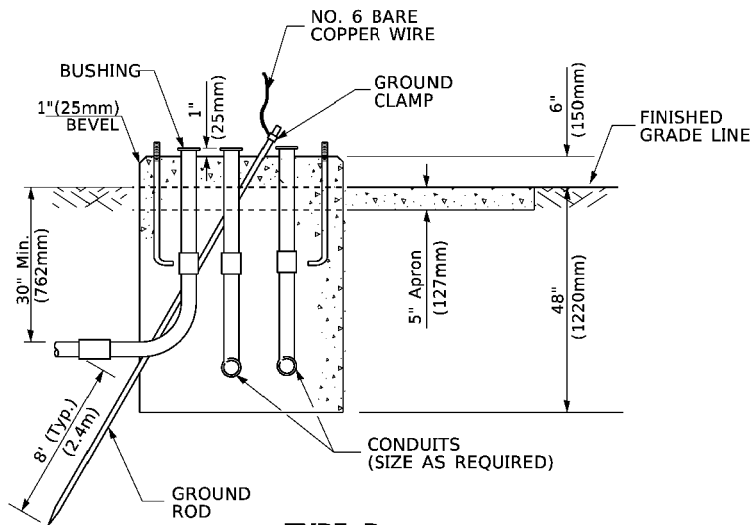
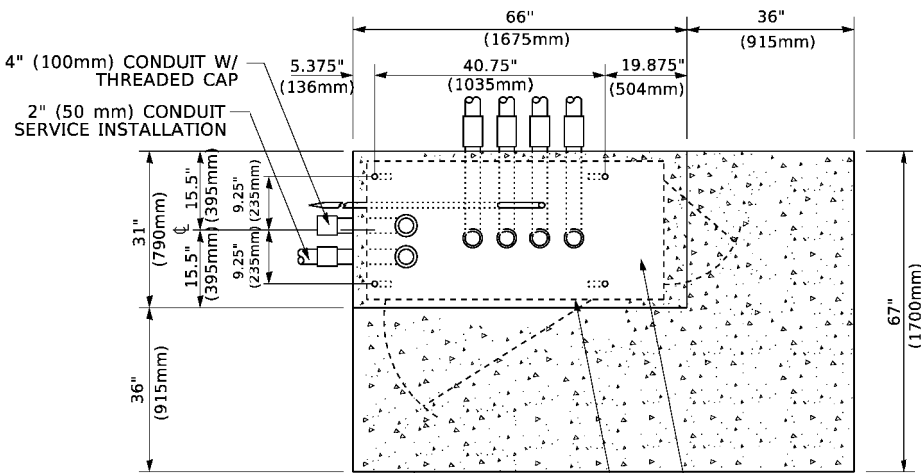


TOP VIEW



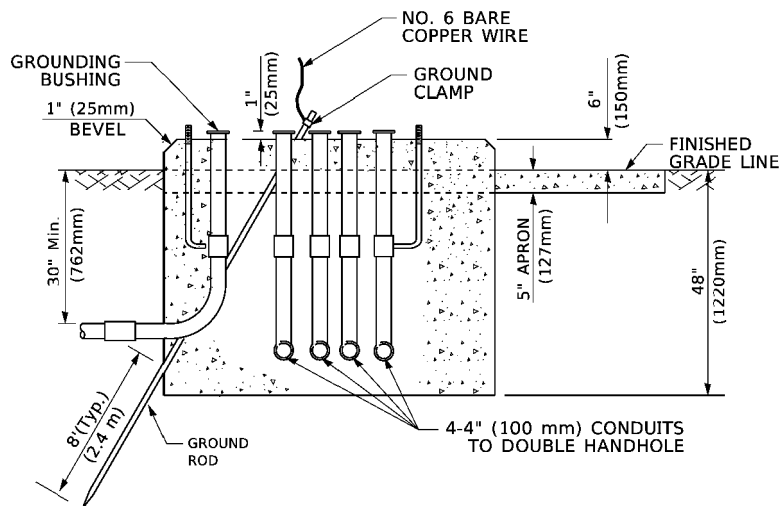
TYPE D
FOR GROUND MOUNTED
CONTROLLER CABINET
AND UPS BATTERY CABINET



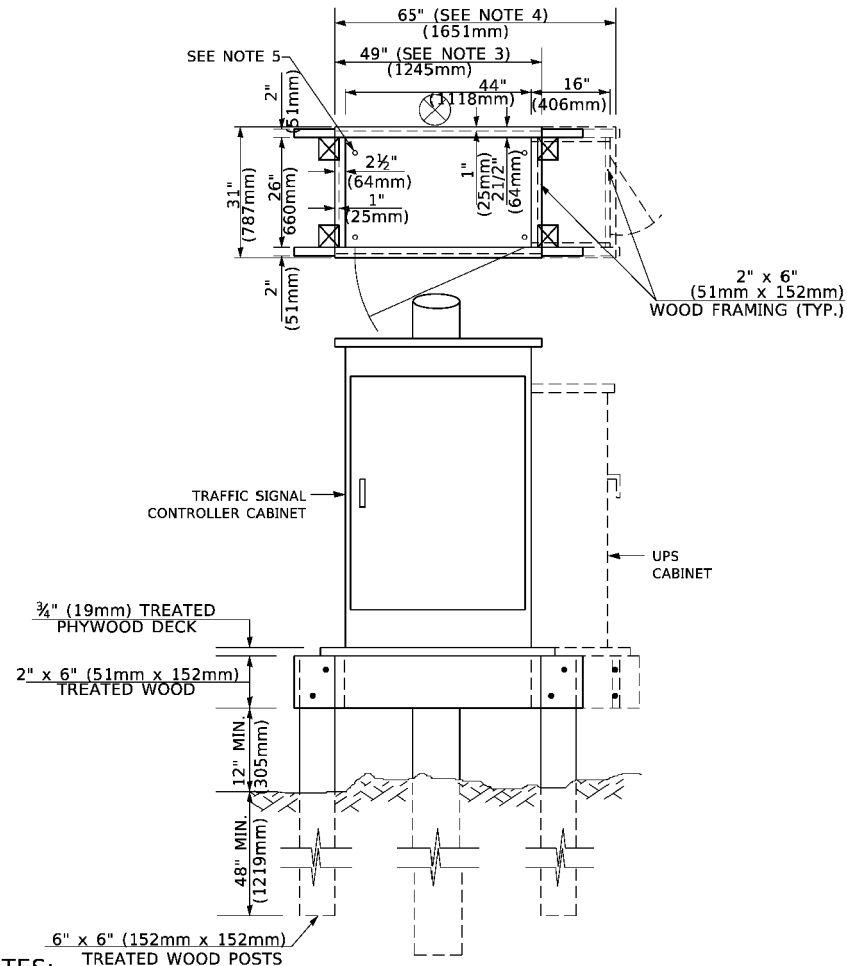
TOP VIEW

NOTE:

TOP OF FOUNDATION SHALL
BE HIGHER THAN TOP OF
DOUBLE HANDHOLE



TYPE C
FOR GROUND MOUNTED
SUPER P (TYPE IV) AND SUPER R (TYPE V)
CONTROLLER CABINETS



NOTES:

1. BASED ON CONTROLLER CABINET TYPE IV WITH BASE DIMENSIONS OF 26" x 44" (660mm x 1118mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
2. BASED ON UNINTERRUPTIBLE POWER SUPPLY CABINET WITH BASE DIMENSIONS OF 16" x 25" (406mm x 635mm). ADJUST PLATFORM SIZE TO FIT CABINET BASE DIMENSIONS BEING SUPPLIED.
3. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV.
4. PLATFORM SIZE FOR CONTROLLER CABINET TYPE IV AND UNINTERRUPTIBLE POWER SUPPLY CABINET.
5. DRILLED HOLES THROUGH THE PLATFORM BASE TO MATCH THE CONTROLLER CABINET BOLT TEMPLATE. FASTEN THE CONTROLLER CABINET TO THE PLATFORM WITH CARRIAGE BOLTS, WASHERS AND NUTS.
6. FASTEN ALL SUPPORT WOOD FRAMING TO THE WOOD POSTS WITH 2 LAG SCREWS FOR EACH CONNECTION..

TEMPORARY SIGNAL CONTROLLER
WOOD SUPPORT PLATFORM

CABLE SLACK LENGTH	FEET	METER
HANDHOLE	6.5	2.0
DOUBLE HANDHOLE	13.0	4.0
SIGNAL POST	2.0	0.6
MAST ARM	2.0	0.6
CONTROLLER CABINET	1.5	0.5
FIBER OPTIC AT CABINET	13.0	4.0
ELECTRIC SERVICE AT (CABINET OR SERVICE LOCATION)	1.5	0.5
GROUND CABLE (SIGNAL POST, MAST ARM, CABINET)	1.5	0.5
GROUND CABLE (BETWEEN FRAME AND COVER)	5.0	1.6

CABLE SLACK

VERTICAL CABLE LENGTH	FEET	METER
MAST ARM POLE (MAST ARM MOUNTED SIGNAL HEAD) (L = MAST ARM LENGTH - DISTANCE TO SIGNAL HEAD FROM END OF ARM)	20.0+L	6.0+L
BRACKET MOUNTED (MAST ARM POLE OR SIGNAL POLE)	13.0	4.0
PEDESTRIAN PUSH BUTTON	6.0	2.0
SERVICE INSTALLATION POLE MOUNT TO SERVICE DROP	13.5	4.1
SERVICE INSTALLATION POLE MOUNT TO GROUND	13.5	4.1
SERVICE INSTALLATION GROUND MOUNT	6.0	2.0
FOUNDATION (SIGNAL POST, MAST ARM POLE, CONTROLLER CABINET, SERVICE-GROUND MOUNT)	3.0	1.0

VERTICAL CABLE LENGTH

FOUNDATION	DEPTH
TYPE A - Signal Post	4'-0" (1.2m)
TYPE C - CONTROLLER W/ UPS	4'-0" (1.2m)
TYPE D - CONTROLLER	4'-0" (1.2m)
SERVICE INSTALLATION, GROUND MOUNT, TYPE A - SQUARE	4'-0" (1.2m)

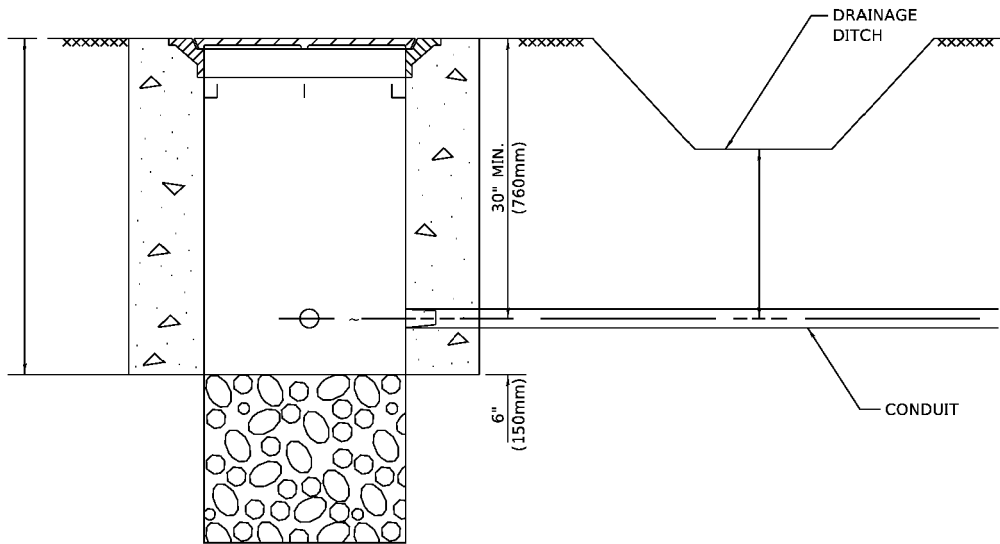
DEPTH OF FOUNDATION

MAST ARM LENGTH	① Foundation Depth	Foundation Diameter	Spiral Diameter	Quantity of Rebars	Size of Rebars
Less than 30' (9.1 m)	10'-0" (3.0 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 30' (9.1 m) and less than 40' (12.2 m)	13'-6" (4.1 m)	30" (750mm)	24" (600mm)	8	6(19)
Greater than or equal to 40' (12.2 m) and less than 50' (15.2 m)	11'-0" (3.4 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 50' (15.2 m) and up to 55' (16.8 m)	13'-0" (4.0 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 55' (16.8 m) and up to 65' (19.8 m)	15'-0" (4.6 m)	36" (900mm)	30" (750mm)	12	7(22)
Greater than or equal to 65' (19.8 m) and less than 75' (22.9 m)	21'-0" (6.4 m)	42" (1060mm)	36" (900mm)	16	8(25)
Greater than or equal to 75' (22.9 m)	25'-0" (7.6 m)	42" (1060mm)	36" (900mm)	16	8(25)

NOTES:

1. These foundation depths are for sites which have cohesive soils (clayey silt, sandy clay, etc.) along the length of the shaft, with an average Unconfined Compressive Strength (qu) > 1.0 tsf (100 kpa). This strength shall be verified by boring data prior to construction or with testing by the Engineer during foundation drilling. The Bureau of Bridges & Structures should be contacted for a revised design if other conditions are encountered.
2. Combination mast arm assemblies under 55 feet (16.8 m) shall use 36" (900 mm) diameter foundations.
3. Combination mast arm assemblies under 56 feet (16.8 m) through 75 feet (22.9 m) shall use 42" (1060 mm) diameter foundations.
4. For mast arm assemblies with dual arms refer to state standard 878001..

DEPTH OF MAST ARM FOUNDATIONS, TYPE E

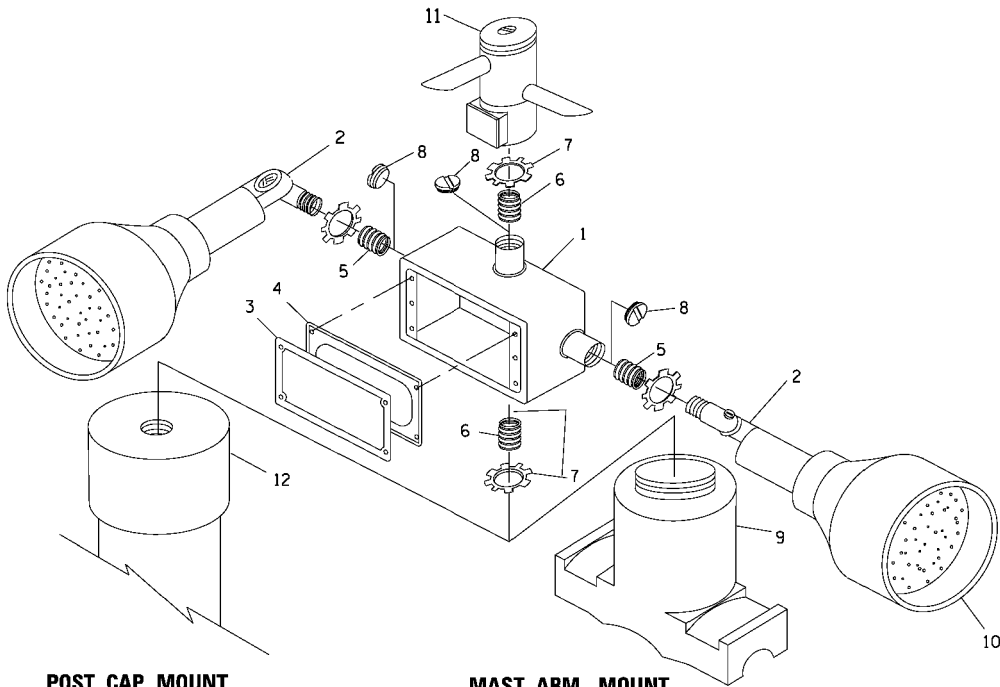


NOTES:

- CONDUIT DEPTH SHALL BE A MINIMUM OF 30" (760mm) BELOW THE BOTTOM OF THE DRAINAGE DITCH OR ANY SLOPING GROUND
- THE MINIMUM CONDUIT DEPTH APPLIES TO ALL CONDUIT PLACED UNDER ROADWAY PAVEMENT, MULTI-USE PATHS, SIDEWALKS AND SOIL SURFACES.
- THE MINIMUM CONDUIT DEPTH APPLIES TO ALL HANDHOLES, HEAVY DUTY HANDHOLES AND DOUBLE HANDHOLES.

HANDHOLE WITH MINIMUM CONDUIT DEPTH

(NOT TO SCALE)

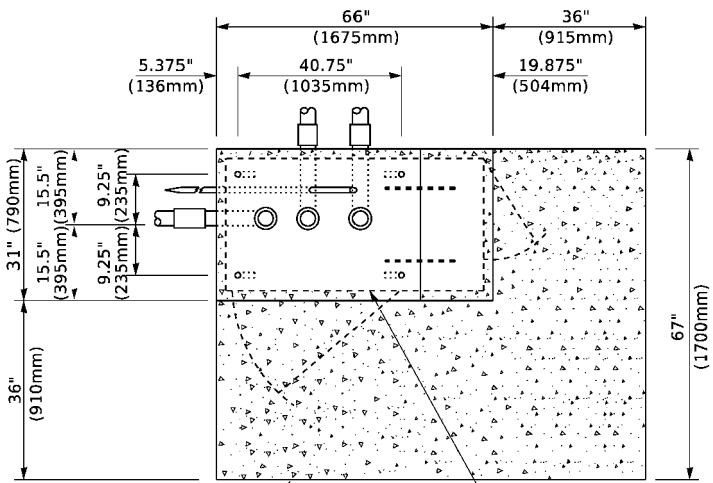


POST CAP MOUNT

MAST ARM MOUNT

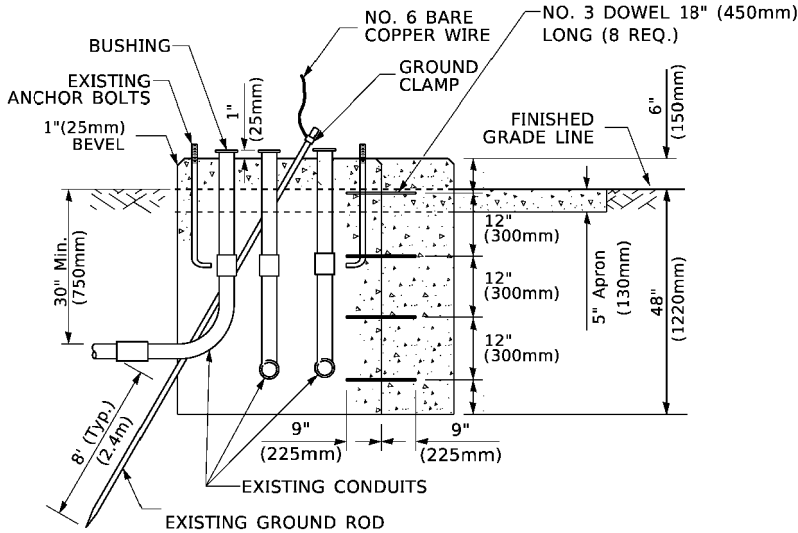
EMERGENCY VEHICLE DETECTOR WITH CONFIRMATION

BEACON MOUNTING DETAIL



TOP VIEW

(NOT TO SCALE)



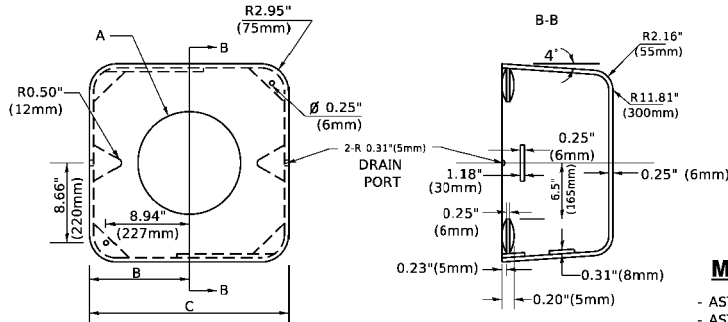
MODIFY EXISTING TYPE "D" FOUNDATION TO TYPE "C" FOUNDATION

(NOT TO SCALE)

ITEM NO.	IDENTIFICATION
1	OUTLET BOX- GALV. 21 CU.IN. (0.000344 CU-M)
2	LAMP HOLDER AND COVER
3	OUTLET BOX COVER
4	RUBBER COVER GASKET
5	REDUCING BUSHING
6	3/4"(19 mm) CLOSE NIPPLE
7	3/4"(19 mm) LOCKNUT
8	3/4"(19 mm) HOLE PLUG
9	SADDLE BRACKET - GALV.
10	6 WATT PAR 38 LED FLOOD LAMP
11	DETECTOR UNIT
12	POST CAP [18 FT. (5.4 m) POST MIN.]

NOTES:

- ALL ELECTRICAL ITEMS, EXCEPT ITEMS #2 AND #11 SHALL BE ALUMINUM OR GALVANIZED
- ITEM #1- OZ/GEDNEY FSX-1-50 OR EQUIVALENT
ITEM #2- MULBERRY CON-O-SHADE LAMP SHIELD OR EQUIVALENT
ITEM #9- "BAND-IT" SADDLE BRACKET OR EQUIVALENT
- WHEN POST MOUNTING IS SPECIFIED, ITEM #9 SHALL NOT BE REQUIRED. THE DETECTION UNIT SHALL BE MOUNTED DIRECTLY ON TOP OF THE CAP BY DRILLING AND TAPPING A 3/4 "(19 mm) HOLE WITH PIPE THREADS. THE POST CAP SHALL EITHER BE SCREWED TO THE TOP OF THE POST OR A MINIMUM OF 3 TIGHTENING SCREWS SHALL BE REQUIRED ON EACH CAP.



MATERIAL

- ASTM A36 STEEL
- ASTM A-123 HOT DIPPED GALVANIZED

A	B	C	HEIGHT	WEIGHT
VARIES	9.5"(241mm)	19"(483mm)	7" (178mm) - 12" (300mm)	53 lbs (24kg)
VARIES	10.75"(273mm)	21.5"(546mm)	7" (178mm) - 12" (300mm)	68 lbs (31 kg)
VARIES	13.0"(330mm)	26"(660mm)	7" (178mm) - 12" (300mm)	81 lbs (37 kg)
VARIES	18.5"(470mm)	37"(940mm)	7" (178mm) - 12" (300mm)	126 lbs (57 kg)

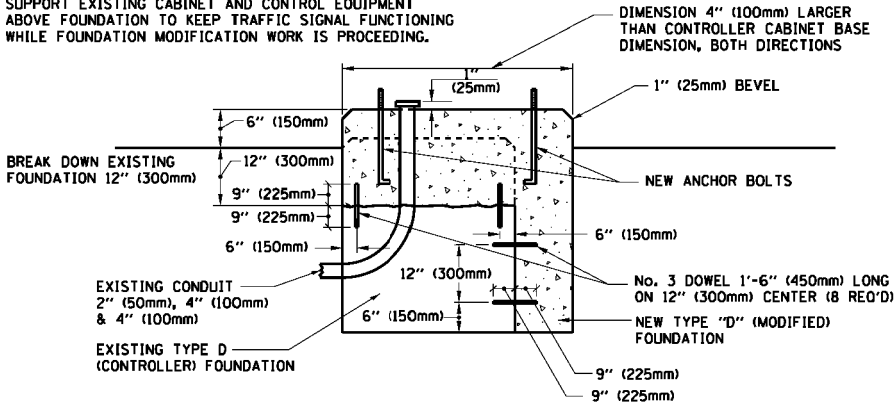
SHROUD

NOTES:

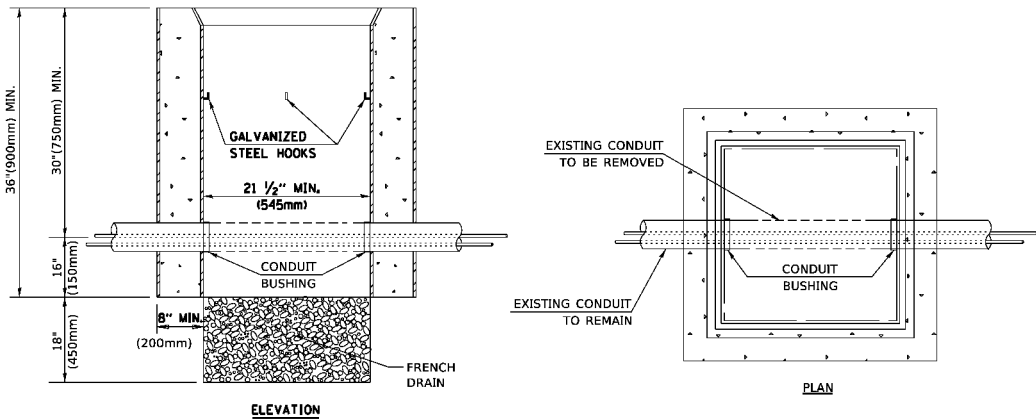
- DIMENSION "A" IS EQUAL TO THE DIAMETER OF THE MAST ARM POLE AT THE TOP OF THE SHROUD. THE SHROUD SHALL BE TIGHT TO THE MAST ARM POLE.
- THE SUPPLIER SHALL VERIFIED THE ABOVE DIMENSIONS BASED ON MAST ARM REQUIREMENTS.
- THE HEIGHT OF THE SHROUD SHALL COVER THE ANCHOR BOLTS, NUTS AND MAST ARM POLE BASE.

NOTE:

SUPPORT EXISTING CABINET AND CONTROL EQUIPMENT ABOVE FOUNDATION TO KEEP TRAFFIC SIGNAL FUNCTIONING WHILE FOUNDATION MODIFICATION WORK IS PROCEEDING.



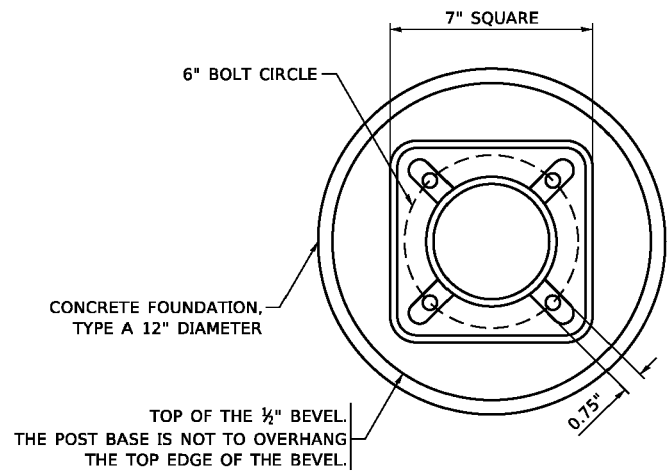
MODIFY EXISTING TYPE "D" FOUNDATION



NOTES:

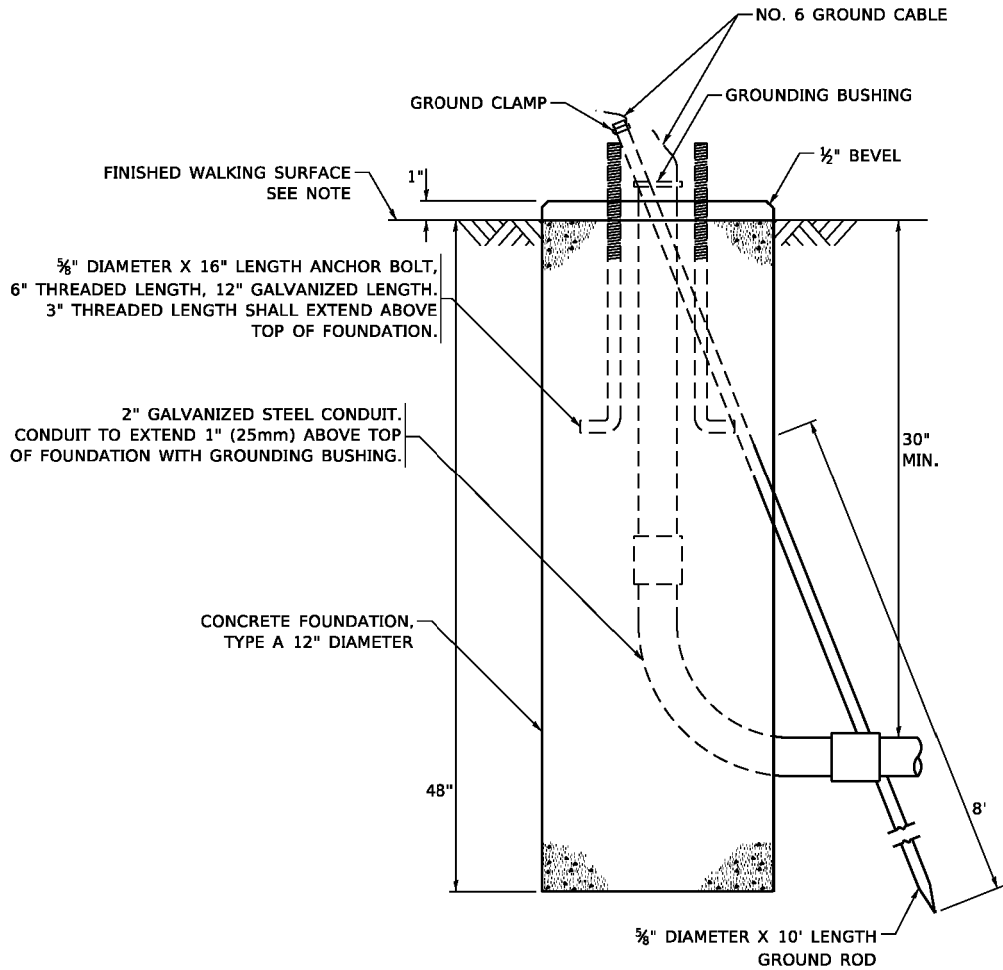
- HANDHOLE CONSTRUCTED PER STATE STANDARD 814001.
- REMOVAL OF THE EXISTING CONDUIT FROM THE HANDHOLE AND THE INSTALLATION OF THE CONDUIT BUSHINGS SHALL BE INCLUDED WITH THE COST OF THE HANDHOLE.

HANDHOLE TO INTERCEPT EXISTING CONDUIT

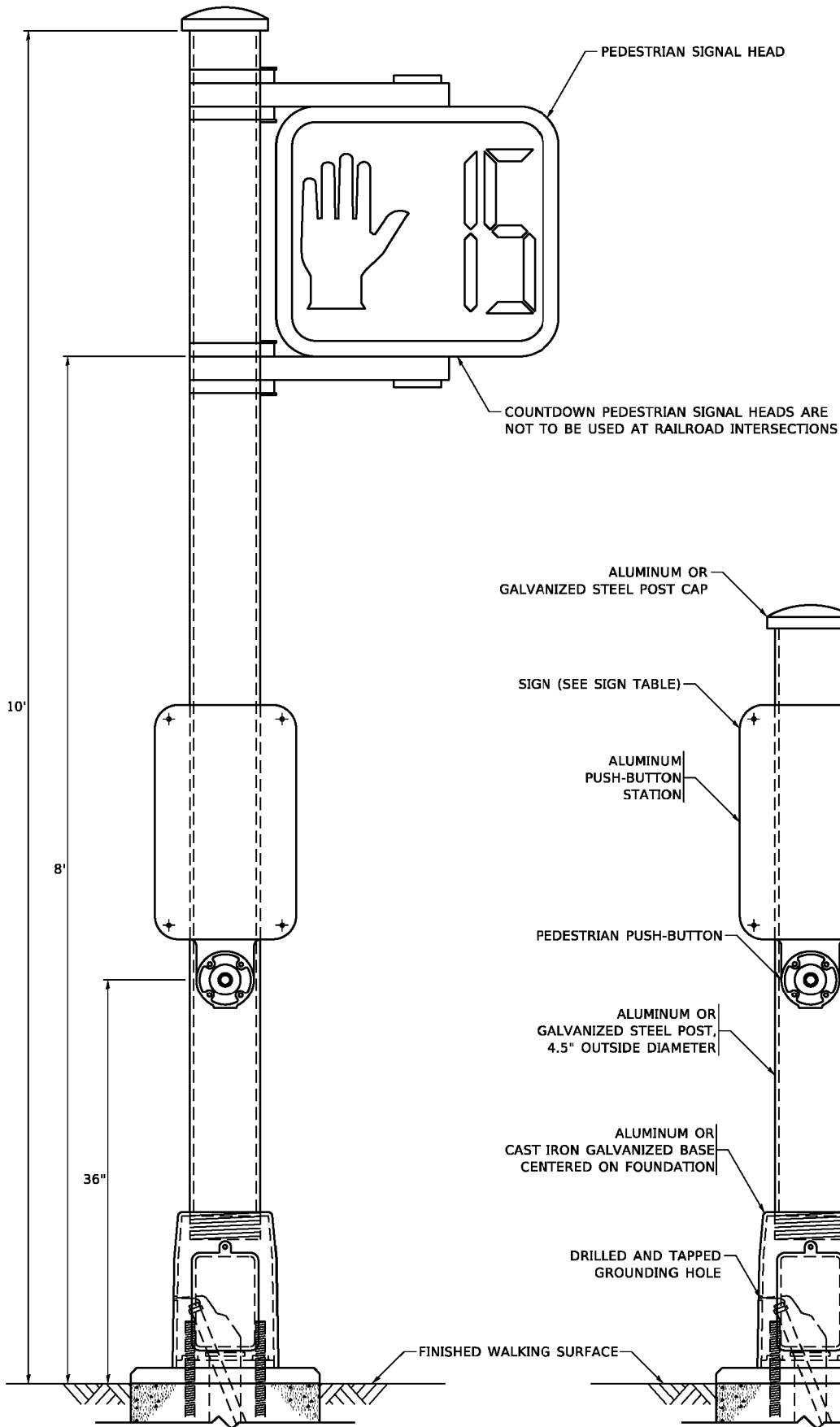


BOLT PATTERN

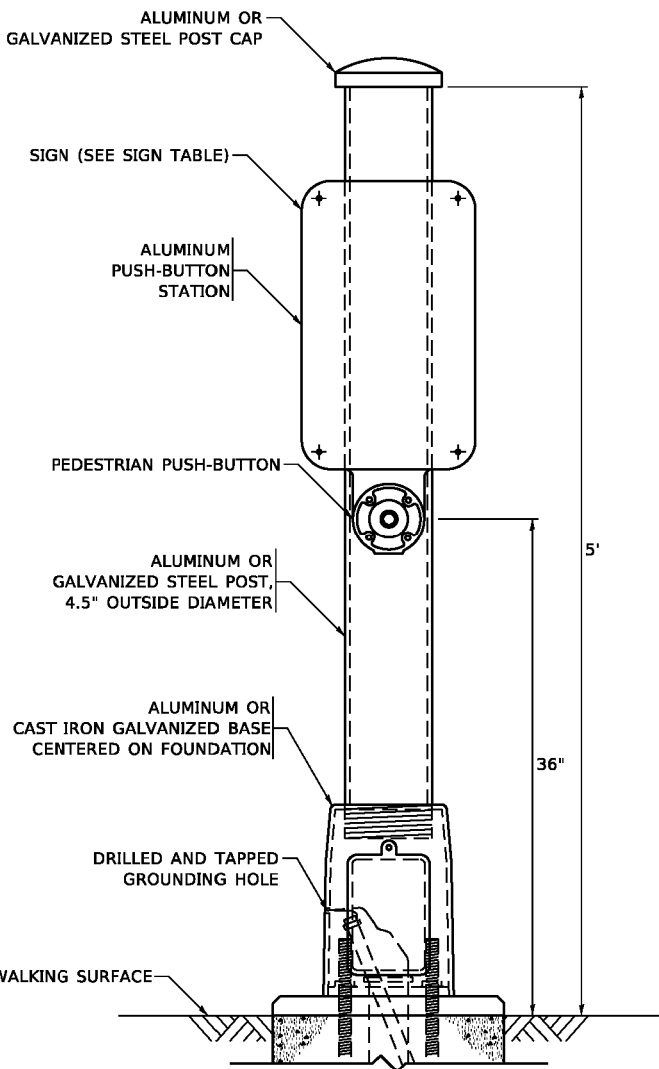
NOTE:
1. IF THE PEDESTRIAN SIGNAL POST FOUNDATION IS INSTALLED WITHIN OR BEHIND A BARRIER CURB, THE TOP OF THE FOUNDATION SHALL BE INSTALLED FLUSH WITH THE TOP OF THE BARRIER CURB.



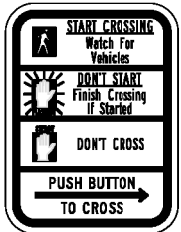
CONCRETE FOUNDATION, TYPE A 12-INCH DIAMETER



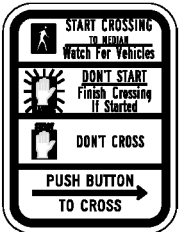
PEDESTRIAN SIGNAL POST, 10 FT.



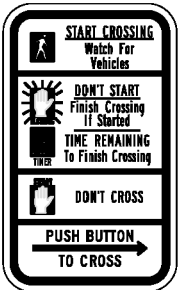
PEDESTRIAN SIGNAL POST, 5 FT.



R10-3b



R10-3d



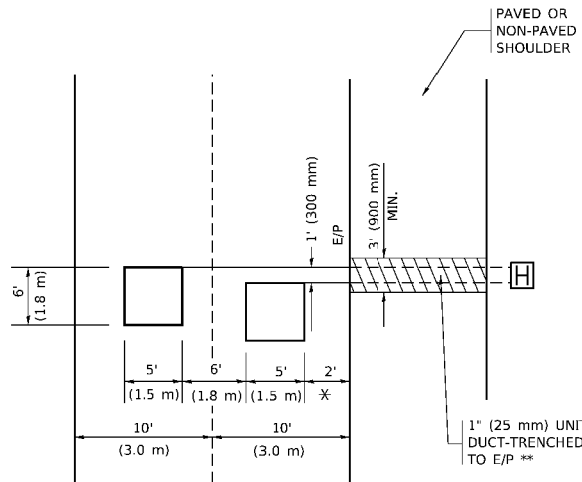
R10-3e

SIGN TABLE

SIGN	DIMENSIONS
R10-3b (RAILROAD ONLY)	9" X 12"
R10-3d (RAILROAD ONLY)	9" X 12"
R10-3e	9" X 12"

NOTES:
1. THE SIGN PANELS SHALL BE TYPE AP SHEETING.
2. THE ARROW ON SIGNS FOR PUSH-BUTTONS SERVING TWO DIRECTIONS ON THE SAME PHASE SHALL BE BI-DIRECTIONAL.
3. THE SIGN FOR DUAL-CALL PUSH-BUTTONS SHALL HAVE NO ARROW.

PROVIDE A PAVEMENT REPLACEMENT
NOTE WHICH SHOULD EQUAL
3' (900 mm) X WIDTH OF
PAVED SHOULDER.

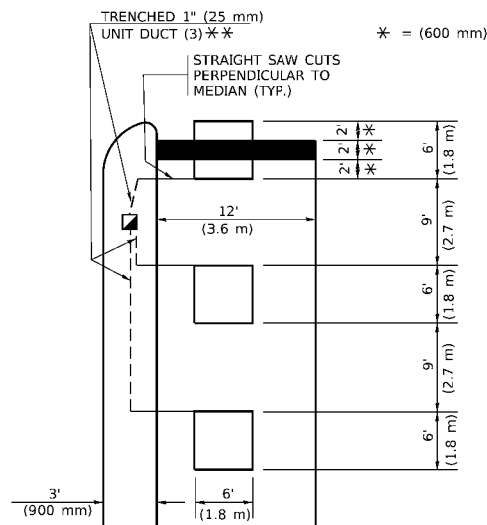


* = (600 mm)

* * UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY VARY DEPENDING ON GEOMETRICS AND DESIGN OF TRAFFIC SIGNALS. HEAVY-DUTY HANDHOLES TO BE USED WHEN THE MEDIAN IS MOUNTABLE. REFER TO STANDARD 814001 TO ENSURE THAT HANDHOLE FITS IN MEDIAN.

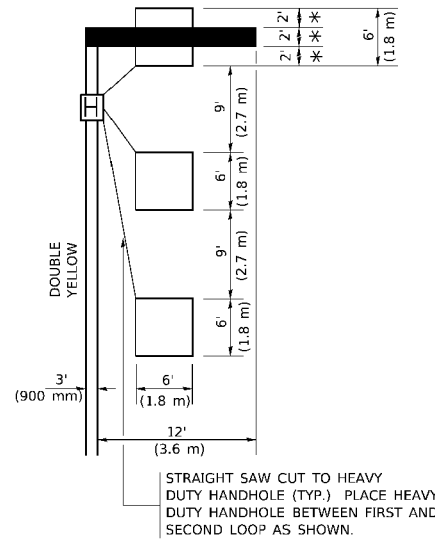


UNIT DUCT IS TO BE SHOWN ON PLAN SHEETS
BUT SHALL NOT BE INCLUDED IN THE PAY ITEMS.

NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

* = (600 mm)



NOTE: DUAL LEFT TURNS NOT SHOWN REFER TO
PLAN SHEET FOR DETECTOR LOOP REPLACEMENT

* = (1.8m)
 ** = (1.5m)

CROSS STREET

ARTERIAL

DO NOT INSTALL CALLING LOOP IN RIGHT TURN LANE.

DRIVEWAY

CALLING LOOPS

STRAIGHT SAW CUTS TO HEAVY-DUTY HANDHOLE IN PAVEMENT (TYP.)

250' (75m) DENSITY "FAR OUT" DETECTION

OFF SET LOOPS 1' (300mm) FOR STRAIGHT SAW CUTS

N.T.S.

DETAIL 1
N.T.S.

The diagram illustrates various street intersection configurations and dimensions:

- Top Left:** Shows a cross-section of a street with "OFFSET LOOPS BY 1' (300mm) FOR STRAIGHT SAW CUTS". A vertical dimension of 250' (75m) is indicated.
- Top Right:** Shows a street intersection with an "ARTERIAL" road. Dimensions include 3' (900mm), 1" (25 mm) UNIT DUCT (TYP.), and 3' (900mm).
- Middle Left:** Shows a street intersection with a "CROSS STREET". Dimensions include 11' (3.3m), 6' (1.8m), 9' (2.7m), and 3' (900mm).
- Middle Right:** Shows a street intersection with dimensions 10' (3.0m) PREFERRED and 15' (4.5m) MAXIMUM. It also includes dimensions like 6' (1.8m), 9' (2.7m), and 3' (900mm).
- Bottom Left:** Shows a street intersection with dimensions 11' (3.3m), 6' (1.8m), 9' (2.7m), and 3' (900mm). It also includes dimensions like 2' (600mm) and 3' (900mm).
- Bottom Right:** Shows a street intersection with a "DRIVEWAY". Dimensions include 6' (1.8m), 9' (2.7m), and 3' (900mm).

+ - THESE DIMENSIONS WILL BE VARIABLE

[6' (1.8m) MINIMUM, 25' (7.6 m) MAXIMUM]

Δ - THESE DIMENSIONS SHALL BE 5' (1.5m) FOR 10' (3.0m) LANE WIDTHS

IF "FAR OUT" LOOPS

DETAIL 2
N.T.S.

NOTES:

VEHICLES LOOP DETECTORS

- * ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE PAVEMENT TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- * EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATLY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- * ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- * EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- * WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- * WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1
TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER
FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE
DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS
FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

REMOVAL NOTES

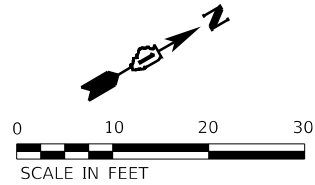
THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 1 EACH CONTROLLER AND CABINET (COMPLETE)
- 4 EACH STEEL MAST ARM ASSEMBLY AND POLE
- 12 EACH 3-SECTION SIGNAL HEAD
- 8 EACH PEDESTRIAN SIGNAL HEAD
- 6 EACH PEDESTRIAN PUSH BUTTON
- 2 EACH TRAFFIC SIGNAL POST
- 8 EACH TRAFFIC SIGNAL BACKPLATE
- 1 EACH SERVICE INSTALLATION

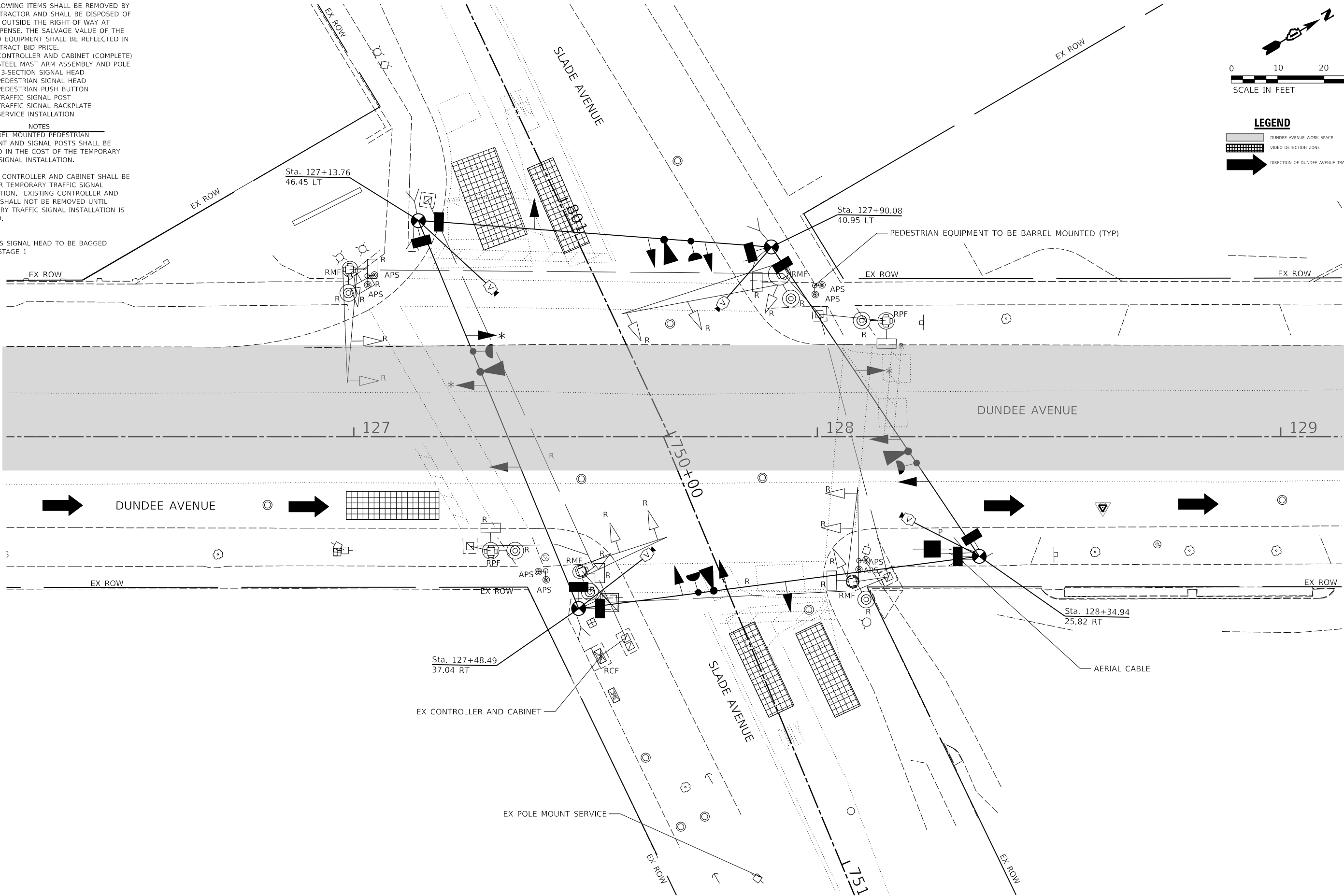
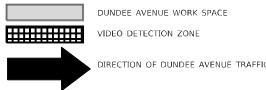
NOTES

- ALL BARREL MOUNTED PEDESTRIAN EQUIPMENT AND SIGNAL POSTS SHALL BE INCLUDED IN THE COST OF THE TEMPORARY TRAFFIC SIGNAL INSTALLATION.
- EXISTING CONTROLLER AND CABINET SHALL BE USED FOR TEMPORARY TRAFFIC SIGNAL INSTALLATION. EXISTING CONTROLLER AND CABINET SHALL NOT BE REMOVED UNTIL TEMPORARY TRAFFIC SIGNAL INSTALLATION IS REMOVED.

* INDICATES SIGNAL HEAD TO BE BAGGED DURING STAGE 1



LEGEND



USER NAME = jmarvig PLOT SCALE = 1:20
PLOT DATE = 11/5/2024 1:05:28 PM
MODEL Default
FILE NAME = H:\505K\Proj\EG_Elgin\2021EG2102.Dgn Final_EngEG2102-shr-us01.dgn

Engineering Enterprises, Inc.
CONSULTING ENGINEERS
52 Wheeler Road
Sugar Grove, Illinois 60554
630.466.6700 / www.eelweb.com

CITY OF ELGIN
150 DEXTER COURT
ELGIN, IL. 60120

DESIGNED -	REVISED -
DRAWN -	REVISED -
CHECKED -	REVISED -
DATE -	REVISED -

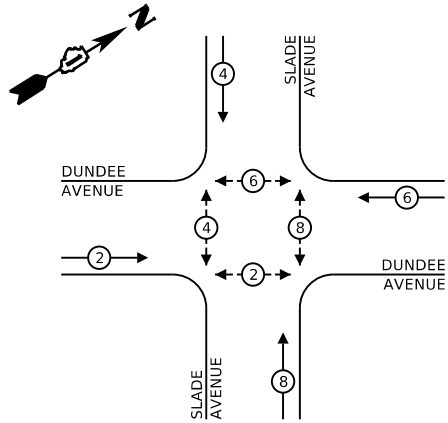
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DUNDEE AVENUE
TEMPORARY TRAFFIC SIGNAL INSTALLATION - STAGE 1
AND REMOVAL OF EXISTING TRAFFIC SIGNAL EQUIPMENT PLAN

SCALE: 1" = 10' SHEET 1 OF 7 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2525	20-00189-00-PV	KANE	184	105
CONTRACT NO. 61K68				
ILLINOIS FED. AID PROJECT				

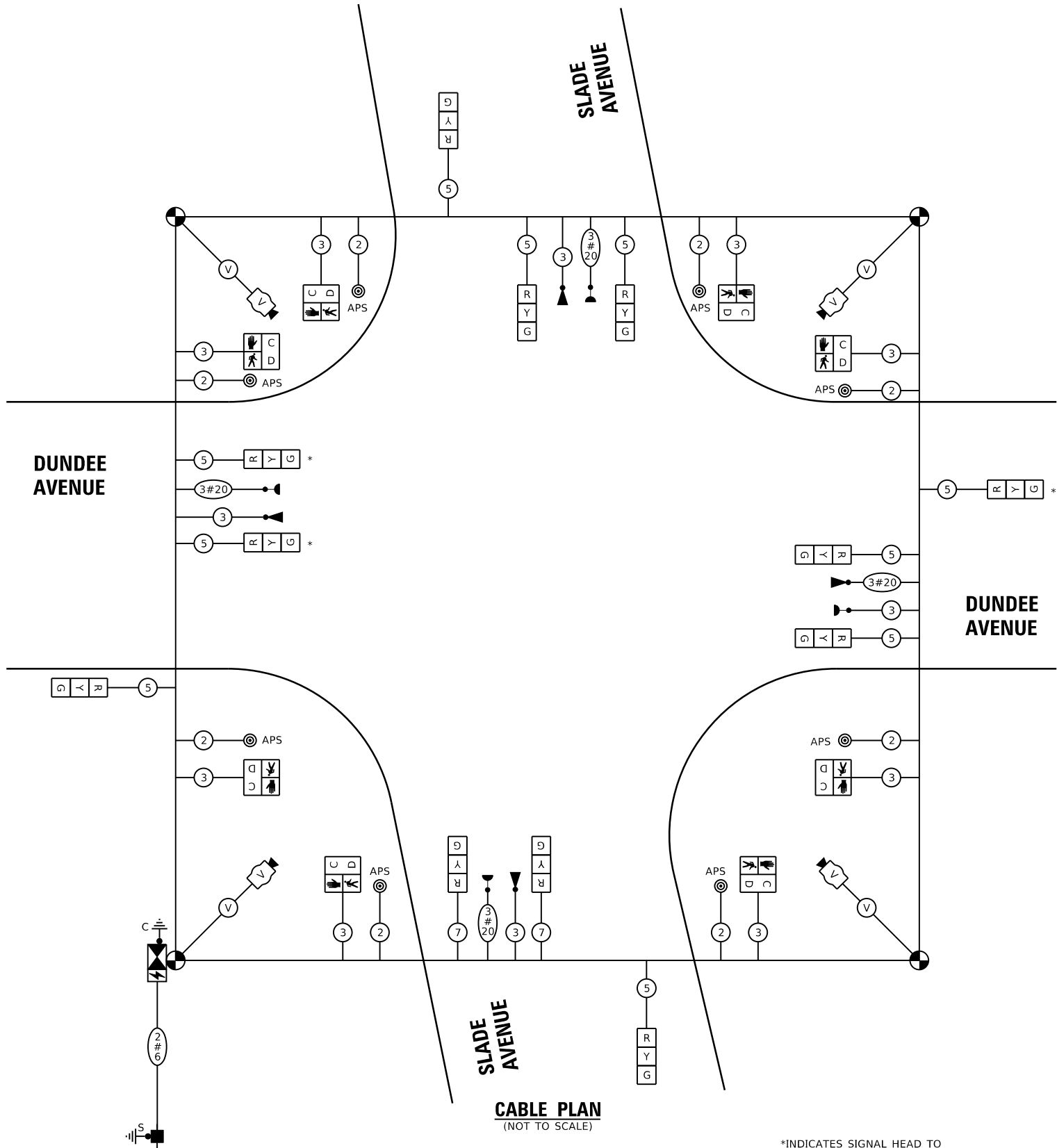
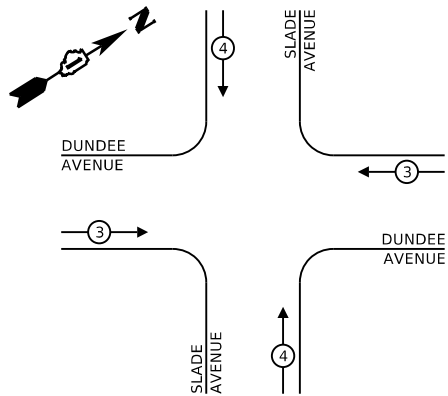
TEMPORARY CONTROLLER SEQUENCE



LEGEND:

- DUAL ENTRY PHASE
- SINGLE ENTRY PHASE
- PEDESTRIAN PHASE
- OVERLAP

TEMPORARY EMERGENCY VEHICLE
PREEMPTION SEQUENCE



CABLE PLAN
(NOT TO SCALE)

*INDICATES SIGNAL HEAD TO
BE BAGGED DURING STAGE 1

Traffic Signal Electric Service Requirements			
Type	Quantity	Unit Wattage	Total Wattage
Signal Head 3 - Section	12	11	132
4 - Section		14	
5 - Section		13	
Programmable Signals			
3 - Section		22	
4 - Section		32	
5 - Section		28	
Ped. Signal	8	15	120
Controller	1	150	150
Master Controller		100	
UPS		25	
Detection Radar		20	
Video	1	20	20
Blank-Out Sign		25	
Network Switch II or III		35	
Cellular Modem		15	
TOTAL UPS SIZING			422
UPS Charging		225	
Battery Heater Mat		180	
Cabinet Heater		200	
Flasher		15	
LED Street Name Sign		120	
Luminaire		240	0
TOTAL SERVICE WIRE SIZING			0

ENERGY COSTS TO:

CITY OF ELGIN
150 DEXTER COURT
ELGIN, IL 60120

ENERGY SUPPLY: CONTACT: -

PHONE: 1-866-639-3532

COMPANY: COMMONWEALTH EDISON

ACCOUNT NUMBER: -



Engineering Enterprises, Inc.
CONSULTING ENGINEERS
52 Wheeler Road
Sugar Grove, Illinois 60554
630.466.6700 / www.eefweb.com

CITY OF ELGIN
150 DEXTER COURT
ELGIN, IL 60120

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DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY CABLE PLAN, TEMPORARY PHASE DESIGNATION DIAGRAM,
AND TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE
STAGE 1 - DUNDEE AVENUE

SCALE: N.T.S. SHEET 2 OF 7 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2525	20-00189-00-PV	KANE	134	106
CONTRACT NO. 61K68				

ILLINOIS FED. AID PROJECT

REMOVAL NOTES

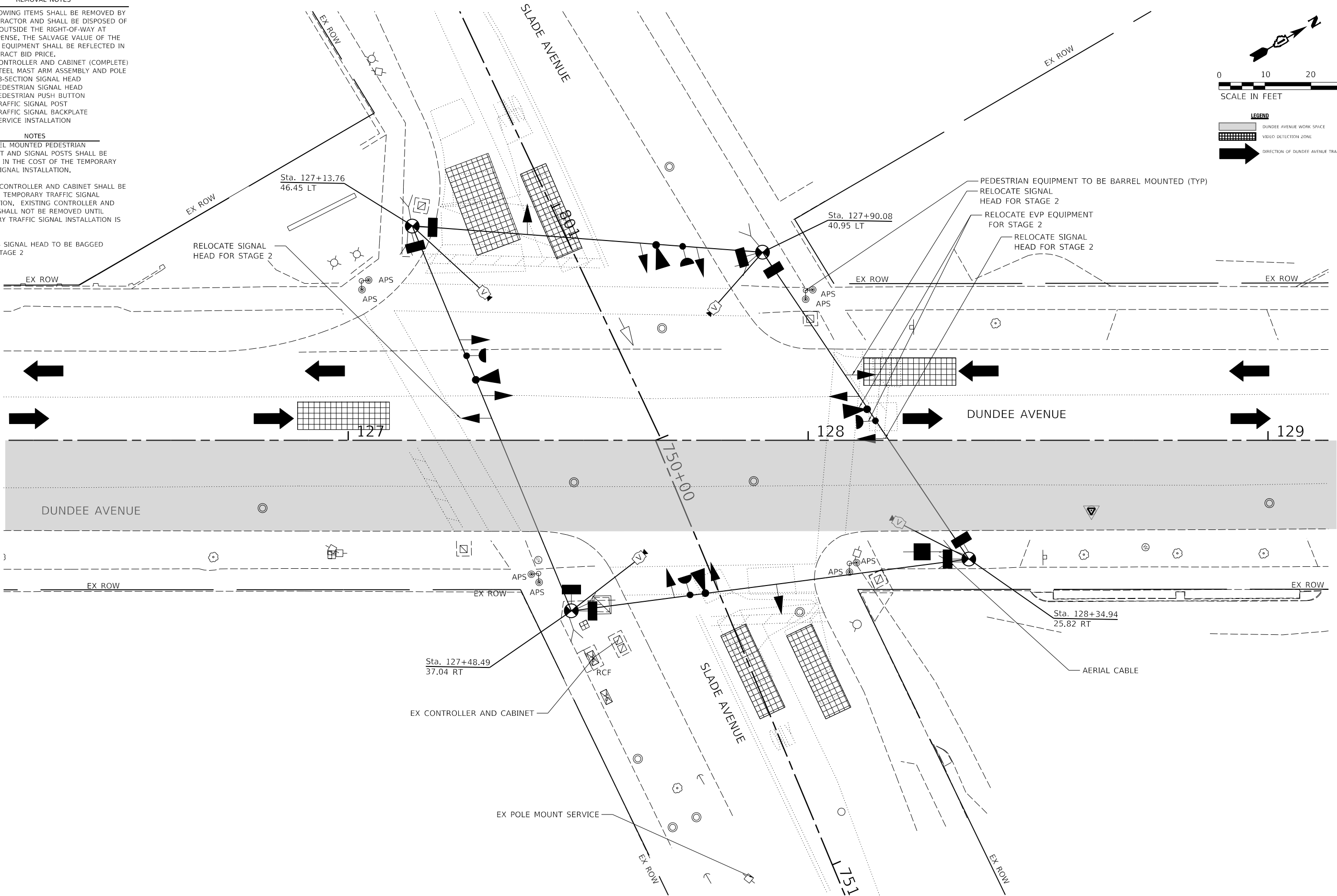
THE FOLLOWING ITEMS SHALL BE REMOVED BY THE CONTRACTOR AND SHALL BE DISPOSED OF BY THEM OUTSIDE THE RIGHT-OF-WAY AT THEIR EXPENSE. THE SALVAGE VALUE OF THE REMOVED EQUIPMENT SHALL BE REFLECTED IN THE CONTRACT BID PRICE.

- 1 EACH CONTROLLER AND CABINET (COMPLETE)
- 4 EACH STEEL MAST ARM ASSEMBLY AND POLE
- 12 EACH 3-SECTION SIGNAL HEAD
- 8 EACH PEDESTRIAN SIGNAL HEAD
- 6 EACH PEDESTRIAN PUSH BUTTON
- 2 EACH TRAFFIC SIGNAL POST
- 8 EACH TRAFFIC SIGNAL BACKPLATE
- 1 EACH SERVICE INSTALLATION

NOTES

- ALL BARREL MOUNTED PEDESTRIAN EQUIPMENT AND SIGNAL POSTS SHALL BE INCLUDED IN THE COST OF THE TEMPORARY TRAFFIC SIGNAL INSTALLATION.
- EXISTING CONTROLLER AND CABINET SHALL BE USED FOR TEMPORARY TRAFFIC SIGNAL INSTALLATION. EXISTING CONTROLLER AND CABINET SHALL NOT BE REMOVED UNTIL TEMPORARY TRAFFIC SIGNAL INSTALLATION IS REMOVED.

* INDICATES SIGNAL HEAD TO BE BAGGED DURING STAGE 2



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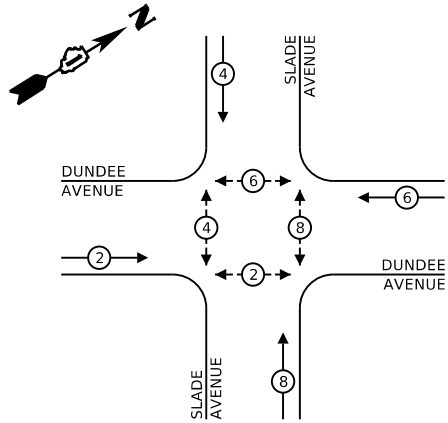
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DUNDEE AVENUE
TEMPORARY TRAFFIC SIGNAL INSTALLATION – STAGE 2

SCALE: 1" = 10' SHEET 3 OF 7 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2525	20-00189-00-PV	KANE	184	107
CONTRACT NO. 61K68				
ILLINOIS FED. AID PROJECT				

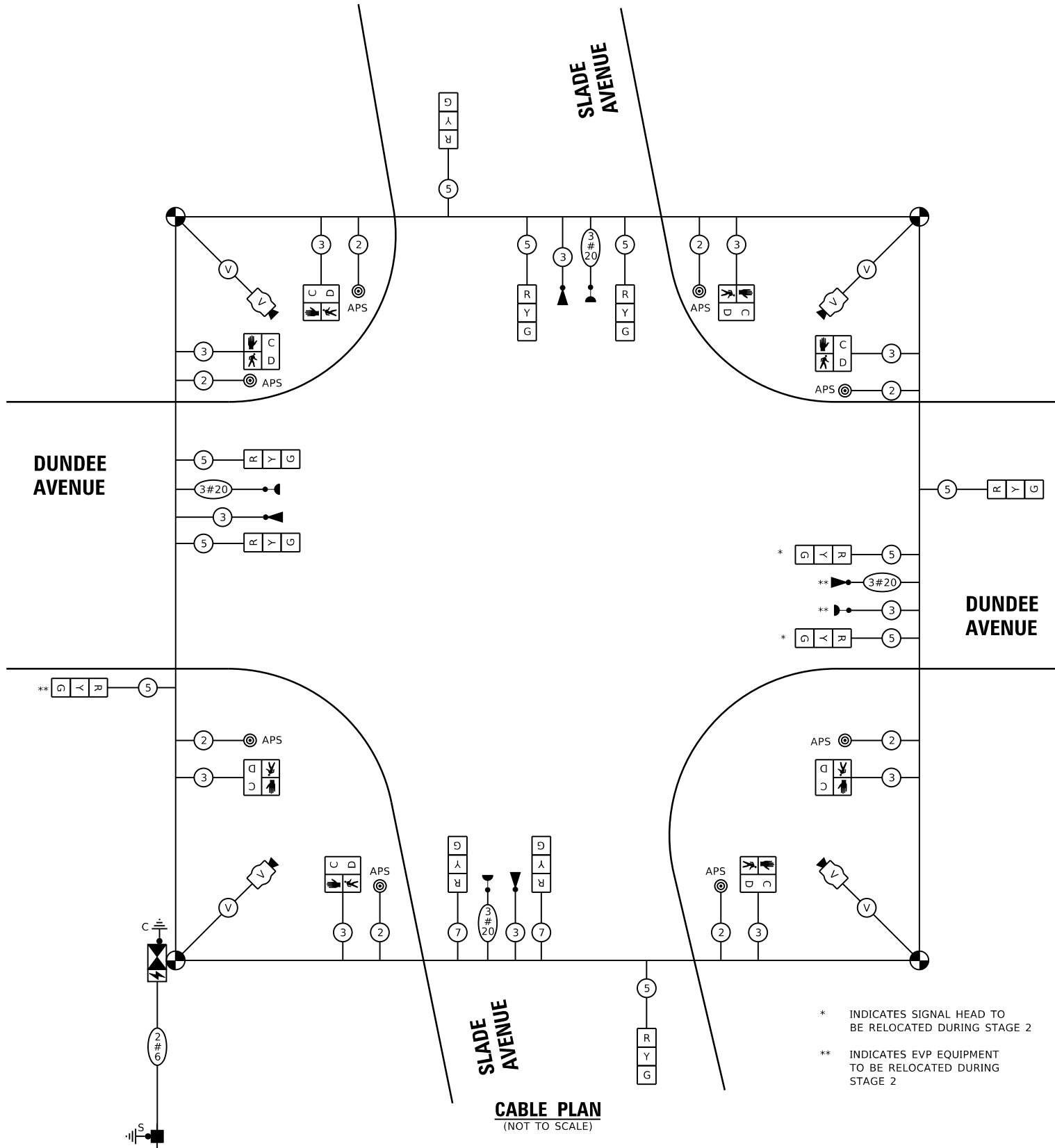
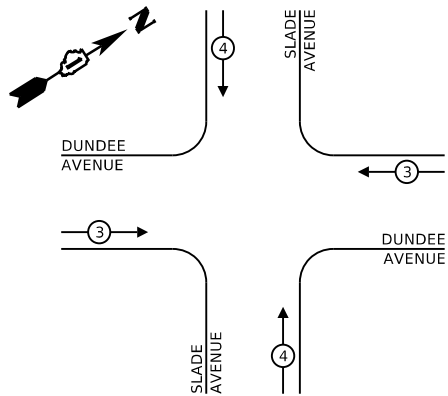
TEMPORARY CONTROLLER SEQUENCE



LEGEND:

- DUAL ENTRY PHASE
- SINGLE ENTRY PHASE
- PEDESTRIAN PHASE
- OVERLAP

TEMPORARY EMERGENCY VEHICLE
PREEMPTION SEQUENCE



- * INDICATES SIGNAL HEAD TO BE RELOCATED DURING STAGE 2
- ** INDICATES EVP EQUIPMENT TO BE RELOCATED DURING STAGE 2

Traffic Signal Electric Service Requirements			
Type	Quantity	Unit Wattage	Total Wattage
Signal Head 3 - Section	12	11	132
4 - Section		14	
5 - Section		13	
Programmable Signals			
3 - Section		22	
4 - Section		32	
5 - Section		28	
Ped. Signal	8	15	120
Controller	1	150	150
Master Controller		100	
UPS		25	
Detection Radar		20	
Video	1	20	20
Blank-Out Sign		25	
Network Switch II or III		35	
Cellular Modem		15	
TOTAL UPS SIZING			422
UPS Charging		225	
Battery Heater Mat		180	
Cabinet Heater		200	
Flasher		15	
LED Street Name Sign		120	
Luminaire		240	0
TOTAL SERVICE WIRE SIZING			0

ENERGY COSTS TO:

CITY OF ELGIN
150 DEXTER COURT
ELGIN, IL 60120

ENERGY SUPPLY: CONTACT: -

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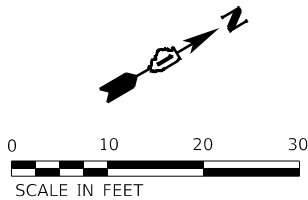
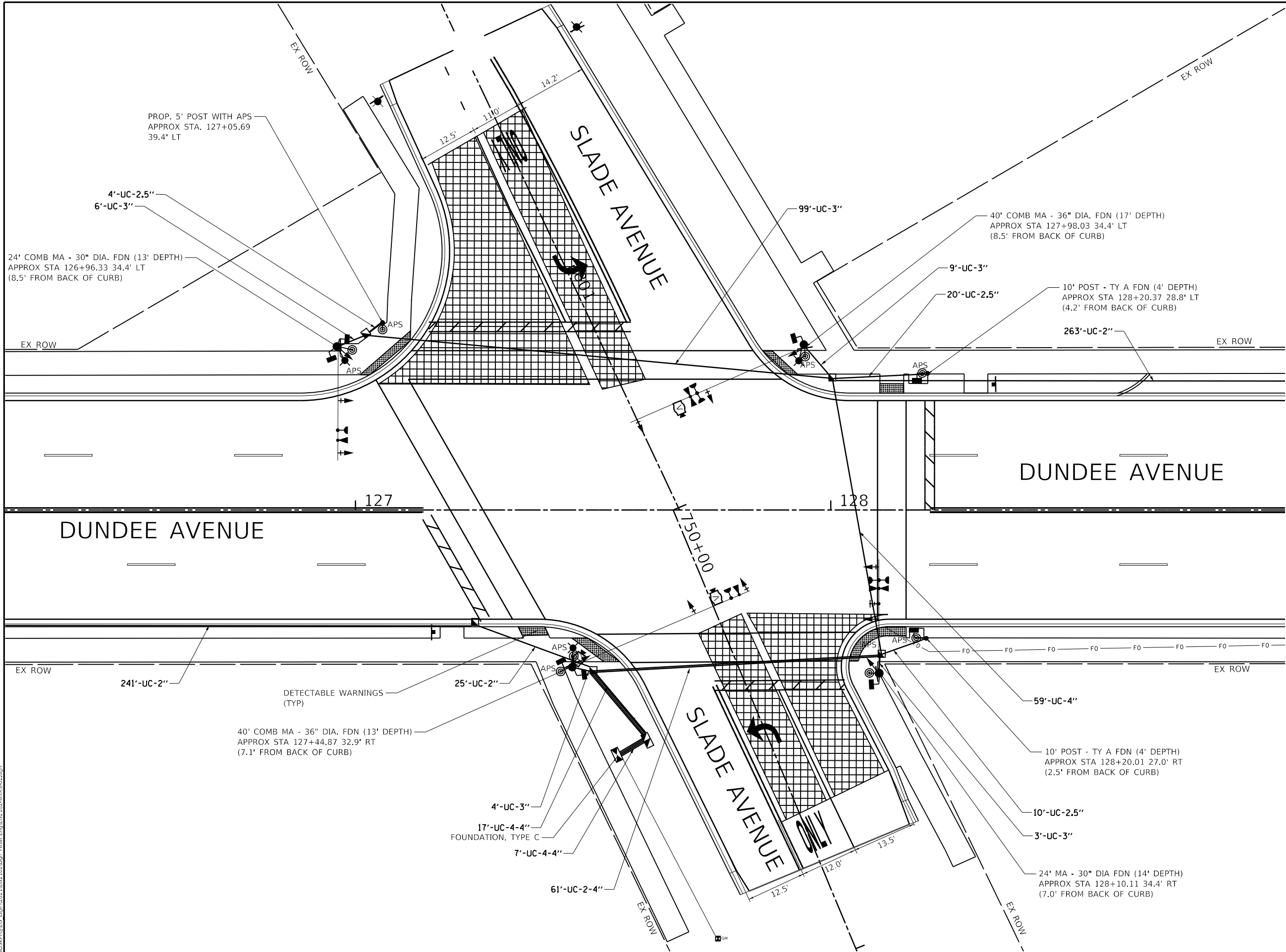
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TEMPORARY CABLE PLAN, TEMPORARY PHASE DESIGNATION DIAGRAM,
AND TEMPORARY EMERGENCY VEHICLE PREEMPTION SEQUENCE
STAGE 2 – DUNDEE AVENUE

SCALE: N.T.S. SHEET 4 OF 7 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2525	20-00189-00-PV	KANE	134	108
CONTRACT NO. 61K68				

ILLINOIS FED. AID PROJECT

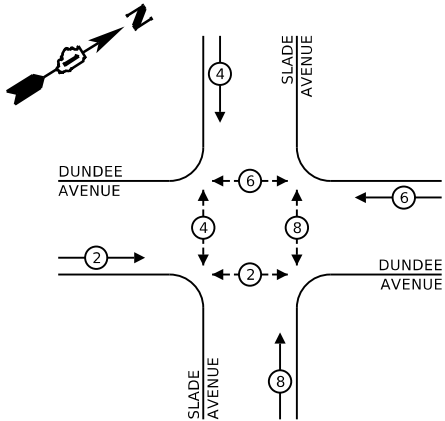


- NOTES:
1. ALL TRAFFIC SIGNAL POSTS, COMBINATION MAST ARM ASSEMBLIES AND POLES, AND PEDESTRIAN PUSH BUTTON POSTS SHALL BE PAINTED BLACK.
 2. THE VIDEO DETECTION EQUIPMENT SHALL BE ITERIS VANTAGE NEXT AS INDICATED IN THE SPECIFICATIONS.
 3. THE CONTROLLER SHALL CONFORM TO THE ITE ATC VERSION 6. CONTROLLERS SHALL BE NTCIP COMPLIANT EAGLE/SIEMENS M60. THE CONTROLLER SHALL BE OF THE MOST RECENT MODEL AND SOFTWARE VERSION SUPPLIED BY THE EQUIPMENT SUPPLIER AT THE TIME OF THE TRAFFIC SIGNAL TURN-ON. A REMOVABLE CONTROLLER DATA KEY SHALL ALSO BE PROVIDED. THE CONTROLLER SHALL HAVE A MINIMUM OF 4 RJ-45 ETHERNET 2 USB PORTS. THE CONTROLLER SHALL HAVE THE LATEST VERSION OF NTCIP SOFTWARE INSTALLED.
 4. VIDEO DETECTION CAMERAS SHALL BE MOUNTED ON RISER BRACKETS AT 8 FEET IN LENGTH. FINAL CAMERA LOCATION SHALL BE OPTIMAL TO MINIMIZE OCCLUSION BASED FALSE CALLS FROM TRAFFIC IN ADJACENT LANES.
 5. VIDEO DETECTION ZONES SHALL BE PROVIDED AND PROGRAMMED FOR COUNTING VEHICLES.

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<div><div>50 YEARS</div><div></div></div> <div>Engineering Enterprises, Inc. CONSULTING ENGINEERS 52 Wheeler Road Sugar Grove, Illinois 60554 630.466.6700 / www.eelweb.com</div>	<div>CITY OF ELGIN 150 DEXTER COURT ELGIN, IL. 60120</div>	DESIGNED -	REVISED -	<div>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</div>	<div>DUNDEE AVENUE TRAFFIC SIGNAL MODIFICATION PLAN</div>				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -		2525	20-00189-00-PV	KANE	184	109				
		CHECKED -	REVISED -		CONTRACT NO. 61K68								
		DATE -	REVISED -		ILLINOIS FED. AID PROJECT								
					SCALE: 1" = 10'	SHEET 5	OF 7 SHEETS	STA.	TO STA.				

CONTROLLER SEQUENCE

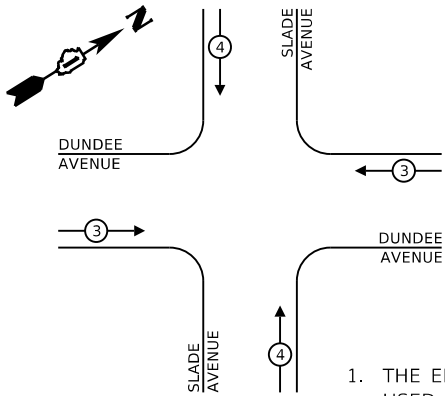


LEGEND:

- DUAL ENTRY PHASE
- SINGLE ENTRY PHASE
- PEDESTRIAN PHASE
- OVERLAP

NOTE: PHASE 2 AND PHASE 6 SHALL BE ON RECALL.

EMERGENCY VEHICLE
PREEMPTION SEQUENCE



NOTES

- THE EMERGENCY VEHICLE PREEMPTION EQUIPMENT USED FOR THIS PROJECT SHALL BE "TOMAR"
- THE CONTROLLER FOR THIS PROJECT SHALL BE SIEMENS/EAGLE
- THE WIRELESS ANTENNA SHALL BE UBIQUITI AND THE NETWORK SWITCHES SHALL BE EXTREME NETWORKS
- THE WIRELESS ANTENNA SHALL BE POINTED TOWARDS ELGIN TOWER IN DOWNTOWN ELGIN

Traffic Signal Electric Service Requirements			
Type	Quantity	Unit Wattage	Total Wattage
Signal Head 3- Section	12	11	132
4- Section		14	
5- Section		13	
Programmable Signals			
3- Section		22	
4- Section		32	
5- Section		28	
Ped. Signal	8	15	120
Controller	1	150	150
Master Controller		100	
UPS	1	25	25
Detection Radar		20	
Video	1	20	20
Blank-Out Sign		25	
Network Switch II or III		35	
Cellular Modem	1	15	15
TOTAL UPS SIZING			462
UPS Charging		225	
Battery Heater Mat		180	
Cabinet Heater		200	
Flasher		15	
LED Street Name Sign		120	
Luminaire	3	240	720
TOTAL SERVICE WIRE SIZING			720

ENERGY COSTS TO:

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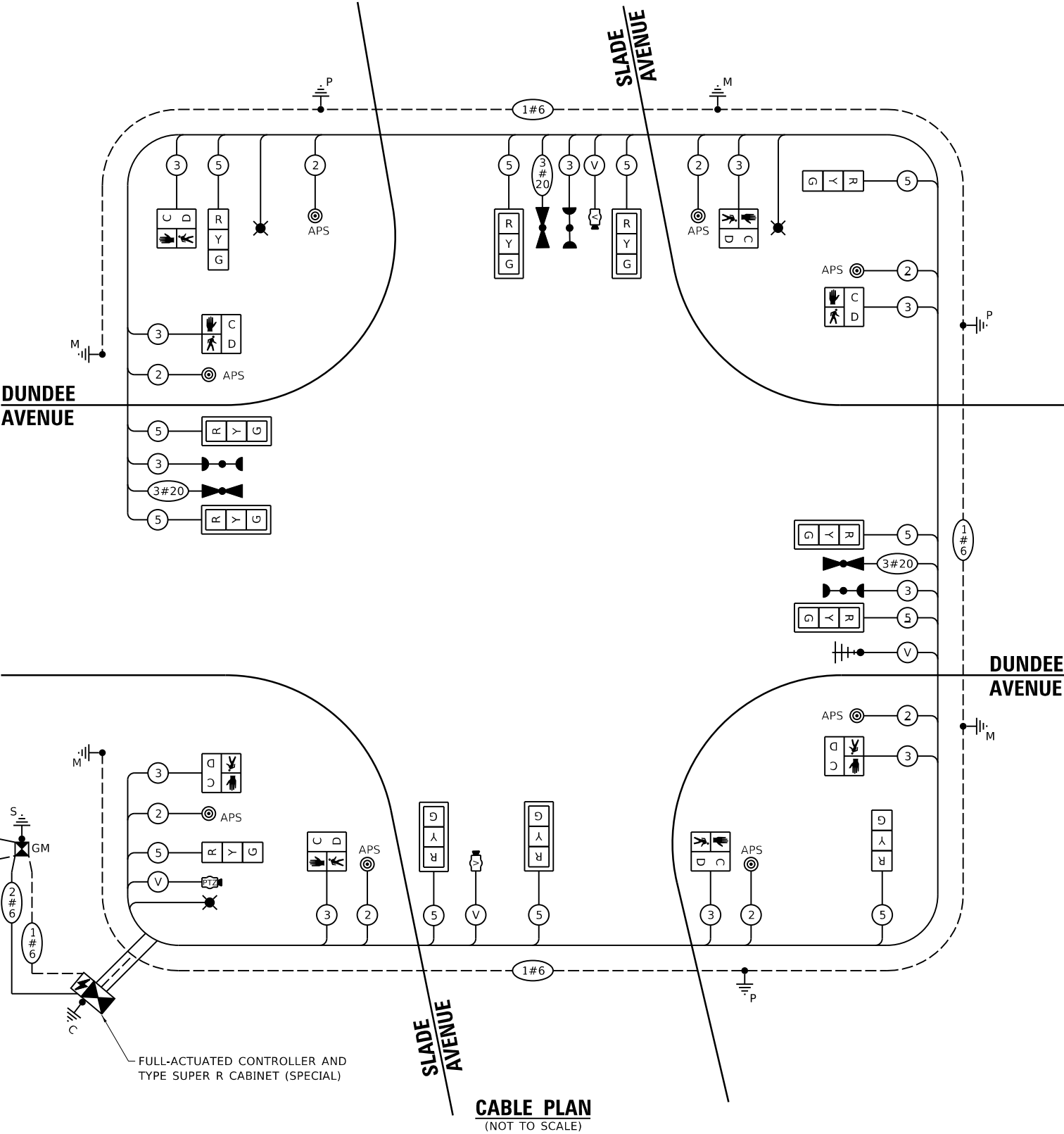
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DUNDEE AVENUE
CABLE PLAN, PHASE DESIGNATION DIAGRAM, EVP SEQUENCE

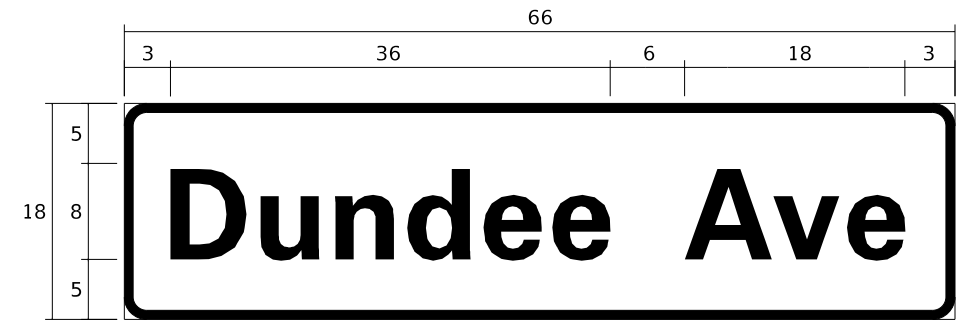
SCALE: N.T.S. SHEET 6 OF 7 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2525	20-00189-00-PV	KANE	184	110
CONTRACT NO. 61K68				
ILLINOIS FED. AID PROJECT				

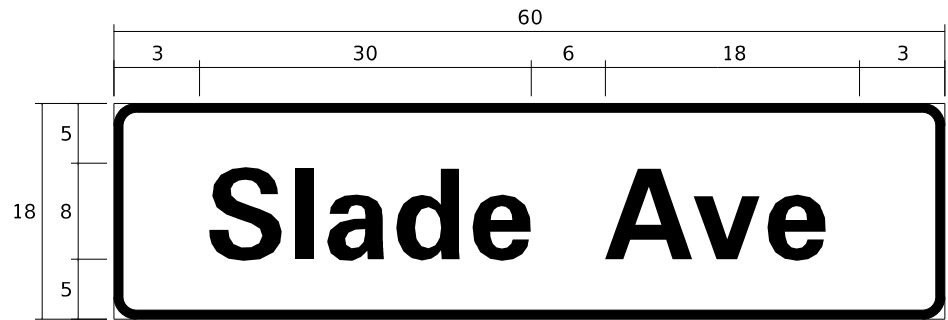


SIGN PANEL – TYPE 1 OR TYPE 2

ALL DIMENSIONS ARE IN INCHES UNLESS NOTED OTHERWISE



DESIGN SERIES	AREA (SQ FT)	SIGN PANEL TYPE	SHEETING TYPE	QTY REQUIRED
D	8.25	1	ZZ	2

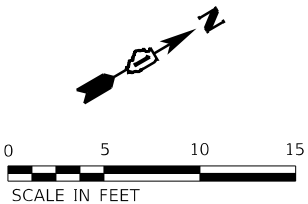
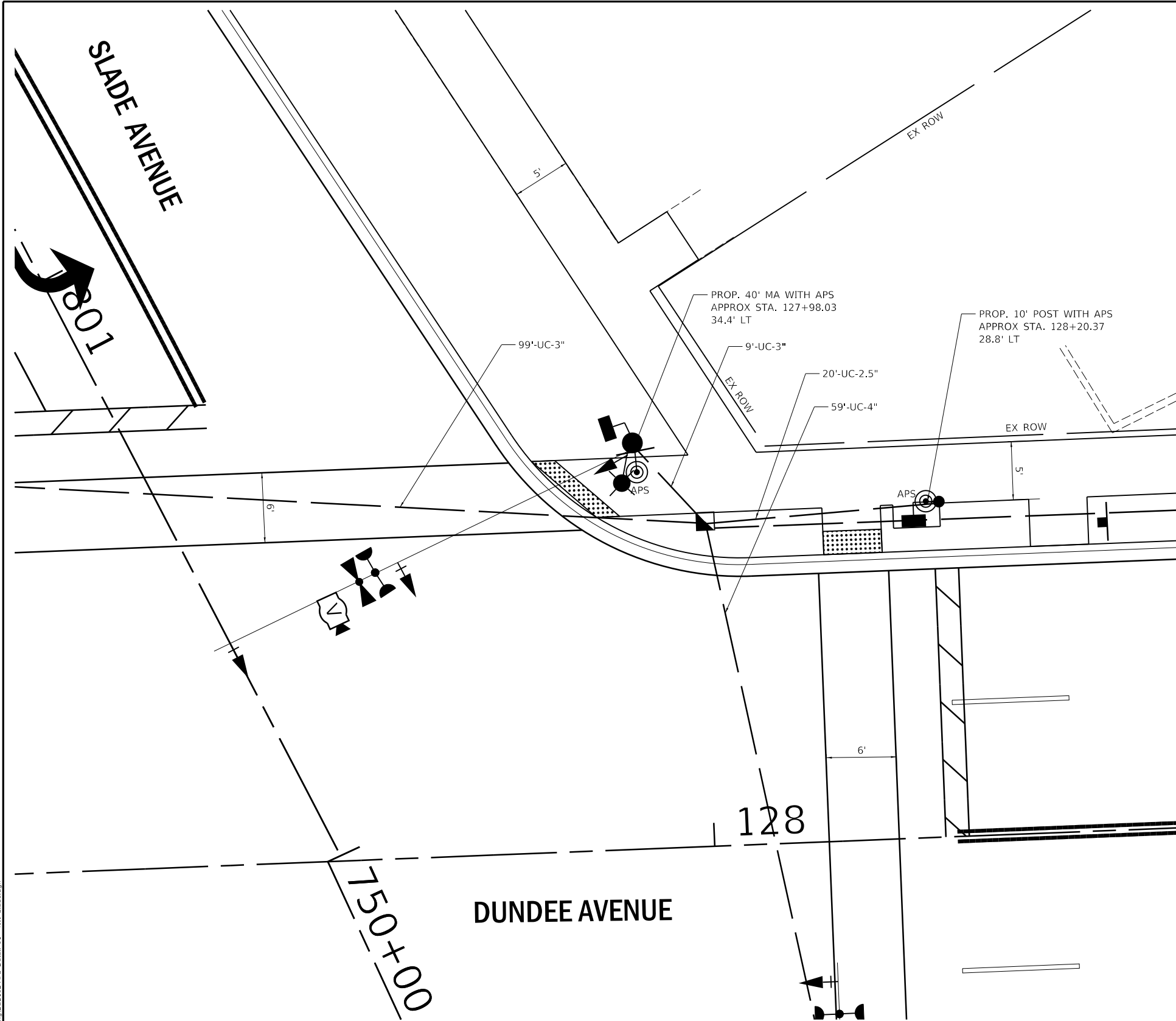


DESIGN SERIES	AREA (SQ FT)	SIGN PANEL TYPE	SHEETING TYPE	QTY REQUIRED
D	7.5	1	ZZ	2

NOTE: FOR ADDITIONAL DESIGN AND INSTALLATION INFORMATION PLEASE SEE DISTRICT ONE MAST ARM MOUNTED STREET NAME SIGNS DETAIL.

SCHEDULE OF QUANTITIES

ITEM DESCRIPTION	UNIT	TOTAL QTY.
SIGN PANEL - TYPE 1	SQ FT	31.5
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2" DIA.	FOOT	838
UNDERGROUND CONDUIT, GALVANIZED STEEL, 2 1/2" DIA.	FOOT	34
UNDERGROUND CONDUIT, GALVANIZED STEEL, 3" DIA.	FOOT	121
UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	1,191
HANDHOLE	EACH	9
DOUBLE HANDHOLE	EACH	1
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 2C	FOOT	2,905
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 3C	FOOT	2,240
ELECTRIC CABLE IN CONDUIT, SIGNAL NO. 14 5C	FOOT	2,359
ELECTRIC CABLE IN CONDUIT, SERVICE, NO. 6 2C	FOOT	50
ELECTRIC CABLE IN CONDUIT, EQUIPMENT GROUNDING CONDUCTOR, NO. 6 1C	FOOT	1,055
TRAFFIC SIGNAL POST, 10 FT	EACH	2
STEEL MAST ARM ASSEMBLY AND POLE, 24 FT	EACH	1
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE, 24 FT	EACH	1
STEEL COMBINATION MAST ARM ASSEMBLY AND POLE, 40 FT	EACH	2
CONCRETE FOUNDATION, TYPE A	FOOT	28
CONCRETE FOUNDATION, TYPE C	FOOT	3
CONCRETE FOUNDATION, TYPE E 30-INCH DIAMETER	FOOT	27
CONCRETE FOUNDATION, TYPE E 36-INCH DIAMETER	FOOT	30
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, MAST-ARM MOUNTED	EACH	8
SIGNAL HEAD, LED, 1-FACE, 3-SECTION, BRACKET MOUNTED	EACH	4
PEDESTRIAN SIGNAL HEAD, LED, 1-FACE, POST MOUNTED WITH COUNTDOWN TIMER	EACH	8
TRAFFIC SIGNAL BACKPLATE, LOUVERED, FORMED PLASTIC	EACH	8
DETECTOR LOOP, TYPE I	FOOT	96
LIGHT DETECTOR	EACH	4
TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1
REMOVE EXISTING SERVICE INSTALLATION	EACH	1
REMOVE EXISTING CABLE FROM CONDUIT	FOOT	6,000
REMOVE EXISTING TRAFFIC SIGNAL EQUIPMENT	EACH	1
REMOVE EXISTING HANDHOLE	EACH	9
REMOVE EXISTING DOUBLE HANDHOLE	EACH	1
REMOVE EXISTING CONCRETE FOUNDATION	EACH	11
EMERGENCY VEHICLE PRIORITY SYSTEM LINE SENSOR CABLE, NO. 20 3C	FOOT	266
OUTDOOR RATED NETWORK CABLE	FOOT	210
SERVICE INSTALLATION, GROUND MOUNTED, METERED	EACH	1
WIRELESS TRANSMISSION SYSTEM POINT TO POINT	EACH	1
PEDESTRIAN SIGNAL POST, 5 FT.	EACH	1
MAINTENANCE OF TEMPORARY TRAFFIC SIGNAL INSTALLATION	EACH	1
FULL-ACTUATED CONTROLLER AND TYPE SUPER R CABINET (SPECIAL)	EACH	1
UNINTERRUPTABLE POWER SUPPLY (SPECIAL)	EACH	1
ACCESSIBLE PEDESTRIAN SIGNALS	EACH	8
CONCRETE FOUNDATION, TYPE A 12-INCH DIAMETER	EACH	4
EMERGENCY VEHICLE PRIORITY SYSTEM	EACH	1
REMOVE EXISTING UNDERGROUND CONDUIT	FOOT	404
VIDEO DETECTION SYSTEM COMPLETE INTERSECTION	EACH	1
REMOTE CONTROLLED VIDEO SYSTEM	EACH	1
ETHERNET SWITCH, TYPE 1	EACH	1
CAMERA MOUNTING ASSEMBLY	EACH	1
RE-OPTIMIZE TRAFFIC SIGNAL SYSTEM LEVEL 1	EACH	1
OPTIMIZE TRAFFIC SIGNAL SYSTEM	EACH	1
TEMPORARY TRAFFIC SIGNAL TIMING	EACH	1



LEGEND

- ⊙ PROP PEDESTRIAN PUSH BUTTON
- PROP UNDERGROUND CONDUIT
- ▤ PROP ADA DETECTABLE WARNING
- ◼ PROP HANDHOLE
- PROP TRAFFIC SIGNAL POST
- +▶ PROP TRAFFIC SIGNAL HEAD
- PROP TRAFFIC STREET SIGN

- GENERAL NOTES**
- ALL STATION AND OFFSETS ARE REFERENCED TO THE DUNDEE AVENUE CENTERLINE ALIGNMENT
 - ALL PEDESTRIAN PUSH BUTTON POSTS SHALL BE LOCATED AS SHOWN IN THE PLANS.
 - ALL PEDESTRIAN PUSH BUTTONS SHALL BE WITHIN 10 FT. FROM THE BACK OF THE CURB OR THE EDGE OF PAVEMENT FOR EACH CROSSING. THE CONTRACTOR SHALL INSTALL PEDESTRIAN PUSH BUTTON EXTENSIONS TO MEET THIS REQUIREMENT.
 - MOUNTING HEIGHT OF PEDESTRIAN PUSH BUTTONS SHALL BE 36 INCHES (PREFERRED) FROM THE SIDEWALK SURFACE.
 - SHOP DRAWINGS FOR ALL SIGNAL IMPROVEMENTS INCLUDING APS IMPROVEMENTS SHALL BE PROVIDED TO THE CITY OF ELGIN.
 - A WALK THROUGH AND FINAL INSPECTION OF THE TRAFFIC SIGNAL IMPROVEMENTS SHALL BE COORDINATED WITH THE ENGINEER.
 - THE INSTALLATION OF ACESIBLE PEDESTRIAN SIGNALS SHALL INCLUDE THE INTERACTIVE VIBROTACTILE PEDESTRIAN PUSH BUTTON WITH SPEAKER, AN INFORMATIONAL SIGN, A LIGHT-EMITTING DIODE (LED) INDICATOR LIGHT, A SOLID-STATE ELECTRONIC CONTROL BOARD, A POWER SUPPLY, INTERNAL WIRING AND MOUNTING HARDWARE.
 - THE INFORMATIONAL SIGN SHALL CONFORM TO THE MUTCD STANDARD R10-3-1 (BRAILLE SIGN). THE SELECTION OF THE SIGN SHALL BE APPROVED BY THE ENGINEER.

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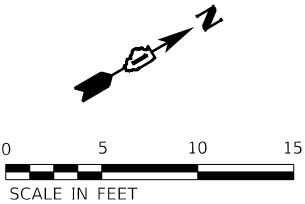
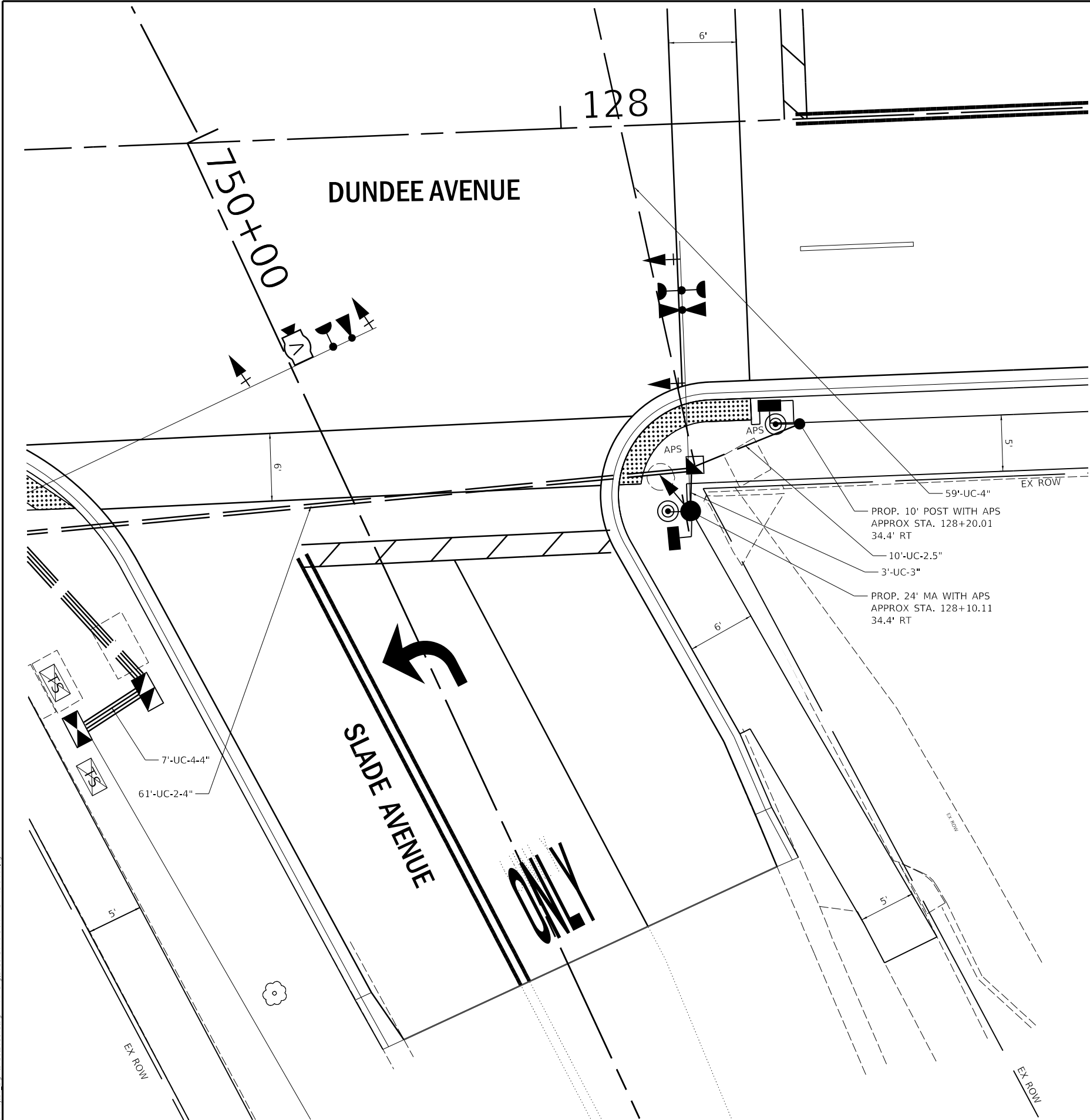
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DUNDEE AVENUE

NORTHWEST CORNER ACCESSIBLE PEDESTRIAN SIGNAL DETAIL

SCALE: 1" = 5' SHEET 1 OF 4 SHEETS STA. N/A TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2525	20-00189-00-PV	KANE	184	112
CONTRACT NO. 61K68				
ILLINOIS FED. AID PROJECT				



LEGEND

- ⊙ PROP PEDESTRIAN PUSH BUTTON
- PROP UNDERGROUND CONDUIT
- ▤ PROP ADA DETECTABLE WARNING
- ◼ PROP HANDHOLE
- PROP TRAFFIC SIGNAL POST
- ▶ PROP TRAFFIC SIGNAL HEAD
- PROP TRAFFIC STREET SIGN

GENERAL NOTES

- ALL STATION AND OFFSETS ARE REFERENCED TO THE DUNDEE AVENUE CENTERLINE ALIGNMENT
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- MOUNTING HEIGHT OF PEDESTRIAN PUSH BUTTONS SHALL BE 36 INCHES (PREFERRED) FROM THE SIDEWALK SURFACE.
- SHOP DRAWINGS FOR ALL SIGNAL IMPROVEMENTS INCLUDING APS IMPROVEMENTS SHALL BE PROVIDED TO THE CITY OF ELGIN.
- A WALK THROUGH AND FINAL INSPECTION OF THE TRAFFIC SIGNAL IMPROVEMENTS SHALL BE COORDINATED WITH THE ENGINEER.
- THE INSTALLATION OF ACCESSIBLE PEDESTRIAN SIGNALS SHALL INCLUDE THE INTERACTIVE VIBROTACTILE PEDESTRIAN PUSH BUTTON WITH SPEAKER, AN INFORMATIONAL SIGN, A LIGHT-EMITTING DIODE (LED) INDICATOR LIGHT, A SOLID-STATE ELECTRONIC CONTROL BOARD, A POWER SUPPLY, INTERNAL WIRING AND MOUNTING HARDWARE.
- THE INFORMATIONAL SIGN SHALL CONFORM TO THE MUTCD STANDARD R10-3-i (BRAILLE SIGN). THE SELECTION OF THE SIGN SHALL BE APPROVED BY THE ENGINEER.

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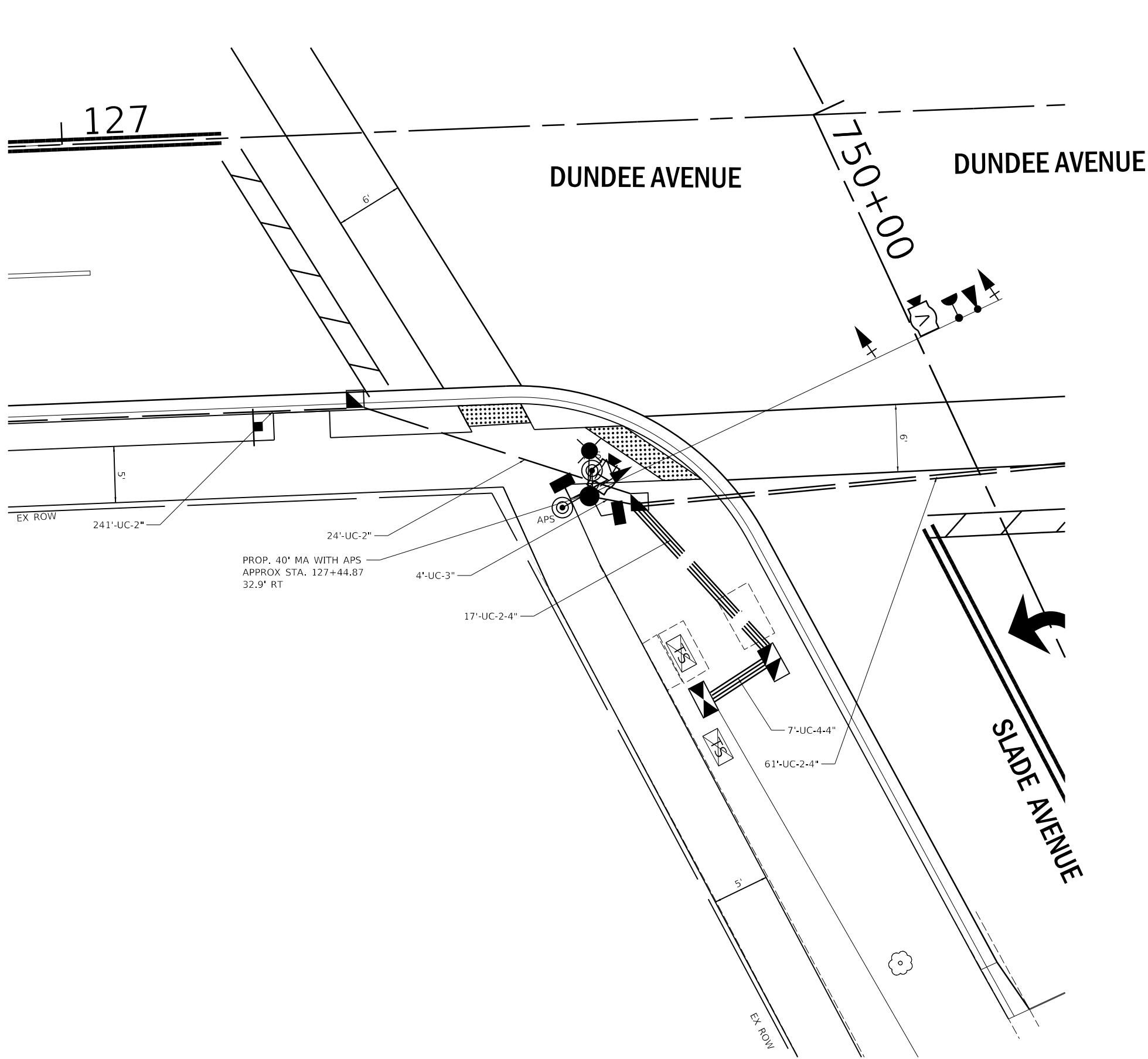
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DUNDEE AVENUE

NORTHEAST CORNER ACCESSIBLE PEDESTRIAN SIGNAL DETAIL

SCALE: 1" = 5' SHEET 2 OF 4 SHEETS STA. N/A TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2525	20-00189-00-PV	KANE	184	113
CONTRACT NO. 61K68				
ILLINOIS FED. AID PROJECT				



LEGEND

- ⊙ PROP PEDESTRIAN PUSH BUTTON
- PROP UNDERGROUND CONDUIT
- ▤ PROP ADA DETECTABLE WARNING
- ◼ PROP HANDHOLE
- PROP TRAFFIC SIGNAL POST
- +▶ PROP TRAFFIC SIGNAL HEAD
- PROP TRAFFIC STREET SIGN

- GENERAL NOTES**
- ALL STATION AND OFFSETS ARE REFERENCED TO THE DUNDEE AVENUE CENTERLINE ALIGNMENT
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 - THE INFORMATIONAL SIGN SHALL CONFORM TO THE MUTCD STANDARD R10-3-i (BRAILLE SIGN). THE SELECTION OF THE SIGN SHALL BE APPROVED BY THE ENGINEER.

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CITY OF ELGIN
150 DEXTER COURT
ELGIN, IL. 60120

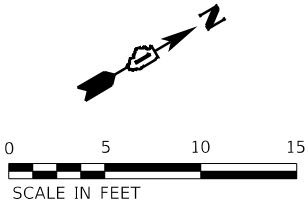
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DUNDEE AVENUE
SOUTHEAST CORNER ACCESSIBLE PEDESTRIAN SIGNAL DETAIL

SCALE: 1" = 5' SHEET 3 OF 4 SHEETS STA. N/A TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2525	20-00189-00-PV	KANE	184	114
CONTRACT NO. 61K68				
ILLINOIS FED. AID PROJECT				



LEGEND

PROP PEDESTRIAN PUSH BUTTON

PROP UNDERGROUND CONDUIT

PROP ADA DETECTABLE WARNING

PROP HANDHOLE

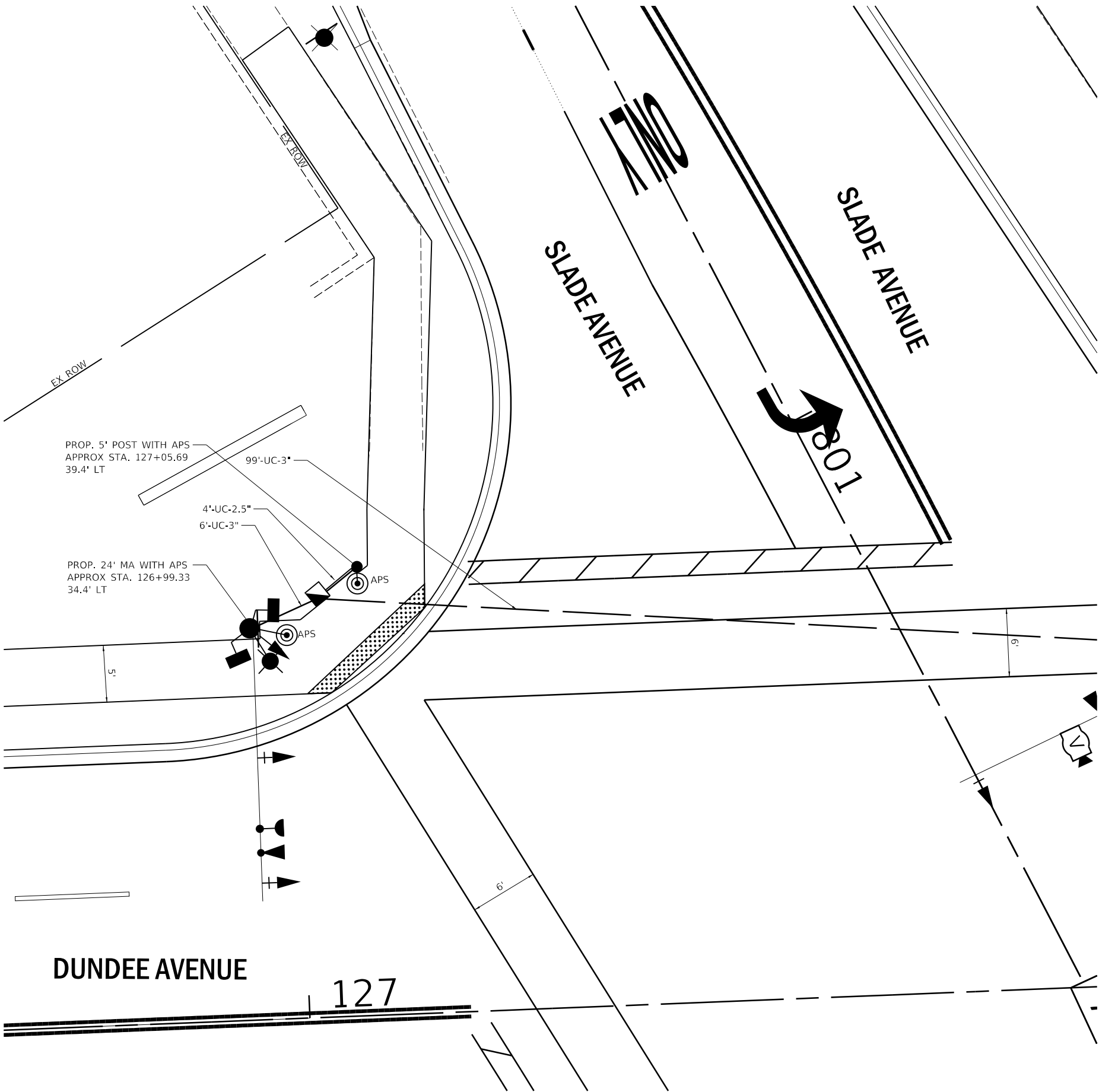
PROP TRAFFIC SIGNAL POST

PROP TRAFFIC SIGNAL HEAD

PROP TRAFFIC STREET SIGN

GENERAL NOTES

1. ALL STATION AND OFFSETS ARE REFERENCED TO THE DUNDEE AVENUE CENTERLINE ALIGNMENT
2. ALL PEDESTRIAN PUSH BUTTON POSTS SHALL BE LOCATED AS SHOWN IN THE PLANS.
3. ALL PEDESTRIAN PUSH BUTTONS SHALL BE WITHIN 10 FT. FROM THE BACK OF THE CURB OR THE EDGE OF PAVEMENT FOR EACH CROSSING. THE CONTRACTOR SHALL INSTALL PEDESTRIAN PUSH BUTTON EXTENSIONS TO MEET THIS REQUIREMENT.
4. MOUNTING HEIGHT OF PEDESTRIAN PUSH BUTTONS SHALL BE 36 INCHES (PREFERRED) FROM THE SIDEWALK SURFACE.
5. SHOP DRAWINGS FOR ALL SIGNAL IMPROVEMENTS INCLUDING APS IMPROVEMENTS SHALL BE PROVIDED TO THE CITY OF ELGIN.
6. A WALK THROUGH AND FINAL INSPECTION OF THE TRAFFIC SIGNAL IMPROVEMENTS SHALL BE COORDINATED WITH THE ENGINEER.
7. THE INSTALLATION OF ACESIBLE PEDESTRIAN SIGNALS SHALL INCLUDE THE INTERACTIVE VIBROTACTILE PEDESTRIAN PUSH BUTTON WITH SPEAKER, AN INFORMATIONAL SIGN, A LIGHT-EMITTING DIODE (LED) INDICATOR LIGHT, A SOLID-STATE ELECTRONIC CONTROL BOARD, A POWER SUPPLY, INTERNAL WIRING AND MOUNTING HARDWARE.
8. THE INFORMATIONAL SIGN SHALL CONFORM TO THE MUTCD STANDARD R10-3-i (BRAILLE SIGN). THE SELECTION OF THE SIGN SHALL BE APPROVED BY THE ENGINEER.



USER NAME = jmarvig PLOT SCALE = 1:10

PLOT DATE = 11/5/2024 1:05:37 PM

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FILE NAME: H:\SDS\6054\EG_Elgin\2021EG2102.Dgn Final EngEG2102-APS Detail.dwg - SW Slade.dgn

Engineering Enterprises, Inc.
CONSULTING ENGINEERS
52 Wheeler Road
Sugar Grove, Illinois 60554
630.466.6700 / www.eeiweb.com

CITY OF ELGIN
150 DEXTER COURT
ELGIN, IL. 60120

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DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DUNDEE AVENUE
SOUTHWEST CORNER ACCESSIBLE PEDESTRIAN SIGNAL DETAIL

SCALE: 1" = 5' SHEET 4 OF 4 SHEETS STA. N/A TO STA. N/A

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2525	20-00189-00-PV	KANE	184	115
CONTRACT NO. 61K68				
ILLINOIS FED. AID PROJECT				

GENERAL NOTES:

1.

THIS PROJECT INCLUDES INSTALLATION OF NEW LIGHTING ALONG DUNDEE AVE. FROM PAGE AVE. TO ENTERPRISE ST. THIS PROJECT ALSO INCLUDES THE REPLACEMENT OF LUMINAIRES ON EXISTING TRAFFIC SIGNAL COMBINATION POLES WITH LED LUMINAIRES AT THE INTERSECTION OF DUNDEE AVE. AT SLADE AVE. THE PROPOSED LIGHTING SHALL BE OWNED AND MAINTAINED BY THE CITY OF ELGIN.
2.

THE QUANTITIES OF RACEWAYS WHEREVER INDICATED ON THESE PLANS ARE APPROXIMATIONS ONLY. THE CONTRACTOR SHALL FIELD VERIFY ALL LENGTHS AND SHALL INSTALL RACEWAYS IN COMPLETE COMPLIANCE WITH THE SPECIFIED REQUIREMENTS.
3.

THE CONTRACTOR SHALL NOTIFY J.U.L.I.E. TO LOCATE AND MARK/STAKE ALL UNDERGROUND UTILITIES.
4.

THE CONTRACTOR SHALL VERIFY LOCATIONS OF UNDERGROUND/OVERHEAD UTILITIES PRIOR TO INSTALLATION OF LIGHT POLES AND CONDUITS. IF THERE IS A CONFLICT WITH THE LIGHT POLES/CONDUITS INSTALLATION AS SHOWN ON THE PLANS, THE CONTRACTOR SHALL SUGGEST ALTERNATIVE LOCATIONS AND COORDINATE WITH THE ENGINEER PRIOR TO PERFORMING ANY CONSTRUCTION WORK. IT SHALL ALSO BE THE CONTRACTORS RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION ACTIVITIES.
5.

TRENCHES FOR LIGHTING RACEWAYS SHALL HAVE A MINIMUM DEPTH OF 30".
6.

LIGHTING SYSTEM INSTALLATION SHALL CONFORM TO THE LATEST IDOT STANDARDS, NEC AND LOCAL CODES.
7.


ALL ELECTRICAL EQUIPMENT AND PRODUCTS SHALL BE UL LISTED AND LABELED.
8.


THE CONTRACTOR SHALL TAKE PRECAUTION WHEN INSTALLING UNIT DUCT TO AVOID CONFLICTS WITH EXISTING UNDERGROUND UTILITIES AND TREES ROOTS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE AS DETERMINED BY THE ENGINEER.


BILL OF MATERIALS


DESCRIPTION	UNIT	QUANTITY
ELECTRIC SERVICE INSTALLATION	EACH	1
ELECTRIC UTILITY SERVICE CONNECTION	L SUM	1
UNDERGROUND CONDUIT, GALVANIZED STEEL, 4" DIA.	FOOT	932
UNIT DUCT, 600V, 3-1C NO.4, 1/C NO.6 GROUND, (XLP-TYPE USE), 1 1/4" DIA. POLYETHYLENE	FOOT	2680
ELECTRIC CABLE IN CONDUIT, 600V (XLP-TYPE USE) 1/C NO. 1/0	FOOT	105
LUMINAIRE, LED, ROADWAY, OUTPUT DESIGNATION G	EACH	20
LIGHTING CONTROLLER, BASE MOUNTED, 240VOLT, 100AMP	EACH	1
LIGHT POLE, ALUMINUM, 35 FT. M.H., 8 FT. MAST ARM	EACH	17
LIGHT POLE FOUNDATION, METAL, 11 1/2" BOLT CIRCLE, 8 5/8" X 6"	EACH	17
LIGHT POLE FOUNDATION, 24" DIAMETER, OFFSET	FOOT	42
LUMINAIRE SAFETY CABLE ASSEMBLY	EACH	20
MAINTENANCE OF LIGHTING SYSTEM	CAL MO	6


LEGEND


- 


EXISTING LIGHTING UNIT TO REMAIN
- 


PROPOSED LIGHTING UNIT, 35FT. MH., 8FT. MAST ARM, LED LUMINAIRE
- 


EXISTING COMBINATION SIGNAL/LIGHT POLE, EXISTING LUMINAIRE TO BE REPLACED WITH NEW LED LUMINAIRE
- 

UNIT DUCT, 600V, 3-1C NO. 4, 1/C NO. 6 GROUND, (XLP-TYPE USE) 1¼" DIA. POLYETHYLENE
- 

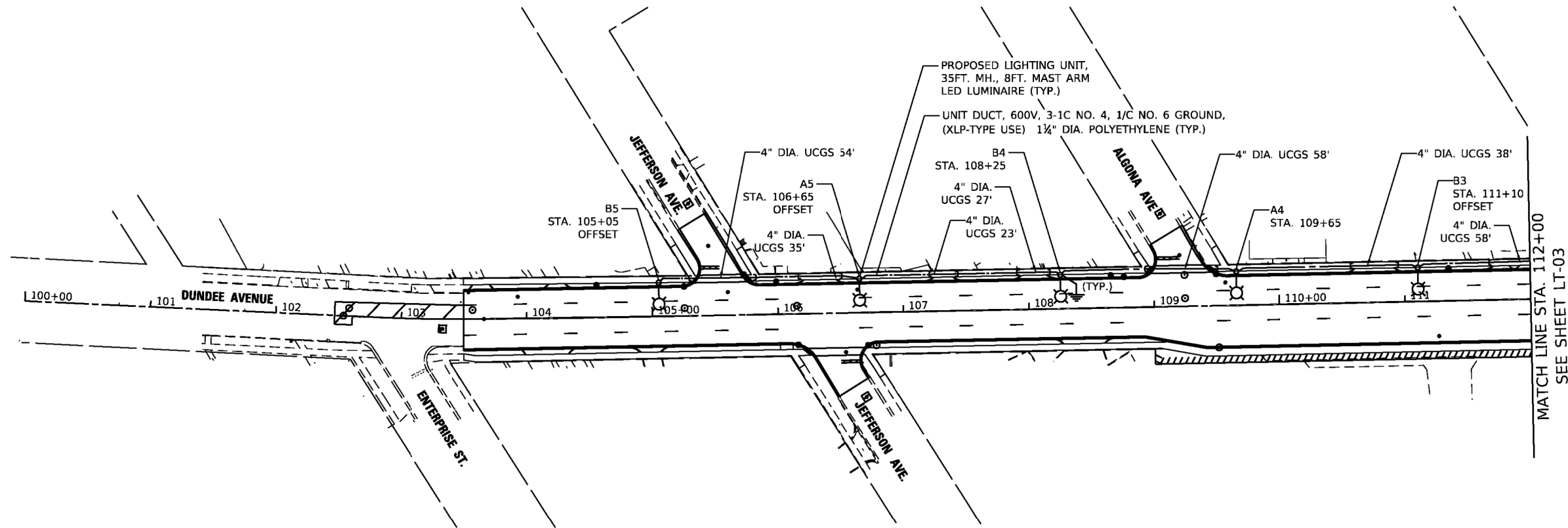
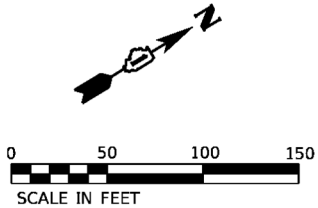
ComEd ELECTRIC SERVICE, 120/240V, 1 PHASE 3 WIRE
- 

PROPOSED LIGHTING CONTROLLER "LC" 120/240V, 3 WIRE 100 AMP, BASE MOUNTED
- 

UNDERGROUND CONDUIT, GALVANIZED STEEL
- 

ELECTRIC CABLE IN CONDUIT 4" DIA., 3-1/C NO. 1/0
- 

GROUND ROD 5/8" DIA. X 10 FT



NOTES:

- FOR GENERAL NOTES AND LEGEND, SEE SHEET LT-01.
- SETBACK IS 3FT. FROM FACE OF CURB TO CENTER OF POLE.
- UCGS STANDS FOR UNDERGROUND CONDUIT GALVANIZED STEEL.
- 'OFFSET' INDICATED WHERE THE LIGHTING UNIT HAS AN UNDERGROUND CONFLICT AND OFFSET FOUNDATION MAY BE REQUIRED.

LT-02

AMES Engineering, Inc.
CONSULTING ENGINEERS
6330 Belmont Road, Suite 4B
Downers Grove, IL 60516

CITY OF ELGIN
150 DEXTER COURT
ELGIN, IL. 60120

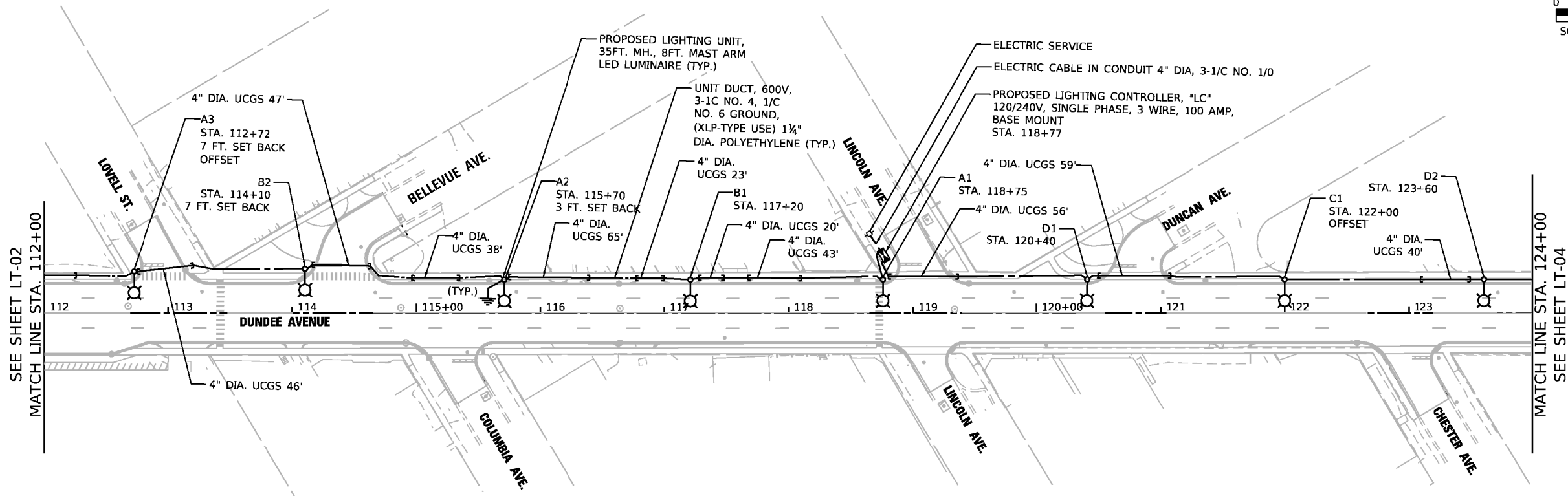
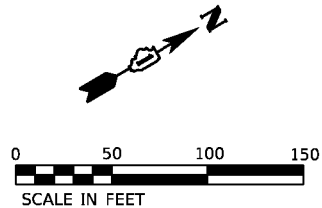
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DRAWN -	SR	REVISED -	
CHECKED -	BL	REVISED -	
DATE -	02-22-2024	REVISED -	

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PROPOSED LIGHTING PLAN
DUNDEE AVENUE

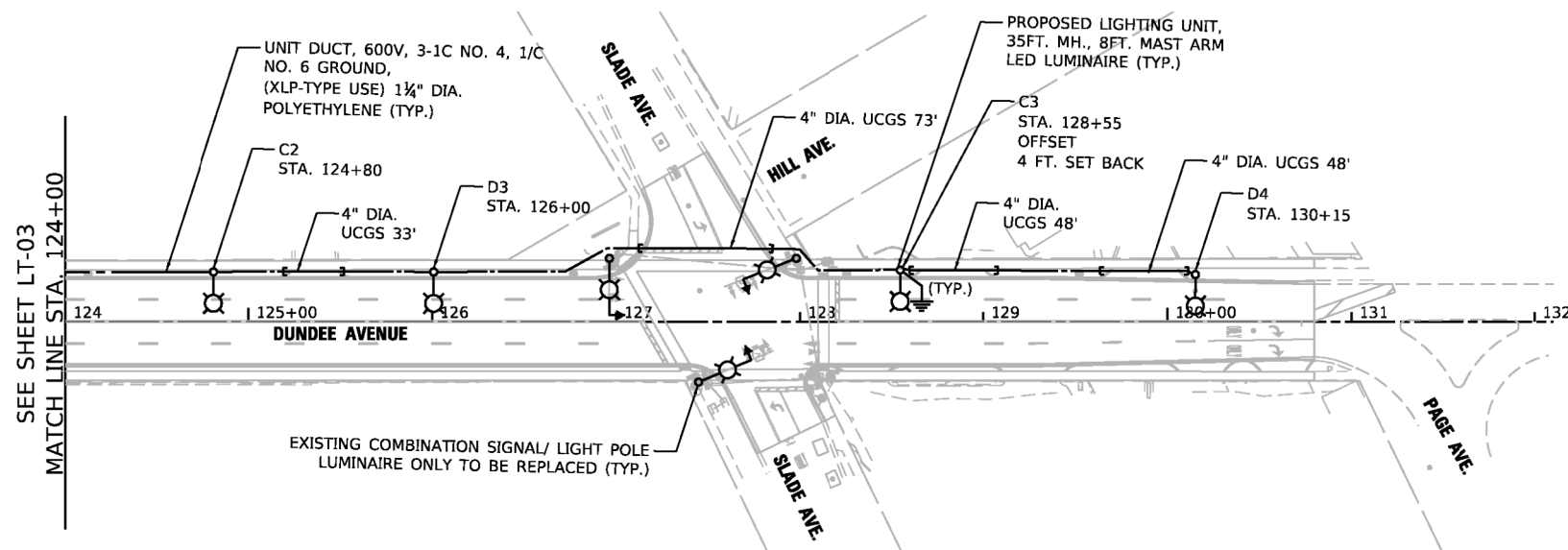
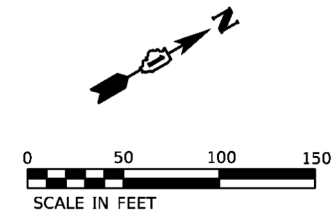
SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2525	20-00189-00-PV	KANE	184	117
CONTRACT NO. 61K68				
ILLINOIS FED. AID PROJECT				



NOTES:

- 1. FOR GENERAL NOTES AND LEGEND, SEE SHEET LT-01.
- 2. SETBACK IS 3FT. FROM FACE OF CURB TO CENTER OF POLE, UNLESS NOTED OTHERWISE.
- 3. UCGS STANDS FOR UNDERGROUND CONDUIT GALVANIZED STEEL.
- 4. 'OFFSET' INDICATED WHERE THE LIGHTING UNIT HAS AN UNDERGROUND CONFLICT AND OFFSET FOUNDATION MAY BE REQUIRED.



NOTES:

- FOR GENERAL NOTES AND LEGEND, SEE SHEET LT-01.
- SETBACK IS 3FT. FROM FACE OF CURB TO CENTER OF POLE, UNLESS NOTED OTHERWISE.
- UCGS STANDS FOR UNDERGROUND CONDUIT GALVANIZED STEEL.
- 'OFFSET' INDICATED WHERE THE LIGHTING UNIT HAS AN UNDERGROUND CONFLICT AND OFFSET FOUNDATION MAY BE REQUIRED.

LT-04

AMES Engineering, Inc.
CONSULTING ENGINEERS
6330 Belmont Road, Suite 4B
Downers Grove, IL 60516

CITY OF ELGIN
150 DEXTER COURT
ELGIN, IL. 60120

DESIGNED	-	MH
DRAWN	-	SR
CHECKED	-	BL
DATE	-	02-22-2024

REVISED	-
REVISED	-
REVISED	-
REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

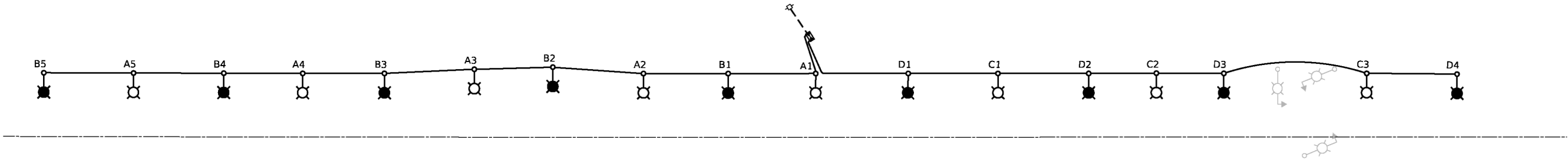
PROPOSED LIGHTING PLAN
DUNDEE AVENUE

SCALE: 1"=50'

SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2525	20-00189-00-PV	KANE	184	119
CONTRACT NO. 61K68				
ILLINOIS FED. AID PROJECT				

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LEGEND

- LUMINAIRE, LED, 120V, 1.30A, 140W, 1 PHASE, ON BLACK WIRE,
- LUMINAIRE, LED, 120V, 1.30A, 140W, 1 PHASE, ON RED WIRE,
- UNIT DUCT, 3-1C NO. 4, 1/C NO. 6 GROUND (XLP-TYPE USE)
1 1/4" DIA. POLYETHYLENE
- comed ELECTRIC SERVICE, 120/240V, 1 PHASE 3 WIRE
- ELECTRIC CABLE IN CONDUIT 4" DIA., 3-1/C NO. 1/0
- PROPOSED LIGHTING CONTROLLER "LC" 120/240V, 3 WIRE
100 AMP, BASE MOUNTED

LOAD TABLE LIGHTING CONTROLLER "LC"

120/240V AC, 1 PHASE, 3 WIRE, 100A

ON RED WIRE				ON BLACK WIRE			
CIRCUIT	TOTAL LUM.	TOTAL CURRENT IN AMPS	TOTAL WATTAGE	CIRCUIT	TOTAL LUM.	TOTAL CURRENT IN AMPS	TOTAL WATTAGE
A	5	6.5	700	B	5	6.5	700
C	3	3.9	420	D	4	5.2	560
E	-	-	-	F	-	-	-
G	-	-	-	H	-	-	-
TOTAL	8	10.4	1120	TOTAL	9	11.7	1260

TOTAL LOAD IN WATTS 2380
TOTAL LOAD IN AMPS 22.10

AMES Engineering, Inc.
CONSULTING ENGINEERS
6330 Belmont Road, Suite 4B
Downers Grove, IL 60516

CITY OF ELGIN
150 DEXTER COURT
ELGIN, IL. 60120

DESIGNED -	MH	REVISED -	
DRAWN -	SR	REVISED -	
CHECKED -	BL	REVISED -	
DATE -	02-22-2024	REVISED -	

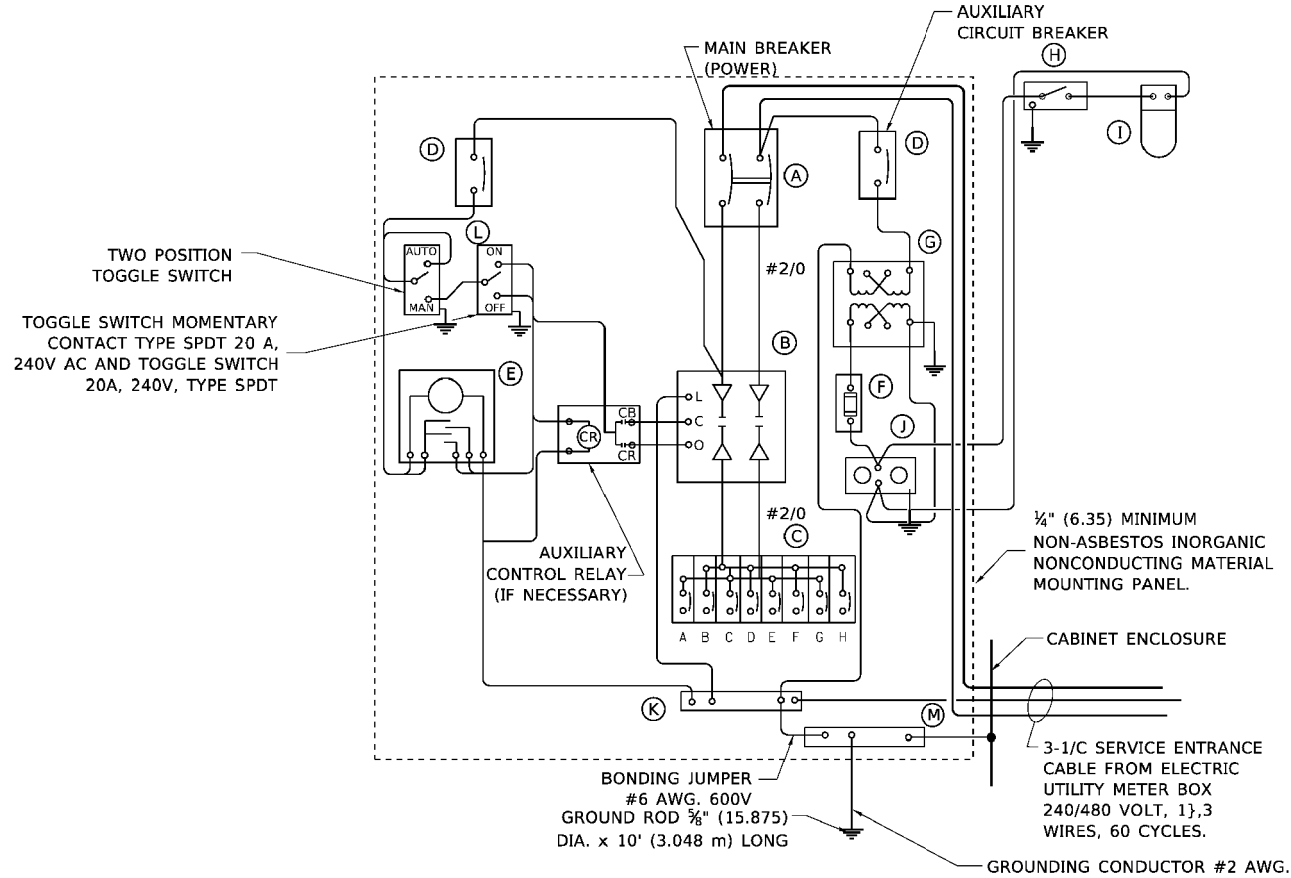
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SINGLE LINE WIRING DIAGRAM
DUNDEE AVENUE

SCALE: 1"=50' SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2525	20-00189-00-PV	KANE	184	120
CONTRACT NO. 61K68				
ILLINOIS FED. AID PROJECT				

LT-05



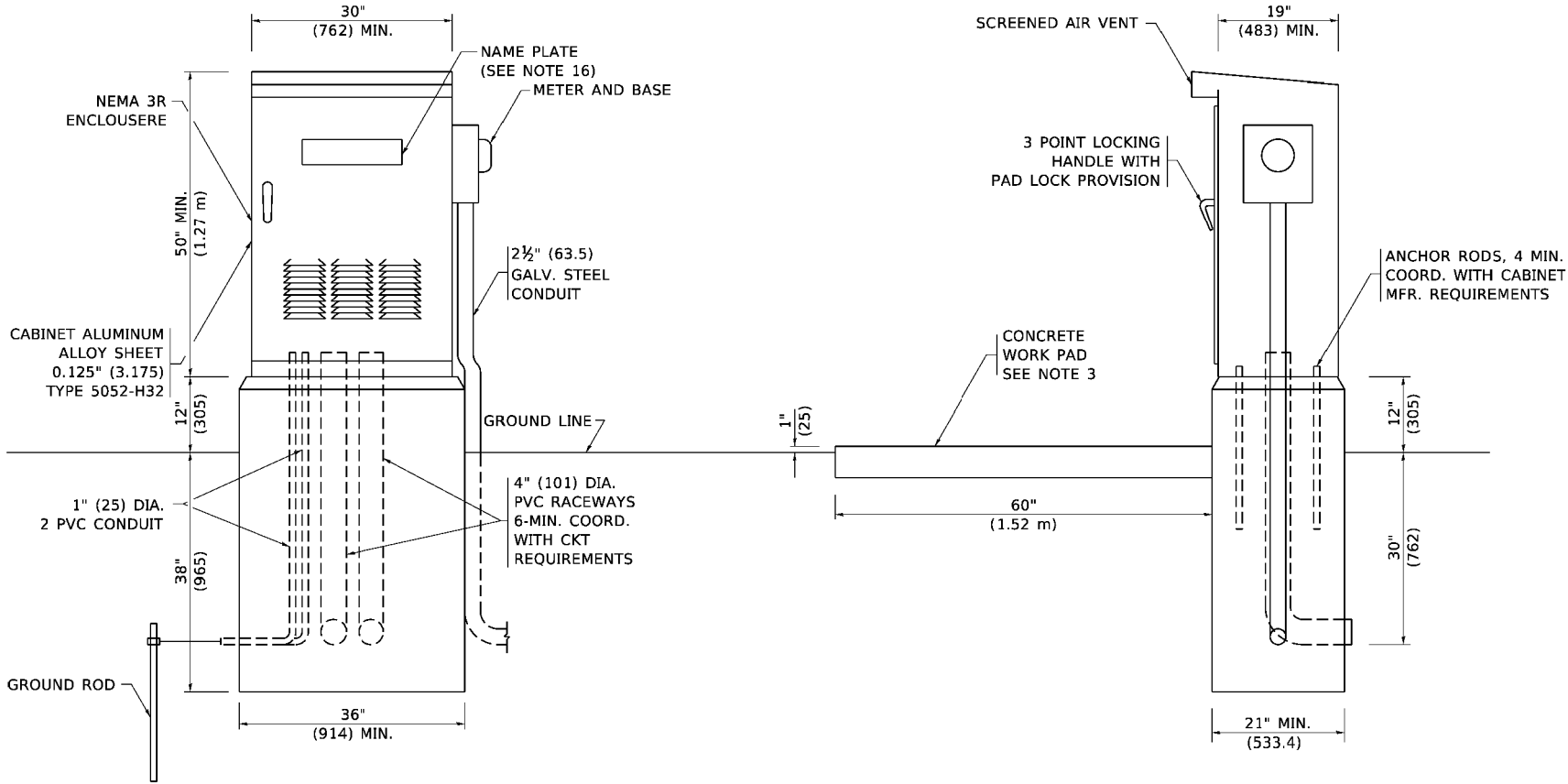
PANEL WIRING DIAGRAM

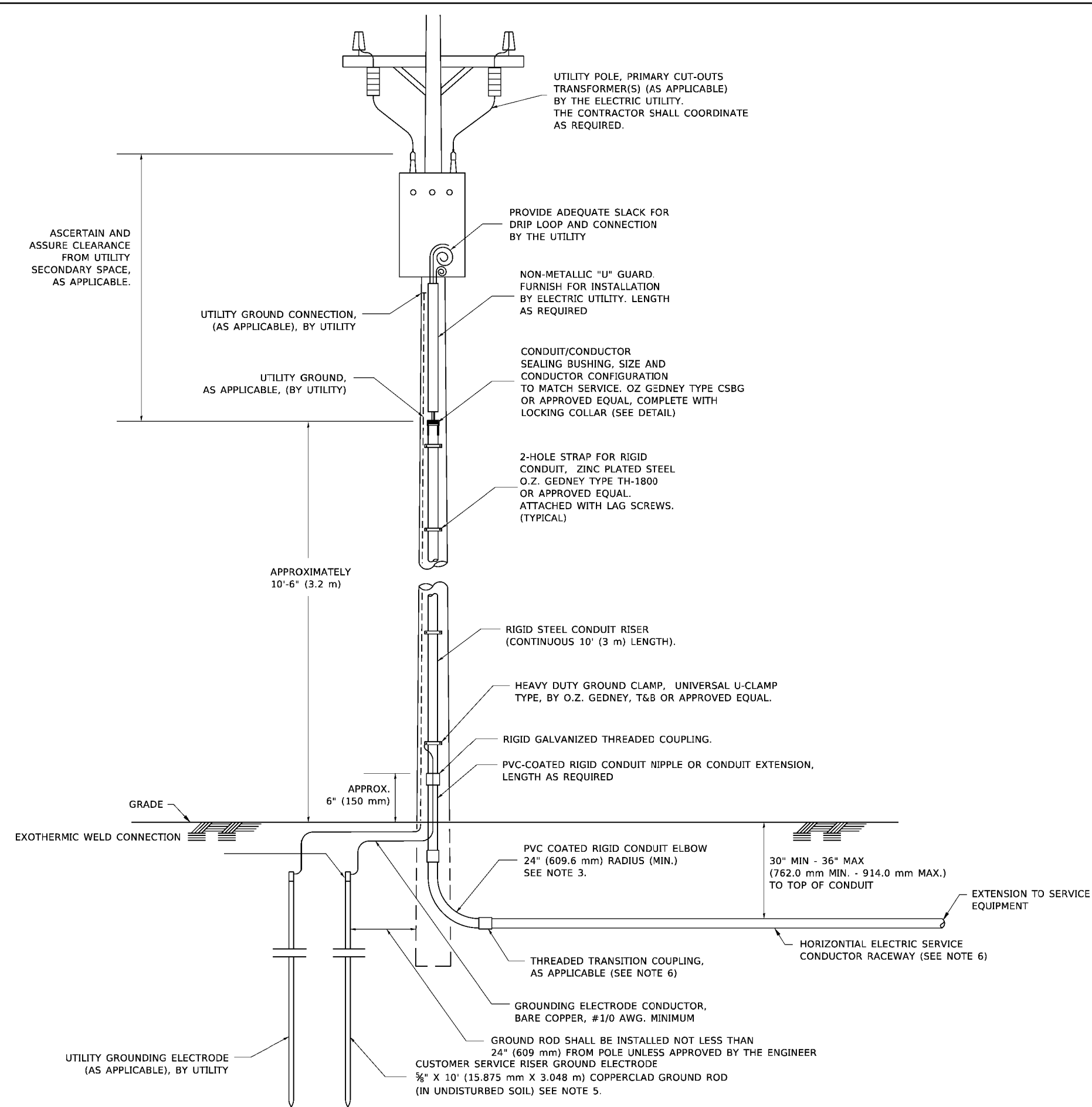
PANEL EQUIPMENT

BILL OF MATERIAL		
ITEM	QUANTITY	DESCRIPTION
A	1	MAIN CIRCUIT BREAKER, 2 POLE, 600 VOLT 100 AMP. FRAME, 100 AMP. NON-INTERCHANGEABLE TRIP INTERRUPTING RATING NEMA-22000 AMP. AT 480 VOLT.
B	1	REMOTE CONTROL SWITCH, ELECTRICALLY OPERATED, MECHANICALLY HELD, 2 POLE, SINGLE THROW, 100 AMP., 600 VOLTS CONTROL CIRCUIT 240 VOLT.
C	8	CIRCUIT BREAKERS, 1 POLE, 100AMP. FRAME, 50 AMP. NON-INTERCHANGEABLE TRIP INTERRUPTING RATING NEMA-10,000 AMP. AT 240 V.
D	2	CONTROL CIRCUIT-CIRCUIT BREAKER. 1 POLE, 240 V., 100 AMP. FRAME, 15 AMP. NON-INTERCHANGEABLE TRIP INTERRUPTING RATING NEMA-5000 AMP. AT 240 V.
E	1	ASTRONOMIC MICROPROCESSOR-BASED 2-CHANNEL CONTROLLER (TIME SWITCH).
F	1	20 AMP, 120 VOLT FUSE.
G	1	1.5 KVA, SINGLE PHASE, ENCAPSULATED TRANSFORMER 240 X 480 / 120 X 240 VOLT, 60 Hz.
H	1	SPST 20A SWITCH ON DOOR, TO TURN LIGHT ON WHEN DOOR IS OPEN.
I	1	INCANDESCENT LIGHTING FIXTURE ENCLOSED AND GASKETED WITH 60 WATT, 120 V. LAMP.
J	1	20 AMP, 120 VOLT, DUPLEX RECEPTACLE, GFCI.
K	1	COPPER GROUND BUS 1#4" (6.35) X 1" (25.4) X 12" (304.8 mm) LONG MOUNTED ON PANEL WITH LUGS AND 4 SPARE LUGS
L	1	TOGGLE SWITCHES MOUNTED IN 4" (101.6) X 4" (101.6 mm) BOX.
M	1	COPPER GROUND BUS 1#4" (6.35) X 1" (25.4) X 12" (304.8 mm) LONG MOUNTED ON PANEL WITH LUGS AND SPARE LUGS.

NOTES

1. ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
2. FOUNDATION SIZE SHALL BE COORDINATED WITH CABINET SIZE AND MFR.
3. IN FRONT OF CONTROL CABINET DOOR, REMOVE VEGETATION AND 2" (50.8 mm) TOP SOIL. LEVEL THE AREA AND ON TOP, PLACE LENGTH WISE PARALLEL TO CONTROL CABINET, A CONCRETE PAD 36" (914.4 mm) x 60" (18.288 m) x 4" (101 mm) MIN. SIZE. THE COST OF LABOR AND MATERIALS ARE INCLUDED IN THE COST OF THE CONTROLLER.
4. DOOR SHALL BE CONSTRUCTED FROM SAME TYPE OF MATERIAL AND THICKNESS AS CABINET.
5. DOOR SHALL BE EQUIPPED WITH THREE POINT LATCHING MECHANISM WITH NYLON ROLLERS AT TOP THE BOTTOM.
6. DOOR HINGE SHALL BE A HEAVY GAUGE CONTINUOUS HINGE WITH A 1#4" (6.35 mm) DIA. STAINLESS STEEL HINGE PIN.
7. ALL EXTERNAL HARDWARE SHALL BE STAINLESS STEEL.
8. CONTROL WIRING TO BE #12 AWG, 600V, TYPE "SIS" GRAY SWITCH BOARD WIRE, STRANDED COPPER.
9. METER BOX SHALL BE MOUNTED ON THE SIDE OF CONTROL CABINET, NEAR TO THE SERVICE POLE.
10. CABINETS SHALL BE PRIMED AND PAINTED AS SPECIFIED.
11. THE HEADS OF CONNECTORS SCREWS SHALL BE PAINTED WHITE FOR NEUTRAL BAR CONNECTION AND GREEN FOR GROUND BAR CONNECTORS.
12. ALL WIRING WITHIN THE CABINET SHALL BE COLOR CODED AS INDICATED.
R = RED BL = BLUEW = WHITE
B = BLACK Y = YELLOW G = GREEN
13. PROVIDE SEALING GROMMETS FOR ALL OPEN WIRING EXTENDED FROM DEVICES IN BOXES OR CABINETS WITHIN THE CONTROL CABINET.
14. ALL WIRING SHALL BE NEATLY DRESSED AND SUPPORTED.
15. THE CONTROLLER SHALL BE CONSTRUCTED TO U.L. STD. 508 AND BEAR THE U.L. LABEL "ENCLOSED INDUSTRIAL CONTROL PANEL".
16. 12" (304.8) X 16" (406.4 mm) STAINLESS STEEL EXTERIOR NAMEPLATE SHALL BE ENGRAVED TO "STATE OF ILLINOIS LIGHTING CONTROLS" UNLESS OTHERWISE SPECIFIED.



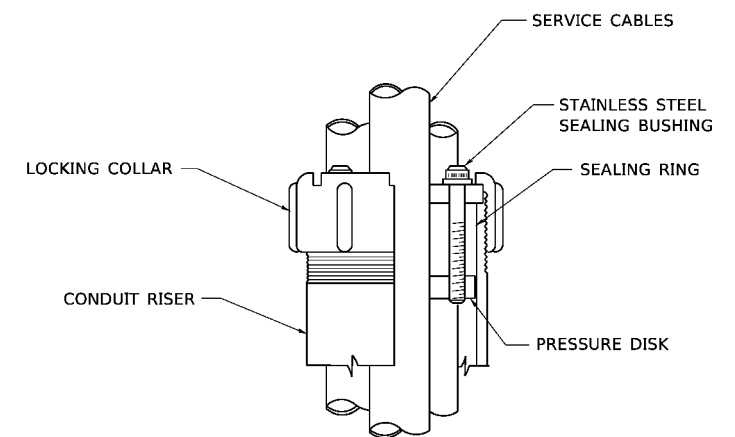


APPLICATION

THIS DETAIL APPLIES FOR LOW VOLTAGE ELECTRIC SERVICE (660 V OR LESS) FROM AN OVERHEAD UTILITY SUPPLY TO SEPERATLY-MOUNTED SERVICE EQUIPMENT.

NOTES

- SERVICE VOLTAGE SHALL BE AS INDICATED ELSEWHERE IN THE DRAWINGS.
- UNLESS OTHERWISE INDICATED, ITEMS AND WORK SHALL BE INCLUDED AND PAID AS PART OF THE ELECTRIC UTILITY SERVICE INSTALLATION PAY ITEM.
- CONDUIT AND CONNECTOR DIAMETER SHALL MATCH THE DIAMETER OF THE SERVICE CONDUCTOR RACEWAY AS INDICATED ON THE PLANS.
- PVC COATED RACEWAYS AND ACCESSORIES SHALL BE CAREFULLY INSTALLED WITH MFR RECOMMENDED TOOLS AND PROCEDURES TO AVOID DAMAGE. ANY DAMAGE SHALL BE REPAIRED WITH COMPATIBLE PVC TOUCH-UP MATERIAL TO THE SATISFACTION OF THE ENGINEER OR THE DAMAGED MATERIAL SHALL BE REPLACED AT NO ADDITIONAL COST.
- THE CONTRACTOR SHALL OBTAIN INSPECTION AND APPROVAL BY THE ENGINEER OF SERVICE RISER GROUND ELECTRODE, RISER ELBOW, NIPPLE AND CONNECTION TO SERVICE CONDUCTOR RACEWAY EXTENSION BEFORE BACKFILL AND SHALL ALSO OBTAIN INSPECTION OF SERVICE RISER AND SEALING BUSHING BEFORE UTILITY "U" GUARD INSTALLATION AND SERVICE CONNECTION.
- THE HORIZONTAL ELECTRIC SERVICE CONDUCTOR RACEWAY SHALL BE AS INDICATED AND SHALL BE MEASURED SEPARATELY FOR PAYMENT. WHEN THE RACEWAY IS PVC-COATED RIGID GALVANIZED STEEL, THE COUPLING SHALL BE THE SAME. WHEN THE RACEWAY IS PVC CONDUIT (IN CONCRETE), THE COUPLING SHALL BE A METALIC TO NON METALIC ADAPTER. WHEN THE RACEWAY IS ENCASED IN CONCRETE, THE CONCRETE SHALL EXTEND TO COVER THE COUPLING.
- PLANS AND DETAILS INDICATE THE GENERAL NATURE AND REQUIREMENTS. THEY DO NOT SHOW EVERY ACCESSORY AND ATTACHMENT, AND THEY DO NOT RELIEVE THE CONTRACTOR OF THE REQUIREMENTS OF THE SPECIFICATIONS AND SPECIAL PROVISIONS TO ASCERTAIN UTILITY REQUIREMENTS AND TO COORDINATE ACCORDINGLY, FURNISHING ALL ITEMS AND WORK NOT PROVIDED BY THE UTILITY, BUT NECESSARY FOR A COMPLETE SERVICE INSTALLATION IS REQUIRED AND SHALL BE INCLUDED IN THE ELECTRIC UTILITY SERVICE INSTALLATION PAY ITEM.



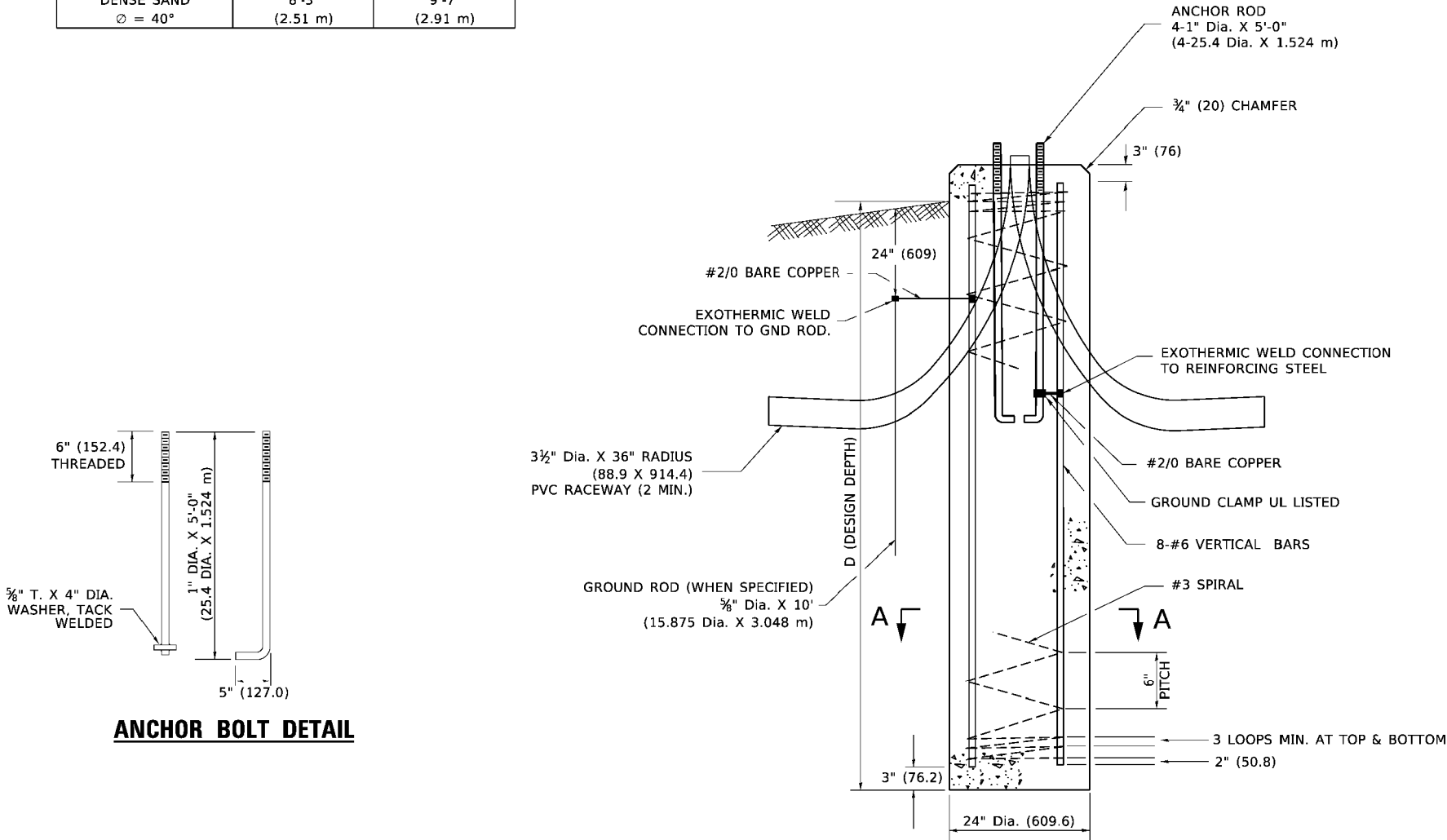
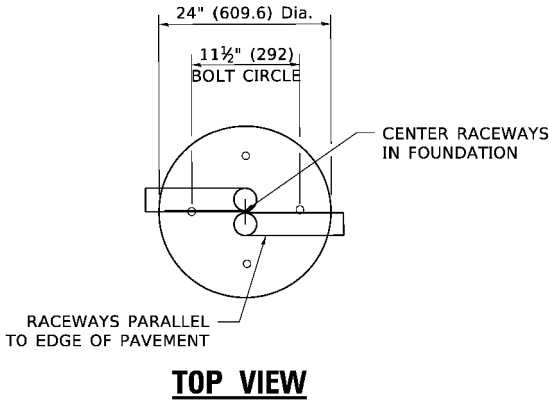
SEALING BUSHING DETAIL

LT-07

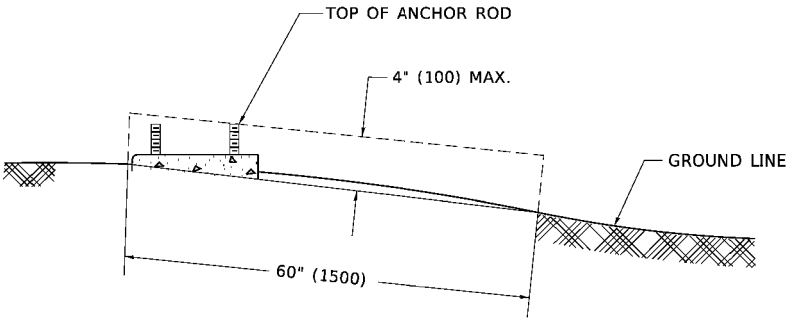
 <div>AMES Engineering, Inc. CONSULTING ENGINEERS 6330 Belmont Road, Suite 4B Downers Grove, IL 60516</div>	USER NAME = footemj	DESIGNED -	REVISED - 03-03-06	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ELECTRIC SERVICE INSTALLATION AERIAL, REMOTE DISCONNECT				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED -						2525	20-00189-00-PV	KANE	184	122
	PLOT SCALE = 50.0000' / in.	CHECKED - MEA	REVISED -		BE-220		CONTRACT NO. 61K68						
	PLOT DATE = 4/19/2019	DATE -	REVISED -		ILLINOIS FED. AID PROJECT								
					SCALE: NONE	SHEET 1	OF 1 SHEETS	STA.	TO STA.				

LIGHT POLE FOUNDATION DEPTH TABLE
30 FT. (9.144 m) TO 35 FT. (10.668 m) MOUNTING HEIGHT

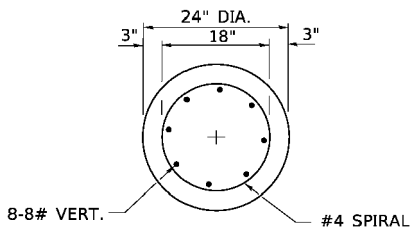
SOIL CONDITIONS	DESIGN DEPTH "D" OF FOUNDATION	
	SINGLE ARM POLE	TWIN ARM POLE
SOFT CLAY Qu = 0.375 TON/SQ. FT.	11'-0" (3.35 m)	12'-8" (3.85 m)
MEDIUM CLAY Qu = 0.75 TON/SQ.FT	9'-0" (2.74 m)	14'-10" (4.52 m)
STIFF CLAY Qu = 1.50 TON/SQ. FT.	7'-6" (2.29 m)	8'-7" (2.61 m)
LOOSE SAND Ø = 34°	9'-6" (2.90 m)	10'-7" (3.22 m)
MEDIUM SAND Ø = 37.5°	9'-0" (2.74 m)	9'-10" (2.99 m)
DENSE SAND Ø = 40°	8'-3" (2.51 m)	9'-7" (2.91 m)



FOUNDATION DETAIL



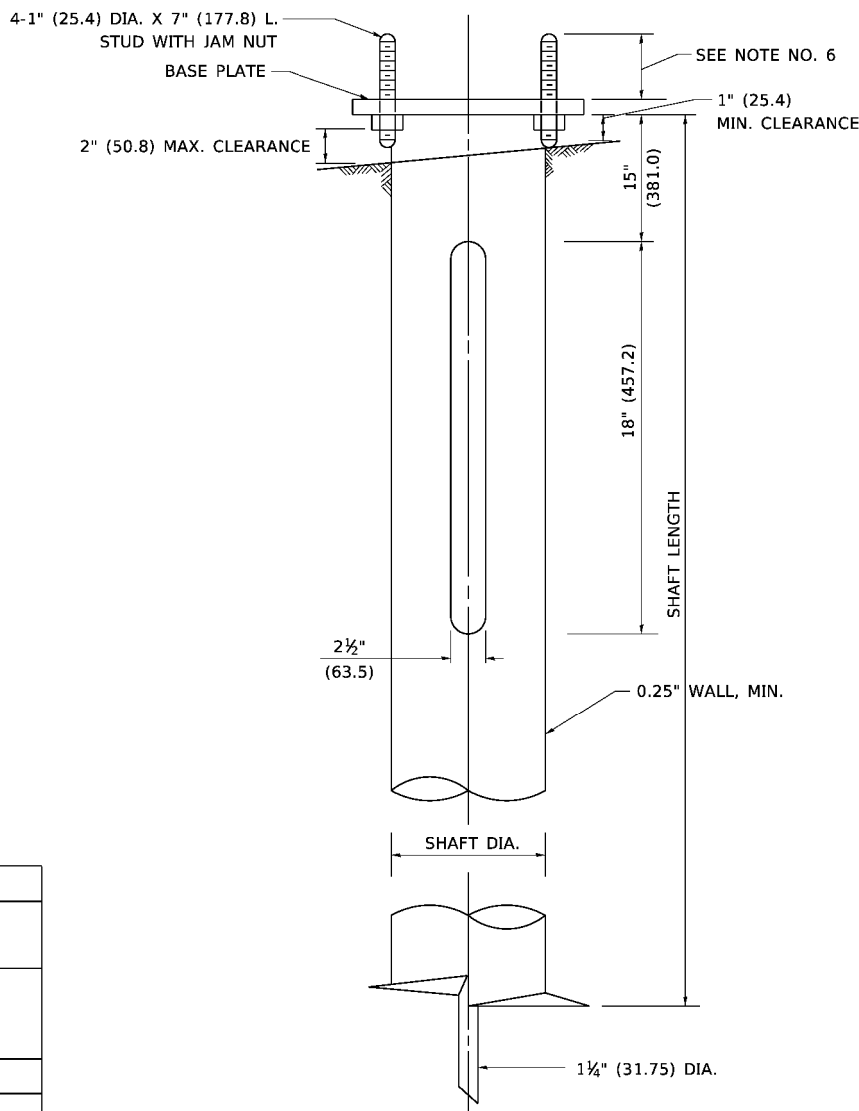
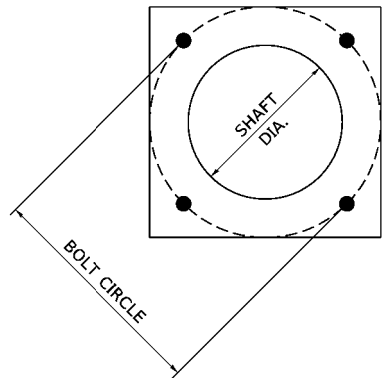
FOUNDATION EXTENSION DETAIL



SECTION A-A

NOTES

- ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
- THE ANCHOR RODS AND RACEWAYS SHALL BE PROPERLY SECURED IN PLACE BEFORE THE CONCRETE IS PLACED.
- THE FOUNDATION SHALL NOT PROTRUDE MORE THAN 4 IN. (100 mm) ABOVE THE FINISHED GRADE WITHIN A 60 IN. (1.5 m) CHORD ACROSS THE FOUNDATION, WITH ANCHOR RODS INCLUDED, IN ACCORDANCE WITH AASHTO GUIDELINES. IF THE FOUNDATION HEIGHT, INCLUDING ANCHOR RODS, EXTENDS BEYOND THESE SPECIFIED LIMITS, THE FOUNDATION SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE. SEE FOUNDATION EXTENSION DETAIL.
- THE HOLE FOR THE FOUNDATION SHALL BE MADE BY DRILLING WITH AN AUGER, OF THE SAME DIAMETER AS THE FOUNDATION. IF SOIL CONDITIONS REQUIRE THE USE OF A LINER TO FORM THE HOLE, THE LINER SHALL BE WITHDRAWN AS THE CONCRETE IS DEPOSITED.
- THE TOP OF THE FOUNDATION SHALL BE CONSTRUCTED LEVEL. A LINER OR FORM SHALL BE USED TO PRODUCE A UNIFORM SMOOTH SIDE TO THE TOP OF THE FOUNDATION. FOUNDATION TOP SHALL BE CHAMFERED 3#4-IN. (20 mm).
- THE CONCRETE SHALL BE CLASS SI. CONCRETE SHALL CURE ACCORDING TO ARTICLE 1020.13 BEFORE LIGHT POLES ARE INSTALLED.
- THE ANCHOR ROD SHALL BE A HOOK ROD TYPE. COLD BENDING OF THE ANCHOR ROD WILL NOT BE ALLOWED. THE RADIUS OF THE HOOK BEND SHALL NOT BE LESS THAN 4 TIMES THE NOMINAL DIAMETER OF THE ANCHOR ROD. A TACK WELDED ANCHOR ROD MAY BE SUBSTITUTED WITH THE APPROVAL OF THE ENGINEER.
- ANCHOR RODS, NUTS AND WASHERS SHALL BE COMPLETELY GALVANIZED BY EITHER THE HOT-DIPPED PROCESS CONFORMING WITH AASHTO M 232, THE MECHANICAL PLATING METHOD CONFORMING TO AASHTO M 298, CLASS 50 WITH A MAXIMUM COATING THICKNESS OF 150 UM(6 MILS) OR THE ELECTROLYTIC PROCESS ACCORDING TO ASTM F 1136.
- THE ANCHOR RODS SHALL BE THREADED A MINIMUM OF 6 INCHES (150 mm) WITH A MINIMUM OF 3 INCHES (75 mm) OF THREADED ANCHOR ROD EMBEDDED IN THE FOUNDATION.
- ANCHOR RODS SHALL PROJECT 23#4" (69.9 mm) ABOVE THE TOP OF THE FOUNDATION. IF BREAKAWAY COUPLINGS ARE SPECIFIED, THE CONTRACTOR SHALL CAREFULLY COORDINATE THE ANCHOR ROD PROJECTION WITH THE INSTALLATION REQUIREMENTS OF THE BREAKAWAY COUPLINGS.
- THE CONTRACTOR SHALL USE A #3 SPIRAL AT 6" (152.4 mm) PITCH OR MAY SUBSTITUTE #3 TIES AT 12" (304.8 mm) O.C. WITH THE APPROVAL OF THE ENGINEER.
- THE CABLE TRENCHES AND FOUNDATION SHALL BE BACK FILLED AND COMPACTED AS SPECIFIED BEFORE THE LIGHT POLE IS ERECTED.
- THE RACEWAYS SHALL PROJECT 1" (25.4 mm) ABOVE THE TOP OF THE FOUNDATION.



NOTES

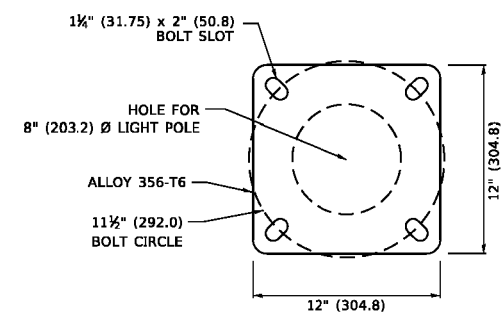
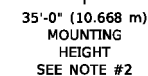
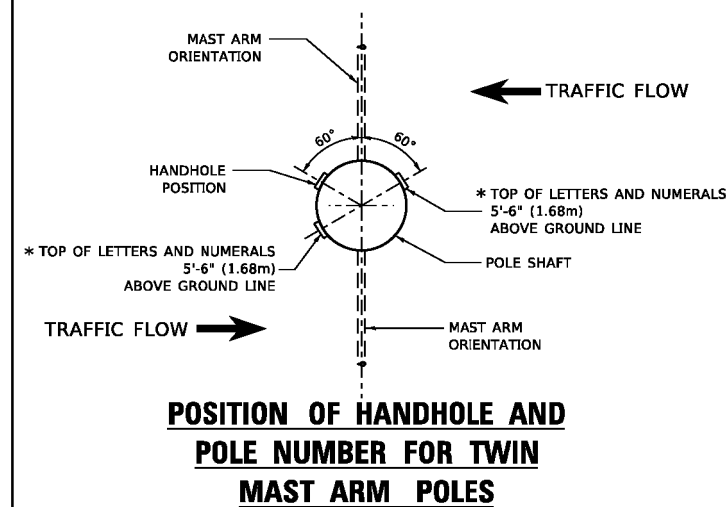
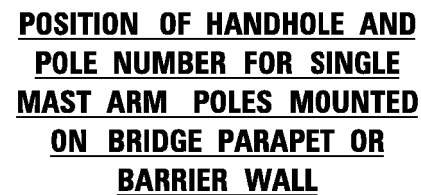
1. ALL DIMENSION IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
2. ALL MATERIAL SHALL BE GALVINIZED ACCORDING TO AASHTO M111, UNLESS OTHERWISE SPECIFIED.
3. ALL WELDS SHALL BE CONTINUOUS AND NOT LESS THAN 1#4" (6.35 mm) FILLET WELDS. THE WELDED FOUNDATION SHALL BE CAPABLE OF WITHSTANDING 10,000 FT/LBS (13558.18 n.m) OF INSTALLATION TORQUE APPLIED ABOUT THE AXIS OF THE FOUNDATION.
4. THE HELIX FOUNDATION SHAFT SHALL BE INSTALLED VERTICAL AND THE BASE PLATE SHALL BE IN LEVEL. THE BREAKAWAY COUPLINGS AND HARDWARE SHALL NOT BE USED TO ALIGN THE POLE INSTALLATION.
5. THE CABLE TRENCH SHALL BE BACKFILLED AND FIRMLY COMPACTED BEFORE THE INSTALLATION OF THE LIGHT POLE.
6. THE CONTRACTOR SHALL COORDINATE EXTENSION OF ANCHOR BOLTS ABOVE TOP OF THE BASE PLATE WITH THE BREAKAWAY DEVICE MANUFACTURER'S REQUIREMENTS.
7. ANY VOIDS WITHIN THE METAL FOUNDATION SHALL BE FILLED WITH FINE AGGREGATE.
8. METAL FOUNDATIONS SHALL BE INSTALLED IN UNDISTURBED SOIL. PREDRILLING A PILOT HOLE AND/OR BACKFILLING AROUND THE FOUNDTION IS NOT ALLOWED.
9. THE METAL FOUNDATION SHALL NOT BE INSTALLED TO A TORQUE WHICH EXCEEDS THE MANUFACTURER'S MAXIMUM TORQUE RATING NOR SHALL IT BE INSTALLED TO AN INSTALLATION TORQUE VALUE OF LESS THAN 3,500 FT LB (4,750 KNM). METAL FOUNDATIONS THAT ARE NOT INSTALLED TO FULL INSTALLATION DEPTH OR DO NOT ACHIEVE THE MINIMUM INSTALLATION TORQUE SHALL BE REMOVED AND REPLACED WITH A CONCRETE FOUNDATION AT NO ADDITIONAL COST.
10. THE BASEPLATE SHALL BE PERPENDICULAR TO THE SHAFT AXIS ($\pm 1^\circ$) AND THE HOLE CENTERLINE SHALL BE CONCENTRIC (± 0.188) TO THE SHAFT AXIS.
11. THE PILOT POINT AND SHAFT AXIS SHALL BE CONCENTRIC (± 0.125) AND IN LINE ($\pm 2^\circ$).
12. THE BASEPLATE SHALL BE STAMPED WITH THE MANUFACTURERS NAME AND DATE OF MANUFACTURE.

HELIX FOUNDATION SIZE

POLE MOUNTING HEIGHT	BOLT CIRCLE	SHAFT DIAMETER	SHAFT LENGTH	BASEPLATE
30 FT.	11½"	8⅝"	6 FT.	12"x12"x1"
31 FT.-35 FT.	11½"	8⅝"	6 FT.	12"x12"x1"
36 FT.-40FT.	15"	8⅝"	6 FT.	15"x15"x1¼"
41 FT.-45 FT.	15"	8⅝"	6 FT.	15"x15"x1¼"
46 FT.-50 FT.	15"	10"	8 FT.	15"x15"x1¼"

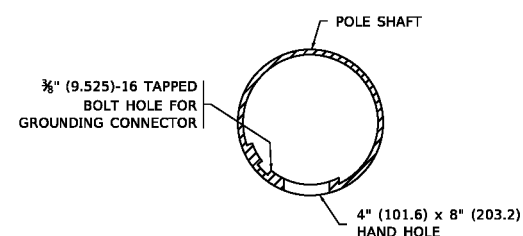
METAL HELIX FOUNDATION MATERIALS

ITEM	MATERIAL REQUIREMENT
BASEPLATE	AASHTO M 270M, GRADE 36 (M270M, GRADE 250)
SHAFT	ASTM A 252, GRADE 2 (PHOSPHOROUS 0.04% MAXIMUM, SULFUR 0.05% MAXIMUM)
HELIX SCREW	AASHTO M 183 (ASTM A 635)
PILOT POINT	AASHTO M 270 (ASTM A 575)
ANCHOR RODS/STUDS	AASHTO M 314 (ASTM F 1554)
HEXAGON NUTS	AASHTO M 291M (ASTM A 563) GRADE DH, OR AASHTO M 292 (ASTM A 194) GRADE 2H
WASHERS	AASHTO M 293 (ASTM F 436)



LIGHT POLE BASE PLATE DETAIL

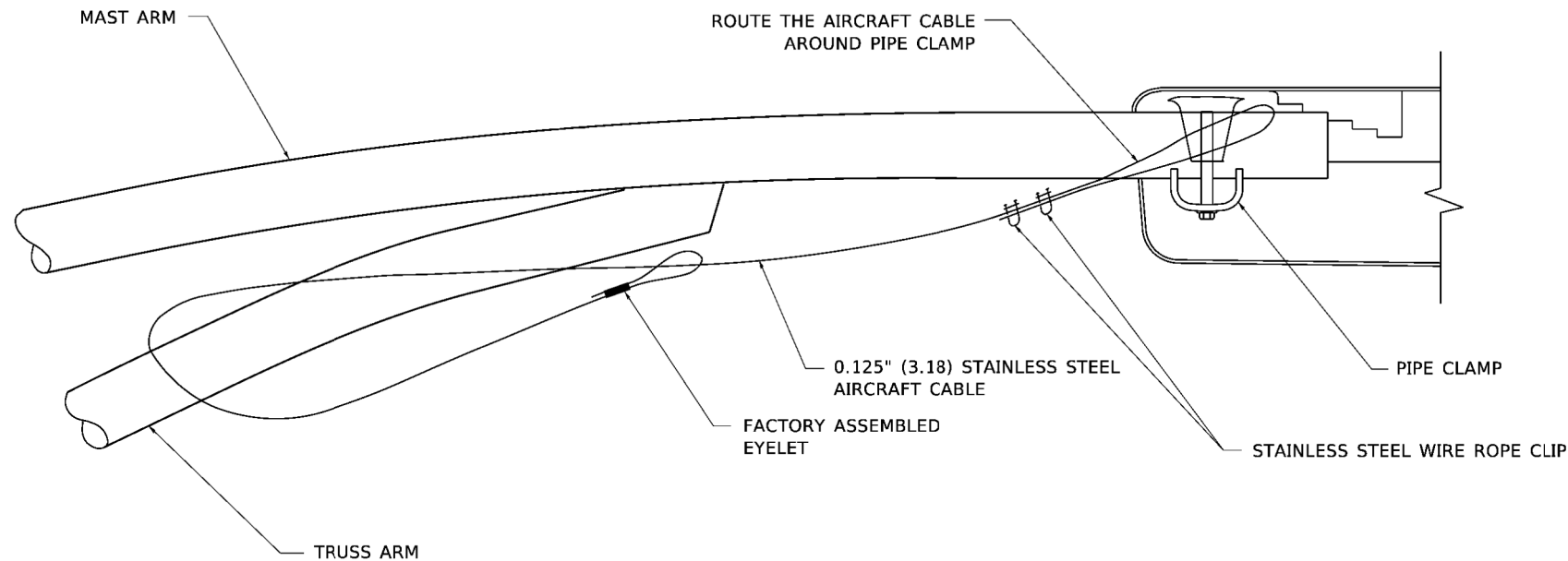
11½" (292.0) BOLT CIRCLE



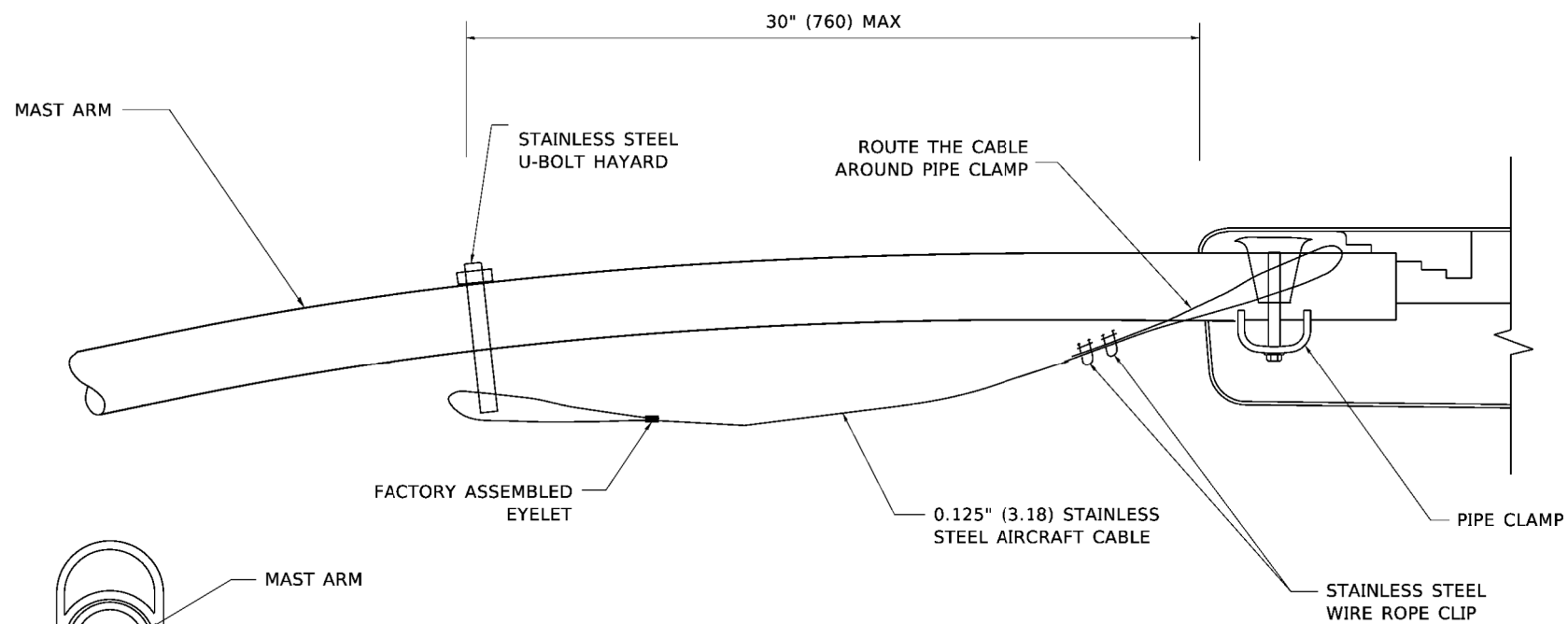
HANDHOLE DETAIL
(N.T.S.)

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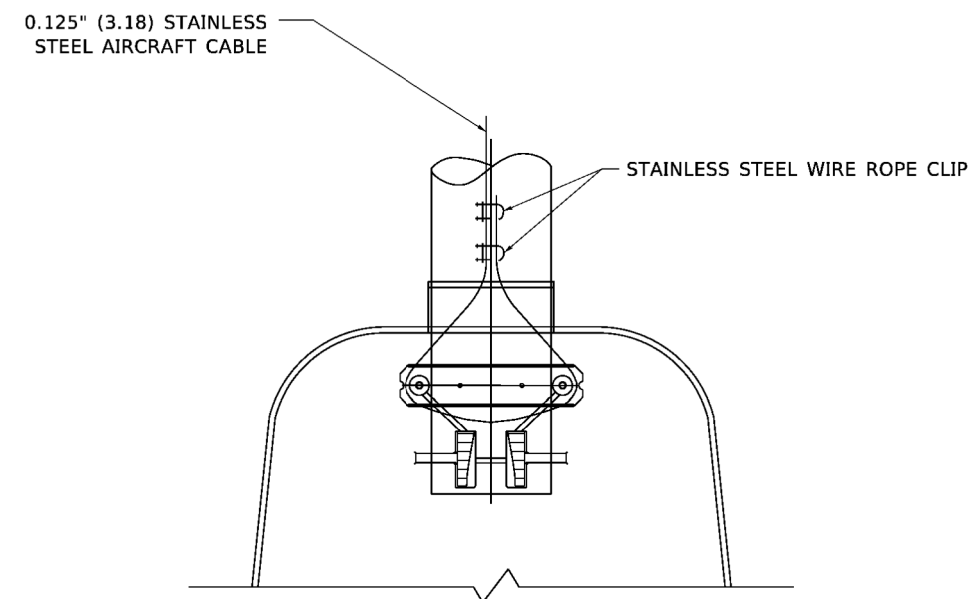
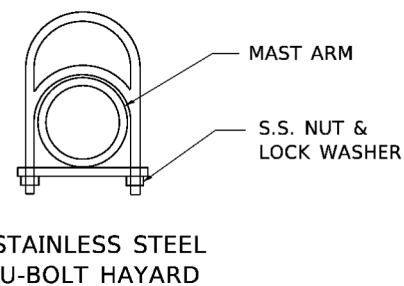
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2. MOUNTING HEIGHT IS DEFINED AS THE DISTANCE FROM THE CENTERLINE OF THE TENSION TO THE BOTTOM OF THE ANCHOR BASE.
3. THE LIGHT POLE WILL MEET AASHTO DESIGN CRITERIA AS SPECIFIED.
4. THE INSTALLING CONTRACTOR WILL PROVIDE A UL LISTED GRONDING CONNECTOR. BURNDY K2C23, T&B SP4DL OR APPROVED EQUAL.
5. LIGHT POLES WILL NOT BE INSTALLED WITHOUT MAST ARMS AND LUMINAIRES.
6. LIGHT POLES WILL BE SET PLUMB ON THE FOUNDATION WITHOUT THE USE OF LEVELING NUTS, WASHERS OR SHIMS.
7. LIGHTING UNIT IDENTIFICATION NUMBERS SHALL BE INSTALLED BEFORE THE LIGHTING UNIT IS ENERGIZED.



SIDE VIEW (TRUSS ARM)
N.T.S.



SIDE VIEW (SINGLE MEMBER OR DAVIT ARM)
N.T.S.



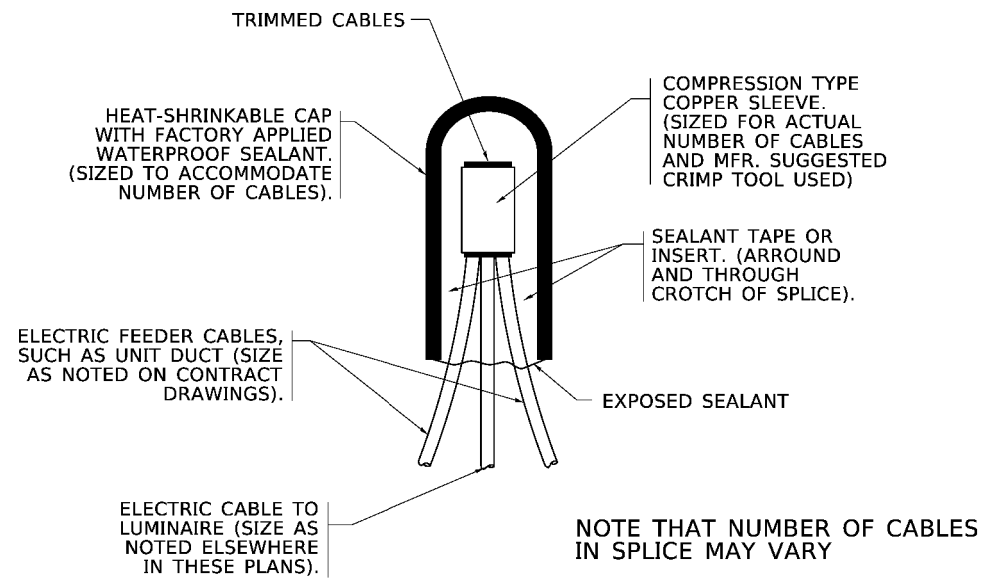
BOTTOM VIEW
N.T.S.

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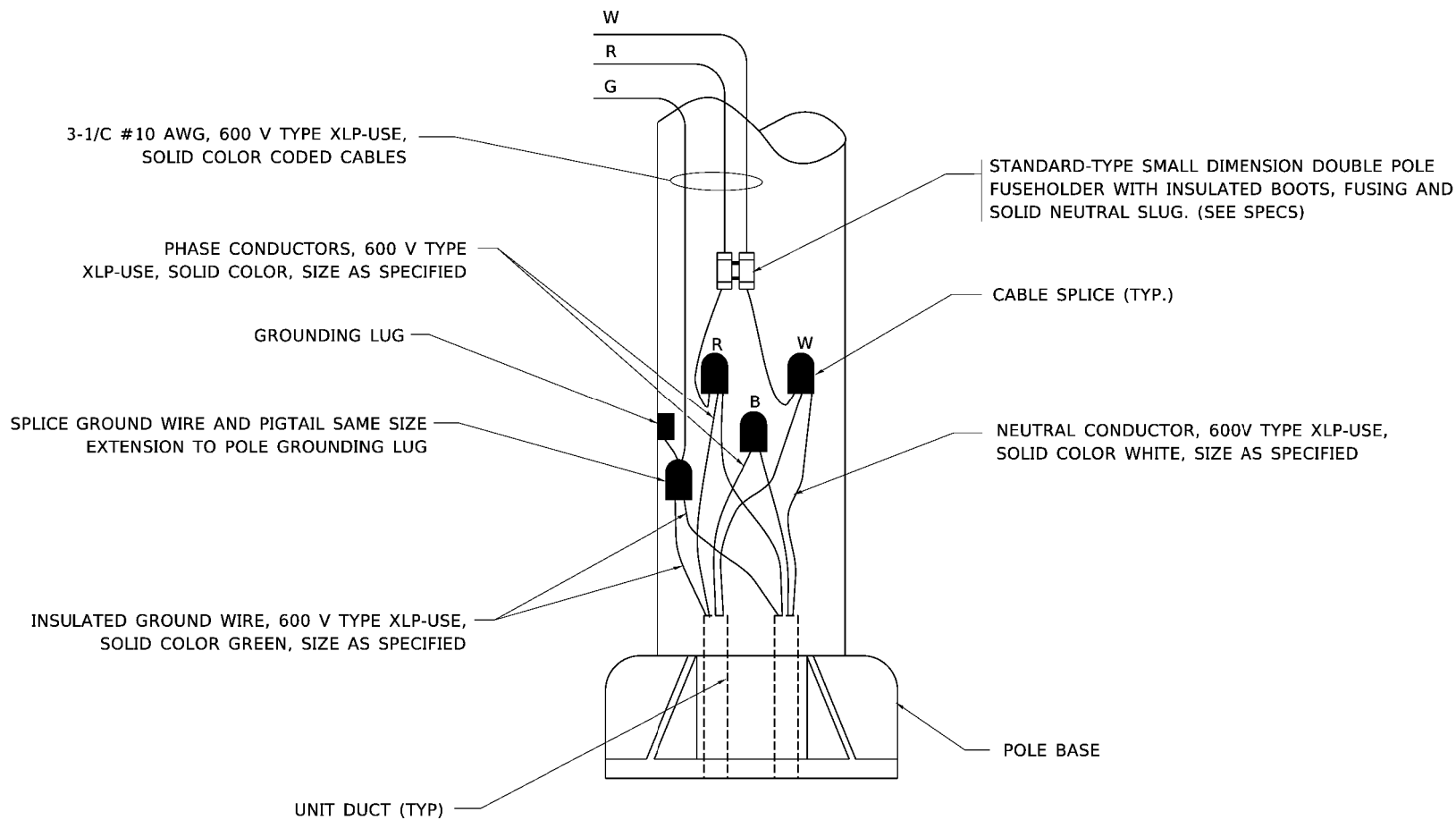
1. ALL DIMENSIONS ARE IN MILLIMETERS (INCHES) UNLESS OTHERWISE SHOWN.
2. CONTRACTOR SHALL ADJUST THE WIRE CLIP TO ELIMINATE ANY SLACK FROM THE WIRE ROPE.
3. THE 0.125" (3.18) STAINLESS STEEL AIRCRAFT CABLE SHALL REMAIN VISIBLE FROM THE GROUND LEVEL.
4. THE BREAKING STRENGTH OF THE CABLE SHALL BE 1700 LBS. MIN.

LT-11

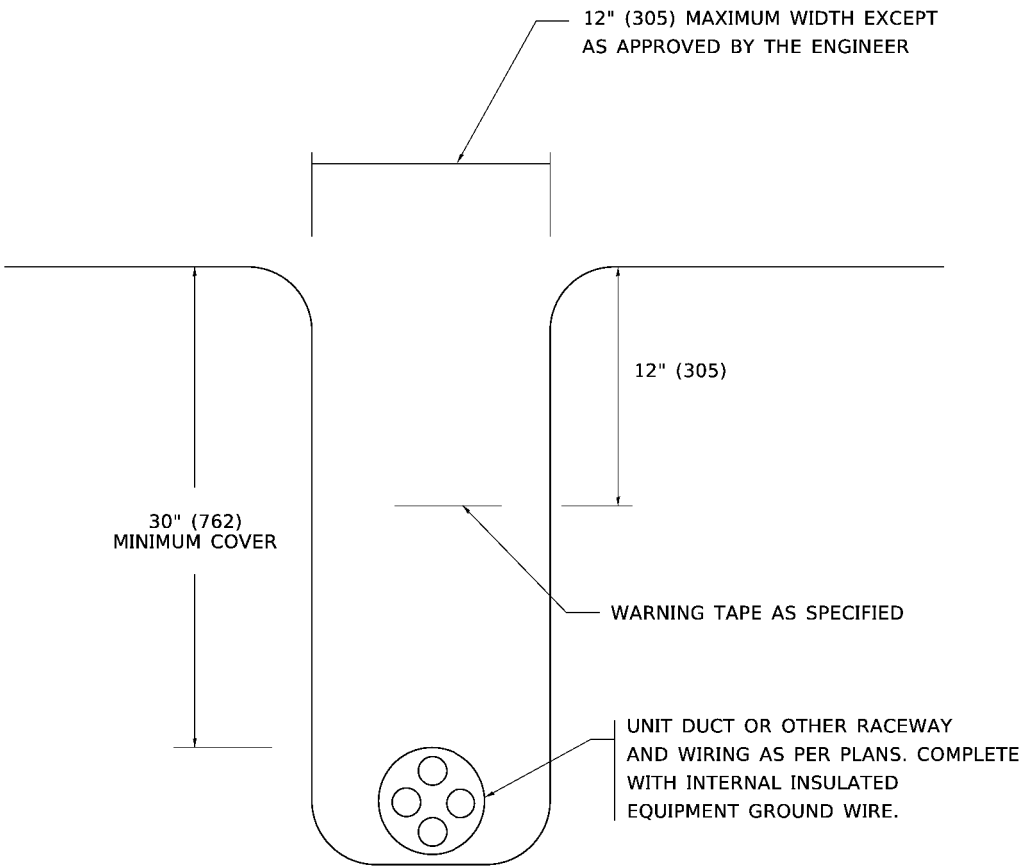
 <div>AMES Engineering, Inc. CONSULTING ENGINEERS 6330 Belmont Road, Suite 4B Downers Grove, IL 60516</div>	USER NAME = footemj	DESIGNED -	REVISED - 08-08-03	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	LUMINAIRE SAFETY CABLE ASSEMBLY					F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50.0000' / in.	DRAWN -	REVISED -							2525	20-00189-00-PV	KANE	184	126
	PLOT DATE = 4/19/2019	CHECKED -	REVISED -		BE-701 CONTRACT NO. 61K68									
		DATE -	REVISED -		ILLINOIS FED. AID PROJECT									
				SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.					



TYPICAL SPLICE DETAIL
N.T.S.



POLE WIRING DETAIL
N.T.S.



TYPICAL WIRING IN TRENCH DETAIL
N.T.S.

FOUNDATION DESIGN TABLE

TYPE OF SOIL	DESIGN DEPTH OF FOUNDATION		REINFORCEMENT IN FOUNDATION			
	SINGLE ARM D	TWIN ARM D	SINGLE ARM		TWIN ARM	
			VERT BARS	SPIRAL	VERT BARS	SPIRAL
SOFT CLAY	13'-0" (3.962 m)	15'-0" (4.572 m)	8-#6X12'-6" (3.810 m)	#3X122' (37.186 m)	8-#6X14'-3" (4.343 m)	#3X141' (42.977 m)
MEDIUM CLAY	9'-6" (2.896 m)	10'-9" (3.277 m)	8-#6X9'-0" (2.743 m)	#3X90' (27.432 m)	8-#6X10'-0" (3.048 m)	#3X100' (30.480 m)
STIFF CLAY	7'-0" (2.134 m)	8'-0" (2.438 m)	8-#6X6'-6" (1.981 m)	#3X66' (20.112 m)	8-#6X7'-6" (2.286 m)	#3X76' (23.165 m)
LOOSE SAND	9'-0" (2.743 m)	10'-0" (3.048 m)	8-#6X8'-6" (2.591 m)	#3X85' (25.908 m)	8-#6X9'-6" (2.896 m)	#3X94' (28.651 m)
MEDIUM SAND	8'-3" (2.515 m)	9'-0" (2.743 m)	8-#6X8'-0" (2.438 m)	#3X78' (23.774 m)	8-#6X8'-6" (2.591 m)	#3X85' (25.908 m)
DENSE SAND	7'-9" (2.362 m)	9'-0" (2.743 m)	8-#6X7'-6" (2.286 m)	#3X73' (22.250 m)	8-#6X8'-6" (2.591 m)	#3X85' (25.908 m)
ROCK OR SOLIDIFIED SLAG	5'-0" (1.524 m)	5'-0" (1.524 m)	NONE	NONE	NONE	NONE

OFFSET SCHEDULE

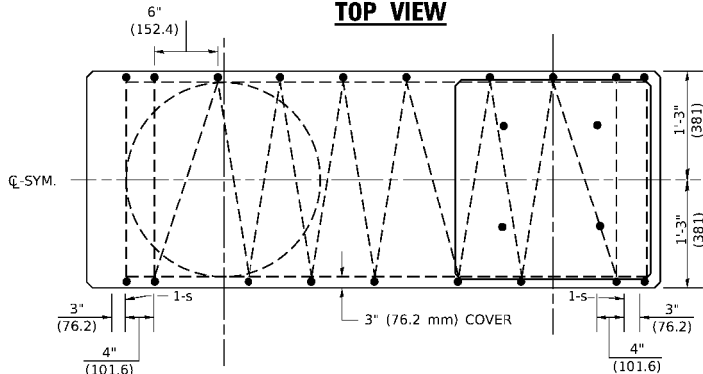
SEWER DIAM. d IN.	PILE OFFSET from C-MED'N FT.	LENGTH of BAR a FT.
UP TO 24" (609.6 mm)	3'-3" (0.991 m)	#6 x 5'-3" (1.600 m)
27" (685.8 mm) TO 36" (914.4 mm)	3'-9" (1.143 m)	5'-9" (1.753 m)
42" (1066.8 mm) TO 48" (1219.2 mm)	4'-6" (1.372 m)	6'-6" (1.981 m)
54" (1371.6 mm) TO 60" (1524.0 mm)	5'-0" (1.524 m)	7'-0" (2.134 m)
66" (1676.4 mm) TO 72" (1828.8 mm)	5'-6" (1.676 m)	7'-6" (2.286 m)

NOTES

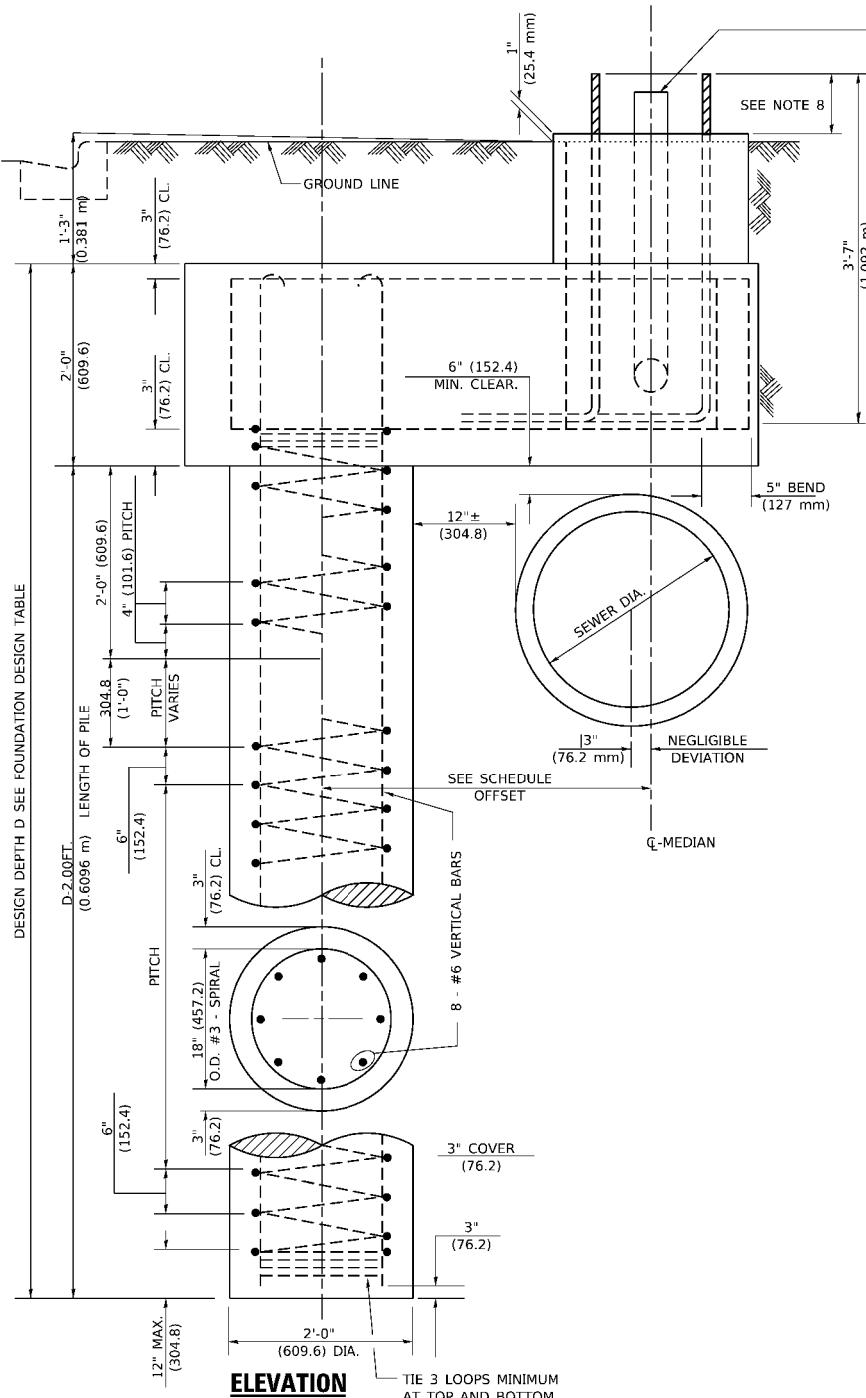
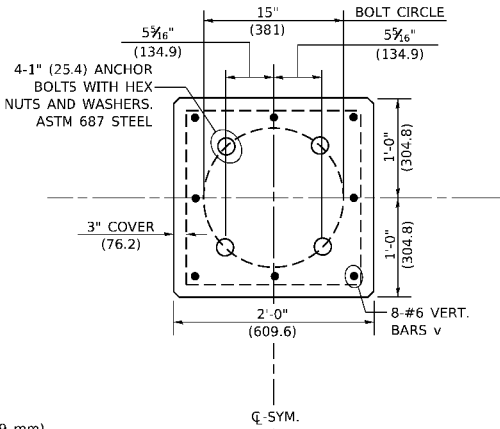
- ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.
- THE ENGINEER SHALL DETERMINE THE CLASS OF SOIL DURING EXCAVATION AND SELECT THE DESIGN DEPTH OF FOUNDATION FROM THE DESIGN TABLE.
- EXCAVATION OF THE POLE FOUNDATION SHALL BE MADE WITH AN AUGER, 24" (609.6 mm) OR 30" (762.0 mm) IN DIAMETER.
- THE ANCHOR ROD SHALL BE A HOOK ROD TYPE. COLD BENDING OF THE ANCHOR ROD WILL NOT BE ALLOWED. THE RADIUS OF THE HOOK BEND SHALL NOT BE LESS THAN 4 TIMES THE NOMINAL DIAMETER OF THE ANCHOR ROD. A TACK WELDED ANCHOR ROD MAY BE SUBSTITUTED WITH THE APPROVAL OF THE ENGINEER.
- THE ANCHOR BOLTS AND RACEWAYS SHALL BE PROPERLY SECURED IN PLACE BEFORE THE CONCRETE IS PLACED IN THE FORM.
- THE ANCHOR RODS SHALL BE ACCORDING TO ASTM F1554 GRADE 725 (GRADE 105). NUTS SHALL BE HEXAGON NUTS ACCORDING TO ASTM A 194 2H OR ASTM A 563 DH, AND WASHERS SHALL BE ACCORDING TO ASTM F 436.
- THE CONTRACTOR SHALL COORDINATE EXTENSION OF ANCHOR BOLTS ABOVE TOP OF FOUNDATION WITH THE BREAKAWAY DEVICE MANUFACTURER'S REQUIREMENTS. IF LIGHT POLE IS MOUNTED WITHOUT BREAKAWAY DEVICE, ANCHOR BOLTS SHALL PROJECT 23#4" (69.9 mm) ABOVE TOP OF THE FOUNDATION. THE CONTRACTOR SHALL CONFIRM ANCHOR BOLT EXTENTION WITH ENGINEER.
- RACEWAYS SHALL PROJECT 1" (25.4 mm) ABOVE THE TOP OF THE FOUNDATION.
- THE CABLE TRENCH SHALL BE BACKFILLED AND FIRMLY COMPACTED BEFORE THE LIGHT IS ERCTED.

PLAN-CAP BEAM

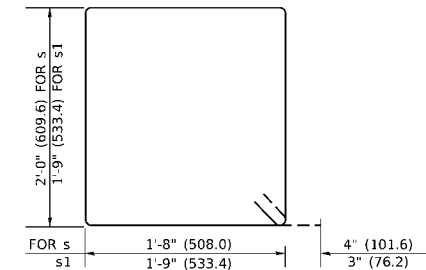
TOP VIEW



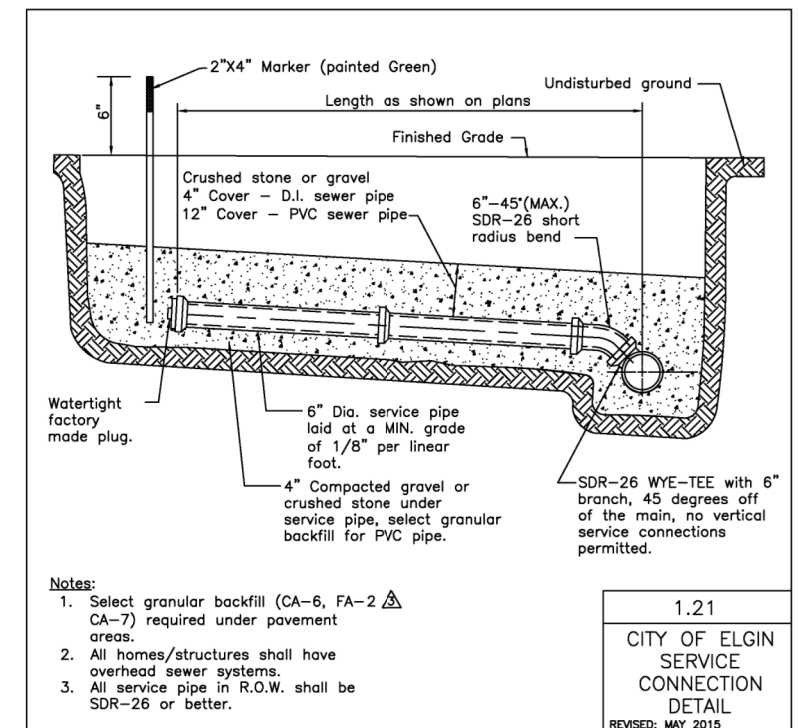
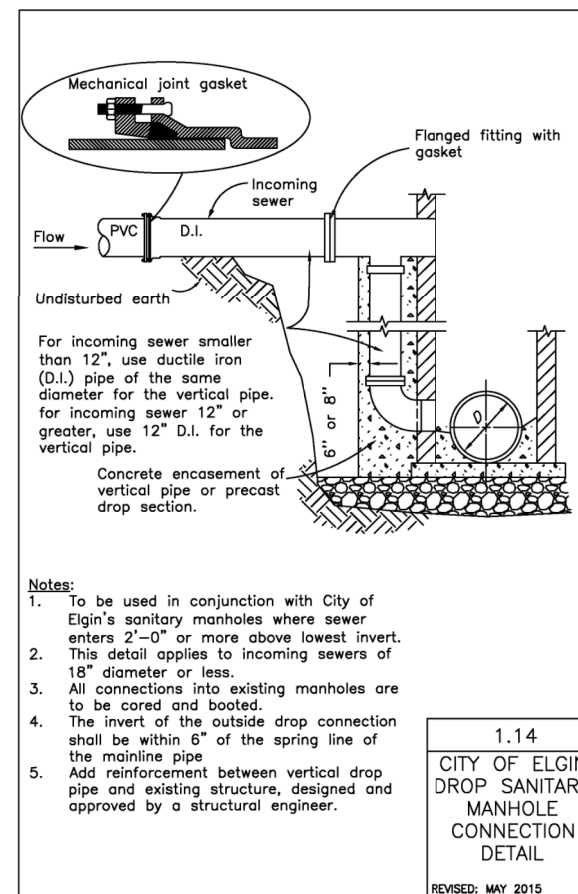
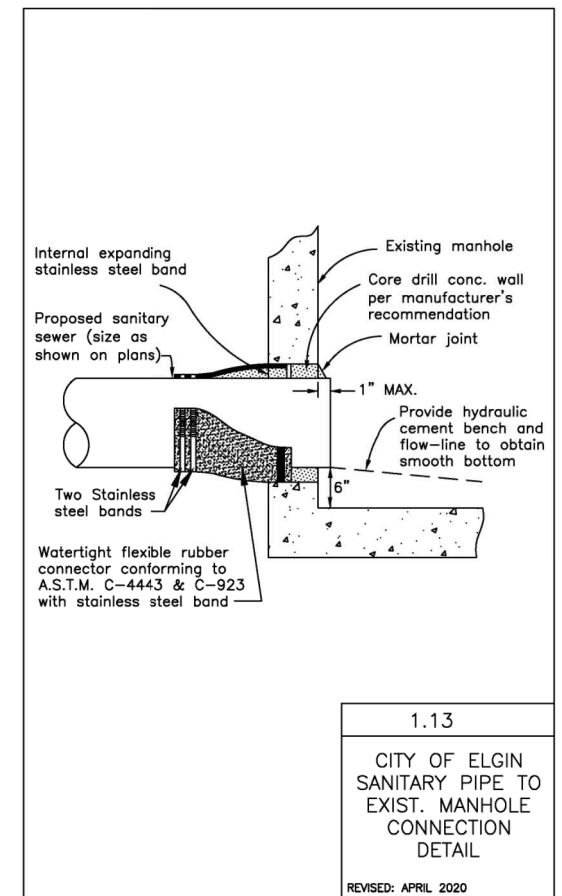
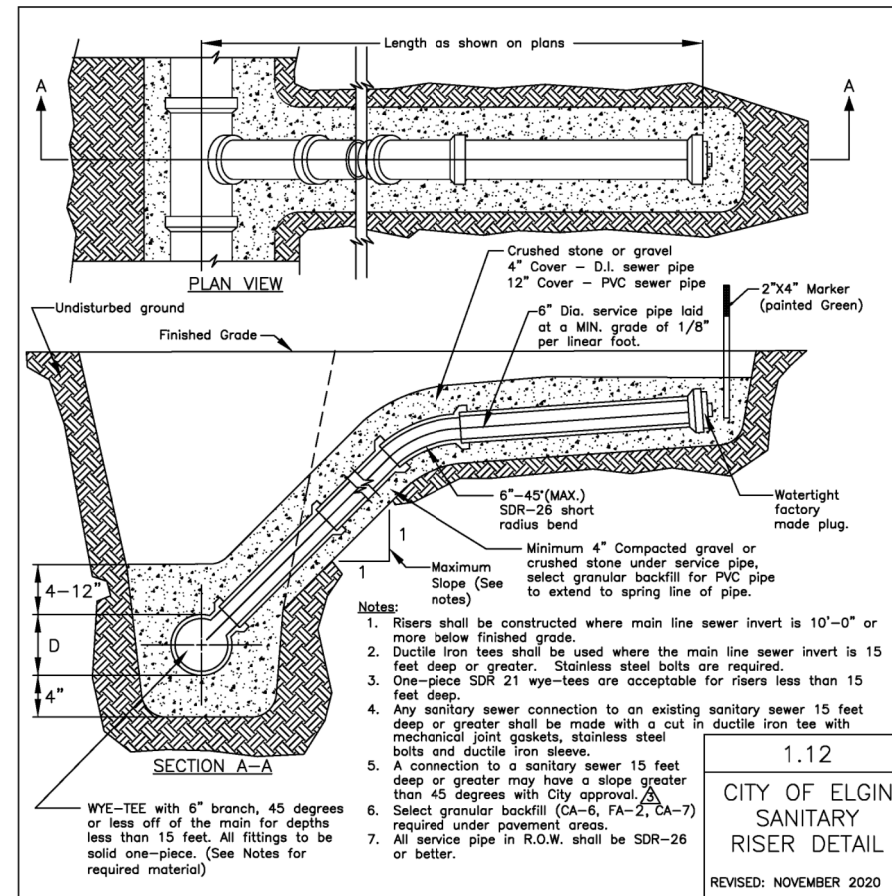
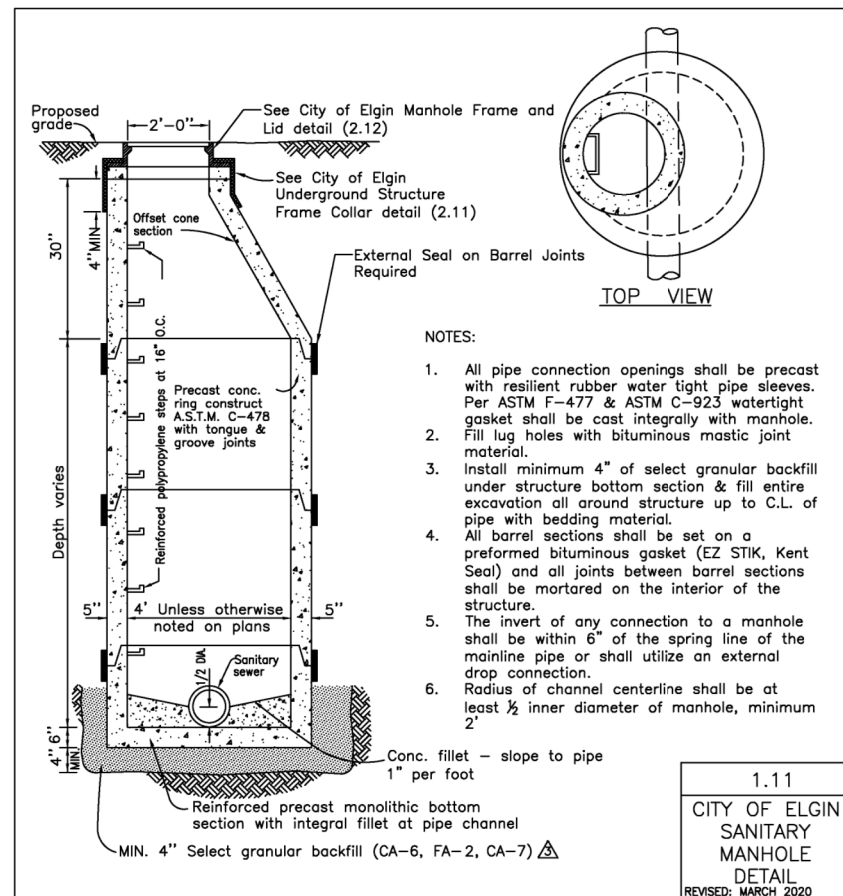
TOP VIEW



END VIEW



BARS s, s1



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YEARS



Engineering Enterprises, Inc.

CONSULTING ENGINEERS

52 Wheeler Road

Sugar Grove, Illinois 60554

630.466.6700 / www.eeifweb.com

CITY OF ELGIN
150 DEXTER COURT
ELGIN, IL 60120

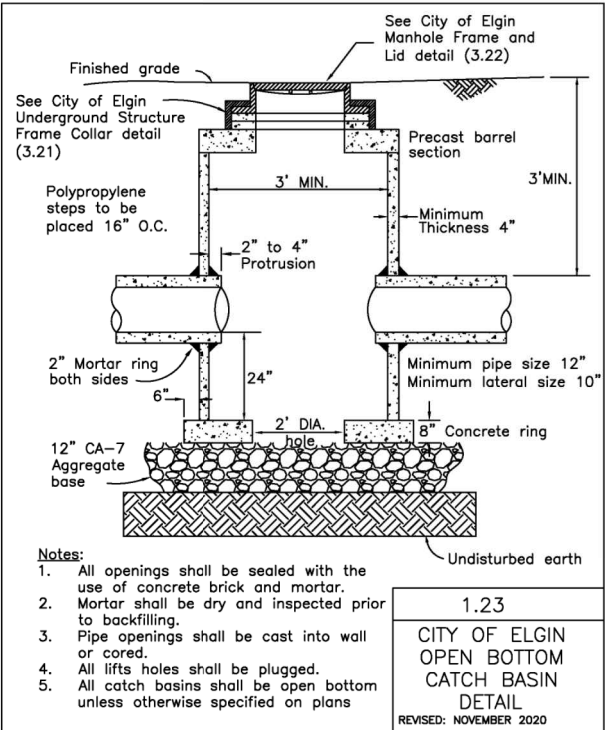
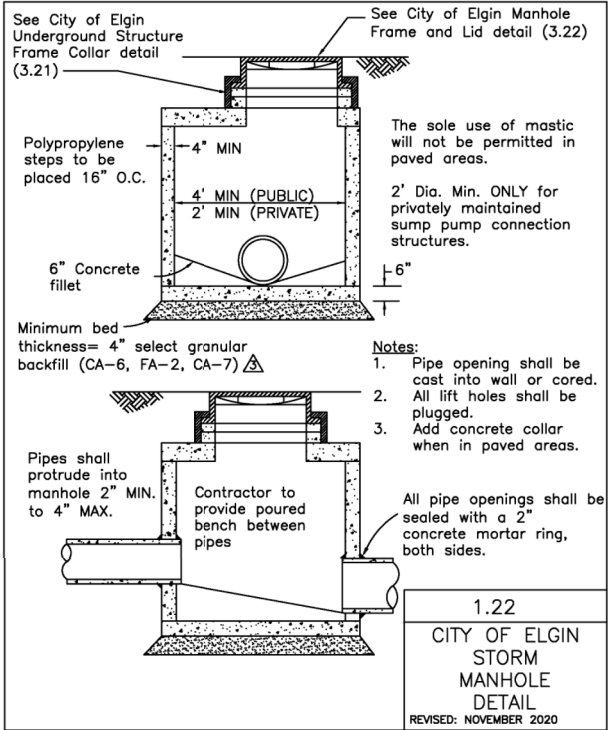
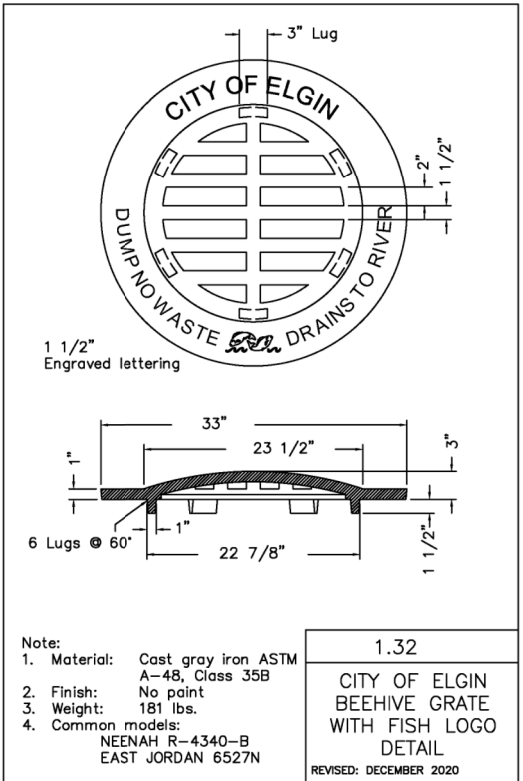
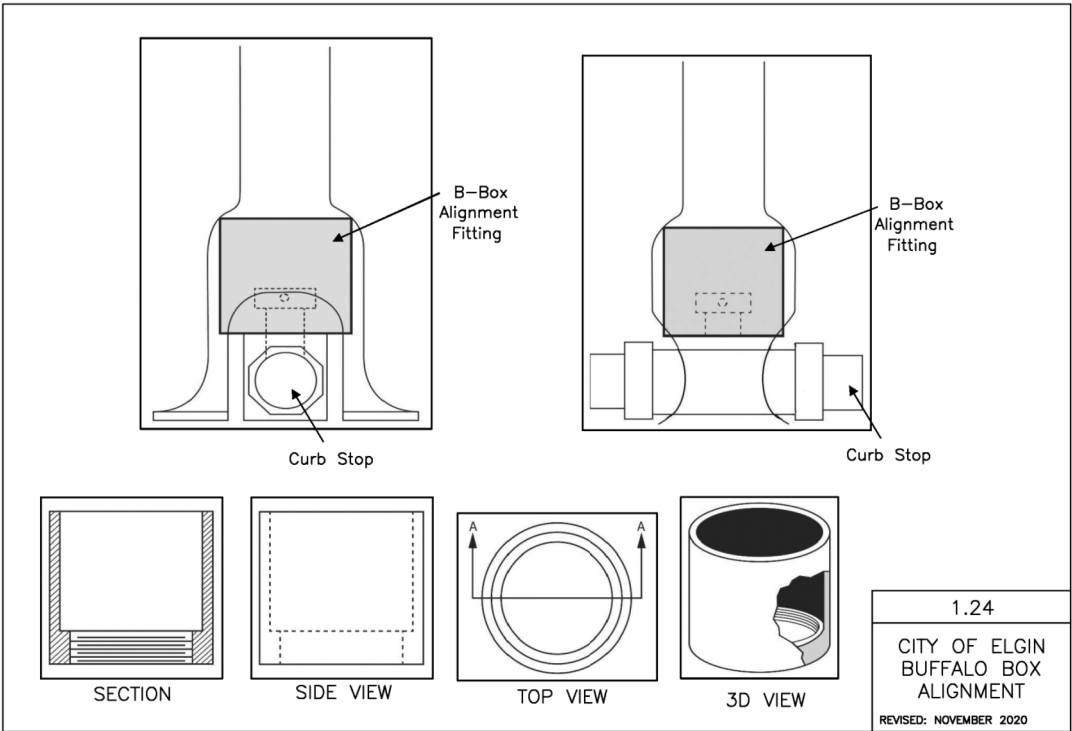
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DUNDEE AVENUE
CITY OF ELGIN DETAILS

SCALE: SHEET 2 OF 7 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2525	20-00189-00-PV	KANE	184	130
CONTRACT NO. 61K68				
ILLINOIS FED. AID PROJECT				



STANDARD
DETAILS

CITY OF ELGIN

1900 Holmes Road
Elgin, IL 60123
www.cityofelgin.org

Phone: 847-931-6100
Fax: 847-931-5965

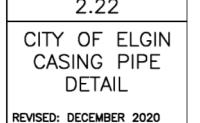
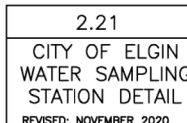
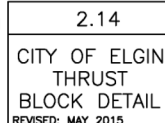
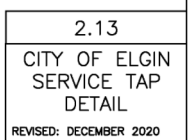
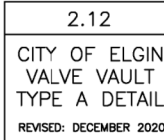
ENGINEERING
DEPARTMENT

ELGIN
THE CITY IN THE SUBURBS

PROJ. MOR.:
PROJ. ENG.:
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CHECKED BY:
DATE: 12-04-20
SCALE: N.T.S.

SHEET
1B OF 3

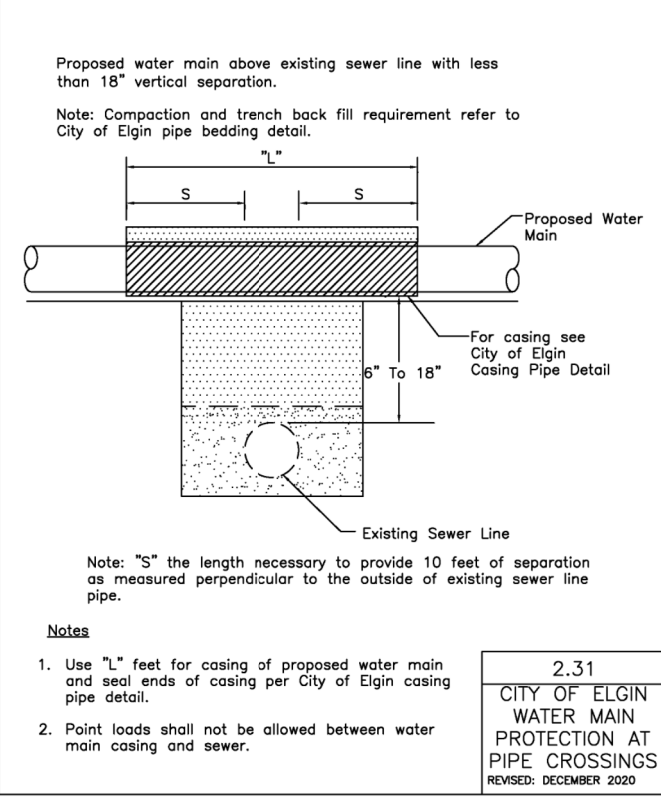
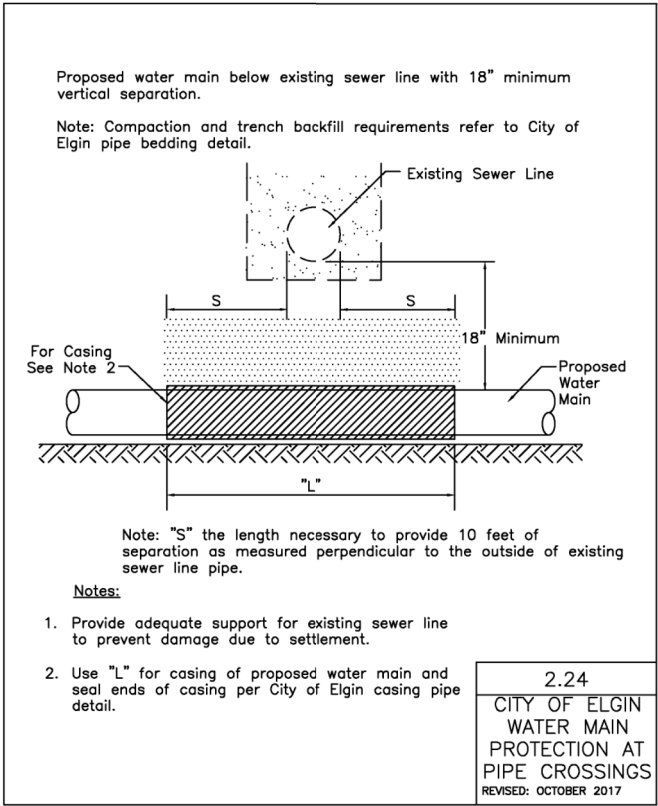
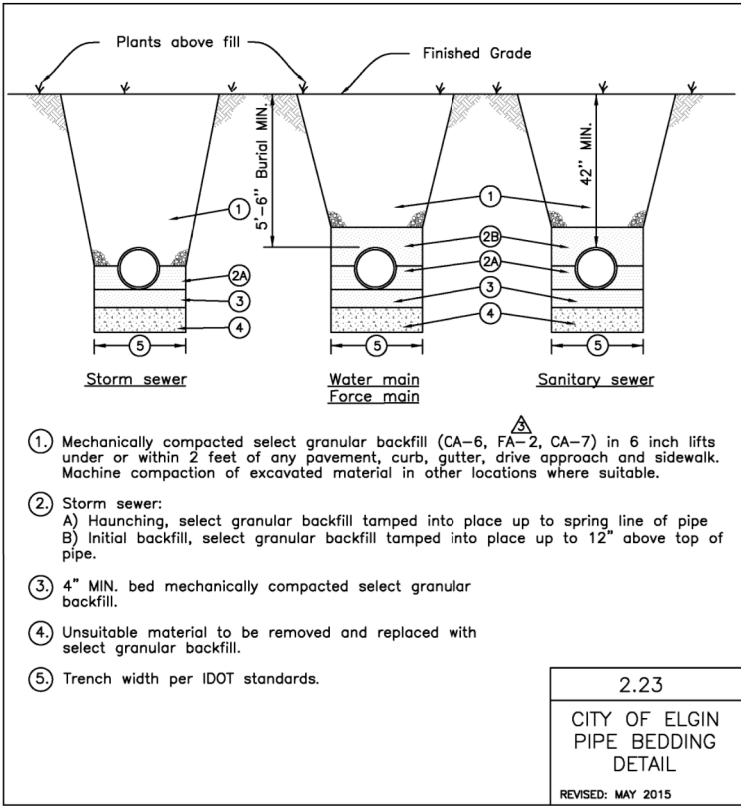
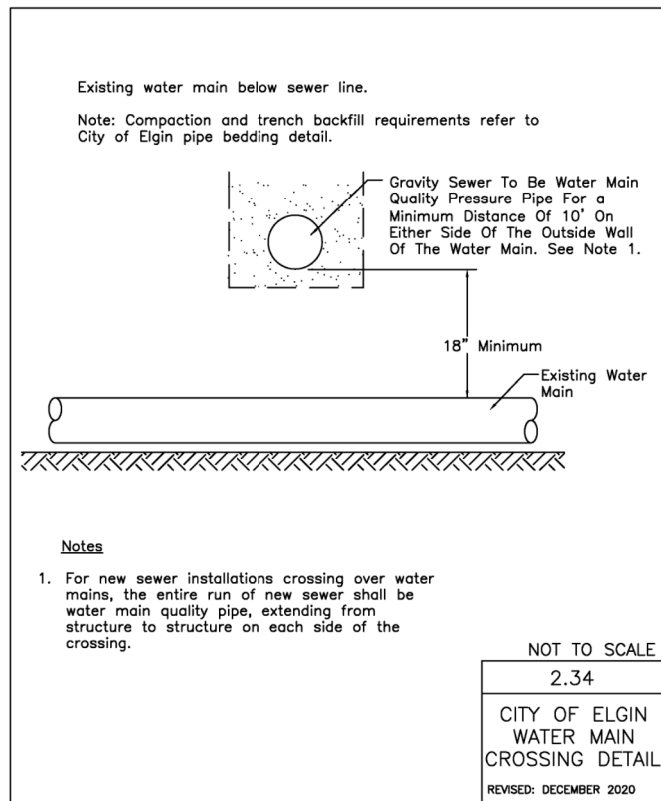
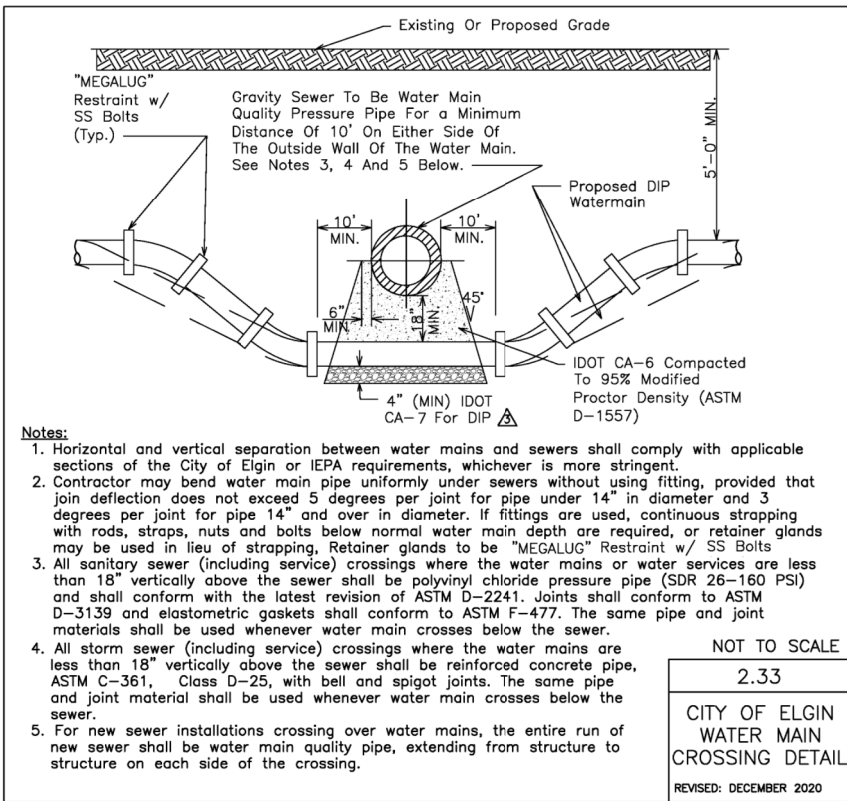
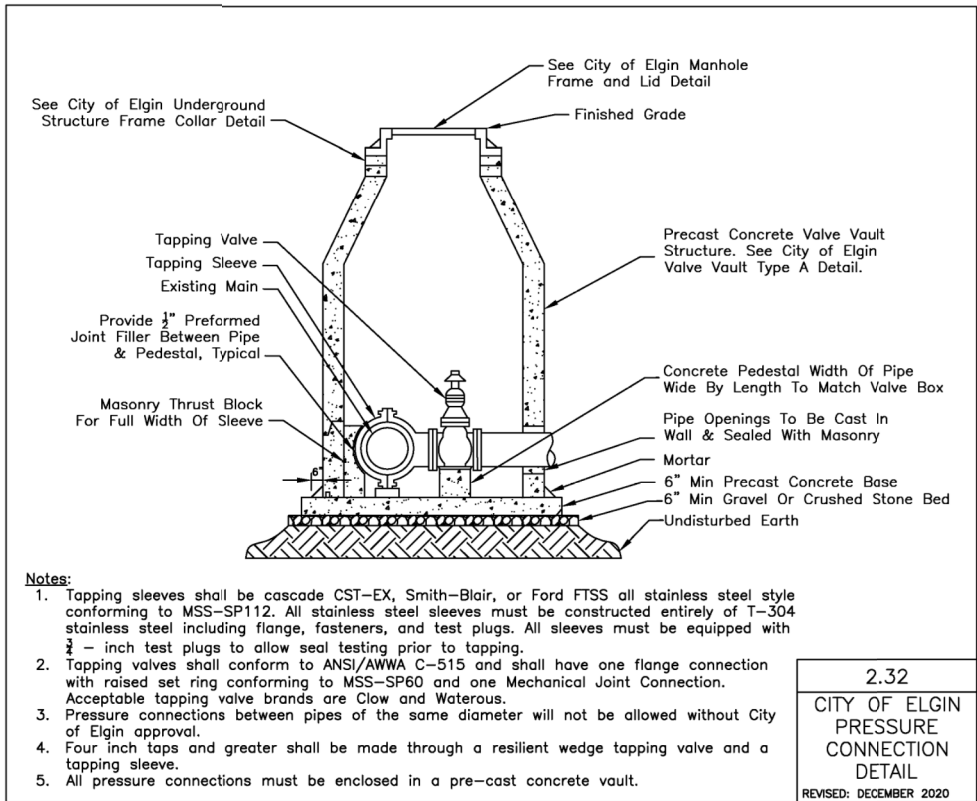
2.11
CITY OF ELGIN
FIRE HYDRANT
DETAIL
REVISED: DECEMBER 2020



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DATE -	REVISED -

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2525	20-00189-00-PV	KANE	184	132
CONTRACT NO. 61K68				
ILLINOIS FED. AID PROJECT				



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YEARS



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Sugar Grove, Illinois 60554
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CITY OF ELGIN
150 DEXTER COURT
ELGIN, IL. 60120

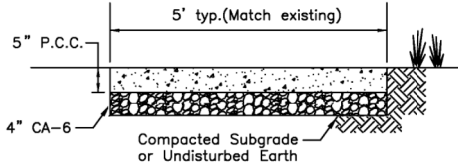
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DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DUNDEE AVENUE
CITY OF ELGIN DETAILS

SCALE: SHEET 5 OF 7 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2525	20-00189-00-PV	KANE	184	133
CONTRACT NO. 61K68				
ILLINOIS FED. AID PROJECT				



5' P.C.C.
4' CA-6
Compacted Subgrade or Undisturbed Earth

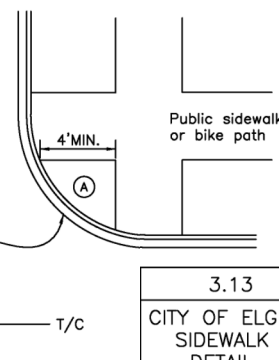
5' typ. (Match existing)

Notes:

- Expansion joints located at 50' o.c. using full depth 1/2" pre-molded expansion joint filler recessed 1/2" from the surface
- 1/2" tooled contraction joints located at 5' o.c.
- Concrete to be light broom finish in direction of sidewalk width
- All sidewalk not to exceed 2% cross slope and meet current ADA requirements
- Use two (2) #4 reinforcing bars across utility trenches with a 2' overhang outside the trench limits
- Dowel two (2) #4 reinforcing bars when tying into existing.
- Expansion joints located on one side for replacements less than 15', both sides for replacements more than 15'.

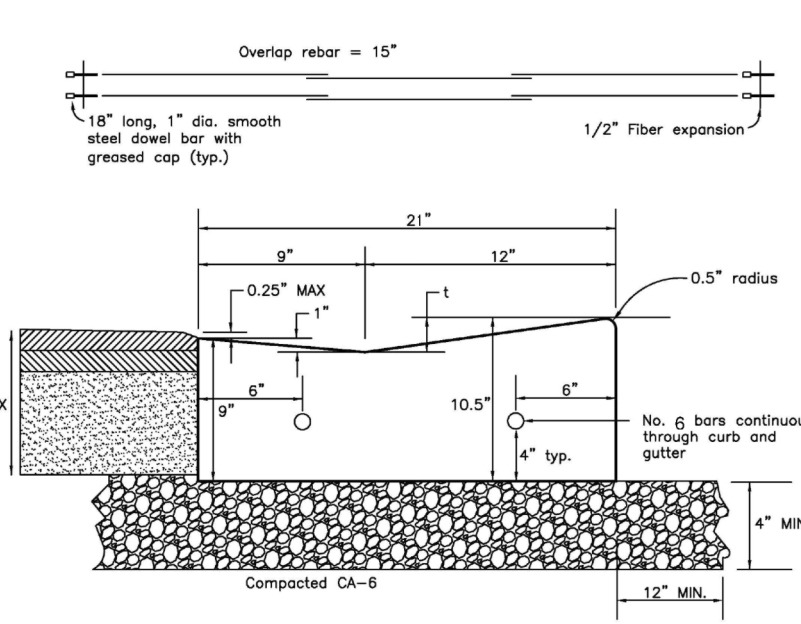
(A) If the minimum dimensions can be met for barrier curb to be placed at the corner, this area shall be in grass.

If the minimum dimensions cannot be met due to right-of-way restrictions, depressed curb shall be continued and this area shall be the same cross section as the sidewalk or bike path.



4' MIN.
5' MIN.
6' MIN.
T/C

3.13
CITY OF ELGIN
SIDEWALK
DETAIL
REVISED: APRIL 2020



Overlap rebar = 15"

18" long, 1" dia. smooth steel dowel bar with greased cap (typ.)

1/2" Fiber expansion

0.25" MAX

9"

12"

21"

10.5"

6"

4" typ.

No. 6 bars continuous through curb and gutter

4" MIN.

12" MIN.

Compacted CA-6

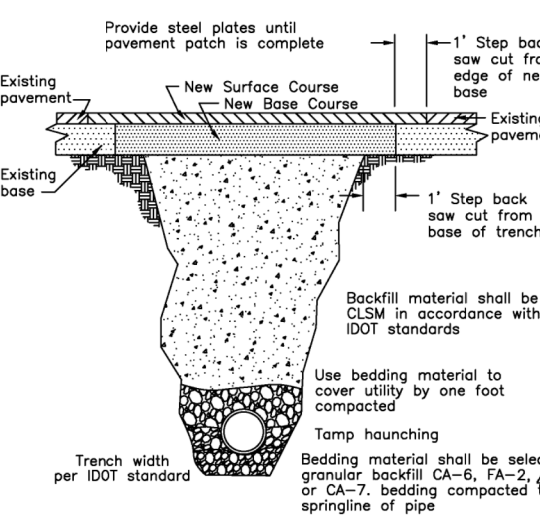
Notes:

- Contraction joints to be tooled or sawcut every 10'
- Fiber expansion joints to be placed every 60' (every 3rd rebar of 20' length) use 1/2" expansion material full depth with two - 18" long, 1" dia. smooth steel dowel bars with greased caps.

t = Standard curb 2.50"
Handicap ramp 1.00"

X = 2" HMA Surface Course
2.25" HMA Binder Course
4.75" HMA Base Course

B-MODIFIED
CURB DETAIL
REVISED: JUNE 2024



Provide steel plates until pavement patch is complete

Existing pavement

New Surface Course

New Base Course

Existing pavement

Existing base

1' Step back saw cut from edge of new base

1' Step back saw cut from base of trench

Backfill material shall be CLSM in accordance with IDOT standards

Use bedding material to cover utility by one foot compacted

Tamp haunching

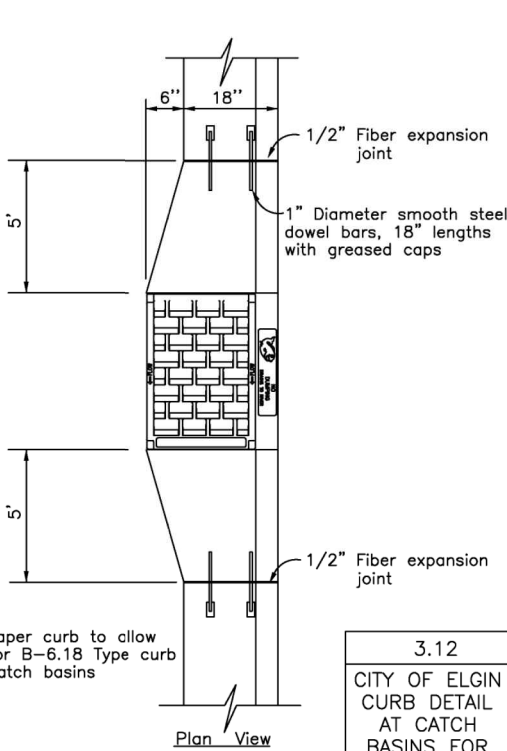
Bedding material shall be select granular backfill CA-6, FA-2, or CA-7, bedding compacted to springline of pipe

Trench width per IDOT standard

Notes:

- All saw cuts shall be full depth
- For concrete/brick base streets, repair of pavement shall include PCC base equal or greater thickness than existing with a 6" MIN. 2-1/2" binder course followed by a 1-1/2" surface course.
- For all other base types the street repair shall be the same except the pcc base shall be replaced with a bituminous base course.
- No blocking for pipe construction shall be left under pipe

3.11
CITY OF ELGIN
STREET CUT
REPAIR
STANDARD
REVISED: MARCH 2020



6"

18"

1/2" Fiber expansion joint

1" Diameter smooth steel dowel bars, 18" lengths with greased caps

1/2" Fiber expansion joint

Taper curb to allow for B-6.18 Type curb catch basins

Plan View

3.12
CITY OF ELGIN
CURB DETAIL
AT CATCH
BASINS FOR
B-6.12 C&G
REVISED: MAY 2015

STANDARD
DETAILS

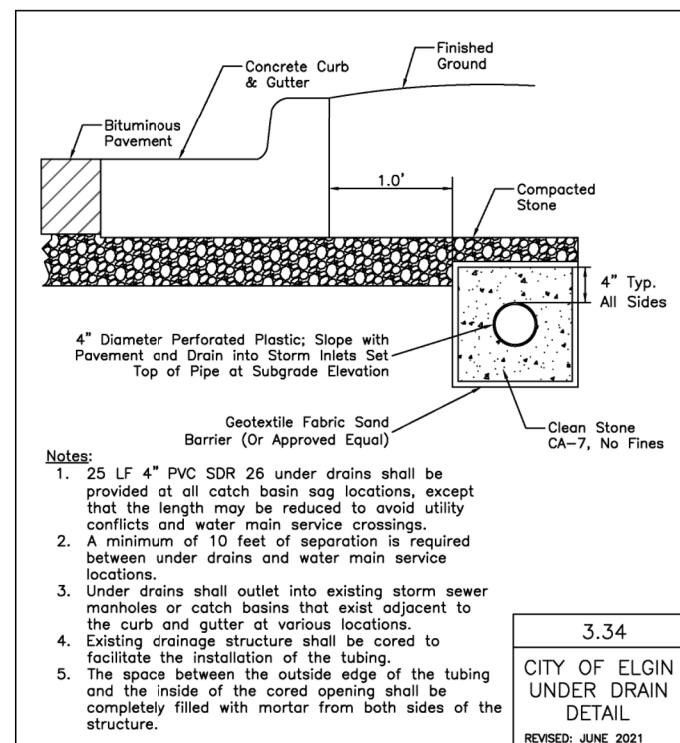
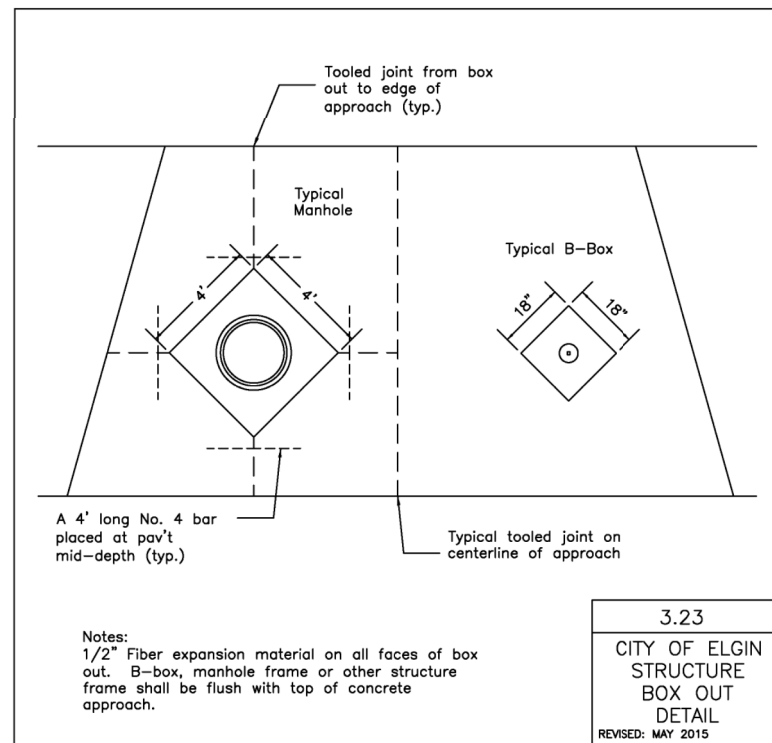
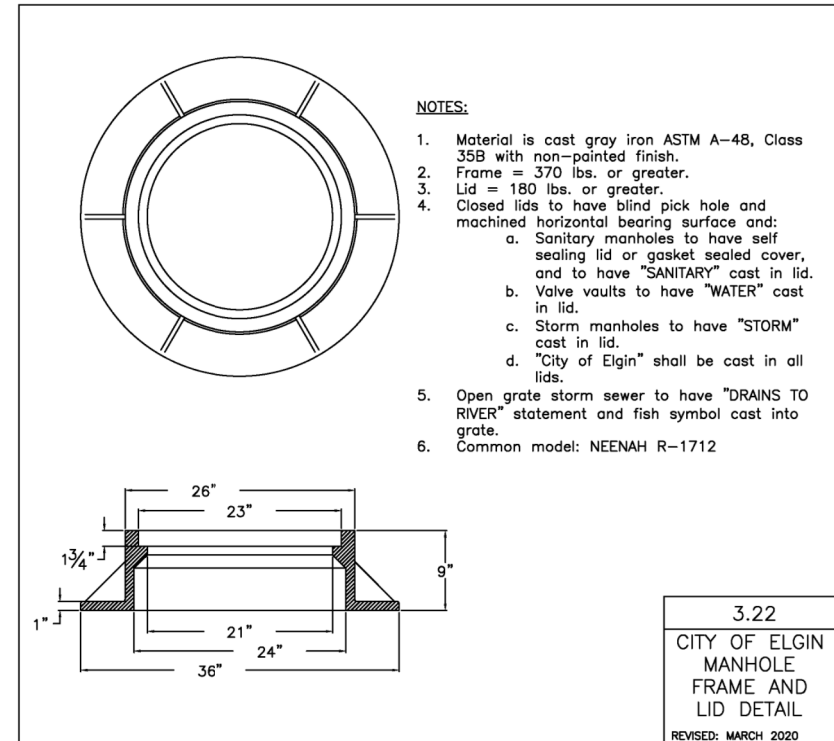
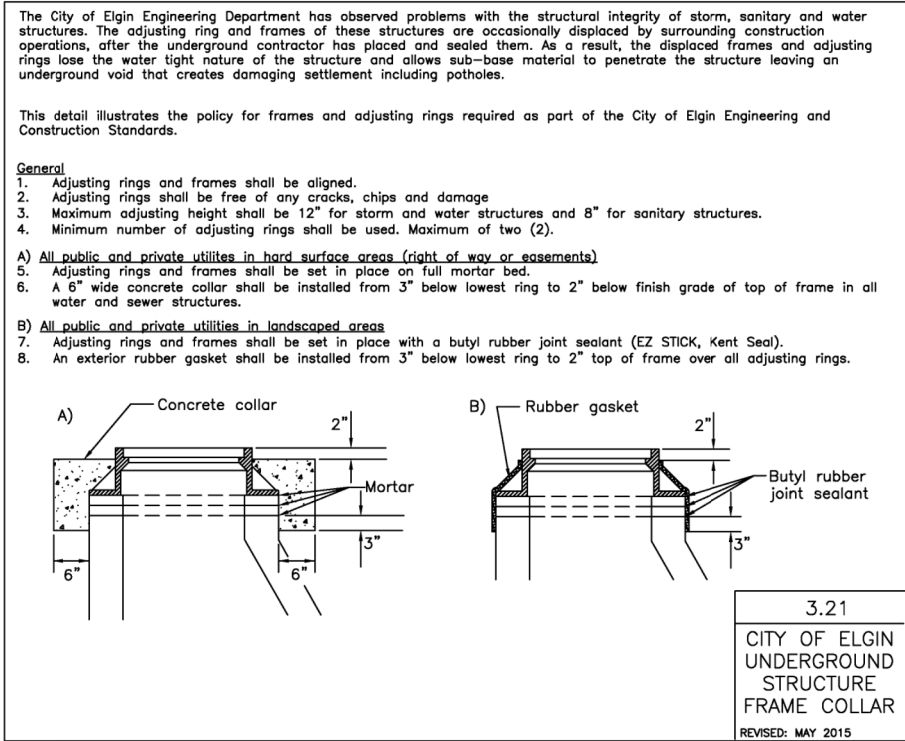
CITY OF ELGIN
1900 Holmes Road
Elgin, IL 60123
Phone: 847-931-6100
Fax: 847-931-5965
www.cityofelgin.org

ENGINEERING
DEPARTMENT



PROJ. NO.:
PROJ. ENG.:
DRAWN BY: BAS
CHECKED BY:
DATE: 12-04-20
SCALE: N.T.S.
SHEET
3A OF 3

DATE	REVISIONS
11/17/2020	WATER DEPARTMENT - CHANGES TO 3.12
12/07/2021	ADD 3.14 BRIDGEMAN DETAIL
02/17/2024	CHANGE 3.12 TO 3.12.2



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Engineering Enterprises, Inc.
CONSULTING ENGINEERS
52 Wheeler Road
Sugar Grove, Illinois 60554
630.466.6700 / www.eelweb.com

CITY OF ELGIN
150 DEXTER COURT
ELGIN, IL 60120

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CHECKED -	REVISED -
DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DUNDEE AVENUE
CITY OF ELGIN DETAILS

SCALE: SHEET 6 OF 7 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2525	20-00189-00-PV	KANE	184	134
CONTRACT NO. 61K68				
ILLINOIS FED. AID PROJECT				

CITY OF ELGIN
1900 Holmes Road
Elgin, IL 60123
Phone: 847-931-6100
Fax: 847-931-5965
www.cityofelgin.org

**ENGINEERING
DEPARTMENT**



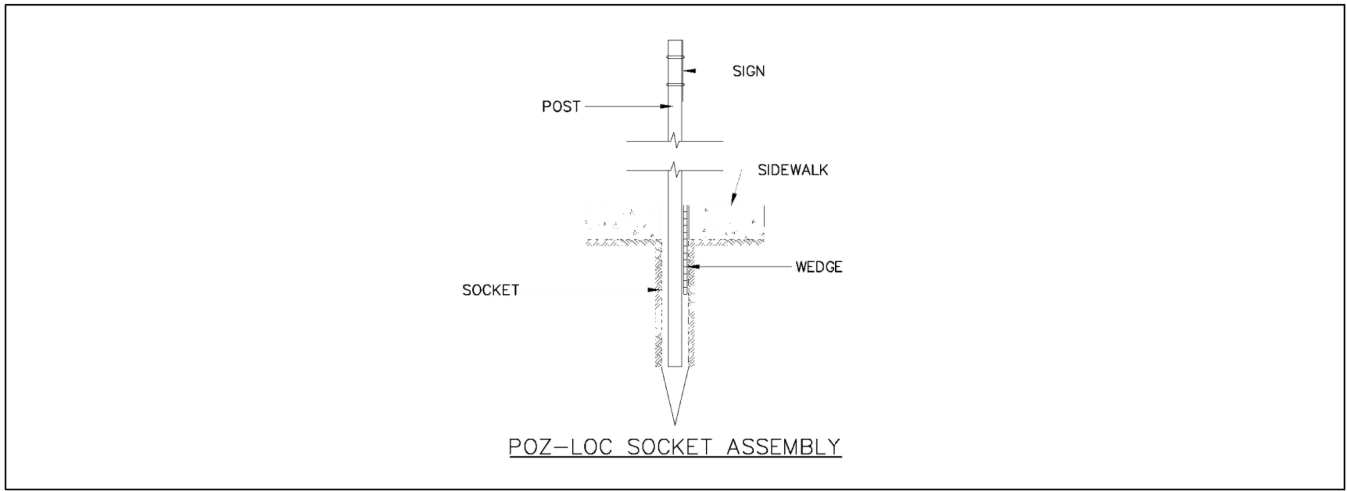
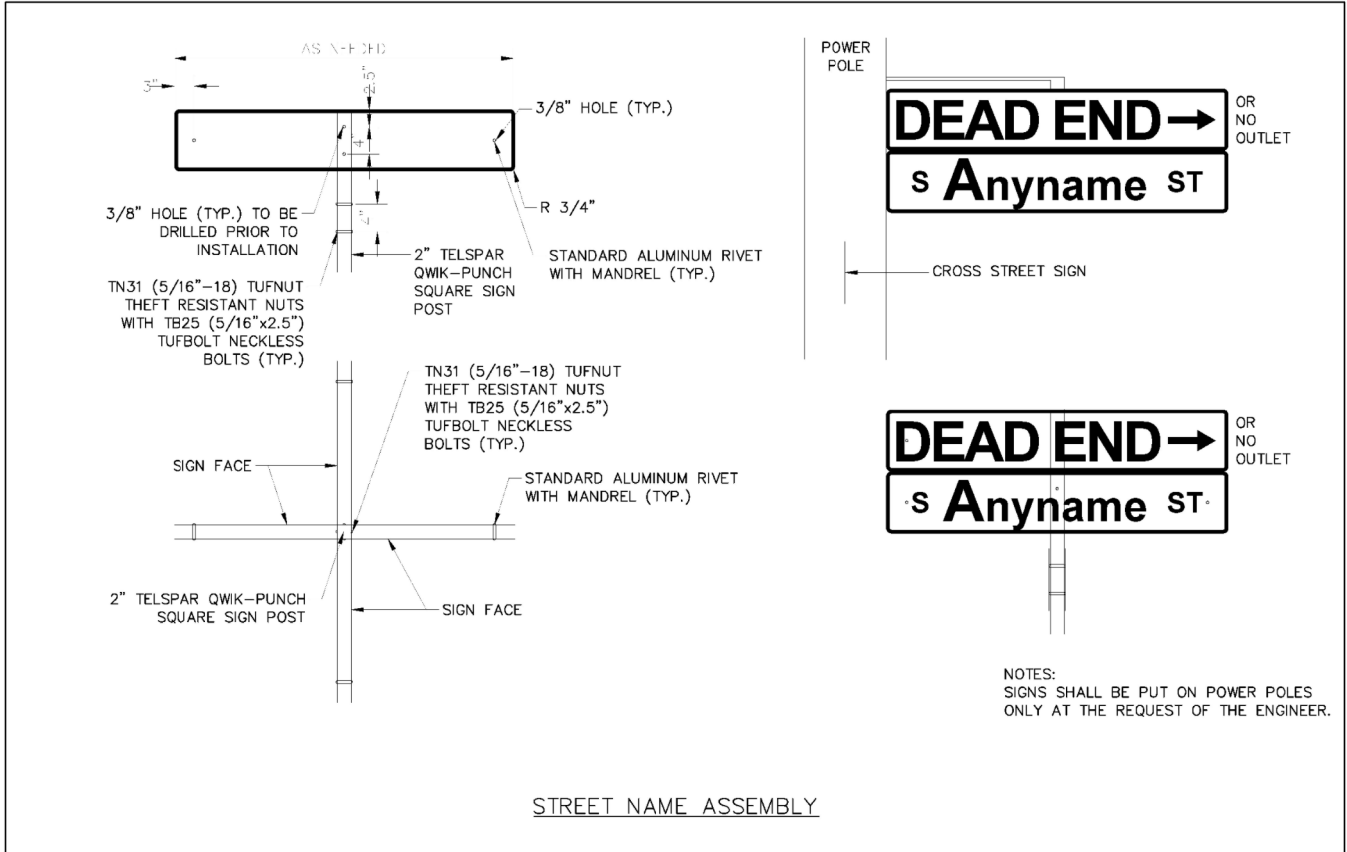
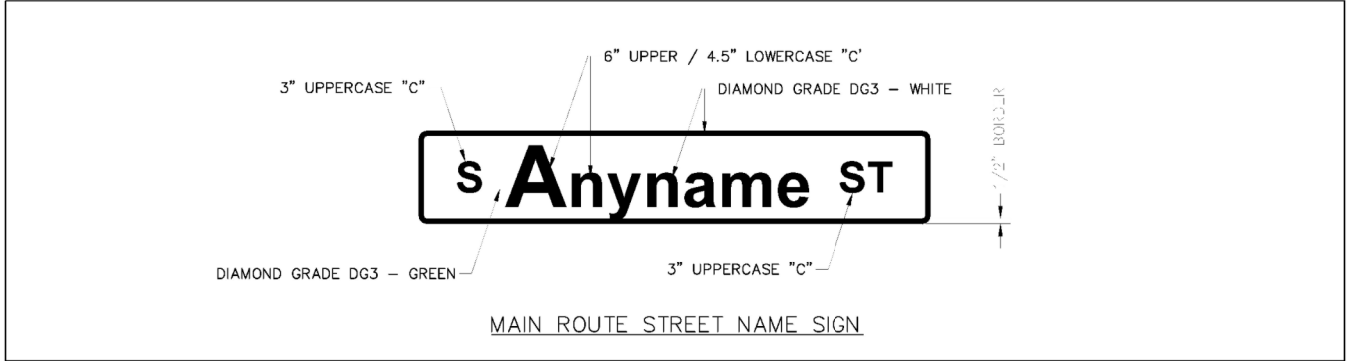
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PROJ. ENG.:
DRAWN BY: BAS
CHECKED BY:
DATE: 12-04-20
SCALE: N.T.S.

SHEET
3B OF 3

DATE	REVISIONS	BY	CHKD BY
11/12/2020	WATER DEPARTMENT - CHANGES TO 2.12	BS	AD
10/14/2024	CHANGES TO 2.12	BS	AD
10/14/2024	CHANGES TO 2.12	BS	AD

DESIGNED -	REVISED -
DRAWN -	REVISED -
CHECKED -	REVISED -
DATE -	REVISED -

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2525	20-00189-00-PV	KANE	184	135
CONTRACT NO. 61K68				



City of Elgin Sign Standards

Purpose
To establish positive guidelines for the manufacture, location, and installation of traffic, informational and warning signs in the City of Elgin. Requests for clarification should be addressed to the Streets Supervisor, Public Works. Guidelines for sign manufacture, placement and clearance shall be followed as set forth by the latest Manual On Uniform Traffic Control Devices Edition (hereafter referred to as the MUTCD).

Sign Panels

Manufacture:
All sign faces shall be applied to .080" aluminum blank which has been properly cleaned and degreased prior to the application of the sign face. All blanks shall have standard corner radii as set forth by the MUTCD. The sign back shall not be covered or painted in any way without consent of the Director of Public Works. Holes shall be drilled and deburred to provide clearance for a 5/16" bolt and space for use on standard traffic sign posts. The words "City of Elgin" shall be stamped on all blanks.

Sign Sheeting:
Signs shall be sheeted with 3M materials. Either 3M Electro-Cut Film or silk-screening is permitted although silk-screened signs shall have permanent, protective overlay film (3M Series 1160) applied to resist graffiti. The words "City of Elgin" and date of manufacture in month/year code shall be visible in the lower right hand corner of the border using a maximum of 3/8" letters.

Sign Location:
It is the City's intent to minimize the number of sign posts within the City limits. Therefore, care should be taken to install signs on existing structures whenever possible. These structures include street light poles, traffic signal poles, and utility poles. Care should also be taken to ensure that signs are still placed in accordance with all applicable MUTCD standards. Signs shall be placed at MUTCD minimum distance or per City Engineer approval. Strict adherence to section 2A-22 and 2A-23 must be maintained. In instances of a curb-side sidewalk less than 5" wide, the post should be placed immediately behind the sidewalk. In cases where the sidewalk is wider or circumstances prevent it, the post shall be mounted in a Poz-Loc socket though a cored hole and held in place with a wedge (southwestern Pipe part numbers 6601 and 6603 respectively). Mortar should be applied to fill any voids between socket and sidewalk. In the event that core drilling is not feasible, an aluminum surface base as previously described may be used.

Stop Signs:
Elgin has adopted a 30" x 30" (for single lane) and 36" x 36" (for 2 or more lanes) Diamond Grade DG3 stop sign protected by 3M 1160 Overlay Film as its standard. The common installation is one stop sign on each pole. All-way placards placed below the stop sign are ASTM Type III Sheeting, if applicable. MUTCD figure 2A-2 illustrates the required setback for stop signs prior to the crosswalk. A minimum of 72" must be maintained between the post and preceding edge of the crosswalk whether it is painted or not.

Warning Signs:
Due to the nature of their message, warning signs shall be made using Diamond Grade DG3 material and shall be placed in accordance with the Table 2C-4 of the MUTCD. Care must be taken to match the correct message with road conditions.

Street Signs:
Except where later noted, all street name signs shall use a 9" tall blank utilizing Diamond Grade DG3 6" upper/lower case Highway Gothic "C" letters on a reflective green field with a border. Road type designations, i.e. "Rd", "St", "Ln", etc. shall be of two letters except where longer abbreviations are necessary, such as "Blvd".

Hardware:
All signs are to have a nylon washer between the face of the sign and the fastener. Signs shall be mounted using a combination of stainless steel and aluminum fasteners and brackets.

- Double-face signs shall be mounted using a Vulcan VS-318 double bracket with SS 5/16" - 18 x 3/4" Torx button pin head bolts and lock washers.
- Signs will be mounted to wooden utility poles using SS 5/16" x 1 1/2" or 2" lag bolts.
- Signs of less than 9 sq. ft. that are banded to poles shall use SS 3/4" x .025 strapping, SS straight leg brackets and SS 5/16"-18 x 3/4" Torx button pin head bolts with lock washers.
- Signs over 9 sq. ft that are banded to poles shall use Signfix medium extruded channel (code #MAC MIL) and Signfix SS Universal Channel Clamps.
- Single-face signs on 2" posts shall use TN31 (5/16-18) Tufnut Theft Resistant Nuts with TB25 (5/16x2.5") Tufbolt Neckless Bolts.
- Nine inch street name blades are either mounted onto 2" posts illustrated or mounted to metal light/traffic poles with a B36 Wing Bracket and steel shank rivets.

Cantilever
VS-1C Cantilever
• 14 1/2" Long Arm
• For Flat and Extruded Blades
• All Aluminum
• New Item: VS-1 Extension
- Extends arm to 29" long

VS-B36 Metro Cantilever (For use with 9" or larger blanks)
• 36" Extra Long arm
• No Hardware Included-Must Field Drill
• All Aluminum
• Attach Sign Using Steel Shank Rivets

Basis of Payment:
• This work will be paid for at the contract unit price per Each for STREET NAME SIGN ASSEMBLY - 9"
• This work will be paid for at the contract unit price per Each for STOP SIGN.
• This work will be paid for at the contract unit price per SQ FT for SIGN PANEL.

Traffic Post

Sign Posts:
Posts shall be Telespar Qwik-Punch Square Sign Posts (2" x 2"), galvanized steel tubing welded to A.S.T.M. specification A-525, having a wall thickness of .083 and weighing approximately 2.16 lbs per foot. Posts shall be powder-coated using Glidden Black applied to a minimum thickness of three mils over Bondrite "37" zinc phosphate pre-treatment. Approved vendors include:

TAPCO 800 Wall St. Elm Grove, WI 53122 630-561-5495	TCP 31 W. 351 North Ave. West Chicago, IL 60185 630-293-0026	Hwy Technologies 880 N. Addison Rd. Villa Park, IL 60181 630-932-4600
--	---	--

No retroreflective material shall be used on the sign support.
Specification Traffic Post:
• Outside Diameter: Shall be 2 inches
• Wall Thickness: Shall be .083
• Weight Per Foot: Shall be 2.16
• Length: Shall be 10ft. - 11ft. - 12ft. - 13ft. - 14ft.

Post:
Post shall be welded steel tubing conforming to A.S.T.M. A-513 specifications made from hot dipped galvanized steel sheet conforming to A.S.T.M. specifications A-525 or the tube may be hot dipped galvanized to obtain a zinc weight of 1.25 oz/ft.2 of sheet, which is a G-90 commercial weight.

Coating Properties:
Polyester powder coating of the post shall be Glidden P616 Black. The coating shall be applied over the galvanized post to minimum dry mil thickness of 3.0 mils. The tubing shall be properly cleaned and pre-treated to achieve the coating properties below. The following properties are based on the application of 3.0 mils of TGIC cured thermosetting polyester powder coatings applied over Bondrite "37" zinc phosphate pretreatment galvanized steel.

Damage Resistance	
Pencil Hardness	H
Gardner Impact	160 Inch Pounds
Flexibility	Pass 1.8" Mandrel
Adhesion	No failure with 1/16" cross hatch

Corrosion Resistance	
Salt Spray	1/16" Creepage at 1000 hours (ASTM B 117 - Scribed)
Humidity Cabinet	1000 hours - no blisters (ASTM D-1735)

Weathering Resistance	
Weathermeter	Minimal change after 100 hours
ASTM G26	No loss in adhesion
	Excellent color retention; Minimal chalking

Chemical Resistance	
Substance Effect on Coating	
Gasoline	None
Alcohol	None
Sodium Hydroxide	None
Ammonium Hydroxide	None
Nitric Acid	None
Sulfuric Acid	None
Mineral Spirits	None

Post Anchors:
Post will be anchored into the ground using the following methods:
• Direct into soil - Tapco V-Loc, part #34-3 with #34-4 wedge
• Through concrete - Poz Loc Socket #6601 with wedge #6603 as supplied by Southwestern Pipe

The POZ-LOC Sign Post Anchor System is a tubular socket system designed to be used for Type I small sign supports. The assembly consists of:

- A galvanized 2-3/8" O.D. traffic post available in various wall thicknesses.
- A tubular socket 2-7/8" O.D. x 12 GA wall thickness x 27" long. The socket is pointed to facilitate driving into the ground and accept a standard 2-3/8" O.D. sign post, which inserts into the socket.
- A wedge which is driven between the socket and the post and functions to lock the post into the socket.
- Sign mounting brackets which clamp onto the post. These brackets allow the sign to be mounted at any angle or front and back of the post. Pre-punched 12 holes in the traffic post may be substituted for brackets.

Should the post be damaged, or otherwise need to be removed, the wedge can be removed with a wedge puller, another post inserted, and the wedge replaced without disturbing the footing. The use of a special wedge puller discourages vandalism of the sign system. This system does not require any nuts or bolts for installation of the socket system.

*The POZ-LOC Sign Post and Socket System meets all the requirements of the present FHWA 2200# auto crash test and also the 1800# auto crash test criteria. The POZ-LOC system is approved by the FHWA.

- Bolted to Sidewalk - 2 1/4" x 10" aluminum tube passing through and welded to a 6" x 1/4" aluminum plate. Plate is to be clearance drilled at each corner to accept a 1/2" HiTi Quick-bolt. Post is to be attached to the base using two aluminum drive rivets opposed to each other at 90 degrees.

Sign Removal

All sign panels shall be removed from the posts, the hardware and the sign posts shall be completely removed. All items shall be transported to the Elgin Public Works Building. This work shall be coordinated no less 48 hours prior to the anticipated delivery with:

Elgin Public Works
Traffic Division
1900 Holmes Road
Elgin, IL 60123
847-697-3160

The new sign panels shall be completely installed prior to removal of the existing assembly. Duplicate assemblies shall not exist for periods in excess of 24 hours

Basis of Payment:
This work will be paid for at the contract unit price per Each for SIGN TO BE

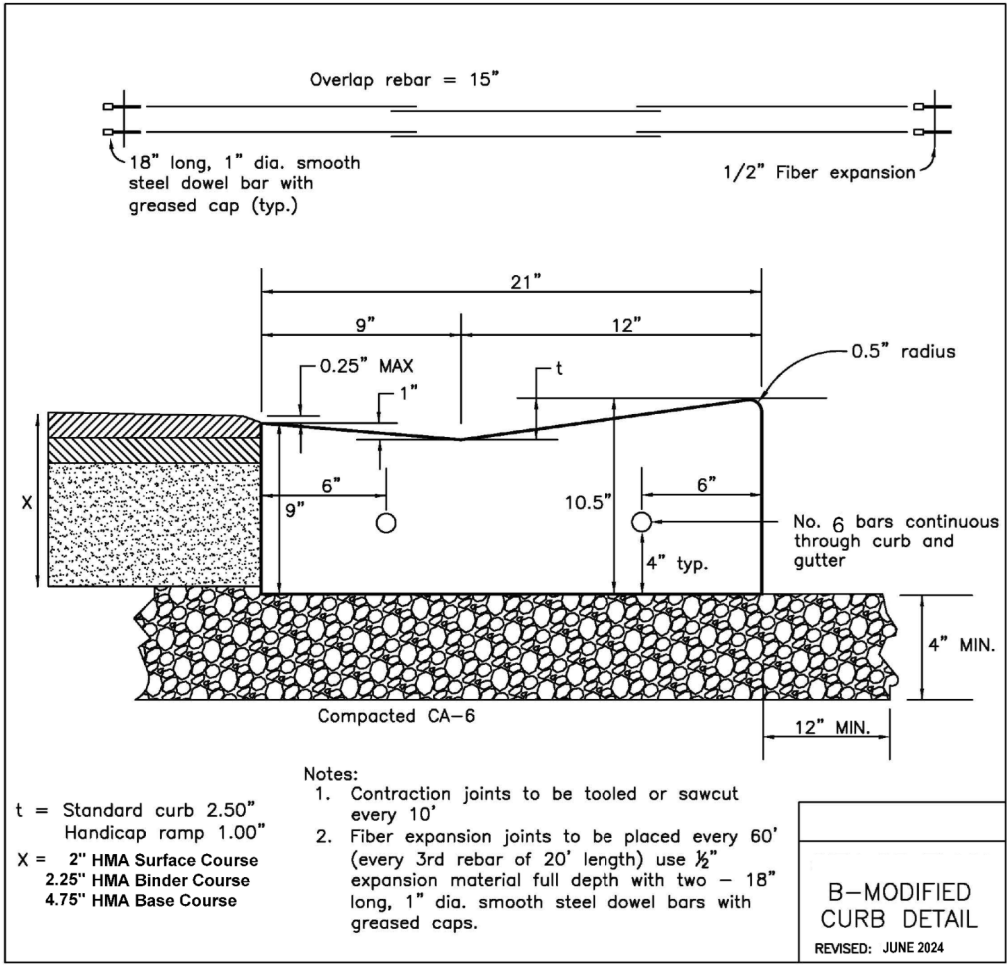
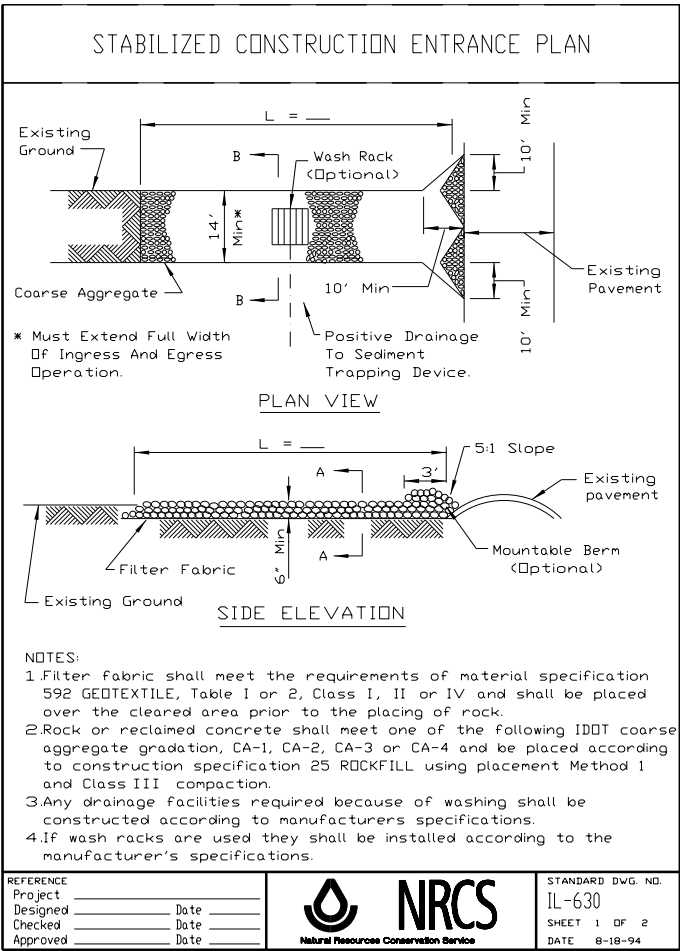
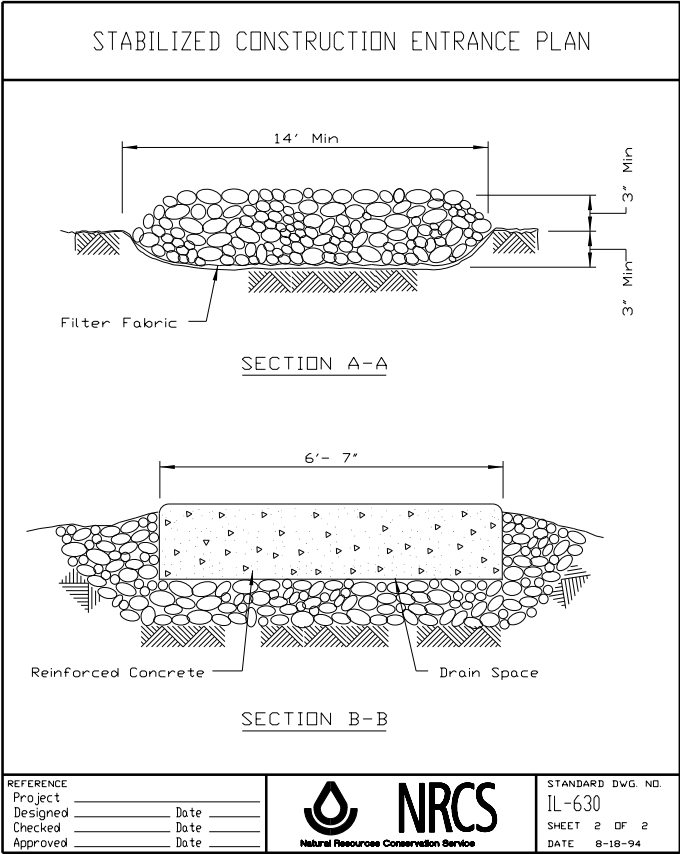
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YEARS



Engineering Enterprises, Inc.
CONSULTING ENGINEERS
52 Wheeler Road
Sugar Grove, Illinois 60554
630.466.6700 / www.eeiweb.com

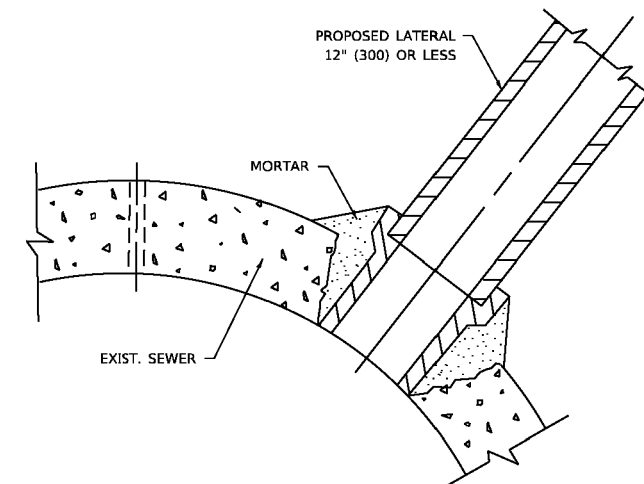
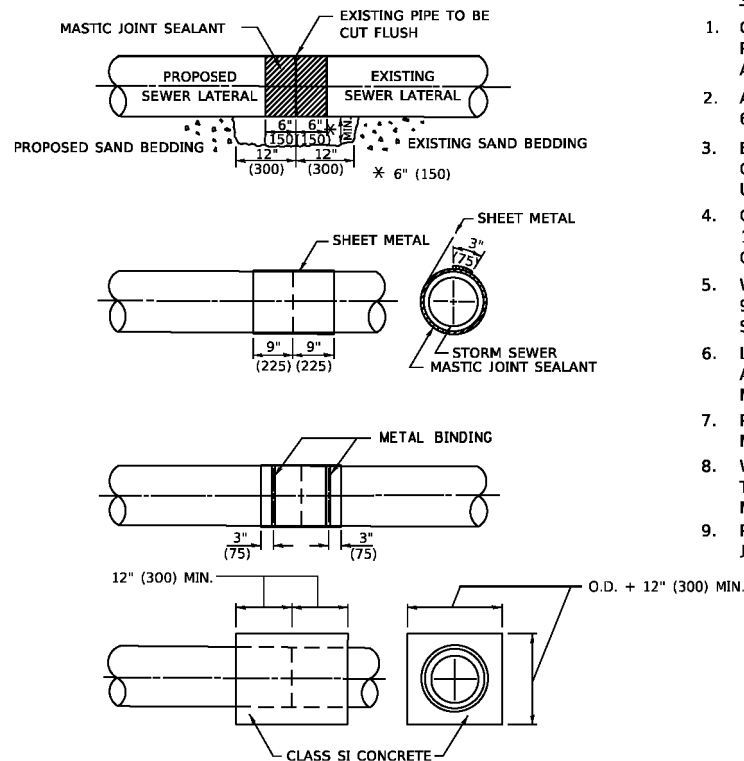
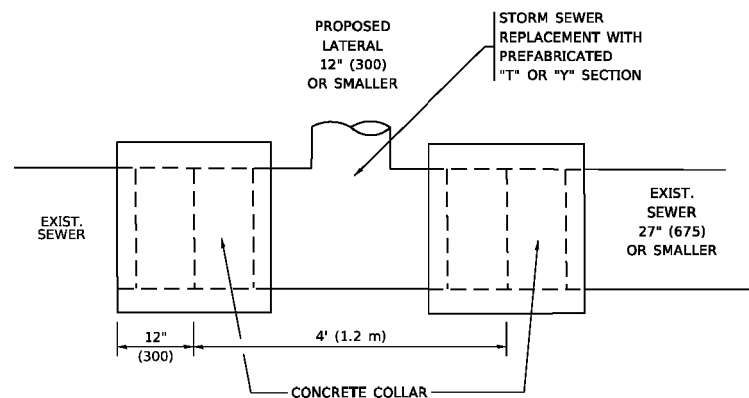


STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DUNDEE AVENUE
SITE DETAILS

SCALE: SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2525	20-00189-00-PV	KANE	184	136
CONTRACT NO. 61K68				
ILLINOIS FED. AID PROJECT				



- ## CONSTRUCTION SEQUENCE

1. CUT THE EXISTING END OF THE PIPE SO AS TO PRESENT A FLUSH BUTT JOINT. BRUSH AND CLEAN ALL PIPES.
2. APPLY THE MASTIC JOINT SEALANT TO THE FIRST 6" (150) OF EACH PIPE.
3. BUTT THE PIPES TOGETHER LEAVING A MINIMUM OF 12' x 6' (300 x 150) DEEP EXCAVATION UNDER AND AROUND EACH PIPE END.
4. CUT A PIECE OF SHEET METAL GAGE NO. 19 1.1 (0.0418) 18" (450) WIDE BY THE OUTSIDE CIRCUMFERENCE OF THE PIPE PLUS 3" (75) LONG.
5. WRAP THE SHEET METAL AROUND THE PIPES, 9" (225) ON EACH SIDE OF THE JOINT, STARTING AT THE TOP OF THE PIPE.
6. LAP THE SHEET METAL AT LEAST 3" (75) AT THE TOP OF THE PIPE AND PLACE THE MASTIC JOINT SEALANT BETWEEN THE LAP.
7. PLACE TWO METAL BANDS AROUND THE SHEET METAL AND TIGHTEN.
8. WIPE OFF ANY EXCESS MASTIC JOINT SEALANT THAT OZES OUT FROM BETWEEN THE SHEET METAL AND THE PIPES.
9. PLACE CLASS SI CONCRETE AROUND THE JOINT.

NOTES:

MATERIAL

MATERIAL USED FOR THE TEE OR WYE SECTION SHALL BE COMPATIBLE WITH THE EXISTING STORM SEWER OR THE PROPOSED STORM SEWER.

CONSTRUCTION METHODS

- I. THIS WORK SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE PORTIONS OF SECTION 550 OF THE STANDARD SPECIFICATIONS.
- II. CONNECTION TO AN EXISTING STORM SEWER SHALL BE BY EITHER OF THE FOLLOWING METHODS:
- A) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 27" (675) OR SMALLER SEE DETAIL "A" AND "B".
 - B) PROPOSED STORM SEWER CONNECTION TO EXISTING SEWER OF 30" (750) OR LARGER SEE DETAIL "C".

IF THE EXISTING SEWER PIPE IS CRACKED, BROKEN OR OTHERWISE DAMAGED BY THE CONTRACTOR IN MAKING THE CIRCULAR OPENING, THE CONTRACTOR SHALL REPLACE THAT SECTION OF PIPE WITH PIPE EQUAL AND SIMILAR IN ALL RESPECTS TO THE PIPE IN THE EXISTING SEWER, IN A CAREFUL WORKMANLIKE MANNER, WITHOUT EXTRA COMPENSATION.

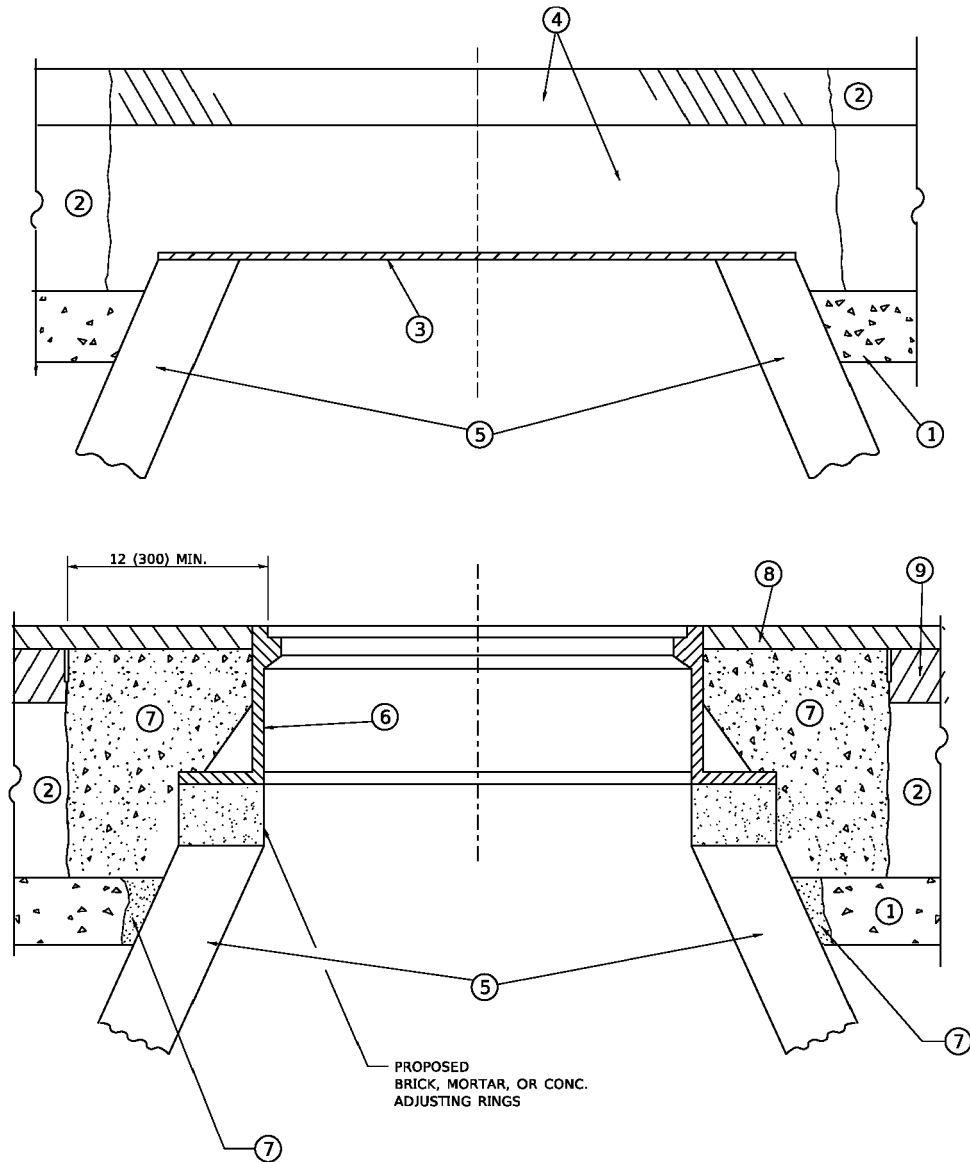
GENERAL

1. CARE MUST BE TAKEN TO PREVENT DEBRIS FROM ENTERING THE SEWER.
ALL DEBRIS WHICH ENTERS THE SEWER MUST BE REMOVED. THE SEWER MUST
BE LEFT CLEAN AND UNOBSTRUCTED UPON COMPLETION OF THE CONTRACT.
2. CARE MUST BE TAKEN TO PREVENT ANY PART OF THE NEW PIPE
CONNECTION FROM PROJECTING INTO THE EXISTING SEWER.

BASIS OF PAYMENT

1. TEE OR WYE CONNECTIONS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR STORM SEWER TEE OR WYE OF THE TYPE AND SIZE SPECIFIED IN THE PLANS, THIS PRICE SHALL INCLUDE ALL EXCAVATION OF THE TRENCH, REMOVAL OF THE EXISTING STORM SEWER, FURNISHING AND INSTALLING THE SPECIFIED TEE OR WYE SECTION, FURNISHING AND INSTALLING THE REQUIRED CONCRETE COLLAR, AND ALL OTHER MATERIAL NECESSARY TO COMPLETE THIS WORK AS SHOWN AND SPECIFIED.
2. REMOVAL AND REINSTALLATION OF EXISTING STORM SEWER ADJACENT TO THE PROPOSED TEE OR WYE SECTION, FOR THE PURPOSE OF FACILITATING THE INSTALLATION OF THE TEE OR WYE SECTION, WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE WORK.
3. TRENCH BACKFILL, EXCAVATION IN ROCK AND REMOVAL AND REPLACEMENT OF UNSUITABLE MATERIAL BELOW PLAN BEDDING GRADE WILL BE PAID FOR SEPARATELY.
4. CONCRETE COLLAR FOR CONNECTING A PROPOSED STORM SEWER TO AN EXISTING STORM SEWER WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE COST OF THE PROPOSED STORM SEWER.

* ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.



DETAILS FOR FRAMES AND LIDS ADJUSTMENT
WITH MILLING

NOTES

- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.
- CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.
- THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.
- THE CONTRACTOR SHALL REMOVE ALL TRAFFIC CONTROL DEVICES BY THE END OF EACH WORK SHIFT.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- BACKFILL WITH CRUSHED STONE AND HMA SURFACE MIX APPROVED BY THE ENGINEER. (MIN. 3 (80) HMA TO REMAIN AFTER MILLING).

STAGE 2 (AFTER PAVEMENT MILLING)

- REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-2* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- | | |
|--|-------------------------------|
| ① SUB-BASE GRANULAR MATERIAL | ⑥ FRAME AND LID (SEE NOTES) |
| ② EXISTING PAVEMENT | ⑦ CLASS PP-2* CONCRETE |
| ③ 36 (900) DIAMETER METAL PLATE | ⑧ PROPOSED HMA SURFACE COURSE |
| ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX | ⑨ PROPOSED HMA BINDER COURSE |
| ⑤ EXISTING STRUCTURE | |

LOCATION OF STRUCTURES

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT

- REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."
- THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.
- NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.
- WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

USER NAME = Lawrence.DeManche	DESIGNED - R. SHAH	REVISED - R. BORO 03-09-11
	DRAWN -	REVISED - R. BORO 12-06-11
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - K. SMITH 11-18-22
PLOT DATE = 9/15/2023	DATE - 10-25-94	REVISED - K. SMITH 09-15-23

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2525	20-00189-00-PV	KANE	184	138
BD600-03 (BD-08)		CONTRACT NO. 61K68		
ILLINOIS		FED. AID PROJECT		



1. ALL STEEL ANGLES AND PLATES TO BE GALVANIZED AFTER FABRICATION.

1. TO BE PAID FOR AS "MANHOLES ,TYPE A, 6 FT. (1.8 m)-DIAMETER, TYPE 1 FRAME, CLOSED LID, RESTRICTOR PLATE" EACH

2. ALL RESTRICTOR PLATES, ANGLES AND HARDWARE TO BE INCLUDED IN THE COST OF THE MANHOLE.

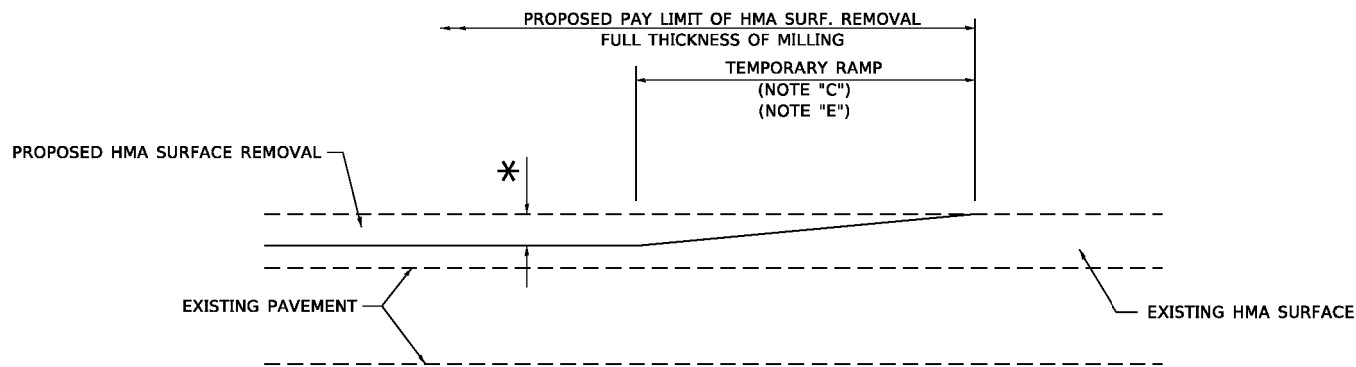


VALUES OF "C" FOR CIRCULAR
AND SQUARE ORIFICES



ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

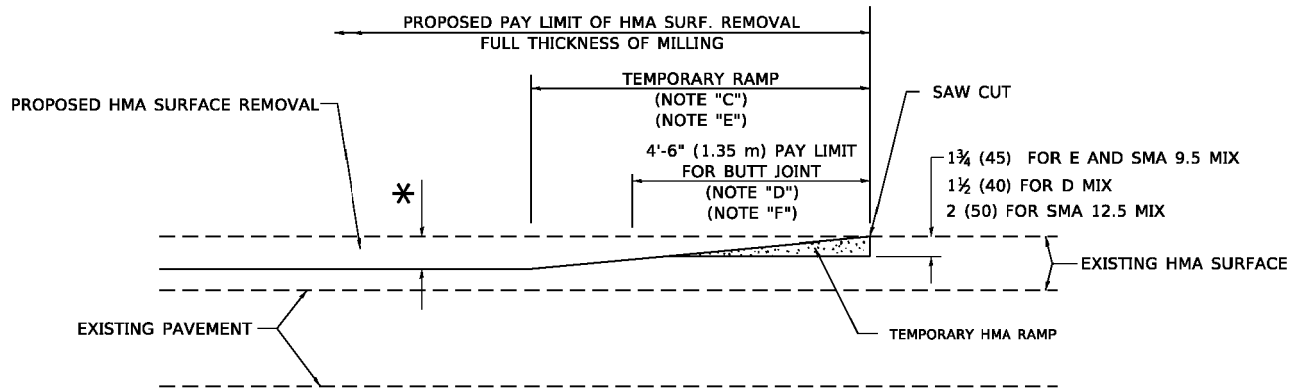
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2525	20-00189-00-PV	KANE	184	139
BD600-04		(BD-12)		
		CONTRACT NO. 61K68		
		ILLINOIS FED. AID PROJECT		



MILLED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

OPTION 1

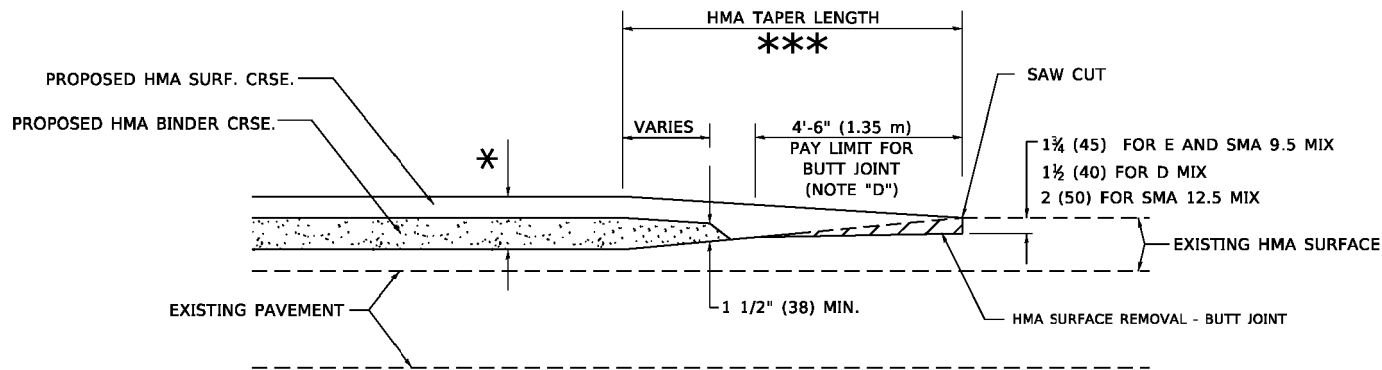


HMA CONSTRUCTED TEMPORARY RAMP

(FOR BUTT JOINT AND HMA TAPER SEE DETAIL BELOW)

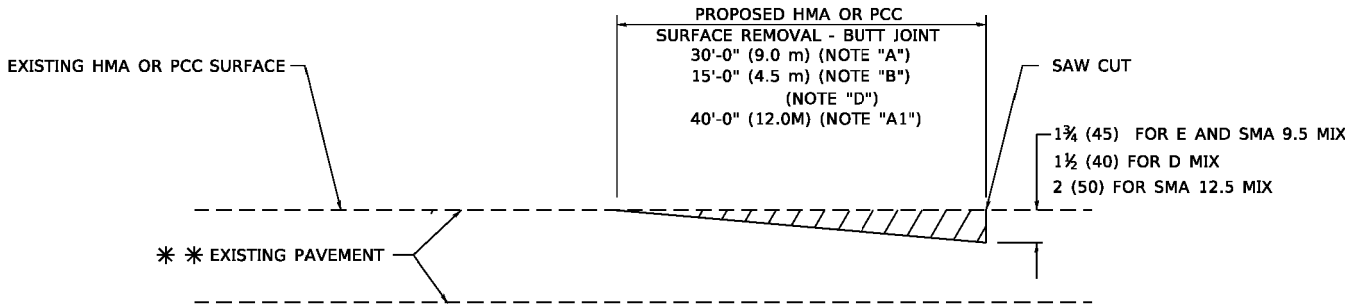
OPTION 2

TYPICAL TEMPORARY RAMP

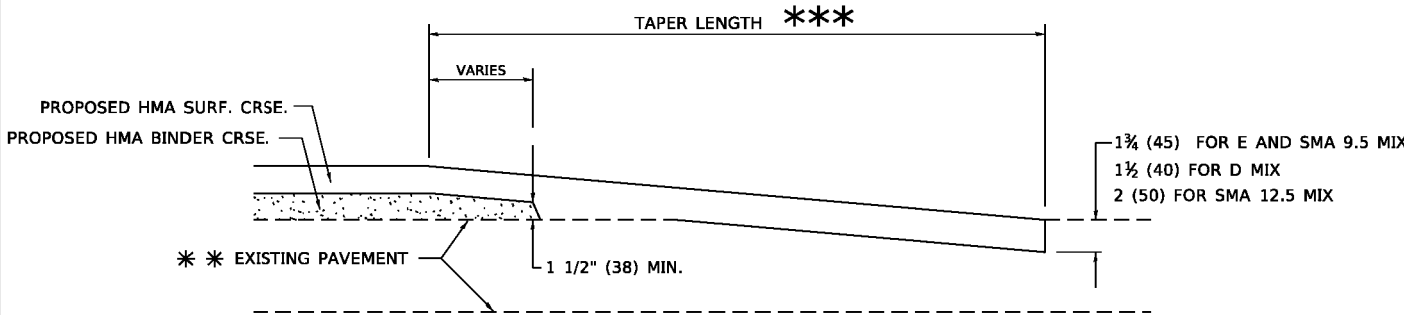


BUTT JOINT AND HMA TAPER

TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

GENERAL NOTES

- MAINLINE ARTERIAL ROADWAYS AND MAJOR SIDE ROADS.
- INTERSTATES.
- MINOR SIDE ROADS.
- THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- TAPER THE TEMP. RAMP AT A RATE OF 3' - 4" (1.02m) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
 - 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
 - 10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT

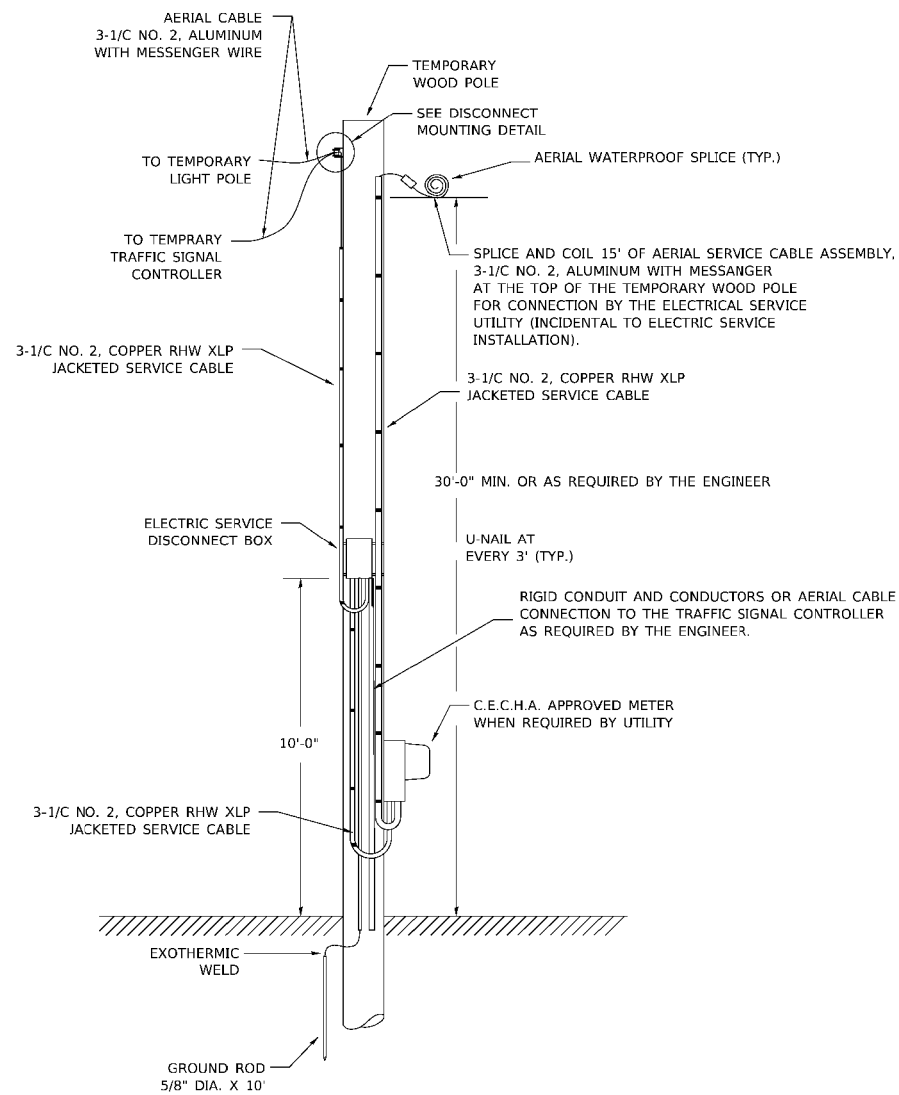
- THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL- BUTT JOINT".
- THE TEMPORARY RAMP AND SAW CUT SHALL BE INCLUDED IN THE UNIT COST FOR HMA OR PCC SURFACE REMOVAL-BUTT JOINT.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



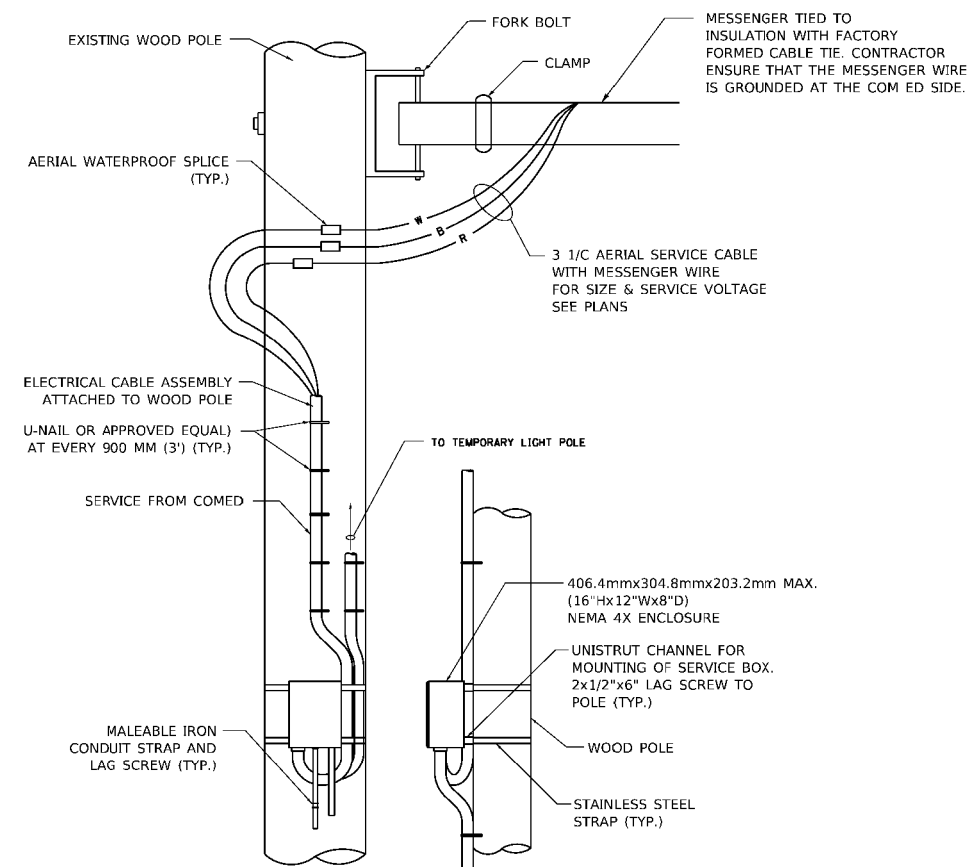
1. 4 LUMINAIRES PER CIRCUIT, MAXIMUM.
2. TWO #10 (XLP-TYPE USE) CABLES TO BE USED FOR LIGHTING CIRCUITS.
3. ROUTE LIGHTING CIRCUITS IN TRAFFIC SIGNAL CONDUIT SYSTEM.
4. ALL SPLICES AND CONNECTIONS FOR ROADWAY LIGHTING SHALL BE AT POLE BASE ONLY.
5. FOR LIGHTING CIRCUITS, CONNECT TWO CIRCUIT BREAKERS TO AC SERVICE TERMINAL BLOCK.
6. ALL WIRING SHALL BE NEATLY DRESSED, IDENTIFIED BY TAGS, AND SUPPORTED.
(UNDERGROUND SPLICING OF LIGHTING CONDUCTORS IS NOT PERMITTED).
7. THE H.O.A. SWITCH SHALL BE LABELED AS "LIGHTING CONTROL" WITH THE POSITIONS "AUTO", "OFF" AND "TEST" WITH ENGRAVED NAME PLATES.
8. LIGHTING CONNECTED TO UPS BYPASS CIRCUIT.
9. COMBINATION LIGHTING MUST BE INSTALLED PRIOR TO SIGNAL TURN ON.
10. LUMINAIRE VOLTAGE SHALL BE 120V
11. POLE WIRING & FUSE KITS ARE INCLUDED IN THE LUMINAIRE PAY ITEM.
12. THE UNDERGROUND EQUIPMENT GROUND WIRE IS SHOWN IN THE TRAFFIC SIGNAL PLANS AND IS INCLUDED IN THE SIGNAL PLANS. IT IS SHARED GROUND BETWEEN SIGNALS AND LIGHTING.





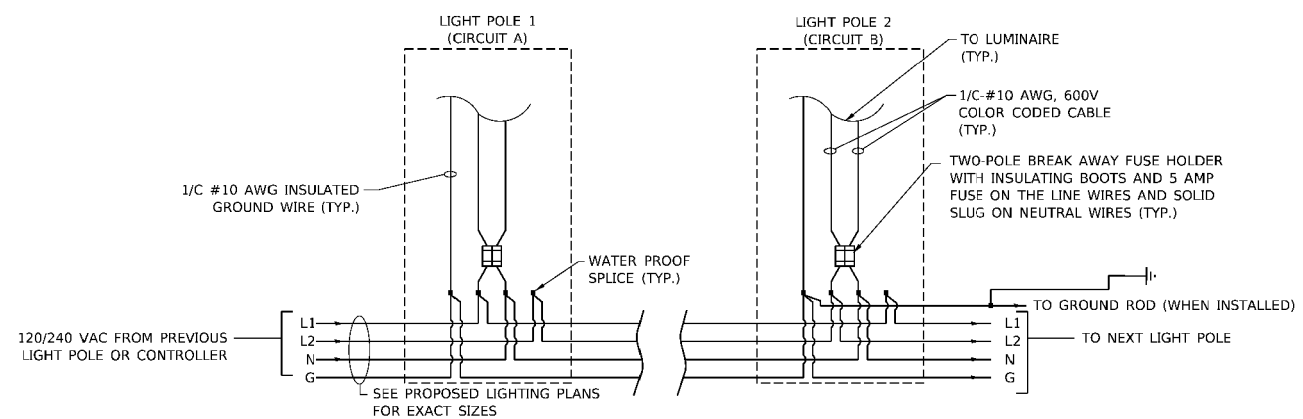
TEMPORARY SERVICE INSTALLATION DETAIL

NOT TO SCALE



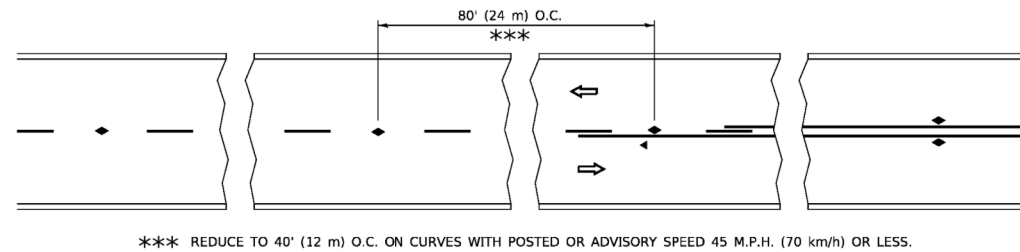
DISCONNET MOUNTING DETAIL

NOT TO SCALE

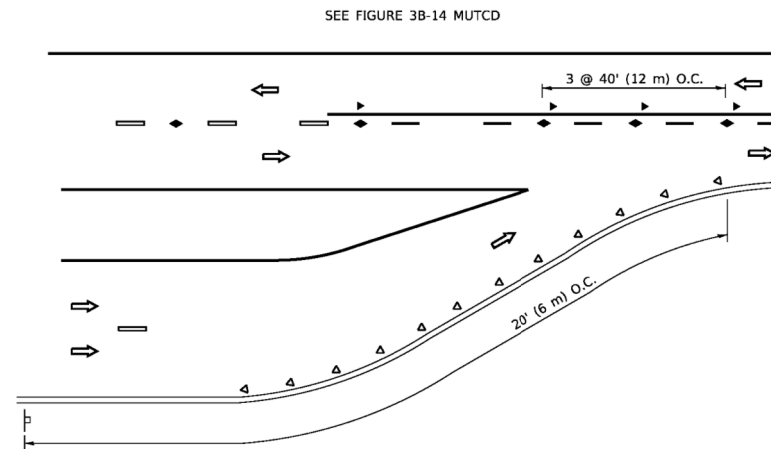


LIGHT POLE WIRING DETAIL

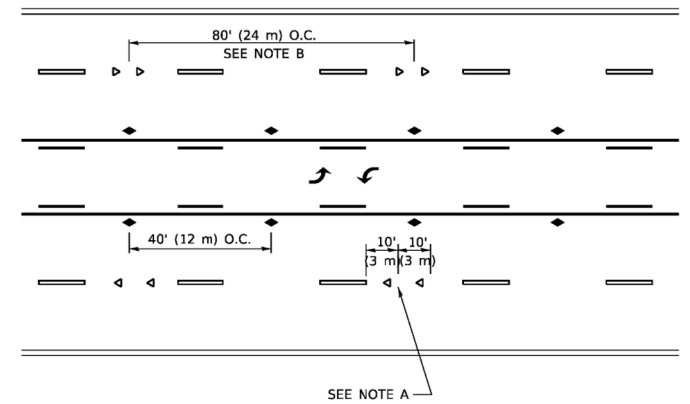
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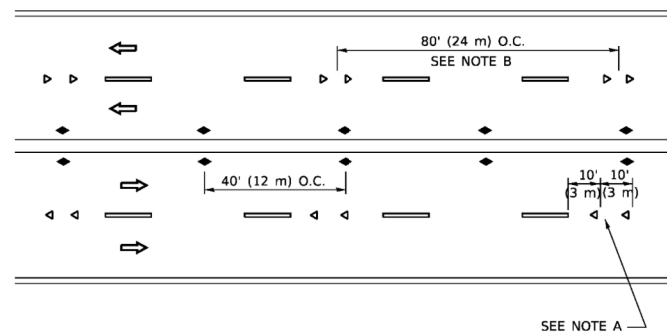
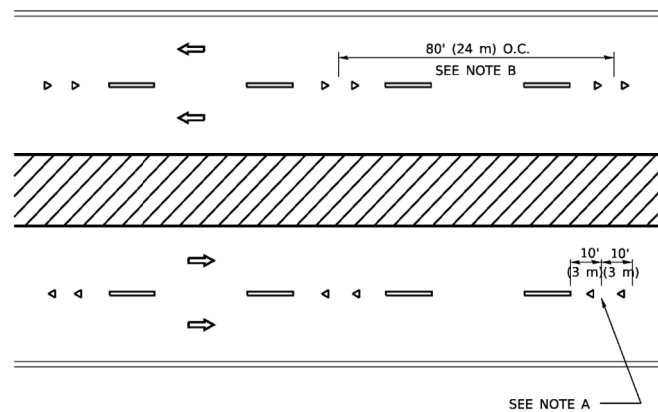
TWO-LANE/TWO-WAY



LANE REDUCTION TRANSITION



TWO-WAY LEFT TURN






**MULTI-LANE/UNDIVIDED**

MULTI-LANE/DIVIDED

GENERAL NOTES

1. MARKERS USED WITH DASHED LINES SHALL BE CENTERED IN THE GAP BETWEEN SEGMENTS.
2. MARKERS USED ADJACENT TO SOLID LINES SHALL BE OFFSET 2 TO 3 (50 TO 75) TOWARD TRAFFIC AS SHOWN.
3. MARKERS THROUGH TANGENTS LESS THAN 500' (150 m) IN LENGTH BETWEEN CURVES SHALL BE INSTALLED AT THE LESSER OF THE TWO CURVE SPACINGS.
4. MARKERS ARE TO BE USED ADJACENT TO BOTH SOLID WHITE LINES IN DUAL LEFT TURN LANES

SYMBOLS

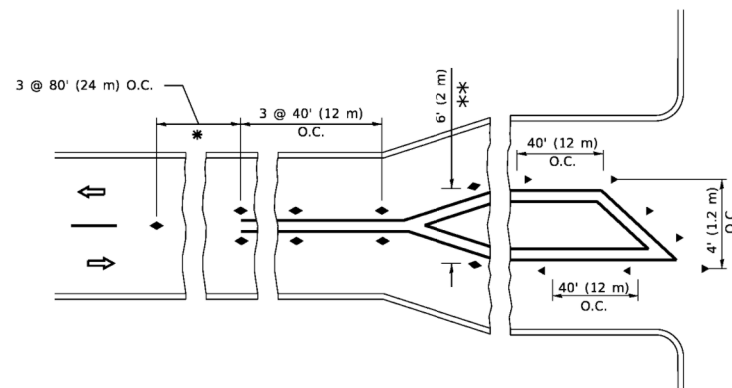
-  YELLOW STRIPE
-  WHITE STRIPE
-  ONE-WAY AMBER MARKER
-  ONE-WAY CRYSTAL MARKER (W/O)
-  TWO-WAY AMBER MARKER

LANE MARKER NOTES

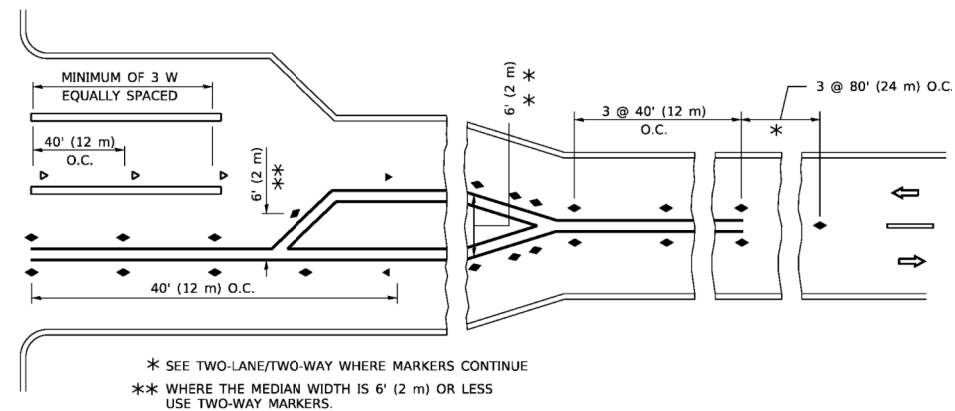
- A. USE DOUBLE LANE LINE MARKERS SPACED AS SHOWN.
- B. REDUCE TO 40' (12 m) O.C. ON CURVES WHERE ADVISORY SPEEDS ARE 10 M.P.H (20 km/h) LOWER THAN POSTED SPEEDS.

DESIGN NOTES

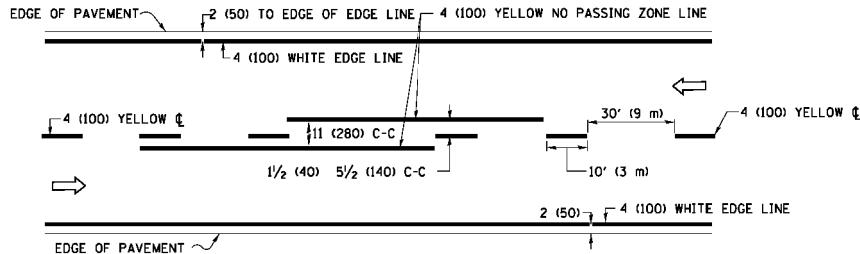
1. DOUBLE LANE LINE MARKERS SHALL BE USED UNLESS SPECIFIED OTHERWISE.
2. EXCEPT AS SHOWN ON THE LANE REDUCTION TRANSITION AND FREEWAY EXIT RAMP DETAIL, MARKERS ARE NOT TO BE SPECIFIED ON RIGHT EDGE LINES.
3. THE EXACT MARKER LIMITS, SPACING, AND COLOR SHALL BE INCLUDED IN THE PLANS WHEN STANDARD SPECIFICATIONS ARE NOT BEING USED.
4. MARKERS SHOULD NOT BE USED ALONGSIDE CURBS EXCEPT FOR EXTREMELY SHORT SECTIONS OF CURBS WHERE NOT MORE THAN TWO MARKERS WOULD BE INVOLVED.



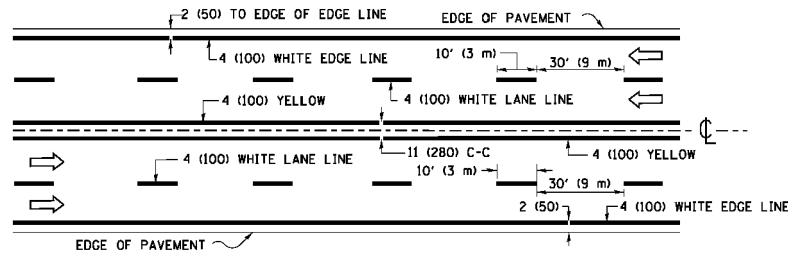
TURN LANES



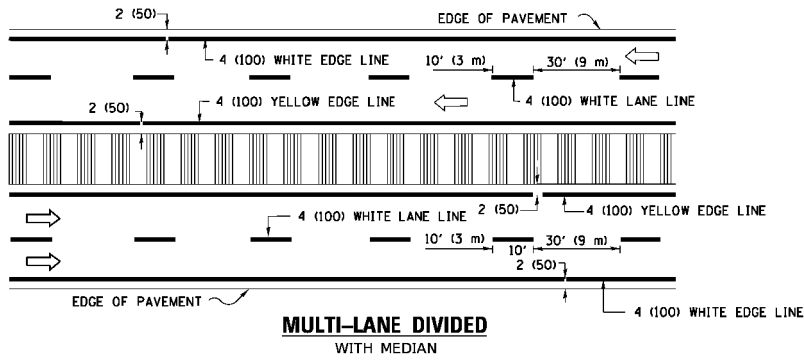
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2-LANE ROADWAY

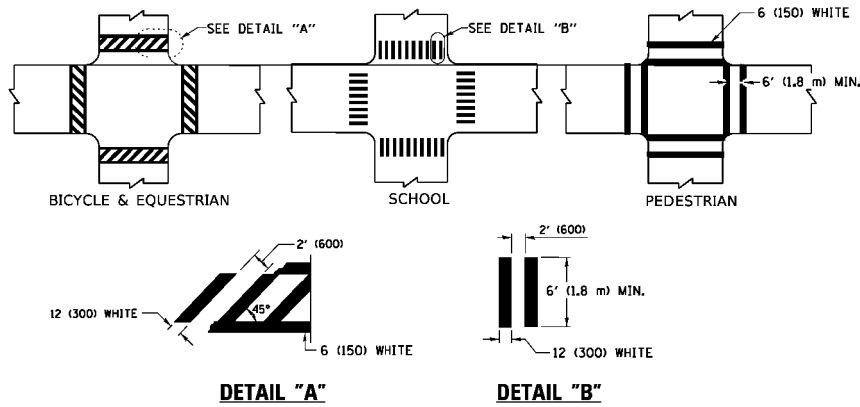


MULTI-LANE UNDIVIDED



MULTI-LANE DIVIDED
WITH MEDIAN

TYPICAL LANE AND EDGE LINE MARKING

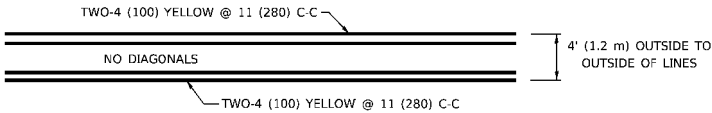


DETAIL "A"

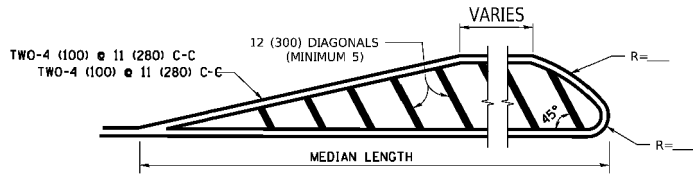
DETAIL "B"

TYPICAL CROSSWALK MARKING

* MARKINGS SHALL BE INSTALLED PARALLEL TO THE CENTERLINE OF THE ROAD WHICH IT CROSSES

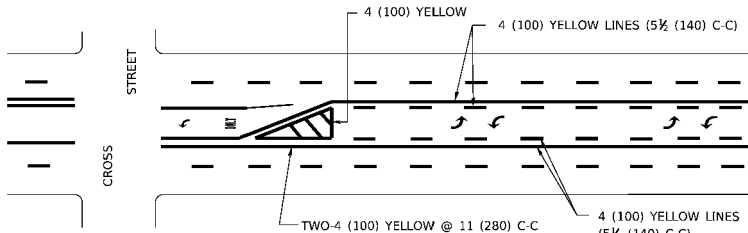


4' (1.2 m) WIDE MEDIANS ONLY



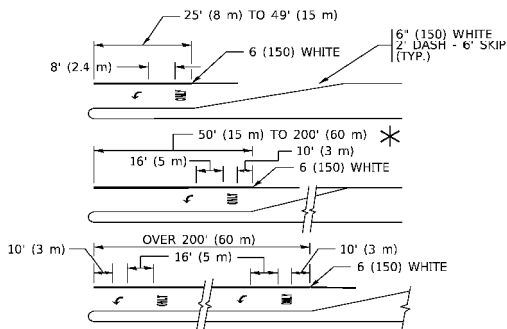
DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE



MEDIAN WITH TWO-WAY LEFT TURN LANE

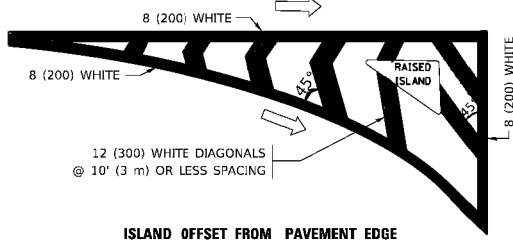
TYPICAL PAINTED MEDIAN MARKING



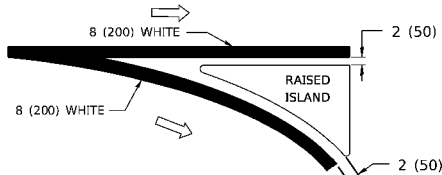
FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING

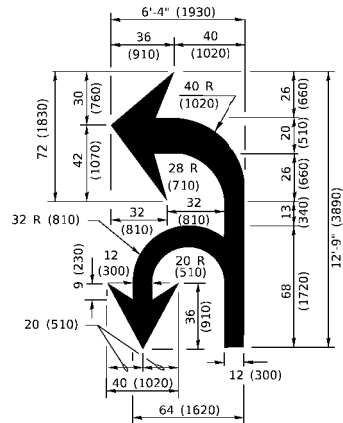


ISLAND OFFSET FROM PAVEMENT EDGE

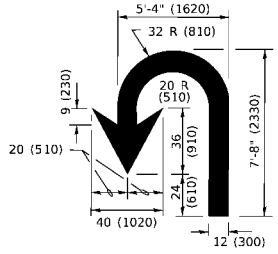


ISLAND AT PAVEMENT EDGE

TYPICAL ISLAND MARKING



COMBINATION
LEFT AND U-TURN



U-TURN

LANE REDUCTION TRANSITION

* LANE REDUCTION ARROWS REQUIRED AT SPEEDS OF 45 MPH OR GREATER OR WHEN SPECIFIED IN PLANS.

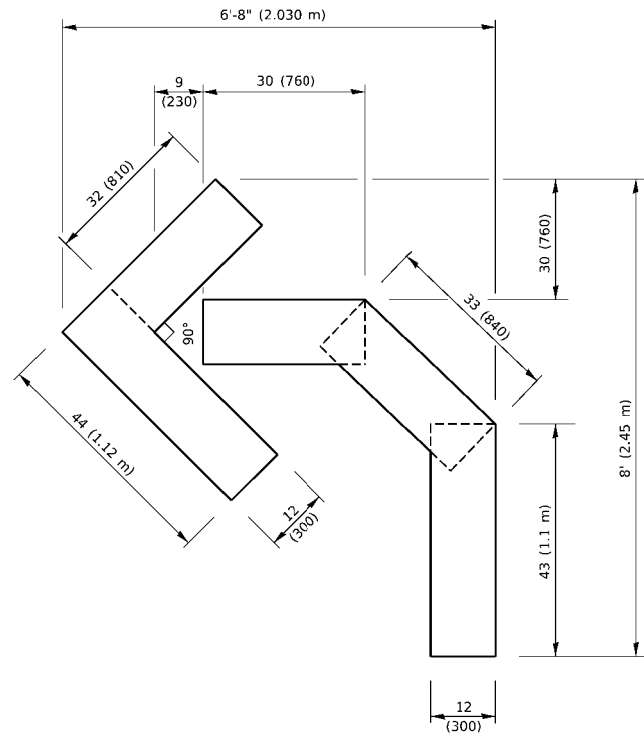
TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5 1/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MEDIANS IN YELLOW
TURN LANE MARKINGS	6 (150) LINE: FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5 1/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE.
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW: TWO WAY TRAFFIC WHITE: ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: *R*=3.6 SQ. FT. (0.33 m ²) EACH *X*=54.0 SQ. FT. (5.0 m ²)
SHOULDER DIAGONALS (REQUIRED FOR SHOULDERS > 8')	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h) TO 45MPH (70 km/h)) 150' (45 m) C-C (OVER 45MPH (70 km/h))
U TURN ARROW	SEE DETAIL	SOLID	WHITE	16.3 SF
2 ARROW COMBINATION LEFT AND U TURN	SEE DETAIL	SOLID	WHITE	30.4 SF

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

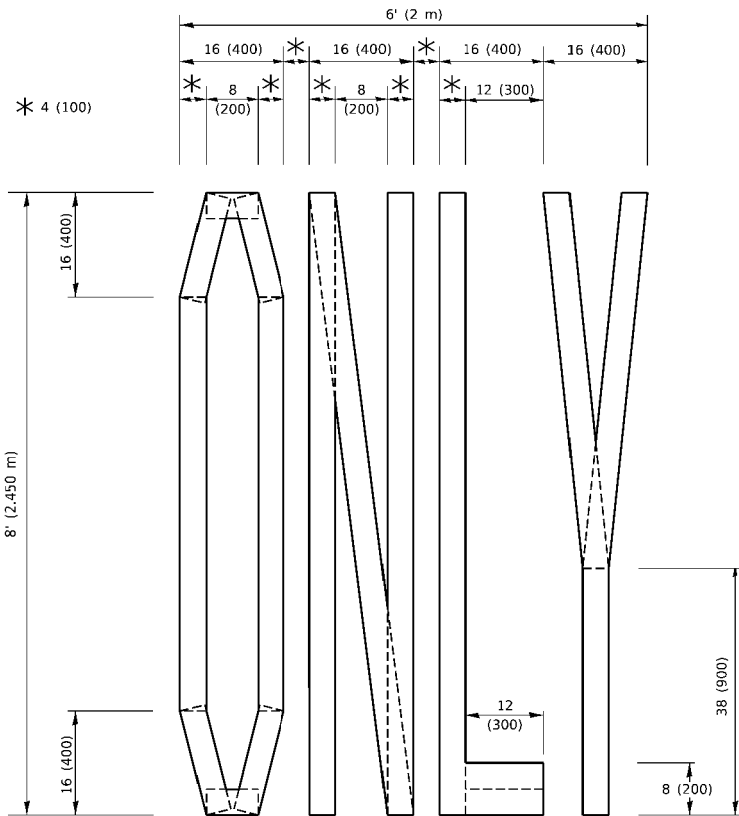
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2525	20-00189-00-PV	KANE	184	146
TC-13		CONTRACT NO. 61K68		
ILLINOIS		FED. AID PROJECT		



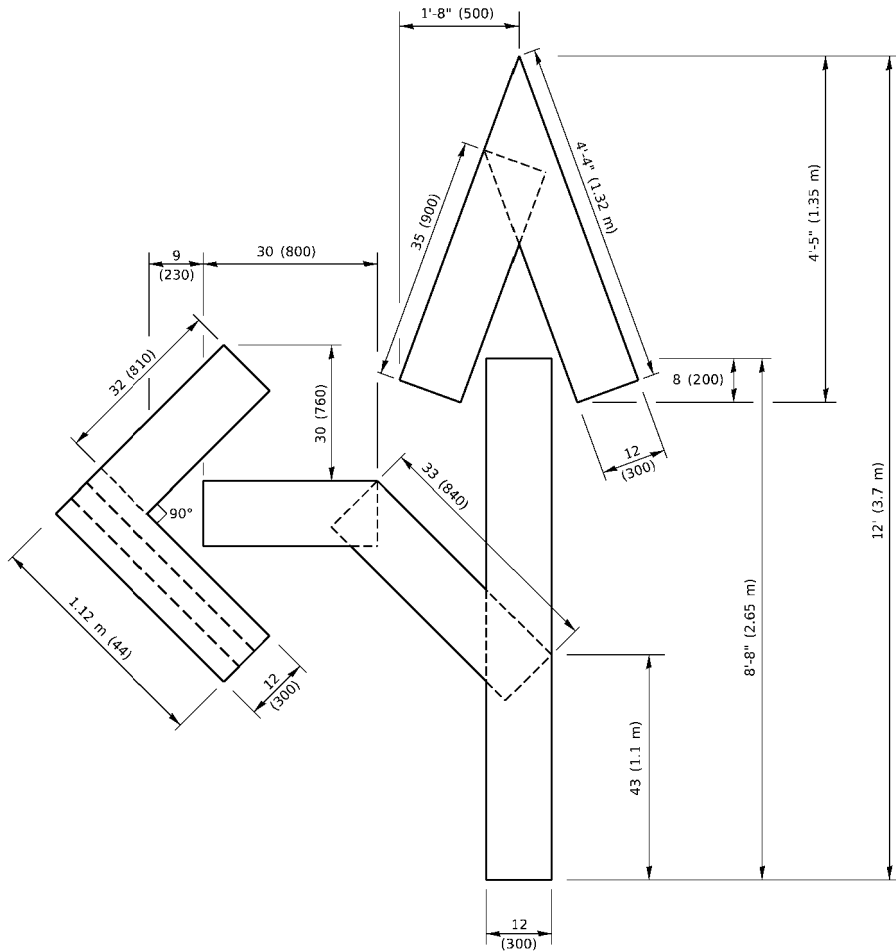
QUANTITY

4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.41 sq. m)



QUANTITY

4 (100) LINE = 64.1 ft. (19.5 m)
21.4 sq. ft. (1.99 sq. m)

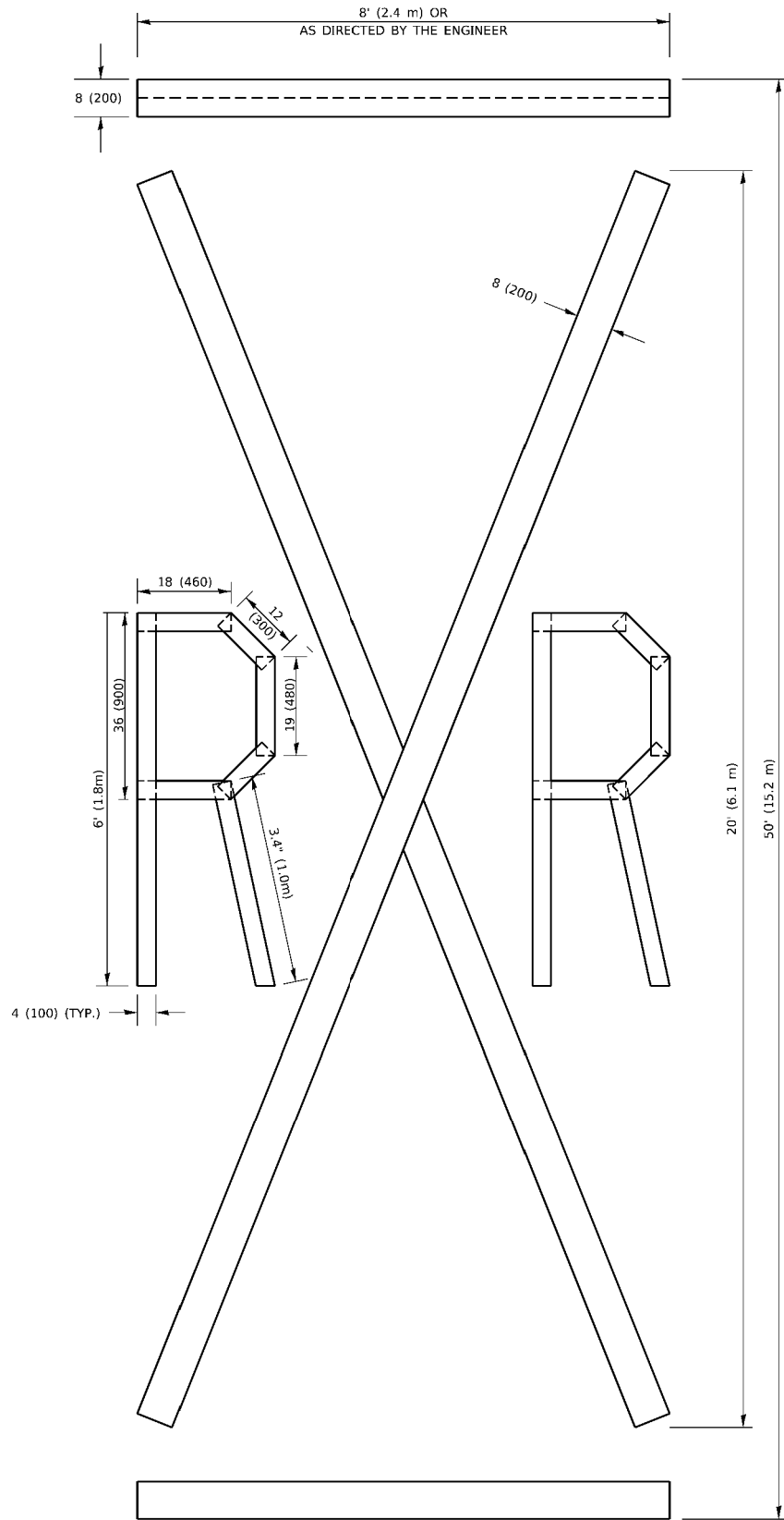


QUANTITY

4 (100) LINE = 82.5 ft. (25.1 m)
27.5 sq. ft. (2.53 sq. m)

NOTE:

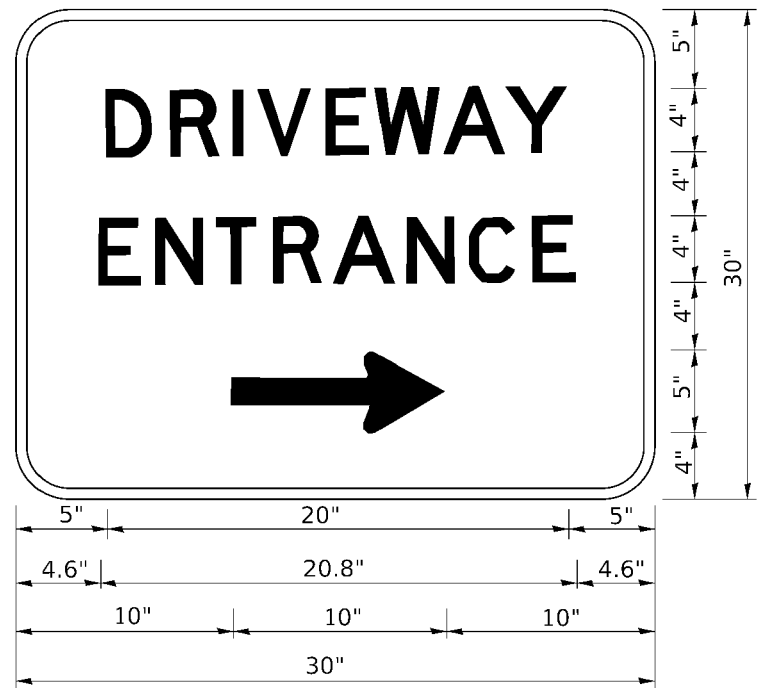
ALL QUANTITIES OF PLACEMENT ARE REPRESENTED IN LINEAR FEET OF 4" LINES TO MATCH THE 4" TEMPORARY TAPE PAY ITEM AND REPRESENTS THE TOTAL QUANTITY OF 4" TAPE REQUIRED.



QUANTITY

4 (100) LINE = 225.9 ft. (68.9 m)
75.3 sq. ft. (6.99 sq. m)



All dimensions are in inches (millimeters) unless otherwise shown.



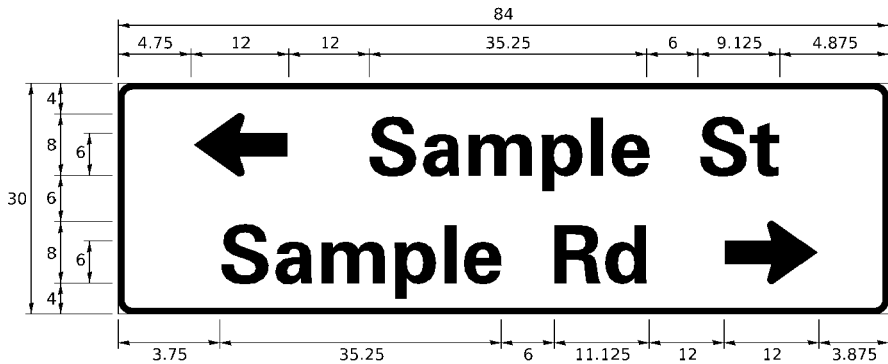
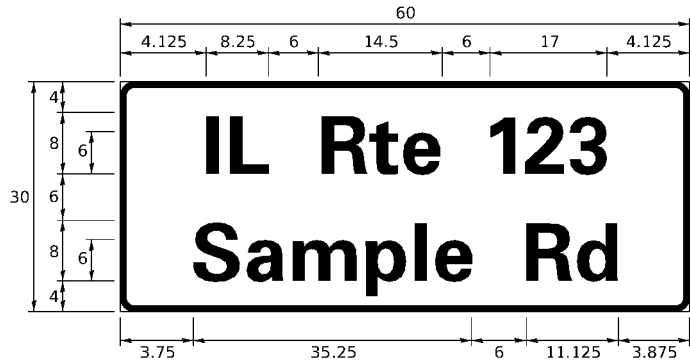
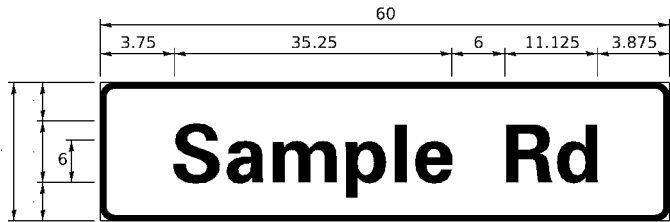
3.0" RADIUS, 0.5" BORDER, WHITE ON GREEN; REFLECTORIZED
"DRIVEWAY" D; "ENTRANCE" D; STANDARD ARROW CUSTOM 12.0" x 5.0"

- NOTES:
1. HALF OF THE SIGNS WILL REQUIRE A LEFT HAND FACING ARROW.
 2. TWO SIGNS SHALL BE USED AT EACH COMMERCIAL ENTRANCE
PLACED BACK-TO-BACK: ONE WITH A RIGHT HAND ARROW (SHOWN)
SHALL BE PLACED ON THE NEAR RIGHT SIDE THE DRIVEWAY
AND ONE WITH A LEFT HAND ARROW SHALL BE PLACED ON THE
FAR LEFT SIDE OF THE DRIVEWAY.
 3. SIGNS TO BE PAID FOR AS ITEM "TEMPORARY INFORMATION SIGNING".

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MODEL Default
FILE NAME = p\w\planroom.dct Illinois.gov\PW\DOT\Documents\DOT Office\District 1\Projects\4545472\341CAD\Bna\CA\Sheet126.dgn

  <div>Engineering Enterprises, Inc. CONSULTING ENGINEERS 52 Wheeler Road Sugar Grove, Illinois 60554 630.466.6700 / www.eeiweb.com</div>	USER NAME = leysa	DESIGNED -	REVISED - C. JUCIUS 02-15-07	<div>STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION</div>	<div>DRIVEWAY ENTRANCE SIGNING</div>					F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 50.0000' / in.	DRAWN -	REVISED -							2525	20-00189-00-PV	KANE	184	149
	PLOT DATE = 8/6/2021	CHECKED -	REVISED -							TC-26		CONTRACT NO. 61K68		
	DATE -	REVISED -	REVISED -							ILLINOIS FED. AID PROJECT				
						SCALE: NONE	SHEET 1	OF 1	SHEETS	STA.	TO STA.			

SIGN PANEL – TYPE 1 OR TYPE 2



DESIGN SERIES	AREA (SQ FT)	SIGN PANEL TYPE	SHEETING TYPE	QTY. REQUIRED
D OR C	-	1 OR 2	ZZ	-

COMMON STREET NAME ABBREVIATIONS AND WIDTHS

NAME	ABBREVIATION	WIDTH (INCH)	
		SERIES "C"	SERIES "D"
AVENUE	Ave	15.000	18.250
BOULEVARD	Blvd	17.125	20.000
CIRCLE	Cir	11.125	13.000
COURT	Ct	8.250	9.625
DRIVE	Dr	8.625	10.125
HIGHWAY	Hwy	18.375	22.000
ILLINOIS	IL	7.000	8.250
LANE	Ln	9.125	10.750
PARKWAY	Pkwy	23.375	27.375
PLACE	Pl	7.125	7.750
ROAD	Rd	9.625	11.125
ROUTE	Rte	12.625	14.500
STREET	St	8.000	9.125
TERRACE	Ter	12.625	14.625
TRAIL	Tr	7.750	9.125
UNITED STATES	US	10.375	12.250

GENERAL NOTES

- WHERE MAST ARM MOUNTED STREET NAME SIGNS ARE SPECIFIED, THE MAST ARM ASSEMBLY AND POLES SHALL BE DESIGNED TO SUPPORT THE LOADINGS CALLED FOR ON STANDARDS 877001, 877002, 877006, 877011 AND 877012, AS APPLICABLE, PLUS TWO (2) SIGN PANELS 2'-6" x 8'-0" MOUNTED AS SHOWN. THE DESIGN SHALL BE IN ACCORDANCE WITH THE REQUIREMENTS OF THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES, AND TRAFFIC SIGNALS" AS PUBLISHED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS FOR 80 M.P.H. WIND VELOCITY.
- ALL SIGNS SHALL CONSIST OF A WHITE LEGEND AND BORDER (TYPE ZZ SHEETING) ON A GREEN BACKGROUND (TYPE ZZ SHEETING)
- THE SIGN LENGTH SHALL BE IN 6-INCH INCREMENTS, BUT THE OVERALL LENGTH SHALL NOT EXCEED 8'-0". ALL BORDERS IF POSSIBLE, BUT MAY BE REDUCED TO 5" WHEN SPACING IS CRITICAL. A MINIMUM OF 2-1/2" SHALL BE INCLUDED BETWEEN THE WORD AND THE RIGHT AND LEFT EDGES OF THE SIGN.
- A PREFERRED METHOD FOR THE SIGN DESIGN IS TO USE SERIES "D" LETTER ON A ONE-LINE SIGN 18" IN HEIGHT AND A MAXIMUM OF 8'-0" IN WIDTH. IF SERIES "D" DOES NOT FIT ON A 8'-0" SIGN, THEN SERIES "C" SHOULD BE TRIED. IF SERIES "C" DOES NOT FIT ON A 8'-0" SIGN, A 30" HIGH TWO-LINE SIGN CAN BE USED. THE CROSSROAD DESIGNATION AS TO STREET, AVENUE, ETC. SHOULD BE SPELLED OUT ON THE SECOND LINE, IF THE ABBREVIATION CANNOT FIT ON THE FIRST LINE.
- LED ILLUMINATED STREET NAME SIGNS CAN BE USED IN PLACE OF REGULAR SIGN PANELS BUT ANY SPECIAL WORDING AND SYMBOLOGY MUST BE APPROVED BY THE DEPARTMENT. GENERAL DESIGN REQUIREMENT AS LISTED ABOVE (COLOR, FONT, SIZE, ETC.) MUST BE FOLLOWED.
- SIGNFIX ALUMINUM CHANNEL FRAMING SYSTEM SHALL BE USED FOR ALL SIGNS ATTACHED TO SIGNAL POLES AND POSTS.

LOCAL SUPPLIERS:

- J.O. HERBERT COMPANY, INC
MIDLOTHIAN, VA

- WESTERN REMAC, INC.
WOODRIDGE, IL

PARTS LISTING:

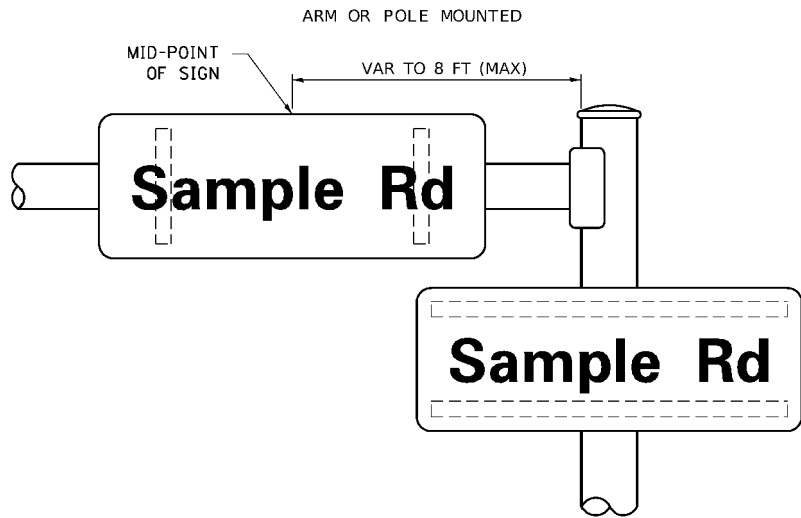
SIGN CHANNEL
SIGN SCREWS

BRACKETS

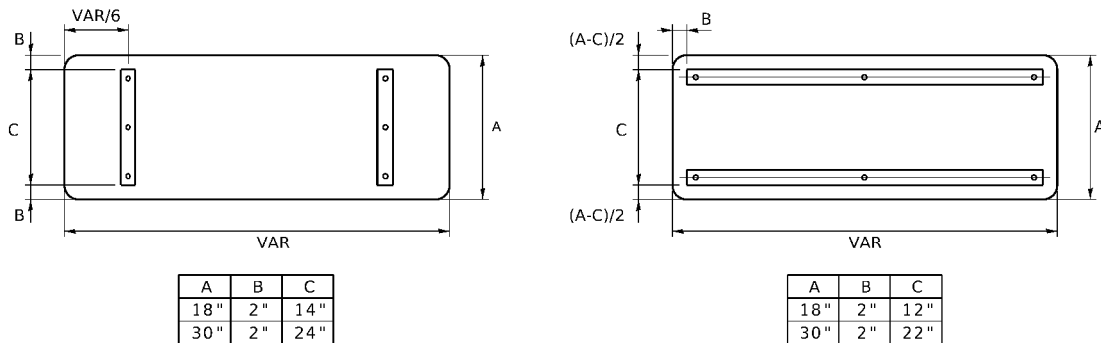
PART #HPN053 (MED. CHANNEL)
1/4" x 14 x 1" H.W.H. #3
SELF TAPPING WITH NEOPRENE WASHER
PART #HPN034 (UNIVERSAL)
CHANNEL CLAMPS WITH STAINLESS STEEL STRAPPING

OTHER BRANDS OF MOUNTING HARDWARE ARE ACCEPTABLE, BASED UPON THE DEPARTMENT'S APPROVAL AND COMPATIBILITY WITH THE CHANNEL/BACKET OF THE ABOVE PRODUCT.

MOUNTING LOCATION



SUPPORTING CHANNELS



STANDARD ALPHABETS SPACING CHART

(8") UPPER CASE AND (6") LOWER CASE

FHWA SERIES "C"				FHWA SERIES "D"			
CHARACTER	LEFT SPACING (INCH)	WIDTH (INCH)	RIGHT SPACING (INCH)	CHARACTER	LEFT SPACING (INCH)	WIDTH (INCH)	RIGHT SPACING (INCH)
A	0.240	5.122	0.240	A	0.240	6.804	0.240
B	0.880	4.482	0.480	B	0.960	5.446	0.400
C	0.720	4.482	0.720	C	0.800	5.446	0.800
D	0.880	4.482	0.720	D	0.960	5.446	0.800
E	0.880	4.082	0.480	E	0.960	4.962	0.400
F	0.880	4.082	0.240	F	0.960	4.962	0.240
G	0.720	4.482	0.720	G	0.800	5.446	0.800
H	0.880	4.482	0.880	H	0.960	5.446	0.960
I	0.880	1.120	0.880	I	0.960	1.280	0.960
J	0.240	4.082	0.880	J	0.240	5.122	0.960
K	0.880	4.482	0.480	K	0.960	5.604	0.400
L	0.880	4.082	0.240	L	0.960	4.962	0.240
M	0.880	5.284	0.880	M	0.960	6.244	0.960
N	0.880	4.482	0.880	N	0.960	5.446	0.960
O	0.720	4.722	0.720	O	0.800	5.684	0.800
P	0.880	4.482	0.720	P	0.960	5.446	0.240
Q	0.720	4.722	0.720	Q	0.800	5.684	0.800
R	0.880	4.482	0.480	R	0.960	5.446	0.400
S	0.480	4.482	0.480	S	0.400	5.446	0.400
T	0.240	4.082	0.240	T	0.240	4.962	0.240
U	0.880	4.482	0.880	U	0.960	5.446	0.960
V	0.240	4.962	0.240	V	0.240	6.084	0.240
W	0.240	6.084	0.240	W	0.240	7.124	0.240
X	0.240	4.722	0.240	X	0.400	5.446	0.400
Y	0.240	5.122	0.240	Y	0.240	6.884	0.240
Z	0.480	4.482	0.480	Z	0.400	5.446	0.400
a	0.320	3.842	0.640	a	0.400	4.562	0.720
b	0.720	4.082	0.480	b	0.800	4.802	0.480
c	0.480	4.002	0.240	c	0.480	4.722	0.240
d	0.480	4.082	0.720	d	0.480	4.802	0.800
e	0.480	4.082	0.320	e	0.480	4.722	0.320
f	0.320	2.480	0.160	f	0.320	2.882	0.160
g	0.480	4.082	0.720	g	0.480	4.802	0.800
h	0.720	4.082	0.640	h	0.800	4.722	0.720
i	0.720	1.120	0.720	i	0.800	1.280	0.800
j	0.000	2.320	0.720	j	0.000	2.642	0.800
k	0.720	4.322	0.160	k	0.800	5.122	0.160
l	0.720	1.120	0.720	l	0.800	1.280	0.800
m	0.720	6.724	0.640	m	0.800	7.926	0.720
n	0.720	4.082	0.640	n	0.800	4.722	0.720
o	0.480	4.082	0.480	o	0.480	4.882	0.480
p	0.720	4.082	0.480	p	0.800	4.802	0.480
q	0.480	4.082	0.720	q	0.480	4.802	0.800
r	0.720	2.642	0.160	r	0.800	3.042	0.160
s	0.320	3.362	0.240	s	0.320	3.762	0.240
t	0.080	2.882	0.080	t	0.080	3.202	0.080
u	0.640	4.082	0.720	u	0.720	4.722	0.800
v	0.160	4.722	0.160	v	0.160	5.684	0.160
w	0.160	7.524	0.160	w	0.160	9.046	0.160
x	0.000	5.202	0.000	x	0.000	6.244	0.000
y	0.160	4.962	0.160	y	0.160	6.004	0.160
z	0.240	3.362	0.240	z	0.240	4.002	0.240
1	0.720	1.680	0.880	1	0.800	2.000	0.960
2	0.480	4.482	0.480	2	0.800	5.446	0.800
3	0.480	4.482	0.480	3	1.440	5.446	0.800
4	0.240	4.962	0.720	4	0.160	6.004	0.960
5	0.480	4.482	0.480	5	0.800	5.446	0.800
6	0.720	4.482	0.720	6	0.800	5.446	0.800
7	0.240	4.482	0.720	7	0.560	5.446	0.560
8	0.480	4.482	0.480	8	0.800	5.446	0.800
9	0.480	4.482	0.480	9	0.800	5.446	0.800
0	0.720	4.722	0.720	0	0.800	5.684	0.800
-	0.240	2.802	0.240	-	0.240	2.802	0.240

EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION																												PREEMPTOR NUMBER 3	PREEMPTOR NUMBER 4	CLEAR TO NORMAL SEQUENCE									
CHANGE FROM NORMAL SEQUENCE OF OPERATION INTERVAL NUMBER	1	5			5			8			8			11	11				14	18				18			22				22			26			26		
EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1A	1B	1C	1D	1E	1F	1G	1H	1J	1K	1L	1M	1N	1P	1Q	1R	1S	1T	1U	1V	1W	1X	1Y	1Z	1AA	1BB	1CC	1DD	1EE		1FF	2	3						
CHANGE TO EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	2 OR 3	1C	2	1E	1F	3	1H	2	1K	1L	3	2	1P	1Q	3	2 OR 3	1T	1U	2	1W	3	1Y	1Z	2	1BB	3	1DD	1EE	2		3								
MAIN STREET END MAST ARM AND FAR LEFT SIGNALS	E/B ←Y	R	R	R	R	R	←G	G	←Y	←G	Y	R	G	G	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	R	◇						
MAIN STREET FAR RIGHT SIGNAL	E/B	R	R	R	R	R	G	G	G	Y	R	G	G	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	R	◇						
MAIN STREET END MAST ARM AND FAR LEFT SIGNALS	W/B ←Y	G	←G	G	←Y	←G	Y	R	R	R	R	R	G	G	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	R	◇							
MAIN STREET FAR RIGHT SIGNAL	W/B	R	G	G	G	Y	R	R	R	R	R	R	G	G	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	R	◇							
CROSS STREET END MAST ARM AND FAR LEFT SIGNALS	S/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	←Y	R	R	R	R	R	←G	Y	R	←G	G	←Y	G	Y	R	G	R	G	◇				
CROSS STREET FAR RIGHT SIGNAL	S/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	Y	R	G	G	G	G	Y	R	G	R	G	◇					
CROSS STREET END MAST ARM AND FAR LEFT SIGNALS	N/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	←Y	←G	Y	R	←G	G	←Y	R	R	R	R	R	G	Y	R	G	R	G	◇				
CROSS STREET FAR RIGHT SIGNAL	N/B	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	Y	R	G	G	R	R	R	R	R	R	G	Y	R	G	R	G	◇					
PEDESTRIAN SIGNALS CROSSING CROSS STREET ON NORTHSIDE OF MAIN STREET		H	FH	H	FH	H	H	H	H	H	H	FH	FH	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	◇				
PEDESTRIAN SIGNALS CROSSING CROSS STREET ON SOUTHSIDE OF MAIN STREET		H	H	H	H	H	FH	H	FH	H	H	FH	FH	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	◇				
PEDESTRIAN SIGNALS CROSSING MAIN STREET ON EASTSIDE OF CROSS STREET		H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	FH	H	H	FH	H	H	H	H	H	H	H	FH	H	H	FH	H	H	◇					
PEDESTRIAN SIGNALS CROSSING MAIN STREET ON WESTSIDE OF CROSS STREET		H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	FH	H	H	FH	H	FH	H	H	FH	H	H	H	◇					

EMERGENCY VEHICLE SEQUENCE SHALL PROVIDE THE PROPER CLEARANCE INTERVAL TO RESUME THE NORMAL SEQUENCE OF OPERATION OR PROPER CLEARANCE INTERVAL TO DISPLAY A DIFFERENT EMERGENCY INTERVAL AFTER EMERGENCY VEHICLE 2 OR 3 IS TERMINATED.

USER NAME = footernj	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 50.0000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 3/4/2019	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION – MAIN STREET AND CROSS STREET				
SCALE:	SHEET 1	OF 2	SHEETS	STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2525	20-00189-00-PV	KANE	184	151
TS-08		CONTRACT NO. 61K68		
ILLINOIS		FED. AID PROJECT		

MOVEMENT																																	F L A S H
PHASE	1 + 5				1 + 6			2 + 5			2 + 6				3 + 7				3 + 8					4 + 7					4 + 8				
INTERVAL	1	2	3	4	5	6	7	8	9	10	11	12	13A	13B	14	15	16	17	18	19	20A	20B	21	22	23	24A	24B	25	26	27	28A	28B	
CHANGE TO		1+6	2+5	2+6			2+6			2+6			3+7 3+8 4+7 4+8		1+5 1+6 2+5 2+6 4+8	3+8	4+7			1+5 1+6 2+5 2+6	4+8			1+5 1+6 2+5 2+6	4+8			1+5 1+6 2+5 2+6					
MAIN STREET E/B END MAST ARM AND FAR LEFT SIGNALS					R	R	R				G	G	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	
MAIN STREET E/B FAR RIGHT SIGNAL	R	R	R	R	R	R	R	G	G	G	G	G	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	
MAIN STREET W/B END MAST ARM AND FAR LEFT SIGNALS								R	R	R	G	G	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	
MAIN STREET W/B FAR RIGHT SIGNAL	R	R	R	R	G	G	G	R	R	R	G	G	Y	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	
CROSS STREET S/B END MAST ARM AND FAR LEFT SIGNALS	R	R	R	R	R	R	R	R	R	R	R	R	R	R					R	R	R	R	R			Y	R		G	G	Y	R	
CROSS STREET S/B FAR RIGHT SIGNAL	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	G	Y	R	G	G	G	Y	R	
CROSS STREET N/B END MAST ARM AND FAR LEFT SIGNALS	R	R	R	R	R	R	R	R	R	R	R	R	R	R							Y	R		R	R	R	R	R	G	G	Y	R	
CROSS STREET N/B FAR RIGHT SIGNAL	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	R	G	G	Y	R	G	R	R	R	R	R	G	G	Y	R	
PEDESTRIAN SIGNALS CROSSING CROSS STREET ON NORTHSIDE OF MAIN STREET	H	H	H	H	*P	**FH	H	H	H	H	*P	**FH	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	
PEDESTRIAN SIGNALS CROSSING CROSS STREET ON SOUTHSIDE OF MAIN STREET	H	H	H	H	H	H	H	*P	**FH	H	*P	**FH	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	
PEDESTRIAN SIGNALS CROSSING MAIN STREET ON EASTSIDE OF CROSS STREET	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	*P	**FH	H	H	H	H	H	H	H	H	*P	**FH	H	H	
PEDESTRIAN SIGNALS CROSSING MAIN STREET ON WESTSIDE OF CROSS STREET	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	H	*P	**FH	H	H	H	*P	**FH	H	H	

H = ILLUMINATED SOLID HAND = DON'T WALK

CHANGE FROM NORMAL SEQUENCE OF OPERATION INTERVAL NUMBER	1	5	8	11	14	18	22	26																		
CHANGE FROM EMERGENCY VEHICLE PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER															2				3							
RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	1A	1B	1C	1D	1E	1F	1G	1H	1J	1K	1L	1M	1N	1P	1Q	1R	1S	2		3	4	5	CLEAR TO NORMAL SEQUENCE			
CHANGE TO RAILROAD PREEMPTION SEQUENCE OF OPERATION INTERVAL NUMBER	2	1C	2	1E	2	1G	2	2	1K	2	2	1N	2	1Q	2	1S	2	3		4	5					
MAIN STREET E/B END MAST ARM AND FAR LEFT SIGNALS	R ← Y	Y	R	R	R	Y	R	R	R	R	R	R	R	Y	R	R	R	R		R	R	R		Δ		
MAIN STREET E/B FAR RIGHT SIGNAL	R	Y	R	R	R	Y	R	R	R	R	R	R	R	Y	R	R	R	R		R	R	R		Δ		
MAIN STREET W/B END MAST ARM AND FAR LEFT SIGNALS	R ← Y	R	R	Y	R	Y	R	R	R	R	R	R	R	Y	R	R	R	R		R	R	G ← G	Δ			
MAIN STREET W/B FAR RIGHT SIGNAL	R	R	R	Y	R	Y	R	R	R	R	R	R	R	Y	R	R	R	R		R	R	G	Δ			
CROSS STREET S/B END MAST ARM AND FAR LEFT SIGNALS	R	R	R	R	R	R	R	R ← G	R	R	R ← G	G	G	R	R	G	G	G ← G		Y	R	R	Δ			
CROSS STREET S/B FAR RIGHT SIGNAL	R	R	R	R	R	R	R	R	R	R	G	G	G	R	R	G	G	G		Y	R	R	Δ			
CROSS STREET N/B END MAST ARM AND FAR LEFT SIGNALS	R	R	R	R	R	R	R	R ← Y	Y	R	R	Y	R	R	R	Y	R	R		R	R	R	Δ			
CROSS STREET N/B FAR RIGHT SIGNAL	R	R	R	R	R	R	R	R	Y	R	R	Y	R	R	R	Y	R	R		R	R	R	Δ			
PEDESTRIAN SIGNALS CROSSING CROSS STREET ON NORTHSIDE OF MAIN STREET	H	FH	H	H	H	FH	H	H	H	H	H	H	H	H	H	H	H	H		H	H	H	Δ			
PEDESTRIAN SIGNALS CROSSING CROSS STREET ON SOUTHSIDE OF MAIN STREET	H	H	H	FH	H	FH	H	H	H	H	H	H	H	H	H	H	H	H		H	H	H	Δ			
PEDESTRIAN SIGNALS CROSSING MAIN STREET ON EASTSIDE OF CROSS STREET	H	H	H	H	H	H	H	H	FH	H	H	FH	H	H	H	H	H	H		H	H	H	Δ			
PEDESTRIAN SIGNALS CROSSING MAIN STREET ON WESTSIDE OF CROSS STREET	H	H	H	H	H	H	H	H	H	H	FH	FH	H	H	H	H	H	H		H	H	H	Δ			
INTERNALLY ILLUMINATED NRT SIGNS	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT	NRT		NRT	NRT	NRT	Δ			
INTERNALLY ILLUMINATED NLT SIGNS	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT	NLT		NLT	NLT	NLT	Δ			

HOLD

FINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

ORIGINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

MODEL XS_Sht_01

FILE NAME: H:\5054949\EG_EPlan\2021\EG210230.dgn Plot Date: 11/5/2024

50

YEARS



Engineering Enterprises, Inc.

CONSULTING ENGINEERS

52 Wheeler Road

Sugar Grove, Illinois 60554

630.466.6700 / www.eeiweb.com

USER NAME	= JMarvlg
PLOT SCALE	= 1:20
PLOT DATE	= 11/5/2024

DESIGNED	-
DRAWN	-
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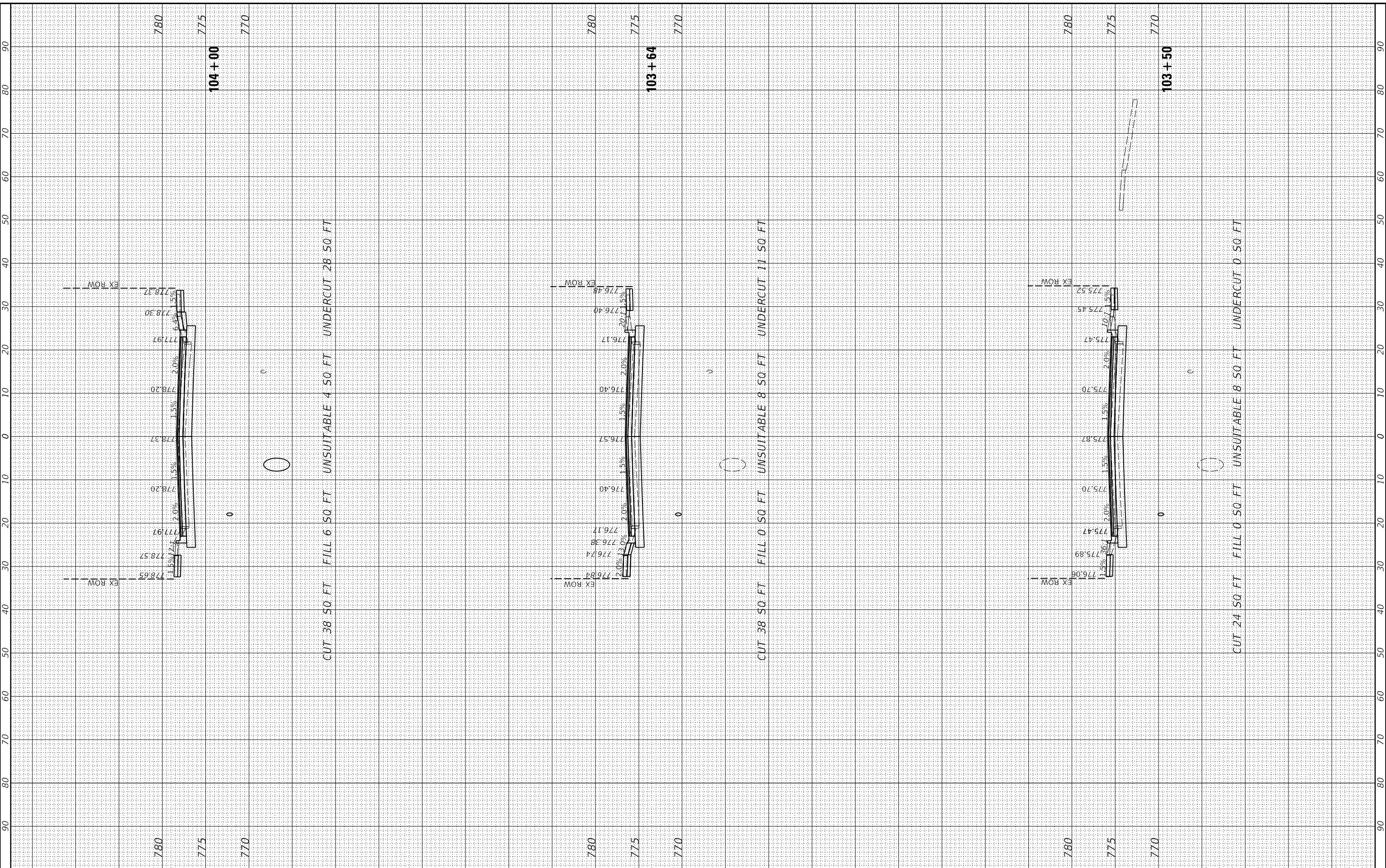
REVISED	-
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REVISED	-
REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DUNDEE AVENUE
CROSS SECTIONS

SCALE: 1"=10'H, 5'V SHEET 1 OF 32 SHEETS STA. 103+50 TO STA. 104+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2525	20-00189-00-PV	KANE	184	153
CONTRACT NO. 61K68				
ILLINOIS FED. AID PROJECT				



ORIGINAL SURVEY	SURVEYED _____	BY _____	DATE _____
	PLOTTED _____		
	TEMPLATE _____		
	AREAS _____		
	AREAS CHECKED _____		
NO. _____			

MODEL: XS_Sht_02
FILE NAME: H:\SDSK



521

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USER NAME	= JMarvig
PLOT SCALE	= 1:20
PLOT DATE	= 11/5/2024

DESIGNED	-
DRAWN	-
CHECKED	-
DATE	-

REVISED	-
REVISED	-
REVISED	-
REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DUNDEE AVENUE CROSS SECTIONS

SCALE: 1"=10'H, 5'V	SHEET 2 OF 32 SHEETS	STA. 104+25 TO STA. 105+00
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2525	20-00189-00-PV	KANE	184	154
		CONTRACT NO. 61K68		
ILLINOIS		FED. AID PROJECT		

FINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

ORIGINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

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USER NAME = JMarvlg
PLOT SCALE = 1:20
PLOT DATE = 11/5/2024

DESIGNED -
DRAWN -
CHECKED -
DATE -

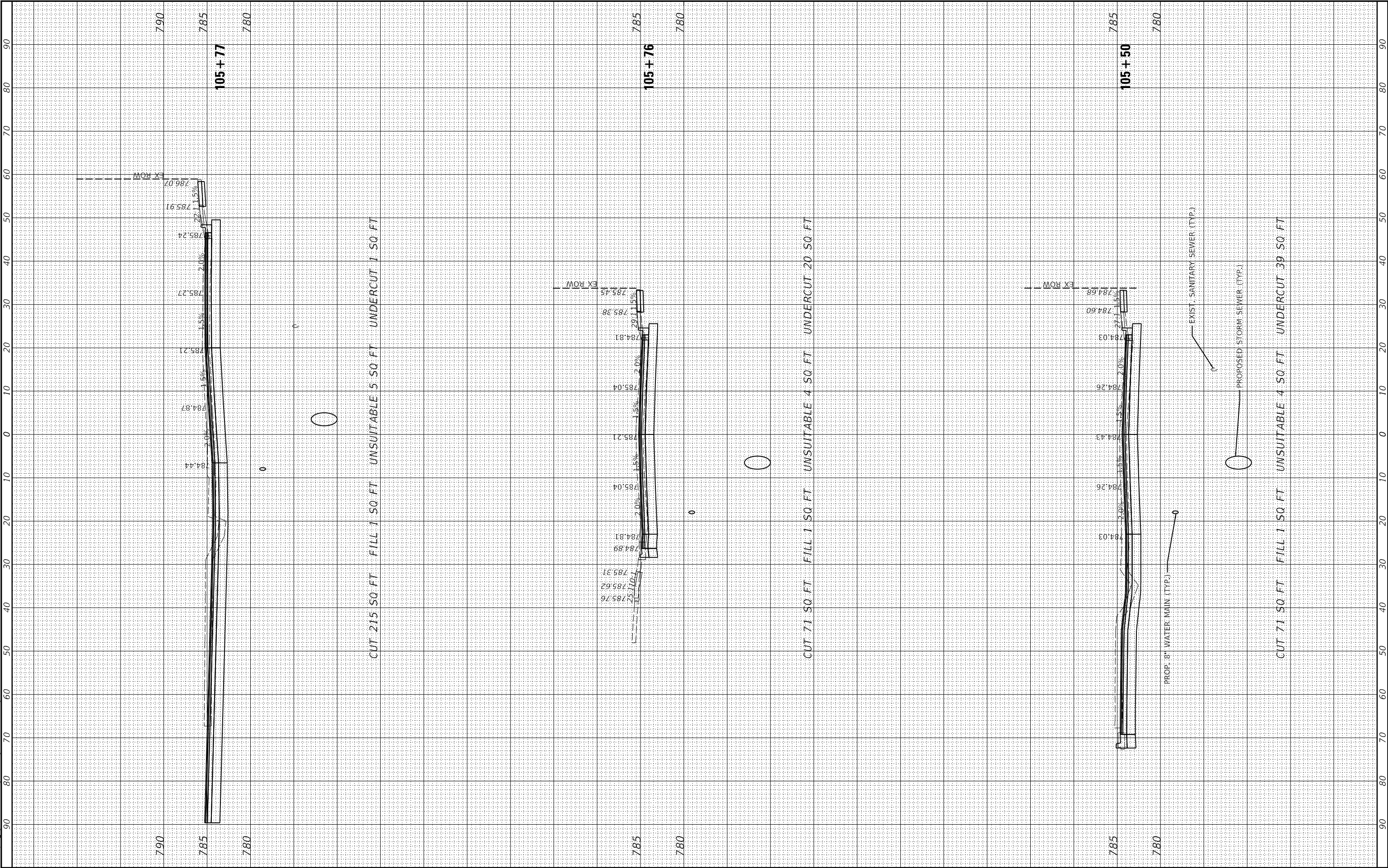
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REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DUNDEE AVENUE
CROSS SECTIONS

SCALE: 1"=10'H, 5'V SHEET 3 OF 32 SHEETS STA. 105+50 TO STA. 105+77

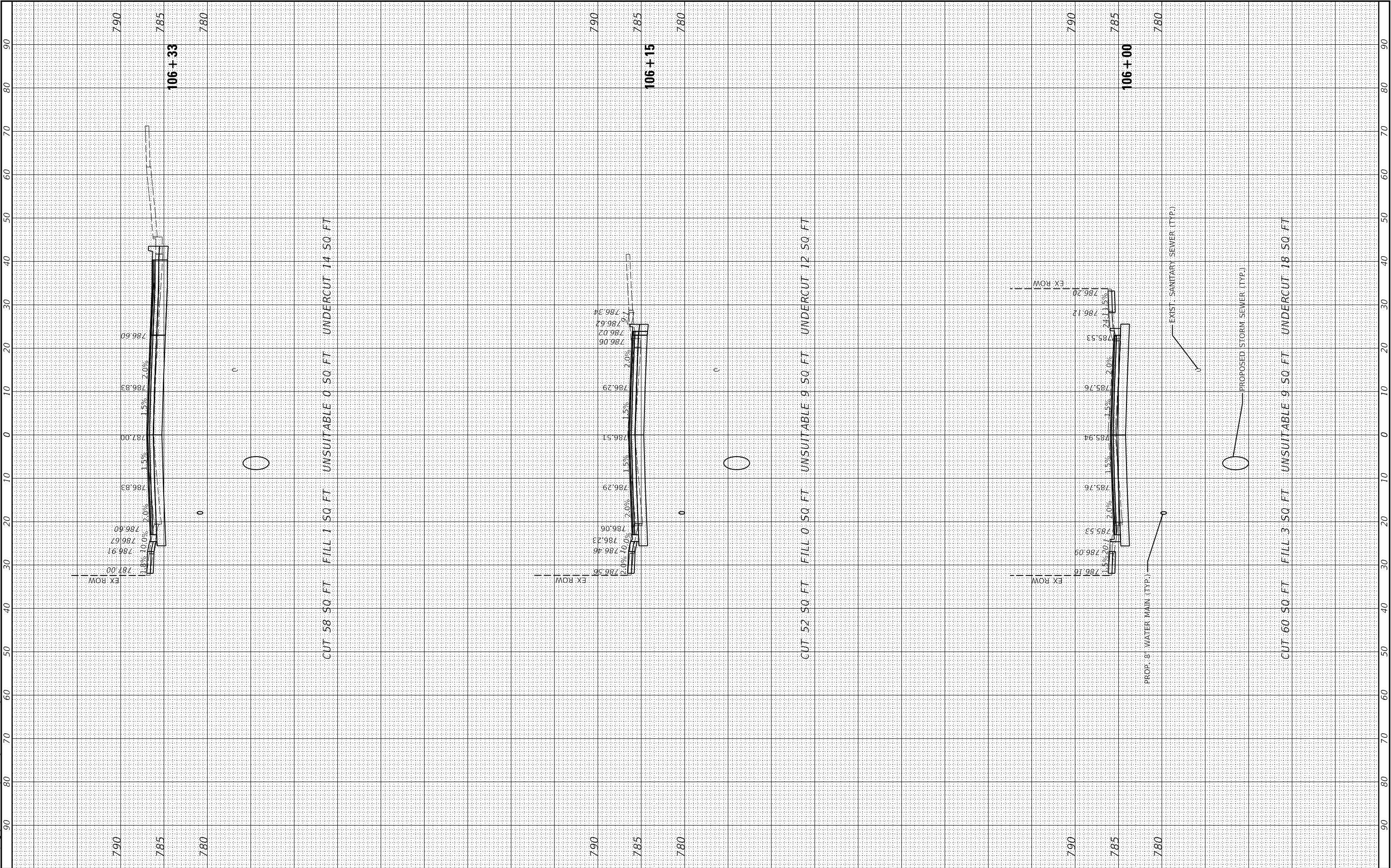
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2525	20-00189-00-PV	KANE	184	155
CONTRACT NO. 61K68				
ILLINOIS FED. AID PROJECT				



FINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

ORIGINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

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USER NAME = JMarvlg	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 1:20	CHECKED -	REVISED -
PLOT DATE = 11/5/2024	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DUNDEE AVENUE
CROSS SECTIONS

SCALE: 1"=10'H, 5'V

SHEET 4 OF 32 SHEETS

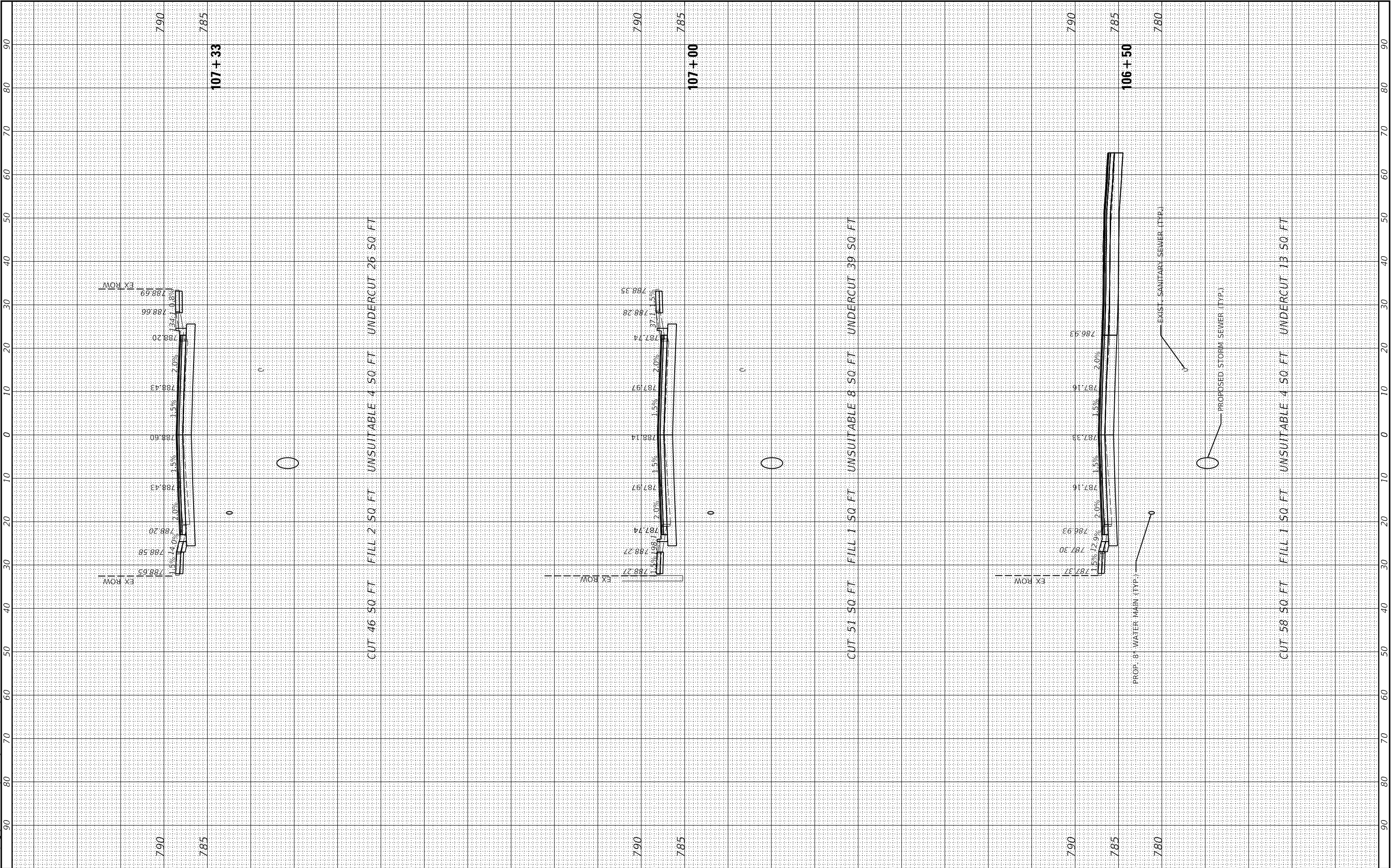
STA. 106+00 TO STA. 106+33

F.A.U. RTE. 2525	SECTION 20-00189-00-PV	COUNTY KANE	TOTAL SHEETS 184	SHEET NO. 156
ILLINOIS FED. AID PROJECT			CONTRACT NO. 61K68	

FINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

ORIGINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

MODEL XS_SHL_05
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USER NAME = jmarvlg	DESIGNED -	REVISED -
PLOT SCALE = 1:20	DRAWN -	REVISED -
PLOT DATE = 11/5/2024	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DUNDEE AVENUE
CROSS SECTIONS

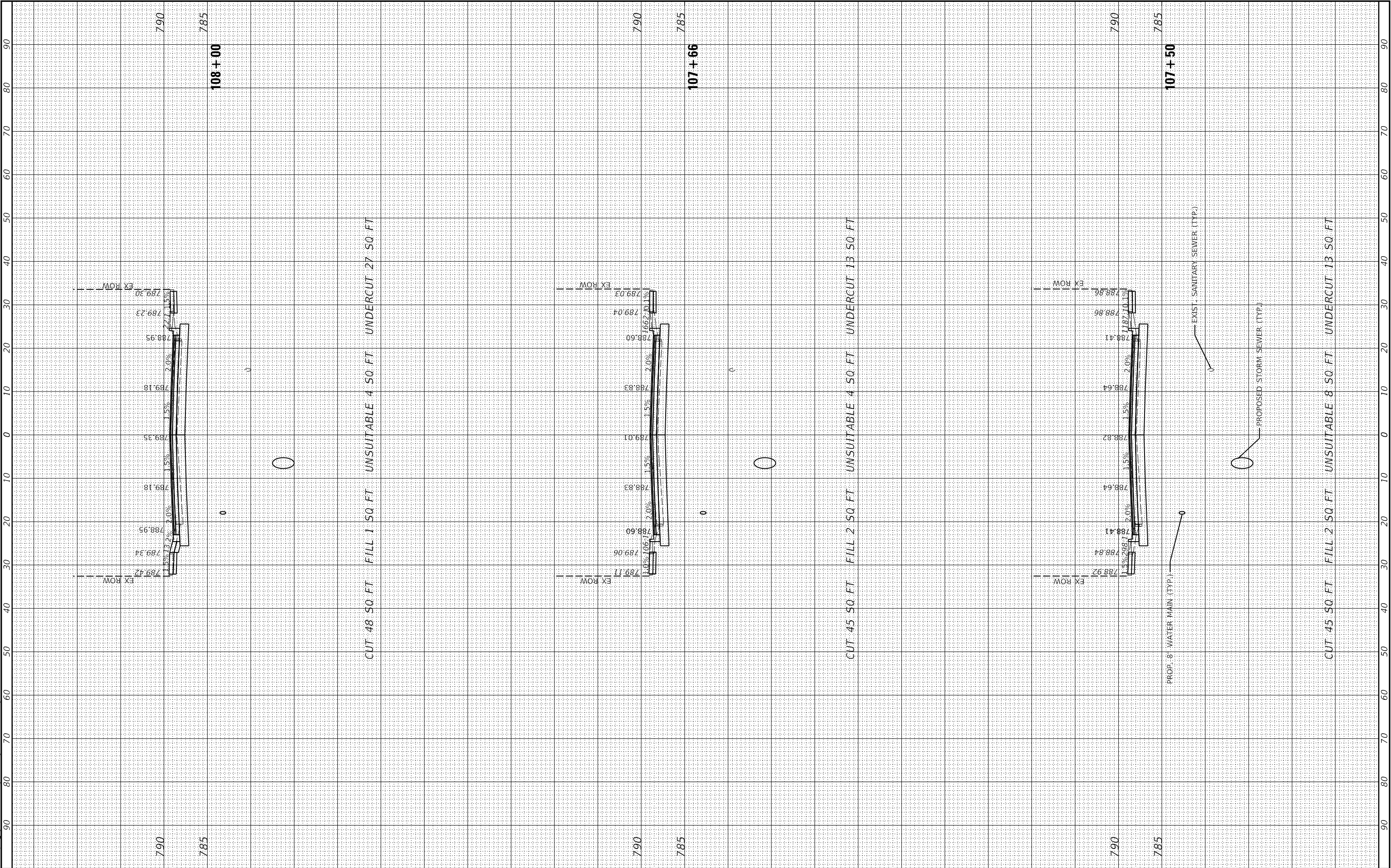
SCALE: 1"=10'H, 5'V SHEET 5 OF 32 SHEETS STA. 106+50 TO STA. 107+33

F.A.U. RTE. 2525	SECTION 20-00189-00-PV	COUNTY KANE	TOTAL SHEETS 184	SHEET NO. 157
ILLINOIS FED. AID PROJECT			CONTRACT NO. 61K68	

FINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS		
	CHECKED		

ORIGINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS		
	CHECKED		

MODEL X5_S1L_06
FILE NAME H:\SDS\K96\LEG_Elin\2021\EG21EG2102.Dgn Final EG2102-2KASEC.dgn



<div><div>50 YEARS</div><div></div></div> <div>Engineering Enterprises, Inc. CONSULTING ENGINEERS 52 Wheeler Road Sugar Grove, Illinois 60554 630.466.6700 / www.eelweb.com</div>	USER NAME = JMarvlg	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DUNDEE AVENUE CROSS SECTIONS				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 1:20	DRAWN -	REVISED -						2525	20-00189-00-PV	KANE	184	158
	PLOT DATE = 11/5/2024	CHECKED -	REVISED -		CONTRACT NO. 61K68								
	DATE -	REVISED -	SCALE: 1"=10'H, 5'V				SHEET 6 OF 32 SHEETS	STA. 107+50	TO STA. 108+00		ILLINOIS	FED. AID PROJECT	

FINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS		
	CHECKED		

ORIGINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS		
	CHECKED		

MODEL XS_SHL_07

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USER NAME = JMarvlg
PLOT SCALE = 1:20
PLOT DATE = 11/5/2024

DESIGNED -
DRAWN -
CHECKED -
DATE -

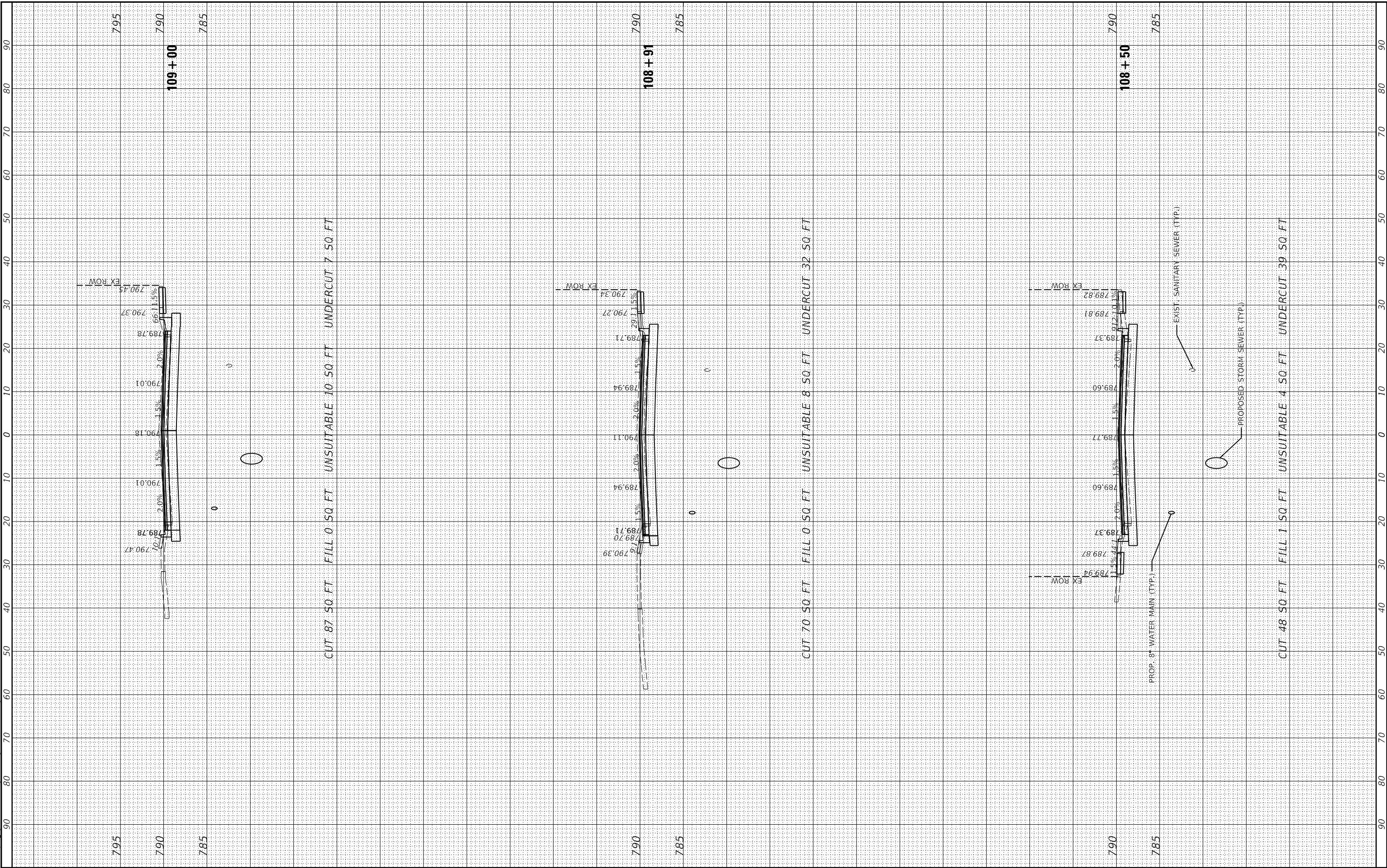
REVISED -
REVISED -
REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DUNDEE AVENUE
CROSS SECTIONS

SCALE: 1"=10'H, 5"V SHEET 7 OF 32 SHEETS STA. 108+50 TO STA. 109+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2525	20-00189-00-PV	KANE	184	159
CONTRACT NO. 61K68				
ILLINOIS FED. AID PROJECT				



ORIGINAL SURVEY	SURVEYED _____	BY _____	DATE _____
	PLOTTED _____		
NOTE BOOK	TEMPLATE _____	NO. _____	
	AREAS _____		
	AREAS CHECKED _____		

MODEL: XS_Sht_08

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USER NAME	= JMarvlg
PLOT SCALE	= 1:20
PLOT DATE	= 11/5/2024

DESIGNED	-
DRAWN	-
CHECKED	-
DATE	-

REVISED	-
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REVISED	-
REVISED	-

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DUNDEE AVENUE CROSS SECTIONS

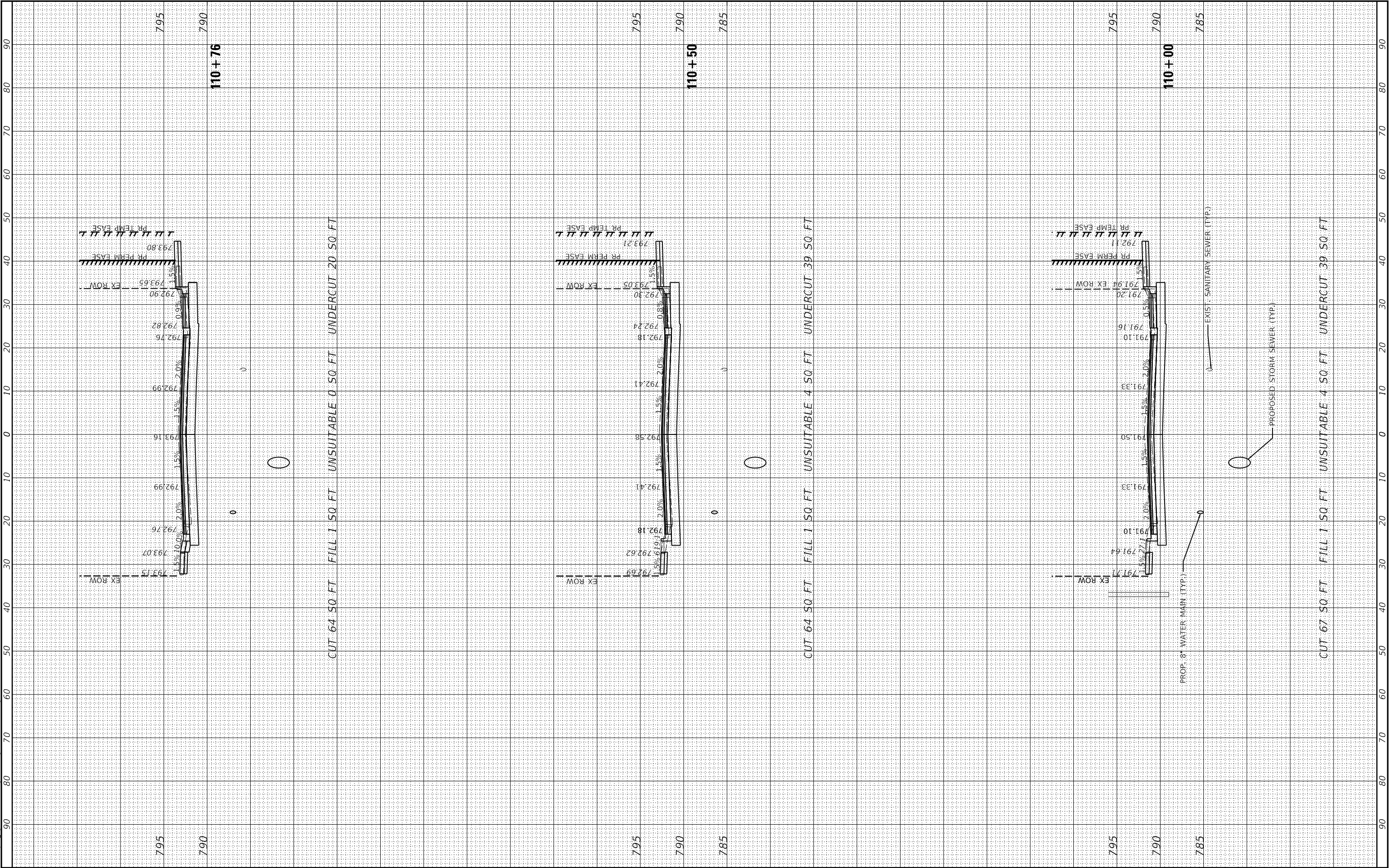
SCALE: 1"=10'H, 5'V	SHEET 8 OF 32 SHEETS	STA. 109+41 TO STA. 109+50
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2525	20-00189-00-PV	KANE	184	160
		CONTRACT NO. 61K68		
ILLINOIS		FED. AID PROJECT		

FINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

ORIGINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

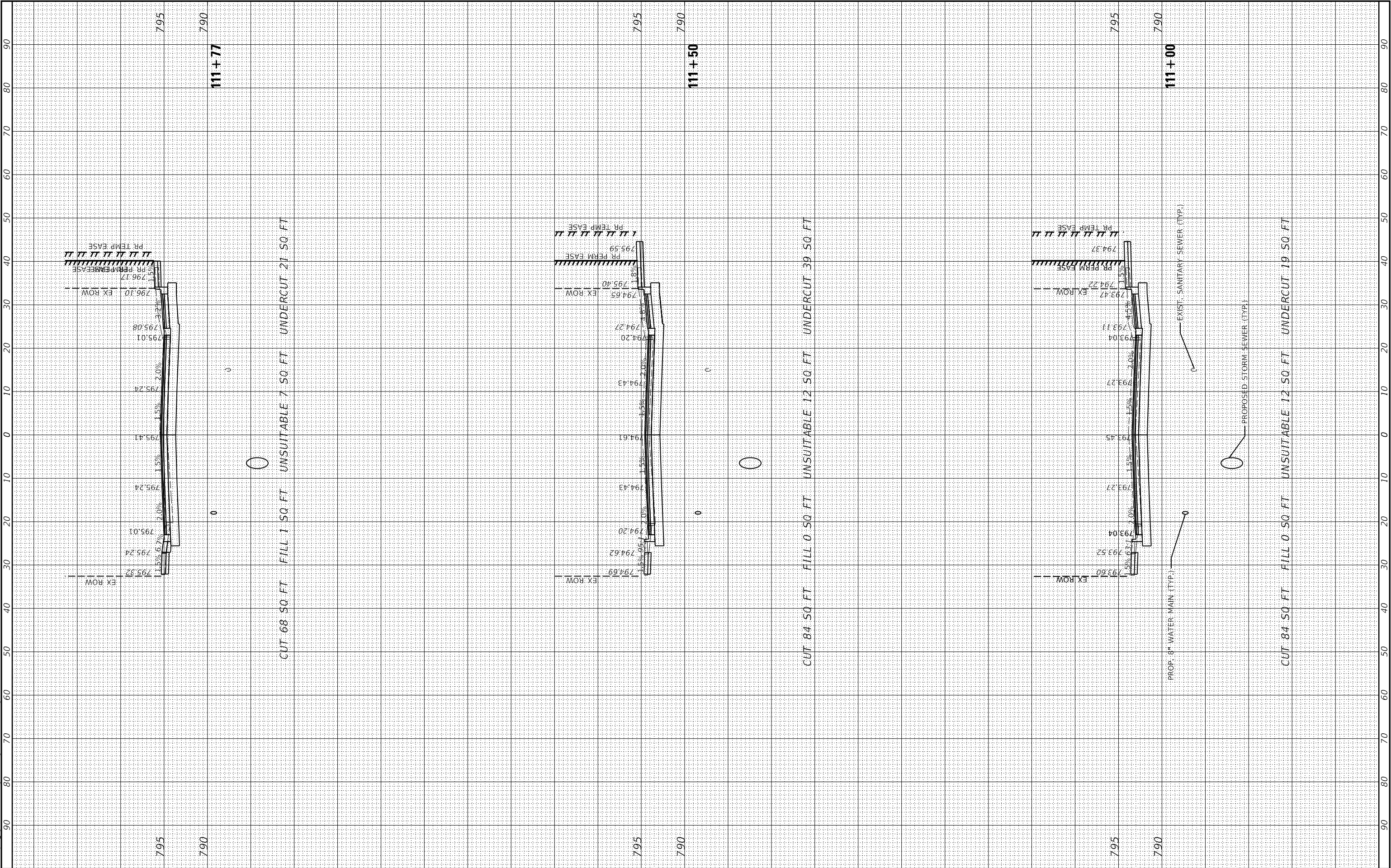
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NO.	NO.		
NO.	NO.		
NO.	NO.		

ORIGINAL	SURVEYED	BY	DATE
NO.	NO.		
NO.	NO.		
NO.	NO.		

MODEL XS_Sht_10
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<div><div>50 YEARS</div><div></div></div> <div>Engineering Enterprises, Inc. CONSULTING ENGINEERS 52 Wheeler Road Sugar Grove, Illinois 60554 630.466.6700 / www.eelweb.com</div>	USER NAME = JMarvlg	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DUNDEE AVENUE CROSS SECTIONS					F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 1:20	DRAWN -	REVISED -							2525	20-00189-00-PV	KANE	184	162
	PLOT DATE = 11/5/2024	CHECKED -	REVISED -		CONTRACT NO. 61K68									
		DATE -	REVISED -		ILLINOIS FED. AID PROJECT									
					SCALE: 1"=10'H, 5'V	SHEET 10	OF 32	SHEETS	STA. 111+00	TO STA. 111+77				

FINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS	CHECKED	

ORIGINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS	CHECKED	

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USER NAME = JMarvlg
PLOT SCALE = 1:20
PLOT DATE = 11/5/2024

DESIGNED -
DRAWN -
CHECKED -
DATE -

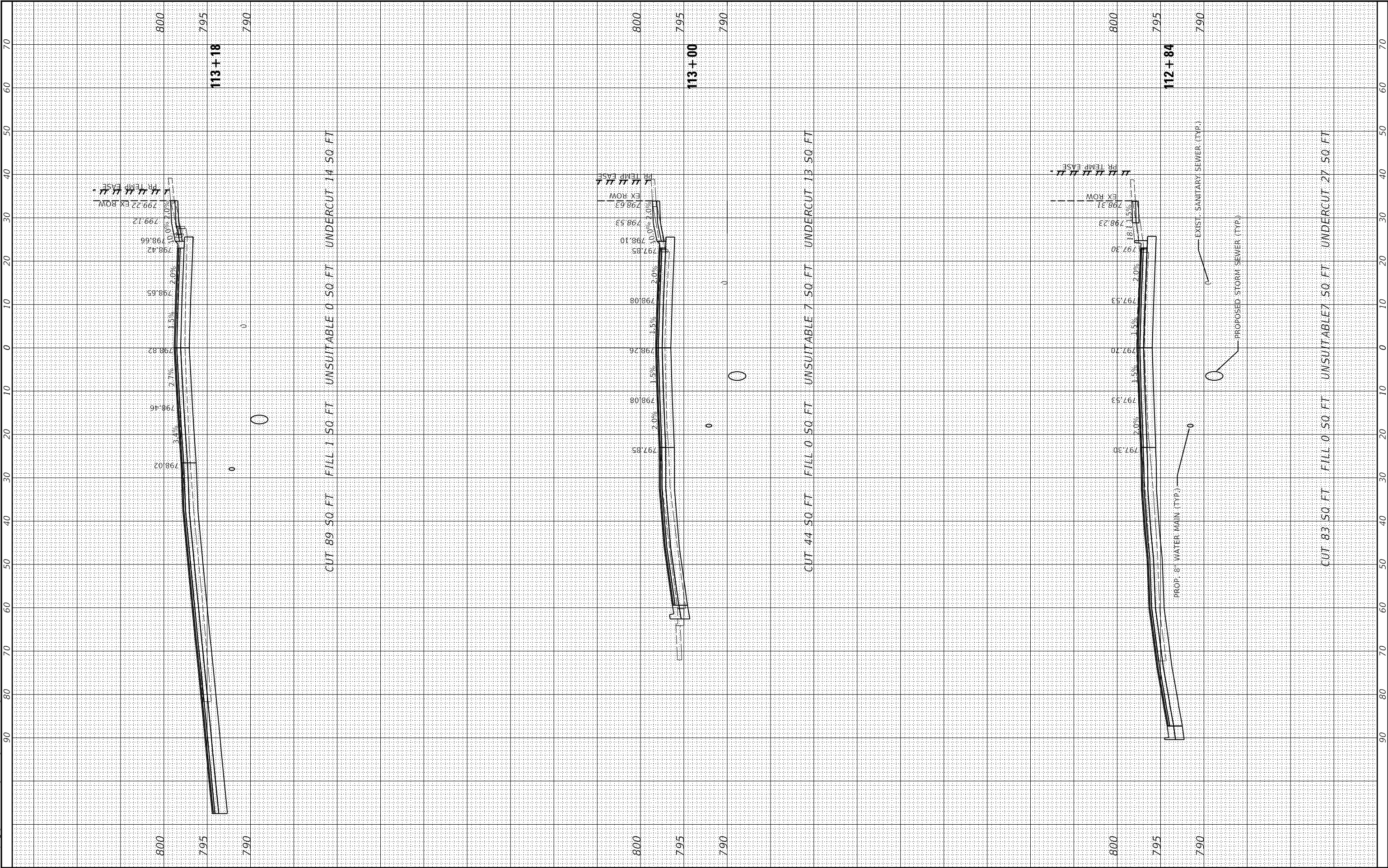
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REVISED -
REVISED -
REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DUNDEE AVENUE
CROSS SECTIONS**

SCALE: 1"=10'H, 5'V SHEET 12 OF 32 SHEETS STA. 112+84 TO STA. 113+18

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2525	20-00189-00-PV	KANE	184	164
CONTRACT NO. 61K68				
ILLINOIS FED. AID PROJECT				



ORIGINAL SURVEY	SURVEYED _____	BY _____	DATE _____
	PLOTTED _____		
	TEMPLATE _____		
	AREAS _____		
	AREAS CHECKED _____		
NOTE BOOK			
NO. _____			

MODEL: XS_Sht_13

FILE NAME: H:\SDSK



521

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USER NAME	= JMarvlg
PLOT SCALE	= 1:20
PLOT DATE	= 11/5/2024

DESIGNED	-
DRAWN	-
CHECKED	-
DATE	-

REVISED	-
REVISED	-
REVISED	-
REVISED	-

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DUNDEE AVENUE CROSS SECTIONS

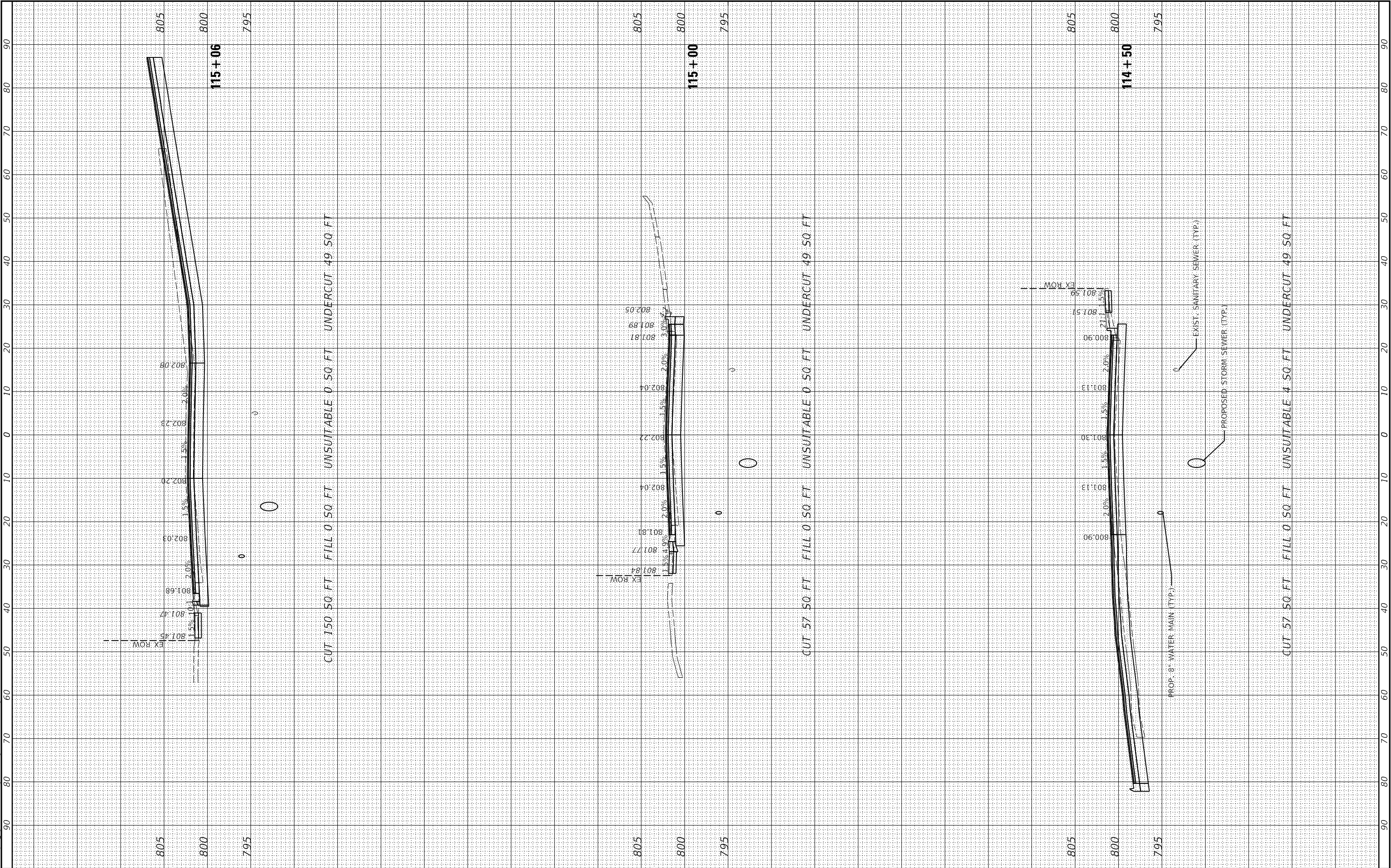
SCALE: 1"=10'H, 5'V	SHEET 13 OF 32 SHEETS	STA. 113+19 TO STA. 114+00
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2525	20-00189-00-PV	KANE	184	165
		CONTRACT NO. 61K68		
ILLINOIS		FED. AID PROJECT		

FINAL SURVEY	SURVEYED PLOTTED	BY	DATE
NO.	NO.		

ORIGINAL SURVEY	SURVEYED PLOTTED	BY	DATE
NO.	NO.		

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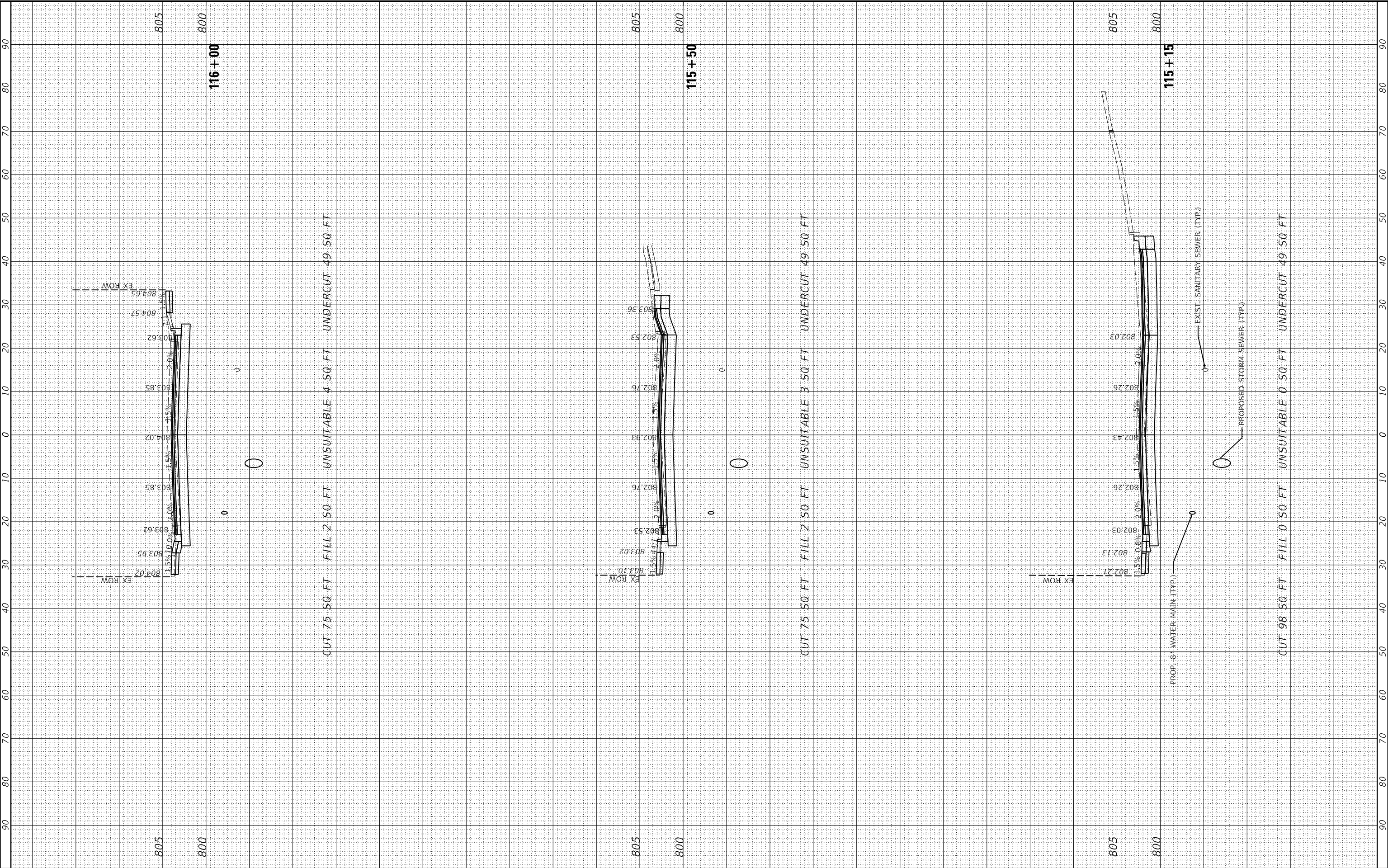



<div>50 YEARS</div> <div></div>	Engineering Enterprises, Inc. CONSULTING ENGINEERS 52 Wheeler Road Sugar Grove, Illinois 60554 630.466.6700 / www.eetweb.com	USER NAME = JMarvlg	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DUNDEE AVENUE CROSS SECTIONS				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.	
		PLOT SCALE = 1:20	CHECKED -	REVISED -						2525	20-00189-00-PV	KANE	184	166	
		PLOT DATE = 11/5/2024	DATE -	REVISED -		SCALE: 1"=10'H, 5'V				SHEET 14 OF 32 SHEETS	STA. 114+00 TO STA. 115+06	CONTRACT NO. 61K68			
											ILLINOIS FED. AID PROJECT				

FINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

ORIGINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

MODEL XS_Sht_15
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	DRAWN -		DESIGNED -	REVISED -									2525	20-00189-00-PV	KANE	184	167
	PLOT SCALE = 1:20		CHECKED -	REVISED -									CONTRACT NO. 61K68				
	PLOT DATE = 11/5/2024		DATE -	REVISED -									ILLINOIS FED. AID PROJECT				
SCALE: 1"=10'H, 5'V										SHEET 15	OF 32 SHEETS	STA. 115+15	TO STA. 116+00				

ORIGINAL SURVEY	SURVEYED _____	BY _____	DATE _____
	PLOTTED _____		
NOTE BOOK	TEMPLATE _____	AREAS _____	AREAS CHECKED _____
	NO. _____		

MODEL: XS_Sht_16



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USER NAME	= JMarvlg
PLOT SCALE	= 1:20
PLOT DATE	= 11/5/2024

DESIGNED	-
DRAWN	-
CHECKED	-
DATE	-

REVISED	-
REVISED	-
REVISED	-
REVISED	-

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DUNDEE AVENUE CROSS SECTIONS

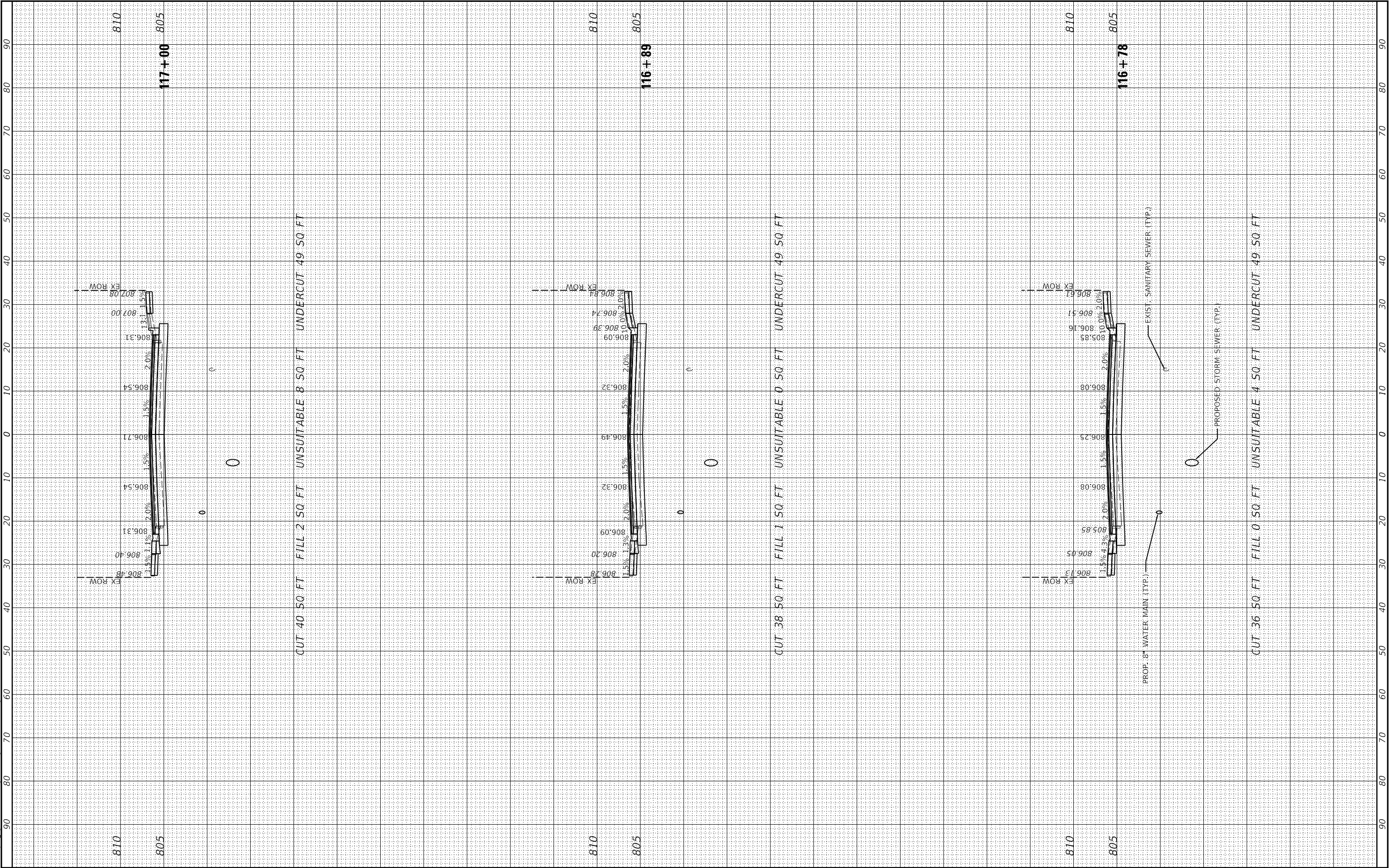
SCALE: 1"=10'H, 5'V	SHEET 16 OF 32 SHEETS	STA. 116+27 TO STA. 116+64
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2525	20-00189-00-PV	KANE	184	168
		CONTRACT NO. 61K68		
		ILLINOIS FED. AID PROJECT		

FINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS	CHECKED	

ORIGINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS	CHECKED	

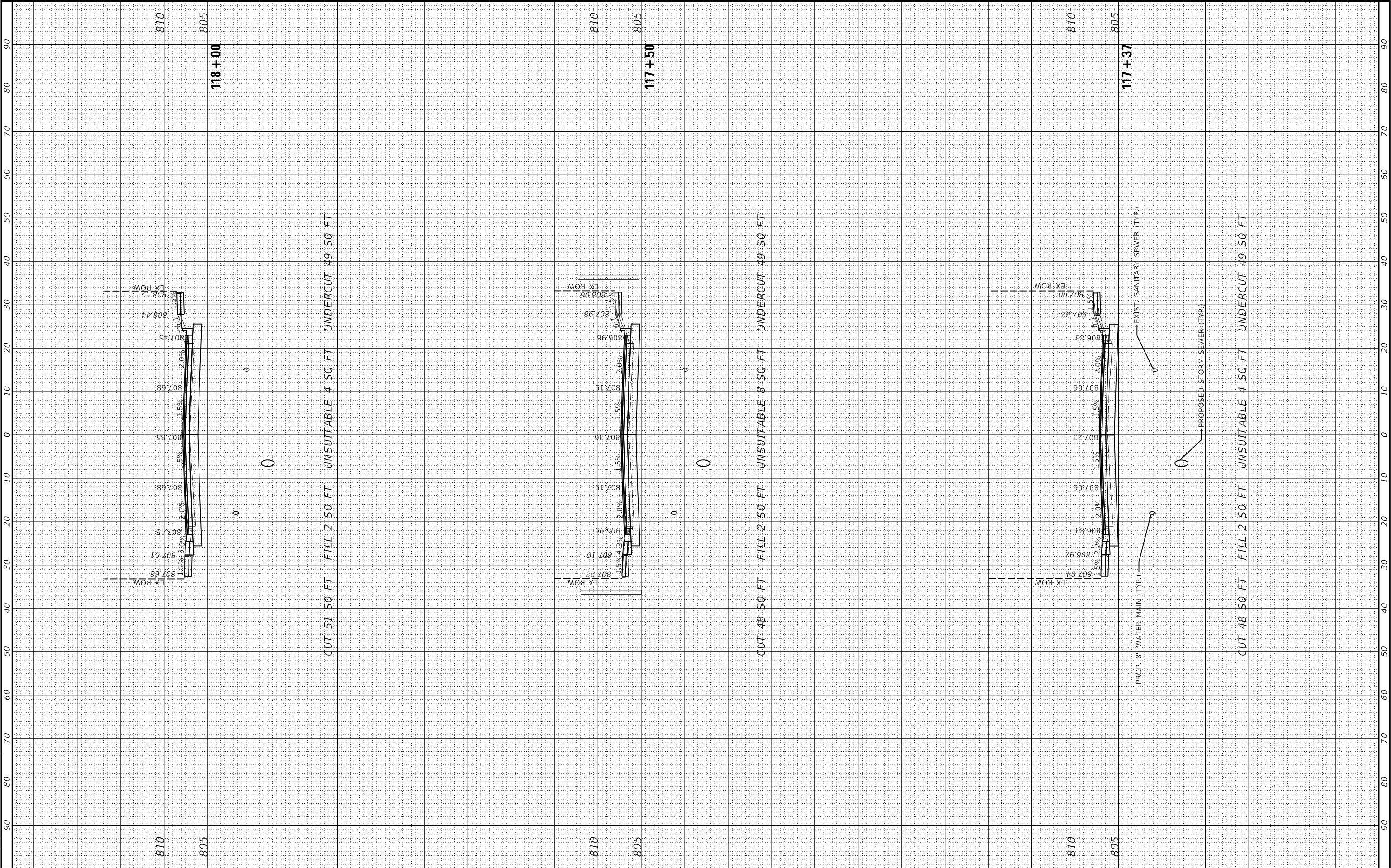
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FINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

ORIGINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

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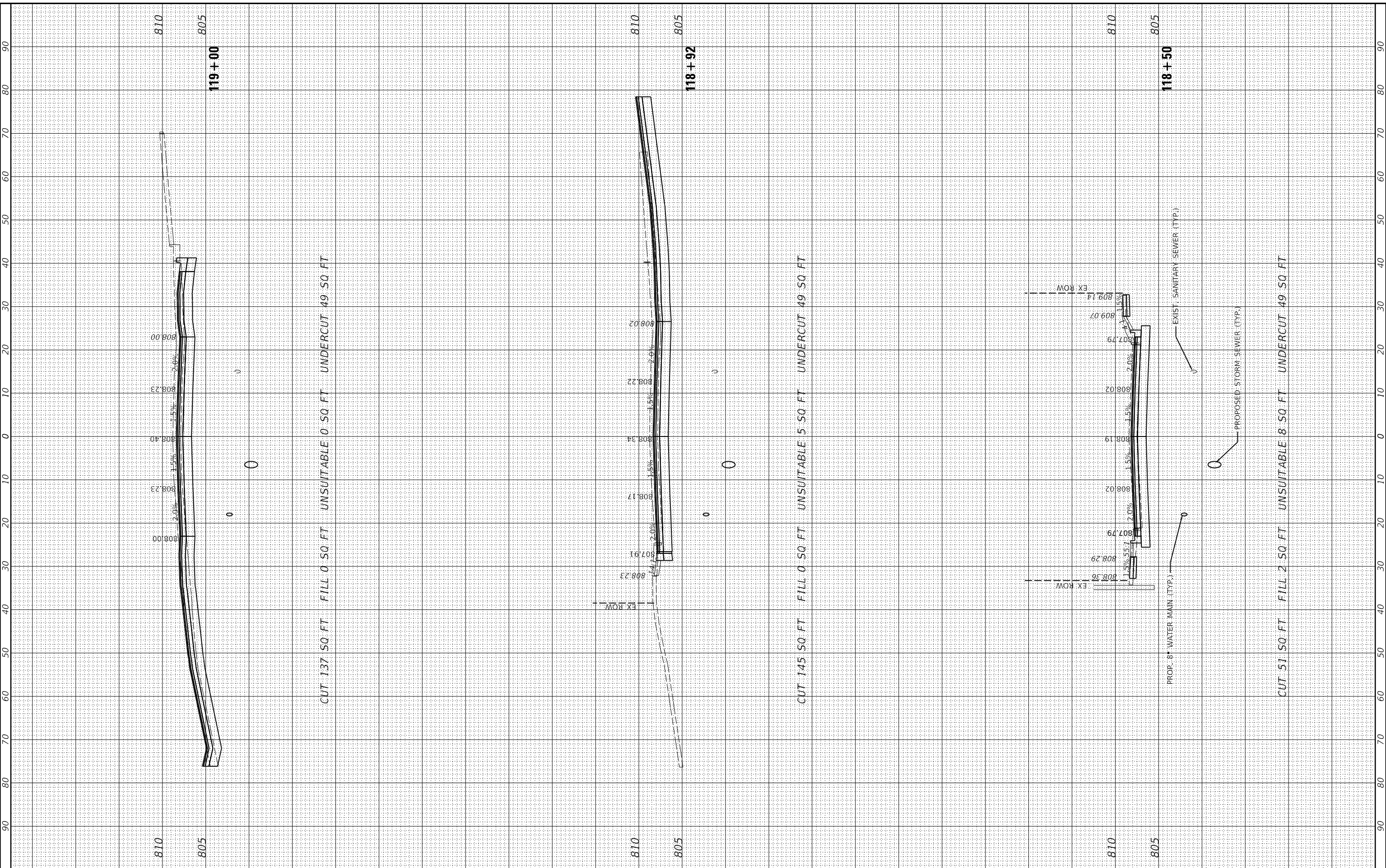
<div><div>50 YEARS</div><div></div></div> <div>Engineering Enterprises, Inc. CONSULTING ENGINEERS 52 Wheeler Road Sugar Grove, Illinois 60554 630.466.6700 / www.eeiweb.com</div>	USER NAME = jmarvlg	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DUNDEE AVENUE CROSS SECTIONS				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 1:20	DRAWN -	REVISED -						2525	20-00189-00-PV	KANE	184	170
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					SCALE: 1"=10'H, 5'V	SHEET 18	OF 32 SHEETS	STA. 117+37	TO STA. 118+00	ILLINOIS FED. AID PROJECT			



FINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

ORIGINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

MODEL XS_Sht_19

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USER NAME = jmarvig	DESIGNED -	REVISED -
PLOT SCALE = 1:20	DRAWN -	REVISED -
PLOT DATE = 11/5/2024	CHECKED -	REVISED -
	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DUNDEE AVENUE
CROSS SECTIONS

SCALE: 1"=10'H, 5'V

SHEET 19 OF 32 SHEETS

STA. 118+50 TO STA. 119+00

F.A.U. RTE. 2525	SECTION 20-00189-00-PV	COUNTY KANE	TOTAL SHEETS 184	SHEET NO. 171
ILLINOIS			FED. AID PROJECT	

CONTRACT NO. 61K68

ORIGINAL SURVEY	SURVEYED _____	BY _____	DATE _____
	PLOTTED _____		
NOTE BOOK	TEMPLATE _____	AREAS _____	AREAS CHECKED _____
	NO. _____		

MODEL: XS_Sht_20
FILE NAME: H:\SDSK



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USER NAME	= JMarvig
PLOT SCALE	= 1:20
PLOT DATE	= 11/5/2024

DESIGNED	-
DRAWN	-
CHECKED	-
DATE	-

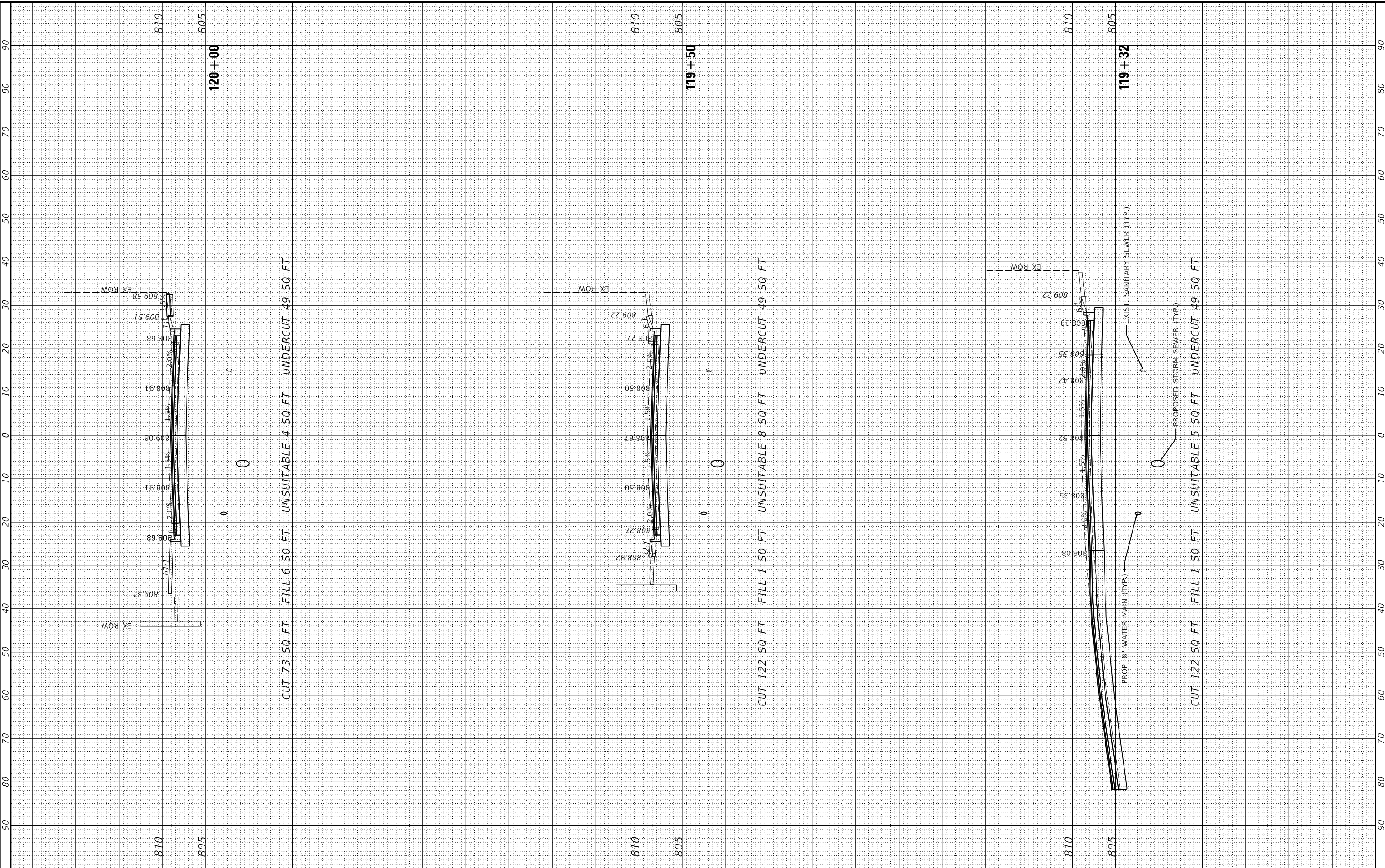
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REVISED	-
REVISED	-
REVISED	-

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DUNDEE AVENUE CROSS SECTIONS

SCALE: 1"=10'H, 5'V	SHEET 20 OF 32 SHEETS	STA. 119+32 TO STA. 120+00
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2525	20-00189-00-PV	KANE	184	172
		CONTRACT NO. 61K68		
ILLINOIS		FED. AID PROJECT		



FINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

ORIGINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

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USER NAME = jmarvig
PLOT SCALE = 1:20
PLOT DATE = 11/5/2024

DESIGNED -
DRAWN -
CHECKED -
DATE -

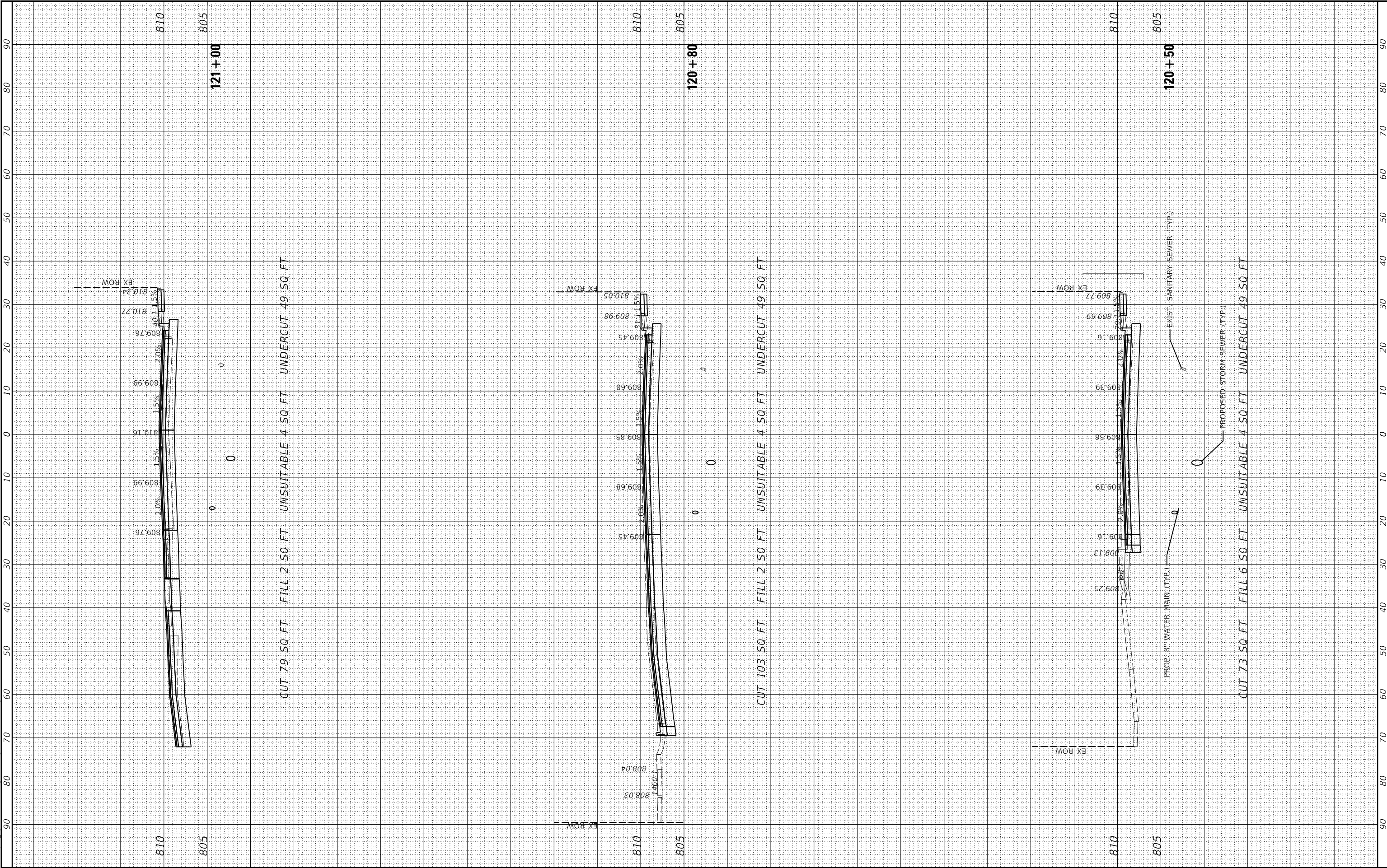
REVISED -
REVISED -
REVISED -
REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**DUNDEE AVENUE
CROSS SECTIONS**

SCALE: 1"=10'H, 5'V SHEET 21 OF 32 SHEETS STA. 120+50 TO STA. 121+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2525	20-00189-00-PV	KANE	184	173
CONTRACT NO. 61K68				
ILLINOIS FED. AID PROJECT				

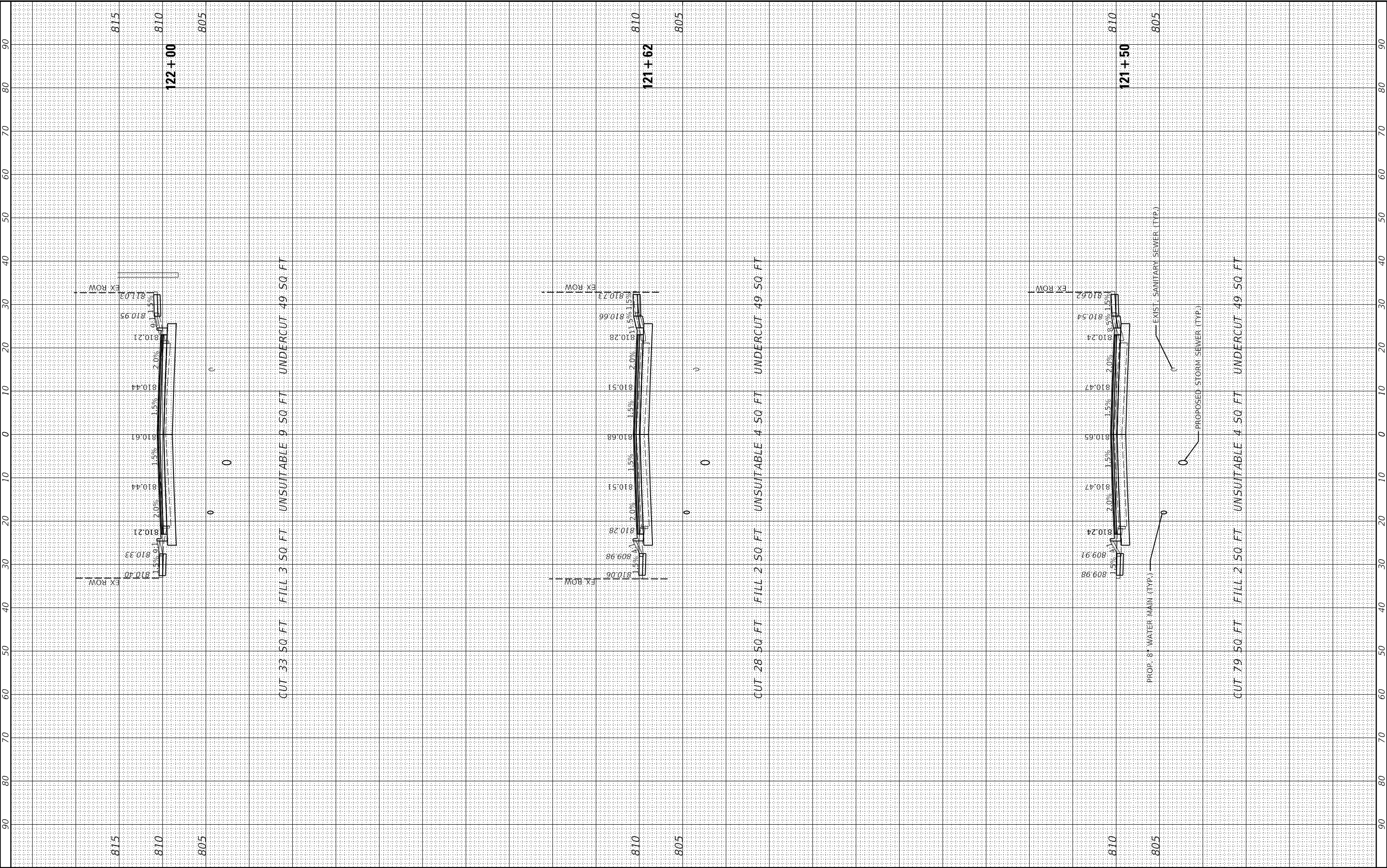


FINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

ORIGINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

MODEL XS_Sht_22

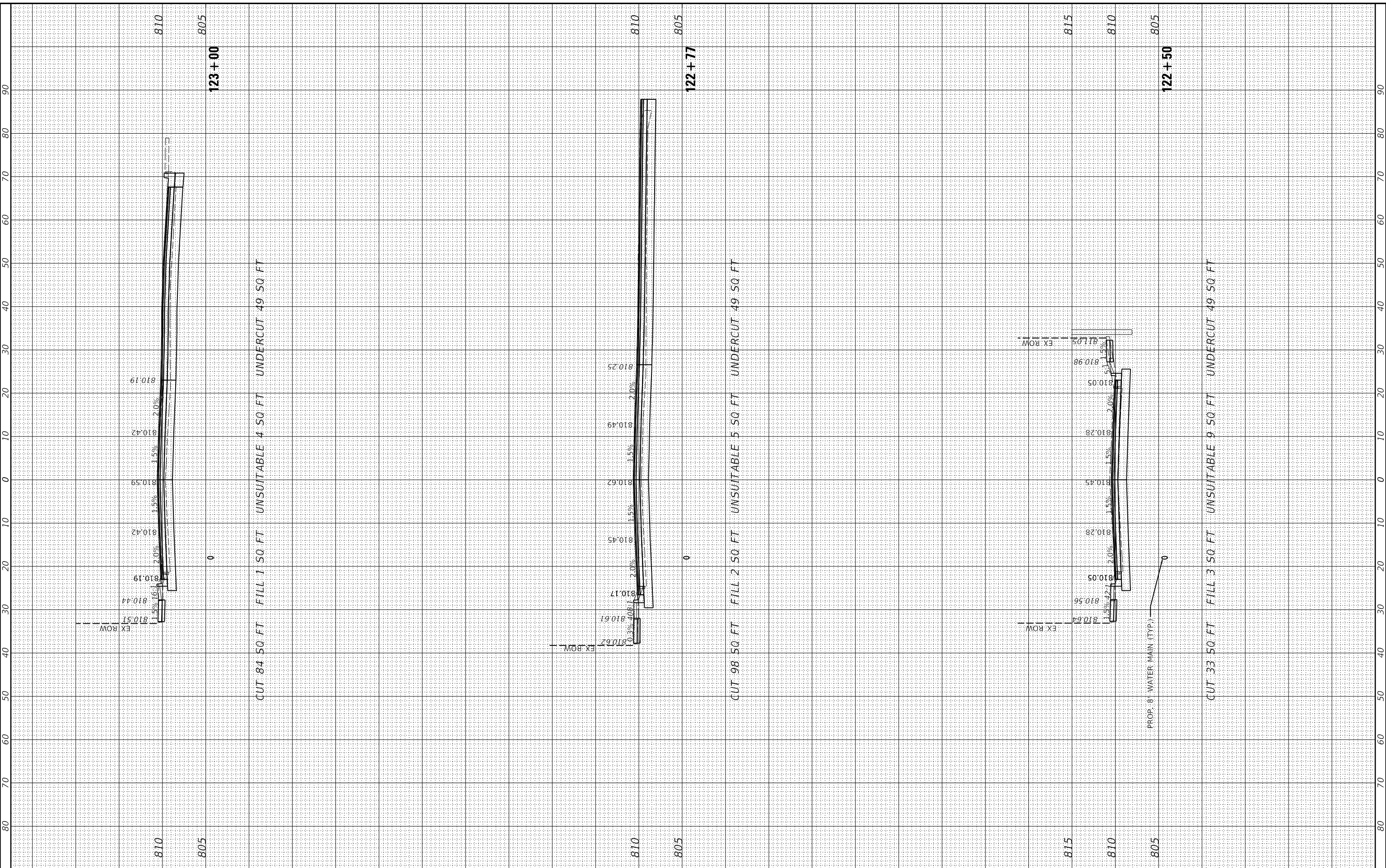
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FINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

ORIGINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

MODEL XS_Sht_23
FILE NAME H:\SDS\p06\IG_Elin\2021EG21EG2102.dgn Final ElinEG2102-2RXCSEC.dgn



50 YEARS

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USER NAME = jmarvig	DESIGNED -	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 1:20	CHECKED -	REVISED -
PLOT DATE = 11/5/2024	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DUNDEE AVENUE
CROSS SECTIONS

SCALE: 1"=10'H, 5'V

SHEET 23

OF 32 SHEETS

STA. 122+50

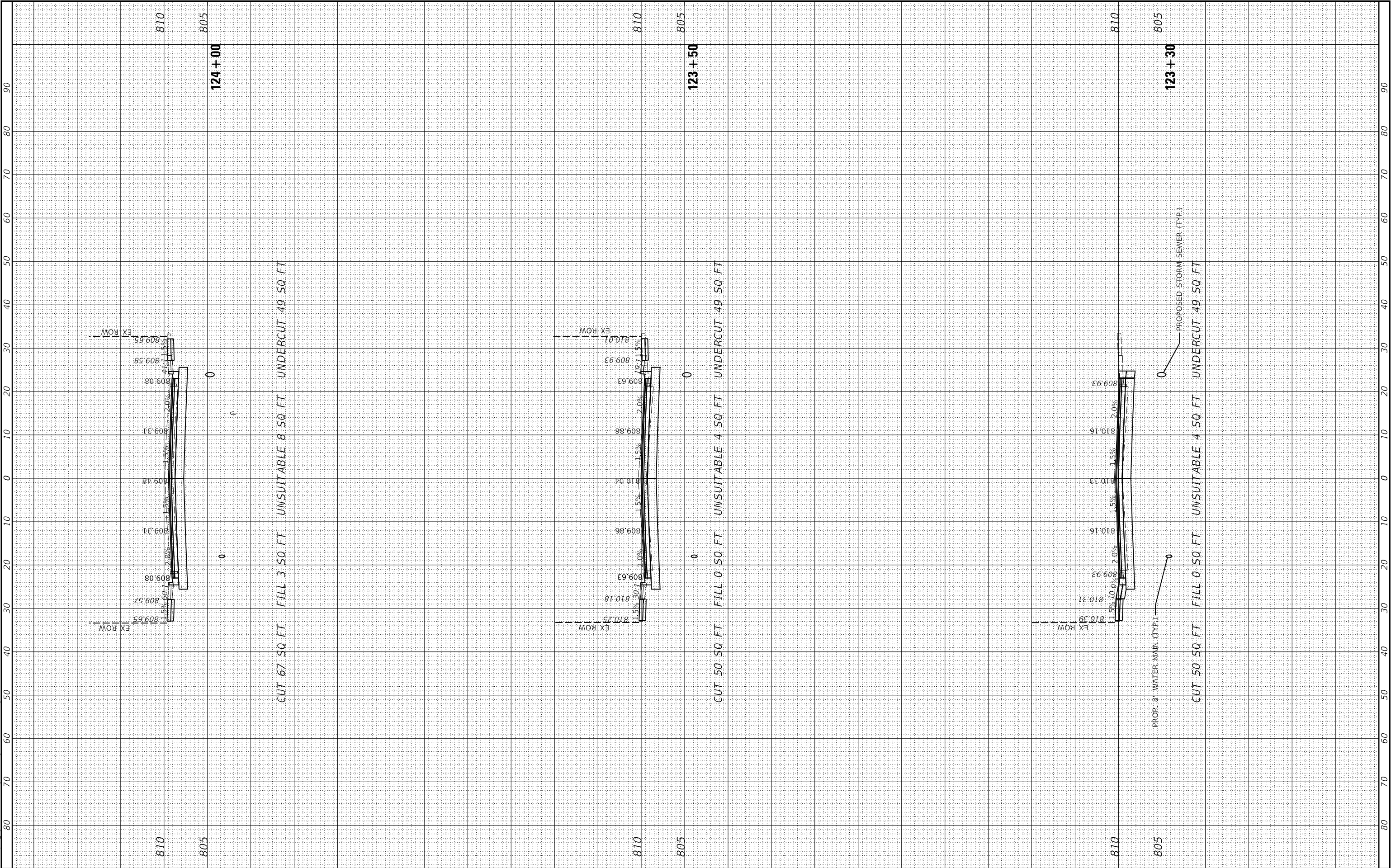
TO STA. 123+00


F.A.U. RTE. 2525	SECTION 20-00189-00-PV	COUNTY KANE	TOTAL SHEETS 184	SHEET NO. 175
ILLINOIS			FED. AID PROJECT	

FINAL SURVEY	SURVEYED PLOTTED	BY	DATE
NO.	NO.		
NO.	NO.		

ORIGINAL SURVEY	SURVEYED PLOTTED	BY	DATE
NO.	NO.		
NO.	NO.		

MODEL XS_Sht_24
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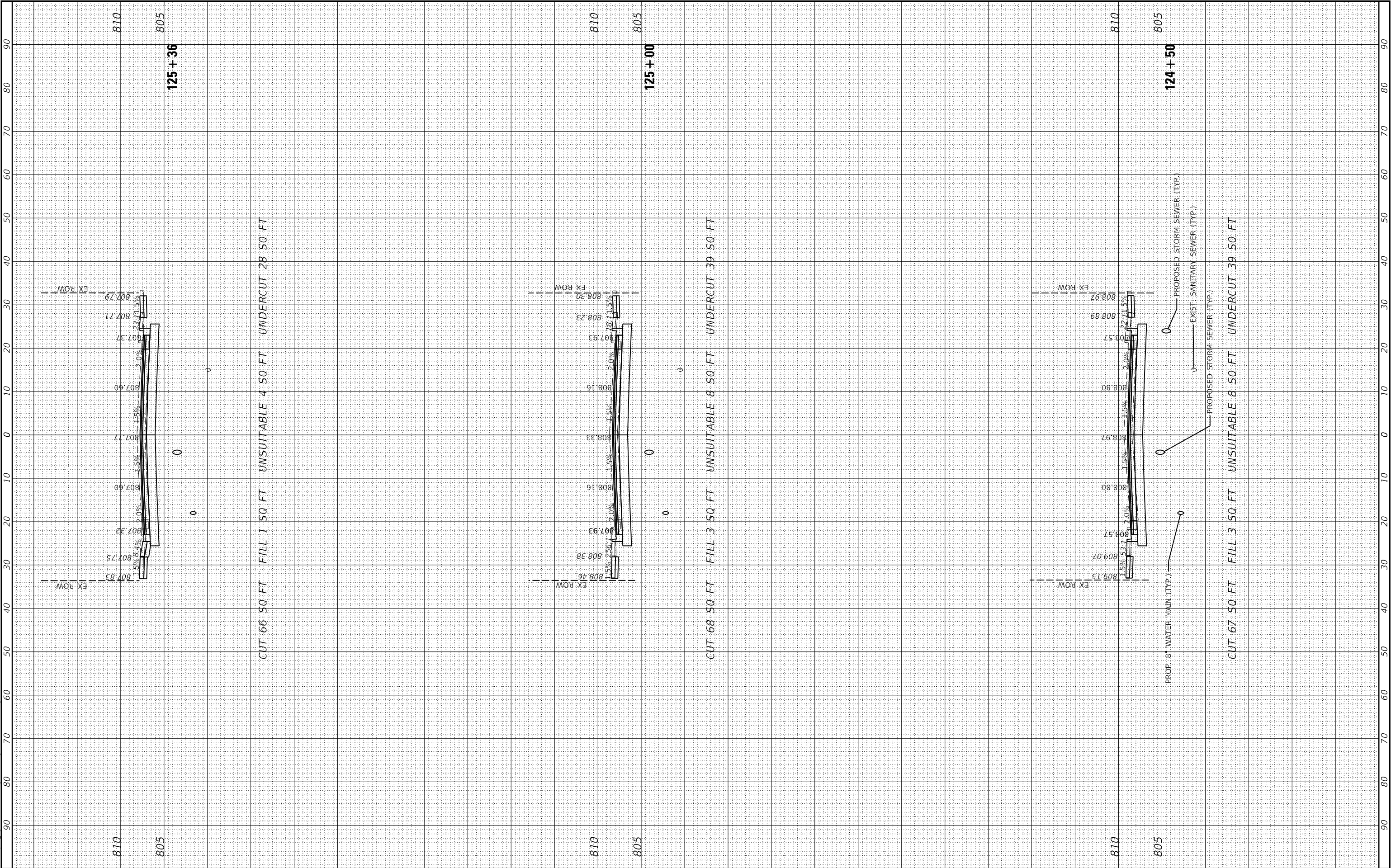


<div><div>50 YEARS</div><div></div><div>Engineering Enterprises, Inc. CONSULTING ENGINEERS 52 Wheeler Road Sugar Grove, Illinois 60554 630.466.6700 / www.eeiweb.com</div></div>	USER NAME = jmarvig		DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DUNDEE AVENUE CROSS SECTIONS				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 1:20		DRAWN -	REVISED -						2525	20-00189-00-PV	KANE	184	176
	PLOT DATE = 11/5/2024		CHECKED -	REVISED -		CONTRACT NO. 61K68								
			DATE -	REVISED -										
	SCALE: 1"=10'H, 5'V		SHEET 24	OF 32 SHEETS		STA. 123+30	TO STA. 124+00		ILLINOIS FED. AID PROJECT					

FINAL	SURVEYED	BY	DATE
NO.	NO.		
NOTE BOOK	PLOTTED		
AREAS	TEMPLATE		
CHECKED	AREAS		

ORIGINAL	SURVEYED	BY	DATE
NO.	NO.		
NOTE BOOK	PLOTTED		
AREAS	TEMPLATE		
CHECKED	AREAS		

MODEL XS_Sht_25
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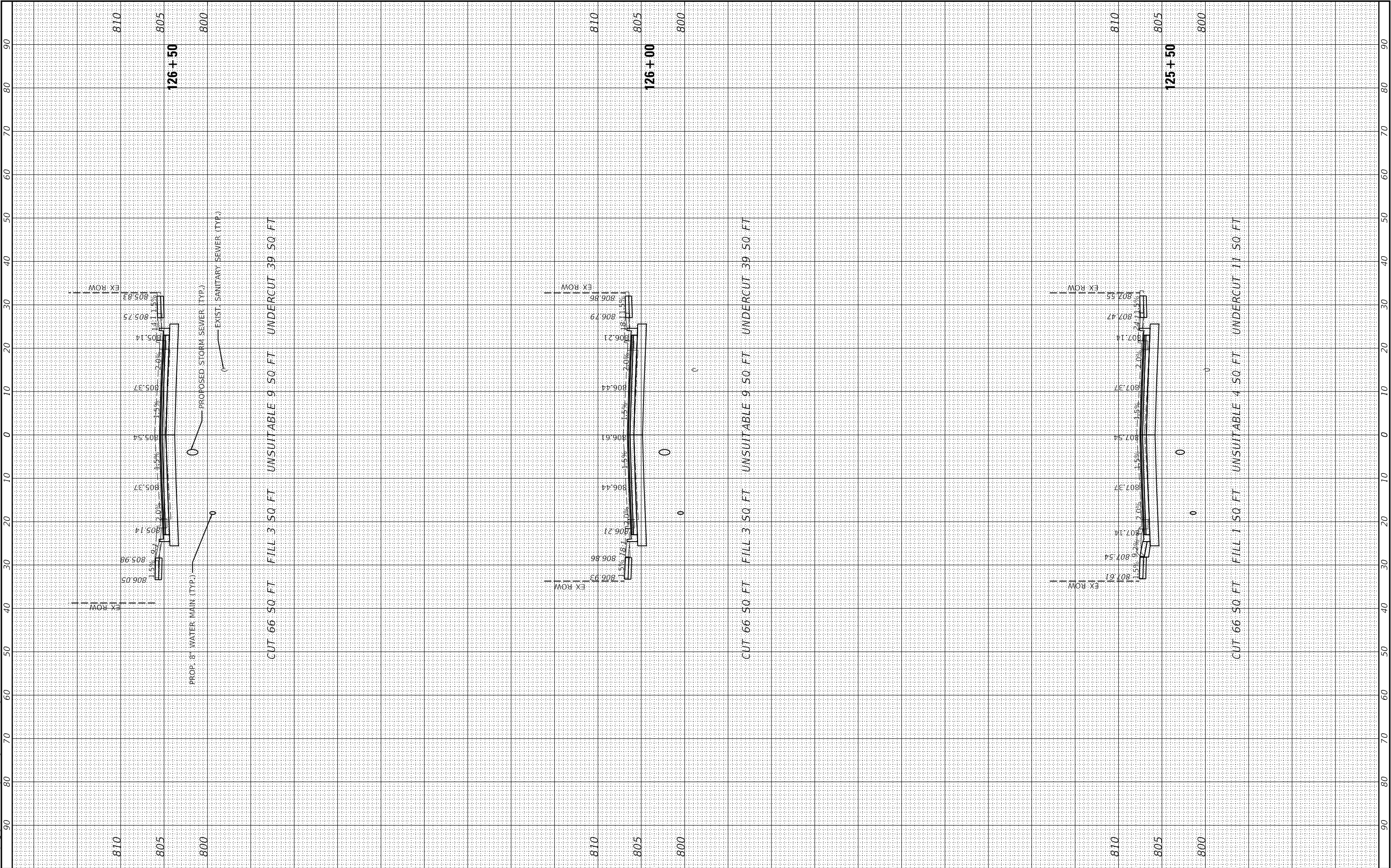


<div><div>50 YEARS</div><div></div></div> <div>Engineering Enterprises, Inc. CONSULTING ENGINEERS 52 Wheeler Road Sugar Grove, Illinois 60554 630.466.6700 / www.eelweb.com</div>	USER NAME = JMarvlg	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DUNDEE AVENUE CROSS SECTIONS				F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 1:20	DRAWN -	REVISED -		2525	20-00189-00-PV	KANE	184	177				
	PLOT DATE = 11/5/2024	CHECKED -	REVISED -		CONTRACT NO. 61K68								
	DATE -	REVISED -	ILLINOIS FED. AID PROJECT										
		SCALE: 1"=10'H, 5'V		SHEET 25 OF 32 SHEETS	STA. 124+50 TO STA. 125+36								

FINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

ORIGINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

MODEL XS_Sht_26
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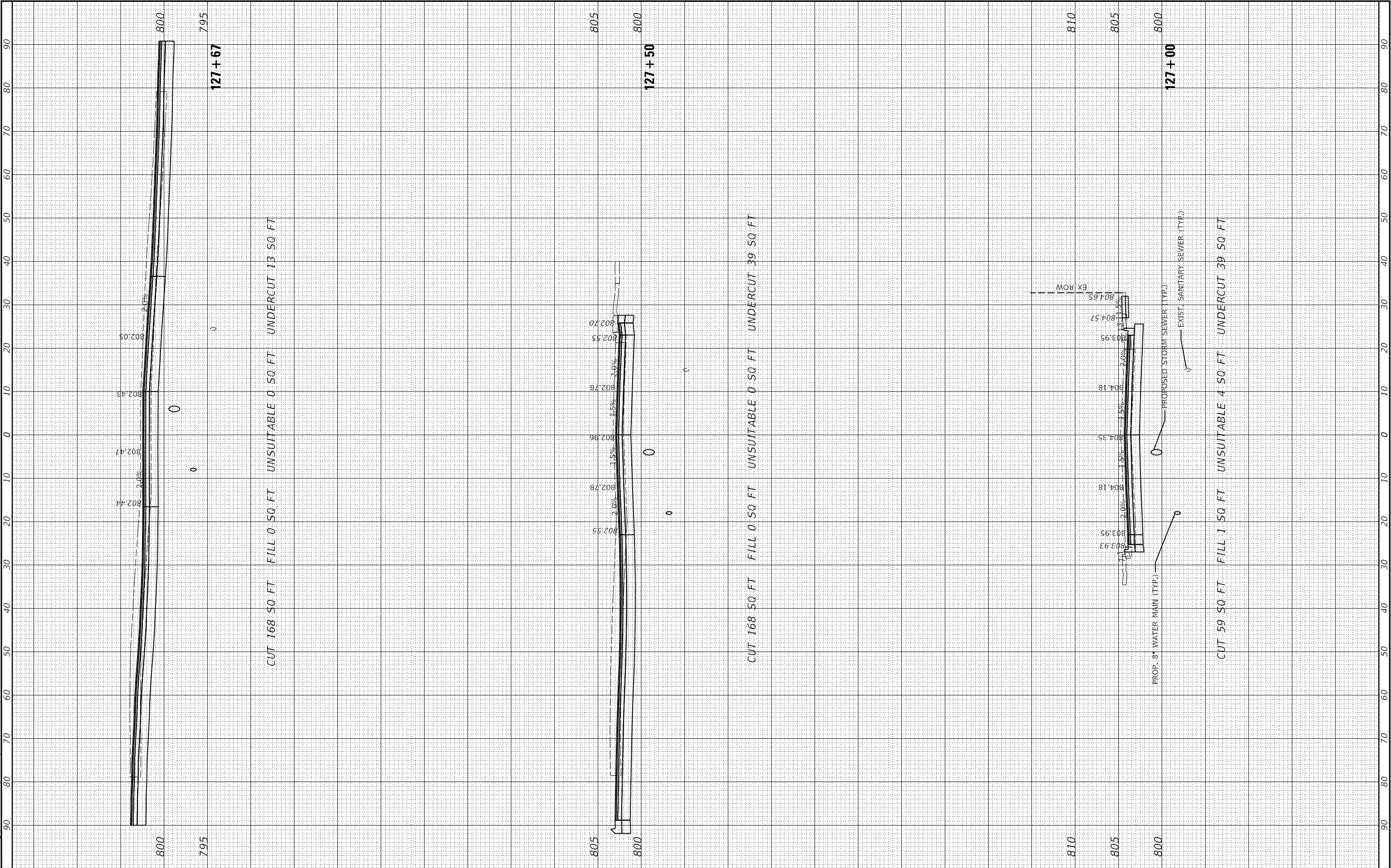


<div><div>50 YEARS</div><div></div></div> <div>Engineering Enterprises, Inc. CONSULTING ENGINEERS 52 Wheeler Road Sugar Grove, Illinois 60554 630.466.6700 / www.eelweb.com</div>	USER NAME = JMarvlg	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION						DUNDEE AVENUE CROSS SECTIONS						F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	PLOT SCALE = 1:20	CHECKED -	REVISED -							2525	20-00189-00-PV	KANE	184	178						
	PLOT DATE = 11/5/2024	DATE -	REVISED -	SCALE: 1"=10'H, 5'V						SHEET 26	OF 32 SHEETS	STA. 125+50	TO STA. 126+50	CONTRACT NO. 61K68						
																	ILLINOIS FED. AID PROJECT			

FINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

ORIGINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

MODEL XS_Sht_27
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FINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

ORIGINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

MODEL XS_Sht_28

FILE NAME: H:\SDS\proj\IG_Elin\2021EG21EG2102.dgn Final ElinEG2102-28XSEC.dgn

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YEARS



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USER NAME = JMarvlg
PLOT SCALE = 1:20
PLOT DATE = 11/5/2024

DESIGNED -
DRAWN -
CHECKED -
DATE -

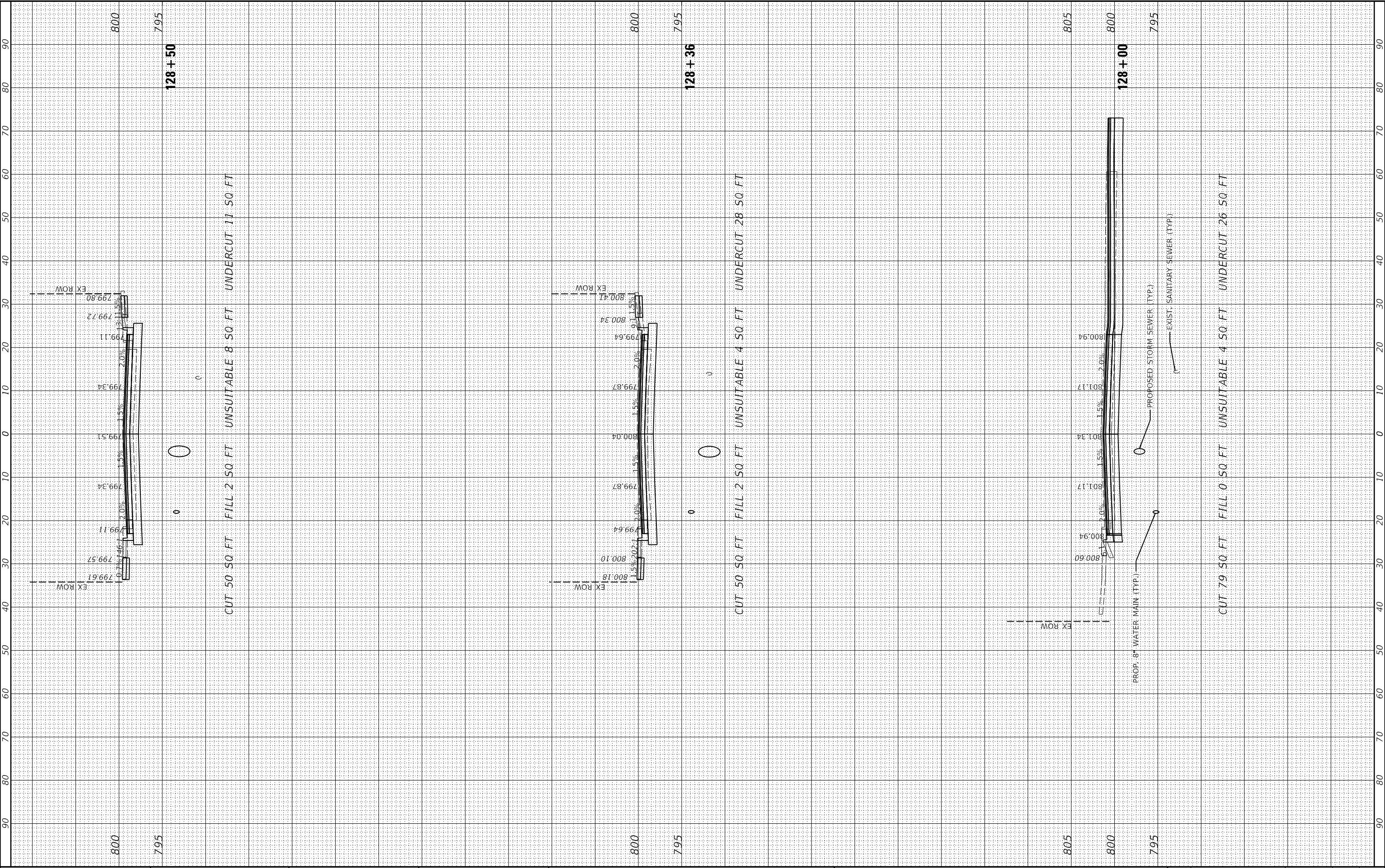
REVISED -
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REVISED -
REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DUNDEE AVENUE
CROSS SECTIONS

SCALE: 1"=10'H, 5'V SHEET 28 OF 32 SHEETS STA. 128+00 TO STA. 128+50

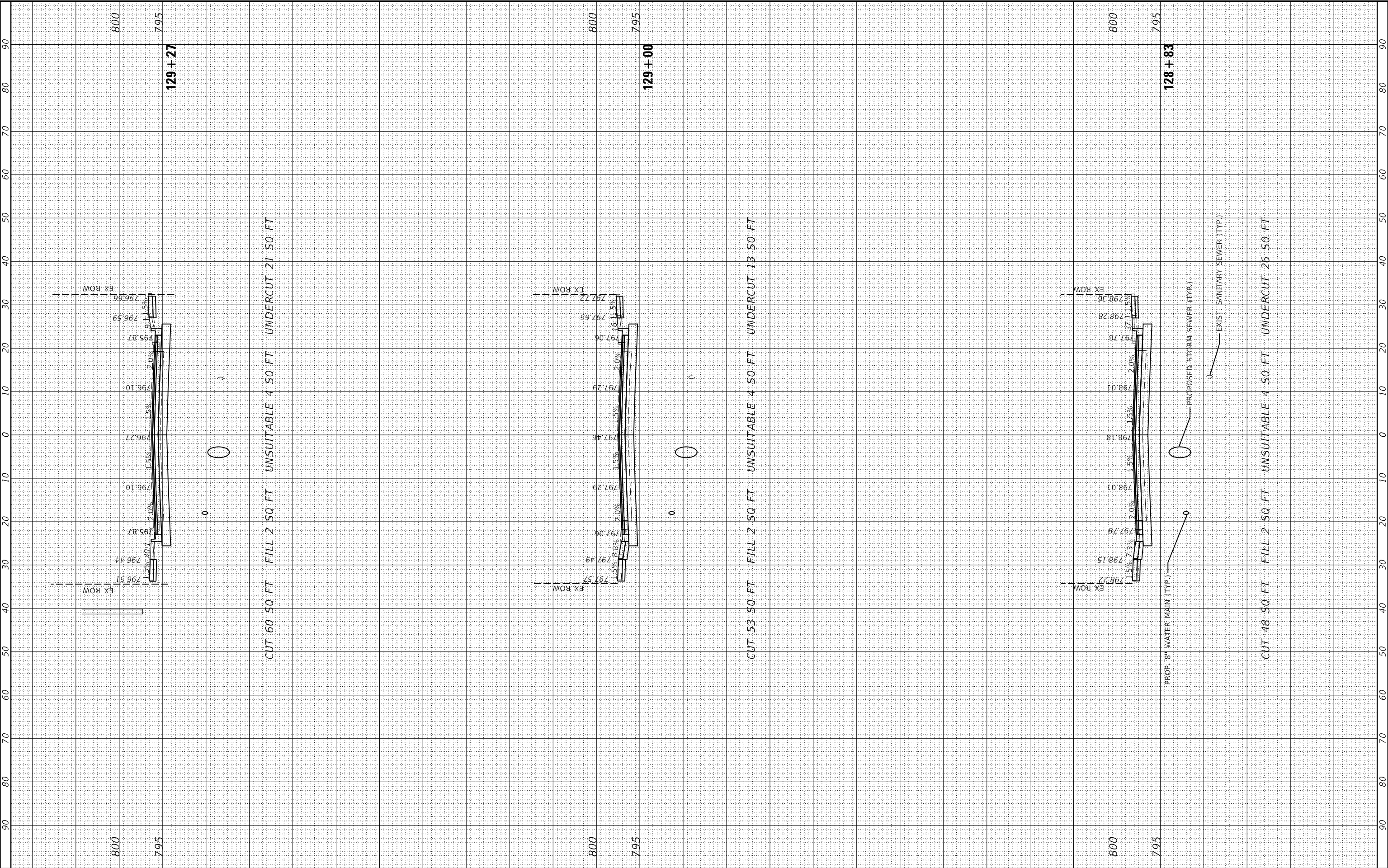
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2525	20-00189-00-PV	KANE	184	180
CONTRACT NO. 61K68				
ILLINOIS FED. AID PROJECT				



FINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

ORIGINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

MODEL XS_Sht_29
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USER NAME = JMarvlg

DESIGNED -

DRAWN -

REVISD -

REVISD -

REVISD -

REVISD -

REVISD -

REVISD -

STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION

DUNDEE AVENUE

CROSS SECTIONS

SCALE: 1"=10'H, 5'V

SHEET 29 OF 32 SHEETS

STA. 128+83 TO STA. 129+27

F.A.U. RTE. 2525

SECTION 20-00189-00-PV

COUNTY KANE

TOTAL SHEETS 184

SHEET NO. 181

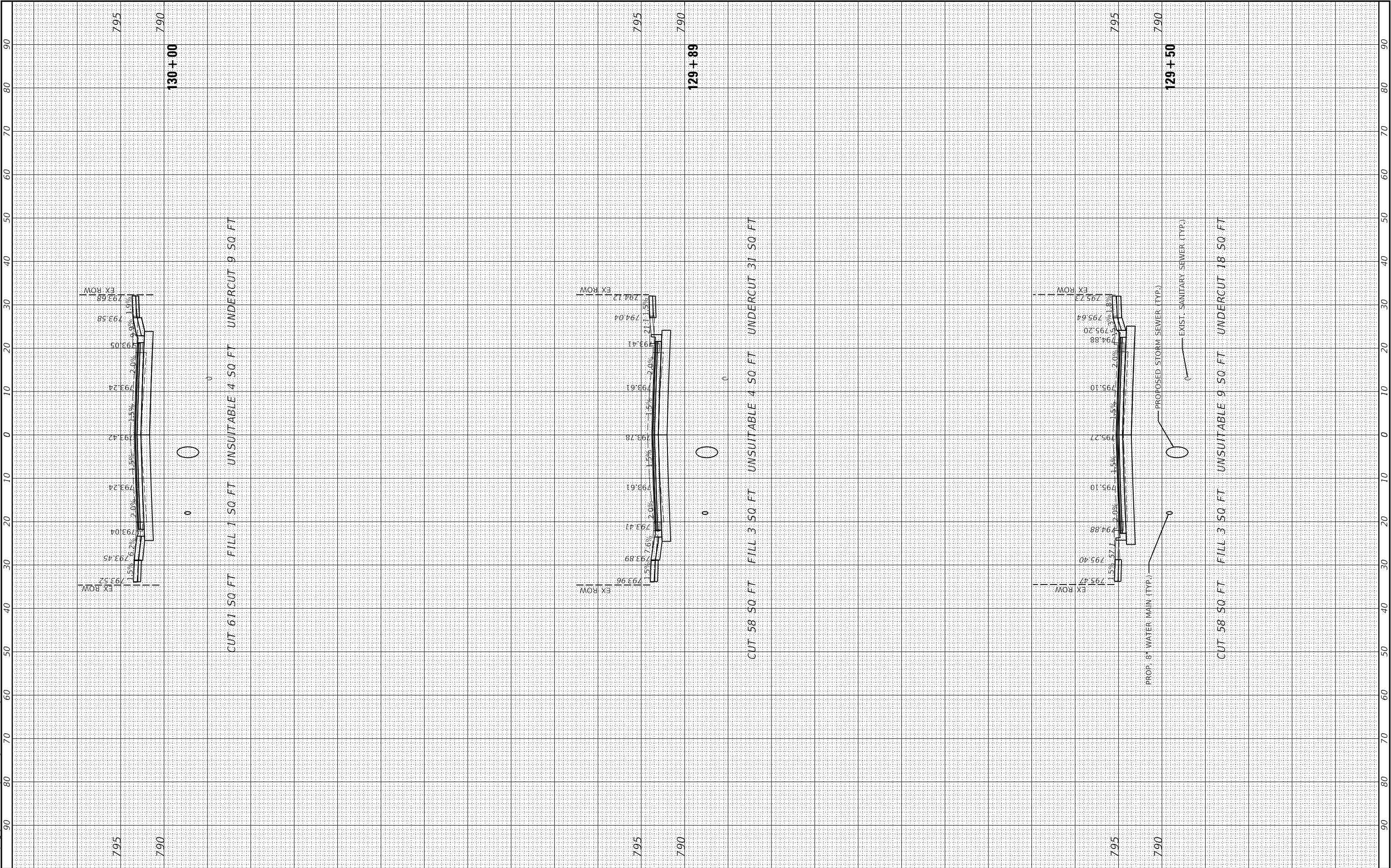
CONTRACT NO. 61K68

ILLINOIS FED. AID PROJECT

FINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

ORIGINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

MODEL XS_SHT_30
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Sugar Grove, Illinois 60554

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USER: Elin, T. (Marvlg)

DESIGNED -

DRAWN -

CHECKED -

DATE -

REVISED -

REVISED -

REVISED -

REVISED -

STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION

DUNDEE AVENUE

CROSS SECTIONS

SCALE: 1"=10'H, 5'V

SHEET 30 OF 32 SHEETS

STA. 129+50 TO STA. 130+00

F.A.U. RTE. 2525

SECTION 20-00189-00-PV

COUNTY KANE

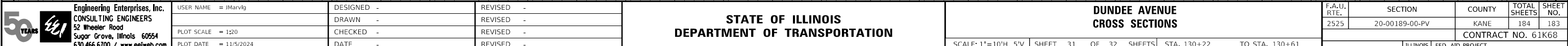
TOTAL SHEETS 184

SHEET NO. 182

CONTRACT NO. 61K68

ILLINOIS FED. AID PROJECT

ORIGINAL SURVEY	SURVEYED _____	BY _____	DATE _____
	PLOTTED _____		
	TEMPLATE _____		
	AREAS _____		
	AREAS CHECKED _____		
NOTE BOOK			
NO. _____			



F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2525	20-00189-00-PV	KANE	184	183
		CONTRACT NO. 61K68		
ILLINOIS		FED. AID PROJECT		

FINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

ORIGINAL	SURVEYED	BY	DATE
SURVEY	PLOTTED		
NOTE BOOK	TEMPLATE		
NO.	AREAS CHECKED		

MODEL XS_Sht_32

FILE NAME: H:\SDS\p06\LEG_E\p0621EG21EG2102.dgn Final EngEG2102-2R\XSEC.dgn

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YEARS



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52 Wheeler Road

Sugar Grove, Illinois 60554

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USER NAME	= JMarvlg
PLOT SCALE	= 1:20
PLOT DATE	= 11/5/2024

DESIGNED	-
DRAWN	-
CHECKED	-
DATE	-

REVISED	-
REVISED	-
REVISED	-
REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DUNDEE AVENUE
CROSS SECTIONS

SCALE: 1"=10'H, 5'V SHEET 32 OF 32 SHEETS STA. 130+70 TO STA. 130+77

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2525	20-00189-00-PV	KANE	184	184
CONTRACT NO. 61K68				
		ILLINOIS	FED. AID PROJECT	