

FAU RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2663	19-03126-00-RS	DUPAGE	14	1
F.H.W.A. REG. I ILLINOIS PROJECT: 804V(658)				
CONTRACT NO. 61G59				

01-15-2021 LETTING ITEM 143

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS PLANS FOR PROPOSED FEDERAL AID HIGHWAY

(FAU 2663) CLARENDON HILLS ROAD
FROM 91ST STREET TO 79TH STREET

RESURFACING

SECTION NO. 19-03126-00-RS
PROJECT 804V(658)
DOWNERS GROVE TOWNSHIP
DUPAGE COUNTY
JOB NO. C-91-401-19

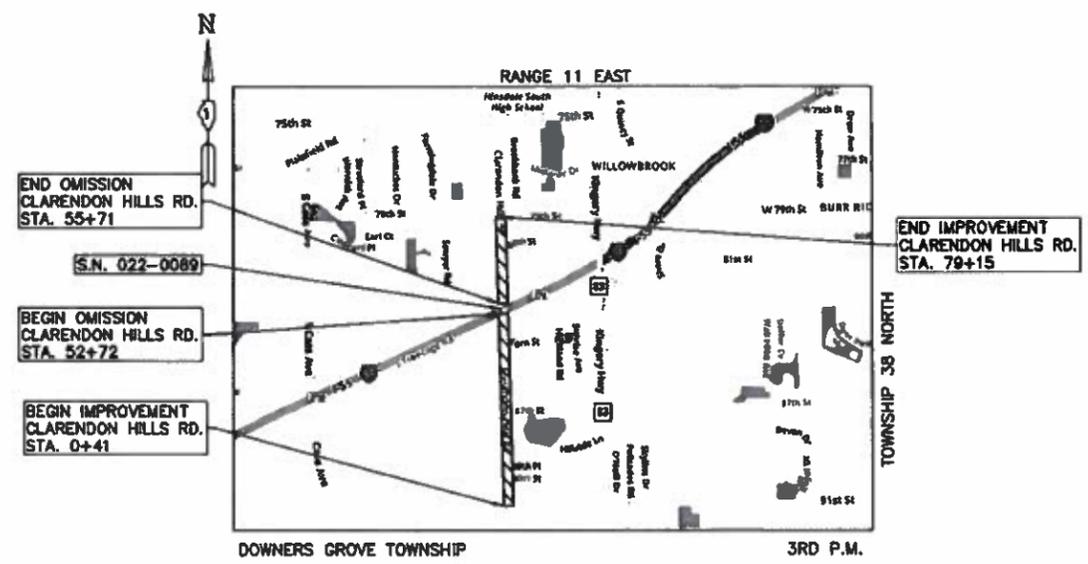
FOR INDEX OF SHEETS, SEE SHEET NO. 2
FOR LIST OF HIGHWAY STANDARDS, SEE SHEET NO. 2

MAJOR COLLECTOR

2,850 ADT (2016)
0% TRUCKS
POSTED SPEED LIMIT = 30 MPH



LOCATION OF SECTION INDICATED THUS: — ■



LOCATION MAP

■■■■ - DENOTES PROJECT LOCATION
 NET LENGTH OF PROJECT = 7575 FEET (1.43 MILES)
 LENGTH OF OMISSION = 299 FEET (0.06 MILES)
 GROSS LENGTH OF PROJECT = 7874 FEET (1.49 MILES)

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

APPROVED 10/11/2020
Laurie C. Anderson
SUPERINTENDENT, DISTRICT ONE, ILLINOIS

PASSED NOV 17 2020
[Signature]
DISTRICT IN CHARGE OF LOCAL, NORTH AND SOUTH

RELEASING FOR BID
BASED ON LIMITED
REVIEW NOVEMBER 17 2020
Anthony J. Ramsey/AB
SOUTH DISTRICT OF GENERAL SESSION IN CHARGE



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

SCALES
PLAN 1 INCH = 50 FEET



SIGNED 9/28/2020
EXPIRES 11/30/2021

PRINTED BY AUTHORITY OF THE STATE OF ILLINOIS

CONTRACT NO. 61G59

FEDERAL AID PROGRAM ENGINEER: CARMEN E. RAMOS, P.E., SCHAUMBURG, IL

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GENERAL NOTES

1. ALL EARTHWORK, GRADING, UTILITIES, AND STREET IMPROVEMENTS WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE APRIL 1, 2016 EDITION OF THE ILLINOIS DEPARTMENT OF TRANSPORTATION DIVISION OF HIGHWAYS' STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, THE STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS, AND ALL REVISIONS THERETO.
2. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING SAFE AND HEALTHFUL WORKING CONDITIONS IN ACCORDANCE WITH SECTION 107 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION THROUGHOUT THE CONSTRUCTION OF THE PROPOSED IMPROVEMENTS.
3. SOIL EROSION AND SEDIMENTATION CONTROL PRACTICES AND DEVICES SHALL BE IN ACCORDANCE WITH THE LATEST EDITION OF THE URBAN COMMITTEE OF THE ASSOCIATION OF ILLINOIS SOIL AND WATER CONSERVATION DISTRICTS' PROCEDURES AND STANDARDS FOR URBAN SOIL EROSION AND SEDIMENTATION CONTROL IN ILLINOIS AND ALL REVISIONS THERETO AND IN ACCORDANCE WITH THE DETAILS ON THE PLANS.
4. THE CONTRACTOR SHALL BE AWARE OF POTENTIAL CONFLICTS WITH EXISTING UTILITIES AS INDICATED ON THE PLANS. THE CONTRACTOR SHALL EXCAVATE AROUND UTILITIES TO DETERMINE ELEVATIONS BEFORE BEGINNING CONSTRUCTION.
5. THE CONTRACTOR SHALL BE RESPONSIBLE FOR NOTIFYING EACH OF THE UTILITY COMPANIES BEFORE ANY WORK COMMENCES. ALL UTILITIES SHALL BE STAKED PRIOR TO CONSTRUCTION.
6. THE OWNER WILL FURNISH THE CONTRACTOR WITH LINES, GRADES AND ELEVATIONS NECESSARY TO THE PROPER PROSECUTION AND CONTROL OF THE WORK ONCE.
7. THE CONTRACTOR SHALL GIVE THE ENGINEER AT LEAST SEVENTY-TWO (72) HOURS NOTICE FOR ANY STAKING TO BE DONE. EACH OF THE VARIOUS ITEMS OF WORK COVERED BY THIS CONTRACT WILL BE STAKED ONCE.
8. THE CONTRACTOR SHALL INFORM THE RESIDENT ENGINEER, CITY ENGINEER, AND THE MUNICIPALITY AT LEAST FORTY-EIGHT (48) HOURS PRIOR TO COMMENCEMENT OF CONSTRUCTION.

TELEPHONE NUMBERS: TOWNSHIP ENGINEER - (630) 271-0770
DOWNERS GROVE TOWNSHIP HIGHWAY DEPARTMENT - (630) 719-6620
9. THE CONTRACTOR RESPONSIBLE FOR DRAINAGE IMPROVEMENTS (UNDERGROUND STRUCTURES AND CONDUITS) SHALL DISPOSE OF ALL SURPLUS EXCAVATED MATERIAL FROM TRENCHES OR STRUCTURE EXCAVATIONS AND SHALL DEPOSIT SAID SURPLUS MATERIALS ON THE SITE IN ACCORDANCE WITH THE GRADING PLAN OR AS DIRECTED BY THE ENGINEER.
10. THE CONTRACTOR SHALL NOT PLACE ANY EXCAVATED MATERIAL UPON ANY TOPSOIL. THE TOPSOIL SHALL BE REMOVED FROM ALL AREAS TO BE FILLED AND SHALL BE STOCKPILED IN AREAS AS DIRECTED BY THE ENGINEER.
11. THE CONTRACTOR SHALL NOT DISCHARGE INTO STREAMS, PONDS, WETLANDS OR ITS TRIBUTARIES ANY MOTOR OIL, TRANSMISSION FLUID, LUBRICANTS OR ANY OTHER PETROLEUM DISTILLATES, ANY PETROLEUM DISTILLATES DISCHARGED ON THE GROUND SURFACE SHALL BE PROMPTLY AND PROPERLY REMOVED PRIOR TO THE RESUMPTION OF ANY WORK ON THE PROJECT.

12. THE CONTRACTOR SHALL PRESERVE AND PROTECT ALL EXISTING FIELD TILES. ANY FIELD TILES DAMAGED BY THE CONTRACTOR'S OPERATIONS SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR AT HIS SOLE EXPENSE. INVESTIGATION SHALL BE MADE TO INSURE THAT FIELD TILES DO NOT CONVEY OFF SITE WATER. TILES THAT CONVEY OFF SITE WATER SHALL BE REROUTED THROUGH THE SITE. TILES THAT DO NOT CONVEY OFF SITE WATER SHALL BE ABANDONED IN AN APPROPRIATE MANNER APPROVED BY THE MUNICIPALITY. FIELD TILES WITHIN A RIGHT-OF-WAY SHALL BE REMOVED AND BACKFILLED WITH CA-6 COMPACTED IN EIGHT INCH LIFTS TO THE BOTTOM OF THE ROADWAY BASE. EXISTING FIELD TILES SHALL BE REMOVED BY SLIT TRENCHING.
13. THE CONTRACTOR RESPONSIBLE FOR DRAINAGE IMPROVEMENTS SHALL BE RESPONSIBLE TO PLACE ALL FIRE HYDRANTS, FRAMES AND LIDS OR GRATES, AND ALL GRATES FOR MANHOLES, CATCH BASINS, INLETS AND VALVE VAULTS AT THE ELEVATIONS SHOWN AND SPECIFIED ON THE PLANS.
14. THE CONTRACTOR SHALL KEEP PUBLIC STREET PAVEMENTS CLEAN OF DIRT AND DEBRIS AND, WHEN NECESSARY, SHALL ON A DAILY BASIS CLEAN THE PAVEMENT OF SUCH DIRT AND DEBRIS RESULTING FROM CONSTRUCTION OPERATIONS.
15. THE CONTRACTOR SHALL EXAMINE THE DRAINAGE PATTERNS SHOWN ON THE PLANS AND MAKE CERTAIN THAT ALL OVERFLOW POINT ELEVATIONS AND CROSS SECTIONS ARE CONSTRUCTED STRICTLY IN ACCORDANCE WITH THOSE SHOWN ON THE PLANS.
16. THE CONTRACTOR SHALL CONTACT J.U.L.I.E. (1-800-892-0123) PRIOR TO ANY WORK IN THE RIGHT OF WAY OR EASEMENTS TO LOCATE UTILITIES, AND CONTACT THE OWNER'S REPRESENTATIVE SHOULD PUBLIC UTILITIES APPEAR TO BE IN CONFLICT WITH THE PROPOSED IMPROVEMENTS.
17. CURB SHALL BE REMOVED IN A WAY TO PREVENT DAMAGE TO EXISTING PAVEMENT.
18. THE LOCATION OF CLASS D PATCHING OF VARIOUS TYPES WILL BE DIRECTED BY THE ENGINEER IN THE FIELD PRIOR TO STARTING CONSTRUCTION.

TRAFFIC CONTROL AND PROTECTION

1. AT THE PRECONSTRUCTION MEETING THE CONTRACTOR SHALL FURNISH THE NAME OF THE INDIVIDUAL IN HIS DIRECT EMPLOY WHO IS TO BE RESPONSIBLE FOR THE INSTALLATION AND MAINTENANCE OF THE TRAFFIC CONTROL FOR THIS PROJECT. IF THE ACTUAL INSTALLATION AND MAINTENANCE ARE TO BE ACCOMPLISHED BY A SUBCONTRACTOR, CONSENT SHALL BE REQUESTED OF THE ENGINEER AT THE TIME OF THE PRECONSTRUCTION MEETING IN ACCORDANCE WITH ARTICLE 108.01 OF THE STANDARD SPECIFICATIONS. THIS SHALL NOT RELIEVE THE CONTRACTOR OF THE FOREGOING REQUIREMENT FOR A RESPONSIBLE INDIVIDUAL IN HIS DIRECT EMPLOY TO SUPERVISE THIS WORK.
2. THE GOVERNING FACTOR IN THE EXECUTION AND STAGING OF WORK FOR THIS PROJECT IS TO PROVIDE THE MOTORING PUBLIC WITH THE SAFEST POSSIBLE TRAVEL CONDITIONS ALONG THE ROADWAY THROUGH THIS CONSTRUCTION ZONE. THE CONTRACTOR SHALL SO ARRANGE HIS OPERATION AS TO KEEP THE CLOSING OF ANY LANE OF THE ROADWAY TO A MINIMUM.
3. THE CONTRACTOR SHALL ENSURE THAT ALL TRAFFIC CONTROL DEVICES INSTALLED BY HIM/HER ARE OPERATIONAL 24 HOURS A DAY, INCLUDING SUNDAYS AND HOLIDAYS.
4. CONTRACTOR VEHICLES SHALL ENTER OR LEAVE WORK AREAS IN A MANNER WHICH WILL NOT BE HAZARDOUS TO, OR WILL NOT INTERFERE WITH, TRAFFIC AND SHALL NOT PARK OR STOP EXCEPT WITHIN DESIGNATED WORK AREAS. PERSONAL VEHICLES SHALL NOT PARK WITHIN THE RIGHT-OF-WAY EXCEPT IN SPECIFIC AREAS DESIGNATED BY THE ENGINEER.
5. DELAYS TO THE CONTRACTOR CAUSED BY COMPLYING WITH THESE REQUIREMENTS WILL BE INCLUDED IN THE COST OF THE ITEM FOR TRAFFIC CONTROL AND PROTECTION.
6. THE CONTRACTOR SHALL CONTACT THE IDOT TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV AT LEAST 72 HOURS IN ADVANCE OF BEGINNING WORK.

EXCAVATION

1. ALL SITE CLEARING, EXCAVATION, GRADING, COMPACTION, SUBGRADE PREPARATION, BASE COURSE, SURFACE COURSE, PCC CURB AND GUTTER AND SIDEWALKS SHALL BE CONSTRUCTED IN CONFORMANCE WITH THE APPLICABLE SECTIONS OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, DEPARTMENT OF TRANSPORTATION, STATE OF ILLINOIS, LATEST EDITION.
2. EARTHWORK UNDER THIS CONTRACT SHALL INCLUDE THE FOLLOWING:
 - A. REMOVAL OF EXISTING VEGETATION WITHIN CONSTRUCTION LIMITS FROM THE SITE.
 - B. PROTECTION OF CERTAIN TREES AS SHOWN ON PLANS.
 - C. STRIPPING OF ALL TOPSOIL AND OTHER UNSUITABLE MATERIALS FROM BUILDING AND/OR PAVEMENT AREAS AND REMOVAL FROM SITE OF ALL EXCESS.
 - D. COMPLETE REMOVAL AND DISPOSAL OF THE NATURAL GROUND TO THE PROPOSED SUB-GRADE ELEVATION OF NEW PAVEMENT.

SOIL EROSION AND SEDIMENTATION CONTROL NOTES

1. THE FOLLOWING PRINCIPLES SHALL APPLY TO ALL MOVEMENT OF EARTH AND STORM DRAINAGE. SEDIMENT AND EROSION CONTROL DEVICES SHALL BE FUNCTIONAL BEFORE LAND IS OTHERWISE DISTURBED ON SITE.
2. THE SMALLEST PRACTICAL AREA OF LAND IS TO BE EXPOSED AT ANY GIVEN TIME DURING CONSTRUCTION. EXPOSURE SHALL BE KEPT TO AS SHORT A DURATION OF TIME AS IS PRACTICAL.
3. STABILIZE AND PROTECT DISTURBED AREAS. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. SOIL STABILIZATION MEASURES SHALL CONSIDER THE TIME OF YEAR, SITE CONDITIONS, AND THE USE OF TEMPORARY OR PERMANENT MEASURES. MECHANICAL, STRUCTURAL, AND/OR VEGETATIVE CONTROL METHODS SHALL BE USED IN ORDER TO RETARD SOIL EROSION IF DIRECTED BY THE OWNER OR HIS AUTHORIZED REPRESENTATIVE.
4. KEEP RUN-OFF VELOCITIES LOW WITH SHORT SLOPES AND LOW GRADIENTS. THE INSTALLATION OF NATURAL VEGETATIVE COVER HELPS TO KEEP STORM WATER VELOCITIES LOW, AND THUS LIMIT SOIL EROSION EFFECTS.
5. PROTECT DISTURBED AREAS FROM STORM WATER RUN-OFF. SOIL EROSION AND SEDIMENT CONTROL FEATURES SHALL BE CONSTRUCTED PRIOR TO THE COMMENCEMENT OF UPLAND DISTURBANCE. PROTECTIVE MEASURES SHALL BE UTILIZED TO DIVERT RUN-OFF FROM DISTURBED AREAS.
6. ALL TEMPORARY EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED. TRAPPED SEDIMENT AND OTHER DISTURBED SOIL AREAS SHALL BE PERMANENTLY STABILIZED.
7. NATURAL PLANT COVER SHALL BE MAINTAINED AND PROTECTED AND ALL TEMPORARY AND PERMANENT EROSION CONTROL MEASURES SHALL BE MAINTAINED AND REPAIRED AS NEEDED. THE GENERAL CONTRACTOR WILL BE RESPONSIBLE FOR INSPECTION AND REPAIR DURING CONSTRUCTION.
8. PROTECTION OF EXISTING FACILITIES AND UTILITIES - THE CONTRACTOR SHALL PROVIDE AND MAINTAIN ALL NECESSARY FACILITIES FOR THE PROTECTION OF ALL EXISTING UTILITIES ON OR ADJACENT TO THE PROJECT. ALL STORM SEWER FACILITIES THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED, FILTERED, OR OTHERWISE TREATED TO REMOVE SEDIMENT. FURTHERMORE, ANY DAMAGE DONE BY HIM, HIS AGENTS, OR ASSIGNS SHALL BE REPAIRED AT NO ADDITIONAL COMPENSATION.
9. DIRT ON PAVEMENT - WHERE A CONTRACTOR'S EQUIPMENT IS OPERATED UPON AN EXISTING PAVEMENT USED BY TRAFFIC THE CONTRACTOR SHALL CLEAN THE PAVEMENT OF ALL DIRT AND DEBRIS AT THE END OF EACH DAY'S OPERATIONS AND AT OTHER TIMES AS DIRECTED BY THE OWNER, THE ENGINEER, OR THE GOVERNING MUNICIPALITY.
10. DUST CONTROL - THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE CONTROL OF EXCESSIVE DUST DURING THE CONSTRUCTION PERIOD UNTIL PROPOSED IMPROVEMENTS COMPLETED. THE REQUIREMENT FOR DUST CONTROL SHALL BE AS DIRECTED BY THE LOCAL APPROVING AUTHORITIES OR THE ENGINEER.
11. THE EROSION CONTROL MEASURES INDICATED ON THE PLANS ARE THE MINIMUM REQUIREMENTS. ADDITIONAL MEASURES MAY BE REQUIRED, AS DIRECTED BY THE ENGINEER OR DOWNERS GROVE TOWNSHIP.
12. THE CONTRACTOR SHALL MAKE AN EFFORT TO MINIMIZE USE OF HEAVY EQUIPMENT WITHIN THE DESIGNATED DRAINAGE FACILITIES.

LIST OF ILLINOIS DOT HIGHWAY STANDARDS

280001-07	TEMPORARY EROSION CONTROL SYSTEMS
442201-03	CLASS C AND D PATCHES
701301-04	LANE CLOSURE, 2L, 2W, SHORT TIME OPERATIONS
701311-03	LANE CLOSURE 2L, 2W MOVING OPERATIONS – DAY ONLY
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701801-06	SIDEWALK CORNER OR CROSSWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS

D1 STANDARD DETAILS

TC-13	TYPICAL PAVEMENT MARKINGS
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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**GENERAL NOTES
CLARENDON HILLS ROAD - 91ST STREET TO 79TH STREET**

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2663	19-03126-00-RS	DUPAGE	14	2
CONTRACT NO. 61G59				
ILLINOIS FED. AID PROJECT 804V(658)				

SCALE: — SHEET NO. 1 OF 1 SHEETS STA. TO STA.

PLOT DATE: Nov 13, 2020 FILENAME: H:\187-PA\3893 - Clarendon Hills Road Improvements\Plans\Rev-3\187-TP-3009-SITE 2020-11-02.dwg

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PLOT SCALE *	CHECKED - AS	REVISED -
PLOT DATE = Nov 13, 2020	DATE -	REVISED -

SUMMARY OF QUANTITIES				CONSTRUCTION CODE 0005
CODE NO.	ITEM DESCRIPTION	UNIT	TOTAL	SUBTOTAL
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	17781	17781
40603200	POLYMERIZED HMA BINDER COURSE, IL-4.75, N50	TON	1216	1216
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	360	360
40604062	HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MX "D", N70	TON	2213	2213
42300100	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT - 5"	SQ YD	186	186
44000200	DRIVEWAY PAVEMENT REMOVAL	SQ YD	1004	1004
44000159	HMA SURFACE REMOVAL, 2.5"	SQ YD	583	583
44201713	CLASS D PATCHES, TYPE I, 6 INCH	SQ YD	184	184
44201717	CLASS D PATCHES, TYPE II, 6 INCH	SQ YD	919	919
44201721	CLASS D PATCHES, TYPE III, 6 INCH	SQ YD	735	735
44300100	AREA REFLECTIVE CRACK CONTROL TREATMENT	SQ YD	21699	21699
48203100	HOT-MIX ASPHALT SHOULDERS	TON	390	390
60300105	FRAMES AND GRATES TO BE ADJUSTED	EACH	4	4
67100100	MOBILIZATION	L SUM	1	1
70102620	TRAFFIC CONTROL AND PROTECTION, STANDARD 701501	L SUM	1	1
70102640	TRAFFIC CONTROL AND PROTECTION, STANDARD 701801	L SUM	1	1
70300100	SHORT TERM PAVEMENT MARKING	FOOT	7360	7360
70300150	SHORT TERM PAVEMENT MARKING REMOVAL	SQ FT	2453	2453
78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	17090	17090
78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	78	78
XX006215	BRICK PAVER REMOVAL AND REPLACEMENT	SQ YD	38	38
Z0004518	HMA DRIVEWAY PAVEMENT 5"	SQ YD	911	911
X4401198	HMA SURFACE REMOVAL, VARIABLE DEPTH	SQ YD	214	214
Z0030850	TEMPORARY INFORMATION SIGNING	SQ FT	50	50

* DENOTES SPECIALTY ITEMS

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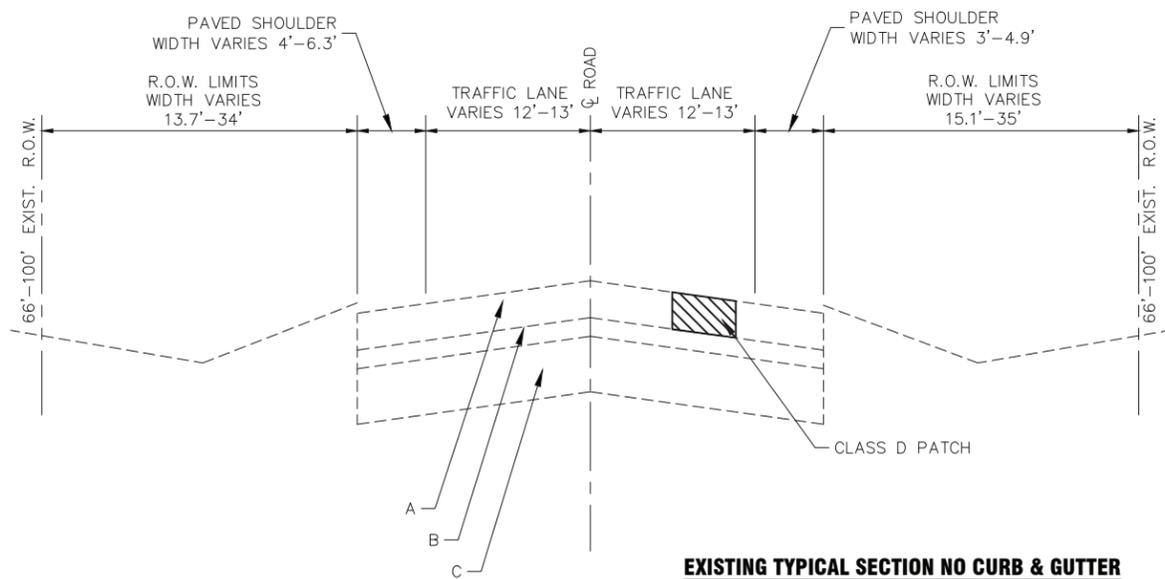
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES
CLARENDON HILLS ROAD - 91ST STREET TO 79TH STREET

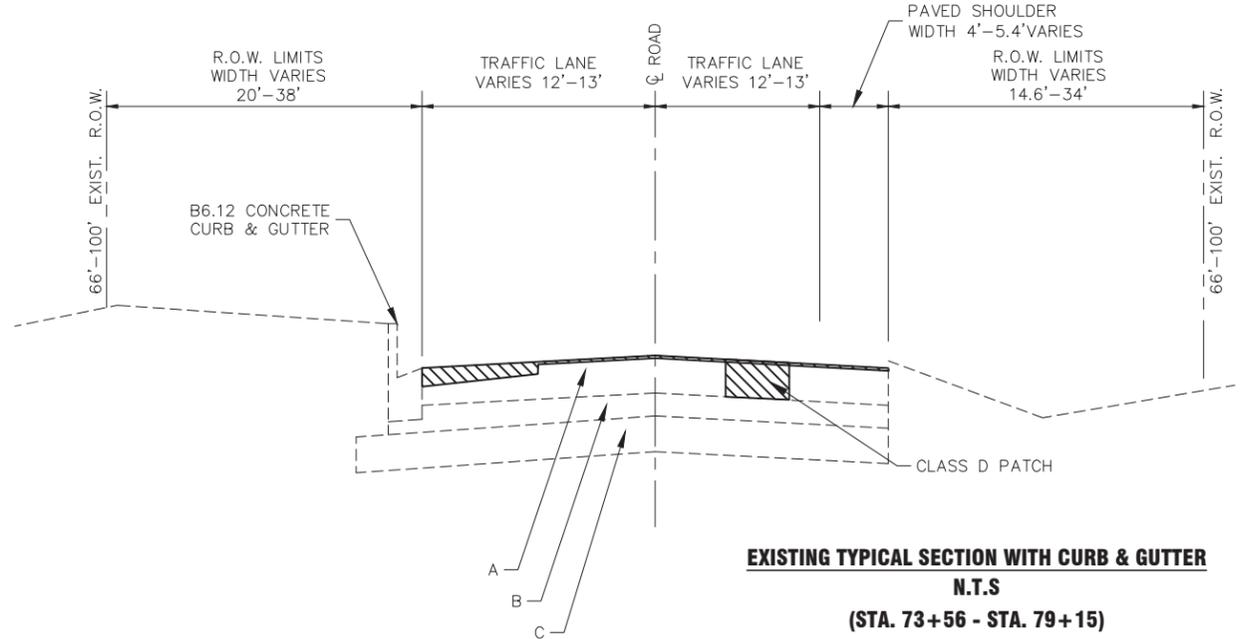
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2663	19-03126-00-RS	DUPAGE	14	3
CONTRACT NO. 61G59			ILLINOIS FED. AID PROJECT 804V(658)	



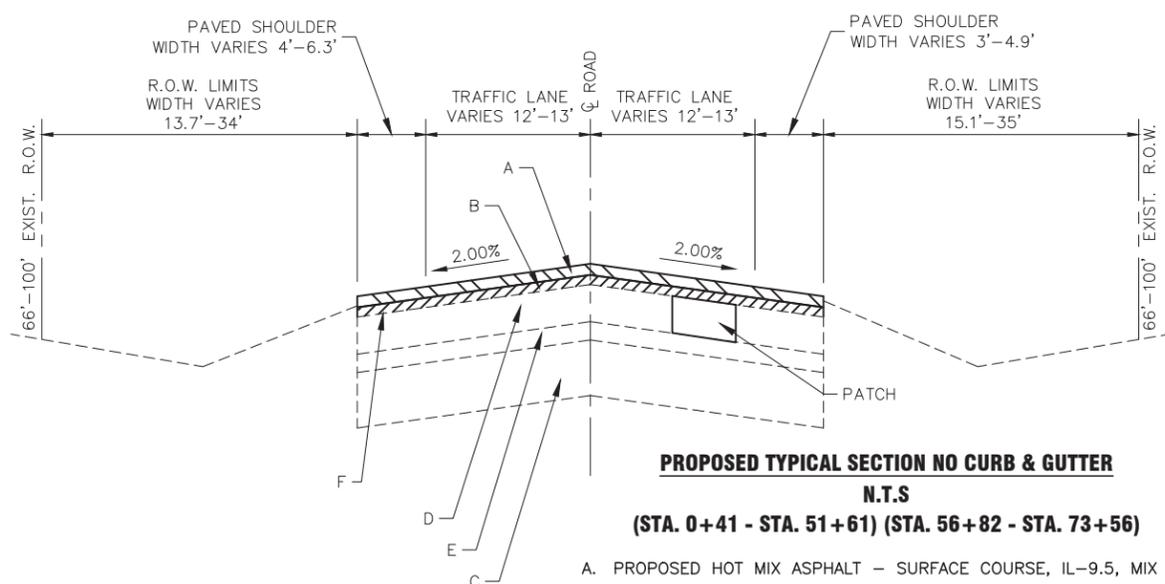
EXISTING TYPICAL SECTION NO CURB & GUTTER
N.T.S
(STA. 0+41 - STA. 51+61) (STA. 56+82 - STA. 73+56)

- A. EXISTING PAVEMENT (5" - 12")
- B. EXISTING AGGREGATE BASE COURSE (3.5" - 9")
- C. EXISTING SUBGRADE



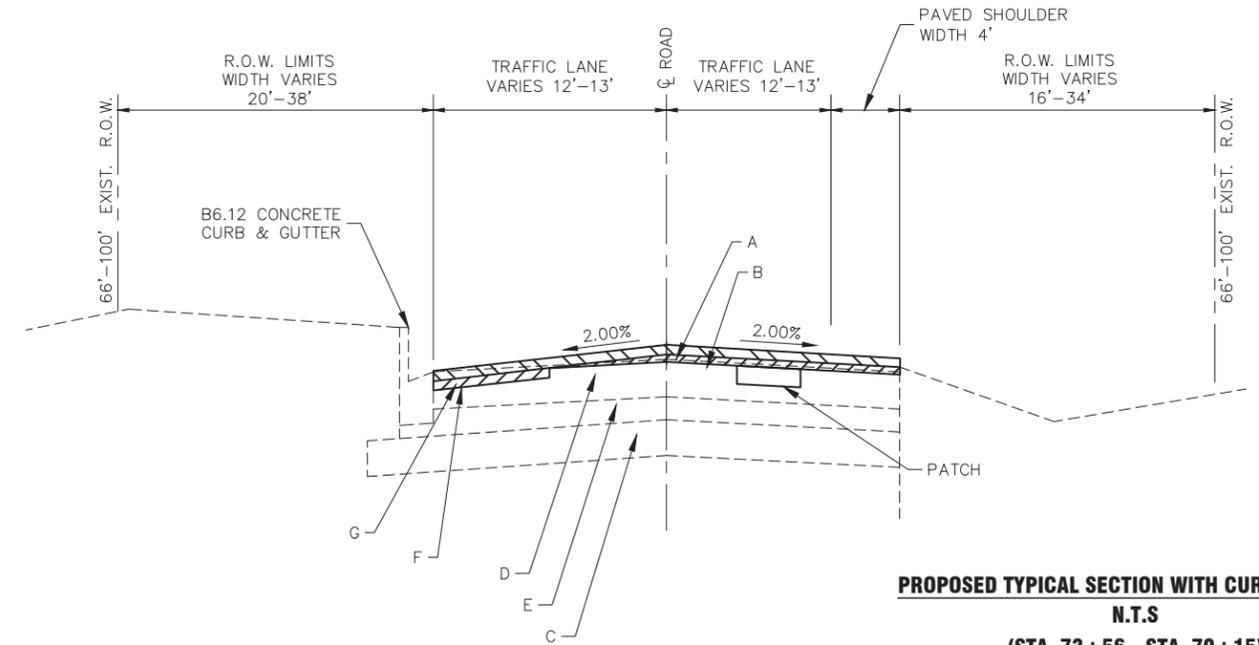
EXISTING TYPICAL SECTION WITH CURB & GUTTER
N.T.S
(STA. 73+56 - STA. 79+15)

- A. EXISTING PAVEMENT (5" - 12")
- B. EXISTING AGGREGATE BASE COURSE (3.5" - 9")
- C. EXISTING SUBGRADE



PROPOSED TYPICAL SECTION NO CURB & GUTTER
N.T.S
(STA. 0+41 - STA. 51+61) (STA. 56+82 - STA. 73+56)

- A. PROPOSED HOT MIX ASPHALT - SURFACE COURSE, IL-9.5, MIX "D", N70; 1.5"
- B. PROPOSED POLYMERIZED HMA BINDER COURSE, IL-4.75, N50; 1"
- C. EXISTING SUBGRADE
- D. EXISTING PAVEMENT (5" - 12")
- E. EXISTING AGGREGATE BASE COURSE (3.5" - 9")
- F. AREA REFLECTIVE CRACK CONTROL TREATMENT



PROPOSED TYPICAL SECTION WITH CURB & GUTTER
N.T.S
(STA. 73+56 - STA. 79+15)

- A. PROPOSED HOT MIX ASPHALT - SURFACE COURSE, IL-9.5 MIX "D", N70; 1.5"
- B. PROPOSED POLYMERIZED HMA BINDER COURSE, IL-4.75, N50; 1"
- C. EXISTING SUBGRADE
- D. EXISTING PAVEMENT (5" - 12")
- E. EXISTING AGGREGATE BASE COURSE (3.5" - 9")
- F. AREA REFLECTIVE CRACK CONTROL TREATMENT
- G. HMA SURFACE REMOVAL (2")

- PROPOSED HOT MIX ASPHALT SURFACE COURSE
- PROPOSED POLYMERIZED BINDER
- PROPOSED FILL
- REMOVAL

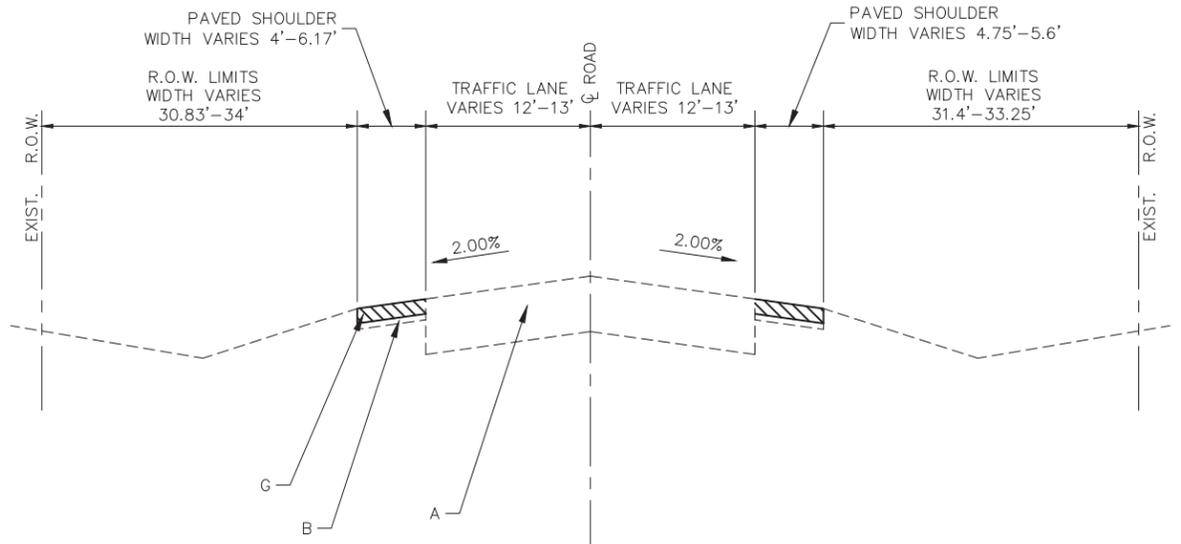
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DEPARTMENT OF TRANSPORTATION

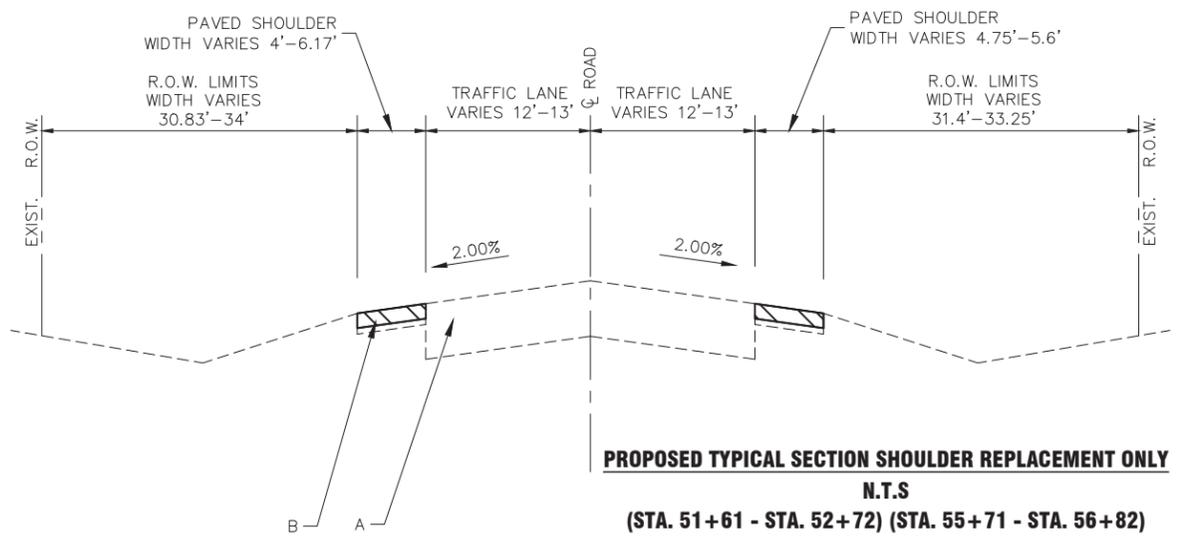
TYPICAL SECTIONS	
CLARENDON HILLS ROAD - 91ST STREET TO 79TH STREET	
SCALE: N.T.S.	SHEET NO. 1 OF 2 SHEETS
STA. _____	TO STA. _____

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2663	19-03126-00-RS	DUPAGE	14	4
				CONTRACT NO. 61G59
ILLINOIS FED. AID PROJECT 804V(658)				



EXISTING TYPICAL SECTION SHOULDER REPLACEMENT ONLY
N.T.S
(STA. 51+61 - STA. 52+72) (STA. 55+71 - STA. 56+82)

- A. EXISTING PCC PAVEMENT
- B. EXISTING HMA SHOULDER.
- G. HMA SURFACE REMOVAL (2")



PROPOSED TYPICAL SECTION SHOULDER REPLACEMENT ONLY
N.T.S
(STA. 51+61 - STA. 52+72) (STA. 55+71 - STA. 56+82)

- A. EXISTING PCC PAVEMENT
- B. PROPOSED HOT MIX ASPHALT - SHOULDER (2")

- PROPOSED HOT MIX ASPHALT SURFACE COURSE
- PROPOSED POLYMERIZED BINDER
- PROPOSED FILL
- REMOVAL

HOT-MIX MIXTURE REQUIREMENTS			
	MIXTURE TYPE	VOIDS	LIFT THICKNESS
HMA DRIVEWAY PAVEMENT 5"	HMA SURFACE, MIX "D", N50, (IL-9.5mm) 2" HMA BINDER COURSE, 3"	4% @ 50 Gyr	
		4% @ 50 Gyr	
PATCHING	CLASS D PATCH HMA BINDER COURSE, IL-19	4% @ 70 Gyr	
PAVEMENT & SHOULDER RESURFACING	HMA SURFACE COURSE, MIX "D", N70, (IL-9.5mm) POLYMERIZED HMA BINDER COURSE, IL-4.75, N50	4% @ 70 Gyr	1.5"
		3.5% @ 50 Gyr	1" MAX.
HMA SHOULDER	HMA SURFACE COURSE, MIX "D", N70, (IL-9.5mm)	4% @ 70 Gyr	2"

THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.

FOR USE OF RECYCLED MATERIALS, SEE SPECIAL PROVISIONS.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY SPECIAL PROVISIONS.

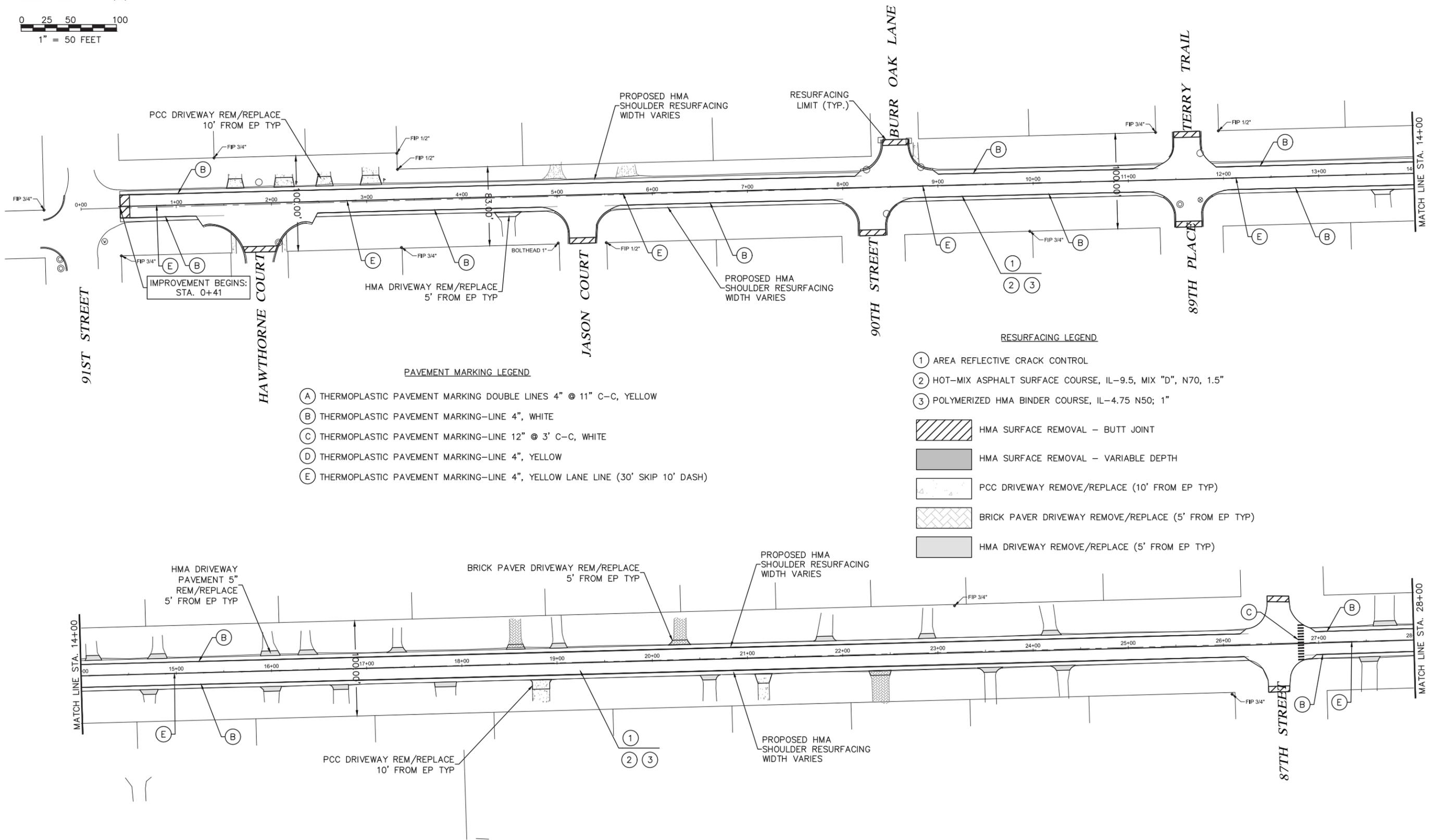
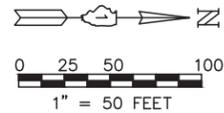
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USER NAME *	DESIGNED - EF	REVISED - 7/10/2020
	DRAWN - CJS	REVISED -
PLOT SCALE *	CHECKED - AS	REVISED -
PLOT DATE = Nov 17, 2020	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS
CLARENDON HILLS ROAD - 91ST STREET TO 79TH STREET
 SCALE: N.T.S. SHEET NO. 2 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2663	19-03126-00-RS	DUPAGE	14	5
CONTRACT NO. 61G59				
ILLINOIS FED. AID PROJECT 804V(658)				



- PAVEMENT MARKING LEGEND**
- (A) THERMOPLASTIC PAVEMENT MARKING DOUBLE LINES 4" @ 11" C-C, YELLOW
 - (B) THERMOPLASTIC PAVEMENT MARKING-LINE 4", WHITE
 - (C) THERMOPLASTIC PAVEMENT MARKING-LINE 12" @ 3' C-C, WHITE
 - (D) THERMOPLASTIC PAVEMENT MARKING-LINE 4", YELLOW
 - (E) THERMOPLASTIC PAVEMENT MARKING-LINE 4", YELLOW LANE LINE (30' SKIP 10' DASH)

- RESURFACING LEGEND**
- (1) AREA REFLECTIVE CRACK CONTROL
 - (2) HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 1.5"
 - (3) POLYMERIZED HMA BINDER COURSE, IL-4.75 N50; 1"
 - [Hatched Box] HMA SURFACE REMOVAL - BUTT JOINT
 - [Solid Grey Box] HMA SURFACE REMOVAL - VARIABLE DEPTH
 - [Dotted Box] PCC DRIVEWAY REMOVE/REPLACE (10' FROM EP TYP)
 - [Cross-hatched Box] BRICK PAVER DRIVEWAY REMOVE/REPLACE (5' FROM EP TYP)
 - [Light Grey Box] HMA DRIVEWAY REMOVE/REPLACE (5' FROM EP TYP)

NOTES:
 1. REFER TO DISTRICT 1 TYPICAL PAVEMENT MARKINGS (TC-13) FOR ADDITIONAL INFORMATION.
 2. REFER TO TYPICAL SECTIONS.

PLOT DATE: Nov 17, 2020
 FILENAME: H:\18-TP-3005-SITE_2020-11-02.dwg
 Clarendon Hills Road Improvements\Plans\Rev-3\18-TP-3005-SITE_2020-11-02.dwg

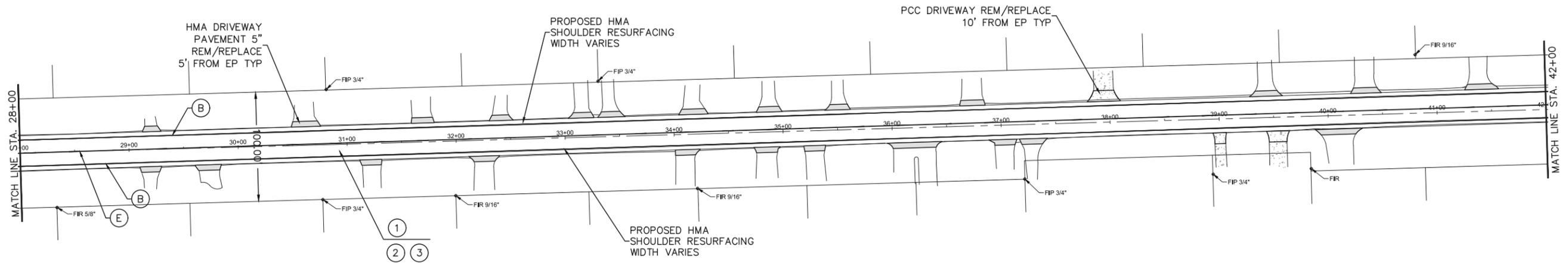
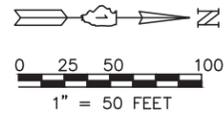
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PLOT SCALE *	CHECKED - AS	REVISED -
PLOT DATE = Nov 17, 2020	DATE -	REVISED -

**STATE OF ILLINOIS
 DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING PLANS - CLARENDON HILLS RD.
 CLARENDON HILLS ROAD - 91ST STREET TO 79TH STREET**

SCALE: 1"=50' SHEET NO. 1 OF 3 SHEETS STA. 0+00 TO STA. 28+00

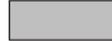
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2663	19-03126-00-RS	DUPAGE	14	6
CONTRACT NO. 61G59			ILLINOIS FED. AID PROJECT 804V(658)	

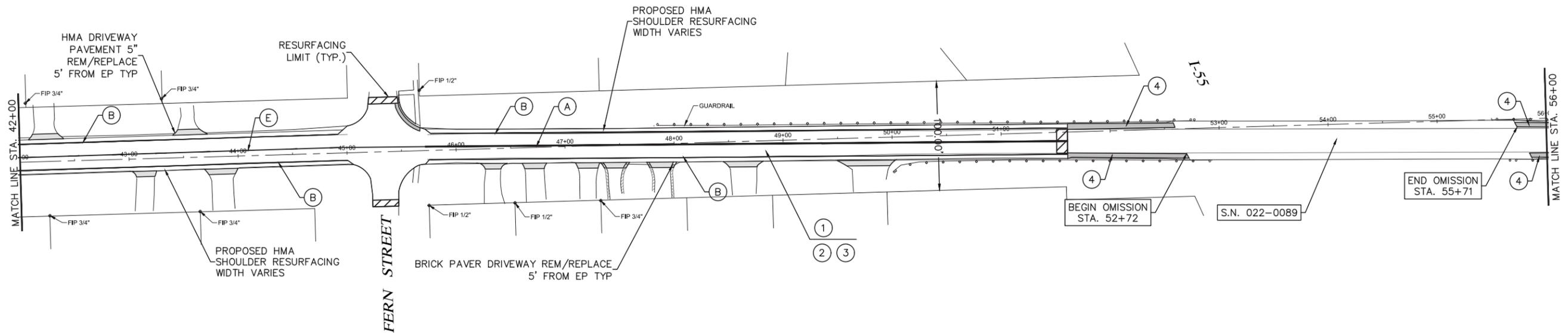


PAVEMENT MARKING LEGEND

- (A) THERMOPLASTIC PAVEMENT MARKING DOUBLE LINES 4" @ 11" C-C, YELLOW
- (B) THERMOPLASTIC PAVEMENT MARKING-LINE 4", WHITE
- (C) THERMOPLASTIC PAVEMENT MARKING-LINE 12" @ 3' C-C, WHITE
- (D) THERMOPLASTIC PAVEMENT MARKING-LINE 4", YELLOW
- (E) THERMOPLASTIC PAVEMENT MARKING-LINE 4", YELLOW LANE LINE (30' SKIP 10' DASH)

RESURFACING LEGEND

- (1) AREA REFLECTIVE CRACK CONTROL
 - (2) HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 1.5"
 - (3) POLYMERIZED HMA BINDER COURSE IL-4.75 N50; 1"
 - (4) HOT-MIX ASPHALT SHOULDER (2")
-  HMA SURFACE REMOVAL - BUTT JOINT
 HMA SURFACE REMOVAL - VARIABLE DEPTH



**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

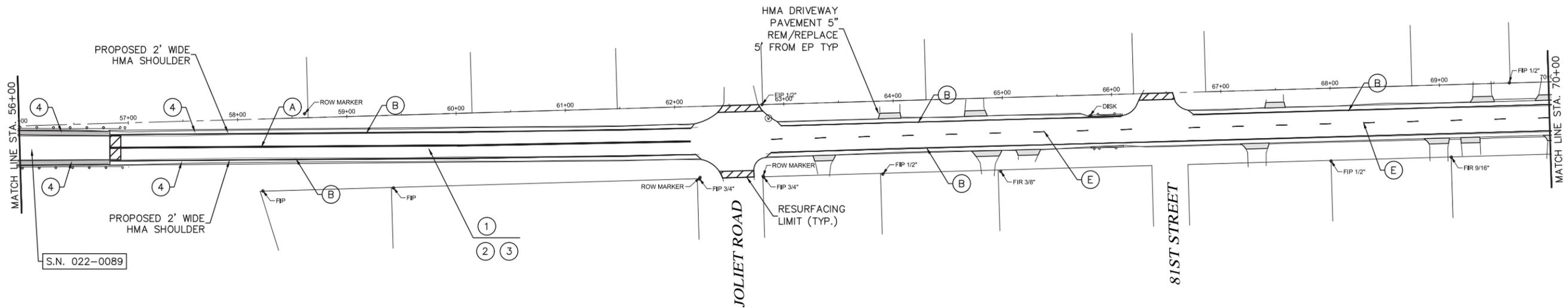
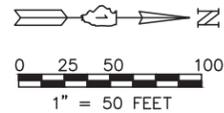
**ROADWAY AND PAVEMENT MARKING PLANS - CLARENDON HILLS RD.
CLARENDON HILLS ROAD - 91ST STREET TO 79TH STREET**

USER NAME *	DESIGNED - EF	REVISED - 7/10/2020
PLOT SCALE *	DRAWN - CJS	REVISED -
PLOT DATE = Nov 17, 2020	CHECKED - AS	REVISED -
	DATE -	REVISED -

SCALE: 1"=50' SHEET NO. 2 OF 3 SHEETS STA. 28+00 TO STA. 56+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2663	19-03126-00-RS	DUPAGE	14	7
CONTRACT NO. 61G59				
ILLINOIS FED. AID PROJECT 804V(658)				

PLOT DATE: Nov 17, 2020
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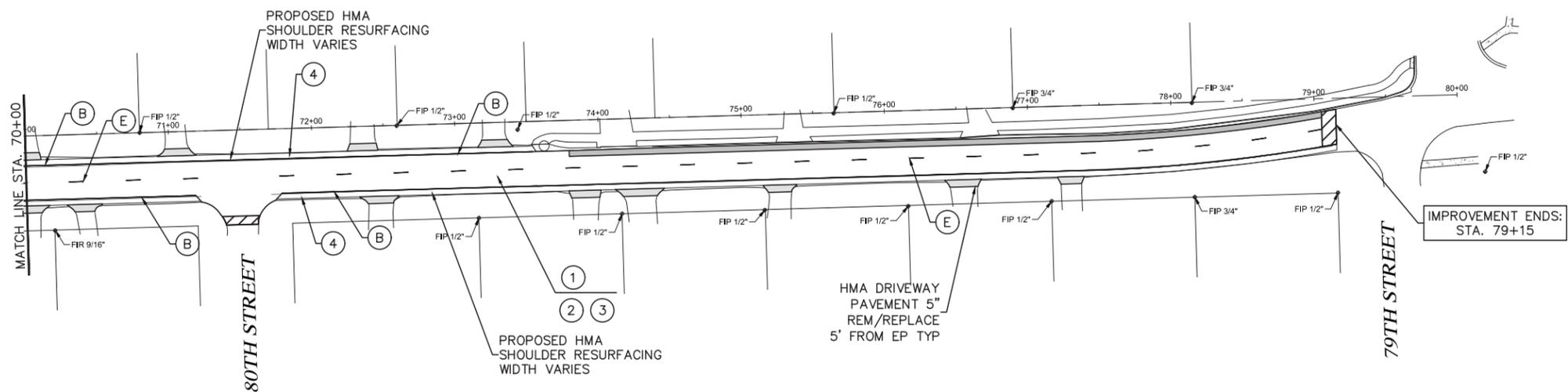
S.N. 022-0089

PAVEMENT MARKING LEGEND

- (A) THERMOPLASTIC PAVEMENT MARKING DOUBLE LINES 4" @ 11" C-C, YELLOW
- (B) THERMOPLASTIC PAVEMENT MARKING-LINE 4", WHITE
- (C) THERMOPLASTIC PAVEMENT MARKING-LINE 12" @ 3' C-C, WHITE
- (D) THERMOPLASTIC PAVEMENT MARKING-LINE 4", YELLOW
- (E) THERMOPLASTIC PAVEMENT MARKING-LINE 4", YELLOW LANE LINE (30' SKIP 10' DASH)

RESURFACING LEGEND

- (1) AREA REFLECTIVE CRACK CONTROL
 - (2) HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 1.5"
 - (3) POLYMERIZED HMA BINDER COURSE IL-4.75 N50; 1"
 - (4) HOT-MIX ASPHALT SHOULDER (2")
- HMA SURFACE REMOVAL - BUTT JOINT
 HMA SURFACE REMOVAL - VARIABLE DEPTH



IMPROVEMENT ENDS:
STA. 79+15

PLOT DATE: Nov 17, 2020
 FILENAME: H:\18-TP\3099 - Clarendon Hills Road Improvements\Plans\Rev-3\18-TP-3099-SITE 2020-11-02.dwg

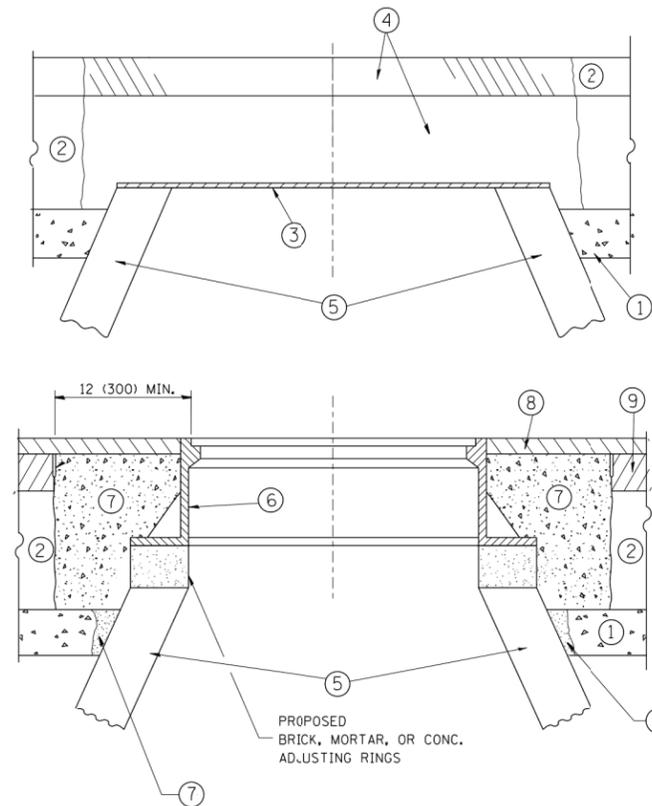
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PLOT DATE = Nov 17, 2020	DATE -	REVISED -

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ROADWAY AND PAVEMENT MARKING PLANS - CLARENDON HILLS RD.
CLARENDON HILLS ROAD - 91ST STREET TO 79TH STREET**

SCALE: 1"=50' SHEET NO. 3 OF 3 SHEETS STA. 56+00 TO STA. 79+15

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2663	19-03126-00-RS	DUPAGE	14	8
			CONTRACT NO. 61G59	
ILLINOIS FED. AID PROJECT 804V(658)				



CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1* CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

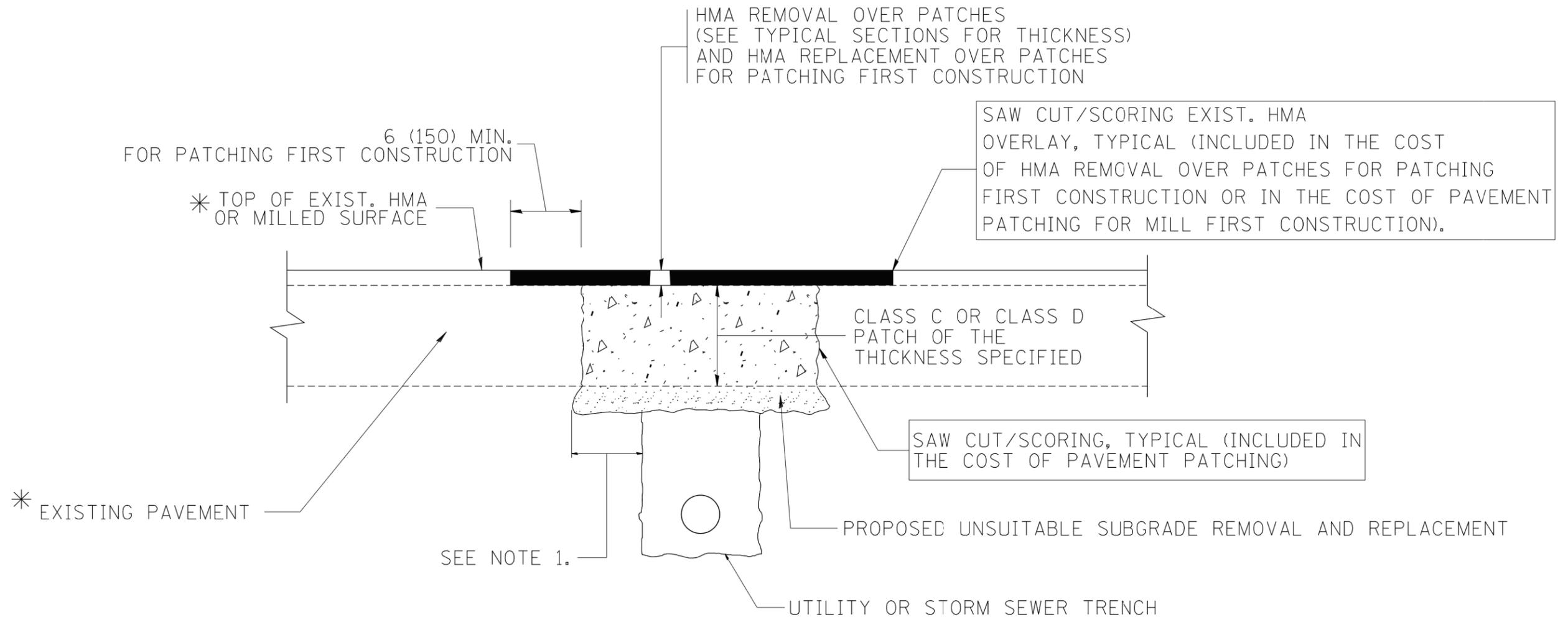
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = bauerdl	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04
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	PLOT DATE = 12/6/2011	DATE - 10-25-94	REVISED - R. BORO 12-06-11

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2663	19-03126-10-RS	DUPAGE	14	9
BD600-03 (BD-8)		CONTRACT NO. 61G59		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT 804V(658)				



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

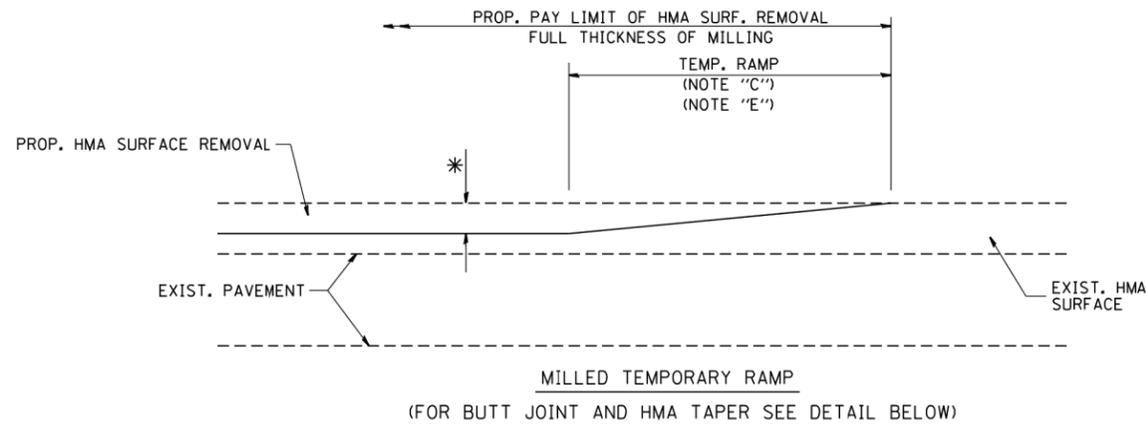
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		CHECKED -	REVISED - R. BORO 09-04-07
		DATE - 10-25-94	REVISED - K. ENG 10-27-08

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

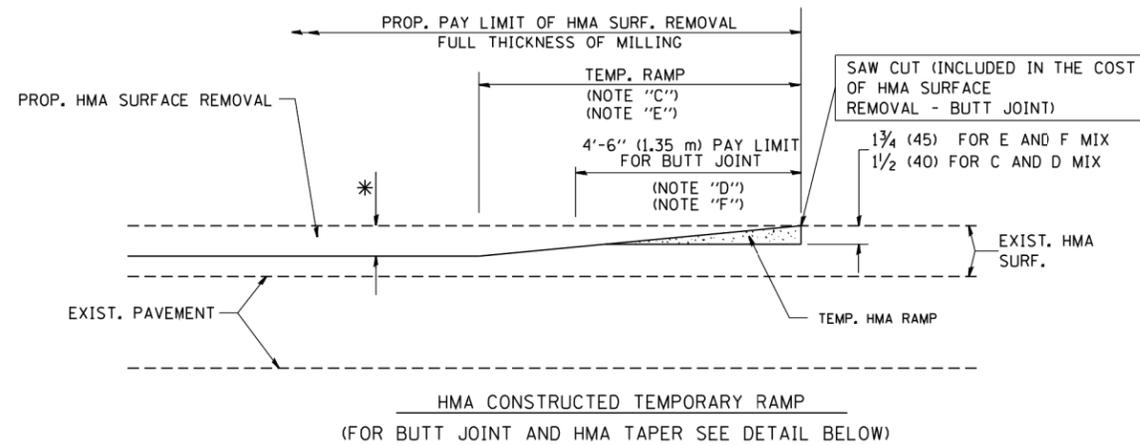
**PAVEMENT PATCHING FOR
HMA SURFACED PAVEMENT**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2663	19-03126-10-RS	DUPAGE	14	10
BD400-04 (BD-22)		CONTRACT NO. 61G59		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT 804V(658)				

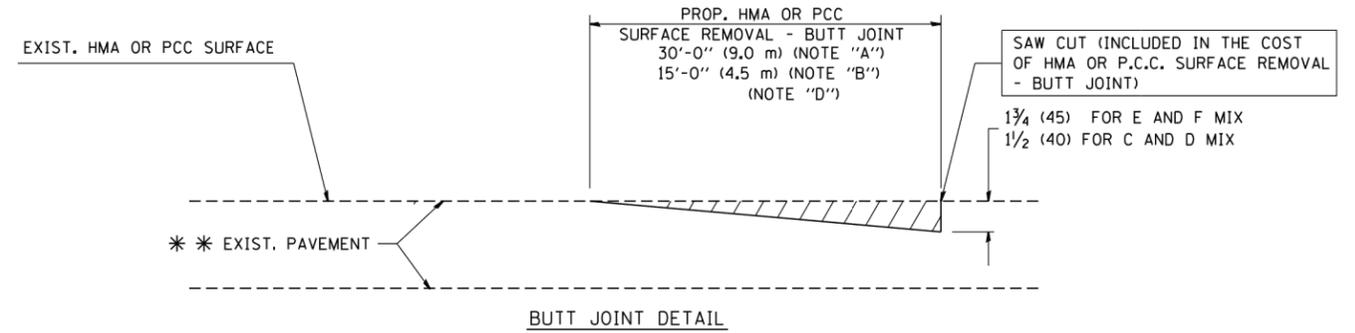


OPTION 1

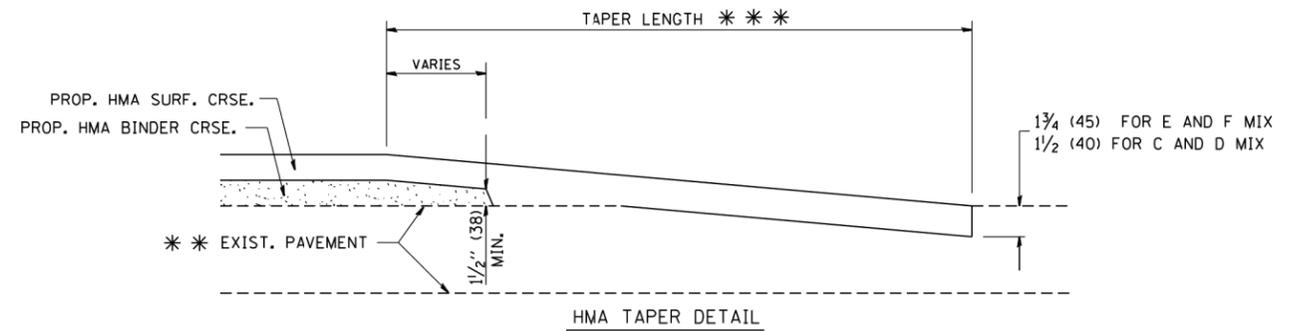


OPTION 2

TYPICAL TEMPORARY RAMP



BUTT JOINT DETAIL



HMA TAPER DETAIL

TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

* * * PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

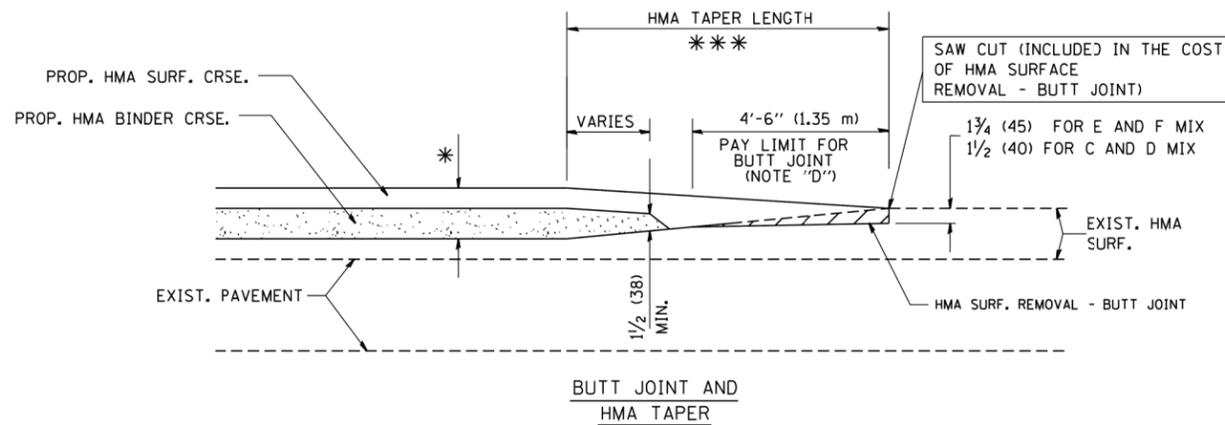
NOTES

- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
 - B: MINOR SIDE ROADS.
 - C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
 - D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
 - E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
 - F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
 - G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- * * * 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.



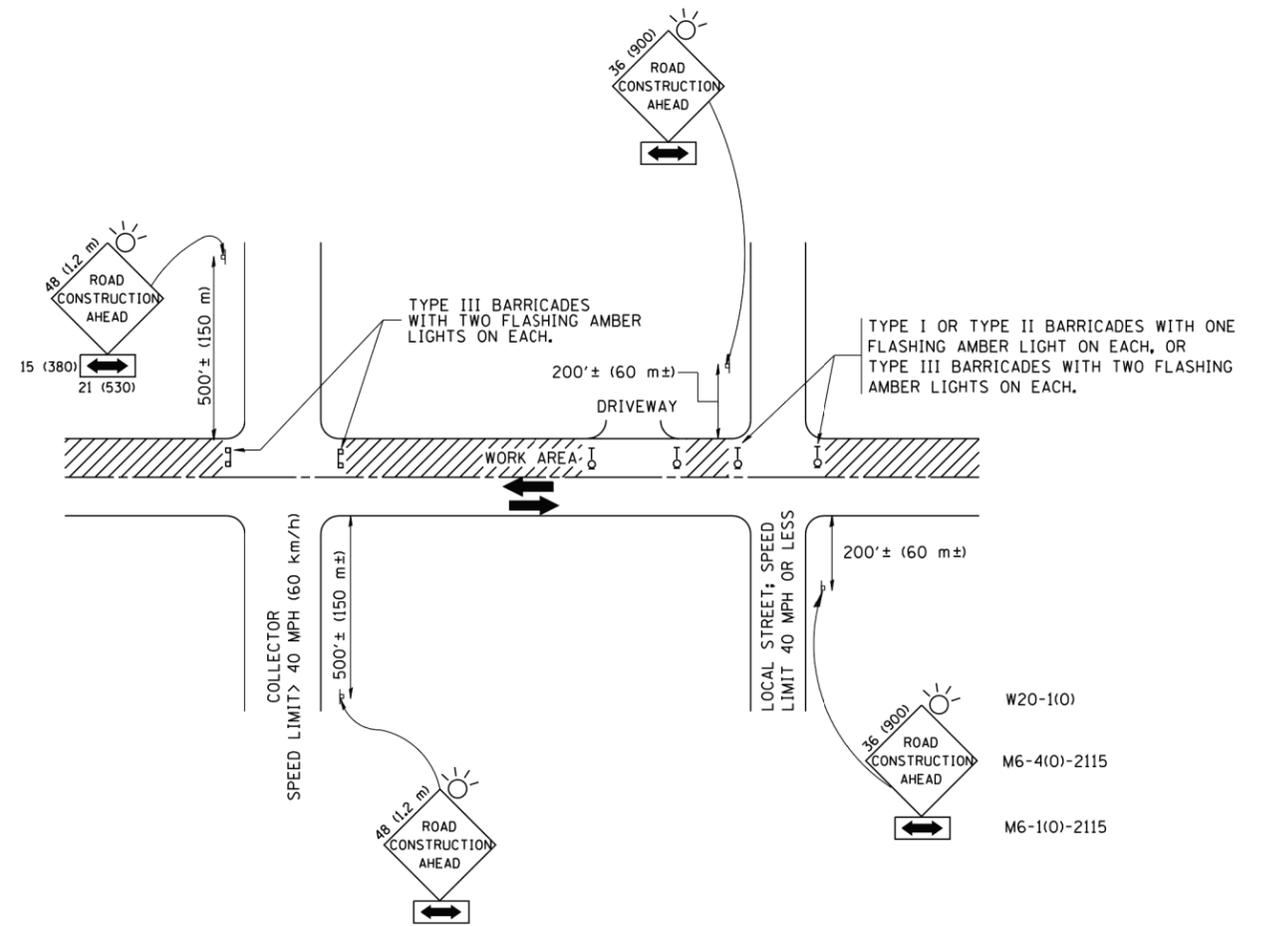
TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING

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		DRAWN -	REVISED - A. ABBAS 03-21-97
	PLOT SCALE = 50.0000' / IN.	CHECKED -	REVISED - M. GOMEZ 04-06-01
	PLOT DATE = 1/4/2008	DATE - 06-13-90	REVISED - R. BORO 01-01-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

BUTT JOINT AND HMA TAPER DETAILS	
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS
STA.	TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2663	19-03126-10-RS	DUPAGE	14	11
BD400-05 BD32		CONTRACT NO. 61G59		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT 804V(658)				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.

2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:

a) ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.

b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.

3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.

C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.

D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

- W20-1(0)
- M6-4(0)-2115
- M6-1(0)-2115

All dimensions are in millimeters (inches) unless otherwise shown.

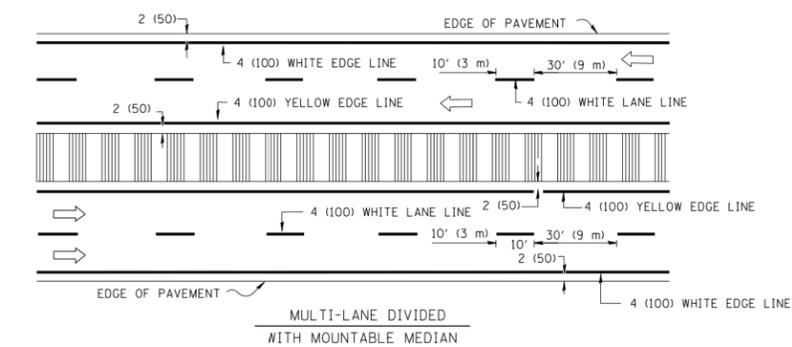
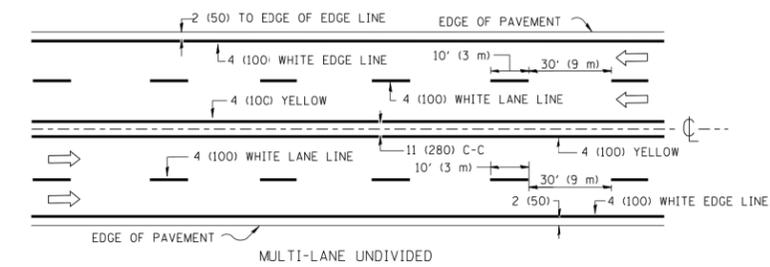
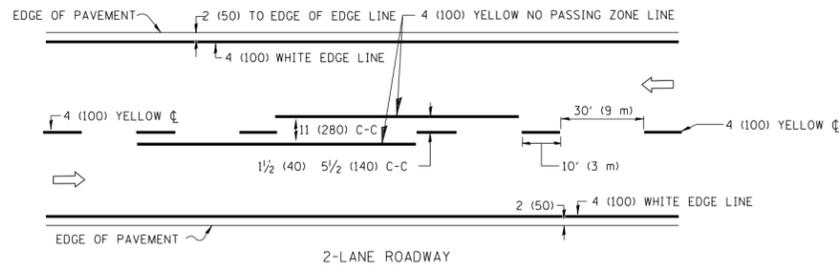
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		DRAWN -	REVISED - A. HOUSEH 03-06-96
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED - A. HOUSEH 10-15-96
	PLOT DATE = 1/4/2008	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

TRAFFIC CONTROL AND PROTECTION FOR
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

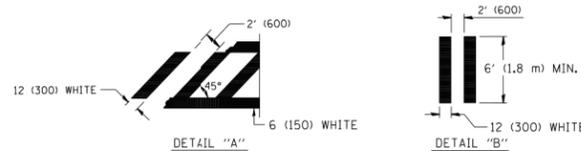
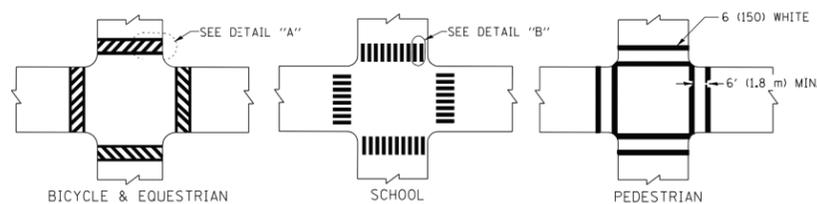
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2663	19-03126-10-RS	DUPAGE	14	12
TC-10			CONTRACT NO. 61G59	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT 804V(658)				

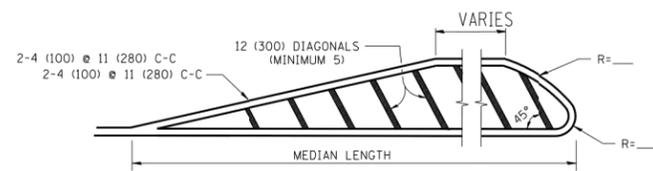
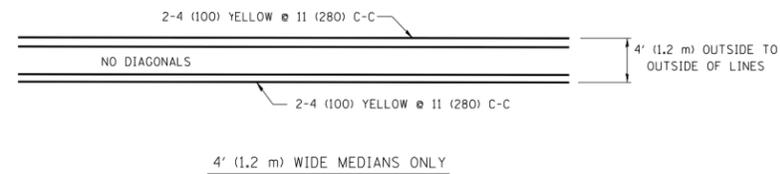


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

TYPICAL LANE AND EDGE LINE MARKING

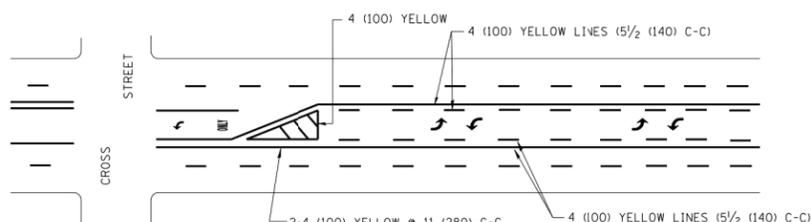


TYPICAL CROSSWALK MARKING

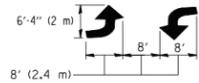


DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

MEDIANS OVER 4' (1.2 m) WIDE

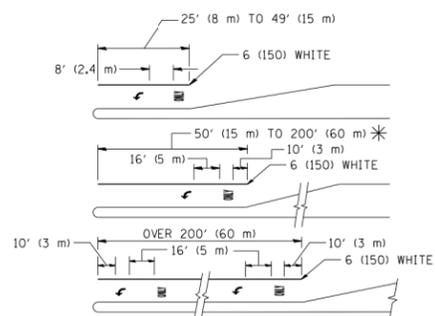


A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

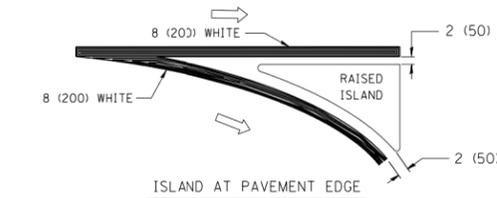
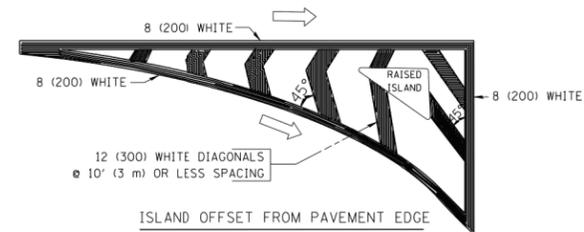
TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED.
AREA = 15.6 SQ. FT. (1.5 m²) ONLY AREA = 20.8 SQ. FT. (1.9 m²)
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5/2 (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5/2 (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 @ 6 (150) 12 (300) @ 45° 12 (300) @ 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45° NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h) 30' (9 m) C-C (OVER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES: "RR" 15' (4.5 m) MIN. LETTERS: 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33 m²) EACH "X"=54.0 SQ. FT. (5.0 m²)
SHOULDER DIAGONALS	12 (300) @ 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C 30 MPH (50 km/h) TO 45MPH (70 km/h) 150' (45 m) C-C (OVER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

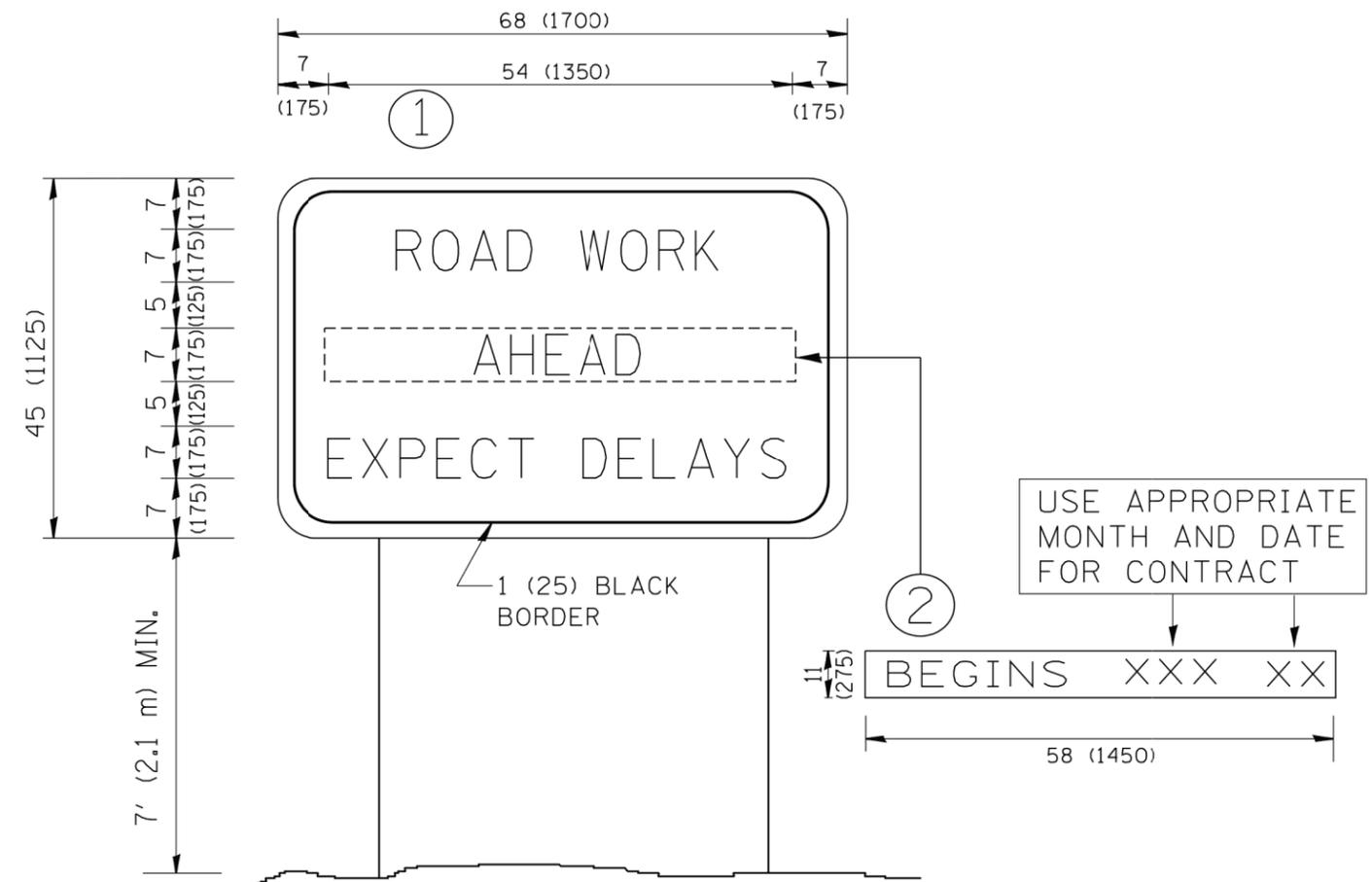
All dimensions are in inches (millimeters) unless otherwise shown.

FILE NAME =	USER NAME = drivakosgn	DESIGNED - EVERS	REVISED - T. RAMMACHER 10-27-94
c:\pwwork\pwwork\drivakosgn\d0108315\td0108315.dgn		DRAWN -	REVISED - C. JUCIUS 09-09-09
	PLOT SCALE = 50.000' / IN.	CHECKED -	REVISED -
	PLOT DATE = 9/9/2009	DATE - 03-19-90	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DISTRICT ONE			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.

F.A.U. RT.:	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2663	19-03126-10-RS	DUPAGE	14	13
TC-13		CONTRACT NO. 61G59		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT 804V(658)				



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME = W:\diststd\22x34\tc22.dgn	USER NAME = geglanoht	DESIGNED - DRAWN -	REVISED - R. MIRS 09-15-97
		CHECKED -	REVISED - R. MIRS 12-11-97
	PLOT SCALE = 50.000' / IN.	DATE -	REVISED - T. RAMMACHER 02-02-99
	PLOT DATE = 1/4/2008		REVISED - C. JUCIUS 01-31-07

**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD
INFORMATION SIGN**

SCALE: NONE SHEET NO. 1 OF 1 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2663	19-03126-10-RS	DUPAGE	14	14
TC-22			CONTRACT NO. 61G59	
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT 804V(658)				