

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

F.A.P. RITE	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
IL 501 921	(7R-VB)BRR	ROCK ISLAND	# 39	1
			CONTRACT NO. 64N10	
ILLINOIS REPAIR AND PROJECT				

* 39 - 1 = 38 TOTAL SHEETS
D-92-055-18

FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROPOSED HIGHWAY PLANS

FAP ROUTE 300 (IL-5 / IL 92)
SECTION (7R-VB) BRR
PROJECT STP-X39N(298)
BRIDGE REHABILITATION &
APPROACH ROADWAY RESURFACING
ROCK ISLAND COUNTY

C-92-082-19

BEGIN IMPROVEMENT
STA 631+06.33
END IMPROVEMENT
STA 638+10.39

BRIDGE REHABILITATION
SN 081-0036
CENTER STA 634+61.43



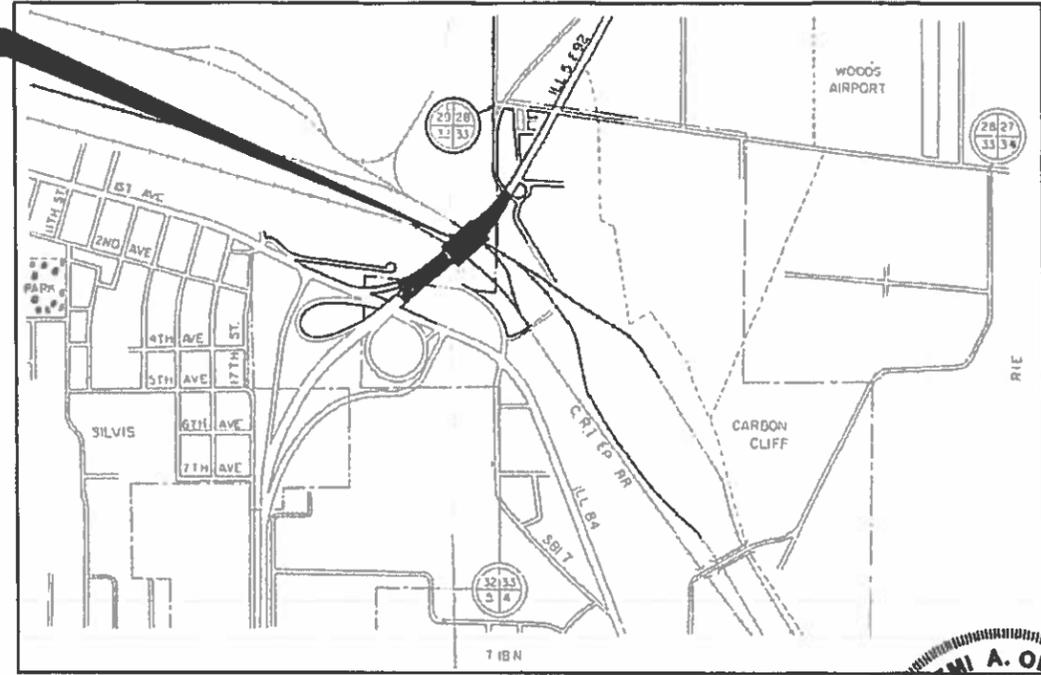
FUNCTIONAL CLASSIFICATION
OTHER PRINCIPAL ARTERIAL
AADT = 16400
P.V = 84% EST. TRUCK 16%
HAMPTON TOWNSHIP



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.L.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

CITY
OF
SILVIS



LOCATION MAP
(NOT TO SCALE)

GROSS LENGTH = 704.06 FT. (0.1333 MILE)
NET LENGTH = 704.06 FT. (0.1333 MILE)

PROJECT ENGINEER OLUFEMI OLADEINDE, P.E., S.E. (312) 551-9780
PROJECT MANAGER MAHMOUD ETEMADI, P.E. (815) 284-5359

CONTRACT NO. 64N10



Signed: *[Signature]* 07/19/2019
OLUFEMI A. OLADEINDE
LICENSE EXPIRES 11-30-2019
Date

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUBMITTED *October 20 19*
[Signature]
REGIONAL ENGINEER

Dec 16 19
[Signature]
ENGINEER OF DESIGN AND ENVIRONMENT

Dec 16 19
[Signature]
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION 3

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

INDEX OF SHEETS

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07	STAGING NOTES & CROSS SECTIONS
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14-30	BRIDGE PLANS
31-38	STANDARDS

GENERAL NOTES

- IN ADDITION TO THE REQUIREMENTS OF ARTICLE 107.16, THE CONTRACTOR SHALL PROTECT THE SURFACE OF ALL BRIDGE DECKS AND BRIDGE APPROACH PAVEMENTS IN A MANNER SATISFACTORY TO THE ENGINEER BEFORE ANY EQUIPMENT IS ALLOWED TO CROSS THE STRUCTURE. PROTECTION SHALL BE PROVIDED FOR ALL EQUIPMENT, AS DEFINED IN ARTICLE 101.17 IF TRACK MOUNTED OR WHEELED.
- THE AREA TO BE TACKED OR PRIMED SHALL BE LIMITED TO THAT WHICH CAN BE COVERED WITH HMA ON THE NEXT DAY'S PRODUCTION, BUT NO MORE THAN FIVE DAYS IN ADVANCE OF THE PLACEMENT OF THE HMA, UNLESS APPROVED BY THE ENGINEER.
- BITUMINOUS AND AGGREGATE PRIME COAT SHALL BE PLACED IN ACCORDANCE WITH SECTION 406 OF THE STANDARD SPECIFICATIONS. THE COST OF THE PRIME AND TACK COATS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER TON FOR LEVELING BINDER (MACHINE METHOD) OF THE TYPE SPECIFIED.
- BEFORE STARTING ANY EXCAVATION, THE CONTRACTOR SHALL CALL "JULIE." AT 800-892-0123 FOR FIELD LOCATIONS OF BURIED ELECTRIC, TELEPHONE, AND GAS FACILITIES. 48 HOUR NOTIFICATION IS REQUIRED.
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH THE UTILITY COMPANIES, AND THE CITY OF SILVIS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND PROTECTING UTILITY PROPERTY DURING CONSTRUCTION OPERATIONS AS OUTLINED IN ARTICLE 107.39 OF THE STANDARD SPECIFICATIONS. A MINIMUM OF 48 HOURS ADVANCE NOTICE IS REQUIRED FOR NON-EMERGENCY WORK. THE JULIE NUMBER IS 800-892-0123.

IDOT IS NOT A MEMBER OF JULIE. IF YOU ARE NEAR ANY OVERHEAD LIGHTING, INTERSECTION LIGHTING OR TRAFFIC SIGNALS, CONTACT THE IDOT TRAFFIC OFFICE AT 815/284-5469 AT LEAST 48 HOURS PRIOR TO WORK.

- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CONTACT THE MUNICIPALITY TO DETERMINE APPROVED METHODS OF UTILITY STRUCTURE ADJUSTMENT. UTILITY STRUCTURES MAY INCLUDE, BUT ARE NOT LIMITED TO, MANHOLES, WATER VALVES, HANDHOLES, ETC. ALL MATERIALS AND WORK NECESSARY TO COMPLETE ADJUSTMENTS PER MUNICIPALITY REQUIREMENTS SHALL BE CONSIDERED INCLUDED IN THE COST OF THE ASSOCIATED ADJUSTMENT PAY ITEM.
- ALL DAMAGE TO EXISTING PAVEMENT MARKING OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS SHALL BE REPLACED AT THE CONTRACTORS EXPENSE. NO ADDITIONAL COST TO THE DEPARTMENT.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCES, ALL EXISTING PAVEMENT MARKING LINES AND RAISED REFLECTIVE PAVEMENT MARKERS IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL STRIPING SHALL BE AS DIRECTED BY THE ENGINEER.
- PAVEMENT MARKING SHALL BE DONE ACCORDING TO STANDARD 780001, EXCEPT AS FOLLOWS:
 - ALL WORDS, SUCH AS ONLY, SHALL BE 8 FEET HIGH.
 - ALL NON-FREEWAY ARROWS SHALL BE THE LARGE SIZE.
 - THE DISTANCE BETWEEN YELLOW NO-PASSING LINES SHALL BE 8 INCHES, NOT 7 INCHES, AS SHOWN IN THE DETAIL OF TYPICAL LANE AND EDGE LINES.
 - CENTERLINE SKIP DASH PAVEMENT MARKING ON MULTI-LANE DIVIDED, MULTI-LANE UNDIVIDED, AND ONE-WAY ROADWAY SHALL BE ACCORDING TO DISTRICT STANDARD 41.1.
- FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
- DRAINAGE ADJUSTMENT OR RECONSTRUCTION LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING MATERIALS.

- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
- THE FINAL TOP FOUR INCHES OF SOIL IN ANY RIGHT-OF-WAY AREA DISTURBED BY THE CONTRACTOR MUST BE CAPABLE OF SUPPORTING VEGETATION. THE SOIL MUST BE FROM A HORIZON (ZERO TO 2' DEEP) OF SOIL PROFILES OF LOCAL SOILS. THE COST OF THIS WORK SHALL BE INCLUDED IN THE UNIT PRICES BID AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- THE CONTRACTOR SHALL REMOVE AND DISPOSE OF ALL LOGS, SHRUBS, BUSHES, SAPLINGS, UNDERBRUSH OR DEBRIS ACCORDING TO SECTION 201 OF THE STANDARD SPECIFICATIONS AT LOCATIONS REQUIRING ACCESS TO THE SUBSTRUCTURE. THIS WORK WILL NOT BE PAID FOR SEPARATELY, BUT THE COST SHALL BE CONSIDERED AS INCLUDED IN THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
- ALL BORROW/WASTE/USE SITES MUST BE APPROVED BY THE DEPARTMENT PRIOR TO REMOVING ANY MATERIAL FROM THE PROJECT OR INITIATING ANY EARTHMOVING ACTIVITIES, INCLUDING TEMPORARY STOCKPILING OUTSIDE THE LIMITS OF CONSTRUCTION.
- THE REMOVAL OF GUARDRAIL TERMINAL SECTIONS SHALL BE INCLUDED IN THE UNIT PRICE PER FOOT FOR "GUARDRAIL REMOVAL".
- THE CONTRACTOR SHALL VERIFY THE EXISTING TYPE/HEIGHT OF EXISTING GUARDRAIL BEFORE ORDERING THE NEW TERMINAL SECTION. COST INCLUDED WITH THE COST OF THE TERMINAL THE TERMINAL SECTION SHALL MATCH THE HEIGHT OF THE EXISTING GUARDRAIL.
- THE CONTRACTOR SHALL SUPPLY THE RESIDENT ENGINEER WITH THE MANUFACTURER'S INSTALLATION REQUIREMENTS FOR THE TYPE OF STEEL PLATE BEAM GUARDRAIL TERMINAL TYPE I SPECIAL (TANGENT) OR STEEL PLATE BEAM GUARDRAIL TERMINAL TYPE I SPECIAL (FLARED).
- WHEN RELOCATE TEMPORARY CONCRETE BARRIER IS SPECIFIED, THE WALL SHALL BE REMOVED, STORED AND TRANSPORTED TO AND FROM STORAGE, WHEN THE WALL IS NOT NEEDED FOR A TIME AS SHOWN ON THE STAGING PLANS, AND RELOCATED AND REINSTATED AT THE NEW LOCATION. THE REINSTALLATION REQUIREMENTS SHALL BE THE SAME AS THOSE FOR A NEW INSTALLATION. THIS SHALL BE PAID FOR AT THE CONTRACT UNIT PRICE PER FOOT FOR RELOCATE TEMPORARY CONCRETE BARRIER.
- RELOCATE TEMPORARY IMPACT ATTENUATORS SHALL INCLUDE STORAGE AND TRANSPORTATION TO AND FROM STORAGE, WHEN THE DEVICE IS NOT NEEDED FOR A TIME, AS SHOWN ON THE STAGING PLANS. THIS SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE PER EACH FOR IMPACT ATTENUATORS, RELOCATE OF THE TYPE SPECIFIED.

HIGHWAY STANDARDS

000001-07	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
482011-03	HMA SHLD. STRIPS/SHLDS. WITH RESURFACING OR WIDENING AND RESURFACING PROJECTS
630001-12	STEEL PLATE BEAM GUARDRAIL
630201-07	PCC / HMA STABILIZATION AT STEEL PLATE BEAM GUARDRAIL
631031-16	TRAFFIC BARRIER, TYPE 6
642006	SHOULDER RUBMLE STRIPS, 8 INCH
701101-05	OFF-ROAD OPERATIONS, MULTILANE, 15' (4.5 M) TO 24" (600 MM) FROM PAVEMENT EDGE
701411-09	LANE CLOSURE, MULTILANE, AT ENTRANCE, FOR SPEEDS ≥ 45 MPH
701423-10	LANE CLOSURE, MULTILANE, WITH BARRIER, FOR SPEEDS ≥ 45 MPH TO 55 MPH
701426-09	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPERATION, FOR SPEEDS ≥ 45 MPH
701901-08	TRAFFIC CONTROL DEVICES
704001-08	TEMPORARY CONCRETE BARRIER
720011-01	METAL POSTS FOR SIGNS, MARKERS & DELINEATORS
728001-01	TELESCOPING STEEL SIGN SUPPORT
729001-01	APPLICATIONS OF TYPES A & B METAL POSTS (FOR SIGN & MARKERS)
780001-05	TYPICAL PAVEMENT MARKINGS
782006-01	GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS
781001-04	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
782006-01	GUARDRAIL AND BARRIER WALL REFLECTOR MOUNTING DETAILS

UTILITY NOTES

NONE.

HMA MIXTURE REQUIREMENT TABLE

LOCATION(S):	HMA MIXTURE REQUIREMENT TABLE	
MIXTURE USE(S):	HMA SURFACE COURSE	HMA LEVELING BINDER (MACHINE METHOD)
BINDER GRADE (PG):	64-22	64-22
DESIGN AIR VOIDS:	4.0% @ N70	4.0% @ N70
MIXTURE COMPOSITION: (MIXTURE GRADATION)	IL 9.5	IL 9.5 FG
FRICTION AGGREGATE:	D	N/A
MIXTURE WEIGHT:	112.0 LB/SY/IN	-
QUALITY MANAGEMENT PROGRAM:	QC/QA	QC/QA
SUBLOT SIZE:	-	-
NUMBER OF ROLLER PASSES	-	-

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PLOT DATE = 9/18/2019	DATE - 9/18/2019	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL NOTES AND HIGHWAY STANDARDS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
300	(7R-VB)BRR	ROCK ISLAND	38	02
CONTRACT NO. 64N10			ILLINOIS FED. AID PROJECT	

SUMMARY OF QUANTITIES				80% FED 20% STATE	CONSTRUCTION CODE	
CODE NO.	ITEM	UNIT	TOTAL QUANTITY			
				BRIDGE		
				0047	081-0036	
20600200	GRANULAR EMBANKMENT, SPECIAL	CU YD	18.6	18.6		
28100105	STONE RIPRAP, CLASS A3	SQ YD	35	35		
28200200	FILTER FABRIC	SQ YD	35	35		
40600635	LEVELING BINDER (MACHINE METHOD), N70	TON	45	45		
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	260	260		
40603340	HOT-MIX ASPHALT SURFACE COURSE, MIX "D", N70	TON	65	65		
44000153	HOT-MIX ASPHALT SURFACE REMOVAL, 1"	SQ YD	510	510		
50102400	CONCRETE REMOVAL	CU YD	43	43		
50157300	PROTECTIVE SHIELD	SQ YD	1208	1208		
50300255	CONCRETE SUPERSTRUCTURE	CU YD	48.3	48.3		
50300300	PROTECTIVE COAT	SQ YD	4840	4840		
* Z0010501	CLEANING AND PAINTING STEEL BRIDGE NO. 1	L SUM	1	1		
50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	5300	5300		
50800515	BAR SPLICERS	EACH	96	96		
52000110	PREFORMED JOINT STRIP SEAL	FOOT	268	268		
* 63000001	STEEL PLATE BEAM GUARDRAIL, TYPE A, 6 FOOT POSTS	FOOT	750	750		
* 63100085	TRAFFIC BARRIER TERMINAL, TYPE 6	EACH	3	3		
* 63100169	TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) FLARED	EACH	3	3		
63200310	GUARDRAIL REMOVAL	FOOT	976	976		
64200108	SHOULDER RUMBLE STRIPS, 8 INCH	FOOT	90	90		

*= SPECIALTY ITEM

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 DRAWN - ZP
 CHECKED - OAO
 DATE - #DATE#

REVISED -
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 300	SECTION (7R-VB)BRR	COUNTY ROCK ISLAND	TOTAL SHEETS 38	SHEET NO. 03
CONTRACT NO. 64N10				
ILLINOIS FED. AID PROJECT				

SUMMARY OF QUANTITIES				CONSTRUCTION CODE	
CODE NO.	ITEM	UNIT	TOTAL QUANTITY	80% FED 20% STATE	
				100% STATE	
				BRIDGE	
				0047	
				081-0036	
67100100	MOBILIZATION	L SUM	1	1	
70100325	TRAFFIC CONTROL AND PROTECTION, STANDARD 701423	EACH	1	1	
70100420	TRAFFIC CONTROL AND PROTECTION, STANDARD 701411	EACH	2	2	
70300904	PAVEMENT MARKING TAPE, TYPE IV 4"	FOOT	12000	12000	
70400100	TEMPORARY CONCRETE BARRIER	FOOT	1700	1700	
70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	1700	1700	
70600250	IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE), TEST LEVEL 3	EACH	2	2	
70600350	IMPACT ATTENUATORS, RELOCATE (NON-REDIRECTIVE), TEST LEVEL 3	EACH	2	2	
* 78001110	PAINT PAVEMENT MARKING - LINE 4"	FOOT	2352	2352	
* 78001130	PAINT PAVEMENT MARKING - LINE 6"	FOOT	1161	1161	
* 78001140	PAINT PAVEMENT MARKING - LINE 8"	FOOT	830	830	
* 78001150	PAINT PAVEMENT MARKING - LINE 12"	FOOT	187	187	
* 78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	2	2	
Z0001700	APPROACH SLAB REPAIR (FULL DEPTH)	SQ YD	159	159	
Z0006014	BRIDGE DECK LATEX CONCRETE OVERLAY, 2 1/2 INCHES	SQ YD	4015	4015	
* X5060601	CONTAINMENT AND DISPOSAL OF NON-LEAD PAINT CLEANING RESIDUES NO. 1	L SUM	1	1	
Z0012130	BRIDGE DECK SCARIFICATION 3/4"	SQ YD	4015	4015	
Z0012754	STRUCTURAL REPAIR OF CONCRETE (DEPTH EQUAL TO OR LESS THAN 5 INCHES)	SQ FT	1129	1129	
Z0012755	STRUCTURAL REPAIR OF CONCRETE (DEPTH GREATER THAN 5 INCHES)	SQ FT	108	108	
Z0016001	DECK SLAB REPAIR (FULL DEPTH, TYPE I)	SQ YD	33	33	

*= SPECIALTY ITEM

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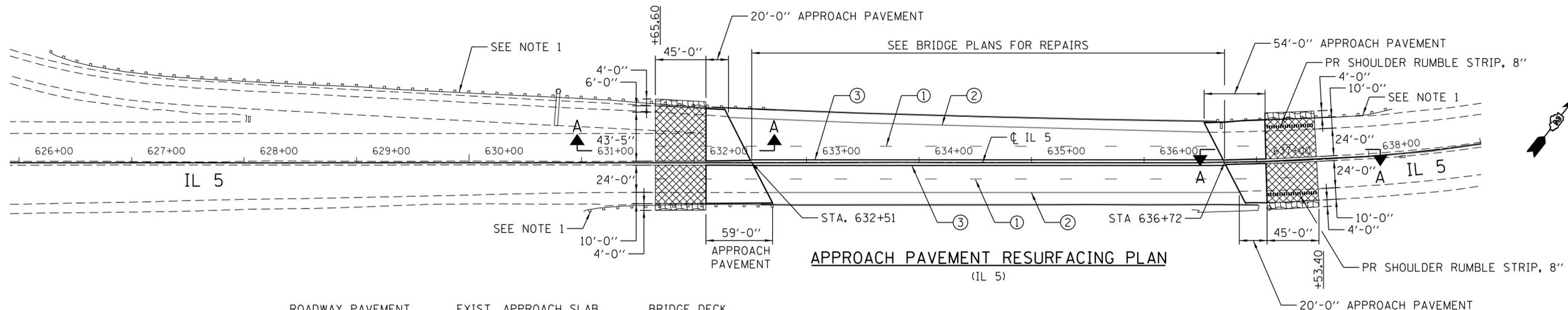
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STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION

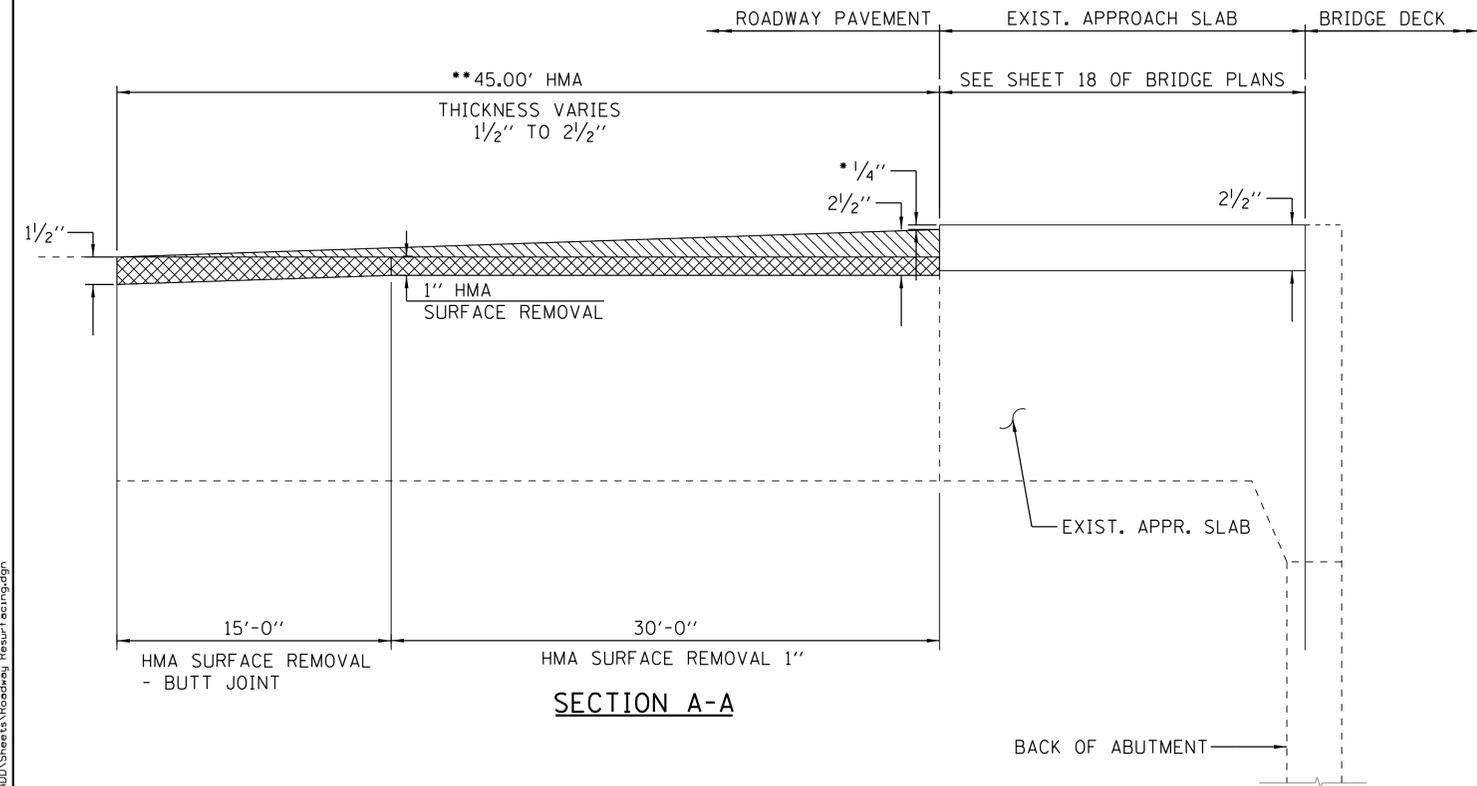
SUMMARY OF QUANTITIES

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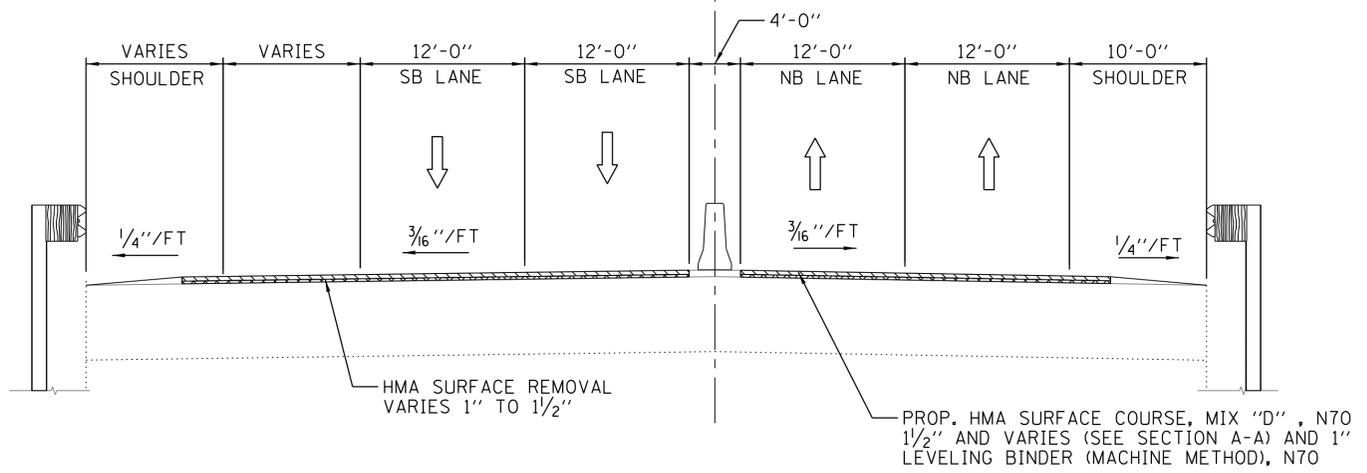
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
300	(7R-VB)BRR	ROCK ISLAND	38	04
CONTRACT NO. 64N10				



APPROACH PAVEMENT RESURFACING PLAN
(IL 5)



SECTION A-A



TYPICAL ROADWAY RESURFACING SECTION
(LOOKING NORTH)

LEGEND

- HMA SURFACE COURSE, MIX "D", N70
- HMA SURFACE REMOVAL
- HMA SURFACE REMOVAL & HMA REPLACEMENT
- SEEDING, CLASS 2A, NITROGEN FERTILIZER NUTRIENT, PHOSPHORUS FERTILIZER NUTRIENT, POTASSIUM FERTILIZER NUTRIENT, EROSION CONTROL BLANKET, TEMPORARY EROSION CONTROL SEEDING
- ① 6" WHITE LANE (30' SKIP 10' DASH)
- ② 4" WHITE SOLID LINE
- ③ 4" YELLOW SOLID LINE

NOTES

1. REMOVE EXIST. G.R. AS REQ'D TO INSTALL NEW:
TRAFFIC BARRIER TERMINAL, TYPE 6
STEEL PLATE BEAM GUARDRAIL, TYPE A
TRAFFIC BARRIER TERMINAL, TYPE 1 (SPECIAL) FLARED
2. THE PROPOSED PAVEMENT MARKINGS SHALL BE PLACED IN SAME LOCATION AS EXISTING PAVEMENT MARKINGS OR AS DIRECTED BY THE ENGINEER USING 2 COATS OF PAINT PAVEMENT MARKING.

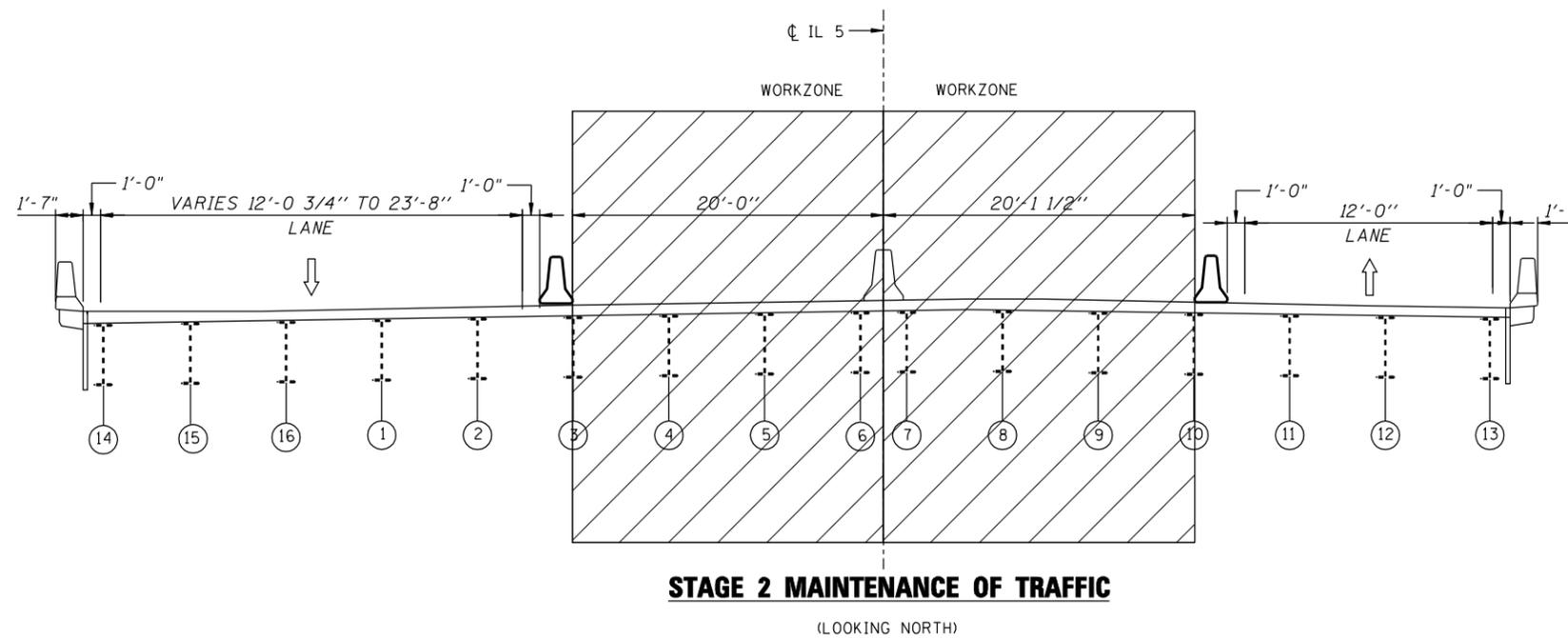
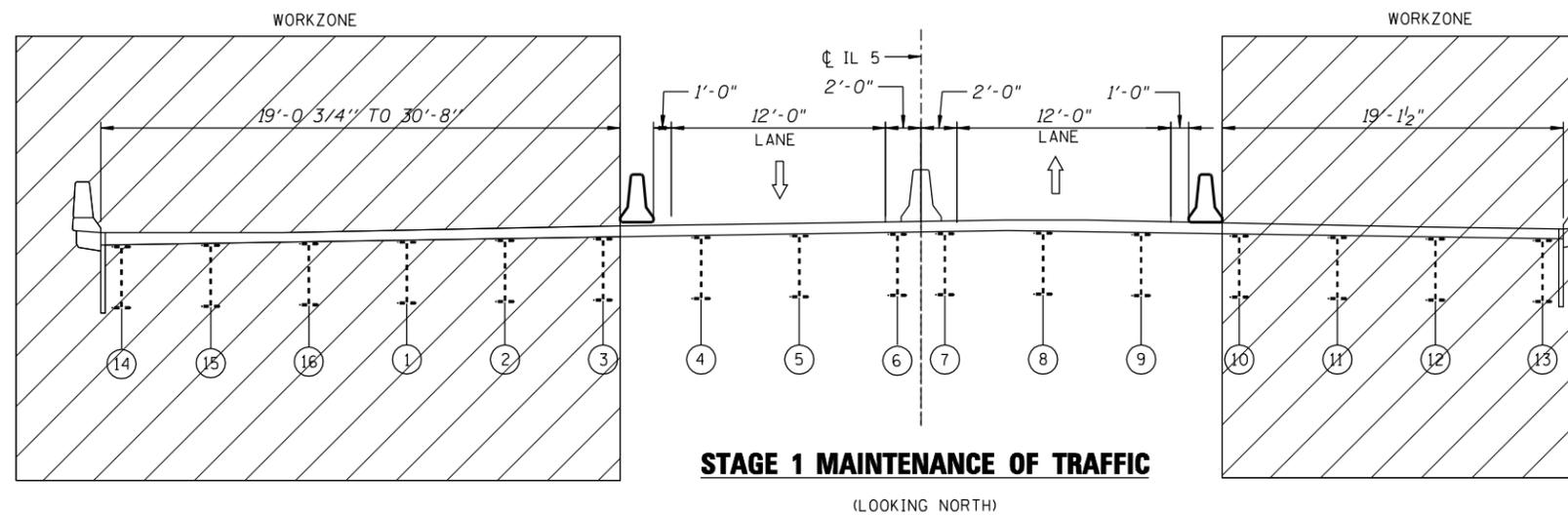
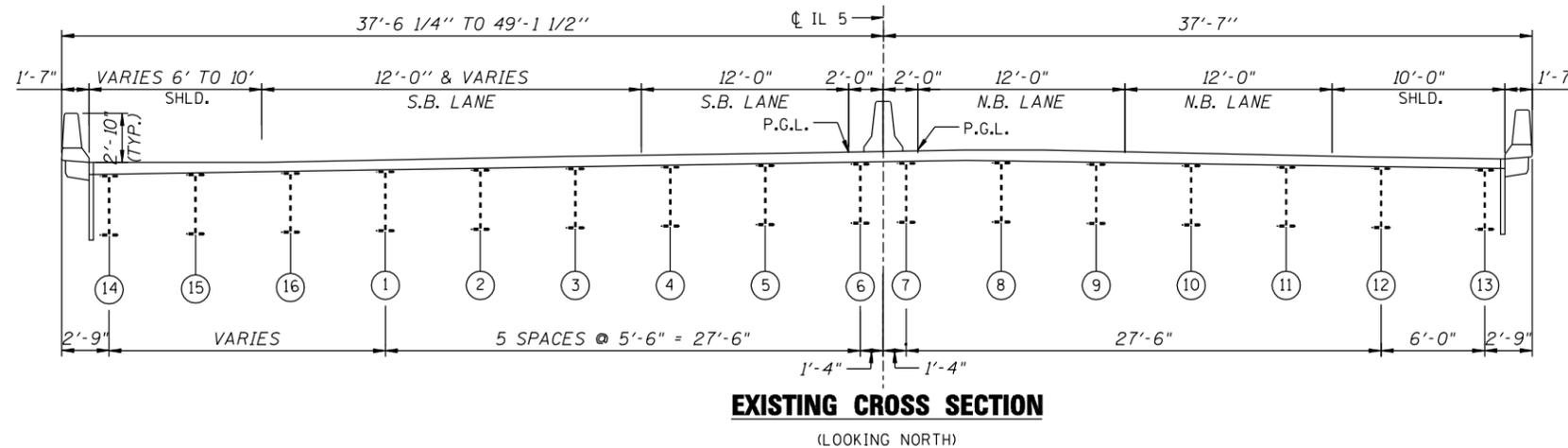
- * PRIOR TO 1/4" DIAMOND GRINDING (BRIDGE SECTION)
- ** HMA SURFACE COURSE, MIX "D", N70 1 1/2" AND VARIES, AND 1" LEVELING BINDER (MACHINE METHOD), N70
- *** ALL DISTURBED AREAS SHALL BE SEEDED, FERTILIZED AND BLANKETED, AND SHALL NOT BE PAID SEPARATELY BUT INCLUDED IN THE COST OF HMA SURFACE COURSE, MIX D, N70.

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WYNDALCO ENTERPRISES 19081 Old LaGrange Rd, Ste. 106 Mokena, IL 60448 312.256.9090	USER NAME = Yun Kim	DESIGNED - YK	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	APPROACH PAVEMENT PLAN AND SECTIONS			F.A.P. RTE. = 300	SECTION = (7R-VB)BRR	COUNTY = ROCK ISLAND	TOTAL SHEETS = 38	SHEET NO. = 06
	PLOT SCALE = 50.0000' / 1"	CHECKED - YK	REVISED -					SCALE: N.T.S.	SHEET OF SHEETS	STA. TO STA.	CONTRACT NO. 64N10	
	PLOT DATE = 8/13/2019	DATE = 7/8/2019	REVISED -									

CONSTRUCTION STAGING GENERAL NOTES

- ALL SIGNING MUST BE IN ACCORDANCE WITH THE APPLICABLE PROVISIONS OF THE STATE OF ILLINOIS "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED APRIL 1 2016. THE DETAILS IN THESE PLANS, THE LATEST EDITION OF THE IDOT BUREAU OF DESIGN AND ENVIRONMENT HIGHWAY STANDARDS AND THE LATEST EDITION OF THE "MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES".
- LONGITUDINAL DIMENSIONS SHOWN ON THESE PLANS MAY BE ADJUSTED TO FIT FIELD CONDITIONS AS DIRECTED BY THE ENGINEER.
- THE CONTRACTOR MUST BE RESPONSIBLE FOR ENSURING THAT ALL BARRICADES, SIGNS, LIGHTS AND OTHER DEVICES INSTALLED ARE IN PLACE AND OPERATING 24 HOURS EACH DAY INCLUDING SUNDAYS AND HOLIDAYS DURING THE TIME THIS CONSTRUCTION IS IN EFFECT.
- ALL EXISTING SIGNING THAT IS NOT APPLICABLE WHILE THE CONSTRUCTION IS IN EFFECT MUST BE COMPLETELY COVERED BY THE CONTRACTOR.
- THE SIZES OF ALL SIGNS NOT SPECIFIED IN THESE PLANS MUST BE AS REQUIRED BY THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.
- AS A MINIMUM, ALL AMBER FLASHING LIGHTS THAT ARE REQUIRED MUST MEET THE REQUIREMENTS FOR TYPE A - LOW INTENSITY FLASHING LIGHTS IN ARTICLE 702.04 OF THE STANDARD SPECIFICATIONS. ALL LIGHTS SHALL OPERATE DURING HOURS OF DARKNESS. ONLY LIGHTS THAT HAVE BEEN APPROVED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION MUST BE USED.
- PROPOSED MAINTENANCE OF TRAFFIC SIGNING MUST BE COVERED OR REMOVED WHEN NOT REQUIRED DURING A SPECIFIC STAGE OF CONSTRUCTION.
- SEE SUGGESTED MAINTENANCE OF TRAFFIC PLAN FOR ADDITIONAL SIGNING.
- CHANGEABLE MESSAGE SIGNS TO BE PROVIDED AT LOCATIONS SHOWN ON PLANS OR AS DIRECTED BY THE ENGINEER.
- THE CONTRACTOR MUST CONDUCT HIS WORK IN SUCH A MANNER THAT EMERGENCY VEHICLES WILL HAVE ACCESS TO THE AREA AT ALL TIMES.
- THE CONTRACTOR MUST NOTIFY THE IDOT BUREAU OF TRAFFIC AS REQUIRED 72 HOURS IN ADVANCE OF BEGINNING WORK (815) 284-5474.
- SIGN SPACING SHALL BE PER IDOT STANDARDS 701411-09, 701423-10, 701456-05.
- THE CONTRACTOR SHALL PROVIDE ADEQUATE TEMPORARY DRAINAGE DURING ALL PHASES OF CONSTRUCTION.
- THE CONTRACTOR SHALL PROVIDE, INSTALL, MAINTAIN AND REMOVE ALL SIGNS AND SIGN SUPPORTS REQUIRED FOR TRAFFIC CONTROL AND PROTECTION.
- ALL TEMPORARY CONCRETE BARRIER WALL AND TEMPORARY IMPACT ATTENUATORS MUST BE SEATED ON BARE, CLEAN PAVEMENT OR PAVED SHOULDER.



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PLOT DATE = 9/18/2019	DATE - 9/18/2019	REVISIONS -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

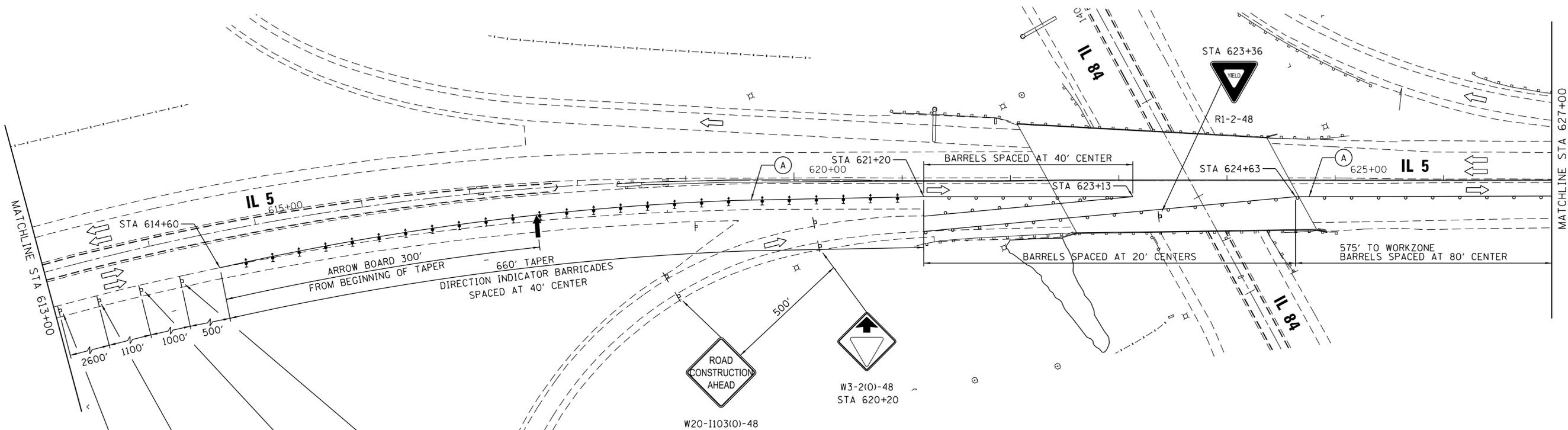
STAGING NOTES & CROSS SECTIONS

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 300	SECTION (7R-VBIBRR)	COUNTY ROCK ISLAND	TOTAL SHEETS 38	SHEET NO. 07
CONTRACT NO. 64N10				
ILLINOIS FED. AID PROJECT				

LEGEND

-  - WORK ZONE
-  - TEMPORARY CONCRETE BARRIER WALL
-  - TEMPORARY PAVEMENT MARKING
-  - DRUM WITH STEADY BURNING BI-DIRECTIONAL LIGHT
-  - DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
-  - TYPE III BARRICADE
-  - ARROW BOARD
-  - IMPACT ATTENUATOR TEST LEVEL 3
-  - TRAFFIC DIRECTION
-  - PAVEMENT MARKING TAPE, TYPE IV 4"
-  - TEMPORARY TRAFFIC SIGN



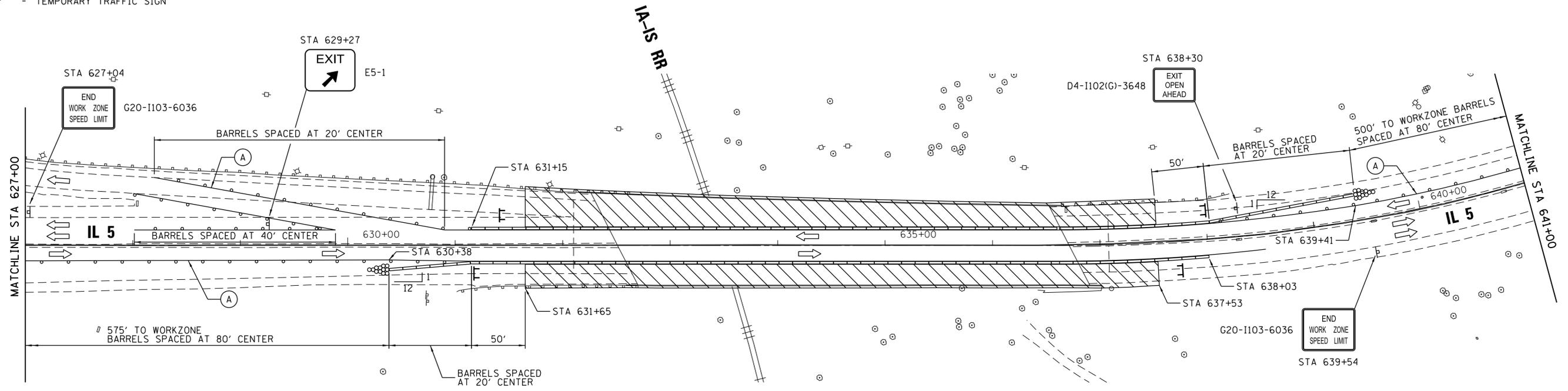
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	 R2-1-3648		
	 R10-1108P-3618		
	 R2-1106P-3618		

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 19081 Old LaGrange Rd, Ste. 106 Mokena, IL 60448 312.256.9090	USER NAME = Yun Kim PLOT SCALE = 50.0000' / in. PLOT DATE = 7/8/2019	DESIGNED - YK DRAWN - SP CHECKED - YK DATE - 7/8/2019	REVISED - REVISED - REVISED - REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	MAINTENANCE OF TRAFFIC STAGE 1 - STA 613+00 TO STA 627+00	F.A.P. RTE. 300 SECTION (7R-VB)BRR COUNTY ROCK ISLAND TOTAL SHEETS 38 SHEET NO. 08 CONTRACT NO. 64N10	SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.	ILLINOIS FED. AID PROJECT
	ILLINOIS FED. AID PROJECT							

LEGEND

-  - WORK ZONE
-  - TEMPORARY CONCRETE BARRIER WALL
-  - TEMPORARY PAVEMENT MARKING
-  - DRUM WITH STEADY BURNING BI-DIRECTIONAL LIGHT
-  - DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
-  - TYPE III BARRICADE
-  - ARROW BOARD
-  - IMPACT ATTENUATOR TEST LEVEL 3
-  - TRAFFIC DIRECTION
-  - PAVEMENT MARKING TAPE, TYPE IV 4"
-  - TEMPORARY TRAFFIC SIGN



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ENTERPRISES
19081 Old LaGrange Rd, Ste. 106
Mokena, IL 60448
312.256.9090

USER NAME = Yun Kim	DESIGNED - YK	REVISED -
	DRAWN - SP	REVISED -
PLOT SCALE = 50.0000' / in.	CHECKED - YK	REVISED -
PLOT DATE = 7/8/2019	DATE - 7/8/2019	REVISED -

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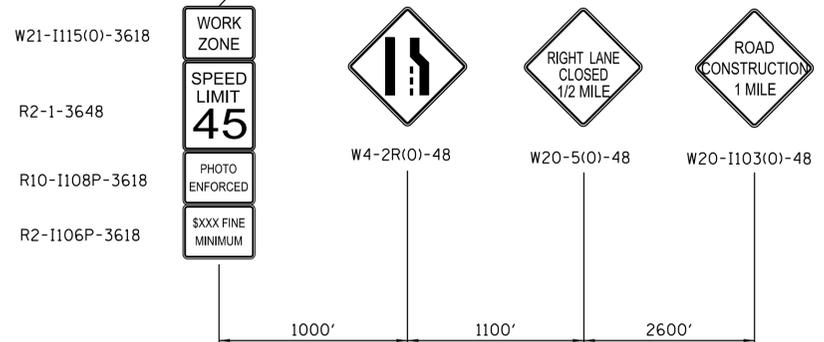
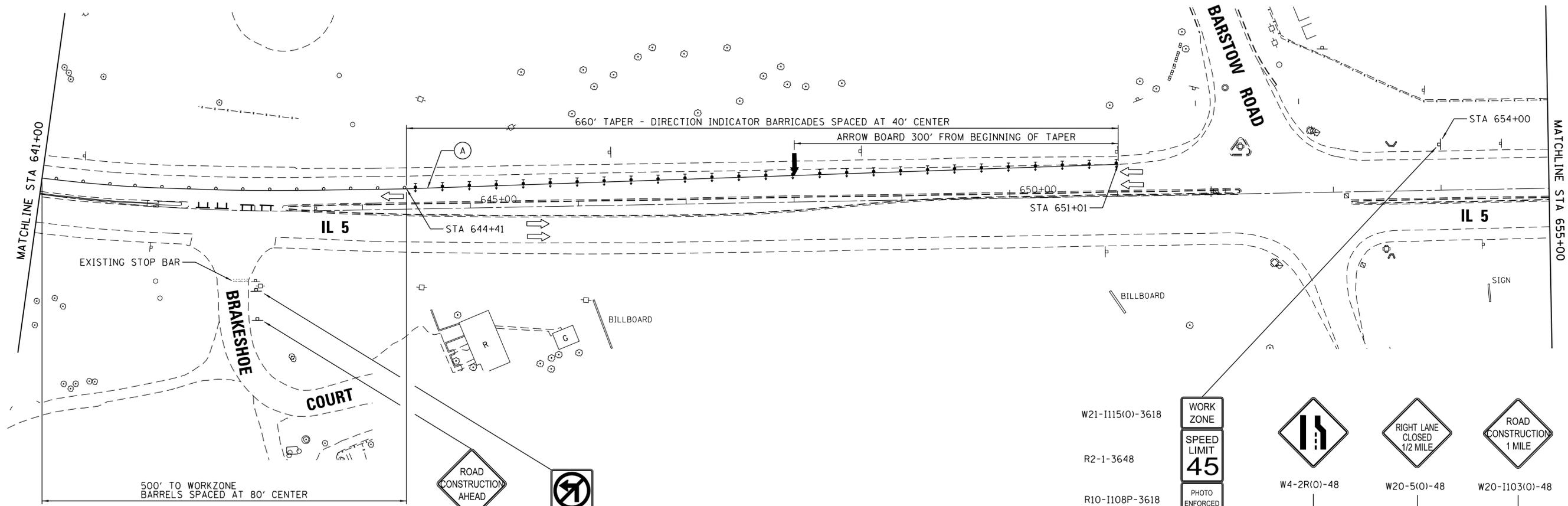
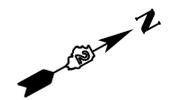
MAINTENANCE OF TRAFFIC
STAGE 1 - STA 627+00 TO STA 641+00

SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
300	(7R-VB)BRR	ROCK ISLAND	38	09
CONTRACT NO. 64N10				
ILLINOIS FED. AID PROJECT				

LEGEND

-  - WORK ZONE
-  - TEMPORARY CONCRETE BARRIER WALL
-  - TEMPORARY PAVEMENT MARKING
-  - DRUM WITH STEADY BURNING BI-DIRECTIONAL LIGHT
-  - DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
-  - TYPE III BARRICADE
-  - ARROW BOARD
-  - IMPACT ATTENUATOR TEST LEVEL 3
-  - TRAFFIC DIRECTION
-  - PAVEMENT MARKING TAPE, TYPE IV 4"
-  - TEMPORARY TRAFFIC SIGN



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USER NAME = Yun Kim	DESIGNED - YK	REVISED -
	DRAWN - SP	REVISED -
PLOT SCALE = 50.0000' / 1"	CHECKED - YK	REVISED -
PLOT DATE = 7/8/2019	DATE - 7/8/2019	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

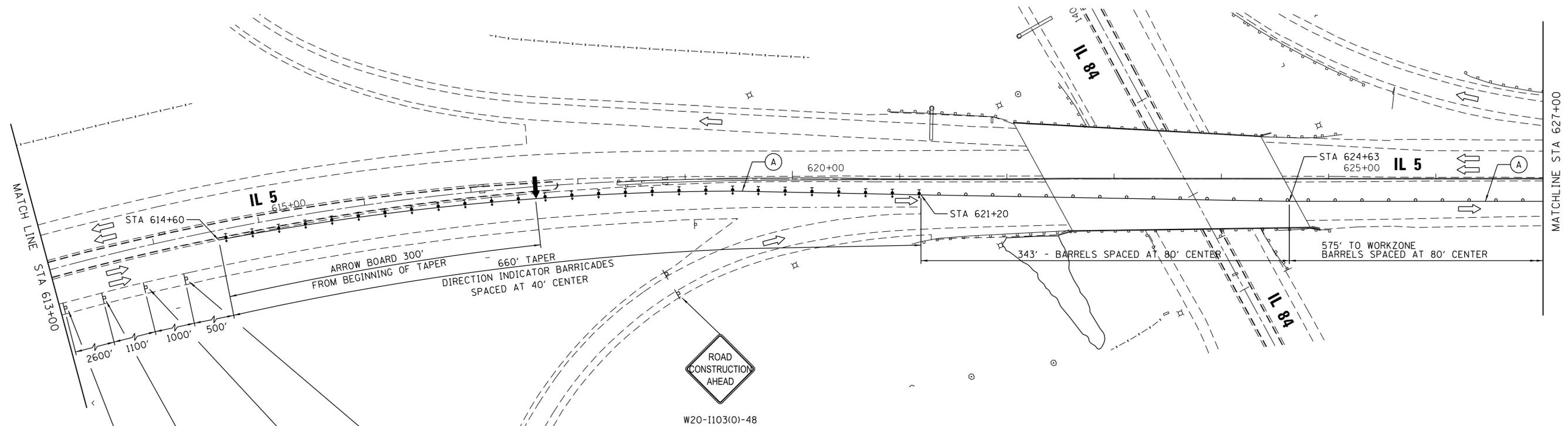
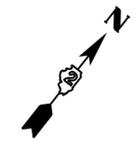
**MAINTENANCE OF TRAFFIC
STAGE 1 - STA 641 + 00 TO STA 655 + 00**

F.A.P. RTE. 300	SECTION (7R-VB)BRR	COUNTY ROCK ISLAND	TOTAL SHEETS 38	SHEET NO. 10
CONTRACT NO. 64N10			ILLINOIS FED. AID PROJECT	

SCALE: SHEET OF SHEETS STA. TO STA.

LEGEND

-  - WORK ZONE
-  - TEMPORARY CONCRETE BARRIER WALL
-  - TEMPORARY PAVEMENT MARKING
-  - DRUM WITH STEADY BURNING BI-DIRECTIONAL LIGHT
-  - DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
-  - TYPE III BARRICADE
-  - ARROW BOARD
-  - IMPACT ATTENUATOR TEST LEVEL 3
-  - TRAFFIC DIRECTION
-  - PAVEMENT MARKING TAPE, TYPE IV 4"
-  - TEMPORARY TRAFFIC SIGN



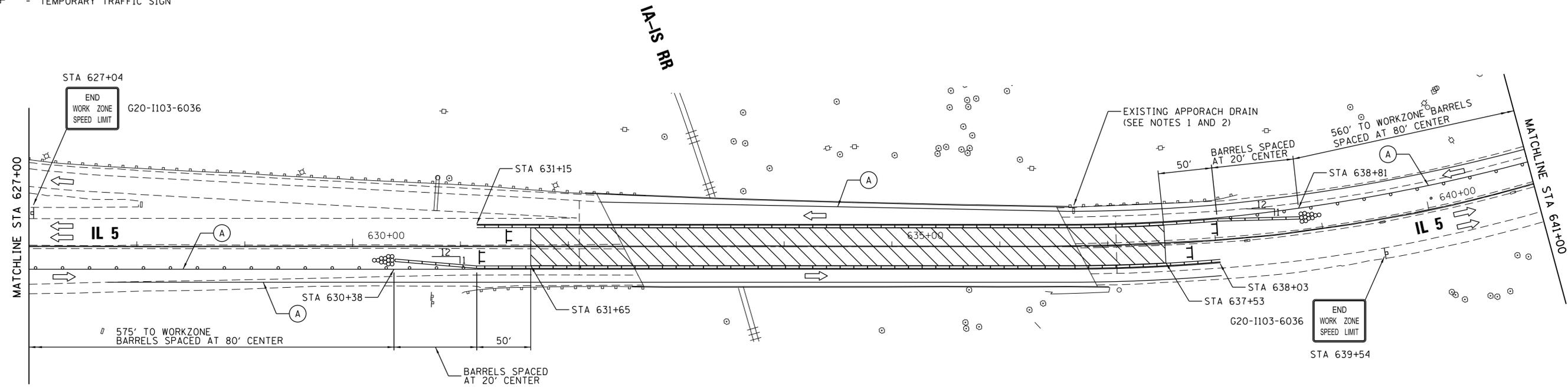
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<table border="1" style="margin: auto; border-collapse: collapse;"> <tr><td style="padding: 2px;">WORK ZONE</td><td style="padding: 2px;">W21-1115(0)-3618</td></tr> <tr><td style="padding: 2px;">SPEED LIMIT</td><td style="padding: 2px;">R2-1-3648</td></tr> <tr><td style="padding: 2px; font-size: 1.2em; font-weight: bold;">45</td><td style="padding: 2px;"></td></tr> <tr><td style="padding: 2px;">PHOTO ENFORCED</td><td style="padding: 2px;">R10-1108P-3618</td></tr> <tr><td style="padding: 2px;">\$XXX FINE MINIMUM</td><td style="padding: 2px;">R2-1106P-3618</td></tr> </table>			WORK ZONE	W21-1115(0)-3618	SPEED LIMIT	R2-1-3648	45		PHOTO ENFORCED	R10-1108P-3618	\$XXX FINE MINIMUM	R2-1106P-3618
WORK ZONE	W21-1115(0)-3618											
SPEED LIMIT	R2-1-3648											
45												
PHOTO ENFORCED	R10-1108P-3618											
\$XXX FINE MINIMUM	R2-1106P-3618											

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	PLOT SCALE = 50.0000' / in.	CHECKED - YK	REVISED -			300	(7R-VB)BRR	ROCK ISLAND	38	11
PLOT DATE = 7/8/2019	DATE = 7/8/2019	REVISED -		SCALE: 1" = 50'		SHEET OF SHEETS		STA. TO STA.		ILLINOIS FED. AID PROJECT

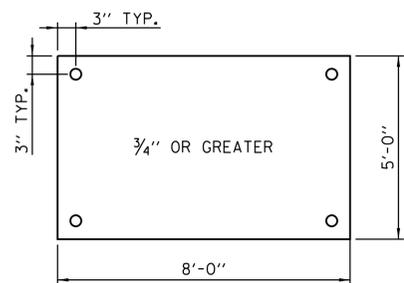
LEGEND

-  - WORK ZONE
-  - TEMPORARY CONCRETE BARRIER WALL
-  - TEMPORARY PAVEMENT MARKING
-  - DRUM WITH STEADY BURNING BI-DIRECTIONAL LIGHT
-  - DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
-  - TYPE III BARRICADE
-  - ARROW BOARD
-  - IMPACT ATTENUATOR TEST LEVEL 3
-  - TRAFFIC DIRECTION
-  - PAVEMENT MARKING TAPE, TYPE IV 4"
-  - TEMPORARY TRAFFIC SIGN



NOTES

1. DURING MOT STAGE 2, THE APPROACH DRAIN AT THE NORTHWEST CORNER OF THE BRIDGE NEEDS TO BE PROTECTED WITH 3/4" (MIN.) STEEL PLATE ANCHORED WITH 4 ANCHOR BOLTS INTO THE PAVEMENT.
2. THE EDGES OF THE STEEL PLATE SHALL BE TAPERED WITH HMA TO AVOID BUMP FOR TRAFFIC.



STEEL PLATE DETAIL
N.T.S.

FILE NAME = G:\Shared drives\Projects\2018-0000\IDOT D2 Var Ph2\1.0 Work Order\CADD\Sheets\Staging-2-ah-02.dgn

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19081 Old LaGrange Rd, Ste. 106
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USER NAME = Yun Kim	DESIGNED - YK	REVISED -
PLOT SCALE = 50.0000' / 1"	DRAWN - SP	REVISED -
PLOT DATE = 7/8/2019	CHECKED - YK	REVISED -
	DATE - 7/8/2019	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

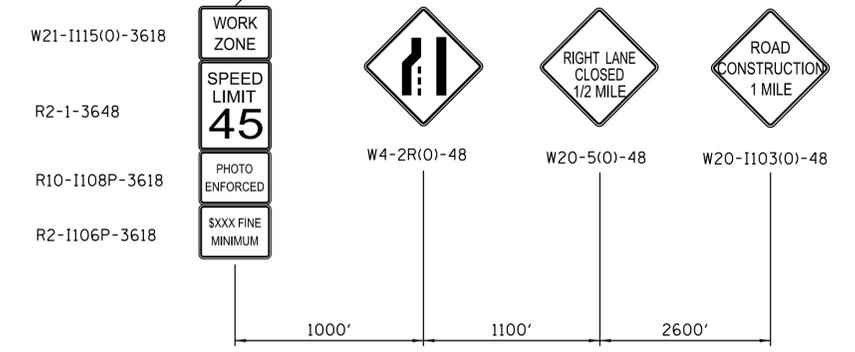
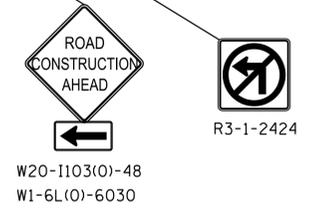
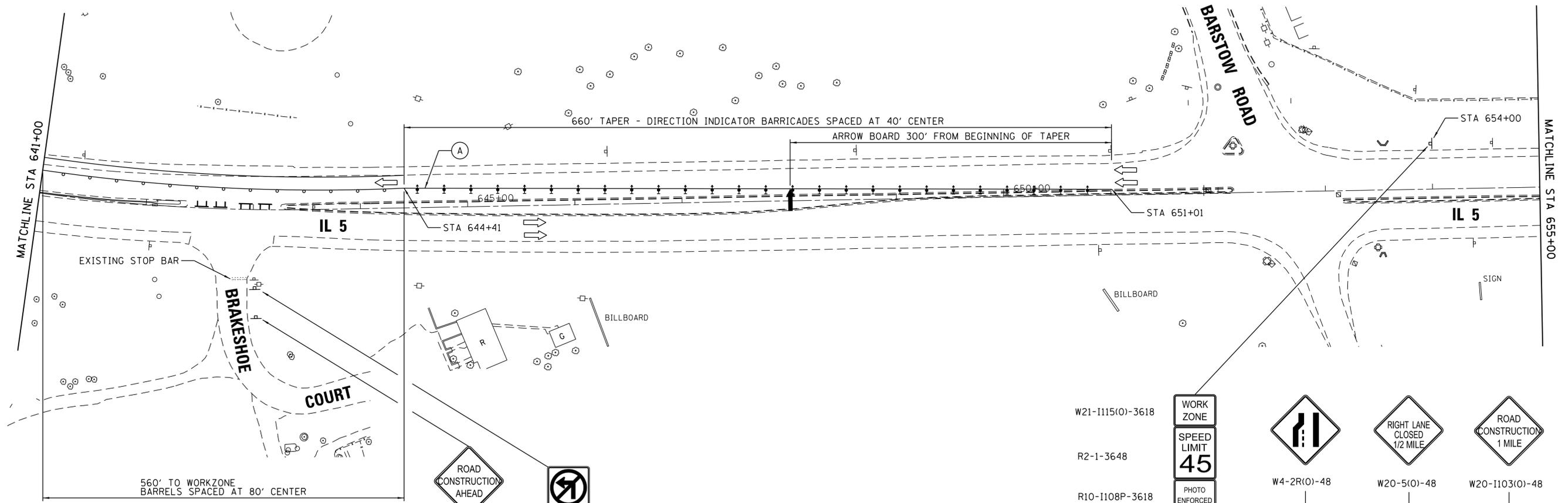
MAINTENANCE OF TRAFFIC
STAGE 2 - STA 627+00 TO STA 641+00

F.A.P. RTE. 300	SECTION (7R-VB)BRR	COUNTY ROCK ISLAND	TOTAL SHEETS 38	SHEET NO. 12
CONTRACT NO. 64N10				
ILLINOIS FED. AID PROJECT				

SCALE: 1" = 50' SHEET OF SHEETS STA. TO STA.

LEGEND

-  - WORK ZONE
-  - TEMPORARY CONCRETE BARRIER WALL
-  - TEMPORARY PAVEMENT MARKING
-  - DRUM WITH STEADY BURNING BI-DIRECTIONAL LIGHT
-  - DIRECTION INDICATOR BARRICADE WITH STEADY BURN MONODIRECTIONAL LIGHT
-  - TYPE III BARRICADE
-  - ARROW BOARD
-  - IMPACT ATTENUATOR TEST LEVEL 3
-  - TRAFFIC DIRECTION
-  - PAVEMENT MARKING TAPE, TYPE IV 4"
-  - TEMPORARY TRAFFIC SIGN



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Mokena, IL 60448
312.256.9090

USER NAME = Yun Kim	DESIGNED - YK	REVISED -
	DRAWN - SP	REVISED -
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PLOT DATE = 7/8/2019	DATE - 7/8/2019	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC
STAGE 2 - STA 641+00 TO STA 655+00**

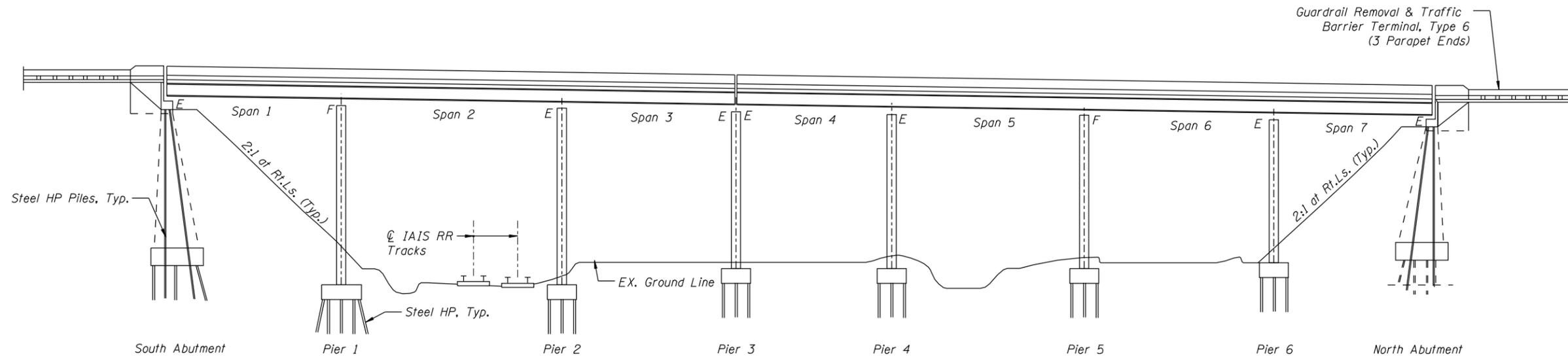
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300	(7R-VB)BRR	ROCK ISLAND	38	13
CONTRACT NO. 64N10				
ILLINOIS FED. AID PROJECT				

SCALE: SHEET OF SHEETS STA. TO STA.

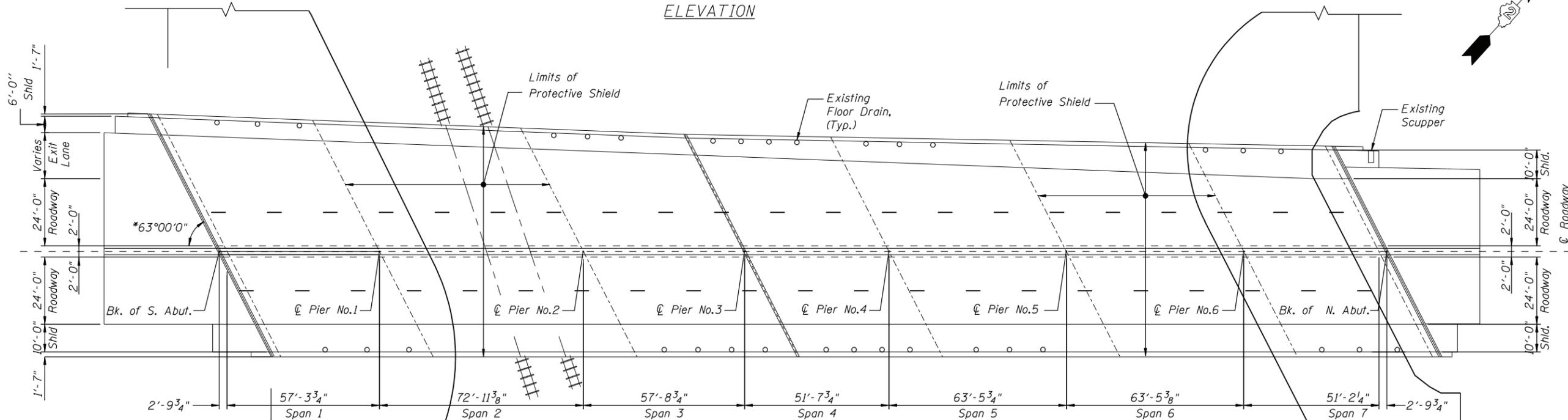
Existing Structure Consists of a 7 Span Steel WF-Beam Bridge with a 7" Concrete deck and 4'-0" median with a double jersey face concrete barrier dividing the roadway in each direction. Structure No. S.N. 081-0036

INDEX OF SHEETS

- S01 General Plan & Elevation
- S02 General Data
- S03 Stage I & II Construction
- S04 Bridge Deck Repair
- S05 Approach Slab Repair
- S06 N. & S. Abut. Joint Removal & Replacement
- S07 Pier 3 Joint Removal & Replacement
- S08 Parapet Repairs
- S09 Modified Preformed Joint Strip Seal
- S10 Adjusting existing scupper
- S11 Framing Plan & Painting Details
- S12 Piers 1 & 2
- S13 Piers 3 & 4
- S14 Piers 5 & 6
- S15 Slopewall Repairs
- S16 Bar Splicer Assembly and Mechanical Splicer Details
- S17 Temporary Concrete Barrier for Stage Construction



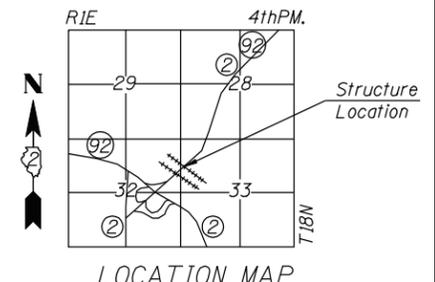
ELEVATION



PLAN



LIC. EXP: 11/30/2020 *[Signature]* 9/18/2019 Date



LOCATION MAP



USER NAME = MJGC07	DESIGNED - AA	REVISED -
	DRAWN - SD	REVISED -
PLOT SCALE = 48,0000 1/1	CHECKED - OAO	REVISED -
PLOT DATE = 9/18/2019	DATE - 9/18/2019	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GENERAL PLAN & ELEVATION
STRUCTURE NO. 081-0036**

SHEET S01 OF S17 SHEETS

F.A.P. RTE. 300	SECTION (7R-VBIBRR)	COUNTY ROCK ISLAND	TOTAL SHEETS 38	SHEET NO. 14
CONTRACT NO. 64N10				
ILLINOIS FED. AID PROJECT				

GENERAL NOTES

1. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.
2. Reinforcement bars designated (E) shall be epoxy coated.
3. Bars noted thus, 3 x 2-#5 indicates 3 lines of bars with 2 lengths of bars per line.
4. All structural steel shall conform to AASHTO Classification M-270 Gr 36, unless otherwise noted.
5. All exposed concrete edges shall have a 3/4" x 45° chamfer, except where shown.
6. The contractor shall take all necessary precautions for the protection of passing vehicles from falling objects and/or materials until completion of the work.
7. Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with "Concrete Removal".
8. Areas of deck repairs shown are estimated. The Engineer shall show actual locations of deck repairs on As-built Plans.
9. Joint openings shall be adjusted according to Article 520.04 of the Std. Specs. when the deck is poured at an ambient temperature other than 50°F.
10. Containment of cleaning residue is required to control nuisance dust. See Special Provisions.

SCOPE OF WORK

1. Provide protective shielding within limits as indicated on the plans.
2. Scarify 3/4" from the bridge deck slab.
3. Perform full depth deck and approach slab repairs.
4. Clear all deck drains.
5. Remove and replace transverse expansion joints at the abutment and pier No. 3 with preformed joint strip seals.
6. Apply 2 1/2" bridge deck latex concrete overlay to the bridge deck and approach slabs
7. Perform bridge deck and approach slab diamond grinding.
8. Perform structural concrete repairs to the parapets.
9. Apply protective coat on the reconstructed transverse joint areas and latex overlay.
10. Apply acrylic paint for the inside face, back face and top of existing and reconstructed parapets.
11. Clean and repaint all steel elements located under the expansion joints at Abutments and Pier 3, as indicated in plans.
12. Blast and paint expansion bearings at Abutments and Pier 3.
13. Perform structural concrete repairs for the piers.
14. Clean bridge seats and apply concrete sealant at the Abutments and Pier No. 3.
15. Repair bituminous coated gravel slopewalls.

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Concrete Removal	Cu. Yd.	43	-	43
Concrete Superstructure	Cu. Yd.	48.3	-	48.3
Bridge Deck Grooving (Longitudinal)	Sq. Yd.	2590	-	2590
Protective Coat	Sq. Yd.	4840	-	4840
Reinforcement Bars, Epoxy Coated	Pound	5300		5300
Preformed Joint Strip Seal	Foot	268	-	268
Acrylic Coating	Sq. Yd.	724	-	724
Bridge Deck Latex Concrete Overlay, 2 1/2 inches	Sq. Yd.	4015	-	4015
Containment and Disposal of Non-Lead Paint Cleaning Residues No. 1	L Sum	1	-	1
Cleaning and Painting Steel Bridge No. 1	L Sum	1	-	1
Bridge Deck Scarification 3/4"	Sq. Yd.	4015	-	4015
Structural Repair of Concrete (Depth Equal To or Less Than 5 Inches)	Sq. Ft.	1120	9	1129
Structural Repair of Concrete (Depth Greater Than 5 Inches)	Sq. Ft.	-	108	108
Deck Slab Repair (Full Depth, Type I)	Sq. Yd.	33	-	33
Deck Slab Repair (Full Depth, Type II)	Sq. Yd.	59	-	59
Approach Slab Repair (Full Depth)	Sq. Yd.	159	-	159
Protective Shield	Sq. Yd.	1208	-	1208
Bar Splicers	Each	96	-	96
Stone Riprap, Class A3	Sq. Yd.	-	35	35
Filter Fabric	Sq. Yd.	-	35	35
Granular Embankment, Special	Cu. Yd.	-	18.6	18.6
Diamond Grinding (Bridge Section)	Sq. Yd.	4015	-	4015



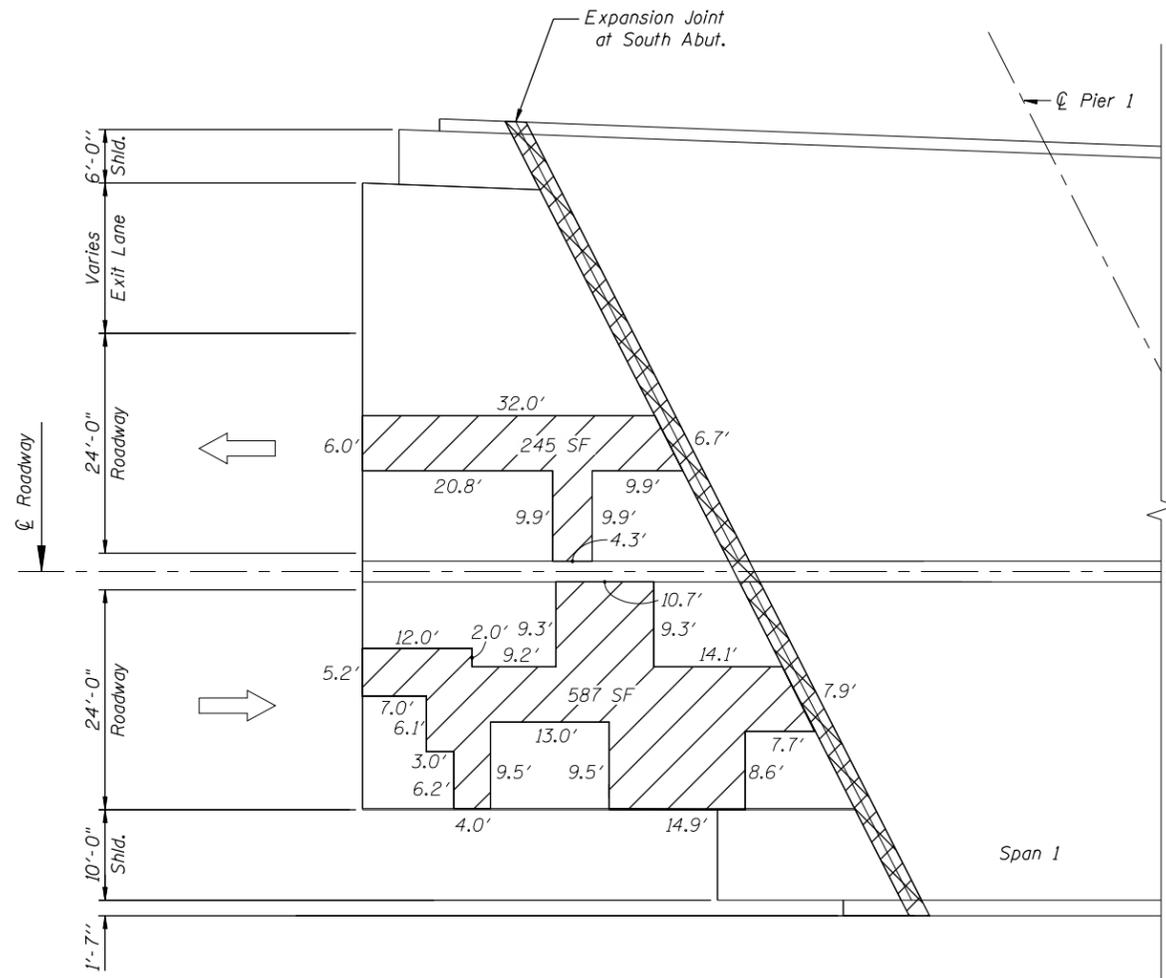
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

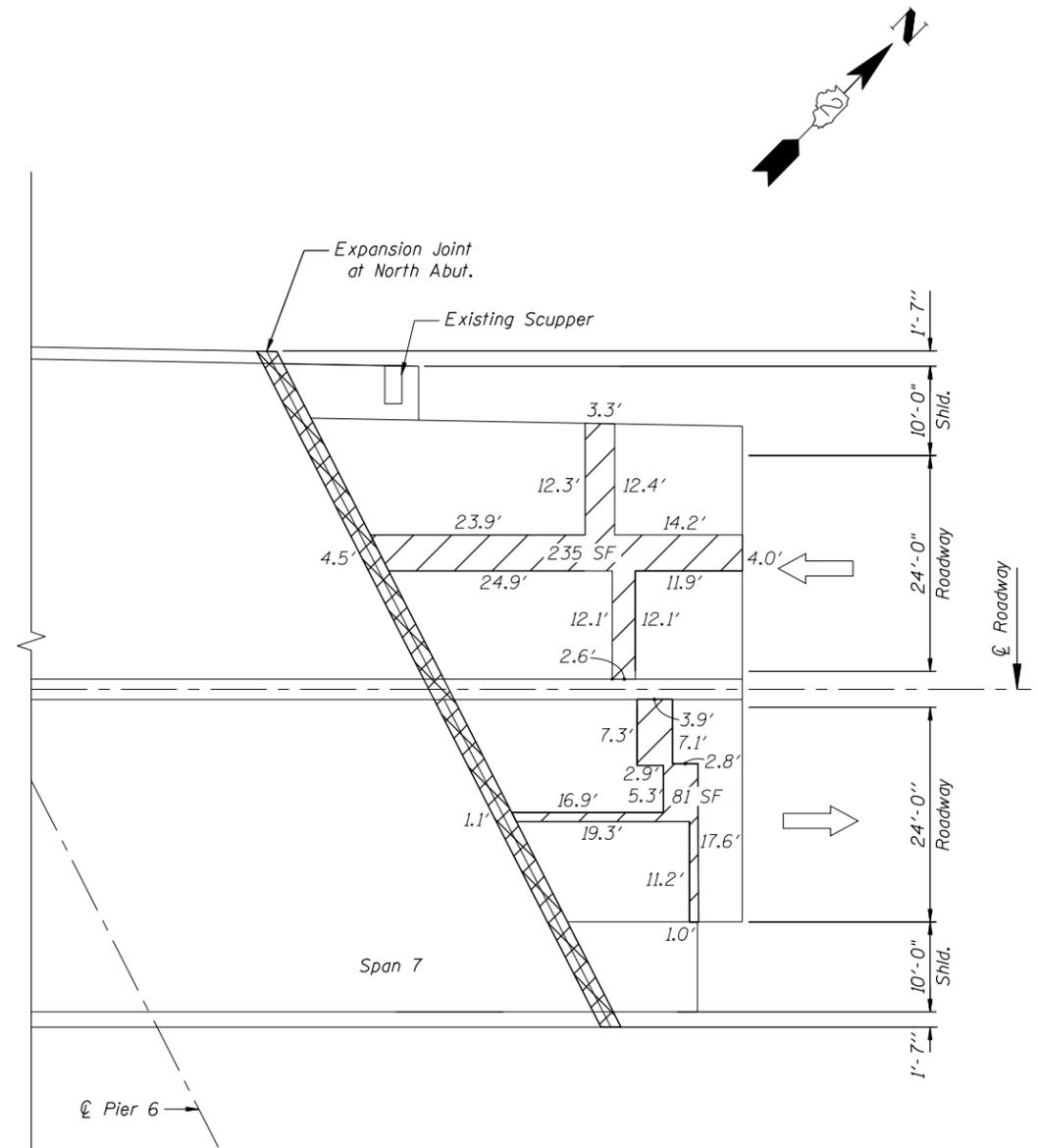
**GENERAL DATA
STRUCTURE NO. 081-0036**

SHEET S02 OF S17 SHEETS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
300	(7R-VB)BRR	ROCK ISLAND	38	15
			CONTRACT NO. 64N10	
ILLINOIS FED. AID PROJECT				



SOUTH APPROACH SLAB REPAIR PLAN



NORTH APPROACH SLAB REPAIR PLAN

Legend

Approach Slab Repair (Full Depth)



Expansion Joint Repair



Note:

For Transverse joint at Abutments,
See Sheet 19 of 38.

Item	Unit	Quantity
Approach Slab Repair (Full Depth)	SY	159



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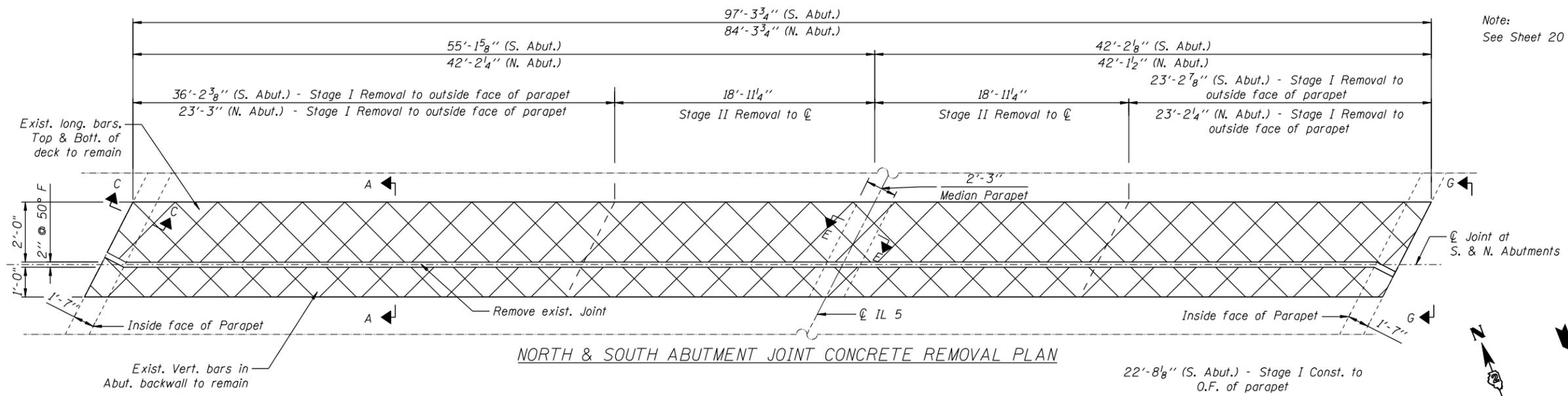
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**APPROACH SLAB REPAIR
STRUCTURE NO. 081-0036**

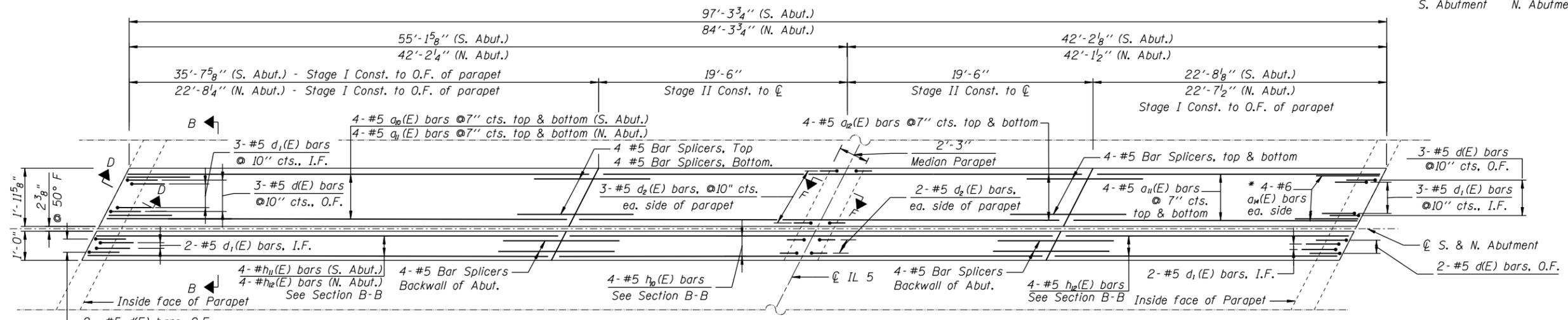
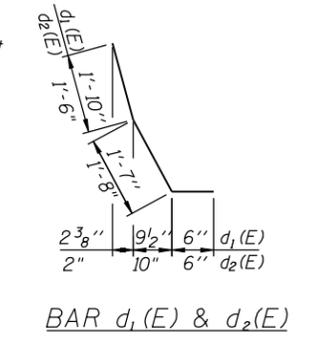
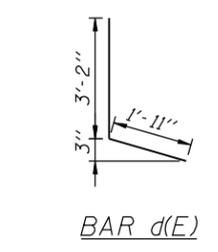
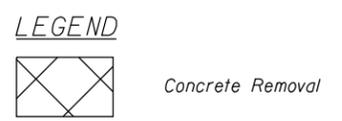
SHEET S05 OF S17 SHEETS

F.A.P. RTE. 300	SECTION (7R-VB)RR	COUNTY ROCK ISLAND	TOTAL SHEETS 38	SHEET NO. 18
			CONTRACT NO. 64N10	
ILLINOIS FED. AID PROJECT				

Note:
See Sheet 20 of 38 for Sections C-C, D-D, E-E and F-F.



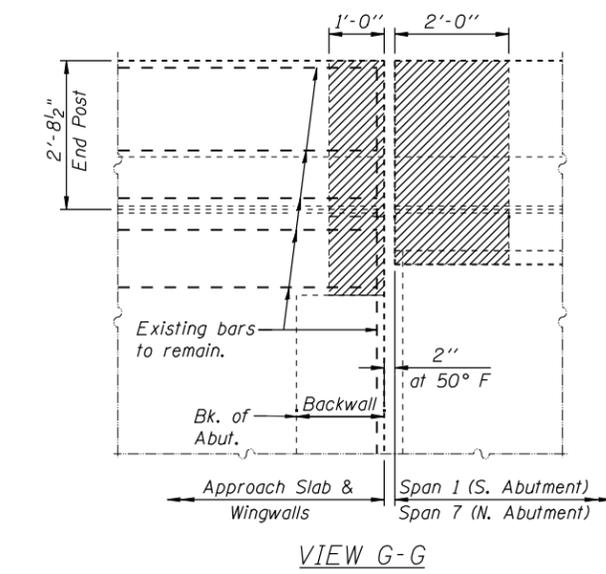
NORTH & SOUTH ABUTMENT JOINT CONCRETE REMOVAL PLAN



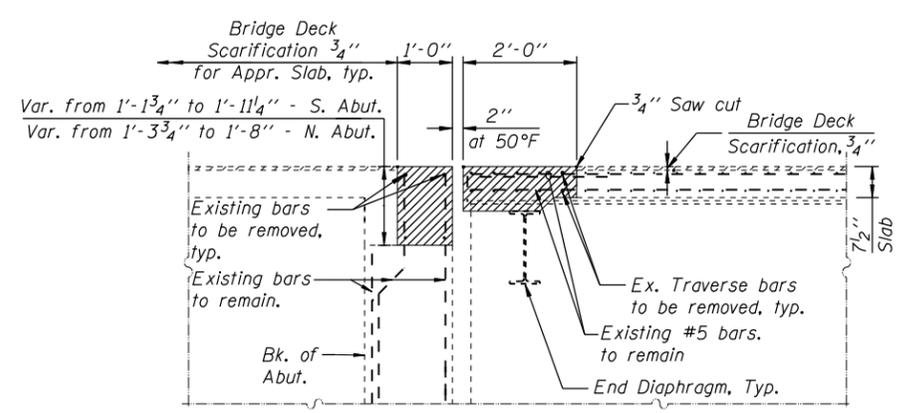
NORTH & SOUTH ABUTMENT JOINT CONCRETE CONSTRUCTION PLAN

BILL OF MATERIAL
(Both Abutments)

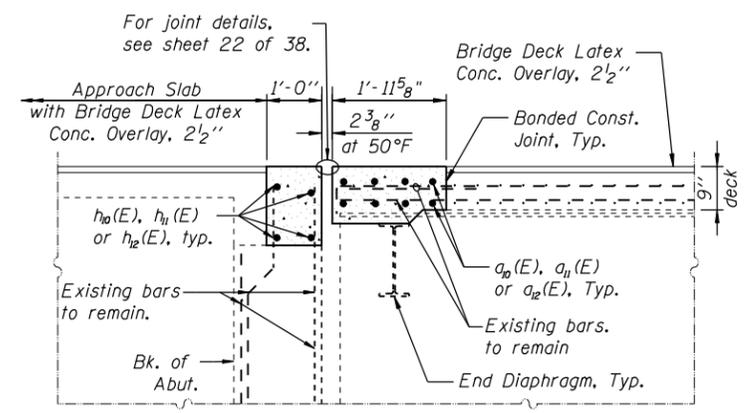
Bar	No.	Size	Length	Shape
$a_0(E)$	8	#5	35'-1"	—
$a_1(E)$	24	#5	22'-1"	—
$a_2(E)$	16	#5	37'-8"	—
$a_4(E)$	16	#6	6'-6"	—
$d(E)$	20	#5	5'-1"	L
$d_1(E)$	20	#5	3'-11"	L
$d_2(E)$	20	#5	3'-8"	L
$h_0(E)$	8	#5	37'-8"	—
$h_1(E)$	4	#5	35'-3"	—
$h_2(E)$	12	#5	22'-3"	—
Concrete Removal			Cu. Yd.	29.9
Concrete Superstructure			Cu. Yd.	32.6
Reinforcement Bars, Epoxy coated			Pound	2640



VIEW G-G



SECTION A-A
Dimensions @ Rt. L's to Joint



SECTION B-B
Dimensions @ Rt. L's to Joint



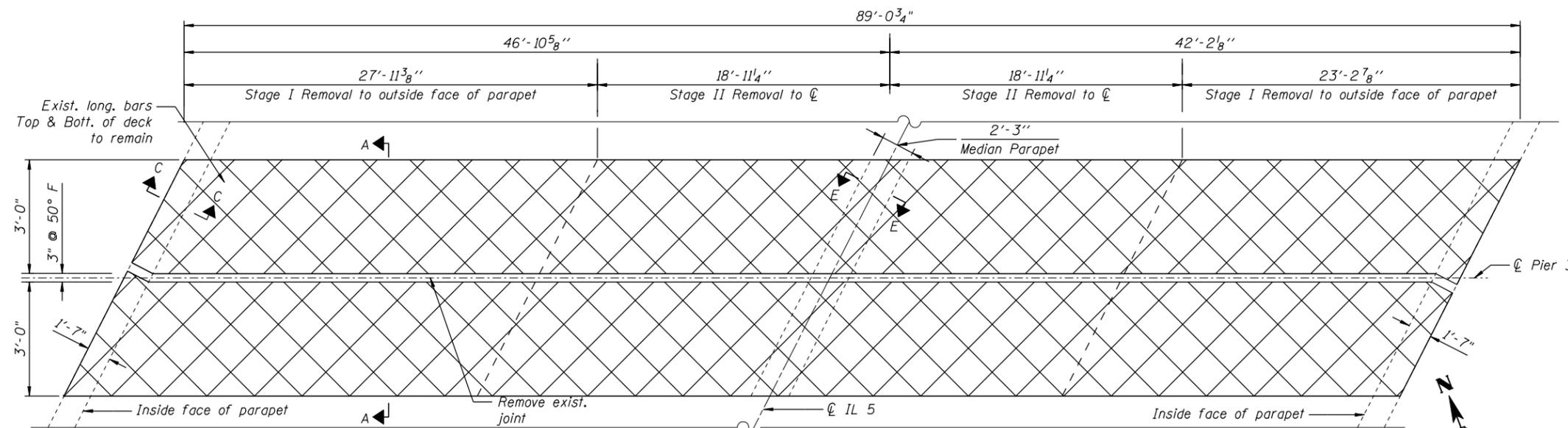
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

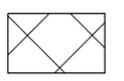
N. & S. ABUT. JOINT REMOVAL & REPLACEMENT
STRUCTURE NO. 081-0036

SHEET S06 OF S17 SHEETS

F.A.P. RTE. 300	SECTION (7R-VB)BRR	COUNTY ROCK ISLAND	TOTAL SHEETS 38	SHEET NO. 19
ILLINOIS FED. AID PROJECT			CONTRACT NO. 64N10	

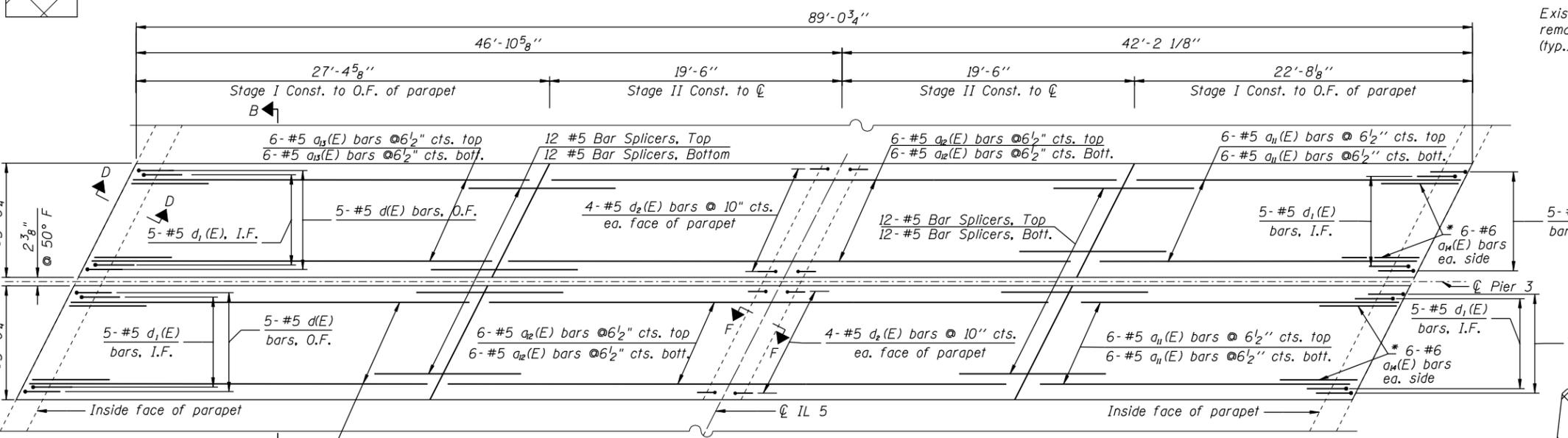


LEGEND

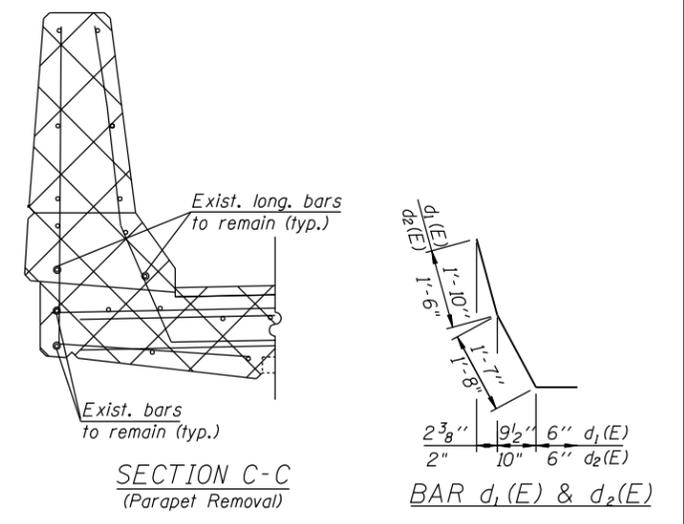


Concrete Removal

PIER 3 JOINT CONCRETE REMOVAL PLAN

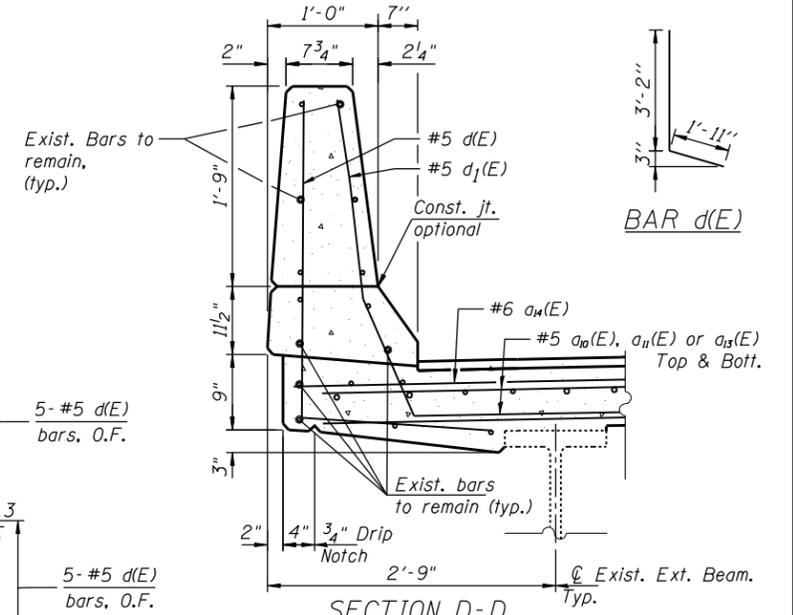


PIER 3 JOINT CONCRETE CONSTRUCTION PLAN



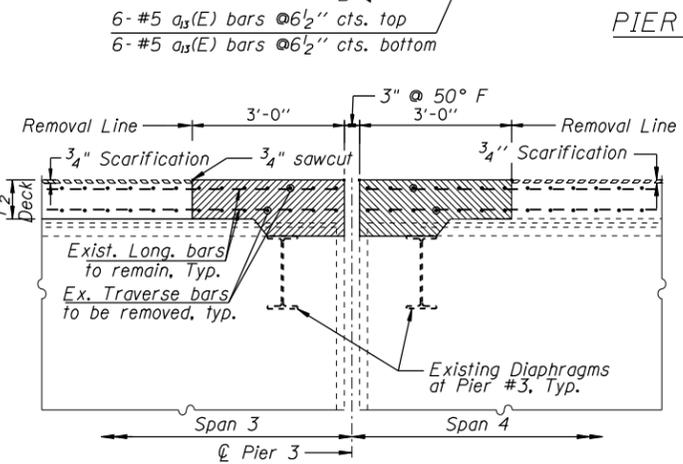
SECTION C-C
(Parapet Removal)

BAR d1(E) & d2(E)

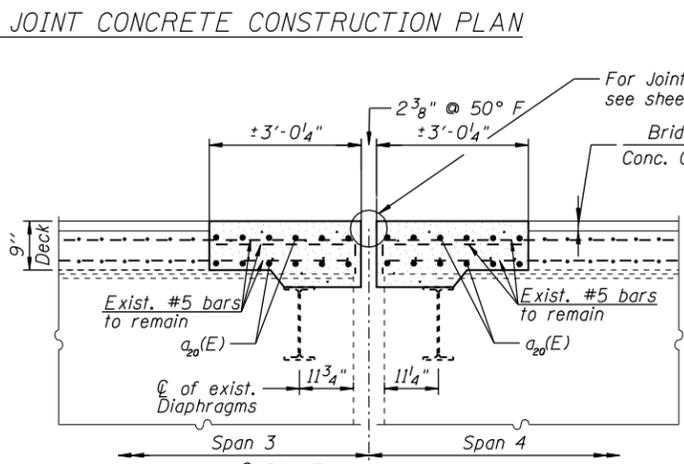


SECTION D-D
(Parapet Replacement)

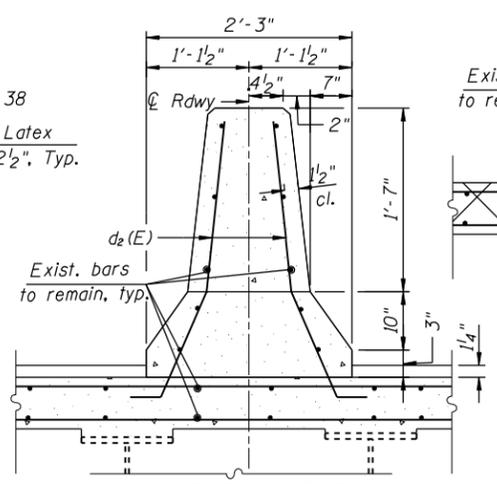
* Lap #6 a4(E) bars with each #5 a0(E), a1(E), a3(E) top bars, ea. side (at Abuts. and Pier #3)



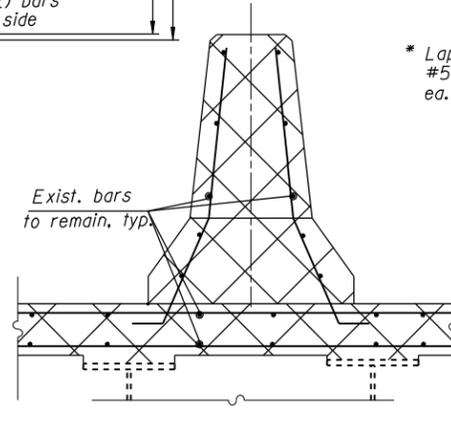
SECTION A-A
Dimensions @ Rt. L's to Joint



SECTION B-B
Dimensions @ Rt. L's to Joint



SECTION F-F
@ Rt. L's to Joint



SECTION E-E
@ Rt. L's to Joint

BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a1(E)	24	#5	22'-1"	—
a2(E)	24	#5	37'-8"	—
a3(E)	24	#5	26'-11"	—
a4(E)	24	#6	6'-6"	—
d(E)	20	#5	5'-1"	L
d1(E)	20	#5	3'-11"	L
d2(E)	16	#5	3'-8"	L
Concrete Removal			Cu. Yd.	13.1
Concrete Superstructure			Cu. Yd.	15.7
Reinforcement Bars, Epoxy coated			Pound	2660



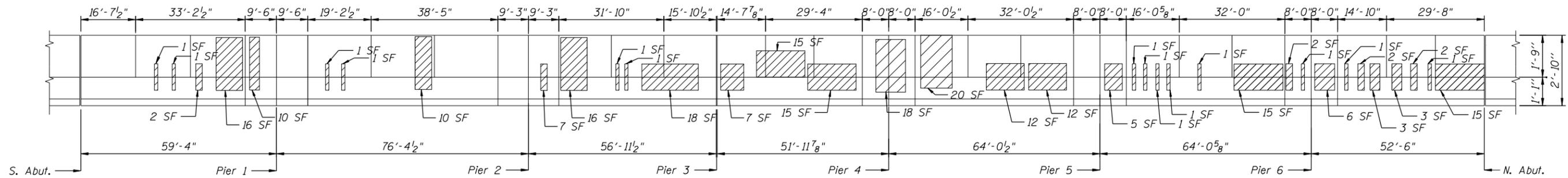
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PLOT SCALE = 48.0000' / 1"	DRAWN - ZP	REVISED -
PLOT DATE = 9/18/2019	CHECKED - OAO	REVISED -
	DATE - 9/18/2019	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

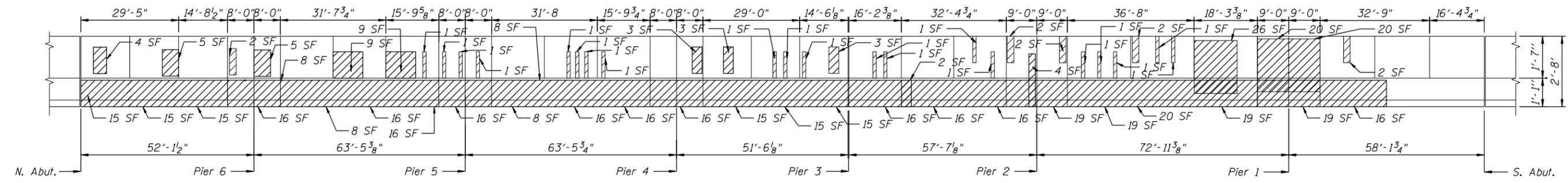
PIER 3 JOINT REMOVAL & REPLACEMENT
STRUCTURE NO. 081-0036

SHEET 507 OF 517 SHEETS

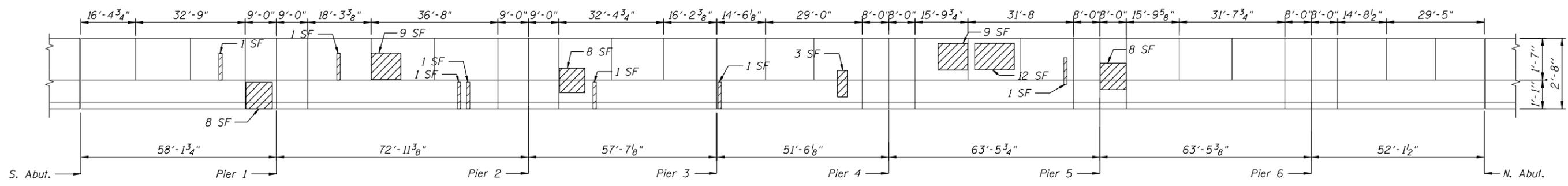
F.A.P. RTE. 300	SECTION (7R-VB)BRR	COUNTY ROCK ISLAND	TOTAL SHEETS 38	SHEET NO. 20
ILLINOIS FED. AID PROJECT			CONTRACT NO. 64N10	



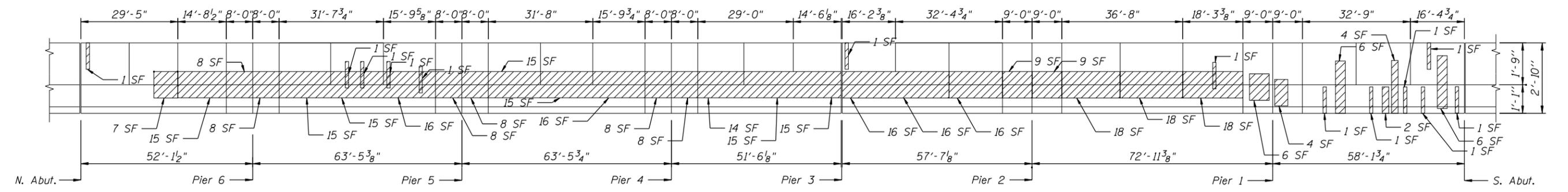
ELEVATION OF INSIDE FACE OF WEST PARAPET



ELEVATION OF WEST FACE OF MEDIAN PARAPET



ELEVATION OF EAST FACE OF MEDIAN PARAPET



ELEVATION OF INSIDE FACE OF EAST PARAPET

BILL OF MATERIALS

Item	Unit	Quantity
Structural Repair of Concrete (Depth Equal to or less than 5")	Sq Ft	1120

LEGEND
 Structural Repair of Concrete (Depth Equal to or Less than 5")

Note:
 Saw cut the perimeter of the damaged area 3/4" deep.
 Dimensions along inside face of parapet.



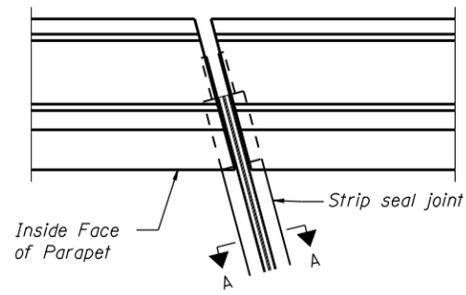
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DATE - 9/18/2019		REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

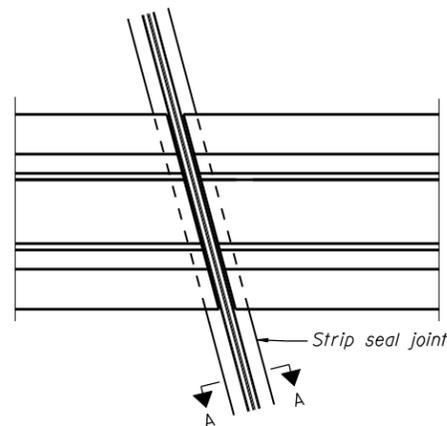
PARAPET REPAIRS
STRUCTURE NO. 081-0036

SHEET 508 OF 517 SHEETS

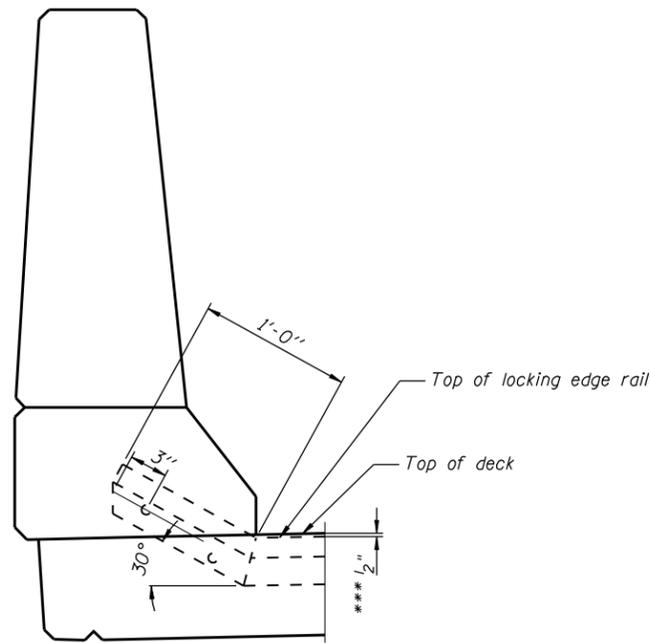
F.A.P. RTE. 300	SECTION (7R-VBIBRR)	COUNTY ROCK ISLAND	TOTAL SHEETS 38	SHEET NO. 21
CONTRACT NO. 64N10				
ILLINOIS FED. AID PROJECT				



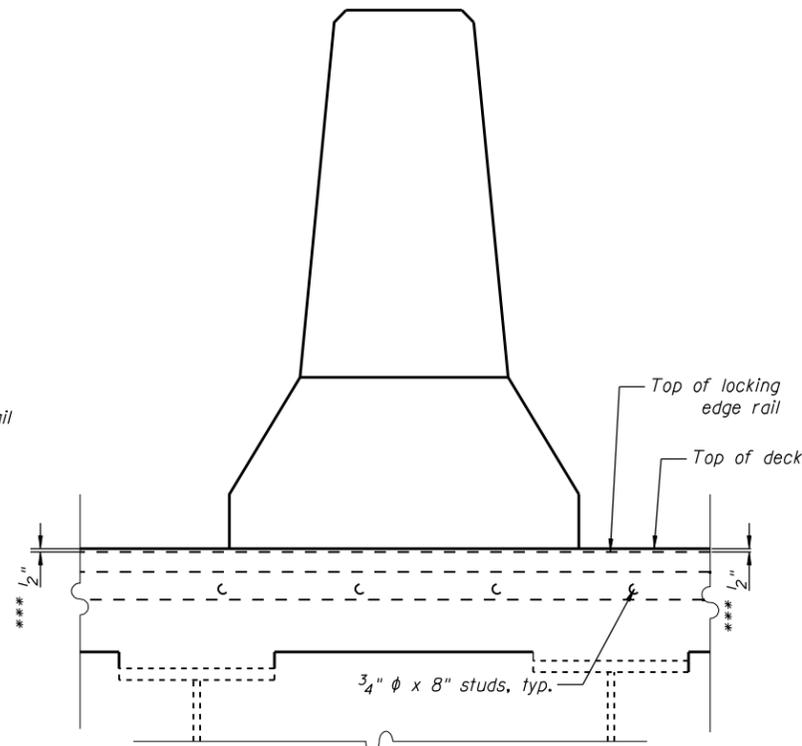
PLAN AT EXTERIOR PARAPET



PLAN AT MEDIAN PARAPET



EXTERIOR PARAPET



MEDIAN PARAPET

NOTES:

The strip seal shall be made continuous and shall have a minimum thickness of 1/4". The configuration of the strip seal shall match the configuration of the locking edge rails. Open or "webbed" strip seal gland configurations are not permitted. The gland shall be sized for a maximum rated movement of 4 inches.

The locking edge rails depicted are configured for typical applications and are conceptual only. The actual configuration of the locking edge rails and matching strip seal may vary from manufacturer to manufacturer provided they fit the application and meet the minimum anchorage shown. Flanged edge rails, however, will not be allowed. Locking edge rails may exceed the 4 1/2" maximum depth provided the anchorage system is revised according to the manufacturer's recommendation.

The manufacturer's recommended installation methods shall be followed.

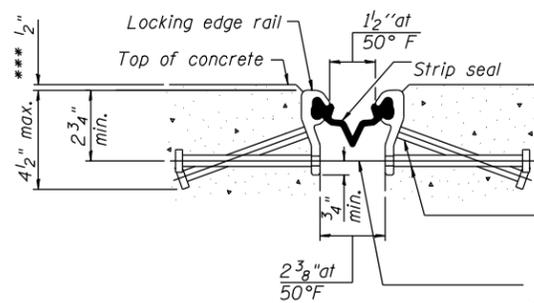
All steel components shall be galvanized after fabrication according to Article 520.03 of the Standard Specifications.

The Maximum space between locking edge rail segments shall be 3/16" and sealed with a suitable sealant; however, any rail joint within 10' measured perpendicular to the face of the curb or parapet shall be welded as shown in the locking edge rail splice detail.

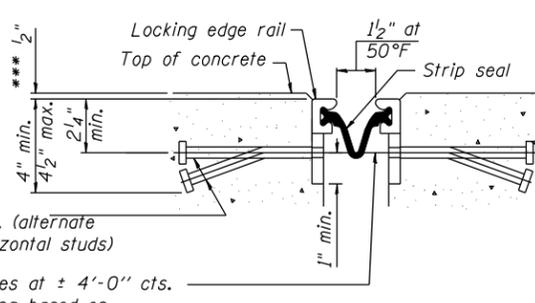
Cost of parapet sliding plates, embedded plates, and anchorage studs included with Preformed Joint Strip Seal.

The concrete opening below the strip seal will vary based on the locking edge rail chosen by the Contractor. Deck and parapet lengths shown elsewhere in the plans are dimensioned to the concrete opening, not the joint opening, and are based on the rolled locking edge rail. If the Contractor elects to use a different locking edge rail, dimensional adjustments may be required. One exception to this would be the strip seal joint at the end of the precast bridge approach slab. For these cases the pavement connector length shall be adjusted, not the length of the bridge approach slab.

*** Prior to 1/4" grinding

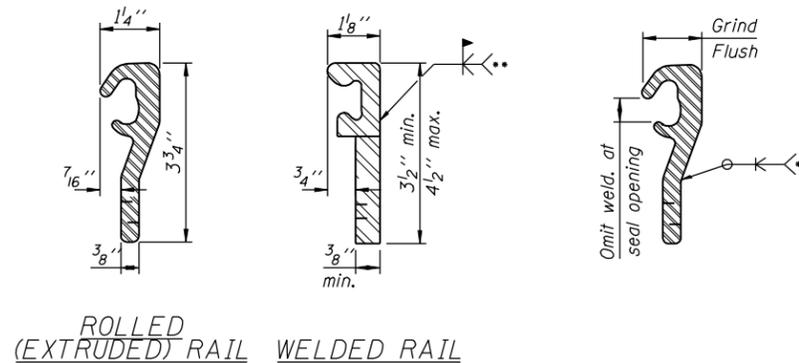


SHOWING ROLLED RAIL JOINT



SHOWING WELDED RAIL JOINT

* 5/8" ϕ x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)
 3/8" ϕ threaded rods in 7/16" ϕ holes at \pm 4'-0" cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.



ROLLED (EXTRUDED) RAIL WELDED RAIL

LOCKING EDGE RAILS

LOCKING EDGE RAIL SPLICE

** Back gouge not required if complete joint penetration is verified by mock-up.

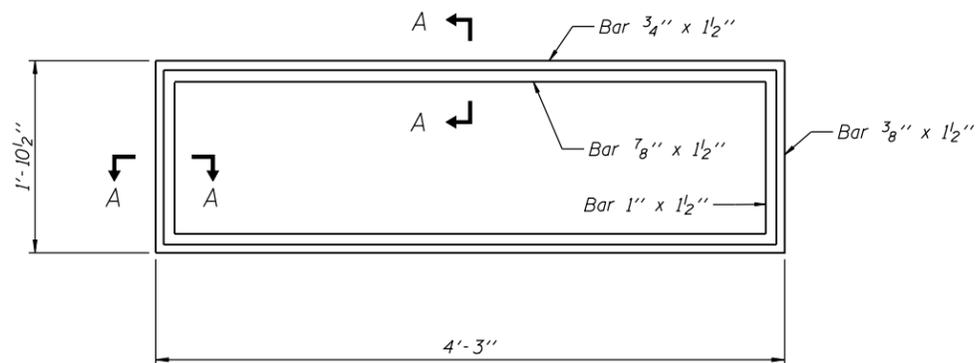
The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

SECTION A-A

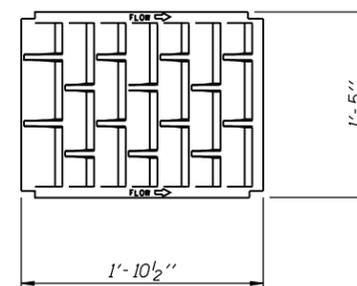
* Granular or solid Flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

BILL OF MATERIAL

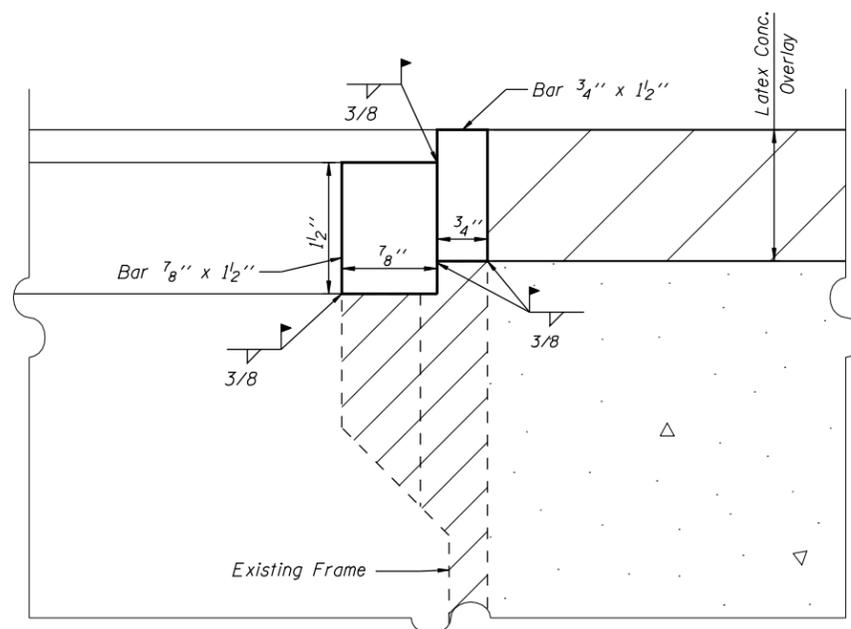
Item	Unit	Total
Preformed Joint Strip Seal	Foot	268



CAST FRAME



EXISTING CAST GRATE (x3)
(to be re-used)



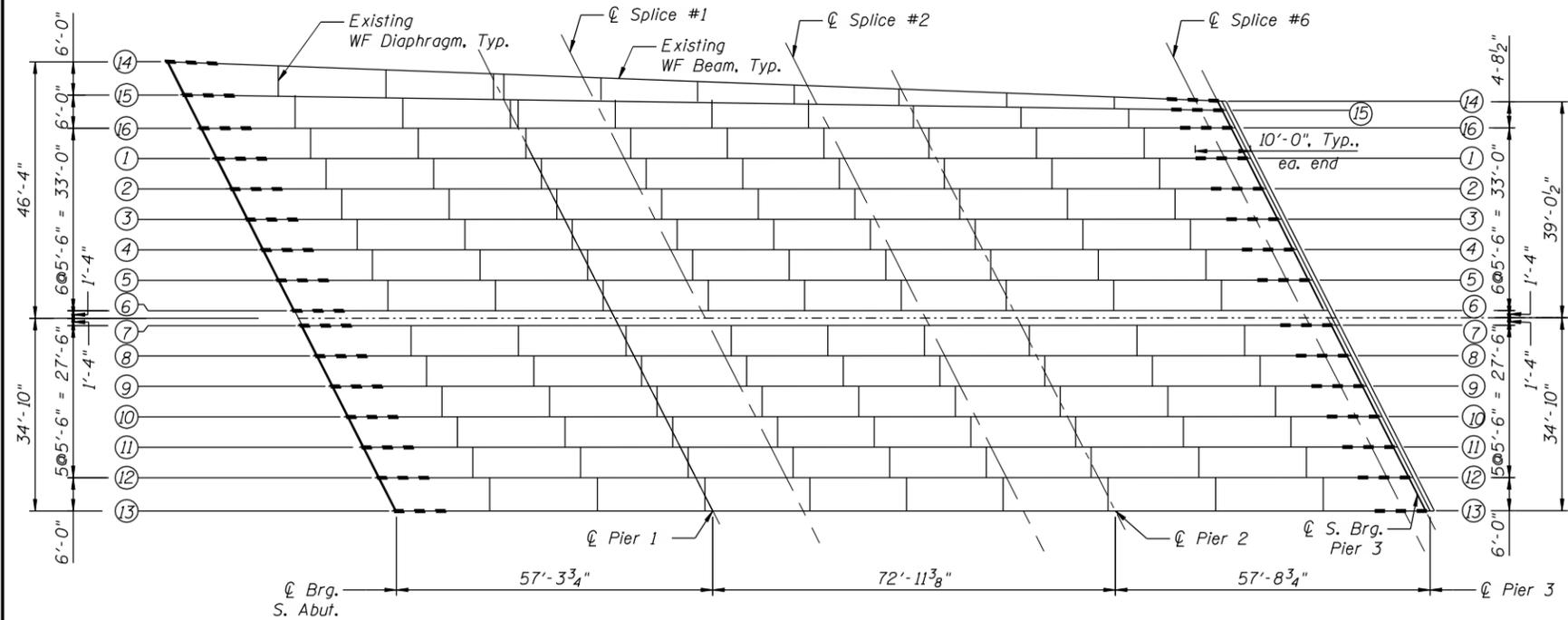
SECTION A-A

GENERAL NOTES:

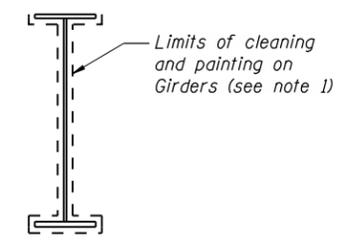
1. All structural steel shall conform to AASHTO Classification M-270 Gr. 36. The adjusting scupper ring shall be galvanized.
2. Bolts, shall be 1/2" ϕ , AASHTO M164 type I, mechanically galvanized.
3. Plan dimensions and details relative to existing plans are subject to nominal construction variations. The contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the contractor will be paid for the quantity actually furnished at the unit price bid for the work.
4. The contractor shall ensure that no damage is done to existing grates to be reused. Shop plans for proposed adjusting scupper ring shall be submitted for approval prior to fabrication.
5. Cost of all labor and material necessary to remove existing grates, clean existing scuppers, install adjusting scupper rings and reinstalling grates is included in the cost per unit each for drainage scuppers to be adjusted.
6. All Cast Iron Parts shall be Grey Iron conforming to the requirements of AASHTO M 105, Class 35B.
7. Bolts, Anchor Studs, Washers and Nuts shall conform to the requirements of ASTM A 307 and shall be Galvanized according to the requirements of AASHTO M 232.
8. Cast Iron Parts shall be unfinished.
9. Adjusting Ring shall be from Neenah and approved equal. Structural Steel weldments or equal sections and of the same configuration may be submitted for Cast Iron. Fillet or full Penetration Welds may be used for weldments. Details shall be submitted to the Engineer for approval.
10. Provide a 1/8" Fillet Weld around perimete of new Adjusting Ring to secure to existing Scupper. Electrode shall be compatible with cast Iron.

BILL OF MATERIALS

Item	Unit	Quantity
Drainage Scuppers to be Adjusted	Each	1



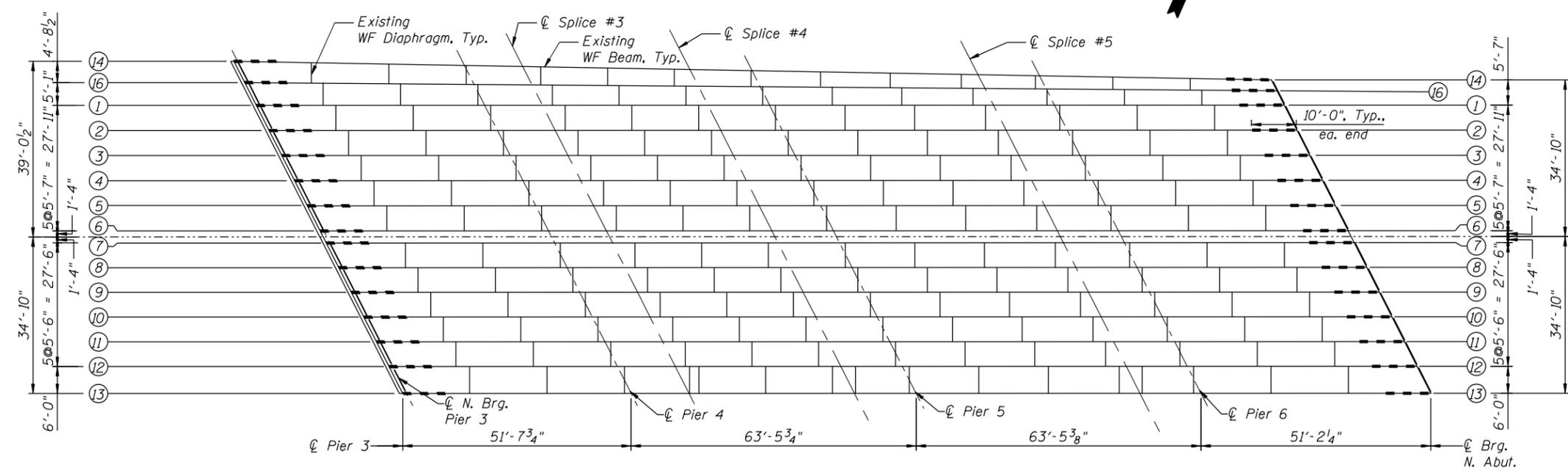
PARTIAL FRAMING PLAN
(Spans 1 thru 3)



TYP. INTERIOR AND EXTERIOR
W30 & W33 BEAMS

NOTES:

1. Structural steel within the 10'-0" limits near bearings, including splices, end diaphragms and beam extensions behind bearings, shall be cleaned according to SSPC-SP10.
2. The Designated areas cleaned per Near White Blast Cleaning - SSPC-SP10 and per Commercial Grade Power Tool Cleaning - SSPC-SP15 shall be painted according to the requirement of Paint System 1-OZ/E/U. The color of the final finish coat shall be Interstate Green, Munsell No.7.5G 4/8.



PARTIAL FRAMING PLAN
(Spans 4 thru 7)

LEGEND:

--- Cleaning and Painting Structural Steel

BILL OF MATERIAL

ITEMS	UNIT	QUANTITY
Cleaning and Painting Steel Bridge No. 1	L Sum	1
Containment and Disposal of Non-Lead Paint Cleaning Residues No. 1	L Sum	1



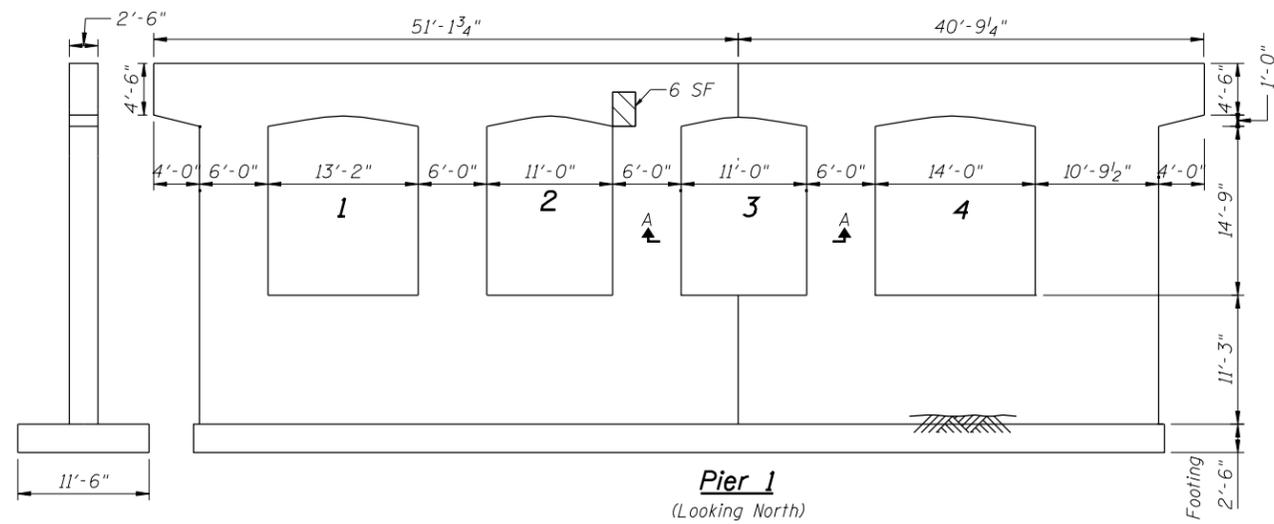
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PLOT SCALE = *SCALE*	DRAWN - SD	REVISED -
PLOT DATE = *DATE*	CHECKED - OAO	REVISED -
	DATE - 11/25/2019	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

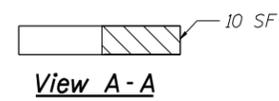
FRAMING PLAN AND PAINTING DETAILS
STRUCTURE NO. 081-0036

SHEET S11 OF S17 SHEETS

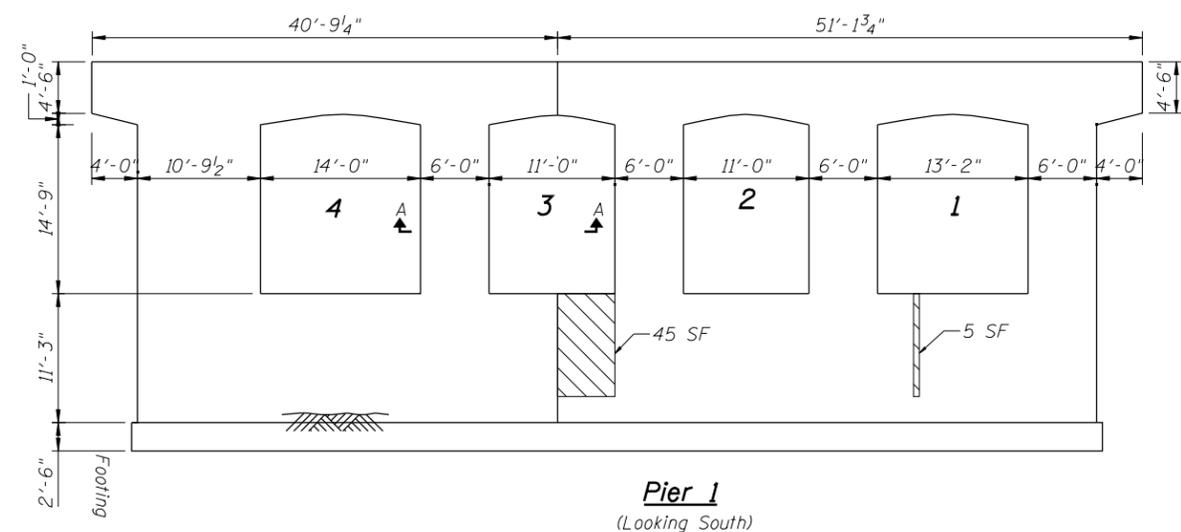
F.A.P. RTE. 300	SECTION (7R-VBIBRR)	COUNTY ROCK ISLAND	TOTAL SHEETS 38	SHEET NO. 24
ILLINOIS FED. AID PROJECT			CONTRACT NO. 64N10	



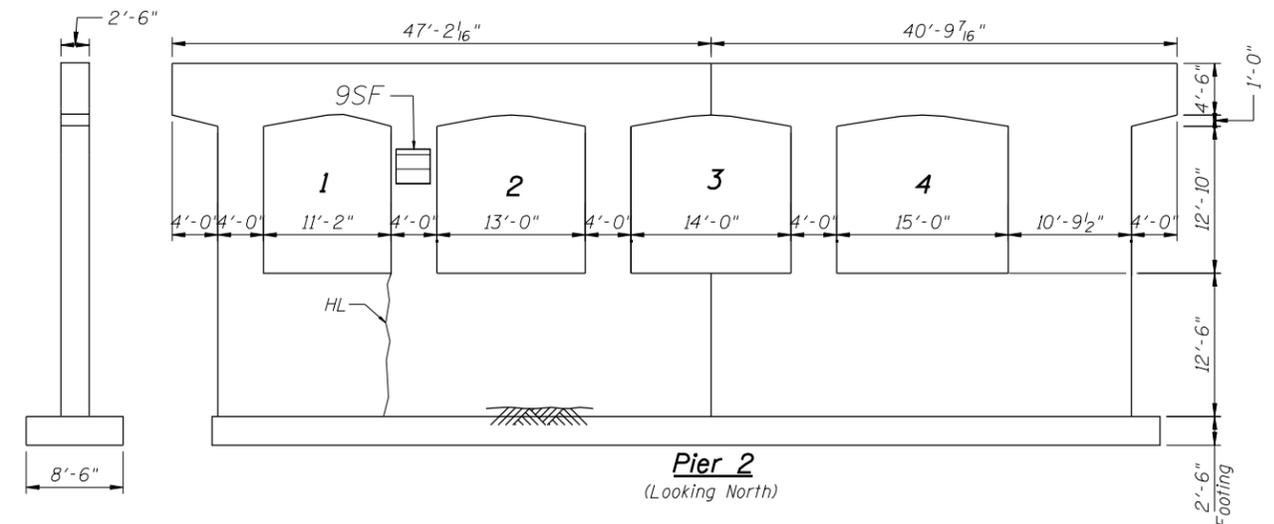
Pier 1
(Looking North)



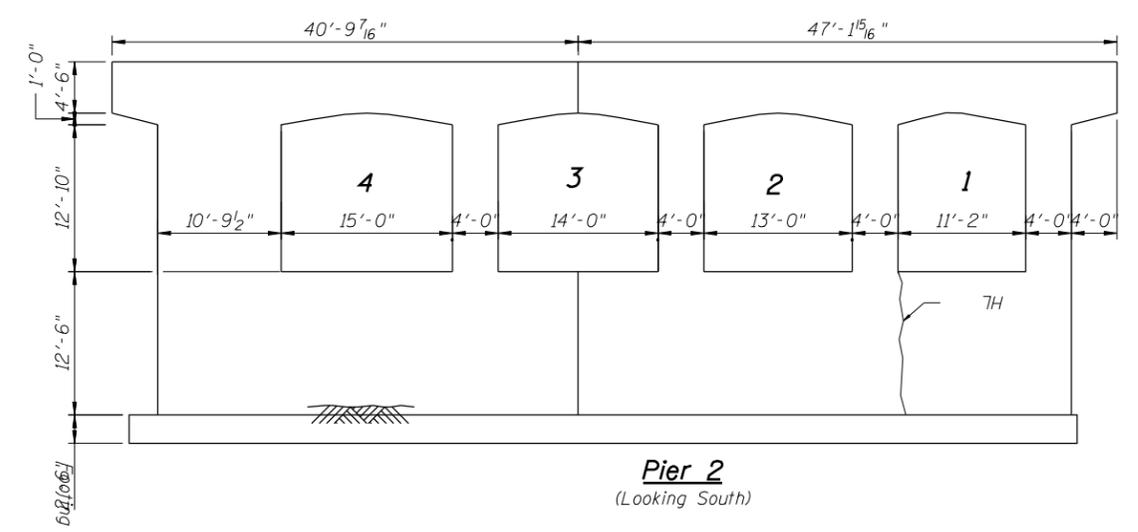
View A - A



Pier 1
(Looking South)



Pier 2
(Looking North)



Pier 2
(Looking South)

Legend:

 Structural Repair of Concrete
(Depth Greater than 5")

BILL OF MATERIAL

Item	Unit	Quantity
Structural Repair Of Concrete (Depth Greater than 5")	Sq.Ft.	66

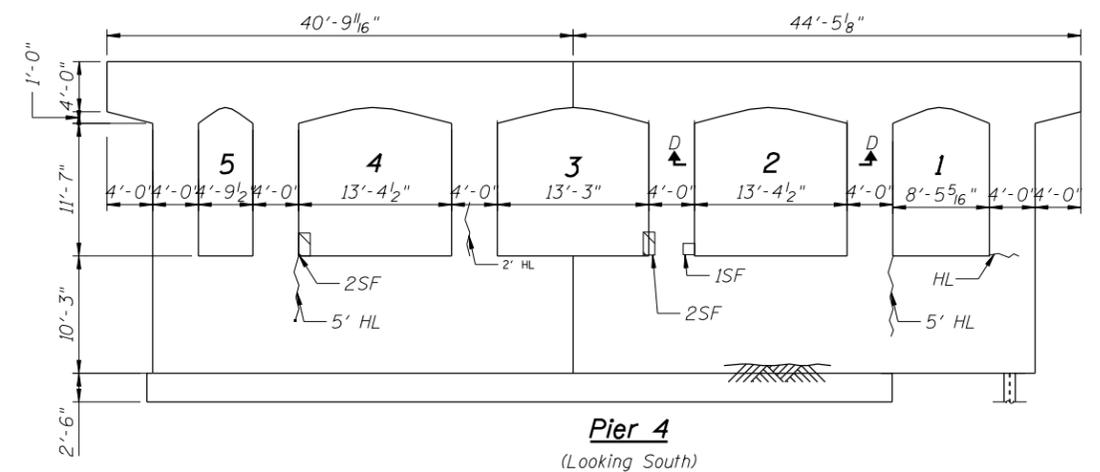
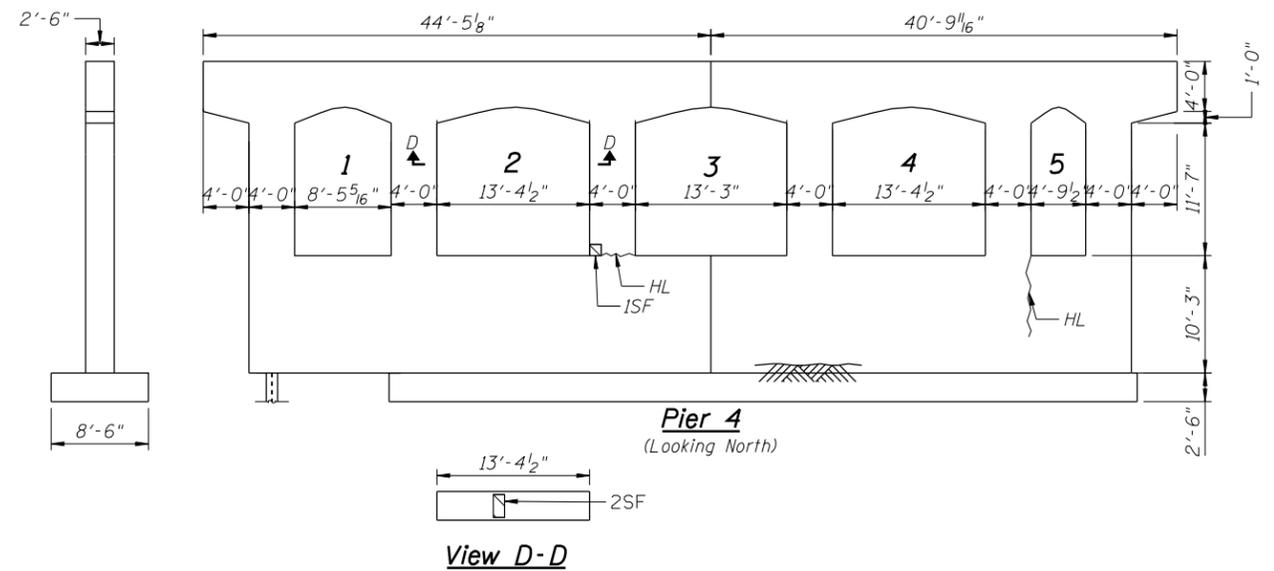
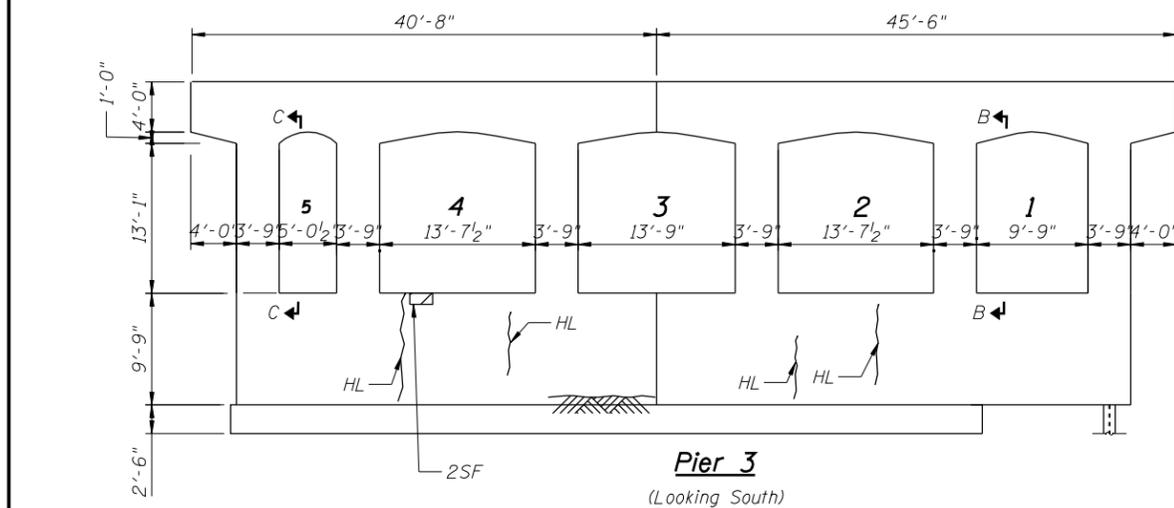
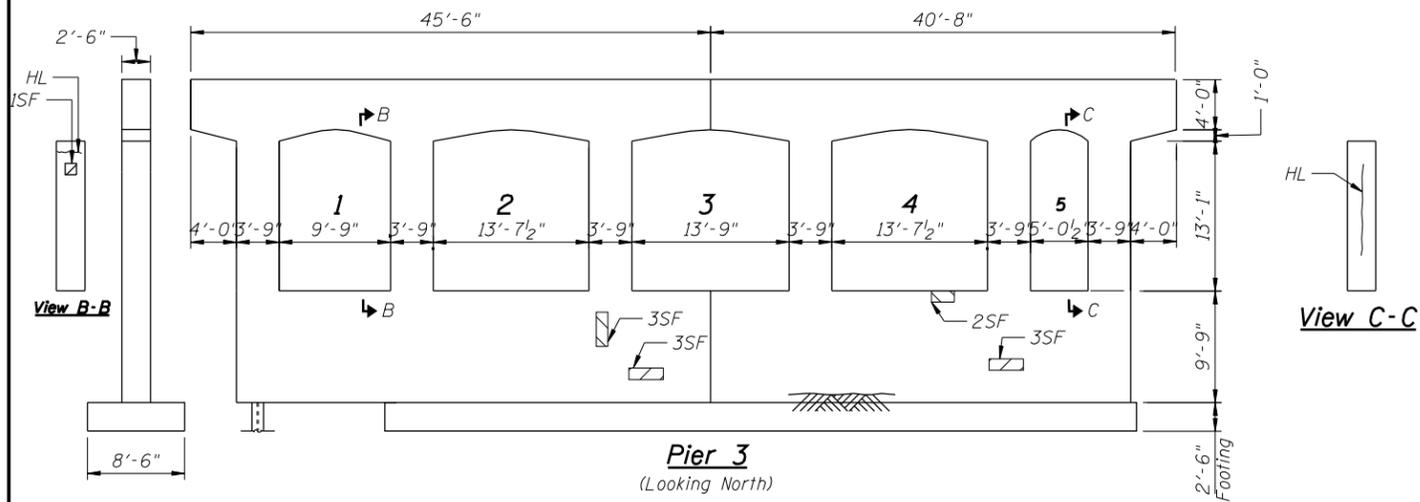
Legend:

 Structural Repair of Concrete
(Depth Equal to or Less Than 5")

 Crack (Hairline, Open Crack)

BILL OF MATERIAL

Item	Unit	Quantity
Structural Repair Of Concrete (Depth Equal to or Less Than 5")	Sq.Ft.	9



Legend:

- Structural Repair of Concrete (Depth Greater than 5")
- Crack (Hairline, Open Crack)

BILL OF MATERIAL

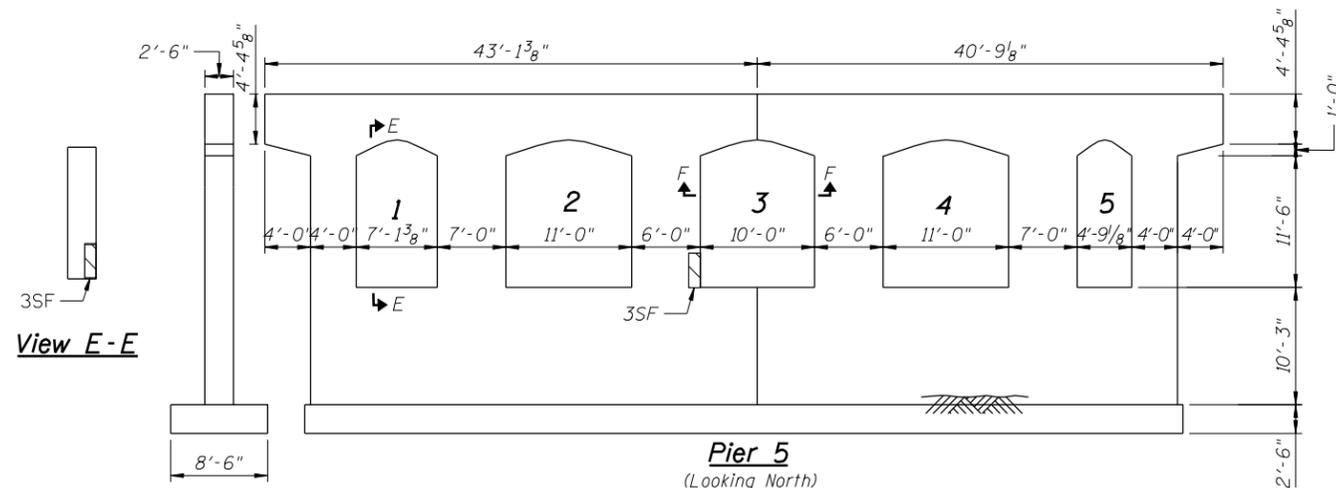
Item	Unit	Quantity
Structural Repair Of Concrete (Depth Greater than 5")	Sq.Ft	14

Legend:

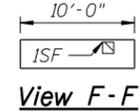
- Structural Repair of Concrete (Depth Greater than 5")
- Crack (Hairline, Open Crack)

BILL OF MATERIAL

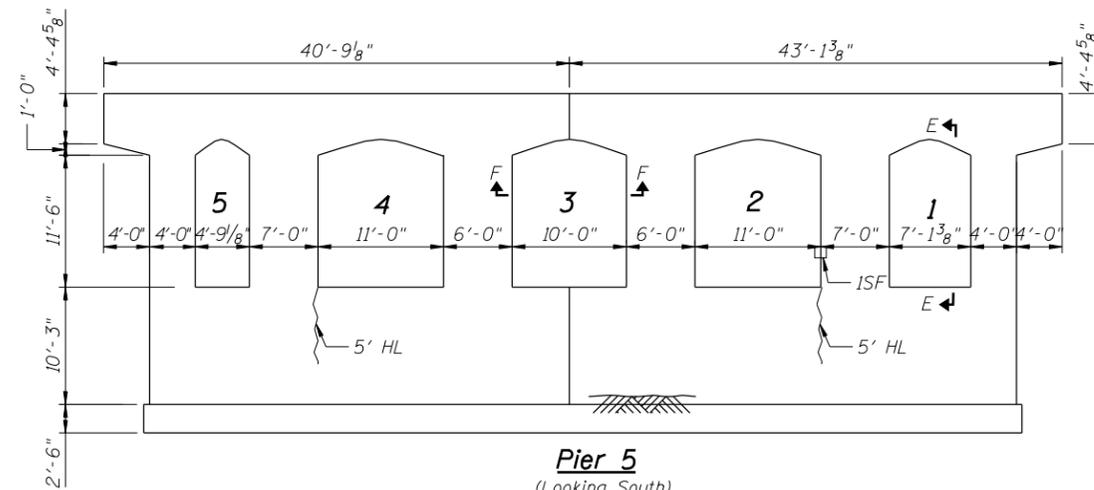
Item	Unit	Quantity
Structural Repair Of Concrete (Depth Greater than 5")	Sq.Ft.	8



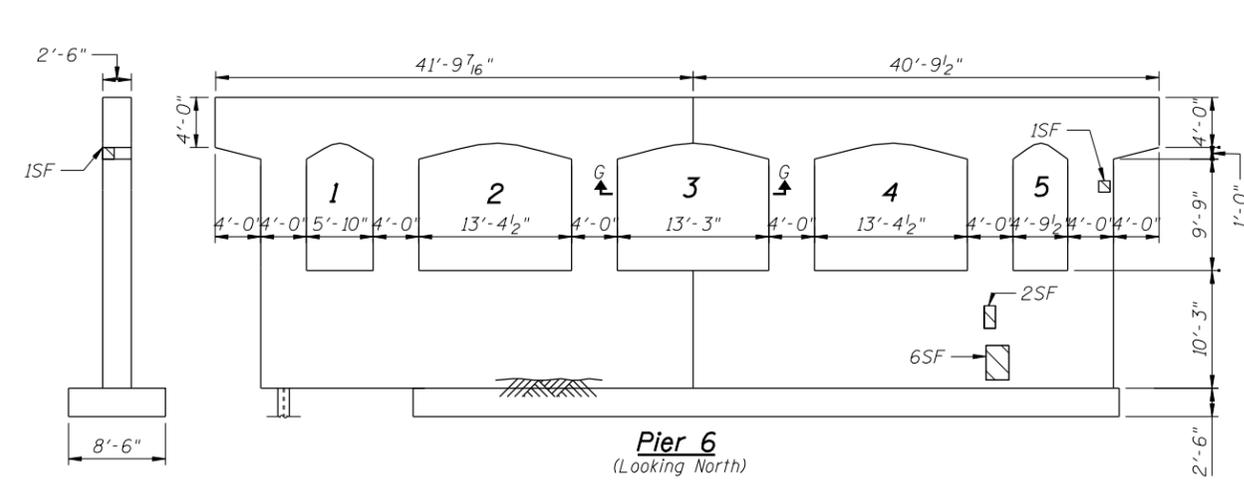
Pier 5
(Looking North)



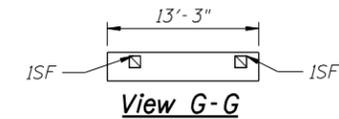
View F-F



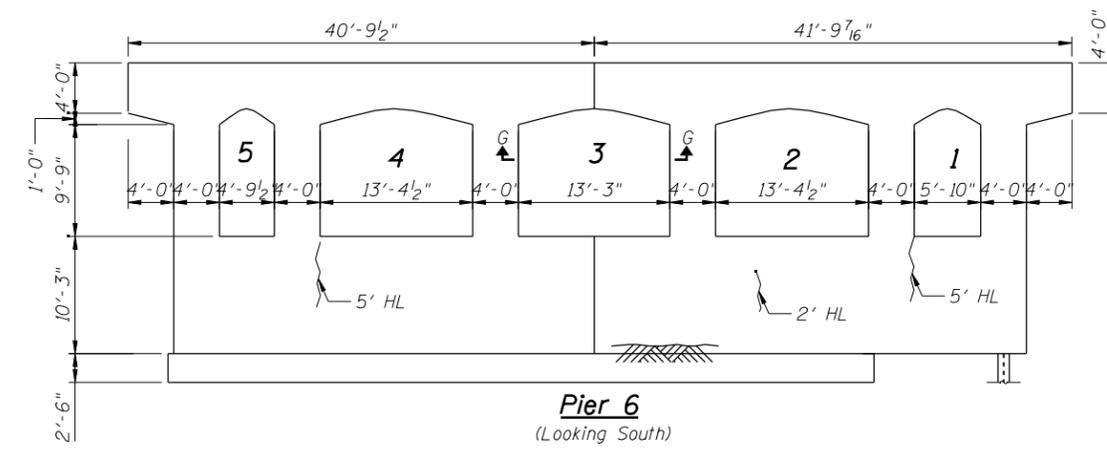
Pier 5
(Looking South)



Pier 6
(Looking North)



View G-G



Pier 6
(Looking South)

Legend:

- Structural Repair of Concrete (Depth Greater than 5")
- Crack (Hairline, Open Crack)

BILL OF MATERIAL

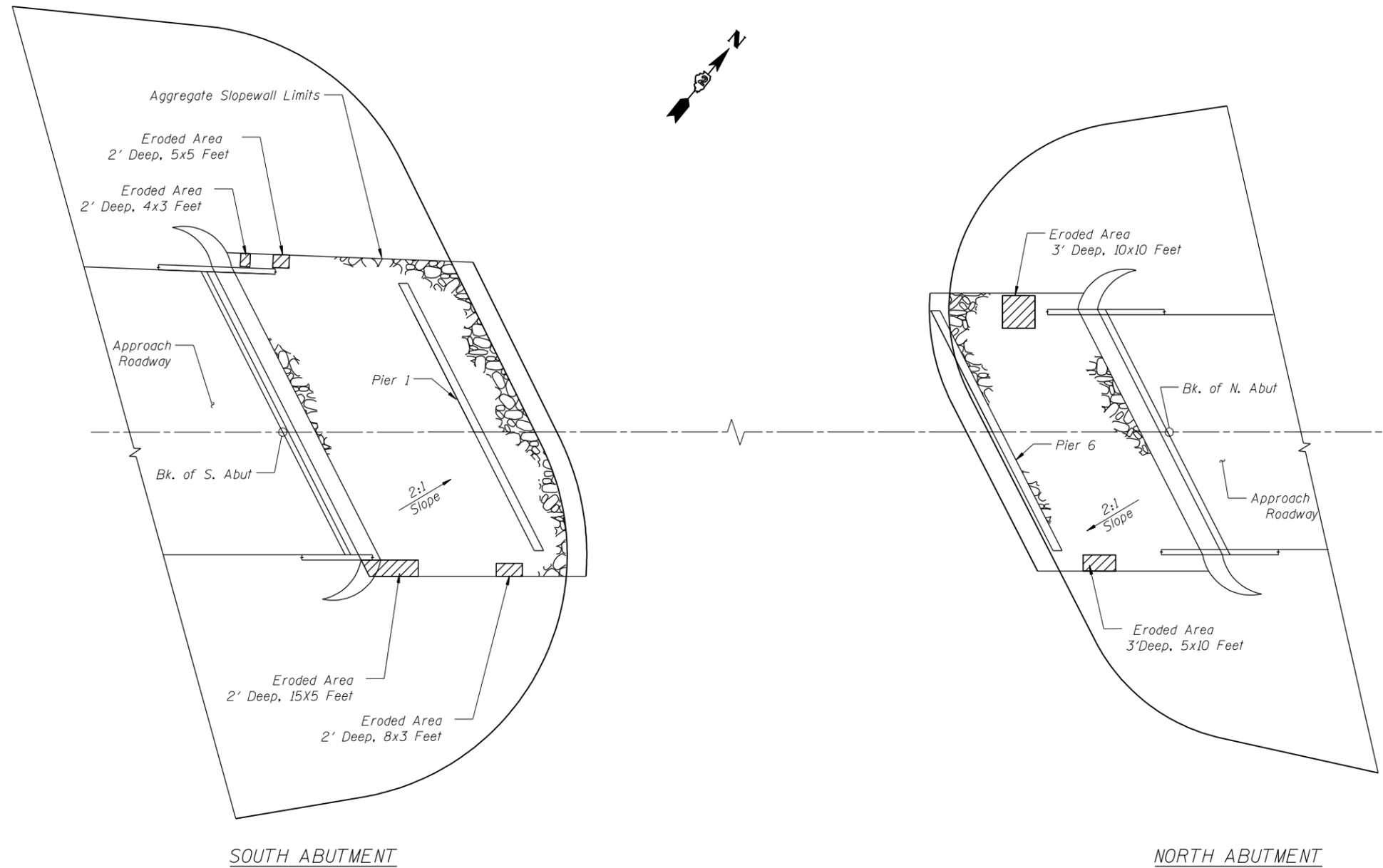
Item	Unit	Quantity
Structural Repair Of Concrete (Depth Greater than 5")	Sq.Ft.	8

Legend:

- Structural Repair of Concrete (Depth Greater than 5")
- Crack (Hairline, Open Crack)

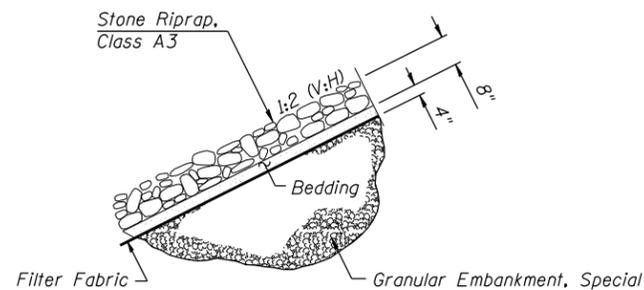
BILL OF MATERIAL

Item	Unit	Quantity
Structural Repair Of Concrete (Depth Greater than 5")	Sq.Ft.	12



REPAIR OF AREAS OF ERODED SLOPEWALLS

1. Contractor is to verify the location and extent of erosion, since the areas may have enlarged after the last date of inspections.
2. All exposed existing filter fabrics shall be cut into large strips without detaching them from the unexposed parts. The strips are then pulled back to reveal eroded embankment underneath.
3. Eroded embankment cavities shall be filled with Granular Embankment Special, according to the Standard Specifications.
4. New Filter Fabric shall then be placed to cover the extent of the exposed areas. The existing old fabric strips are then lapped on top of the new fabric.
5. Bedding material is placed and Stone Riprap Class A3 is then used to restore the slope of the slopewall. See Details of Repair, this sheet.



DETAILS OF REPAIR

BILL OF MATERIALS

Item	Unit	Quantity
Stone Riprap, Class A3	SY	35.0
Filter Fabric	SY	35.0
Granular Embankment, Special	CY	18.6



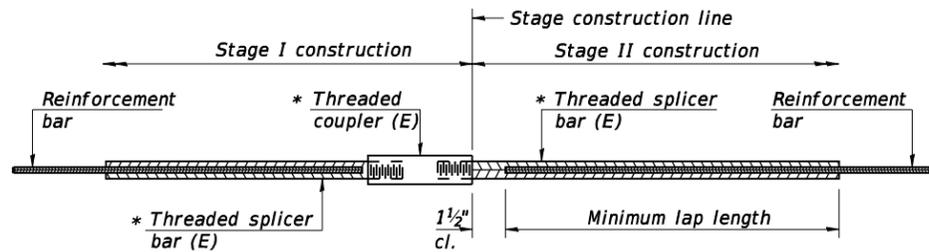
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	DRAWN - SD	REVISED -
PLOT SCALE = 40,0000 1/1	CHECKED - OAO	REVISED -
PLOT DATE = 9/18/2019	DATE - 9/18/2019	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**SLOPEWALL REPAIRS
STRUCTURE NO. 081-0036**

SHEET S15 OF S17 SHEETS

F.A.P. RTE. 300	SECTION (7R-VB)BRR	COUNTY ROCK ISLAND	TOTAL SHEETS 38	SHEET NO. 28
				CONTRACT NO. 64N10
ILLINOIS FED. AID PROJECT				

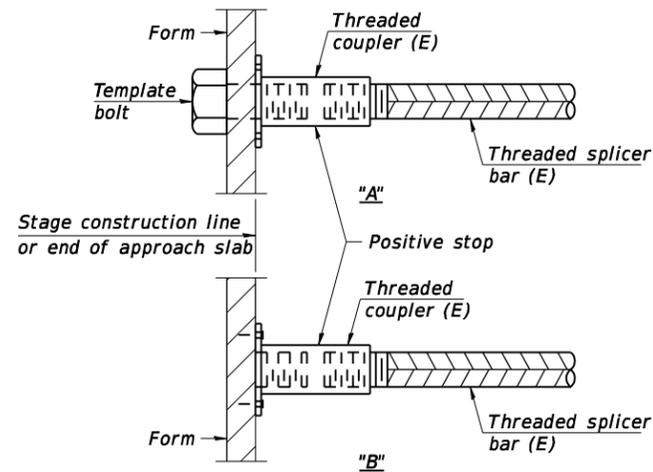


STANDARD BAR SPLICER ASSEMBLY

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

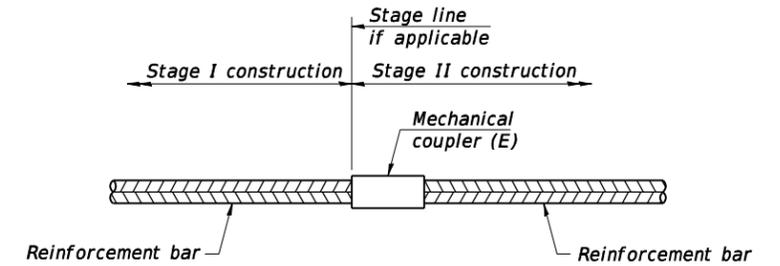
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
Abutment Backwalls	#5	16	3'-9"
Edge of Deck at Abutments	#5	32	3'-9"
Edge of Deck at Pier #3	#5	48	3'-9"



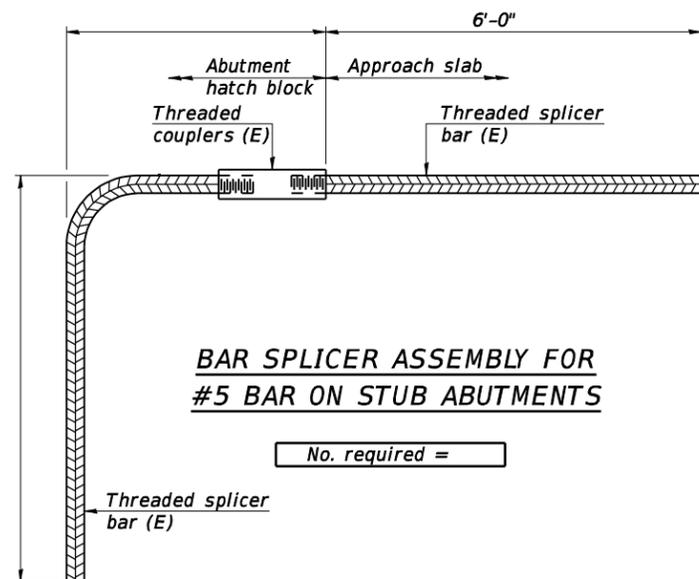
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
 "B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
 (E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required

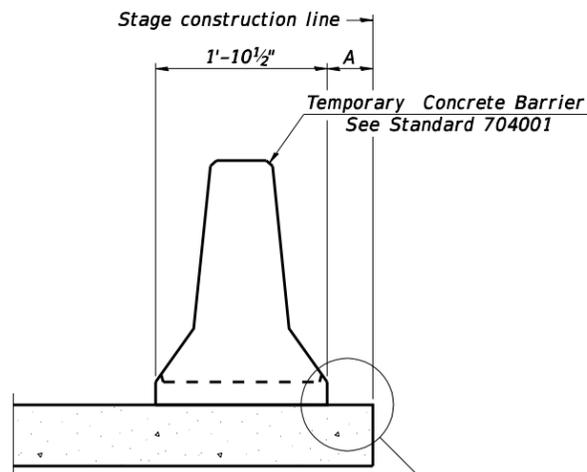


BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

No. required =

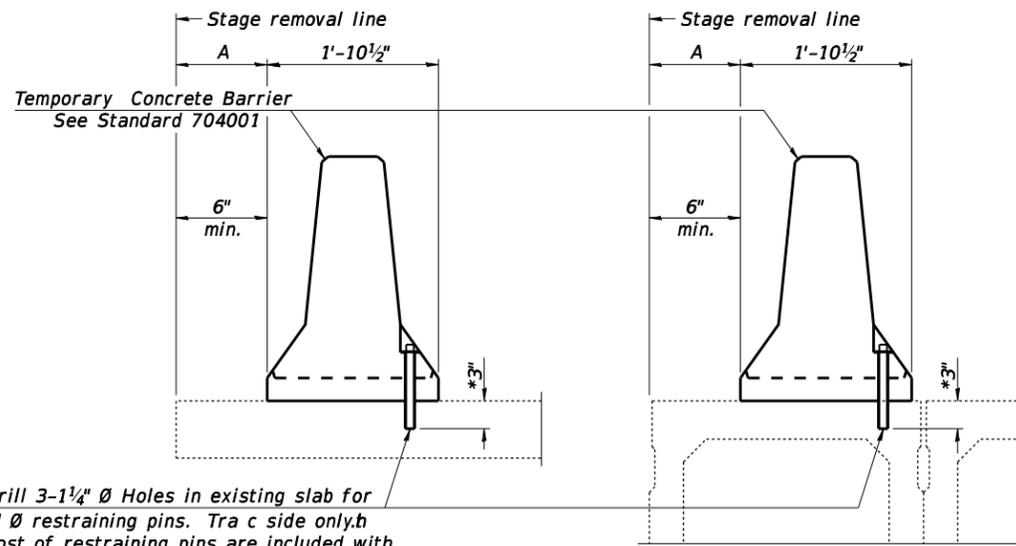
NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
 All reinforcement shall be lapped and tied to the splicer bars.
 Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
 See approved list of bar splicer assemblies and mechanical splicers for alternatives.



When "A" is 3'-1" or less, the temporary concrete barrier shall be restrained to the new slab according to Detail I, II or III. No restraint is required when "A" is greater than 3'-1".

NEW SLAB OR NEW DECK BEAM

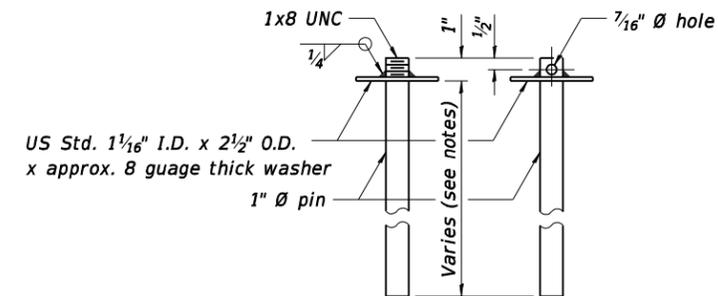


Drill 3-1 1/4" Ø Holes in existing slab for 1" Ø restraining pins. Trace side only. Cost of restraining pins are included with Temporary Concrete Barrier. No restraint is required when "A" is greater than 3'-1".

EXISTING SLAB

EXISTING DECK BEAM

SECTIONS THRU SLAB OR DECK BEAM



RESTRAINING PIN

US Std. 1 1/16" I.D. x 2 1/2" O.D. x approx. 8 gauge thick washer

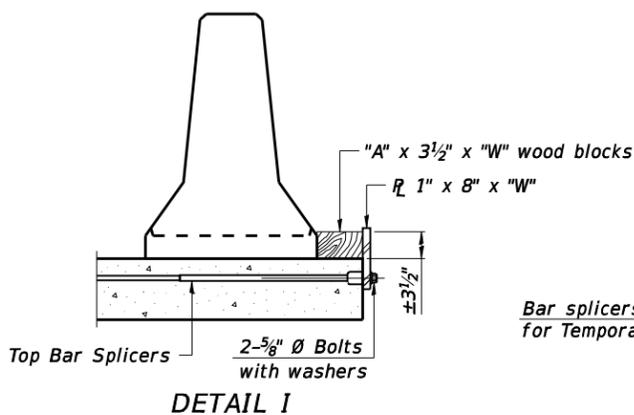
1" Ø pin

1 1/2"

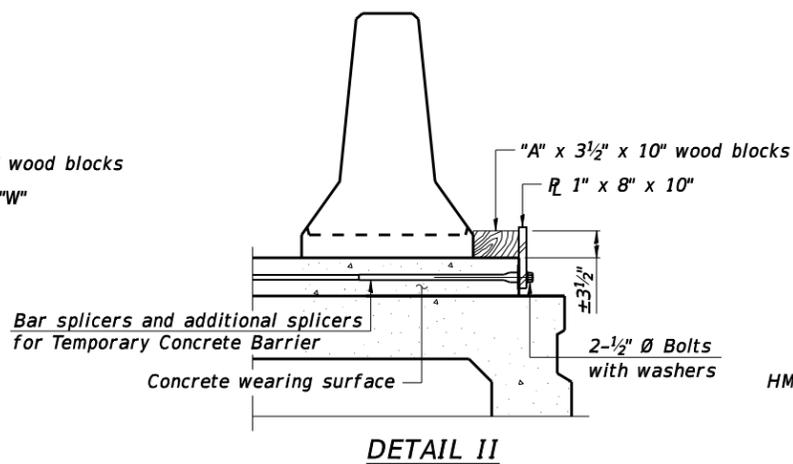
1"

Varies (see notes)

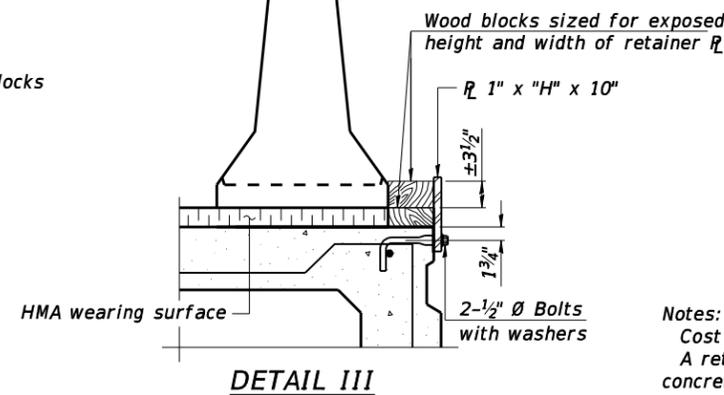
7/16" Ø hole



DETAIL I

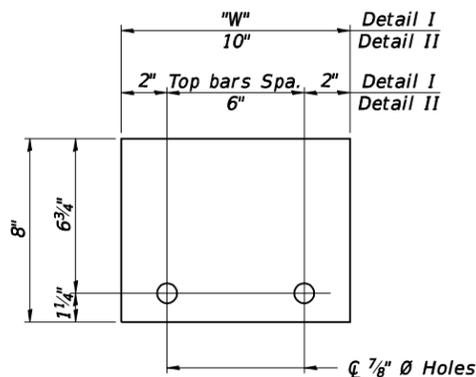
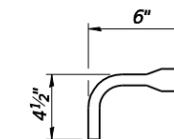


DETAIL II

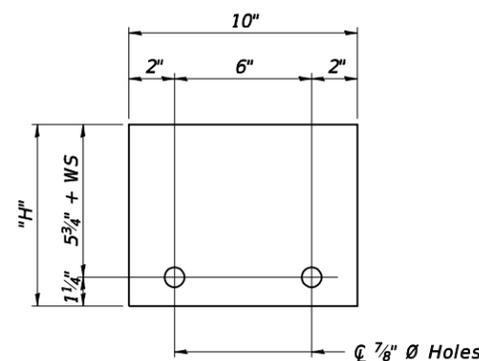


DETAIL III

BAR SPLICER FOR #4 BAR - DETAIL III



STEEL RETAINER R 1" x 8" x "W"
(Detail I and II)



STEEL RETAINER R 1" x "H" x 10"
(Detail III)

Notes:

Cost of retainer assembly is included with Temporary Concrete Barrier. A retainer assembly shall be located at the approximate C of each temporary concrete barrier.

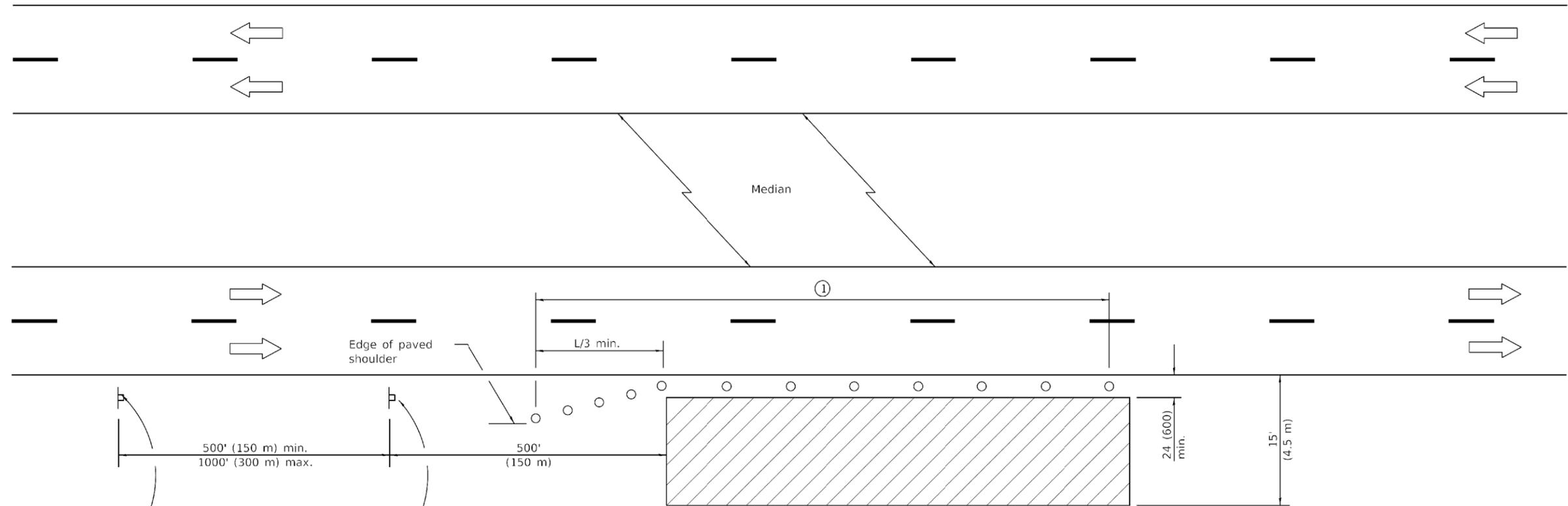
The retainer plate shall not be removed until the concrete on the adjacent stage is ready to be poured. For Detail III applications the retainer plate shall not be removed until just prior to placing the adjacent beam.

When the 'A' dimension is less than 1 1/2", the wood block shall be omitted and the barrier shall be placed in direct contact with the steel retainer plate. For deck beam applications the minimum required 'A' distance is 6" to accommodate the shear key clamping device.

Detail I - Installation for a new bridge deck or bridge slab.

Detail II - Installation for a new deck beam with an initial concrete wearing surface. Additional bar splicers shall be provided at 6'-0" centers and paired with the bar splicers of the concrete wearing surface reinforcement to accommodate the installation of the retainer assemblies. The cost of the additional bar splicers is included with the concrete wearing surface.

Detail III - Installation for a new deck beam with no initial wearing surface or with an initial hot-mix asphalt (HMA) wearing surface present. The deck beam directly beneath the temporary concrete barrier shall be fabricated with bar splicer inserts in the side of the beam, as detailed, to accommodate the installation of the retainer assemblies. A pair of bar splicers, 6" apart, shall be placed at 6'-0" centers along the length of the beam. The cost of the bar splicers is included with the deck beam.



For contract construction projects



W20-1103(0)-48



W21-1(0)-48

For maintenance and utility projects



W20-1(0)-48

TYPICAL APPLICATIONS

- Utility operations
- Culvert extensions
- Side slope changes
- Guardrail installation and maintenance
- Delineator installation
- Landscaping operations
- Shoulder repair
- Sign installation and maintenance

① When the work operation exceeds one hour, cones, drums or barricades shall be placed at 25' (8 m) centers for L/3 distance, and at 50' (15 m) centers through the remainder of the work area.

SYMBOLS

- Work area
- Sign
- Cone, drum or barricade

GENERAL NOTES

This Standard is used where any vehicles, equipment, workers or their activities will encroach in the area 15' (4.5 m) to 24' (600) from the edge of pavement.

Calculate L as follows:

SPEED LIMIT	FORMULAS	
	English	(Metric)
40 mph (70 km/h) or less:	$L = \frac{WS^2}{60}$	$L = \frac{WS^2}{150}$
45 mph (80 km/h) or greater:	$L = (W)(S)$	$L = 0.65(W)(S)$

W = Width of offset in feet (meters).
S = Normal posted speed mph (km/h).

All dimensions are in inches (millimeters) unless otherwise shown.

OFF-RD OPERATIONS, MULTILANE, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE

STANDARD 701101-05

DATE	REVISIONS
4-1-16	Corrected typo in title.
1-1-14	Revised workers sign number to agree with current MUTCD.

Illinois Department of Transportation

PASSED April 1, 2016
ENGINEER OF SAFETY ENGINEERING

APPROVED April 1, 2016
ENGINEER OF DESIGN AND ENVIRONMENT

ISSUED 1-1-97

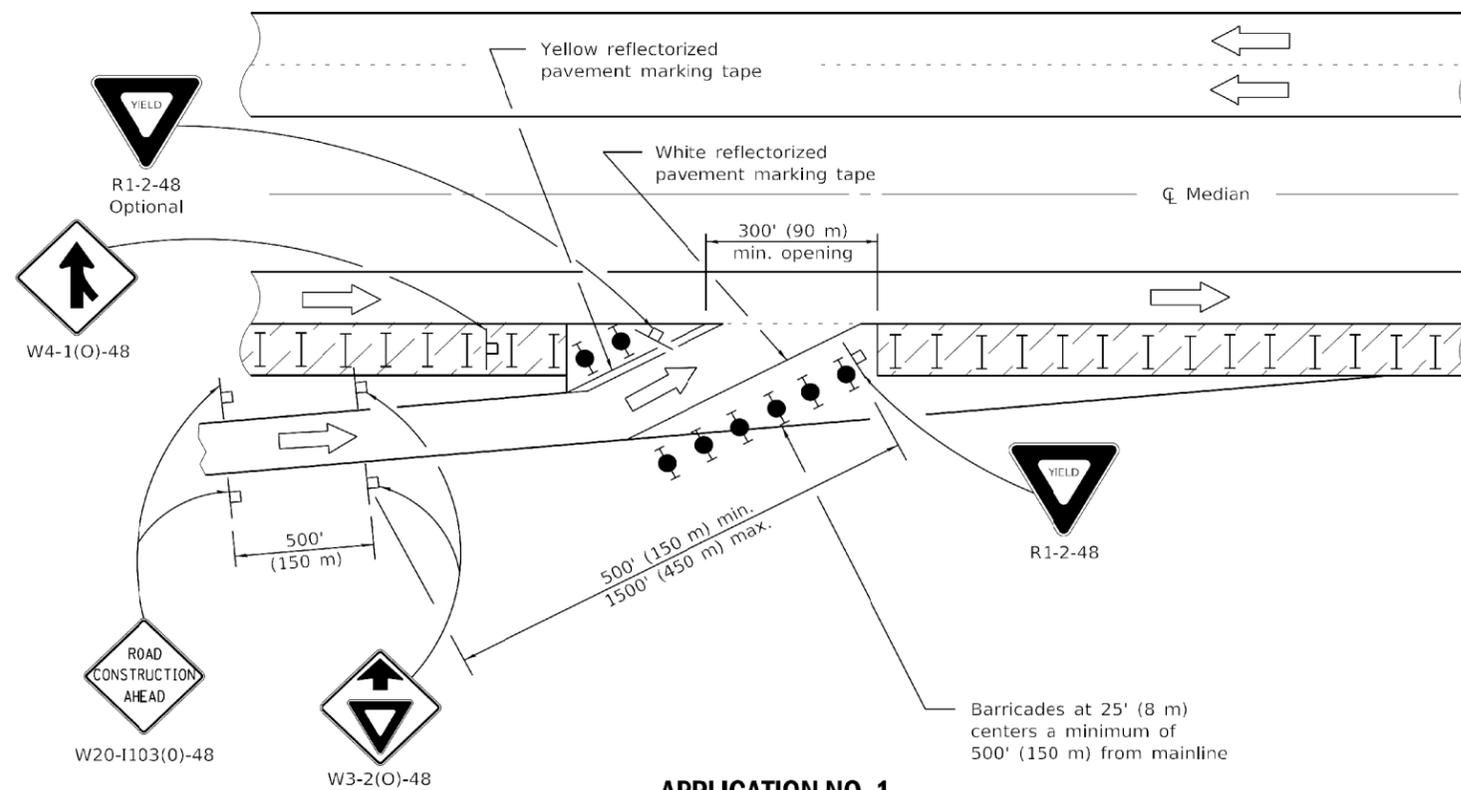
USER NAME = MJKGC07	DESIGNED - AA	REVISED -
PLOT SCALE = 10,0000 "/ IN.	DRAWN -	REVISED -
PLOT DATE = 9/18/2019	CHECKED - OAO	REVISED -
	DATE - 9/18/2019	REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

STANDARD 701101-05

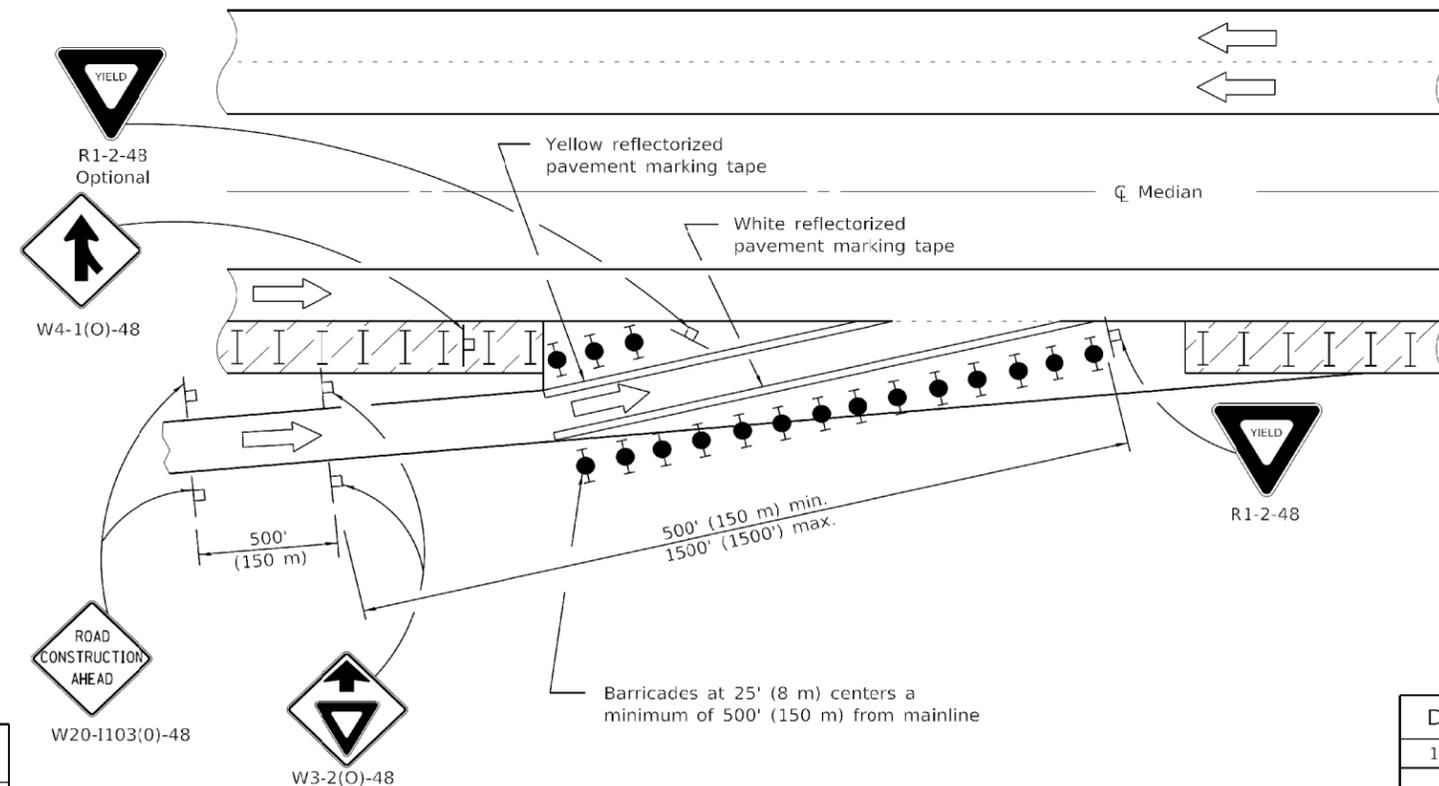
SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE. 300	SECTION (7R-VB)BRR	COUNTY ROCK ISLAND	TOTAL SHEETS 38	SHEET NO. 31
				CONTRACT NO. 64N10
ILLINOIS FED. AID PROJECT				



APPLICATION NO. 1

Application No. 1 depicts a modified entrance ramp. This method shall be utilized whenever existing entrance tapers cannot be retained due to the close proximity of the work zone. The entrance location may be shifted, with the approval of the Engineer, to perform work in the entrance area. Application No. 2 shall be put into effect as soon as possible.



APPLICATION NO. 2

Application No. 2 depicts a shortening of the normal entrance ramp. This method shall be used whenever the existing geometrics can be retained. Consideration should be given to the entering motorists' line of sight, through, between, or over the delineation devices.

- SYMBOLS**
- Work area
 - Sign
 - Type II barricades or drums with steady burning monodirectional light
 - Type II barricades or drums
 - Drums with steady burning monodirectional light

GENERAL NOTES

This Standard is used where, at any time any vehicle, equipment, workers or their activities require a lane closure in close proximity of an exit or entrance ramp and supplements other traffic control Standards for lane closures.

These applications also apply when work is being performed in the left lanes and the ramps enter and exit on the left. Under these conditions, the Exit sign arrow and the Side road symbol sign shall be changed.

Cones may be utilized during daylight operations, at one half the spacing of drums/barricades.

Use of these APPLICATION NO. 1 and APPLICATION NO. 3 shall be limited to five days per location.

When work does not exceed five days, pavement marking tape may be omitted.

All dimensions are in inches (millimeters) unless otherwise shown.

DATE	REVISIONS
1-1-15	Revised gen. notes to limit App's 1 and 3 to five days, omit pvt. tape for ≤ 5 days.
1-1-12	Revised merge sign to agree with MUTCD. Dimensioned EXIT OPEN AHEAD sign.

LANE CLOSURE, MULTILANE, AT ENTRANCE OR EXIT RAMP, FOR SPEEDS ≥ 45 MPH
(Sheet 1 of 2)

STANDARD 701411-09

FILE NAME = 0:\06180201\DWG\266410-01-32- IL 5-STD 701411-09.dgn

Illinois Department of Transportation

PASSED January 1, 2015
Amelia O'Shea
 ENGINEER OF SAFETY ENGINEERING

APPROVED January 1, 2015
[Signature]
 ENGINEER OF DESIGN AND ENVIRONMENT

ISSUED 1-1-97

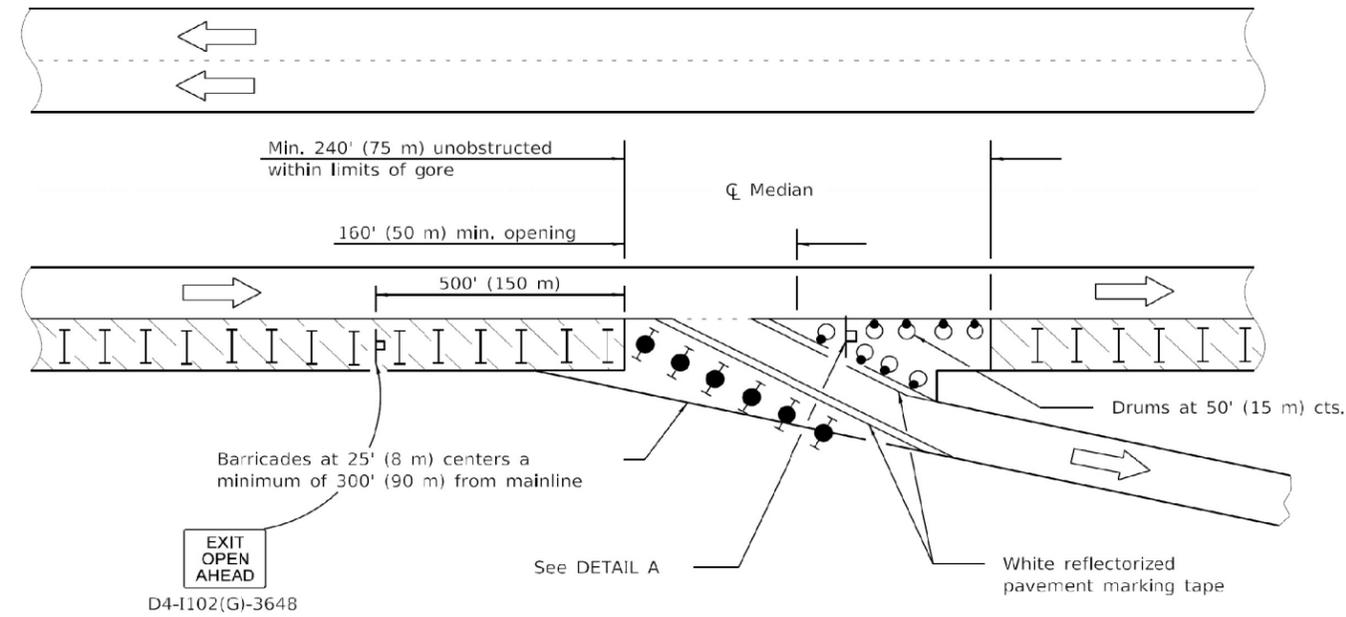
S D I ENGINEERING CORP.
www.sdiengr.com

USER NAME = MJKGC07	DESIGNED - AA	REVISED -
PLOT SCALE = 2.0000" / IN.	DRAWN -	REVISED -
PLOT DATE = 9/18/2019	CHECKED - OAO	REVISED -
	DATE - 9/18/2019	REVISED -

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

STANDARD 701411-09	
SCALE:	SHEET OF SHEETS STA. TO STA.

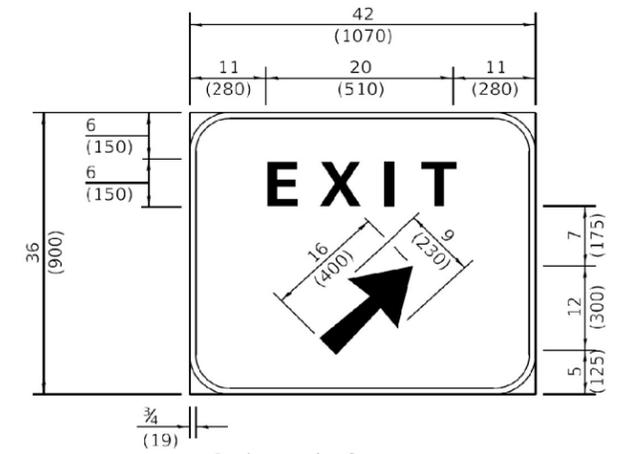
F.A.P. RTE. 300	SECTION (7R-VB)BRR	COUNTY ROCK ISLAND	TOTAL SHEETS 38	SHEET NO. 32
CONTRACT NO. 64N10				
ILLINOIS FED. AID PROJECT				



D4-1102(G)-3648

APPLICATION NO. 3

Application No. 3 depicts a modified exit ramp. The channelizing devices shall provide a clearly defined path for the exiting motorists. The minimum dimensions shown shall be increased as soon as the progress of the work will permit. The open portion of the ramp may be shifted, with the approval of the Engineer, to perform work in stages on the area adjacent to the ramp exit. Application No. 4 shall be put into effect as soon as possible.

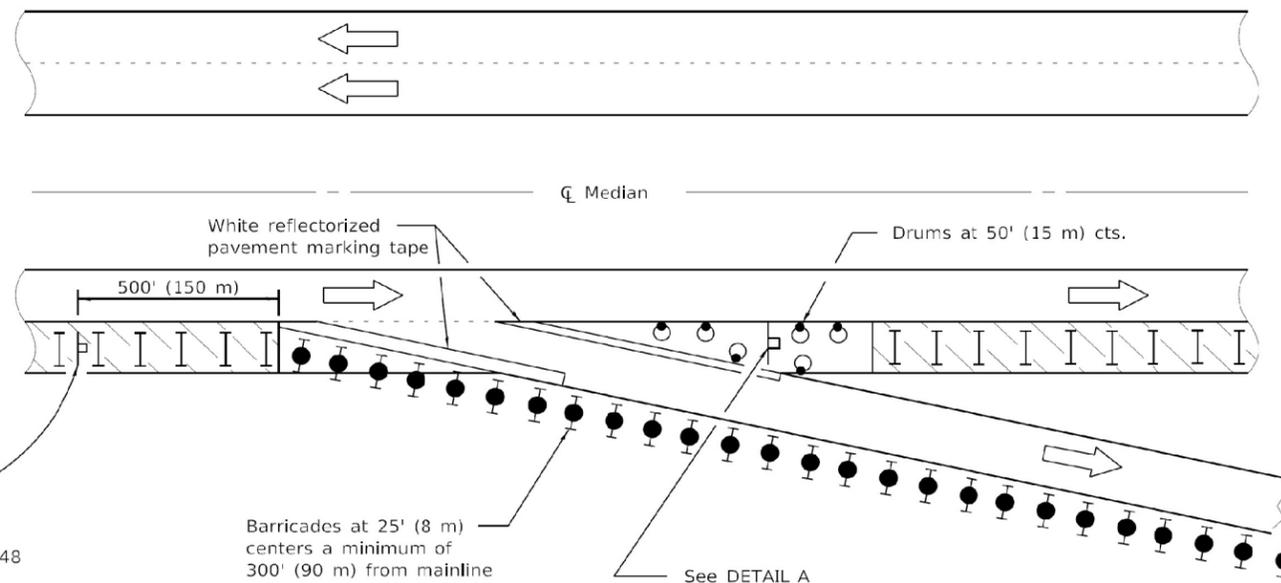


Background - Green
Border and legend - White
"D" size letters

EXIT SIGN - SPECIAL

DETAIL A

(To be utilized where distance between the two rows of channelizing devices is 6' (1.8 m) in width.)



D4-1102(G)-3648

APPLICATION NO. 4

Application No. 4 depicts an extension of the normal exit ramp. This method shall be used whenever existing geometrics can be retained. Consideration should be given to the exiting motorist's line of sight through, between or over the delineation devices.

**LANE CLOSURE, MULTILANE,
AT ENTRANCE OR EXIT RAMP,
FOR SPEEDS ≥ 45 MPH**

(Sheet 2 of 2)

STANDARD 701411-09

FILE NAME = 0:\C0618C20\DWG\026410-INT-33- IL 5-STD 701411-09.dgn

Illinois Department of Transportation

PASSFD January 1, 2015
ENGINEER OF SAFETY ENGINEERING

APPROVED January 1, 2015
ENGINEER OF DESIGN AND ENVIRONMENT

ISSUED 1.1.97

S D I ENGINEERING CORP.
www.sdiengr.com

USER NAME = MJKGC07	DESIGNED - AA	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 2.0000' / IN.	CHECKED - OAO	REVISED -
PLOT DATE = 9/18/2019	DATE - 9/18/2019	REVISED -

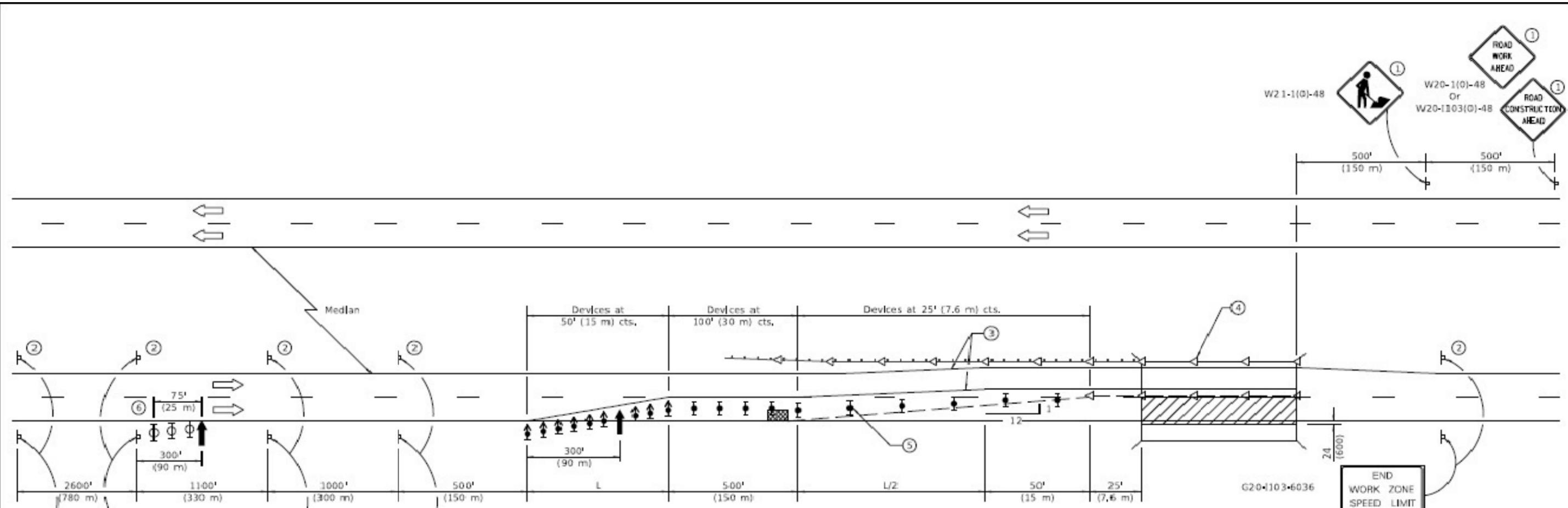
**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

STANDARD 701411-09

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
300	(7R-VB)BRR	ROCK ISLAND	38	33
CONTRACT NO. 64N10				
ILLINOIS FED. AID PROJECT				

FILE NAME = 0:\06180201\DWG\0264109-141-34- IL 5-STD 701423-10.dgn



W20-1103(0)-48
W20-5(0)-48
W4-2R(0)-48

For contract construction projects

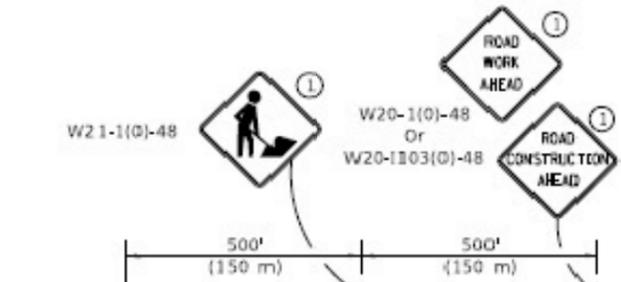
W20-1(0)-48
For maintenance and utility projects

WORK ZONE W21-1115(0)-3618
SPEED LIMIT R2-1-3648
45
PHOTO ENFORCED R10-1108p-3618
\$300 FINE MINIMUM R2-1106p-3618

SYMBOLS

↑ Arrow board
▨ Work area
T Sign
D Direction Indicator barricade with steady burn monodirectional light
I Type II barricade, drum, or vertical barricade with steady burn monodirectional light
--- Temporary concrete barrier
◁ Monodirectional guardrail/barrier wall reflector
▩ Impact attenuator
D Type II barricade, drum, or vertical barricade with monodirectional flashing light

- ① Undivided roadway only with left lane closure in opposite direction.
- ② Sign in median may be omitted when median is less than 10' (3 m).
- ③ Temporary pavement marking tape shall be placed throughout the taper and along side the work area. The right edge line shall be white and the left edge line shall be yellow.
- ④ Guardrail/barrier wall reflectors at 25' (7.6 m). Markers on right shall be crystal and markers on left shall be amber. See Standards 704001 and 782006.
- ⑤ Vertical barricades shall not be used in lane shift taper.
- ⑥ Three Type II barricades, drums, or vertical barricades at 25' (8 m) centers.



G20-103-6036
END WORK ZONE SPEED LIMIT

GENERAL NOTES

This standard is used where at any time any vehicle, equipment, workers or their activities will encroach on the pavement or on the shoulder within 24 (600) of the edge of pavement for daylight operation exceeding one day and where temporary concrete barrier is utilized.

When work is being performed in the left lane, the set up would be a mirror image to what is shown.

Calculate L as follows:

NORMAL POSTED SPEED	FORMULAS	
	English	(Metric)
45 mph (80 km/h) or more	$L=(W)(S)$	$L=0.65(W)(S)$

W = Width of offset in feet (meters).

S = Normal posted speed in mph (km/h).

All dimensions are in inches (millimeters) unless otherwise shown.

Illinois Department of Transportation

PASSED January 3, 2017
ENGINEER OF SAFETY PROGS. AND ENGINEERING

APPROVED January 1, 2017
ENGINEER OF DESIGN AND ENVIRONMENT

ISSUED 5-1-00

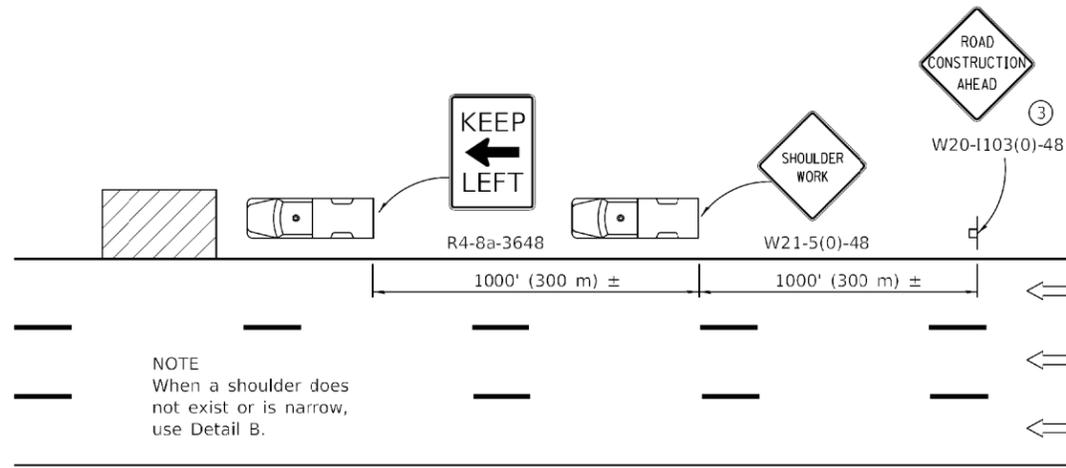
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DRAWN -	REVISIONS -	
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PLOT DATE = 9/18/2019	DATE - 9/18/2019	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

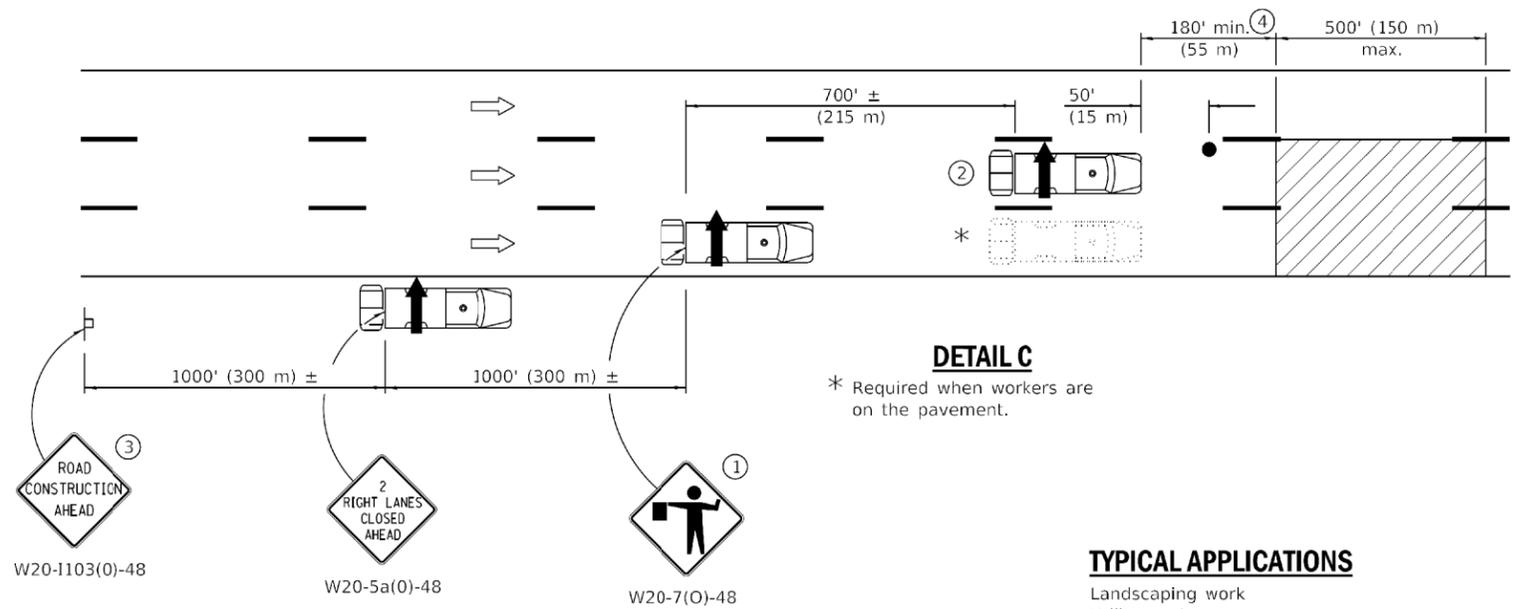
DATE	REVISIONS
1-1-17	Revised END WORK ZONE SPEED LIMIT sign. Changed device spacing at first arr. brd.
4-1-16	Corrected reference to standard in note (4).

LANE CLOSURE, MULTILANE, WITH BARRIER, FOR SPEEDS ≥ 45 MPH TO 55 MPH

STANDARD 701423-10



DETAIL A

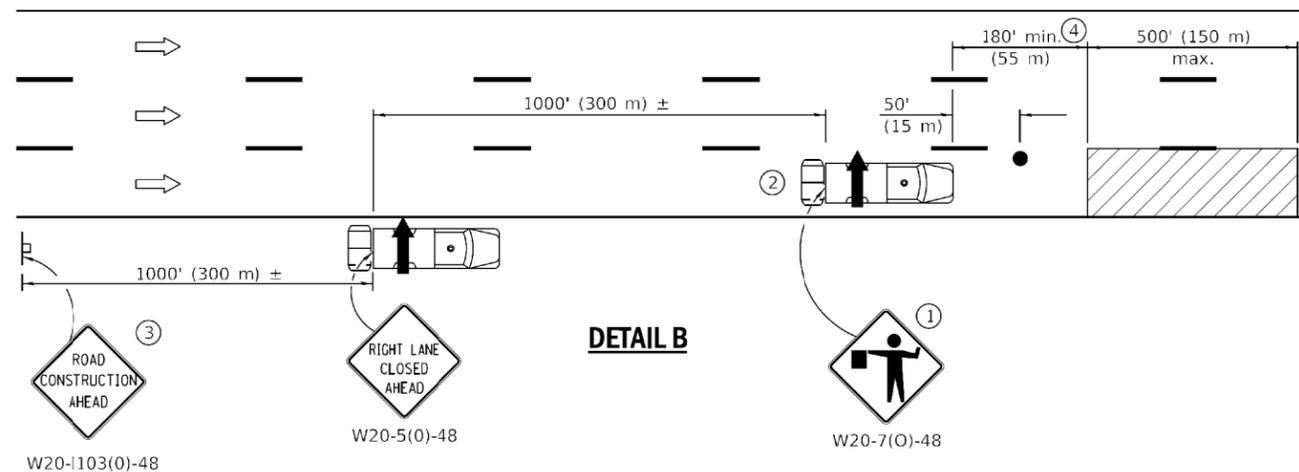


DETAIL C

* Required when workers are on the pavement.

TYPICAL APPLICATIONS

- Landscaping work
- Utility work
- Pavement marking
- Weed spraying
- Roadometer measurements
- Debris cleanup
- Crack pouring



DETAIL B

- ① Flaggers are required when workers are on the pavement.
- ② For striping operations only. See sign arrow detail on this standard.
- ③ For stationary operations which are on the roadway or shoulder, greater than 15 minutes and up to 1 hour.
- ④ The distance between the work and the lead truck may vary according to terrain or paint/crack sealing drying time.



G20-1101-2430 (appropriate arrow) ② (when striping only)

GENERAL NOTES

This Standard is used where any vehicle, equipment, workers or their activities will require: 1) stationary operations up to 1 hour, or 2) a continuous or intermittent moving operation where the average speed of movement is greater than 1 mph (2 km/h).

This Standard is also applicable when work is being performed in the left lane(s) or on the median shoulder. Under these conditions, KEEP RIGHT signs shall be substituted for KEEP LEFT signs and arrow board indications shall be directed to the right.

All dimensions are in inches (millimeter) unless otherwise shown.

SYMBOLS

- ↑ Arrow board
- ▨ Work area
- Truck with flashing amber light
- Truck/Trailer mounted attenuator
- Flagger with traffic control sign
- Sign

DATE	REVISIONS
1-1-17	Revised 'NOTE' on DETAIL A to use DETAIL B in lieu of DETAIL C.
4-1-16	Added trailer option for attenuator symbol. Added note ④. Revised gen. notes.

LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS ≥ 45 MPH

STANDARD 701426-09

Illinois Department of Transportation

PASSED January 1, 2017
Paul J. ...
 ENGINEER OF SAFETY PROG. AND ENGINEERING

APPROVED January 1, 2017
Maureen M. ...
 ENGINEER OF DESIGN AND ENVIRONMENT

ISSUED 1-1-97

USER NAME = MJKGC07	DESIGNED - AA	REVISED -
PLOT SCALE = 10,0000 "/ IN.	DRAWN -	REVISED -
PLOT DATE = 9/18/2019	CHECKED - OAO	REVISED -
	DATE - 9/18/2019	REVISED -

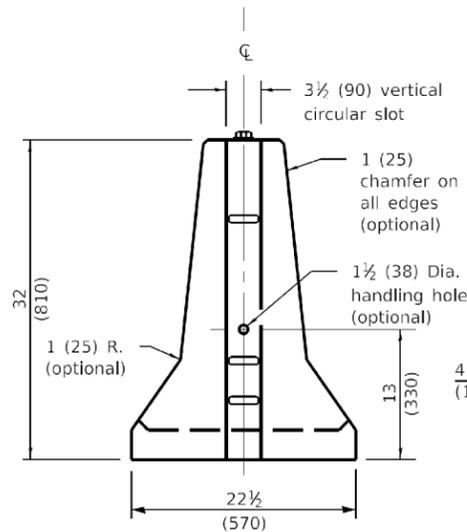
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

STANDARD 701426-09

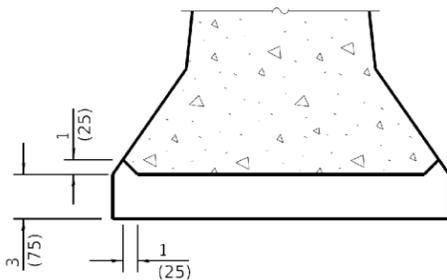
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F.A.P. RTE. 300	SECTION (7R-VB)BRR	COUNTY ROCK ISLAND	TOTAL SHEETS 38	SHEET NO. 35
CONTRACT NO. 64N10				
ILLINOIS FED. AID PROJECT				

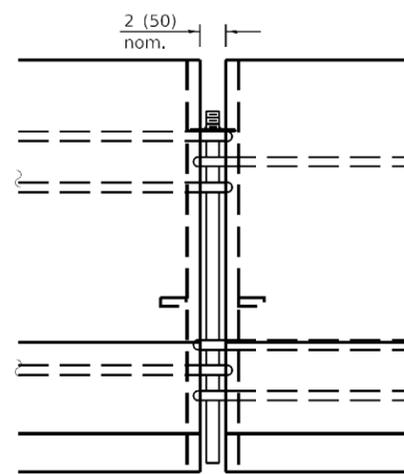
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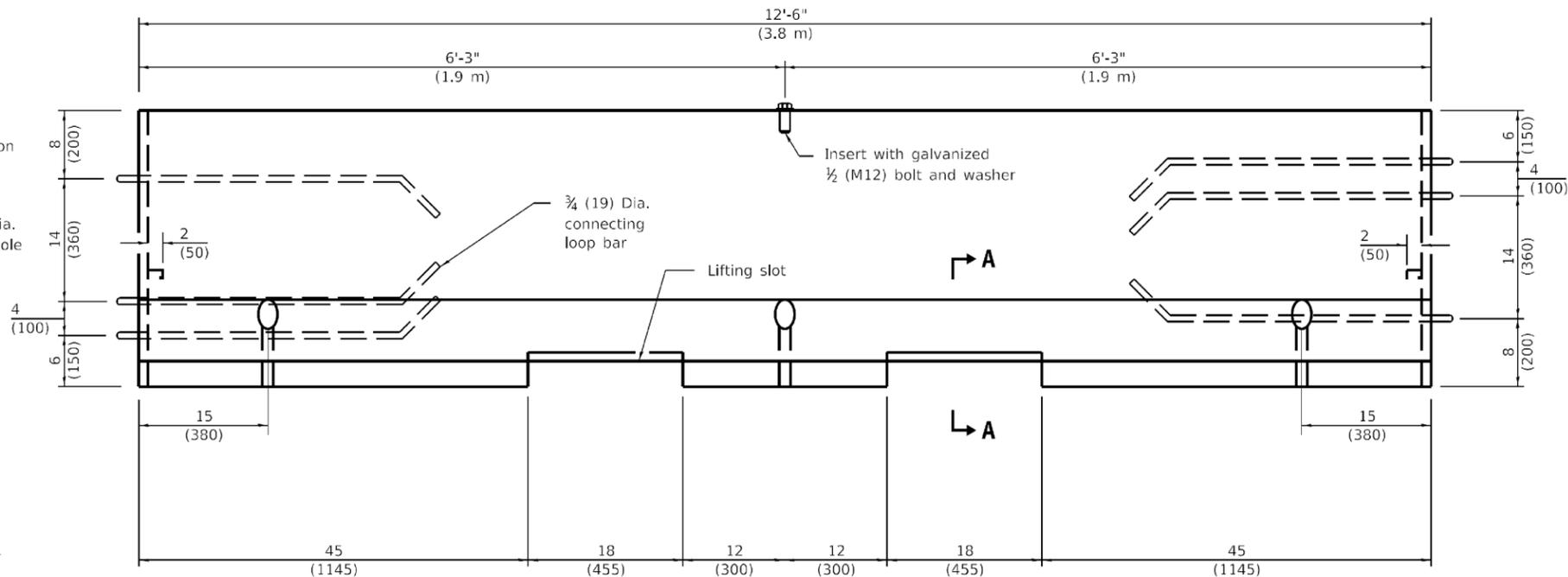
END VIEW
(Showing lifting slot)



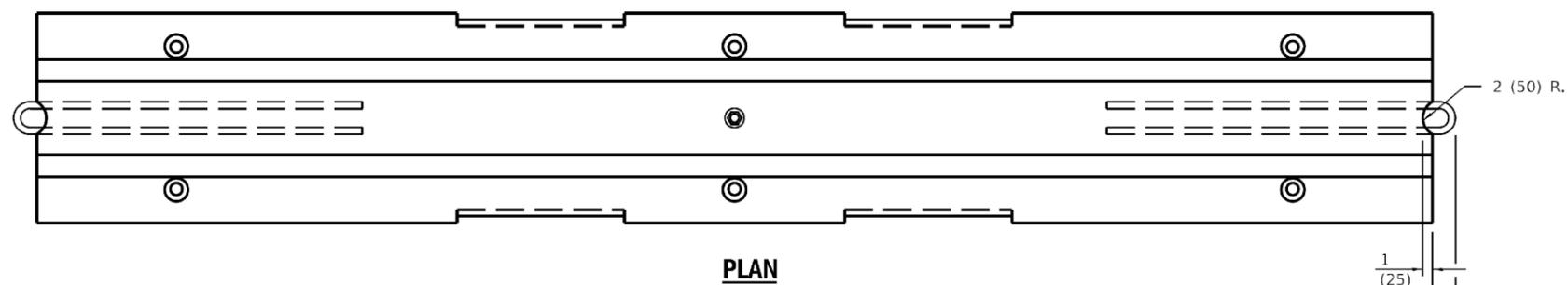
SECTION A-A
LIFTING SLOT



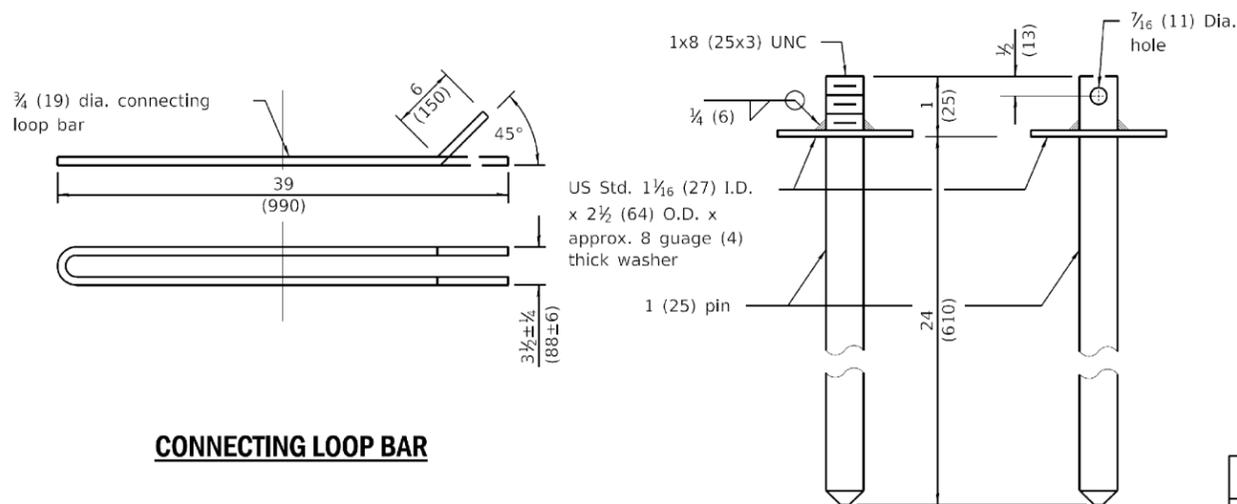
CONNECTING DETAIL



ELEVATION
(Showing connecting loop bars and vertical panel bolt/insert)



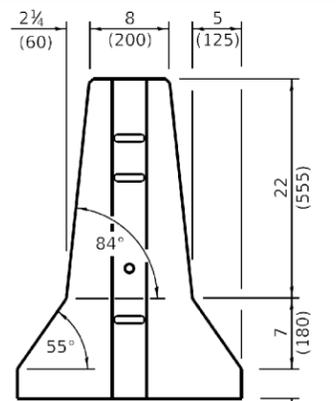
PLAN



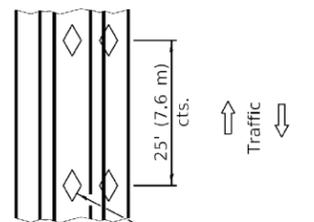
CONNECTING LOOP BAR

CONNECTING AND ANCHOR PINS
(End may be beveled 1/4 (6) max.)

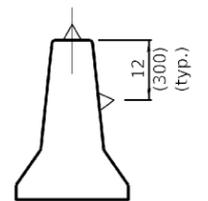
F SHAPE DESIGN



END VIEW



TOP VIEW



BARRIER WALL REFLECTORS

GENERAL NOTES

Each F shape barrier shall be clearly marked with "ILLINOIS F SHAPE", the Producer's mark and the date of manufacture. The markings shall be indented on the barrier or painted thereon with waterproof paint/ink.

The insert for the 1/2 (M12) bolt shall be capable of 3,000 lb (13 kN) pull-out strength.

When barrier separates opposing flows of traffic markers shall be on both sides of barrier.

See Standard 782006 for dimensions of Type C reflector.

All dimensions are in inches (millimeters) unless otherwise shown.

DATE	REVISIONS
4-1-16	Rev. opt. chamfer on all edges to 1 (25). Reference to Std. 635011 now 782006.
1-1-12	Omitted 'ALTERNATE' from connecting and anchoring pins detail.

TEMPORARY CONCRETE BARRIER

(Sheet 1 of 2)

STANDARD 704001-08

Illinois Department of Transportation

PASSED April 1, 2016
Michael Beard
ENGINEER OF POLICY AND PROCEDURES

APPROVED April 1, 2016
ENGINEER OF DESIGN AND ENVIRONMENT

ISSUED 10-1-02

USER NAME	DESIGNED	REVISIONS
MJKGC07	AA	-
	DRAWN	-
	CHECKED	-
	DATE	-

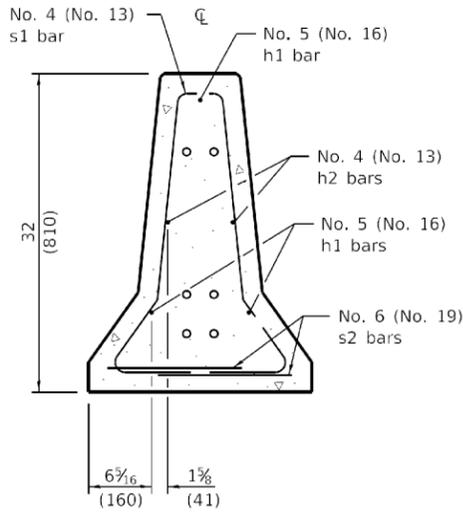
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

STANDARD 704001-08

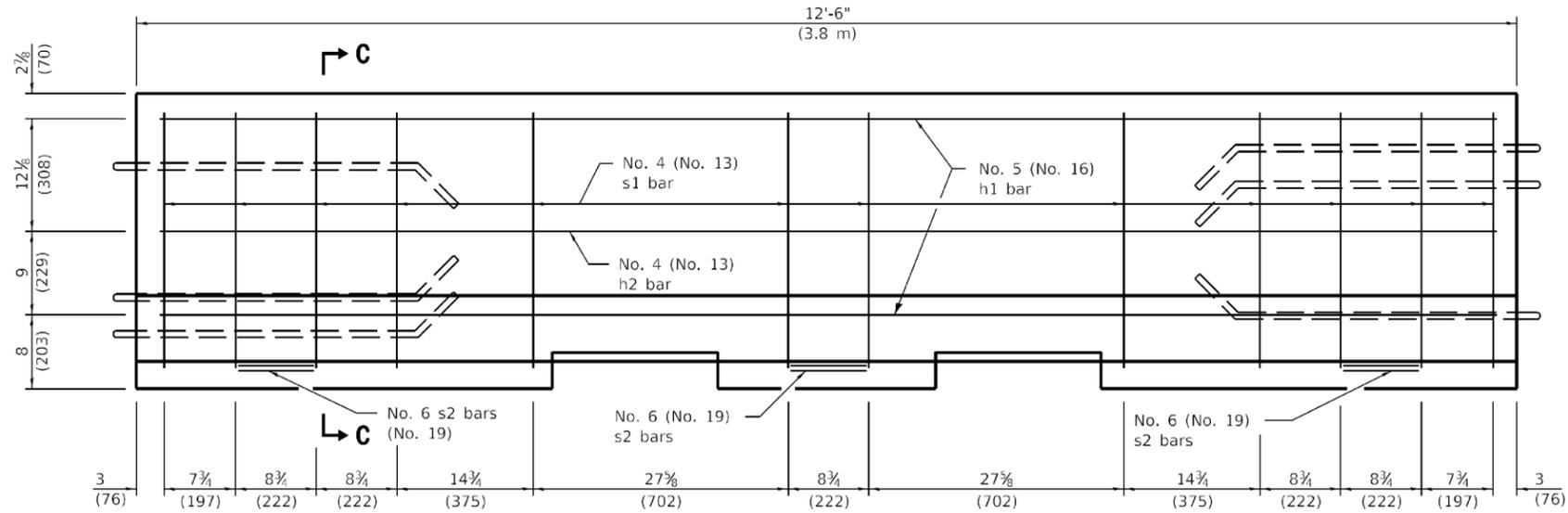
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CONTRACT NO. 64N10				

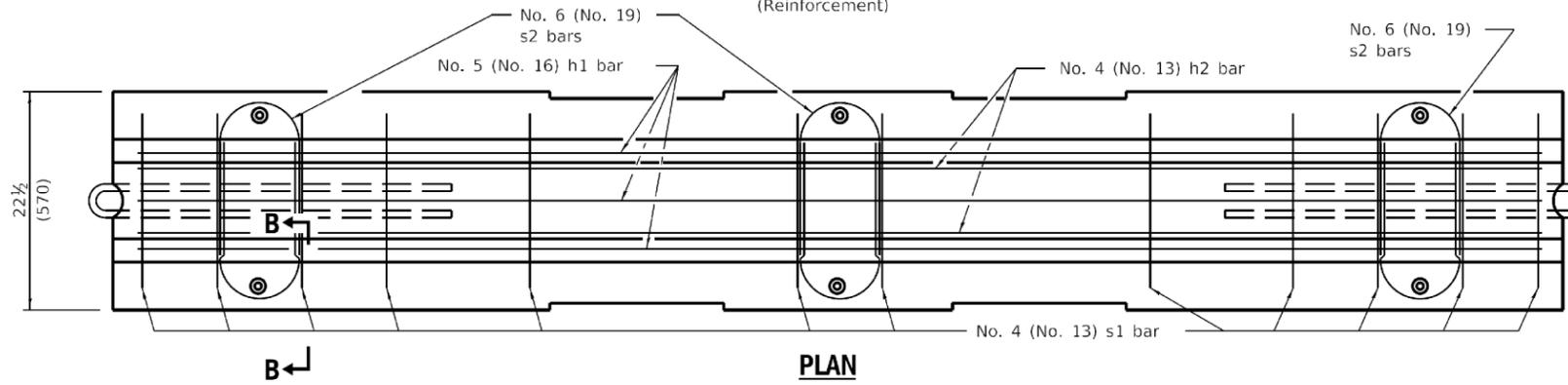
ILLINOIS FED. AID PROJECT



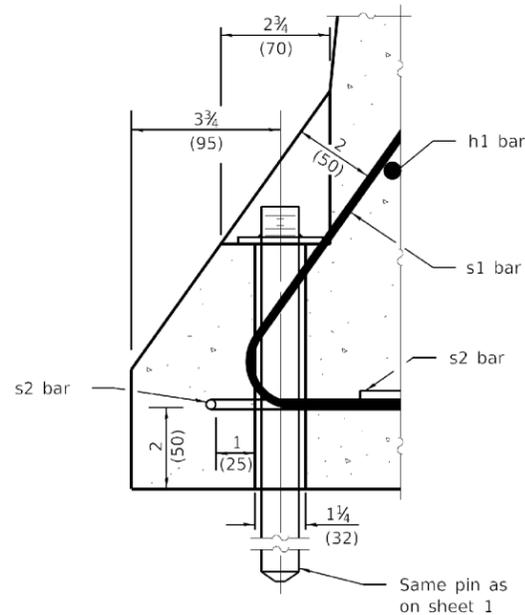
SECTION C-C



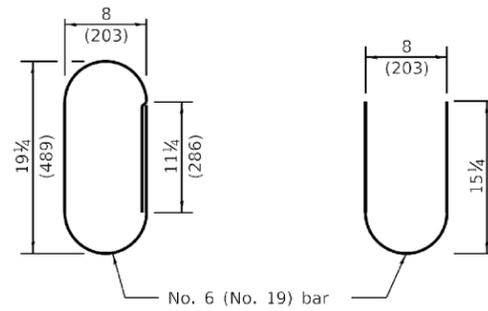
ELEVATION
(Reinforcement)



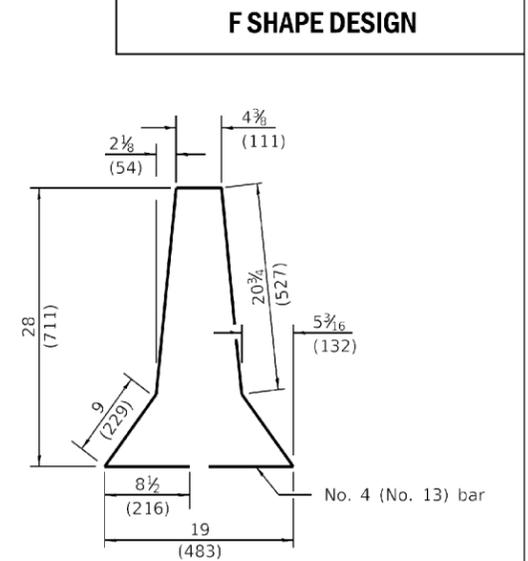
PLAN



SECTION B-B
ANCHORING DETAIL

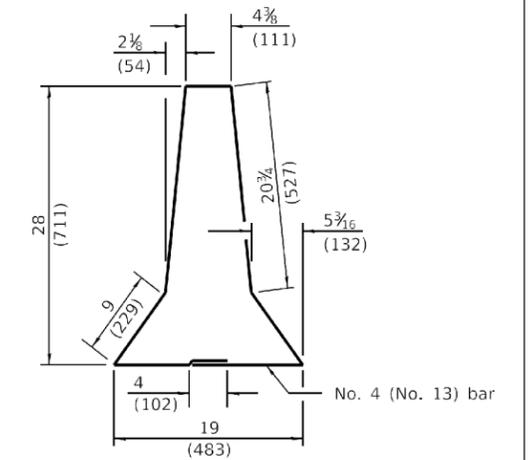


ALTERNATE s2 BARS



F SHAPE DESIGN

s1 BAR



ALTERNATE s1 BAR

TEMPORARY CONCRETE BARRIER

(Sheet 2 of 2)

STANDARD 704001-08

Illinois Department of Transportation
 PASSED April 1, 2016
 Michael Brand
 ENGINEER OF POLICY AND PROCEDURES
 APPROVED April 1, 2016
 ENGINEER OF DESIGN AND ENVIRONMENT

USER NAME = MJKGC07	DESIGNED - AA	REVISED -
	DRAWN -	REVISED -
PLOT SCALE = 10,0000 ' / IN.	CHECKED - OAO	REVISED -
PLOT DATE = 9/18/2019	DATE - 9/18/2019	REVISED -

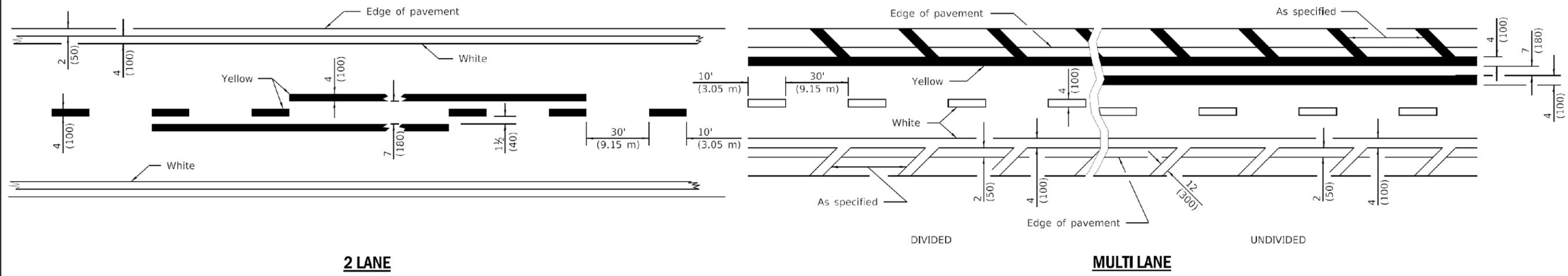
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STANDARD 704001-08

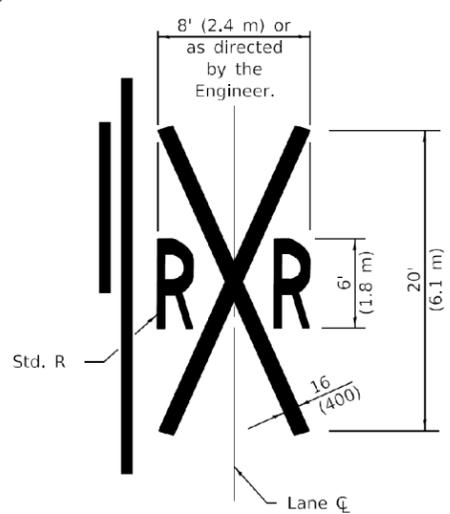
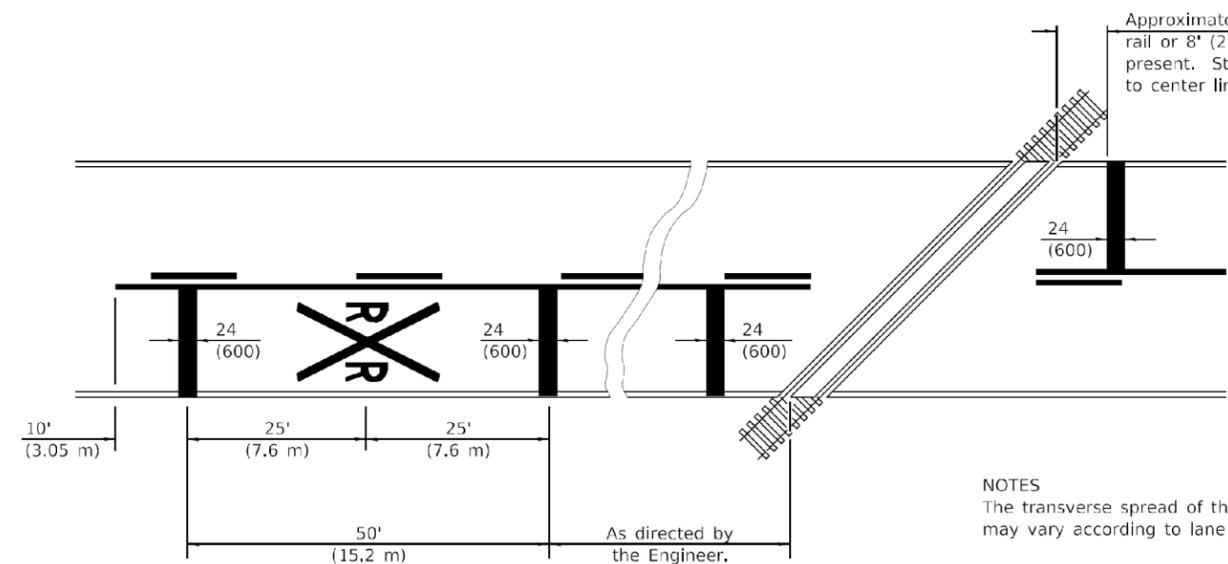
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F.A.P. RTE. 300	SECTION (7R-VBIBRR)	COUNTY ROCK ISLAND	TOTAL SHEETS 38	SHEET NO. 37
CONTRACT NO. 64NIO				
ILLINOIS FED. AID PROJECT				

FILE NAME = Q:\06180201\DWG\0264NIO-std-37-IL-5-STD 704001-08_2.dwg



LANE AND EDGE LINES



NOTES
 The transverse spread of the "X" may vary according to lane width.
 On multi-lane roads, the stop lines shall extend across all approach lanes and separate RXR symbols shall be placed adjacent to each other in each lane.
 When the pavement marking symbol is used, a portion of the symbol should be located directly adjacent to the Advance Warning Sign (W10-1) as placed by Table 2C-4, Condition B of the MUTCD.

PAVEMENT MARKINGS AT RAILROAD-HIGHWAY GRADE CROSSING

All dimensions are in inches (millimeters) unless otherwise shown.

Illinois Department of Transportation

PASSED January 1, 2015
 ENGINEER OF OPERATIONS
 APPROVED January 1, 2015
 ENGINEER OF DESIGN AND ENVIRONMENT

DATE	REVISIONS
1-1-15	Added symbols. Revised bike symbol. Revised note for stop line at RR crossing.
1-1-14	Added bike symbol. Renamed 'LANE DROP ARROW' detail to 'LANE-REDUCTION ARROW'.

TYPICAL PAVEMENT MARKINGS
 (Sheet 1 of 3)
STANDARD 780001-05

FILE NAME = 0:\V0618201\DWG\226410-INT-38-IL-5-STD 780001-05.dgn

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STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

STANDARD 780001-05

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F.A.P. RTE. 300	SECTION (7R-VB)BRR	COUNTY ROCK ISLAND	TOTAL SHEETS 38	SHEET NO. 38
CONTRACT NO. 64N10				
ILLINOIS FED. AID PROJECT				