

CHICAGO EXECUTIVE AIRPORT WHEELING/PROSPECT HEIGHTS, ILLINOIS

CONSTRUCTION PLANS FOR CHICAGO EXECUTIVE AIRPORT REHABILITATE EAST QUADRANT GENERAL AVIATION APRON



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JOINT UTILITY LOCATING
INFORMATION FOR EXCAVATORS
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THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE. SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

ILLINOIS PROJECT: PWK-4262
S.B.G. PROJECT: 3-17-0018-B49

DATE : NOVEMBER 15, 2013

PROJECT INFORMATION	
CONTRACTOR:	
RESIDENT ENGINEER:	
ORIGINAL CONTRACT AMOUNT:	
FINAL CONSTRUCTION COST:	
IDOT LETTING DATE:	
IDOT AWARD DATE:	
NOTICE TO PROCEED:	
START OF CONSTRUCTION:	
SUBSTANTIAL COMPLETION:	
LOCAL AGENCY CONTACT INFORMATION	
VILLAGE OF WHEELING -	847.459.2600
CITY OF PROSPECT HEIGHTS -	847.398.6070
ENGINEER'S PROJECT PERMIT LOG	
NPDES #	
FAA AIRSPACE #	
CDD LPC-663 DATED	11/16/12
MWRDGC PERMIT #	03-246 & RL 09-063
VILLAGE APP FOR CONSTRUCTION PERMIT #	
VILLAGE FLOODPLAIN PERMIT #	
CONTRACTORS REGISTRATION WITH VILLAGE	
VILLAGE SITE ALTERATION PERMIT #	
CITY APPLICATION FOR PERMIT #	
CITY FLOODPLAIN PERMIT #	
CITY SITE GRADING PERMIT #	
CONTRACTORS REGISTRATION WITH CITY	

CHICAGO EXECUTIVE AIRPORT

TOWNSHIP: 42 NORTH WHEELING TOWNSHIP
RANGE: 11 EAST (SECTION: 13)
COOK COUNTY

CMT 12290-07
CRAWFORD MURPHY & TILLY, INC.
CONSULTING ENGINEERS

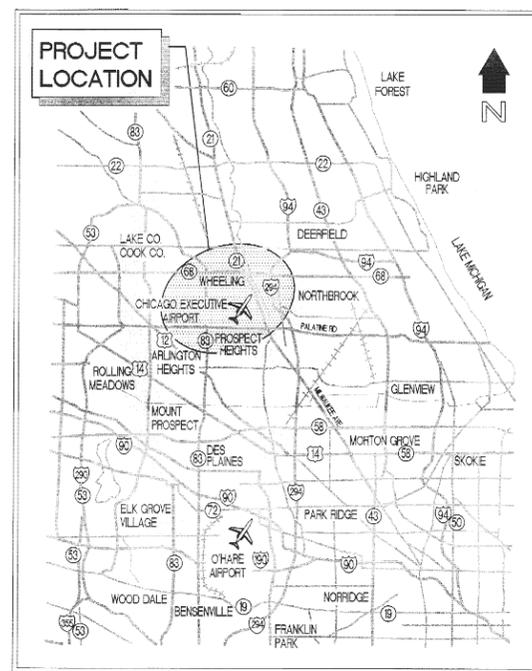
SUBMITTED BY *J.R. Tinkf*
JEREMY R. TINKF, P.E.

DATE 1.29.13

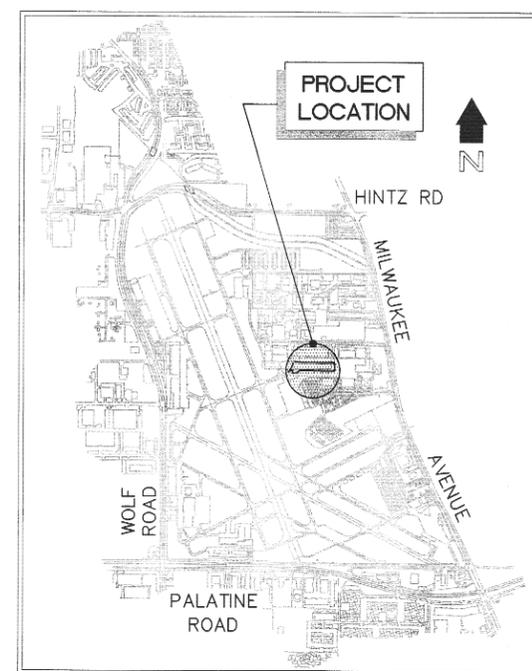
CHICAGO EXECUTIVE AIRPORT

APPROVED *Dennis G. Rouleau* AIRPORT MANAGER
DENNIS G. ROULFAU

DATE 1/31/13



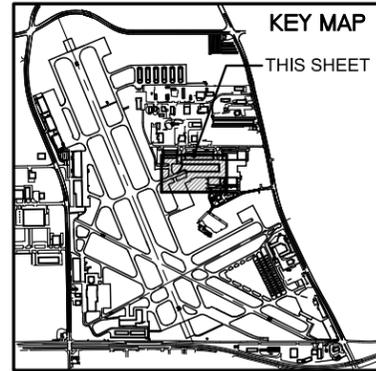
LOCATION MAP



SITE PLAN

INDEX TO SHEETS	
1	COVER SHEET
2	SUMMARY OF QUANTITIES
3	SITE PLAN - PROJECT CONTROL PLAN
4	SEQUENCE OF CONSTRUCTION PER AC 150-5370-2F (LATEST EDITION)
5	SEQUENCE OF CONSTRUCTION GENERAL NOTES
6	STORM WATER POLLUTION PREVENTION PLAN
7	STORM WATER POLLUTION PREVENTION PLAN NOTES AND DETAILS
8	EXISTING CONDITIONS - PROPOSED REMOVALS
9	TYPICAL SECTIONS
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19	CROSS SECTIONS - SHEET 1
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21	ENGINEERING INFORMATION

DATE: Friday, November 15, 2013, 2:41:29 PM
 FILE: K:\Chicago\12290-07-00_Rehab_East_Quad_Apron\Draw_Sheets\rehab-joining.dwg
 UPDATE BY: Jeremy Linke
 LAYOUT: 10_PAVEMENT_JOINING_PLAN
 IMAGE FILES: pwknewLOGO_GEA-4_Color-Std.dwg (2).ppt
 XREF DWG: 10_PAVEMENT_JOINING_PLAN.dwg
 REVISION: 07-00-00

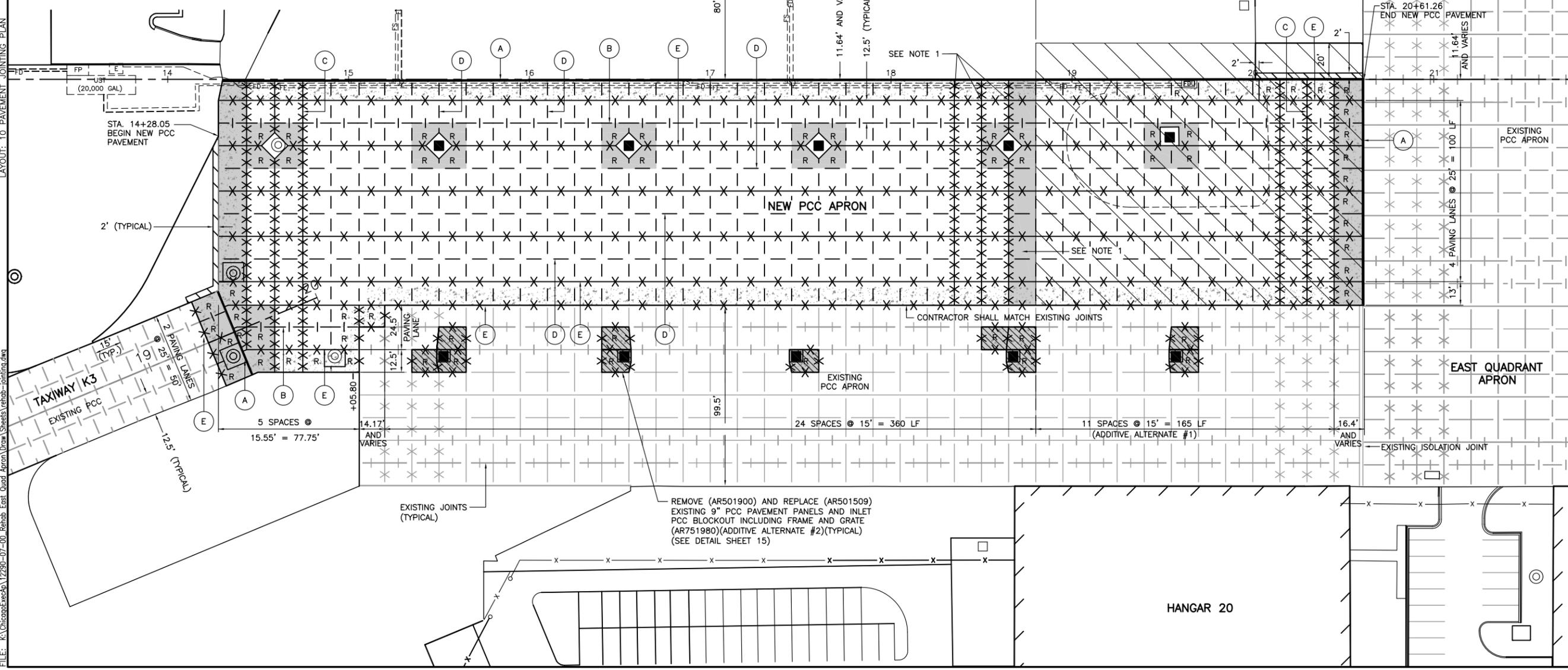
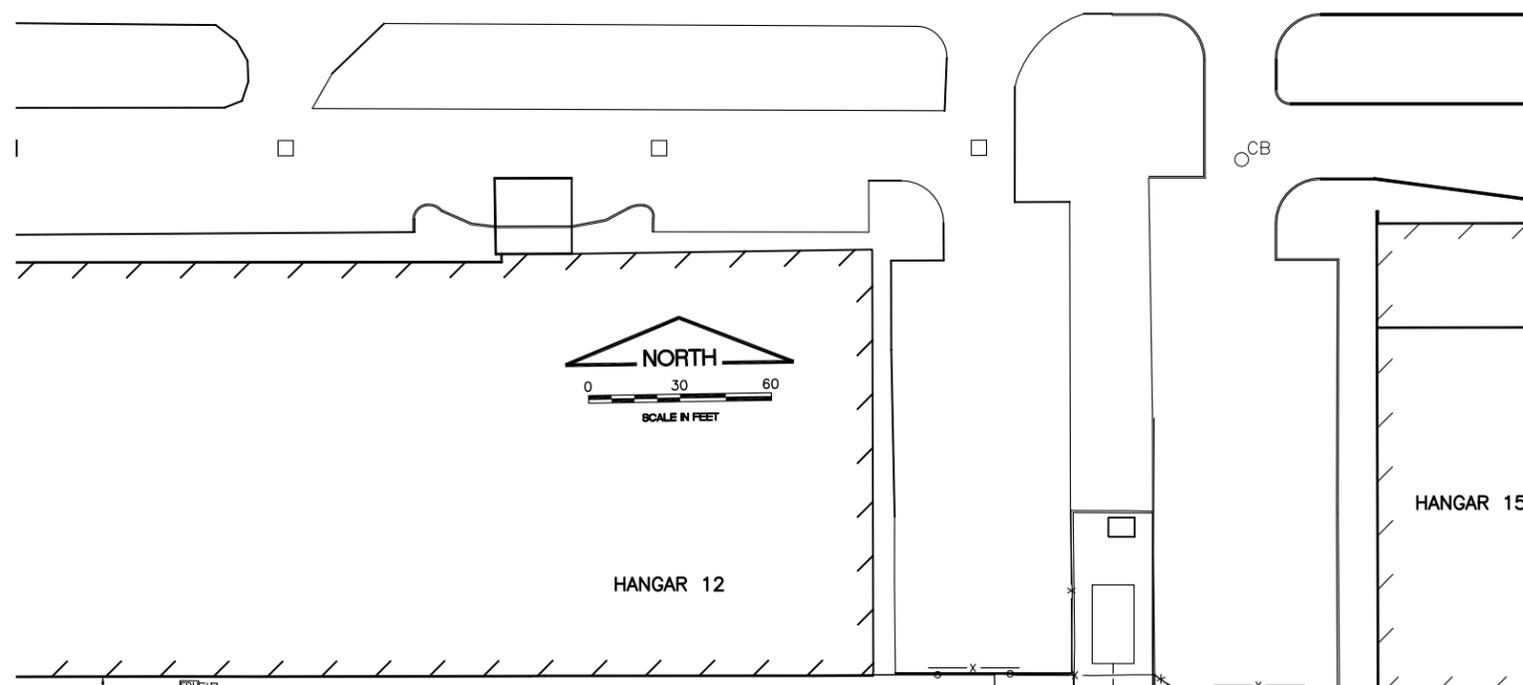


LEGEND

	NEW 2" BITUMINOUS MILL (AR401650) AND REPLACEMENT (AR401610)
	NEW PCC PAVEMENT LIMITS (AR501509)
	ADDITIVE ALTERNATE #1 LIMITS
	ADDITIVE ALTERNATE #2 LIMITS
	APRON STRUCTURE PAVEMENT BLOCKOUT
	THICKENED EDGE
	TYPE (A) ISOLATION JOINT
	TYPE (B) - HINGED CONTRACTION JOINT
	TYPE (C) - DOWELED CONTRACTION JOINT
	TYPE (D) - DUMMY CONTRACTION JOINT
	TYPE (E) - DOWELED CONSTRUCTION JOINT
	ODD SHAPED PANEL REINFORCEMENT (SOME R SYMBOLS NOT SHOWN FOR CLARITY)
	FULL DEPTH (4" AVG.) BITUMINOUS REMOVAL AND REPLACEMENT (AR401910)
	EXISTING INLET/MANHOLE
	EXISTING 3" FUEL SUPPLY LINE (FIBERGLASS)
	EXISTING 2" FUEL DRAIN LINE (FIBERGLASS)
	EXISTING FUELING ELECTRICAL
	EXISTING 4'± TALL FUEL DISPENSING CABINET
	EXISTING UNDERGROUND STORAGE TANK

NOTES

- IF ADDITIVE ALTERNATE #1 IS AWARDED, THE LAST THREE TRANSVERSE JOINTS IN THE BASE BID SHALL BE TYPE D DUMMY CONTRACTION JOINTS WITH NO THICKENED EDGE.



IL. CONTRACT: PA054
 IL. LETTING ITEM: 6A
 IL. PROJECT: PWK-4262
 S.B.G. PROJECT: 3-17-0018-B49

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**CHICAGO EXECUTIVE AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 REHABILITATE EAST QUADRANT GENERAL AVIATION APRON
 PAVEMENT JOINTING PLAN**

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CHICAGO EXECUTIVE AIRPORT

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DRAWN BY:	JRO
CHECKED BY:	JRL
APPROVED BY:	DKP
DATE:	11/13/2013
JOB No:	12290-07-00

FINAL

SHEET 10 OF 21 SHEETS

DATE: Friday, November 15, 2013, 2:41:32 PM
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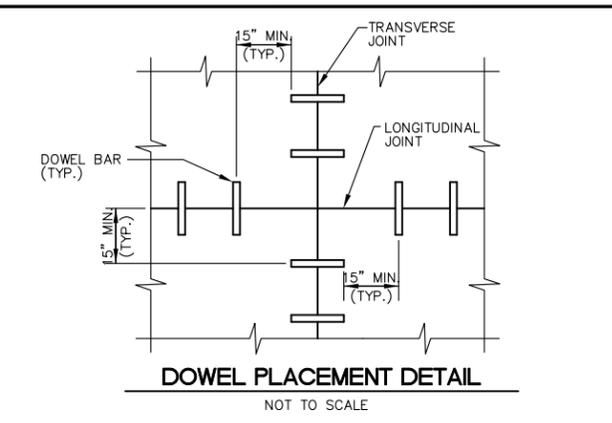
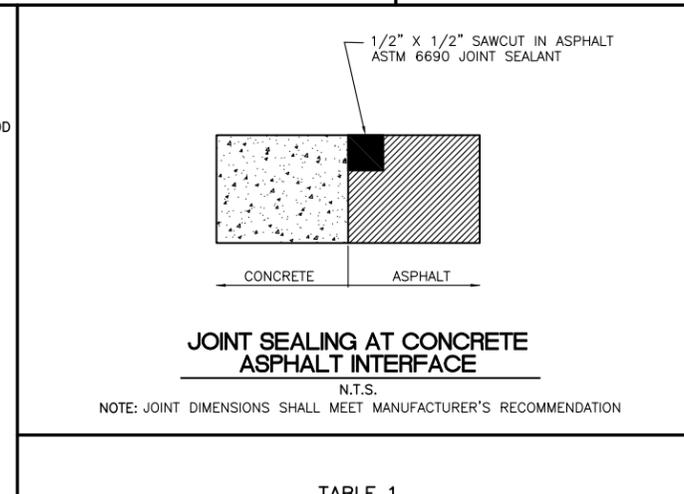
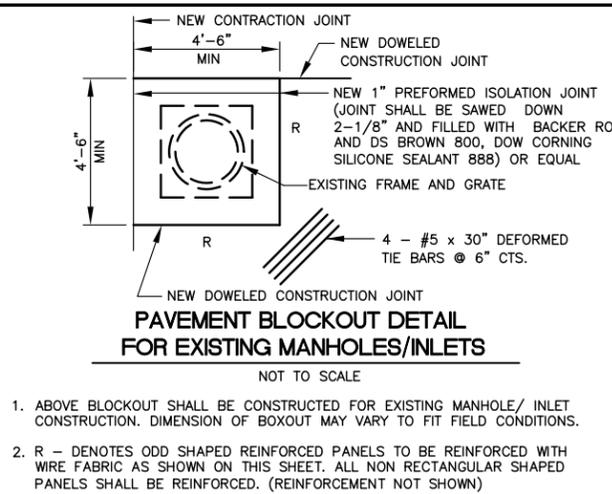
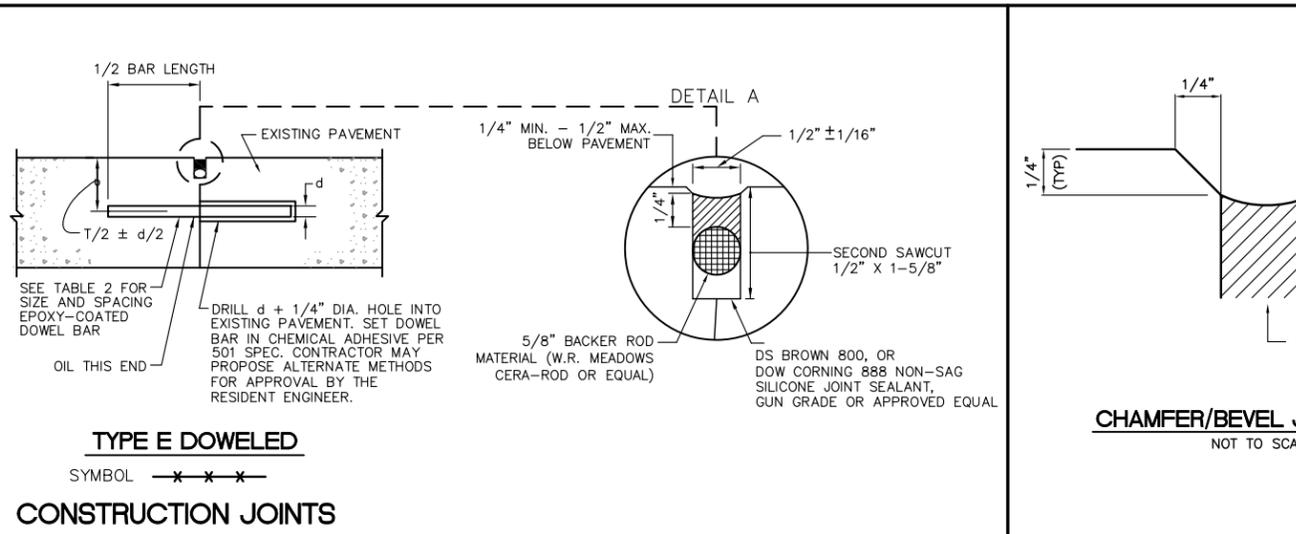
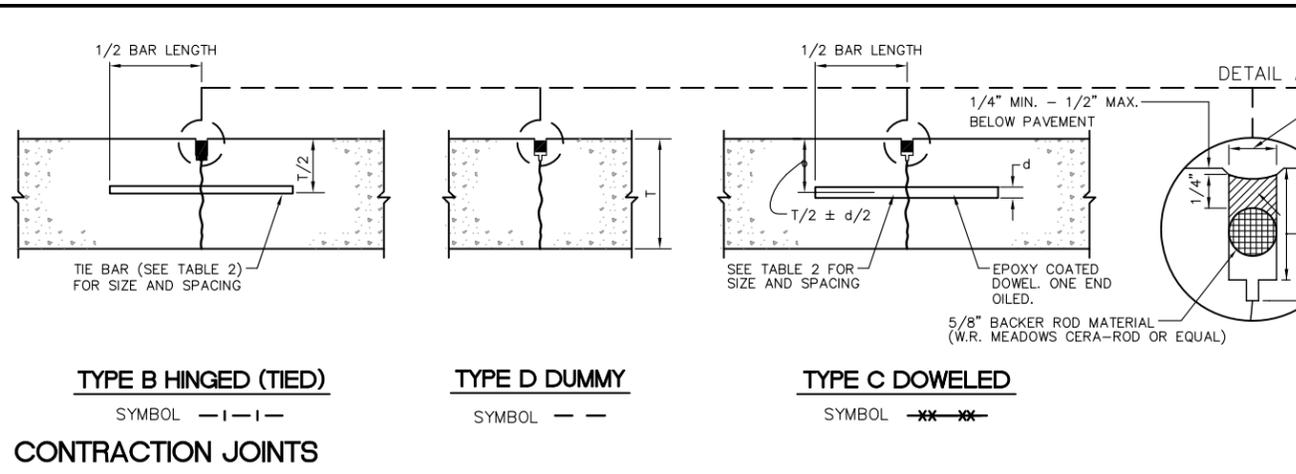
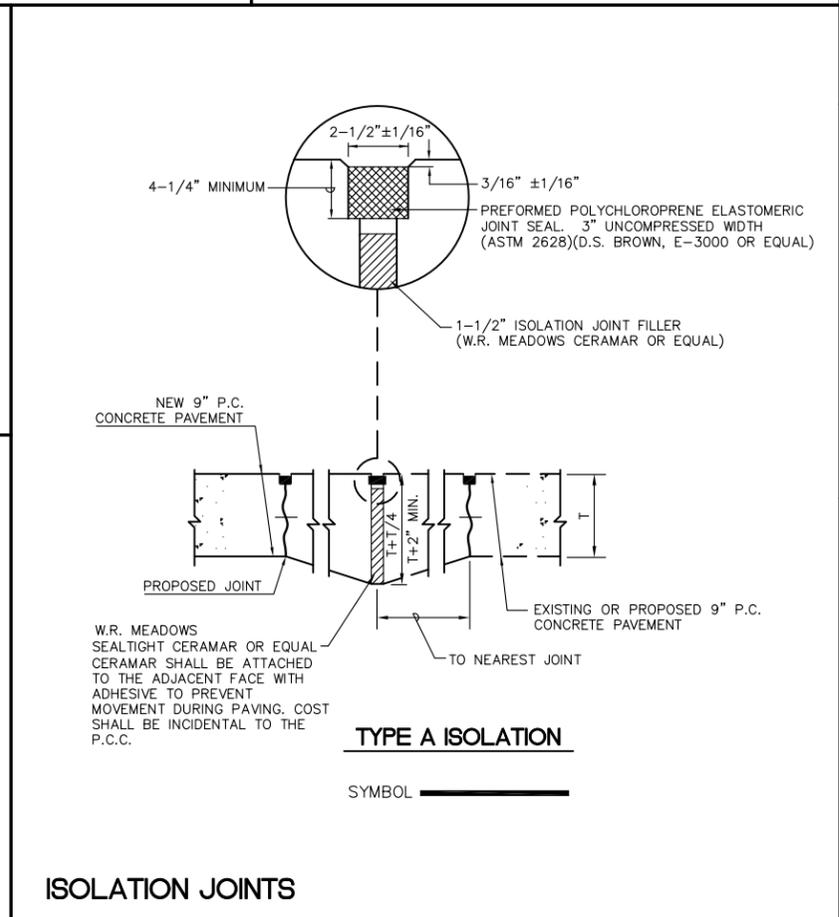
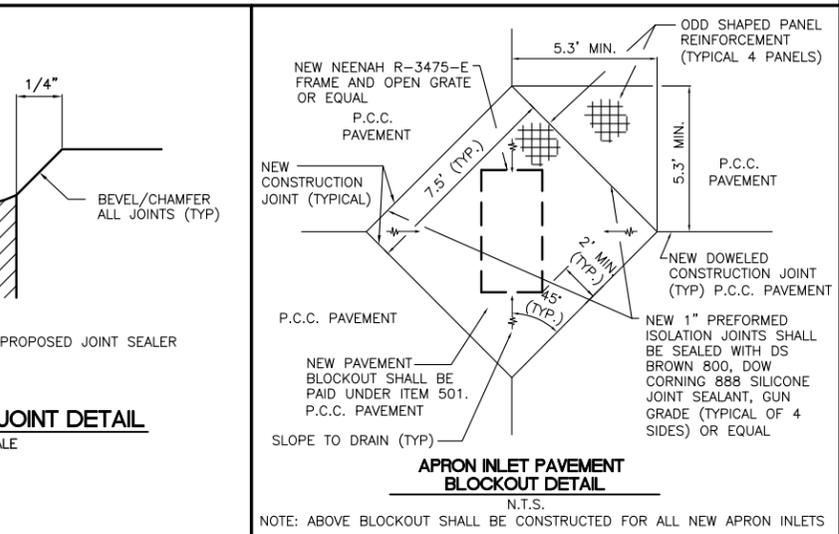
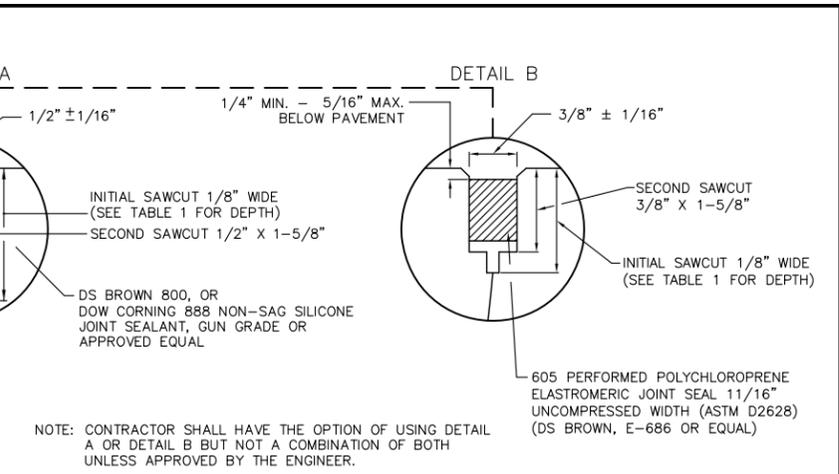


TABLE 1

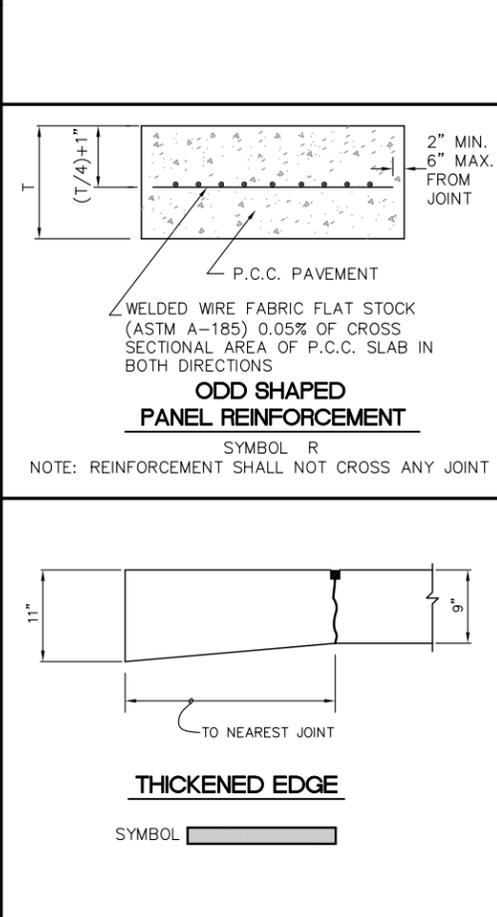
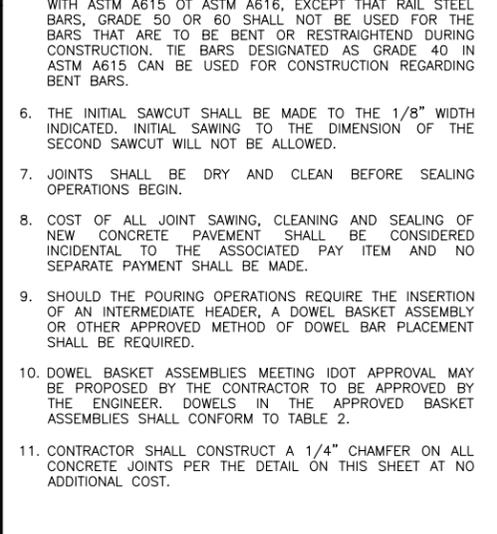
PAVEMENT THICKNESS T - INCHES	DEPTH OF CONTRACTION JOINT INITIAL SAW CUT T, INCHES $T = (T/4) \pm 1/4"$
9	2.25"

TABLE 2

PAVEMENT THICKNESS T - INCHES	DOWEL BAR DETAILS			TIE BAR DETAILS		
	DIA. (d)	LENGTH	SPACING	BAR SIZE	LENGTH	SPACING
9	1"	19"	12"	#5	30"	30"



- JOINT NOTES**
- ALL EDGES OF NEW SLABS, FREE STANDING OR CLOSURE, SHALL BE EDGED WITH AN APPROVED TOOL HAVING A RADIUS OF 1/8" TO 1/4" TO FACILITATE SAWING OF THE SEALANT RESERVOIR. A RADIUS > 1/4" WILL NOT BE ACCEPTABLE.
 - THE INITIAL SAWCUT FOR ALL LONGITUDINAL AND TRANSVERSE CONTRACTION JOINTS SHALL BE SAWS AS SOON AS POSSIBLE AFTER PLACEMENT OF THE PAVEMENT. SAWING OF LONGITUDINAL CONTRACTION JOINTS ADJACENT TO THE THICKENED EDGES SHALL BE GIVEN PRIORITY OVER OTHER LONGITUDINAL JOINT SAWING.
 - ALL DOWEL BARS SHALL BE SECURELY HELD IN PLACE BY MEANS OF A DOWEL BAR ASSEMBLY, WHICH WILL INSURE THAT THEY WILL REMAIN PARALLEL TO THE PAVEMENT LANES. THE DOWEL BAR ASSEMBLIES SHALL BE APPROVED BY THE ENGINEER PRIOR TO INSTALLATION.
 - ALL TIE BARS AND MESH SHALL BE SECURELY HELD IN PLACE BY SUPPORT PINS OR PLACED BY OTHER APPROVED METHODS TO PREVENT SHIFTING DURING AND AFTER CONCRETE PLACEMENT.
 - TIE BARS SHALL BE DEFORMED BARS IN CONFORMANCE WITH ASTM A615 OR ASTM A616, EXCEPT THAT RAIL STEEL BARS, GRADE 50 OR 60 SHALL NOT BE USED FOR THE BARS THAT ARE TO BE BENT OR RESTRAIGHTENED DURING CONSTRUCTION. TIE BARS DESIGNATED AS GRADE 40 IN ASTM A615 CAN BE USED FOR CONSTRUCTION REGARDING BENT BARS.
 - THE INITIAL SAWCUT SHALL BE MADE TO THE 1/8" WIDTH INDICATED. INITIAL SAWING TO THE DIMENSION OF THE SECOND SAWCUT WILL NOT BE ALLOWED.
 - JOINTS SHALL BE DRY AND CLEAN BEFORE SEALING OPERATIONS BEGIN.
 - COST OF ALL JOINT SAWING, CLEANING AND SEALING OF NEW CONCRETE PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ASSOCIATED PAY ITEM AND NO SEPARATE PAYMENT SHALL BE MADE.
 - SHOULD THE POURING OPERATIONS REQUIRE THE INSERTION OF AN INTERMEDIATE HEADER, A DOWEL BASKET ASSEMBLY OR OTHER APPROVED METHOD OF DOWEL BAR PLACEMENT SHALL BE REQUIRED.
 - DOWEL BASKET ASSEMBLIES MEETING IDOT APPROVAL MAY BE PROPOSED BY THE CONTRACTOR TO BE APPROVED BY THE ENGINEER. DOWELS IN THE APPROVED BASKET ASSEMBLIES SHALL CONFORM TO TABLE 2.
 - CONTRACTOR SHALL CONSTRUCT A 1/4" CHAMFER ON ALL CONCRETE JOINTS PER THE DETAIL ON THIS SHEET AT NO ADDITIONAL COST.



IL. CONTRACT: PA054
IL. LETTING ITEM: 6A
IL. PROJECT: PWK-4262
S.B.G. PROJECT: 3-17-0018-B49

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0 1 2
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 REHABILITATE EAST QUADRANT GENERAL AVIATION APRON
 PAVEMENT JOINTING DETAILS

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CHICAGO EXECUTIVE AIRPORT

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CHECKED BY: JRL
APPROVED BY: DKP
DATE: 11/13/2013
JOB No: 12290-07-00

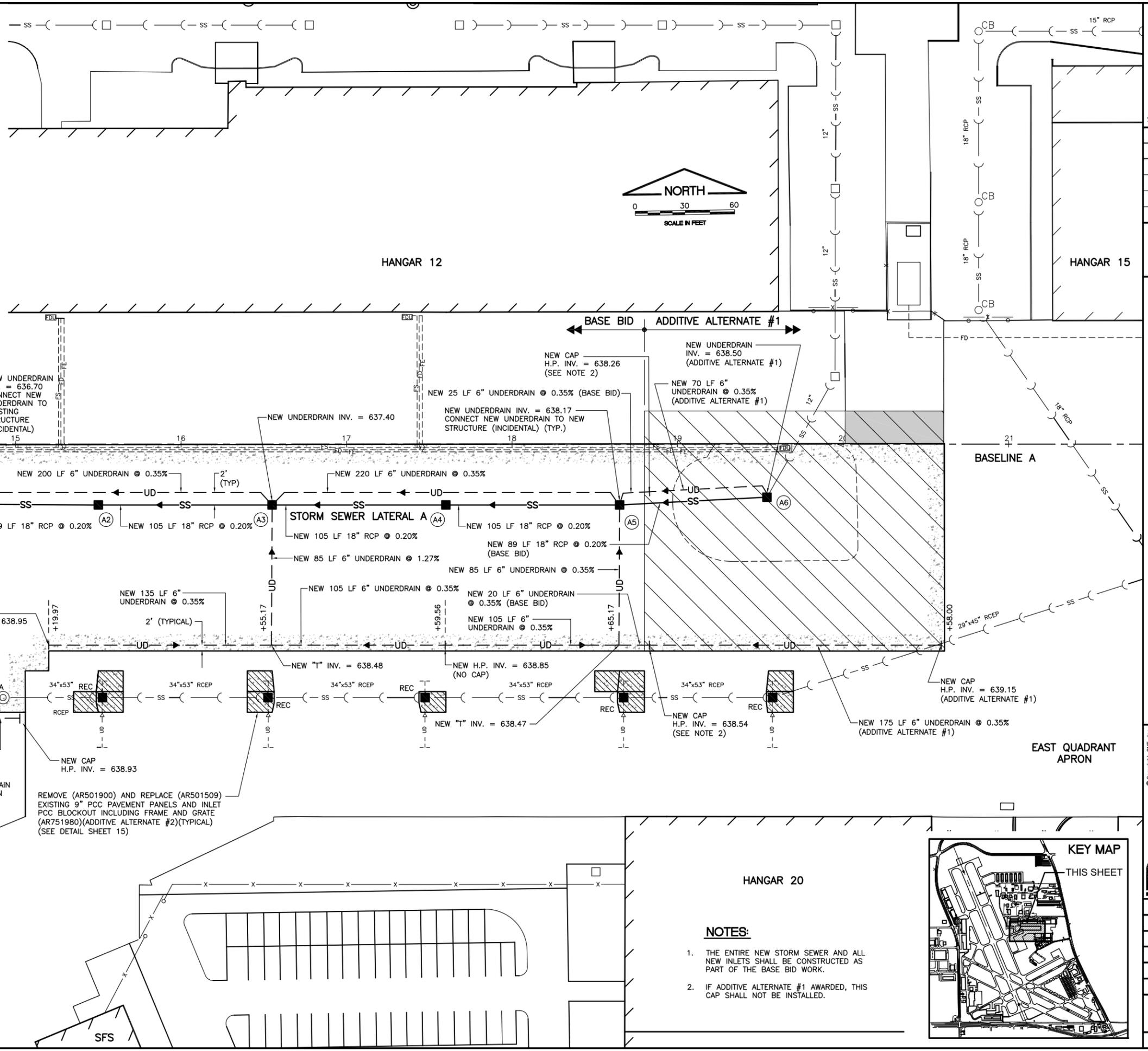
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SHEET 11 OF 21 SHEETS

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 UPDATE BY: Jeremy Linke
 LAYOUT: 13_DRAINAGE PLAN
 KREF DWG: rehab east quadrant apron base.dwg
 IMAGE FILES: pwkrmlw\LOGO_CEA-4-Color-Small.pdf
 KREF DWG: rehab east quadrant apron base.dwg
 IMAGE FILES: pwkrmlw\LOGO_CEA-4-Color-Small.pdf

LEGEND

- NEW PCC PAVEMENT LIMITS
- ADDITIVE ALTERNATE #1 LIMITS
- ADDITIVE ALTERNATE #2 LIMITS
- EXISTING MANHOLE/INLET/CATCH BASIN
- NEW STORM STRUCTURE (AR751416)
- STRUCTURE TO BE ADJUSTED
- EXISTING INLET AND PCC BLOCKOUT TO BE RECONSTRUCTED (AR751980) (ADDITIVE ALTERNATE #2)(SEE DETAIL SHEET 15)
- EXISTING STORM SEWER
- EXISTING UNDERDRAIN
- EXISTING UNDERDRAIN COLLECTION STRUCTURE
- NEW 6" PERFORATED UNDERDRAIN (AR705506)
- NEW 18" RCP STORM SEWER (AR701518)
- EXISTING 3" FUEL SUPPLY LINE (FIBERGLASS)
- EXISTING 2" FUEL DRAIN LINE (FIBERGLASS)
- EXISTING FUELING ELECTRICAL
- EXISTING FUEL DISPENSING CABINET
- EXISTING UNDERGROUND STORAGE TANK



IL. CONTRACT: **PA054**
 IL. LETTING ITEM: **6A**
 IL. PROJECT: **PWK-4262**
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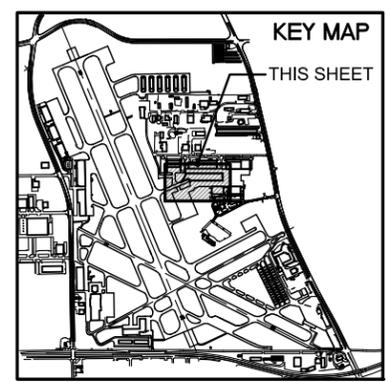
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**CHICAGO EXECUTIVE AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 REHABILITATE EAST QUADRANT GENERAL AVIATION APRON
 DRAINAGE PLAN**

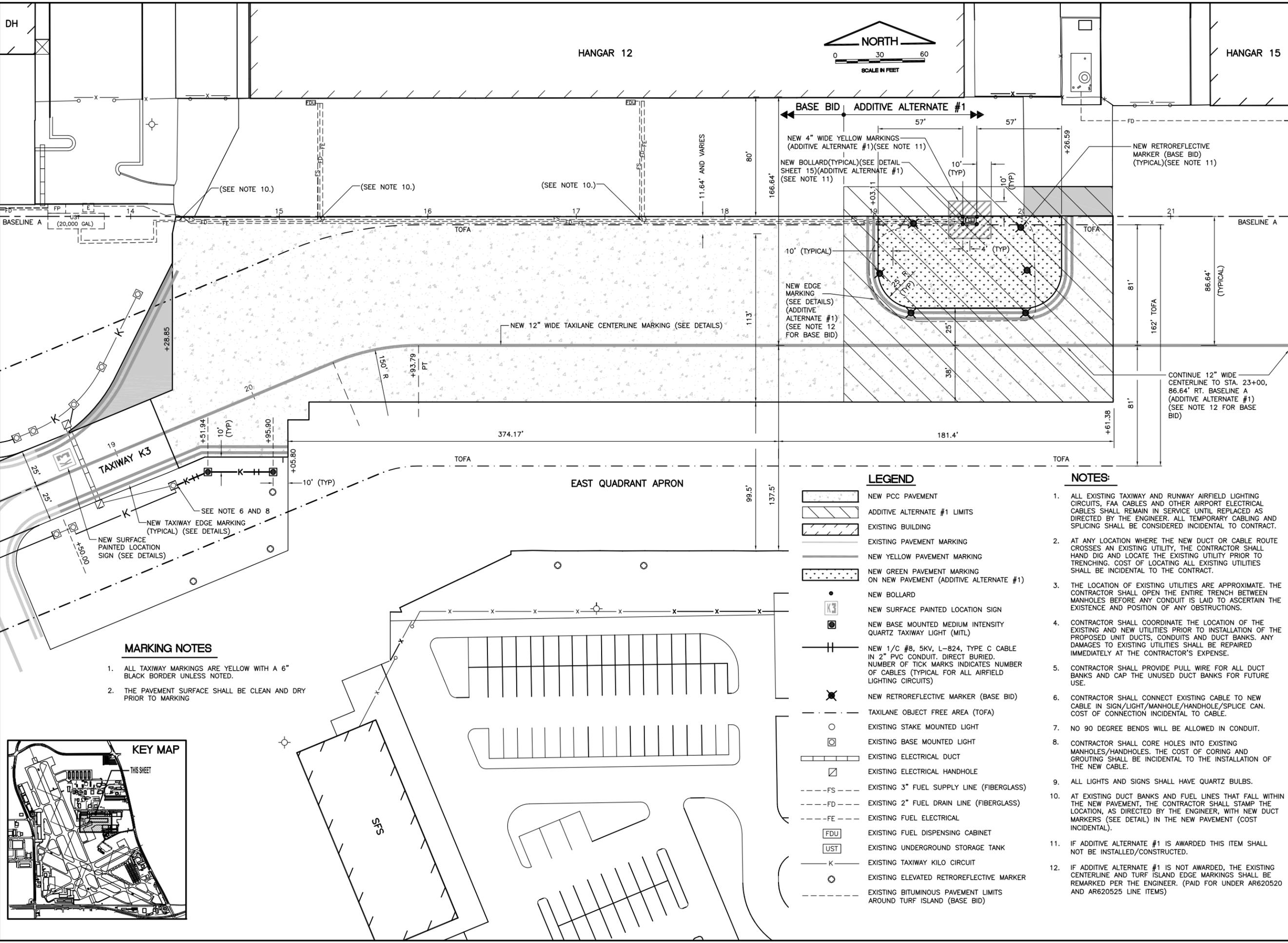
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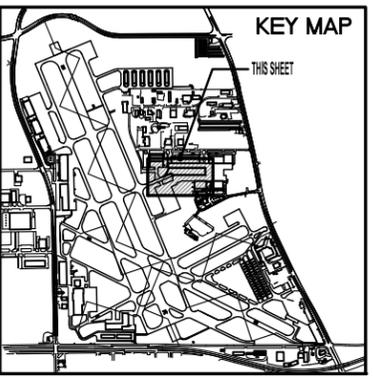
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DRAWN BY:	JRO
CHECKED BY:	JRL
APPROVED BY:	DKP
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JOB No:	12290-07-00



DATE: Friday, November 15, 2013, 2:42:09 PM
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 UPDATE BY: Jeremy Linke
 LAYOUT: 16 LIGHTING AND MARKING PLAN
 REF: DWG: 16enb_rehab.dwg
 Tenth quadrant upon base.dwg
 IMAGE FILES: pwknew\LOGO_GEA-4_Color_Small (2).jpg



- MARKING NOTES**
- ALL TAXIWAY MARKINGS ARE YELLOW WITH A 6" BLACK BORDER UNLESS NOTED.
 - THE PAVEMENT SURFACE SHALL BE CLEAN AND DRY PRIOR TO MARKING



- LEGEND**
- NEW PCC PAVEMENT
 - ADDITIVE ALTERNATE #1 LIMITS
 - EXISTING BUILDING
 - EXISTING PAVEMENT MARKING
 - NEW YELLOW PAVEMENT MARKING
 - NEW GREEN PAVEMENT MARKING ON NEW PAVEMENT (ADDITIVE ALTERNATE #1)
 - NEW BOLLARD
 - NEW SURFACE PAINTED LOCATION SIGN
 - NEW BASE MOUNTED MEDIUM INTENSITY QUARTZ TAXIWAY LIGHT (MITL)
 - NEW 1/C #8, 5KV, L-824, TYPE C CABLE IN 2" PVC CONDUIT. DIRECT BURIED. NUMBER OF TICK MARKS INDICATES NUMBER OF CABLES (TYPICAL FOR ALL AIRFIELD LIGHTING CIRCUITS)
 - NEW RETROREFLECTIVE MARKER (BASE BID)
 - TAXIWAY OBJECT FREE AREA (TOFA)
 - EXISTING STAKE MOUNTED LIGHT
 - EXISTING STAKE MOUNTED LIGHT
 - EXISTING ELECTRICAL DUCT
 - EXISTING ELECTRICAL HANDHOLE
 - EXISTING 3" FUEL SUPPLY LINE (FIBERGLASS)
 - EXISTING 2" FUEL DRAIN LINE (FIBERGLASS)
 - EXISTING FUEL ELECTRICAL
 - EXISTING FUEL DISPENSING CABINET
 - EXISTING UNDERGROUND STORAGE TANK
 - EXISTING TAXIWAY KILO CIRCUIT
 - EXISTING ELEVATED RETROREFLECTIVE MARKER
 - EXISTING BITUMINOUS PAVEMENT LIMITS AROUND TURF ISLAND (BASE BID)

- NOTES:**
- ALL EXISTING TAXIWAY AND RUNWAY AIRFIELD LIGHTING CIRCUITS, FAA CABLES AND OTHER AIRPORT ELECTRICAL CABLES SHALL REMAIN IN SERVICE UNTIL REPLACED AS DIRECTED BY THE ENGINEER. ALL TEMPORARY CABLING AND SPLICING SHALL BE CONSIDERED INCIDENTAL TO CONTRACT.
 - AT ANY LOCATION WHERE THE NEW DUCT OR CABLE ROUTE CROSSES AN EXISTING UTILITY, THE CONTRACTOR SHALL HAND DIG AND LOCATE THE EXISTING UTILITY PRIOR TO TRENCHING. COST OF LOCATING ALL EXISTING UTILITIES SHALL BE INCIDENTAL TO THE CONTRACT.
 - THE LOCATION OF EXISTING UTILITIES ARE APPROXIMATE. THE CONTRACTOR SHALL OPEN THE ENTIRE TRENCH BETWEEN MANHOLES BEFORE ANY CONDUIT IS LAID TO ASCERTAIN THE EXISTENCE AND POSITION OF ANY OBSTRUCTIONS.
 - CONTRACTOR SHALL COORDINATE THE LOCATION OF THE EXISTING AND NEW UTILITIES PRIOR TO INSTALLATION OF THE PROPOSED UNIT DUCTS, CONDUITS AND DUCT BANKS. ANY DAMAGES TO EXISTING UTILITIES SHALL BE REPAIRED IMMEDIATELY AT THE CONTRACTOR'S EXPENSE.
 - CONTRACTOR SHALL PROVIDE PULL WIRE FOR ALL DUCT BANKS AND CAP THE UNUSED DUCT BANKS FOR FUTURE USE.
 - CONTRACTOR SHALL CONNECT EXISTING CABLE TO NEW CABLE IN SIGN/LIGHT/MANHOLE/HANDHOLE/SPLICE CAN. COST OF CONNECTION INCIDENTAL TO CABLE.
 - NO 90 DEGREE BENDS WILL BE ALLOWED IN CONDUIT.
 - CONTRACTOR SHALL CORE HOLES INTO EXISTING MANHOLES/HANDHOLES. THE COST OF CORING AND GROUTING SHALL BE INCIDENTAL TO THE INSTALLATION OF THE NEW CABLE.
 - ALL LIGHTS AND SIGNS SHALL HAVE QUARTZ BULBS.
 - AT EXISTING DUCT BANKS AND FUEL LINES THAT FALL WITHIN THE NEW PAVEMENT, THE CONTRACTOR SHALL STAMP THE LOCATION, AS DIRECTED BY THE ENGINEER, WITH NEW DUCT MARKERS (SEE DETAIL) IN THE NEW PAVEMENT (COST INCIDENTAL).
 - IF ADDITIVE ALTERNATE #1 IS AWARDED THIS ITEM SHALL NOT BE INSTALLED/CONSTRUCTED.
 - IF ADDITIVE ALTERNATE #1 IS NOT AWARDED, THE EXISTING CENTERLINE AND TURF ISLAND EDGE MARKINGS SHALL BE REMARKED PER THE ENGINEER. (PAID FOR UNDER AR620520 AND AR620525 LINE ITEMS)

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0 1 2
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 REHABILITATE EAST QUADRANT GENERAL AVIATION APRON**

LIGHTING AND PAVEMENT MARKING PLAN

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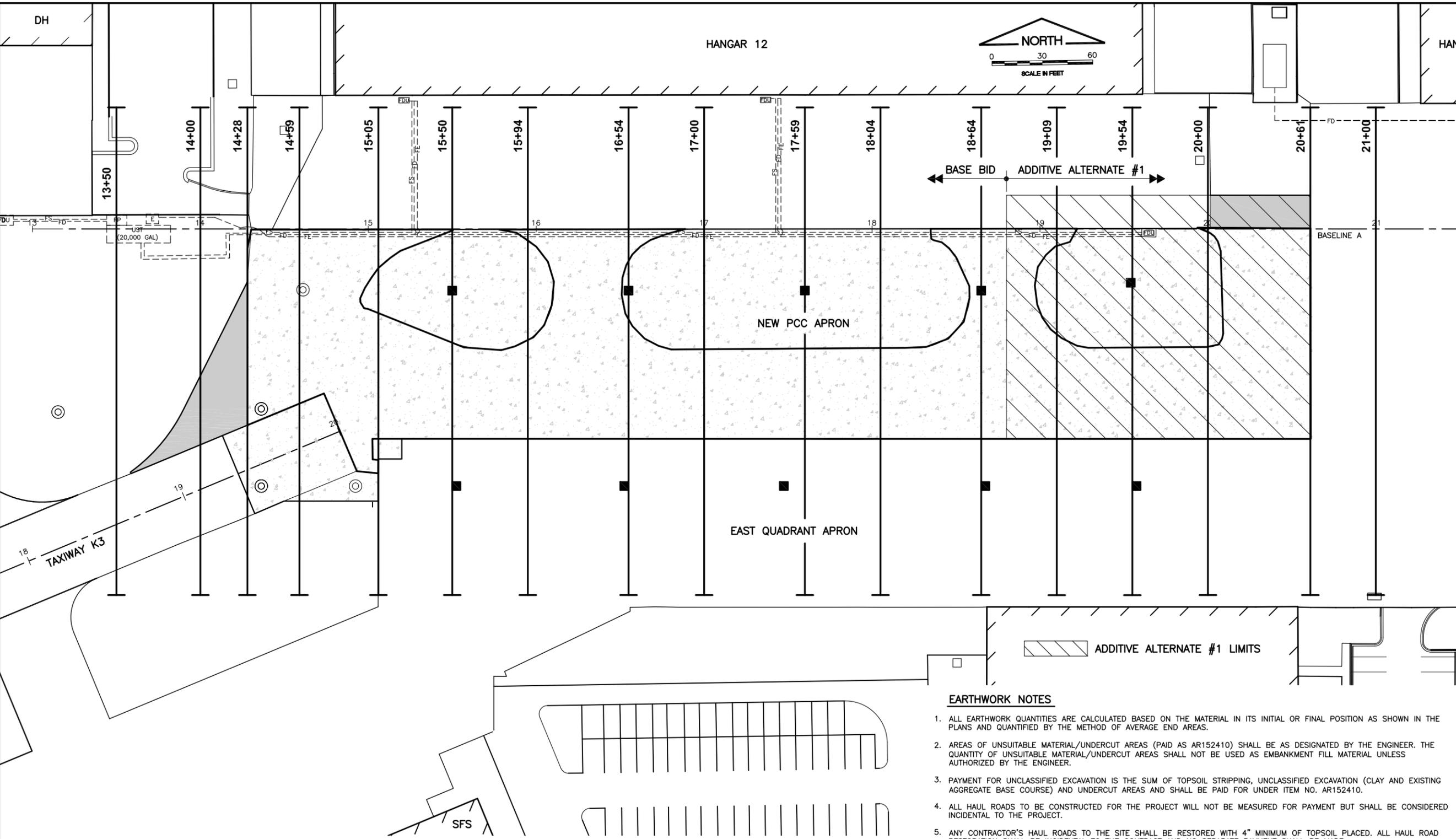
CHICAGO EXECUTIVE AIRPORT

DESIGN BY: JRL
 DRAWN BY: JRO
 CHECKED BY: JRL
 APPROVED BY: DKP
 DATE: 11/13/2013
 JOB No: 12290-07-00

FINAL

SHEET 16 OF 21 SHEETS

DATE: Friday, November 15, 2013, 2:42:16 PM
 FILE: K:\Chicago\12290-07-00_Retab_East_Quad_Apron\Drawn_Sheets\retab-index.dwg
 UPDATE BY: Jeremy Linke
 LAYOUT: 18 INDEX TO CROSS SECTIONS - EARTHWORK
 REF: DWG: Rehab east quadrant apron base course
 12/20/07/00-0-0-S-14.dwg



EARTHWORK SUMMARY TABLE (SEE NOTES)	BASE BID QUANTITY	ADD. ALT. #1 QUANTITY	TOTAL QUANTITY	UNIT
UNCLASSIFIED EXCAVATION (CLAY AND/OR AGGREGATE)(INITIAL POSITION)	835	231	1066	CY
TOPSOIL STRIPPING (INITIAL POSITION)	588	260	848	CY
EXCESS MILLINGS PLACED IN ADDITIONAL UNDERCUT AREAS (INITIAL POSITION)	323	65	388	CY
UNDERCUT / PGE AREAS (AS FIELD CONDITIONS WARRANT) (INITIAL POSITION)	200	85	285	CY
TOTAL CUT (UNCLASSIFIED EXCAVATION AR152410)	1946	641	2587	CY
EMBANKMENT FILL MILLINGS (FINAL POSITION)	255	182	437	CY
SHOULDER FILL (FINAL POSITION)	0	0	0	CY
TOPSOIL PLACEMENT (FINAL POSITION)	28	0	28	CY
TOTAL FILL	283	182	465	CY

NOTE: 1. BASE BID: AR401652 PAVEMENT GRINDINGS MATERIAL AT 4 INCHES THICK (AVG.) = 445 CY x 1.3 = 578 CY MILLINGS AVAILABLE (FINAL POSITION). 578 CY - 255 CY REQUIRED FOR EMBANKMENT FILL = 323 CY EXCESS MILLINGS.
 2. ADDITIVE ALTERNATE #1: AR401652 PAVEMENT GRINDINGS MATERIAL AT 4 INCHES THICK (AVG.) = 190 CY x 1.3 = 247 CY MILLINGS AVAILABLE (FINAL POSITION). 247 CY - 182 CY REQUIRED FOR EMBANKMENT FILL = 65 CY EXCESS MILLINGS.

EARTHWORK NOTES

- ALL EARTHWORK QUANTITIES ARE CALCULATED BASED ON THE MATERIAL IN ITS INITIAL OR FINAL POSITION AS SHOWN IN THE PLANS AND QUANTIFIED BY THE METHOD OF AVERAGE END AREAS.
- AREAS OF UNSUITABLE MATERIAL/UNDERCUT AREAS (PAID AS AR152410) SHALL BE AS DESIGNATED BY THE ENGINEER. THE QUANTITY OF UNSUITABLE MATERIAL/UNDERCUT AREAS SHALL NOT BE USED AS EMBANKMENT FILL MATERIAL UNLESS AUTHORIZED BY THE ENGINEER.
- PAYMENT FOR UNCLASSIFIED EXCAVATION IS THE SUM OF TOPSOIL STRIPPING, UNCLASSIFIED EXCAVATION (CLAY AND EXISTING AGGREGATE BASE COURSE) AND UNDERCUT AREAS AND SHALL BE PAID FOR UNDER ITEM NO. AR152410.
- ALL HAUL ROADS TO BE CONSTRUCTED FOR THE PROJECT WILL NOT BE MEASURED FOR PAYMENT BUT SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT.
- ANY CONTRACTOR'S HAUL ROADS TO THE SITE SHALL BE RESTORED WITH 4" MINIMUM OF TOPSOIL PLACED. ALL HAUL ROAD RESTORATION SHALL BE INCIDENTAL TO THE CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.
- TOPSOIL PLACEMENT AND SHOULDER FILL SHALL BE CONSIDERED INCLUDED IN THE CONTRACT UNIT PRICE FOR TOPSOIL STRIPPING (ITEM AR152410). NO SEPARATE PAYMENT WILL BE MADE FOR TOPSOIL PLACEMENT AND SHOULDER FILL.
- MILLINGS RESULTING FROM AR401652 BITUMINOUS PAVEMENT GRINDING SHALL BE USED FOR EMBANKMENT FILL (PAID UNDER AR401652.)
- IT IS ANTICIPATED THAT THERE WILL BE MILLINGS REMAINING AFTER ALL EMBANKMENT FILL IS COMPLETED. A 30% EXPANSION FACTOR WAS USED TO DETERMINE THE AVAILABLE MILLINGS FOR EMBANKMENT FILL. THE REMAINING MILLINGS SHALL BE USED FOR BASE COURSE STABILIZATION IN ADDITIONAL UNDERCUT AREAS (AR152410) AS DETERMINED BY THE ENGINEER.
- ANY EXCESS MATERIAL INCLUDING CLAY, EXISTING AGGREGATE BASE, MILLINGS, AND TOPSOIL SHALL BE HAULED OFF AND DISPOSED OF BY THE CONTRACTOR.
- A 15% SHRINKAGE FACTOR WAS USED TO DETERMINE THE REQUIRED FILL IN ITS INITIAL POSITION. THE DIFFERENCE BETWEEN THE REQUIRED FILL AND REQUIRED CUT QUANTITY WAS USED TO DETERMINE THE NUMBER OF CUBIC YARDS OF MATERIAL TO BE DISPOSED OF OFF AIRPORT PROPERTY. NO ADJUSTMENTS IN EARTHWORK QUANTITIES WILL BE ALLOWED FOR VARIATIONS IN ACTUAL SHRINKAGE AND OR EXPANSION FACTORS ENCOUNTERED DURING CONSTRUCTION.
- CCDD AND ENVIRONMENTAL TESTING RESULTS FOR THE TESTED SAMPLES ARE LOCATED IN THE APPENDIX OF THE CONTRACT DOCUMENTS. ANY ADDITIONAL TESTING AND HANDLING REQUIRED BY THE CONTRACTOR AND/OR CONTRACTORS DISPOSAL FACILITY, SHALL BE COMPLETED AT THE CONTRACTORS EXPENSE. IF THE CONTRACTOR ENCOUNTERS ANY SOIL FROM THIS SITE/PROJECT THAT IS POTENTIALLY CONTAMINATED, THE ENGINEER AND OWNER SHALL BE NOTIFIED PRIOR TO HAULING THE POTENTIALLY CONTAMINATED SOIL OFF SITE.

IL. CONTRACT: PA054
 IL. LETTING ITEM: 6A
 IL. PROJECT: PWK-4262
 S.B.G. PROJECT: 3-17-0018-B49

SURVEY BOOK # BOOK #

REVISIONS		
NUMBER	BY	DATE

0 1 2
 THIS BAR IS EQUAL TO 2" AT FULL SCALE (34X22).

**CHICAGO EXECUTIVE AIRPORT
 WHEELING/PROSPECT HEIGHTS, ILLINOIS
 REHABILITATE EAST QUADRANT GENERAL AVIATION APRON**

**INDEX TO CROSS SECTIONS -
 EARTHWORK SUMMARY**

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CHICAGO EXECUTIVE AIRPORT

DESIGN BY: JRL
 DRAWN BY: JRO
 CHECKED BY: JRL
 APPROVED BY: DKP
 DATE: 11/13/2013
 JOB No: 12290-07-00

FINAL

SHEET 18 OF 21 SHEETS

