

PROP. CURVE DET1-1
 PI STA. = 2532+19.18
 $\Delta = 6^\circ 26' 48''$ (LT)
 D = 1° 30' 00"
 R = 3,820.00'
 T = 215.13'
 L = 429.81'
 E = 6.05'
 e = N.C. (60 MPH)
 T.R. = N.A.
 S.E. RUN = N.A.
 P.C. STA. = 2530+04.05
 P.T. STA. = 2534+33.86

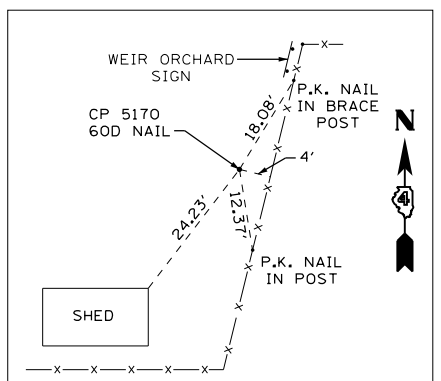
PROP. CURVE DET1-2
 PI STA. = 2539+88.11
 $\Delta = 16^\circ 30' 39''$ (RT)
 D = 1° 30' 00"
 R = 3,820.00'
 T = 554.25'
 L = 1,100.81'
 E = 40.00'
 e = N.C. (60 MPH)
 T.R. = N.A.
 S.E. RUN = N.A.
 P.C. STA. = 2534+33.86
 P.T. STA. = 2545+34.67

PROP. CURVE DET1-3
 PI STA. = 2548+80.37
 $\Delta = 10^\circ 20' 31''$ (LT)
 D = 1° 30' 00"
 R = 3,820.00'
 T = 345.70'
 L = 689.52'
 E = 15.61'
 e = N.C. (60 MPH)
 T.R. = N.A.
 S.E. RUN = N.A.
 P.C. STA. = 2545+34.67
 P.T. STA. = 2552+24.19

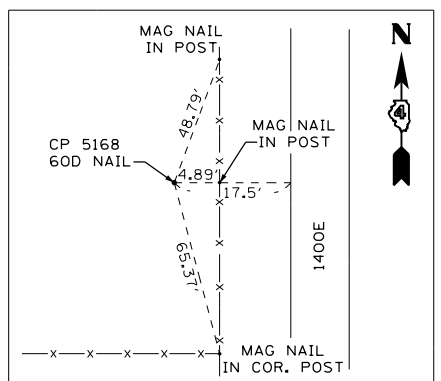
PROP. CURVE US34-8
 PI STA. = 542+92.54
 $\Delta = 21^\circ 06' 55''$ (RT)
 D = 1° 24' 22"
 R = 4,075.01'
 T = 759.50'
 L = 1,501.77'
 E = 70.17'
 e = 4.35% (70 MPH)
 T.R. = 65'
 S.E. RUN = 131'
 P.C. STA. = 535+33.04
 P.T. STA. = 550+34.81

ALIGNMENT COORDINATES - DETOUR NO 1			
DET #1	STATION	N	E
POT	2527+03.44	1524801.4572	2084758.0206
PC	2530+04.05	1524799.9143	2085058.6275
PI	2532+19.18	1524798.8101	2085273.7559
PRC	2534+33.86	1524821.8671	2085487.6480
PI	2539+88.11	1524881.2693	2086038.7027
PRC	2545+34.67	1524781.6129	2086583.9168
PI	2548+80.37	1524719.4548	2086923.9800
PT	2552+24.19	1524719.3558	2087269.6772
POT	2555+72.56	1524719.2559	2087618.0487

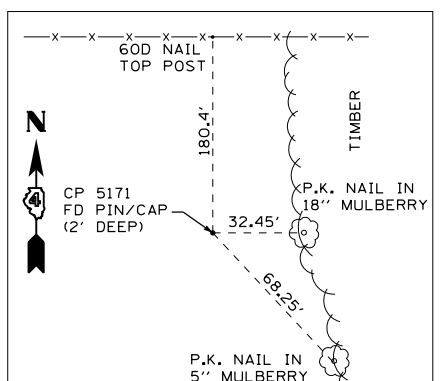
ALIGNMENT COORDINATES - PR U.S. 34			
U.S.34	STATION	N	E
PC	535+33.04	1524760.1820	2085590.8560
PI	542+92.54	1524756.2830	2086350.3450
PT	550+34.81	1524479.0432	2087057.4354



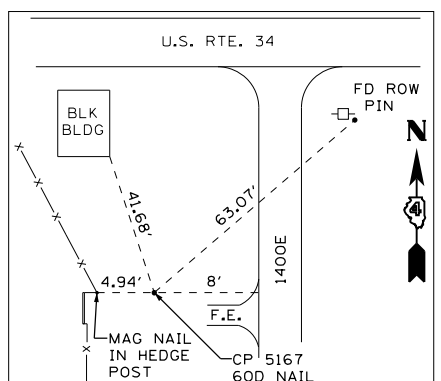
CONTROL POINT #5170
 N 1524640.1510, E 2086423.4220, ELEV. 701.83
 STA. 543+78.24, 29.04' RT (RTE 34)



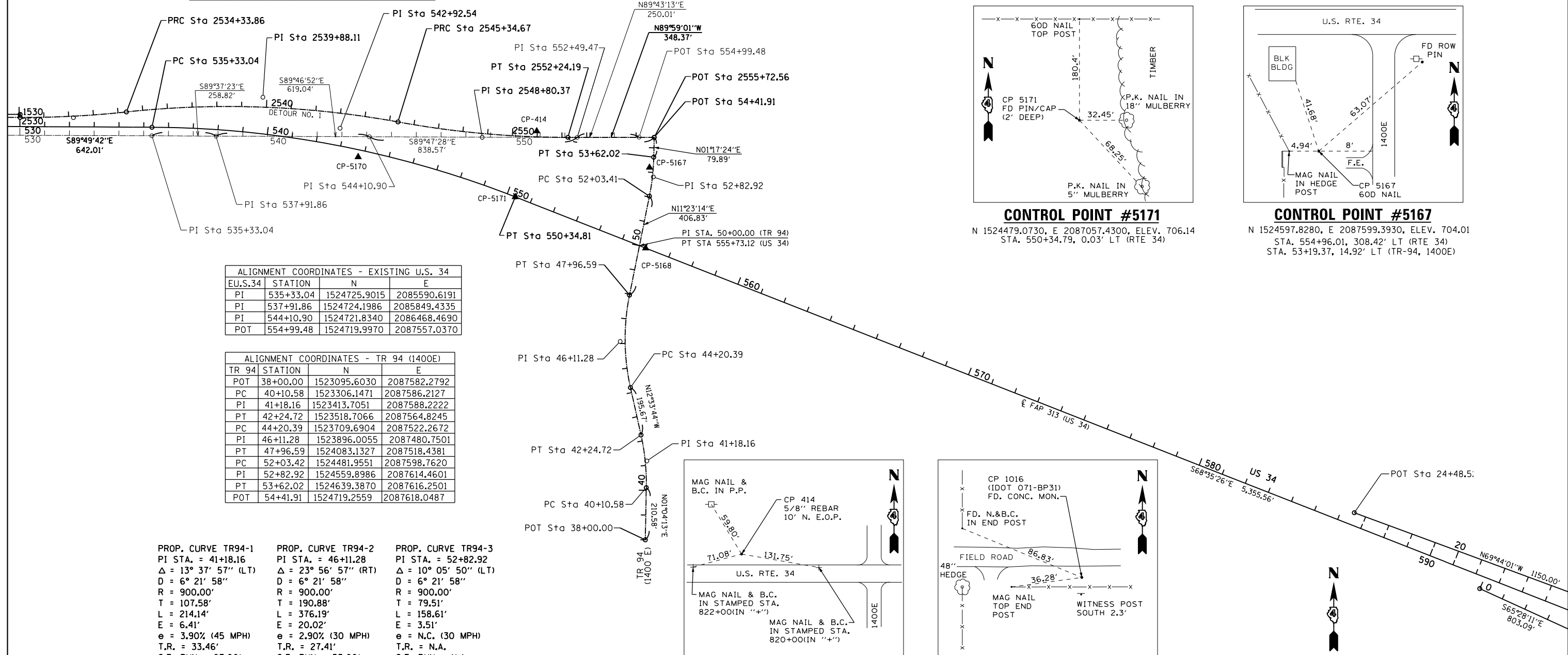
CONTROL POINT #5168
 N 1524272.3360, E 2087583.5640, ELEV. 707.30
 STA. 556+00.09, 0.39' RT (RTE 34)
 STA. 49+94.92, 26.49' RT (TR-94 1400E)



CONTROL POINT #5171
 N 1524479.0730, E 2087057.4300, ELEV. 706.14
 STA. 550+34.79, 0.03' LT (RTE 34)



CONTROL POINT #5167
 N 1524597.8280, E 2087599.3930, ELEV. 704.01
 STA. 554+96.01, 308.42' LT (RTE 34)
 STA. 53+19.37, 14.92' LT (TR-94, 1400E)



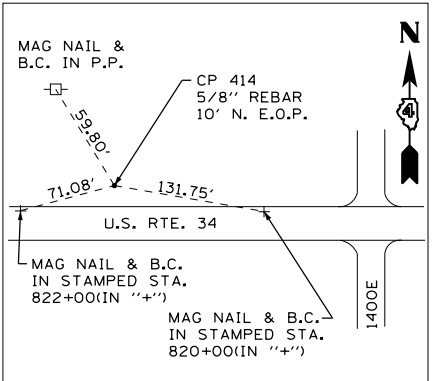
ALIGNMENT COORDINATES - EXISTING U.S. 34			
U.S.34	STATION	N	E
PI	535+33.04	1524725.9015	2085590.6191
PI	537+91.86	1524724.1986	2085849.4335
PI	544+10.90	1524721.8340	2086468.4690
POT	554+99.48	1524719.9970	2087557.0370

ALIGNMENT COORDINATES - TR 94 (1400E)			
TR 94	STATION	N	E
POT	38+00.00	1523095.6030	2087582.2792
PC	40+10.58	1523306.1471	2087586.2127
PI	41+18.16	1523413.7051	2087588.2222
PT	42+24.72	1523518.7066	2087564.8245
PC	44+20.39	1523709.6904	2087522.2672
PI	46+11.28	1523896.0055	2087480.7501
PT	47+96.59	1524083.1327	2087518.4381
PC	52+03.42	1524481.9551	2087598.7620
PI	52+82.92	1524559.8986	2087614.4601
PT	53+62.02	1524639.3870	2087616.2501
POT	54+41.91	1524719.2559	2087618.0487

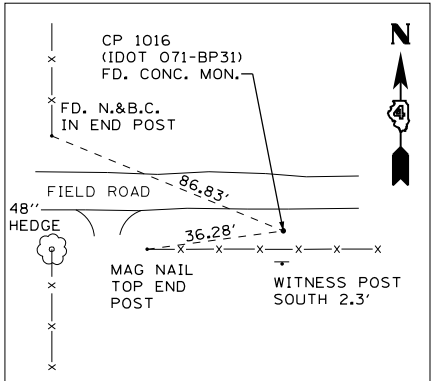
PROP. CURVE TR94-1
 PI STA. = 41+18.16
 $\Delta = 13^\circ 37' 57''$ (LT)
 D = 6° 21' 58"
 R = 900.00'
 T = 107.58'
 L = 214.14'
 E = 6.41'
 e = 3.90% (45 MPH)
 T.R. = 33.46'
 S.E. RUN = 87.00'
 P.C. STA. = 40+10.58
 P.T. STA. = 42+24.72

PROP. CURVE TR94-2
 PI STA. = 46+11.28
 $\Delta = 23^\circ 56' 57''$ (RT)
 D = 6° 21' 58"
 R = 900.00'
 T = 190.88'
 L = 376.19'
 E = 20.02'
 e = 2.90% (30 MPH)
 T.R. = 27.41'
 S.E. RUN = 53.00'
 P.C. STA. = 44+20.39
 P.T. STA. = 47+96.59

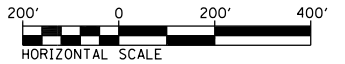
PROP. CURVE TR94-3
 PI STA. = 52+82.92
 $\Delta = 10^\circ 05' 50''$ (LT)
 D = 6° 21' 58"
 R = 900.00'
 T = 79.51'
 L = 158.61'
 E = 3.51'
 e = N.C. (30 MPH)
 T.R. = N.A.
 S.E. RUN = N.A.
 P.C. STA. = 52+03.42
 P.T. STA. = 53+62.02



CONTROL POINT #414
 N 1524744.4900, E 2087144.1030, ELEV. 701.79
 STA. 550+19.64, 278.80' LT (RTE 34)



CONTROL POINT #1016
 N 1522049.8505, E 2088943.2832, ELEV. 713.44
 STA. 576+77.25, 1573.18' RT (RTE 34)



FILE NAME =
 D468409-SHT-ATB-02.dgn

USER NAME = zachl	DESIGNED - KEF	REVISED -
PLOT SCALE = 400.0000' / in.	DRAWN - PSBA	REVISED -
PLOT DATE = 10/16/2012	CHECKED - CSB	REVISED -
	DATE - 10/2012	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

ALIGNMENT TIES AND BENCHMARKS US ROUTE 34	
SCALE: 1" = 200'	SHEET NO. 2 OF 8 SHEETS
STA. 530+00	TO STA. 595+00

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
313	7-2; 6-1	HENDERSON	976	100
CONTRACT NO. 68409				
ILLINOIS FED. AID PROJECT				