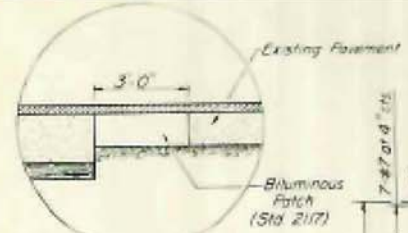


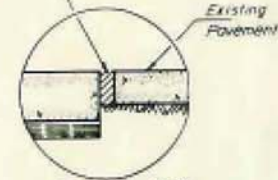
**SECTION C-C**

\*Stagger alternate #7 bars as shown on plan - full width



**DETAIL "A"**

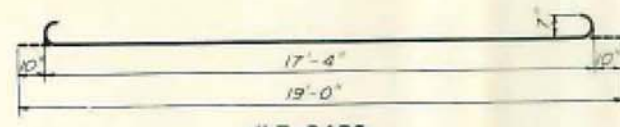
(When bituminous surface is being placed)



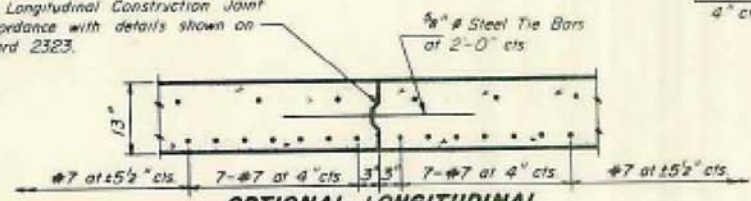
**DETAIL "A"**

(P.C.C. Pavement Construction)

Keyed Longitudinal Construction Joint in accordance with details shown on Standard 2323.



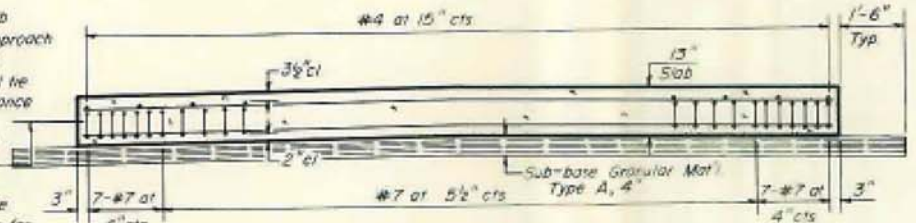
**#7 BARS**



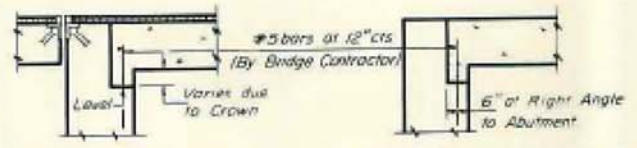
**OPTIONAL LONGITUDINAL CONSTRUCTION JOINT**

As approved by the Engineer, the Contractor may elect to reduce the widths of pair by use of the Optional Longitudinal Construction Joint shown. Joints shall be located at the edge of a traffic lane.

When the road plans show curb and gutter, gutter, or bridge approach shoulder pavement adjacent to approach slabs, place 1/2 inch # steel tie bars at 2'-6 inch centers in accordance with the detail for Bulkhead Longitudinal Construction Joint shown on Standard 2323. Cost of the tie bars will be included in the contract unit price for the adjacent item. Transitions for curb and gutter or gutter shall be as shown in the plans.

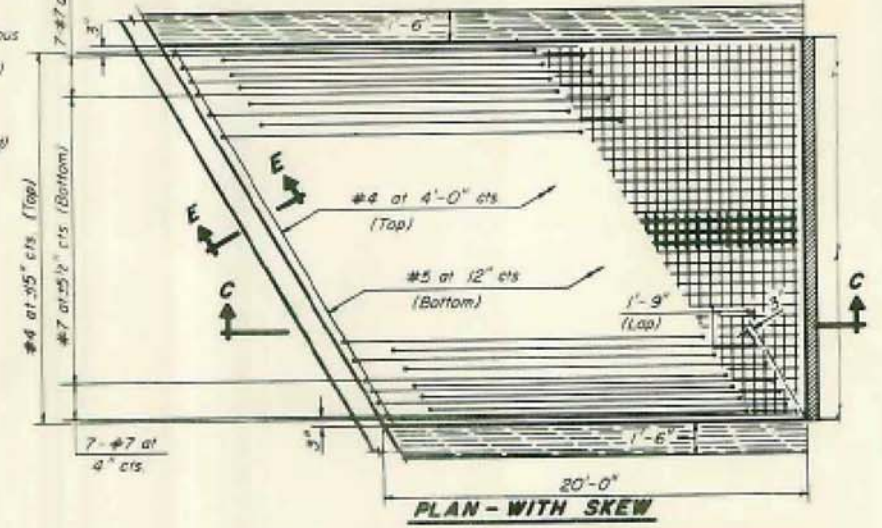


**SECTION D-D**

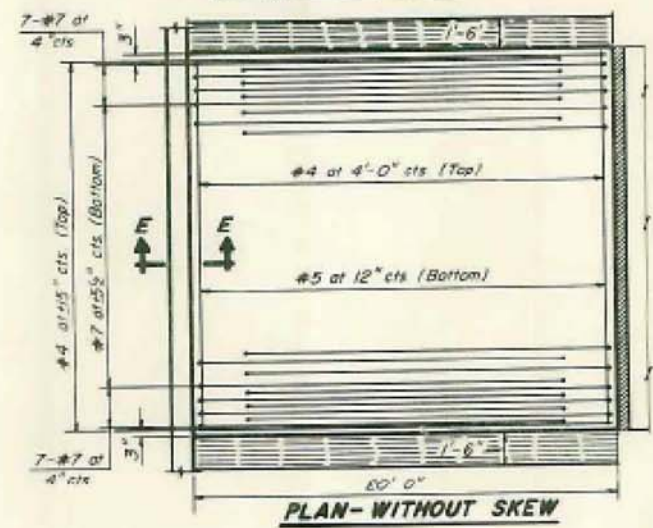


**SECTION E-E**

**Notes**  
For skews of less than 10° omit wire fabric. For skews of 10° or more use Welded Wire Fabric, 6"x6"-W5.5 x W5.5, placed 3/2" below top of slab. Expanded Metal weighing not less than 78 Pounds per 100 Sq Ft or a welded bar mat weighing not less than 78 Pounds per 100 Sq Ft having members of equal size in both directions and spaced not over 8" apart may be used instead of the Welded Wire Fabric, 6"x6"-W5.5 x W5.5, provided the expanded metal or bar mat is furnished at no additional cost to the State. Reinforcement bars shall conform to the requirements of A.A.S.H.T.O. M-31 or M-53, Grade 60.



**PLAN - WITH SKEW**



**PLAN - WITHOUT SKEW**

**FOR INFORMATION ONLY**

**GENERAL NOTES**

The cost of tie bars, expansion joint filler, sub-base, welded wire fabric and bituminous prime when required shall be considered as included in the unit cost of the Bridge Approach Pavement.  
Preformed Expansion Joint Filler shall conform to Section 715 of the Standard Specifications.  
Width of Bridge Approach Slab shall be determined before the reinforcement bars are fabricated.  
The bituminous patch, when required, will be paid for in accordance with Section 620 of the Standard Specifications.

**DESIGN STRESSES**

$f_y = 60,000 \text{ psi}$   
 $f_c = 3500 \text{ psi}$   
 $n = 8.5$

Illinois Department of Transportation  
 PASSED: [Signature] 4, 1979  
 APPROVED: [Signature] 4, 1979  
 ISSUED 3-9-79

**BRIDGE APPROACH PAVEMENT**  
 Sheet 1 of 2  
**STANDARD 2382-1**

H-530a

FILE NAME: #FILE#	USER NAME: #USERS#	DESIGNED: -	REVISED: -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	EXISTING APPROACH PAVEMENT SN 027-0072 &027-0073-FOR INFORMATION ONLY	F.A.P. RTE. 796	SECTION 1106 BR-1 & BR-4BR	COUNTY FORD	TOTAL SHEETS 78	SHEET NO. 59		
#MODELNAME#	PLOT SCALE: #SCALE#	DRAWN: -	REVISED: -			SCALE: _____	SHEET _____	OF _____	SHEETS _____	STA. _____	TO STA. _____	CONTRACT NO. 66980
	PLOT DATE: #DATE#	CHECKED: -	REVISED: -									ILLINOIS FED. AID PROJECT
		DATE: -	REVISED: -									