

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION  
**PROPOSED  
 HIGHWAY PLANS**

F.A.P. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	125RS-2	COOK	33	1
		ILLINOIS CONTRACT NO. 62F23		

D-91-267-17



FOR INDEX OF SHEETS, SEE SHEET NO. 2

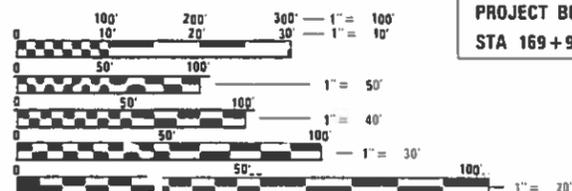
THIS PROJECT IS LOCATED IN THE VILLAGES OF ARLINGTON HEIGHTS, WHEELING AND THE CITY OF PROSPECT HEIGHTS

1) F.A.P. ROUTE 305: PALATINE ROAD  
 U.S. ROUTE 12 (RAND ROAD) TO IL. ROUTE 21  
 2) F.A.P. ROUTE 343: DUNDEE ROAD  
 EAST OF KENNICOTT AVE. TO FIRST STREET  
 SECTION: 2017-020RS  
 PROJECT: NHPP-97ID(676)  
 1) RESURFACING (3P); 2) PATCHING  
 COOK COUNTY  
 C-91-267-17

**TRAFFIC DATA**

2014 ADT=41,200

SPEED LIMIT = 35-55 MPH



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.  
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION  
 1-800-892-0123  
 OR 811

PROJECT ENGINEER: ALAIN MIDY (847) 221-3056

PROJECT MANAGER: FAWAD AQUEEL (847) 705-4247

CONTRACT NO. 62F23

OMISSIONS  
 STA. 402+48 TO STA. 404+98

LOCATION 1)  
 PROJECT BEGINS  
 STA 169+95

R 11 E



LOCATION 2)  
 DUNDEE RD.(PATCHING)

LOCATION 1)  
 PROJECT ENDS  
 STA 408+25

PALATINE & WHEELING TOWNSHIPS  
 LOCATION 1) GROSS LENGTH OF IMPROVEMENT= 23,830 FT. = 4.51 MILES  
 LOCATION 2) NET LENGTH OF IMPROVEMENT= 25,580 FT. = 4.47 MILES

STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION  
 SUBMITTED August 16, 2018  
Anthony J. Quigley REGIONAL ENGINEER  
Dec 7, 2018  
Paul P. Chaf ENGINEER OF DESIGN AND ENVIRONMENT  
Dec 7, 2018  
Paul P. Chaf DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

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 OF THE STATE OF ILLINOIS

**INDEX OF SHEETS**

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32	ARTERIAL ROAD INFORMATION SIGN (TC-22)
33	DISTRICT ONE - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING (TS-07)

**STATE STANDARDS**

STANDARD NO	DESCRIPTION
000001-07	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
442201-03	CLASS C AND D PATCHES
604001-04	FRAME AND LIDS, TYPE 1
606001-07	COMBINATION CONCRETE CURB AND GUTTER
606301-04	PCC ISLANDS & MEDIANS
606306-04	CORRUGATED PCC MEDIANS
701101-05	OFF-ROAD OPERATIONS, MULTILANE, 15' TO 2' FROM PAVEMENT EDGE
701421-08	LANE CLOSURE, MULTILANE, DAY OPERATIONS ONLY, FOR SPEEDS > 45 TO 55 MPH
701426-09	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION, FOR SPEEDS ≥ 45
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION, FOR SPEEDS ≤ 40
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NON-TRAVERSABLE MEDIAN
701606-10	LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701901-08	TRAFFIC CONTROL DEVICES

**GENERAL NOTES:**

BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE" AT 800-892-0123 OR 811 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE AND GAS FACILITIES (48 HOUR NOTIFICATION IS REQUIRED).

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES AND THE VILLAGES OF ARLINGTON HEIGHTS, WHEELING & THE CITY OF PROSPECT HEIGHTS.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.

ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1/2 INCHES (40MM) WHERE THE SPEED LIMIT IS 40 MPH (80KM/H) OR LESS AND 1 INCH (25 MM) WHERE THE SPEED LIMIT IS GREATER THAN 40 MPH (80 KM/H), WITH WRITTEN APPROVAL FROM THE ENGINEER, A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES (75 MM) MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 (V:H) OR A NOTCHED LONGITUDINAL WEDGE IS USED.

BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES AND REVISED REFLECTIVE PAVEMENT MARKERS IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.

THE RESIDENT ENGINEER SHALL CONTACT CORY JUCIUS, ARTERIAL TRAFFIC FIELD ENGINEER AT CORY.JUCIUS@ILLINOIS.GOV A MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT CORY.JUCIUS@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.

ALL PAVEMENT PATCHING LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.

THESE PLANS HAVE BEEN PREPARED FROM NOTES RECEIVED FROM THE BUREAU OF CONSTRUCTION.

BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT) ACCORDING TO THE "BUTT JOINT AND HOT-MIX ASPHALT TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.

PAVEMENT MARKING TAPE TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKING ON FINAL SURFACES.

ASPHALTIC EMULSION SLURRY SEAL, MIXTURE C AND FIBER-MODIFIED MODIFIED ASPHALT CRACK SEALING WILL BE PLACED ALONG MEDIANS AND UNDERNEATH GUARDRAILS BETWEEN THE MAINLINE AND FRONTAGE ROADS.

CONTRACTOR SHALL MILL TO THE EDGE OF THE BARRIER WALL.

FILE NAME =	USER NAME = guilleumefp	DESIGNED -	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>INDEX OF SHEETS, STATE STANDARDS AND GENERAL NOTES PALATINE ROAD (U.S. ROUTE 12 (RAND ROAD) TO IL. ROUTE 21)</b>	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
\\IL084E8I0INTEG.illinois.gov\PIWIDOT\Documents\IDOT Offices\District 1\Projects\DI26717\DRM\Date\Design\DI26717-sht-plan.dgn		CHECKED -	REVISED -			305	2017-020RS	COOK	33	2	
Default		DATE -	REVISED -			CONTRACT NO. 62F23		ILLINOIS FED. AID PROJECT			
						SCALE:	SHEET OF SHEETS	STA. TO STA.	REV. 11/21/18		

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		PALATINE ROAD ROADWAY 0005 80% FED 20% STATE	DUNDEE ROAD ROADWAY 0005 80% FED 20% STATE			
Z0033700	LONGITUDINAL JOINT SEALANT	FOOT	49760	49760				
Z0064800	SELECTIVE CLEARING	UNIT	8.6	8.6				
21101615	TOPSOIL FURNISH AND PLACE, 4"	SO YD	1867	1867				
25000400	NITROGEN FERTILIZER NUTRIENT	POUND	27	27				
25000600	POTASSIUM FERTILIZER NUTRIENT	POUND	27	27				
25200110	SODDING, SALT TOLERANT	SO YD	1867	1867				
25200200	SUPPLEMENTAL WATERING	UNIT	20	20				
40400070	ASPHALTIC EMULSION SLURRY SEAL, MIXTURE C	SO YD	8505	8505				
40500001	FIBER-MODIFIED ASPHALT CRACK SEALING	FOOT	8430	8430				
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	86990	86990				
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	200	200				
40600827	POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4, 75, N50	TON	5325	5325				
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	847	847				

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		PALATINE ROAD ROADWAY 0005 80% FED 20% STATE	DUNDEE ROAD ROADWAY 0005 80% FED 20% STATE			
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	1017	1017				
42001300	PROTECTIVE COAT	SO YD	5850	5850				
44000159	HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"	SO YD	129000	129000				
44002216	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 4"	SO YD	4542	4542				
44003100	MEDIAN REMOVAL	SO FT	495	495				
44003510	MEDIAN REMOVAL PARTIAL DEPTH	SO FT	29830	29830				
44201789	CLASS D PATCHES, TYPE II, 12 INCH	SO YD	3781	3781				
44201794	CLASS D PATCHES, TYPE III, 12 INCH	SO YD	96	96				
44201796	CLASS D PATCHES, TYPE IV, 12 INCH	SO YD	54	54				
44201815	CLASS D PATCHES, TYPE II, 14 INCH	SO YD	2810	2810				
44201819	CLASS D PATCHES, TYPE III, 14 INCH	SO YD	550	550				
44201821	CLASS D PATCHES, TYPE IV, 14 INCH	SO YD	395	395				
60252800	CATCH BASINS TO BE RECONSTRUCTED	EACH	30	30				
60257900	MANHOLES TO BE RECONSTRUCTED	EACH	4	4				

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		PALATINE ROAD ROADWAY 0005 80% FED 20% STATE	DUNDEE ROAD ROADWAY 0005 80% FED 20% STATE				
60262700	INLETS TO BE RECONSTRUCTED	EACH	15	15					
60300305	FRAMES AND LIDS TO BE ADJUSTED	EACH	130	130					
60406000	FRAMES AND LIDS, TYPE 1, OPEN LID	EACH	64	64					
60406100	FRAMES AND LIDS, TYPE 1, CLOSED LID	EACH	41	41					
60618210	HOT-MIX ASPHALT MEDIAN SURFACE, 4 INCH	SO FT	21420	21420					
60618300	CONCRETE MEDIAN SURFACE, 4 INCH	SO FT	8410	8410					
60624600	CORRUGATED MEDIAN	SO FT	435	435					
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	12	12					
67100100	MOBILIZATION	L SUM	1	0.70	0.30				
70100310	TRAFFIC CONTROL AND PROTECTION, STANDARD 701421	L SUM	1	0.70	0.30				
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	0.70	0.30				
70102630	TRAFFIC CONTROL AND PROTECTION, STANDARD 701601	L SUM	1	0.70	0.30				
70102635	TRAFFIC CONTROL AND PROTECTION, STANDARD 701701	L SUM	1	0.70	0.30				
70300100	SHORT TERM PAVEMENT MARKING	FOOT	34000	34000					

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		PALATINE ROAD ROADWAY 0005 80% FED 20% STATE	DUNDEE ROAD ROADWAY 0005 80% FED 20% STATE				
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SO FT	11350	11350					
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	301	301					
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	104600	104600					
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	1500	1500					
70300250	TEMPORARY PAVEMENT MARKING - LINE 8"	FOOT	1500	1500					
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	500	500					
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	300	300					
70300520	PAVEMENT MARKING TAPE, TYPE III 4"	FOOT	8500	8500					
* 78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	301	301					
* 78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	104600	104600					
* 78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1500	1500					
* 78000500	THERMOPLASTIC PAVEMENT MARKING - LINE 8"	FOOT	1500	1500					
* 78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	500	500					

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		PALATINE ROAD ROADWAY 0005 80% FED 20% STATE	DUNDEE ROAD ROADWAY 0005 80% FED 20% STATE			
* 78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	300	300				
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	700	700				
78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	500	500				
* 88600600	DETECTOR LOOP REPLACEMENT	FOOT	600	600				
K0029614	WEED CONTROL, AQUATIC	GALLON	0.5	0.5				
K0029624	WEED CONTROL, TEASEL	GALLON	1	1				
X2010200	TREE LIMB REMOVAL (OVER 10 INCHES DIAMETER)	EACH	1	1				
X4060004	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, STONE MATRIX ASPHALT, 9.5, N80	TON	12645	12645				
△ X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	500	500				
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	90	90				
X7030005	TEMPORARY PAVEMENT MARKING REMOVAL	SO FT	40610	40610				
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	21900	21900				
△ Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	293	293				

SUMMARY OF QUANTITIES			URBAN TOTAL QUANTITIES	CONSTRUCTION TYPE CODE				
CODE NO	ITEM	UNIT		PALATINE ROAD ROADWAY 0005 80% FED 20% STATE	DUNDEE ROAD ROADWAY 0005 80% FED 20% STATE			
Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	257	257				
* Z0076600	TRAINEES	HOURS	500	500				
* Z0076604	TRAINEES TRAINING PROGRAM GRADUATE	HOURS	500	500				

△ NON PARTICIPATING  
\* SPECIALTY ITEM  
\* =0042

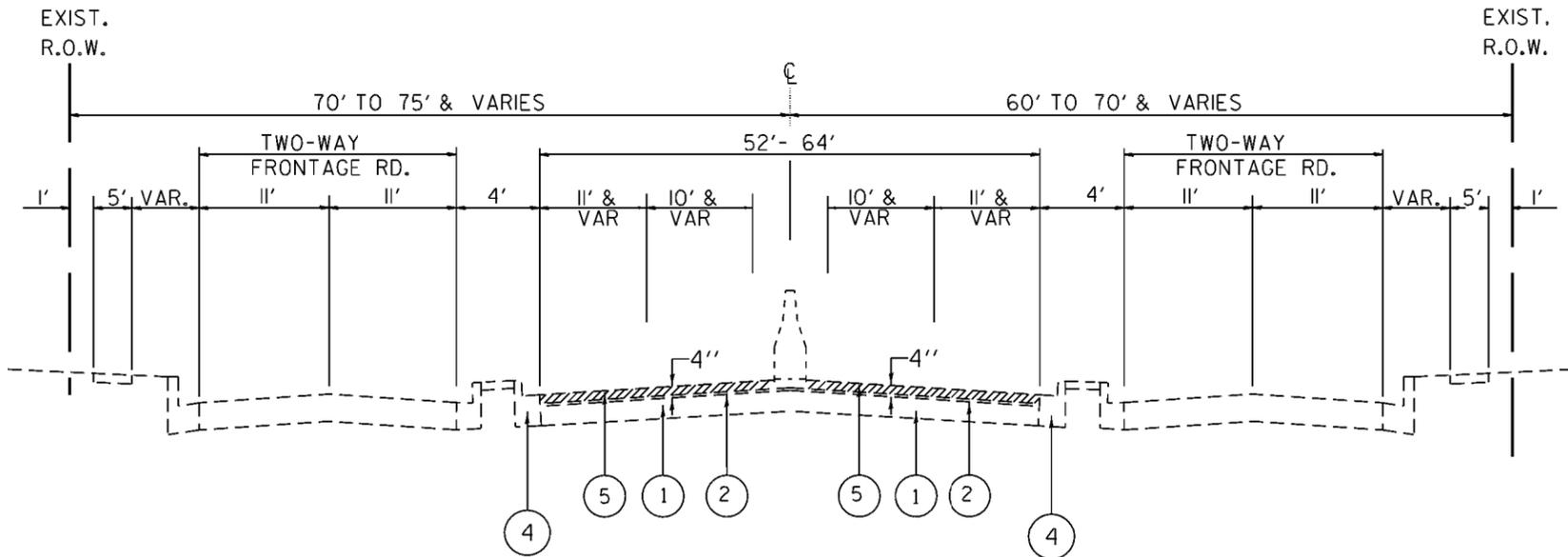
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STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES			
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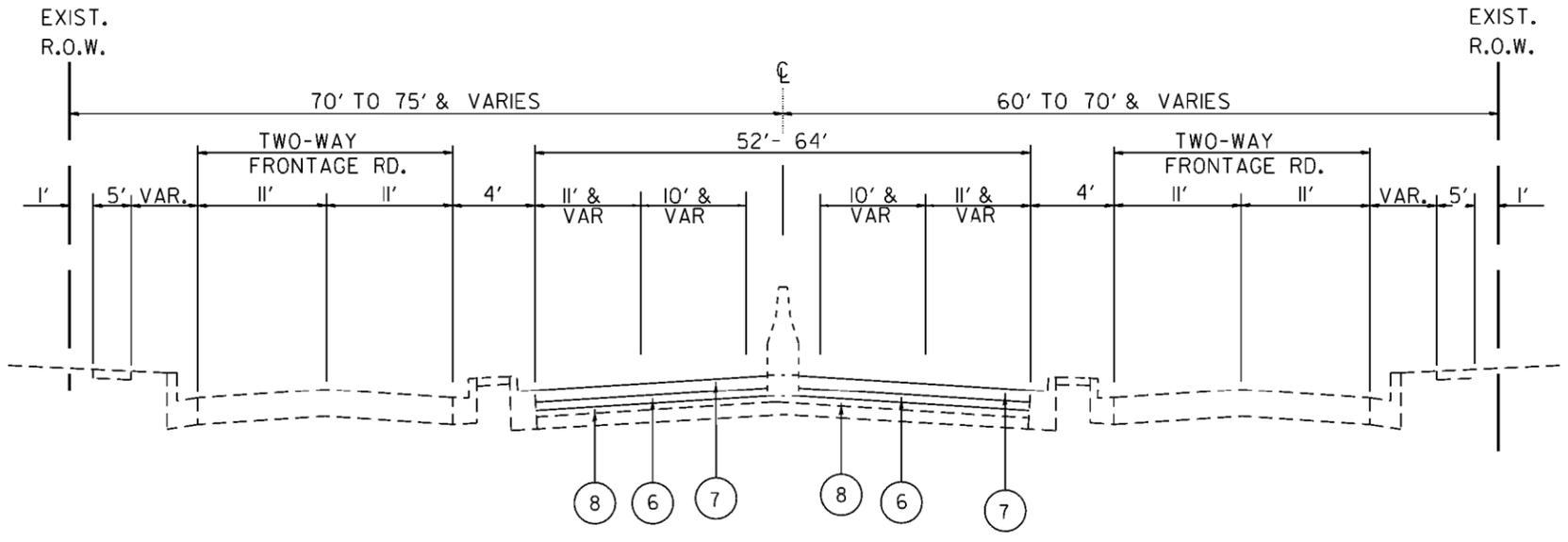
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2017-020RS	COOK	33	5
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT			CONTRACT NO. 62F23	





EXISTING TYPICAL SECTION  
PALATINE ROAD  
STA. 186+85 TO STA. 408+25  
(NOT TO SCALE)

- LEGEND**
- ① EXISTING P.C.C. PAVEMENT, 10''
  - ② EXISTING HMA PAVEMENT, ±4''
  - ③ EXISTING CONCRETE MEDIAN
  - ④ EXISTING CURB AND GUTTER
  - ⑤ PROPOSED HMA SURFACE REMOVAL, 2½''
  - ⑥ PROPOSED POLYMERIZED LEVELING BINDER (MM), IL 4.75, ¾''
  - ⑦ PROPOSED POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, N80, 1¾''
  - ⑧ HMA OVERLAY AFTER MILLING, ±1½''



PROPOSED TYPICAL SECTION  
PALATINE ROAD  
STA. 186+85 TO STA. 408+25  
(NOT TO SCALE)

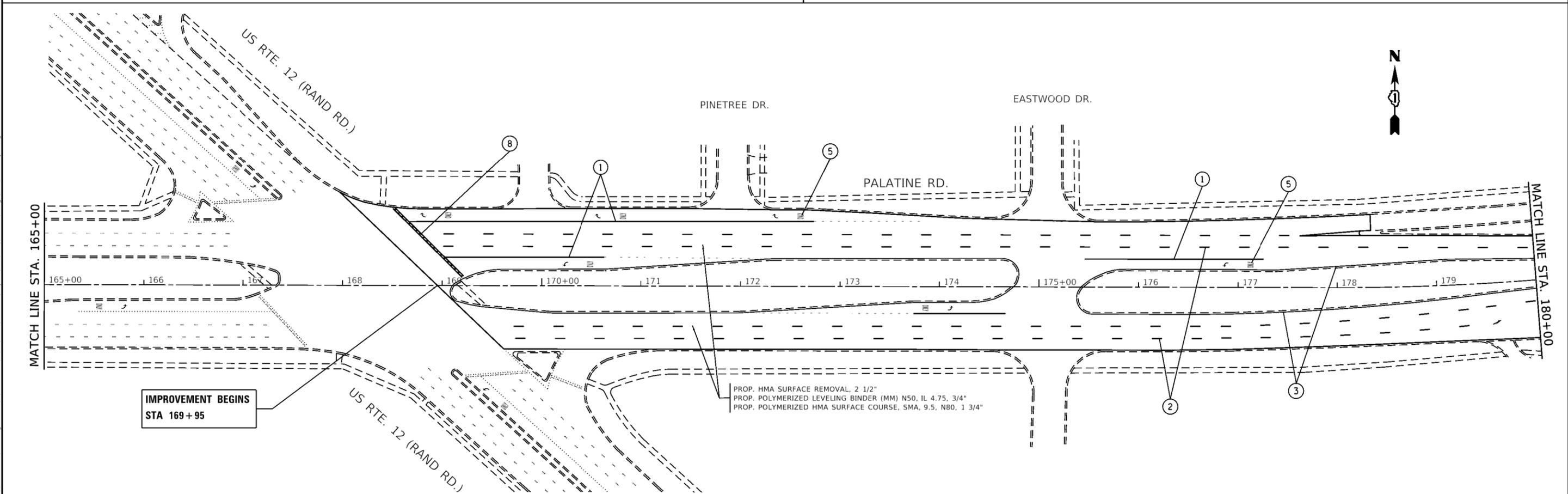
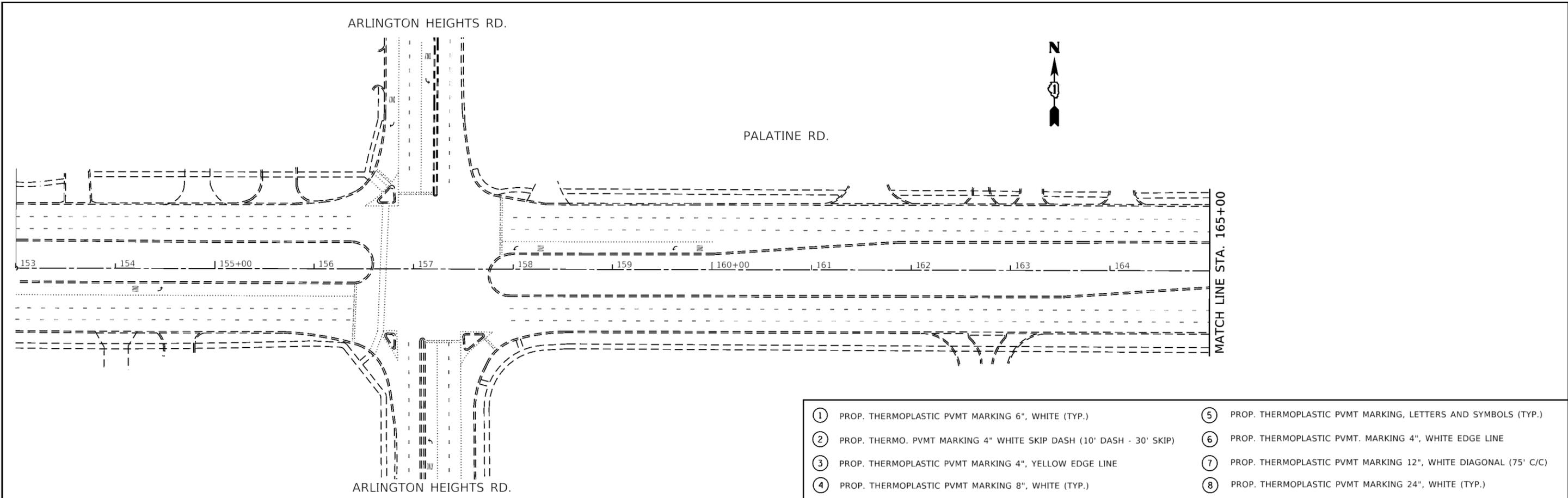
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>TYPICAL SECTIONS</b>			
<b>PALATINE ROAD (US 12 (RAND ROAD) TO IL 21)</b>			
SCALE: NONE	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2017-020RS	COOK	33	7
			CONTRACT NO. 62F23	
		ILLINOIS FED. AID PROJECT		



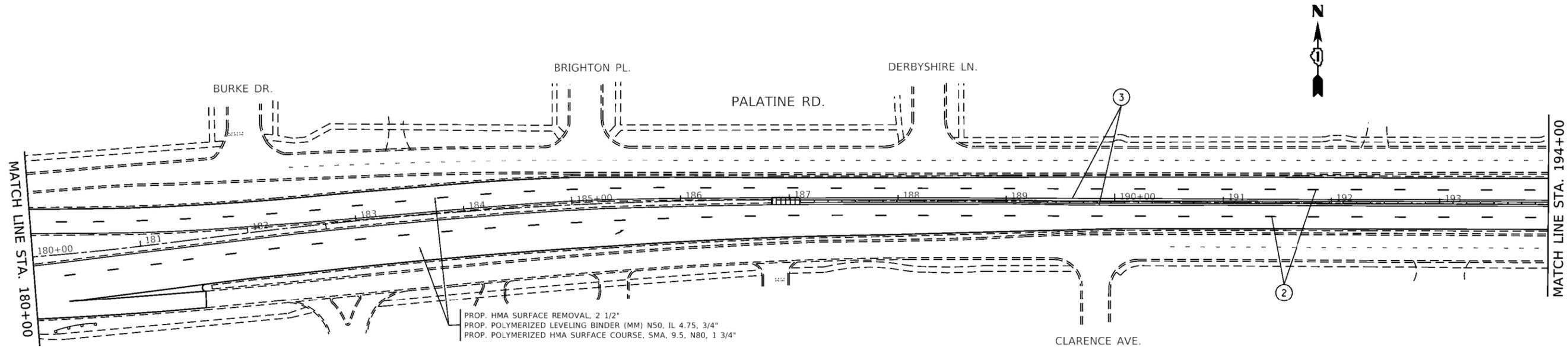
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**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

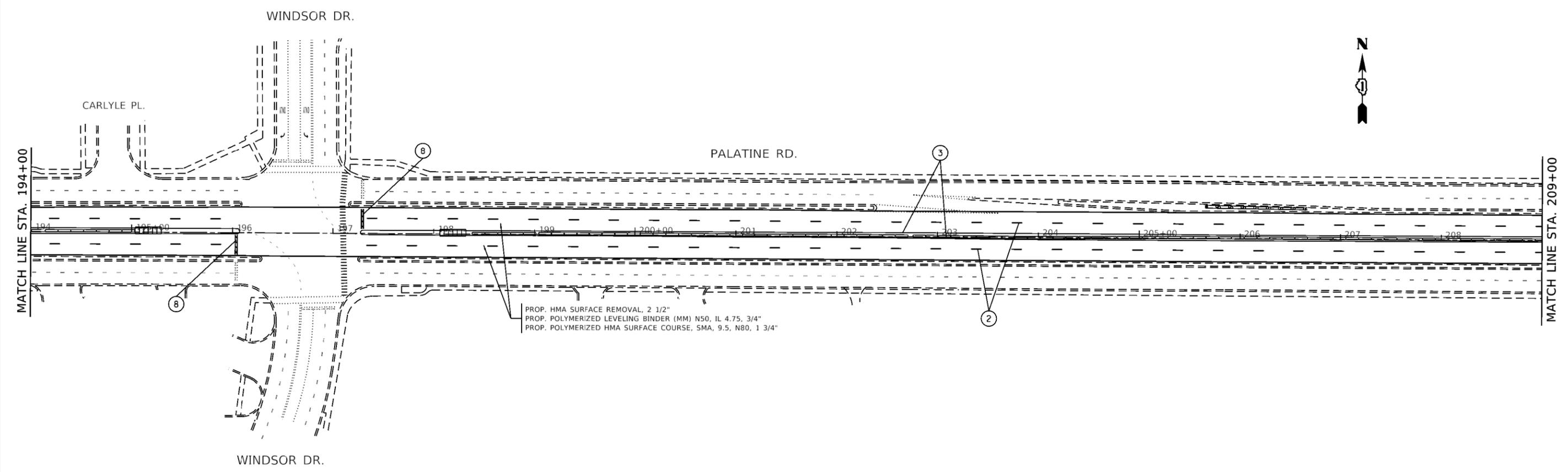
<b>EXISTING AND PROPOSED ROADWAY PLAN</b> <b>PALATINE ROAD (U.S. ROUTE 12 (RAND ROAD) TO IL. ROUTE 21)</b>			
SCALE: 1"=50'	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE. 305	SECTION 2017-020RS	COUNTY COOK	TOTAL SHEETS 33	SHEET NO. 8
ILLINOIS FED. AID PROJECT				



PROP. HMA SURFACE REMOVAL, 2 1/2"  
 PROP. POLYMERIZED LEVELING BINDER (MM) N50, IL 4.75, 3/4"  
 PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, N80, 1 3/4"

- |   |  |
|---|--|
| ① PROP. THERMOPLASTIC PVMT MARKING 6", WHITE (TYP.)                   | ⑤ PROP. THERMOPLASTIC PVMT MARKING, LETTERS AND SYMBOLS (TYP.)   |
| ② PROP. THERMO. PVMT MARKING 4" WHITE SKIP DASH (10' DASH - 30' SKIP) | ⑥ PROP. THERMOPLASTIC PVMT. MARKING 4", WHITE EDGE LINE          |
| ③ PROP. THERMOPLASTIC PVMT MARKING 4", YELLOW EDGE LINE               | ⑦ PROP. THERMOPLASTIC PVMT MARKING 12", WHITE DIAGONAL (75' C/C) |
| ④ PROP. THERMOPLASTIC PVMT MARKING 8", WHITE (TYP.)                   | ⑧ PROP. THERMOPLASTIC PVMT MARKING 24", WHITE (TYP.)             |



PROP. HMA SURFACE REMOVAL, 2 1/2"  
 PROP. POLYMERIZED LEVELING BINDER (MM) N50, IL 4.75, 3/4"  
 PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, N80, 1 3/4"

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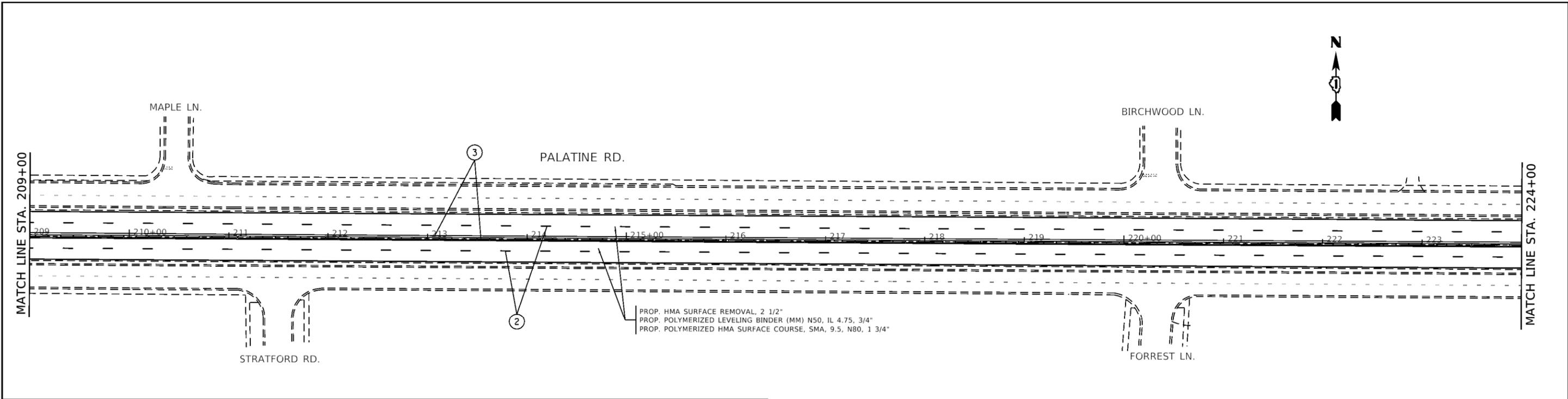
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**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

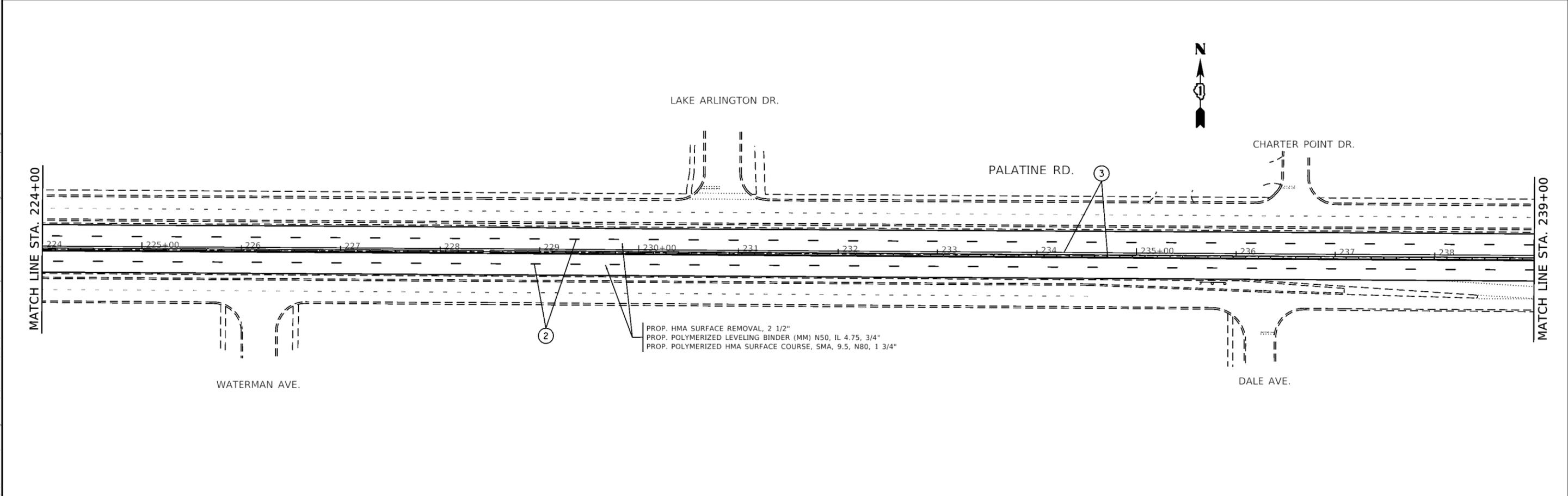
**EXISTING AND PROPOSED ROADWAY PLAN**  
**PALATINE ROAD (U.S. ROUTE 12 (RAND ROAD) TO IL. ROUTE 21)**

SCALE: 1"=50'    SHEET    OF    SHEETS    STA.    TO    STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2017-020R5	COOK	33	9
CONTRACT NO. 62F23			ILLINOIS FED. AID PROJECT	



- |   |  |
|---|--|
| ① PROP. THERMOPLASTIC PVMT MARKING 6", WHITE (TYP.)                   | ⑤ PROP. THERMOPLASTIC PVMT MARKING, LETTERS AND SYMBOLS (TYP.)   |
| ② PROP. THERMO. PVMT MARKING 4" WHITE SKIP DASH (10' DASH - 30' SKIP) | ⑥ PROP. THERMOPLASTIC PVMT. MARKING 4", WHITE EDGE LINE          |
| ③ PROP. THERMOPLASTIC PVMT MARKING 4", YELLOW EDGE LINE               | ⑦ PROP. THERMOPLASTIC PVMT MARKING 12", WHITE DIAGONAL (75' C/C) |
| ④ PROP. THERMOPLASTIC PVMT MARKING 8", WHITE (TYP.)                   | ⑧ PROP. THERMOPLASTIC PVMT MARKING 24", WHITE (TYP.)             |



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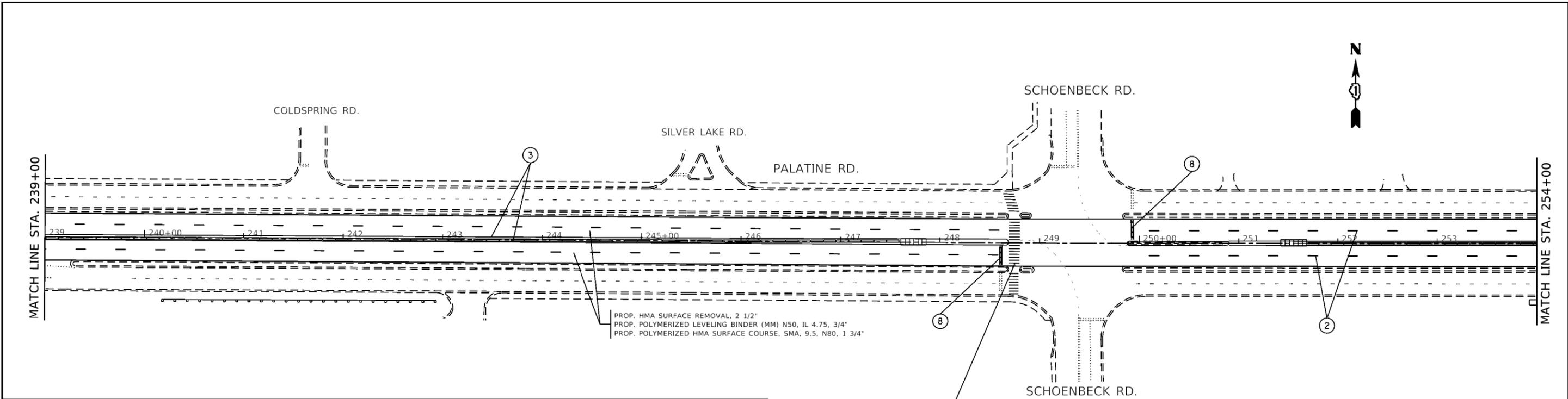
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	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

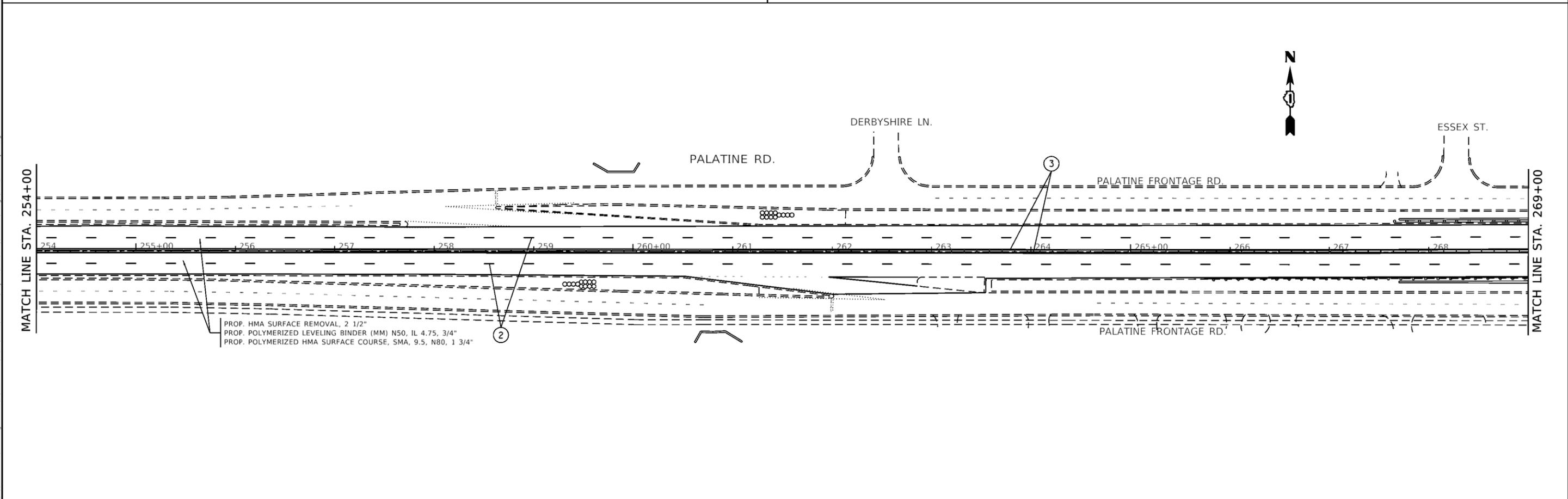
**EXISTING AND PROPOSED ROADWAY PLAN**  
**PALATINE ROAD (U.S. ROUTE 12 (RAND ROAD) TO IL. ROUTE 21)**

SCALE: 1"=50'    SHEET    OF    SHEETS    STA.    TO    STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2017-020R5	COOK	33	10
CONTRACT NO. 62F23				
ILLINOIS FED. AID PROJECT				



- |  |   |
|--|---|
| ① PROP. THERMOPLASTIC PVT MARKING 6", WHITE (TYP.)                   | ⑤ PROP. THERMOPLASTIC PVT MARKING, LETTERS AND SYMBOLS (TYP.)   |
| ② PROP. THERMO. PVT MARKING 4" WHITE SKIP DASH (10' DASH - 30' SKIP) | ⑥ PROP. THERMOPLASTIC PVT MARKING 4", WHITE EDGE LINE           |
| ③ PROP. THERMOPLASTIC PVT MARKING 4", YELLOW EDGE LINE               | ⑦ PROP. THERMOPLASTIC PVT MARKING 12", WHITE DIAGONAL (75' C/C) |
| ④ PROP. THERMOPLASTIC PVT MARKING 8", WHITE (TYP.)                   | ⑧ PROP. THERMOPLASTIC PVT MARKING 24", WHITE (TYP.)             |



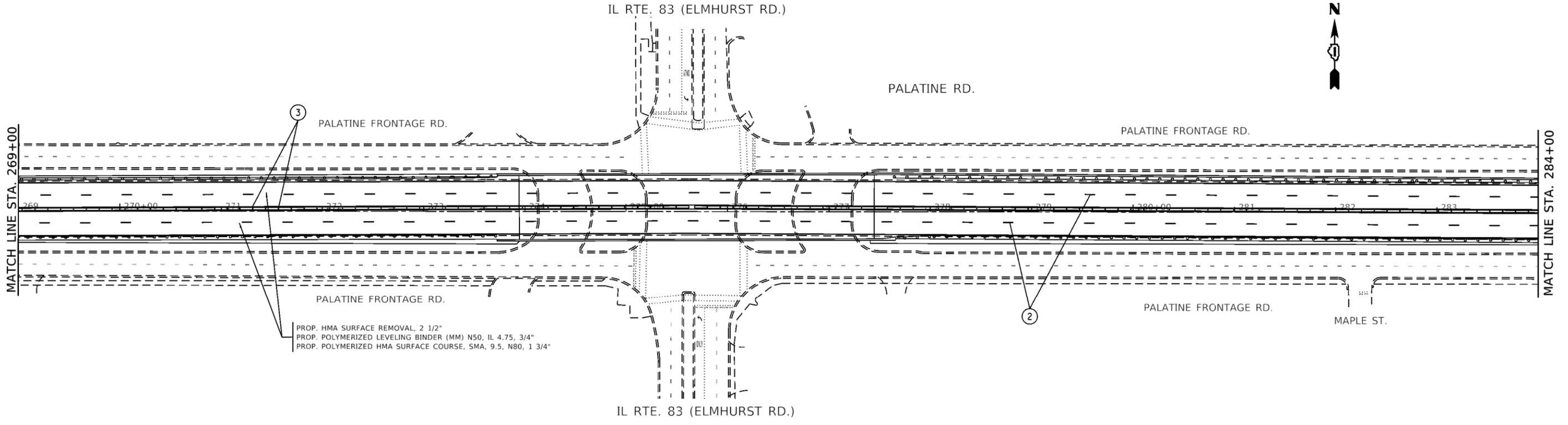
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USER NAME = quillaumefp	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/19/2018	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

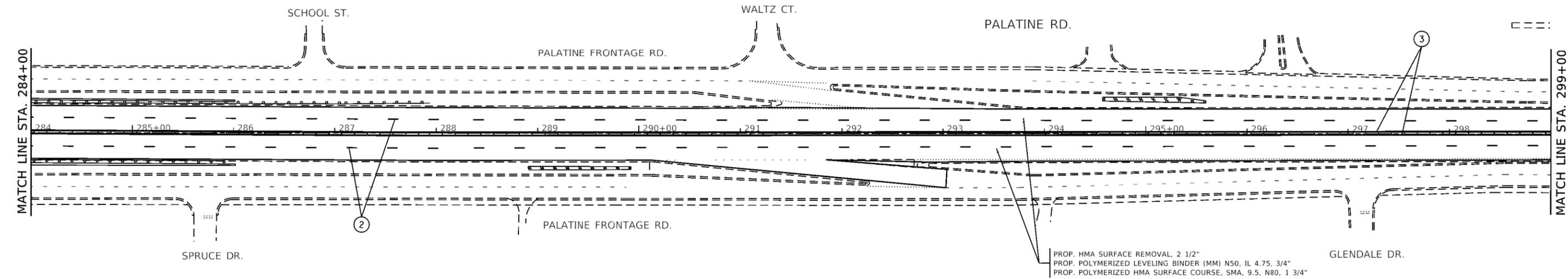
<b>EXISTING AND PROPOSED ROADWAY PLAN</b> <b>PALATINE ROAD (U.S. ROUTE 12 (RAND ROAD) TO IL. ROUTE 21)</b>			
SCALE: 1"=50'	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE. 305	SECTION 2017-020RS	COUNTY COOK	TOTAL SHEETS 33	SHEET NO. 11
ILLINOIS			FED. AID PROJECT	



PROP. HMA SURFACE REMOVAL, 2 1/2"  
 PROP. POLYMERIZED LEVELING BINDER (MM) N50, IL 4.75, 3/4"  
 PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, N80, 1 3/4"

- |   |  |
|---|--|
| ① PROP. THERMOPLASTIC PVMT MARKING 6", WHITE (TYP.)                   | ⑤ PROP. THERMOPLASTIC PVMT MARKING, LETTERS AND SYMBOLS (TYP.)   |
| ② PROP. THERMO. PVMT MARKING 4" WHITE SKIP DASH (10' DASH - 30' SKIP) | ⑥ PROP. THERMOPLASTIC PVMT. MARKING 4", WHITE EDGE LINE          |
| ③ PROP. THERMOPLASTIC PVMT MARKING 4", YELLOW EDGE LINE               | ⑦ PROP. THERMOPLASTIC PVMT MARKING 12", WHITE DIAGONAL (75° C/C) |
| ④ PROP. THERMOPLASTIC PVMT MARKING 8", WHITE (TYP.)                   | ⑧ PROP. THERMOPLASTIC PVMT MARKING 24", WHITE (TYP.)             |



PROP. HMA SURFACE REMOVAL, 2 1/2"  
 PROP. POLYMERIZED LEVELING BINDER (MM) N50, IL 4.75, 3/4"  
 PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, N80, 1 3/4"

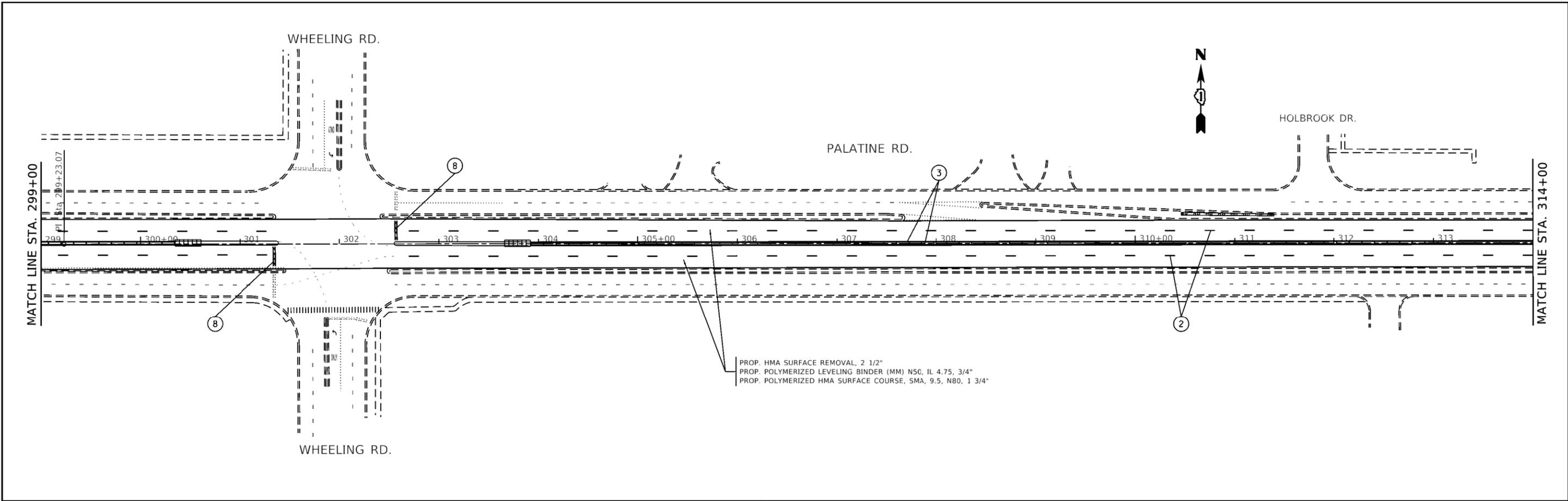
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USER NAME = quillaumefp	DESIGNED -	REVISED -
PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 10/19/2018	CHECKED -	REVISED -
	DATE -	REVISED -

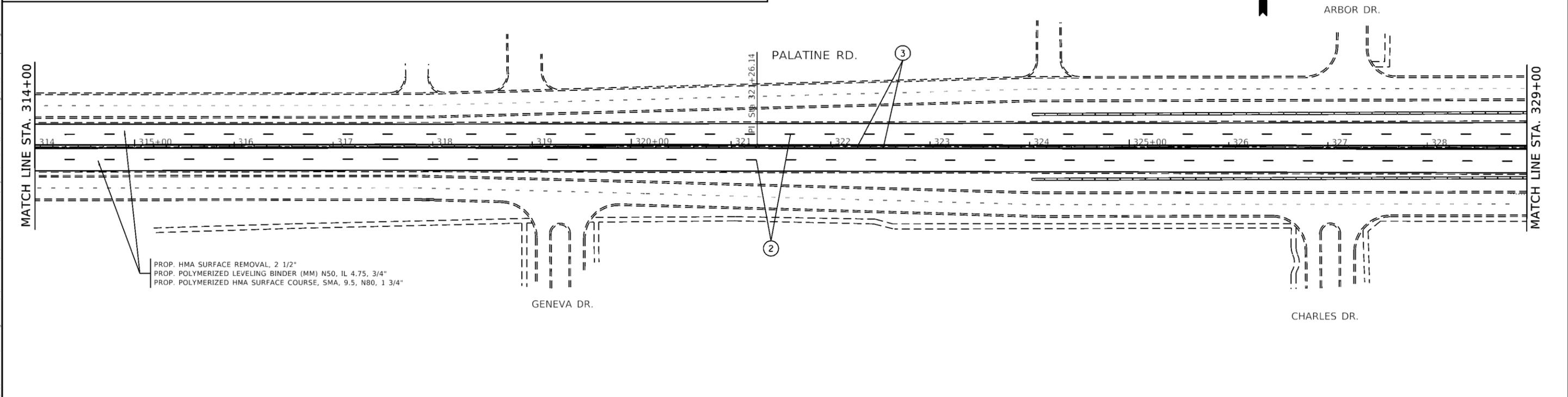
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>EXISTING AND PROPOSED ROADWAY PLAN</b> <b>PALATINE ROAD (U.S. ROUTE 12 (RAND ROAD) TO IL. ROUTE 21)</b>			
SCALE: 1"=50'	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE. 305	SECTION 2017-020RS	COUNTY COOK	TOTAL SHEETS 33	SHEET NO. 12
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62F23	



- |   |  |
|---|--|
| ① PROP. THERMOPLASTIC PVMT MARKING 6", WHITE (TYP.)                   | ⑤ PROP. THERMOPLASTIC PVMT MARKING, LETTERS AND SYMBOLS (TYP.)   |
| ② PROP. THERMO. PVMT MARKING 4" WHITE SKIP DASH (10' DASH - 30' SKIP) | ⑥ PROP. THERMOPLASTIC PVMT. MARKING 4", WHITE EDGE LINE          |
| ③ PROP. THERMOPLASTIC PVMT MARKING 4", YELLOW EDGE LINE               | ⑦ PROP. THERMOPLASTIC PVMT MARKING 12", WHITE DIAGONAL (75' C/C) |
| ④ PROP. THERMOPLASTIC PVMT MARKING 8", WHITE (TYP.)                   | ⑧ PROP. THERMOPLASTIC PVMT MARKING 24", WHITE (TYP.)             |



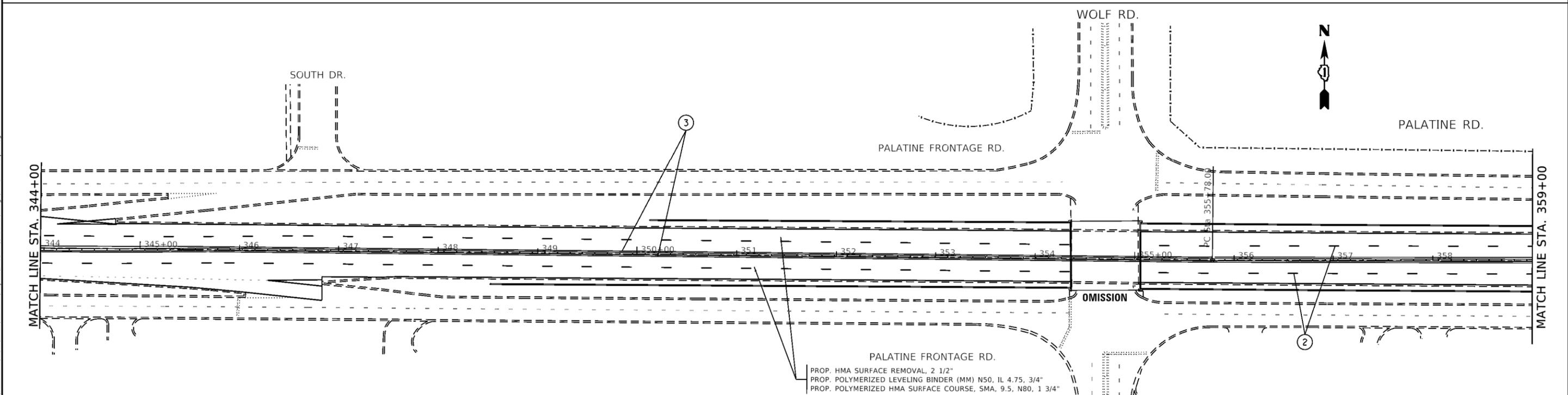
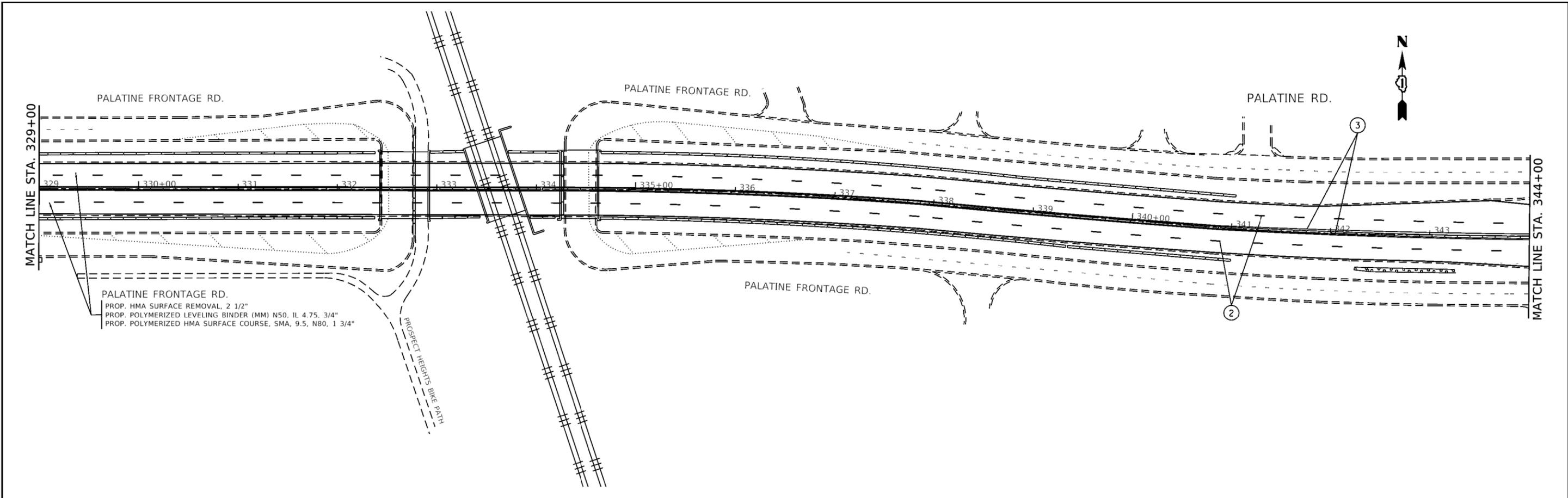
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USER NAME = quillaumefp	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/19/2018	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

<b>EXISTING AND PROPOSED ROADWAY PLAN</b> <b>PALATINE ROAD (U.S. ROUTE 12 (RAND ROAD) TO IL. ROUTE 21)</b>			
SCALE: 1"=50'	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2017-020RS	COOK	33	13
CONTRACT NO. 62F23			ILLINOIS FED. AID PROJECT	



- |   |  |
|---|--|
| ① PROP. THERMOPLASTIC PVMT MARKING 6", WHITE (TYP.)                   | ⑤ PROP. THERMOPLASTIC PVMT MARKING, LETTERS AND SYMBOLS (TYP.)   |
| ② PROP. THERMO. PVMT MARKING 4" WHITE SKIP DASH (10' DASH - 30' SKIP) | ⑥ PROP. THERMOPLASTIC PVMT. MARKING 4", WHITE EDGE LINE          |
| ③ PROP. THERMOPLASTIC PVMT MARKING 4", YELLOW EDGE LINE               | ⑦ PROP. THERMOPLASTIC PVMT MARKING 12", WHITE DIAGONAL (75' C/C) |
| ④ PROP. THERMOPLASTIC PVMT MARKING 8", WHITE (TYP.)                   | ⑧ PROP. THERMOPLASTIC PVMT MARKING 24", WHITE (TYP.)             |

USER NAME = guillaumefp	DESIGNED -	REVISED -
PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 10/19/2018	CHECKED -	REVISED -
	DATE -	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

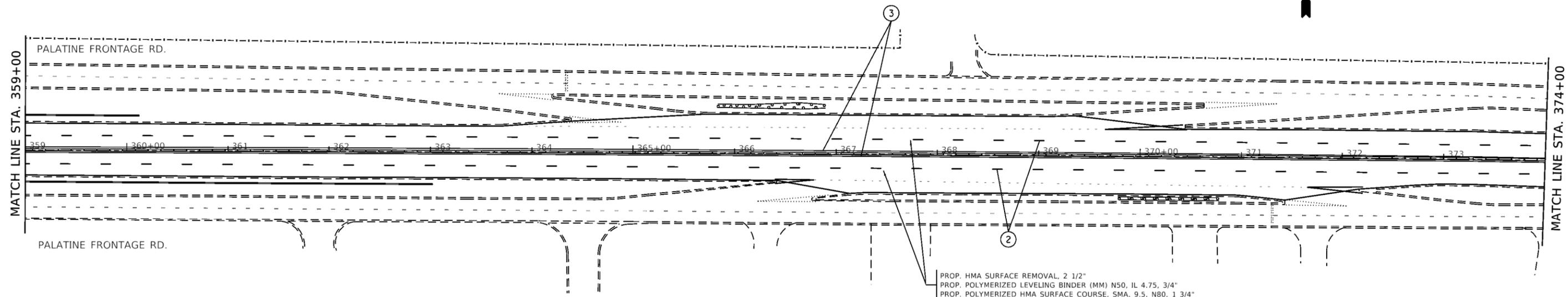
**EXISTING AND PROPOSED ROADWAY PLAN  
PALATINE ROAD (U.S. ROUTE 12 (RAND ROAD) TO IL. ROUTE 21)**

SCALE: 1"=50'    SHEET    OF    SHEETS    STA.    TO    STA.

F.A.P. RTE. 305	SECTION 2017-020RS	COUNTY COOK	TOTAL SHEETS 33	SHEET NO. 14
ILLINOIS FED. AID PROJECT			CONTRACT NO. 62F23	

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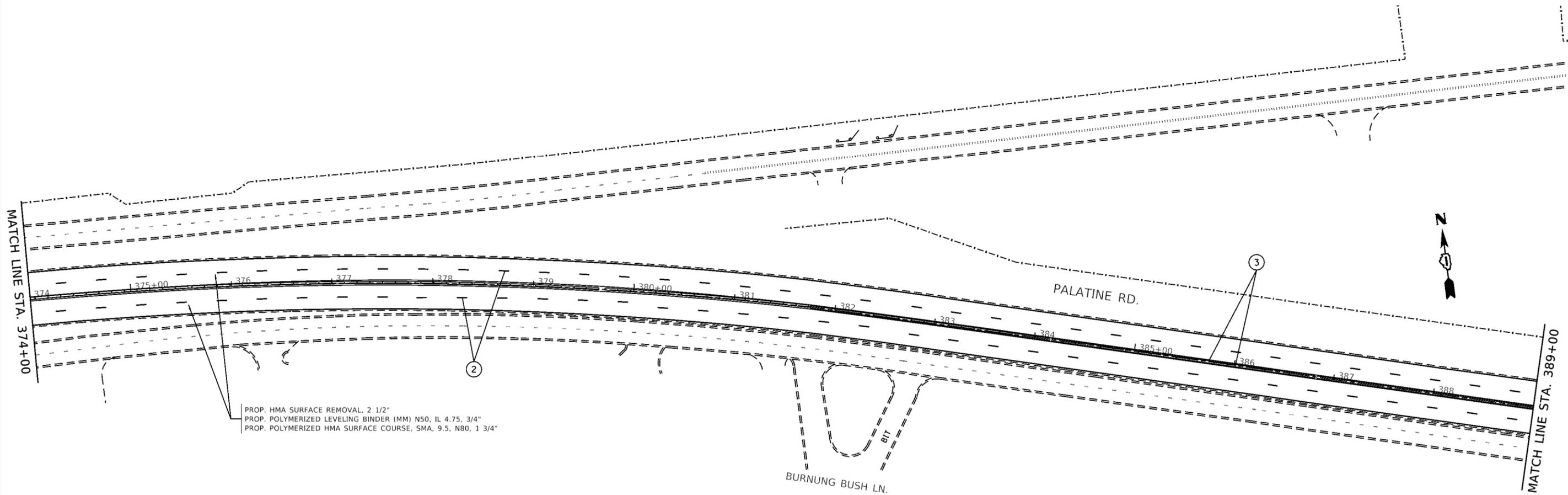
PALATINE RD.



- |   |  |
|---|--|
| ① PROP. THERMOPLASTIC PVMT MARKING 6", WHITE (TYP.)                   | ⑤ PROP. THERMOPLASTIC PVMT MARKING, LETTERS AND SYMBOLS (TYP.)   |
| ② PROP. THERMO. PVMT MARKING 4" WHITE SKIP DASH (10' DASH - 30' SKIP) | ⑥ PROP. THERMOPLASTIC PVMT. MARKING 4", WHITE EDGE LINE          |
| ③ PROP. THERMOPLASTIC PVMT MARKING 4", YELLOW EDGE LINE               | ⑦ PROP. THERMOPLASTIC PVMT MARKING 12", WHITE DIAGONAL (75' C/C) |
| ④ PROP. THERMOPLASTIC PVMT MARKING 8", WHITE (TYP.)                   | ⑧ PROP. THERMOPLASTIC PVMT MARKING 24", WHITE (TYP.)             |

PROP. HMA SURFACE REMOVAL, 2 1/2"  
 PROP. POLYMERIZED LEVELING BINDER (MM) N50, IL 4.75, 3/4"  
 PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, N80, 1 3/4"

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PROP. HMA SURFACE REMOVAL, 2 1/2"  
 PROP. POLYMERIZED LEVELING BINDER (MM) N50, IL 4.75, 3/4"  
 PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, N80, 1 3/4"

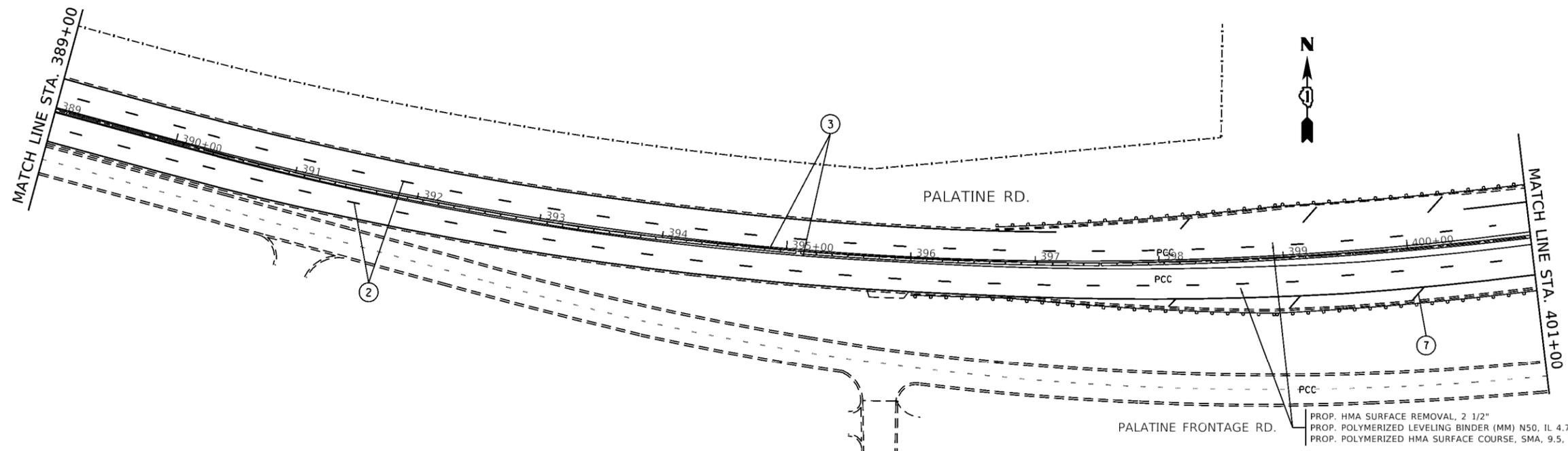
**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED ROADWAY PLAN  
 PALATINE ROAD (U.S. ROUTE 12 (RAND ROAD) TO IL. ROUTE 21)**

USER NAME = quillaumefp	DESIGNED -	REVISED -
PLOT SCALE = 100.0000' / in.	DRAWN -	REVISED -
PLOT DATE = 10/19/2018	CHECKED -	REVISED -
	DATE -	REVISED -

SCALE: 1"=50'    SHEET    OF    SHEETS    STA.    TO    STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2017-020R5	COOK	33	15
CONTRACT NO. 62F23				
ILLINOIS FED. AID PROJECT				



- |   |  |
|---|--|
| ① PROP. THERMOPLASTIC PVMT MARKING 6", WHITE (TYP.)                   | ⑤ PROP. THERMOPLASTIC PVMT MARKING, LETTERS AND SYMBOLS (TYP.)   |
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| ③ PROP. THERMOPLASTIC PVMT MARKING 4", YELLOW EDGE LINE               | ⑦ PROP. THERMOPLASTIC PVMT MARKING 12", WHITE DIAGONAL (75' C/C) |
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PROP. HMA SURFACE REMOVAL, 2 1/2"  
 PROP. POLYMERIZED LEVELING BINDER (MM) N50, IL 4.75, 3/4"  
 PROP. POLYMERIZED HMA SURFACE COURSE, SMA, 9.5, N80, 1 3/4"

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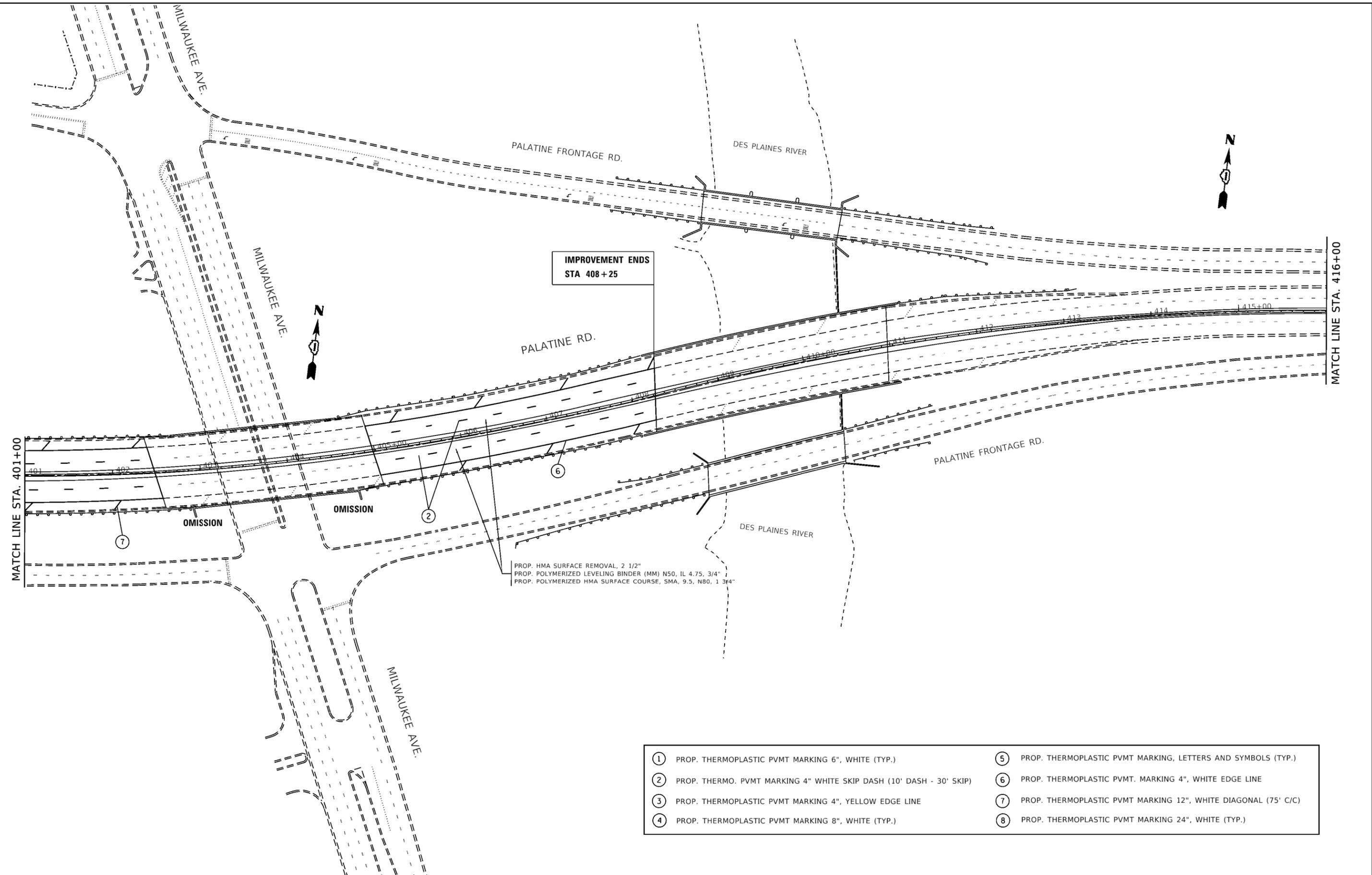
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PLOT DATE = 10/19/2018	DATE -	REVISED -

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

EXISTING AND PROPOSED ROADWAY PLAN			
PALATINE ROAD (U.S. ROUTE 12 (RAND ROAD) TO IL. ROUTE 21)			
SCALE: 1"=50'	SHEET	OF SHEETS	STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2017-020RS	COOK	33	16
CONTRACT NO. 62F23				
ILLINOIS		FED. AID PROJECT		

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- |   |   |   |  |
|---|---|---|--|
| ① | PROP. THERMOPLASTIC PVMT MARKING 6", WHITE (TYP.)                   | ⑤ | PROP. THERMOPLASTIC PVMT MARKING, LETTERS AND SYMBOLS (TYP.)   |
| ② | PROP. THERMO. PVMT MARKING 4" WHITE SKIP DASH (10' DASH - 30' SKIP) | ⑥ | PROP. THERMOPLASTIC PVMT. MARKING 4", WHITE EDGE LINE          |
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| ④ | PROP. THERMOPLASTIC PVMT MARKING 8", WHITE (TYP.)                   | ⑧ | PROP. THERMOPLASTIC PVMT MARKING 24", WHITE (TYP.)             |

USER NAME = guillaumefp	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/19/2018	DATE -	REVISED -

**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**EXISTING AND PROPOSED ROADWAY PLAN**  
**PALATINE ROAD (U.S. ROUTE 12 (RAND ROAD) TO IL. ROUTE 21)**

SCALE: 1"=50'    SHEET    OF    SHEETS    STA.    TO    STA.

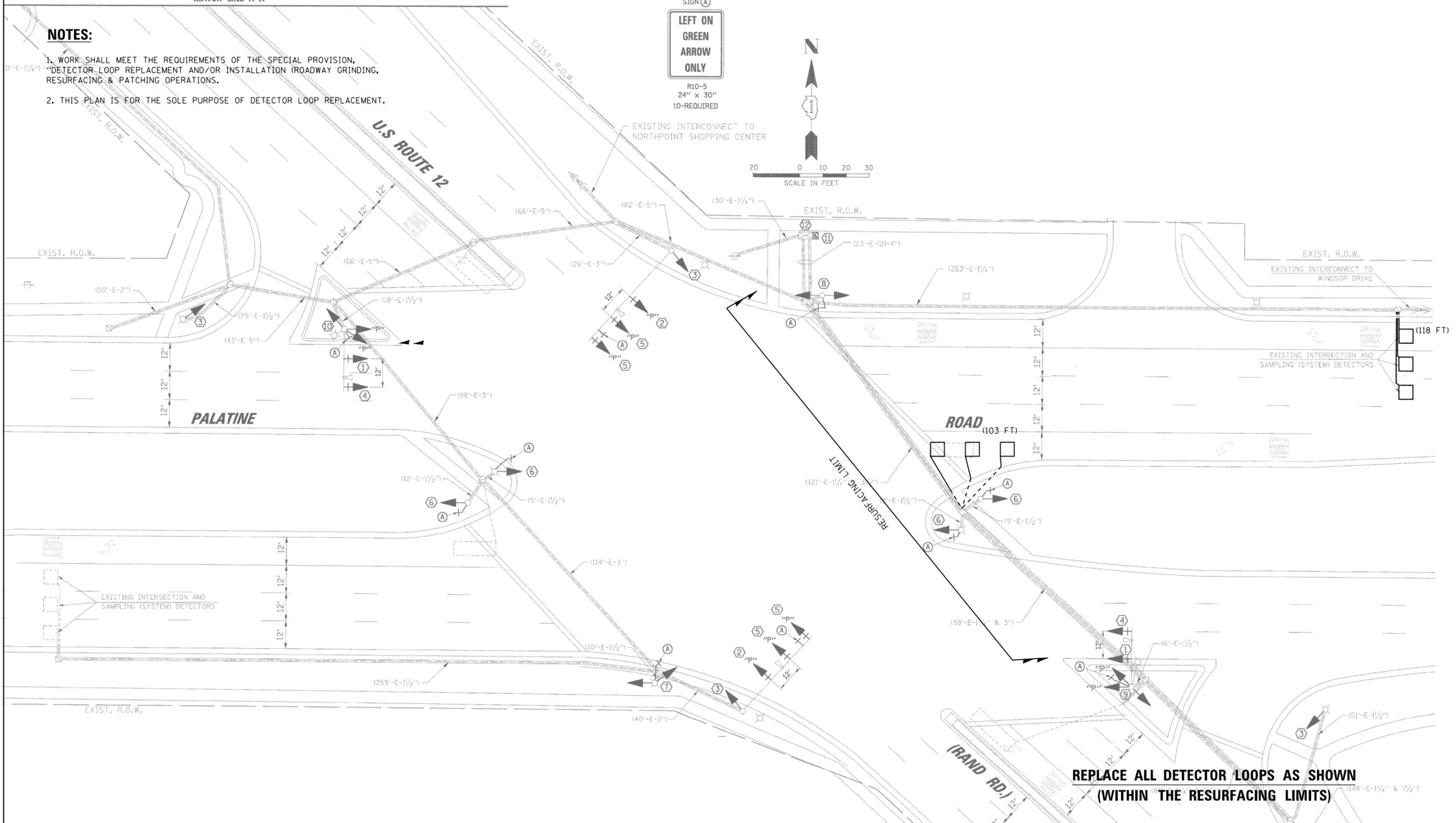
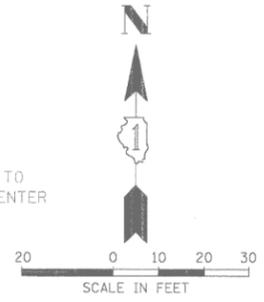
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2017-020RS	COOK	33	17
CONTRACT NO. 62F23				
ILLINOIS FED. AID PROJECT				



MATCH LINE A-A

**NOTES:**

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.



**REPLACE ALL DETECTOR LOOPS AS SHOWN  
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	221	FOOT

FILE NAME = 62F23 - Detector Loop Replacement Sheets.dgn	USER NAME = vargosa	DESIGNED - AV	REVISED -
		DRAWN - AV	REVISED -
	PLOT SCALE = 40.0387' / in.	CHECKED - LP	REVISED -
	PLOT DATE = 9/27/2018	DATE - 04/23/2018	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

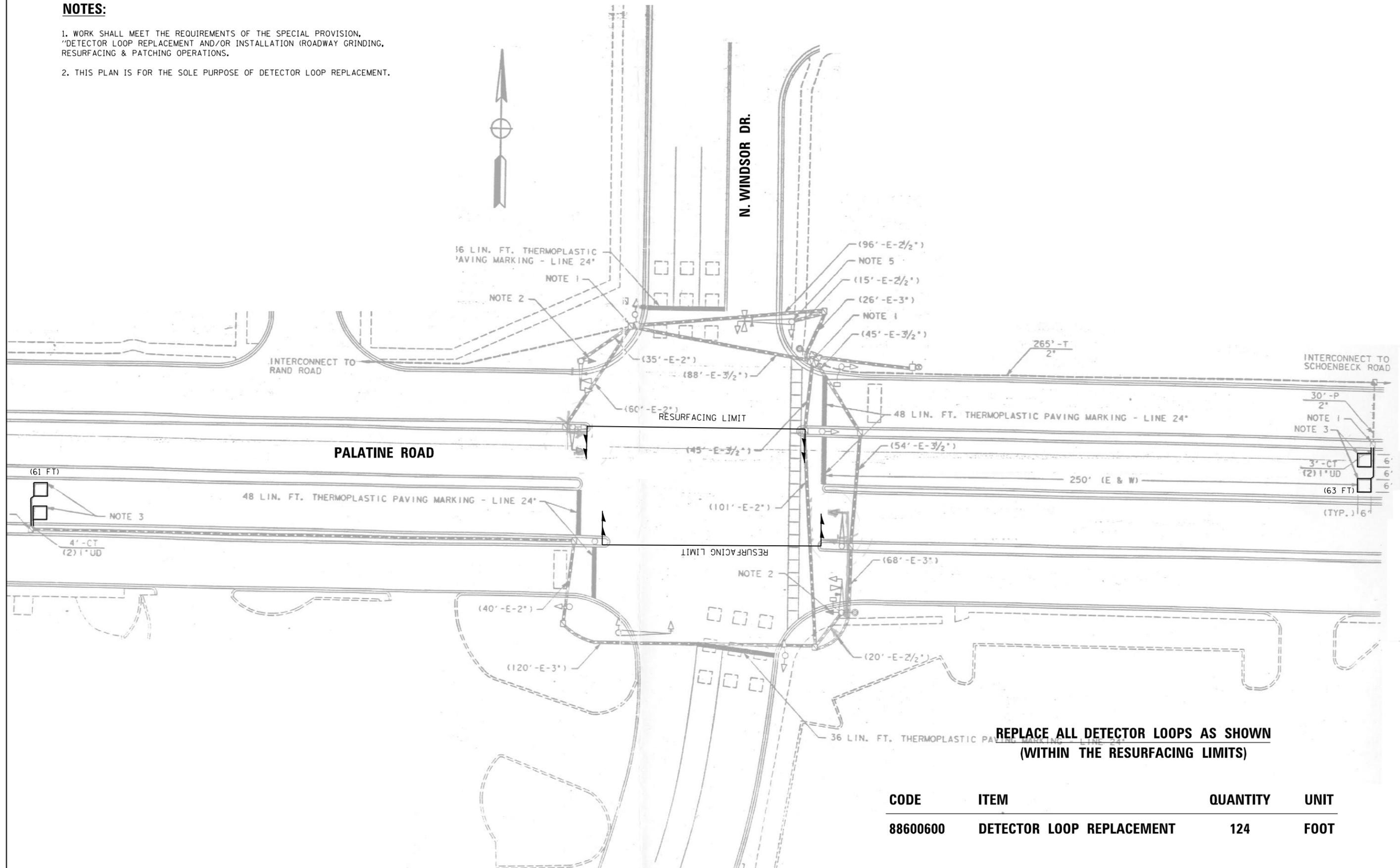
**DETECTOR LOOP REPLACEMENT PLAN  
E RAND RD. AT PALATINE RD.**

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 305	SECTION 2017 - 020RS	COUNTY COOK	TOTAL SHEETS 33	SHEET NO. 19
CONTRACT NO. 62F23				
ILLINOIS FED. AID PROJECT				

**NOTES:**

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

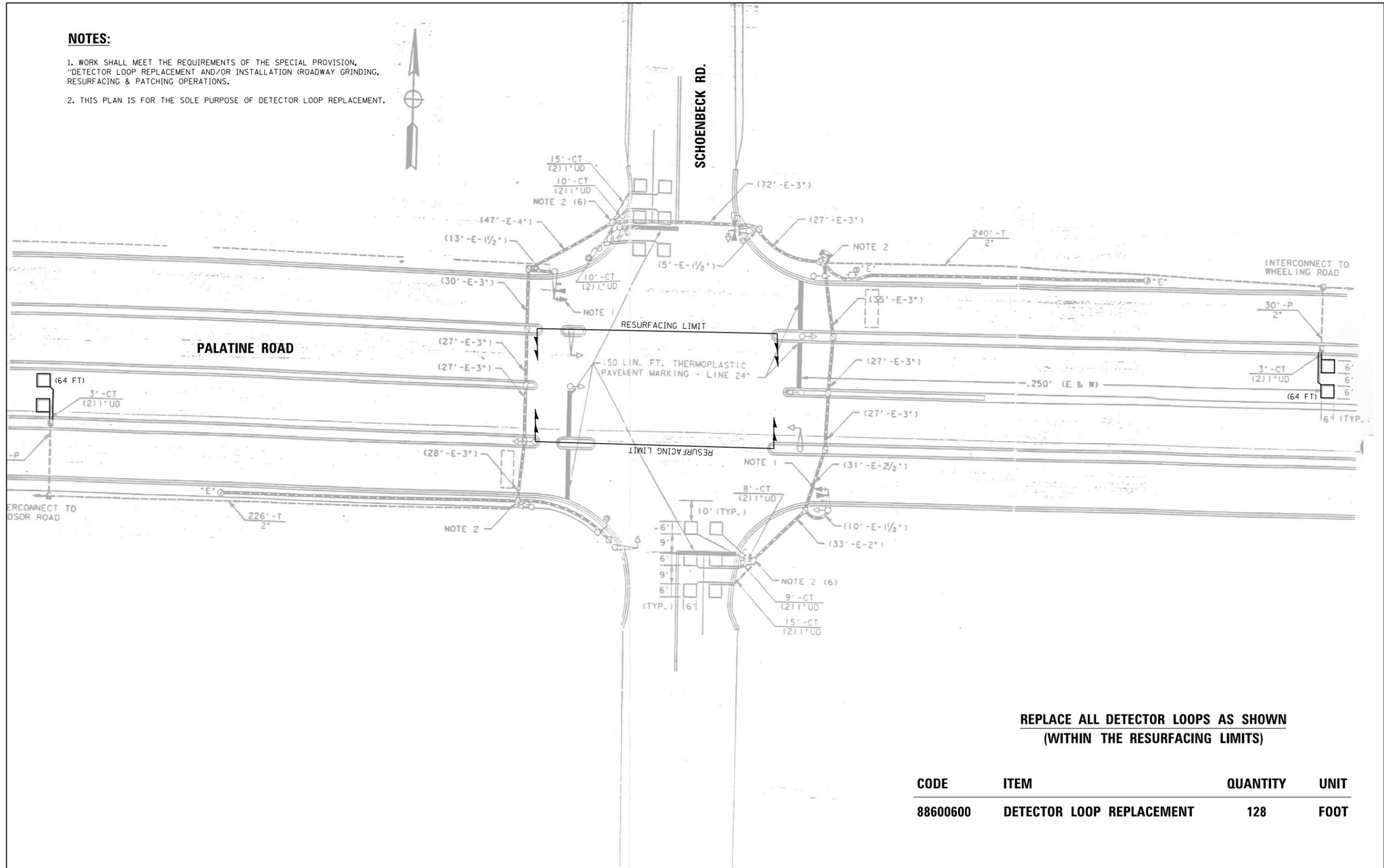


**REPLACE ALL DETECTOR LOOPS AS SHOWN  
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	124	FOOT

**NOTES:**

1. WORK SHALL MEET THE REQUIREMENTS OF THE SPECIAL PROVISION, "DETECTOR LOOP REPLACEMENT AND/OR INSTALLATION (ROADWAY GRINDING, RESURFACING & PATCHING OPERATIONS).
2. THIS PLAN IS FOR THE SOLE PURPOSE OF DETECTOR LOOP REPLACEMENT.

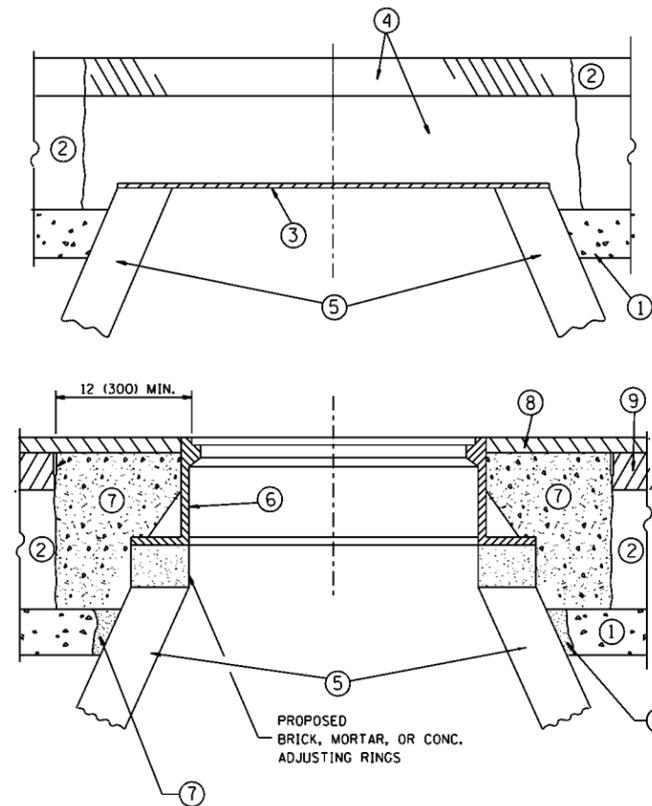


**REPLACE ALL DETECTOR LOOPS AS SHOWN  
(WITHIN THE RESURFACING LIMITS)**

CODE	ITEM	QUANTITY	UNIT
88600600	DETECTOR LOOP REPLACEMENT	128	FOOT

FILE NAME = 62F23 - Detector Loop Replacement Sheets.dgn	USER NAME = vargaso	DESIGNED - AV	REVISED -	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>DETECTOR LOOP REPLACEMENT PLAN N. SCHOENBECK RD. AT PALATINE RD.</b>	F.A.P. RTE. 305	SECTION 2017 - 020RS	COUNTY COOK	TOTAL SHEETS 33	SHEET NO. 21		
PLOT SCALE = 40.0114' / in.	CHECKED - LP	REVISIED -	SCALE:			SHEET OF SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT				
Default	PLOT DATE = 9/27/2018	DATE - 04/23/2018	REVISIED -									





**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1\* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

\* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ⑥ FRAME AND LID (SEE NOTES)
- ② EXISTING PAVEMENT
- ⑦ CLASS PP-1\* CONCRETE
- ③ 36 (900) DIAMETER METAL PLATE
- ⑧ PROPOSED HMA SURFACE COURSE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑨ PROPOSED HMA BINDER COURSE
- ⑤ EXISTING STRUCTURE

**LOCATION OF STRUCTURES:**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT:**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**NOTES:**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

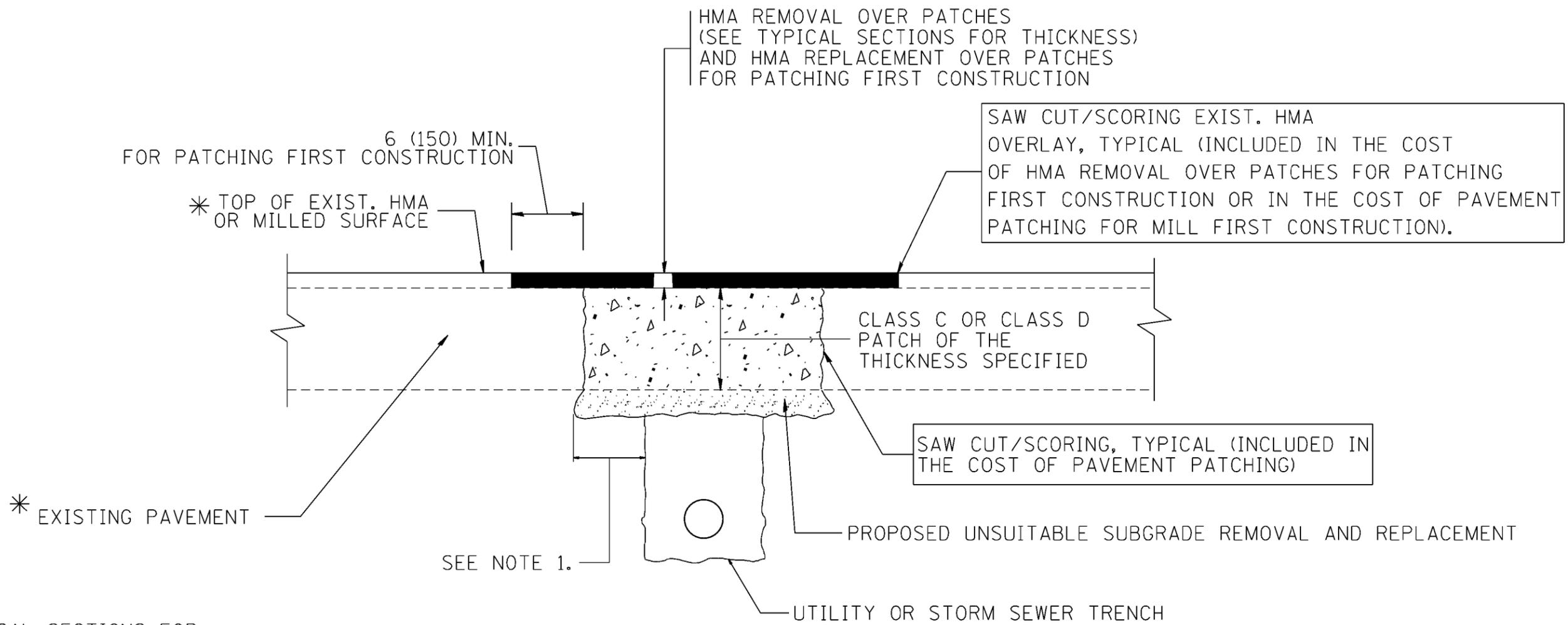
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = gulloumefp	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
p:\11\084EBID\INTEG\illinois.gov\FWIDOT\Documents\IDOT Offices\District 1\Projects\DI287\Drawings\Design\DistStd.dgn			REVISED - R. BORO 01-01-07
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 03-09-11
	PLOT DATE = 9/27/2018	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2017-020RS	COOK	33	23
BD600-03 (BD-8)		CONTRACT NO. 62F23		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



\* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

**NOTES:**

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = gulloumefp	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT</b>			F.A.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
pw\11084EBID\INTEG\illinois.gov\FWIDOT\Documents\IDOT Offices\District 1\Projects\1287\Drawings\Design\DistStd.dgn		REVISED - R. BORO 01-01-07	REVISED - R. BORO 09-04-07					305	2017-020RS	COOK	33	24
PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - R. BORO 09-04-07	REVISED - K. ENG 10-27-08		BD400-04 (BD-22)			CONTRACT NO. 62F23				
PLOT DATE = 9/27/2018	DATE - 10-25-94	REVISED - K. ENG 10-27-08	SCALE: NONE		SHEET NO. 1	OF 1 SHEETS	STA.	TO STA.		FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT		

VARIABLE - TO MEET EXISTING DIMENSIONS AND FIELD CONDITIONS (SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL PAY ITEM.

SEE STATE STANDARD 606001  
EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

18" (450) MAX.

1/4" (5) \*\*

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100) SOD RESTORATION (SEE NOTE ①).

EXISTING CONCRETE PAVEMENT, CONCRETE BASE COURSE OR FLEXIBLE PAVEMENT

3" (75) MIN.

SUITABLE BACKFILL MATERIAL (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT 24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED BY THE ENGINEER. (SEE NOTE ③).

**BASIS OF PAYMENT:**  
THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR "COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

\* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

\*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY,

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

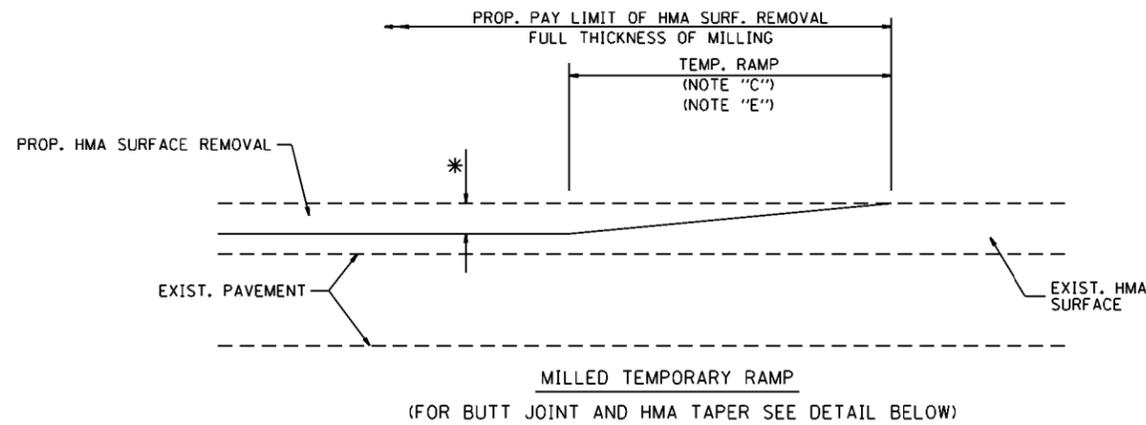
⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606 OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

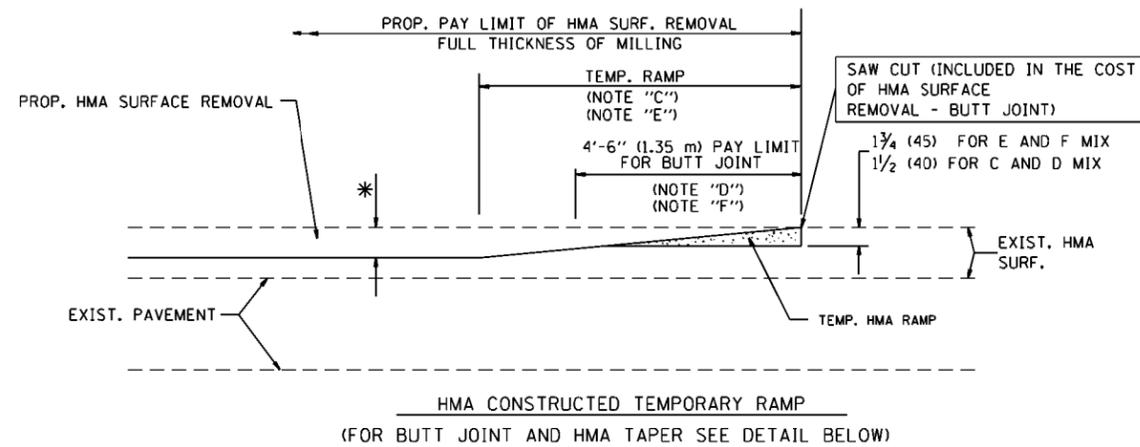
# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = gulloumefp	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>	F.A.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
p:\IL\084EBID\INTEG\illinois.gov\FWIDOT\Documents\IDOT Offices\District 1\Projects\DI287\Drawings\Design\DistStd.dgn			REVISED - A. ABBAS 03-21-97			305	2017-020RS	COOK	33	25
PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - M. GOMEZ 01-22-01				BD600-06 (BD-24)		CONTRACT NO. 62F23		
PLOT DATE = 9/27/2018	DATE - 03-11-94	REVISED - R. BORO 12-15-09	SCALE: NONE			SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT	

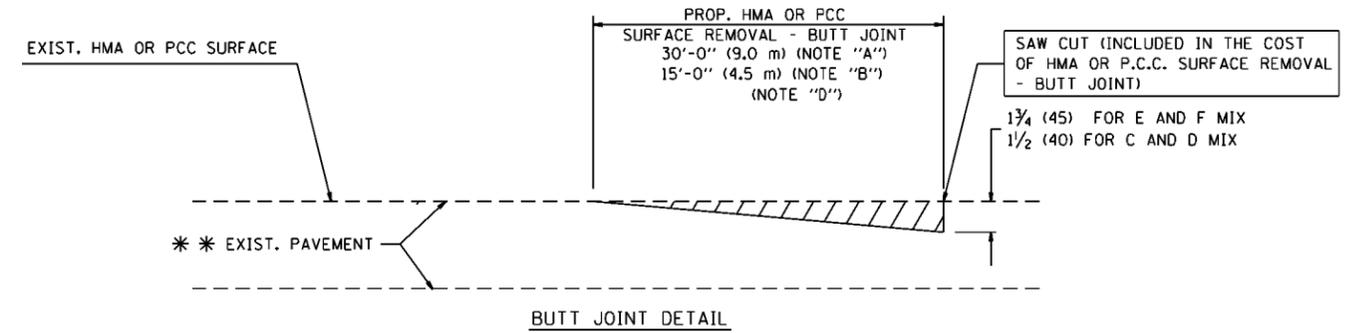


**OPTION 1**

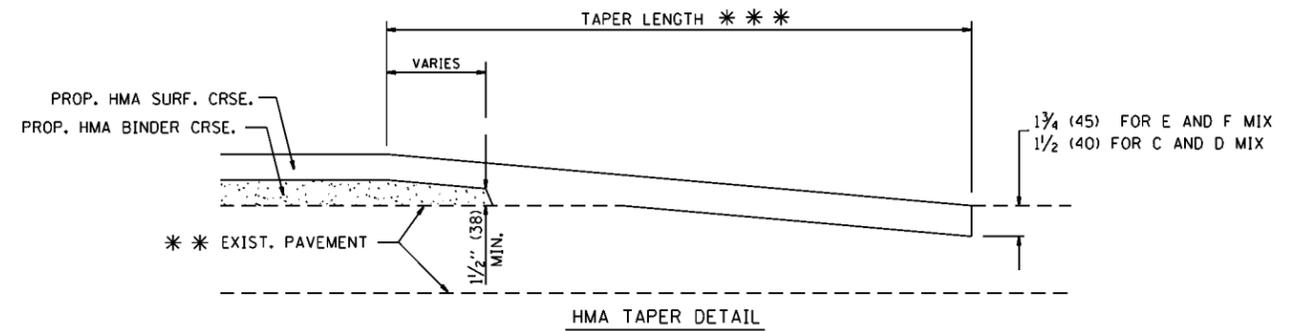


**OPTION 2**

**TYPICAL TEMPORARY RAMP**



**BUTT JOINT DETAIL**



**HMA TAPER DETAIL**

**TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY**

\*\*\* PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

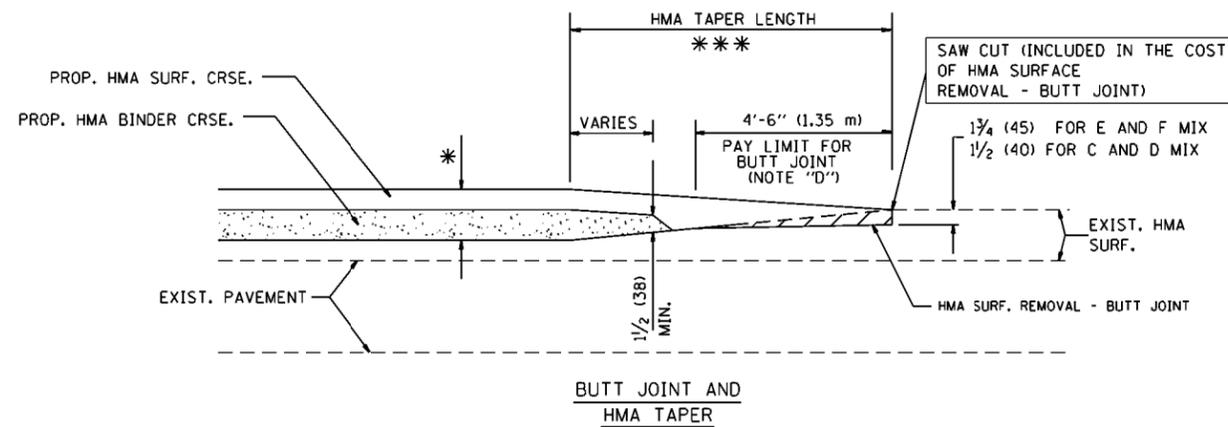
**NOTES**

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
  - B: MINOR SIDE ROADS.
  - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
  - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
  - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
  - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
  - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- \* SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- \*\*\* 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")  
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

**BASIS OF PAYMENT:**

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



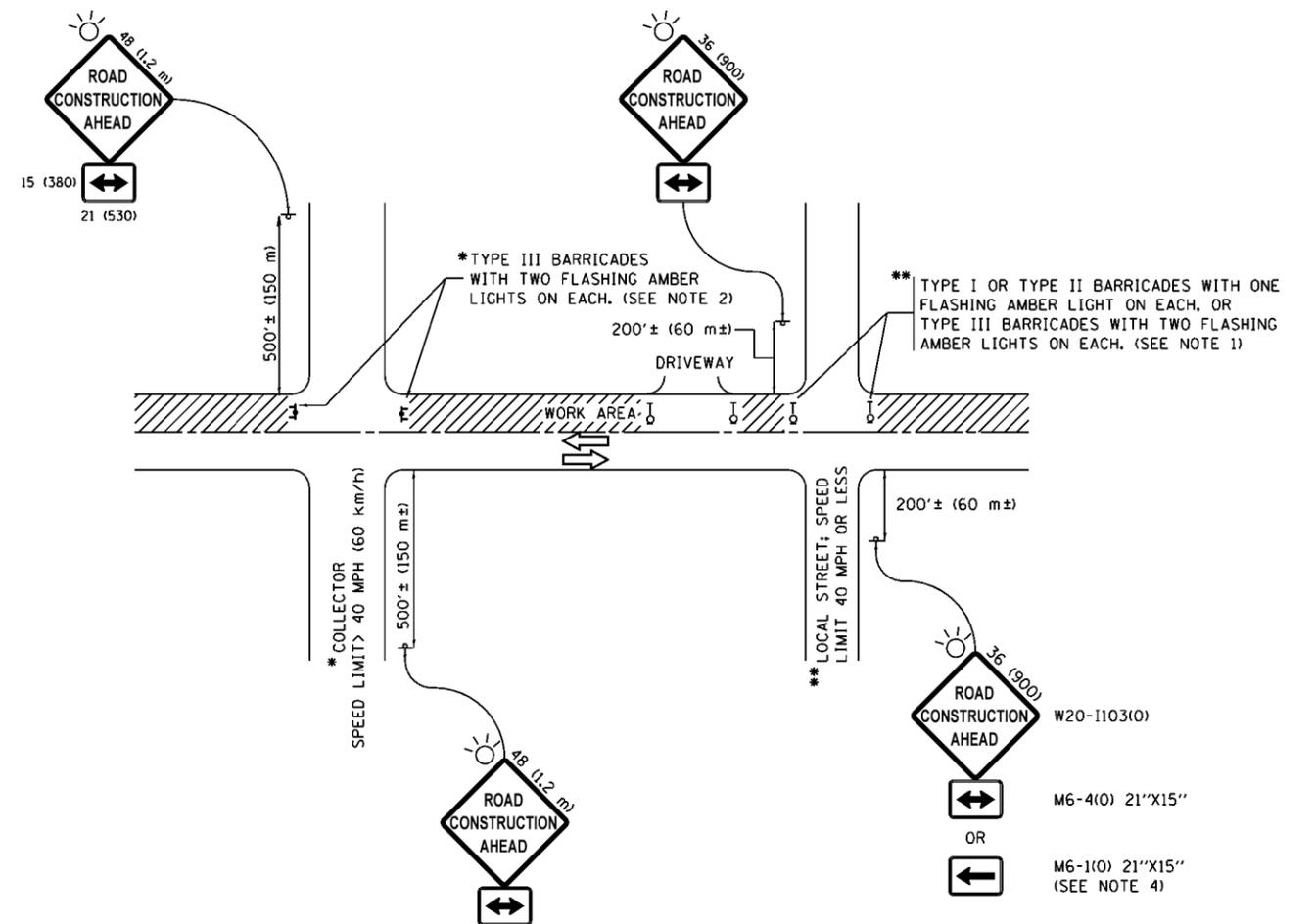
**TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING**

FILE NAME =	USER NAME = gulloumefp	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94
p:\IL\084EBID\INTEG\illinois.gov\FWIDOT\Documents\IDOT Offices\District 1\Projects\1287\Drawings\Design\DistStd.dgn			REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 9/27/2018	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>BUTT JOINT AND HMA TAPER DETAILS</b>	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2017-020RS	COOK	33	26
BD400-05 BD32		CONTRACT NO. 62F23		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**NOTES:**

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - a) ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
3. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
4. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
5. WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
6. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
7. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

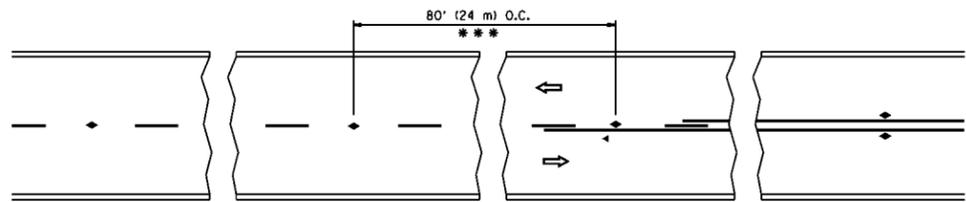
FILE NAME =	USER NAME = gulloumefp	DESIGNED - L.H.A.	REVISED - A. HOUSEH 10-15-96
pwi\IL084EBID\INTEG\illinois.gov\FWIDOT\Documents\IDOT Offices\District 1\Projects\DI287\DRM\Dist\Design\DistStd.dgn			REVISED - T. RAMMACHER 01-06-00
Default	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - A. SCHUETZE 07-01-13
	PLOT DATE = 9/27/2018	DATE - 06-89	REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

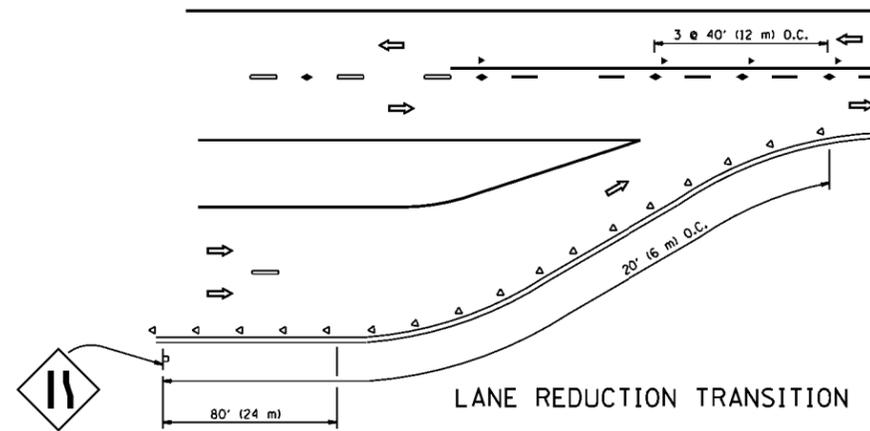
SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2017-020RS	COOK	33	27
TC-10			CONTRACT NO. 62F23	
ILLINOIS FED. AID PROJECT				

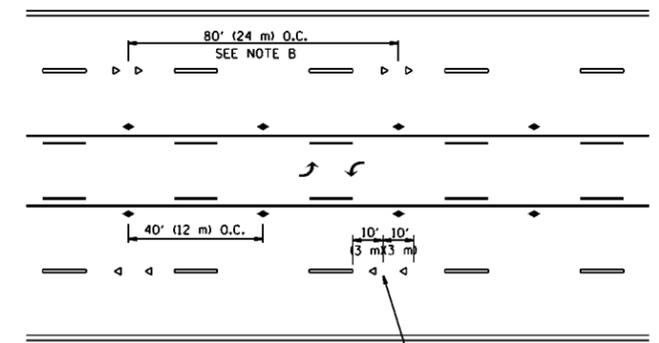


\*\*\* REDUCE TO 40' (12 m) O.C. ON CURVES WITH POSTED OR ADVISORY SPEED 45 M.P.H. (70 km/h) OR LESS.

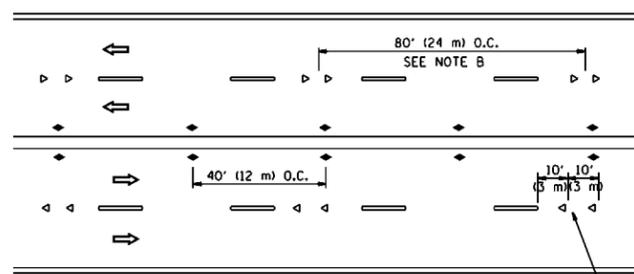
TWO-LANE/TWO-WAY



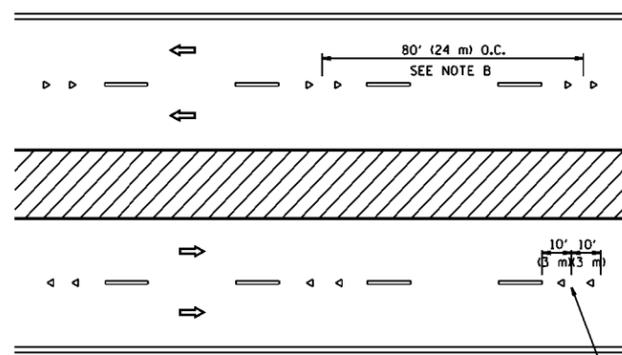
LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN



MULTI-LANE/UNDIVIDED



MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.

SYMBOLS

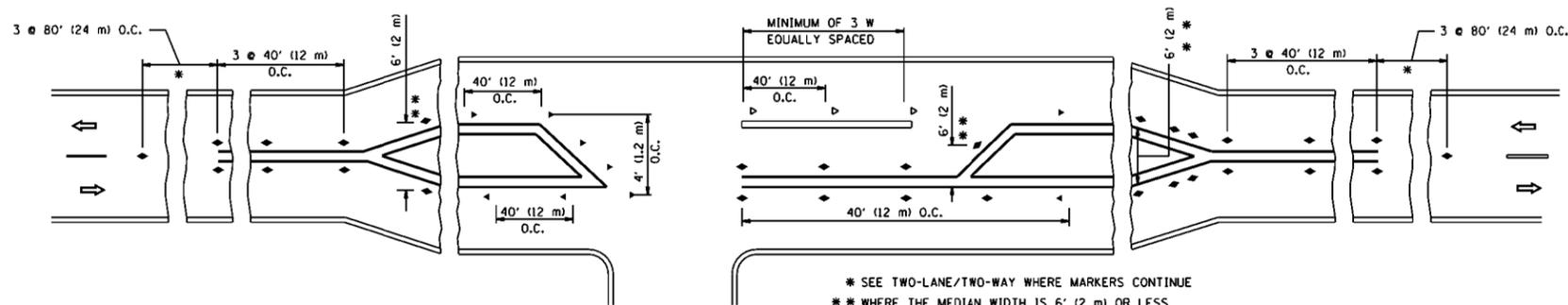
- YELLOW STRIPE
- WHITE STRIPE
- ◀ ONE-WAY AMBER MARKER
- ◀ ONE-WAY CRYSTAL MARKER (W/O)
- ◆ TWO-WAY AMBER MARKER

LANE MARKER NOTES

- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H. (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.

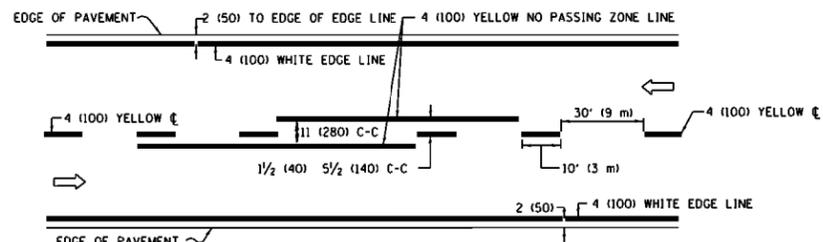


\* SEE TWO-LANE/TWO-WAY WHERE MARKERS CONTINUE  
 \*\* WHERE THE MEDIAN WIDTH IS 6' (2 m) OR LESS USE TWO-WAY MARKERS.

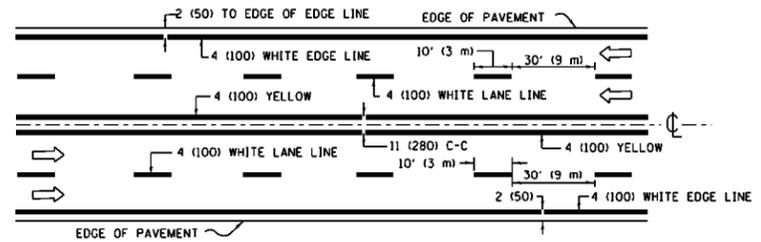
LEFT TURN

All dimensions are in inches (millimeters) unless otherwise shown.

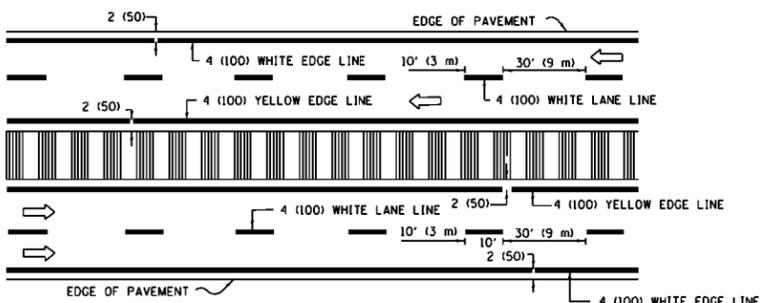
FILE NAME = p:\IL\084EBID\INTEG\illinois.gov\FWIDOT\Documents\IDOT Offices\District 1\Projects\1287\Drawings\Design\DistStd.dgn	USER NAME = gulloumf	DESIGNED -	REVISED - T. RAMMACHER 09-19-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS (SNOW-PLOW RESISTANT)			F.A.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - T. RAMMACHER 03-12-99		305	2017-020RS	COOK	33	28			
PLOT DATE = 9/27/2018	DATE -	REVISED - T. RAMMACHER 01-06-00	REVISED - C. JUCIUS 09-09-09	SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	TC-11 CONTRACT NO. 62F23 FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**2-LANE ROADWAY**

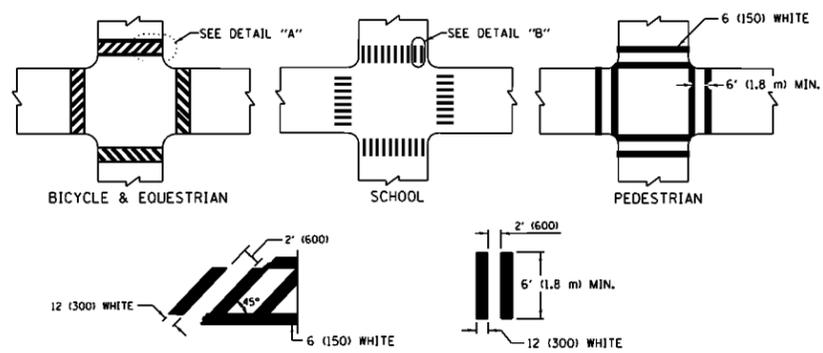


**MULTI-LANE UNDIVIDED**



**MULTI-LANE DIVIDED WITH MEDIAN**

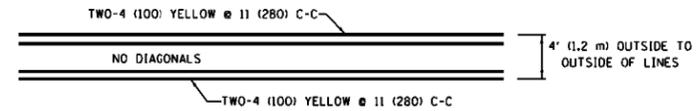
**TYPICAL LANE AND EDGE LINE MARKING**



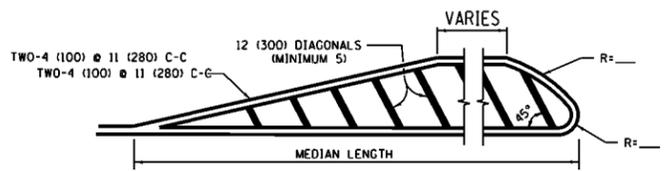
**DETAIL "A"      DETAIL "B"**

**TYPICAL CROSSWALK MARKING**

\* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES



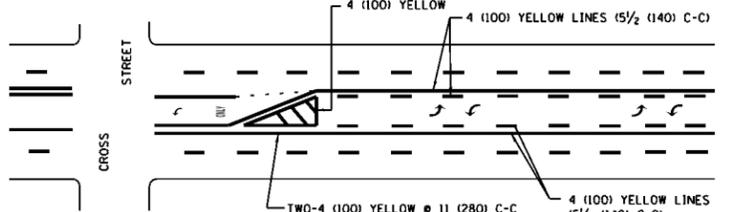
**4' (1.2 m) WIDE MEDIANS ONLY**



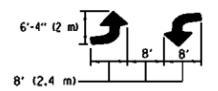
FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))  
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)  
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

**MEDIANS OVER 4' (1.2 m) WIDE**

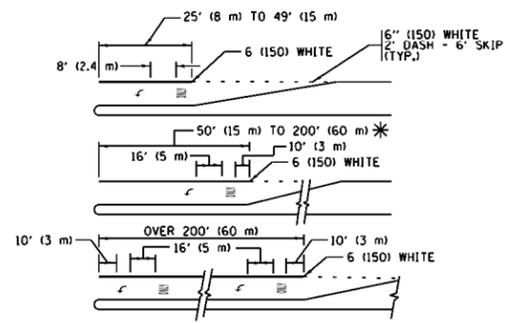


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



**MEDIAN WITH TWO-WAY LEFT TURN LANE**

**TYPICAL PAINTED MEDIAN MARKING**

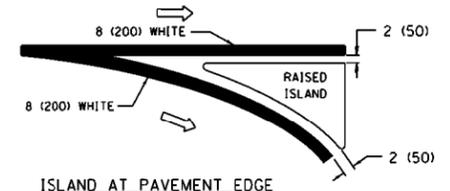
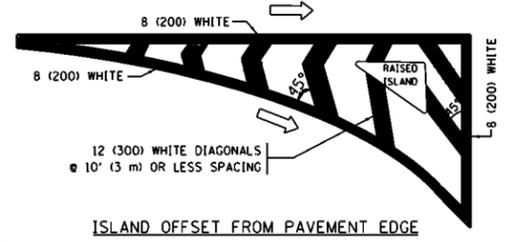


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.  
AREA = 15.6 SQ. FT. (1.5 m<sup>2</sup>)    AREA = 20.8 SQ. FT. (1.9 m<sup>2</sup>)

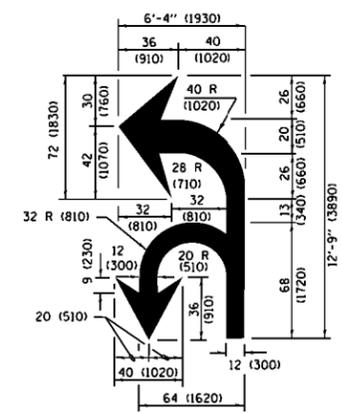
\* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

**TYPICAL LEFT (OR RIGHT) TURN LANE**

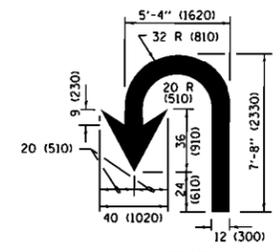
**TYPICAL TURN LANE MARKING**



**TYPICAL ISLAND MARKING**



**COMBINATION LEFT AND U-TURN**



**U-TURN**

**LANE REDUCTION TRANSITION**

\* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

D(FT)	SPEED LIMIT
345	30
425	35
500	40
580	45
665	50
750	55

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING /REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m <sup>2</sup> ) EACH "X"=54.0 SQ. FT. (5.0 m <sup>2</sup> )
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS ≥ 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

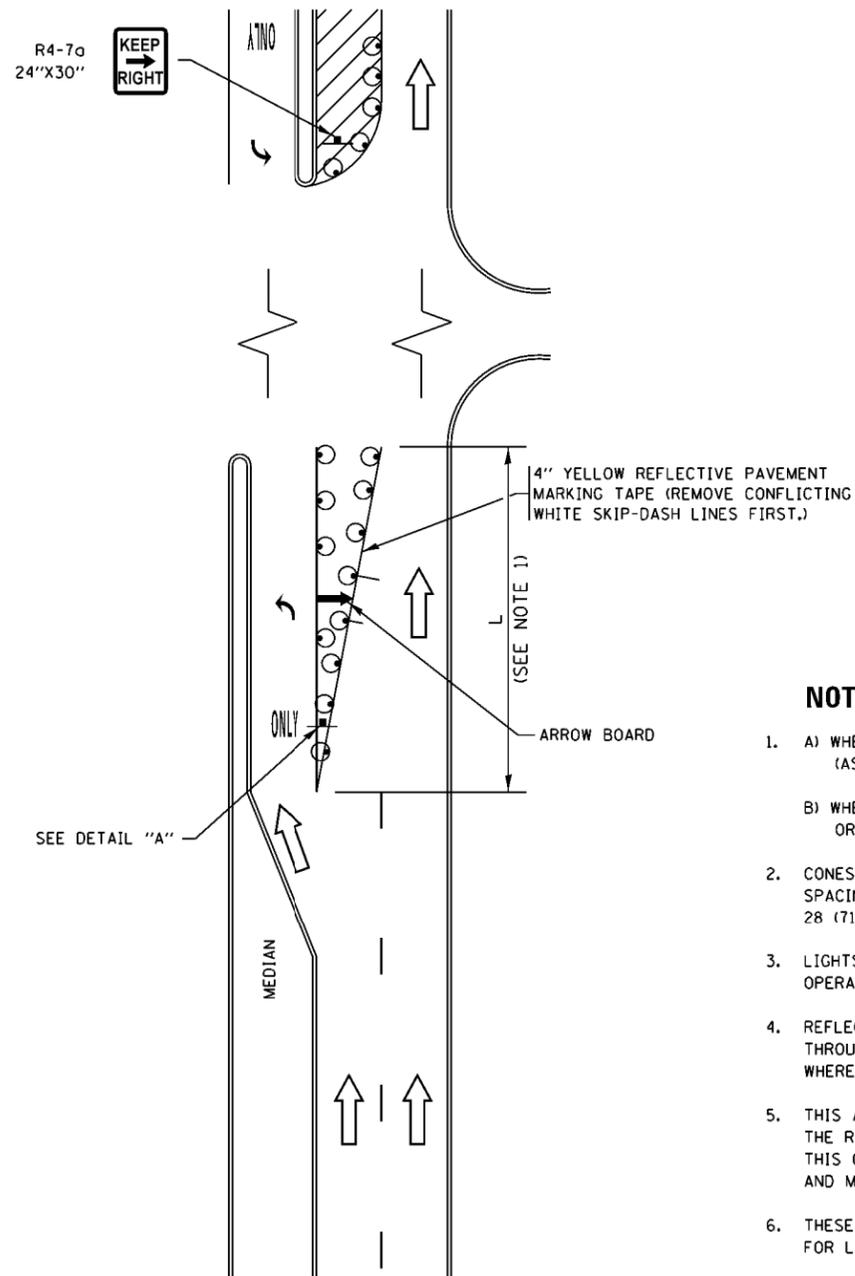
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Default	PLOT SCALE = 100.0000' / 1in.	DATE - 03-19-90	REVISED - C. JUCIUS 12-21-15
	PLOT DATE = 9/27/2018		REVISED - C. JUCIUS 04-12-16

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>DISTRICT ONE</b>			
<b>TYPICAL PAVEMENT MARKINGS</b>			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

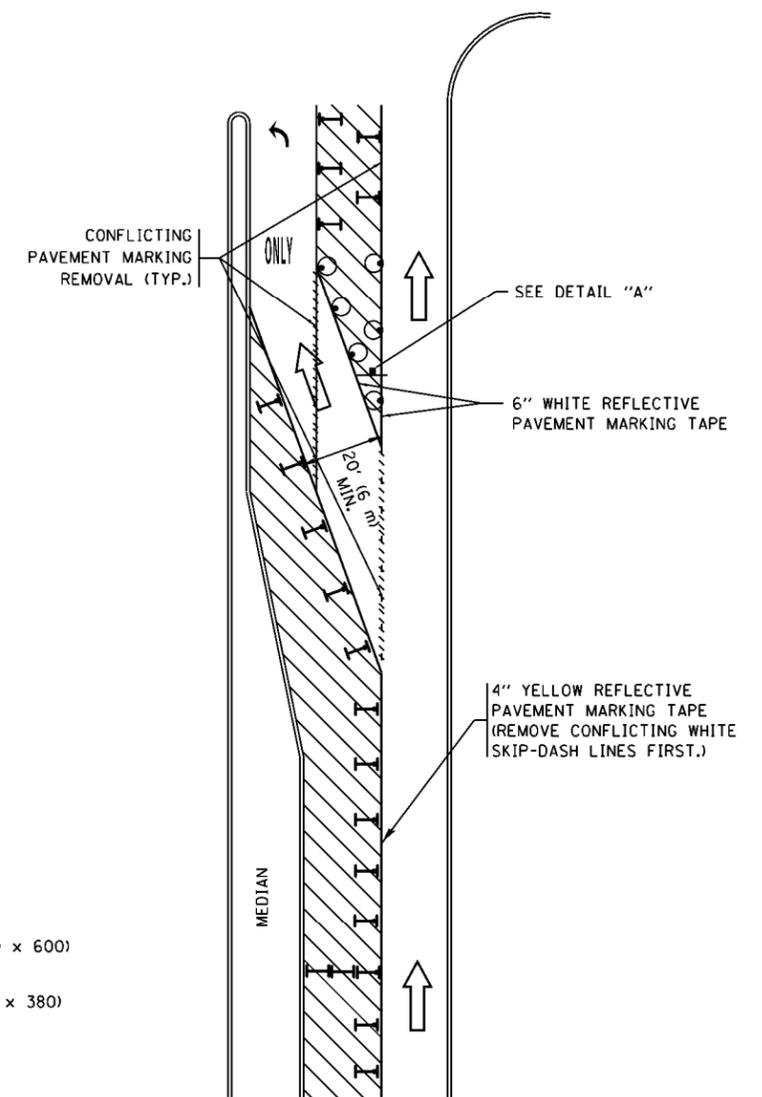
F.A.P. R.T.E.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2017-020RS	COOK	33	29
TC-13			CONTRACT NO. 62F23	
ILLINOIS FED. AID PROJECT				

# TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER



**FIGURE 1**

# TURN BAY ENTRANCE WITHIN A LANE CLOSURE



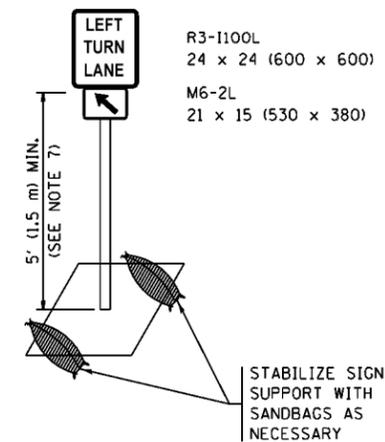
**FIGURE 2**

### LEGEND

- WORK AREA
- LANE OPEN TO TRAFFIC
- ARROW BOARD
- TYPE I OR II BARRICADE OR DRUM WITH STEADY BURN LIGHT
- DRUM WITH STEADY BURN LIGHT
- SIGN ASSEMBLY
- TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

### NOTES:

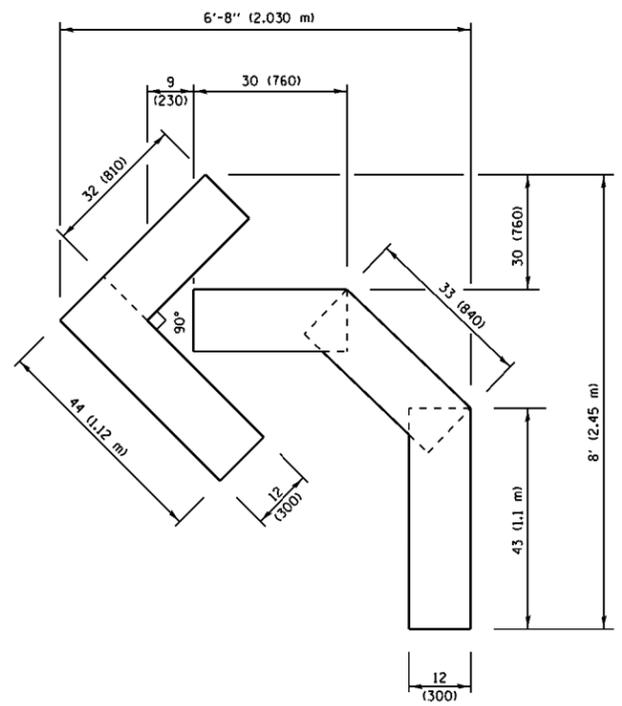
1. A) WHEN "L" IS  $\leq$  THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.  
B) WHEN "L" IS  $>$  THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH PREQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



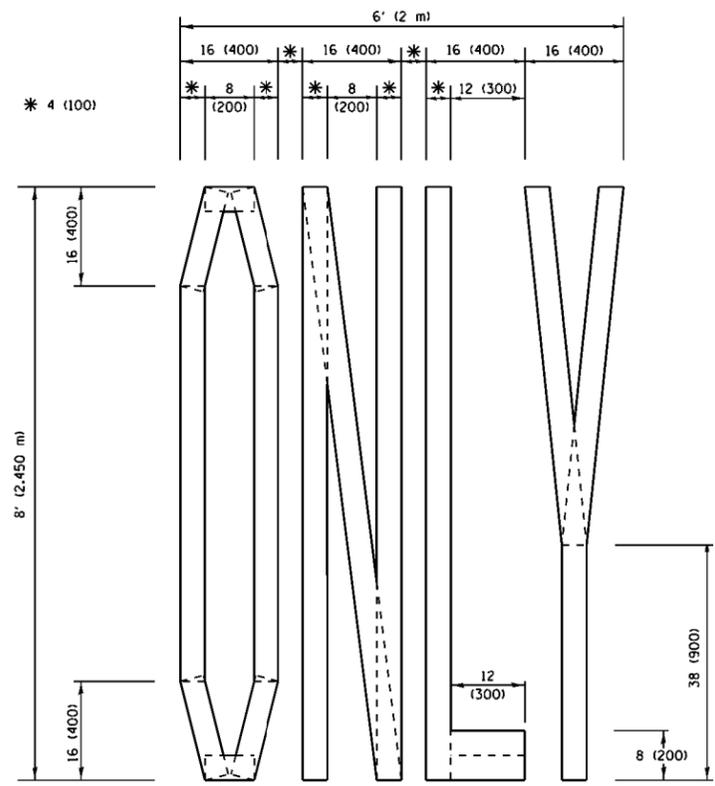
**DETAIL A**

All dimensions are in inches (millimeters) unless otherwise shown.

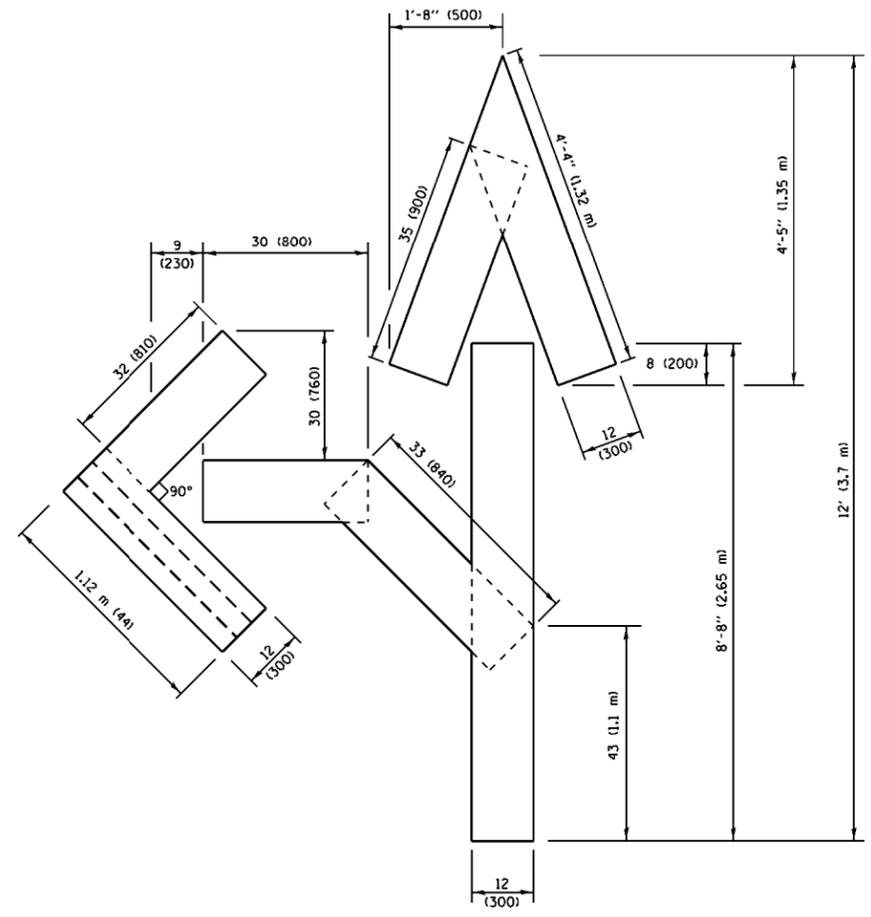
FILE NAME =	USER NAME = gulloumefp	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	<b>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</b>	<b>TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)</b>				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
Default		REVISED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16		SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.	305	2017-020RS	COOK	33	30
		REVISED - T. RAMMACHER 01-06-00	REVISED -								TC-14		CONTRACT NO. 62F23		



**QUANTITY**  
 4 (100) LINE = 45.5 ft. (13.9 m)  
 15.2 sq. ft. (1.41 sq. m)

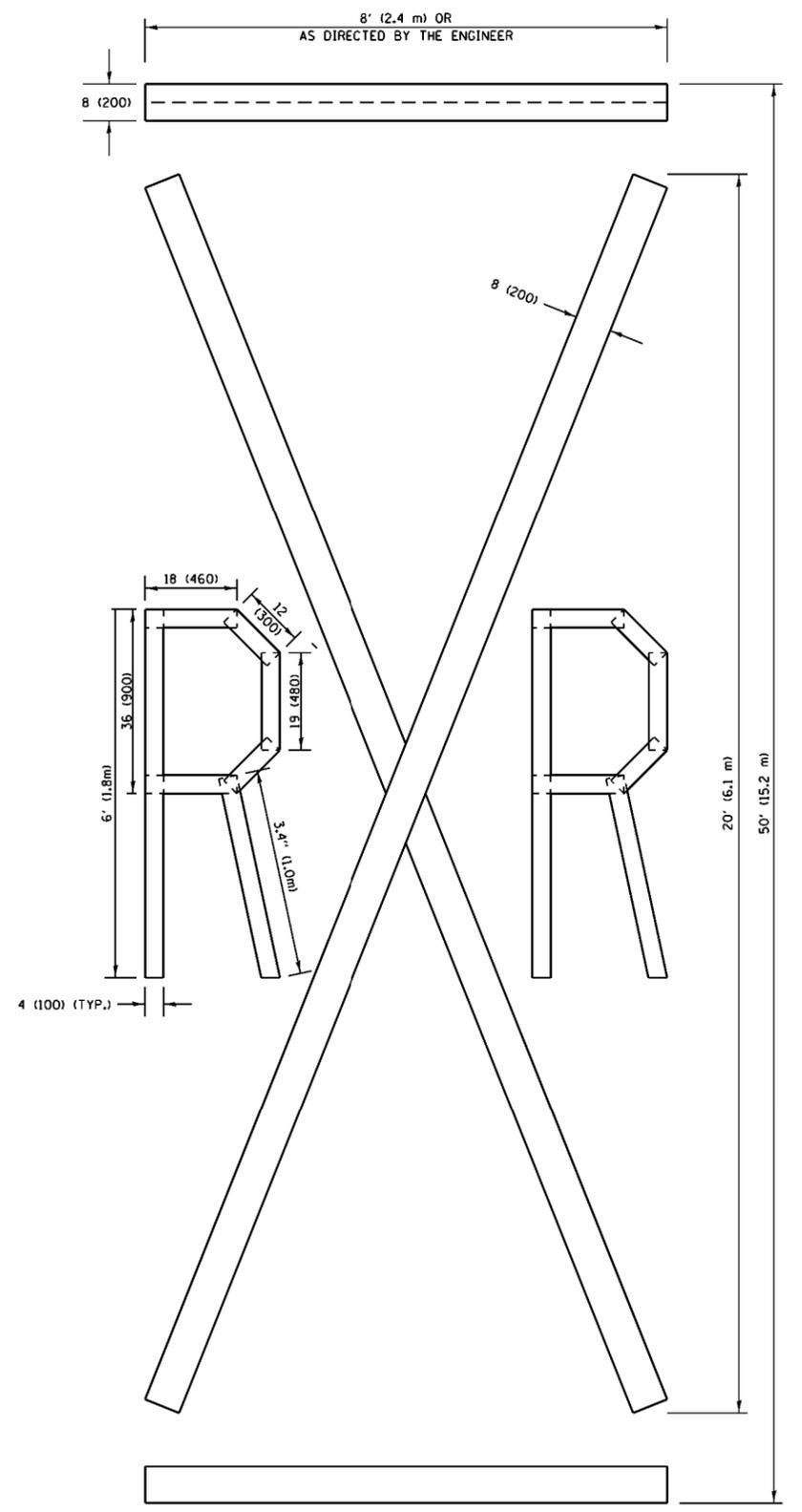


**QUANTITY**  
 4 (100) LINE = 64.1 ft. (19.5 m)  
 21.4 sq. ft. (1.99 sq. m)



**QUANTITY**  
 4 (100) LINE = 82.5 ft. (25.1 m)  
 27.5 sq. ft. (2.53 sq. m)

**NOTE:**  
 ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



**QUANTITY**  
 4 (100) LINE = 225.9 ft. (68.9 m)  
 75.3 sq. ft. (6.99 sq. m)

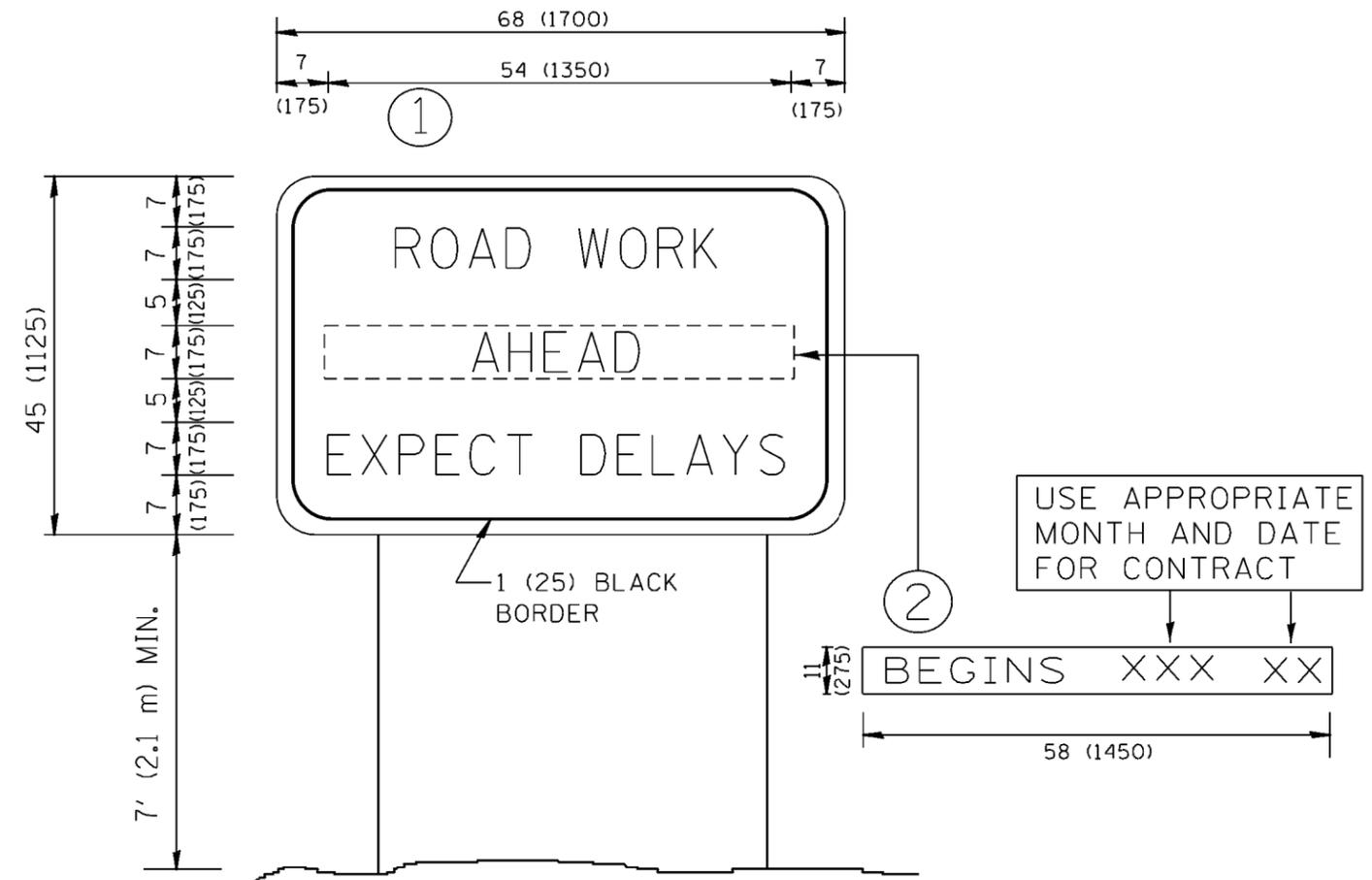
All dimensions are in inches (millimeters) unless otherwise shown.

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pw\11084EBID\INTEG\illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\DI267\Drawings\Design\DistStd.dgn			REVISED - E. GOMEZ 08-28-00
			REVISED - E. GOMEZ 08-28-00
			REVISED - A. SCHUETZE 09-15-16

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

<b>SHORT TERM PAVEMENT MARKING LETTERS AND SYMBOLS</b>			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2017-020R5	COOK	33	31
TC-16		CONTRACT NO. 62F23		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = gulloumefp	DESIGNED -	REVISED - R. MIRS 09-15-97
pw\l\084EBID\INTEG\illinois.gov\PWIDOT\Documents\DOT Offices\District 1\Projects\DI287\Drawings\Design\DistStd.dgn		REVISED -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 100.0000' / 1in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 9/27/2018	DATE -	REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

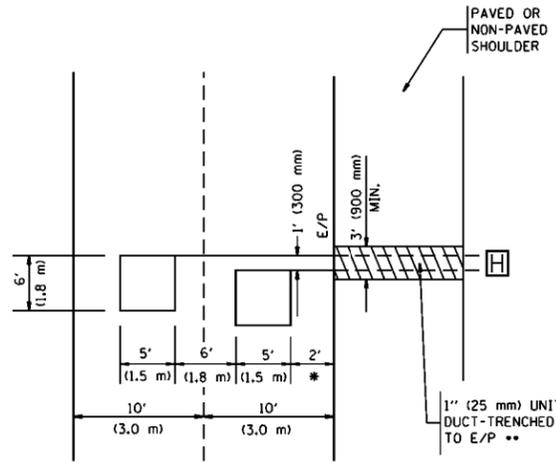
**ARTERIAL ROAD  
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.R. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2017-020RS	COOK	33	32
TC-22		CONTRACT NO. 62F23		
FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT				

**LOOPS NEXT TO SHOULDERS**

PROVIDE A PAVEMENT REPLACEMENT NOTE WHICH SHOULD EQUAL 3' (900 mm) X WIDTH OF PAVED SHOULDER.



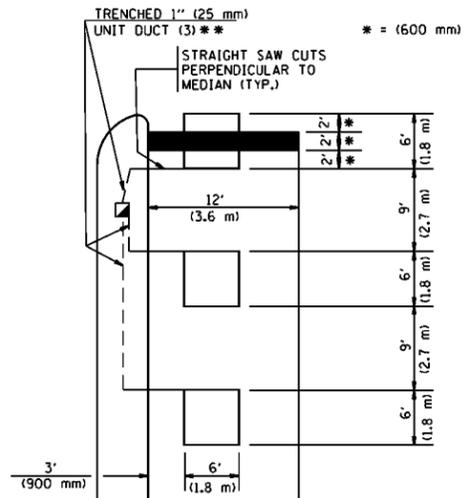
\* = (600 mm)

\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

**LEFT TURN LANES WITH MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

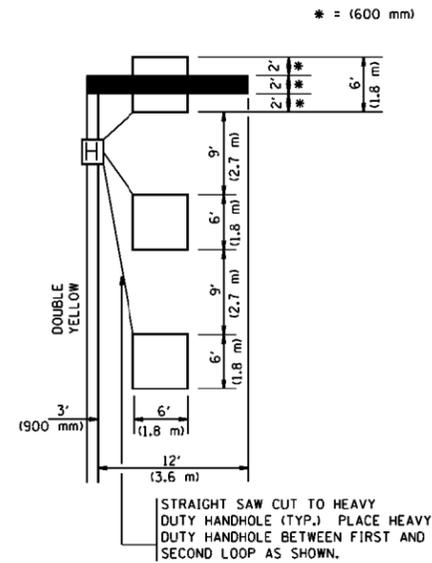


\*\* UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**LEFT TURN LANES WITHOUT MEDIANS  
VOLUME DENSITY ("FAR OUT" DETECTION)  
ON SAME APPROACH**

(PROTECTED / PERMITTED LEFT TURN PHASING)



NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

**NOTES:**

**VEHICLES LOOP DETECTORS**

- \* ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- \* EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- \* ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- \* EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- \* WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- \* WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

**PLACEMENT OF DETECTORS**

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

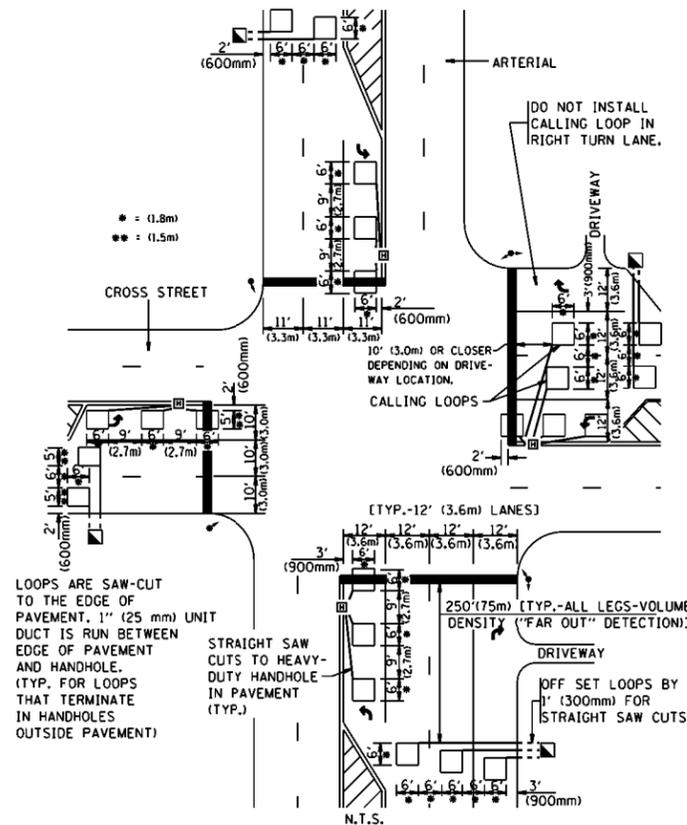
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

**NOTE:**

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

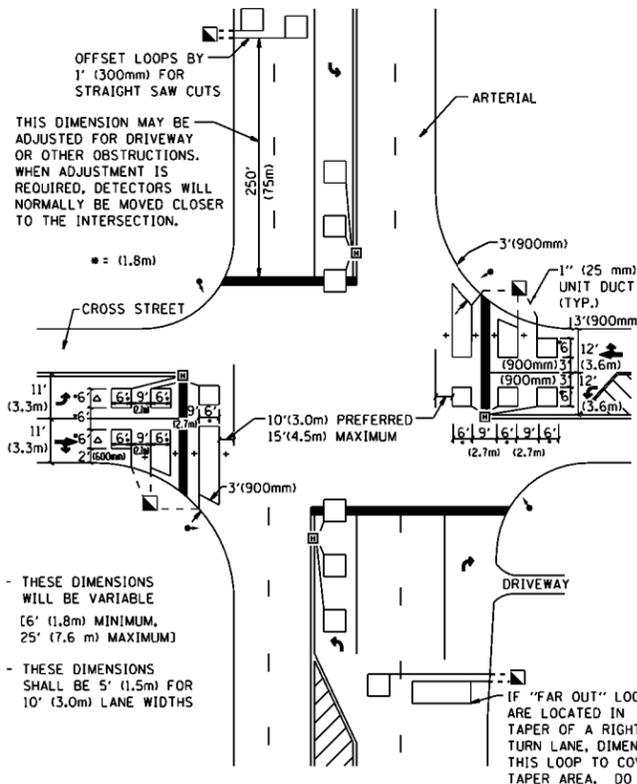
THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)**



**DETAIL 1**  
N.T.S.

**ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)  
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)**



**DETAIL 2**  
N.T.S.

FILE NAME =	USER NAME = gulloumefp	DESIGNED -	REVISED -
p:\il\084EBID\INTEG\illinois.gov\PI\DOT\Documents\DOT Offices\District 1\Projects\DI287\DRM\Info\Design\DistStd.dgn		CHECKED - R.K.F.	REVISED -
PLOT SCALE = 100.0000' / 1in.		DATE -	REVISED -
PLOT DATE = 9/27/2018			

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DISTRICT 1 - DETECTOR LOOP INSTALLATION  
DETAILS FOR ROADWAY RESURFACING**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
305	2017-020RS	COOK	33	33
TS-07			CONTRACT NO. 62F23	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				