

073

01-18-2019 LETTING ITEM 073

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

| F.A.S. RTE | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|------------|-------------|----------|--------------------|-----------|
| 1780 | 26BDR-BJR-1 | CLINTON | 27 | 1 |
| | | ILLINOIS | CONTRACT NO. 76M03 | |

FOR INDEX OF SHEETS, SEE SHEET NO. 2

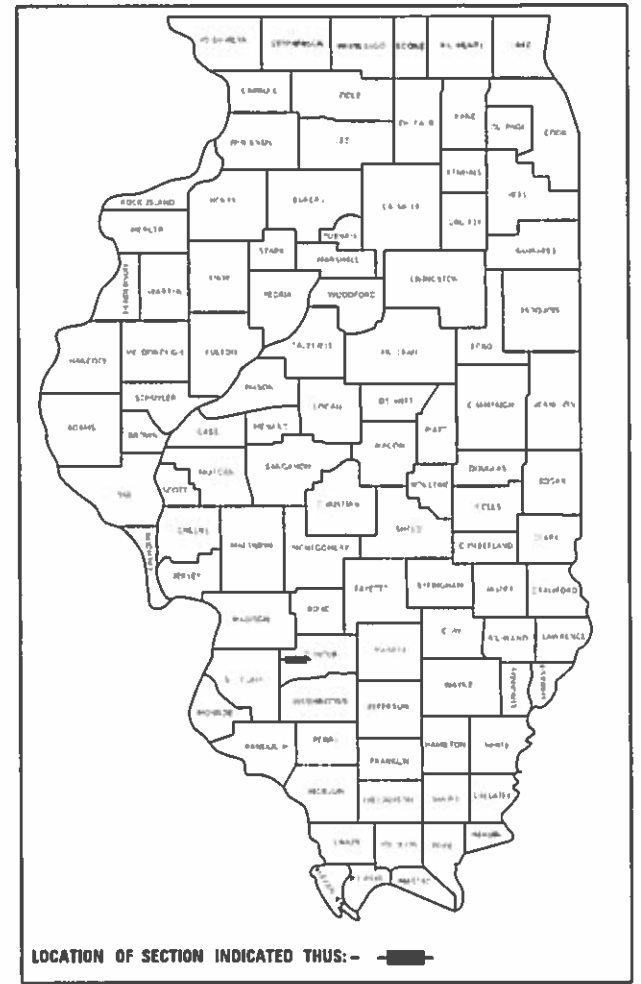
PROPOSED HIGHWAY PLANS

FAS ROUTE 1780 (OLD US 50)
SECTION 26BDR-BJR-1

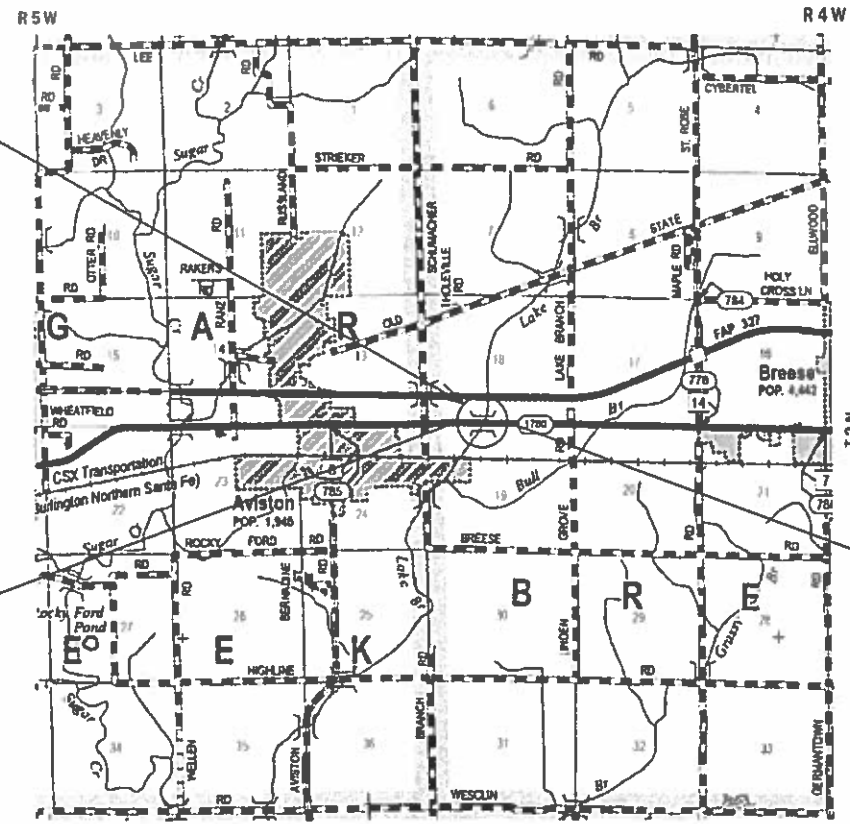
BRIDGE & JOINT REPAIR
CLINTON COUNTY

C-98-011-19

D-98-008-19

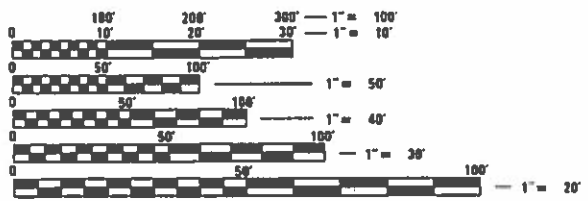


SN 014-0031
 OLD US 50
 STA. 258+14
 LAT: 38.612128490
 LONG: -89.585546680



BEGIN PROJECT
STA. 261+70.00

END PROJECT
STA. 254+60.00



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
 JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
 1-800-892-0123
 OR 811

PROJECT ENGINEER: HERVE GELIN (618) 346-3179
 PROJECT MANAGER: ALVIN NIEVES-ROSARIO (618) 346-3141

GROSS LENGTH = 710 FT. = 0.134 MILE
 NET LENGTH = 710 FT. = 0.134 MILE

TRAFFIC DATA
 2015 ADT = 5250 (ACTUAL)
 2020 ADT = 5500 (ESTIMATED)
 2040 ADT = 6700 (ESTIMATED)
 SU = 3.8% MU = 1.9%

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SUBMITTED *Oct 26 2018*
Jeffrey Z. Kan
 REGIONAL ENGINEER

Dec 7 2018
 ENGINEER OF DESIGN AND ENVIRONMENT

Dec 7 2018
 DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

PRINTED BY THE AUTHORITY
 OF THE STATE OF ILLINOIS

CONTRACT NO. 76M03

INDEX OF SHEETS

- 1. COVER SHEET
- 2. INDEX OF SHEETS, HIGHWAY STANDARDS, GENERAL NOTES, AND COMMITMENTS
- 3-4. SUMMARY OF QUANTITIES
- 5. SCHEDULES OF QUANTITIES
- 6. TYPICAL SECTIONS & BUTT JOINT DETAIL
- 7. WIDE LOAD SIGNING
- 8-10. STAGING PLANS
- 11-27. STRUCTURE PLANS

HIGHWAY STANDARDS

- 000001-07 STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
- 001001-02 AREA OF REINFORCEMENT BARS
- 001006 DECIMAL OF AN INCH AND OF A FOOT
- 482001-02 HMA SHOULDER ADJACENT TO FLEXIBLE PAVEMENT
- 701001-02 OFF-RD OPERATIONS, 2L, 2W, MORE THAN 15' AWAY
- 701006-05 OFF-RD OPERATIONS, 2L, 2W, 15' TO 24" FROM EDGE OF PAVEMENT
- 701011-04 OFF-RD OPERATIONS, 2L, 2W, DAY ONLY
- 701201-05 LANE CLOSURE, 2 LANE, 2 WAY, DAY ONLY, FOR SPEEDS >45 MPH
- 701301-04 LANE CLOSURE, 2 LANE, 2 WAY, SHORT TIME OPERATIONS
- 701306-04 LANE CLOSURE, 2 LANE, 2 WAY, SLOW MOVING OPERATIONS, DAY ONLY, FOR SPEEDS >45 MPH
- 701311-03 LANE CLOSURE, 2 LANE, 2 WAY, MOVING OPERATIONS, DAY ONLY
- 701321-17 LANE CLOSURE, 2 LANE, 2 WAY, BRIDGE REPAIR WITH BARRIER
- 701326-04 LANE CLOSURE, 2 LANE, 2 WAY, PAVEMENT WIDENING FOR SPEEDS >45 MPH
- 701901-08 TRAFFIC CONTROL DEVICES
- 704001-08 TEMPORARY CONCRETE BARRIER
- 780001-05 TYPICAL PAVEMENT MARKINGS
- 781001-04 TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
- 782006 GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS

GENERAL NOTES

1. ILLINOIS STATE LAW REQUIRES A 48-HOUR NOTICE BE GIVEN TO ALL UTILITIES BEFORE DIGGING. FIELD MARKING OF FACILITIES MAY BE OBTAINED BY CONTACTING J.U.L.I.E. OR FOR NON-MEMBERS, THE UTILITY COMPANY DIRECTLY. AGENCIES KNOWN TO HAVE FACILITIES WITHIN THE PROJECT AREA ARE AS FOLLOWS:

| UTILITY | ABOVE GROUND | BELOW GROUND |
|--|--------------|--------------|
| •AT&T ILLINOIS (COMMUNICATIONS) | X | X |
| •VILLAGE OF AVISTON (WATER & SANITARY SEWER) | | X |
| •ST. ROSE PUBLIC WATER DISTRICT (WATER & SANITARY SEWER) | | X |

MEMBERS OF J.U.L.I.E. (800)-892-0123 OR 811 ARE INDICATED BY •. NON J.U.L.I.E. MEMBERS MUST BE NOTIFIED INDIVIDUALLY.

2. THE CONTRACTOR AND ENGINEER SHALL BE AWARE THAT NO SURVEY WAS PERFORMED FOR THIS PROJECT. THE STATIONING AND TOPOGRAPHY SHOWN IN THE PLANS WERE CREATED USING RECORD PLANS AND FIELD MEASUREMENTS MADE BY DESIGN PERSONNEL. BOTH SHALL BE ASSUMED TO BE APPROXIMATE.
3. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
4. THE THICKNESS OF THE BITUMINOUS MIXTURE SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE BITUMINOUS MIXTURE IS PLACED, SUCH AS ON THE NORTH APPROACH.
5. THE PROPOSED PAVEMENT MARKING SHALL MATCH THE LOCATIONS OF THE EXISTING PAVEMENT MARKING, AS DIRECTED BY THE ENGINEER.
6. ANY SEEDING REQUIRED OUTSIDE THE CONSTRUCTION LIMITS WILL NOT BE PAID FOR SEPERATELY AND CONSIDERED AS A CONTRACTOR'S EXPENSE.
7. COMPACTION OF ASPHALT UNDER THE GUARDRAIL MUST MEET TESTING REQUIREMENTS.
8. IT IS INTENDED FOR THE EXISTING GUARDRAIL TO REMAIN IN PLACE DURING HMA WIDENING PLACEMENT. DAMAGE CAUSED BY WIDENING EXCAVATION/HMA PLACEMENT SHALL BE REPAIRED AT THE CONTRACTORS EXPENSE.

THE FOLLOWING MIXTURE REQUIREMENTS ARE APPLICABLE FOR THIS PROJECT:

| MIXTURE USE | SURFACE | SHOULDER ≥ 2.25" | SHOULDER < 2.25" |
|-----------------------------|-----------------------|-----------------------|-----------------------|
| AC/PG | PG 64-22 | PG 64-22 | PG 64-22 |
| RAP %(MAX) | SEE SPECIAL PROVISION | SEE SPECIAL PROVISION | SEE SPECIAL PROVISION |
| DESIGN AIR VOIDS | 4.0% @ Ndes=70 | 4.0% @ Ndes=30 | 4.0% @ Ndes=30 |
| MIX COMPOSITION (GRADATION) | IL 9.5 | IL 19.0L | IL 9.5L |
| FRICITION AGG | MIXTURE D | | |
| QUALITY MGMT PROGRAM | QC/OA | QC/OA | QC/OA |

PLAN QUANTITIES FOR BITUMINOUS CONCRETE SURFACE COURSE ITEMS ARE CALCULATED USING A UNIT WEIGHT OF 112 LBS/SQ YD/IN (59.8 KG/SQ M/25 MM THICKNESS)

COMMITMENTS

NONE

MODEL: \\MODELNAME; FILE NAME: P:\JULI@EID\DOT\Documents\DOT Office\DIR\ct 8\Project\0876M03\CADD\DATA\CAD\Sheet\0876M03-ctc-pln.dgn

| | | |
|-------------------------------|------------------|-----------------|
| USER NAME = davidsonmw | DESIGNED - _____ | REVISED - _____ |
| | DRAWN - _____ | REVISED - _____ |
| PLOT SCALE = 100,0000 ' / in. | CHECKED - _____ | REVISED - _____ |
| PLOT DATE = 11/7/2018 | DATE - _____ | REVISED - _____ |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**INDEX OF SHEETS, HIGHWAY STANDARDS,
GENERAL NOTES AND COMMITMENTS**

SCALE: N/A SHEET 1 OF 1 SHEETS STA. N/A TO STA. N/A

| F.A.S. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-------------|---------|--------------|-----------|
| 1780 | 26BDR-BJR-1 | CLINTON | 27 | 2 |
| CONTRACT NO. 76M03 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | 100% STATE | |
|----------|--|-------|----------------|--------------|----------|
| | | | | CONSTR. CODE | |
| | | | | RURAL | BRIDGE |
| | | | | 0059 | 014-0031 |
| 20200500 | EARTH EXCAVATION (WIDENING) | CU YD | 103 | 103 | |
| 31102500 | SUBBASE GRANULAR MATERIAL, TYPE C 8" | SQ YD | 231 | 231 | |
| 40600275 | BITUMINOUS MATERIALS (PRIME COAT) | POUND | 724 | 724 | |
| 40600290 | BITUMINOUS MATERIALS (TACK COAT) | POUND | 158 | 158 | |
| 40600982 | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT | SQ YD | 200 | 200 | |
| 40600990 | TEMPORARY RAMP | SQ YD | 29 | 29 | |
| 40603340 | HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 | TON | 30 | 30 | |
| 48102100 | AGGREGATE WEDGE SHOULDER, TYPE B | TON | 19 | 19 | |
| 48203029 | HOT-MIX ASPHALT SHOULDERS, 8" | SQ YD | 167 | 167 | |
| 50102400 | CONCRETE REMOVAL | CU YD | 6.2 | 6.2 | |
| 50300100 | FLOOR DRAINS | EACH | 6 | 6 | |
| 50300255 | CONCRETE SUPERSTRUCTURE | CU YD | 6.3 | 6.3 | |
| 50300260 | BRIDGE DECK GROOVING | SQ YD | 345 | 345 | |
| 50800205 | REINFORCEMENT BARS, EPOXY COATED | POUND | 4640 | 4640 | |

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | 100% STATE | |
|----------|---|--------|----------------|--------------|----------|
| | | | | CONSTR. CODE | |
| | | | | RURAL | BRIDGE |
| | | | | 0059 | 014-0031 |
| 50800515 | BAR SPLICERS | EACH | 109 | 109 | |
| 67000400 | ENGINEER'S FIELD OFFICE, TYPE A | CAL MO | 6 | 6 | |
| 67100100 | MOBILIZATION | L SUM | 1 | 1 | |
| 70100405 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701321 | EACH | 1 | 1 | |
| 70100450 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701201 | L SUM | 1 | 1 | |
| 70100460 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701306 | L SUM | 1 | 1 | |
| 70100500 | TRAFFIC CONTROL AND PROTECTION, STANDARD 701326 | L SUM | 1 | 1 | |
| 70103815 | TRAFFIC CONTROL SURVEILLANCE | CAL DA | 10 | 10 | |
| 70106500 | TEMPORARY BRIDGE TRAFFIC SIGNALS | EACH | 1 | 1 | |
| 70106700 | TEMPORARY RUMBLE STRIPS | EACH | 12 | 12 | |
| 70107005 | PAVEMENT MARKING BLACKOUT TAPE, 5" | FOOT | 745 | 745 | |
| 70300240 | TEMPORARY PAVEMENT MARKING - LINE 6" | FOOT | 691 | 691 | |
| 70300280 | TEMPORARY PAVEMENT MARKING - LINE 24" | FOOT | 48 | 48 | |
| 70300520 | PAVEMENT MARKING TAPE, TYPE III 4" | FOOT | 2000 | 2000 | |
| 70400100 | TEMPORARY CONCRETE BARRIER | FOOT | 346 | 346 | |

MODEL: #40DELMN.MXD
 FILE NAME: P:\1118\BID\NTEC\Illinois.gov\PIWDOT\Documents\DOT Offices\District 8\Projects\0876M03\CAD\Drawings\0876M03-Sub-plan.dgn

| | | |
|------------------------------|-------------------|-------------------|
| USER NAME = davidsonmw | DESIGNED - _____ | REVISED - _____ |
| DRAWN - _____ | REVISIONS - _____ | |
| PLOT SCALE = 100.0000' / in. | CHECKED - _____ | REVISIONS - _____ |
| PLOT DATE = 10/23/2018 | DATE - _____ | REVISIONS - _____ |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SCALE: N/A SHEET 1 OF 2 SHEETS STA. N/A TO STA. N/A

| | | | | |
|--------------------|-------------|---------|--------------|-----------|
| F.A.S. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1780 | 26BDR-BJR-1 | CLINTON | 27 | 3 |
| CONTRACT NO. 76M03 | | | REV. - MS | |

ILLINOIS FED. AID PROJECT

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| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | 100% STATE | |
|------------|---|-------|----------------|--------------|----------|
| | | | | CONSTR. CODE | |
| | | | | RURAL | BRIDGE |
| | | | | 0059 | 014-0031 |
| 70400200 | RELOCATE TEMPORARY CONCRETE BARRIER | FOOT | 346 | | 346 |
| 70600260 | IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3 | EACH | 2 | | 2 |
| 70600332 | IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE, NARROW), TEST LEVEL 3 | EACH | 2 | | 2 |
| * 78000200 | THERMOPLASTIC PAVEMENT MARKING - LINE 4" | FOOT | 180 | | 180 |
| * 78003110 | PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 4" | FOOT | 237 | | 237 |
| * 78100100 | RAISED REFLECTIVE PAVEMENT MARKER | EACH | 2 | | 2 |
| * 78100300 | REPLACEMENT REFLECTOR | EACH | 6 | | 6 |
| * 78200005 | GUARDRAIL REFLECTORS, TYPE A | EACH | 26 | | 26 |
| * 78200011 | BARRIER WALL REFLECTORS, TYPE C | EACH | 21 | | 21 |
| 78300200 | RAISED REFLECTIVE PAVEMENT MARKER REMOVAL | EACH | 2 | | 2 |
| X5030304 | CONCRETE WEARING SURFACE, 4" | SQ YD | 345 | | 345 |
| X5870015 | BRIDGE DECK CONCRETE SEALER | SQ FT | 4431 | | 4431 |
| X7030005 | TEMPORARY PAVEMENT MARKING REMOVAL | SQ FT | 667 | | 667 |
| X7040125 | PINNING TEMPORARY CONCRETE BARRIER | EACH | 58 | | 58 |

| CODE NO. | ITEM | UNIT | TOTAL QUANTITY | 100% STATE | |
|----------|--|-------|----------------|--------------|----------|
| | | | | CONSTR. CODE | |
| | | | | RURAL | BRIDGE |
| | | | | 0059 | 014-0031 |
| X7200200 | WIDE LOAD SIGNING | L SUM | 1 | | 1 |
| Z0012140 | BRIDGE DECK SCARIFICATION 2" | SQ YD | 345 | | 345 |
| Z0016002 | DECK SLAB REPAIR (FULL DEPTH, TYPE II) | SQ YD | 1.6 | | 1.6 |
| Z0016200 | DECK SLAB REPAIR (PARTIAL) | SQ YD | 94.1 | | 94.1 |

* SPECIALTY ITEM

REV. - MS

| | | |
|------------------------------|------------------|-----------------|
| USER NAME = davidsonmw | DESIGNED - _____ | REVISED - _____ |
| | DRAWN - _____ | REVISED - _____ |
| PLOT SCALE = 100.0000' / in. | CHECKED - _____ | REVISED - _____ |
| PLOT DATE = 10/23/2018 | DATE - _____ | REVISED - _____ |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES

SCALE: N/A SHEET 2 OF 2 SHEETS STA. N/A TO STA. N/A

| | | | | |
|--------------------|-------------|---------|---------------------------|-----------|
| F.A.S. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1780 | 26BDR-BJR-1 | CLINTON | 27 | 4 |
| CONTRACT NO. 76M03 | | | ILLINOIS FED. AID PROJECT | |

FOR STAGING TYPICAL, SEE SHEET NUMBER 17 OF 27 & 19 OF 27.

| SHOULDER SCHEDULE | | | | | | | | | |
|-------------------|----|-----------|-----------------------------|----------------------------------|--------------------------------------|---------------------------------|------------------------------|----------------------------------|-----|
| STATION | TO | STATION | EARTH EXCAVATION (WIDENING) | BITUMINOUS MATERIAL (PRIME COAT) | SUBBASE GRANULAR MATERIAL TYPE C, 8" | BITUMINOUS MATERIAL (TACK COAT) | HOT-MIX ASPHALT SHOULDER, 8" | AGGREGATE WEDGE SHOULDER, TYPE B | |
| | | | (CU YD) | (POUND) | (SQ YD) | (POUND) | (SQ YD) | (TON) | |
| 256+37.25 | LT | 257+62.25 | LT | 26 | 181 | 58 | 9 | 42 | 4.6 |
| 258+65.75 | LT | 259+90.75 | LT | 26 | 181 | 58 | 9 | 42 | 4.6 |
| 256+37.25 | RT | 257+62.25 | RT | 26 | 181 | 58 | 9 | 42 | 4.6 |
| 258+65.75 | RT | 259+90.75 | RT | 26 | 181 | 58 | 9 | 42 | 4.6 |
| TOTAL | | | | 103 | 724 | 231 | 38 | 167 | 19 |

| STAGING SCHEDULE | | | | | | | | | | |
|------------------|----------------------------|-------------------------------------|--|---|-------------------------------------|-------------------------------------|--------------------------------------|------------------------------------|------------------------------------|-------------------------|
| STAGE | TEMPORARY CONCRETE BARRIER | RELOCATE TEMPORARY CONCRETE BARRIER | IMPACT ATTENUATORS, TEMPORARY (FULLY REDIRECTIVE, NARROW) TEST LEVEL 3 | IMPACT ATTENUATORS, RELOCATE (FULLY REDIRECTIVE, NARROW) TEST LEVEL 3 | PAVEMENT MARKING TAPE, TYPE III, 4" | TEMPORARY PAVEMENT MARKING LINE, 6" | TEMPORARY PAVEMENT MARKING LINE, 24" | PAVEMENT MARKING BLACKOUT TAPE, 5" | TEMPORARY PAVEMENT MARKING REMOVAL | TEMPORARY RUMBLE STRIPS |
| | (FOOT) | (FOOT) | (EACH) | (EACH) | (FOOT) | (FOOT) | (FOOT) | (FOOT) | (SQ FT) | (EACH) |
| I | 346 | 0 | 2 | 0 | 1000 | 346 | 24 | 443 | 333 | 6 |
| II | 0 | 346 | 0 | 2 | 1000 | 346 | 24 | 303 | 333 | 6 |
| TOTAL | 346 | 346 | 2 | 2 | 2000 | 691 | 48 | 745 | 667 | 12 |

SUGGESTED SEQUENCE OF CONSTRUCTION

PRE-STAGE I:

REMOVE EXISTING AGG WEDGE SHOULDER.
 CONSTRUCT HMA SHOULDER, 8" WITH SUBBASE GRANULAR MATERIAL, TYPE C, 8".
 TRAFFIC CONTROL SHALL BE IN ACCORDANCE WITH TRAFFIC CONTROL AND PROTECTION, STANDARD 701326.
 WIDE LOAD SIGNING SHALL BE ERECTED PRIOR TO STAGE I CONSTRUCTION.

STAGE I:

SETUP TRAFFIC CONTROL AND PROTECTION, STANDARD 701321.
 SEE STANDARD 701321 FOR DETAILS NOT SHOWN ON PLANS.
 PERFORM ALL STRUCTURAL WORK FOR STAGE I. SEE STRUCTURE PLANS.
 PERFORM ANY ADDITIONAL/NECESSARY WORK FOR STAGE I CONSTRUCTION.

STAGE II:

RELOCATE TRAFFIC CONTROL AND PROTECTION, STANDARD 701321.
 SEE STANDARD 701321 FOR DETAILS NOT SHOWN ON PLANS.
 PERFORM ALL STRUCTURAL WORK FOR STAGE II. SEE STRUCTURE PLANS.
 PERFORM ANY ADDITIONAL/NECESSARY WORK FOR STAGE II CONSTRUCTION.

STAGE III & IV:

SETUP TRAFFIC CONTROL AND PROTECTION, STANDARD 701306.
 CONSTRUCT HMA SURFACE COURSE, 2".
 APPLY PAVEMENT MARKINGS UTILIZING TRAFFIC CONTROL AND PROTECTION, STANDARD 701311.
 PERFORM ANY ADDITIONAL WORK REQUIRED.

| RESURFACING SCHEDULE | | | |
|----------------------|---|--|--|
| STAGE | BITUMINOUS MATERIALS (TACK COAT) 0.05 LB/SF | HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT | HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70 |
| | (POUND) | (SQ YD) | (TON) |
| III | 60 | 100 | 15 |
| IV | 60 | 100 | 15 |
| TOTAL | 120 | 200 | 30 |

| PAVEMENT MARKING SCHEDULE | | | | | | | | | | |
|---------------------------|----|-----------|--|-------------|--|-------------|------------------------------------|------------------------|---------------------|--------|
| STATION | TO | STATION | PREFORMED PLASTIC PAVEMENT MARKING, TYPE B - LINE 4" | | THERMOPLASTIC PAVEMENT MARKING - LINE 4" | | RAISED REFLECTIVE PAVEMENT MARKERS | | | |
| | | | WHITE SOLID | YELLOW SKIP | WHITE SOLID | YELLOW SKIP | REMOVE REFLECTOR ONLY | REPLACE REFLECTOR ONLY | REPLACE ENTIRE RRPM | AMBER |
| | | | (FOOT) | (FOOT) | (FOOT) | (FOOT) | (EACH) | (EACH) | (EACH) | (EACH) |
| 254+67.25 | | 257+22.25 | | | | | 3 | 3 | | 3 |
| 257+22.25 | | 257+62.25 | | | 80 | 10 | 1 | | 1 | 1 |
| 257+62.25 | | 258+65.75 | 207 | 30 | | | | | | |
| 258+65.75 | | 259+05.75 | | | 80 | 10 | 1 | | 1 | 1 |
| 258+05.75 | | 261+60.75 | | | | | 3 | 3 | | 3 |
| TOTAL | | | 207 | 30 | 160 | 20 | 8 | 6 | 2 | 8 |

MODEL: \\MODELNAME
 FILE: \\NAME.PLOT
 PROJECT: \\PROJECTNAME
 USER: \\USER
 DATE: 11/7/2018

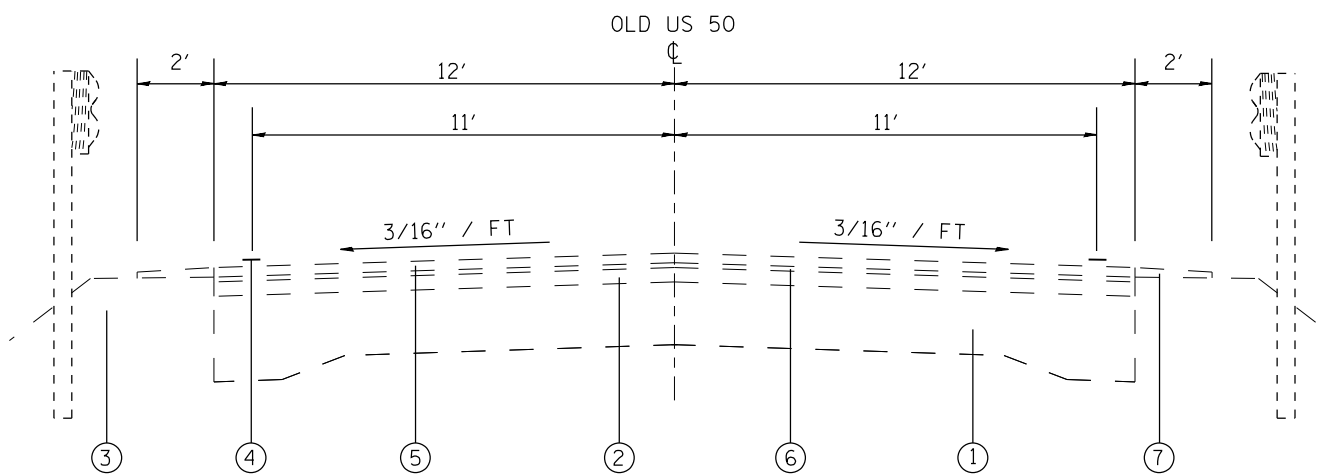
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|------------------------------|------------------|-----------------|
| USER NAME = davidsonmw | DESIGNED - _____ | REVISED - _____ |
| | DRAWN - _____ | REVISED - _____ |
| PLOT SCALE = 100.0086' / in. | CHECKED - _____ | REVISED - _____ |
| PLOT DATE = 11/7/2018 | DATE - _____ | REVISED - _____ |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

SCHEDULE OF QUANTITIES

SCALE: N/A SHEET 1 OF 1 SHEETS STA. N/A TO STA. N/A

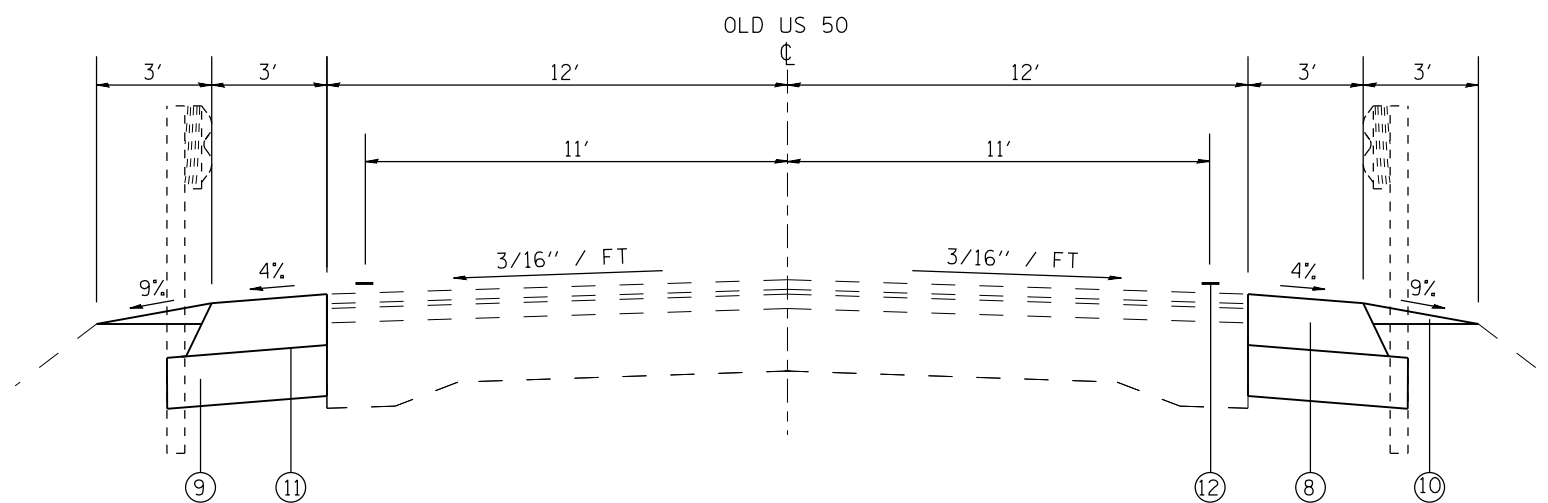
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|---------------------------|-------------|---------|--------------|-----------|
| F.A.S. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1780 | 26BDR-BJR-1 | CLINTON | 27 | 5 |
| CONTRACT NO. 76M03 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



EXISTING TYPICAL SECTION

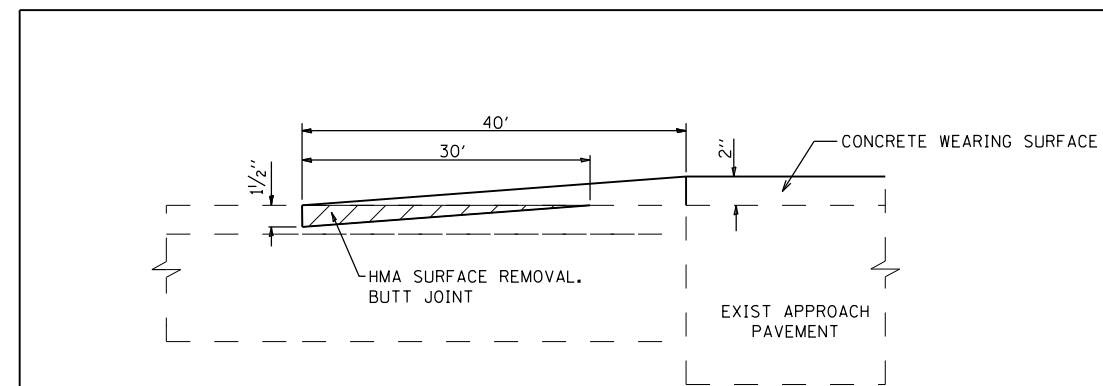
STA. 256+37.25 TO 257+62.25
 STA. 258+65.75 TO 259+90.75

- LEGEND**
- ① EXISTING PAVEMENT
 - ② EXISTING RESURFACING
 - ③ EXISTING TURF SHOULDER
 - ④ EXISTING PAVEMENT MARKINGS
 - ⑤ EXISTING SURFACE COURSE
 - ⑥ EXISTING LEVELING BINDER
 - ⑦ EXISTING AGG WEDGE SHOULDER, TYPE B
 - ⑧ PROPOSED HOT-MIXED APHALT SHOULDER, 8"
 - ⑨ PROPOSED SUBBASE GRANULAR MATERIAL, TYPE C, 8"
 - ⑩ PROPOSED AGG WEDGE SHOULDER, TYPE B
 - ⑪ PROPOSED BITUMINOUS MATERIAL (PRIME COAT)
 - ⑫ PROPOSED THERMOPLASTIC PAVEMENT MARKING, 4"



PROPOSED TYPICAL SECTION

STA. 256+37.25 TO 257+62.25
 STA. 258+65.75 TO 259+90.75



BUTT JOINT DETAIL

NOTE: THE BUTT JOINT EXTENDS ACROSS THE WIDENING AS WELL AS THE PAVEMENT.

MODEL: \\MODELNAME
 FILE NAME: \\FILENAME
 PROJECT: \\PROJECT
 DRAWING: \\DRAWING
 DATE: \\DATE

| | | |
|------------------------------|-------------------|-----------------|
| USER NAME = davidsonmw | DESIGNED - _____ | REVISED - _____ |
| DRAWN - _____ | REVISIONS - _____ | |
| PLOT SCALE = 100.0043' / in. | CHECKED - _____ | REVISED - _____ |
| PLOT DATE = 10/12/2018 | DATE - _____ | REVISED - _____ |

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

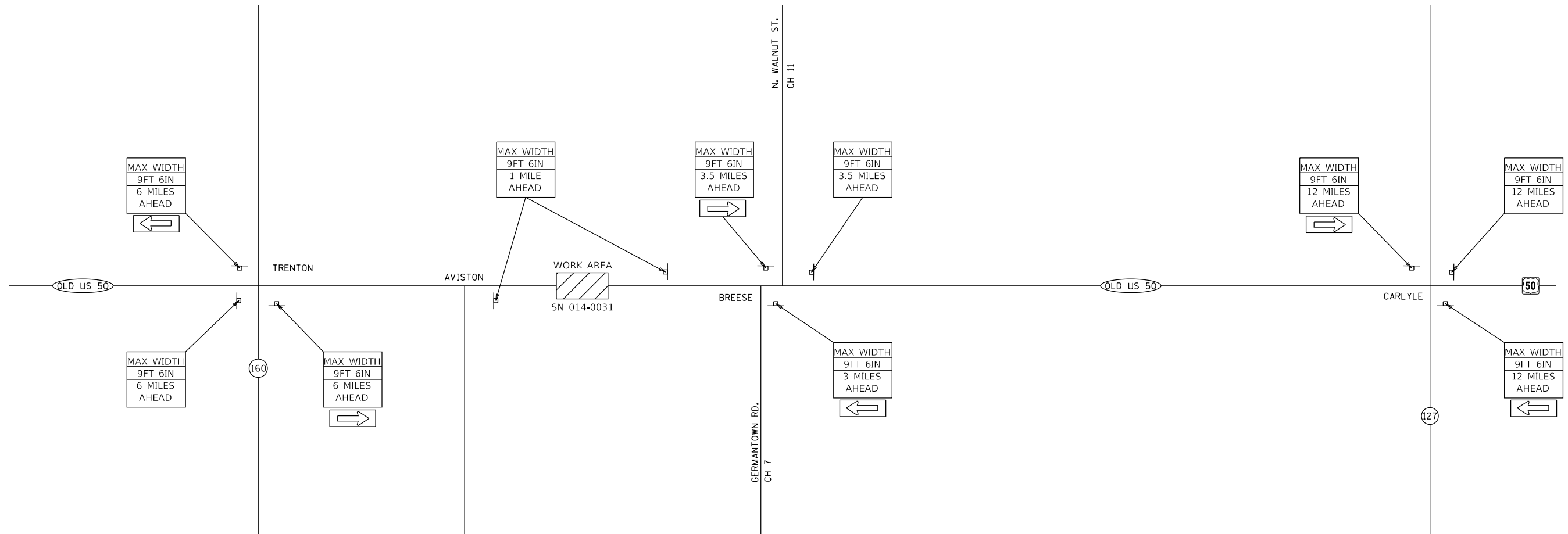
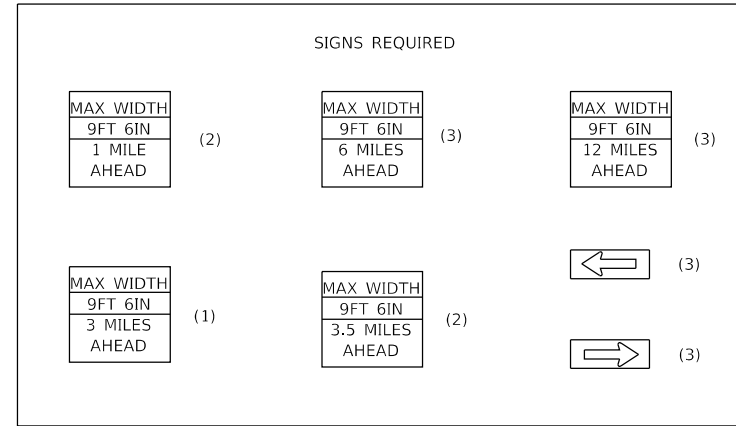
**TYPICAL SECTIONS &
 BUTT JOINT DETAIL**

SCALE: N/A SHEET 1 OF 1 SHEETS STA. N/A TO STA. N/A

| F.A.S. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-------------|---------|--------------------|-----------|
| 1780 | 26BDR-BJR-1 | CLINTON | 27 | 6 |
| | | | CONTRACT NO. 76M03 | |
| ILLINOIS FED. AID PROJECT | | | | |

NOTES:

1. ALL SIGNS REQUIRED ON THIS SHEET WILL BE SUPPLIED TO THE CONTRACTOR BY I.D.O.T.
2. THE CONTRACTOR SHALL FURNISH THE POSTS AND ERECT SIGNS AT THE LOCATIONS SHOWN ON THIS SHEET, AS DIRECTED BY R.E./R.T. THE POSTS SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.
3. THE CONTRACTOR SHALL GIVE ILLINOIS DEPARTMENT OF TRANSPORTATION, BUREAU OF OPERATIONS TWO WEEKS NOTICE FOR SIGNS. THE CONTRACTOR SHALL PICK UP THE SIGNS AT THE T.M. BUILDING IN FAIRVIEW HEIGHTS, AND SHALL RETURN THEM UPON COMPLETION OF THE CONTRACT. CONTACT JEAN SLAPE AT (618)394-2189.
4. THE ABOVE NOTED WORK SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE, LUMP SUM, FOR WIDE LOAD SIGNING AND NO OTHER COMPENSATION WILL BE ALLOWED.
5. SIGN SPACING WILL BE 400' OR TO FIT FIELD CONDITIONS.
6. THE HEIGHT TO THE BOTTOM OF THE LOWEST SIGN SHALL NOT BE LESS THAN 6'.



MODEL: \\MODELNAME
 FILE NAME: \\FILENAME
 PROJECT: \\PROJECTNAME
 DRAWING: \\DRAWINGNAME
 DATE: \\DATE
 SCALE: \\SCALE
 SHEET: \\SHEETNO
 TOTAL SHEETS: \\TOTALSHEETS
 CONTRACT NO.: \\CONTRACTNO

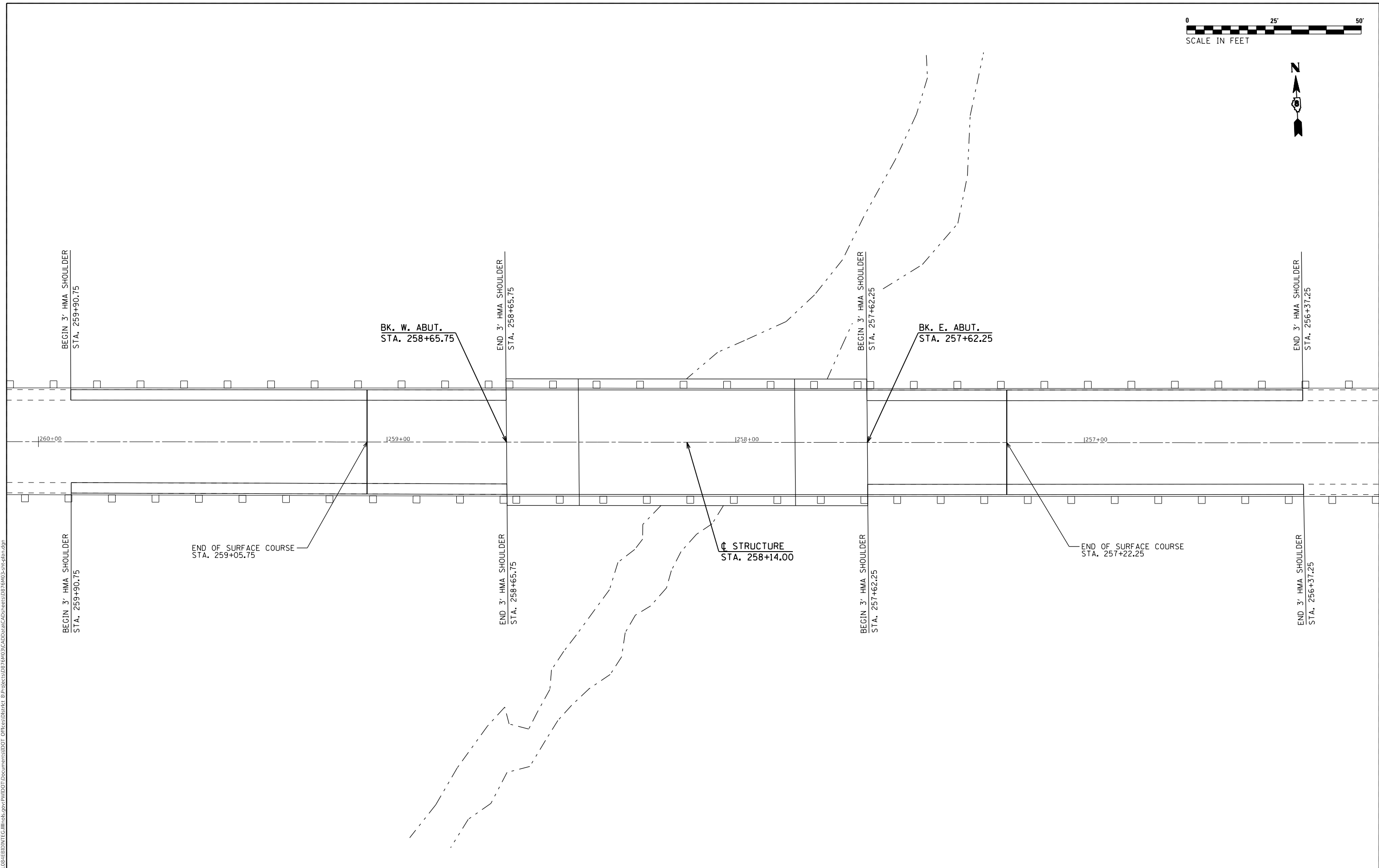
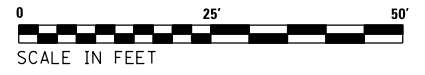
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|------------------------------|-------------------|-------------------|
| USER NAME = davidsonmw | DESIGNED - _____ | REVISED - _____ |
| DRAWN - _____ | REVISIONS - _____ | REVISIONS - _____ |
| PLOT SCALE = 100.0000' / in. | CHECKED - _____ | REVISIONS - _____ |
| PLOT DATE = 10/12/2018 | DATE - _____ | REVISIONS - _____ |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

WIDE LOAD SIGNING

SCALE: N/A SHEET 1 OF 1 SHEETS STA. N/A TO STA. N/A

| F.A.S. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-------------|---------|--------------|-----------|
| 1780 | 26BDR-BJR-1 | CLINTON | 27 | 7 |
| CONTRACT NO. 76M03 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



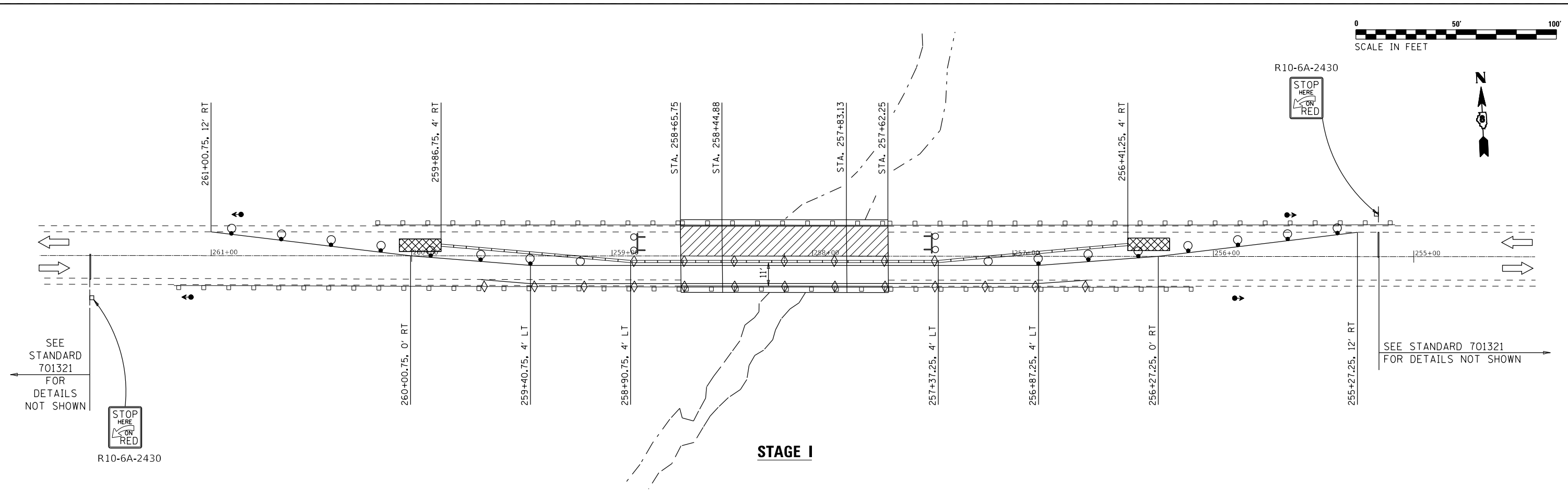
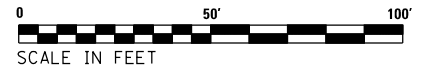
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| | | |
|------------------------------|------------------|-----------------|
| USER NAME = davidsonmw | DESIGNED - _____ | REVISED - _____ |
| | DRAWN - _____ | REVISED - _____ |
| PLOT SCALE = 100.0000' / in. | CHECKED - _____ | REVISED - _____ |
| PLOT DATE = 10/12/2018 | DATE - _____ | REVISED - _____ |

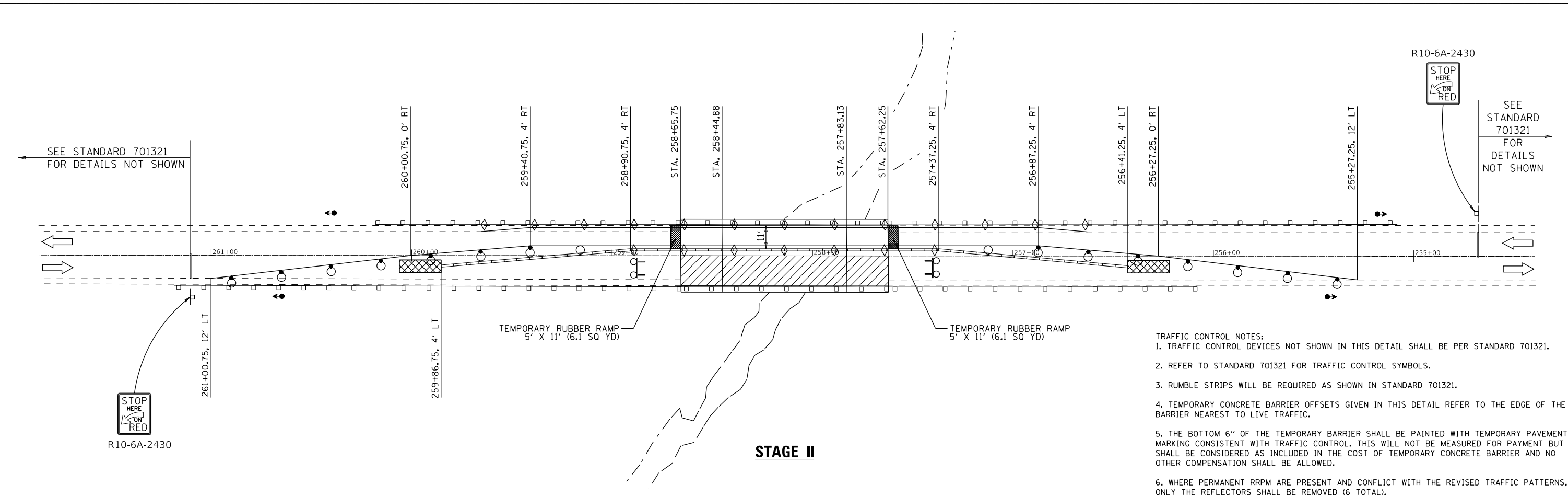
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

| | | | |
|----------------------|---------|-------------|--------------|
| STAGING PLANS | | | |
| SCALE: | SHEET 1 | OF 3 SHEETS | STA. TO STA. |

| | | | | |
|--------------------|-------------|---------|---------------------------|-----------|
| F.A.S. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1780 | 26BDR-BJR-1 | CLINTON | 27 | 8 |
| CONTRACT NO. 76M03 | | | ILLINOIS FED. AID PROJECT | |



STAGE I



STAGE II

- TRAFFIC CONTROL NOTES:**
1. TRAFFIC CONTROL DEVICES NOT SHOWN IN THIS DETAIL SHALL BE PER STANDARD 701321.
 2. REFER TO STANDARD 701321 FOR TRAFFIC CONTROL SYMBOLS.
 3. RUMBLE STRIPS WILL BE REQUIRED AS SHOWN IN STANDARD 701321.
 4. TEMPORARY CONCRETE BARRIER OFFSETS GIVEN IN THIS DETAIL REFER TO THE EDGE OF THE BARRIER NEAREST TO LIVE TRAFFIC.
 5. THE BOTTOM 6" OF THE TEMPORARY BARRIER SHALL BE PAINTED WITH TEMPORARY PAVEMENT MARKING CONSISTENT WITH TRAFFIC CONTROL. THIS WILL NOT BE MEASURED FOR PAYMENT BUT SHALL BE CONSIDERED AS INCLUDED IN THE COST OF TEMPORARY CONCRETE BARRIER AND NO OTHER COMPENSATION SHALL BE ALLOWED.
 6. WHERE PERMANENT RRRP ARE PRESENT AND CONFLICT WITH THE REVISED TRAFFIC PATTERNS, ONLY THE REFLECTORS SHALL BE REMOVED (6 TOTAL).

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 FILE NAME: \\FILENAME
 PROJECT: \\PROJECT
 OFFICE: \\OFFICE
 DRAWING: \\DRAWING
 DATE: \\DATE

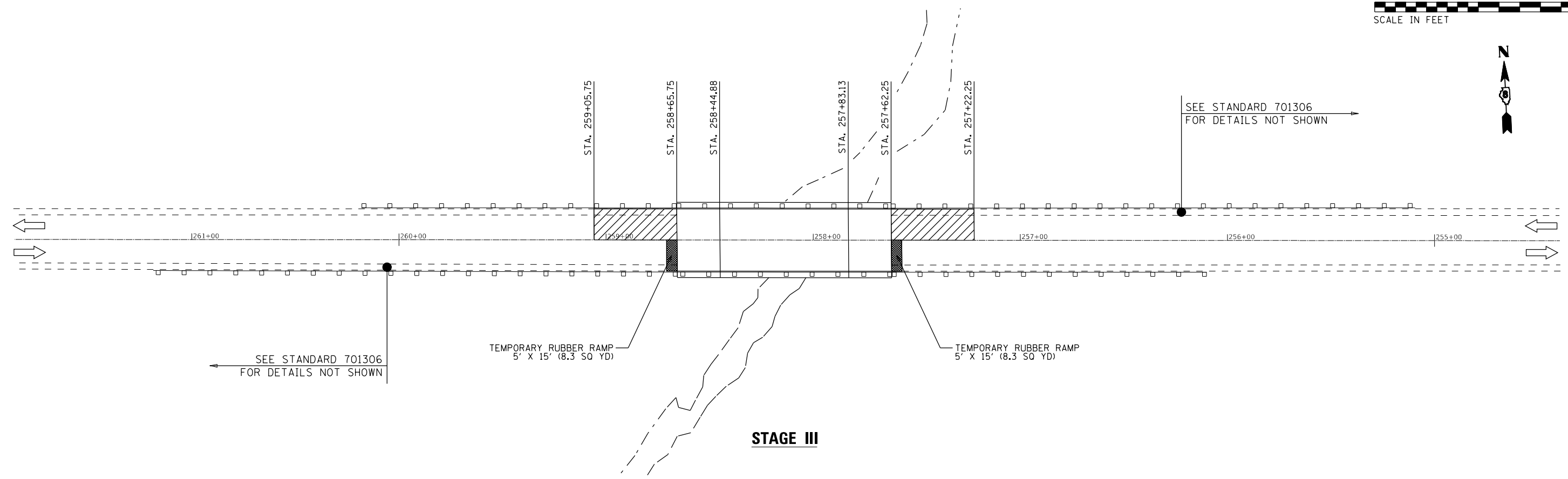
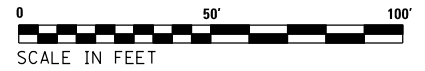
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| DRAWN - _____ | REVISIONS - _____ | REVISIONS - _____ |
| PLOT SCALE = 100,0000' / in. | CHECKED - _____ | REVISIONS - _____ |
| PLOT DATE = 11/8/2018 | DATE - _____ | REVISIONS - _____ |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

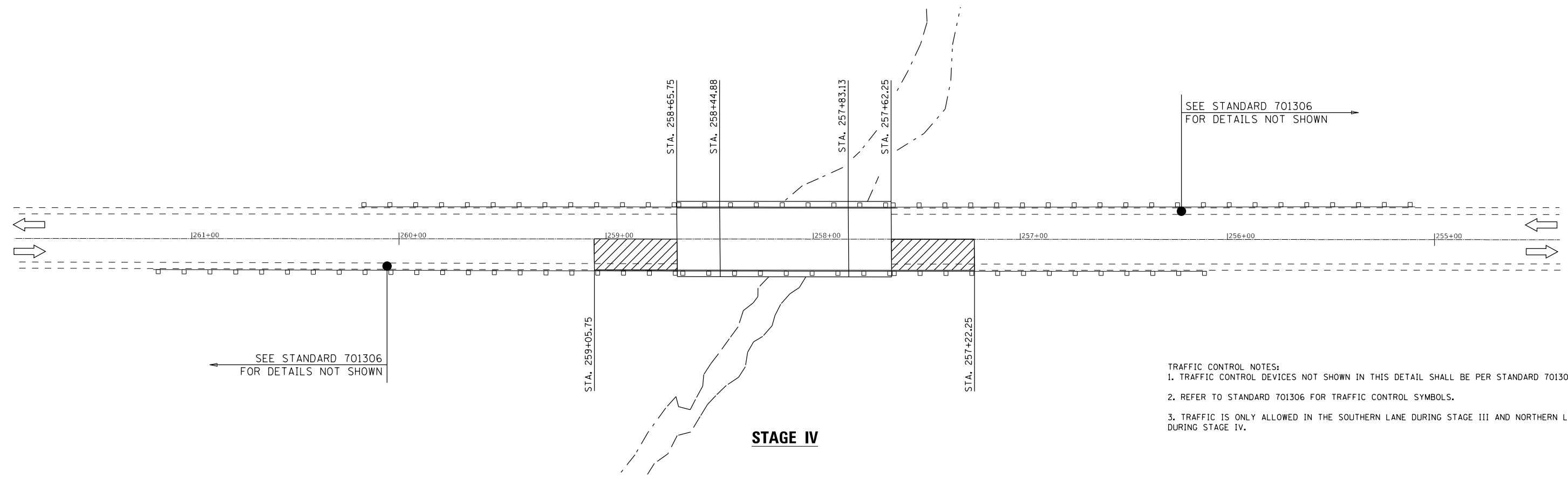
STAGING PLANS

SCALE: SHEET 2 OF 3 SHEETS STA. TO STA.

| F.A.S. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-------------|---------|--------------|-----------|
| 1780 | 26BDR-BJR-1 | CLINTON | 27 | 9 |
| CONTRACT NO. 76M03 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



STAGE III



STAGE IV

- TRAFFIC CONTROL NOTES:**
1. TRAFFIC CONTROL DEVICES NOT SHOWN IN THIS DETAIL SHALL BE PER STANDARD 701306.
 2. REFER TO STANDARD 701306 FOR TRAFFIC CONTROL SYMBOLS.
 3. TRAFFIC IS ONLY ALLOWED IN THE SOUTHERN LANE DURING STAGE III AND NORTHERN LANE DURING STAGE IV.

MODEL: \\MODELNAME
 FILE NAME: \\FILENAME
 PROJECT: \\PROJECT
 OFFICE: \\OFFICE
 DRAWN: \\DRAWN
 CHECKED: \\CHECKED
 DATE: \\DATE

| | | |
|------------------------------|-------------------|-------------------|
| USER NAME = davidsonmw | DESIGNED - _____ | REVISED - _____ |
| DRAWN - _____ | REVISIONS - _____ | REVISIONS - _____ |
| PLOT SCALE = 100.0000' / in. | CHECKED - _____ | REVISIONS - _____ |
| PLOT DATE = 11/8/2018 | DATE - _____ | REVISIONS - _____ |

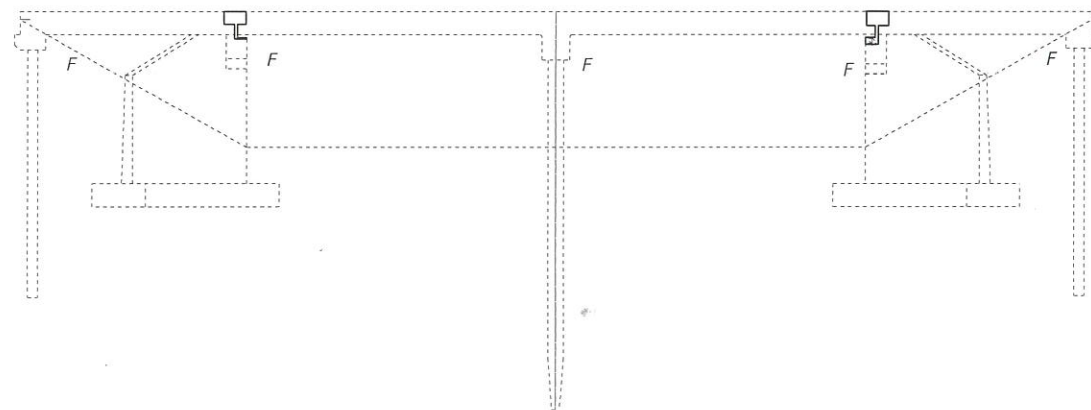
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STAGING PLANS

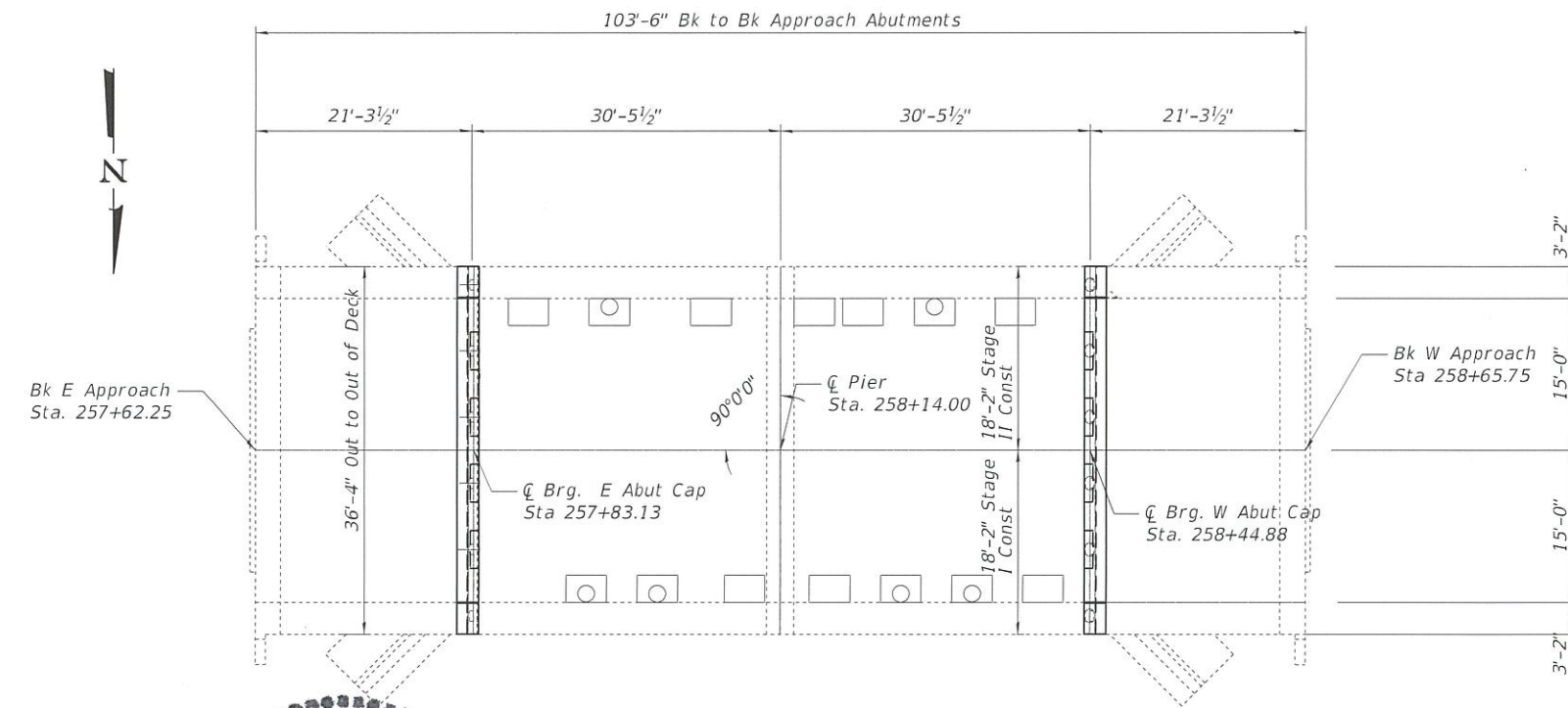
SCALE: SHEET 3 OF 3 SHEETS STA. TO STA.

| F.A.S. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-------------|---------|--------------|-----------|
| 1780 | 26BDR-BJR-1 | CLINTON | 27 | 10 |
| CONTRACT NO. 76M03 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

Existing Structure: SN 014-0031 was built in 1922. In 1958, the 1 span superstructure was removed and replaced with a 4 span slab superstructure under SBI Rt 12. A center pier and Bk approach bents were added. Also: in 1996, the HMA was removed and replaced with microsilica under a Day Labor project. A steel bridge rail (Curb Mounted-2399) was also added.



ELEVATION



PLAN



Expires 11/30/20

GENERAL NOTES

Reinforcement bars designated (E) shall be epoxy coated.

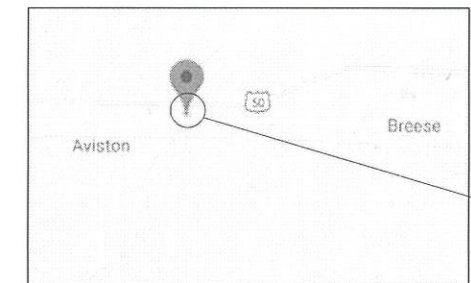
Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Existing reinforcement bars extending into removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal operations shall be repaired or replaced using an approved bar splicer or anchorage system. Cost included with "Concrete Removal".

Concrete sealer shall be placed on existing concrete on top/inside faces of curbs (full length) and the top/bottom/inside faces of the concrete bridge rail (full length). Cost included in "Bridge Deck Concrete Sealer".

Protective Coat shall be placed on the new concrete wearing surface & the new concrete on top/inside faces of the curbs. Cost included in "Bridge Deck Concrete Sealer".

The concrete for Deck Slab Repair (Partial) shall be placed with the "Concrete Wearing Surface, 4".



LOCATION SKETCH

FLOOR DRAIN REPLACEMENT



(6 Req'd)

Existing drains & adjacent concrete to be removed and replaced with 6" circular drains and full depth patch. (See sheet 14 of 17 for details)

FLOOR DRAIN ELIMINATION



(8 Req'd)

Existing drains & adjacent concrete to be removed and replaced with full depth patch. (See sheet 14 of 17 for details)

SCOPE OF WORK

- Remove slab ends in zig-zag pattern as shown in plans and replace solid.
- Remove 2" of microsilica and replace with 4" minimum Concrete Wearing Surface (CWS)
- Eliminate formed deck drains and replace remaining ones with round floor drains.

DESIGN STRESSES

FIELD UNITS
 $f'c = 4,000 \text{ psi}$
 $f_y = 60,000 \text{ psi (Reinforcement)}$

SHEET INDEX

- 1) General Plan and Elevation
- 2) Ex X-Sec (E Abut)
- 3) Ex Plan View (E Abut)
- 4) Ex X-Sec (W. Abut)
- 5) Ex Plan View (W Abut)
- 6) Ex Jt Rem
- 7) Prop X-Sec (E Abut)
- 8) Prop Plan View (E Abut)
- 9) Prop X-Sec (W Abut)
- 10) Prop Plan View (W Abut)
- 11) Prop Jt Details
- 12) Conc Wearing Surface Plan
- 13) Concrete Bridge Rail & Post Removal
- 14) Drain Details
- 15) Temporary Concrete Barrier
- 16) Bar Splicers
- 17) For Information Only- Ex Conc Bridge Rail & Post

TOTAL BILL OF MATERIAL

| ITEM | UNIT | TOTAL |
|---|---------|-------|
| Concrete Removal | Cu. Yd. | 6.2 |
| Concrete Superstructure | Cu. Yd. | 6.3 |
| * Bridge Deck Grooving | Sq. Yd. | 345 |
| Reinforcement Bars, Epoxy Coated | Pound | 4640 |
| ** Deck Slab Repair (Full Depth, Type II) | Sq. Yd. | 1.6 |
| ** Deck Slab Repair (Partial) | Sq. Yd. | 94.1 |
| * Concrete Wearing Surface, 4" | Sq. Yd. | 345 |
| * Bridge Deck Concrete Sealer | Sq. Ft. | 4431 |
| * Bridge Deck Scarification 2" | Sq. Yd. | 345 |
| Bar Splicers | Each | 109 |
| Floor Drains | Each | 6 |

* Including bridge & approach spans

** The quantity of deck slab repair is estimated. The engineer in the field shall determine the actual quantity and locations.

| |
|----------------|
| DESIGNED - AYV |
| CHECKED - JSB |
| DRAWN - AYV |
| CHECKED - JSB |

DATE - 11/29/2018
 PASSED
 [Signature]
 ENGINEER OF BRIDGES AND STRUCTURES

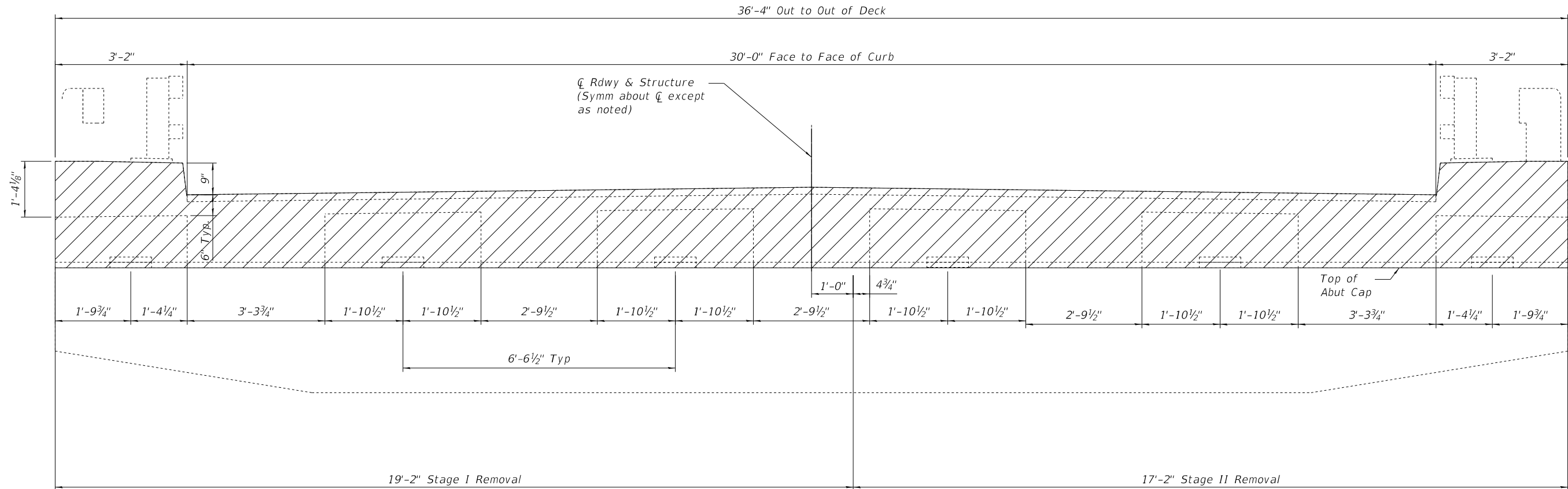
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| REVISIONS |
| REVISIONS |

STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

GENERAL PLAN & ELEVATION (Old US 50 over Lake Branch)
 SN 014-0031

SHEET NO. 1 OF 17 SHEETS

| | | | | |
|---------------------------|---------------------|----------------|--------------------|--------------|
| F.A.S. RTE. 1780 | SECTION 26BDR-BJR-1 | COUNTY CLINTON | TOTAL SHEETS 27 | SHEET NO. 11 |
| | | | CONTRACT NO. 76M03 | |
| ILLINOIS FED. AID PROJECT | | | | |



EXISTING CROSS SECTION
(Looking East)

11/27/2018 11:49:01 AM

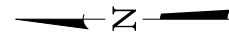
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

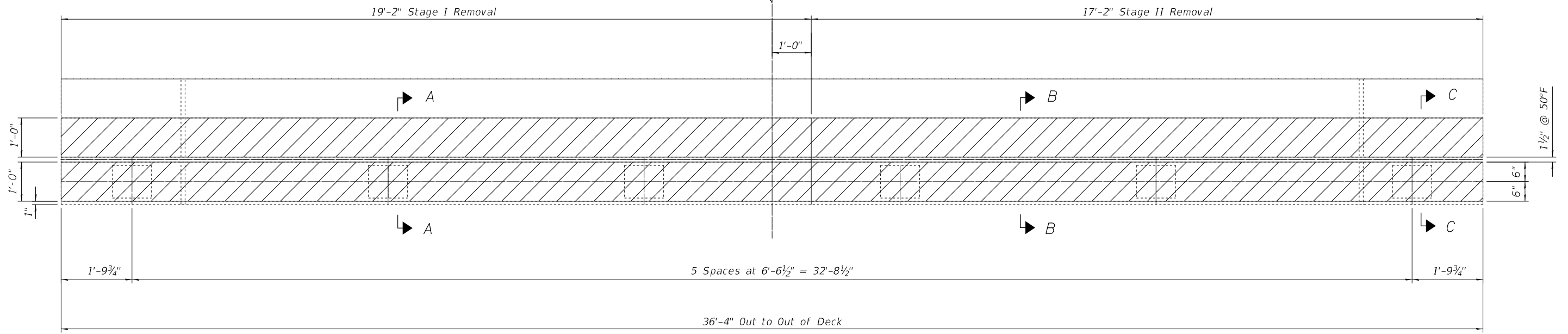
EXISTING CROSS SECTION (E Abut)
SN 014-0031

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|--------------------|-------------|---------|---------------------------|-----------|
| F.A.S. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1780 | 26BDR-BJR-1 | CLINTON | 27 | 12 |
| CONTRACT NO. 76M03 | | | ILLINOIS FED. AID PROJECT | |

PASSED _____
ENGINEER OF BRIDGES AND STRUCTURES



℄ Rdwy & Structure
(Symm about ℄ except
as noted)



EXISTING PLAN

Note:
See sheet 6 of 17 for Bill Of Material
& Sections A-A, B-B & C-C.

11/27/2018 11:49:01 AM

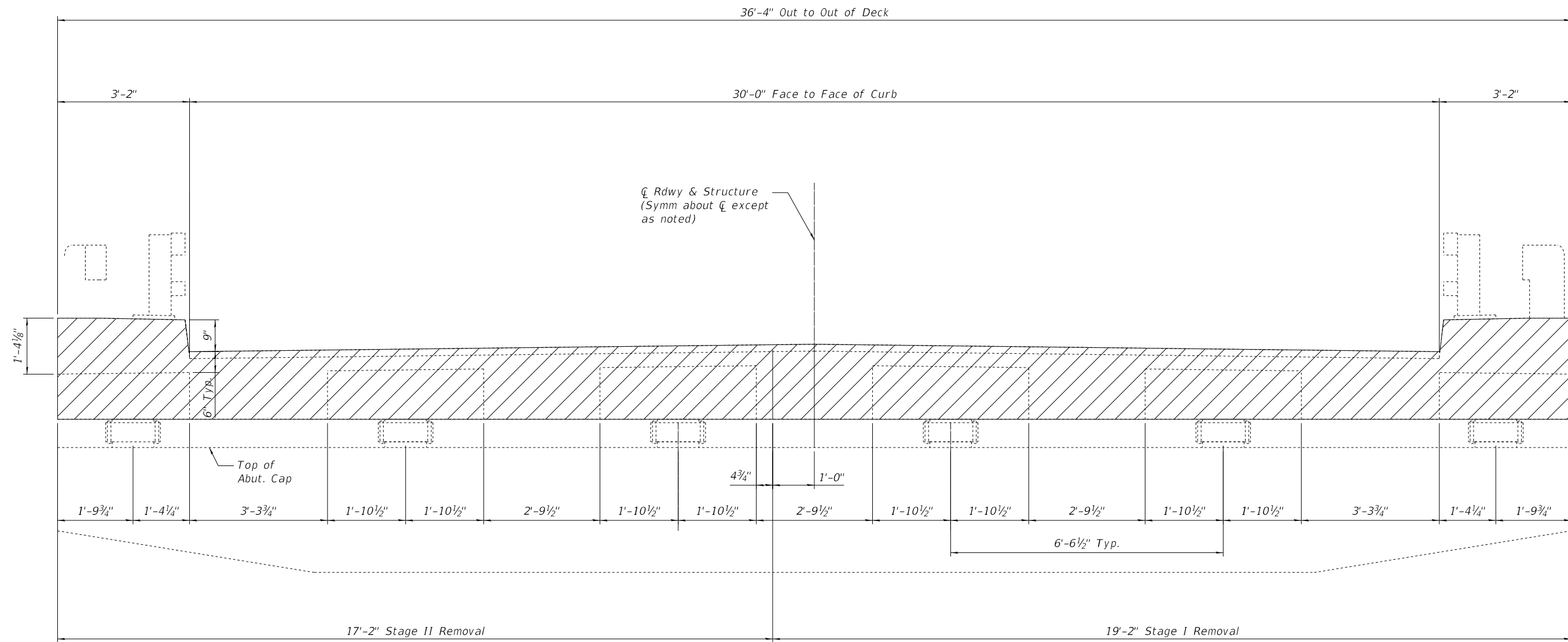
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| CHECKED - JSB | |
| DRAWN - AYV | REVIS |
| CHECKED - JSB | REVIS |
| PASSED | ENGINEER OF BRIDGES AND STRUCTURES |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EXISTING PLAN VIEW (E Abut)
SN 014-0031**

SHEET NO. 3 OF 17 SHEETS

| F.A.S. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|-------------|---------|---------------------------|-----------|
| 1780 | 26BDR-BJR-1 | CLINTON | 27 | 13 |
| CONTRACT NO. 76M03 | | | ILLINOIS FED. AID PROJECT | |



EXISTING CROSS SECTION

(Looking West)

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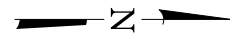
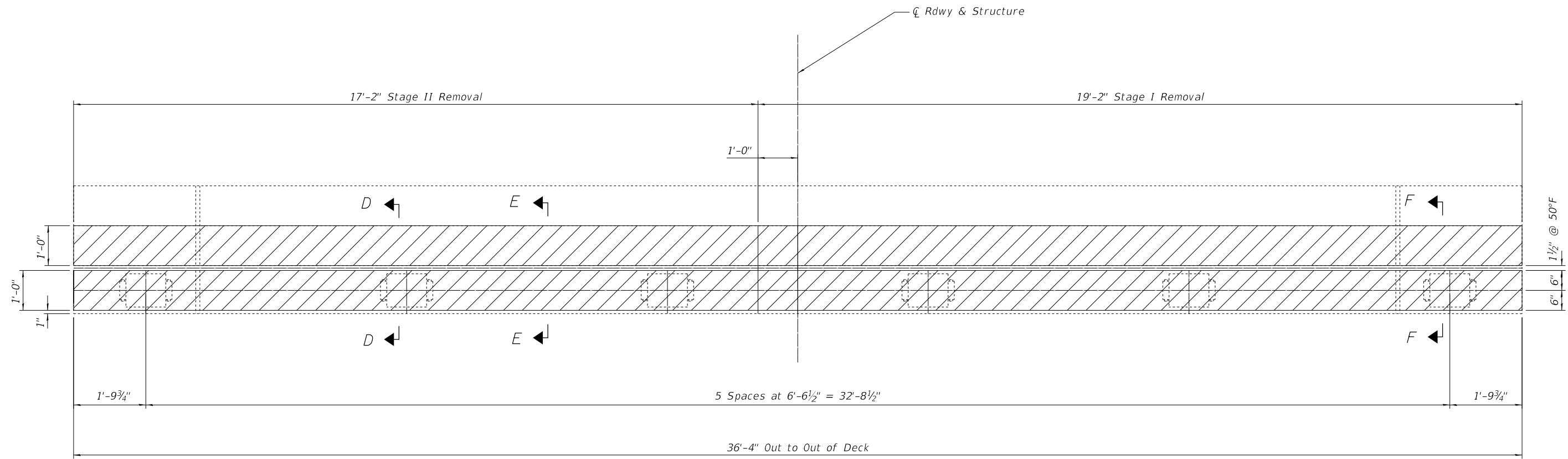
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| CHECKED - JSB | |
| DRAWN - AYV | REVISOR |
| CHECKED - JSB | REVISION |
| PASSED | ENGINEER OF BRIDGES AND STRUCTURES |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EXISTING CROSS SECTION (W. Abut)
SN 014-0031**

SHEET NO. 4 OF 17 SHEETS

| F.A.S. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-------------|---------|--------------------|-----------|
| 1780 | 26BDR-BJR-1 | CLINTON | 27 | 14 |
| ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 76M03 | |



EXISTING PLAN

Note:
See sheet 6 of 17 for Bill
of Material & Sections D-D, E-E & F-F

11/27/2018 11:49:02 AM

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|------------------------------------|--------|---------|
| DESIGNED - AYV | PASSED | DATE - |
| CHECKED - JSB | | REVISOR |
| DRAWN - AYV | | REVISOR |
| CHECKED - JSB | | REVISOR |
| ENGINEER OF BRIDGES AND STRUCTURES | | |

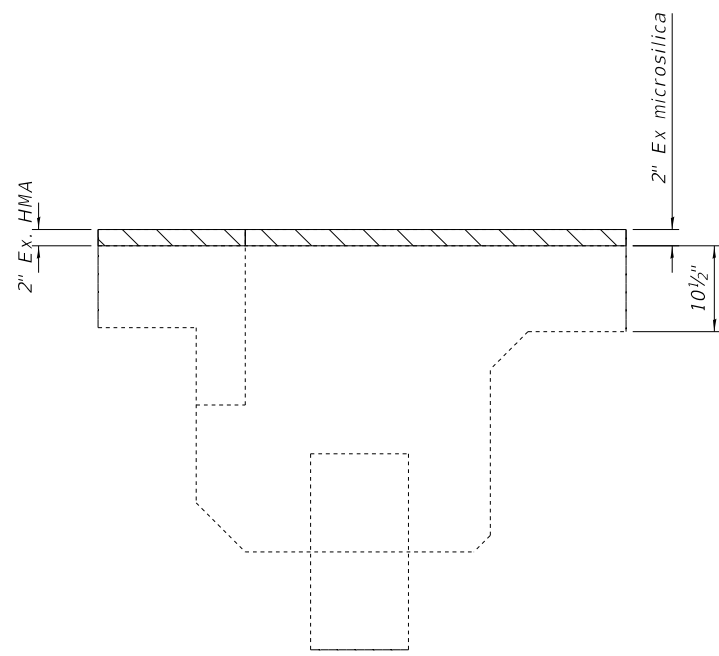
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EXISTING PLAN VIEW (W. Abut)
SN 014-0031**

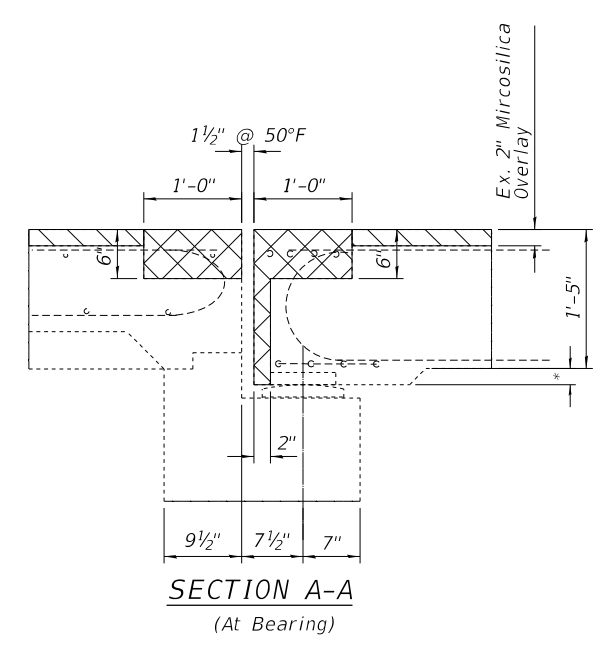
SHEET NO. 5 OF 17 SHEETS

| F.A.S. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-------------|---------|--------------------|-----------|
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| ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 76M03 | |

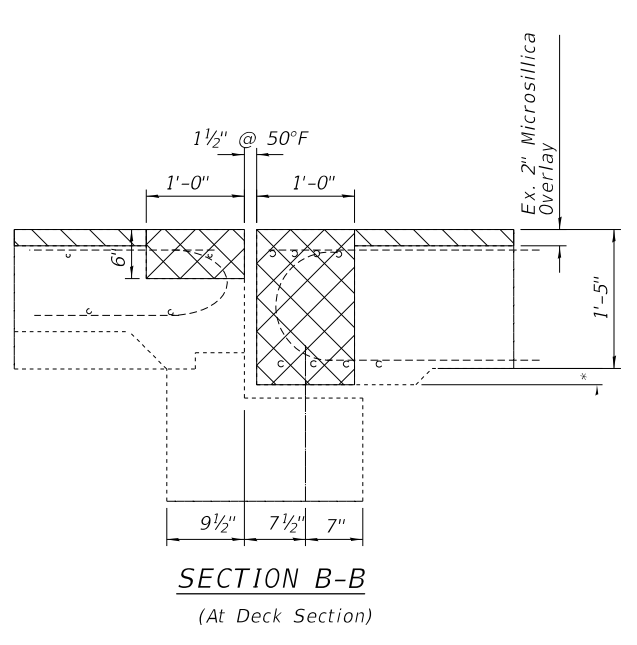
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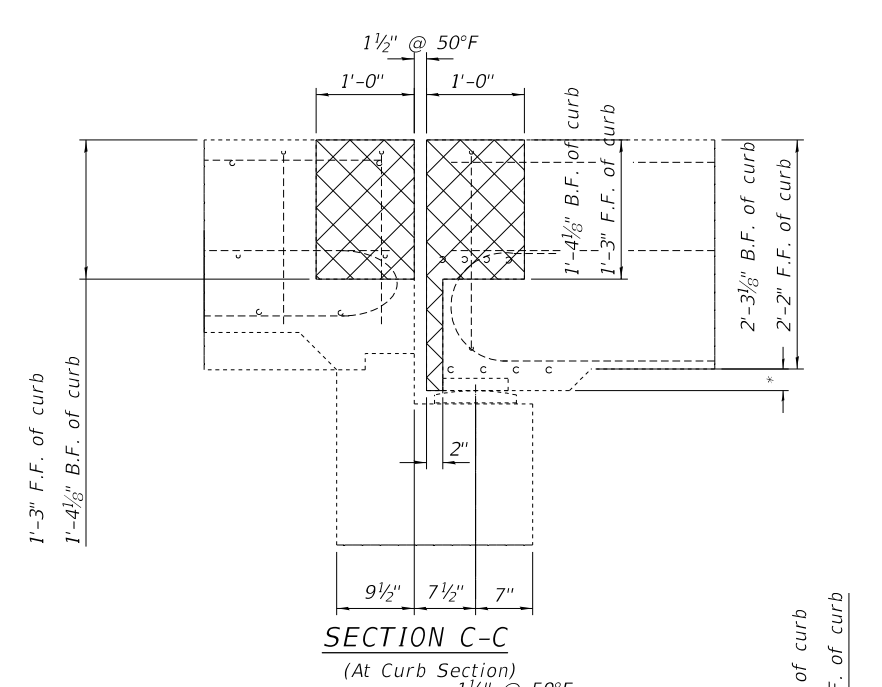
EXISTING APPROACH BENT



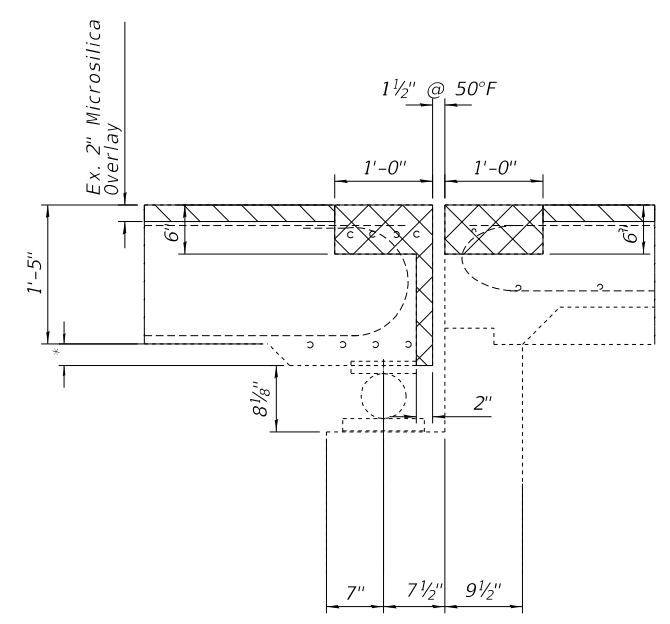
SECTION A-A
(At Bearing)



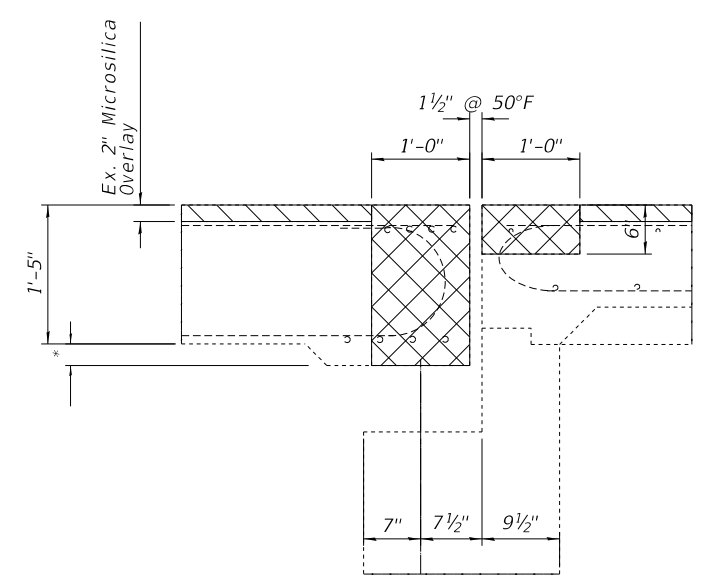
SECTION B-B
(At Deck Section)



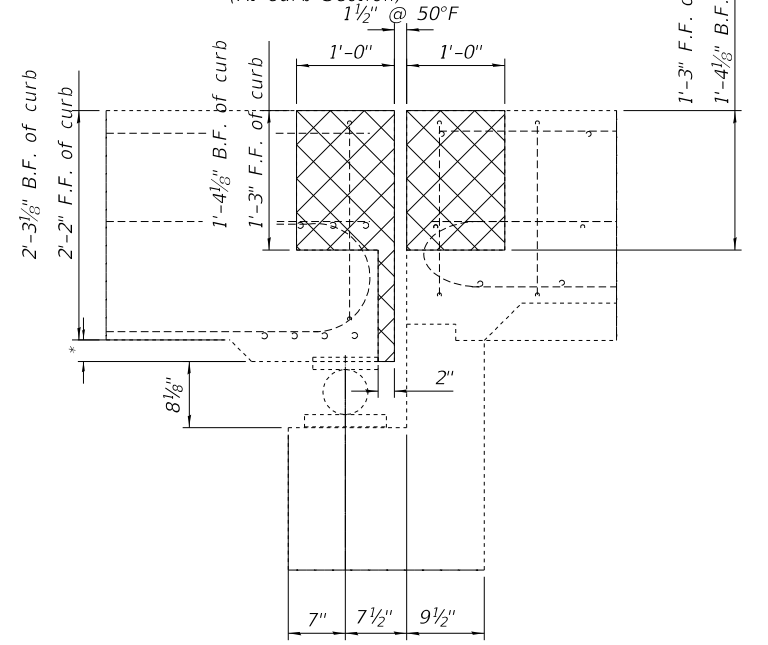
SECTION C-C
(At Curb Section)



SECTION D-D
(At Bearing)



SECTION E-E
(At Deck Section)



SECTION F-F
(At Curb Section)

Cross-hatched areas indicate concrete removal.

* Varies 2" to 4 5/8"

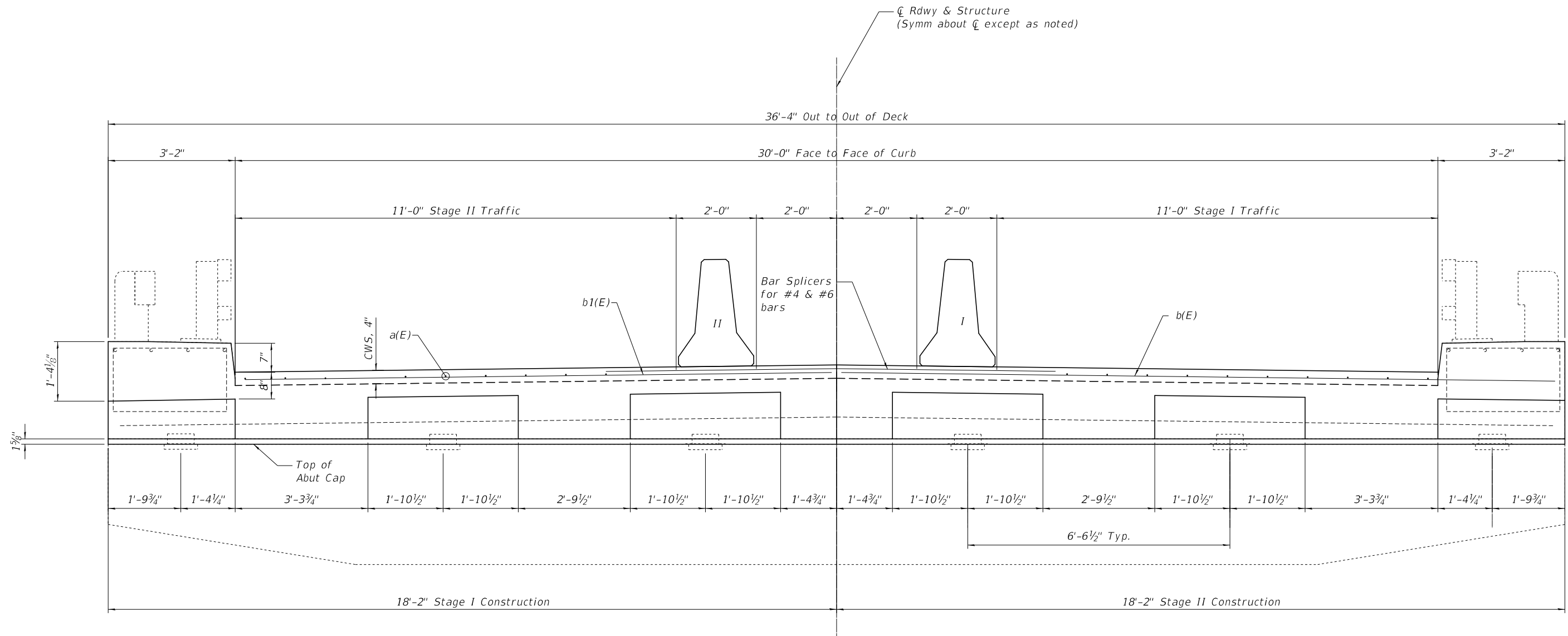
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| CHECKED - JSB | | REVIS |
| DRAWN - AYV | | REVIS |
| CHECKED - JSB | | REVIS |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**EXISTING JOINT REMOVAL DETAILS
SN 014-0031**

SHEET NO. 6 OF 17 SHEETS

| F.A.S. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-------------|---------|--------------------|-----------|
| 1780 | 26BDR-BJR-1 | CLINTON | 27 | 16 |
| | | | CONTRACT NO. 76M03 | |
| ILLINOIS FED. AID PROJECT | | | | |



CROSS SECTION
(Looking East)

11/27/2018 11:49:03 AM

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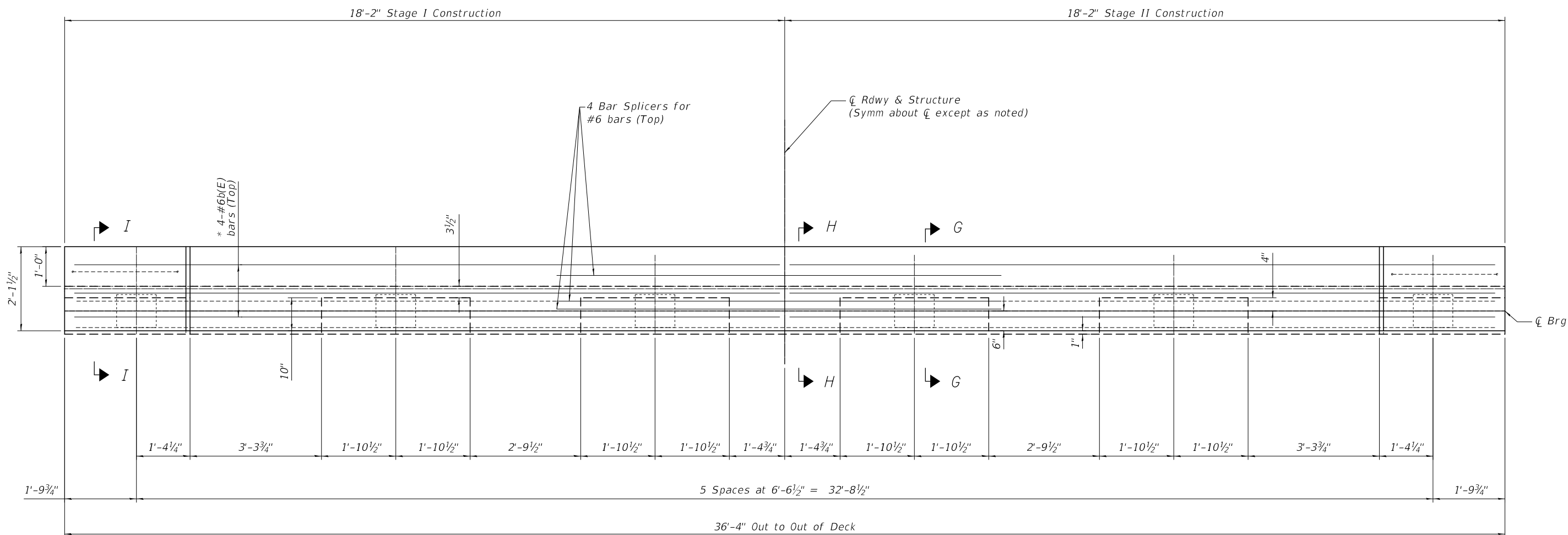
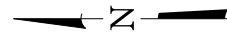
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED X-SEC (E. Abut.)
SN 014-0031

SHEET NO. 7 OF 17 SHEETS

| | | | | |
|--------------------|-------------|---------|---------------------------|--------------|
| F.A.S RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1780 | 26BDR-BJR-1 | CLINTON | 27 | 17 |
| CONTRACT NO. 76M03 | | | ILLINOIS FED. AID PROJECT | |

PASSED
ENGINEER OF BRIDGES AND STRUCTURES



PLAN

Note:
See sheet 11 of 17 for Bill of Material & Sections G-G, H-H & I-I.

* See Cross Sections on sheet 11 of 17 for placement.

11/27/2018 11:49:03 AM

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| DESIGNED - AYV | DATE - _____ |
| CHECKED - JSB | REVISOR _____ |
| DRAWN - AYV | REVISOR _____ |
| CHECKED - JSB | REVISOR _____ |

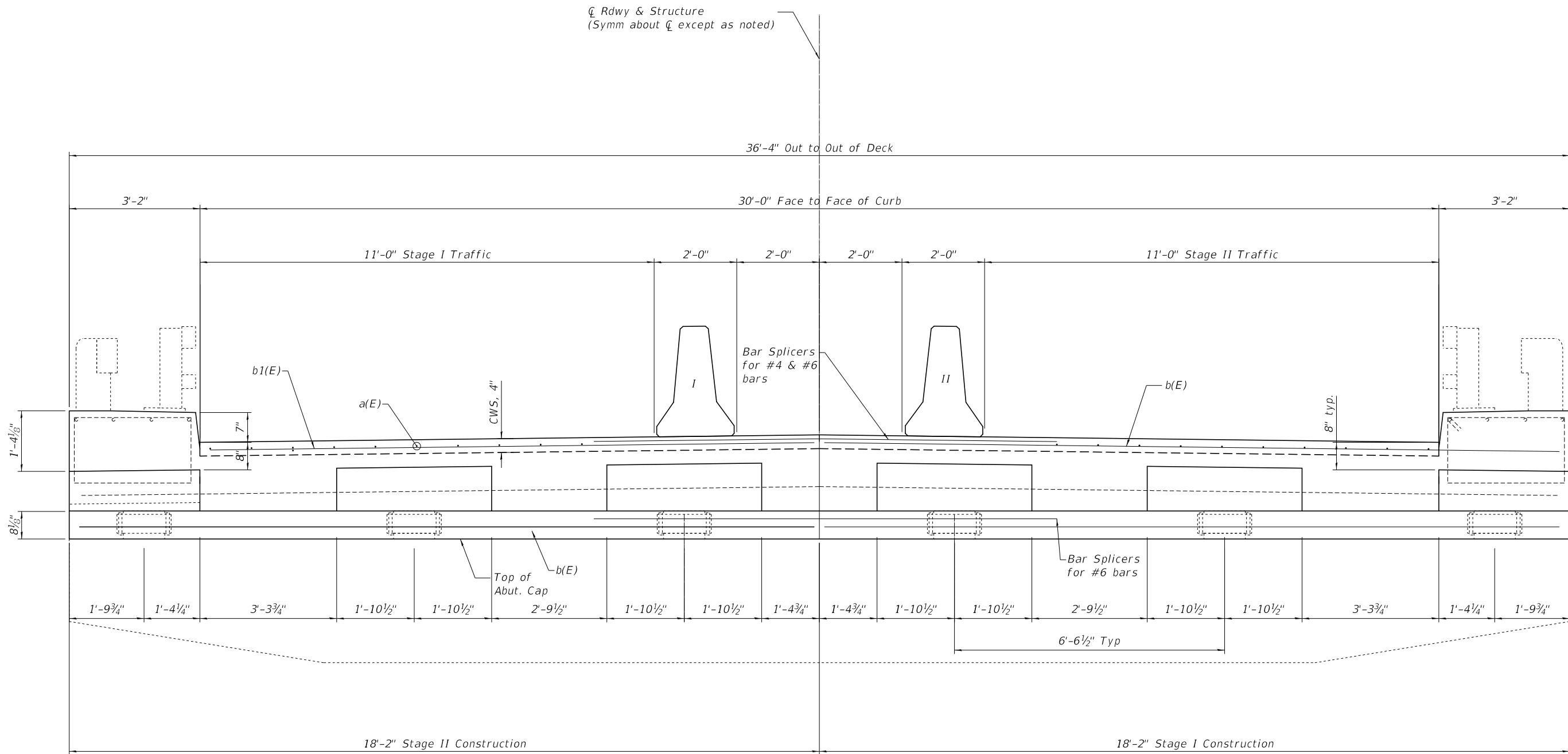
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PROPOSED PLAN VIEW (E. Abut.)
SN 014-0031**

SHEET NO. 8 OF 17 SHEETS

| F.A.S. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-------------|---------|--------------------|-----------|
| 1780 | 26BDR-BJR-1 | CLINTON | 27 | 18 |
| ILLINOIS FED. AID PROJECT | | | CONTRACT NO. 76M03 | |

PASSED _____
ENGINEER OF BRIDGES AND STRUCTURES



EXISTING CROSS SECTION

(Looking West)

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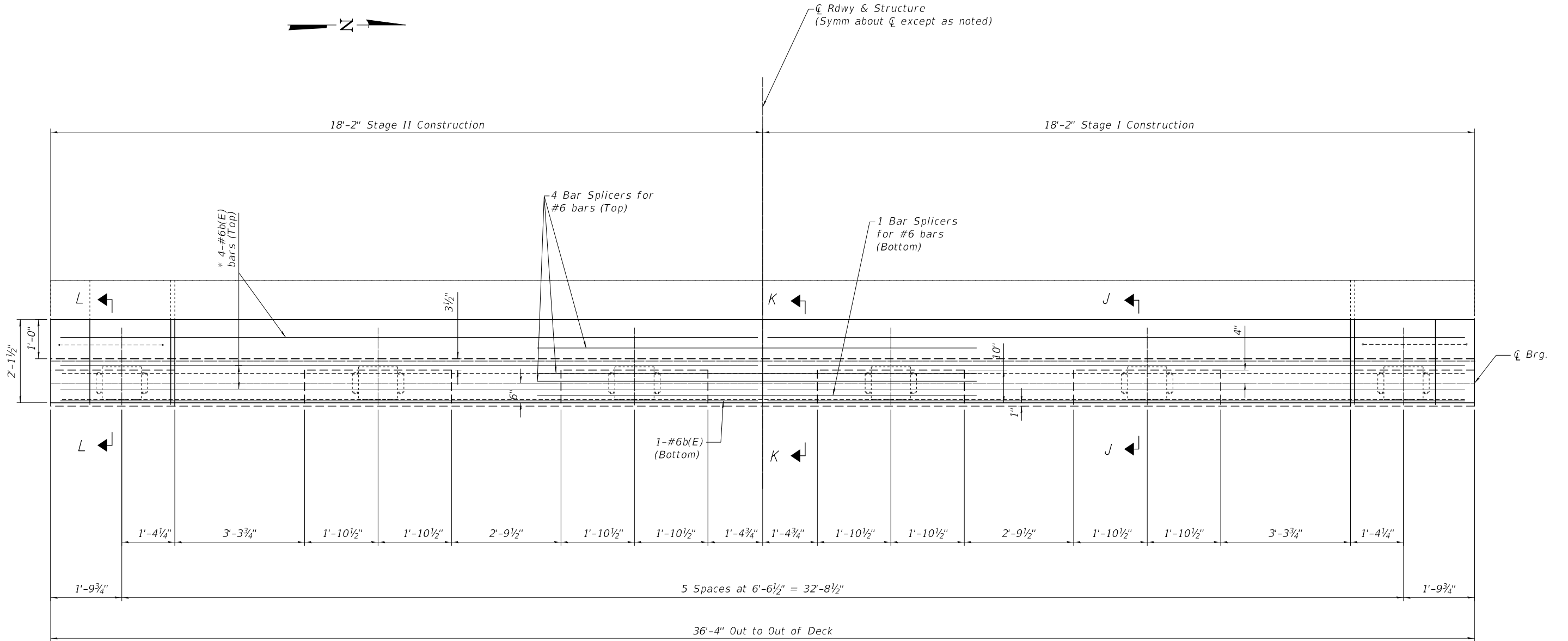
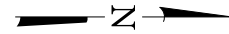
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| CHECKED - JSB | | |
| DRAWN - AYV | | |
| CHECKED - JSB | | |
| PASSED | REVISOR | REVISION |
| ENGINEER OF BRIDGES AND STRUCTURES | | |

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PROPOSED CROSS SECTION (W. Abut.)
SN 014-0031**

SHEET NO. 9 OF 17 SHEETS

| F.A.S RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-------------|---------|-----------------|--------------|
| 1780 | 26BDR-BJR-1 | CLINTON | 27 | 19 |
| CONTRACT NO. 76M03 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



PLAN

Note:
See sheet 11 of 17 for Bill of Material & Sections J-J, K-K & L-L.

* See Cross Section on sheet 11 of 17 for placement.

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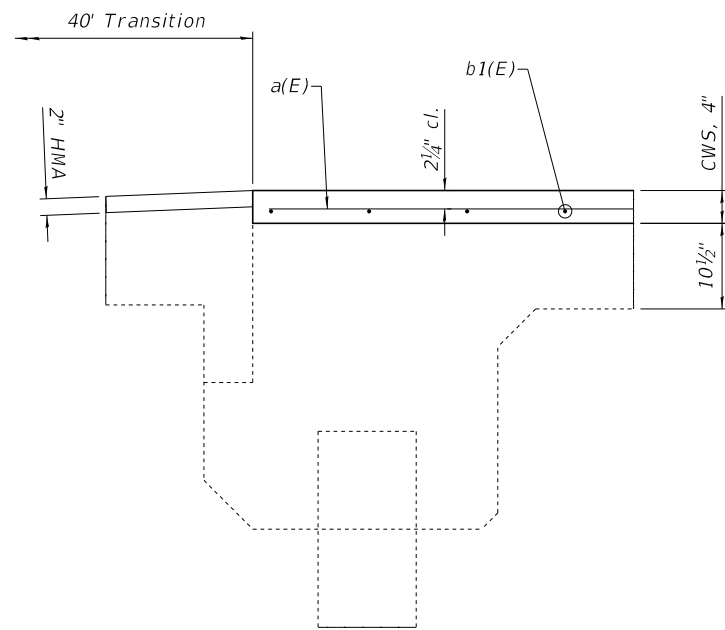
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**PROPOSED PLAN VIEW (W. Abut.)
SN 014-0031**

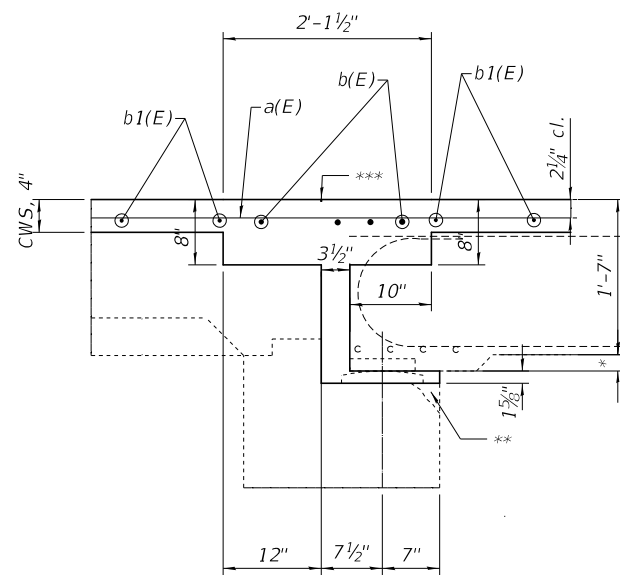
SHEET NO. 10 OF 17 SHEETS

| F.A.S RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|-------------|---------|-----------------|--------------|
| 1780 | 26BDR-BJR-1 | CLINTON | 27 | 20 |
| CONTRACT NO. 76M03 | | | | |

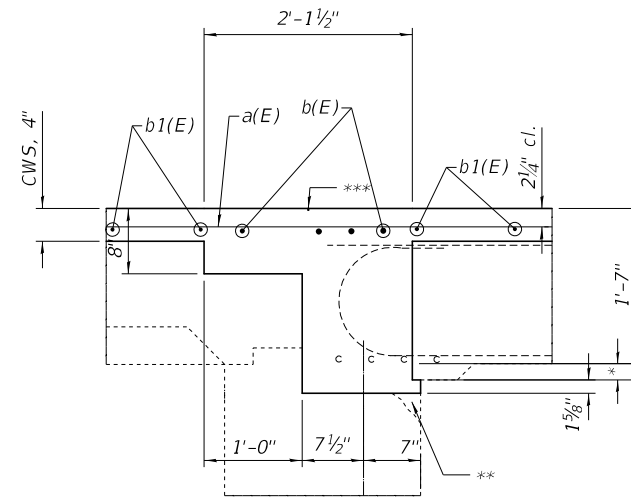
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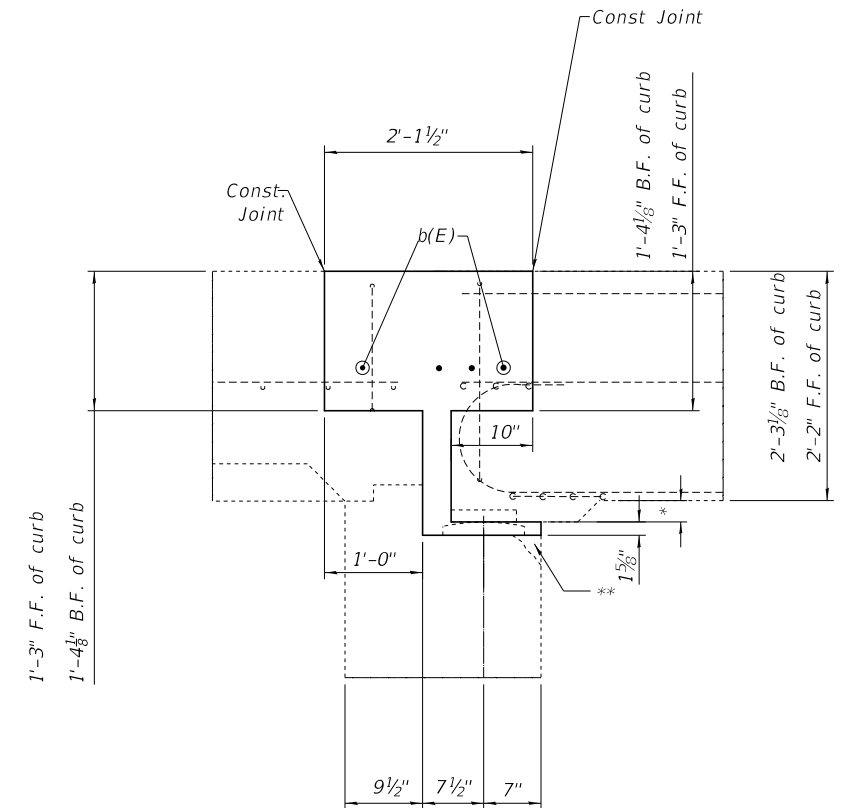
EXISTING APPROACH BENT



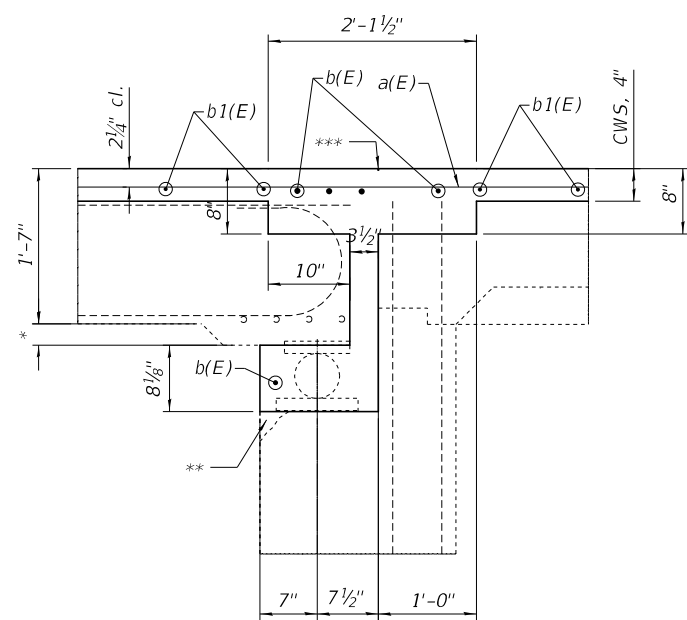
SECTION G-G
(At Bearings)



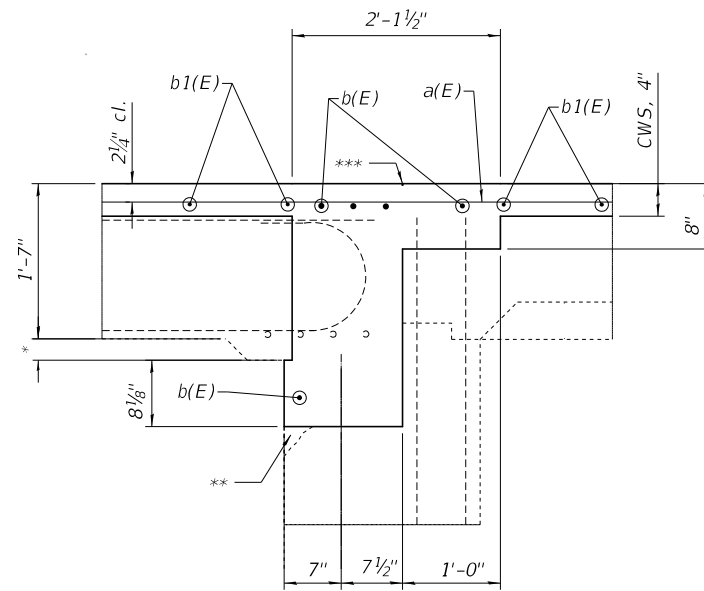
SECTION H-H
(At Deck Section)



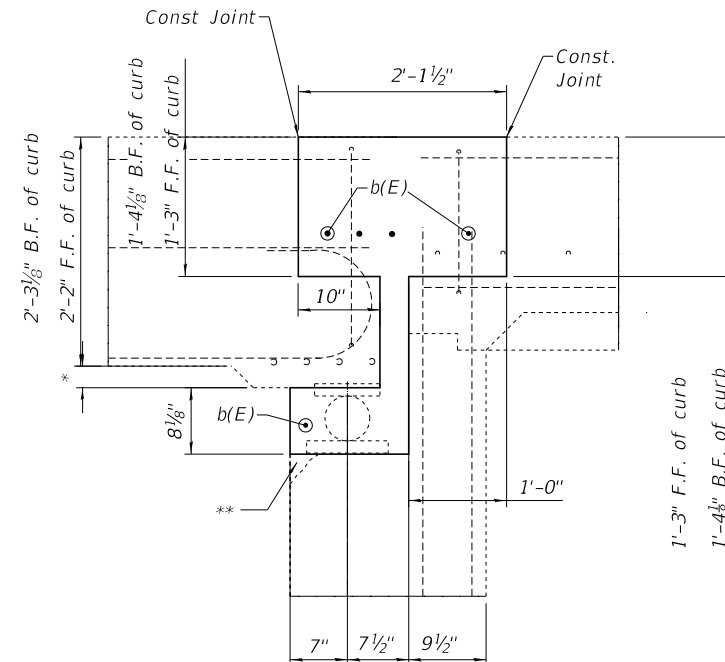
SECTION I-I
(At Curb Section)



SECTION J-J
(At Bearings)



SECTION K-K
(At Deck Section)



SECTION L-L
(At Curb Section)

Note:
Concrete superstructure and concrete wearing surface shall be place monolithically.
Quantities are based on 4" Concrete Wearing Surface over the abutments.

* Varies 2" to 4 5/8"

** Remove loose concrete. Cost included in Concrete Removal

*** 1/4" x 3/4" formed joint with concrete joint sealer without the backer rod (Full width)

BILL OF MATERIAL

| Bar | No. | Size | Length | Shape | |
|--|-----|------|--------|---------|------|
| a(E) | 120 | #4 | 27'-7" | — | |
| b(E) | 18 | #6 | 17'-9" | — | |
| b1(E) | 200 | #4 | 14'-7" | — | |
| Concrete Removal | | | | Cu. Yd. | 6.2 |
| Concrete Superstructure | | | | Cu. Yd. | 6.3 |
| Bridge Deck Grooving | | | | Sq. Yd. | 345 |
| Reinforcement Bars, Epoxy Coated | | | | Pound | 4640 |
| Deck Slab Repair (Full Depth, Type II) | | | | Sq. Yd. | 1.6 |
| Deck Slab Repair (Partial) | | | | Sq. Yd. | 94.1 |
| Concrete Wearing Surface, 4" | | | | Sq. Yd. | 345 |
| Bridge Deck Concrete Sealer | | | | Sq. Ft. | 4431 |
| Bridge Deck Scarification 2" | | | | Sq. Yd. | 345 |
| Bar Splicers | | | | Each | 109 |

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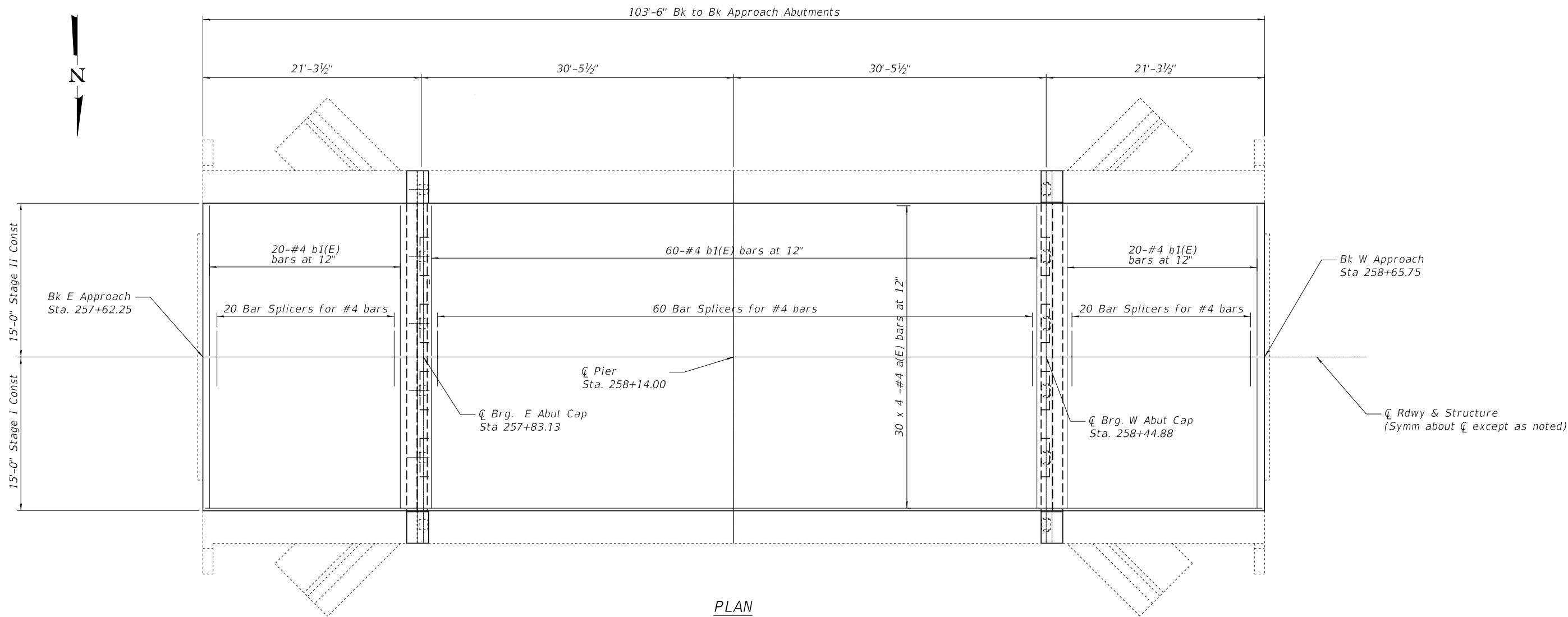
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| ENGINEER OF BRIDGES AND STRUCTURES |
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED JOINT DETAIL
SN 014-0031

SHEET NO. 11 OF 17 SHEETS

| | | | | |
|--------------------|-------------|---------|---------------------------|-----------|
| F.A.S. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1780 | 26BDR-BJR-1 | CLINTON | 27 | 21 |
| CONTRACT NO. 76M03 | | | ILLINOIS FED. AID PROJECT | |



PLAN

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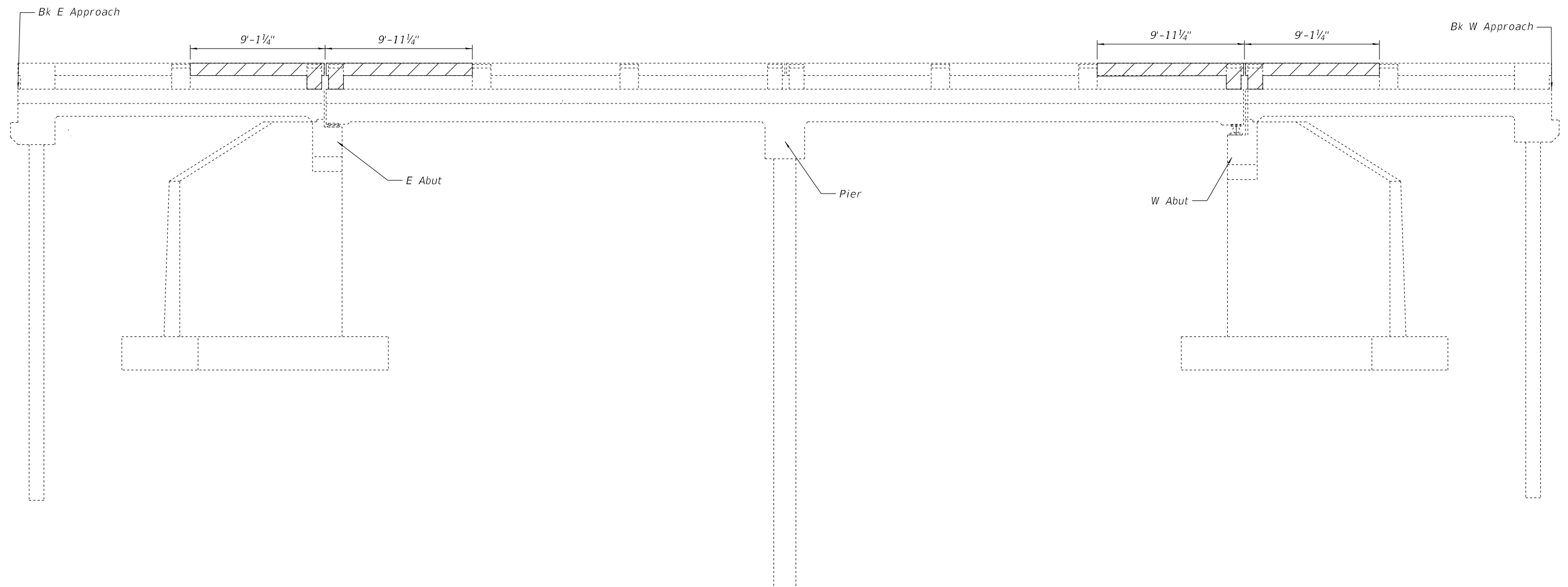
STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

CONCRETE WEARING SURFACE PLAN
 SN 014-0031

SHEET 12 OF 17 SHEETS

| F.A.S. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-------------|---------|--------------|-----------|
| 1780 | 26BDR-BJR-1 | CLINTON | 27 | 22 |
| CONTRACT NO. 76M03 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |

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NORTH ELEVATION



Removal of concrete rail & post
 (Cost included in Concrete Removal)
 (N Elev shown. S Elevation similar)

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| DRAWN - | AYV |
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| | ENGINEER OF BRIDGES AND STRUCTURES |

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| REVISED - | _____ |

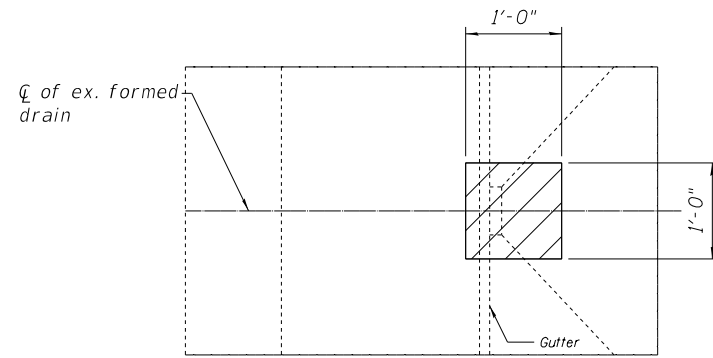
**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**CONCRETE BRIDGE RAIL & POST REMOVAL
 SN 014-0031**

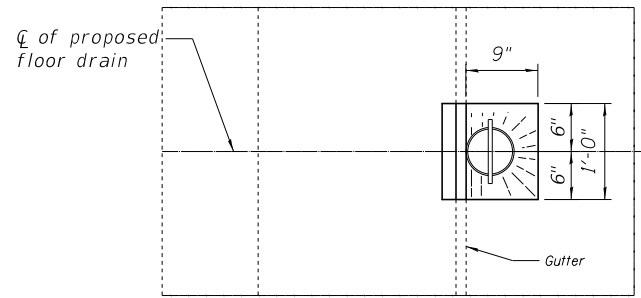
SHEET 13 OF 17 SHEETS

| F.A.S. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|-------------|----------|------------------|-----------|
| 1780 | 26BDR-BJR-1 | CLINTON | 27 | 23 |
| CONTRACT NO. 76M03 | | | | |
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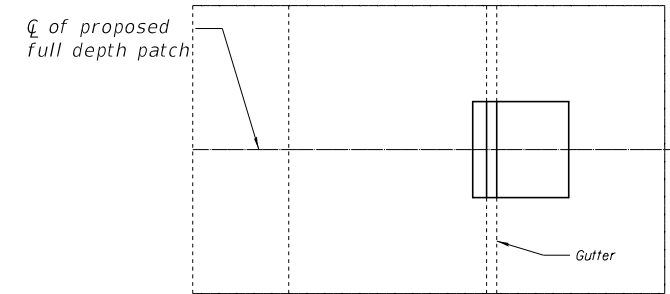


PLAN-REMOVAL OF EXISTING FORMED OPENING AND ADJACENT CONCRETE



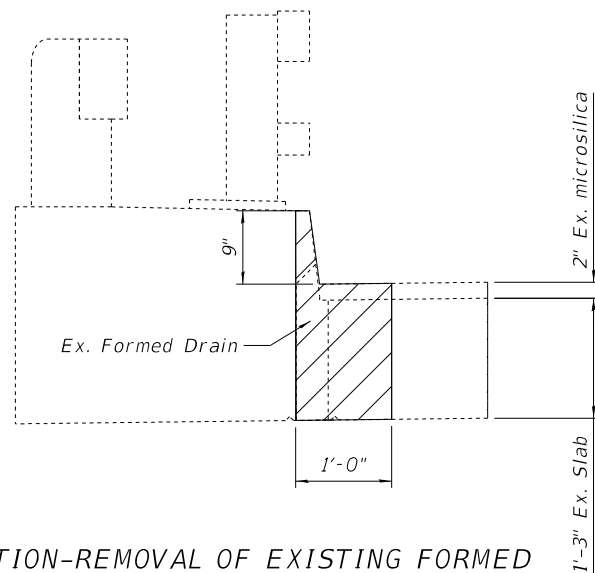
PLAN-PROPOSED FLOOR DRAIN & FULL DEPTH PATCH

(See sheet 1 of 17 for locations to replace formed drain with floor drain & full depth patch)

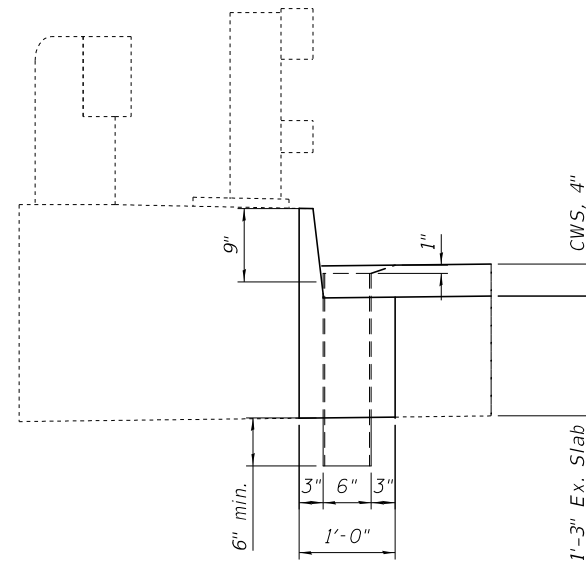


PLAN-PROPOSED FULL DEPTH PATCH @ ELIMINATED FLOOR DRAIN

(See sheet 1 of 17 for locations to replace formed drain with full depth patch)

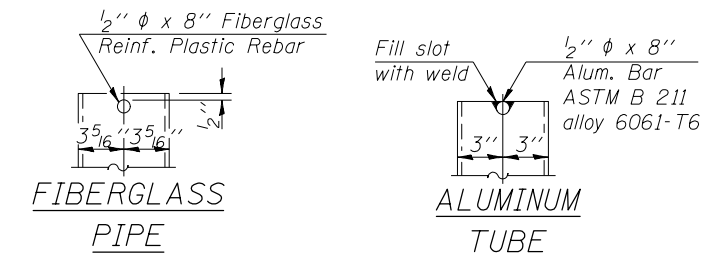


SECTION-REMOVAL OF EXISTING FORMED OPENING AND ADJACENT CONCRETE



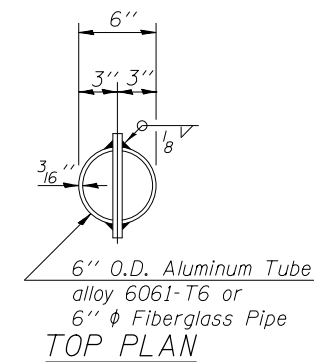
SECTION-PROPOSED FLOOR DRAIN & FULL DEPTH PATCH

(Place top of drain 1" below top of proposed CWS)



FIBERGLASS PIPE

ALUMINUM TUBE



TOP PLAN
(Showing Aluminum Tube)

Notes:
Fiberglass pipe shall conform to ASTM D 2996, with short-time rupture strength hoop tensile stress of 30,000 p.s.i. minimum.
Cost of Concrete Removal and replacement is included in cost of Deck Slab Repair (Full Depth, Type II)

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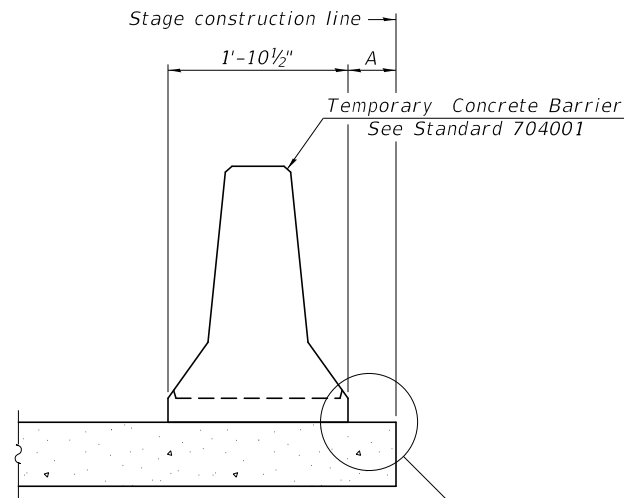
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ENGINEER OF BRIDGES AND STRUCTURES

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DRAIN DETAILS
SN 014-0031**

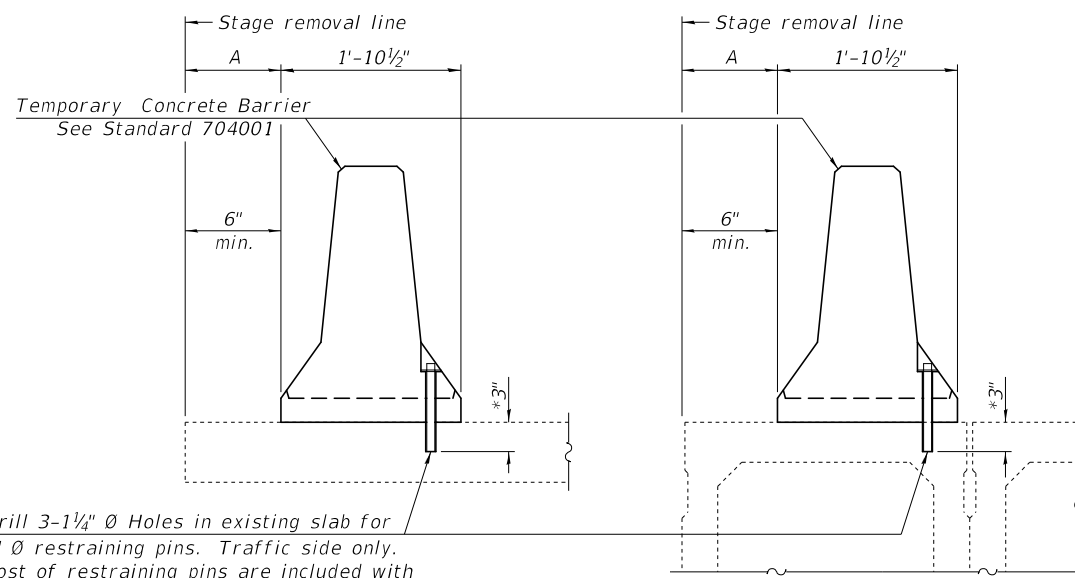
SHEET 14 OF 17 SHEETS

| F.A.S. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-------------|---------|--------------|-----------|
| 1780 | 26BDR-BJR-1 | CLINTON | 27 | 24 |
| CONTRACT NO. 76M03 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |



When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM

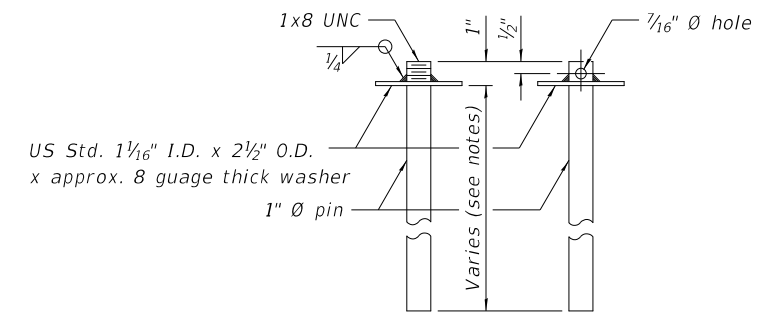


Drill 3-1/4" Ø Holes in existing slab for 1" Ø restraining pins. Traffic side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

EXISTING SLAB

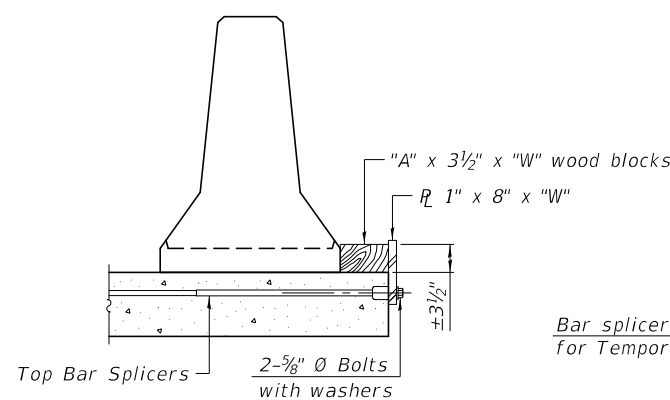
* When hot-mix asphalt wearing surface is present, embedment shall be 3" plus the wearing surface depth.

EXISTING DECK BEAM

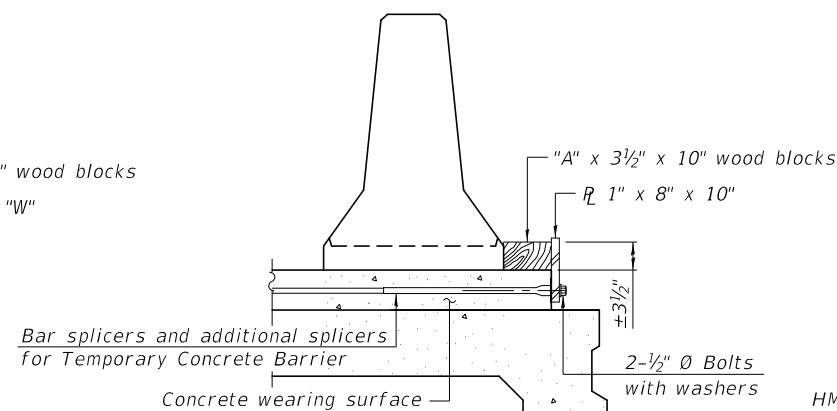


RESTRAINING PIN

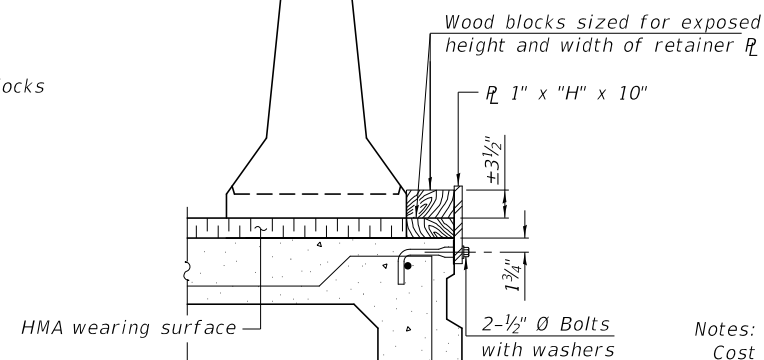
SECTIONS THRU SLAB OR DECK BEAM



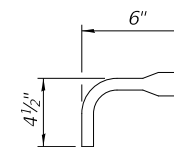
DETAIL I



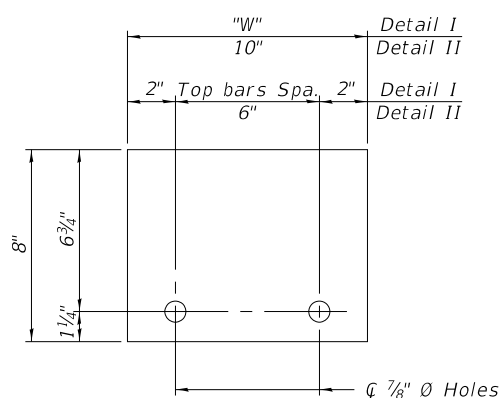
DETAIL II



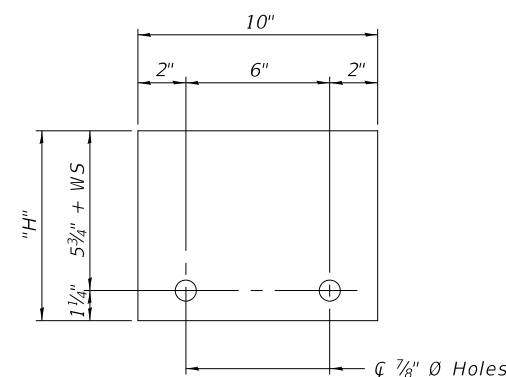
DETAIL III



BAR SPLICER FOR #4 BAR - DETAIL III



STEEL RETAINER R 1" x 8" x "W"
(Detail I and II)



STEEL RETAINER R 1" x "H" x 10"
(Detail III)

Notes:
 Cost of retainer assembly is included with Temporary Concrete Barrier.
 A retainer assembly shall be located at the approximate \bar{C} of each temporary concrete barrier.
 The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.
 When the 'A' dimension is less than 1 1/2", the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.

Detail I - Installation for a new bridge deck or bridge slab.
 Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.
 Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.

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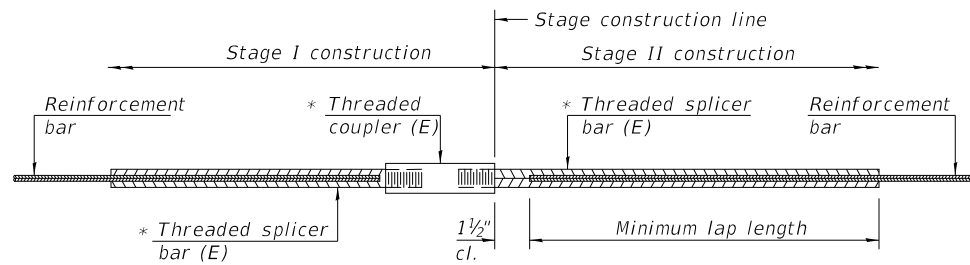
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY CONCRETE BARRIER FOR STAGE CONSTRUCTION
STRUCTURE NO. 014-0031

SHEET NO. 15 OF 17 SHEETS

| | | | | |
|--------------------|-------------|---------|---------------------------|-----------|
| F.A.S. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
| 1780 | 26BDR-BJR-1 | CLINTON | 27 | 25 |
| CONTRACT NO. 76M03 | | | ILLINOIS FED. AID PROJECT | |

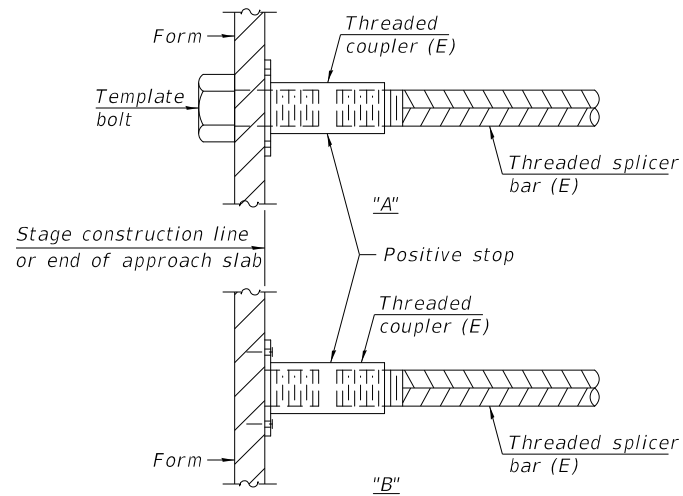


STANDARD BAR SPLICER ASSEMBLY

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

| Location | Bar size | No. assemblies required | Minimum lap length |
|----------|----------|-------------------------|--------------------|
| E. Abut | #6 | 4 | 4'-5" |
| W. Abut | #6 | 5 | 4'-5" |
| CWS | #4 | 100 | 2'-5" |

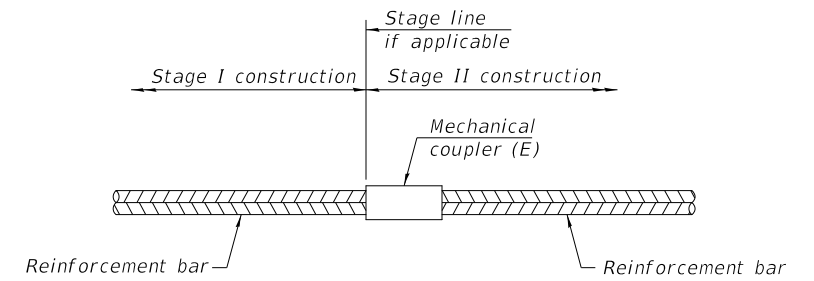


INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.

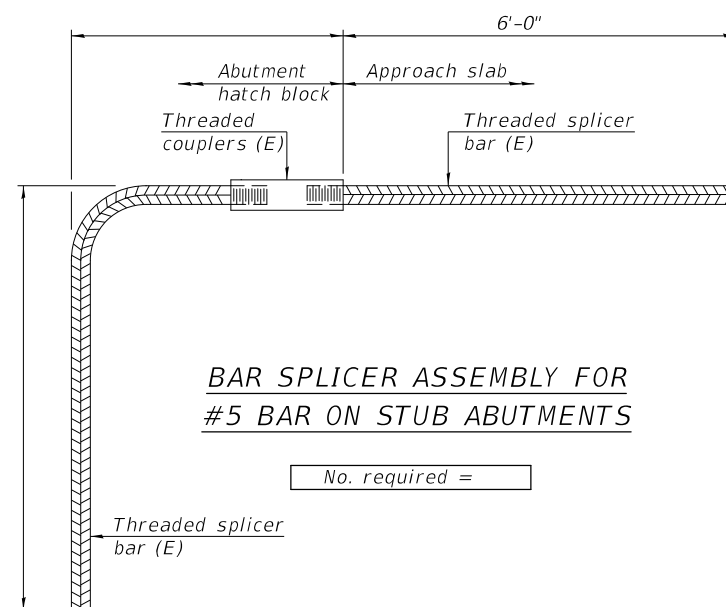
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.

(E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

| Location | Bar size | No. assemblies required |
|----------|----------|-------------------------|
| | | |
| | | |
| | | |
| | | |



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

No. required =

NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.

All reinforcement shall be lapped and tied to the splicer bars. Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.

See approved list of bar splicer assemblies and mechanical splicers for alternatives.

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| REVISED - | |

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DEPARTMENT OF TRANSPORTATION

BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
STRUCTURE NO. 014-0031

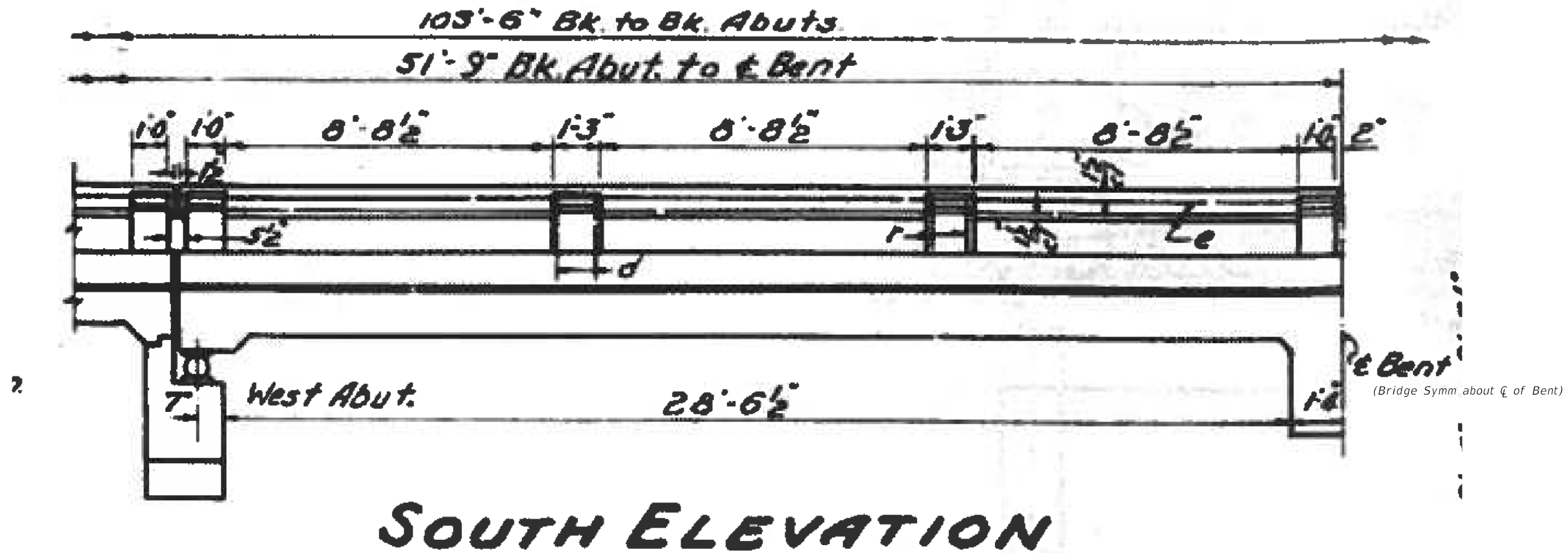
SHEET 16 OF 17 SHEETS

| F.A.S. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|--------------------|-------------|---------|--------------|-----------|
| 1780 | 26BDR-BJR-1 | CLINTON | 27 | 26 |
| CONTRACT NO. 76M03 | | | | |

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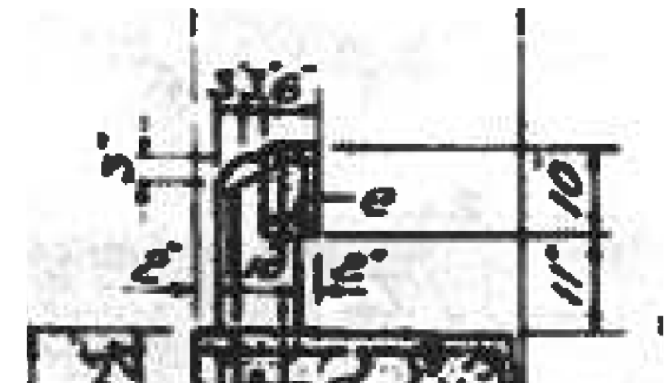
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SOUTH ELEVATION

FOR INFORMATION ONLY



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| DRAWN - | AYV |
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

FOR INFORMATION ONLY - EXISTING CONC BRIDGE RAIL & POST
 SN 014-0031

SHEET 17 OF 17 SHEETS

| F.A.S. RTE. | SECTION | COUNTY | TOTAL SHEETS | SHEET NO. |
|---------------------------|-------------|---------|--------------|-----------|
| 1780 | 26BDR-BJR-1 | CLINTON | 27 | 27 |
| CONTRACT NO. 76M03 | | | | |
| ILLINOIS FED. AID PROJECT | | | | |