

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
307	2020-096-RS	COOK	21	1
		ILLINOIS	CONTRACT NO. 62L74	

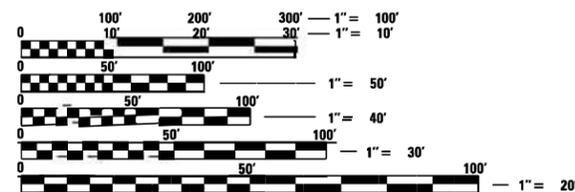
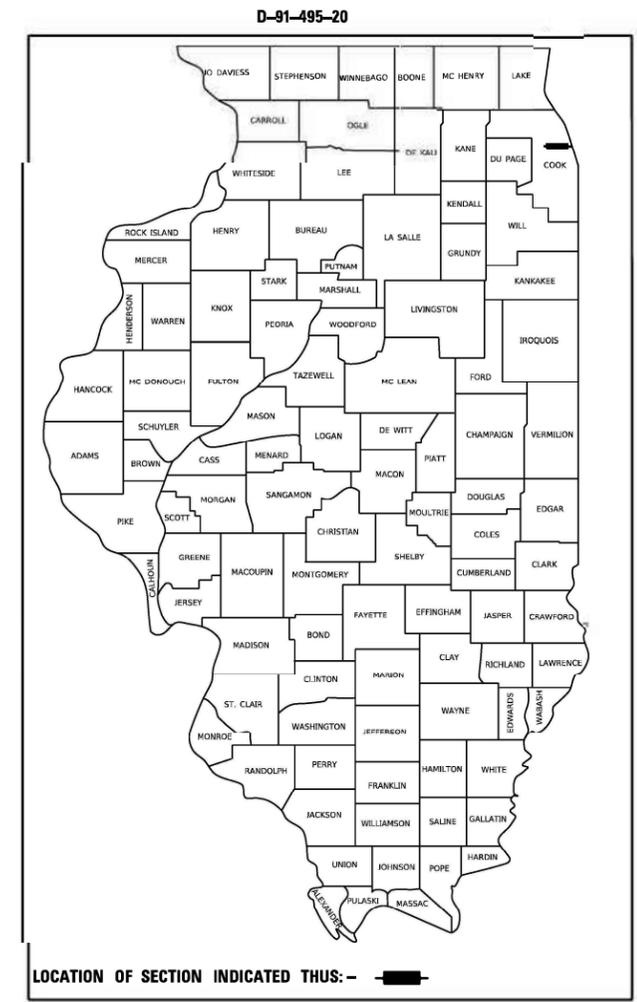
FOR INDEX OF SHEETS, SEE SHEET NO. 2

PROJECT LOCATED IN  
THE CITY OF CHICAGO

**PROPOSED  
HIGHWAY PLANS**  
F.A.P. ROUTE 307: IL 64 (NORTH AVENUE)  
SHEFFIELD AVE. TO HALSTED ST.  
SECTION 2020-096-RS  
SMART OVERLAY & ADA IMPROVEMENTS

TRAFFIC DATA  
ADT 2019: 27,100 VPD  
SPEED LIMIT: 30 MPH

COOK COUNTY  
C-91-293-20

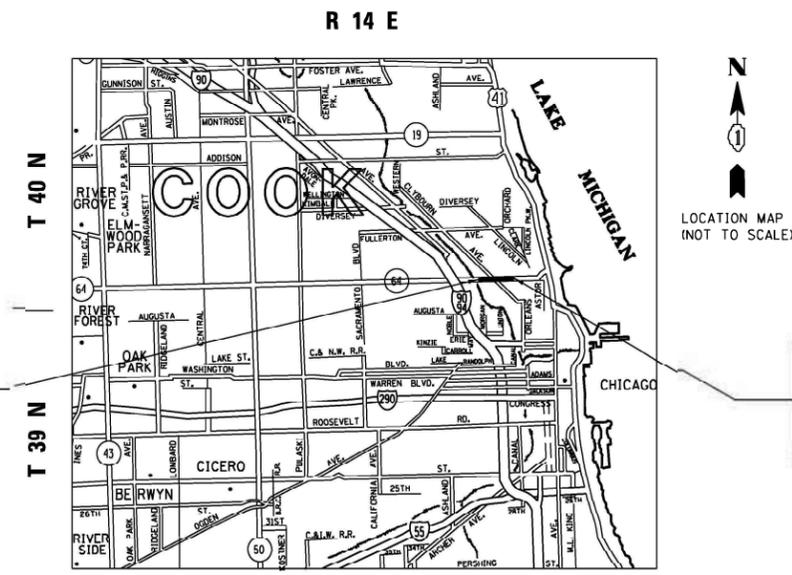


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

C.U.A.N.  
CHICAGO UTILITY ALERT NETWORK  
(312)-744-7000

PROJECT ENGINEER: DAN WILGREEN (847) 705-4240  
PROJECT MANAGER: FAWAD AQUEEL (847) 705-4247

CONTRACT NO. 62L74



PROJECT BEGINS:  
STA. 11 + 76

PROJECT ENDS:  
STA. 26 + 20

GROSS LENGTH = 1,444 FT. = 0.28 MILE  
NET LENGTH = 1,444 FT. = 0.28 MILE

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

SUBMITTED October 6, 2020  
*Anthony J. D'Amico*  
REGIONAL ENGINEER

December 4, 2020  
*Scott A. Etkin*  
ENGINEER OF DESIGN AND ENVIRONMENT

December 4, 2020  
*James J. Quinn*  
DIRECTOR OF HIGHWAYS PROJECT IMPLEMENTATION

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OF THE STATE OF ILLINOIS

INDEX OF SHEETS

STATE STANDARDS

GENERAL NOTES

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	INDEX OF SHEETS
3-4	SUMMARY OF QUANTITIES
5	EXISTING AND PROPOSED TYPICAL SECTIONS
6	ROADWAY AND PAVEMENT MARKING PLAN
7-8	SIDEWALK DETAIL PLAN
9	DRIVEWAY DETAIL (BD-02)
10	DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING (BD-8)
11	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT (BD-22)
12	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT (BD-24)
13	BUTT JOINT AND HMA TAPER DETAILS (BD-32)
14	CITY OF CHICAGO DETECTABLE WARNINGS (BD-58)
15	TRAFFIC CONTROL AND PROTECTION FOR SIDE READS, INTERSECTIONS, AND DRIVEWAYS (TC-10)
16	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) (TC-14)
17	ARTERIAL ROAD INFORMATION SIGN (TC-22)
18-20	CITY OF CHICAGO TYPICAL PAVEMENT MARKINGS (TC-24)
21	DRIVEWAY ENTRANCE SIGNING (TC-26)

STANDARD NO.	DESCRIPTION
000001-08	TYPICAL SYMBOLS, ABBREVIATIONS AND PATTERNS
442201-03	CLASS C AND D PATCHES
604001-05	FRAME AND LIDS, TYPE 1
606001-07	CONCRETE CURB TYPE B AND COMBINATION CONCRETE CURB AND GUTTER
701101-05	OFF-RD OPERATIONS, MULTILANE, 15' (4.5 m) TO 24" (600 mm) FROM PAVEMENT EDGE
701427-05	LANE CLOSURE, MULTILANE, INTERMITENT OR MOVING OPER., FOR SPEEDS = 40 MPH
701602-10	URBAN LANE CLOSURE, MULTILANE, 2W WITH BIDIRECTIONAL LEFT TURN LANE
701606-10	URBAN SINGLE LANE CLOSURE, MULTILANE, 2W WITH MOUNTABLE MEDIAN
701701-10	URBAN LANE CLOSURE, MULTILANE INTERSECTION
701801-06	SIDEWALK, CORNER, OR CROSSWALK CLOSURE
701901-08	TRAFFIC CONTROL DEVICES
780001-05	TYPICAL PAVEMENT MARKINGS
781001-04	TYPICAL APPLICATIONS RAISED REFLECTIVE PAVEMENT MARKERS
814001-03	HANDHOLES

- BEFORE STARTING ANY EXCAVATION THE CONTRACTOR SHALL CALL "CUAN" (CHICAGO UTILITY ALERT NETWORK) AT 312-744-7000 FOR FIELD LOCATIONS OF BURIED ELECTRIC, (48 HOUR NOTIFICATION REQUIRED)
- THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH UTILITY COMPANIES, AND THE CITY OF CHICAGO
- THE CONTRACTOR WILL NOT BE ALLOWED TO SET UP A YARD OR FIELD OFFICE ON STATE PROPERTY WITHOUT WRITTEN PERMISSION OF THE DEPARTMENT.
- ALL DAMAGE TO EXISTING PAVEMENT MARKINGS OR RAISED REFLECTIVE PAVEMENT MARKERS OUTSIDE THE REMOVAL LINE SHOWN ON THE PLANS WILL BE REPLACED AT THE CONTRACTOR'S EXPENSE.
- BEFORE BEGINNING ANY WORK, THE CONTRACTOR SHALL RETAIN AND RECORD FOR FUTURE REFERENCE, ALL EXISTING PAVEMENT MARKING LINES (AND RAISED REFLECTIVE PAVEMENT MARKERS) IN ORDER THAT THESE LOCATIONS CAN BE RE-ESTABLISHED FOR STRIPING. EXACT LOCATIONS OF ALL PAVEMENT MARKINGS SHALL BE AS DIRECTED BY THE ENGINEER.
- ALL PAVEMENT PATCHING, CURB AND GUTTER REMOVAL AND REPLACEMENT, SIDEWALK REMOVAL, P.C.C. SIDEWALK 5", AND DRAINAGE ADJUSTMENT LOCATIONS WILL BE DETERMINED IN THE FIELD BY THE ENGINEER.
- IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS EXISTING IN THE FIELD PRIOR TO CONSTRUCTION AND ORDERING OF MATERIALS.
- FRAMES AND GRATES ADJUSTMENT OF PRIVATE UTILITIES WITHIN THE LIMITS OF THE IMPROVEMENTS SHALL BE DONE BY THEIR RESPECTIVE OWNERS AND ARE NOT PART OF THIS CONTRACT.
- THE CONTRACTOR SHALL CONTACT THE DISTRICT ONE TRAFFIC CONTROL SUPERVISOR AT KALPANA.KANNAN-HOSADURGA@ILLINOIS.GOV A MINIMUM OF 72 HOURS OF WORK
- THE RESIDENT ENGINEER SHALL CONTACT EMAD ALHUSSEINI, ARTERIAL TRAFFIC FIELD ENGINEER, AT EMAD.ALHUSSEINI@ILLINOIS.GOV MINIMUM OF TWO (2) WEEKS PRIOR TO THE PLACEMENT OF PERMANENT PAVEMENT MARKINGS.
- THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.
- EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR ACCORDING TO ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.
- DO NOT SCALE PLANS FOR CONSTRUCTION DIMENSIONS.
- PAVEMENT MARKING TAPE, TYPE III SHALL BE USED FOR SHORT TERM PAVEMENT MARKINGS ON ALL FINAL SURFACES.
- ANY LOOSE MATERIAL DEPOSITED IN THE FLOW LINE OF CURB OR DRAINAGE STRUCTURES, WHICH OBSTRUCTS THE NATURAL FLOW OF WATER, SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY. PRIOR TO ACCEPTANCE OF THE IMPROVEMENT. ALL DRAINAGE STRUCTURES SHALL BE FREE OF DIRT AND DEBRIS. THIS WORK WILL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED AS INCIDENTAL.
- BUTT JOINTS WILL BE INSTALLED AT THE ENDS OF ALL RESURFACING (WHERE RESURFACING MEETS EXISTING PAVEMENT), IN ACCORDANCE WITH THE "BUTT JOINT AND HMA TAPER DETAILS" SHEET INCLUDED IN THE PLANS, UNLESS OTHERWISE SPECIFIED.
- ANY PAVEMENT MARKINGS AND RAISED REFLECTIVE PAVEMENT MARKERS OBLITERATED BY MILLING AND RESURFACING OPERATIONS ON SIDE STREETS AND ENTRANCES SHALL BE REPLACED AND PAID FOR IN KIND.
- THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE ACCESS TO ABUTTING PROPERTY AT ALL TIMES DURING THE CONSTRUCTION OF THIS PROJECT.
- PROPOSED SIDEWALK RAMPS SHALL CONFORM TO CURRENT ADA REQUIREMENTS AND APPLICABLE STATE HIGHWAY STANDARDS OR AS DETERMINED BY THE ENGINEER.
- THE CONTRACTOR SHALL MAINTAIN PEDESTRIAN ACCESS AT ALL TIMES DURING CONSTRUCTION.

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>IL 64 (NORTH AVE.) SHEFFIELD AVE. TO HALSTED ST. INDEX OF SHEETS, STATE STANDARDS &amp; GENERAL NOTES</b>			
SCALE:	SHEET	OF	SHEETS
	STA.	TO	STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
307	2020-096-R5	COOK	21	2
			CONTRACT NO. 62L74	
		ILLINOIS	FED. AID PROJECT	

REV-SEP

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		100% STATE 0005					
20200100	EARTH EXCAVATION	CU YD	8	8					
40600290	BITUMINOUS MATERIALS (TACK COAT)	POUND	4188	4188					
40600400	MIXTURE FOR CRACKS, JOINTS, AND FLANGEWAYS	TON	1	1					
40600982	HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT	SO YD	220	220					
40601005	HOT-MIX ASPHALT REPLACEMENT OVER PATCHES	TON	37	37					
40604172	POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, IL-9.5, MIX "E", N70	TON	912	912					
42300400	PORTLAND CEMENT CONCRETE DRIVEWAY PAVEMENT, 8 INCH	SO YD	45	45					
42400200	PORTLAND CEMENT CONCRETE SIDEWALK 5 INCH	SO FT	578	578					
44000156	HOT-MIX ASPHALT SURFACE REMOVAL, 1 3/4"	SO YD	9306	9306					
44000200	DRIVEWAY PAVEMENT REMOVAL	SO YD	45	45					
44000600	SIDEWALK REMOVAL	SO FT	600	600					
44002216	HOT-MIX ASPHALT REMOVAL OVER PATCHES, 4"	SO YD	161	161					
44201765	CLASS D PATCHES, TYPE II, 10 INCH	SO YD	25	25					
44201769	CLASS D PATCHES, TYPE III, 10 INCH	SO YD	85	85					

SUMMARY OF QUANTITIES			TOTAL QUANTITIES URBAN	CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT		100% STATE 0005					
44201771	CLASS D PATCHES, TYPE IV, 10 INCH	SO YD	30	30					
60237460	INLETS, TYPE A, TYPE 23 FRAME AND GRATE	EACH	1	1					
60255700	MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, OPEN LID	EACH	3	3					
60255800	MANHOLES TO BE ADJUSTED WITH NEW TYPE 1 FRAME, CLOSED LID	EACH	3	3					
* 66900200	NON-SPECIAL WASTE DISPOSAL	CU YD	8	8					
* 66900530	SOIL DISPOSAL ANALYSIS	EACH	2	2					
* 66901001	REGULATED SUBSTANCES PRE-CONSTRUCTION PLAN	LSUM	1	1					
* 66901003	REGULATED SUBSTANCES FINAL CONSTRUCTION REPORT	LSUM	1	1					
* 66901006	REGULATED SUBSTANCES MONITORING	CAL DA	3	3					
67000400	ENGINEER'S FIELD OFFICE, TYPE A	CAL MO	6	6					
67100100	MOBILIZATION	L SUM	1	1					
70102625	TRAFFIC CONTROL AND PROTECTION, STANDARD 701606	L SUM	1	1					
70102632	TRAFFIC CONTROL AND PROTECTION, STANDARD 701602	L SUM	1	1					REV-SEP

\* SPECIALTY ITEM

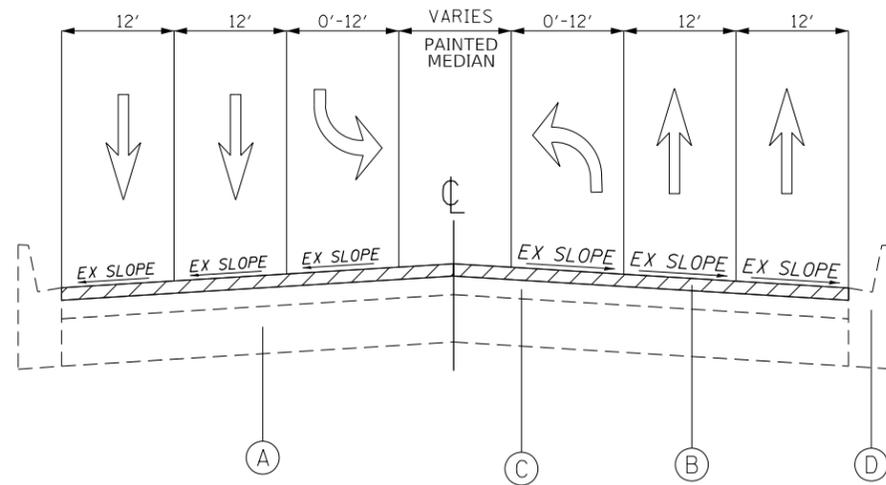
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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

SUMMARY OF QUANTITIES			
SCALE:	SHEET NO.	OF SHEETS	STA. TO STA.

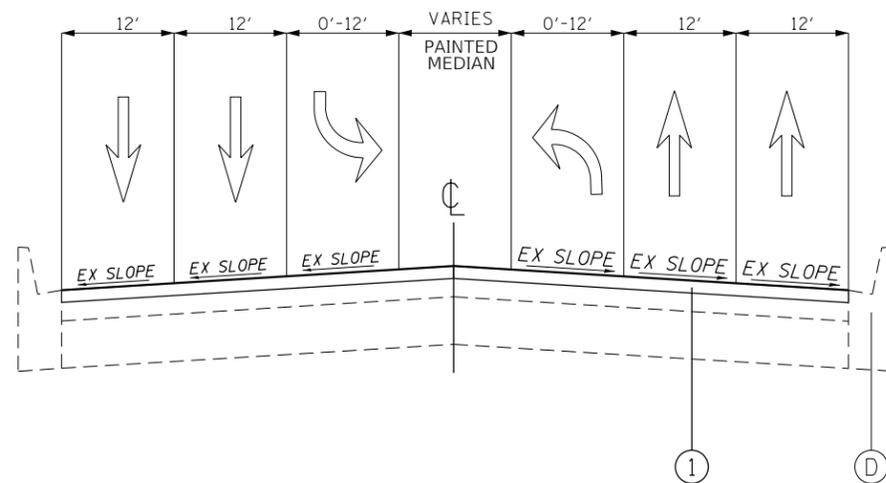
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307	2020-096-R5	COOK	21	3
CONTRACT NO. 62L74				
FED. ROAD DIST. NO. 1   ILLINOIS   FED. AID PROJECT				





**EXISTING TYPICAL SECTION**

STA 11+76 TO STA 26+20



**PROPOSED TYPICAL SECTION**

STA 11+76 TO STA 26+20

**LEGEND - EXISTING:**

- (A) CONCRETE PAVEMENT ± 10"
- (B) HOT MIX ASPHALT SURFACE REMOVAL 1 3/4"
- (C) HOT MIX ASPHALT SURFACE 4"±
- (D) COMBINATION CURB & GUTTER, TYPE B-6.12

**LEGEND - PROPOSED**

- (1) POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", IL-9.5, N70, 1 3/4"

- NOTE 1: THE UNIT WEIGHT USED TO CALCULATE ALL HMA SURFACE MIXTURE QUANTITIES IS 112 LBS/SQ YD/IN.
- NOTE 2: THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76-22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64-22" UNLESS MODIFIED BY SPECIAL PROVISIONS. FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS.
- NOTE 3: QUALITY MANAGEMENT PROGRAM (QMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE.
- NOTE 4: THE CONTRACTOR SHALL PATCH FIRST.
- NOTE 5: THE LONGITUDINAL JOINT SEALANT SHALL BE PLACED OVER THE MILLED SURFACE

HOT-MIX ASPHALT MIXTURE REQUIREMENTS		QUALITY MANAGEMENT PROGRAM (QMP)
MIXTURE TYPE	AIR VOIDS ● Ndes	
<b>PAVEMENT RESURFACING</b>		
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", IL-9.5, N70, 1 3/4"	4.0% AT 70 GYR	QC/OA
<b>PATCHING</b>		
CLASS D PATCHES (HMA BINDER IL-19 mm)	4% AT 70 GYR.	QC/OA
HOT-MIX ASPHALT REPLACEMENT OVER PATCHES (HMA BINDER IL-19 mm)	4% AT 70 GYR.	QC/OA
QMP DESIGNATION: QUALITY CONTROL/QUALITY ASSURANCE (QC/OA); QUALITY CONTROL FOR PERFORMANCE (QCP)		

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**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

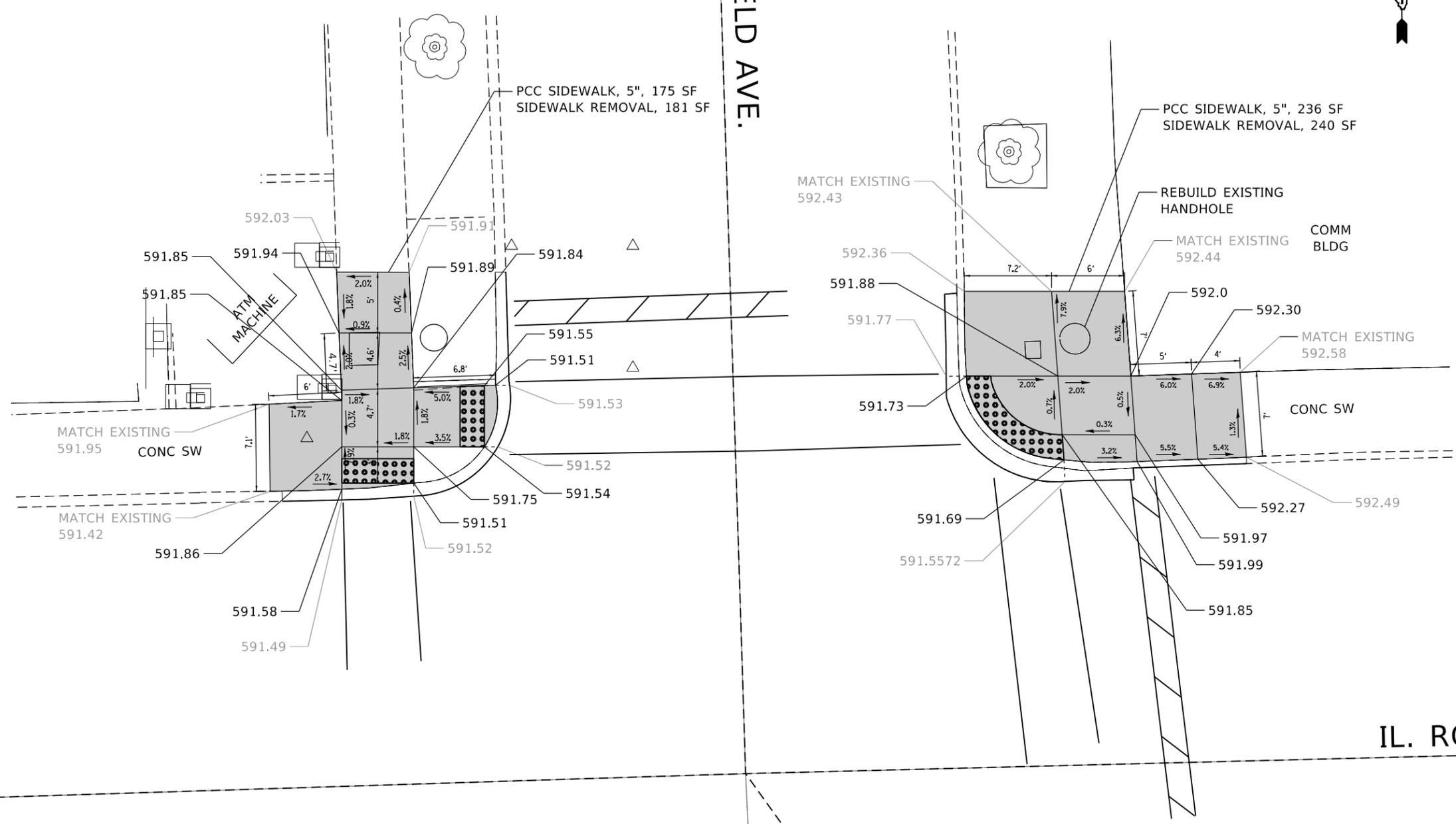
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
307	2020-096-R5	COOK	21	5
CONTRACT NO. 62L74				
SCALE:	SHEET OF SHEETS	STA. TO STA.	ILLINOIS FED. AID PROJECT	





SHEFFIELD AVE.

IL. ROUTE 64



**LEGEND**

- xx.xx' EXISTING LENGTH
- ( ) EXISTING ELEVATION/SLOPE

- PROPOSED SIDEWALK
- PROPOSED SIDE CURB
- SC TOP OF PROPOSED SIDE CURB

- CLASS D PATCH
- DETECTABLE WARNINGS
- SIDEWALK REMOVAL REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 592.51  
 BENCHMARK : NORTH WEST BOLT OF FIRE HYDRANT  
 LOCATION : ON NORTH WEST CORNER OF IL 64 AND SHEFFIELD AVE

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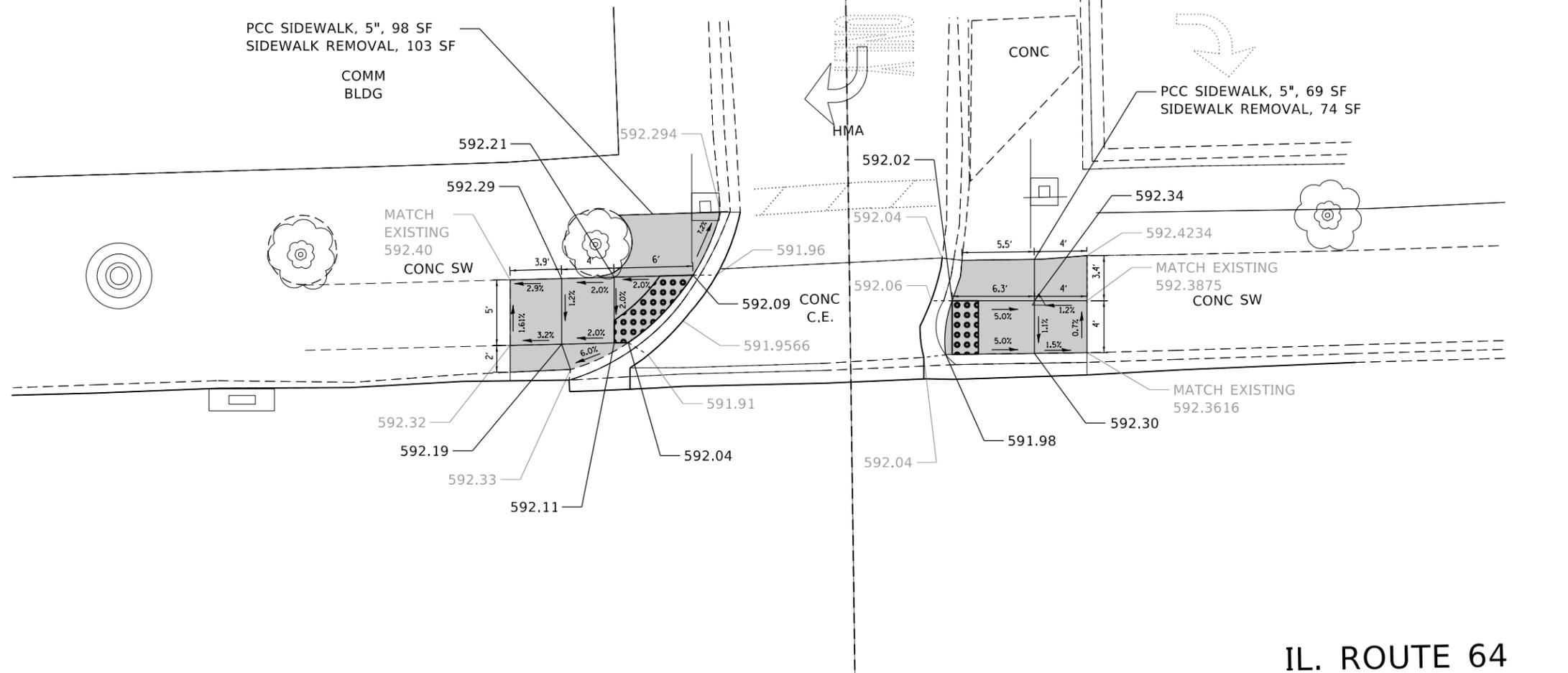
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SIDEWALK DETAIL PLAN  
IL 64(NORTH AVE.) - SHEFFIELD AVE TO HALSTED ST.**

SCALE: SHEET 1 OF 23 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
307	2020-096-RS	COOK	21	7
CONTRACT NO. 62L74				
ILLINOIS FED. AID PROJECT				

ONLY  
CONTAINER ENT.



**LEGEND**



EXISTING LENGTH



EXISTING ELEVATION/SLOPE



PROPOSED SIDEWALK



PROPOSED SIDE CURB

SC

TOP OF PROPOSED SIDE CURB



CLASS D PATCH



DETECTABLE WARNINGS



SIDEWALK REMOVAL  
REPLACE W/TOPSOIL & SOD

REFERENCE BENCHMARK ELEV 593.37

BENCHMARK : NORTH WEST BOLT OF FIRE HYDRANT

LOCATION : NORTH SIDE OF IL 64 APPROXIMATELY 200 FOOT WEST OF CONTAINER STORE ENTRANCE

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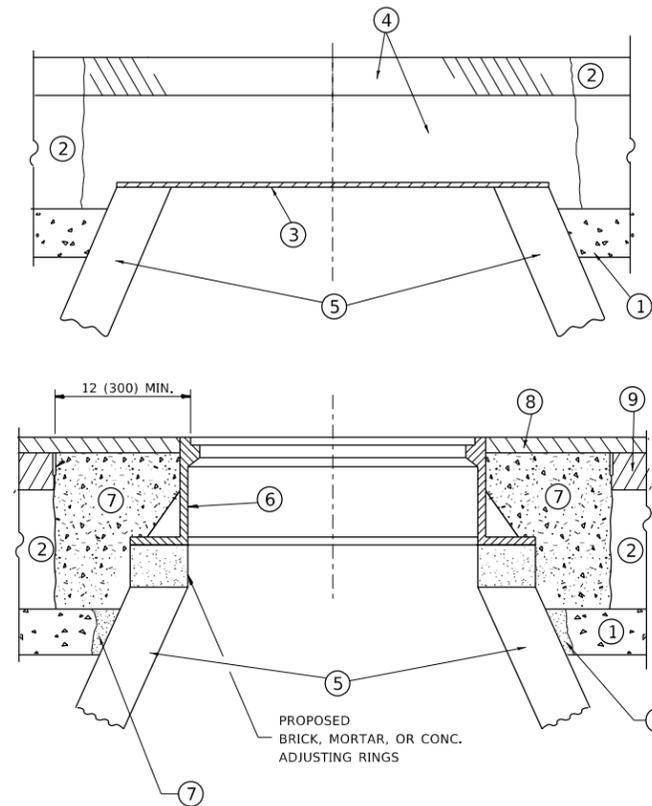
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**SIDEWALK DETAIL PLAN  
IL 64(NORTH AVE.) - SHEFFIELD AVE TO HALSTED ST.**

SCALE: SHEET 1 OF 23 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
307	2020-096-RS	COOK	21	8
CONTRACT NO. 62L74				
ILLINOIS FED. AID PROJECT				





**NOTES**

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

**CONSTRUCTION PROCEDURES**

**STAGE 1 (BEFORE PAVEMENT MILLING)**

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1½ (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

**STAGE 2 (AFTER PAVEMENT MILLING)**

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
- B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
- C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1 \* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.

\* UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT \*THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

**LEGEND**

- ① SUB-BASE GRANULAR MATERIAL
- ② EXISTING PAVEMENT
- ③ 36 (900) DIAMETER METAL PLATE
- ④ PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- ⑤ EXISTING STRUCTURE
- ⑥ FRAME AND LID (SEE NOTES)
- ⑦ CLASS PP-1 \*CONCRETE
- ⑧ PROPOSED HMA SURFACE COURSE
- ⑨ PROPOSED HMA BINDER COURSE

**LOCATION OF STRUCTURES**

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

**BASIS OF PAYMENT**

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR \*FRAMES AND LIDS TO BE ADJUSTED (SPECIAL).\*

THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

**DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING**

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

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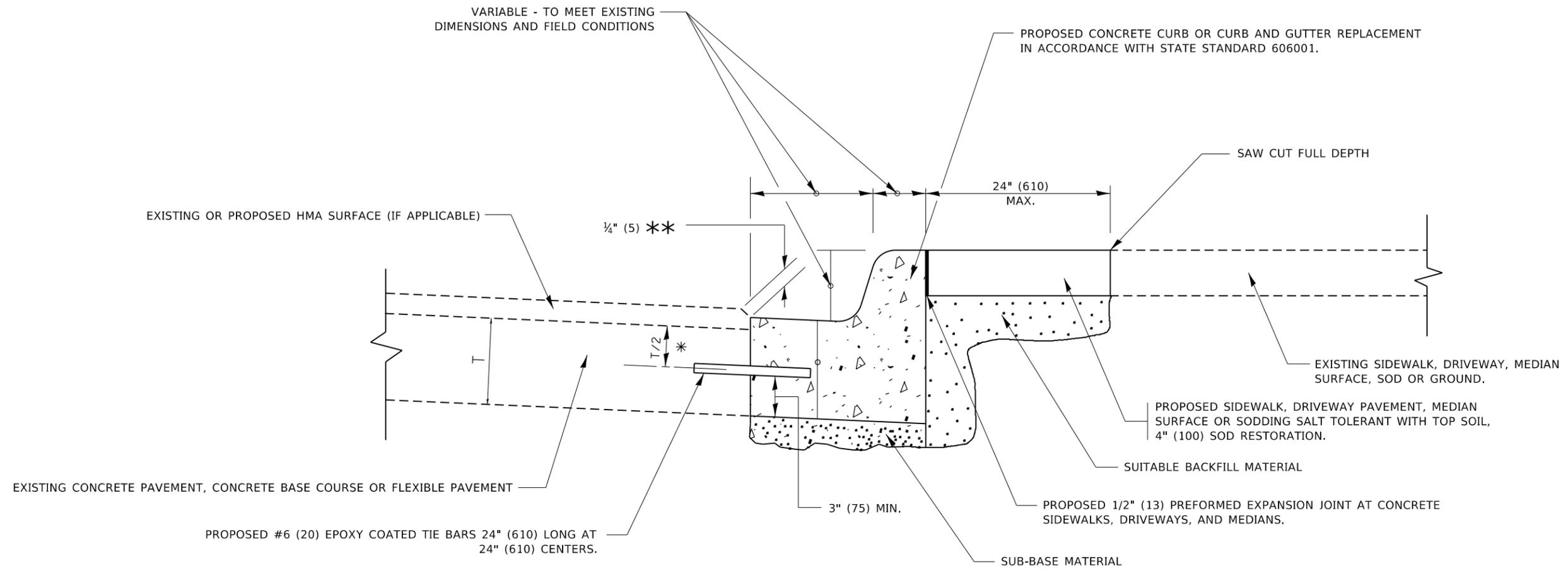
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**DETAILS FOR  
FRAMES AND LIDS ADJUSTMENT WITH MILLING**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
307	2020-096-R5	COOK	21	10
<b>BD600-03 (BD-8)</b>		CONTRACT NO.	62L74	
ILLINOIS		FED. AID PROJECT		





- \* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.
- \*\* IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH WITH THE PAVEMENT.

# CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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	DRAWN -	REVISED - M. GOMEZ 01-22-01
PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - R. BORO 12-15-09
PLOT DATE = 10/22/2020	DATE - 03-11-94	REVISED - K. SMITH 07-11-19

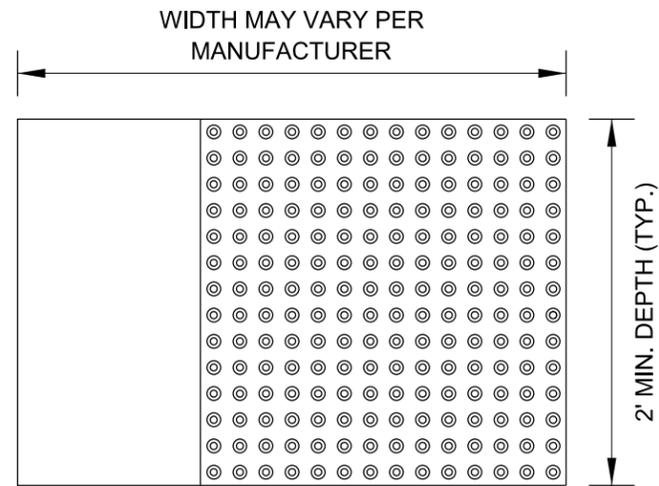
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

<b>CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT</b>			
SCALE: NONE	SHEET 1	OF 1 SHEETS	STA. TO STA.

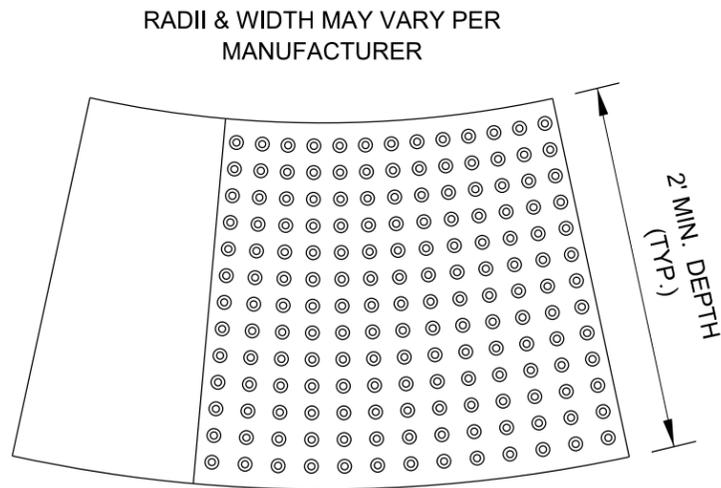
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
307	2020-096-R5	COOK	21	12
<b>BD600-06 (BD-24)</b>		CONTRACT NO. 62L74		
ILLINOIS FED. AID PROJECT				



### STRAIGHT DETECTABLE WARNING UNITS



### RADIAL DETECTABLE WARNING UNITS

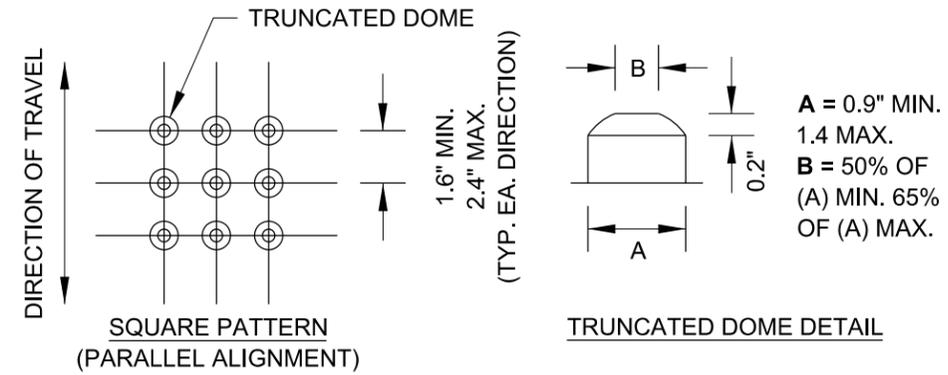


### DETECTABLE WARNING UNIT SIZES

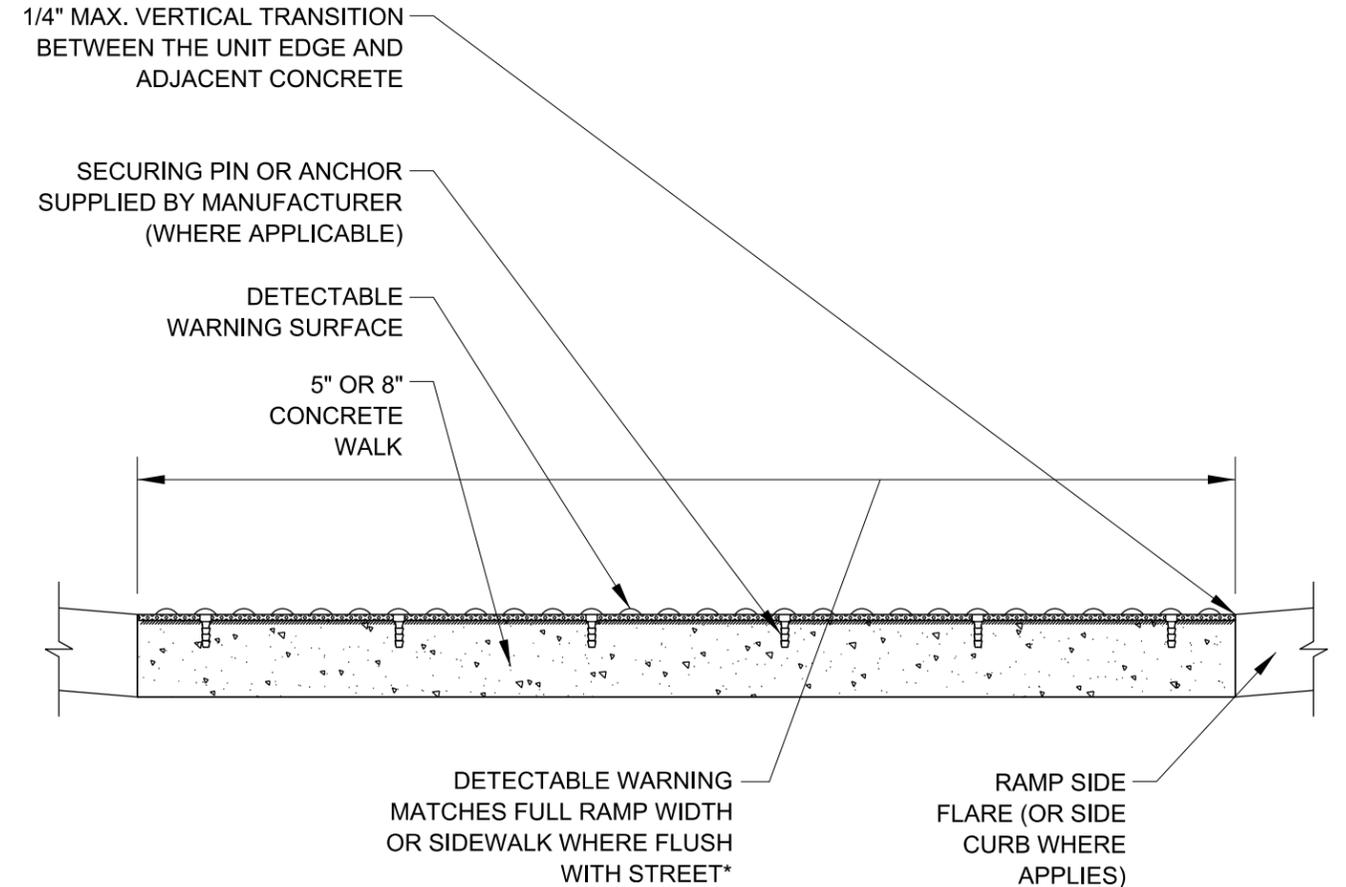
- VERIFY ALL DIMENSIONS WITH THE PRODUCT MANUFACTURER.
- IF USING RADIAL UNITS, VERIFY THAT THE CURB RADIUS MATCHES AVAILABLE UNIT RADII WITH THE PRODUCT MANUFACTURER.

### GENERAL NOTE:

THE ROWS OF DOMES IN THE DETECTABLE WARNING MATERIAL MUST BE ALIGNED WITH THE PATH OF WHEELCHAIR TRAVEL WHICH IS REQUIRED TO BE PERPENDICULAR TO THE GRADE BREAK AT THE BOTTOM OF THE RAMP TO PERMIT TRACKING BETWEEN DOME ROWS. ON BLENDED TRANSITIONS OR FLUSH TRANSITIONS, WHERE RADIAL UNITS ARE SITUATED ABOUT THE CURB RADIUS, DOME ORIENTATION IS NOT SIGNIFICANT.



### UNIT PATTERN & DOME DETAIL



\*A BORDER OF 2 INCHES OR LESS AROUND THE DETECTABLE WARNING SURFACE IS ACCEPTABLE IF REQUIRED FOR PROPER INSTALLATION OF THE DETECTABLE WARNING SURFACE PRODUCT

### DETECTABLE WARNING UNIT SECTION

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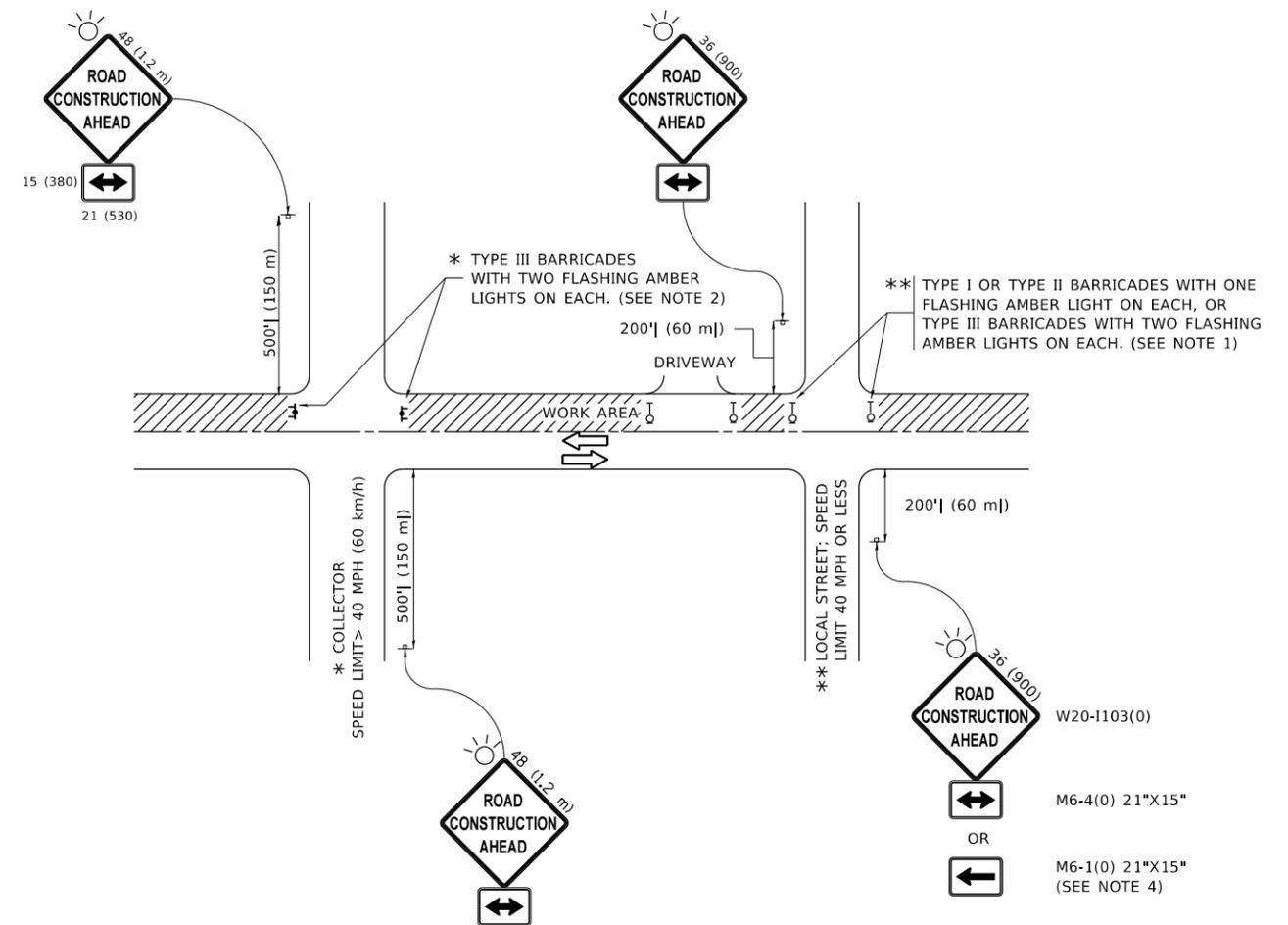
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	DRAWN -	REVISED -
PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED -
PLOT DATE = 10/22/2020	DATE - 06-20-2017	REVISED -

STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO  
DETECTABLE WARNINGS

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
307	2020-096-R5	COOK	21	14
BD 58		CONTRACT NO. 62L74		
ILLINOIS FED. AID PROJECT				



**NOTES:**

- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE "ROAD CONSTRUCTION AHEAD" SIGN 36 x 36 (900x900) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
  - ONE "ROAD CONSTRUCTION AHEAD" SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
  - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
- SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).
- WHEN WORK IS BEING PERFORMED ON A SIDE ROAD OR DRIVEWAY, FOLLOW THE APPLICABLE STANDARD(S). THE DIRECTIONAL ARROW (M6-1 OR M6-4) SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE TRAFFIC CONTROL SET-UP.
- ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAYS UNLESS OTHERWISE SPECIFIED IN THE PLANS OR BY THE ENGINEER.
- THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

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PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - T. RAMMACHER 01-06-00
PLOT DATE = 10/22/2020	DATE - 06-89	REVISED - A. SCHUETZE 07-01-13
		REVISED - A. SCHUETZE 09-15-16

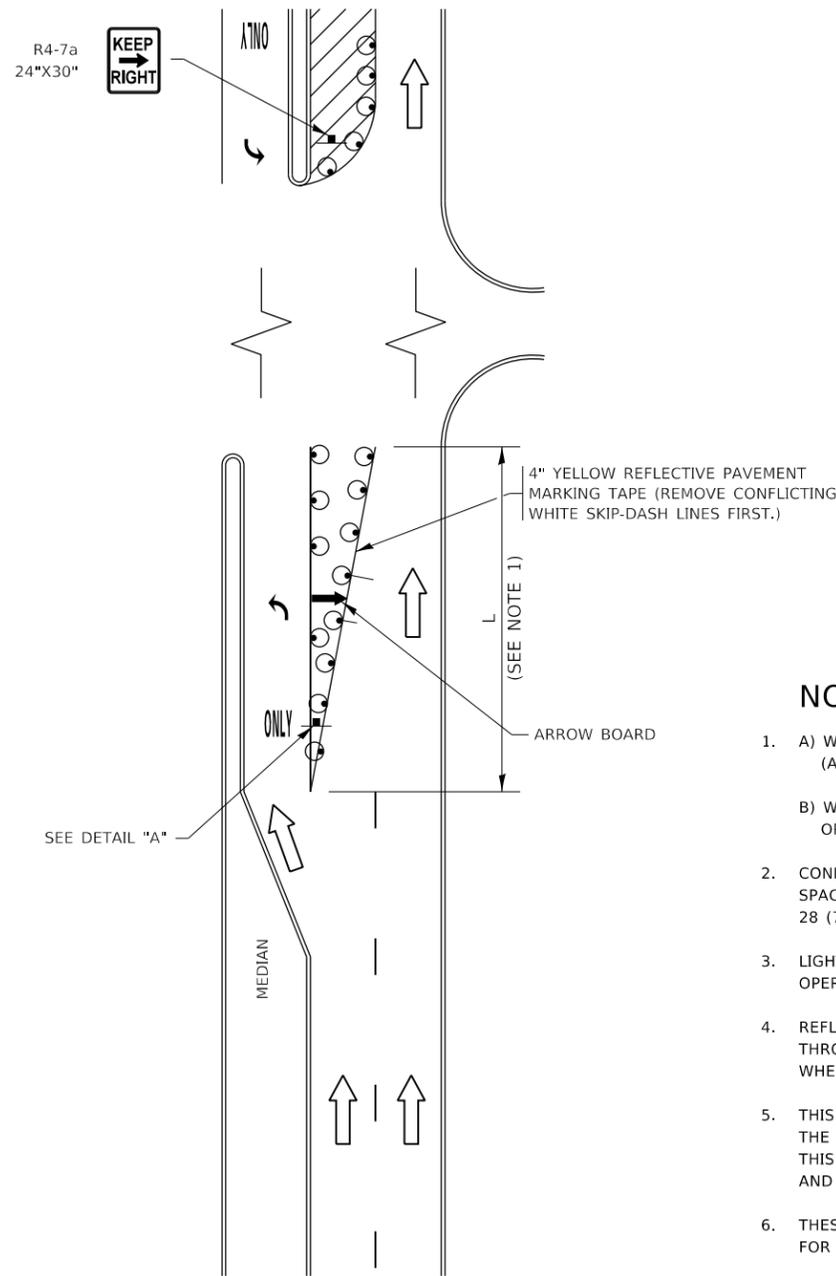
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION FOR  
SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

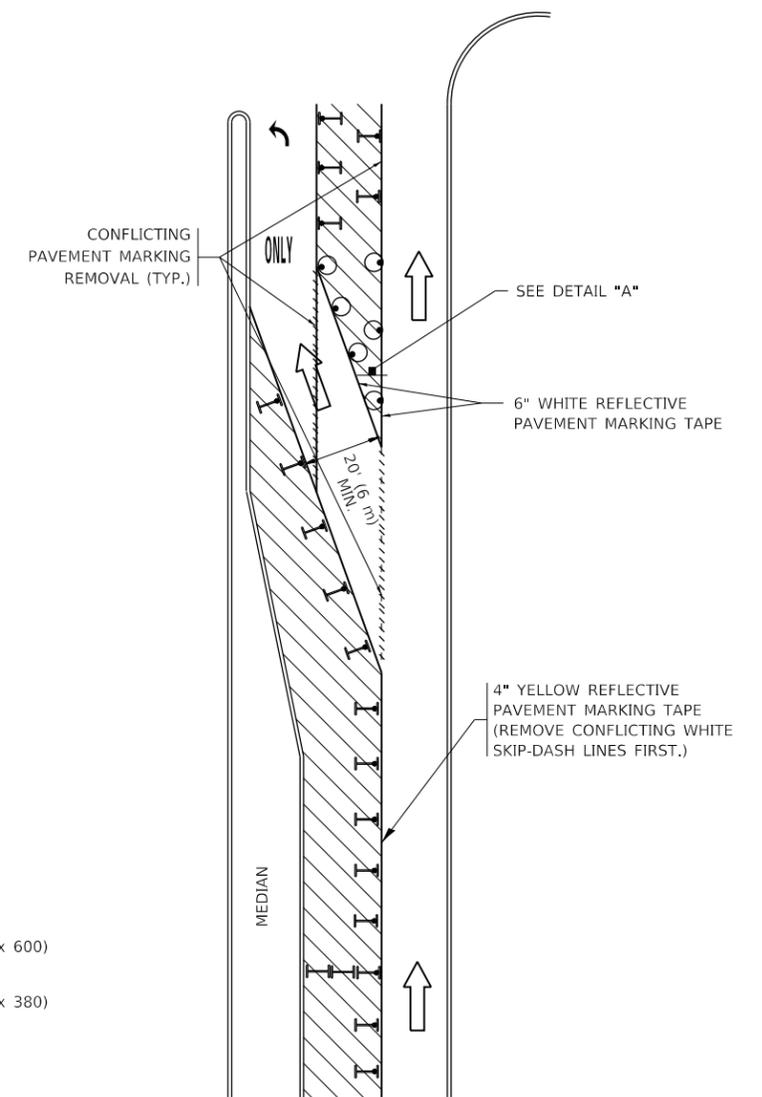
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307	2020-096-R5	COOK	21	15
<b>TC-10</b>			CONTRACT NO. 62L74	
ILLINOIS FED. AID PROJECT				

# TURN BAY ENTRANCE AT START OF LANE CLOSURE TAPER

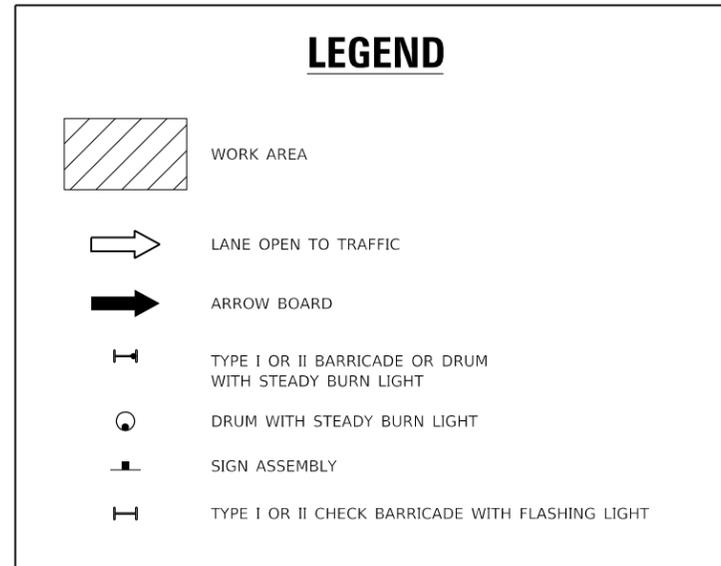


**FIGURE 1**

# TURN BAY ENTRANCE WITHIN A LANE CLOSURE

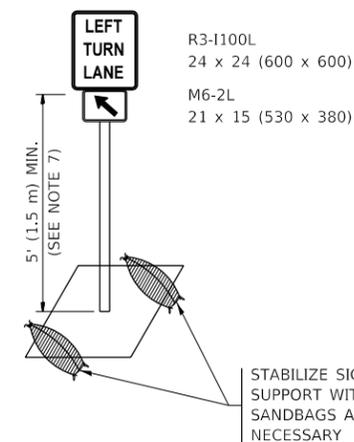


**FIGURE 2**



### NOTES:

1. A) WHEN "L" IS ≤ THE STORAGE LENGTH OF THE TURN LANE (AS SHOWN IN FIG. 1), USE FIGURE 1.  
B) WHEN "L" IS > THE STORAGE LENGTH OF THE TURN LANE OR THE TURN LANE IS WITHIN THE LANE CLOSURE, USE FIGURE 2.
2. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT.
3. LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
4. REFLECTIVE TEMPORARY PAVEMENT MARKINGS SHALL BE PLACED THROUGHOUT THE BARRICADED AREAS OF EACH TURN BAY AS SHOWN WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN (14) DAYS.
5. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-1100R 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
6. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
7. THE SIGNS SHALL BE MOUNTED ABOVE THE BARRICADES/DRUMS ON SEPARATE SIGN SUPPORTS THAT MEET NCHRP 350 OR MASH REQUIREMENTS.
8. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.



**DETAIL A**

All dimensions are in inches (millimeters) unless otherwise shown.

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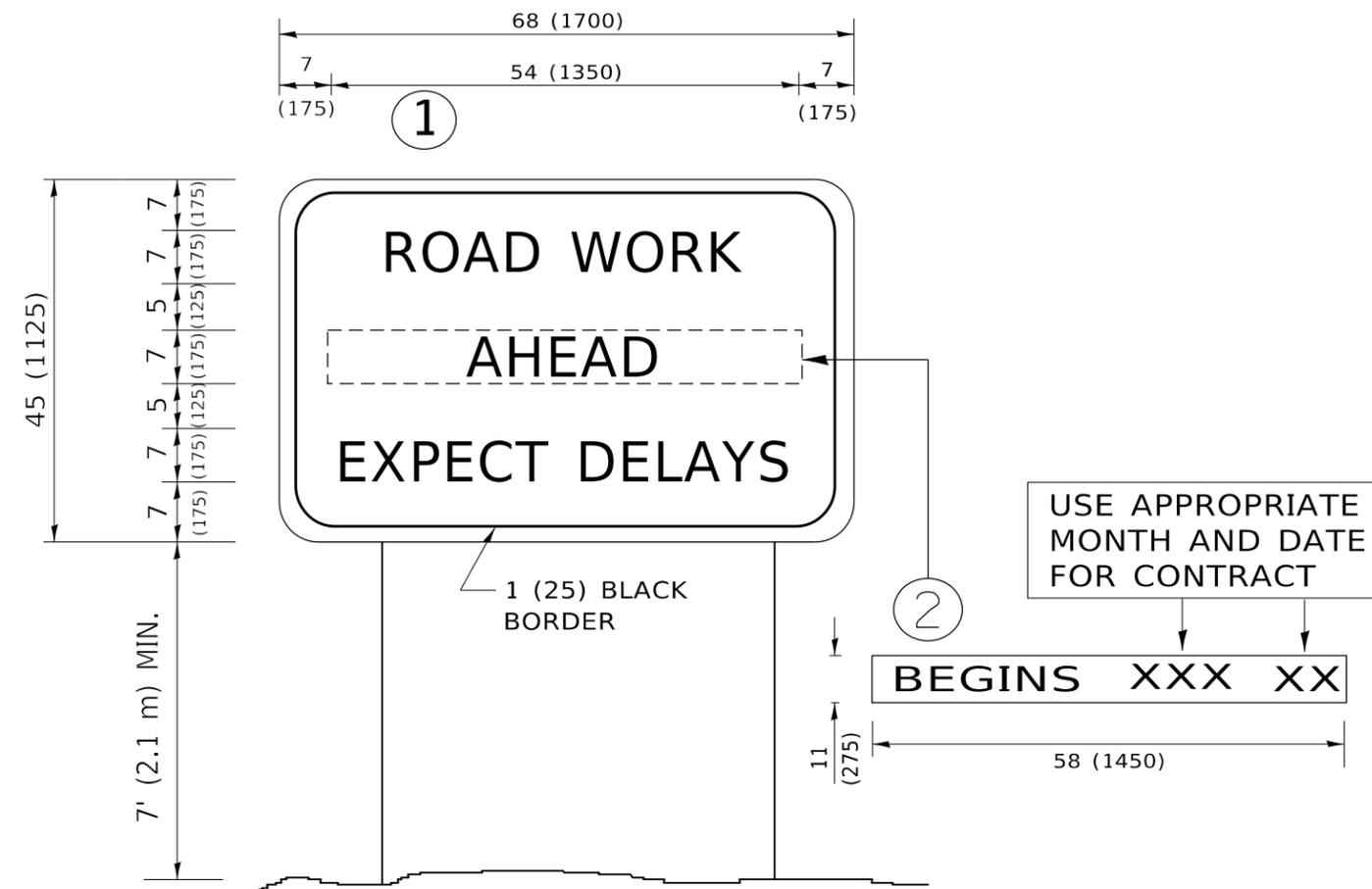
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	DRAWN - A. HOUSEH 11-07-95	REVISED - A. SCHUETZE 07-01-13
PLOT SCALE = 100,0000' / in.	CHECKED - A. HOUSEH 10-12-96	REVISED - A. SCHUETZE 09-15-16
PLOT DATE = 10/22/2020	DATE - T. RAMMACHER 01-06-00	REVISED -

**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**TRAFFIC CONTROL AND PROTECTION AT TURN BAYS  
(TO REMAIN OPEN TO TRAFFIC)**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
307	2020-096-R5	COOK	21	16
TC-14		CONTRACT NO. 62L74		
ILLINOIS FED. AID PROJECT				



**NOTES:**

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

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PLOT SCALE = 100.0000 ' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99
PLOT DATE = 10/22/2020	DATE -	REVISED - C. JUCIUS 01-31-07

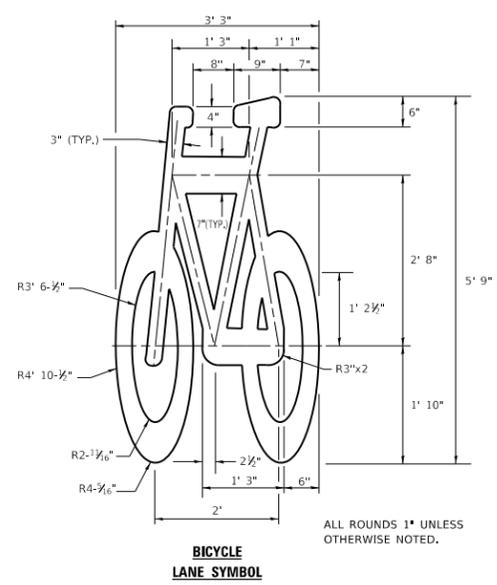
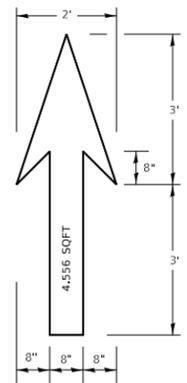
**STATE OF ILLINOIS  
DEPARTMENT OF TRANSPORTATION**

**ARTERIAL ROAD  
INFORMATION SIGN**

SCALE: NONE SHEET 1 OF 1 SHEETS STA. TO STA.

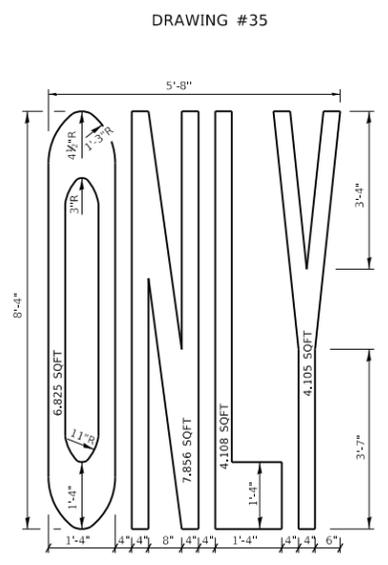
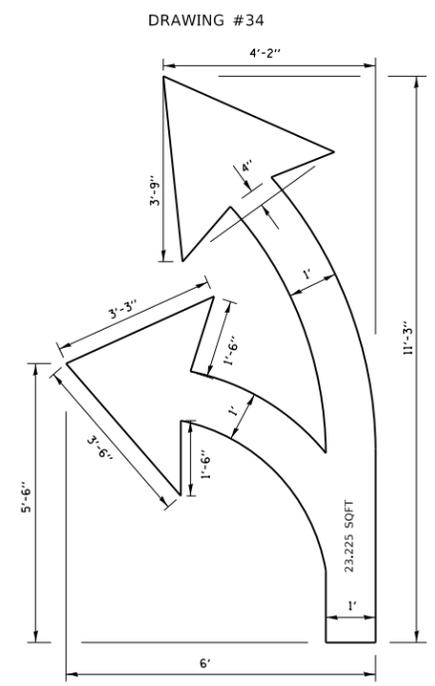
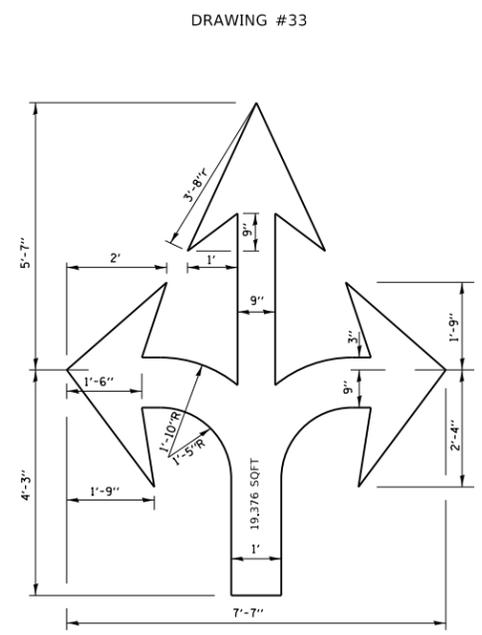
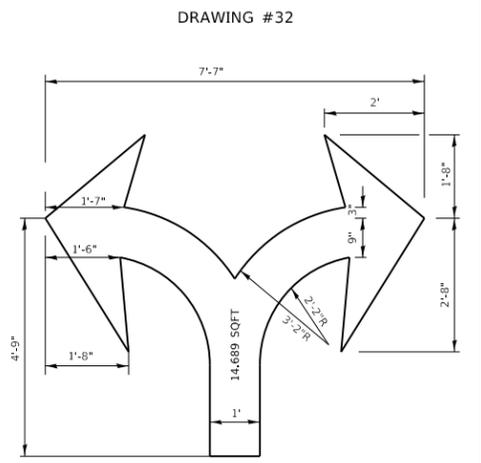
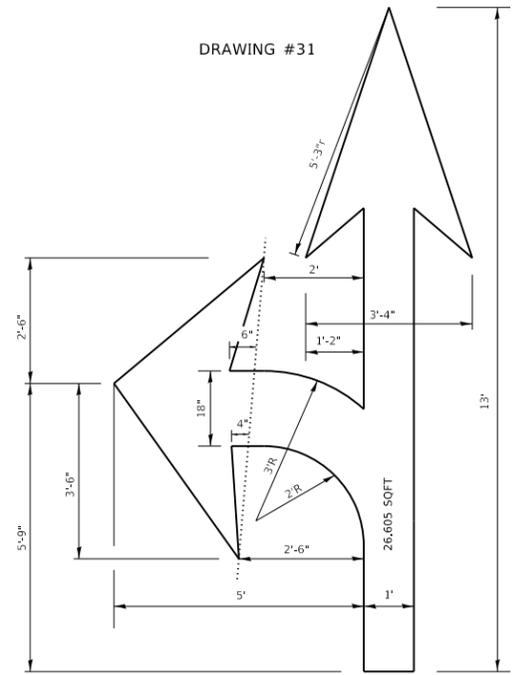
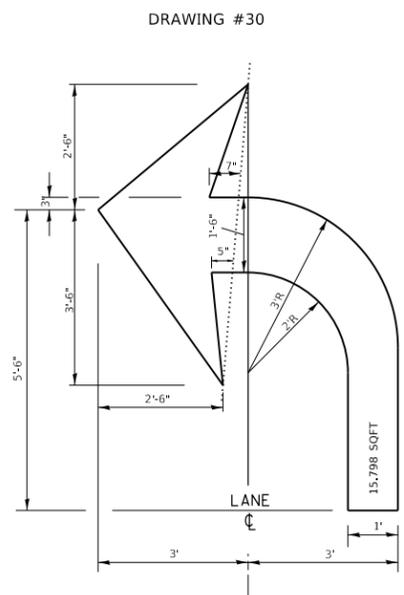
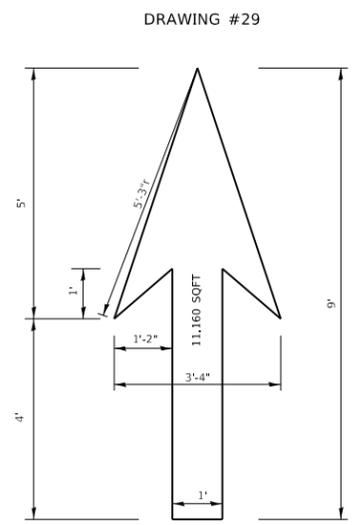
F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
307	2020-096-R5	COOK	21	17
<b>TC-22</b>			CONTRACT NO. 62L74	
ILLINOIS FED. AID PROJECT				





- NOTE:**
- FOR BIKE LANE SYMBOLS ONLY, USE PRE-FORMED THERMOPLASTIC WITH A MINIMUM THICKNESS OF 90 MILS, MINIMUM SKID RESISTANCE VALUE OF 60 BPN, & A MINIMUM INDEX OF REFRACTION OF 1.50.
  - THE RESIDENT ENGINEER SHALL CONTACT MR. BEN GOMBERG AT 312-744-8093 AT LEAST ONE CALENDAR WEEK PRIOR TO INSTALLING BIKE LANE SYMBOLS.

**TYPICAL BIKE LANE SYMBOLS**  
DRAWING #28



**NOTE:**  
ALL MARKINGS SHALL BE SOLID WHITE UNLESS OTHERWISE NOTED IN THE PLANS

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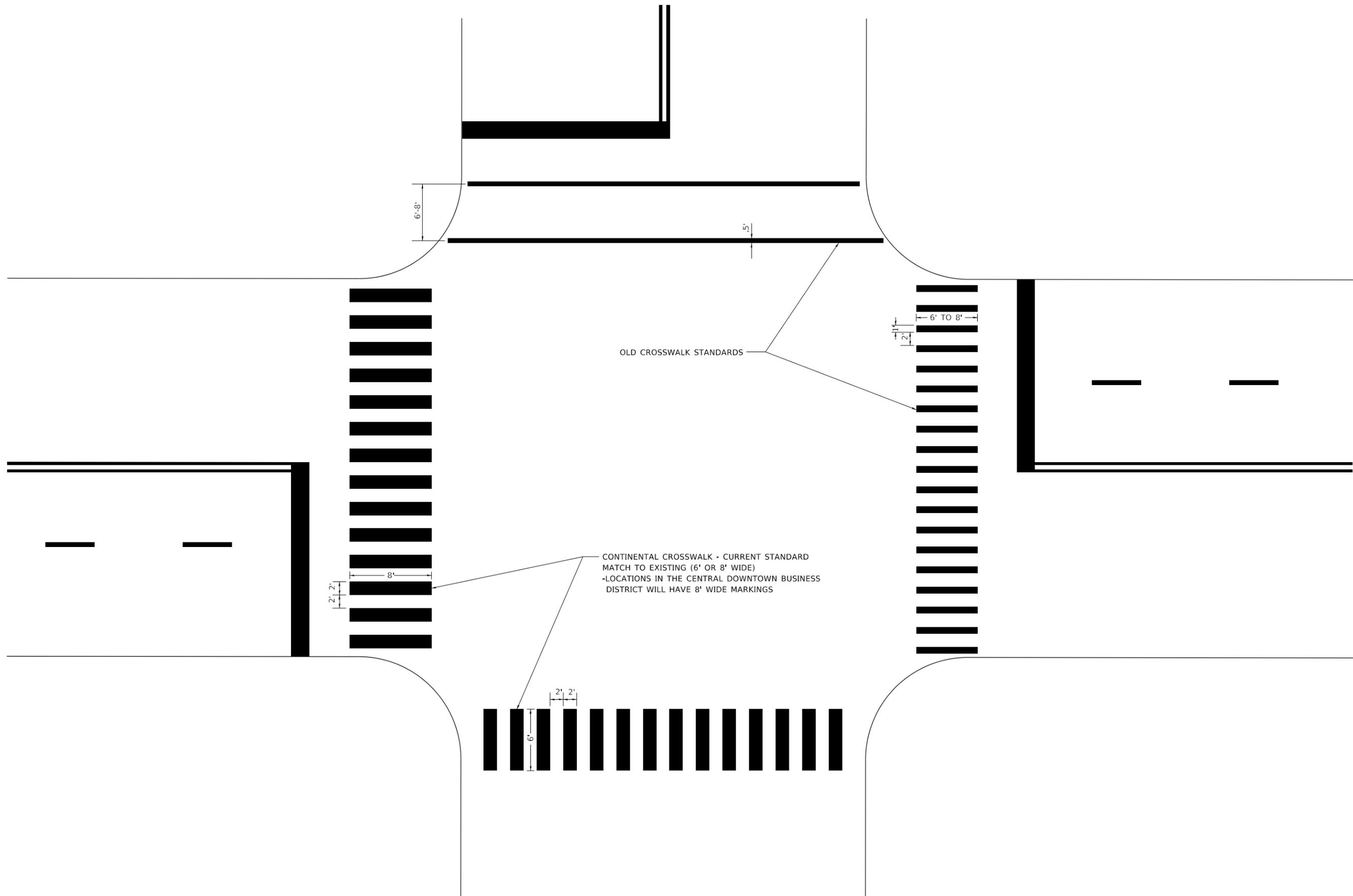
**STATE OF ILLINOIS**  
**DEPARTMENT OF TRANSPORTATION**

**CITY OF CHICAGO**  
**TYPICAL PAVEMENT MARKINGS**

SCALE: NONE SHEET 2 OF 3 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
307	2020-096-R5	COOK	21	19
<b>TC-24</b>		CONTRACT NO. 62L74		
ILLINOIS		FED. AID PROJECT		

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CONTINENTAL CROSSWALK - CURRENT STANDARD  
 MATCH TO EXISTING (6' OR 8' WIDE)  
 -LOCATIONS IN THE CENTRAL DOWNTOWN BUSINESS  
 DISTRICT WILL HAVE 8' WIDE MARKINGS

OLD CROSSWALK STANDARDS

**STATE OF ILLINOIS  
 DEPARTMENT OF TRANSPORTATION**

**CITY OF CHICAGO  
 TYPICAL PAVEMENT MARKINGS**

USER NAME = abunaimtm	DESIGNED -	REVISED - T. RAMMACHER 12-07-00
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PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
PLOT DATE = 10/22/2020	DATE -	REVISED -

SCALE: NONE SHEET 3 OF 3 SHEETS STA. TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
307	2020-096-R5	COOK	21	20
<b>TC-24</b>		CONTRACT NO. 62L74		
ILLINOIS FED. AID PROJECT				

