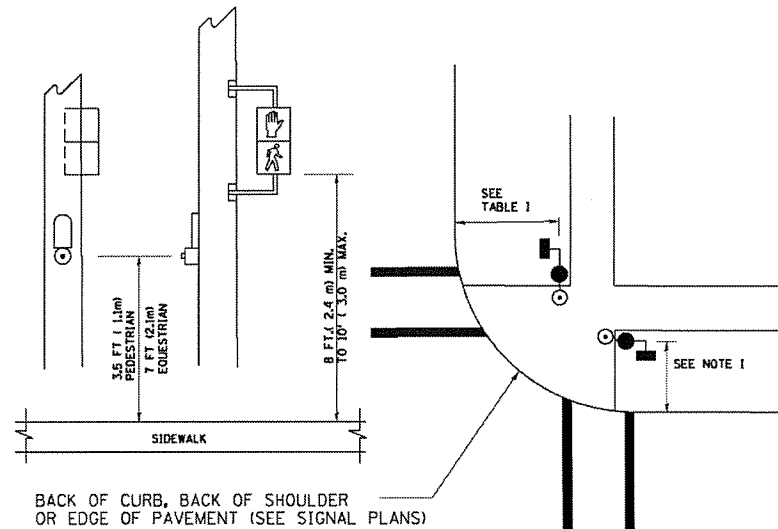
**TRAFFIC SIGNAL MAST ARM AND SIGNAL POST
MAST ARM MOUNTED SIGNALS IN EXISTING, PROPOSED OR
FUTURE SIDEWALK/BICYCLE PATH AREA. INTERSECTION SHOWN
WITH PEDESTRIAN SIGNALS AND PEDESTRIAN PUSHBUTTON DETECTORS.**



NOTES:

1. THE SIGNAL HEAD SPACING IS EQUAL TO THE LANE WIDTH OR AS SHOWN ON THE TRAFFIC SIGNAL PLAN.
2. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
3. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE MAST ARM SHAFT OR THE SIGNAL POST.
4. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
5. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

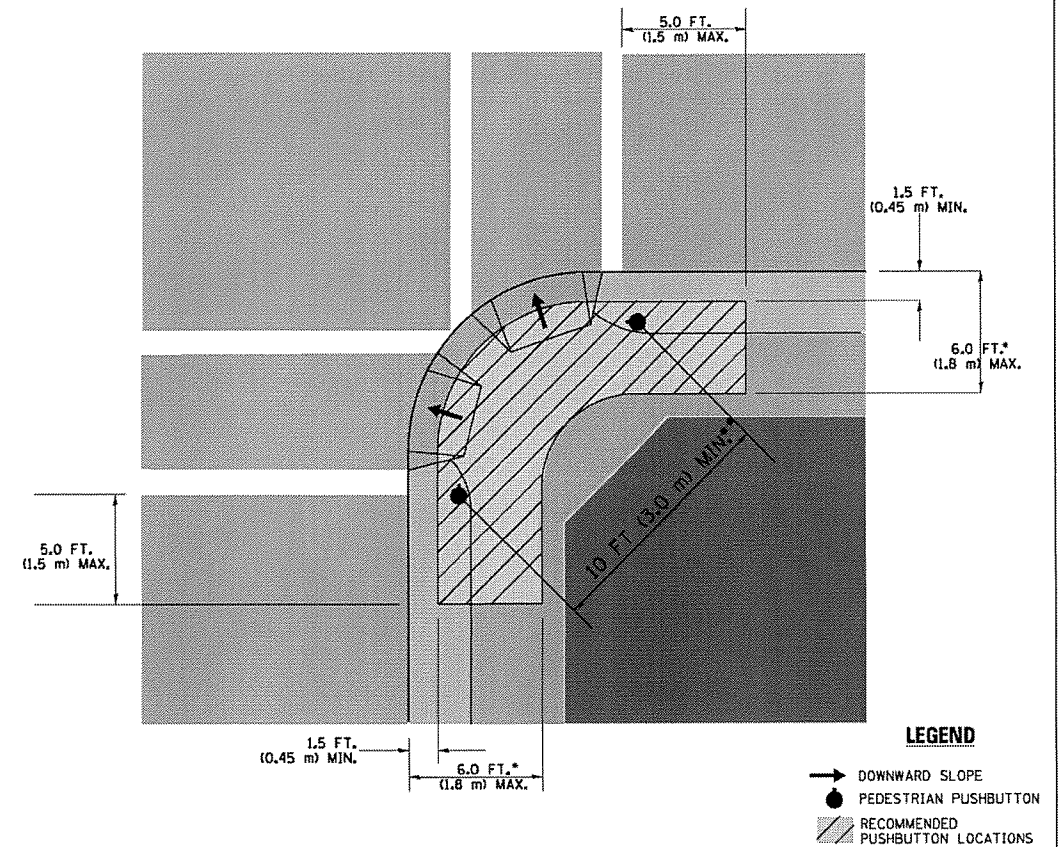
**PEDESTRIAN SIGNAL POST
AND
PEDESTRIAN PUSH BUTTON POST**



NOTES:

1. REFER TO THE TRAFFIC SIGNAL EQUIPMENT OFFSET TABLE.
2. PROVIDE A LEVEL ALL-WEATHER SURFACE (CONCRETE SIDEWALK, ASPHALT BICYCLE PATH SURFACE OR MATCHING MATERIAL TO THE ADJACENT SURFACE) UP TO THE PEDESTRIAN SIGNAL POST OR THE PEDESTRIAN PUSH BUTTON POST.
3. THE FACE OF THE PEDESTRIAN PUSHBUTTON SHALL BE PARALLEL TO THE CROSSWALK TO BE USED.
4. THE LOCATIONS AND INSTALLATION OF PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS SHALL MEET THE REQUIREMENTS OF THE MUTCD AND INFORMATION FOUND IN THE "AMERICANS WITH DISABILITIES ACT ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES."

RECOMMENDED PUSHBUTTON LOCATIONS



LEGEND

- DOWNWARD SLOPE
- PEDESTRIAN PUSHBUTTON
- ▨ RECOMMENDED PUSHBUTTON LOCATIONS

- WHERE THERE ARE CONSTRAINTS THAT MAKE IT IMPRACTICAL TO PLACE THE PEDESTRIAN PUSHBUTTON BETWEEN 1.5 FT (0.45 m) AND 6 FT (1.8 m) FROM THE EDGE OF THE CURB, SHOULDER, OR PAVEMENT, IT SHOULD NOT BE FURTHER THAN 10 FT (3 m) FROM THE EDGE OF CURB, SHOULDER, OR PAVEMENT.
- WHERE THERE ARE CONSTRAINTS ON A PARTICULAR CORNER THAT MAKE IT IMPRACTICAL TO PROVIDE THE 10 FT (3 m) SEPERATION BETWEEN THE TWO PEDESTRIAN PUSHBUTTONS, THE PUSHBUTTONS MAY BE PLACED CLOSER TOGETHER OR ON THE SAME POLE.

NOTES:

1. PEDESTRIAN SIGNAL HEADS SHALL BE MOUNTED WITH THE BOTTOM OF THE SIGNAL HOUSING INCLUDING BRACKETS NOT LESS THAN 8 FT (2.4 m) OR MORE THAN 10 FT (3 m) ABOVE SIDEWALK LEVEL, AND SHALL BE POSITIONED AND ADJUSTED TO PROVIDE MAXIMUM VISIBILITY AT THE BEGINNING OF THE CONTROLLED CROSSWALK.
2. THE BOTTOM OF THE SIGNAL HOUSING (INCLUDING BRACKETS) OF A VEHICULAR SIGNAL FACE THAT IS NOT LOCATED OVER A HIGHWAY SHALL BE AT LEAST 8 FT (2.4 m) BUT NOT MORE THAN 19 FT (5.8 m) ABOVE THE SIDEWALK OR, IF THERE IS NO SIDEWALK, ABOVE THE PAVEMENT GRADE AT THE CENTER OF THE ROADWAY.
3. THE BOTTOM OF THE SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARDS 877001, 877002, 877006, 877011 AND 877012 WITH A MINIMUM OF 16 FT (5.0 m) AND A MAXIMUM OF 18 FT. (5.5 m) FROM THE HIGHEST POINT OF PAVEMENT.
4. THE BOTTOM OF THE TEMPORARY SPAN WIRE MOUNTED SIGNAL HOUSING AND ANY RELATED ATTACHMENTS TO A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL BE ACCORDING TO CURRENT STATE STANDARD 880001 WITH A MINIMUM OF 17 FT (5.18 m) FROM THE HIGHEST POINT OF PAVEMENT.
5. THE TOP OF THE SIGNAL HOUSING OF A SIGNAL FACE LOCATED OVER ANY PORTION OF A HIGHWAY SHALL NOT BE MORE THAN 25.6 FT (7.8 m) ABOVE THE PAVEMENT.

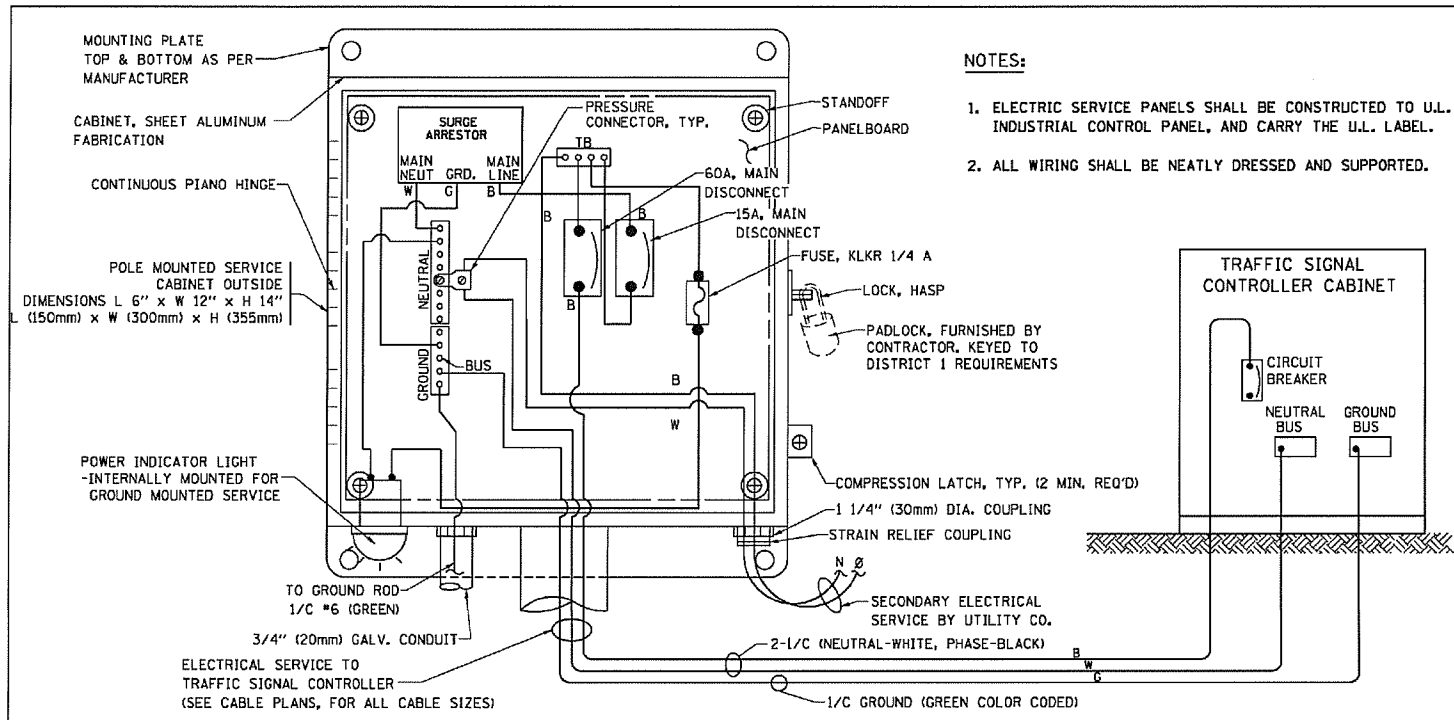
TRAFFIC SIGNAL EQUIPMENT OFFSET

TRAFFIC SIGNAL EQUIPMENT	COMBINATION CONCRETE CURB AND GUTTER (MINIMUM DISTANCE FROM BACK OF CURB TO CENTERLINE OF FOUNDATION)	SHOULDER/NON-CURBED AREA (MINIMUM DISTANCE FROM EDGE OF PAVEMENT TO CENTERLINE OF FOUNDATION)
TRAFFIC SIGNAL MAST ARM POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TRAFFIC SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN SIGNAL POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
PEDESTRIAN PUSHBUTTON POST	4 FT (1.2m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
TEMPORARY WOOD POLE	6 FT (1.8m)	SHOULDER WIDTH + 2 FT (0.6m), MINIMUM 10 FT (3.0m)
CONTROLLER CABINET	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.
SERVICE INSTALLATION, GROUND MOUNT	6 FT (1.8m) MINIMUM DISTANCE SEE NOTE 2	SHOULDER WIDTH + 6 FT (1.8m), MINIMUM 16 FT (4.9m) SEE NOTE 3.

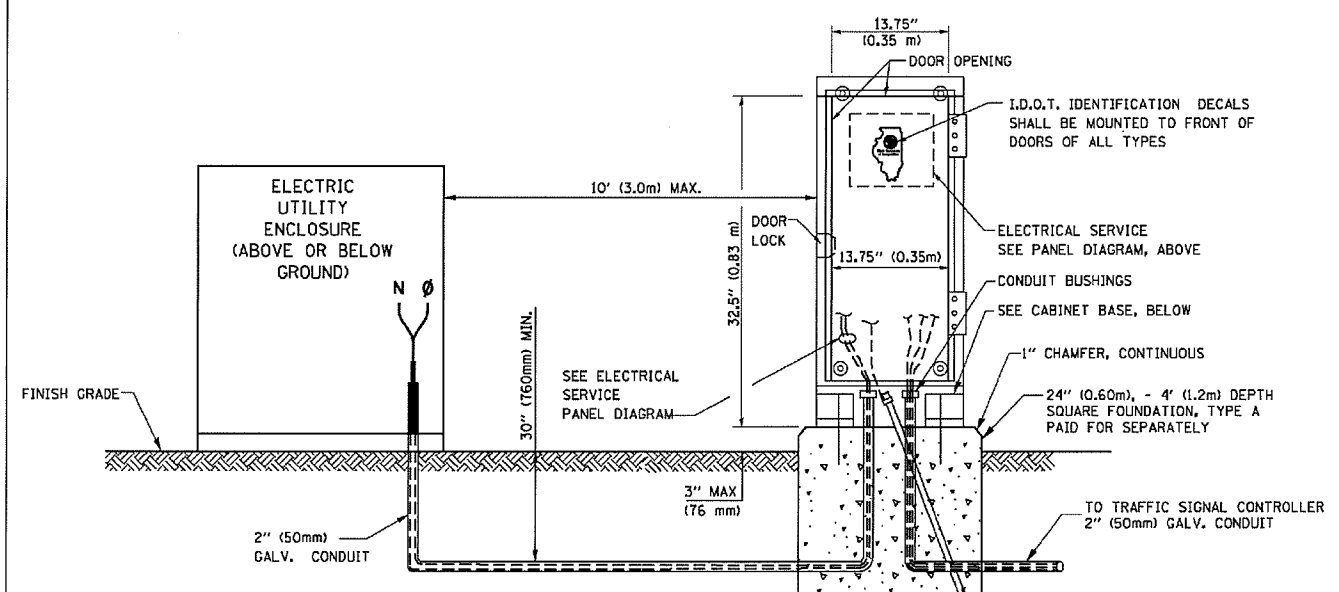
NOTES:

1. CONTACT THE "AREA TRAFFIC SIGNAL MAINTENANCE AND OPERATIONS ENGINEER" FOR ASSISTANCE IN LOCATING THE TRAFFIC SIGNAL EQUIPMENT WHEN THERE ARE CONFLICTS WITH DITCHES OR THE MINIMUM OFFSET DISTANCES CANNOT BE MET.
2. MINIMUM DISTANCE FROM THE BACK OF CURB TO THE ROADWAY SIDE OF THE FOUNDATION.
3. MINIMUM DISTANCE FROM THE EDGE OF PAVEMENT TO THE ROADWAY SIDE OF THE FOUNDATION.
4. ANY CHANGES TO THE OFFSETS OF THE FOUNDATIONS, FROM THE MINIMUM DISTANCES LISTED IN THE "TRAFFIC SIGNAL EQUIPMENT OFFSET" CHART AND THE TRAFFIC SIGNAL INSTALLATION PLAN, COULD EFFECT THE PLACEMENT OF THE SIGNAL HEADS, PEDESTRIAN SIGNAL HEADS AND THE PEDESTRIAN PUSHBUTTONS. THE SIGNAL HEAD PLACEMENT ON THE MAST ARMS SHALL REMAIN AS PER THE TRAFFIC SIGNAL INSTALLATION PLAN AND THE "TRAFFIC SIGNAL MAST ARM AND SIGNAL POST" DETAIL ABOVE. THE PROPOSED MAST ARM LENGTHS MAY NEED TO BE REVISED TO MEET THE ABOVE REQUIREMENTS. THE PEDESTRIAN SIGNAL HEADS AND PEDESTRIAN PUSHBUTTONS MUST MEET THE REQUIREMENTS UNDER THE DETAILS ON THIS SHEET.

FILE NAME =	USER NAME = footomj	DESIGNED - DAD	REVISED - DAG 1-1-14	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE STANDARD TRAFFIC SIGNAL DESIGN DETAILS		F.A.U. RTE. 2678	SECTION 18-00098-00-RS	COUNTY DUPAGE	TOTAL SHEETS 45	SHEET NO. 19
PLOT SCALE = 5/8" = 1' / in.		DRAWN - BCK	REVISED -		SCALE: NONE	SHEET NO. 3 OF 7 SHEETS	STA. TO STA.	TS-05		CONTRACT NO. 61F38	
PLOT DATE = 1/13/2014		CHECKED - DAD	REVISED -		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT						

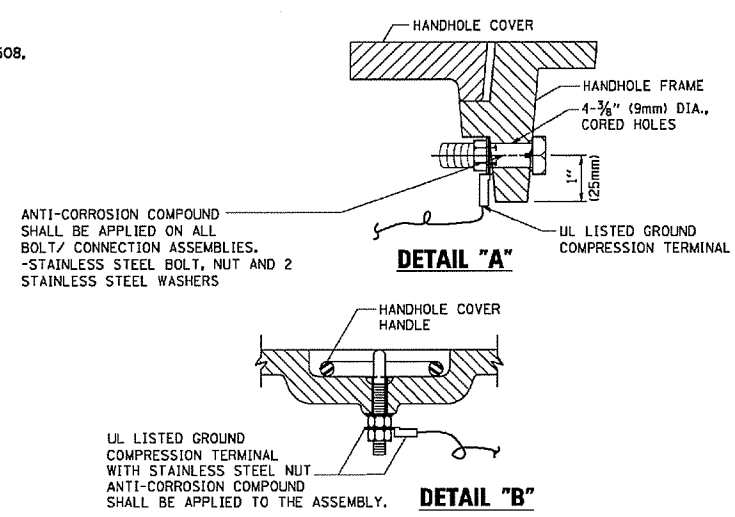


ELECTRICAL SERVICE - PANEL DIAGRAM (TYPICAL FOR POLE AND GROUND MOUNTED SERVICE) SERVICE INSTALLATION POLE MOUNT (SHOWN) (NOT TO SCALE)



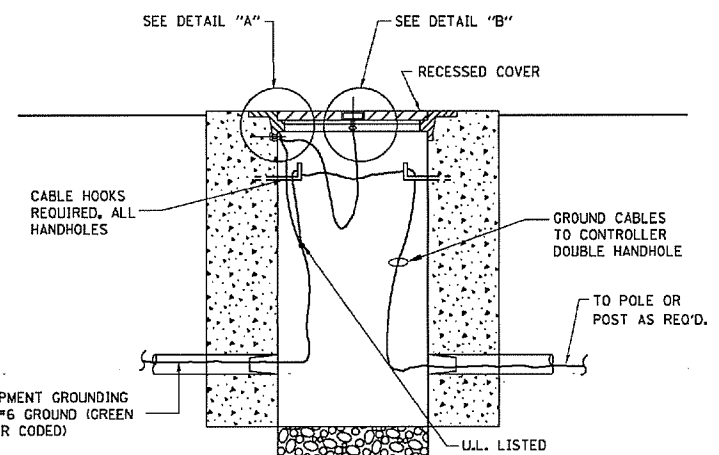
SERVICE INSTALLATION GROUND MOUNT (NOT TO SCALE)

CABINET - BASE BOLT PATTERN (NOT TO SCALE)

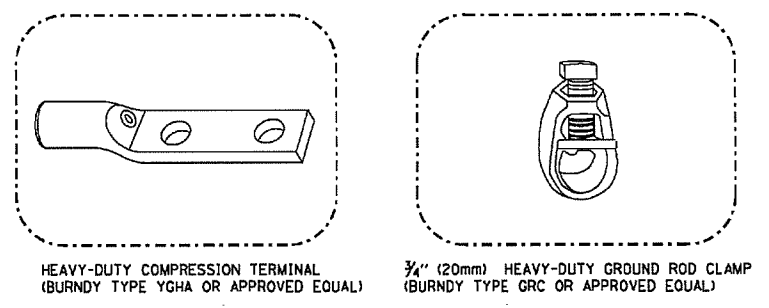


NOTES:
GROUNDING SYSTEM

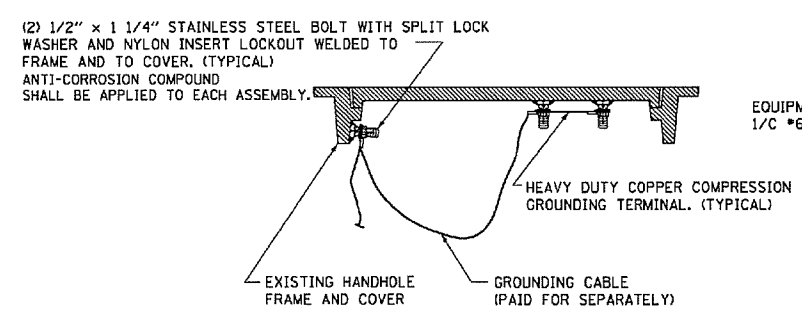
1. THE GROUNDING SYSTEM SHALL CONSIST OF AN INSULATED CONDUCTOR TYPE XLP, NO. 6 A.W.G., STRANDED COPPER TO BE INSTALLED IN RACEWAYS, THE GROUNDING CABLE SHALL BE INSTALLED IN A CONTINUOUS MANNER AS SHOWN ON THE CABLE PLAN PROVIDED. ALL GROUNDING CONDUCTORS SHALL BE BONDED TO METAL ENCLOSURE (HANDHOLE, POST, MAST ARM, CONTROLLER, ETC.). GROUND ROD SHALL BE 3/4" DIA. x 10'-0" (20mm x 3.0m) LONG, COPPER CLAD, ONE GROUND ROD SHALL BE INSTALLED AT ALL POST FOUNDATIONS, POLE FOUNDATIONS, CONTROLLER CABINET FOUNDATION AND ELECTRICAL SERVICE INSTALLATION AS INDICATED ON THE CABLE PLAN. IF THERE ARE ANY SPECIAL CONDITIONS SUCH AS SUB-SURFACE CONDITIONS OR INSTALLATION PROBLEMS, THE RESIDENT ENGINEER SHALL BE NOTIFIED OR CONTACT THE BUREAU OF TRAFFIC, ILLINOIS DEPARTMENT OF TRANSPORTATION DISTRICT ONE AT (847) 705-4139.
2. THE NEUTRAL CONDUCTOR AND THE GROUND CONDUCTOR SHALL BE CONNECTED IN THE SERVICE INSTALLATION. AT NO OTHER POINT IN THE TRAFFIC SIGNAL SYSTEM SHALL THE NEUTRAL AND GROUND CONDUCTORS BE CONNECTED.
3. ALL EQUIPMENT GROUNDING CONDUCTORS SHALL TERMINATE AT THE GROUND BUS IN THE CONTROLLER CABINET.
4. THE CONTRACTOR SHALL PROVIDE A GROUND CABLE WITH CONNECTORS BETWEEN THE HANDHOLE COVER AND HANDHOLE FRAME.



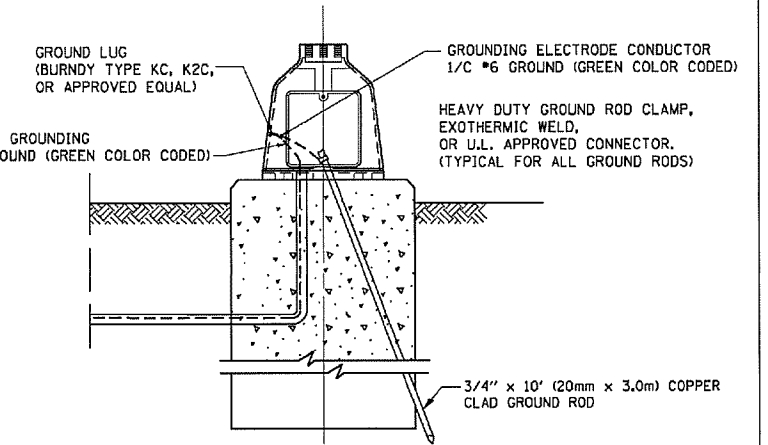
HANDHOLE COVER & FRAME - GROUNDING DETAIL (NOT TO SCALE)



- NOTES:**
- ALL CLAMPS SHALL BE BRONZE OR COPPER, UL APPROVED.
 - GROUND CABLE SHALL BE LOOPED OVER HOOKS IN THE HANDHOLES 6.5' (2.0m) SLACK SHALL BE PROVIDED IN SINGLE HANDHOLES 13' (4.0m) OF SLACK SHALL BE PROVIDED IN DOUBLE HANDHOLES. 5' (1.4m) OF SLACK SHALL BE PROVIDED BETWEEN FRAME AND COVER.



EXISTING HANDHOLE COVER & FRAME - GROUNDING DETAIL (NOT TO SCALE)



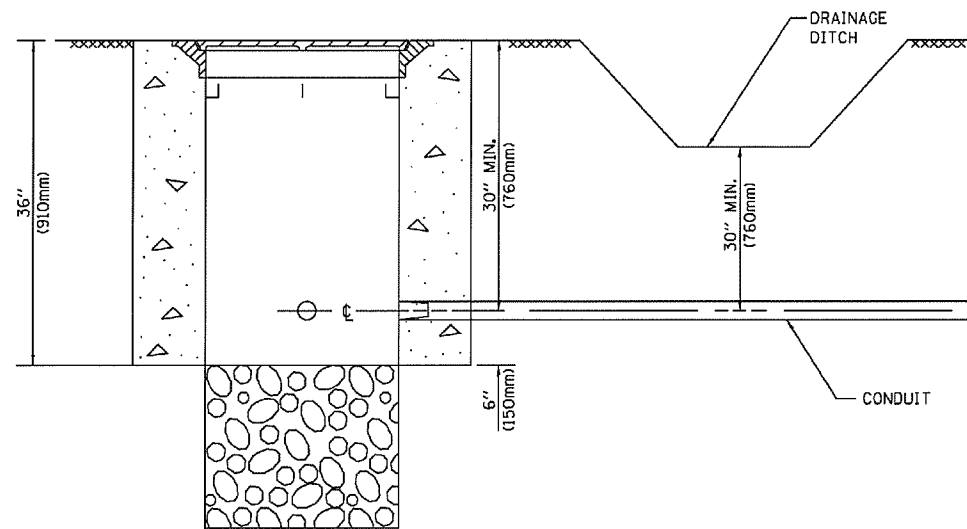
MAST ARM POLE / POST-GROUNDING DETAIL (NOT TO SCALE)

FILE NAME = c:\pwork\pwork\footem\ad018315\ta25.dgn	USER NAME = Footemj	DESIGNED - DAD	REVISED - DAG 1-1-14
		DRAWN - BCK	REVISED -
		CHECKED - DAD	REVISED -
		DATE - 10-28-09	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DISTRICT ONE	
STANDARD TRAFFIC SIGNAL DESIGN DETAILS	
SCALE: NONE	SHEET NO. 4 OF 7 SHEETS
STA.	TO STA.

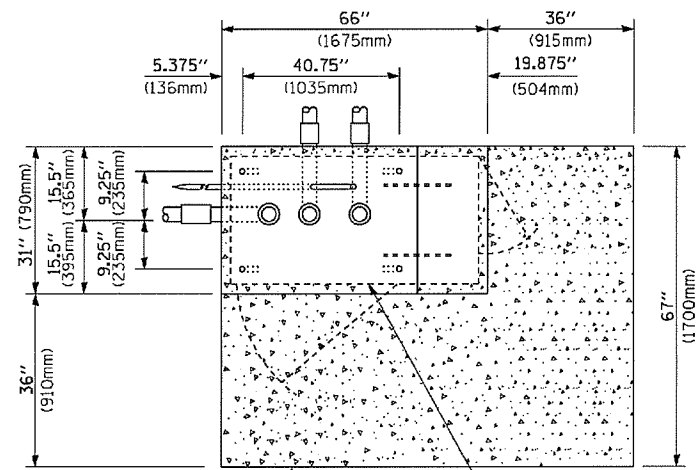
F.A.I. RTE. 2678	SECTION 18-00098-00-RS	COUNTY DUPAGE	TOTAL SHEETS 45	SHEET NO. 20
TS-05		CONTRACT NO. 61F38		
FED. ROAD DIST. NO. 1 ILLINOIS/FED. AID PROJECT				



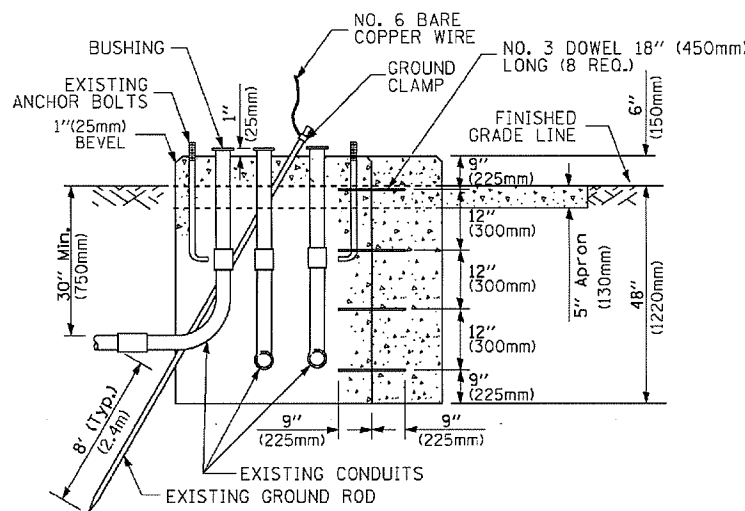
NOTES:

1. CONDUIT DEPTH SHALL BE A MINIMUM OF 30" (760mm) BELOW THE BOTTOM OF THE DRAINAGE DITCH OR ANY SLOPING GROUND
2. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL CONDUIT PLACED UNDER ROADWAY PAVEMENT, MULTI-USE PATHS, SIDEWALKS AND SOIL SURFACES.
3. THE MINIMUM CONDUIT DEPTH APPLIES TO ALL HANDHOLES, HEAVY DUTY HANDHOLES AND DOUBLE HANDHOLES.

HANDHOLE WITH MINIMUM CONDUIT DEPTH
(NOT TO SCALE)



TOP VIEW
(NOT TO SCALE)

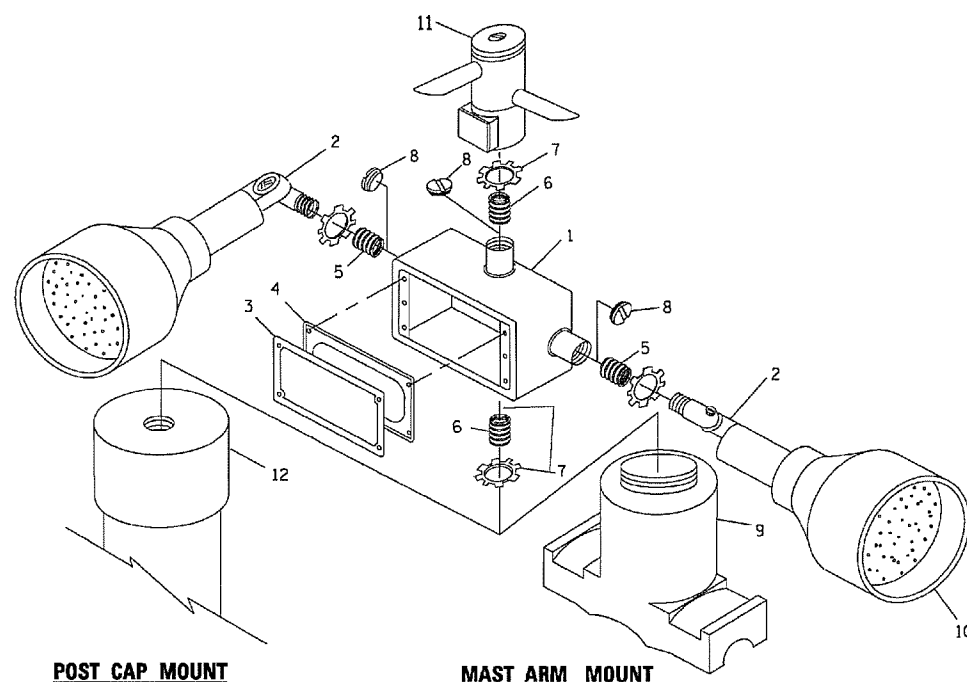


MODIFY EXISTING TYPE "D" FOUNDATION TO TYPE "C" FOUNDATION
(NOT TO SCALE)

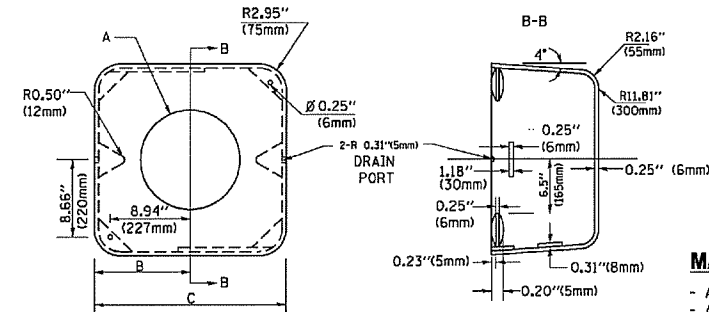
ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4" (19 mm) CLOSE NIPPLE
7	3/4" (19 mm) LOCKNUT
8	3/4" (19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

NOTES:

1. ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
2. ITEM #1 - OZ/GEDNEY FSX-1-50 OR EQUIVALENT
ITEM #2 - MULBERRY CON-O SHADE LAMP SHIELD OR EQUIVALENT
ITEM #9 - BAND-IT SADDLE BRACKET OR EQUIVALENT
3. WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4" (19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION BEACON MOUNTING DETAIL



MATERIAL:
- ASTM A36 STEEL
- ASTM A-123 HOT DIPPED GALVANIZED

A	B	C	HEIGHT	WEIGHT
VARIES	9.5"(241mm)	19"(483mm)	7" (178mm) - 12" (300mm)	53 lbs (24kg)
VARIES	10.75"(273mm)	21.5"(546mm)	7" (178mm) - 12" (300mm)	68 lbs (31 kg)
VARIES	13.0"(330mm)	26"(660mm)	7" (178mm) - 12" (300mm)	81 lbs (37 kg)
VARIES	18.5"(470mm)	37"(940mm)	7" (178mm) - 12" (300mm)	126 lbs (57 kg)

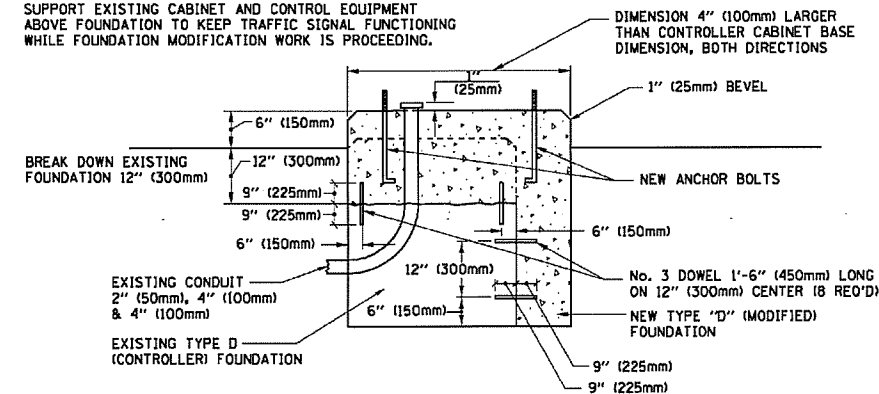
SHROUD

NOTES:

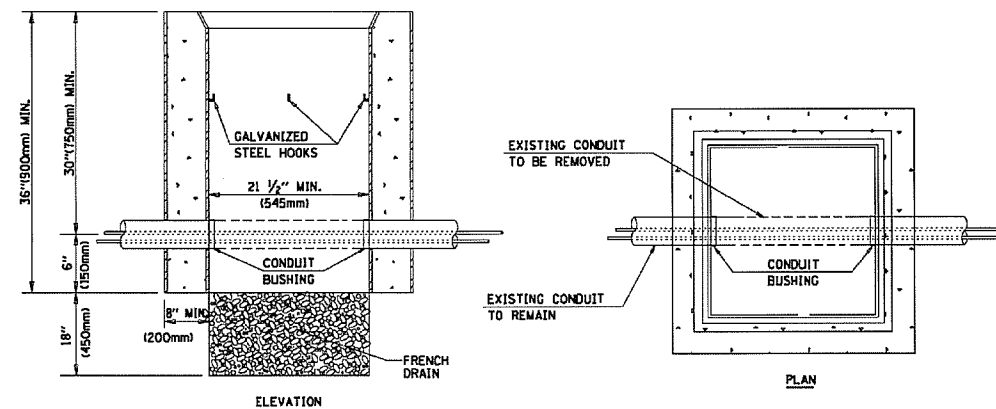
1. DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
2. THE SUPPLIER SHALL VERIFIED THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
3. THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.

NOTE:

SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



MODIFY EXISTING TYPE "D" FOUNDATION



NOTES:

1. HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
2. REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCLUDED WITH THE COST OF THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT

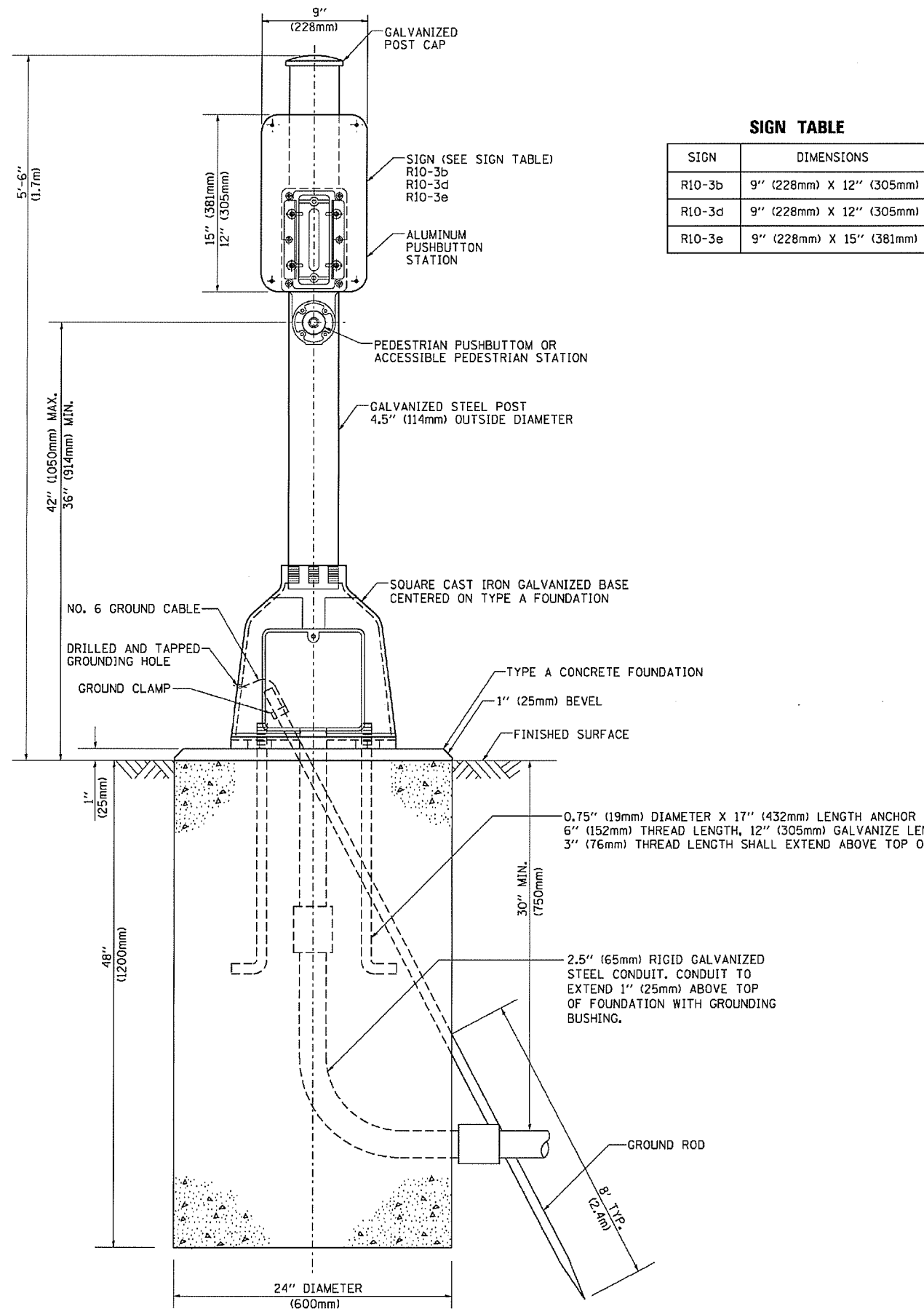
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c:\pwwork\pwwork\footemj\d0188315\ts05.dgn		DRAWN - BCK	REVISED -
	PLOT SCALE = 50.0000' / 1"	CHECKED - DAD	REVISED -
	PLOT DATE = 1/13/2014	DATE - 10-28-09	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS

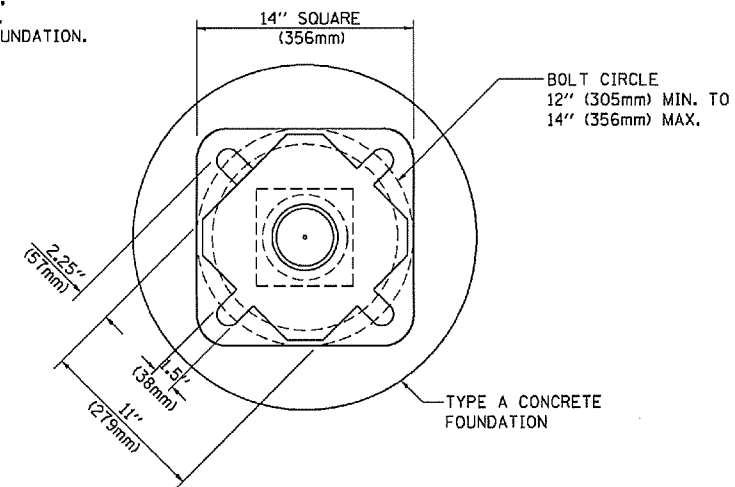
SCALE: NONE SHEET NO. 6 OF 7 SHEETS STA. TO STA.

F.A.J. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2678	18-0098-00-RS	DUPAGE	45	22
TS-05		CONTRACT NO. 61F38		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



SIGN TABLE

SIGN	DIMENSIONS
R10-3b	9" (228mm) X 12" (305mm)
R10-3d	9" (228mm) X 12" (305mm)
R10-3e	9" (228mm) X 15" (381mm)



BOLT PATTERN

PEDESTRIAN PUSH BUTTON POST, TYPE A

FILE NAME =	USER NAME = Footemj	DESIGNED - DAG	REVISED - DAG 1-1-14
c:\pwork\pwork\footemj\d0108315\ta05.dgn		DRAWN - GND	REVISED -
	PLOT SCALE = 58.0000 ' / in.	CHECKED - DAD	REVISED -
	PLOT DATE = 1/13/2014	DATE - 10/1/2012	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

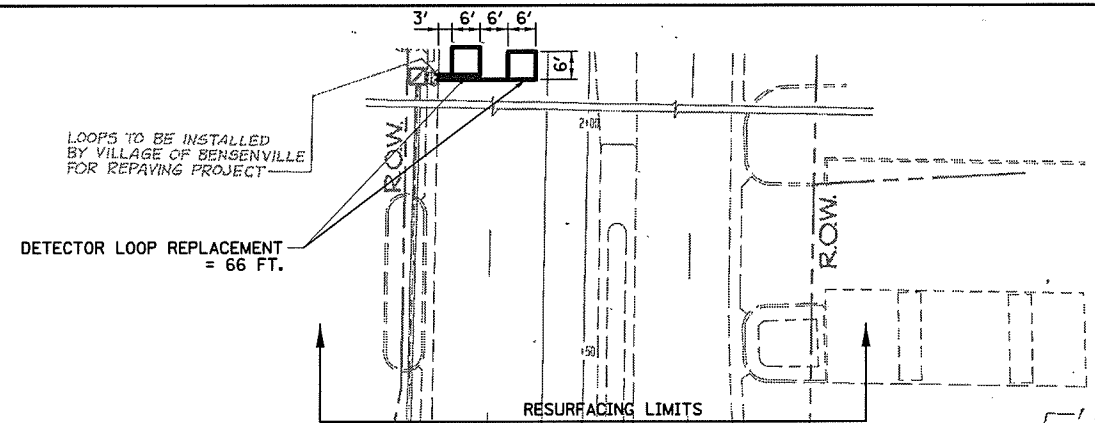
**DISTRICT ONE
STANDARD TRAFFIC SIGNAL DESIGN DETAILS**

SCALE: NONE SHEET NO. 7 OF 7 SHEETS STA. TO STA.

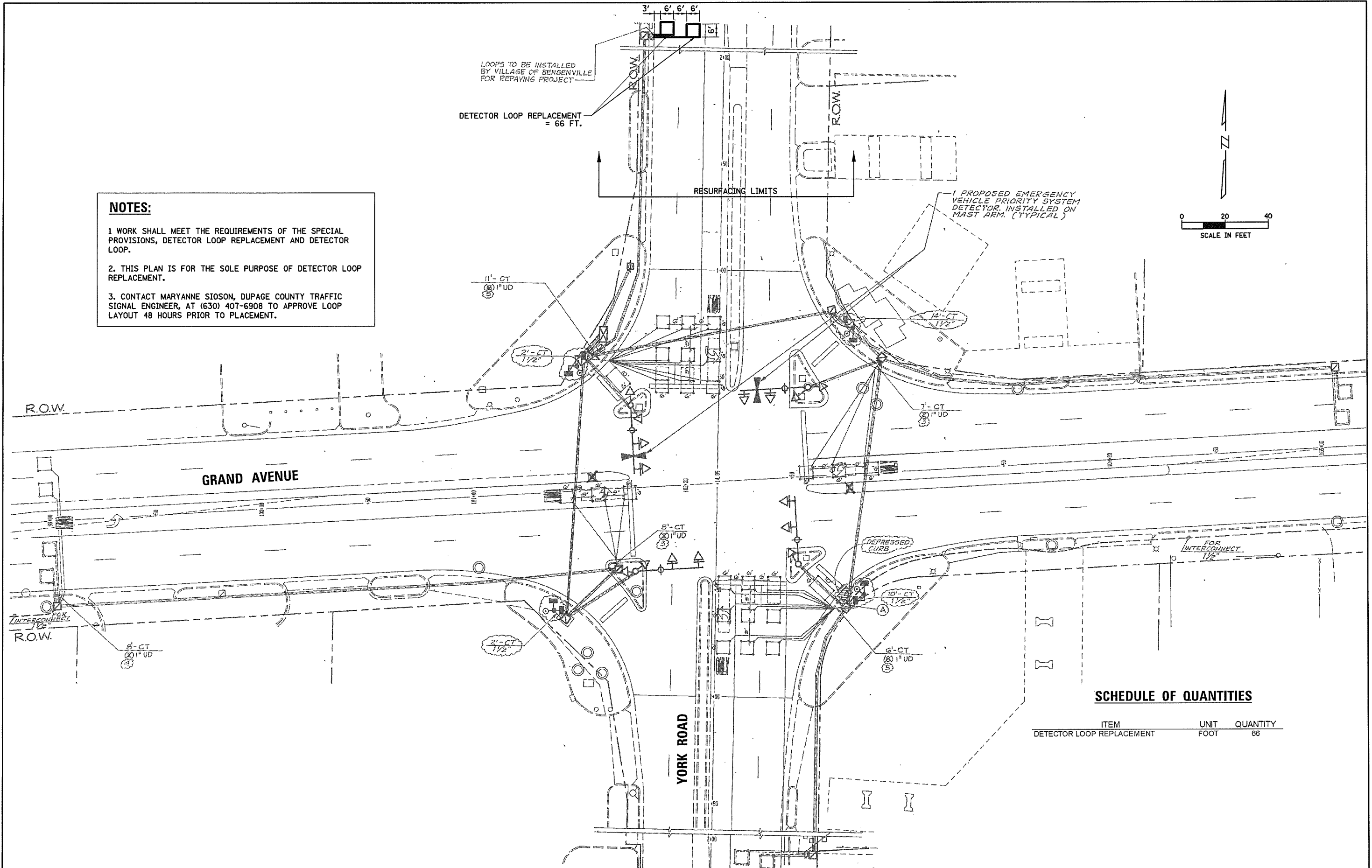
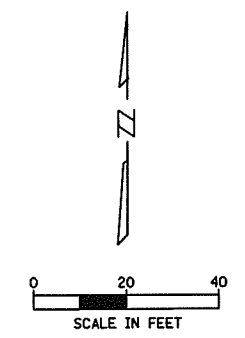
F.A.I. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2678	18-00098-00-RS	DUPAGE	45	23
TS-05			CONTRACT NO. 61F38	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

NOTES:

- 1 WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISIONS, DETECTOR LOOP REPLACEMENT AND DETECTOR LOOP.
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.
3. CONTACT MARYANNE SIOSON, DUPAGE COUNTY TRAFFIC SIGNAL ENGINEER, AT (630) 407-6908 TO APPROVE LOOP LAYOUT 48 HOURS PRIOR TO PLACEMENT.



1 PROPOSED EMERGENCY VEHICLE PRIORITY SYSTEM DETECTOR, INSTALLED ON MAST ARM. (TYPICAL)



SCHEDULE OF QUANTITIES

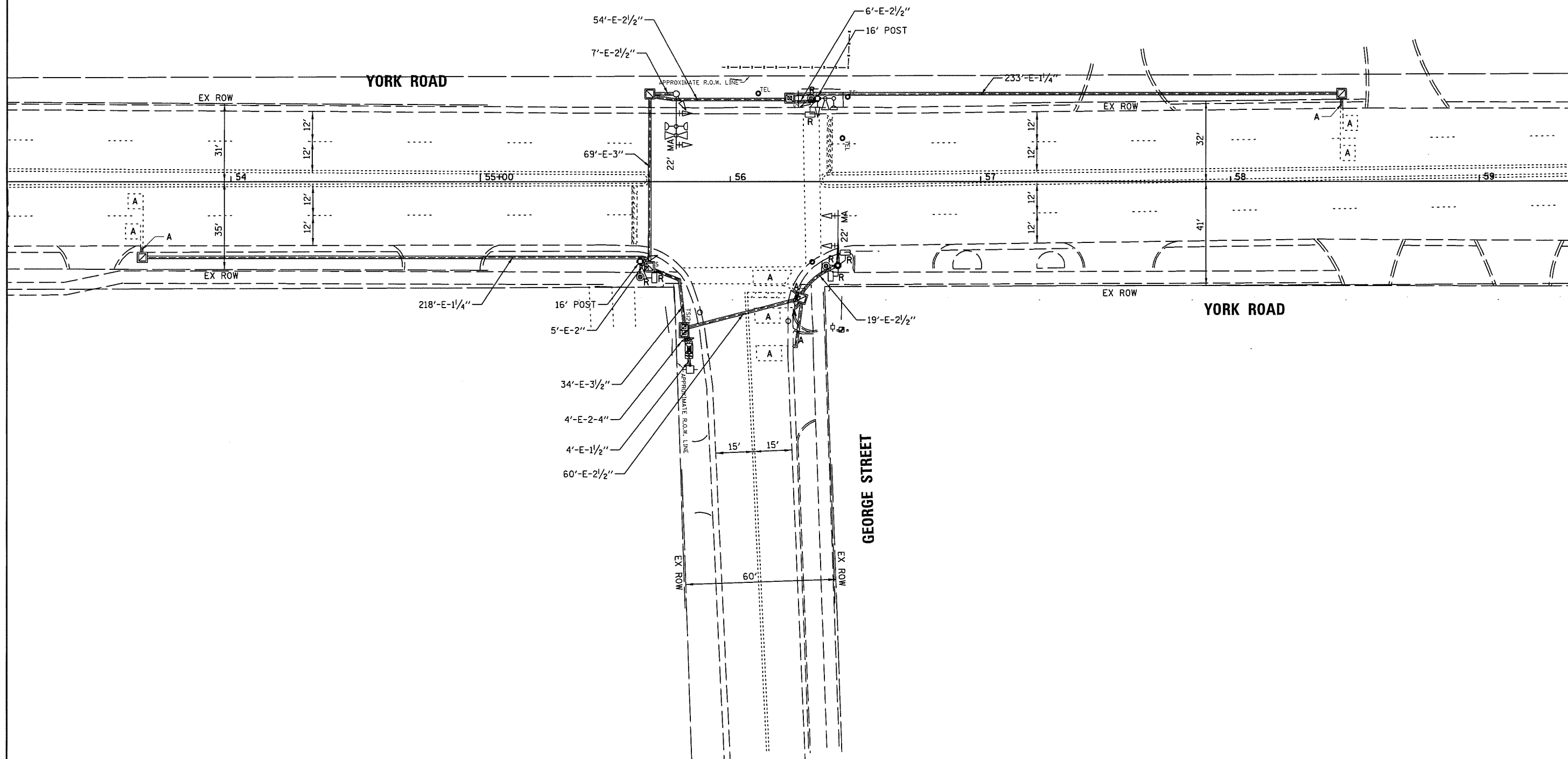
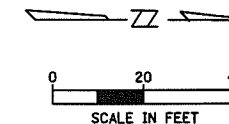
ITEM	UNIT	QUANTITY
DETECTOR LOOP REPLACEMENT	FOOT	66

FILE NAME = N:\Bensenville\180145\Traffic\York-Grand	USER NAME = dconnell	DESIGNED EAJ	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DETECTOR LOOP REPLACEMENT YORK ROAD AND GRAND AVENUE	FAU RTE. 2678	SECTION 18-00098-00-RS	COUNTY DUPAGE	TOTAL SHEETS 45	SHEET NO. 24	
Default	01-Loop.dgn	DRAWN FPB	REVISED -			SCALE:	SHEET OF SHEETS STA. TO STA.	ILLINOIS FED. AID PROJECT			
	PLOT SCALE = 40'	CHECKED GMZ	REVISED -					CONTRACT NO. 61F38			
	PLOT DATE = 11/14/2018	DATE -	REVISED -								

REMOVAL NOTES:

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

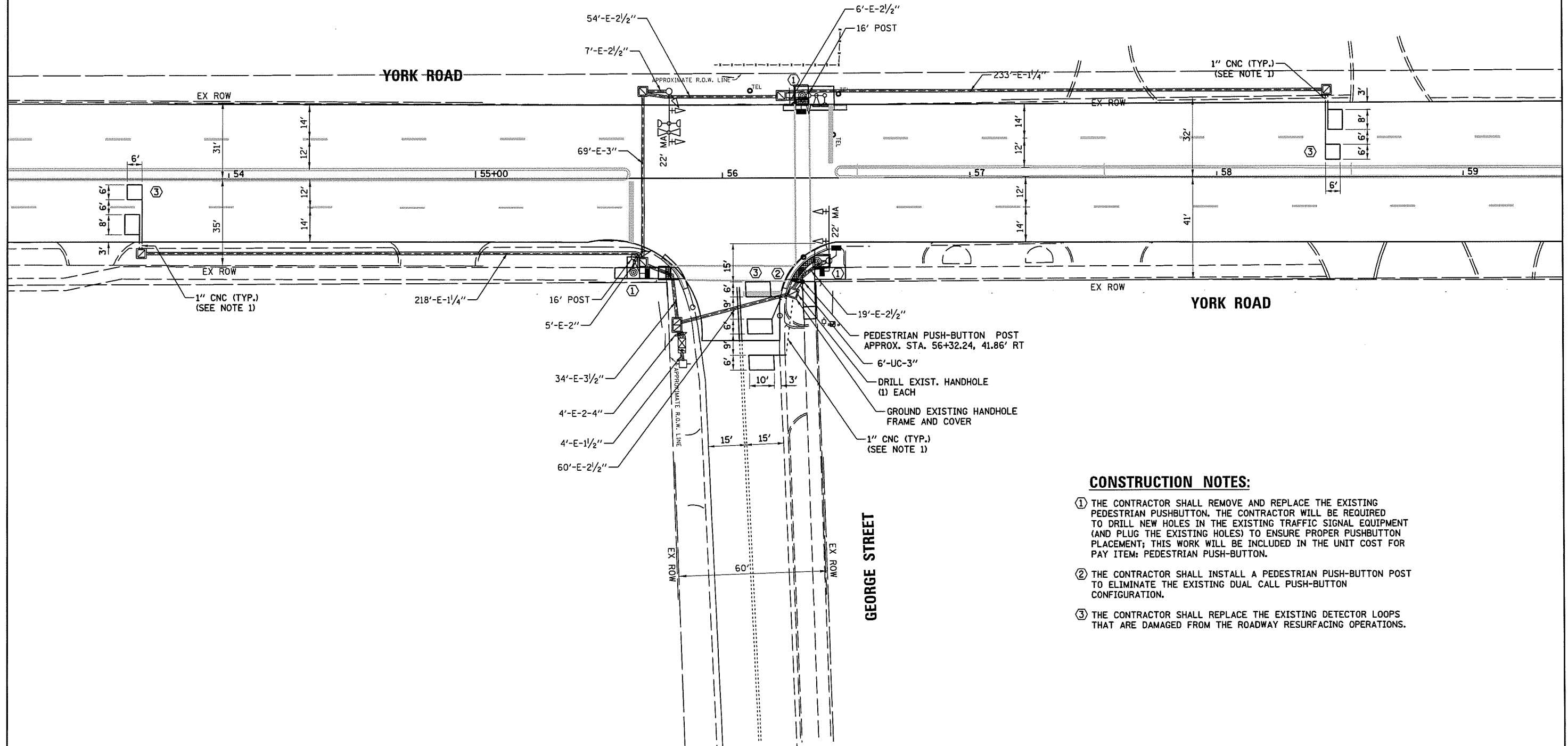
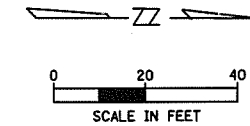
- 4 EACH PEDESTRIAN SIGNAL HEAD
- 3 EACH PEDESTRIAN PUSH-BUTTON



FILE NAME =	USER NAME = doconnell	DESIGNED EAJ	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	REMOVAL PLAN YORK ROAD AND GEORGE STREET	FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
N:\Bensenville\180145\Traffic\York-George	.01-REM.dgn	DRAWN FPB	REVISED -			2678	18-00098-00-RS	DUPAGE	45	25
PLOT SCALE = 40'		CHECKED GMZ	REVISED -			CONTRACT NO. 61F38				
Default	PLOT DATE = 11/14/2018	DATE -	REVISED -			ILLINOIS FED. AID PROJECT				

NOTES:

1. EACH DETECTOR LOOP SHALL HAVE ITS OWN 1" COILABLE NON-METALLIC CONDUIT BETWEEN THE EDGE OF PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND AS STATED IN THE TRAFFIC SIGNAL SPECIFICATIONS.

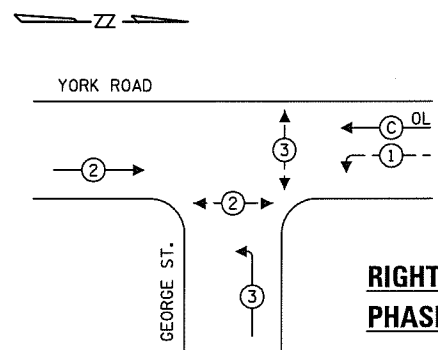


CONSTRUCTION NOTES:

- ① THE CONTRACTOR SHALL REMOVE AND REPLACE THE EXISTING PEDESTRIAN PUSHBUTTON. THE CONTRACTOR WILL BE REQUIRED TO DRILL NEW HOLES IN THE EXISTING TRAFFIC SIGNAL EQUIPMENT (AND PLUG THE EXISTING HOLES) TO ENSURE PROPER PUSHBUTTON PLACEMENT; THIS WORK WILL BE INCLUDED IN THE UNIT COST FOR PAY ITEM: PEDESTRIAN PUSH-BUTTON.
- ② THE CONTRACTOR SHALL INSTALL A PEDESTRIAN PUSH-BUTTON POST TO ELIMINATE THE EXISTING DUAL CALL PUSH-BUTTON CONFIGURATION.
- ③ THE CONTRACTOR SHALL REPLACE THE EXISTING DETECTOR LOOPS THAT ARE DAMAGED FROM THE ROADWAY RESURFACING OPERATIONS.

FILE NAME =	USER NAME = dconnell	DESIGNED EAJ	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC SIGNAL MODIFICATION PLAN YORK ROAD AND GEORGE STREET	FAU	SECTION	COUNTY	TOTAL	SHEET	
N:\Bansenville\180145\Traffic\York-Georg	02-TSM.dgn	DRAWN FPB	REVISED -			2678	18-00098-00-RS	DUPAGE	45	26	
Default	PLOT SCALE = 40'	CHECKED GMZ	REVISED -			CONTRACT NO. 61F38					
	PLOT DATE = 11/14/2018	DATE -	REVISED -			[ILLINOIS] FED. AID PROJECT					

EXISTING CONTROLLER SEQUENCE



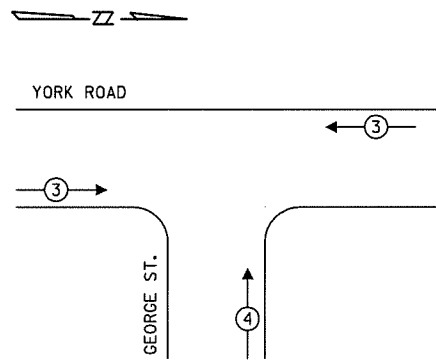
LEGEND:

- ← ⊙ ← PROTECTED PHASE
- ← ⊙ - - PROTECTED/PERMITTED PHASE
- ← ⊙ → PEDESTRIAN PHASE
- ← ⊙ OL OVERLAP

RIGHT TURN OVERLAP PHASE DESIGNATION:

$$\text{OVERLAP LETTER } C = \text{PERMISSIVE PHASE } 1 + \text{PROTECTED PHASE } 2$$

EXISTING EMERGENCY VEHICLE PREEMPTION SEQUENCE



TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	8	11	50	44.0
(YELLOW)	8	20	5	8.0
(GREEN)	8	12	45	43.2
PERMISSIVE ARROW	4	10	10	4.0
PED. SIGNAL	4	20	100	80.0
CONTROLLER	1	100	100	100.0
UPS	-	25	100	-
VIDEO SYSTEM	-	150	100	-
BLANK-OUT SIGN	-	25	5	-
FLASHER	-	-	50	-
STREET NAME SIGN	-	120	50	-
LUMINAIRE	-	-	-	-
TOTAL =				279.2

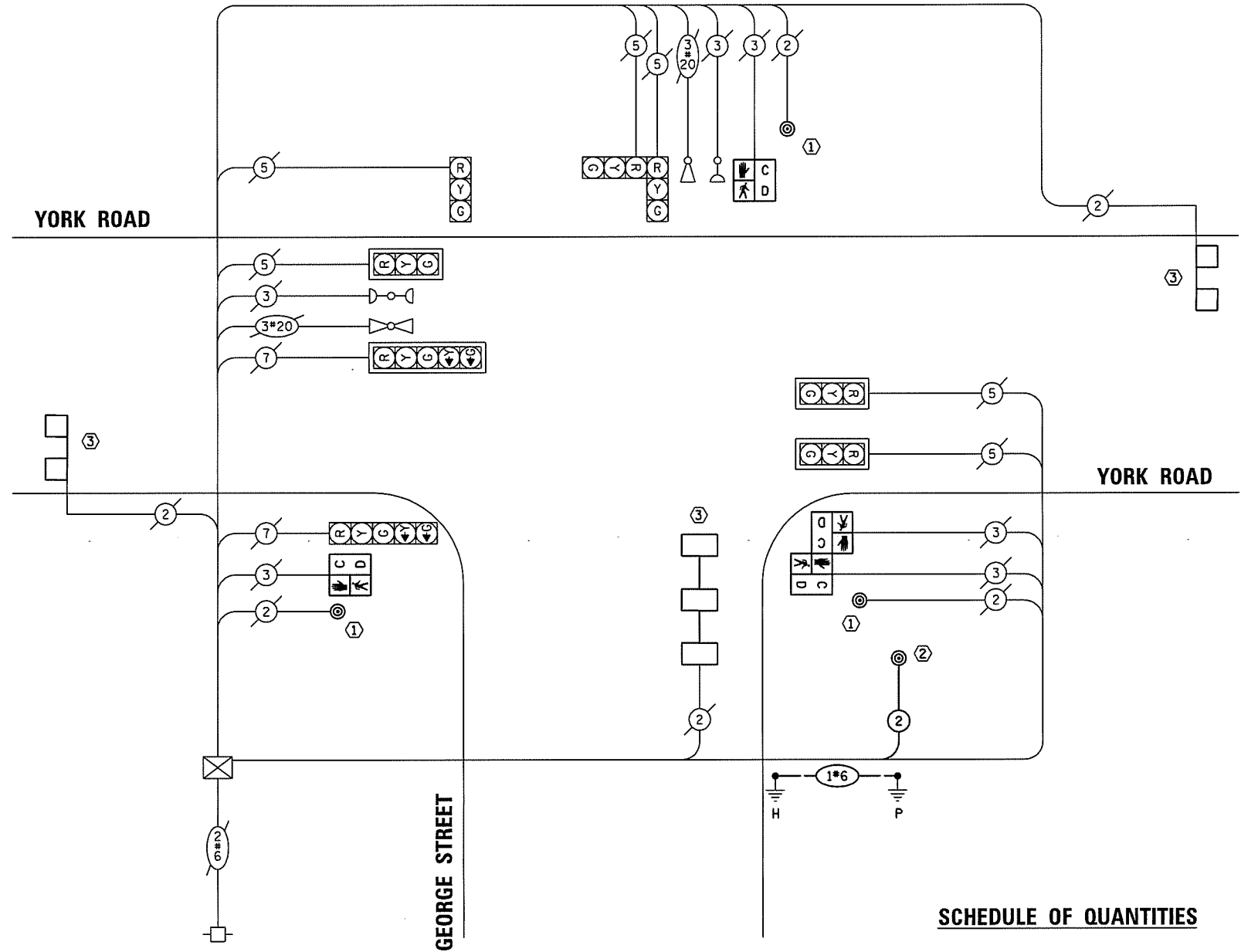
ENERGY COSTS TO:

VILLAGE OF BENSENVILLE
12 S Center Street
Bensenville, IL 60106

ENERGY SUPPLY: CONTACT: NEW BUSINESS
PHONE: NEW PHONE
COMPANY: COMMONWEALTH EDISON
ACCOUNT NUMBER: ---

CONSTRUCTION NOTES:

- ① THE CONTRACTOR SHALL REMOVE AND REPLACE THE EXISTING PEDESTRIAN PUSHBUTTON. THE CONTRACTOR WILL BE REQUIRED TO DRILL NEW HOLES IN THE EXISTING TRAFFIC SIGNAL EQUIPMENT (AND PLUG THE EXISTING HOLES) TO ENSURE PROPER PUSHBUTTON PLACEMENT; THIS WORK WILL BE INCLUDED IN THE UNIT COST FOR PAY ITEM: PEDESTRIAN PUSH-BUTTON.
- ② THE CONTRACTOR SHALL INSTALL A PEDESTRIAN PUSH-BUTTON POST TO ELIMINATE THE EXISTING DUAL CALL PUSH-BUTTON CONFIGURATION.
- ③ THE CONTRACTOR SHALL REPLACE THE EXISTING DETECTOR LOOPS THAT ARE DAMAGED FROM THE ROADWAY RESURFACING OPERATIONS.



CABLE PLAN
(NOT TO SCALE)

SCHEDULE OF QUANTITIES

ITEM	UNIT	QUANTITY
UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	6
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
GROUNDING EXISTING HANDHOLE FRAME AND COVER	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	105
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	24
CONCRETE FOUNDATION, TYPE A	FOOT	4
DRILL EXISTING HANDHOLE	EACH	1
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	4
DETECTOR LOOP REPLACEMENT	FOOT	257
PEDESTRIAN PUSH-BUTTON	EACH	4
MODIFY EXISTING CONTROLLER	EACH	1
MODIFY EXISTING CONTROLLER CABINET	EACH	1
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
PEDESTRIAN PUSH-BUTTON POST, TYPE A	EACH	1

FILE NAME =	USER NAME = dcoconnell	DESIGNED EAJ	REVISED -
N:\Bensenville\180145\Traffic\York-George_03-CAB.dgn		DRAWN FPB	REVISED -
		CHECKED GMZ	REVISED -
Default	PLOT DATE = 11/14/2018	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES, CABLE PLAN, PHASE DESIGNATION DIAGRAM AND EMERGENCY VEHICLE PREEMPTION SEQUENCE YORK ROAD AND GEORGE STREET

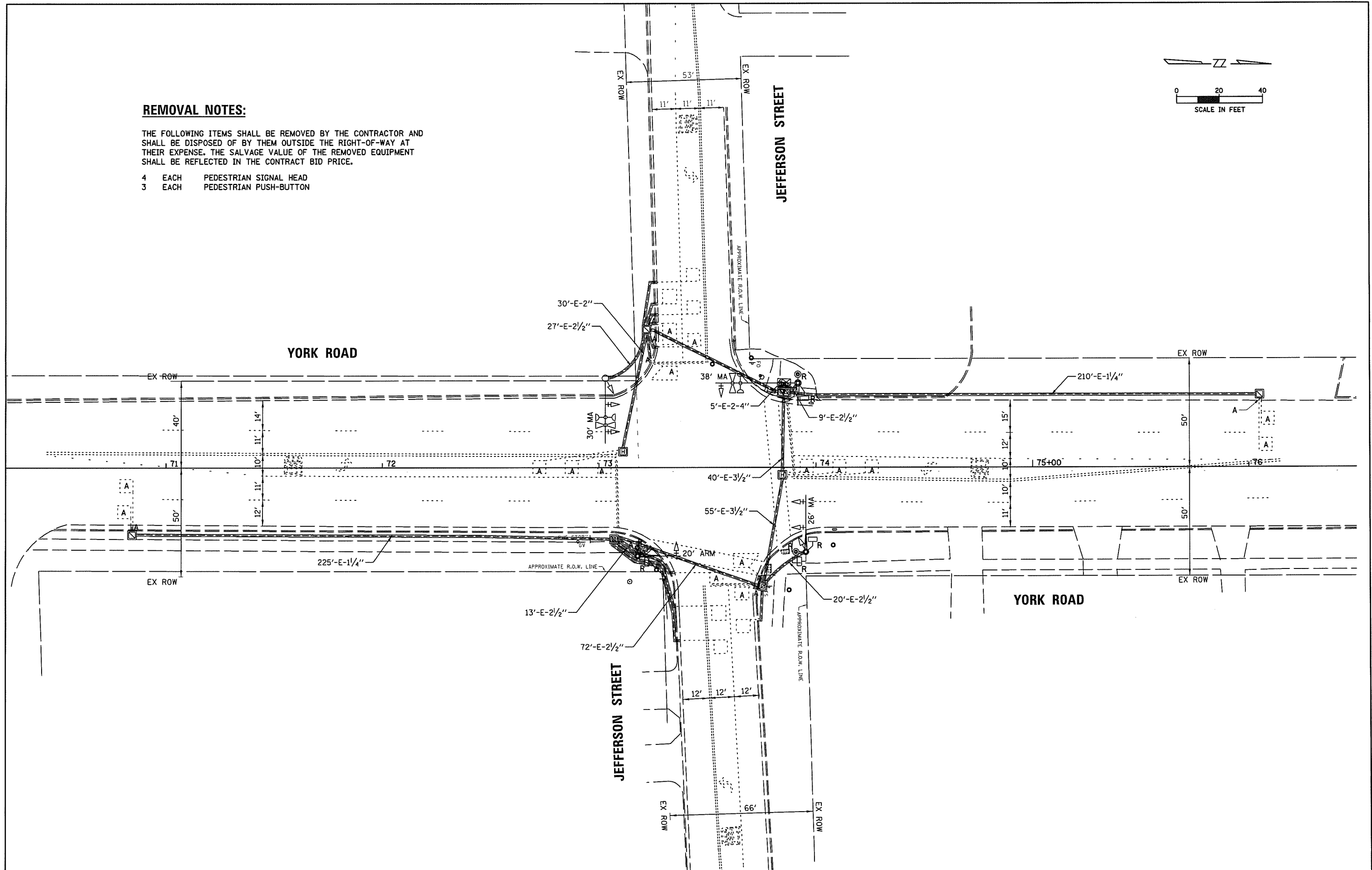
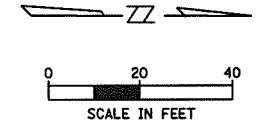
SCALE: SHEET OF SHEETS STA. TO STA.

FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2678	18-00098-00-RS	DUPAGE	45	27
CONTRACT NO. 61F38			ILLINOIS FED. AID PROJECT	

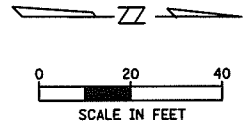
REMOVAL NOTES:

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 4 EACH PEDESTRIAN SIGNAL HEAD
- 3 EACH PEDESTRIAN PUSH-BUTTON

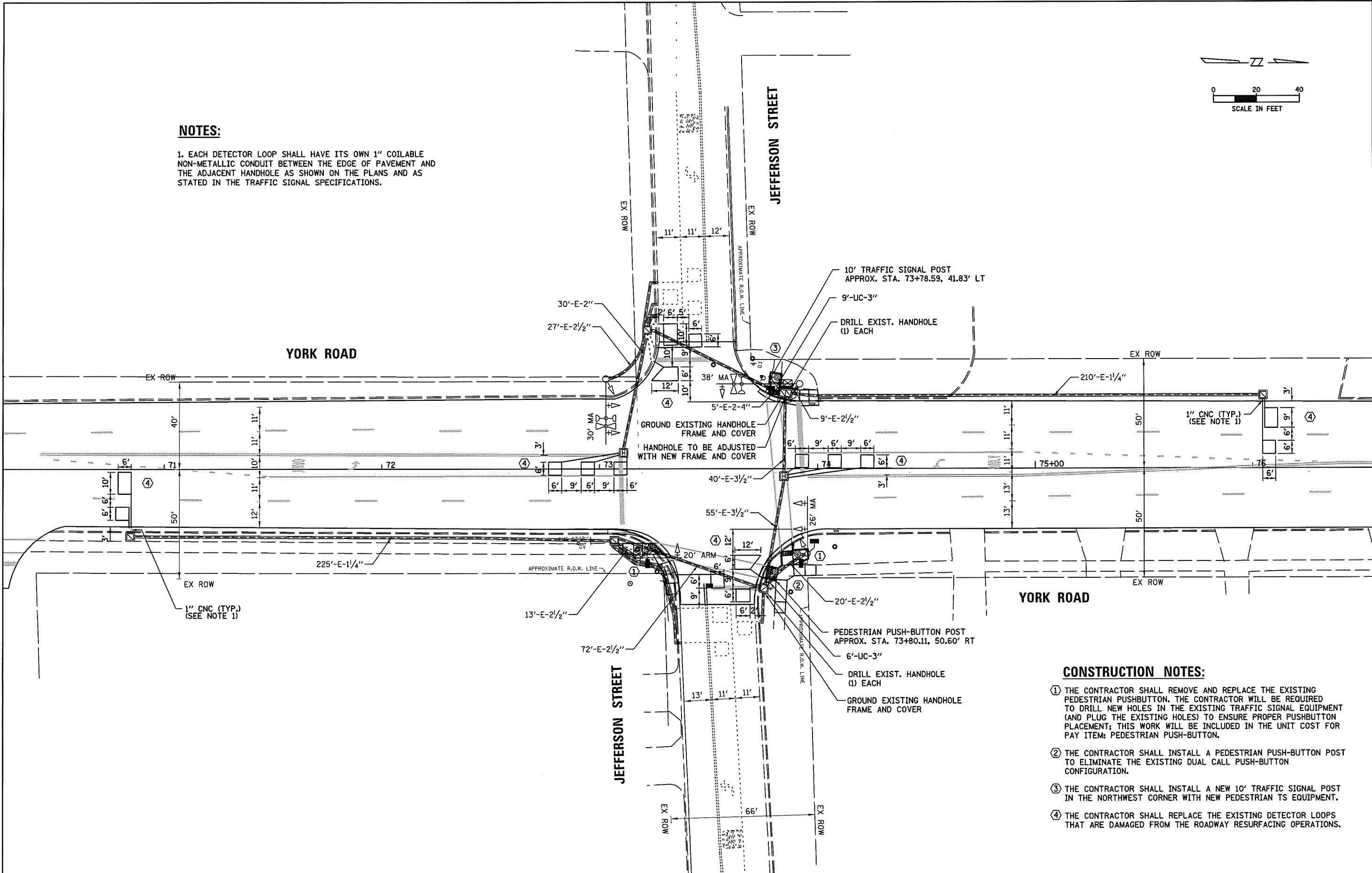


FILE NAME =	USER NAME = dconnell	DESIGNED EAJ	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	REMOVAL PLAN YORK ROAD AND JEFFERSON STREET			FAU	SECTION	COUNTY	TOTAL	SHEET
N:\Bensenville\180145\Traffic\York-Jefferson_01-REM.dgn	son_01-REM.dgn	DRAWN FPB	REVISED -		2678	18-00098-00-RS	DUPAGE	45	28			
Default	PLT SCALE = 40'	CHECKED GMZ	REVISED -		CONTRACT NO. 61F38							
	PLT DATE = 11/14/2018	DATE -	REVISED -		ILLINOIS FED. AID PROJECT							



NOTES:

1. EACH DETECTOR LOOP SHALL HAVE ITS OWN 1" COILABLE NON-METALLIC CONDUIT BETWEEN THE EDGE OF PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND AS STATED IN THE TRAFFIC SIGNAL SPECIFICATIONS.



CONSTRUCTION NOTES:

- ① THE CONTRACTOR SHALL REMOVE AND REPLACE THE EXISTING PEDESTRIAN PUSHBUTTON. THE CONTRACTOR WILL BE REQUIRED TO DRILL NEW HOLES IN THE EXISTING TRAFFIC SIGNAL EQUIPMENT (AND PLUG THE EXISTING HOLES) TO ENSURE PROPER PUSHBUTTON PLACEMENT; THIS WORK WILL BE INCLUDED IN THE UNIT COST FOR PAY ITEM: PEDESTRIAN PUSH-BUTTON.
- ② THE CONTRACTOR SHALL INSTALL A PEDESTRIAN PUSH-BUTTON POST TO ELIMINATE THE EXISTING DUAL CALL PUSH-BUTTON CONFIGURATION.
- ③ THE CONTRACTOR SHALL INSTALL A NEW 10' TRAFFIC SIGNAL POST IN THE NORTHWEST CORNER WITH NEW PEDESTRIAN TS EQUIPMENT.
- ④ THE CONTRACTOR SHALL REPLACE THE EXISTING DETECTOR LOOPS THAT ARE DAMAGED FROM THE ROADWAY RESURFACING OPERATIONS.

FILE NAME =	USER NAME = doconnell	DESIGNED EAJ	REVISED -
N:\Bensenville\180145\Traffic\York-Jefferson_02-TSM.dgn		DRAWN FPB	REVISED -
PLOT SCALE = 40'		CHECKED GMZ	REVISED -
PLOT DATE = 11/14/2018		DATE -	REVISED -

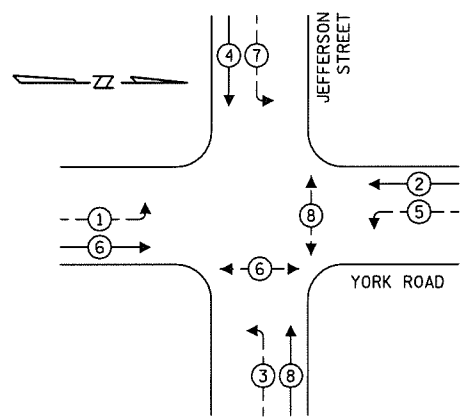
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC SIGNAL MODIFICATION PLAN
YORK ROAD AND JEFFERSON STREET**

SCALE: SHEET OF SHEETS STA. TO STA.

FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2678	18-00098-00-RS	DUPAGE	45	29
CONTRACT NO. 61F38				
ILLINOIS FED. AID PROJECT				

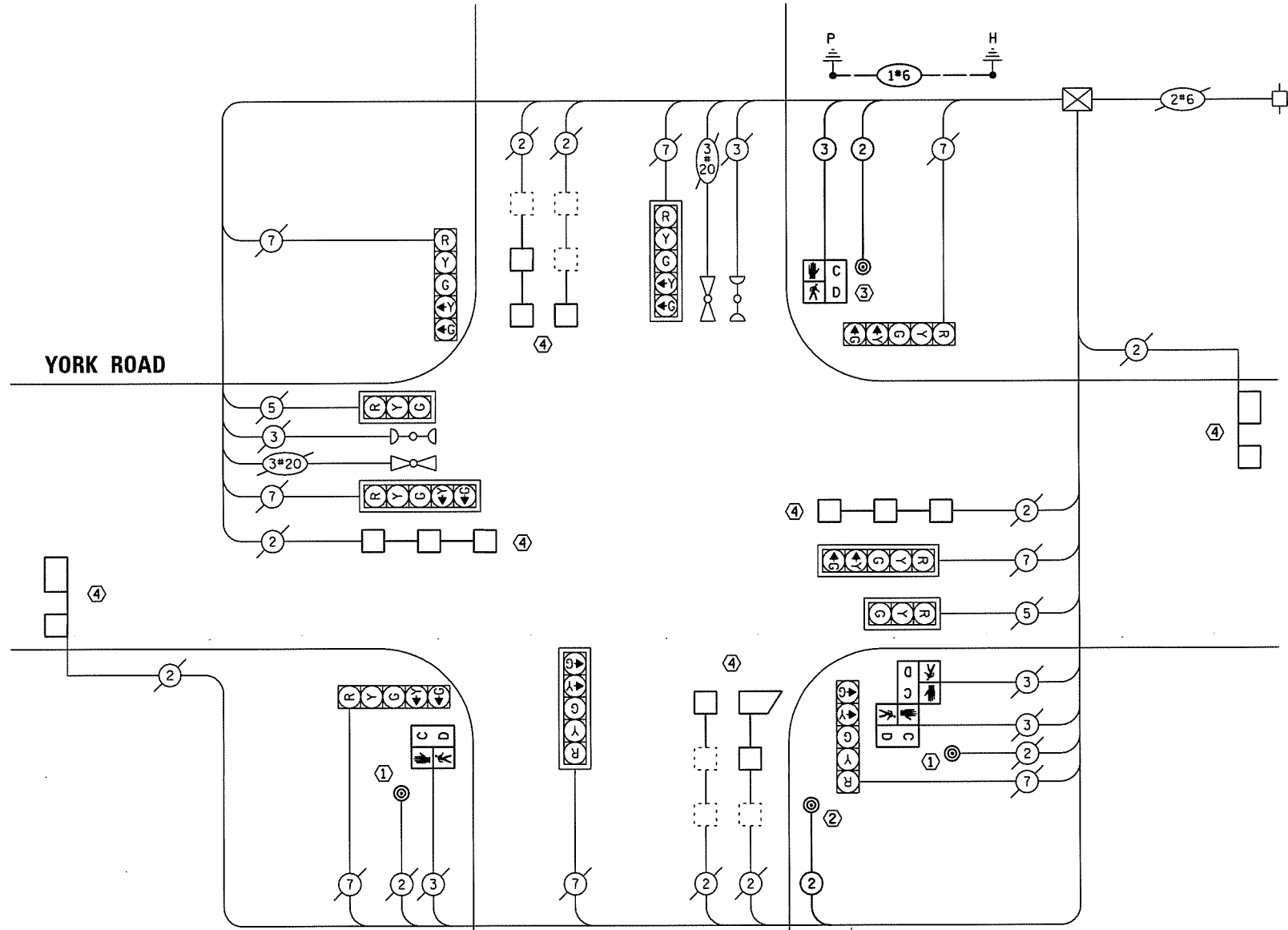
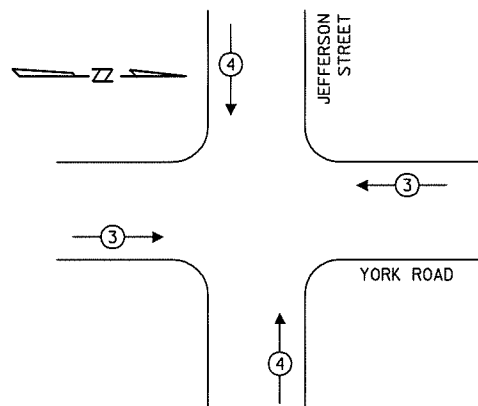
EXISTING CONTROLLER SEQUENCE



LEGEND:

- ← ⊙ → PROTECTED PHASE
- ← ⊙ - - → PROTECTED/PERMITTED PHASE
- ← ⊙ → PEDESTRIAN PHASE
- ← ⊙ OL → OVERLAP

EXISTING EMERGENCY VEHICLE PREEMPTION SEQUENCE



TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	10	11	50	55.0
(YELLOW)	10	20	5	10.0
(GREEN)	10	12	45	54.0
PERMISSIVE ARROW	16	10	10	16.0
PED. SIGNAL	4	20	100	80.0
CONTROLLER	1	100	100	100.0
UPS	-	25	100	-
VIDEO SYSTEM	-	150	100	-
BLANK-OUT SIGN	-	25	5	-
FLASHER	-	-	50	-
STREET NAME SIGN	-	120	50	-
LUMINAIRE	-	-	-	-
TOTAL =				315.0

CONSTRUCTION NOTES:

- ① THE CONTRACTOR SHALL REMOVE AND REPLACE THE EXISTING PEDESTRIAN PUSHBUTTON. THE CONTRACTOR WILL BE REQUIRED TO DRILL NEW HOLES IN THE EXISTING TRAFFIC SIGNAL EQUIPMENT (AND PLUG THE EXISTING HOLES) TO ENSURE PROPER PUSHBUTTON PLACEMENT; THIS WORK WILL BE INCLUDED IN THE UNIT COST FOR PAY ITEM: PEDESTRIAN PUSH-BUTTON.
- ② THE CONTRACTOR SHALL INSTALL A PEDESTRIAN PUSH-BUTTON POST TO ELIMINATE THE EXISTING DUAL CALL PUSH-BUTTON CONFIGURATION.
- ③ THE CONTRACTOR SHALL INSTALL A NEW 10' TRAFFIC SIGNAL POST IN THE NORTHWEST CORNER WITH NEW PEDESTRIAN TS EQUIPMENT.
- ④ THE CONTRACTOR SHALL REPLACE THE EXISTING DETECTOR LOOPS THAT ARE DAMAGED FROM THE ROADWAY RESURFACING OPERATIONS.

SCHEDULE OF QUANTITIES

ITEM	UNIT	QUANTITY
UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	15
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
GROUNDING EXISTING HANDHOLE FRAME AND COVER	EACH	2
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	190
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	50
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	63
TRAFFIC SIGNAL POST, GALVANIZED STEEL 10 FT.	EACH	1
CONCRETE FOUNDATION, TYPE A	FOOT	8
DRILL EXISTING HANDHOLE	EACH	2
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	4
DETECTOR LOOP REPLACEMENT	FOOT	610
PEDESTRIAN PUSH-BUTTON	EACH	4
MODIFY EXISTING CONTROLLER	EACH	1
MODIFY EXISTING CONTROLLER CABINET	EACH	1
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	92
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
PEDESTRIAN PUSH-BUTTON POST, TYPE A	EACH	1
HANDHOLE TO BE ADJUSTED WITH NEW FRAME AND COVER	EACH	1

CABLE PLAN (NOT TO SCALE)

ENERGY COSTS TO:

VILLAGE OF BENSENVILLE
12 S Center Street
Bensenville, IL 60106

ENERGY SUPPLY: CONTACT: NEW BUSINESS
PHONE: NEW PHONE
COMPANY: COMMONWEALTH EDISON
ACCOUNT NUMBER: ---

FILE NAME =	USER NAME = doconnell
N:\Bensenville\180145\Traffic\York-Jefferson_03-CAB.dgn	
PLOT SCALE = 48"	
PLOT DATE = 11/14/2018	

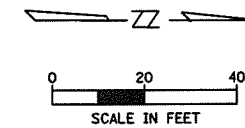
DESIGNED EAJ	REVISED -
DRAWN FPB	REVISED -
CHECKED GMZ	REVISED -
DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES, CABLE PLAN, PHASE DESIGNATION DIAGRAM AND EMERGENCY VEHICLE PREEMPTION SEQUENCE YORK ROAD AND JEFFERSON STREET

SCALE: SHEET OF SHEETS STA. TO STA.

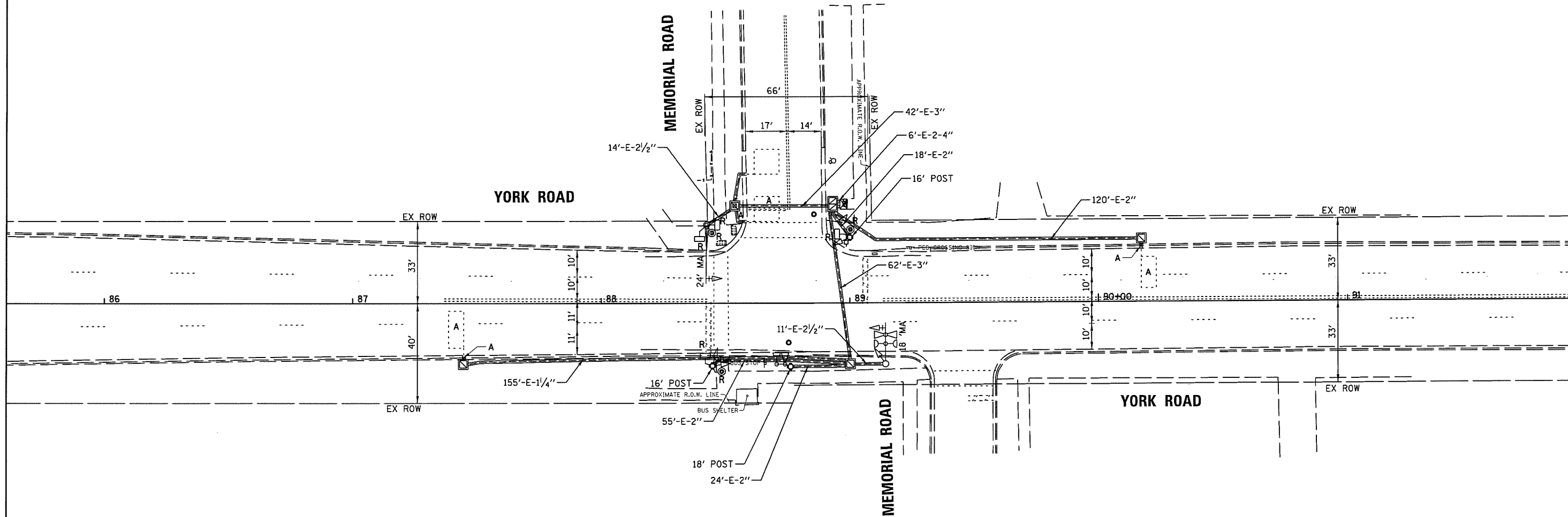
FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2678	18-00098-00-RS	DUPAGE	45	30
CONTRACT NO. 61F38			[ILLINOIS] FED. AID PROJECT	



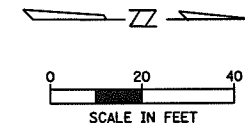
REMOVAL NOTES:

THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 4 EACH PEDESTRIAN SIGNAL HEAD
- 3 EACH PEDESTRIAN PUSH-BUTTON

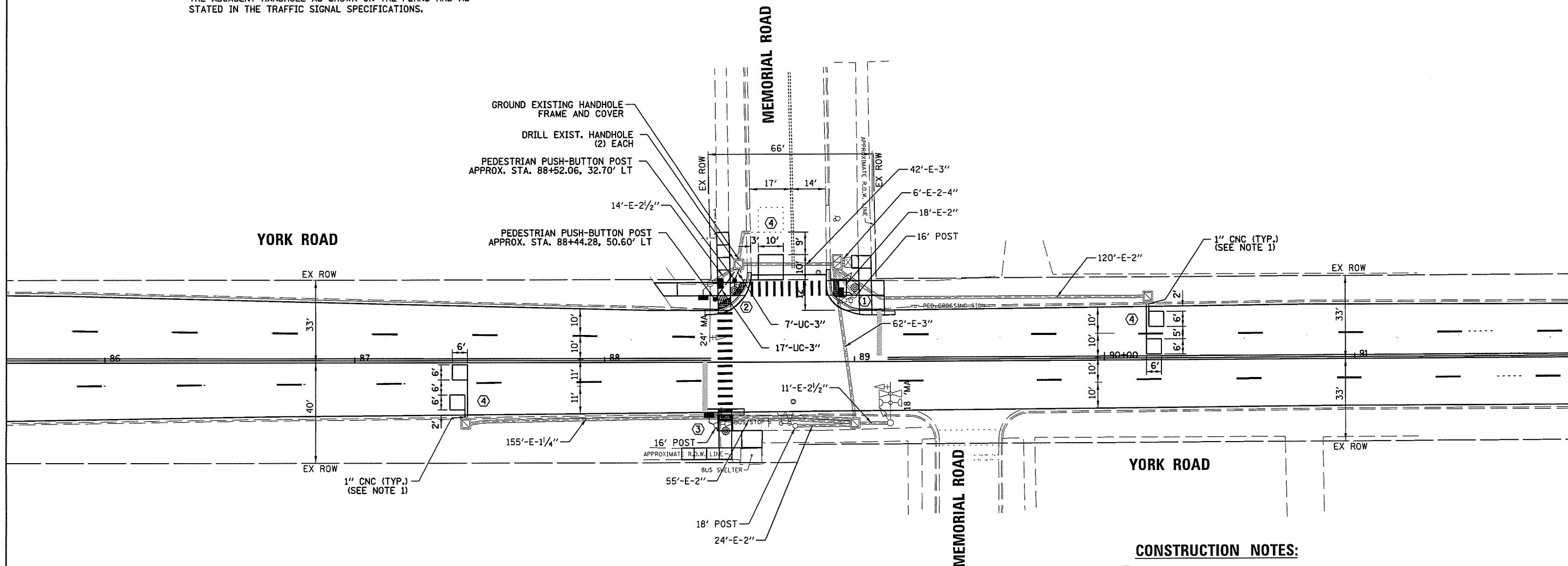


FILE NAME =	USER NAME = doconnell	DESIGNED EAJ	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	REMOVAL PLAN YORK ROAD AND MEMORIAL ROAD	FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
N:\Bensenville\180145\Traffic\York-Memor	a1.01-REM.dgn	DRAWN FPB	REVISED -			2678	18-00098-00-RS	DUPAGE	45	31	
Default	PLOT SCALE = 48'	CHECKED CMZ	REVISED -			CONTRACT NO. 61F38					
	PLOT DATE = 11/14/2018	DATE -	REVISED -			ILLINOIS FED. AID PROJECT					



NOTES:

1. EACH DETECTOR LOOP SHALL HAVE ITS OWN 1" COILABLE NON-METALLIC CONDUIT BETWEEN THE EDGE OF PAVEMENT AND THE ADJACENT HANDHOLE AS SHOWN ON THE PLANS AND AS STATED IN THE TRAFFIC SIGNAL SPECIFICATIONS.

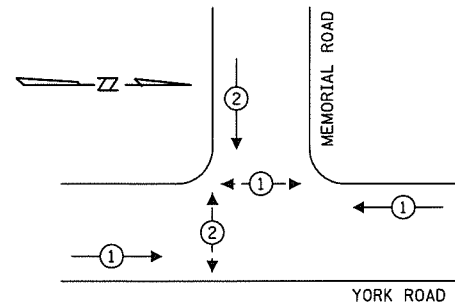


CONSTRUCTION NOTES:

- ① THE CONTRACTOR SHALL REMOVE AND REPLACE THE EXISTING PEDESTRIAN PUSHBUTTON. THE CONTRACTOR WILL BE REQUIRED TO DRILL NEW HOLES IN THE EXISTING TRAFFIC SIGNAL EQUIPMENT (AND PLUG THE EXISTING HOLES) TO ENSURE PROPER PUSHBUTTON PLACEMENT; THIS WORK WILL BE INCLUDED IN THE UNIT COST FOR PAY ITEM: PEDESTRIAN PUSH-BUTTON.
- ② THE CONTRACTOR SHALL INSTALL NEW PEDESTRIAN PUSH-BUTTON POSTS TO ELIMINATE THE EXISTING DUAL CALL PUSH-BUTTON CONFIGURATION.
- ③ THE CONTRACTOR SHALL INSTALL A NEW PEDESTRIAN PUSH-BUTTON AND PEDESTRIAN SIGNAL HEAD ON THE EXISTING POST UTILIZING THE EXISTING SIGNAL CABLE.
- ④ THE CONTRACTOR SHALL REPLACE THE EXISTING DETECTOR LOOPS THAT ARE DAMAGED FROM THE ROADWAY RESURFACING OPERATIONS.

FILE NAME =	USER NAME = doconnell	DESIGNED EAJ	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC SIGNAL MODIFICATION PLAN YORK ROAD AND MEMORIAL ROAD	FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
N:\Bensenville\180145\Traffic\York-Memor	a1_02-TSM.dgn	DRAWN FPB	REVISED -			2678	18-00098-00-RS	DUPAGE	45	32	
PLOT SCALE = 48'	CHECKED GMZ	REVIS	REVISED -			CONTRACT NO. 61F38					
PLOT DATE = 11/14/2018	DATE -	REVISED -	REVISED -			[ILLINOIS] FED. AID PROJECT					

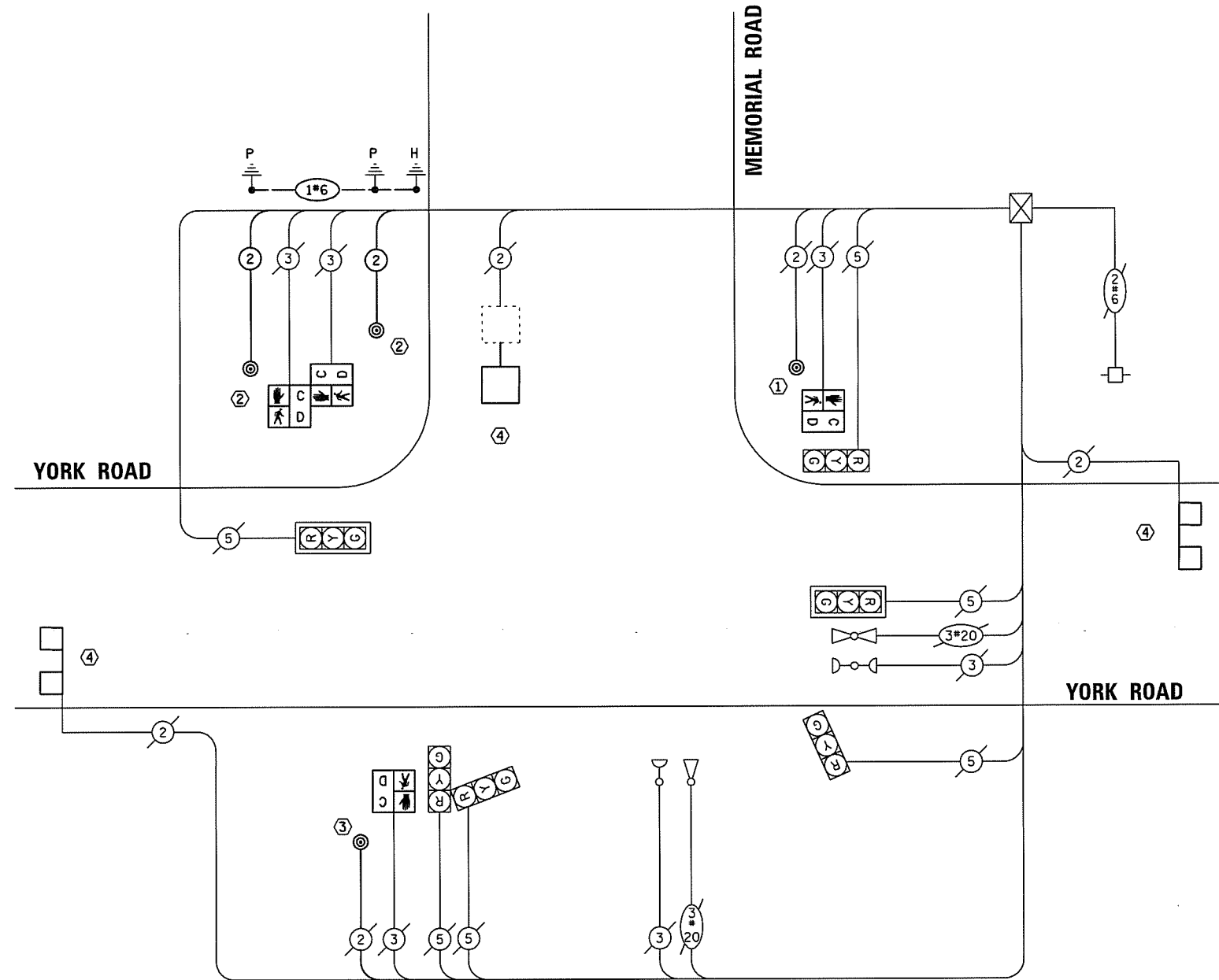
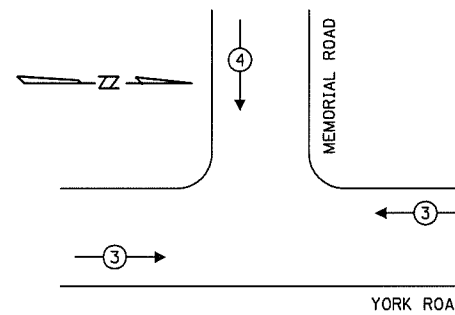
EXISTING CONTROLLER SEQUENCE



LEGEND:

- ← ⊙ → PROTECTED PHASE
- ← ⊙ - - → PROTECTED/PERMITTED PHASE
- ← ⊙ → PEDESTRIAN PHASE
- ← ⊙ OL → OVERLAP

EXISTING EMERGENCY VEHICLE PREEMPTION SEQUENCE



TRAFFIC SIGNAL ELECTRICAL SERVICE REQUIREMENTS

TYPE	NO. OF LAMPS	LED WATTAGE	% OPERATION	TOTAL WATTAGE
SIGNAL (RED)	6	11	50	33.0
(YELLOW)	6	20	5	6.0
(GREEN)	6	12	45	32.4
PERMISSIVE ARROW	-	10	10	-
PED. SIGNAL	4	20	100	80.0
CONTROLLER	1	100	100	100.0
UPS	-	25	100	-
VIDEO SYSTEM	-	150	100	-
BLANK-OUT SIGN	-	25	5	-
FLASHER	-	-	50	-
STREET NAME SIGN	-	120	50	-
LUMINAIRE	-	-	-	-
TOTAL =				251.4

ENERGY COSTS TO:

VILLAGE OF CHICAGO RIDGE
10455 S. Ridgeland Ave.
Chicago Ridge, Illinois 60415

ENERGY SUPPLY: CONTACT: NEW BUSINESS
PHONE: NEW PHONE
COMPANY: COMMONWEALTH EDISON
ACCOUNT NUMBER: ---

CONSTRUCTION NOTES:

- ① THE CONTRACTOR SHALL REMOVE AND REPLACE THE EXISTING PEDESTRIAN PUSHBUTTON. THE CONTRACTOR WILL BE REQUIRED TO DRILL NEW HOLES IN THE EXISTING TRAFFIC SIGNAL EQUIPMENT (AND PLUG THE EXISTING HOLES) TO ENSURE PROPER PUSHBUTTON PLACEMENT; THIS WORK WILL BE INCLUDED IN THE UNIT COST FOR PAY ITEM: PEDESTRIAN PUSH-BUTTON.
- ② THE CONTRACTOR SHALL INSTALL NEW PEDESTRIAN PUSH-BUTTON POSTS TO ELIMINATE THE EXISTING DUAL CALL PUSH-BUTTON CONFIGURATION.
- ③ THE CONTRACTOR SHALL INSTALL A NEW PEDESTRIAN PUSH-BUTTON AND PEDESTRIAN SIGNAL HEAD ON THE EXISTING POST UTILIZING THE EXISTING SIGNAL CABLE.
- ④ THE CONTRACTOR SHALL REPLACE THE EXISTING DETECTOR LOOPS THAT ARE DAMAGED FROM THE ROADWAY RESURFACING OPERATIONS.

CABLE PLAN
(NOT TO SCALE)

SCHEDULE OF QUANTITIES

ITEM	UNIT	QUANTITY
UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	24
MAINTENANCE OF EXISTING TRAFFIC SIGNAL INSTALLATION	EACH	1
GROUNDING EXISTING HANDHOLE FRAME AND COVER	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	190
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	55
CONCRETE FOUNDATION, TYPE A	FOOT	8
DRILL EXISTING HANDHOLE	EACH	2
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, BRACKET MOUNTED WITH COUNTDOWN TIMER	EACH	4
DETECTOR LOOP REPLACEMENT	FOOT	230
PEDESTRIAN PUSH-BUTTON	EACH	4
MODIFY EXISTING CONTROLLER	EACH	1
MODIFY EXISTING CONTROLLER CABINET	EACH	1
REMOVE ELECTRIC CABLE FROM CONDUIT	FOOT	97
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
PEDESTRIAN PUSH-BUTTON POST, TYPE A	EACH	2

FILE NAME =	USER NAME = doconnell
N:\Bensenville\180145\Traffic\York-Memorial_03-CAB.dgn	
PLOT SCALE = 40'	
PLOT DATE = 11/14/2018	

DESIGNED EAJ	REVISED -
DRAWN FPB	REVISED -
CHECKED GMZ	REVISED -
DATE -	REVISED -

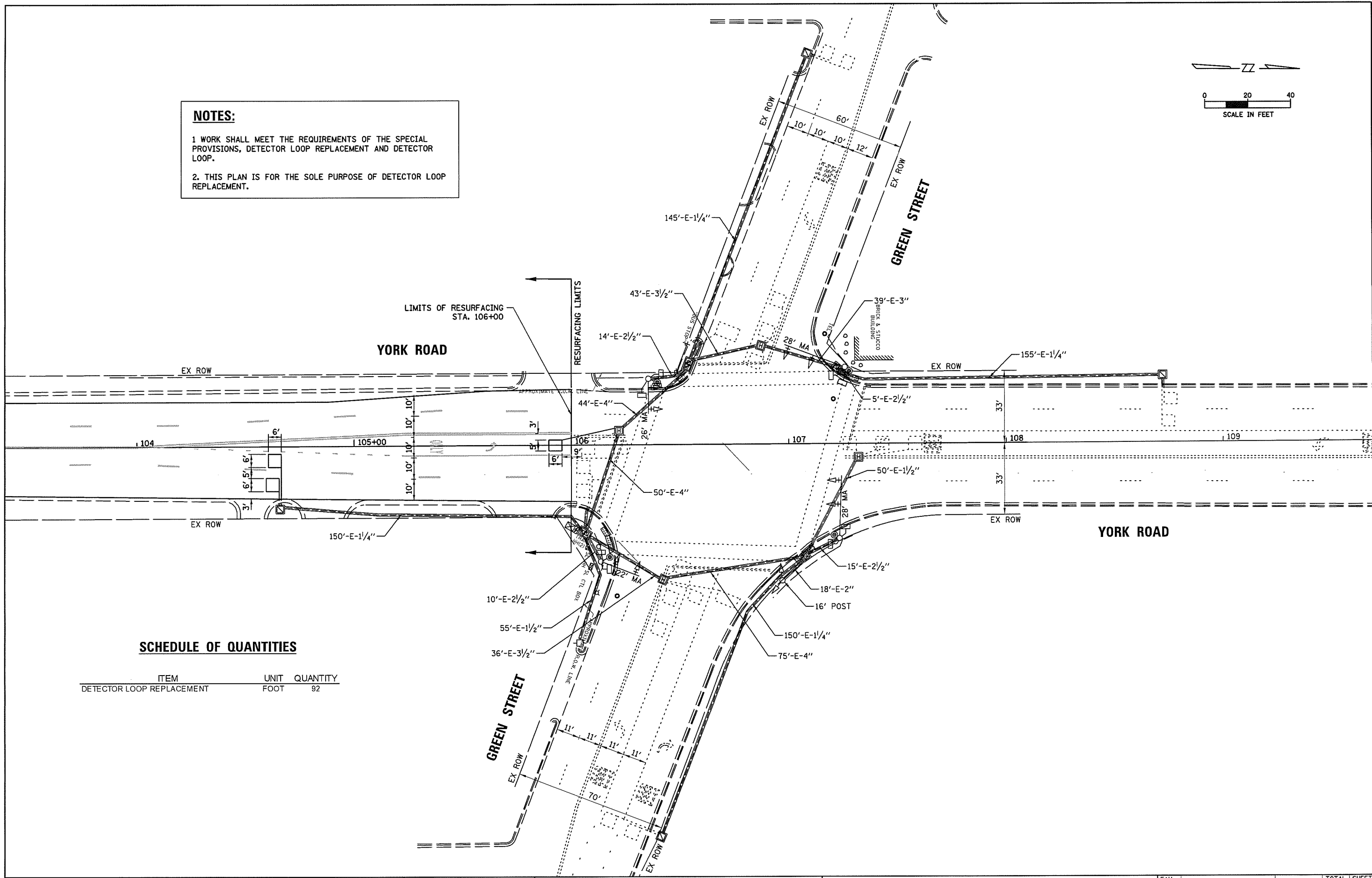
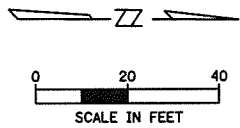
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SCHEDULE OF QUANTITIES, CABLE PLAN, PHASE DESIGNATION
DIAGRAM AND EMERGENCY VEHICLE PREEMPTION SEQUENCE
YORK ROAD AND MEMORIAL ROAD

FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2678	18-00098-00-RS	DUPAGE	45	33
CONTRACT NO. 61F38				
ILLINOIS FED. AID PROJECT				

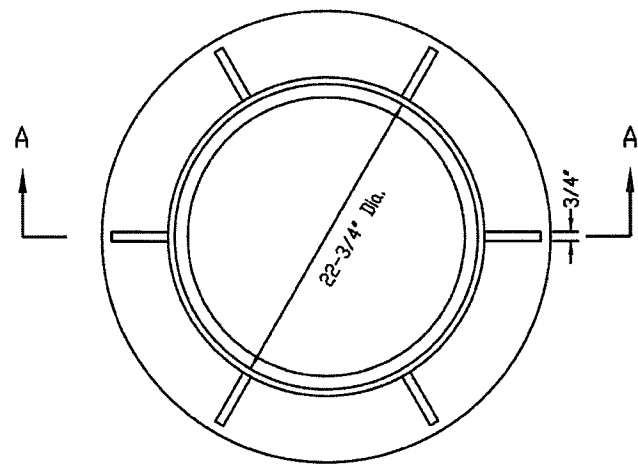
SCALE: SHEET OF SHEETS STA. TO STA.

NOTES:
 1 WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISIONS, DETECTOR LOOP REPLACEMENT AND DETECTOR LOOP.
 2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

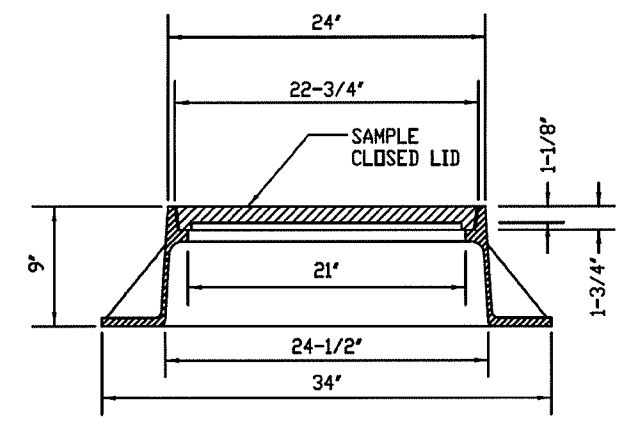


SCHEDULE OF QUANTITIES

ITEM	UNIT	QUANTITY
DETECTOR LOOP REPLACEMENT	FOOT	92

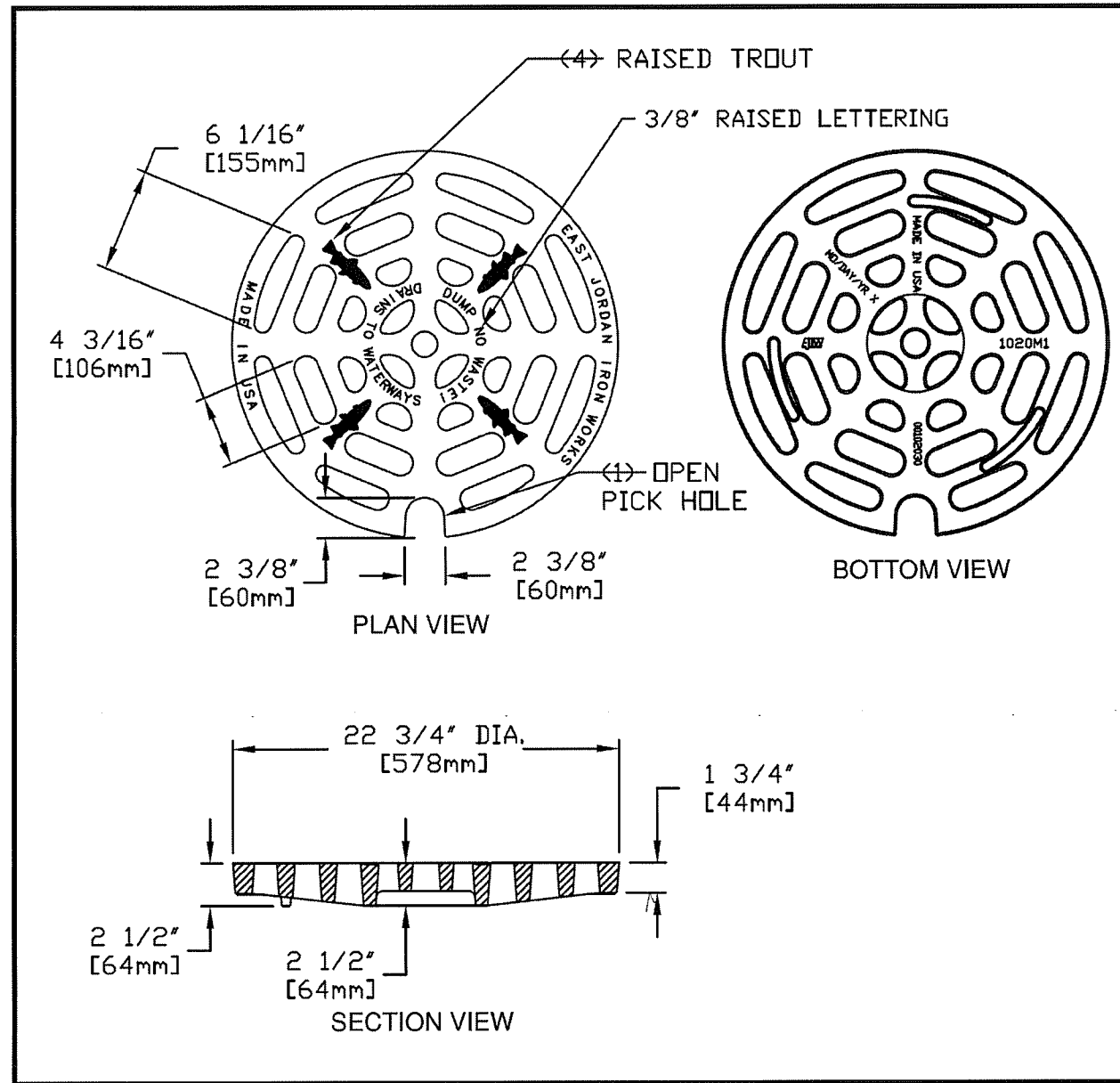


PLAN



SECTION A-A

FURNISH HEAVY DUTY CASTING
AS REQUIRED BY THE VILLAGE ENGINEER



(*) or NEENAH CATALOG R-2015 SHOP DRAWING NF-2015 T17

OPEN LID

(*) EAST JORDAN IRON WORKS, INC. P.O. BOX 439 EAST JORDAN, MI. 49727 1-800-874-4100 FAX 231-536-4458	
DRAWN TCL	DATE 09/26/02
APPROVED	DATE
CATCH BASIN GRATE	
PRODUCT NO. (*) 00102030 DIPPED	
CATALOG NO. (*) 1020M1	
REF. PRODUCT DRAWING 102031	
EST. WT. GRATE: 124 LBS (56 kg)	
OPEN AREA 120 SQ. INCHES (774 SQ. CM)	
MAT'L SPEC. GRATE - GRAY IRON ASTM A45 CL 35B	
LOAD RATING EXTRA HEAVY DUTY	

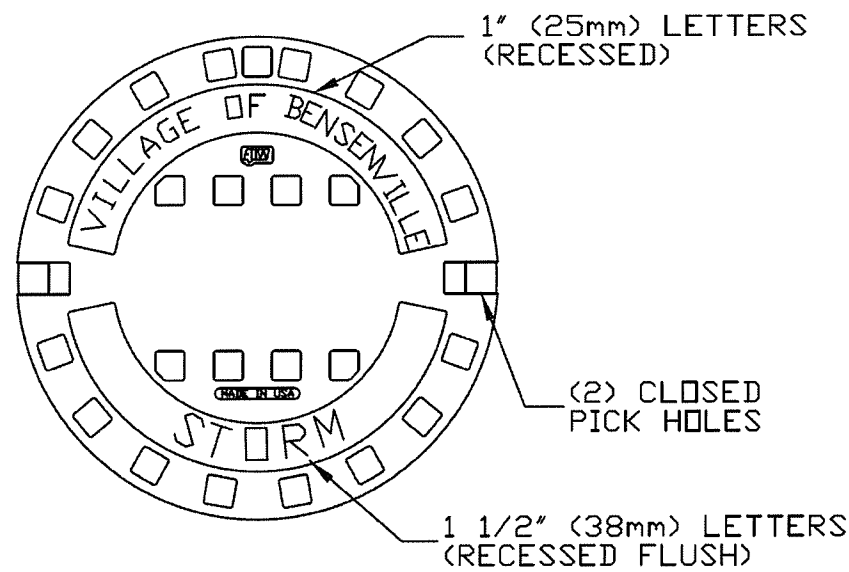


VILLAGE OF BENSENVILLE
12 S. CENTER STREET
BENSENVILLE, IL 60106
WWW.BENSENVILLE.IL.US

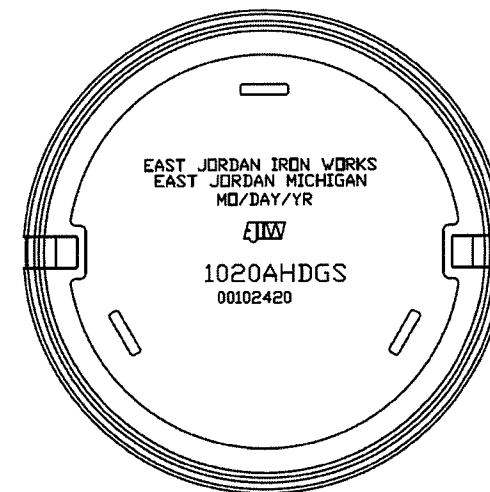
**STORM FRAME AND LID
STANDARD (OPEN)**

VILLAGE OF BENSENVILLE
SPECIFICATIONS AND DETAILS
FILE LOCATION
G:\ENGINEERING\STANDARDS\NEW
STANDARDS\STORM

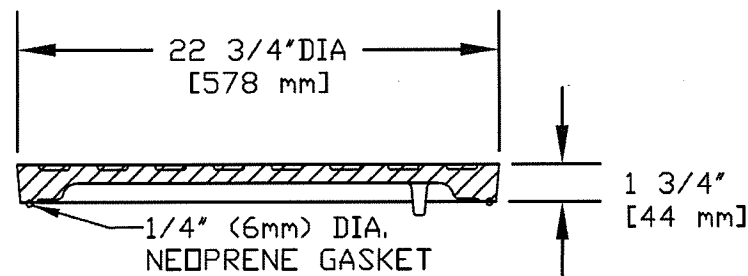
REVISIONS	



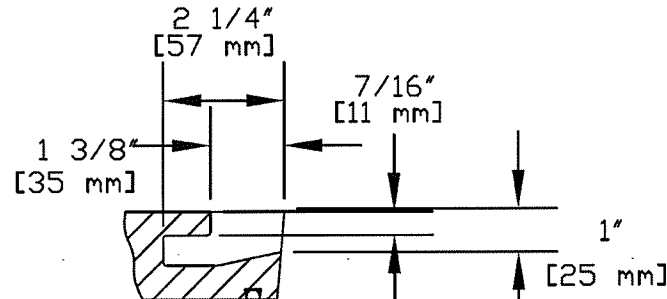
TOP VIEW



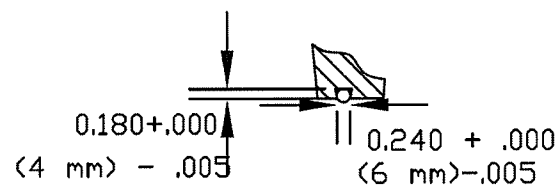
BOTTOM VIEW



CROSS SECTION



PICK HOLE DETAIL



GROOVE DETAIL

(*) OR NEENAH CATALOG R-1713 SHOP DRAWING NF-1015 T45

(*) **EAST JORDAN IRON WORKS, INC.**
 P.O. BOX 439
 EAST JORDAN, MI. 49727
 1-800-874-4100
 FAX 231-536-4458

DRAWN SMH	DATE 11/11/02
APPROVED	DATE

SPECIAL LETTERED COVER

(*) PRODUCT NO.
00102421

CATALOG NO.
 (*) **1020A**

REF. PRODUCT DRAWING
 102067

EST. WT.
 COVER WT: 115 LBS.
 (52 kg)

OPEN AREA
 N/A

MAT'L SPEC.
 COVER - GRAY IRON
 ASTM A48 CL 35

LOAD RATING
HEAVY DUTY



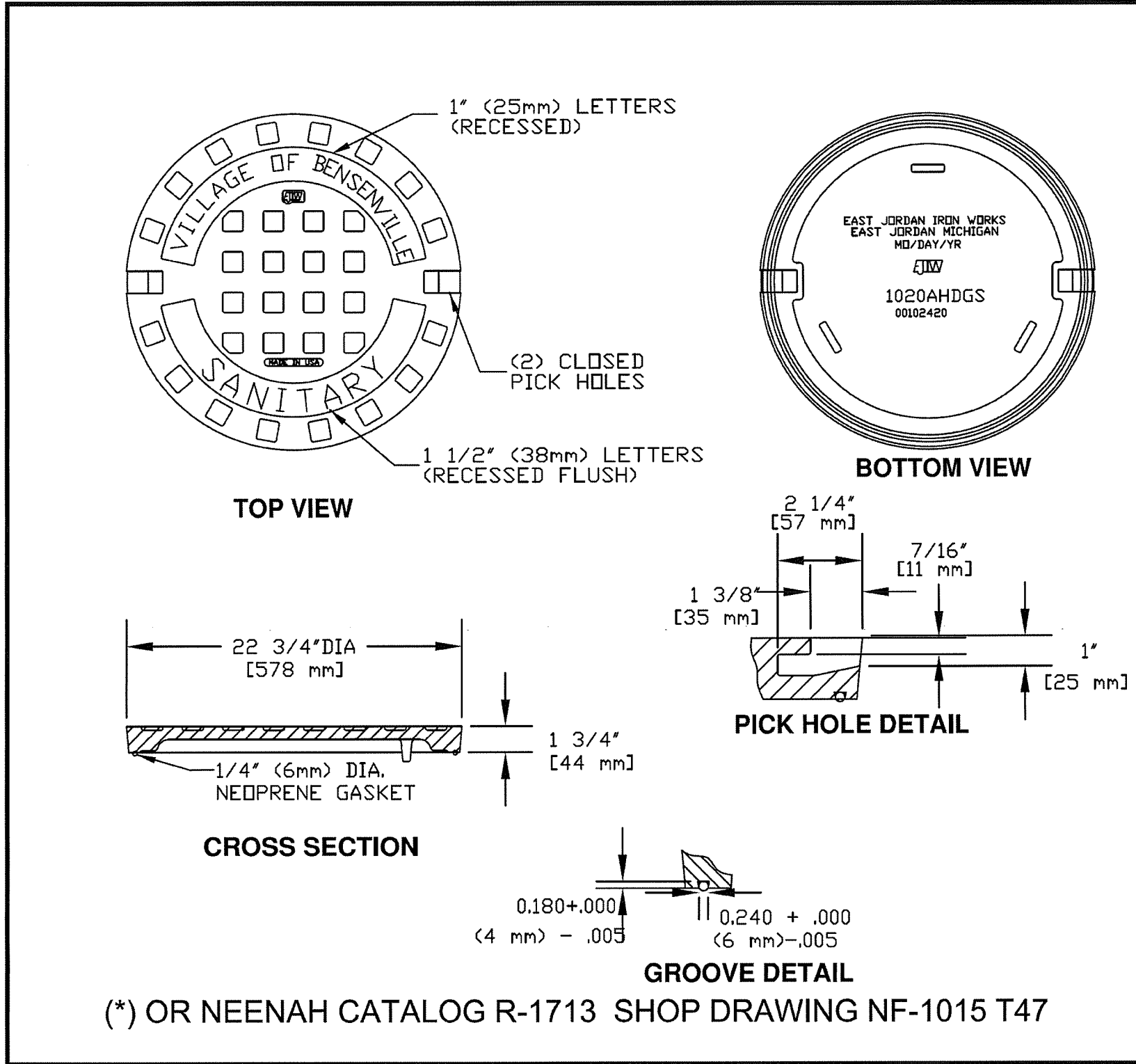
VILLAGE OF BENSENVILLE
 12 S. CENTER STREET
 BENSENVILLE, IL 60106
 WWW.BENSENVILLE.IL.US

**STORM FRAME AND LID
 STANDARD (CLOSED)**

VILLAGE OF BENSENVILLE
 SPECIFICATIONS AND DETAILS
 FILE LOCATION
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 STANDARDS\STORM

REVISIONS

FILE NAME = N:\Bensenville\180145\Civil\DET_LA.180145	USER NAME = doconnell	DESIGNED MLM	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	VILLAGE OF BENSENVILLE YORK ROAD VILLAGE DETAILS	FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
PLOT SCALE = 2"	02.SHT	DRAWN MLM	REVISED -			2678	18-00098-00-RS	DUPAGE	45	36	
PLOT DATE = 11/14/2018		CHECKED JGS	REVISED -			SCALE:	SHEET 2 OF 4 SHEETS	TO STA.	CONTRACT NO. 61F38		
		DATE	REVISED -			ILLINOIS FED. AID PROJECT					



EAST JORDAN IRON WORKS, INC. P.O. BOX 439 EAST JORDAN, MI. 49727 1-800-874-4100 FAX 231-536-4458	
DRAWN SMH	DATE 11/11/02
APPROVED	DATE
SPECIAL LETTERED COVER	
(*) PRODUCT NO. 00102420	
CATALOG NO. (*) 1020AGS	
REF. PRODUCT DRAWING 00102108	
EST. WT. COVER WT: 115 LBS. (52 kg)	
OPEN AREA N/A	
MAT'L SPEC. COVER - GRAY IRON ASTM A48 CL 35	
LOAD RATING HEAVY DUTY	

(*) OR NEENAH CATALOG R-1713 SHOP DRAWING NF-1015 T47

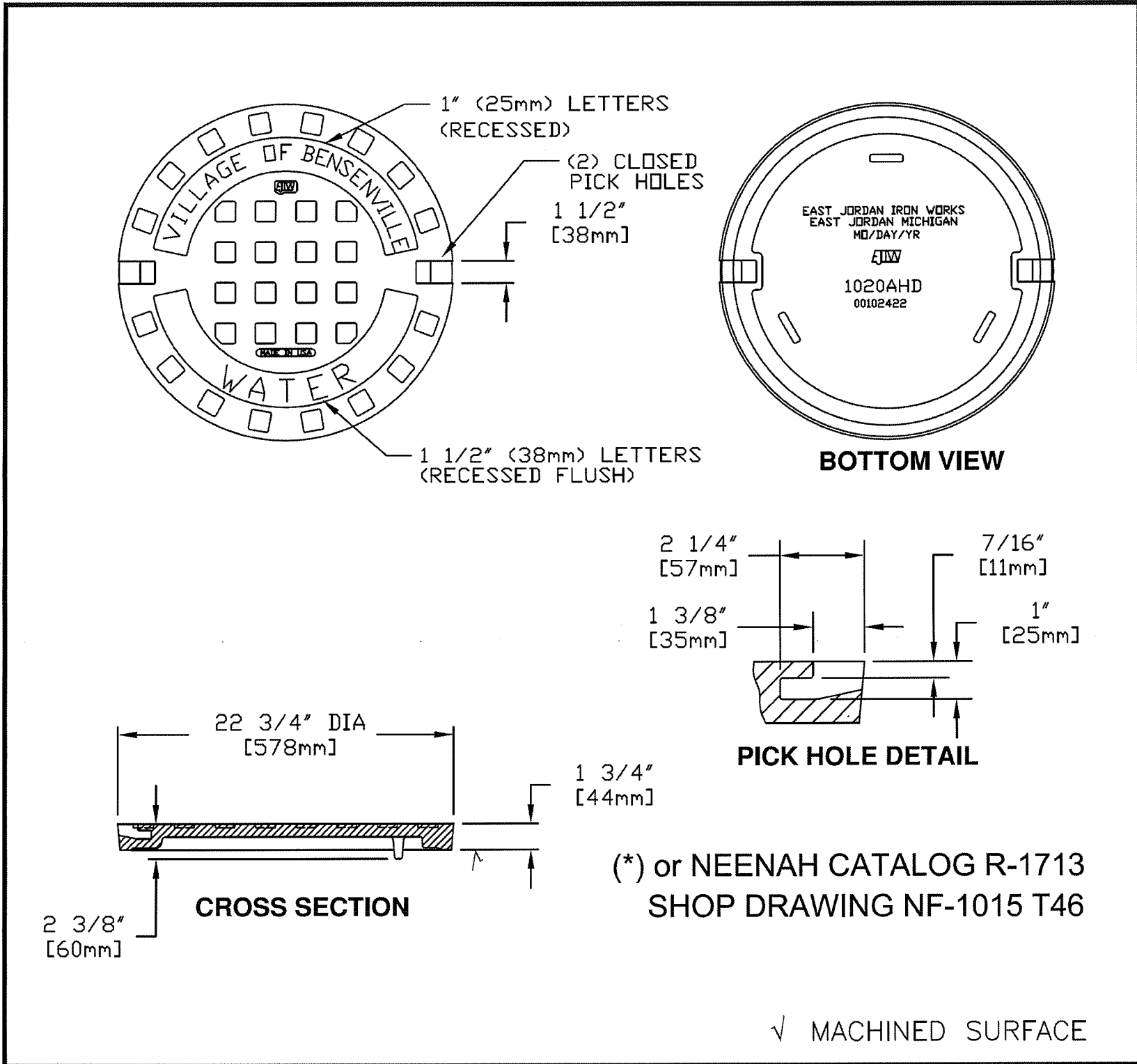


VILLAGE OF BENSENVILLE
12 S. CENTER STREET
BENSENVILLE, IL 60106
WWW.BENSENVILLE.IL.US

SANITARY FRAME AND LID STANDARD

VILLAGE OF BENSENVILLE
SPECIFICATIONS AND DETAILS
FILE LOCATION
G:\ENGINEERING\STANDARDS\NEW STANDARDS\SANITARY

REVISIONS	



(*) **EAST JORDAN IRON WORKS, INC.**
 P.O. BOX 439
 EAST JORDAN, MI. 49727
 1-800-874-4100
 FAX 231-536-4458

DRAWN SMH	DATE 11/11/02
APPROVED	DATE

SPECIAL LETTERED COVER

(*) PRODUCT NO.
00102422

CATALOG NO.
 (*) **1020A**

REF. PRODUCT DRAWING
 00102067

EST. WT.
 COVER WT: 115 LBS
 (52kg)

OPEN AREA
 N/A

MAT'L SPEC.
 COVER - GRAY IRON
 ASTM A48 CL 35

LOAD RATING
HEAVY DUTY

B
 VILLAGE OF BENSENVILLE
 12 S. CENTER STREET
 BENSENVILLE, IL 60106
 WWW.BENSENVILLE.IL.US

**FRAME AND CLOSE LID
 WATER STANDARD**

VILLAGE OF BENSENVILLE
 SPECIFICATIONS AND DETAILS
 FILE LOCATION
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 STANDARDS\WATER

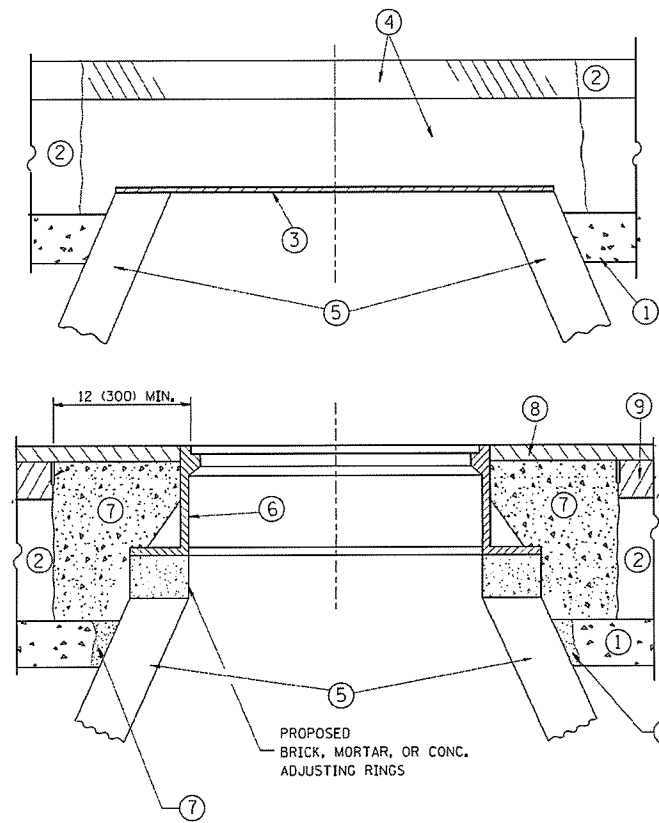
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	PLOT DATE = 11/14/2018	DATE -	REVISED -

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

VILLAGE OF BENSENVILLE
 YORK ROAD
 VILLAGE DETAILS
 SCALE: SHEET 4 OF 4 SHEETS STA. TO STA.

FAU RTE. 2678	SECTION 18-00098-00-RS	COUNTY DUPAGE	TOTAL SHEETS 45	SHEET NO. 38
CONTRACT NO. 61F38				ILLINOIS FED. AID PROJECT



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

*UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

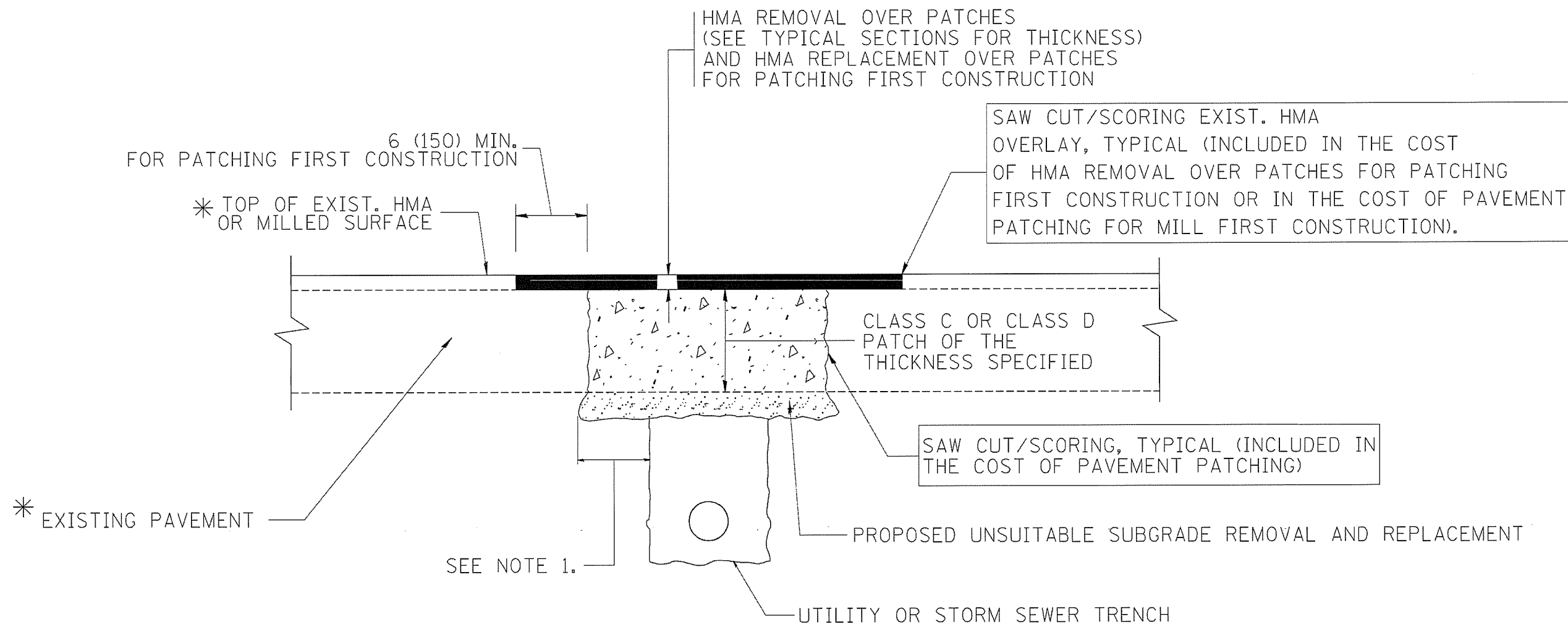
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		CHECKED -	REVISED - R. BORO 03-09-11
		DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2678	18-00098-00-RS	DUPAGE	45	39
BD600-03 (BD-8)		CONTRACT NO. 61F38		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

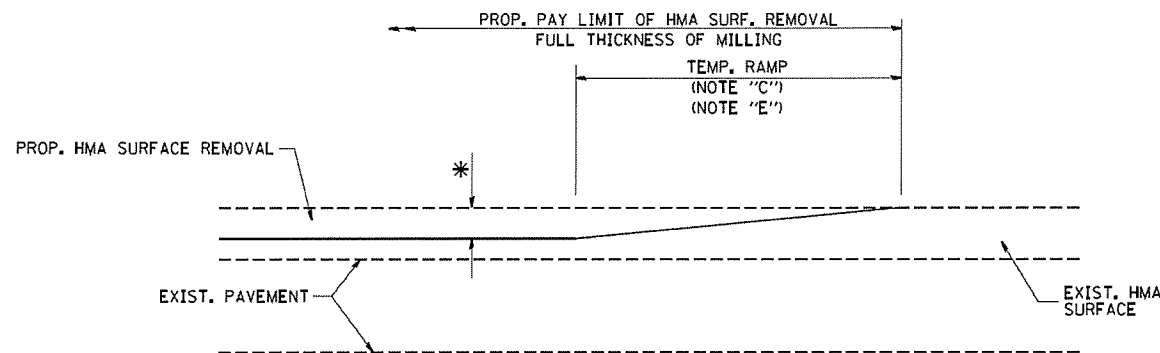
1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

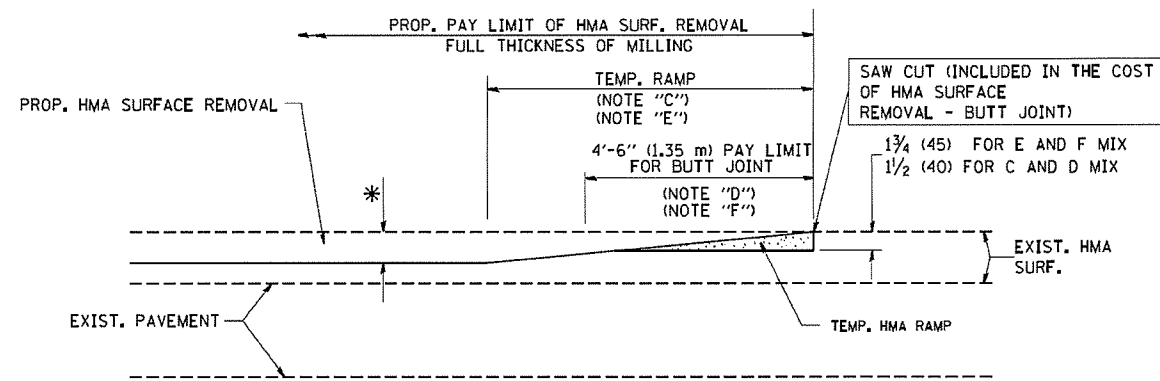
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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	PLOT SCALE = 5/8" = 1' IN.	DRAWN -	REVISED - R. BORO 01-01-07		SCALE: NONE	SHEET NO. 1	OF 1	SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT	
	PLOT DATE = 10/27/2008	CHECKED -	REVISED - R. BORO 09-04-07					BD400-04 (BD-22)		CONTRACT NO. 61F38		
		DATE - 10-25-94	REVISED - K. ENG 10-27-08									



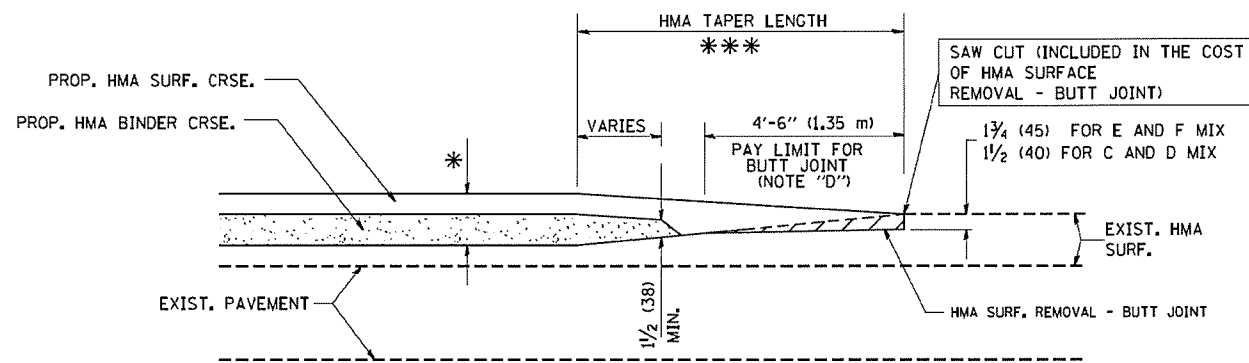
MILLED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

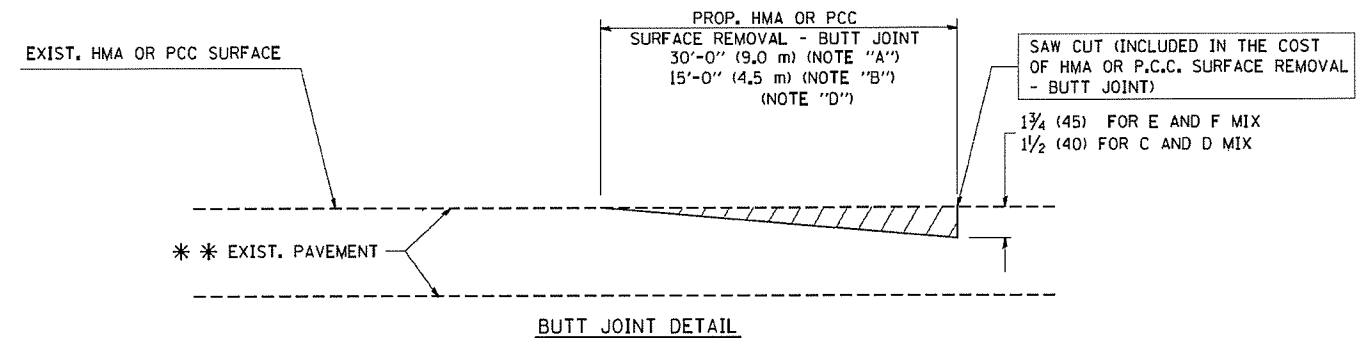


HMA CONSTRUCTED TEMPORARY RAMP
(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

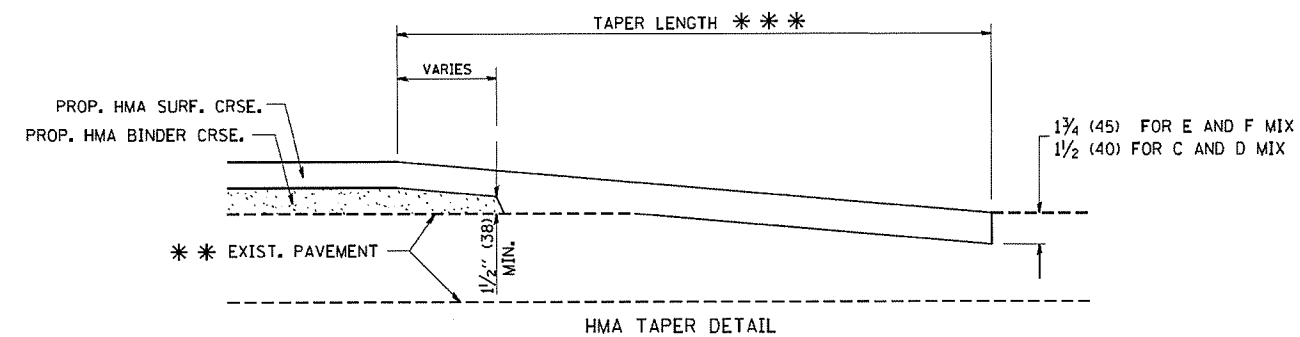
OPTION 2
TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER
FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER
FOR RESURFACING ONLY

*** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

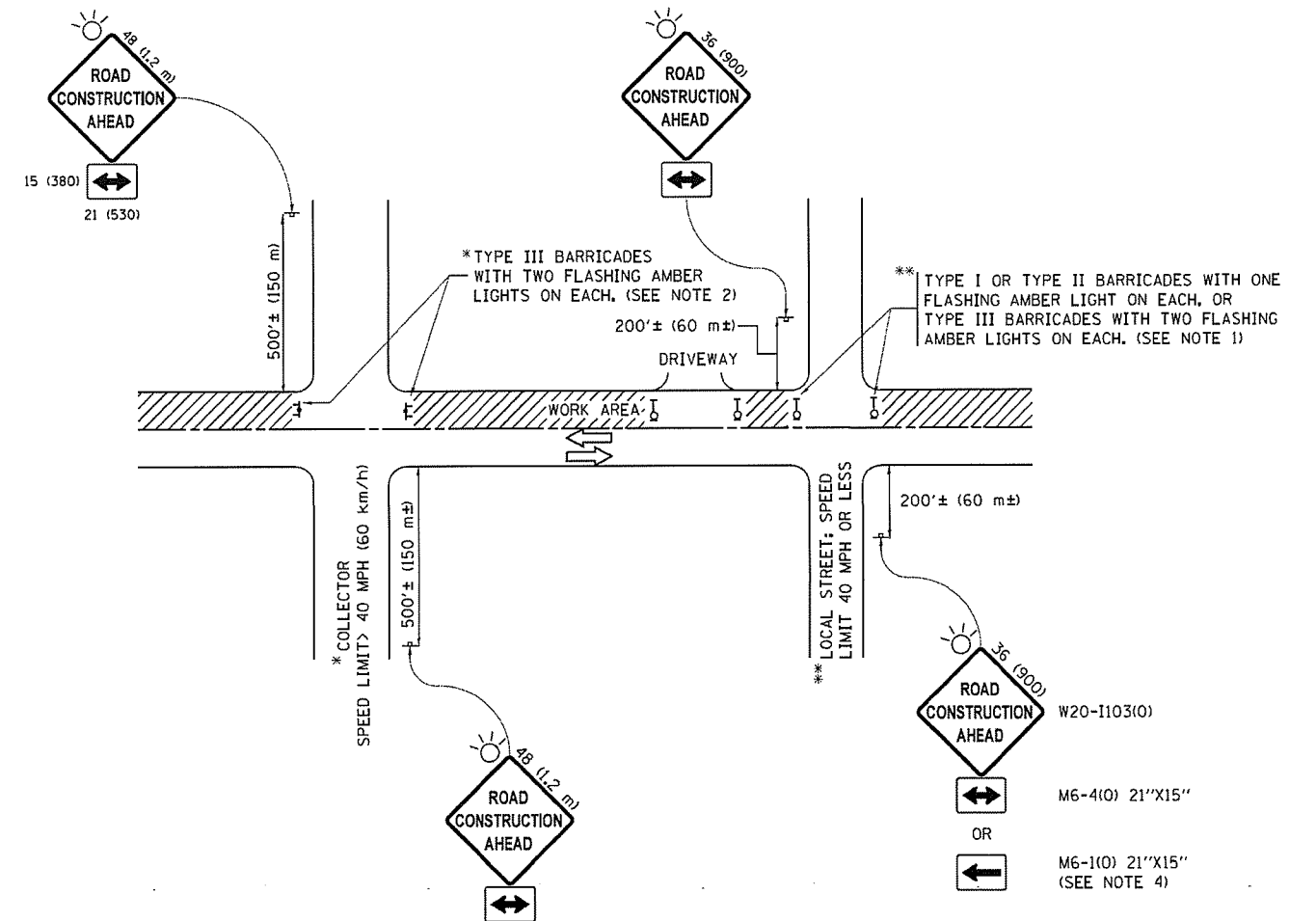
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 5/8" = 1' IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

BUTT JOINT AND HMA TAPER DETAILS			
SCALE: NONE	SHEET NO. 1	OF 1 SHEETS	STA. TO STA.

F.A.U. RTE. 2678	SECTION 18-00098-00-RS	COUNTY DUPAGE	TOTAL SHEETS 45	SHEET NO. 41
BD400-05 BD32		CONTRACT NO. 61F38		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



NOTES:

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

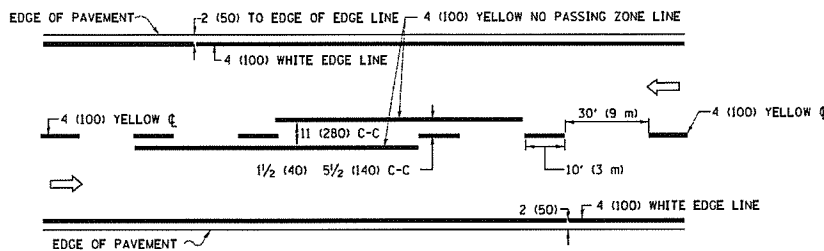
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	PLOT DATE = 9/15/2016		REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

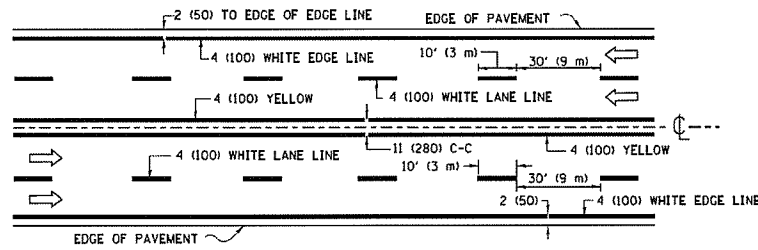
**TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

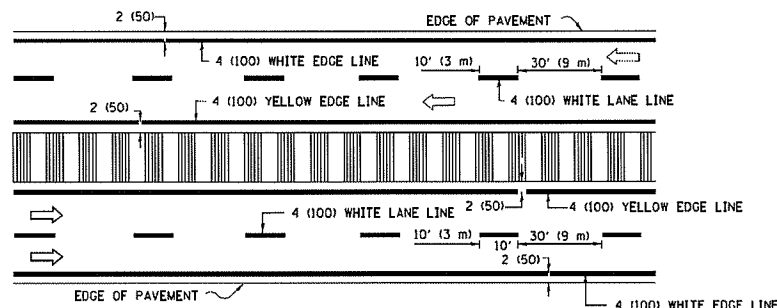
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2678	18-00098-00-RS	DUPAGE	45	42
TC-10			CONTRACT NO. 61F38	
[ILLINOIS] FED. AID PROJECT				



2-LANE ROADWAY

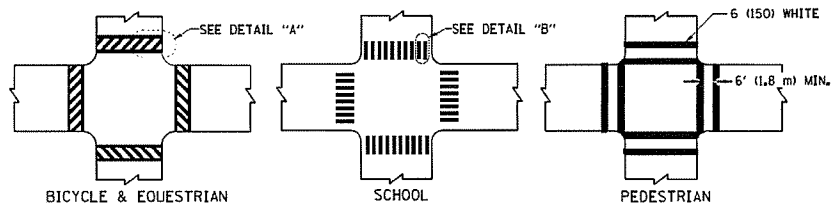


MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

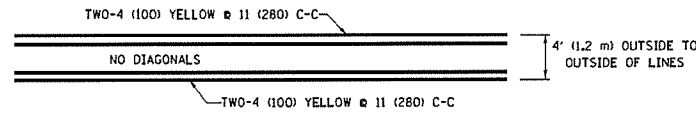


DETAIL "A"

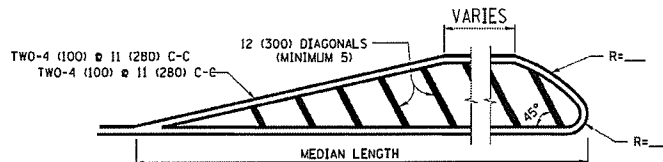
DETAIL "B"

TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

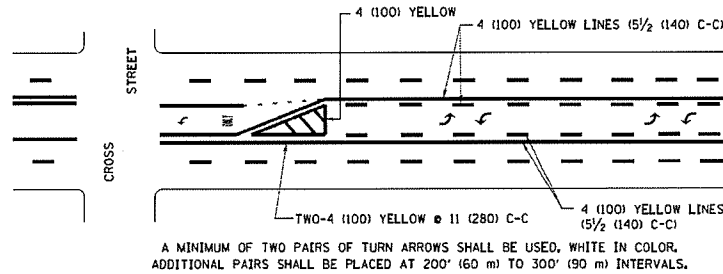


4' (1.2 m) WIDE MEDIANS ONLY

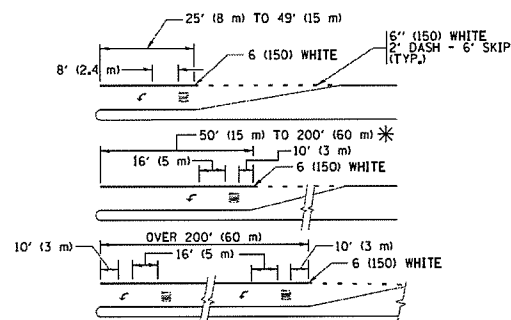


MEDIANS OVER 4' (1.2 m) WIDE

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))



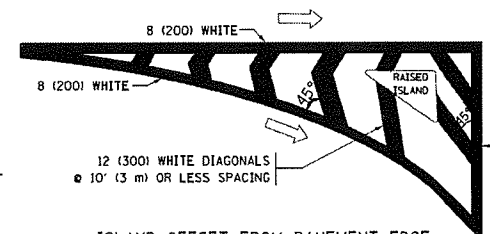
**MEDIAN WITH TWO-WAY LEFT TURN LANE
TYPICAL PAINTED MEDIAN MARKING**



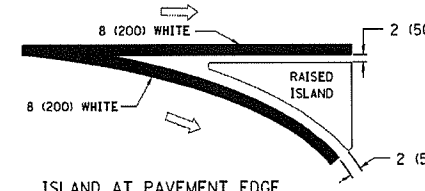
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

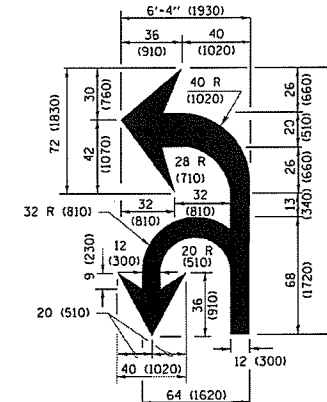
TYPICAL TURN LANE MARKING



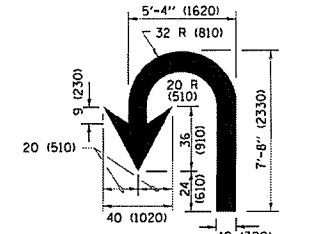
ISLAND OFFSET FROM PAVEMENT EDGE



**ISLAND AT PAVEMENT EDGE
TYPICAL ISLAND MARKING**



COMBINATION LEFT AND U-TURN



U-TURN

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

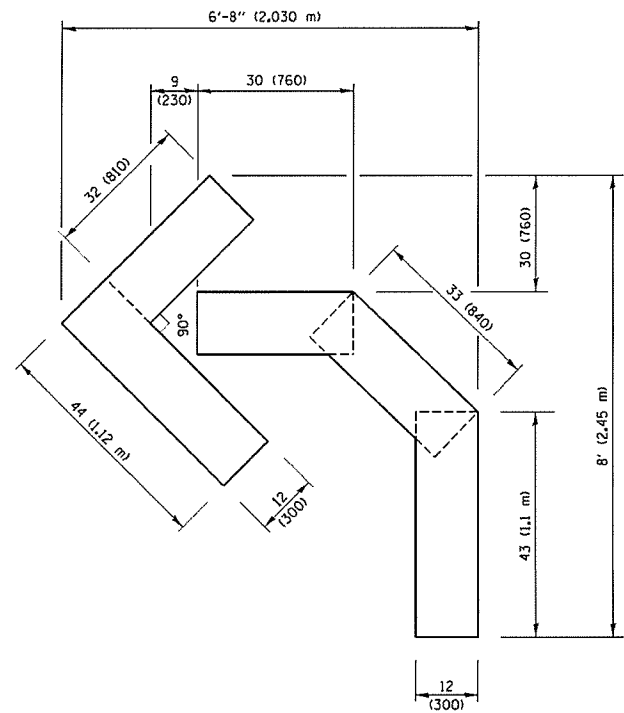
LANE REDUCTION TRANSITION
* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS 18' (2.4m)	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; WHITE; WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" 15' 6" (4.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF "RR"=3.6 SQ. FT. (0.33 m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

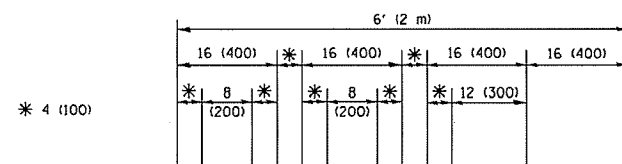
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME = W:\data\22x34\1013.dgn	USER NAME = lrysa	DESIGNED - EVERS	REVISED - C. JUCIUS 09-09-09	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT ONE TYPICAL PAVEMENT MARKINGS	F.A.U. RTE. 2678	SECTION 18-0098-00-RS	COUNTY DuPAGE	TOTAL SHEETS 45	SHEET NO. 43		
PLOT SCALE = 50,000' / 1" =	CHECKED - C. JUCIUS 12-21-15	REVISOR - C. JUCIUS 07-01-13	DATE - 03-19-90			SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.		TC-13 CONTRACT NO. 61F38				
PLOT DATE = 6/23/2017	DATE - 03-19-90	REVISOR - C. JUCIUS 12-21-15	DATE - 04-12-16			(ILLINOIS) FED. AID PROJECT						



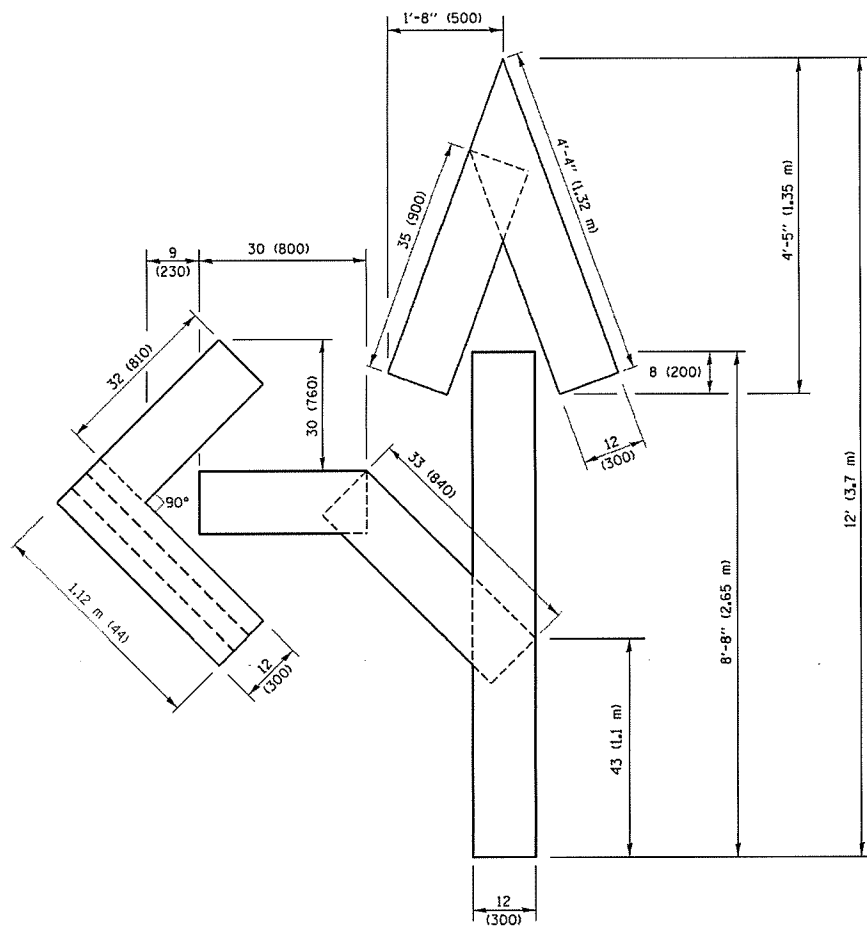
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

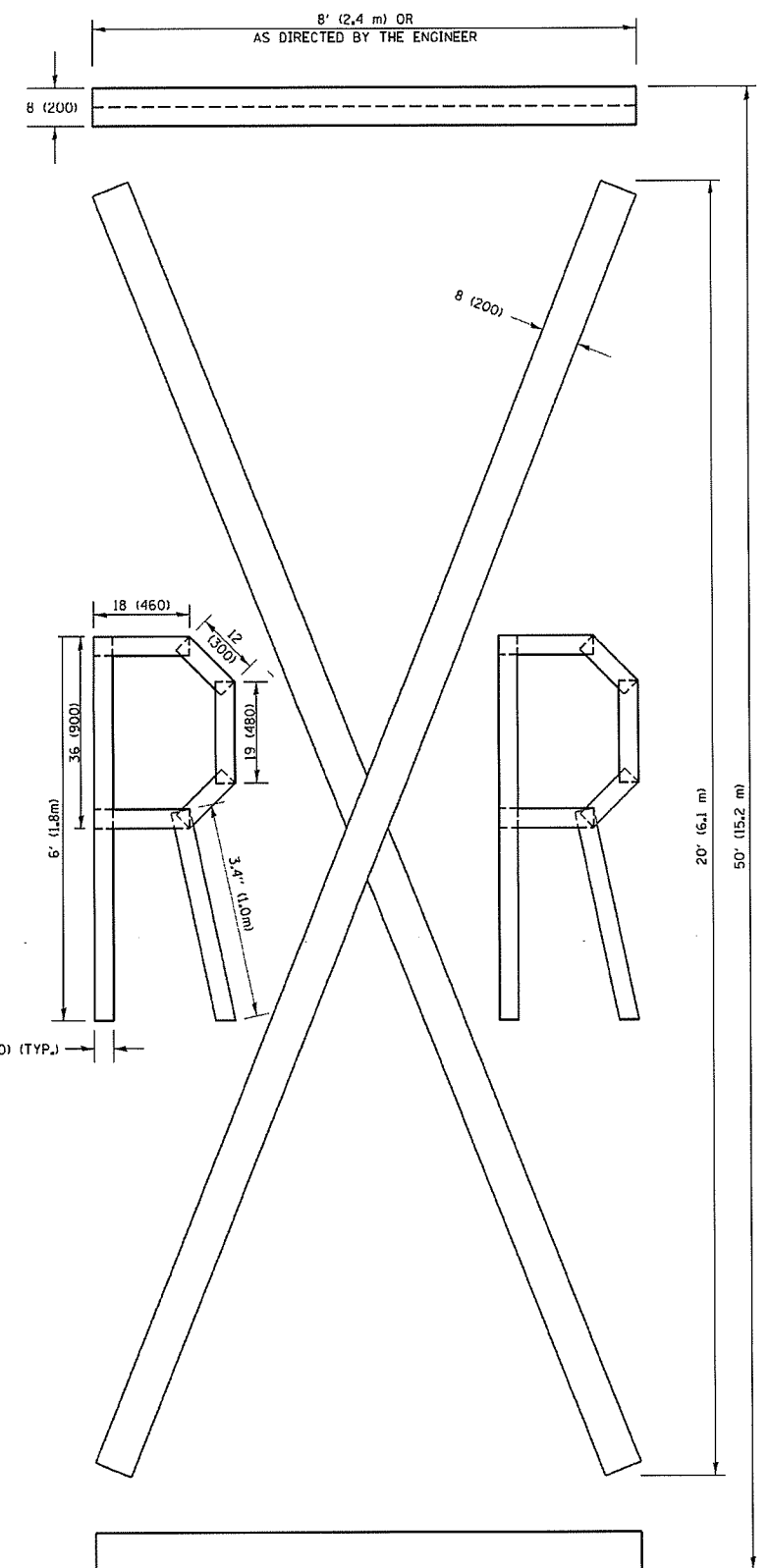


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.

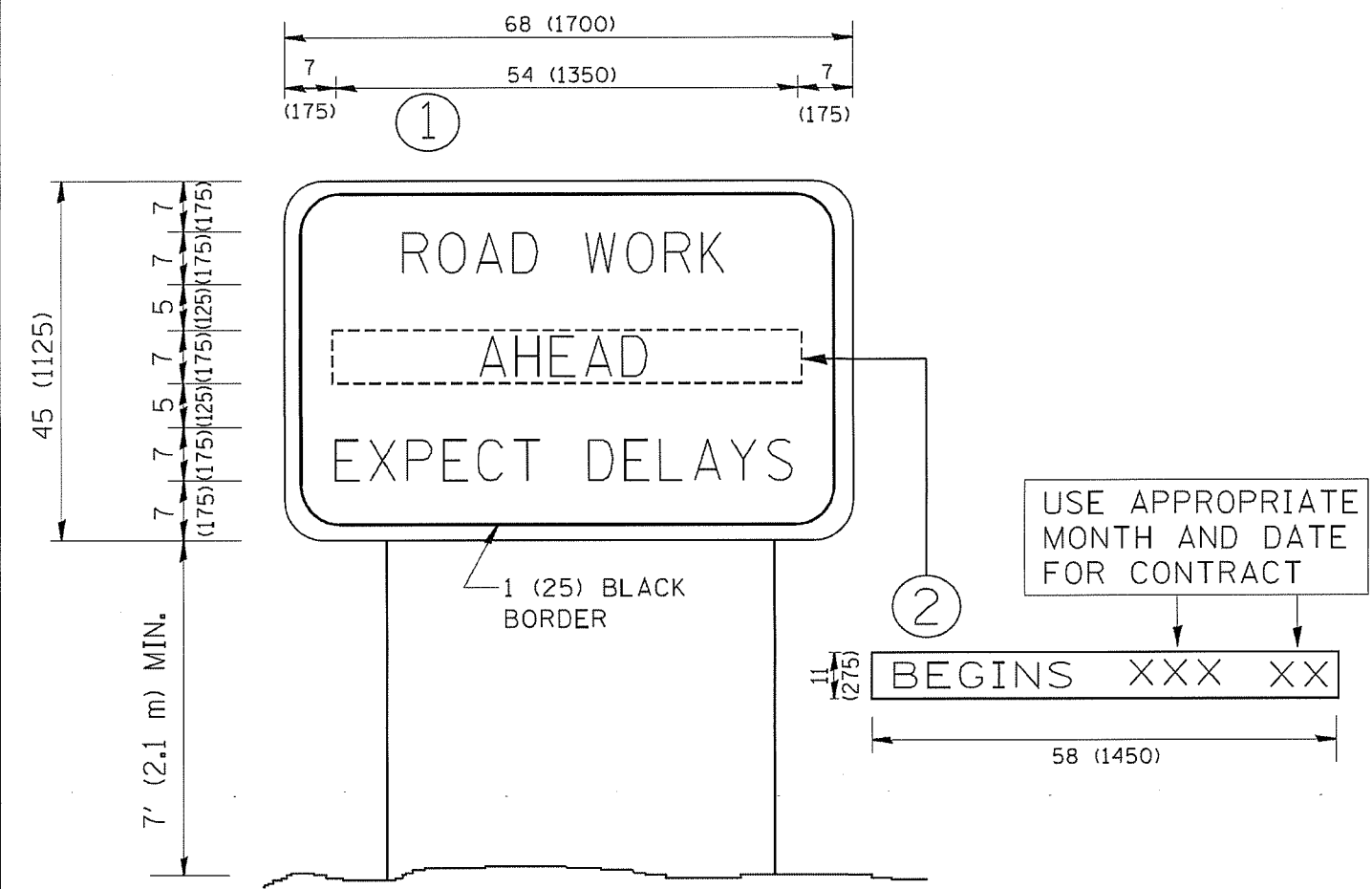


QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)

All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = foatomj	DESIGNED -	REVISED -T, RAMMACHER 03-02-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS			F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
PROJECT =	PROJECT =	DRAWN	REVISED - E. GOMEZ 08-28-00					2678	18-00098-00-RS	DUPAGE	45	44
PLOT SCALE = 50,000' / 1" =	CHECKED -	REVISOR - E. GOMEZ 08-28-00	REVISED - E. GOMEZ 08-28-00		TC-16			CONTRACT NO. 61F38				
PLOT DATE = 9/15/2016	DATE - 09-18-94	REVISED - A. SCHUETZE 09-15-16	SCALE: NONE		SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststa\22x34\cc22.dgn	USER NAME = gegliano1	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN			F.A.U. RTE. 2678	SECTION 18-00098-00-RS	COUNTY DUPAGE	TOTAL SHEETS 45	SHEET NO. 45
	PLOT SCALE = 50.000 ' / IN.	DRAWN -	REVISED - R. MIRS 12-11-97		SCALE: NONE			SHEET NO. 1 OF 1 SHEETS			STA. TO STA.	
	PLOT DATE = 1/4/2008	CHECKED -	REVISED - T. RAMMACHER 02-02-99		TC-22			CONTRACT NO. 61F38				
		DATE -	REVISED - C. JUCIUS 01-31-07		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT							