

INDEX OF SHEETS

1	COVER SHEET
2	INDEX OF SHEETS, HIGHWAY STANDARDS, GENERAL NOTES, COMMITMENTS, AND MIX TABLE
3-4	SUMMARY OF QUANTITIES
5	TYPICAL SECTIONS
6	ALIGNMENT, TIES, AND BENCHMARKS
7	MAINTENANCE OF TRAFFIC - GENERAL NOTES
8-11	MAINTENANCE OF TRAFFIC - PRE-POST STAGE
12-15	MAINTENANCE OF TRAFFIC - STAGE 1
16-19	MAINTENANCE OF TRAFFIC - STAGE 2
20	MAINTENANCE OF TRAFFIC - TEMPORARY SIDEWALK RAMP DETAIL
21-23	EXISTING AND REMOVAL PLAN
24-28	ROADWAY PLAN AND PROFILE
29	APPROACH PAVEMENT HMA OVERLAY DETAIL
30-32	PAVEMENT MARKING AND SIGNING
33-65	STRUCTURE NO. 016-1011 PLANS
66	EXISTING STRUCTURE NO. 016-1011 PLANS
67-79	DISTRICT ONE DETAILS

HIGHWAY STANDARDS

STD NO.	TITLE
000001-08	STANDARD SYMBOLS, ABBREVIATIONS AND PATTERNS
001001-02	AREAS OF REINFORCEMENT BARS
001006	DECIMAL OF AN INCH AND OF A FOOT
420001-09	PAVEMENT JOINTS
515001-04	NAME PLATE FOR BRIDGES
602011-02	CATCH BASIN TYPE C
606001-07	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
606301-04	PC CONCRETE ISLANDS AND MEDIANS
701101-05	OFF-RD OPERATIONS, MULTILANE, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
701427-05	LANE CLOSURE, MULTILANE, INTERMITTENT OR MOVING OPER., FOR SPEEDS ≤ 40 MPH
701501-06	URBAN LANE CLOSURE, 2L, 2W, UNDIVIDED
701601-09	URBAN LANE CLOSURE, MULTILANE, 1W OR 2W WITH NONTRAVERSABLE MEDIAN
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701611-01	URBAN HALF ROAD CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER OR CROSSWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES
704001-08	TEMPORARY CONCRETE BARRIER
780001-05	TYPICAL PAVEMENT MARKINGS
781001-04	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS

GENERAL NOTES

1.

ALL BORROW/WASTE/USE SITES MUST BE APPROVED BY THE DEPARTMENT PRIOR TO REMOVING ANY MATERIAL FROM THE PROJECT OR INITIATING ANY EARTHMOVING ACTIVITIES, INCLUDING TEMPORARY STOCKPILING OUTSIDE THE LIMITS OF CONSTRUCTION.
2.

THE AREA TO BE TACKED OR PRIMED SHALL BE LIMITED TO THAT WHICH CAN BE COVERED WITH HMA ON THE NEXT DAY'S PRODUCTION, BUT NO MORE THAN FIVE (5) DAYS IN ADVANCE OF THE PLACEMENT OF THE HMA, UNLESS APPROVED BY THE ENGINEER.
3.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR COLLECTING AND MAINTAINING AN ELECTRONIC LOG OF ALL STAKEOUT SURVEY THAT IS PERFORMED ON THE JOB, EITHER BY HIM/HER OR ANY SUB-CONTRACTOR PERFORMING THE STAKEOUT. UPON REQUEST, ALL LOGS SHALL BE SUBMITTED TO THE DEPARTMENT. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR THIS WORK, BUT SHALL BE CONSIDERED INCLUDED IN THE COST FOR CONSTRUCTION LAYOUT.
4.

ANY REFERENCE TO A STANDARD IN THESE PLANS SHALL BE INTERPRETED TO MEAN THE LATEST EDITION.
5.

THE RESIDENT ENGINEER SHALL CONTACT THE AREA TRAFFIC FIELD ENGINEER A MINIMUM OF TWO WEEKS PRIOR TO PLACEMENT OF PERMANENT PAVEMENT MARKINGS.

EMAD ALHUSSEINI - emad.alhusseini@illinois.gov
6.

THE CONTRACTOR SHALL CONTACT J.U.L.I.E. (1-800-892-0123) 48 HOURS PRIOR TO ANY WORK IN THE RIGHT OF WAY OR EASEMENTS TO LOCATE UTILITIES, AND CONTACT THE OWNER'S REPRESENTATIVE SHOULD PUBLIC UTILITIES APPEAR TO BE IN CONFLICT WITH THE PROPOSED IMPROVEMENTS.
7.

IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.
8.

THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION FROM THE DEPARTMENT.
9.

THE CONTRACTOR SHALL TAKE CARE TO PROTECT ALL SIGNS ALONG THE ROUTE OF CONSTRUCTION. SIGNS SHALL BE REMOVED IF THEY ARE IN CONFLICT WITH PROPOSED WORK, AND APPROVED BY ENGINEER RED AND ALL LABOR NECESSARY TO COMPLY WITH THE RELOCATION OF SIGNS SHALL BE INCLUDED IN THE CONTRACT WITHOUT ANY EXTRA COMPENSATION ALLOWED TO THE CONTRACTOR ACCORDING TO ART. 107.25, UNLESS MARKED ON PLANS.
10.

THE THICKNESS OF HMA SHOWN ON THE PLANS IS THE NOMINAL THICKNESS. DEVIATIONS FROM THE NOMINAL THICKNESS WILL BE PERMITTED WHEN SUCH DEVIATIONS OCCUR DUE TO IRREGULARITIES IN THE EXISTING SURFACE OR BASE ON WHICH THE HMA IS PLACED.
11.

ALL ELEVATIONS IN THE PLANS ARE BASED UPON THE NORTH AMERICAN VERTICAL DATUM OF 1988 (NAVD88).
12.

ALL REFERENCES IN THE HIGHWAY STANDARDS AND STANDARD SPECIFICATIONS FOR REINFORCEMENT, DOWEL BARS, AND TIE BARS IN PAVEMENT, SHOULDERS, CURB, GUTTER, COMBINATION CURB AND GUTTER AND MEDIAN, AND CHAIR SUPPORTS FOR CRC PAVEMENT SHALL BE EPOXY COATED UNLESS NOTED ON THE PLANS.
13.

WHEN MILLED PAVEMENT IS OPEN TO TRAFFIC THE MAXIMUM GRADE DIFFERENTIAL BETWEEN PASSES OF THE MILLING MACHINE SHALL NOT EXCEED 1 1/2 INCHES WHERE THE SPEED LIMIT IS 45 MPH OR LESS WITH WRITTEN APPROVAL FROM THE ENGINEER. A MAXIMUM GRADE DIFFERENTIAL OF 3 INCHES MAY BE ALLOWED IF THE EDGE OF THE MILLING IS SLOPED A MINIMUM 1:3 V:H.
14.

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
15.

DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
16.

THE DEPARTMENT HAS NOT OBTAINED ANY PERMITS FOR OFFSITE BORROW OR WASTE/USE (BWU) AREAS. PRIOR TO WORKING IN BWU AREAS, IF THE CONTRACTOR CHOOSES TO USE ACTIVITIES REQUIRING PERMITS IT IS THE CONTRACTOR'S RESPONSIBILITY TO SECURE PROPER PERMITS. IN ADDITION TO THE BORROW REVIEW (BDE 2289) AND USE/WASTE REVIEW (BDE 2290) SUBMITTALS, THE CONTRACTOR WILL NEED TO SUBMIT AN EROSION AND SEDIMENT CONTROL (ESC) PLAN FOR EVERY BWU SITE TO THE DEPARTMENT FOR ACCEPTANCE. GUIDELINES FOR ACCEPTABLE BWU PRACTICES CAN BE FOUND IN SECTION II.G.I AND 2 OF THE SWPPP. THE COST OF ALL MATERIALS AND LABOR NECESSARY TO COMPLY WITH THE ABOVE PROVISIONS TO PREPARE AND IMPLEMENT ESC PLANS WILL NOT BE PAID FOR SEPARATELY, BUT SHALL BE CONSIDERED AS INCLUDED IN THE UNIT BID PRICES OF THE CONTRACT AND NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
17.

ANY EXISTING PAVEMENT MARKINGS IN CONFLICT WITH PROPOSED PAVEMENT MARKINGS SHALL BE REMOVED.
18.

THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, THE VILLAGES OF MELROSE PARK AND FRANKLIN PARK, AND THE INDIANA HARBOR BELT RR.
19.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING UTILITY PROPERTY DURING CONSTRUCTION OPERATIONS AS OUTLINED IN ARTICLE 107.31 OF THE STANDARD SPECIFICATIONS. MEMBERS OF J.U.L.I.E. KNOWN TO BE WITHIN THE LIMITS OF THE IMPROVEMENT ARE:

COMMONWEALTH EDISON CO.
ATTN:
1919 SWIFT ROAD
OAK BROOK, IL 60523
630-424-5704

NICOR GAS
ATTN:
1844 FERRY ROAD
NAPERVILLE, IL 60563
630-388-3830
20.

A FULL DEPTH SAWCUT OF REMOVAL ITEMS AS NOTED ON THE PLANS, SPECIFIED IN THE STANDARD SPECIFICATIONS, OR REQUIRED BY THE RESIDENT ENGINEER SHALL BE INCLUDED IN THE COST OF THE ITEM BEING REMOVED.

21. THE CONTRACTOR SHALL CONTACT THE D1 TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNON-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS IN ADVANCE OF BEGINNING WORK.

COMMITMENTS:

NONE

HOT-MIX ASPHALT MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ Ndes	QUALITY MANAGEMENT PROGRAM (QMP)
PAVEMENT RESURFACING		
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70	4% @ 70 GYR	QC/QA
HOT-MIX ASHPALT BINDER COURSE, IL-9.5, N70	4% @ 70 GYR	QC/QA
TEMPORARY PAVEMENT		
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 1.5"	4% @ 70 GYR	QC/QA
HOT-MIX ASHPALT BINDER COURSE, IL-9.5, N70, 1.5"	4% @ 70 GYR	QC/QA
TEMPORARY RAMP		
HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "D", N70, 1.75"	4% @ 70 GYR	QC/QA

QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/QA)

1. THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQYD/IN.
2. FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY SPECIAL PROVISIONS.
3. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.
4. QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.

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ENTIRE SHEET REVISED 1/4/2021



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DRAWN	-	SVJ
CHECKED	-	PJM
DATE	-	08/28/2020

REVISED	-
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

INDEX OF SHEETS, HIGHWAY STANDARDS,
GENERAL NOTES, COMMITMENTS, AND HMA MIX TABLE

SCALE: 2.0000 ' / in. SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62M80				
		ILLINOIS	FED. AID PROJECT	

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SP	CODE NO.	ITEM	UNIT	CONSTRUCTION CODE	
				BRIDGE	
				0059 80% FEDERAL 20% STATE 25th Avenue SN 016-1011	
				URBAN	
	31101100	SUBBASE GRANULAR MATERIAL , TYPE B	CU YD	1	
	40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	446	
	40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SQ YD	351	
	40600990	TEMPORARY RAMP	SQ YD	93	
	40602985	HOT-MIX ASHPALT BINDER COURSE, 1L-9.5, N70	TON	20	
	40604062	HOT-MIX ASPHALT SURFACE COURSE, 1L-9.5, MIX "D", N70	TON	94	
	42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SQ FT	594	
	44000161	HOT-MIX ASPHALT SURFACE REMOVAL , 3"	SQ YD	215	
	44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	72	
	44000600	SIDEWALK REMOVAL	SQ FT	594	
	44003100	MEDIAN REMOVAL	SQ FT	1219	
	50102400	CONCRETE REMOVAL	CU YD	69.6	
	50157300	PROTECTIVE SHIELD	SQ YD	8867	
	50300225	CONCRETE STRUCTURES	CU YD	1	
	50300255	CONCRETE SUPERSTRUCTURE	CU YD	78.8	
	X5030250	BRIDGE DECK GROOVING (LONGITUDINAL)	SQ YD	6686	
	50300300	PROTECTIVE COAT	SQ YD	7333	
	50500405	FURNISHING AND ERECTING STRUCTURAL STEEL	POUND	29400	
	50800205	REINFORCEMENT BARS, EPOXY COATED	POUND	11070	
	50800515	BAR SPLICERS	EACH	98	
	52000110	PREFORMED JOINT STRIP SEAL	FOOT	460	
	59000200	EPOXY CRACK INJECTION	FOOT	24	
	59300100	CONTROLLED LOW-STRENGTH MATERIAL	CU YD	3	
	60250200	CATCH BASINS TO BE ADJUSTED	EACH	2	
	60255500	MANHOLES TO BE ADJUSTED	EACH	1	

SP	CODE NO.	ITEM	UNIT	CONSTRUCTION CODE	
				BRIDGE	
				0059 80% FEDERAL 20% STATE 25th Avenue SN 016-1011	
				URBAN	
	60603800	COMBINATION CONCRETE CURB AND GUTTER , TYPE B-6.12	FOOT	72	
	67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	12	
	67100100	MOBILIZATION	L SUM	1	
	70103815	TRAFFIC CONTROL SURVEILLANCE	CAL DA	195	
	70107025	CHANGEABLE MESSAGE SIGN	CAL DA	390	
	70300900	PAVEMENT MARKING TAPE , TYPE IV - LETTERS AND SYMBOLS	SQ FT	296	
	70300904	PAVEMENT MARKING TAPE , TYPE IV 4"	FOOT	18779	
	70300906	PAVEMENT MARKING TAPE , TYPE IV 6"	FOOT	2418	
	70400100	TEMPORARY CONCRETE BARRIER	FOOT	1825	
	70400200	RELOCATE TEMPORARY CONCRETE BARRIER	FOOT	1713	
	70600305	IMPACT ATTENUATORS, TEMPORARY (NON - REDIRECTIVE, NARROW), TEST LEVEL 2	EACH	1	
	70600341	IMPACT ATTENUATORS, RELOCATE (NON - REDIRECTIVE, NARROW), TEST LEVEL 2	EACH	1	
*	78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ FT	182	
*	78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	3365	
*	78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	873	
*	78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	427	
*	78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	85	
*	78008310	POLYUREA PAVEMENT MARKING TYPE II - LINE 4"	FOOT	3270	
*	78100100	RAISED REFLECTIVE PAVEMENT MARKER	EACH	4	
*	78100300	REPLACEMENT REFLECTOR	EACH	111	
	78300200	RAISED REFLECTIVE PAVEMENT MARKER REMOVAL	EACH	72	
	X0322102	TEMPORARY SIDEWALK RAMP	EACH	1	
	X0325748	ACRYLIC COATING	SQ YD	42	
	X0325749	FIBER WRAP	SQ FT	377	
	X0326766	CLEAN & RESEAL RELIEF JOINT	FOOT	113	
*	X0327980	PAVEMENT MARKING REMOVAL - WATER BLASTING	SQ FT	2930	

1 REVISED SHEET 1/4/2021
* SPECIALTY ITEM REV-SEP



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

SUMMARY OF QUANTITIES

SCALE: 2.0000 ' / in. SHEET 1 OF 2 SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-203-BR	COOK	79	3
CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

MAINTENANCE OF TRAFFIC GENERAL NOTES

1. TRAFFIC CONDITIONS, CRASHES, AND OTHER UNFORESEEN EMERGENCY CONDITIONS MAY REQUIRE THE ENGINEER TO RESTRICT, MODIFY OR REMOVE LANE CLOSURES OR CHANNELIZATION SHOWN ON THE PLANS. THE CONTRACTOR SHALL MAKE THE NECESSARY ADJUSTMENTS AS DIRECTED BY THE ENGINEER WITHOUT DELAY. COMPLIANCE WITH THIS REQUIREMENT SHALL BE CONSIDERED INCLUDED IN THE CONTRACT UNIT PRICE OF THE MAINTENANCE OF TRAFFIC ITEM. NO ADDITIONAL COMPENSATION WILL BE ALLOWED.
2. THE CONTRACTOR IS DIRECTED TO THE FACT THAT OTHER SEPARATE CONTRACTS ARE, OR MAY BE, IN FORCE THAT INTERSECT THE LIMITS OF THIS PROJECT. THE CONTRACTOR SHALL COOPERATE WITH THE OTHER CONTRACTORS IN THE PHASING AND PERFORMANCE OF THIS WORK SO AS NOT TO DELAY, INTERRUPT, OR HINDER THE PROGRESS OR COMPLETION OF THE WORK BEING PERFORMED BY OTHER CONTRACTORS. NO ADDITIONAL COMPENSATION WILL BE ALLOWED FOR COMPLIANCE WITH THE ABOVE REQUIREMENTS, NOR FOR ANY DELAYS OR INCONVENIENCES RESULTING FROM THE ACTIVITIES OF OTHER CONTRACTORS. SHOULD A CONFLICT ARISE BETWEEN THE CONTRACTORS WITH RESPECT TO SEQUENCE OF CONSTRUCTION OR MAINTENANCE OF TRAFFIC REQUIREMENTS, SAID CONFLICTS SHALL BE RESOLVED BY, OR AT THE DIRECTION OF THE ENGINEER.
3. THE MAINTENANCE OF TRAFFIC PLANS SHALL SERVE AS A GUIDE FOR THE SAFE DIVERSION OF TRAFFIC DURING EXECUTION OF THIS CONTRACT. THE CONTRACTOR MAY MODIFY THE MAINTENANCE OF TRAFFIC PLANS TO MEET CONSTRUCTION NEEDS, BUT NOT AT THE EXPENSE OF PUBLIC SAFETY OR CONVENIENCE. ANY CHANGES TO THE TRAFFIC CONTROL SHALL BE SUBMITTED TO THE ENGINEER FOR REVIEW AND APPROVAL. THE ENGINEER SHALL BE INFORMED IN WRITING A MINIMUM OF 48 HOURS IN ADVANCE OF ANY CHANGE TO THE MAINTENANCE OF TRAFFIC PLANS.
4. THE CONTRACTOR SHALL REMOVE ALL TEMPORARY OR EXISTING PAVEMENT MARKINGS THAT CONFLICT WITH THE STAGING. REMOVAL OF PAVEMENT MARKING TAPE WILL BE PAID FOR AS "TEMPORARY PAVEMENT MARKING REMOVAL" (X7030005). REMOVAL OF PAVEMENT MARKINGS ON PERMANENT PAVEMENT WILL BE PAID FOR AS "PAVEMENT MARKING REMOVAL - WATER BLASTING" (X0327980).
5. THE CONTRACTOR SHALL PROVIDE 48 HOURS ADVANCE NOTICE TO THE ENGINEER OF ANY CONSTRUCTION WORK THAT MAY IMPACT ANY ROADWAY LIGHTING.
6. THE CONTRACTOR SHALL PROPERLY DISPOSE OF ANY EXISTING SIGNS REMOVED, BUT NOT RELOCATED, FROM THE PROJECT. THE CONTRACTOR SHALL PROPERLY STORE RELOCATED SIGNS AS APPROVED BY THE ENGINEER UNTIL THEY ARE PROPERLY RE-ERECTED.
7. THE FOLLOWING IS A LIST OF MAINTENANCE OF TRAFFIC ASSOCIATED PAY ITEMS FOR WHICH ESTIMATED QUANTITIES HAVE BEEN PROVIDED. THE CONTRACTOR SHALL RECEIVE WRITTEN AUTHORIZATION FROM THE ENGINEER PRIOR TO USE AND PAYMENT FOR THESE ITEMS:

- *PAVEMENT MARKING TAPE, TYPE IV - LETTERS AND SYMBOLS
- *PAVEMENT MARKING TAPE, TYPE IV 4"
- *PAVEMENT MARKING TAPE, TYPE IV 6"
- *TEMPORARY CONCRETE BARRIER
- *RELOCATE TEMPORARY CONCRETE BARRIER
- *CHANGEABLE MESSAGE SIGN

- IMPACT ATTENUATORS, TEMPORARY (NON- REDIRECTIVE, NARROW), TEST LEVEL 2
- IMPACT ATTENUATORS, RELOCATE (NON- REDIRECTIVE, NARROW), TEST LEVEL 2

MAINTENANCE OF TRAFFIC STAGING DESCRIPTION

PRE-STAGE

CONSTRUCTION:

PRE-STAGE INCLUDES REMOVAL OF MEDIAN CURB AND PLACING TEMPORARY PAVEMENT.

TRAFFIC:

LANE CLOSURE OF INSIDE LANE IN EACH DIRECTION.

25TH AVENUE NORTHBOUND AND SOUTHBOUND 1 LANE IN EACH DIRECTION.

DRUMS WILL BE USED TO SEPARATE TRAFFIC FROM THE WORK ZONE.

STAGE I

CONSTRUCTION:

BRIDGE REPAIRS, PLACE TEMPORARY RAMP, PAVEMENT MARKING ON NORTHBOUND 25TH AVENUE AND OFF-RD IMPROVEMENTS.

CONTRACTOR MUST NOTIFY BUSINESSES 7 CALENDAR DAYS IN ADVANCE OF PROHIBITED LEFT TURN MOVEMENTS.

TRAFFIC:

LANE CLOSURE OF 1 LANE IN EACH DIRECTION AND MEDIAN CROSSOVER OF NB TRAFFIC.

25TH AVENUE NORTHBOUND AND SOUTHBOUND 1 LANE IN EACH DIRECTION.

TEMPORARY CONCRETE BARRIER AND DRUMS WILL BE USED TO SEPARATE TRAFFIC FROM THE WORK ZONE.

TUBULAR MARKERS WILL SEPARATE OPPOSING DIRECTIONS OF TRAFFIC.

STAGE II

CONSTRUCTION:

BRIDGE REPAIRS, PLACE TEMPORARY RAMP, PAVEMENT MARKING ON SOUTHBOUND 25TH AVENUE AND OFF-RD IMPROVEMENTS.

TRAFFIC:

LANE CLOSURE OF 1 LANE IN EACH DIRECTION AND MEDIAN CROSSOVER OF SB TRAFFIC.

25TH AVENUE NORTHBOUND AND SOUTHBOUND 1 LANE IN EACH DIRECTION.

TEMPORARY CONCRETE BARRIER AND DRUMS WILL BE USED TO SEPARATE TRAFFIC FROM THE WORK ZONE.

TUBULAR MARKERS WILL SEPARATE OPPOSING DIRECTIONS OF TRAFFIC.

POST-STAGE

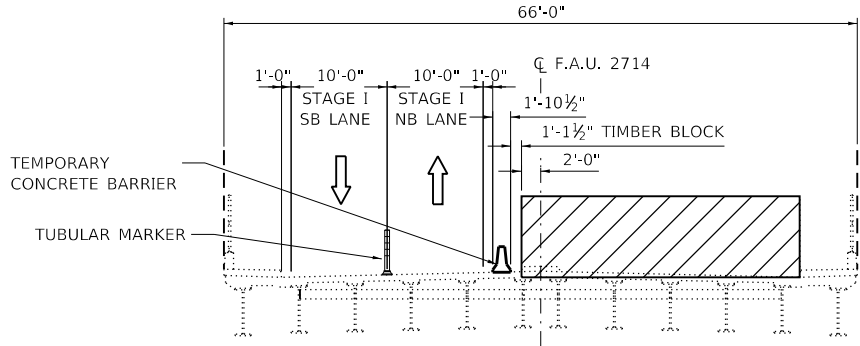
CONSTRUCTION:

POST-STAGE INCLUDES HMA SURFACE REMOVAL, HMA SURFACE REMOVAL - BUTT JOINT, TEMPORARY RAMP REMOVAL, CONSTRUCTION OF CONCRETE MEDIAN, HMA BINDER COURSE, HMA SURFACE COURSE, HMA OVERLAY, AND PAVEMENT STRIPING.

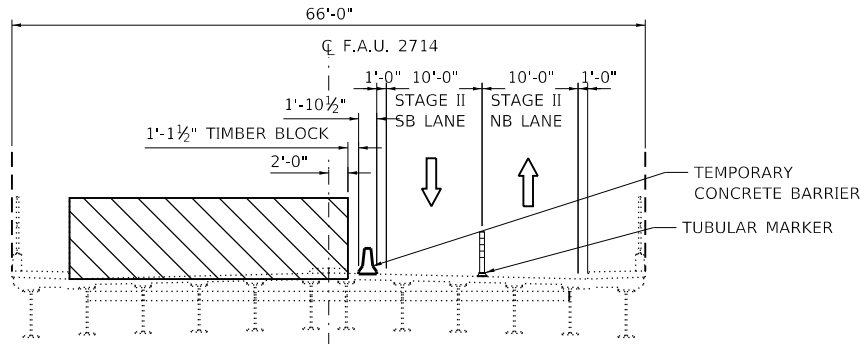
TRAFFIC:

DAYTIME LANE CLOSURE OF INSIDE LANE IN EACH DIRECTION.

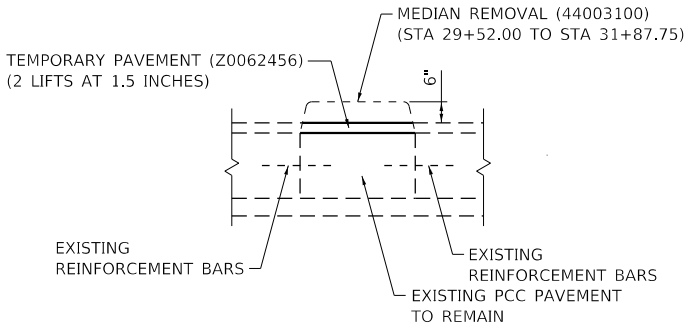
DRUMS WILL BE USED TO SEPARATE TRAFFIC FROM THE WORK ZONE.



ROADWAY TYPICAL SECTION – STAGE I



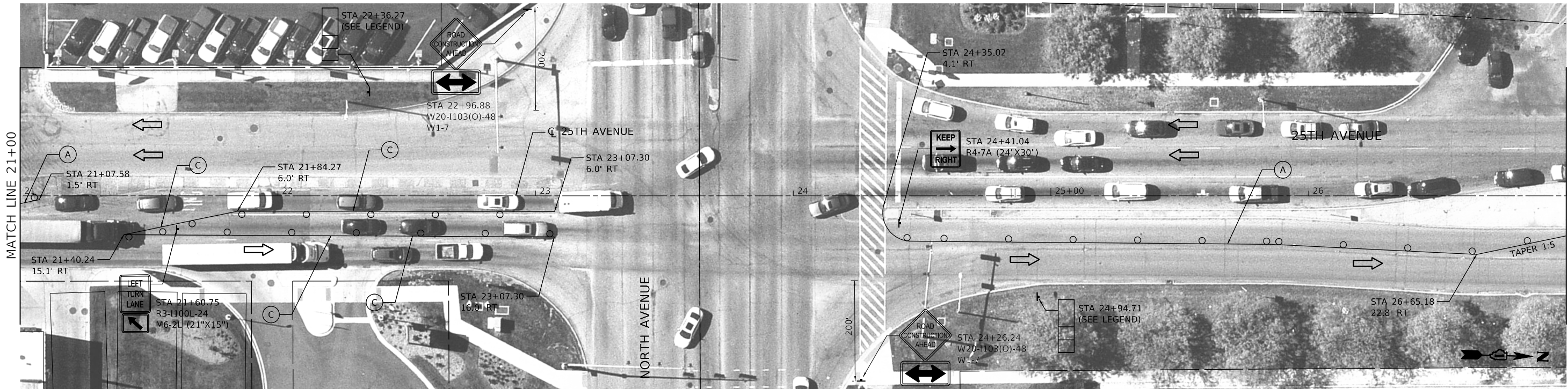
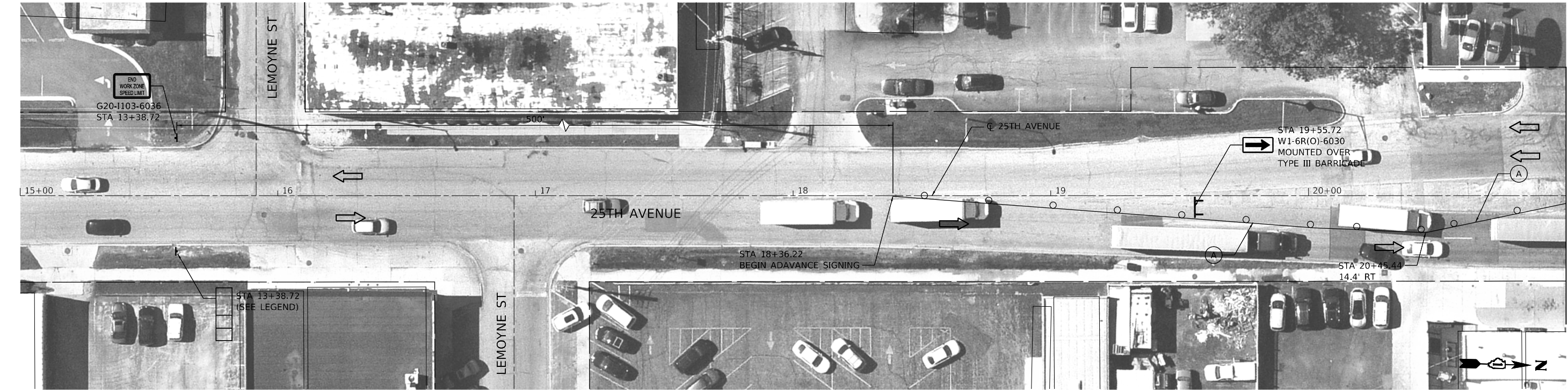
ROADWAY TYPICAL SECTION – STAGE II



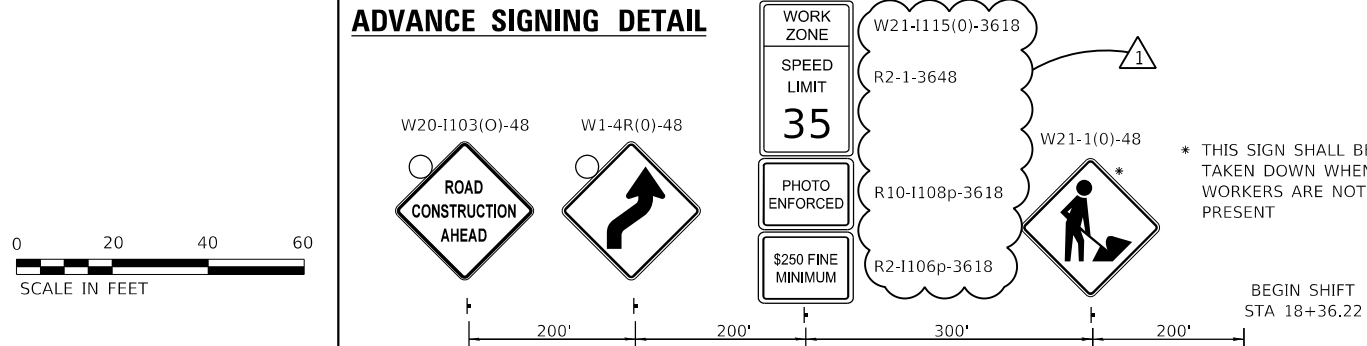
TEMPORARY PAVEMENT DETAIL

(SEE TYPICALS FOR MEDIAN REMOVAL DETAIL)

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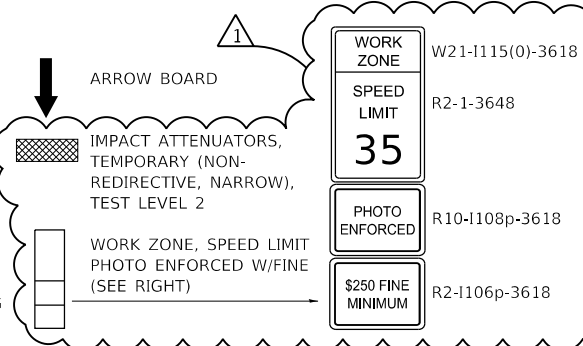


ADVANCE SIGNING DETAIL



LEGEND

- WORK ZONE
- DIRECTION OF TRAFFIC
- DRUMS/TYPE II BARRICADES
50' IN TANGENTS
20' ON TAPERS
10' IN RADI/CURVES
- TRAFFIC SIGN
- TYPE III BARRICADE WITH FLASHING MONODIRECTIONAL LIGHTS



PAVEMENT MARKING LEGEND

- (A) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID YELLOW
- (B) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID WHITE
- (C) PAVEMENT MARKING TAPE, TYPE IV 6", SOLID WHITE
- (D) PAVEMENT MARKING TAPE, TYPE IV - LETTERS AND SYMBOLS, WHITE

1 REVISED SHEET 1/5/2021

SED

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REVISED - 12/22/2020 SVJ
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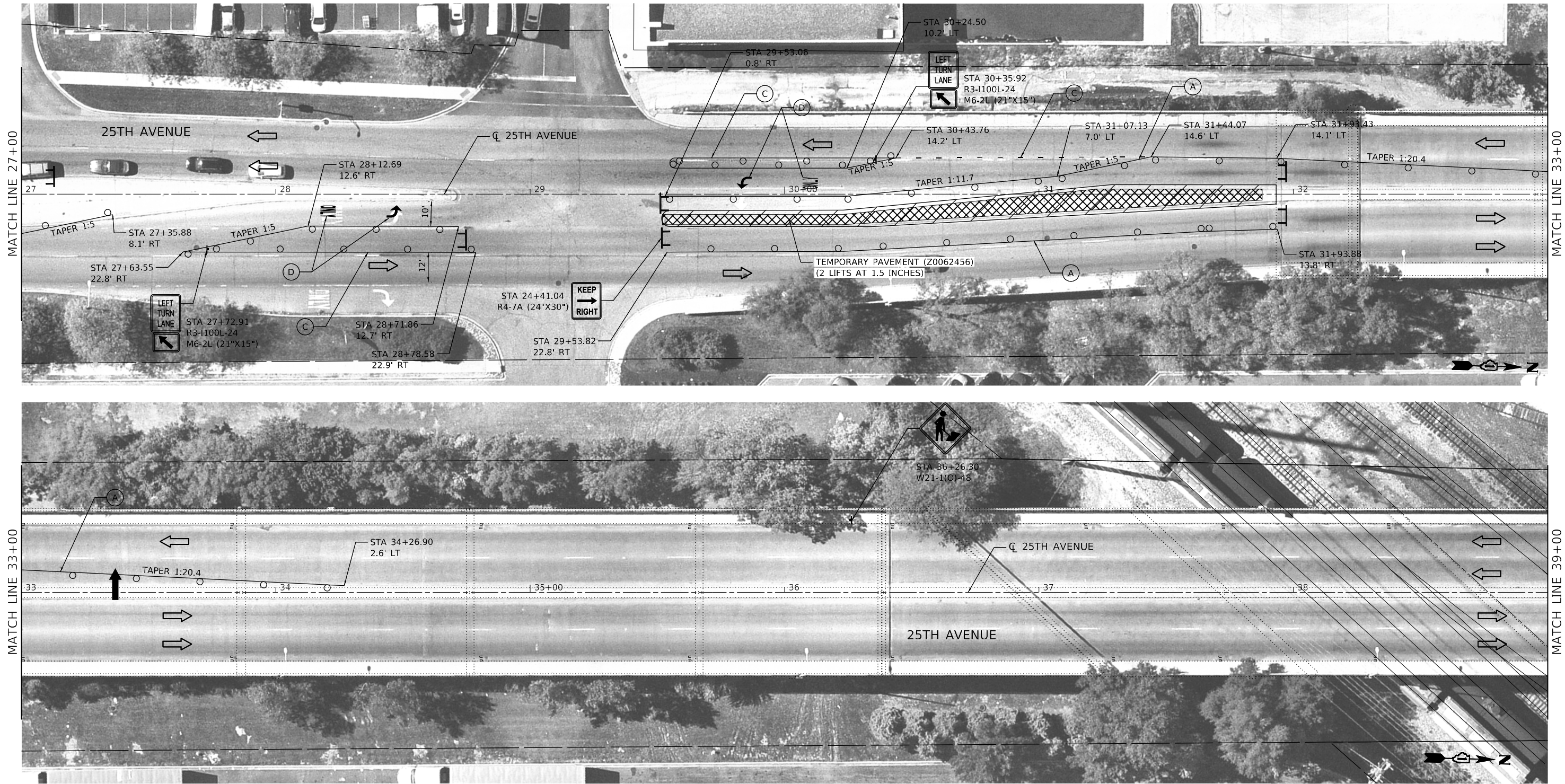
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
PRE & POST STAGE

SCALE: 40,0000 ' / in. SHEET 2 OF 14 SHEETS STA. 15+00 TO STA. 27+00

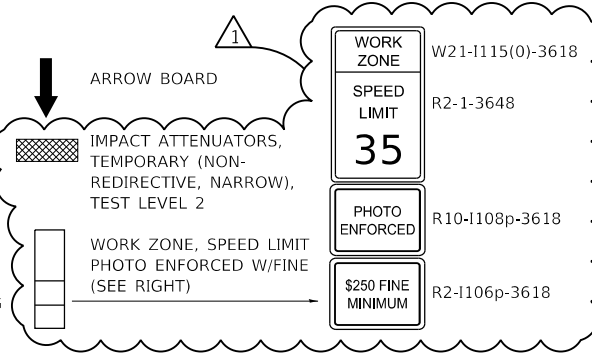
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2714	2020-203-BR	COOK	79	8
CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

USER: idave
MODEL: Default
FILE NAME: PLOT2_209_00a.IDOT.PTB_190_14 Phase II Var-Var-Design\Work Order 141CADD\CADD_Sheets\162M80-SHT-STAGE-PRE-POST-12.dgn



LEGEND

- WORK ZONE
- DIRECTION OF TRAFFIC
- DRUMS/TYPE II BARRICADES
50' IN TANGENTS
20' ON TAPERS
10' IN RADII/CURVES
- TRAFFIC SIGN
- TYPE III BARRICADE WITH FLASHING MONODIRECTIONAL LIGHTS



PAVEMENT MARKING LEGEND

- (A) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID YELLOW
- (B) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID WHITE
- (C) PAVEMENT MARKING TAPE, TYPE IV 6", SOLID WHITE
- (D) PAVEMENT MARKING TAPE, TYPE IV - LETTERS AND SYMBOLS, WHITE

1 REVISED SHEET 1/5/2021



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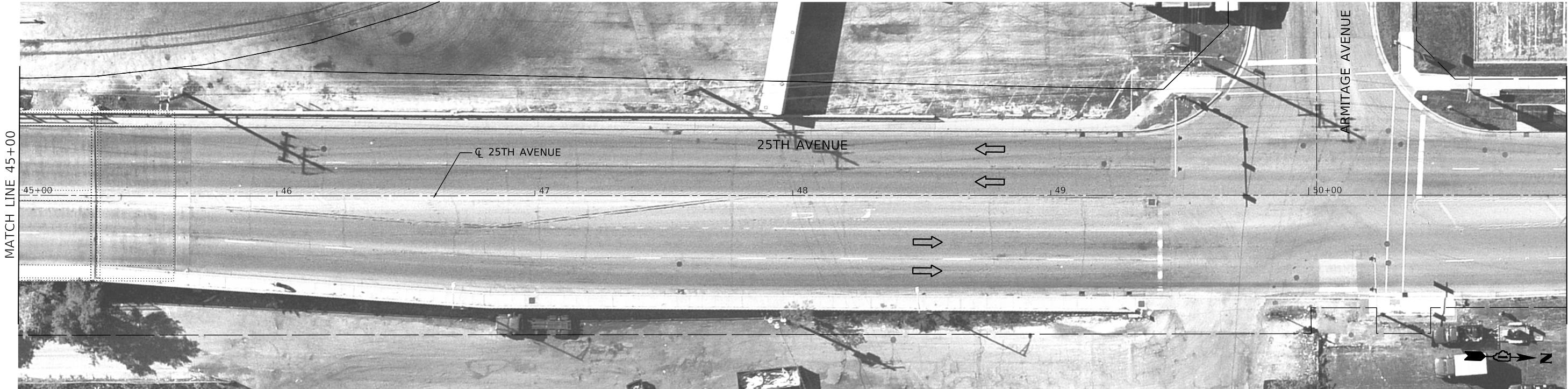
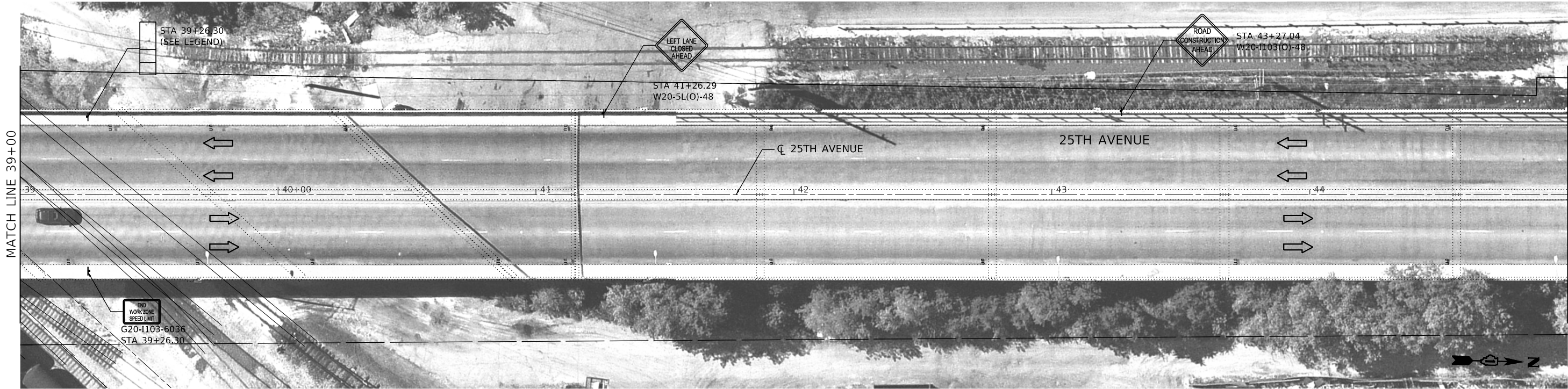
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
PRE & POST STAGE

SCALE: 40,0000 ' / in. SHEET 3 OF 14 SHEETS STA. 27+00 TO STA. 39+00

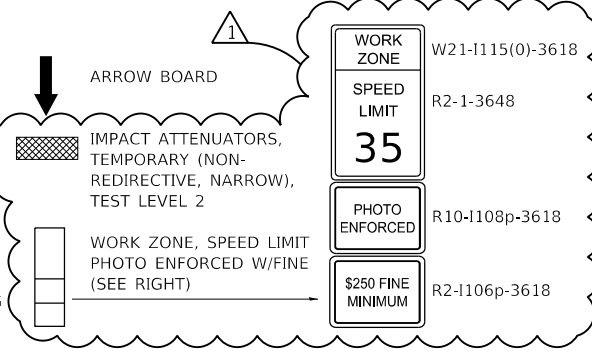
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-203-BR	COOK	79	9
CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

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MODEL: Default
FILE NAME: PLOT2_209_00a.IDOT.PTB_190_14 Phase II Var-Var-Design\Work_Order_141CADD\CADD_Sheets\162M80-SHT-STAGE-PRE-POST-193.dgn



LEGEND

- WORK ZONE
- DIRECTION OF TRAFFIC
- DRUMS/TYPE II BARRICADES
50' IN TANGENTS
20' ON TAPERS
10' IN RADII/CURVES
- TRAFFIC SIGN
- TYPE III BARRICADE WITH FLASHING MONODIRECTIONAL LIGHTS



PAVEMENT MARKING LEGEND

- (A) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID YELLOW
- (B) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID WHITE
- (C) PAVEMENT MARKING TAPE, TYPE IV 6", SOLID WHITE
- (D) PAVEMENT MARKING TAPE, TYPE IV - LETTERS AND SYMBOLS, WHITE

1 REVISED SHEET 1/5/2021



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DESIGNED - AWH
DRAWN - AWH
CHECKED - JWD
DATE - 08/28/2020

REVISED - 12/22/2020 SVJ
REVISED -
REVISED -
REVISED -

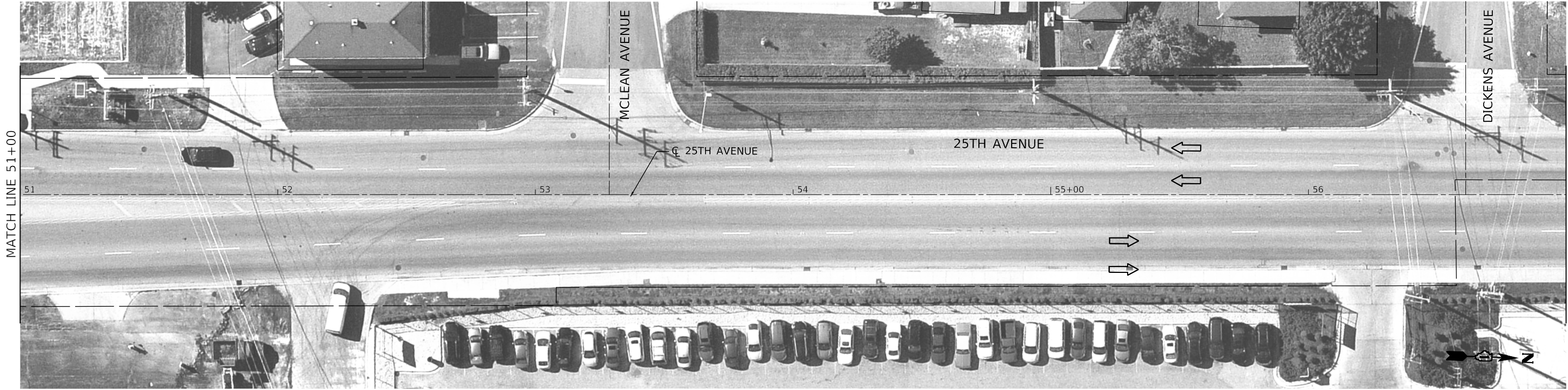
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
PRE & POST STAGE

SCALE: 40,0000 ' / in. SHEET 4 OF 14 SHEETS STA. 39+00 TO STA. 51+00

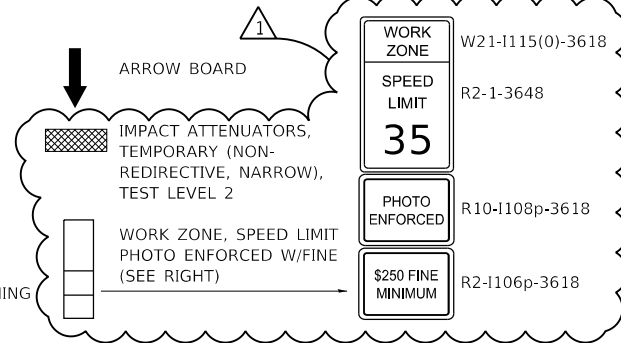
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2714	2020-203-BR	COOK	79	10
CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

USER: jhave
MODEL Default
FILE NAME: PLOT_209_00a_IDOT_PTB_190_14_Phase II Var-Var-Design\Work_Order_14\CADD\CADD_Sheets\0162M80-SHT-STAGE-PRE-POST-P4.dgn



LEGEND

- WORK ZONE
- DIRECTION OF TRAFFIC
- DRUMS/TYPE II BARRICADES
50' IN TANGENTS
20' ON TAPERS
10' IN RADII/CURVES
- TRAFFIC SIGN
- TYPE III BARRICADE WITH FLASHING MONODIRECTIONAL LIGHTS



PAVEMENT MARKING LEGEND

- (A) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID YELLOW
- (B) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID WHITE
- (C) PAVEMENT MARKING TAPE, TYPE IV 6", SOLID WHITE
- (D) PAVEMENT MARKING TAPE, TYPE IV - LETTERS AND SYMBOLS, WHITE

1 REVISED SHEET 1/5/2021



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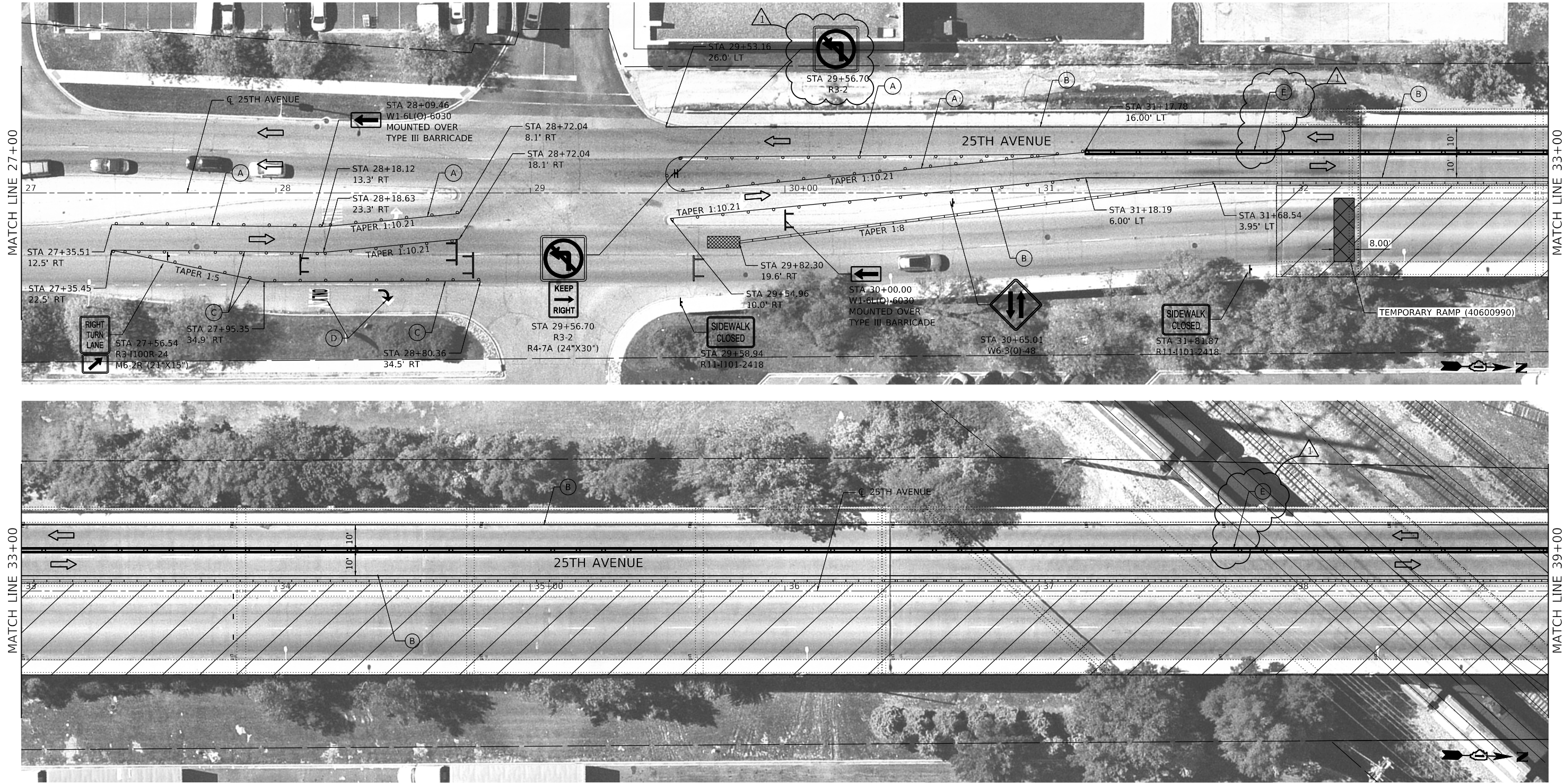
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC PRE & POST STAGE

SCALE: 40.0000 ' / in. SHEET 5 OF 14 SHEETS STA. 51+00 TO STA. 57+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-203-BR	COOK	79	11
CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

USER: idave
MODEL: Default
FILE NAME: PLOT2_209_000a.IDOT.PTB_190_14 Phase II Var-Var-DesignWork_Order_1AICADD/CADD_Sheets/D162M80-SHT-STAGE01-P2.dgn



LEGEND

- WORK ZONE
- DIRECTION OF TRAFFIC
- DRUMS/TYPE II BARRICADES
50' IN TANGENTS
20' ON TAPERS
10' IN RADII/CURVES
- TRAFFIC SIGN
- TYPE III BARRICADE WITH FLASHING MONODIRECTIONAL LIGHTS

ARROW BOARD

IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE, NARROW), TEST LEVEL 2

WORK ZONE, SPEED LIMIT PHOTO ENFORCED W/FINE (SEE RIGHT)

WORK ZONE W21-1115(0)-3618

SPEED LIMIT 35 R2-1-3648

PHOTO ENFORCED R10-1108p-3618

\$250 FINE MINIMUM R2-1106p-3618

PAVEMENT MARKING LEGEND

- (A) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID YELLOW
- (B) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID WHITE
- (C) PAVEMENT MARKING TAPE, TYPE IV 6", SOLID WHITE
- (D) PAVEMENT MARKING TAPE, TYPE IV - LETTERS AND SYMBOLS, WHITE
- (E) PAVEMENT MARKING TAPE, TYPE IV 4", DOUBLE YELLOW
- (F) PAVEMENT MARKING TAPE, TYPE IV 6", 2' DASH 6" SKIP WHITE

REVISED SHEET 1/5/2021



PLOT DRIVER = pdfNOLAYERSbw.pltcfp	DESIGNED - AWH	REVISED - 12/22/2020 SVJ
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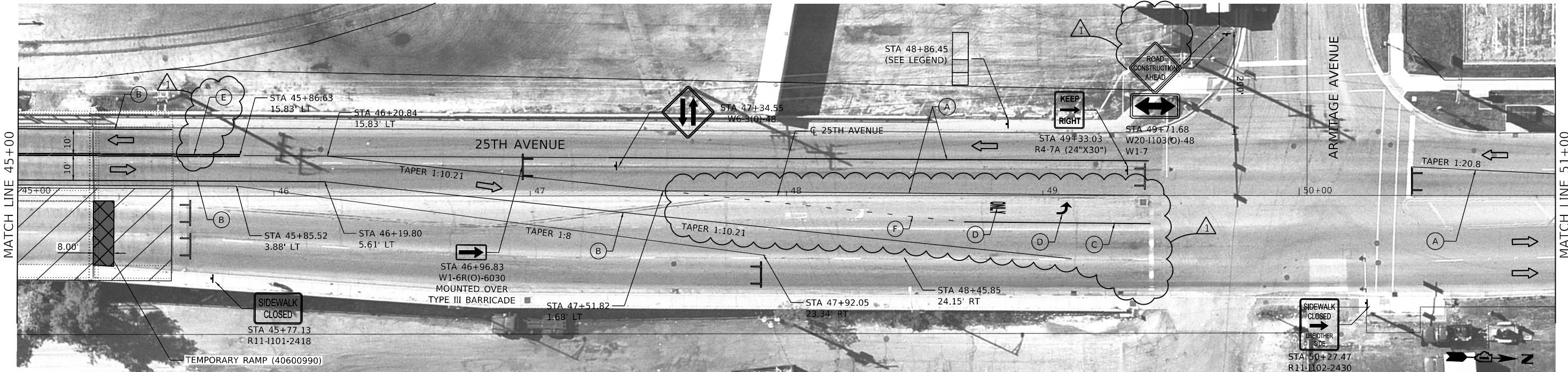
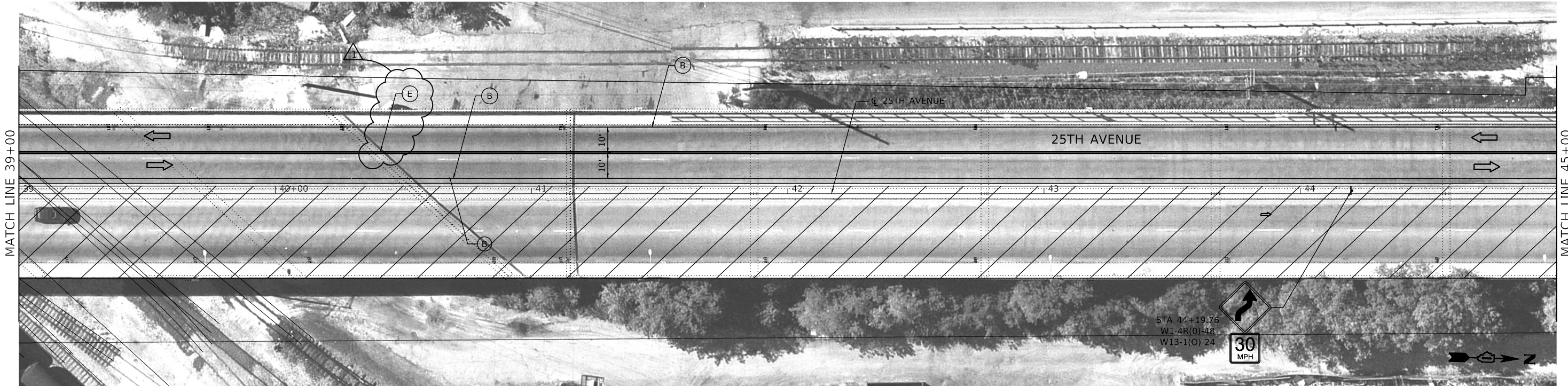
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
STAGE I

SCALE: 40,0000 ' / in. SHEET 7 OF 14 SHEETS STA. 27+00 TO STA. 39+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-203-BR	COOK	79	13
CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

USER: idave
MODEL: Default
FILE NAME: PLOT_209_000a.IDOT.PTB_190_14 Phase II Var-Var-Design\Work Order 141CADD\CADD_Sheets\0162M80-SHT-STAGE01-P3.dgn



LEGEND

- WORK ZONE
- DIRECTION OF TRAFFIC
- DRUMS/TYPE II BARRICADES
50' IN TANGENTS
20' ON TAPERS
10' IN RADII/CURVES
- TRAFFIC SIGN
- TYPE III BARRICADE WITH FLASHING MONODIRECTIONAL LIGHTS

ARROW BOARD

IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE, NARROW), TEST LEVEL 2

WORK ZONE, SPEED LIMIT PHOTO ENFORCED W/FINE (SEE RIGHT)

WORK ZONE

SPEED LIMIT

35

PHOTO ENFORCED

\$250 FINE MINIMUM

W21-1115(0)-3618

R2-1-3648

R10-1108p-3618

R2-1106p-3618

PAVEMENT MARKING LEGEND

- (A) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID YELLOW
- (B) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID WHITE
- (C) PAVEMENT MARKING TAPE, TYPE IV 6", SOLID WHITE
- (D) PAVEMENT MARKING TAPE, TYPE IV - LETTERS AND SYMBOLS, WHITE
- (E) PAVEMENT MARKING TAPE, TYPE IV 4", DOUBLE YELLOW
- (F) PAVEMENT MARKING TAPE, TYPE IV 6", 2' DASH 6' SKIP WHITE

1 REVISED SHEET 1/5/2021



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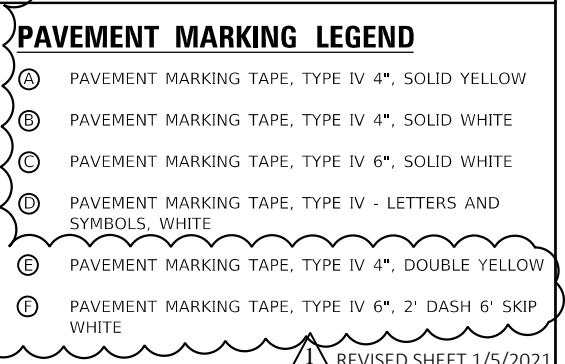
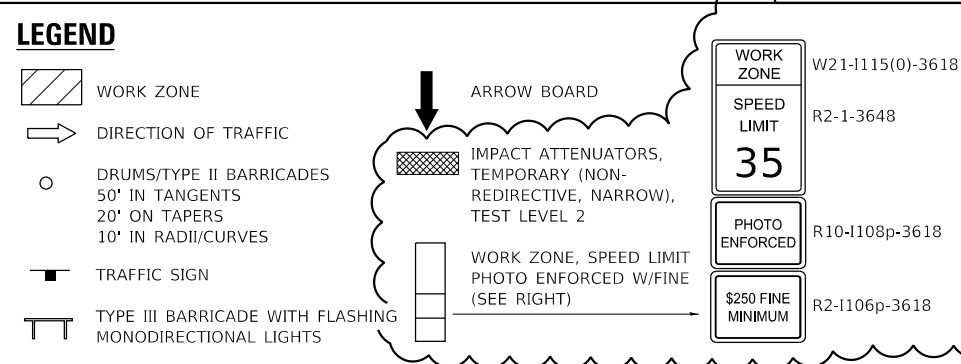
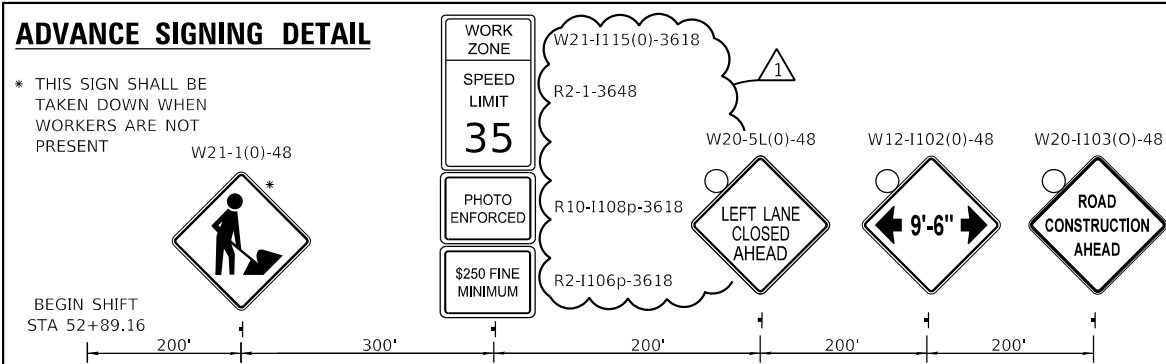
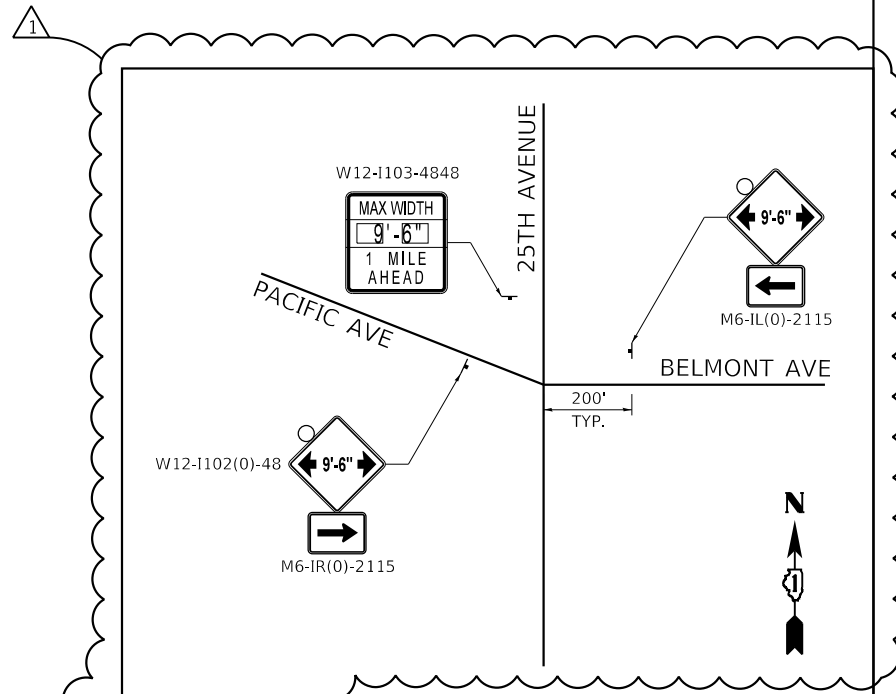
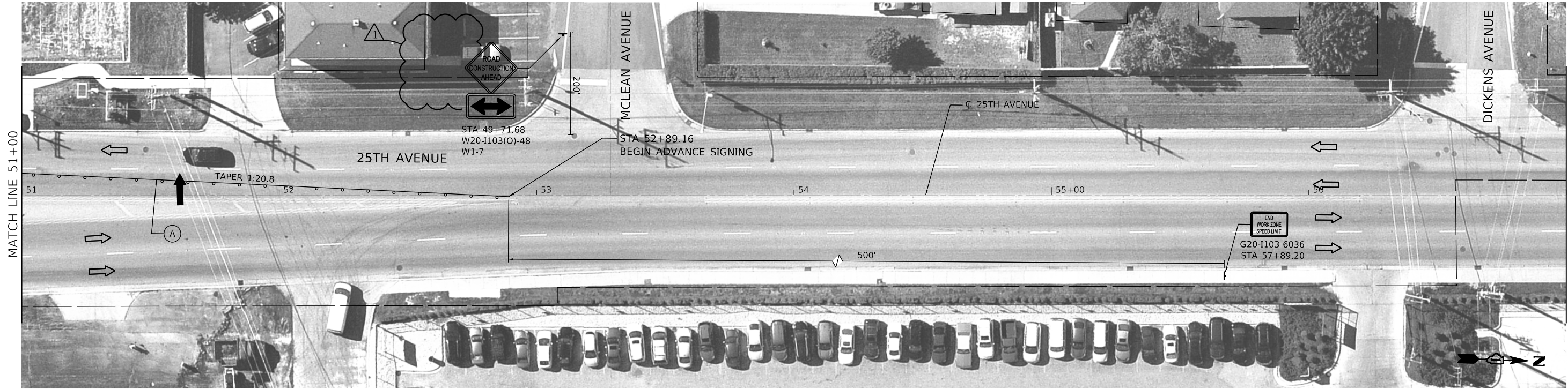
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC STAGE I

SCALE: 40,0000 ' / in. SHEET 8 OF 14 SHEETS STA. 39+00 TO STA. 51+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-203-BR	COOK	79	14
CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

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MODEL: Default
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CHECKED	- JWD
DATE	- 08/28/2020

REVISED	- 12/22/2020 SVJ
REVISED	-
REVISED	-
REVISED	-

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

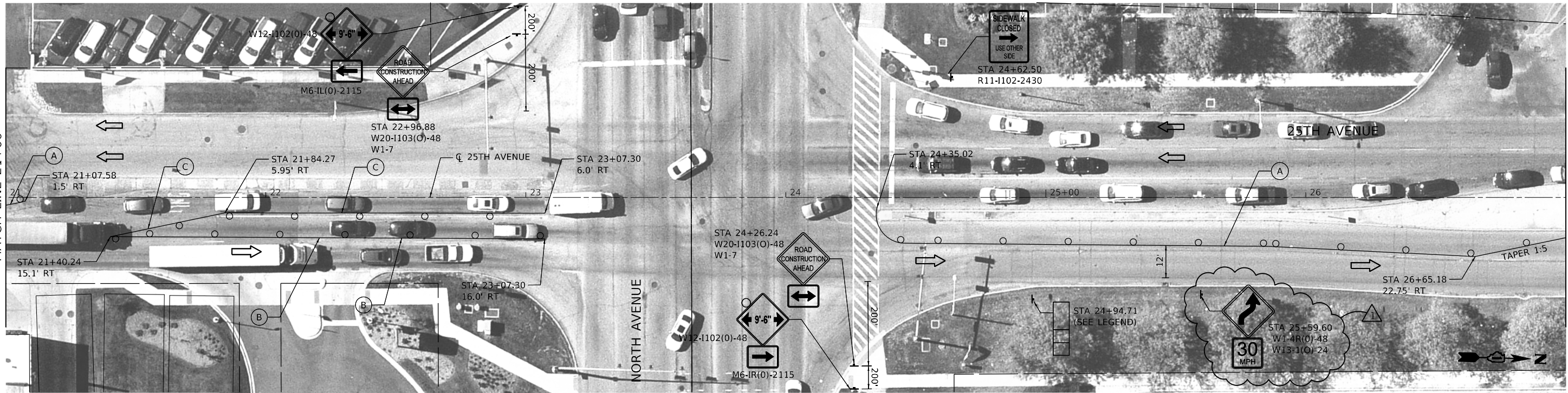
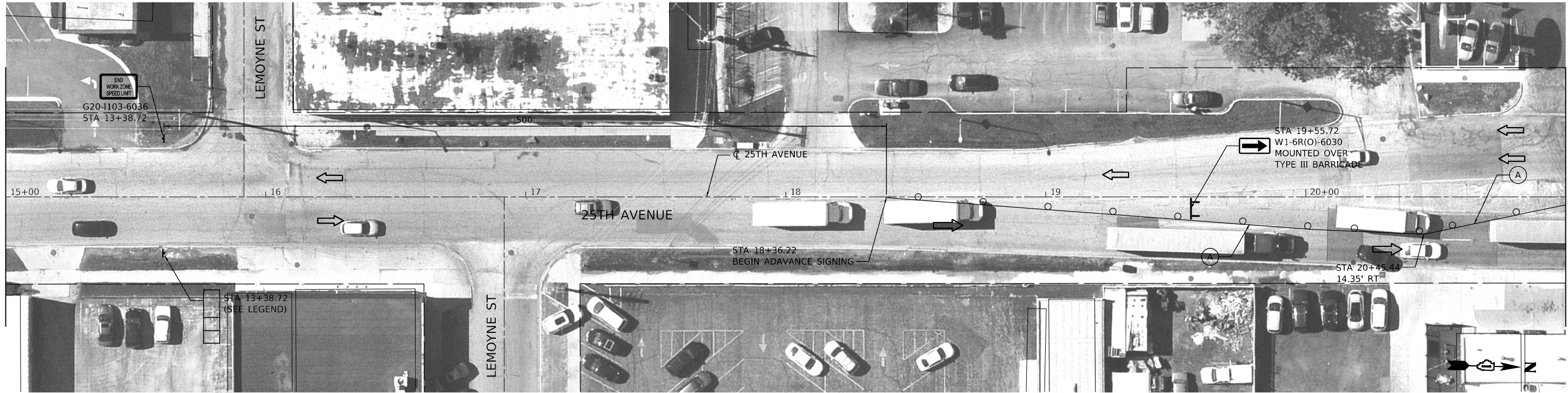
MAINTENANCE OF TRAFFIC
STAGE I

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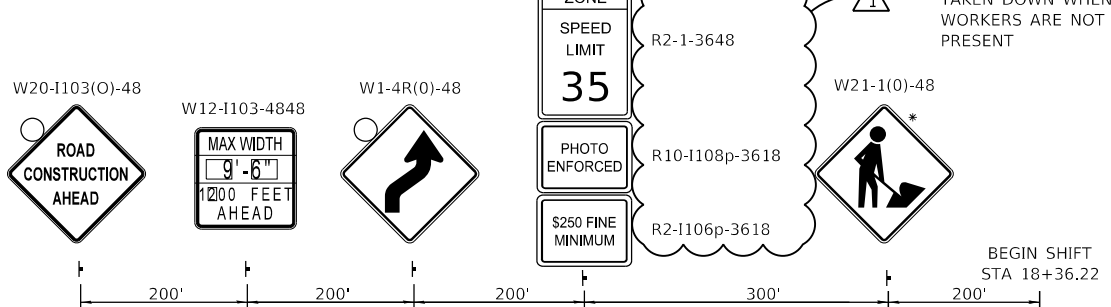
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2714	2020-203-BR	COOK	79	15
CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

1 REVISED SHEET 1/5/2021

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MODEL: Default
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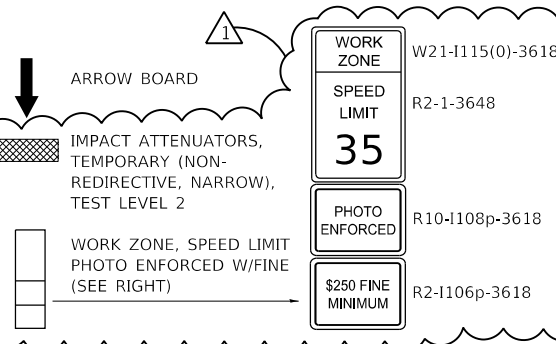


ADVANCE SIGNING DETAIL



LEGEND

- WORK ZONE
- DIRECTION OF TRAFFIC
- DRUMS/TYPE II BARRICADES
50' IN TANGENTS
20' ON TAPERS
10' IN RADII/CURVES
- TRAFFIC SIGN
- TYPE III BARRICADE WITH FLASHING MONODIRECTIONAL LIGHTS



PAVEMENT MARKING LEGEND

- (A) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID YELLOW
- (B) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID WHITE
- (C) PAVEMENT MARKING TAPE, TYPE IV 6", SOLID WHITE
- (D) PAVEMENT MARKING TAPE, TYPE IV - LETTERS AND SYMBOLS, WHITE
- (E) PAVEMENT MARKING TAPE, TYPE IV 4", DOUBLE YELLOW
- (F) PAVEMENT MARKING TAPE, TYPE IV 6", 2' DASH 6' SKIP WHITE

REVISED SHEET 1/5/2021



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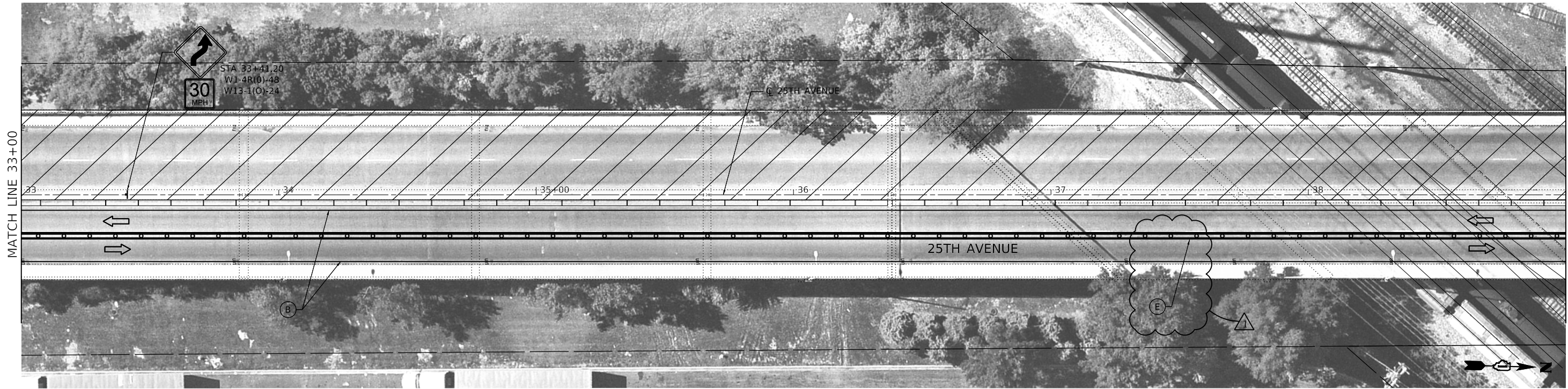
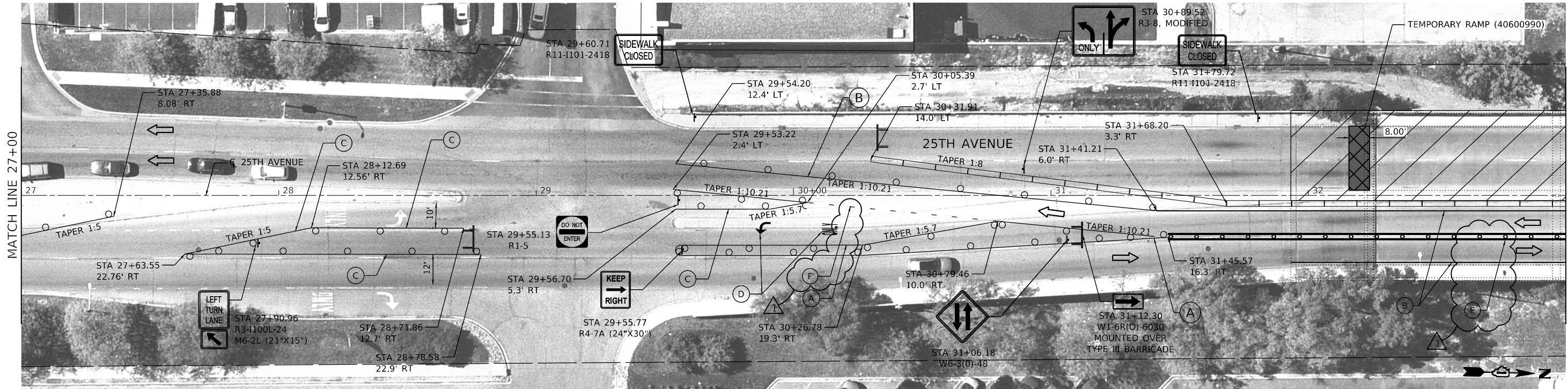
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
STAGE II

SCALE: 40,0000 ' / in. SHEET 10 OF 14 SHEETS STA. 15+00 TO STA. 27+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-203-BR	COOK	79	16
CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

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MODEL: Default
FILE NAME: PLOT_209_000a.IDOT.PTB_190_14 Phase II Var-Var-Design\Work Order 141CADD\CADD_Sheets\162M80-SHT-STAGE03-PT2.dgn



LEGEND

- WORK ZONE
- DIRECTION OF TRAFFIC
- DRUMS/TYPE II BARRICADES
50' IN TANGENTS
20' ON TAPERS
10' IN RADII/CURVES
- TRAFFIC SIGN
- TYPE III BARRICADE WITH FLASHING MONODIRECTIONAL LIGHTS

ARROW BOARD

IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE, NARROW), TEST LEVEL 2

WORK ZONE, SPEED LIMIT PHOTO ENFORCED W/FINE (SEE RIGHT)

WORK ZONE W21-1115(O)-3618

SPEED LIMIT R2-1-3648

35

PHOTO ENFORCED R10-1108p-3618

\$250 FINE MINIMUM R2-1106p-3618

PAVEMENT MARKING LEGEND

- (A) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID YELLOW
- (B) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID WHITE
- (C) PAVEMENT MARKING TAPE, TYPE IV 6", SOLID WHITE
- (D) PAVEMENT MARKING TAPE, TYPE IV - LETTERS AND SYMBOLS, WHITE
- (E) PAVEMENT MARKING TAPE, TYPE IV 4", DOUBLE YELLOW
- (F) PAVEMENT MARKING TAPE, TYPE IV 6", 2' DASH 6' SKIP WHITE

1 REVISED SHEET 1/5/2021



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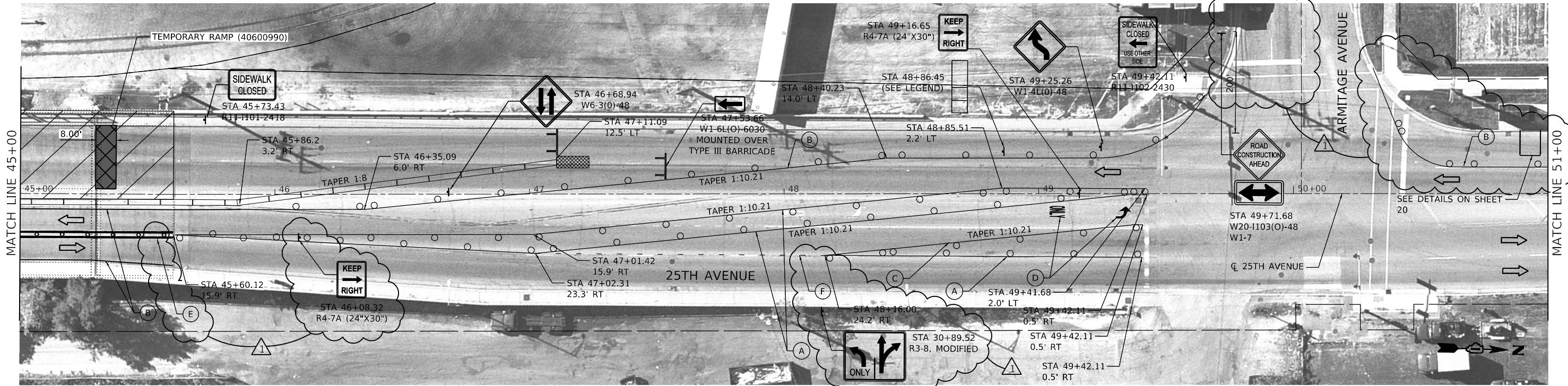
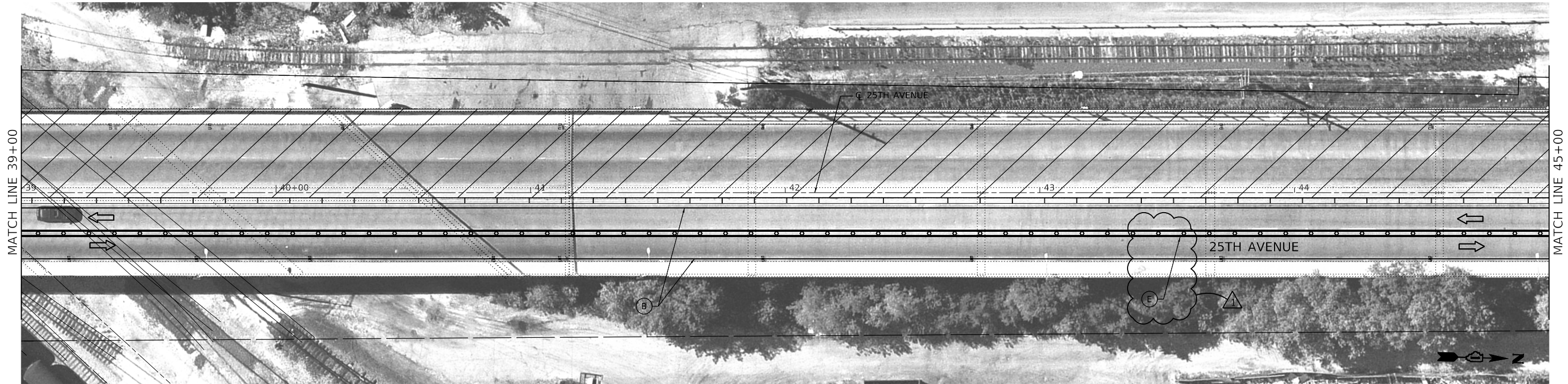
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC STAGE II

SCALE: 40,0000 ' / in. SHEET 11 OF 14 SHEETS STA. 27+00 TO STA. 39+00

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-203-BR	COOK	79	17
CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

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MODEL: Default
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LEGEND

- WORK ZONE
- DIRECTION OF TRAFFIC
- DRUMS/TYPE II BARRICADES
50' IN TANGENTS
20' ON TAPERS
10' IN RADII/CURVES
- TRAFFIC SIGN
- TYPE III BARRICADE WITH FLASHING MONODIRECTIONAL LIGHTS

- ARROW BOARD
- IMPACT ATTENUATORS, TEMPORARY (NON-REDIRECTIVE, NARROW), TEST LEVEL 2
- WORK ZONE, SPEED LIMIT PHOTO ENFORCED W/FINE (SEE RIGHT)

WORK ZONE
SPEED LIMIT
35
PHOTO ENFORCED
R10-1108p-3618
\$250 FINE MINIMUM
R2-1106p-3618

PAVEMENT MARKING LEGEND

- (A) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID YELLOW
- (B) PAVEMENT MARKING TAPE, TYPE IV 4", SOLID WHITE
- (C) PAVEMENT MARKING TAPE, TYPE IV 6", SOLID WHITE
- (D) PAVEMENT MARKING TAPE, TYPE IV - LETTERS AND SYMBOLS, WHITE
- (E) PAVEMENT MARKING TAPE, TYPE IV 4", DOUBLE YELLOW
- (F) PAVEMENT MARKING TAPE, TYPE IV 6", 2' DASH 6' SKIP WHITE

REVISED SHEET 1/5/2021



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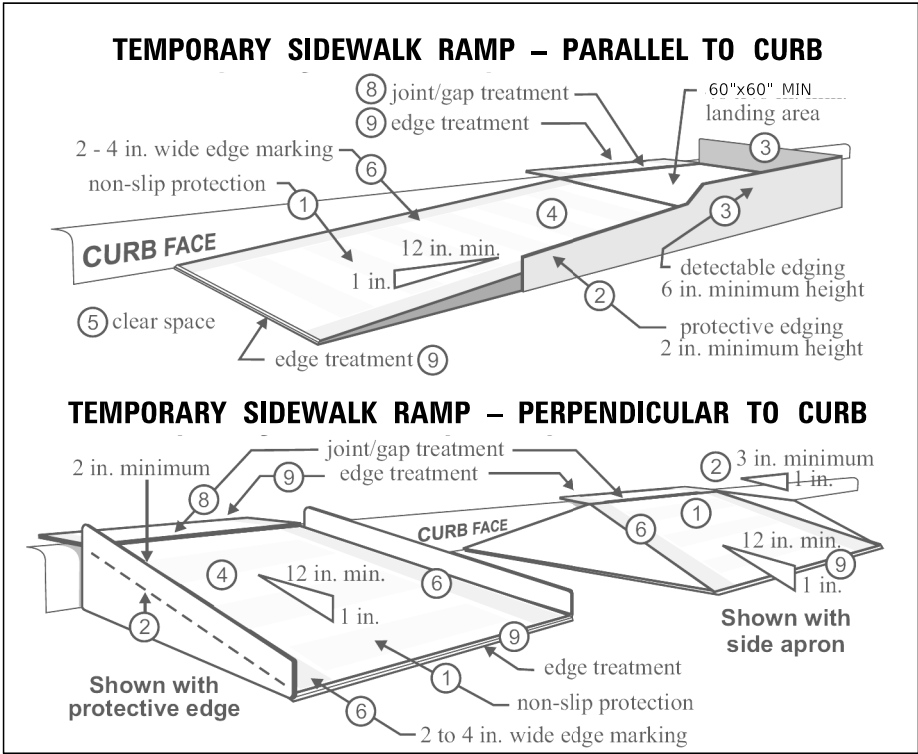
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

MAINTENANCE OF TRAFFIC
STAGE II

SCALE: 40,0000 ' / in. SHEET 12 OF 14 SHEETS STA. 39+00 TO STA. 51+00

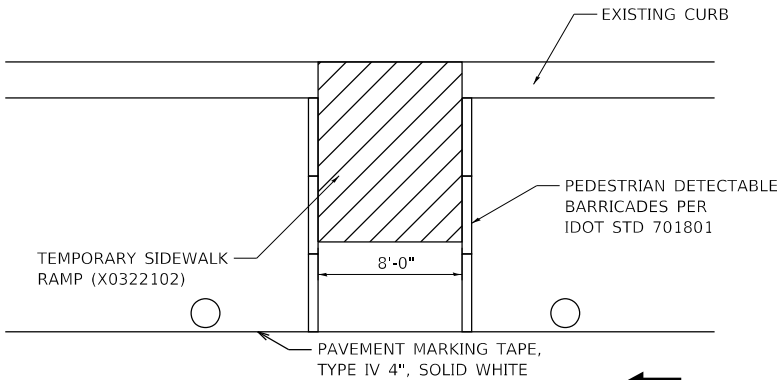
F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-203-BR	COOK	79	18
CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

USER: jhave
MODEL Default
FILE Name: PLOT2_209_004.IDOT.PTB_190_14 Phase II Vee-Van Design\Work Order 141CADD\CD\CD Sheets\0162M80-rt-Stage-Details.dgn



(FIGURE FROM MnDOT TPAR)

PAGE BUS STOP DETAIL



NOTES

1. SPACE TYPE II BARRICADES TO AVOID BLOCKING THE 8'-0" WIDE PEDESTRIAN ACCESS TO THE BUS SHELTER.
2. DECREASE SPACING OF THE TYPE II BARRICADES TO 10' SPACING WITHIN 50' OF THE PEDESTRIAN ACCESS TO THE BUS SHELTER.

12/18/2020 ADDENDUM NO. 1

TEMPORARY SIDEWALK RAMP

DESCRIPTION:

THIS WORK SHALL CONSIST OF INSTALLING AND MAINTAINING TEMPORARY ADA COMPLIANT RAMPS TO MAINTAIN ACCESS TO BUS STOPS DURING CONSTRUCTION AT THE LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.

CONSTRUCTION REQUIREMENTS:

THIS WORK SHALL BE ACCORDANCE TO STANDARD 701801-06. TEMPORARY RAMPS TO MAINTAIN ACCESS SHALL BE COMPLAINT WITH THE ADA REQUIREMENTS AND SHALL BE USED AT LOCATIONS TO MAINTAIN A MINIMUM OF ONE ADA COMPLAINT PEDESTRIAN ACCESS PATH AROUND CONSTRUCTION ACTIVITIES AT ALL TIMES.

1. TRAFFIC CONTROL DEVICES USED ADJACENT TO PEDESTRIAN ACCESS PATHS FOR CHANNELIZING PEDESTRIANS SHALL BE ADA COMPLAINT WITH A BOTTOM EDGE AT LEAST 6 INCHES HIGH FROM THE WALKWAY AND A CONTINUOUS RAIL OR SURFACE AT 3 FEET ABOUT THE WALKWAY.
2. CURB RAMPS SHALL BE 8 FEET MINIMUM WIDTH WITH A FIRM, STABLE AND NON-SLIP SURFACE.
3. PROTECTIVE EDGING WITH A 2 IN. MINIMUM HEIGHT SHALL BE INSTALLED WHEN THE CURB RAMP OR LANDING PLATFORM HAS A VERTICAL DROP OF 6 IN. OR GREATER OR HAS A SIDE APRON SLOPE STEEPER THAN 1:3 (33%). PROTECTIVE EDGING SHOULD BE CONSIDERED WHEN CURB RAMPS OR LANDING PLATFORMS HAVE A VERTICAL DROP OF 3 INCHES OR MORE.
4. DETECTABLE EDGING WITH 6-INCH MINIMUM HEIGHT AND CONTRASTING COLOR SHALL BE INSTALLED ON ALL CURB RAMP LANDINGS WHERE THE WALKWAY CHANGES DIRECTION (TURNS).
5. CURB RAMPS AND LANDINGS SHOULD HAVE A 1:50 (2%) MAX CROSS-SLOPE.
6. CLEAR SPACE OF 5 FT. X 5 FT. MINIMUM SHALL BE PROVIDED ABOVE AND BELOW THE CURB RAMP.
7. THE CURB RAMP WALKWAY EDGE SHALL BE MARKED WITH A CONTRASTING COLOR 2 TO 4 INCHES WIDE MARKING. THE MARKING IS OPTIONAL WHERE COLOR CONTRASTING EDGING IS USED.
8. WATER FLOW IN THE GUTTER SYSTEM SHALL HAVE MINIMAL RESTRICTION.
9. LATERAL JOINTS OR GAPS BETWEEN SURFACES SHALL BE LESS THAN 0.5-INCH WIDTH.
10. CHANGES BETWEEN SURFACE HEIGHTS SHOULD NOT EXCEED 0.5 INCH. LATERAL EDGES SHOULD BE VERTICAL UP TO 0.25-INCH-HIGH AND BEVELED AT 1:2 BETWEEN 0.25 INCH AND 0.5 INCH HEIGHT.

METHOD OF MEASUREMENT:

TEMPORARY SIDEWALK RAMP WILL BE MEASURED FOR PAYMENT IN PLACE ON AN EACH BASIS AT LOCATIONS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER. REINSTALLATION, ADJUSTMENT, OR MODIFICATION OF A TEMPORARY SIDEWALK RAMP AT THE SAME LOCATION WILL NOT BE MEASURED FOR PAYMENT.

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH FOR "TEMPORARY SIDEWALK RAMP" AND SHALL INCLUDE REMOVAL.



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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

**MAINTENANCE OF TRAFFIC
TEMPORARY SIDEWALK RAMP**

SCALE: N/A SHEET 14 OF 14 SHEETS STA. TO STA.

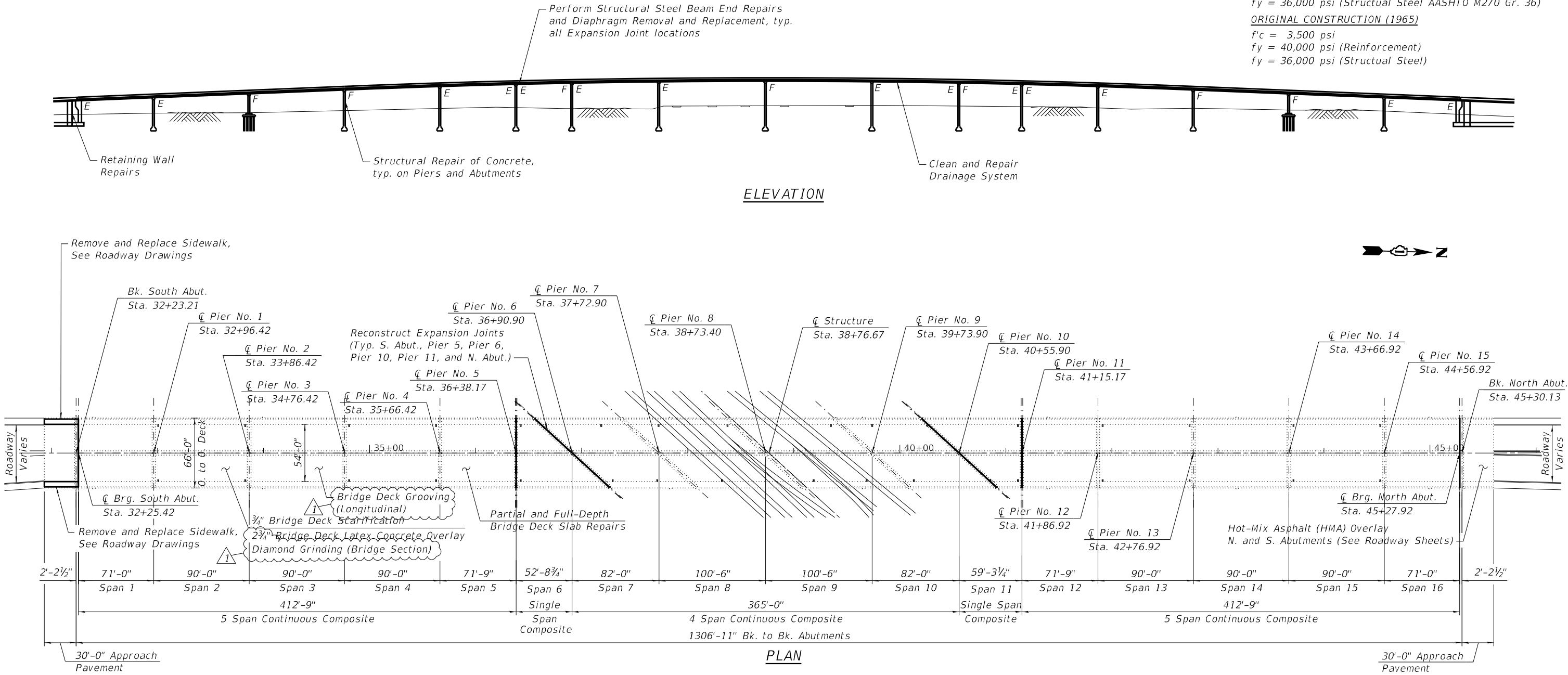
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2714	2020-203-BR	COOK	79	20
CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

Existing Structure: Structure No. 016-1011, built as 25th Avenue, Section 020-1010.2MFT in 1965 is 66'-0" wide by 1302'-6' long. the existing sixteen span reinforced concrete deck and steel wide flange beams are supported on multiple column piers and closed abutments. The structure was rehabilitated in 1994 with a deck replacement, structural steel repair and substructure repairs. One lane of traffic in each direction is to be maintained utilizing stage construction.

No salvage.

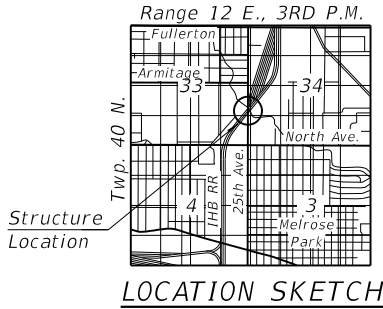
DESIGN SPECIFICATIONS
2002 AASHTO Standard Specifications
for Highway Bridges, 17th Edition

DESIGN STRESSES
FIELD UNITS
f'c = 3,500 psi
f'c = 4,000 psi (Superstructure Concrete)
fy = 60,000 psi (Reinforcement)
fy = 36,000 psi (Structural Steel AASHTO M270 Gr. 36)
RECONSTRUCTION (1994)
f'c = 3,500 psi
fy = 60,000 psi (Reinforcement)
fy = 36,000 psi (Structural Steel AASHTO M270 Gr. 36)
ORIGINAL CONSTRUCTION (1965)
f'c = 3,500 psi
fy = 40,000 psi (Reinforcement)
fy = 36,000 psi (Structual Steel)



NOTE:
Dimension and Clearances shown are from existing drawings.

LICENSED STRUCTURAL ENGINEER
RYAN R. DOERRER
081-007941
STATE OF ILLINOIS
10/6/2020
DATE
11/30/2020
EXPIRES



GENERAL PLAN AND ELEVATION
25TH AVENUE OVER IHB RAILROAD
F.A.U. RTE. 2714 SECTION 1010.2R
COOK COUNTY
STATION 38+76.67
STRUCTURE NO. 016-1011

REVISED SHEET 1/4/2021

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN AND ELEVATION
STRUCTURE NO. 016-1011

SHEET 5-01 OF 5-34 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-161-BR	COOK	79	33
CONTRACT NO. 62M80				
ILLINOIS		FED. AID PROJECT		

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GENERAL NOTES

No field welding is permitted except as specified in the contract documents.

Reinforcement bars designated (E) shall be epoxy coated.

Prior to placing the bridge deck latex concrete overlay, all heavy or loose rust, loose mill scale, and other loose or potentially detrimental foreign material shall be removed from the surfaces in contact with concrete. Tightly adhered paint may remain unless otherwise noted. Removal shall be accomplished by methods that will not damage the steel and the cost will be included in the pay item covering removal of the existing concrete.

As directed by the Engineer, existing construction accessories welded to the top flange of beams and girders shall be removed. The weld areas shall be ground flush and inspected for cracks using magnetic particle testing (MT) or dye penetrant testing (PT) by qualified personnel approved by the Engineer.

Any cracks that cannot be removed by grinding 1/4 inch deep shall be identified and reported to the Bureau of Bridges and Structures for further disposition. The cost of removing welded accessories, grinding and inspecting weld areas and grinding cracks will be paid for according to Article 109.04 of the Standard Specifications.

Plan dimensions and details relative to existing plans are subject to nominal construction variations. The Contractor shall field verify existing dimensions and details affecting new construction and make necessary approved adjustments prior to construction or ordering of materials. Such variations shall not be cause for additional compensation for a change in scope of the work, however, the Contractor will be paid for the quantity actually furnished at the unit price bid for the work.

Proposed profile and elevations are based on the existing drawing profile and adjusted to account for the proposed overlay.

Fasteners shall be ASTM F 3125 Grade A352 Type 1. Fastener shall be hot dip galvanized. See Special Provision for "Hot Dip Galvanizing for Structural Steel." Bolts 3/4 in. Φ holes 1 3/16 in. Φ, unless otherwise noted.

All new structural steel shall be galvanized. See Special Provision for "Hot Dip Galvanizing for Structural Steel."

Existing structural steel that will be in contact with new structural steel shall be cleaned and painted prior to erection as required by the Special Provision "Cleaning and Painting Contact Surface Areas of Existing Steel Structures."

The existing structural steel coating contains lead. The contractor shall take appropriate precautions to deal with the presence of lead on this project.

All exposed concrete edges shall have a 3/4" x 45 degree chamfer except where shown otherwise.

Fibers shall be included according to Note 3 of the Special Provision "Bridge Deck Latex Concrete Overlay".

SCOPE OF WORK

1. Perform 3/4" Bridge Deck Scarification.
2. Perform Deck slab repairs as required.
3. Reconstruct bridge deck expansion joints at the North and South Abutments and Pier 5, 6, 10, and 11. Install New preformed joints Strip Seal.
4. Perform Parapet Repair and Partial Removal/ Replacement of railing as required.
5. Apply a 2 3/4" Bridge Deck Latex Concrete Overlay and Diamond Grinding (Bridge Section) on Bridge Deck.
6. Perform Bridge Deck Grooving.
7. Perform HMA overlay on the North and South Approach Slabs (See Roadway Plans).
8. Apply Protective Coat to the top and inside faces of parapets, sidewalks, reconstructed transverse expansion joints, and surface of the new overlay.
9. Perform Structural Steel Beam End Repairs and diaphragm removal and replacement at the locations shown in the drawings.
10. Perform Structural Repair of Concrete and Epoxy Crack Injection to the abutments and Piers as shown in the drawings.

TOTAL BILL OF MATERIAL

ITEM	UNIT	SUPER	SUB	TOTAL
Concrete Removal	CU YD	69.6	0	69.6
Protective Shield	SQ YD	8,867	0	8,867
Concrete Structures	CU YD	0	1.0	1.0
Concrete Superstructure	CU YD	78.8	0.0	78.8
Protective Coat	SQ YD	7,333	0	7,333
Furnishing And Erecting Structural Steel	POUND	29,400	0	29,400
Reinforcement Bars, Epoxy Coated	POUND	10,910	160	11,070
Bar Splicers	EACH	98	0	98
Preformed Joint Strip Seal	FOOT	460	0	460
Epoxy Crack Injection	FOOT	0	24	24
Acrylic Coating	SQ YD	0	42	42
Fiber Wrap	SQ FT	0	377	377
Bridge Drainage System Repair	FOOT	22	0	22
Bridge Deck Grooving (Longitudinal)	SQ YD	6,686	0	6,686
Jack and Repositioning Bearings	EACH	0	24	24
Structural Steel Removal	POUND	25,860	0	25,860
Structural Steel Repair	POUND	10,650	0	10,650
Bridge Deck Latex Concrete Overlay, 2 3/4 Inches	SQ YD	7,107	0	7,107
Cleaning Drainage System	L SUM	1	0	1
Bridge Deck Scarification 3/4"	SQ YD	7,107	0	7,107
Structural Repair Of Concrete (Depth Equal To Or Less Than 5 Inches)	SQ FT	0	1,672	1,672
Structural Repair Of Concrete (Depth Greater Than 5 Inches)	SQ FT	0	49	49
Deck Slab Repair (Full Depth, Type II)	SQ YD	10	0	10
Diamond Grinding (Bridge Section)	SQ YD	7,267	0	7,267
Temporary Shoring and Cribbing	EACH	20	0	20

INDEX OF SHEETS

- S-01 General Plan and Elevation
S-02 General Data
S-03 Stage Construction Details (1 of 2)
S-04 Stage Construction Details (2 of 2)
S-05 Temporary Concrete Barrier for Stage Construction
S-06 Deck Repair Plan (1 of 5)
S-07 Deck Repair Plan (2 of 5)
S-08 Deck Repair Plan (3 of 5)
S-09 Deck Repair Plan (4 of 5)
S-10 Deck Repair Plan (5 of 5)
S-11 N. and S. Abut. Joint Removal and Reconstruction Plan
S-12 N. and S. Abut. Joint Removal and Reconstruction Details
S-13 Pier 5, 6, 10 and 11 Joint Removal and Reconstruction Plan
S-14 Pier 5, 6, 10 and 11 Joint Removal and Reconstruction Details
S-15 Preformed Joint Strip Seal - Sidewalk (1 of 3)
S-16 Preformed Joint Strip Seal - Sidewalk (2 of 3)
S-17 Preformed Joint Strip Seal - Sidewalk (3 of 3)
S-18 Partial Framing Plan (1 of 2)
S-19 Partial Framing Plan (2 of 2)
S-20 Diaphragm Replacement Details, Pier 5 and 11
S-21 Beam Repair Details, Pier 5 and 11
S-22 Diaphragm Replacement Details, Pier 6 and 10
S-23 Beam Repair Details, Pier 6 and 10
S-24 Drainage System Repair Plan
S-25 Retaining Wall Repairs
S-26 North and South Abutment Repair Details
S-27 Pier 2 and 5 Repair Details
S-28 Pier 6 and 7 Repair Details
S-29 Pier 8 and 9 Repair Details
S-30 Pier 10 and 11 Repair Details
S-31 Pier 12 and 13 Repair Details
S-32 Pier 14 and 15 Repair Details
S-33 Bar Splicer Assembly and Mechanical Splicer Details
S-34 Existing General Plan and Elevation



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

GENERAL DATA
STRUCTURE NO. 016-1011

SHEET S-02 OF S-34 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62M80				
		ILLINOIS	FED. AID PROJECT	

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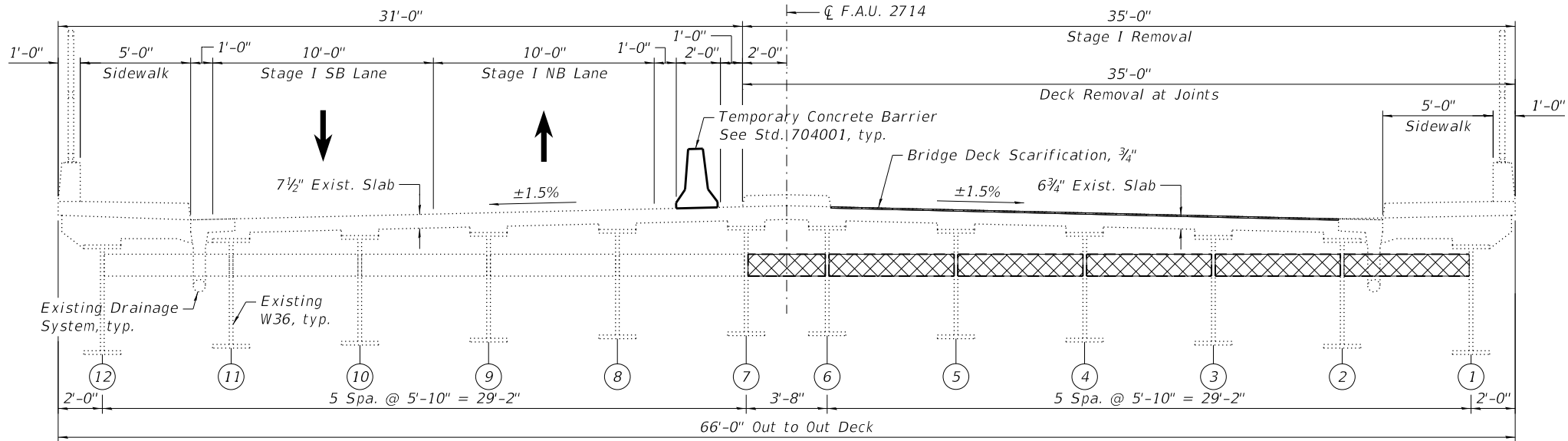
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

STAGE CONSTRUCTION DETAILS (1 OF 2)
STRUCTURE NO. 016-1011

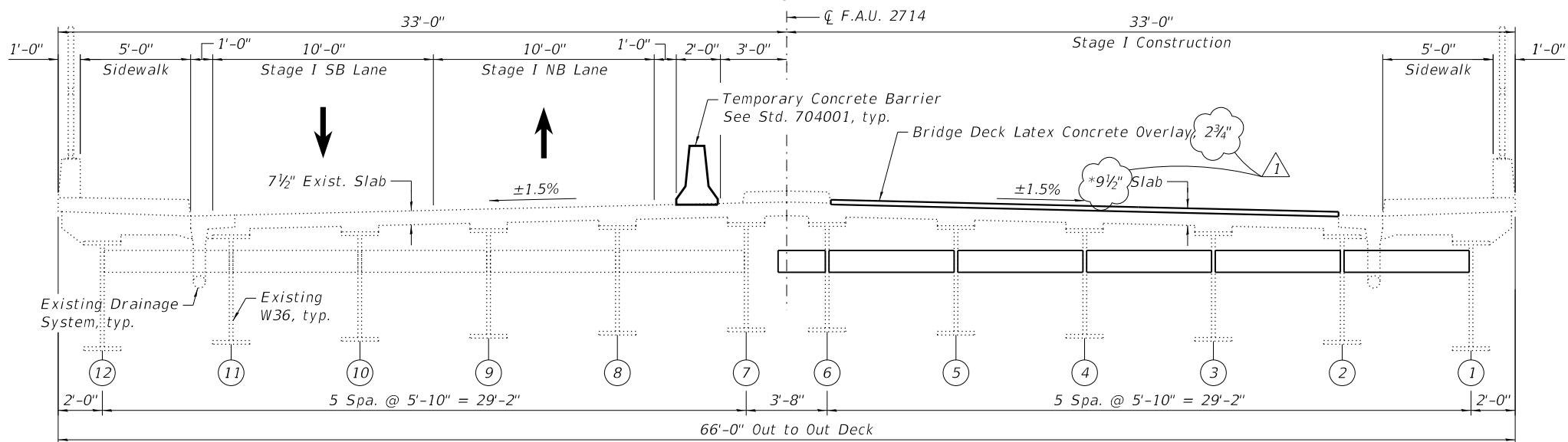
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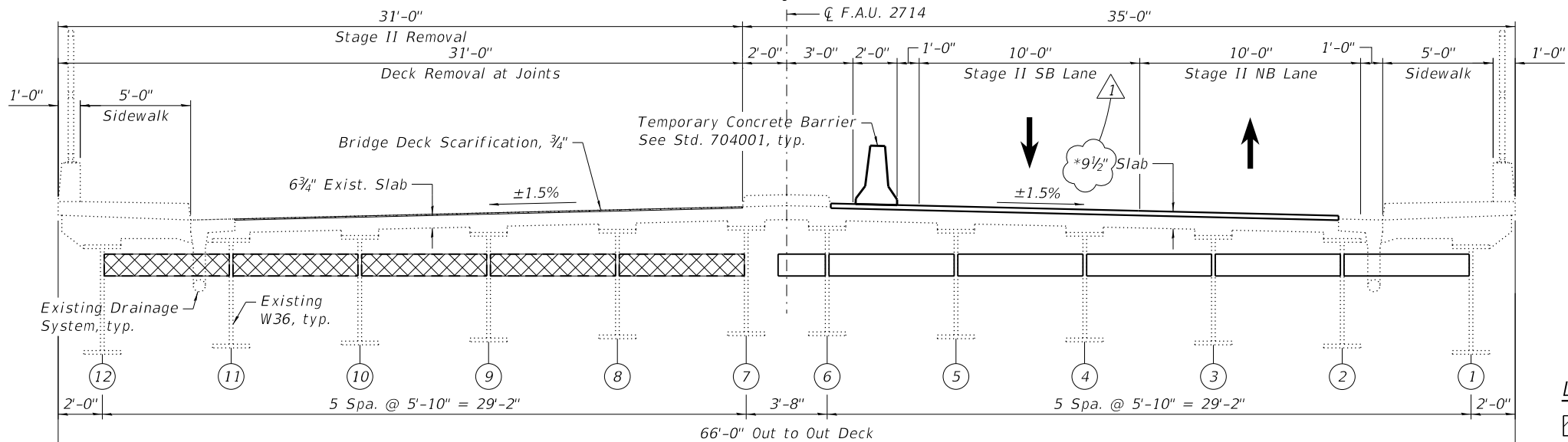
STAGE I REMOVAL

(Looking North)



STAGE I CONSTRUCTION

(Looking North)



STAGE II REMOVAL

(Looking North)

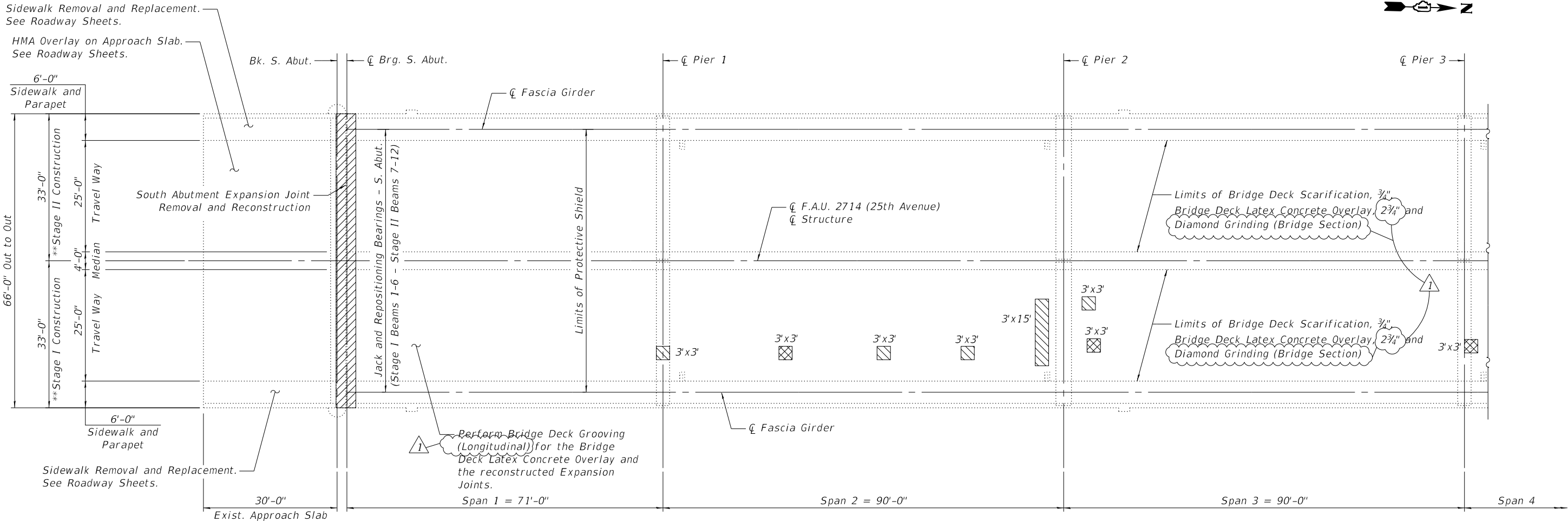
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LEGEND

- Bridge Deck Scarification, 3/4"
- Structural Steel Removal

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** Deck Stage Construction limits shown.
Median construction at joints to be constructed in Stage II.

NOTES:

- Areas of deck repair shown are estimated. The Engineer shall determine actual locations of deck repairs at the time of construction.
- For Abutment and Pier Expansion Joint Removal and Reconstruction, see Sheets S-11 thru S-14.
- Protective coat shall be applied to top and inside face of reconstructed parapets, reconstructed sidewalks, reconstructed transverse joint areas and the surface of the new overlay.
- Any reinforcement bars that are damaged during concrete removal operations shall be repaired or replaced using an approval bar splicer or anchorage system. Cost incidental to Concrete Removal.
- The Contractor shall exercise extreme caution during Concrete Removal to avoid damaging the steel beams and diaphragms to remain. Any damage to the existing steel beams and/or diaphragms to remain caused by the Contractor in the performance of their work shall be repaired by the Contractor, to the satisfaction of the Engineer, at no cost to the Department.
- Existing bridge fencing is to be protected during construction. Post location in the removal area for the joint replacement shall be temporally shored during the concrete removal and replacement. Existing concrete anchor shall be replaced in kind and installed into the proposed parapet. Cost included in Concrete Removal.

- Removal and disposal of the existing expansion joints will not be paid for separately, but are included in the cost of Concrete Removal.
- The Contractor must exercise extreme care with the existing conduits in the sections of the parapets to be removed and to protect and support the conduit during construction. The Contractor will be required to repair any damage done to the conduit to the satisfaction of the Engineer. No splicing will be allowed to any cable damaged resulting from this work. The Contractor will be required to repair the entire span of any damaged cable at no additional cost to the Department.
- If the existing name plate falls within the limits of Concrete Removal, it shall be removed and reinstalled in its original locations in accordance with IDOT Standard 515001. Cost included with Concrete Superstructure.

- The limits of the protective shield may be adjusted per the final Right of Entry agreement with the railroad. If protective shield is not installed in open spans, the contractor shall take proper precautions to prevent debris from falling on people or objects.

DECK PLAN

JACKING LOADS

Bearing Location	DL (kips)	LL (kips)	IM (kips)	Total (kips)
S. Abut. - Beam 1 and 12	22.6	24.1	6.2	52.9
S. Abut. - S. Abut. - Beam 6 and 7	22.2	24.1	6.2	52.5
S. Abut. - Beams 2 thru 5 and 8 thru 11	25.1	32.6	8.4	66.1
Pier 4 - Beam 1 and 12	73.3	28.9	7.1	109.3
Pier 4 - S. Abut. - Beam 6 and 7	71.9	28.9	7.1	107.9
Pier 4 - Beams 2 thru 5 and 8 thru 11	81.1	43.1	10.5	134.7

Service girder self-weight and deck weight prior to overlay are shown. The Contractor shall design and place jacking system to reposition the specified bearing for the stated reactions and as required in the special provision.

Loads are per Girder.

LEGEND:

- * Deck Slab Repair (Partial)
- Deck Slab Repair (Full Depth, Type II)
- Concrete Removal

* Areas of Deck Slab Repair (Partial) are provided for information only. Repair is included in the cost of Bridge Deck Latex Concrete Overlay, 2 3/4".

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Bridge Deck Grooving (Longitudinal)	Sq Yd	6,686
Protective Coat	Sq Yd	7,333
Bridge Deck Latex Concrete Overlay, 2 3/4"	Sq Yd	7,107
Bridge Deck Scarification, 3/4"	Sq Yd	7,107
Protective Shield	Sq Yd	8,867
Jack and Repositioning Bearings	Each	24
Deck Slab Repair (Full Depth, Type II)	Sq Yd	10
Diamond Grinding (Bridge Section)	Sq Yd	7,267

REVISED SHEET 1/4/2021



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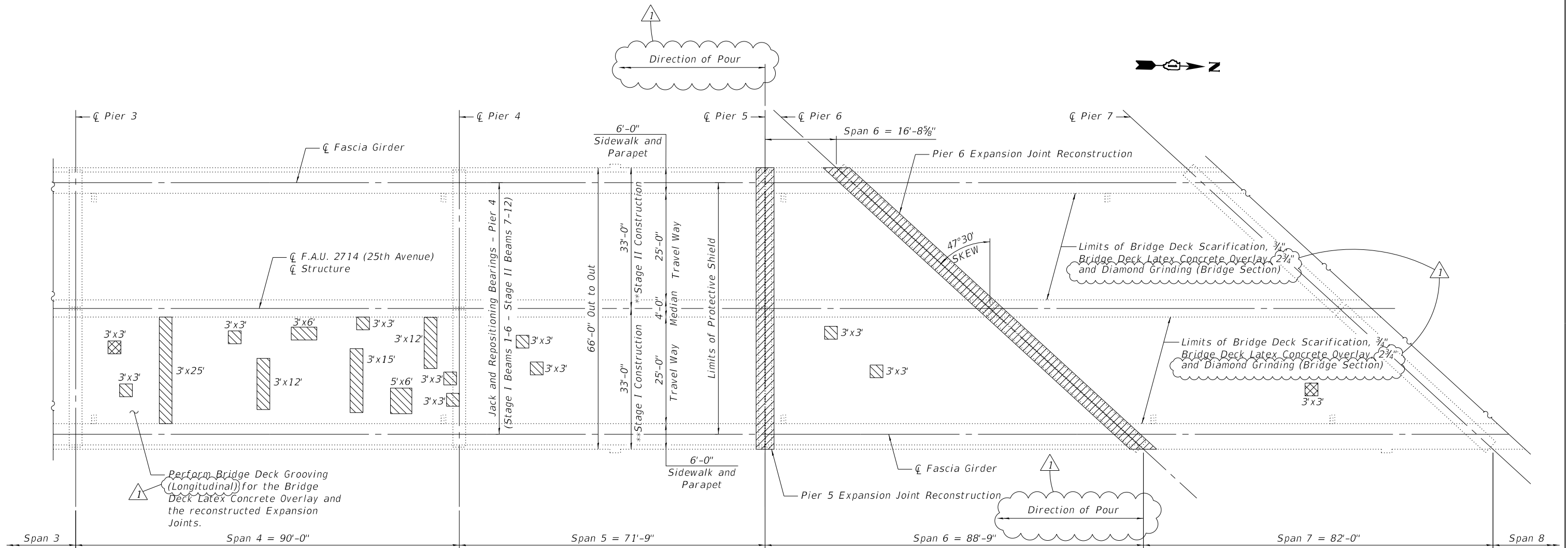
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DECK REPAIR PLAN (1 of 5)
STRUCTURE NO. 016-1011

SHEET 5-06 OF 5-34 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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ILLINOIS		FED. AID PROJECT		

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NOTES:

- See Sheet S-06 for Bill of Material and Notes.
- See Sheet S-06 for Jacking Loads.
- For Spans 1 thru 5, pour Bridge Deck Latex Concrete Overlay, 2 3/4" downhill from Pier 5 to South Abutment.
- For Span 6, pour Bridge Deck Latex Concrete Overlay, 2 3/4" downhill from Pier 6 to Pier 5.

** Deck Stage Construction limits shown.
Median construction at joints to be constructed in Stage II.

DECK PLAN

LEGEND:

- * Deck Slab Repair (Partial)
- Deck Slab Repair (Full Depth, Type II)
- Concrete Removal

* Areas of Deck Slab Repair (Partial) are provided for information only. Repair is included in the cost of Bridge Deck Latex Concrete Overlay, 2 3/4".

1 REVISED SHEET 1/4/2021



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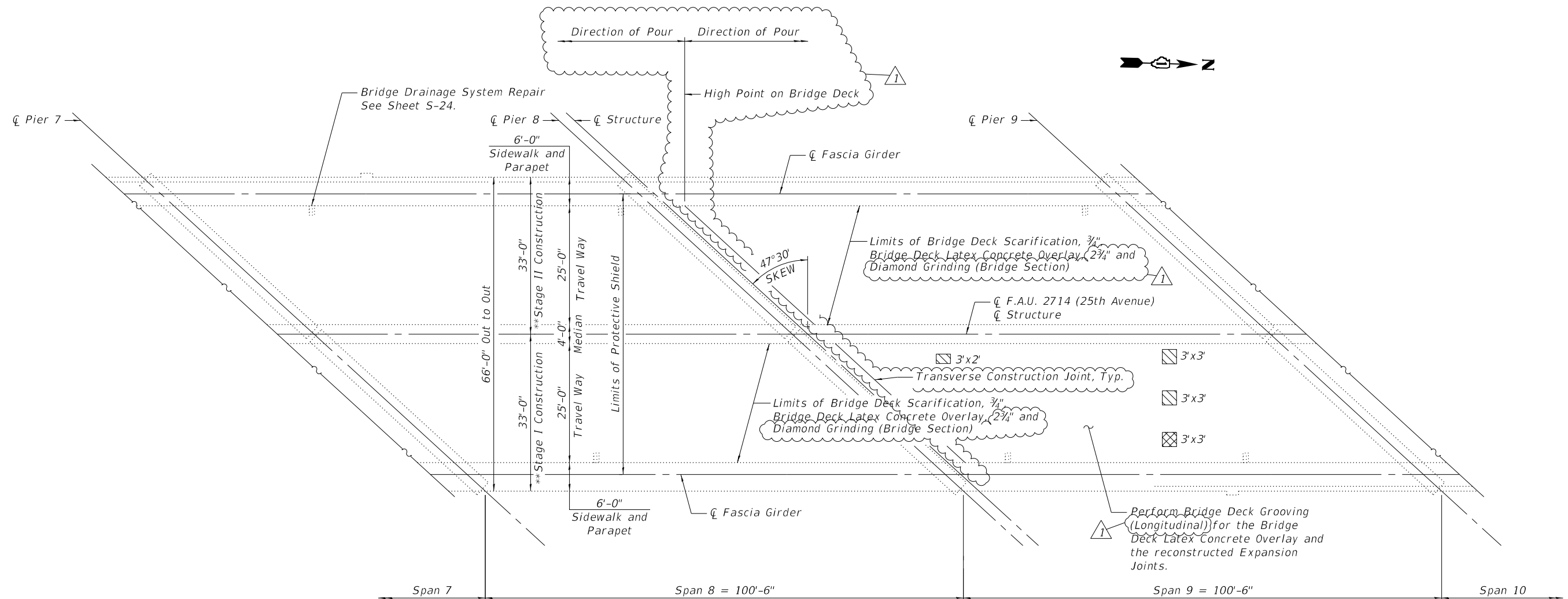
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DECK REPAIR PLAN (2 of 5)
STRUCTURE NO. 016-1011

SHEET S-07 OF S-34 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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DECK PLAN

NOTE:

1. See Sheet S-06 for Bill of Material and Notes.
2. For Spans 7 and 8, pour Bridge Deck Latex Concrete Overlay, 2 3/4\"/>
3. For Spans 9 and 10, pour Bridge Deck Latex Concrete Overlay, 2 3/4\"/>

** Deck Stage Construction limits shown.
Median construction at joints to be
constructed in Stage II.

LEGEND:

- * Deck Slab Repair (Partial)
- Deck Slab Repair (Full Depth, Type II)
- Concrete Removal

* Areas of Deck Slab Repair (Partial) are provided
for information only. Repair is included in the
cost of Bridge Deck Latex Concrete Overlay, 2 3/4\"/>

1 REVISED SHEET 1/4/2021



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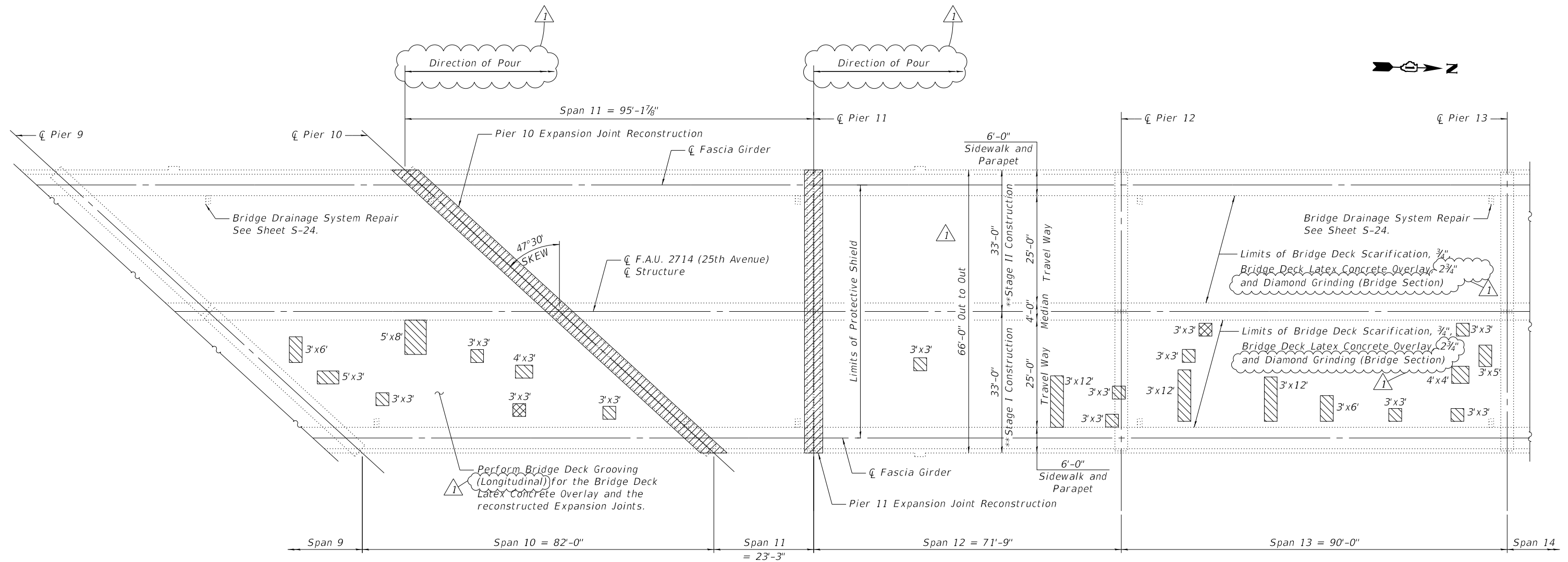
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DECK REPAIR PLAN (3 of 5)
STRUCTURE NO. 016-1011

SHEET S-08 OF S-34 SHEETS

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ILLINOIS		FED. AID PROJECT		

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DECK PLAN

NOTE:

- See Sheet S-06 for Bill of Material and Notes.
- For Span 11, pour Bridge Deck Latex Concrete Overlay, 2 3/4" downhill from Pier 10 to Pier 11.
- For Spans 12 thru 16, pour Bridge Deck Latex Concrete Overlay, 2 3/4" downhill from Pier 11 to North Abutment.

**Deck Stage Construction limits shown.
Median construction at joints to be constructed in Stage II.

LEGEND:

- * Deck Slab Repair (Partial)
- Deck Slab Repair (Full Depth, Type II)
- Concrete Removal

* Areas of Deck Slab Repair (Partial) are provided for information only. Repair is included in the cost of Bridge Deck Latex Concrete Overlay, 2 3/4".



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

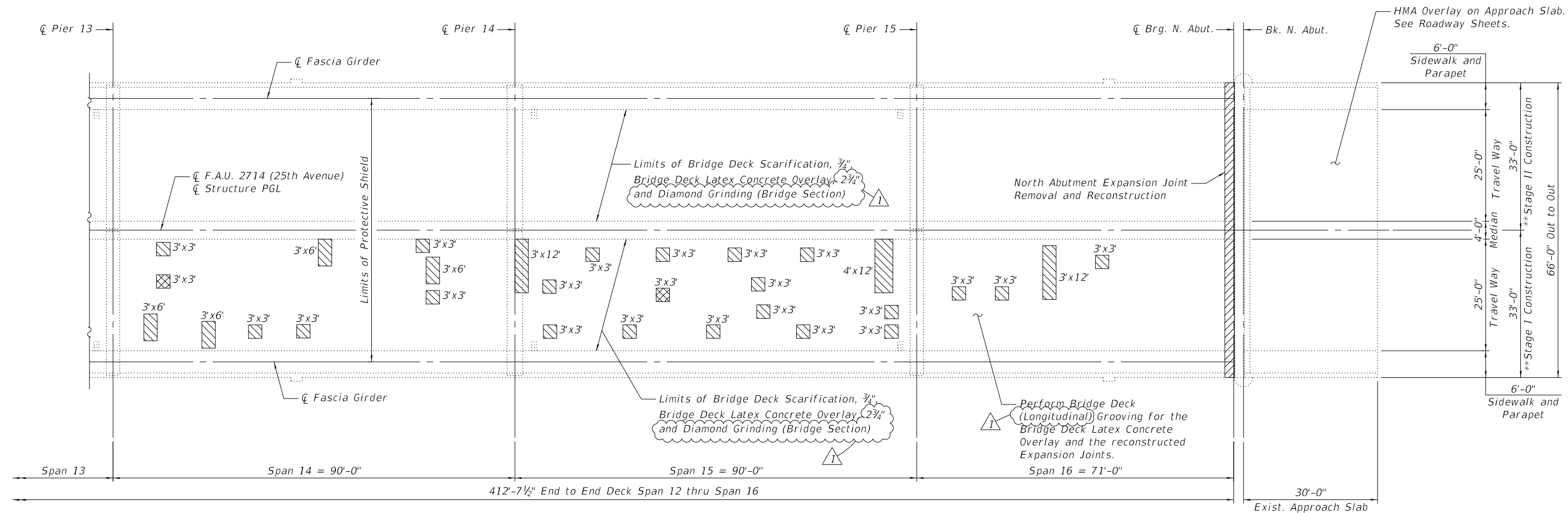
DECK REPAIR PLAN (4 of 5)
STRUCTURE NO. 016-1011

SHEET 5-09 OF 5-34 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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DECK PLAN

NOTE:

1. See Sheet S-06 for Bill of Material and Notes.

** Deck Stage Construction limits shown.
Median construction at joints to be
constructed in Stage II.

LEGEND:

- * Deck Slab Repair (Partial)
- Deck Slab Repair (Full Depth, Type II)
- Concrete Removal

* Areas of Deck Slab Repair (Partial) are provided
for information only. Repair is included in the
cost of Bridge Deck Latex Concrete Overlay, 2 3/4".

1 REVISED SHEET 1/4/2021



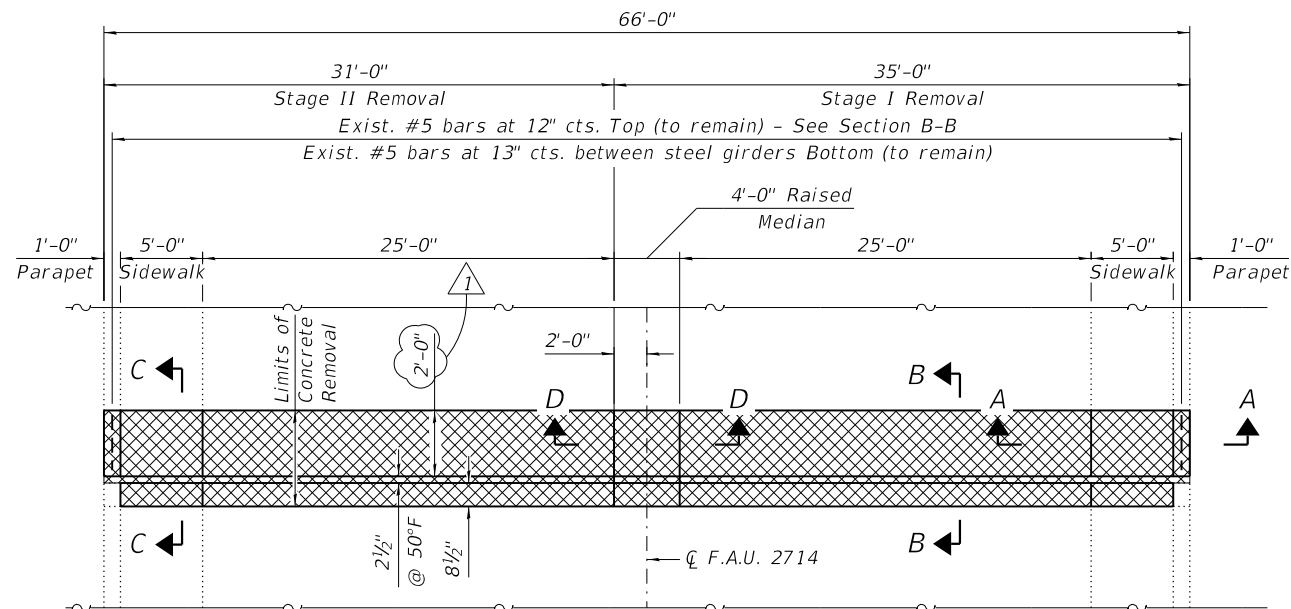
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

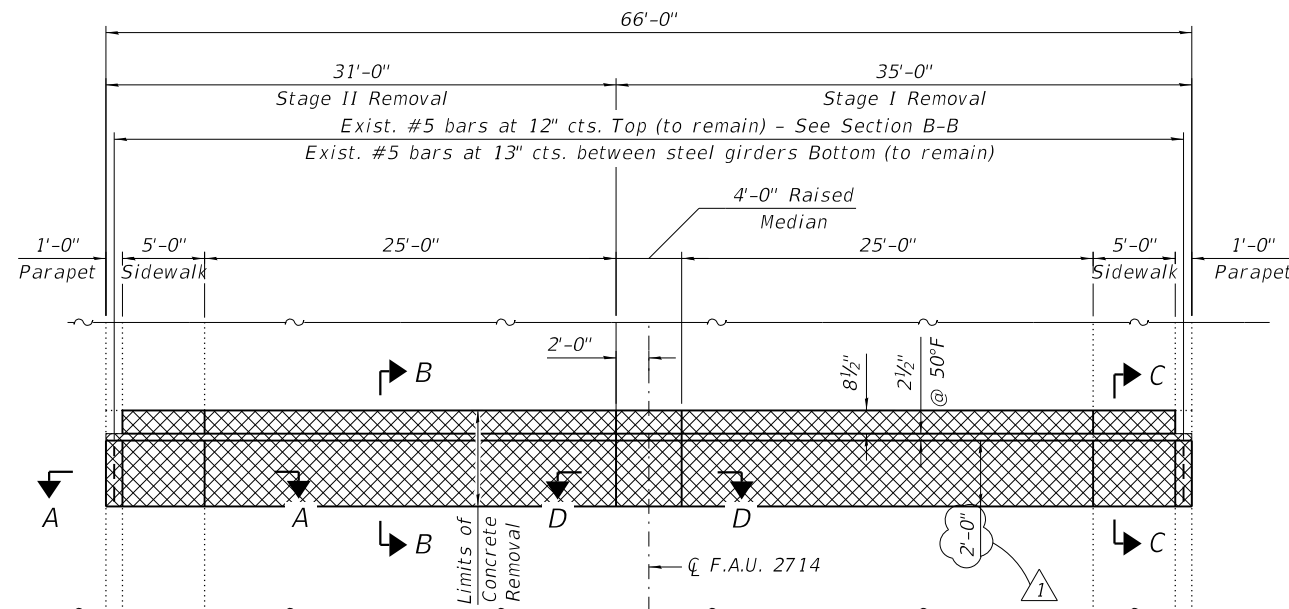
DECK REPAIR PLAN (5 of 5)
STRUCTURE NO. 016-1011

SHEET 5-10 OF 5-34 SHEETS

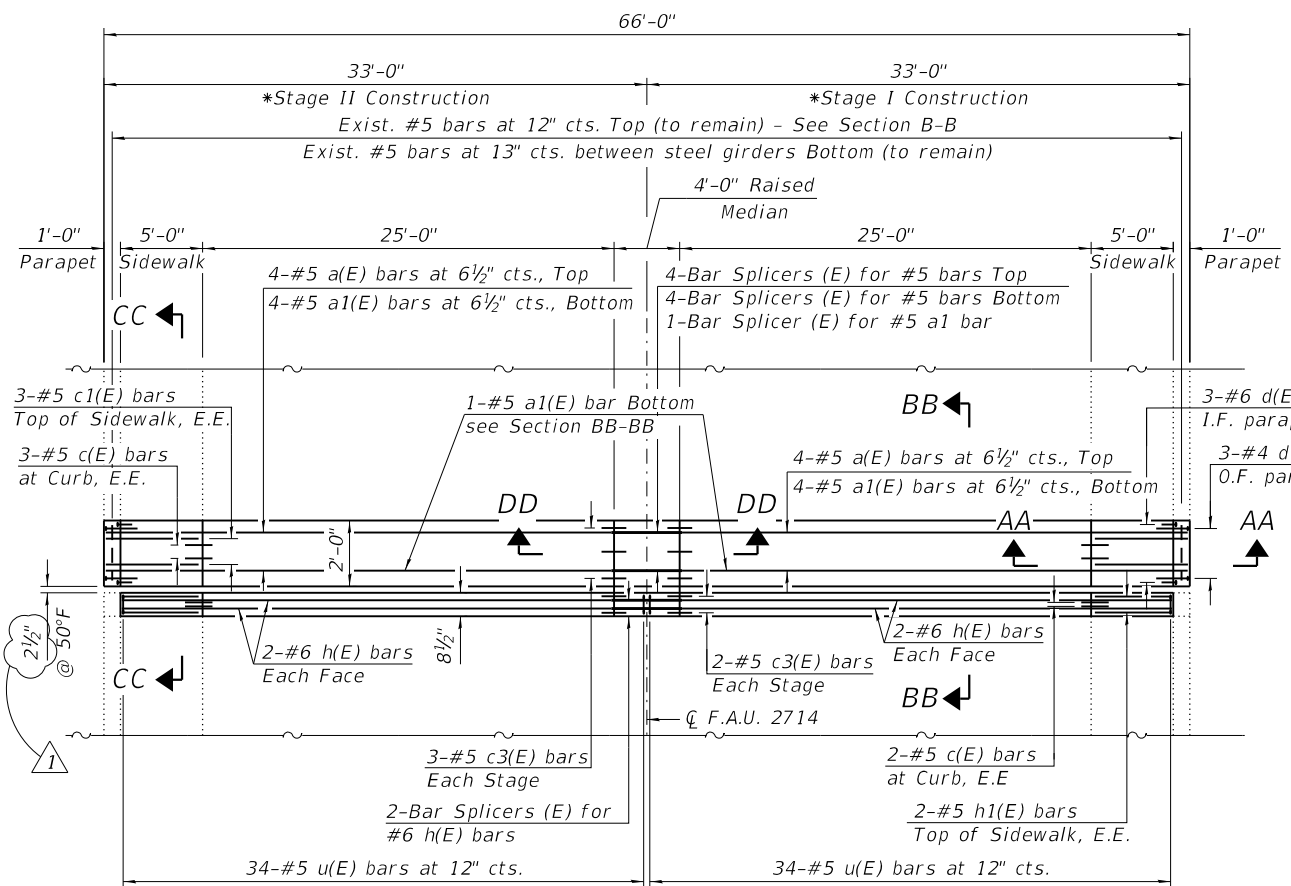
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CONTRACT NO. 62M80				
ILLINOIS		FED. AID PROJECT		



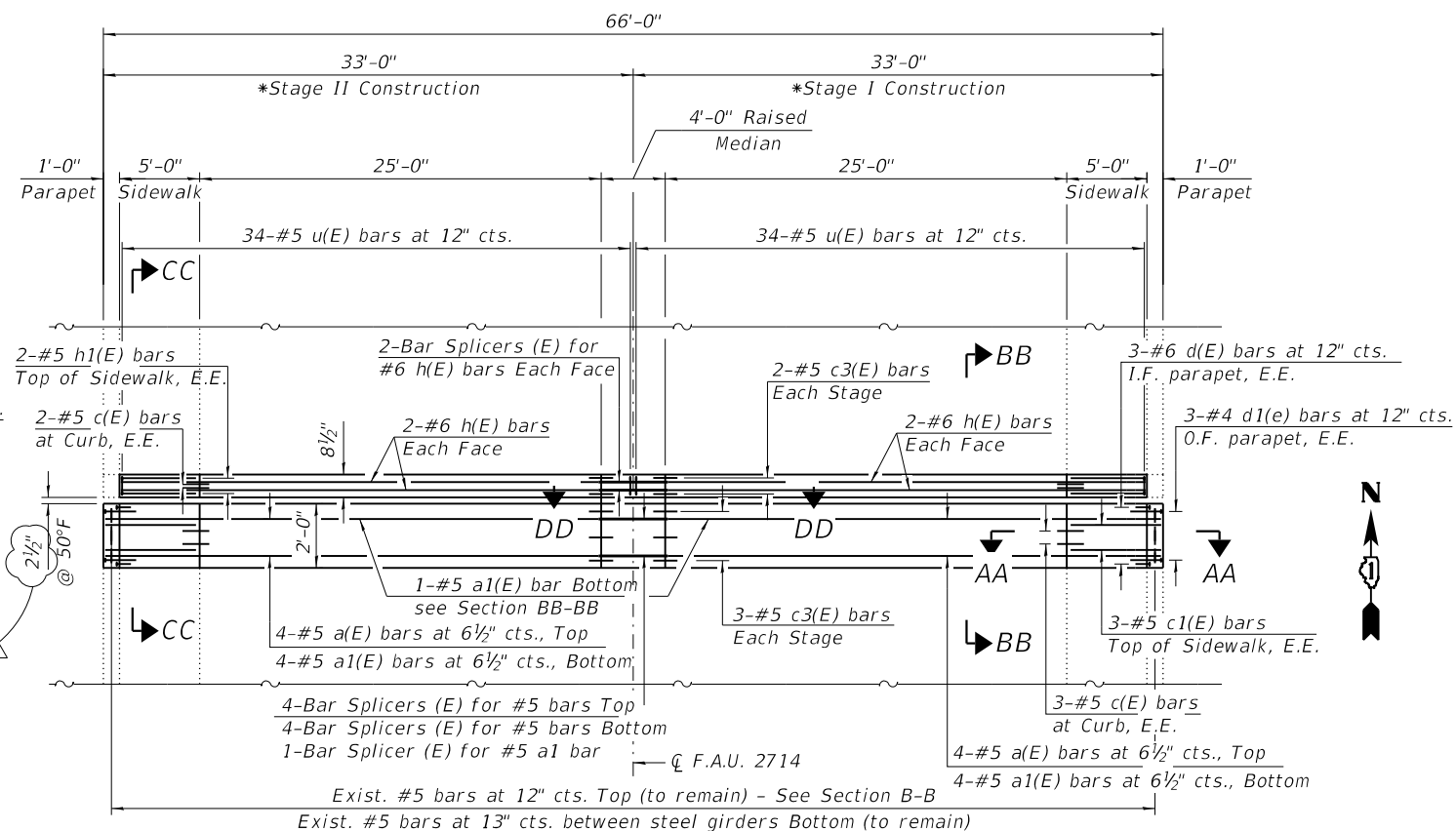
SOUTH ABUTMENT JOINT REMOVAL PLAN



NORTH ABUTMENT JOINT REMOVAL PLAN



SOUTH ABUTMENT JOINT RECONSTRUCTION PLAN



NORTH ABUTMENT JOINT RECONSTRUCTION PLAN

NOTES:

- For preformed joint strip seal details, see Sheet S-15 thru S-17.
- For Bar Splicer Assembly details, see Sheet S-34.
- Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.
- See Sheet S-12 for Sections and Bill of Material.
- Raised Median and c2(E) bar to be constructed in Stage II.

*Deck construction limits shown.
Median to be constructed in Stage II.

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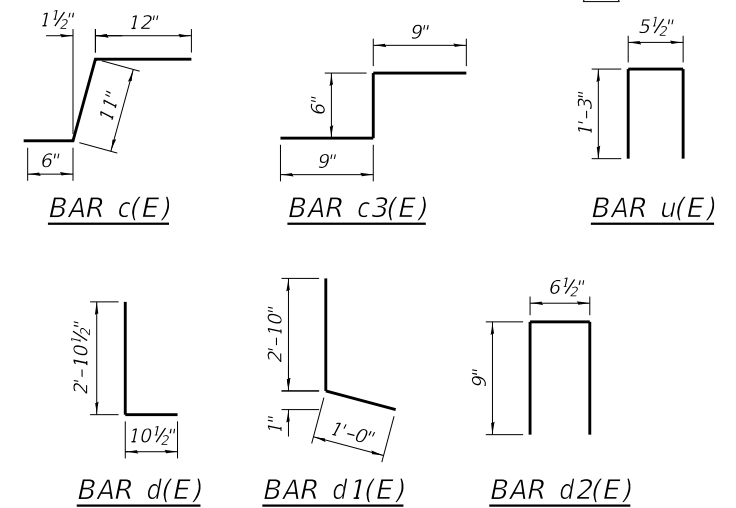
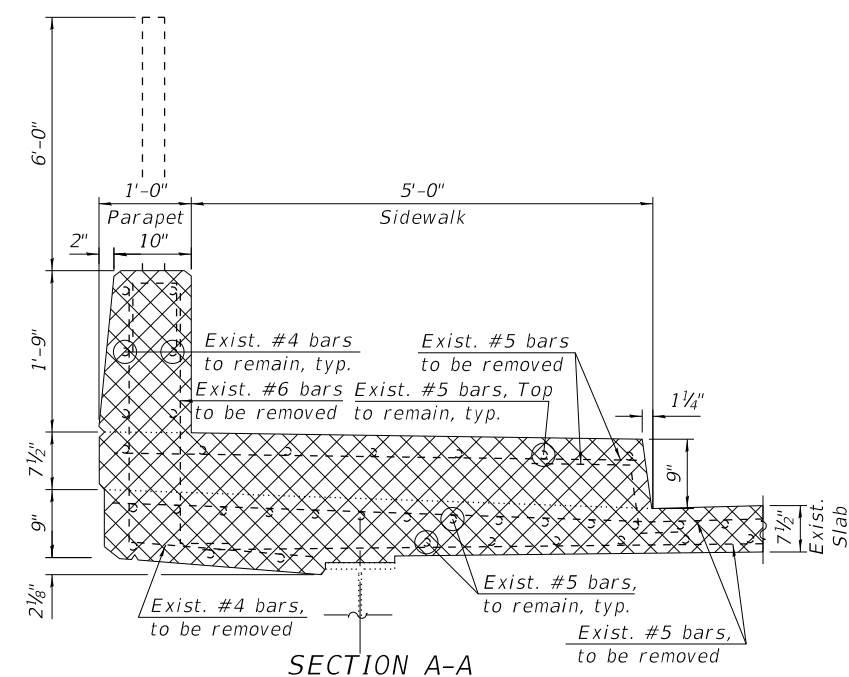
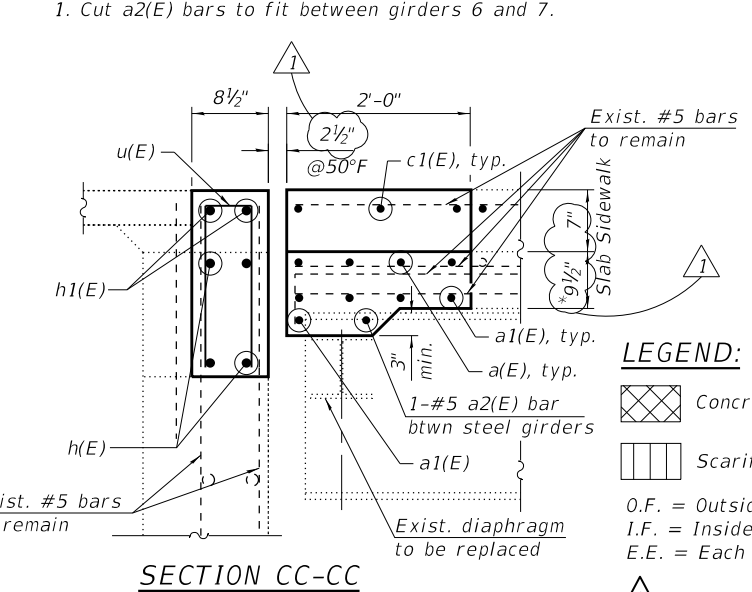
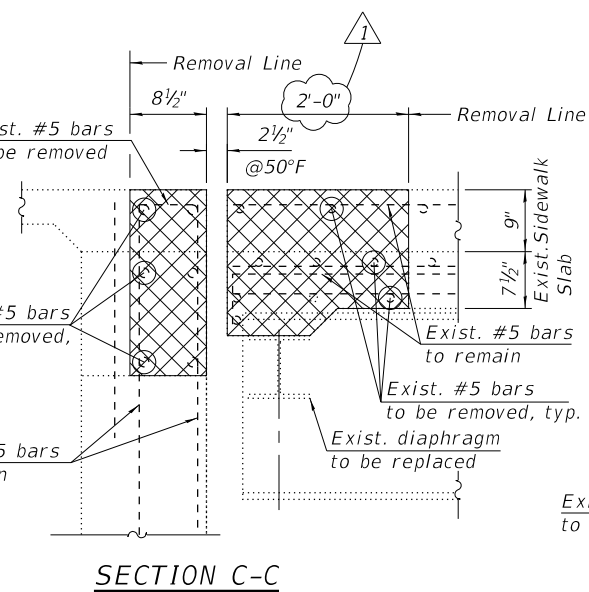
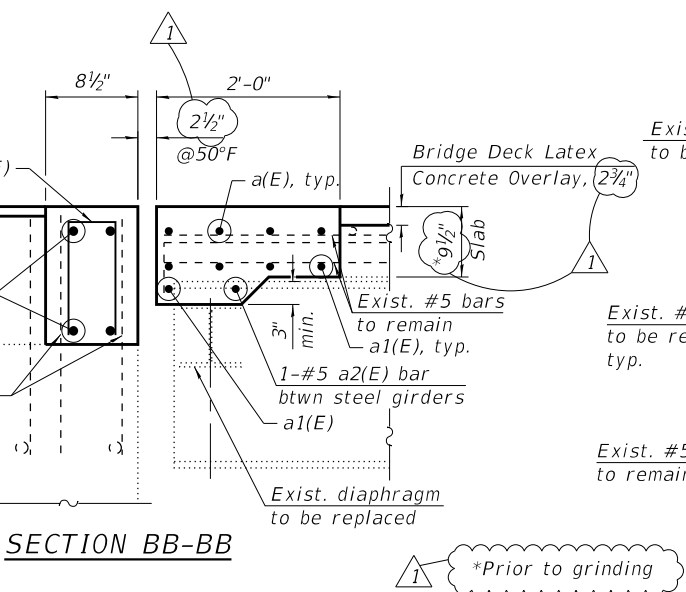
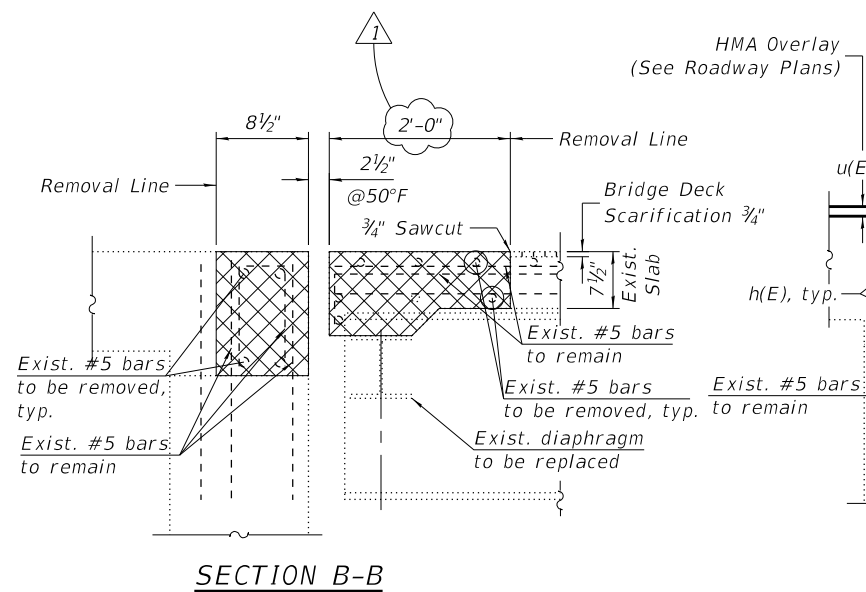
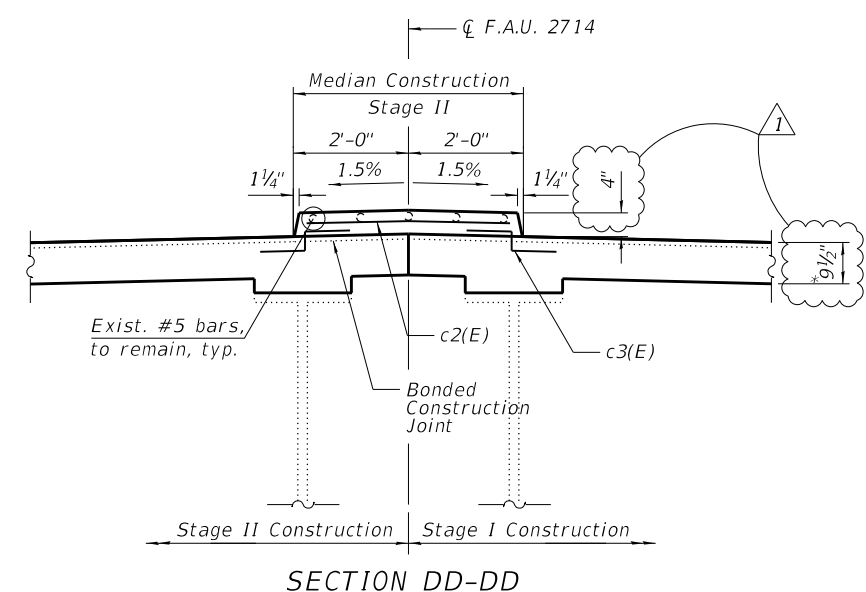
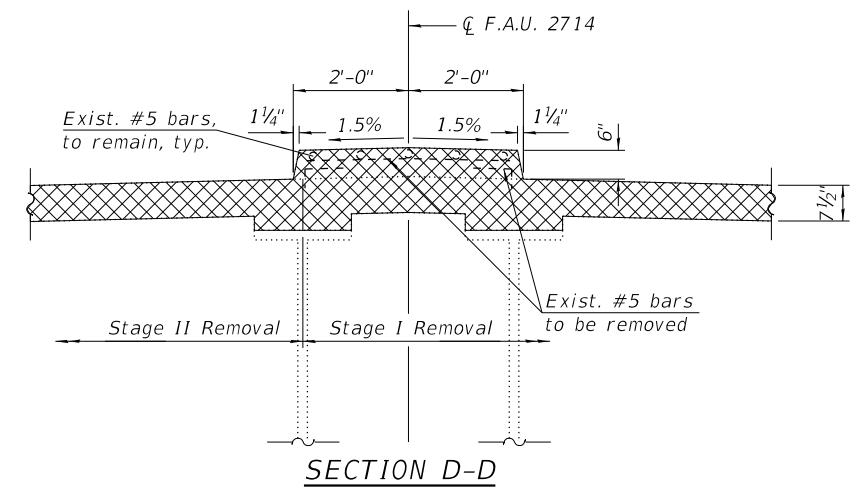
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

N. AND S. ABUT. JOINT REMOVAL AND RECONSTRUCTION PLAN
STRUCTURE NO. 016-1011

SHEET S-11 OF S-34 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-161-BR	COOK	79	43
CONTRACT NO. 62M80				
ILLINOIS		FED. AID PROJECT		

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BILL OF MATERIAL				
Bar	No.	Size	Length	Shape
a(E)	16	#5	32'-6"	
a1(E)	20	#5	31'-8"	
a2(E)	22	#5	5'-5"	
c(E)	20	#5	2'-5"	
c1(E)	12	#5	5'-6"	
c2(E)	10	#5	3'-6"	
c3(E)	20	#5	2'-0"	
d(E)	12	#6	3'-9"	
d1(E)	12	#4	3'-10"	
d2(E)	12	#4	2'-1"	
h(E)	16	#6	31'-8"	
h1(E)	8	#5	4'-6"	
u(E)	136	#5	3'-0"	
Concrete Removal			Cu. Yd.	17.1
Concrete Superstructure			Cu. Yd.	18.0
Reinforcement Bars, Epoxy Coated			Pound	2,870

NOTE:
1. Cut a2(E) bars to fit between girders 6 and 7.

LEGEND:
Concrete Removal
Scarification
O.F. = Outside Face
I.F. = Inside Face
E.E. = Each End

REVISD SHEET 1/4/2021

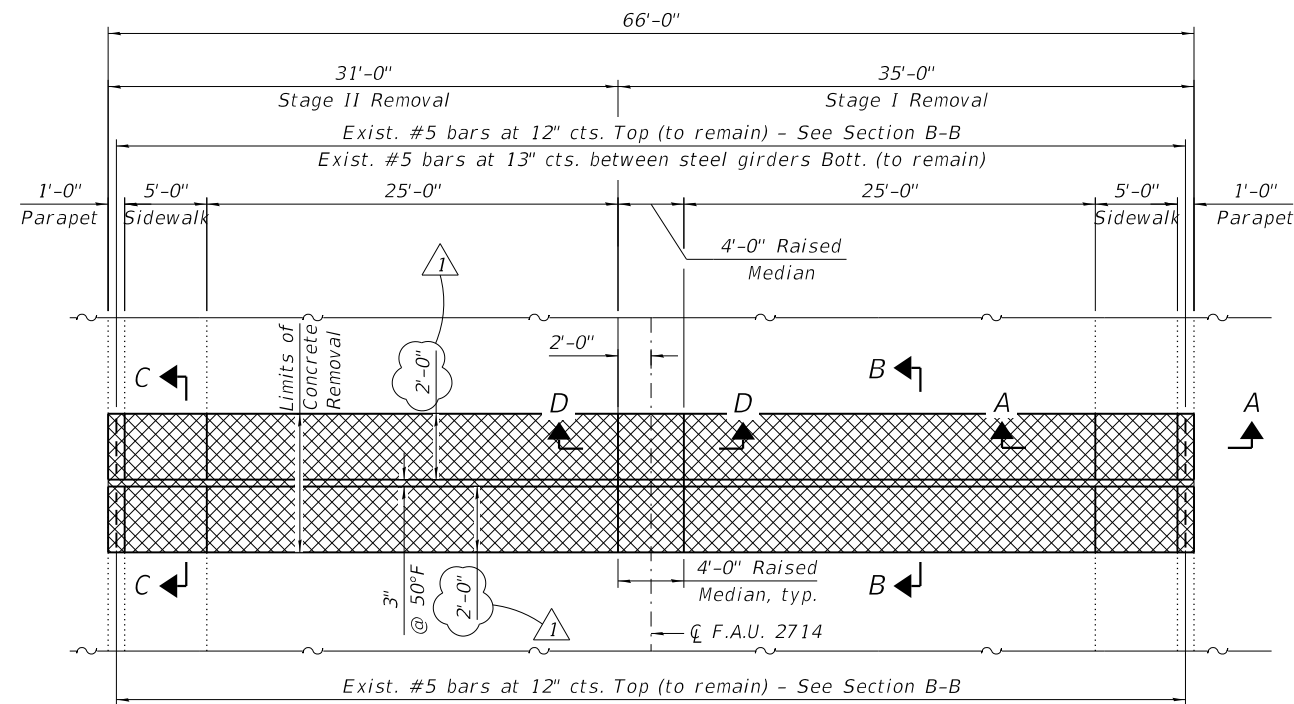


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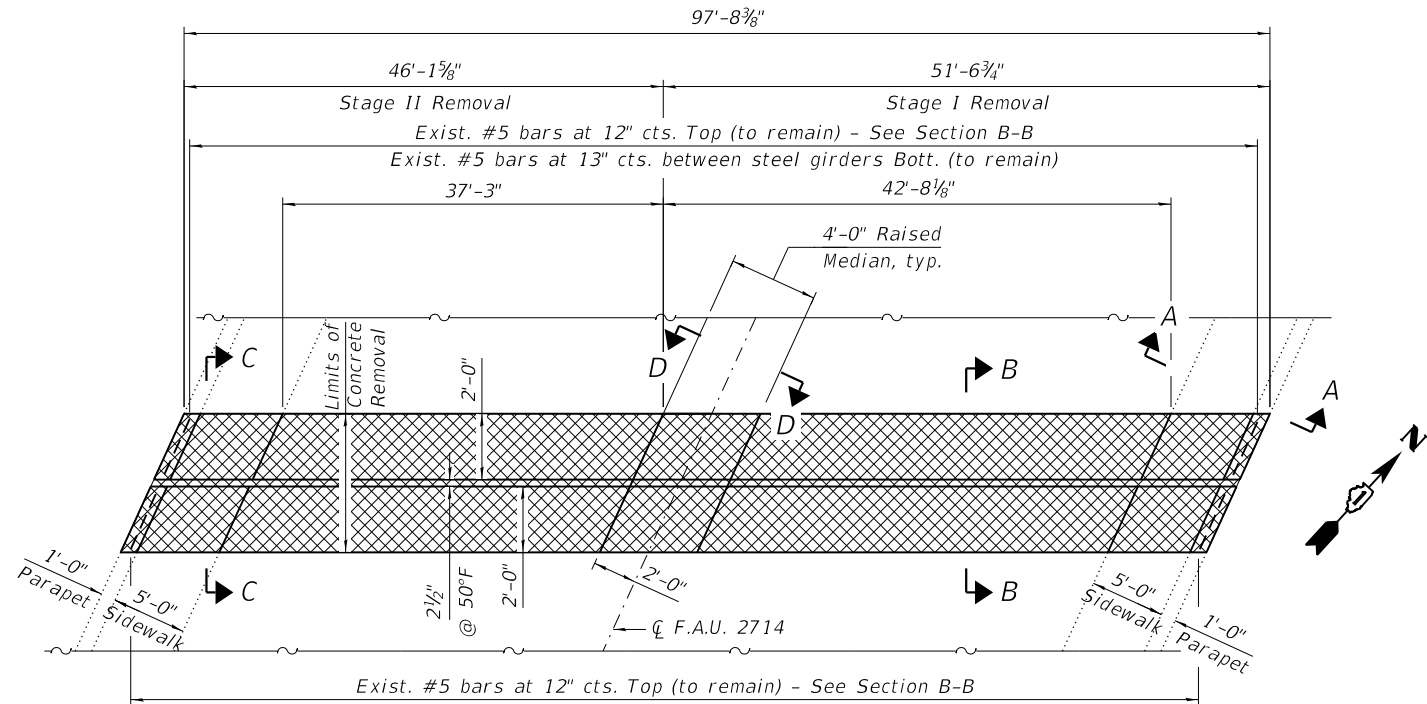
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

N. AND S. ABUT. JOINT REMOVAL AND RECONSTRUCTION DETAILS
STRUCTURE NO. 016-1011

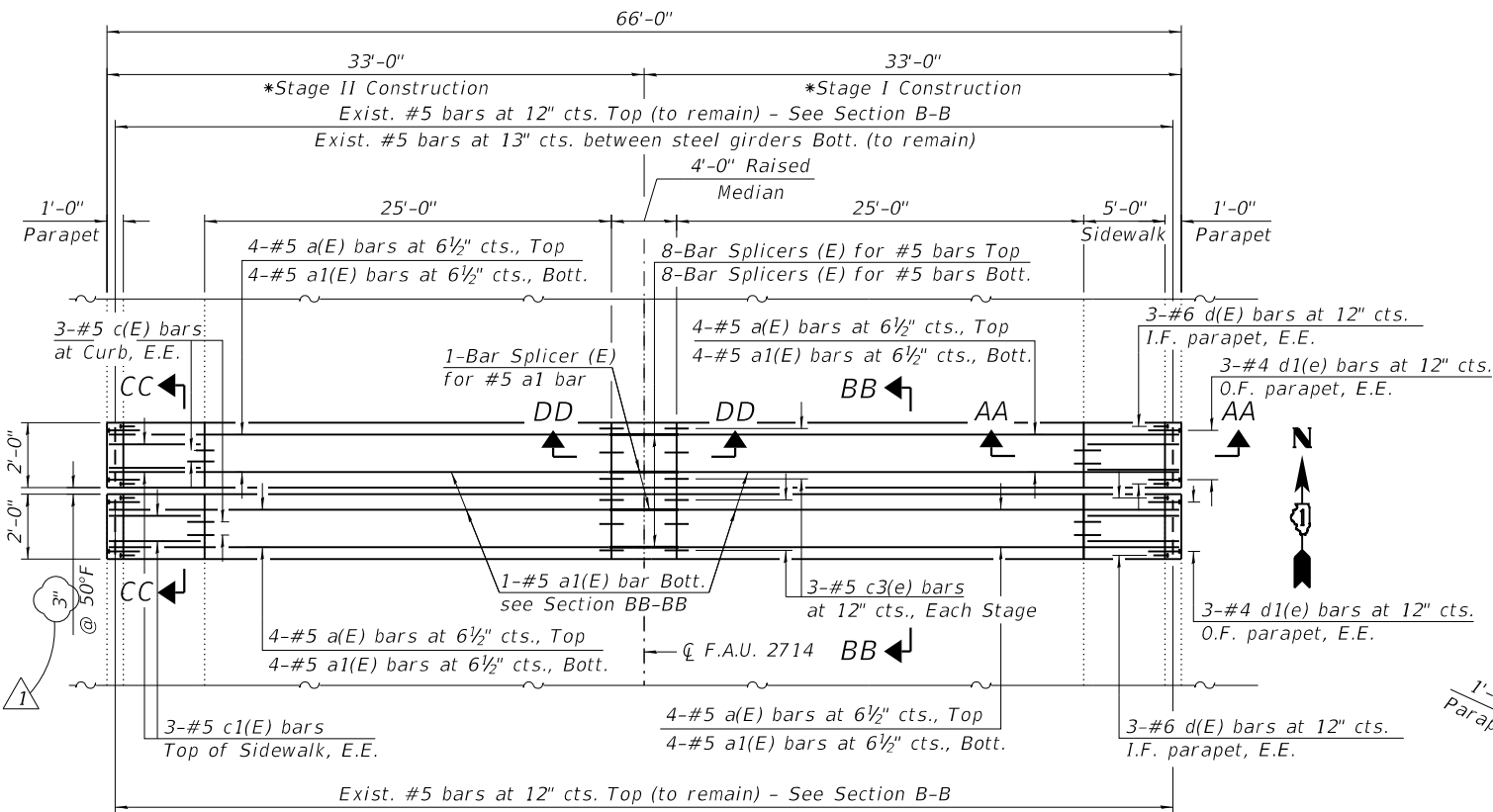
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ILLINOIS FED. AID PROJECT				



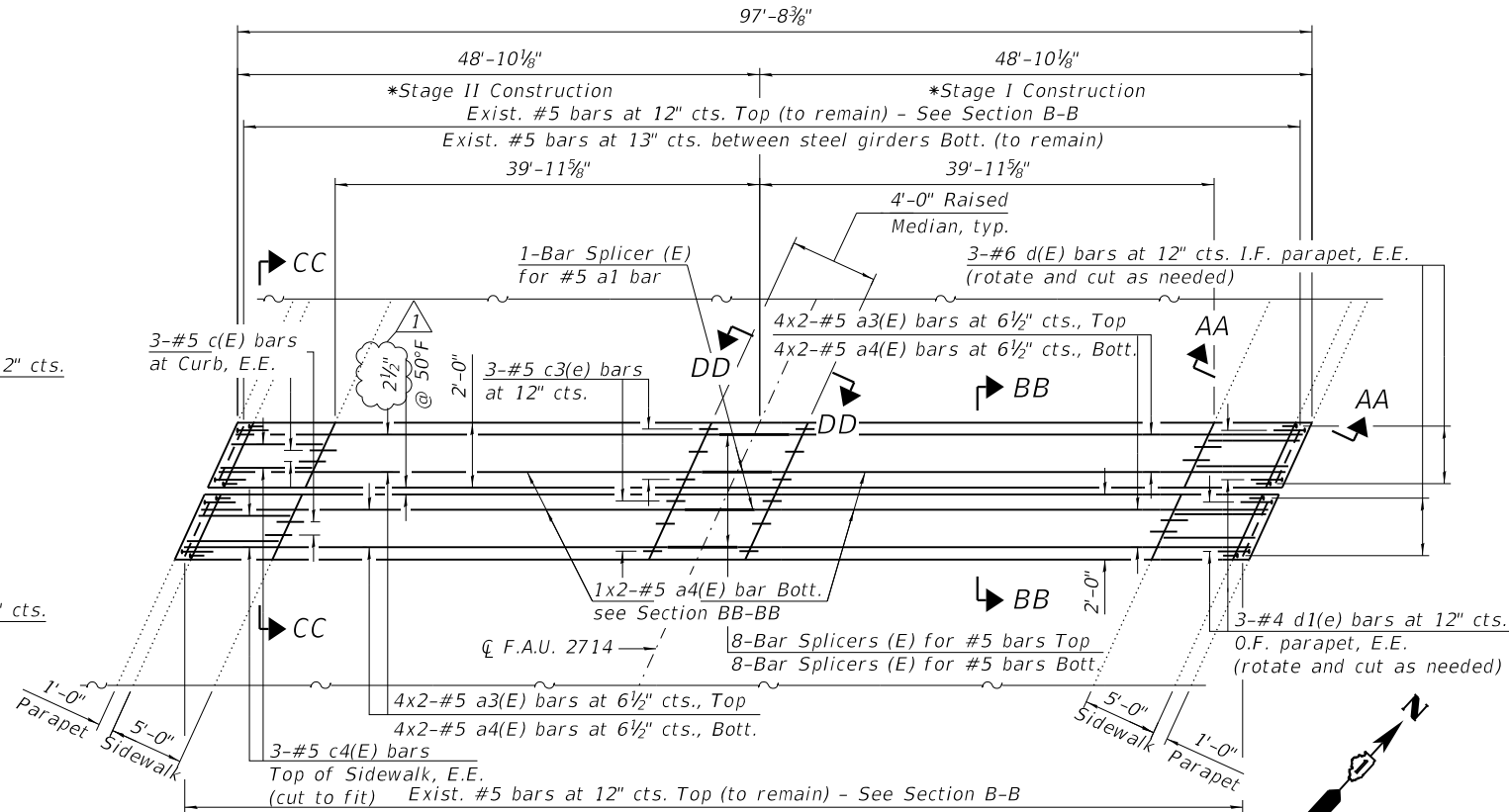
PIER 5 AND 11 JOINT REMOVAL PLAN



PIER 6 AND 10 JOINT REMOVAL PLAN



PIER 5 AND 11 JOINT RECONSTRUCTION PLAN



PIER 6 AND 10 JOINT RECONSTRUCTION PLAN

NOTES:

- For preformed joint strip seal details, see Sheet S-15 thru S-17.
- For Bar Splicer Assembly details, see Sheet S-34.
- Existing reinforcement bars extending into the removal area shall be cleaned, straightened and incorporated into the new construction. Any reinforcement bars that are damaged during concrete removal shall be replaced with an approved bar splicer or anchorage system. Cost included with Concrete Removal.
- See Sheet S-14 for Sections and Bill of Material.
- Raised Median and c2(E) bar to be constructed in Stage II.

*Deck construction limits shown.
Median to be constructed in Stage II.

1 REVISED SHEET 1/4/2021

MINIMUM BAR LAP

#5 bar = 3'-9"

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIERS 5, 6, 10 AND 11 JOINT REMOVAL AND RECONSTRUCTION PLAN
STRUCTURE NO. 016-1011

SHEET S-13 OF S-34 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62M80				
ILLINOIS		FED. AID PROJECT		

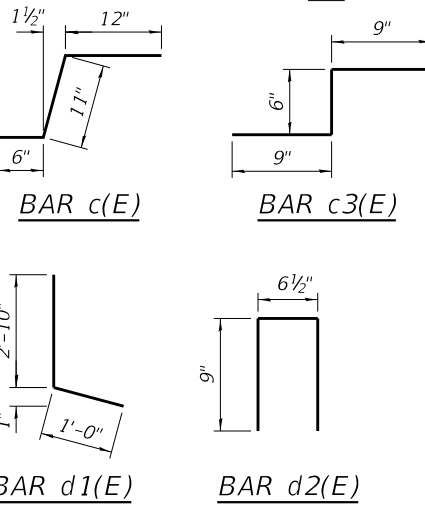
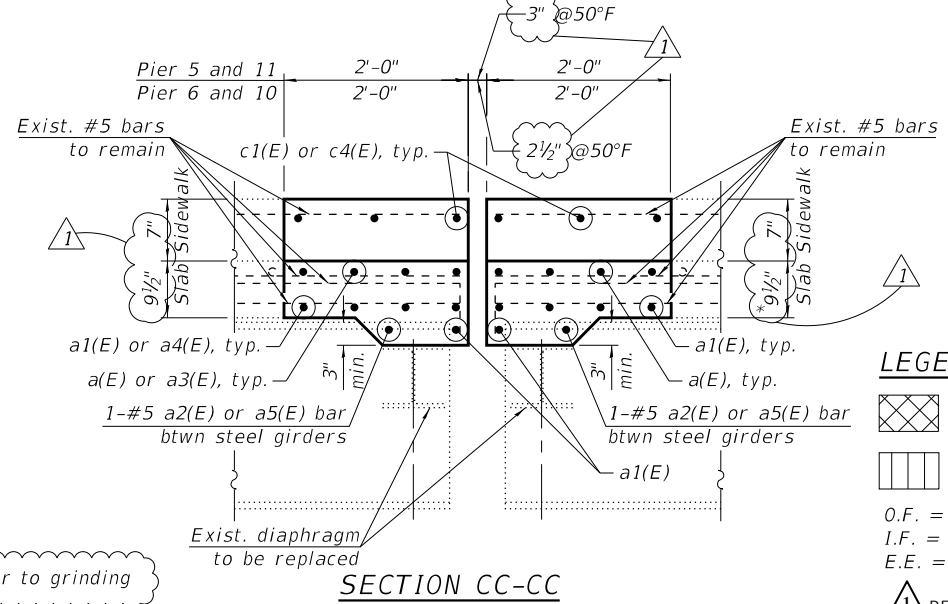
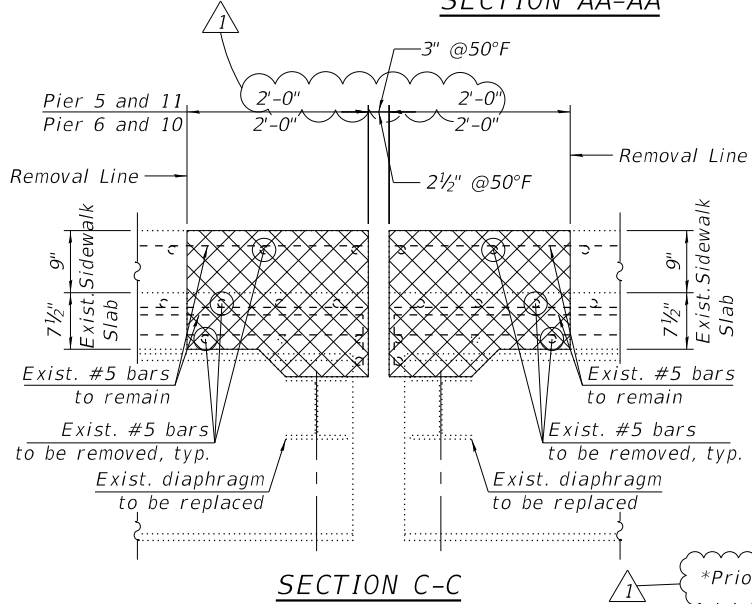
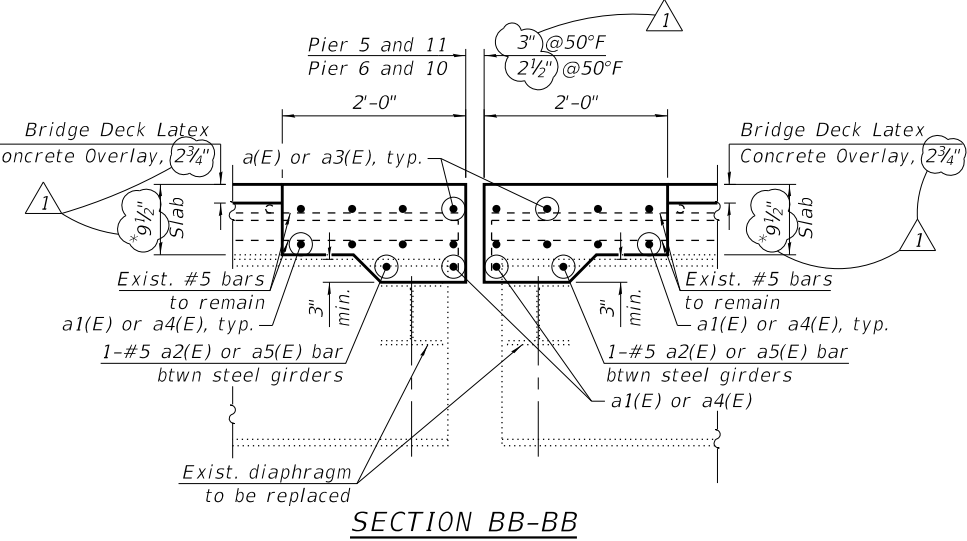
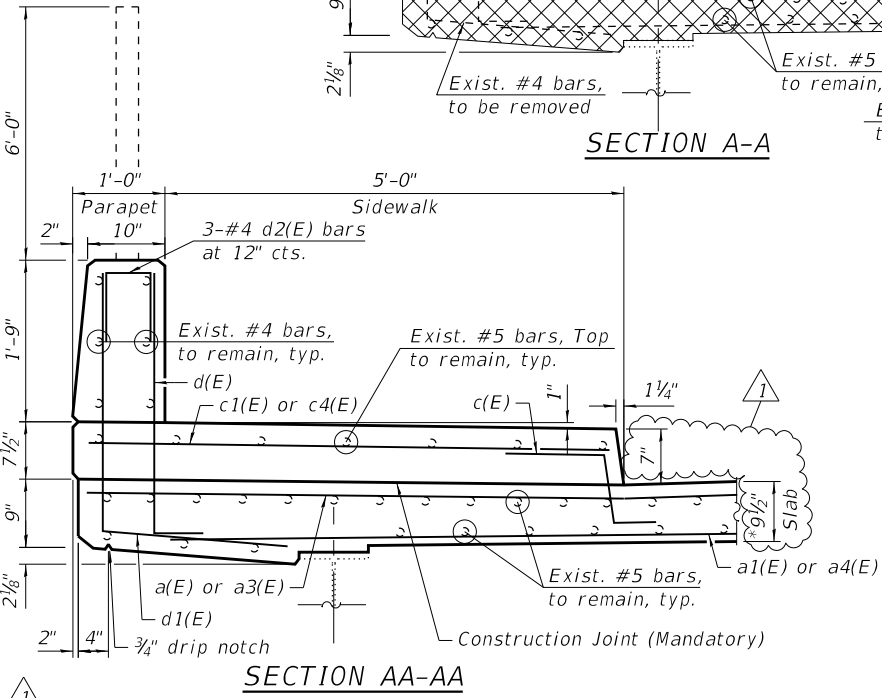
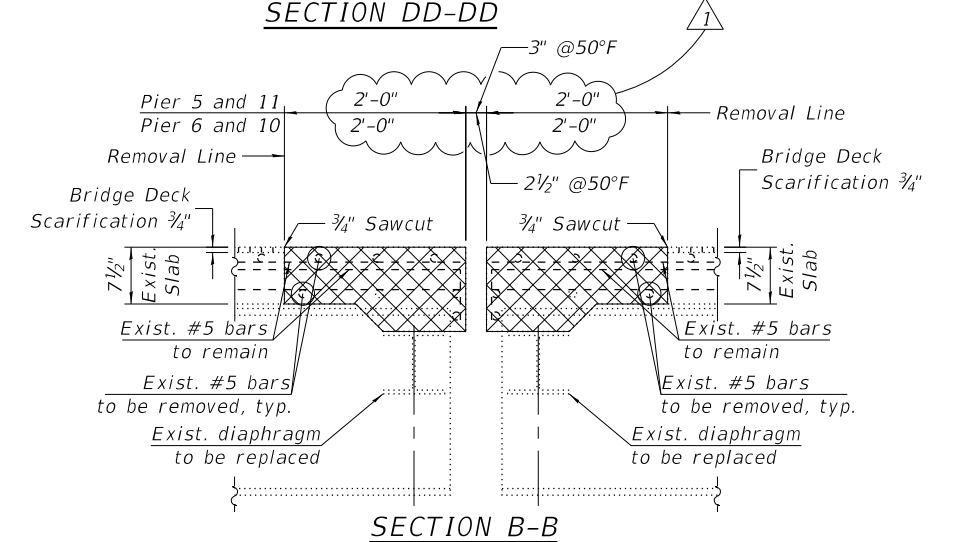
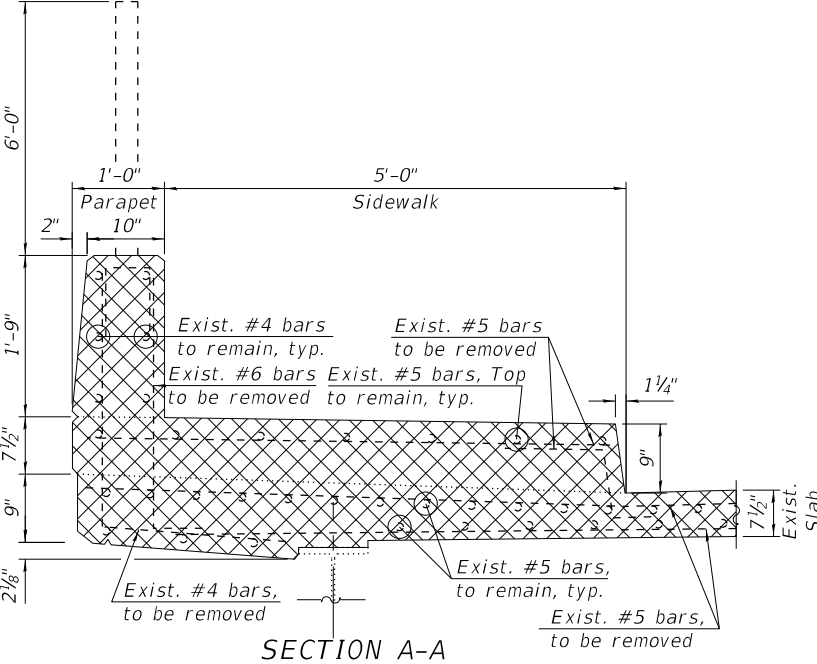
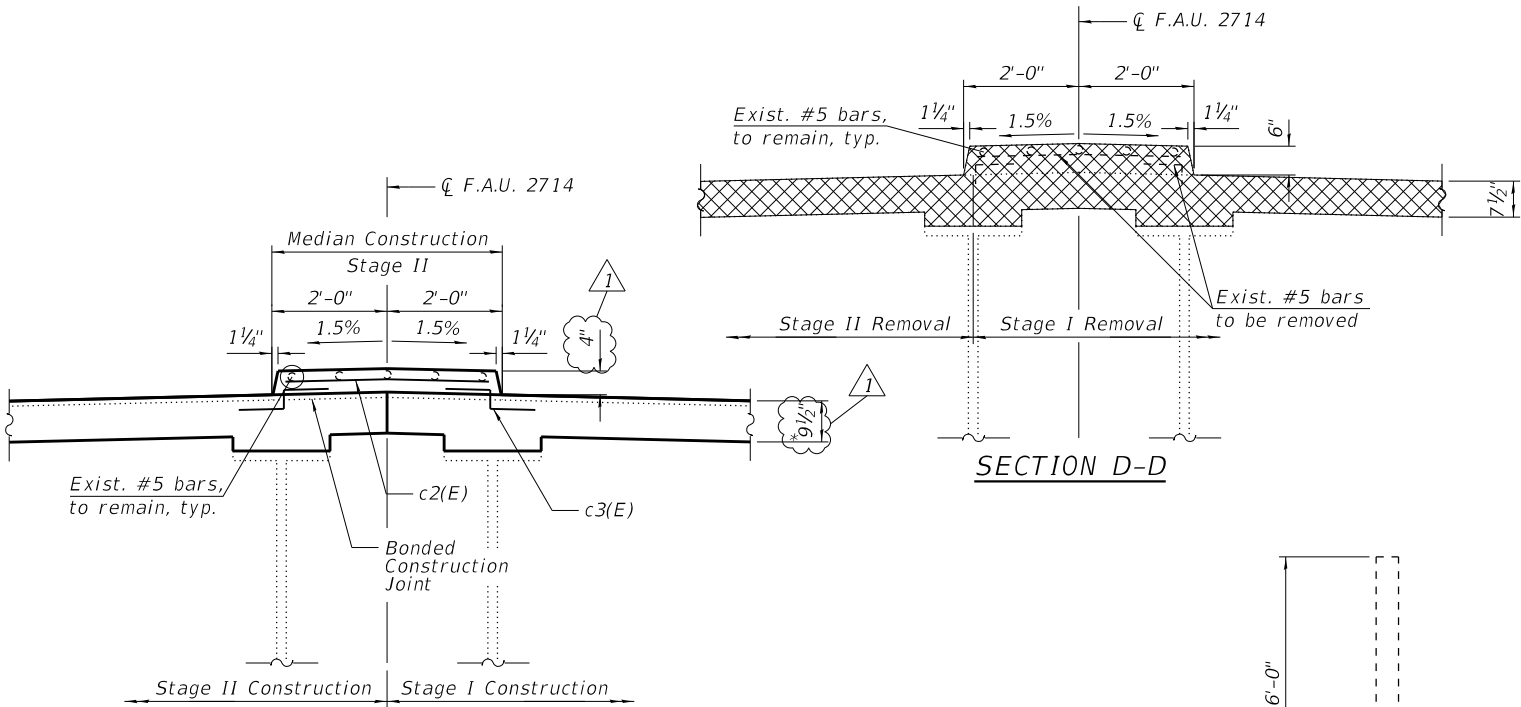
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BILL OF MATERIAL

Bar	No.	Size	Length	Shape
a(E)	32	#5	32'-6"	
a1(E)	40	#5	31'-8"	
a2(E)	44	#5	5'-5"	
a3(E)	64	#5	26'-4"	
a4(E)	80	#5	25'-7"	
a5(E)	44	#5	8'-2"	
c(E)	48	#5	2'-5"	
c1(E)	24	#5	5'-6"	
c2(E)	24	#5	3'-6"	
c3(E)	48	#5	2'-0"	
c4(E)	24	#5	8'-4"	
d(E)	48	#6	3'-9"	
d1(E)	48	#4	3'-10"	
d2(E)	48	#4	2'-1"	
Concrete Removal			Cu. Yd.	52.5
Concrete Superstructure			Cu. Yd.	60.8
Reinforcement Bars, Epoxy Coated			Pound	8,040



NOTE:
1. Cut a2(E) a5(E) bars to fit between girders 6 and 7.

LEGEND:
Concrete Removal
Scarification
O.F. = Outside Face
I.F. = Inside Face
E.E. = Each End

REVISED SHEET 1/4/2021

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER 5, 6, 10 AND 11 JOINT REMOVAL AND RECONSTRUCTION DETAILS
STRUCTURE NO. 016-1011

SHEET 5-14 OF 5-34 SHEETS

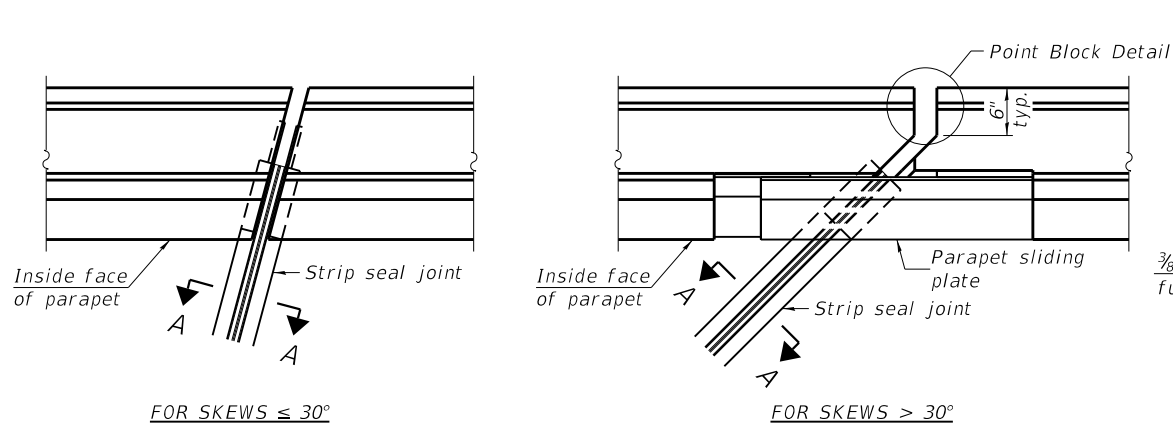
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CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

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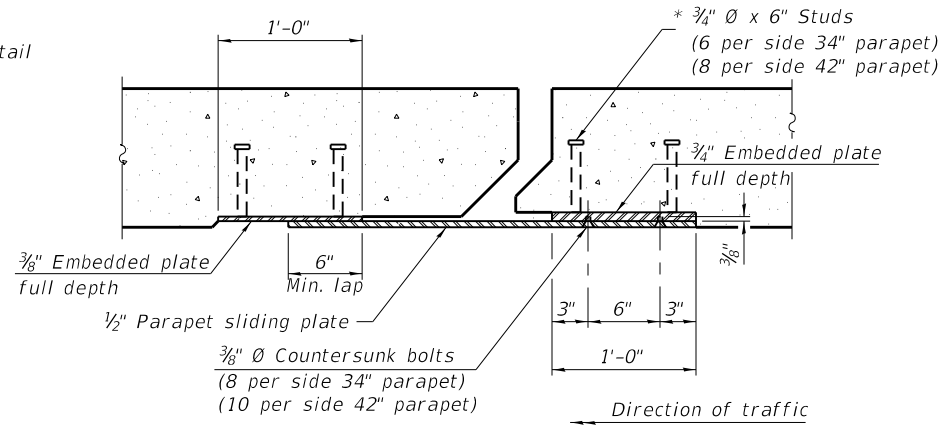
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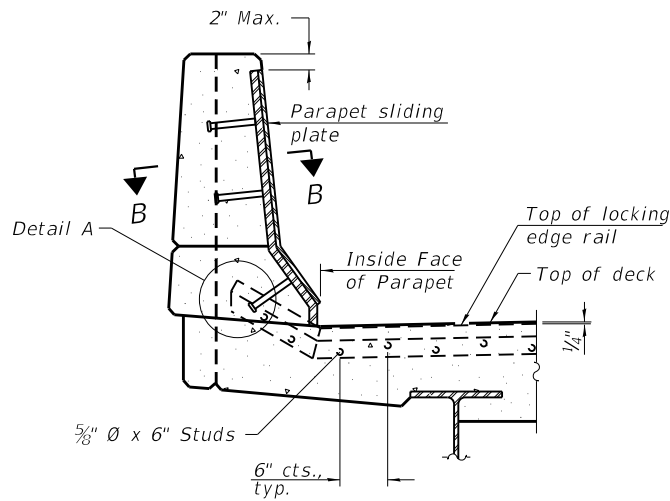
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PLAN AT PARAPET

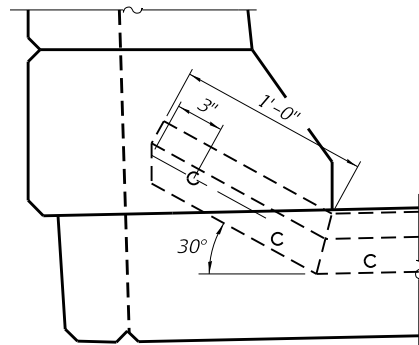


SECTION B-B



ELEVATION AT PARAPET

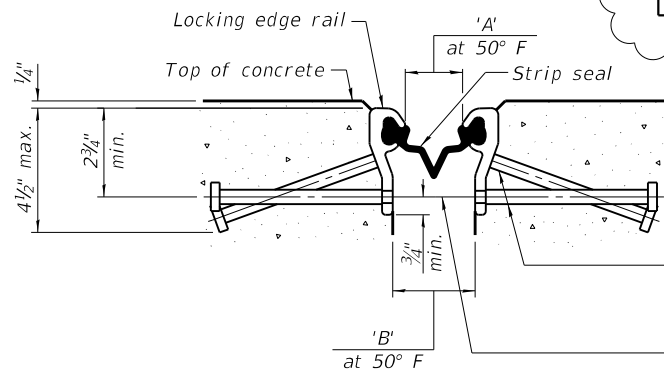
(Skews > 30° shown. Skews ≤ 30° similar except as shown in plan view.)



DETAIL A

JOINT OPENING TABLE

Joint	A	B	C
S. Abut.	1 5/8"	2 1/2"	3 1/8"
Pier 5	2 1/8"	3"	3 5/8"
Pier 6	1 5/8"	2 1/2"	3 1/8"
Pier 10	1 5/8"	2 1/2"	3 1/8"
Pier 11	2 1/8"	3"	3 5/8"
N. Abut.	1 5/8"	2 1/2"	3 1/8"



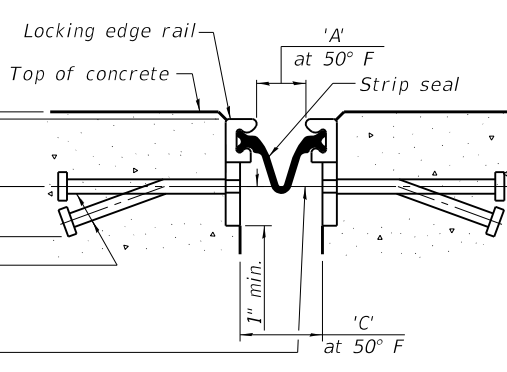
SHOWING ROLLED RAIL JOINT

* 5/8" Ø x 6" studs @ 6" cts. (alternate angled/bent studs with horizontal studs)

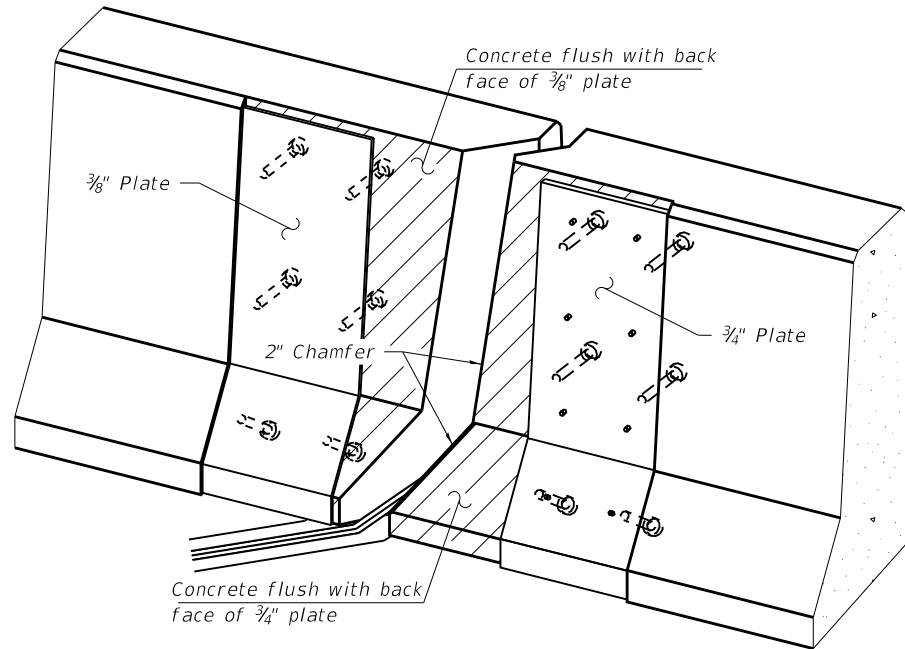
3/8" Ø threaded rods in 7/16" Ø holes at ±4'-0" cts. for holding the proper joint opening based on the temperature during the deck pour. Place to miss studs. All rods shall be burned, or sawed off flush with the plates after concrete is set.

SECTION A-A

* Granular or solid flux filled headed studs conforming to Article 1006.32 of the Std. Specs., automatically end welded.

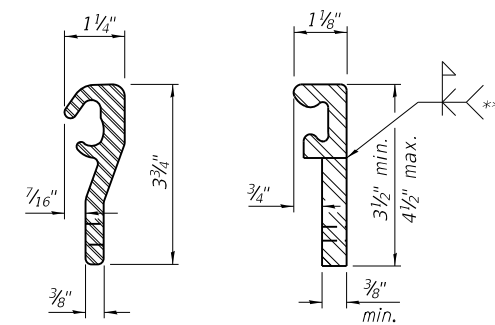


SHOWING WELDED RAIL JOINT



TRIMETRIC VIEW

(Showing embedded plates only)

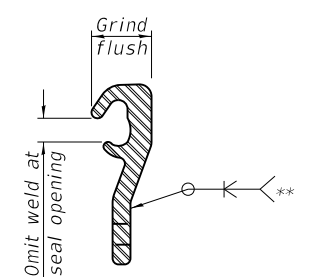


ROLLLED (EXTRUDED) RAIL

WELDED RAIL

LOCKING EDGE RAILS

** Back gouge not required if complete joint penetration is verified by mock-up.



LOCKING EDGE RAIL SPLICE

The inside of the locking edge rail groove shall be free of weld residue. Rolled rail shown, welded rail similar.

BILL OF MATERIAL

Item	Unit	Total
Preformed Joint Strip Seal	Foot	460



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STATE OF ILLINOIS
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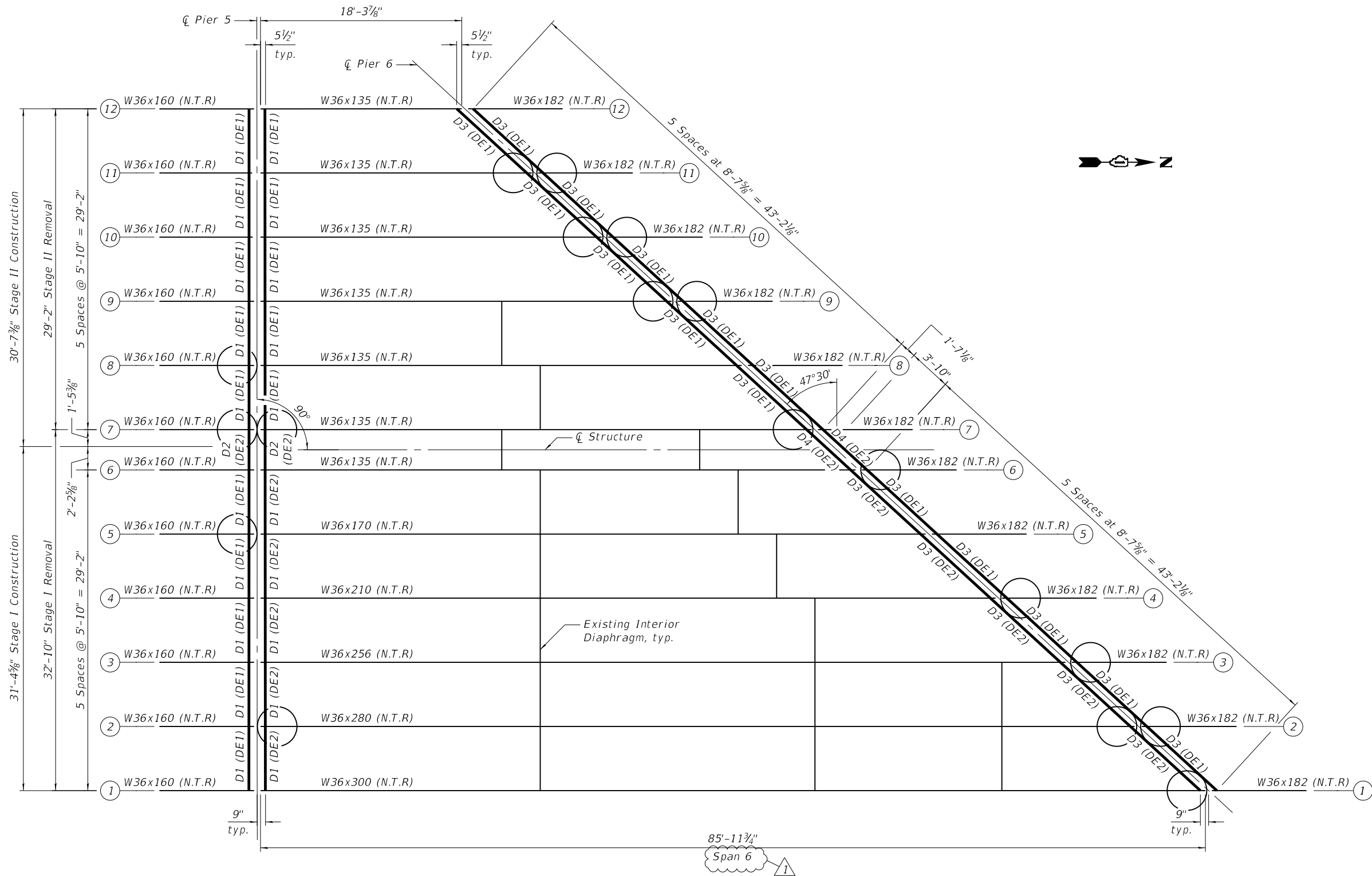
PREFORMED JOINT STRIP SEAL - SIDEWALK (1 OF 3)
STRUCTURE NO. 016-1011

SHEET 5-15 OF 5-34 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62M80				
ILLINOIS		FED. AID PROJECT		

1 REVISED SHEET 1/4/2021

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PARTIAL FRAMING PLAN

NOTES:

1. For Beam End Repairs, Diaphragm Removal and Replacement Details and Bill of Materials, see Sheet S-20 thru S-23.
2. DE1 are W16x36 existing diaphragms and DE2 are W12x40 existing diaphragms. D1, D2, D3 and D4 are proposed W12x40 diaphragms.

Proposed Diaphragm Name — D1 (DE1) — Existing Diaphragm Name

LEGEND

- Perform Beam End Plating. Paid for as Structural Steel Repair.
- Remove and Replace Existing Diaphragm. Paid for as Structural Steel Removal and Furnishing and Erecting Structural Steel.

REVISD SHEET 1/4/2021

PARTIAL FRAMING PLAN (1 OF 2)
STRUCTURE NO. 016-1011

SHEET 5-18 OF 5-34 SHEETS



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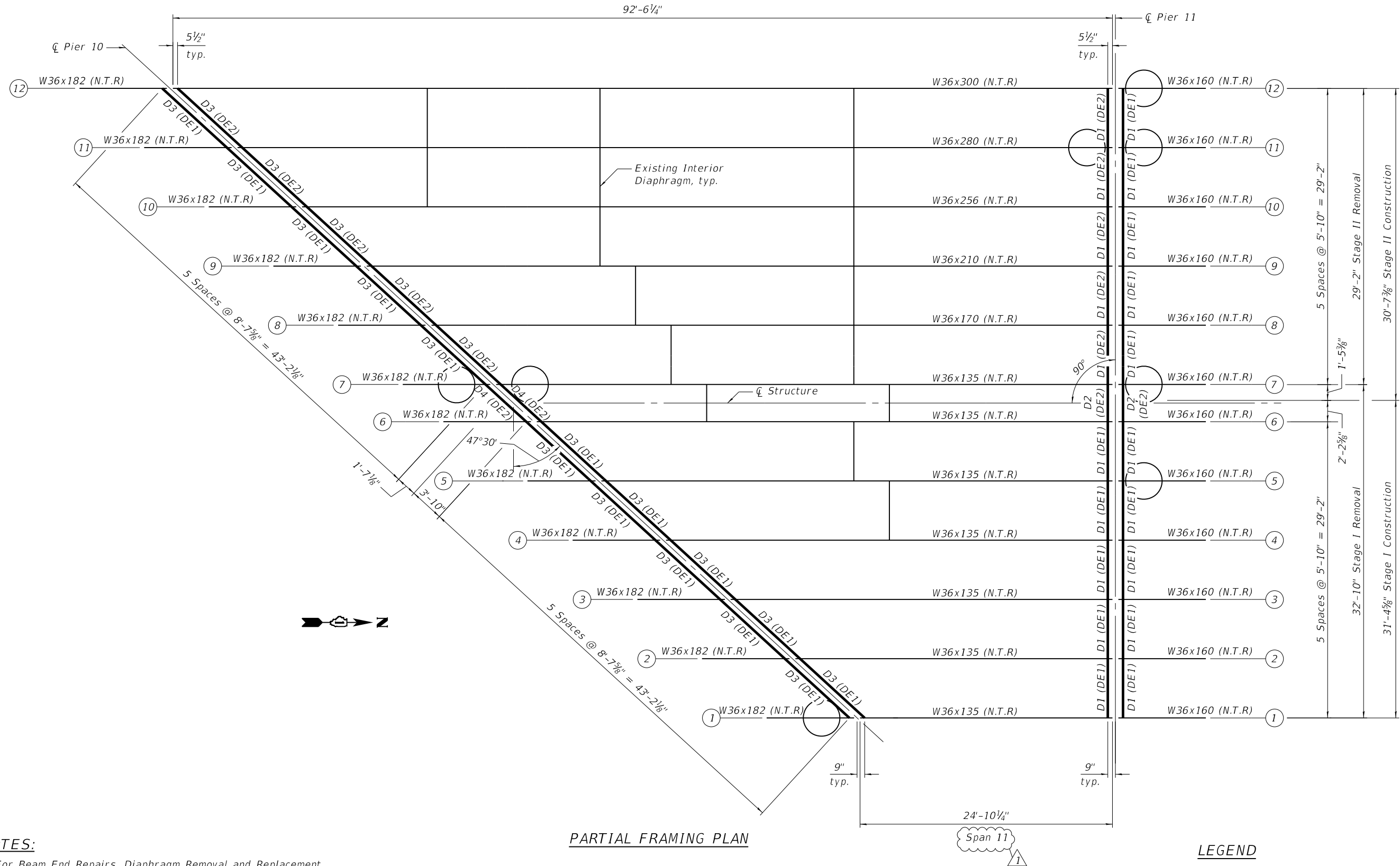
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STATE OF ILLINOIS
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F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62M80				
ILLINOIS FED. AID PROJECT				

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NOTES:

- For Beam End Repairs, Diaphragm Removal and Replacement Details and Bill of Materials, see Sheet S-20 thru S-23.
- DE1 are W16x36 existing diaphragms and DE2 are W12x40 existing diaphragms. D1, D2, D3 and D4 are proposed W12x40 diaphragms.

PARTIAL FRAMING PLAN

Proposed Diaphragm Name — D1 (DE1) — Existing Diaphragm Name

LEGEND

- Perform Beam End Plating. Paid for as Structural Steel Repair.
- Remove and Replace Existing Diaphragm. Paid for as Structural Steel Removal and Furnishing and Erecting Structural Steel.



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

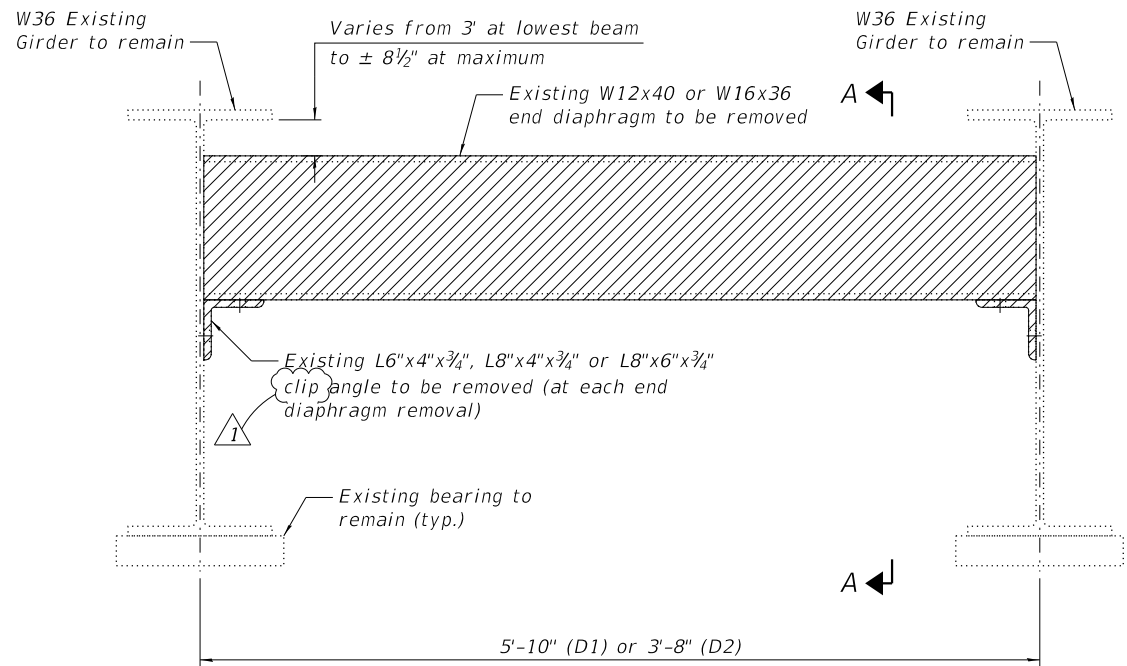
PARTIAL FRAMING PLAN (2 OF 2)
STRUCTURE NO. 016-1011

SHEET 5-19 OF 5-34 SHEETS

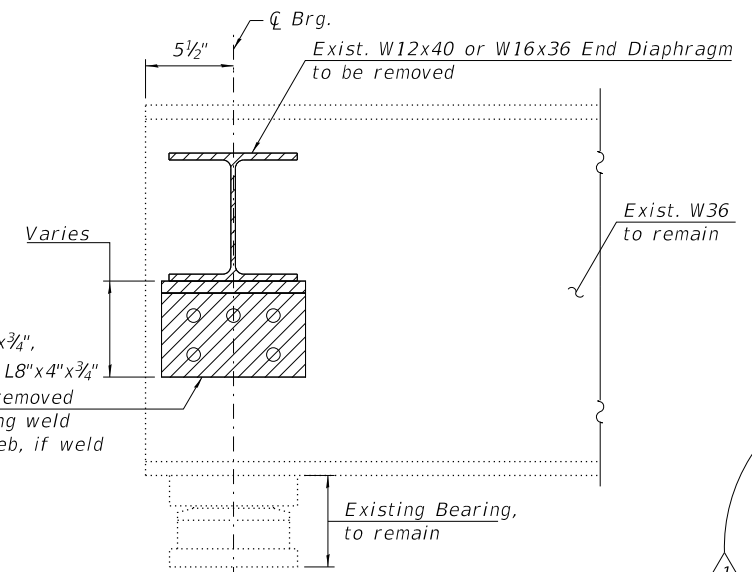
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ILLINOIS		FED. AID PROJECT		

REVISED SHEET 1/4/2021

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EXISTING END DIAPHRAGM



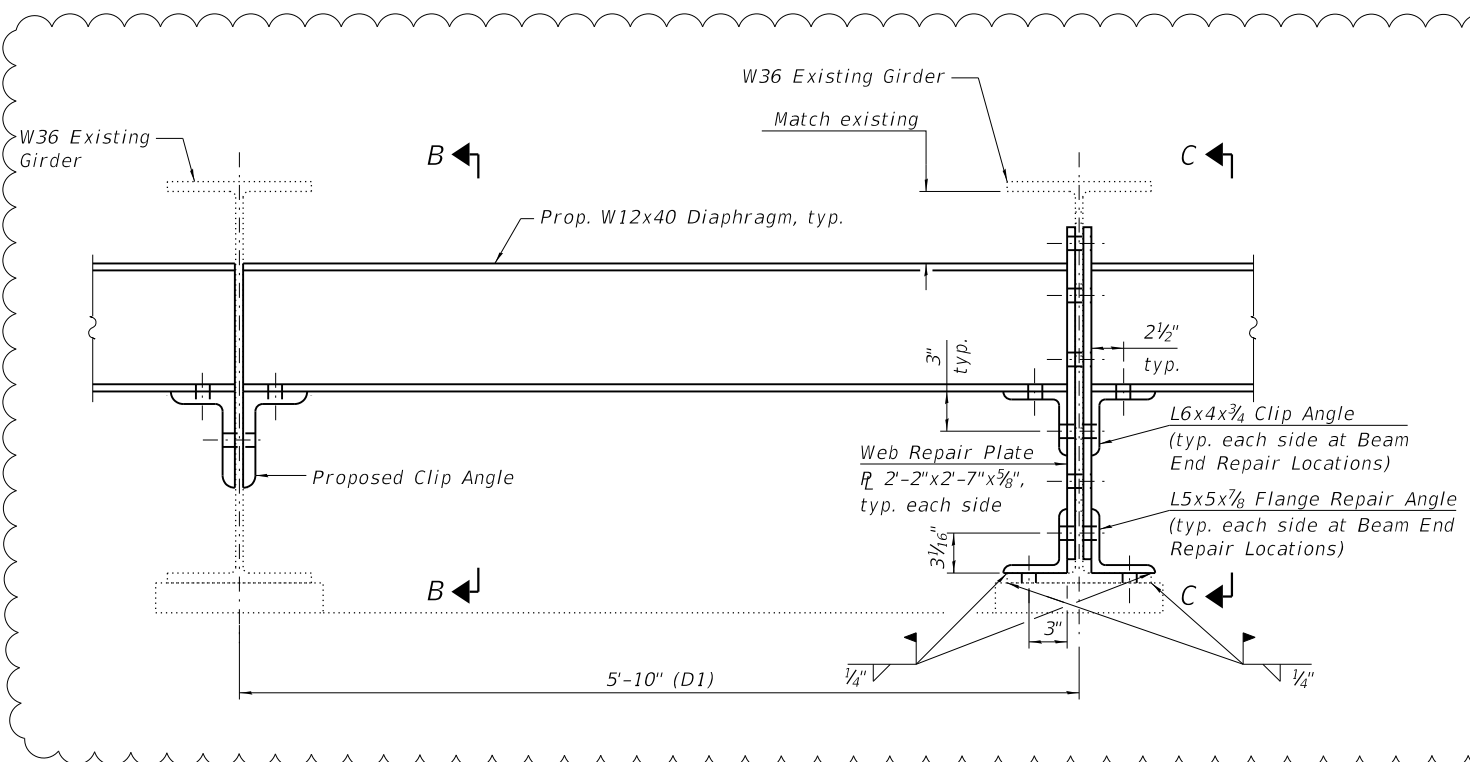
SECTION A-A

NOTES:

1. For locations of Diaphragm Removal/Replacement and Beam End Repairs, see Sheet S-18 and S-19.
2. All proposed beam end repair plates and bottom flange repair angles shall conform to the requirements of AASHTO M270 Grade 50. All proposed diaphragms and their connection angles shall conform to the requirements of AASHTO M270 Grade 36.
3. Diaphragm connection holes shall be $1\frac{3}{16}$ " \varnothing for $\frac{3}{4}$ " \varnothing bolts. Two hardened washers shall be required at all diaphragm connections and all oversized holes.
4. Existing diaphragm and clip angle removal shall be paid for as Structural Steel Removal.
5. All proposed Web Repair Plates, Flange Repair Angles, and associated bolts and fasteners shall be paid for as Structural Steel Repair. All proposed diaphragms, diaphragm splice plates, clip angles and associated bolts, plate washers and fasteners shall be paid for as Furnishing and Erecting Structural Steel.
6. See Sheet S-21 for Section B-B and C-C.

END DIAPHRAGM STAGE CONSTRUCTION SEQUENCE

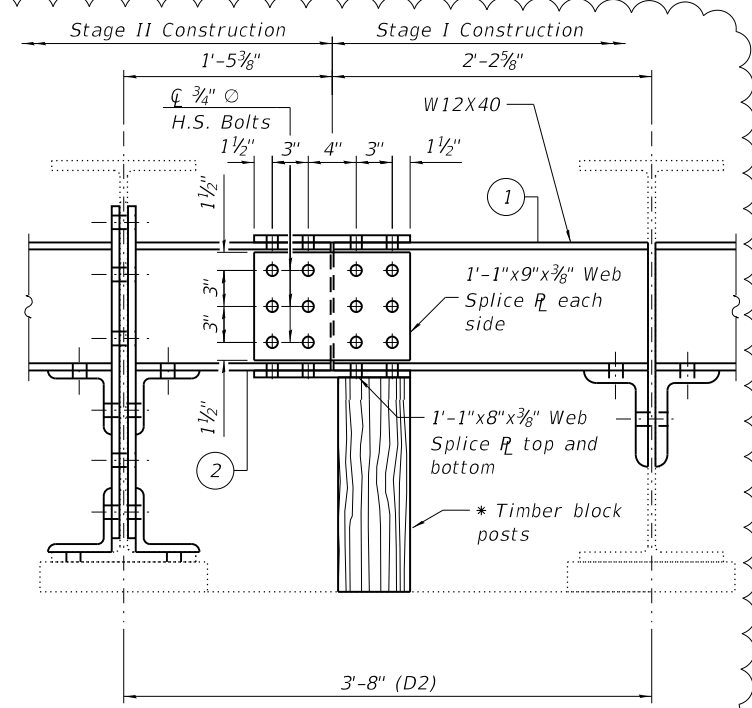
1. Order Diaphragm in two sections.
2. Attach section (1) of Diaphragm to Beam
3. Place Timber Block Posts between section (1) of diaphragm and abutment bearing section.
4. Attach section (2) of diaphragm to both Beam and section (1) of diaphragm during Stage II Construction with splice plates.
5. Remove Timber Block Posts.



SHOWING NON-BEAM END PLATING LOCATIONS

SHOWING BEAM END PLATING LOCATIONS

PROPOSED END DIAPHRAGM
(Adjacent diaphragms not shown)



PROPOSED END DIAPHRAGM AT STAGE CONSTRUCTION JOINT

(See Proposed End Diaphragm detail for Connection Plate and Angle details.)

*Cost of Timber Block Posts is included with Furnishing and Erecting Structural Steel

LEGEND:

Structural Steel Removal

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Furnishing and Erecting Structural Steel	Pound	29,400
Structural Steel Removal	Pound	25,860
Structural Steel Repair	Pound	10,650

REVISED SHEET 1/4/2021



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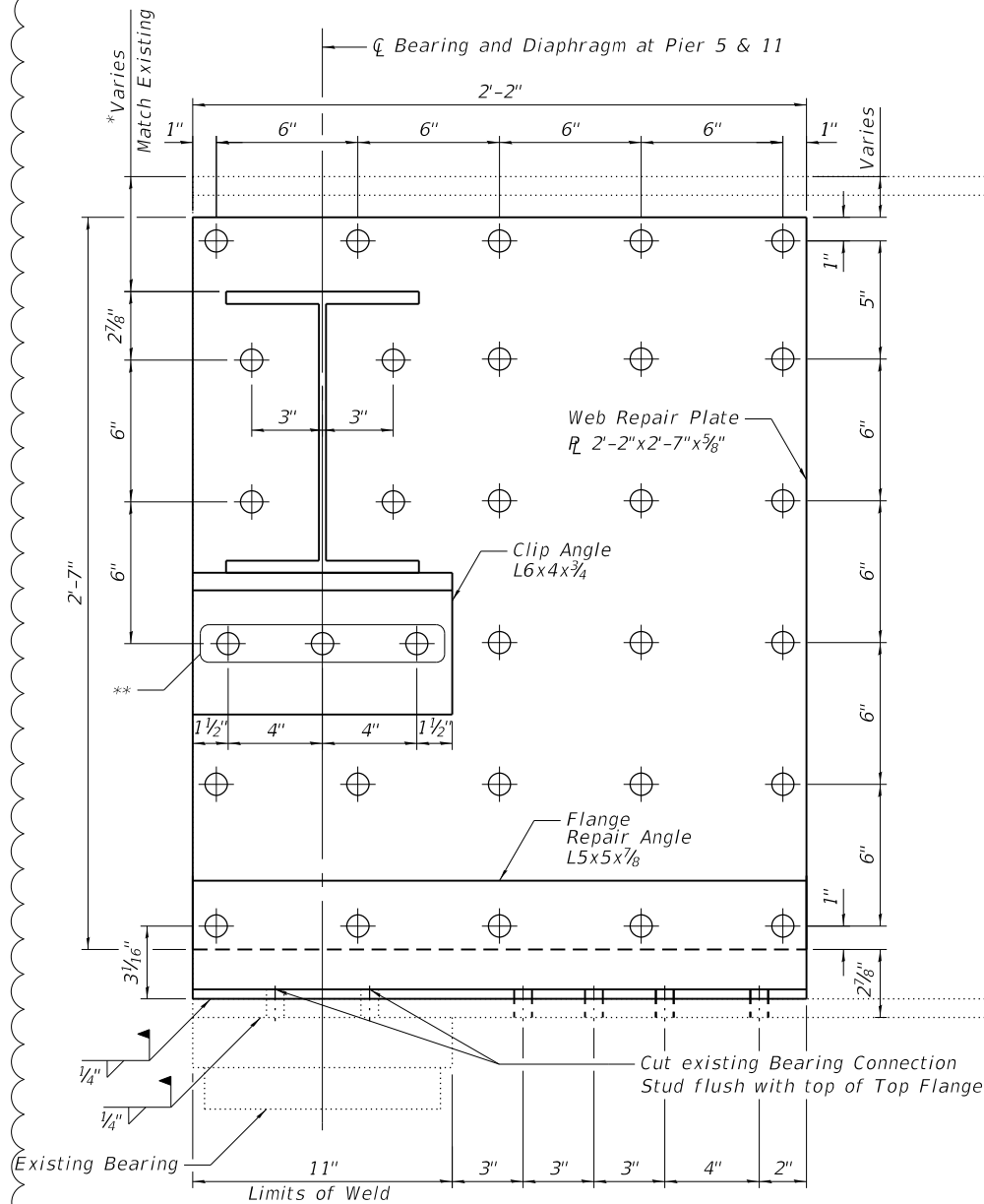
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DIAPHRAGM REPLACEMENT DETAILS, PIER 5 AND 11
STRUCTURE NO. 016-1011

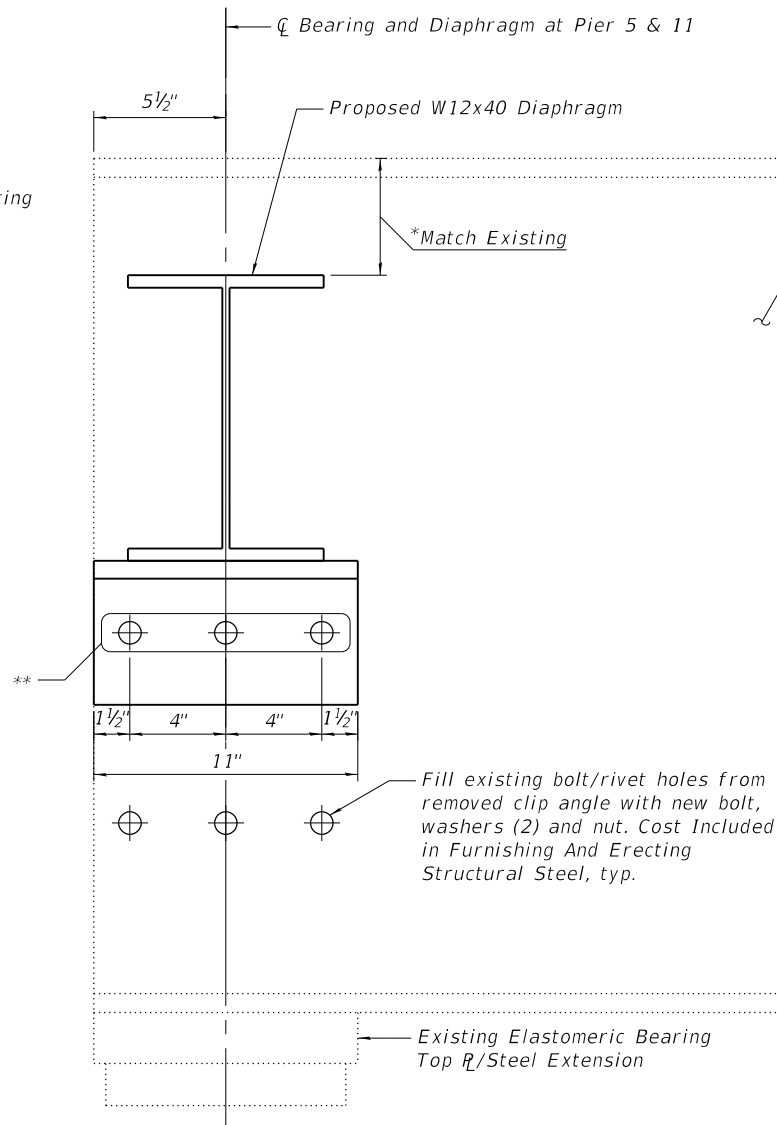
SHEET 5-20 OF 5-34 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62M80				
ILLINOIS		FED. AID PROJECT		

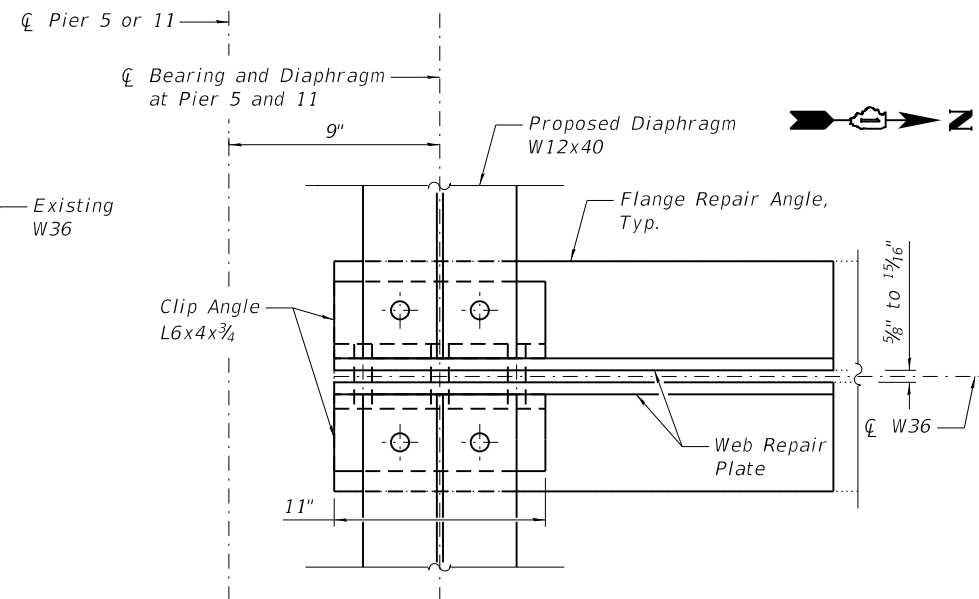
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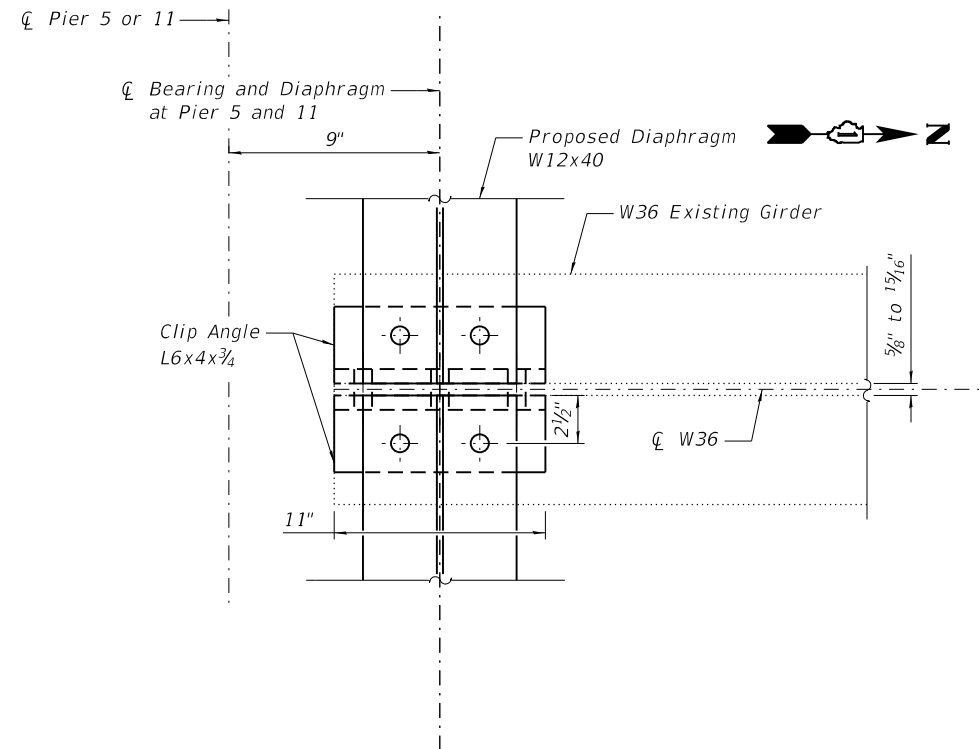
VIEW B-B



VIEW C-C



PLAN VIEW
BEAM END PLATING LOCATIONS
(Pier 5 Span 6 shown, typ. for other repair locations)



PLAN VIEW
BEAM END PLATING LOCATIONS
(Pier 5 Span 6 shown, typ. for other repair locations)

NOTES:

1. For Notes and Bill of Material see Sheet S-20.
2. For Locations of Diaphragm Removal/Replacement and Beam End Repairs, see Sheet S-18 and S-19.
3. No welds are to be installed on the back/end of the beam. All welds are included in the cost of Structural Steel Repairs.
4. Contractor to field verify hole locations before ordering material. Contractor can elect to field drill holes in repair plates.
5. Cost of cutting existing Bearing Connection Studs included in the cost of Structural Steel Repair.
6. Existing Side Retainers can be removed and reinstalled as approved by the Engineer in the field. Removal and reinstallation is included in the cost of Structural Steel Repairs.

* Distance from Top of Girder to Top of Proposed Diaphragm to be Field Verified prior to Ordering Material.

** Contractor to field verify diaphragm location and bolt hole locations before ordering material.



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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

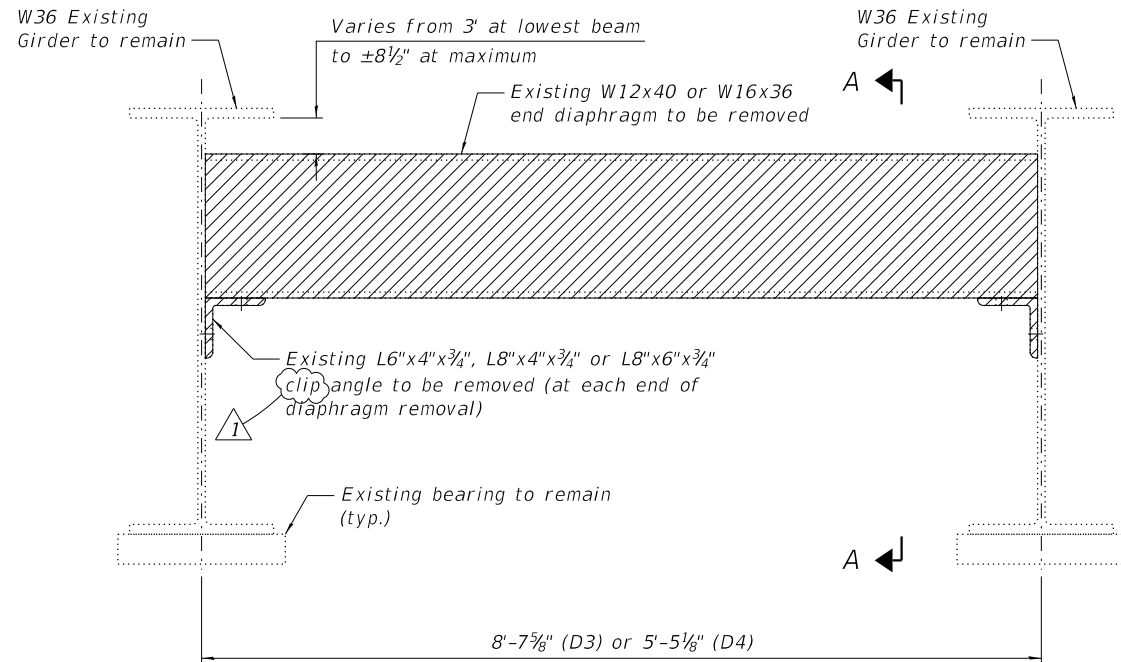
BEAM REPAIR DETAILS, PIER 5 AND 11
STRUCTURE NO. 016-1011

SHEET S-21 OF S-34 SHEETS

ENTIRE SHEET REVISED 1/4/2021

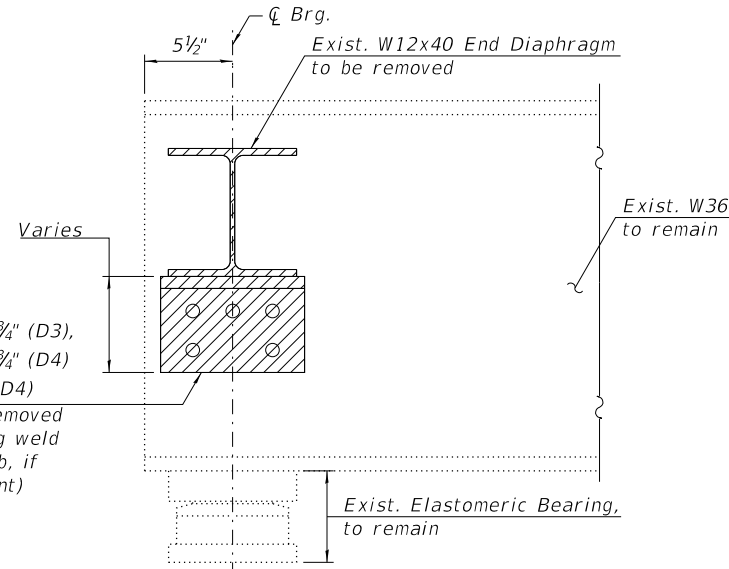
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EXISTING END DIAPHRAGM

Exist. L6"x4"x $\frac{3}{4}$ " (D3),
Exist. L8"x6"x $\frac{3}{4}$ " (D4)
or L8"x4"x $\frac{3}{4}$ " (D4)
angle to be removed
(Grind existing weld
flush with web, if
weld is present)



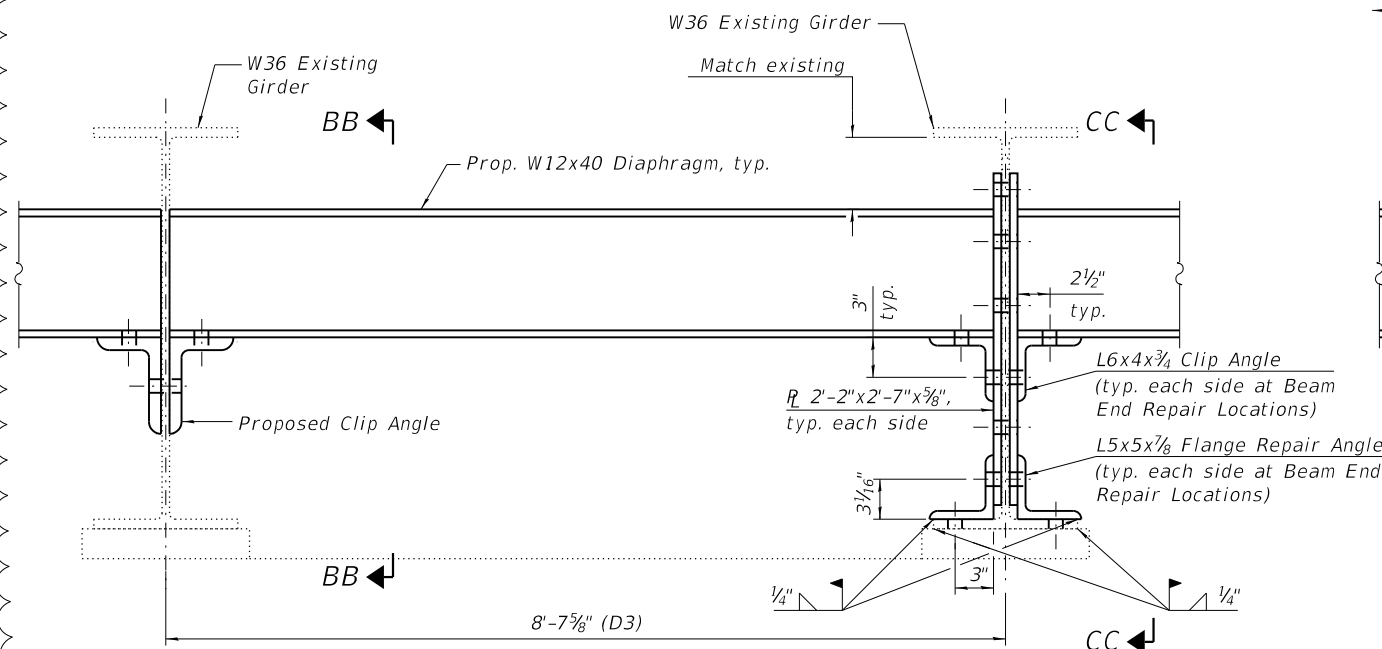
SECTION A-A

NOTES:

1. For locations of Diaphragm Removal/Replacement and Beam End Repairs, see Sheet S-18 and S-19.
2. All proposed beam end repair plates and bottom flange repair angles shall conform to the requirements of AASHTO M270 Grade 50. All proposed diaphragms and their connection angles shall conform to the requirements of AASHTO M270 Grade 36.
3. Diaphragm connection holes shall be $\frac{13}{16}$ " \varnothing for $\frac{3}{4}$ " \varnothing bolts. Two hardened washers shall be required at all diaphragm connections and all oversized holes.
4. Existing diaphragm and clip angle removal shall be paid for as Structural Steel Removal.
5. All proposed Web Repair Plates, Flange Repair Angles, and associated bolts and fasteners shall be paid for as Structural Steel Repair. All proposed diaphragms, diaphragm splice plates, clip angles and associated bolts, plate washers and fasteners shall be paid for as Furnishing and Erecting Structural Steel.
6. See Sheet S-23 for View BB-BB and CC-CC.

END DIAPHRAGM STAGE
CONSTRUCTION SEQUENCE

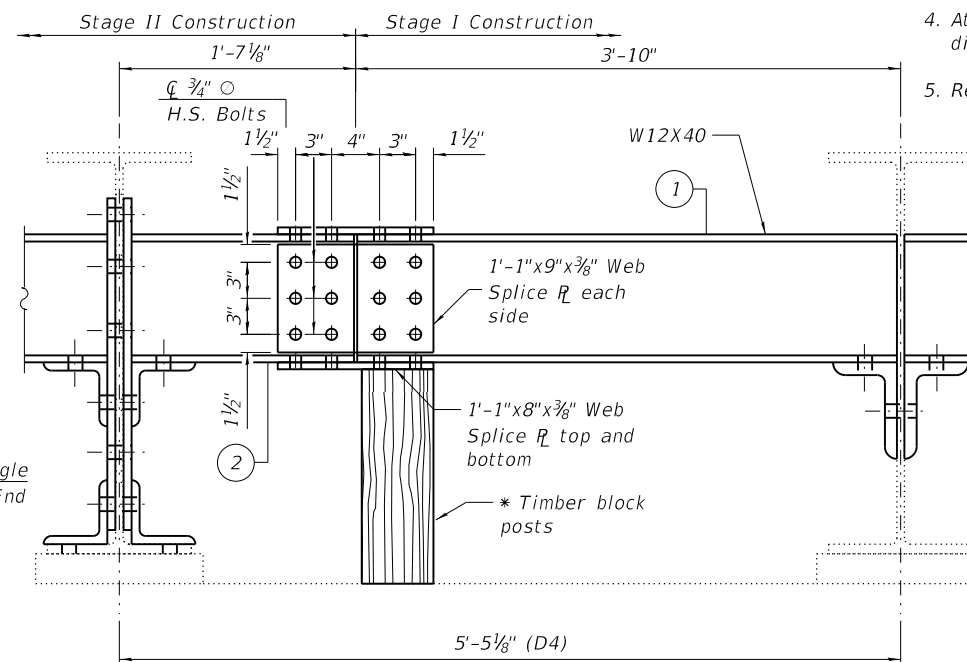
1. Order Diaphragm in two sections.
2. Attach section (1) of Diaphragm to Beam
3. Place Timber Block Posts between section (1) of diaphragm and abutment bearing section.
4. Attach section (2) of diaphragm to both Beam and section (1) of diaphragm during Stage II Construction with splice plates.
5. Remove Timber Block Posts.



SHOWING NON-BEAM END
PLATING LOCATIONS

PROPOSED END DIAPHRAGM
(Adjacent diaphragms not shown)

SHOWING BEAM END
PLATING LOCATIONS



PROPOSED END DIAPHRAGM AT
STAGE CONSTRUCTION JOINT

(Stage II Beam Plating Shown. Stage I Beam Plating Similar.
See Proposed End Diaphragm detail for Connection Plate and Angle details.)

*Cost of Timber Block Posts is included with
Furnishing and Erecting Structural Steel

LEGEND:

Structural Steel Removal



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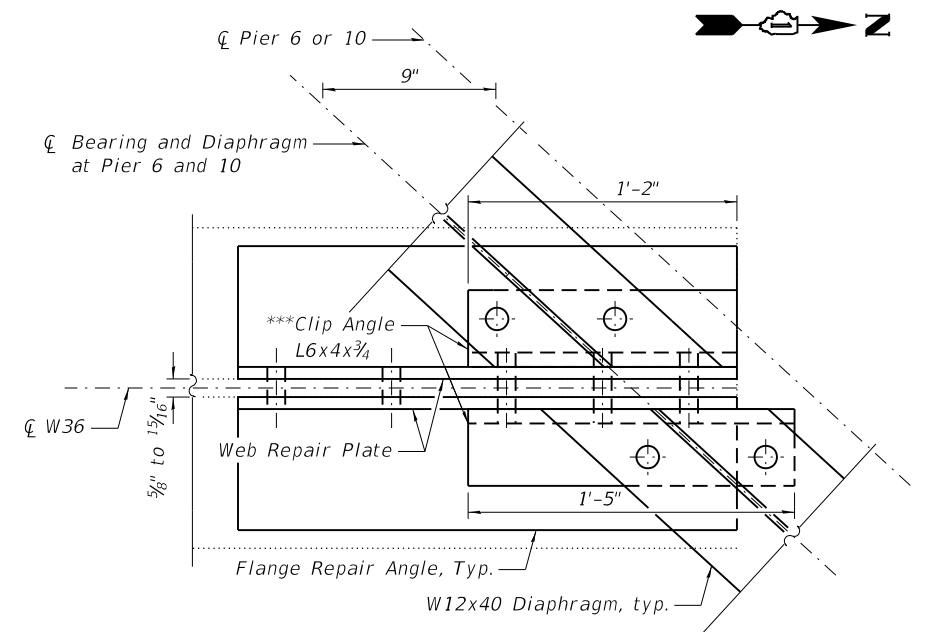
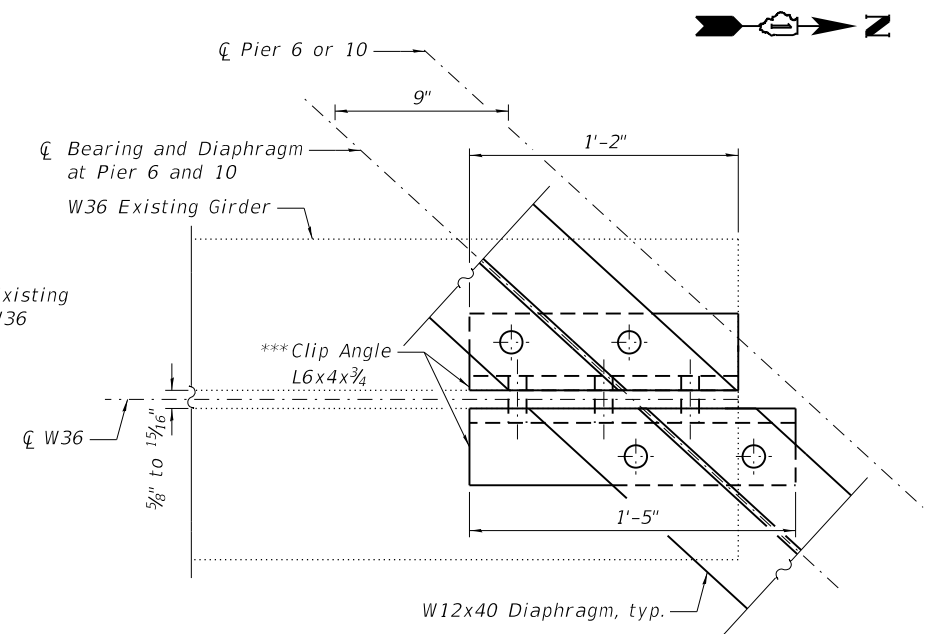
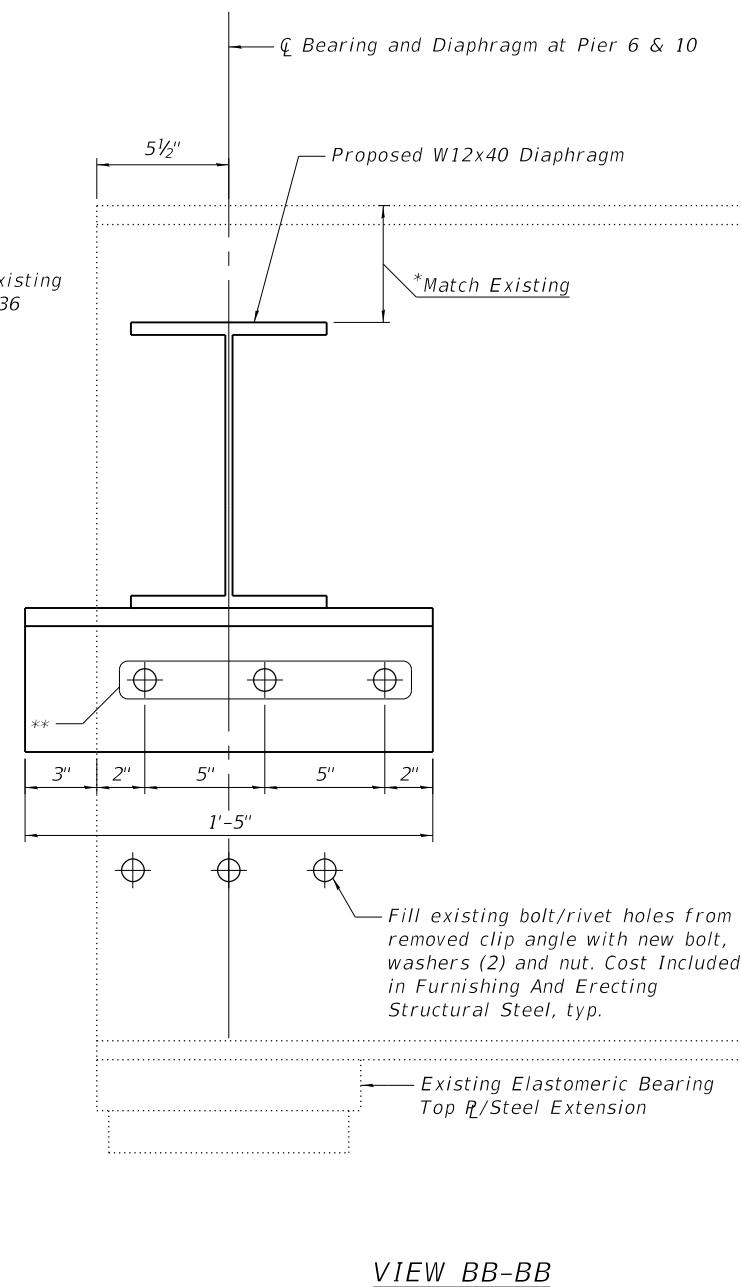
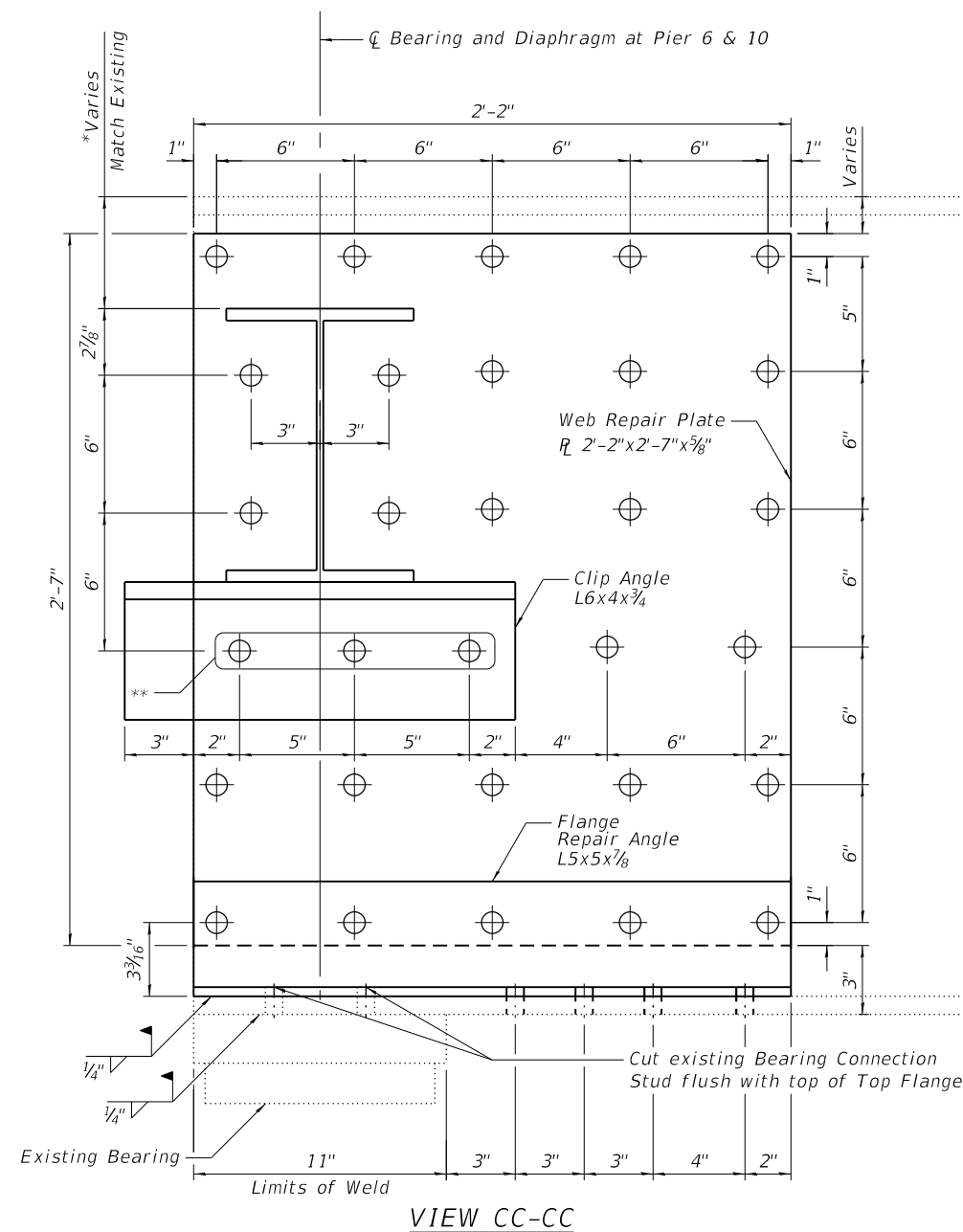
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

DIAPHRAGM REPLACEMENT DETAILS, PIER 6 AND 10
STRUCTURE NO. 016-1011

SHEET S-22 OF S-34 SHEETS

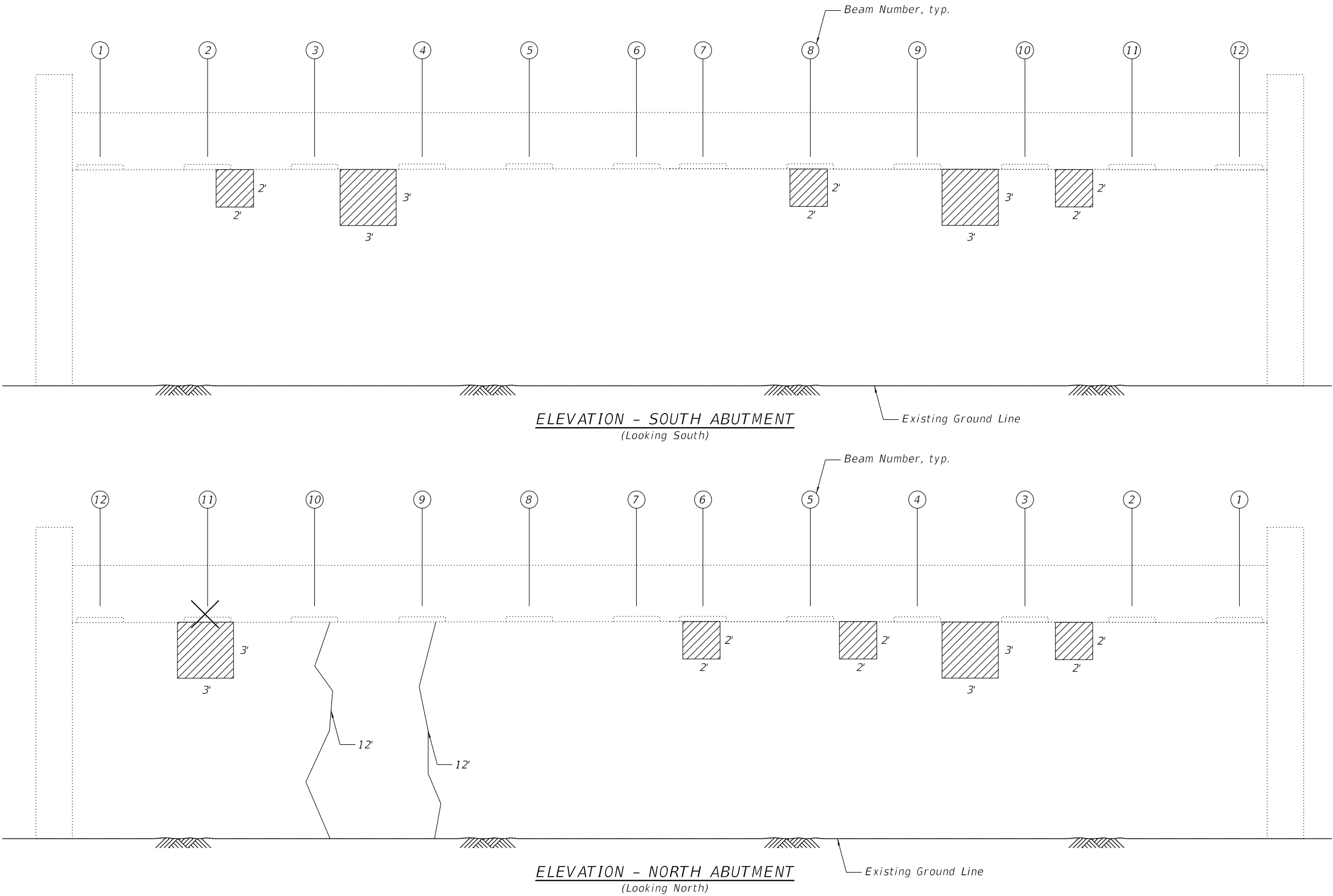
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CONTRACT NO. 62M80				
ILLINOIS		FED. AID PROJECT		

REVISED SHEET 1/4/2021



F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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NOTES:

- Quantities and limits shown are estimated for bidding purposes only. The actual area to be repaired, and the type(s) of repairs to be used, will be determined by the Engineer in the field at the time of construction.
- Concrete repairs directly under Girder 11 shall not start until the Temporary Shoring is installed.
- Removal and reinstallation of drainage pipes, electrical cables conduits, or other items attached to the existing structure are included in the cost of Structural Repair of Concrete.

LEGEND

- Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)
- Epoxy Crack Injection
- Temporary Shoring and Cribbing

GIRDER REACTION TABLE

North Abutment	Load (kips)
DL	25.0
LL	32.6
IM	8.3
Total	65.9

Reactions are per Girder

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth Equal To or Less Than 5 Inches)	Sq Ft	60
Temporary Shoring and Cribbing	Each	1
Epoxy Crack Injection	Foot	24



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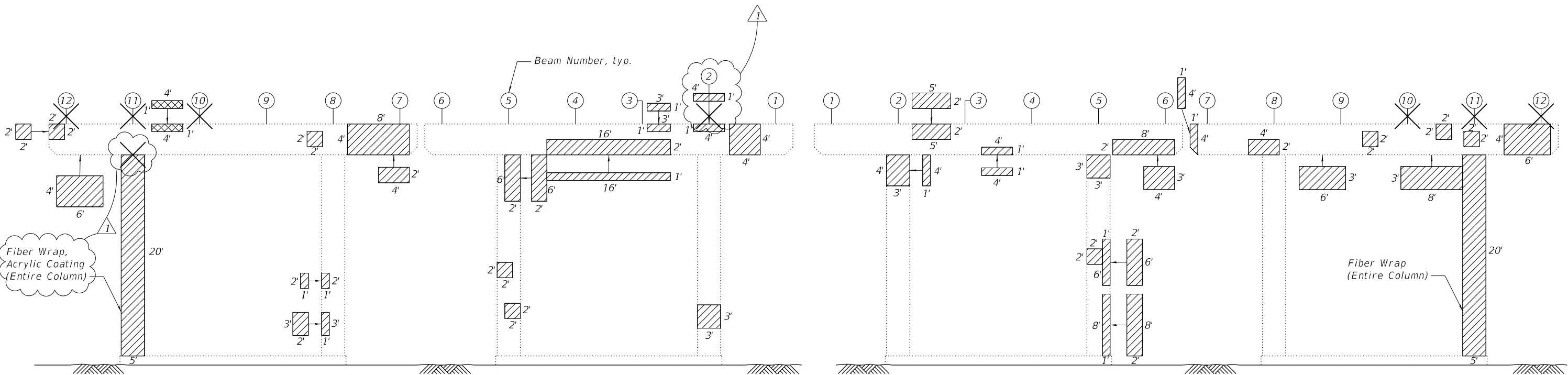
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

NORTH AND SOUTH ABUTMENT REPAIR DETAILS
STRUCTURE NO. 016-1011

SHEET 5-26 OF 5-34 SHEETS

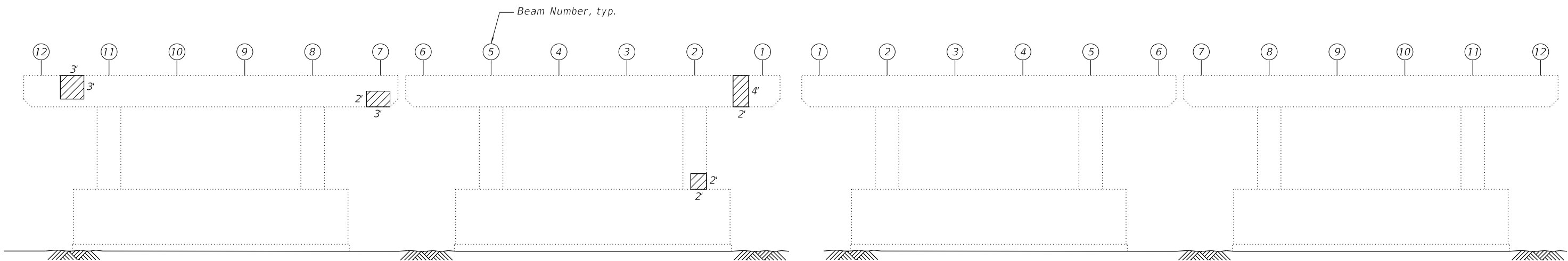
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PIER 6- SOUTH FACE
(Looking North)

PIER 6- NORTH FACE
(Looking South)



PIER 7 - SOUTH FACE
(Looking North)

PIER 7 - NORTH FACE
(Looking South)

NOTES:

- Quantities and limits shown are estimated for bidding purposes only. The actual area to be repaired, and the type(s) of repairs to be used, will be determined by the Engineer in the field at the time of construction.
- Concrete Repairs to the West Column and under Griders 10, 11 and 12, Each Face of Pier 6 and South Face of Girder 2 Pier 6 shall not start until the Temporary Shoring is installed. Each Location will be paid for separately.
- Shoring of the Pier 6 Pier Cap shall be installed before exterior column repairs starts. Shoring of the entire pier cap shall be paid for as Temporary Shoring and Cribbing with a quantity of 1.
- Removal and installation of drainage pipes, electrical cables conduits, or other items attached to the existing structure are included in the cost of Structural Repair of Concrete.
- Overlap each horizontal layer of the Fiber Wrap a minimum of 6" horizontally and vertically. Lap locations to be on alternate sides of the column.

LEGEND

- Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)
- Structural Repair Of Concrete (Depth Greater Than 5 Inches)
- Temporary Shoring and Cribbing

Bottom of Pier Cap

Existing Column

2'-0" Typ.
1'-0" Typ.

FIBER WRAP DETAIL

GIRDER REACTION TABLES

Pier 6 S. Face	Load (kips)
DL	55.8
LL	37.8
IM	8.8
Total	102.4

Pier 6 N. Face	Load (kips)
DL	29.5
LL	33.4
IM	8.1
Total	71.0

Reactions are per Girder

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Acrylic Coating	Sq Yd	42
Fiber Wrap	Sq Ft	377
Structural Repair of Concrete (Depth Equal To or Less Than 5 Inches)	Sq Ft	656
Structural Repair of Concrete (Depth Greater Than 5 Inches)	Sq Ft	8
Temporary Shoring and Cribbing	Each	8

REVISED SHEET 1/4/2021



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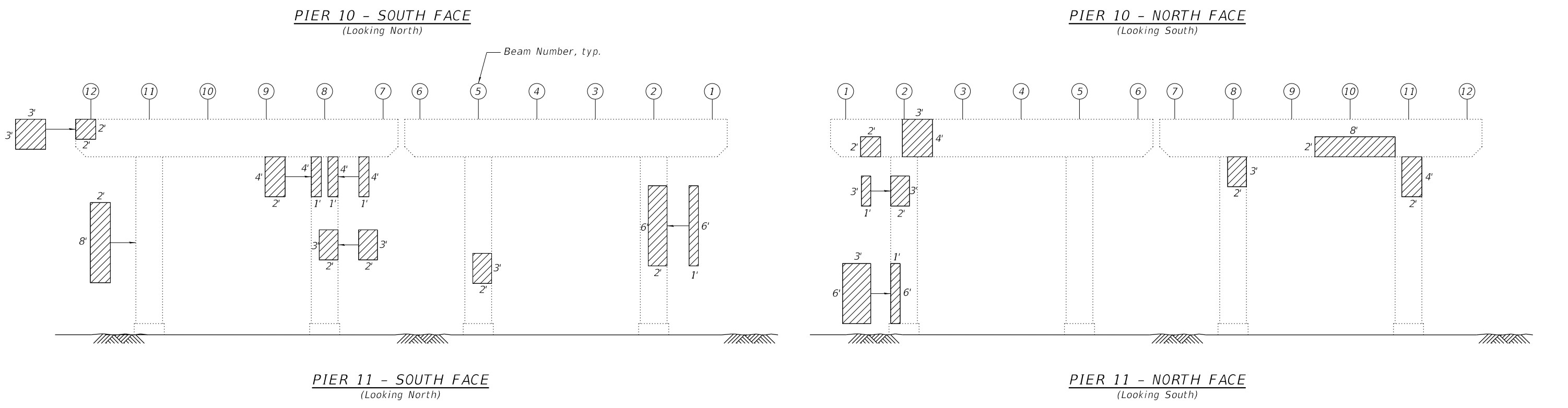
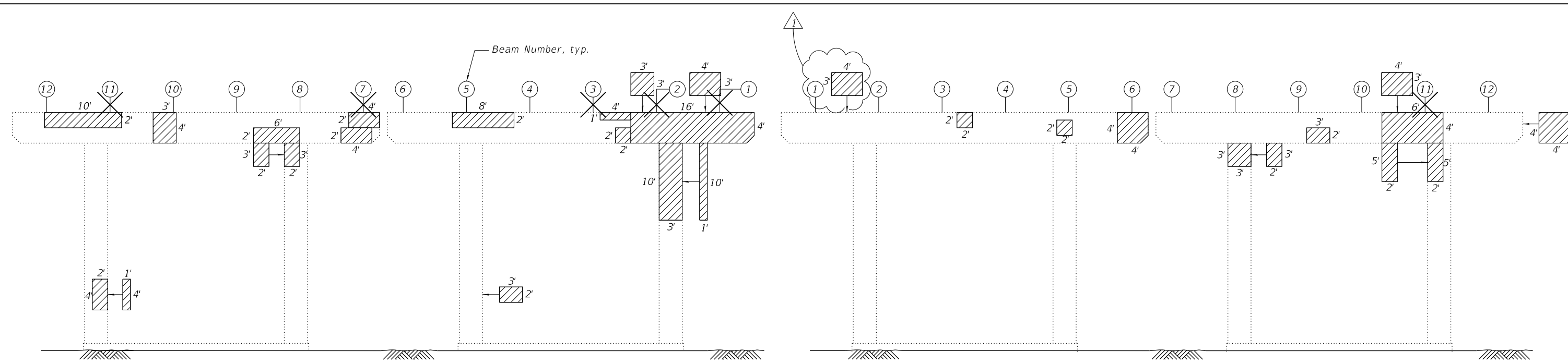
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER 6 AND 7 REPAIR DETAILS
STRUCTURE NO. 016-1011

SHEET 5-28 OF 5-34 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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NOTES:

- Quantities and limits shown are estimated for bidding purposes only. The actual area to be repaired, and the type(s) of repairs to be used, will be determined by the Engineer in the field at the time of construction.
- Concrete repairs under Girder 1, 2, 3, 7 and 11 on Pier 10 South Face and Beam 11 Pier 10 North face shall not start until the Temporary Shoring is installed.
- Removal and reinstallation of drainage pipes, electrical cables conduits, or other items attached to the existing structure are included in the cost of Structural Repair of Concrete.

LEGEND

- Structural Repair of Concrete (Depth Equal to or Less Than 5 Inches)
- Temporary Shoring and Cribbing

GIRDER REACTION TABLES

Pier 10 S. Face	Load (kips)	Pier 10 N. Face	Load (kips)
DL	29.5	DL	55.8
LL	33.4	LL	37.8
IM	8.1	IM	8.8
Total	71.0	Total	102.4

Reactions are per Girder

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth Equal To or Less Than 5 Inches)	Sq Ft	532
Temporary Shoring and Cribbing	Each	6



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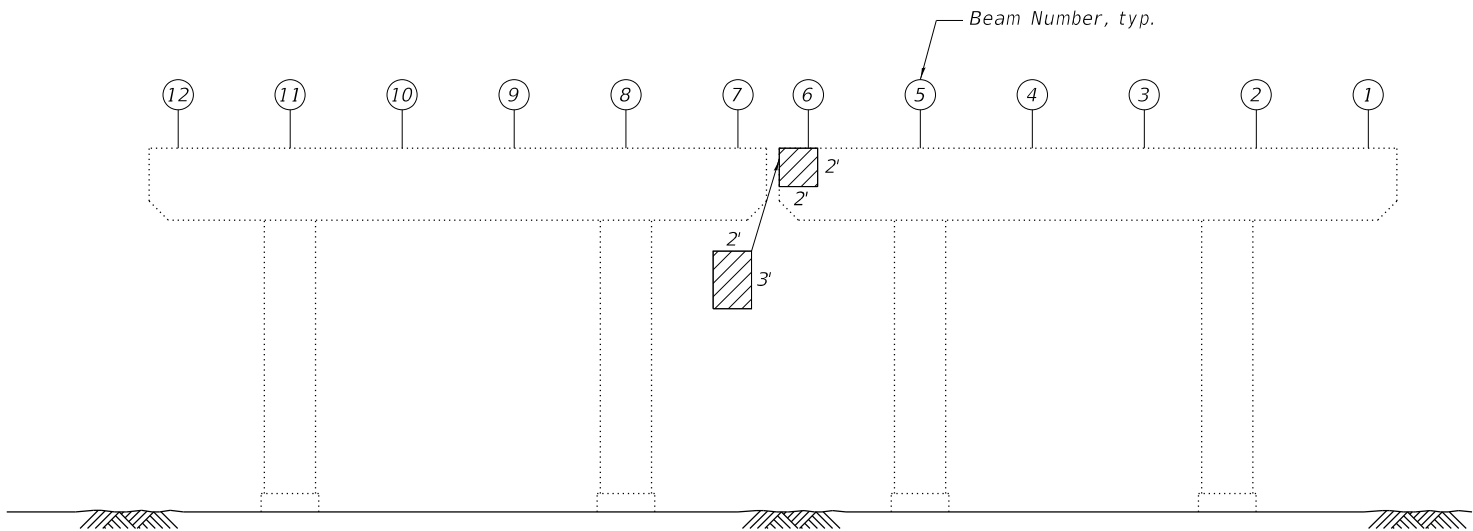
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

PIER 10 AND 11 REPAIR DETAILS
STRUCTURE NO. 016-1011

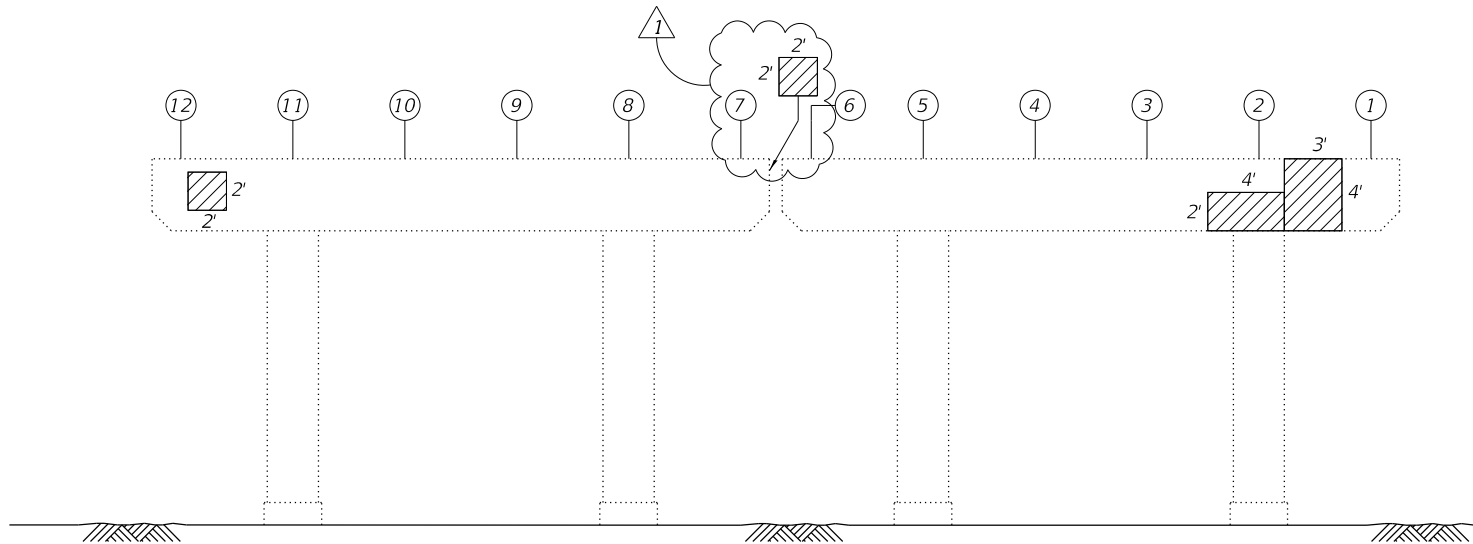
SHEET 5-30 OF 5-34 SHEETS

1 REVISED SHEET 1/4/2021		F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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		ILLINOIS	FED. AID PROJECT			

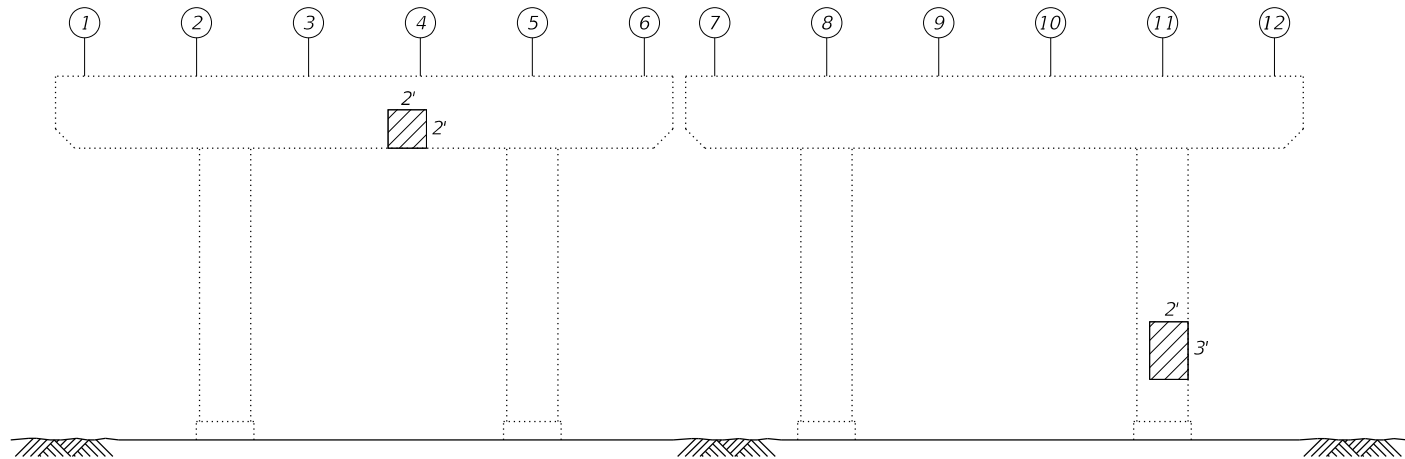
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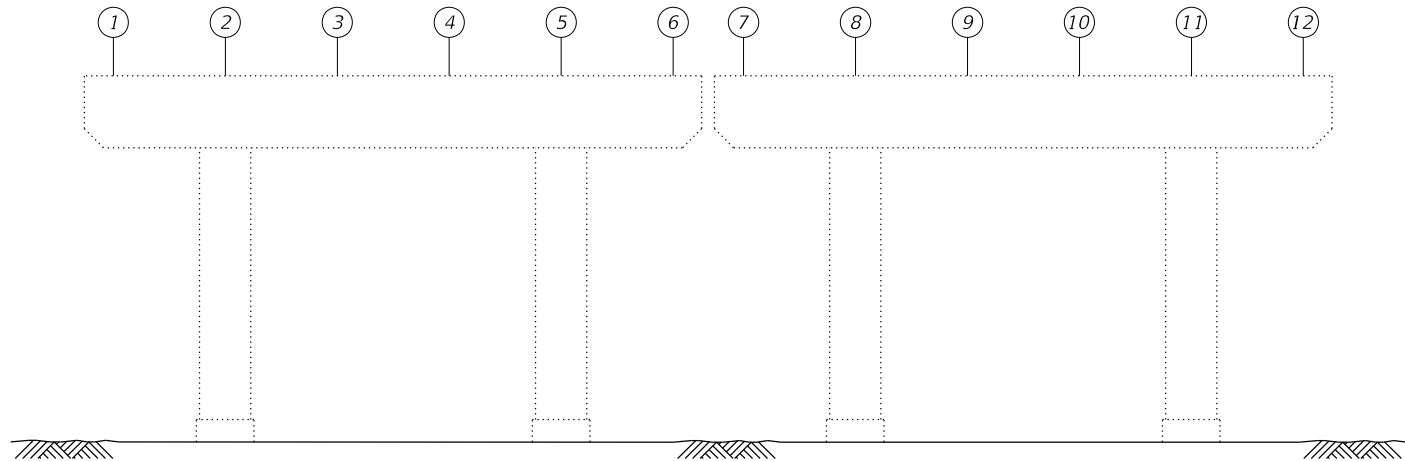
PIER 12 - SOUTH FACE
(Looking North)



PIER 13 - SOUTH FACE
(Looking North)



PIER 12 - NORTH FACE
(Looking South)




PIER 13 - NORTH FACE
(Looking South)

NOTE:

- Quantities and limits shown are estimated for bidding purposes only. The actual area to be repaired, and the type(s) of repairs to be used, will be determined by the Engineer in the field at the time of construction.
- Removal and installation of drainage pipes, electrical cables conduits, or other items attached to the existing structure are included in the cost of Structural Repair of Concrete.

LEGEND

 Structural Repair of Concrete
(Depth Equal to or Less Than 5 Inches)

BILL OF MATERIAL

ITEM	UNIT	QUANTITY
Structural Repair of Concrete (Depth Equal To or Less Than 5 Inches)	Sq Ft	48



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**STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION**

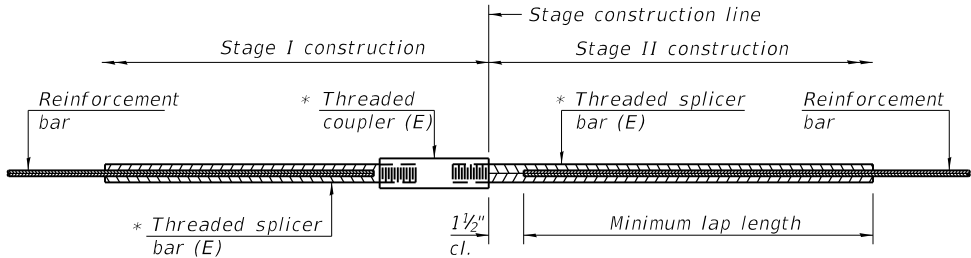
**PIER 12 AND 13 REPAIR DETAILS
STRUCTURE NO. 016-1011**

SHEET 5-31 OF 5-34 SHEETS

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ILLINOIS			FED. AID PROJECT		

 REVISED SHEET 1/4/2021

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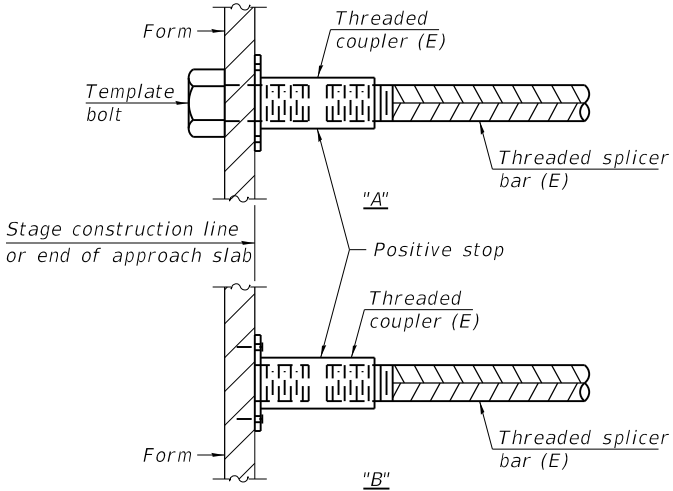


STANDARD BAR SPLICER ASSEMBLY

Threaded splicer bar length = min. lap length + 1 1/2" + thread length

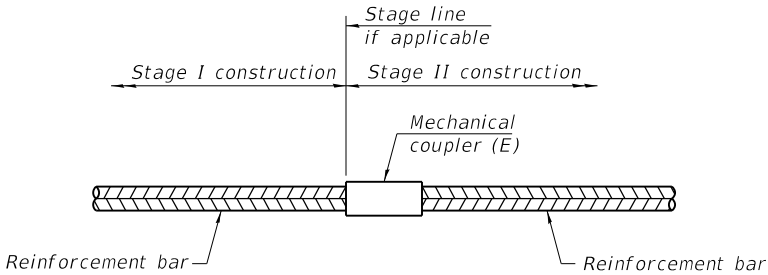
* Epoxy not required on Bar Splicer Assembly components used in conjunction with black bars.

Location	Bar size	No. assemblies required	Minimum lap length
S. Abutment	#5	9	3'-6"
S. Abutment	#6	4	4'-0"
Pier 5	#5	18	3'-6"
Pier 6	#5	18	3'-6"
Pier 10	#5	18	3'-6"
Pier 11	#5	18	3'-6"
N. Abutment	#5	9	3'-6"
N. Abutment	#6	4	4'-0"



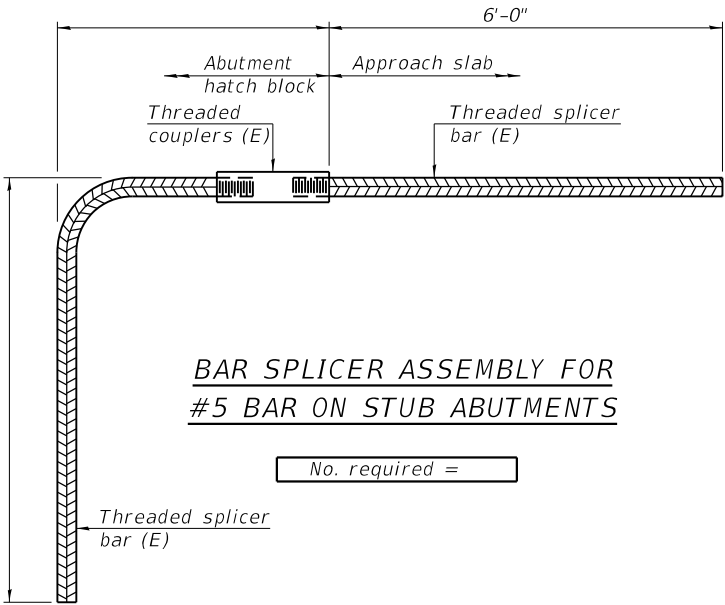
INSTALLATION AND SETTING METHODS

"A" : Set bar splicer assembly by means of a template bolt.
"B" : Set bar splicer assembly by nailing to wood forms or cementing to steel forms.
(E) : Indicates epoxy coating.



STANDARD MECHANICAL SPLICER

Location	Bar size	No. assemblies required



BAR SPLICER ASSEMBLY FOR #5 BAR ON STUB ABUTMENTS

No. required =

NOTES

Splicer bars shall be deformed with threaded ends and have a minimum 60 ksi yield strength.
All reinforcement shall be lapped and tied to the splicer bars.
Bar splicer assemblies shall be epoxy coated according to the requirements for reinforcement bars. See Section 508 of the Standard Specifications.
See approved list of bar splicer assemblies and mechanical splicers for alternatives.

BSD-1

2-17-2017



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REVISED	-	12/22/2020	J.E.S.
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STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION


BAR SPLICER ASSEMBLY AND MECHANICAL SPLICER DETAILS
STRUCTURE NO. 016-1011

SHEET 5-33 OF 5-34 SHEETS

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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CONTRACT NO. 62M80				
ILLINOIS		FED. AID PROJECT		

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	PEN TABLE = 0162M80-PreFinal.tbl	DRAWN -	REvised -					2714	2020-203-BR	COOK	79	71
	PLOT SCALE = 2.0000 ' / in.	CHECKED -	REvised -			CONTRACT NO. 62M80						
	PLOT DATE = 12/16/2020 1:46:41 PM	DATE - 05-08-2015	REvised -			SCALE:	SHEET OF SHEETS	STA.	TO STA.	ILLINOIS FED. AID PROJECT		

 12/18/2020 ADDENDUM NO. 1

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USER: jhave
MODEL: Default
FILE NAME: PROJ_209_00a_IDOT_PTB_190_14_Phase II Var-Vari-Design\Work_Order_14\ICADD\ICADD_Sheets\62M80-rt-det-DJ-TC21.dgn



PLOT DRIVER =	pdfNOLAYERSbw.pltcfgr
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PLOT DATE =	12/16/2020 1:47:41 PM

DESIGNED -	R. BORO
DRAWN -	
CHECKED -	
DATE -	09-14-09

12/18/2020 ADDENDUM NO. 1

REVISED -	12/18/2020 JWD
REVISED -	
REVISED -	
REVISED -	

ENTIRE SHEET REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

(SHEET INTENTIONALLY LEFT BLANK)

SCALE: SHEET OF SHEETS STA. TO STA.

F.A.U. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
2714	2020-203-BR	COOK	79	77
CONTRACT NO. 62M80				
		ILLINOIS	FED. AID PROJECT	