

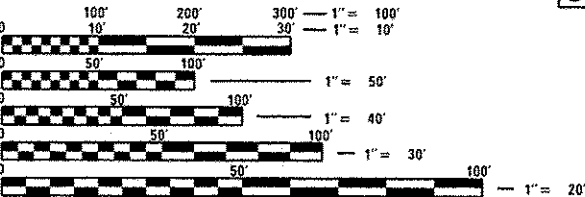
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2015-045RS	COOK	24	1
ILLINOIS CONTRACT NO. 62B05				

FOR INDEX OF SHEETS, SEE SHEET NO. 2

IMPROVEMENT LOCATED IN
THE CITY OF CHICAGO

PROPOSED HIGHWAY PLANS
F.A.P. 347 (ROOSEVELT RD.)
PULASKI RD. TO WESTERN AVE.
RESURFACING (MAINTENANCE)
SECTION NO.: 2015-045RS
COOK COUNTY
C-91-376-15

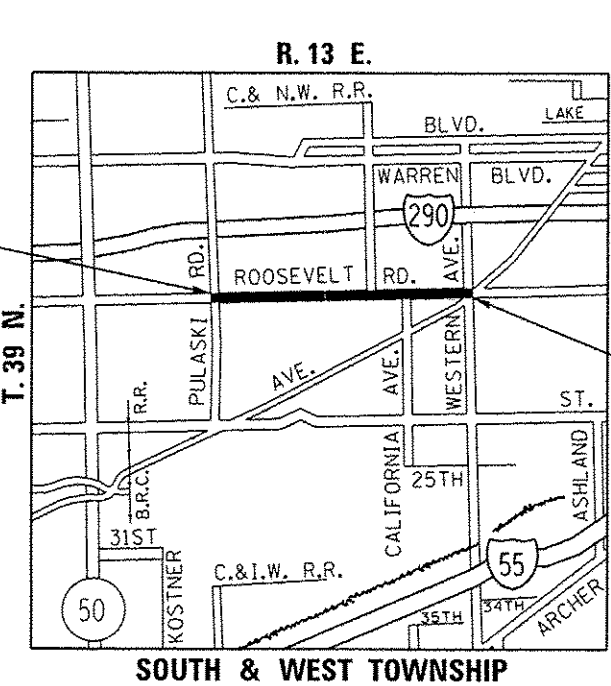


FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD
ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT
CONFORM TO STANDARD SCALES. IN MAKING MEASUREMENTS
ON REDUCED PLANS, THE ABOVE SCALES MAY BE USED.

J.U.L.I.E.
JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION
1-800-892-0123
OR 811

PROJECT ENGINEER: JENPAI CHANG (847) 705-4432
PROJECT MANAGER: KEN ENG
CONTRACT NO. 62B05

GROSS LENGTH OF IMPROVEMENT - 10564 FEET - 2.00 MILES
NET LENGTH OF IMPROVEMENT - 8383 FEET - 1.58 MILES



TRAFFIC DATA:
2010 ADT - 25800
SPEED LIMIT - 30 MPH

OMISSIONS:
S. HOMAN AVE. TO S. ALBANY AVE.
STA. 139+11 TO STA. 160+74
UNION PACIFIC RAILROAD / CSX TRANSPORTATION
STA. 190+49 TO STA. 196+38

IMPROVEMENT BEGINS
STA. 100+32

IMPROVEMENT ENDS
STA. 205+96

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS

SUBMITTED October 27, 2015

John P. Farnsworth Jr.
DEPUTY DIRECTOR OF HIGHWAYS, REGIONAL ENGINEER

Dec 4, 2015
John D. Baranzelli, P.E.
ACTING ENGINEER OF DESIGN AND ENVIRONMENT

Dec 4, 2015
Omer Osman, P.E.
DIRECTOR OF HIGHWAYS, CHIEF ENGINEER

PRINTED BY THE AUTHORITY
OF THE STATE OF ILLINOIS

URBAN

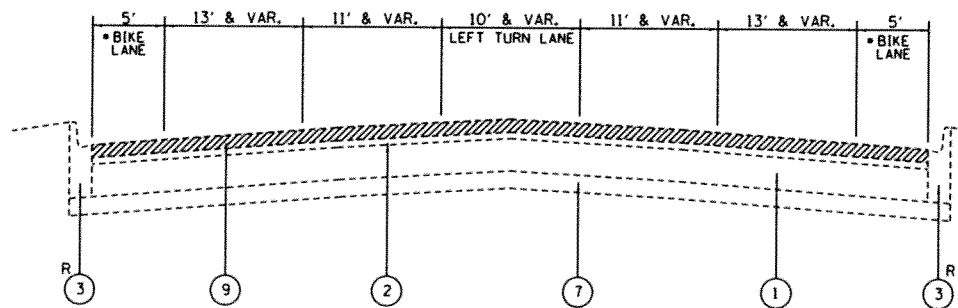
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URBAN

SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	100% STATE	0005				
70300100	SHORT TERM PAVEMENT MARKING	FOOT	9501	9501					
70300210	TEMPORARY PAVEMENT MARKING LETTERS AND SYMBOLS	SO FT	834	834					
70300220	TEMPORARY PAVEMENT MARKING - LINE 4"	FOOT	20701	20701					
70300240	TEMPORARY PAVEMENT MARKING - LINE 6"	FOOT	1812	1812					
70300260	TEMPORARY PAVEMENT MARKING - LINE 12"	FOOT	2204	2204					
70300280	TEMPORARY PAVEMENT MARKING - LINE 24"	FOOT	445	445					
70301000	WORK ZONE PAVEMENT MARKING REMOVAL	SO FT	11947	11947					
*78000100	THERMOPLASTIC PAVEMENT MARKING - LETTERS AND SYMBOLS	SO FT	834	834					
*78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 4"	FOOT	20701	20701					
*78000400	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	1812	1812					
*78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"	FOOT	2204	2204					
*78000650	THERMOPLASTIC PAVEMENT MARKING - LINE 24"	FOOT	445	445					
*88600600	DETECTOR LOOP REPLACEMENT	FOOT	384	384					

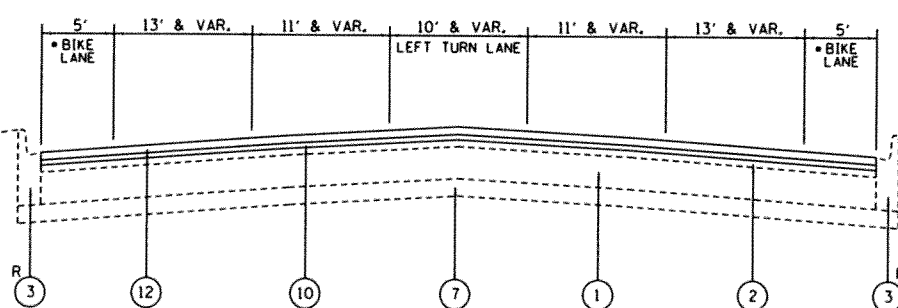
SUMMARY OF QUANTITIES				CONSTRUCTION TYPE CODE					
CODE NO	ITEM	UNIT	TOTAL QUANTITIES	100% STATE	0005				
X5537800	STORM SEWERS TO BE CLEANED 12"	FOOT	1080	1080					
X6030310	FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)	EACH	107	107					
Z0004562	COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT	FOOT	2370	2370					
Z0018500	DRAINAGE STRUCTURES TO BE CLEANED	EACH	72	72					
Z0018600	DRAINAGE STRUCTURES TO BE RECONSTRUCTED	EACH	2	2					
Z0030850	TEMPORARY INFORMATION SIGNING	SO FT	51.4	51.4					
Z0048665	RAILROAD PROTECTIVE LIABILITY INSURANCE	LSUM	1	1					

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION				SUMMARY OF QUANTITIES FAP 347 (ROOSEVELT RD.)—PULASKI RD. TO WESTERN AVE.				F.A.P. RTE. 347 SECTION 2015-045RS COUNTY COOK TOTAL SHEET NO. 24 4 CONTRACT NO. 62805			
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EXISTING TYPICAL SECTION
STA. 100+31 TO 101+77
STA. 124+78 TO 126+11
STA. 126+87 TO 127+87
STA. 137+87 TO 139+09
STA. 164+53 TO 165+99
STA. 166+75 TO 168+22
STA. 177+40 TO 179+25
STA. 180+14 TO 181+94
STA. 198+35 TO 199+56

LEGEND:
BIKE LANE
STA. 180+14 TO 181+94



PROPOSED TYPICAL SECTION
STA. 100+31 TO 101+77
STA. 124+78 TO 126+11
STA. 126+87 TO 127+87
STA. 137+87 TO 139+09
STA. 164+53 TO 165+99
STA. 166+75 TO 168+22
STA. 177+40 TO 179+25
STA. 180+14 TO 181+94
STA. 198+35 TO 199+56

LEGEND:

- 1 EXISTING P.C.C. PAVEMENT ±9"
- 2 EXISTING HOT-MIX ASPHALT SURFACE ±3" (AFTER MILLING)
- 3 EXISTING COMBINATION OF CURB & GUTTER TYPE B-6.12
- 4 EXISTING CORRUGATED MEDIAN
- 5 EXISTING P.C.C. SIDEWALK
- 6 EXISTING CORRUGATED MEDIAN
- 7 EXISTING STABILIZED SUB-BASE
- 8 PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 1"
- 9 PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 2 1/2"
- 10 PROPOSED POLYMERIZED LEVELING BINDER, (MACHINE METHOD), IL-4.75, N50, 3/4"
- 11 PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70, 1"
- 12 PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70, 1 3/4"
- R CURB AND GUTTER REMOVAL AND REPLACEMENT (AS DIRECTED BY THE ENGINEER)

MIXTURE REQUIREMENTS

MIXTURE TYPE	AIR VOIDS @ Ndes	OMP
PATCHING		
CLASS "D" PATCHES (HMA BINDER COURSE, IL-19MM)	4% @ 70 GYR.	OC / OA
PAVEMENT RESURFACING		
POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70 (IL 9.5MM)	4% @ 70 GYR.	OCP
POLYMERIZED LEVELING BINDER (MACHINE METHOD), IL-4.75, N50	3.5% @ 50 GYR.	OCP
OMP DESIGNATION: QUALITY CONTROL FOR PERFORMANCE (OCP); QUALITY CONTROL / QUALITY ASSURANCE (OC / OA); PAY FOR PERFORMANCE (PFP)		

NOTES:

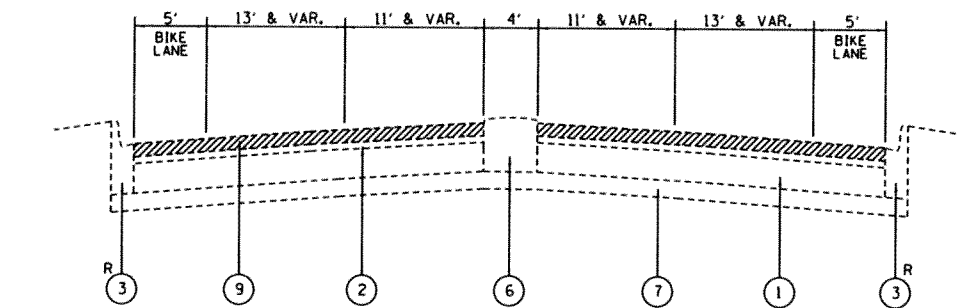
THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT QUANTITIES IS 112 LBS./SQ. YD./ IN.

THE "AC TYPE" FOR POLYMERIZED HMA MIXES SHALL BE "SBS/SBR PG 76 -22" AND FOR NON-POLYMERIZED HMA THE "AC TYPE" SHALL BE "PG 64 -22" UNLESS MODIFIED BY DISTRICT ONE SPECIAL PROVISIONS.

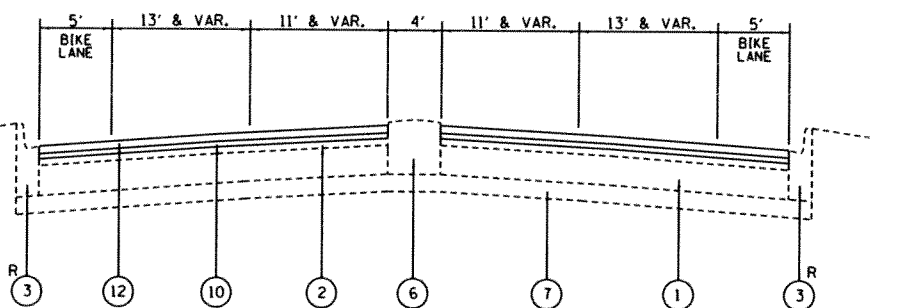
THE MILLING SHALL BE DONE PRIOR TO PATCHING

QUALITY MANAGEMENT PROGRAM (OMP) IDENTIFIES THE PARTICULAR QUALITY CONTROL SPECIFICATION THAT APPLIES TO THE HMA MIXTURE

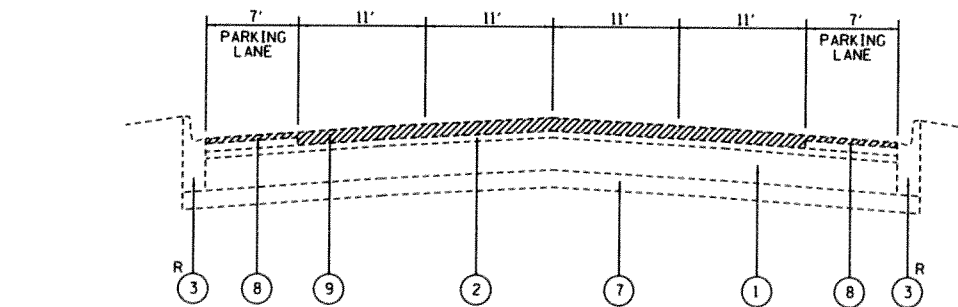
FOR USE OF RECYCLED MATERIALS SEE SPECIAL PROVISIONS



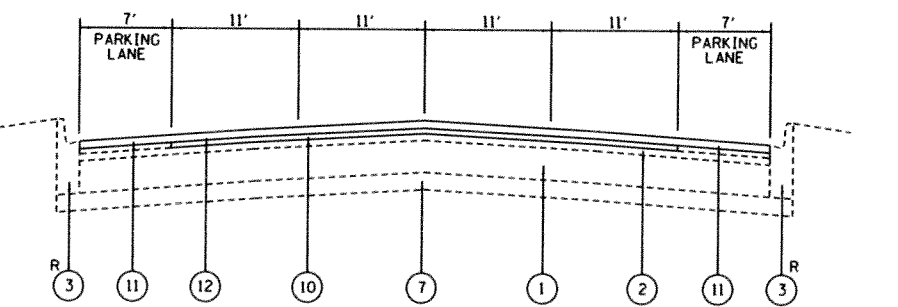
EXISTING TYPICAL SECTION
STA. 189+88 TO 190+49
STA. 196+38 TO 197+71



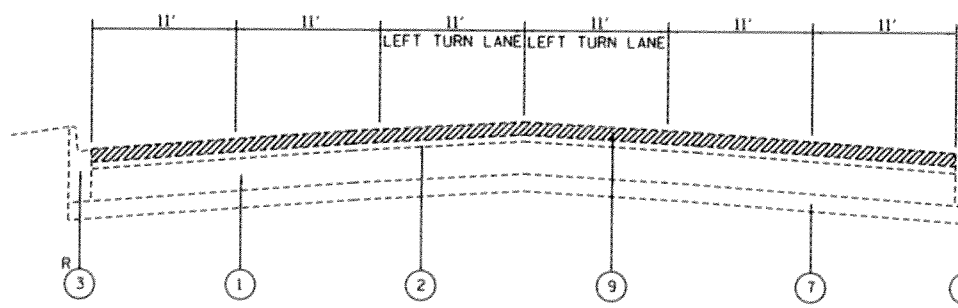
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STA. 196+38 TO 197+71



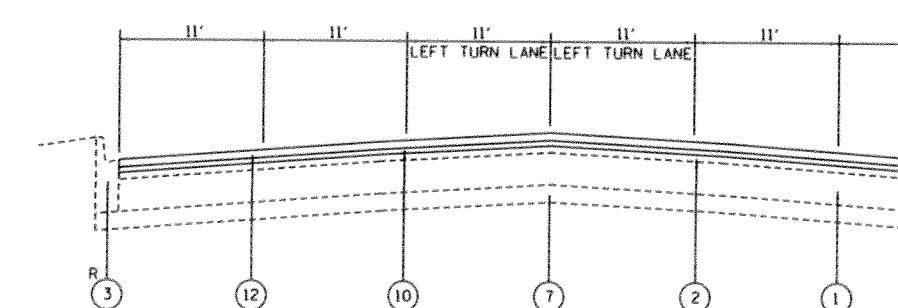
EXISTING TYPICAL SECTION
STA. 101+77 TO 110+89
STA. 127+87 TO 137+87
STA. 160+90 TO 164+53
STA. 168+22 TO 177+40



PROPOSED TYPICAL SECTION
STA. 101+77 TO 110+89
STA. 127+87 TO 137+87
STA. 160+90 TO 164+53
STA. 168+22 TO 177+40



EXISTING TYPICAL SECTION
STA. 114+03 TO 114+96

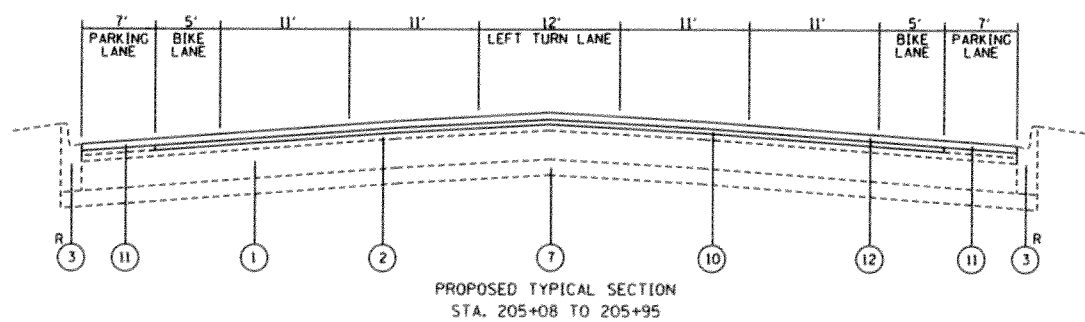
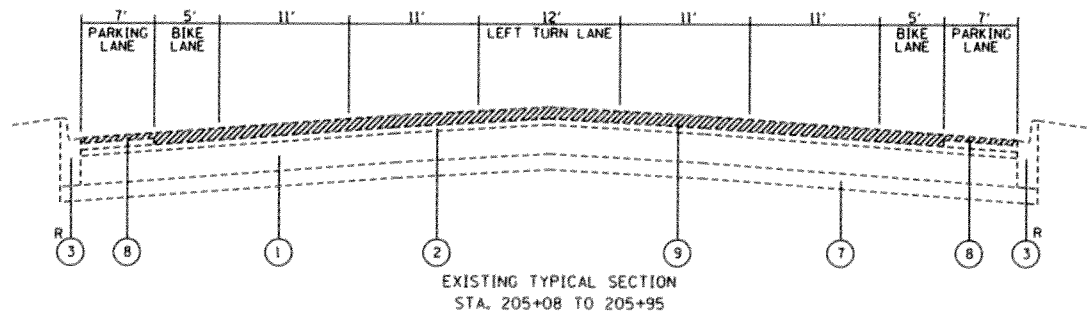
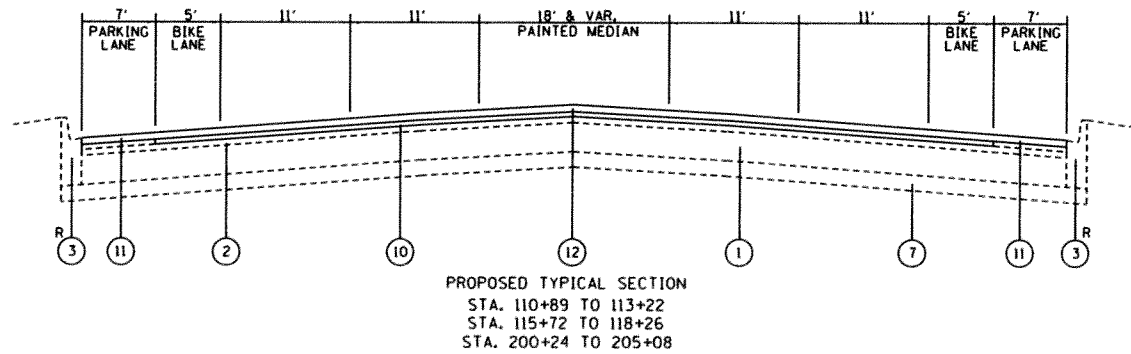
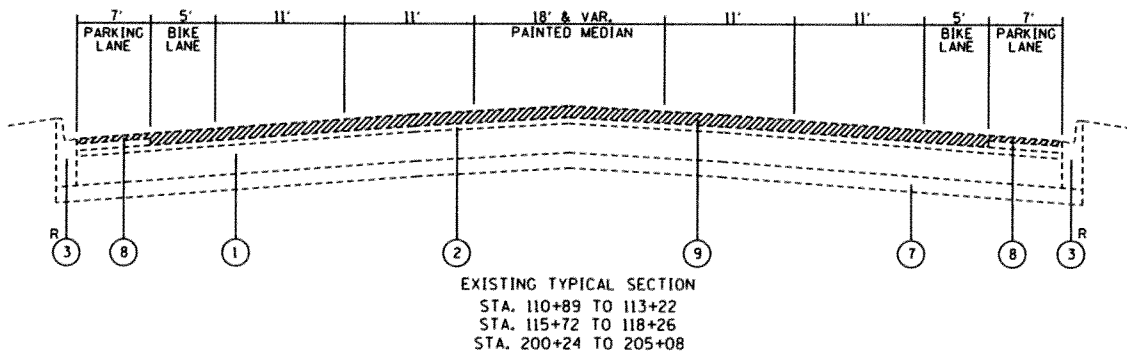
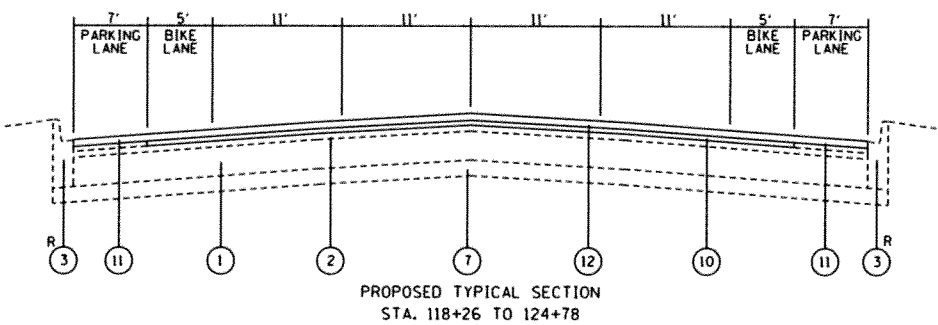
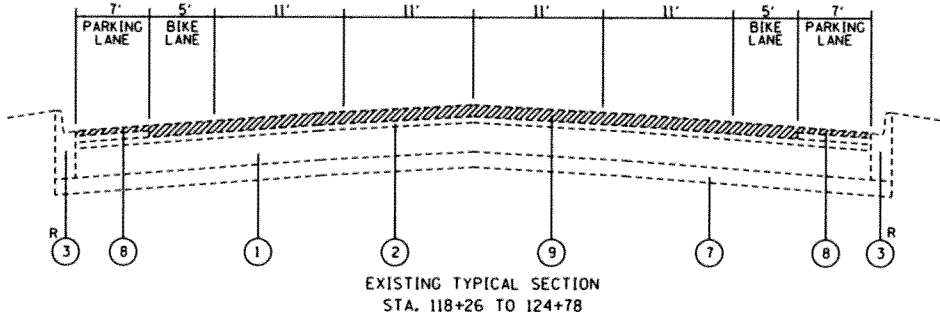


PROPOSED TYPICAL SECTION
STA. 114+03 TO 114+96

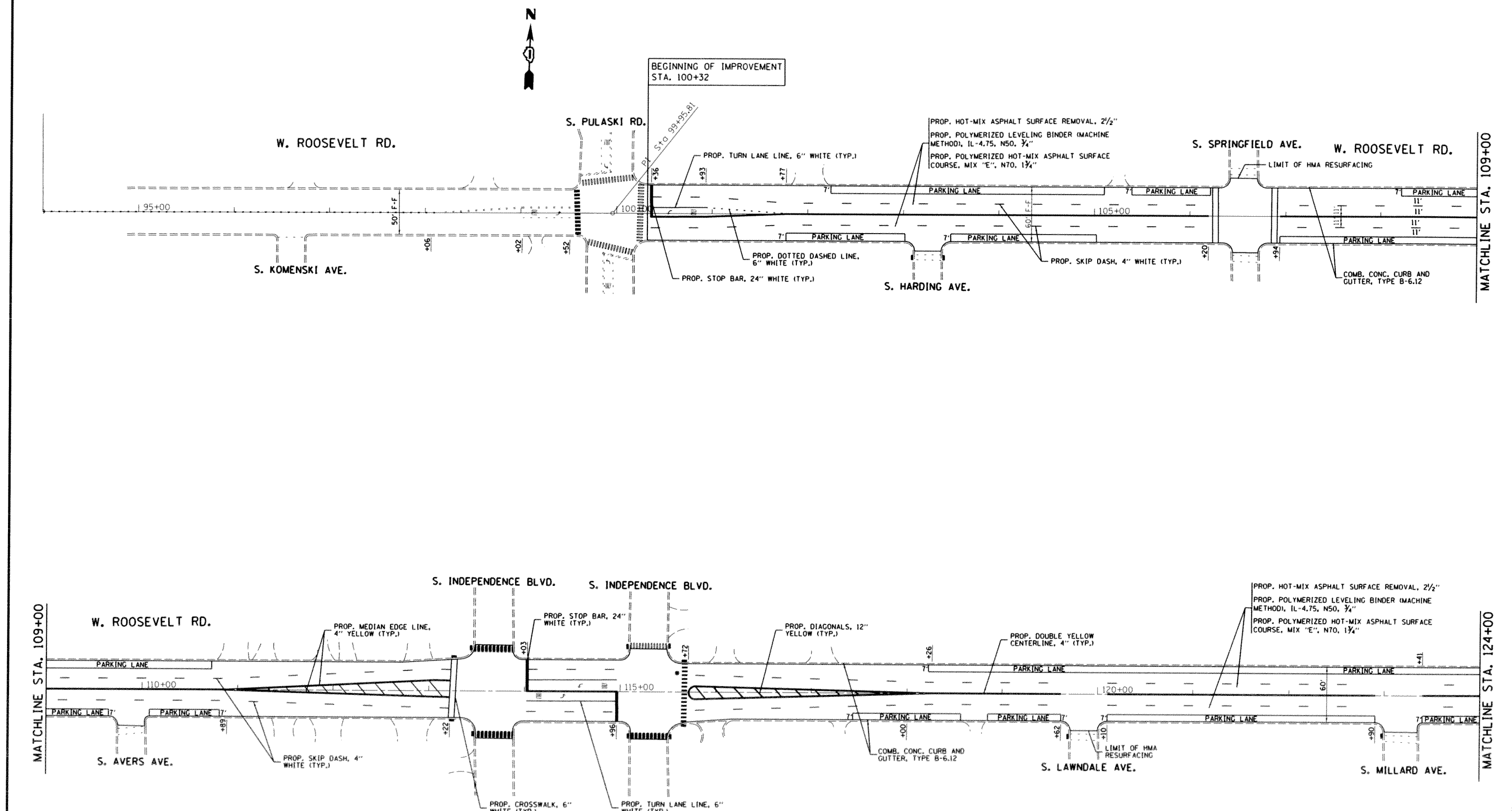
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Default		CHECKED =	REVISED =					CONTRACT NO. 62B05				
		DATE =	REVISED =					ILLINOIS FED. AID PROJECT				
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LEGEND:

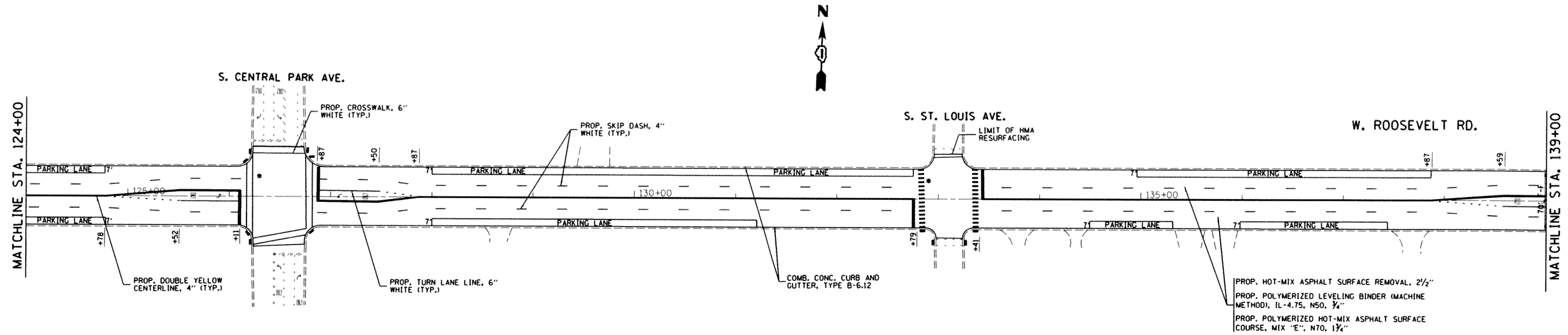
- ① EXISTING P.C.C. PAVEMENT ±9"
- ② EXISTING HOT-MIX ASPHALT SURFACE ±3" (AFTER MILLING)
- ③ EXISTING COMBINATION OF CURB & GUTTER TYPE B-6.12
- ④ EXISTING CORRUGATED MEDIAN
- ⑤ EXISTING P.C.C. SIDEWALK
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- ⑧ PROPOSED HOT-MIX ASPHALT SURFACE REMOVAL, 1"
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- ⑪ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70, 1"
- ⑫ PROPOSED POLYMERIZED HOT-MIX ASPHALT SURFACE COURSE, MIX "E", N70, 1 3/4"
- R CURB AND GUTTER REMOVAL AND REPLACEMENT
(AS DIRECTED BY THE ENGINEER)



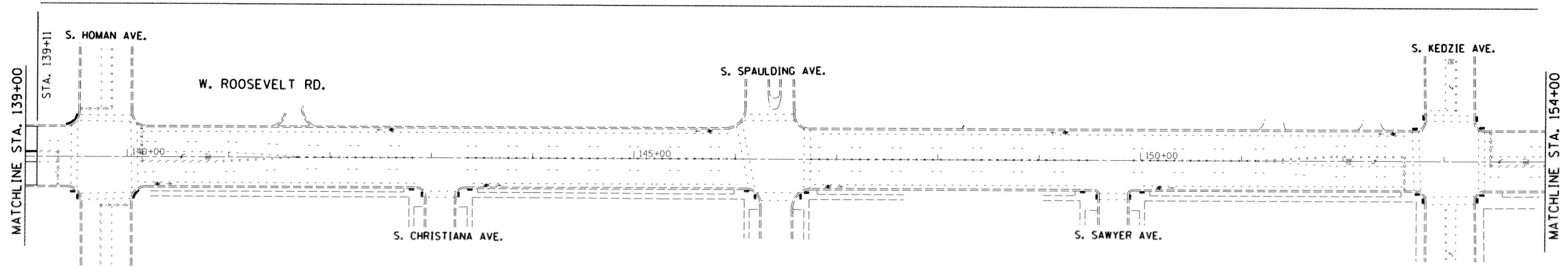
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Default	PLOT DATE = 10/21/2015	DATE -	REVISED -							
						SCALE:	SHEET	OF	SHEETS	STA.
										CONTRACT NO. 62805
ILLINOIS FED. AID PROJECT										



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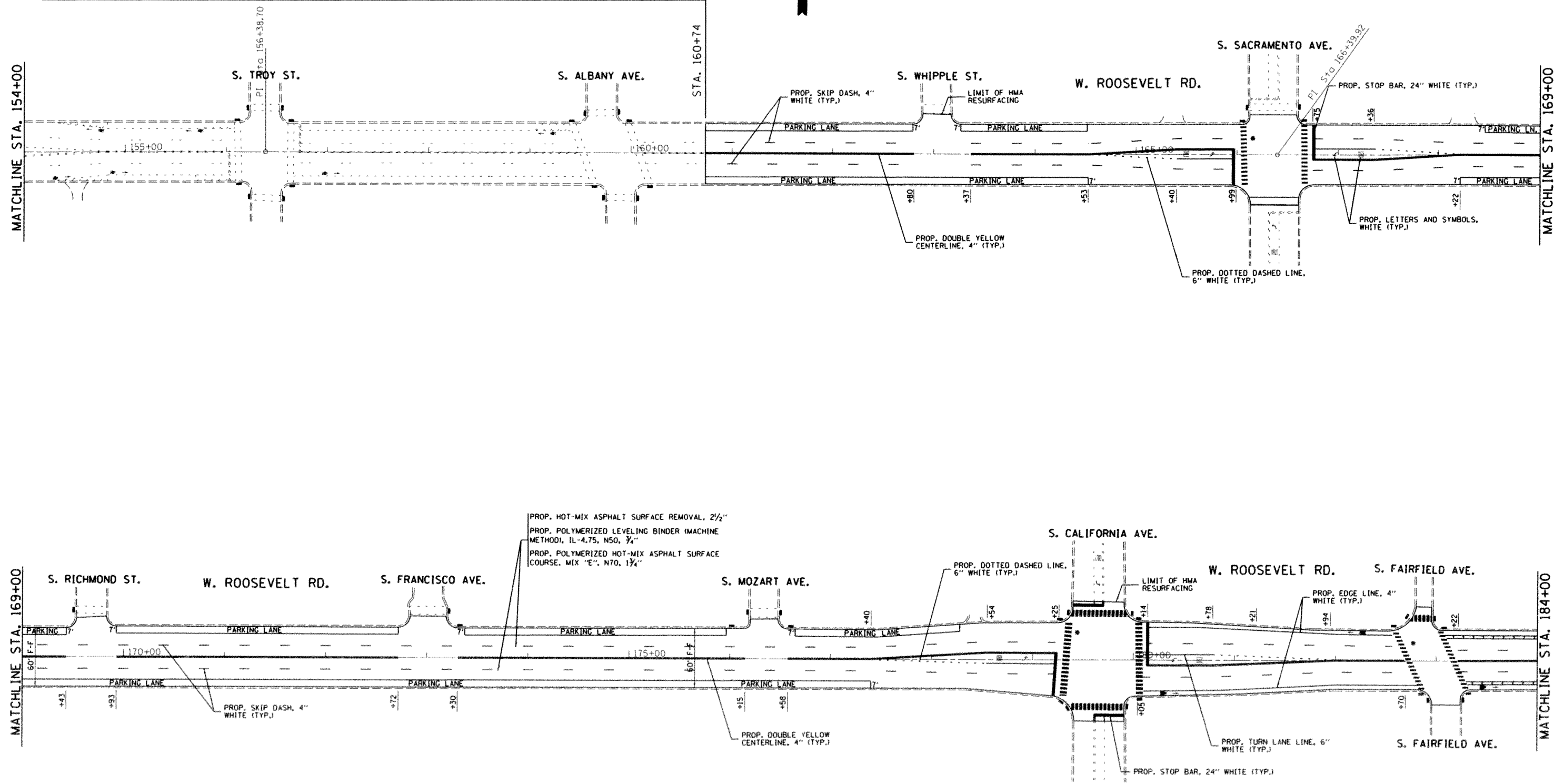
OMISSION FOR HMA RESURFACING



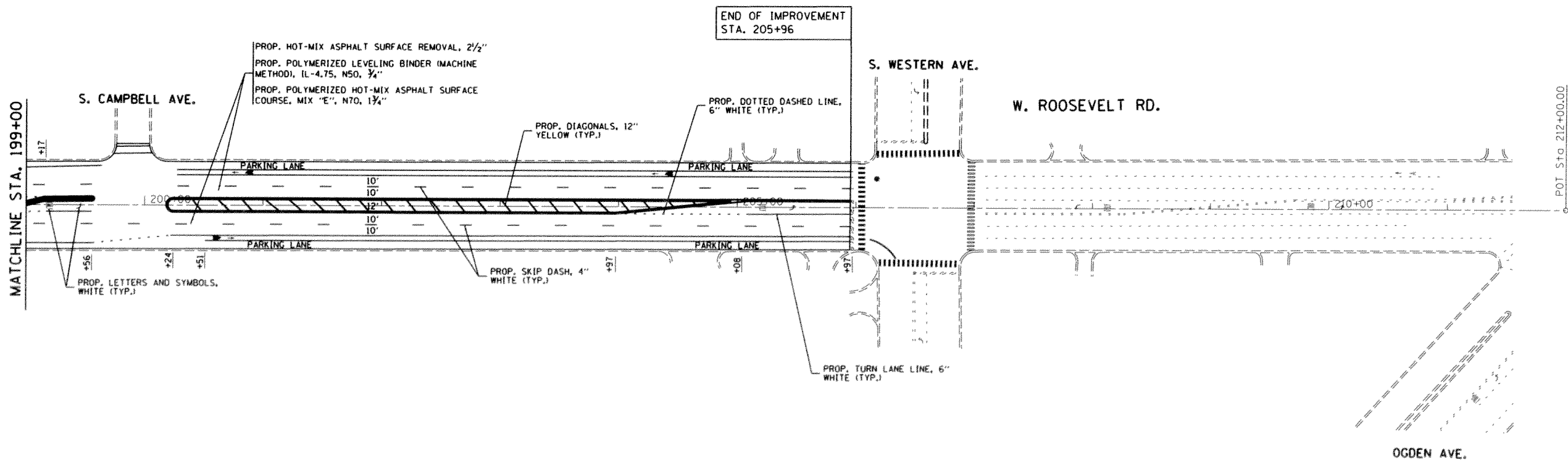
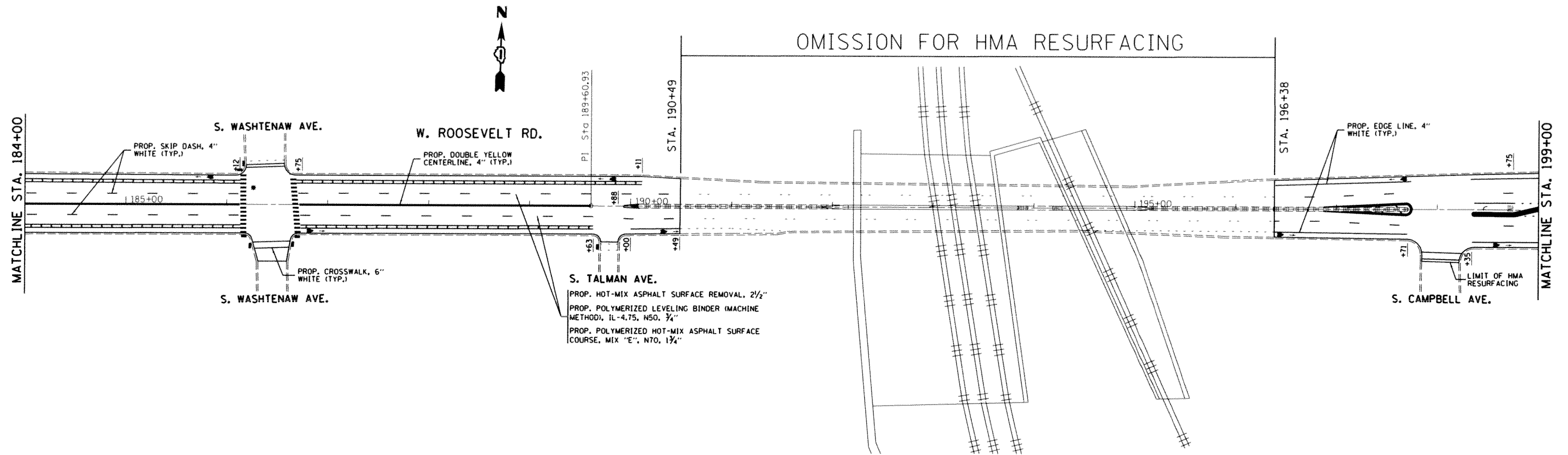
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Default	PLOT DATE = 10/22/2015		CHECKED -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.		CONTRACT NO. 62B05				
			DATE -	REVISED -				ILLINOIS FED. AID PROJECT				



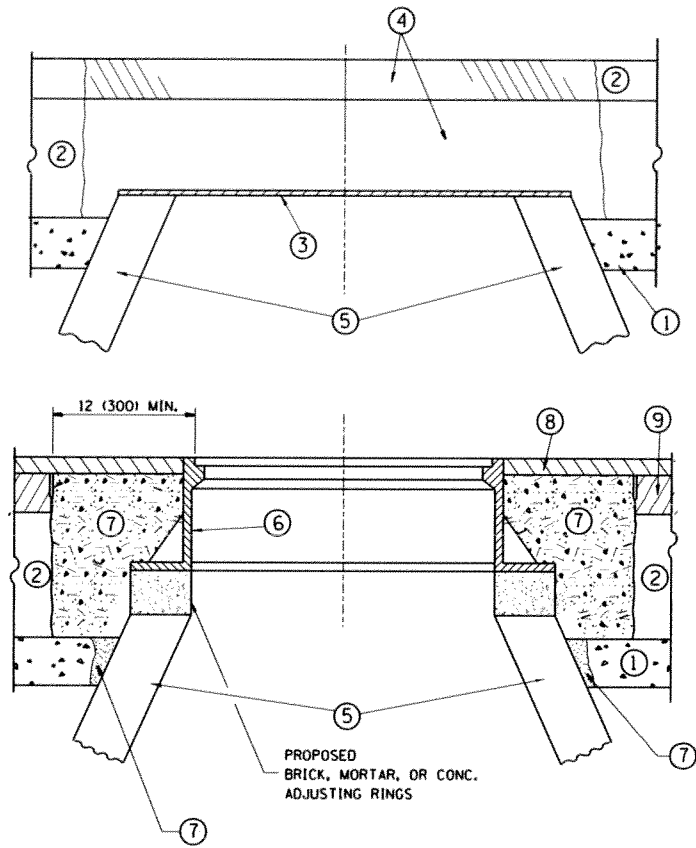
OMISSION FOR HMA RESURFACING



FILE NAME =		USER NAME = STEEDPA	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		ROADWAY AND PAVEMENT MARKING PLANS FAP 347 (ROOSEVELT RD.)—PULASKI RD. TO WESTERN AVE.		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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			DATE -	REVISED -					ILLINOIS FED. AID PROJECT				
SCALE:		SHEET		OF		SHEETS		STA.		TO STA.			



FILE NAME =	USER NAME = STEEDPA	DESIGNED -	REVISED -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ROADWAY AND PAVEMENT MARKING PLANS				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
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	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -						CONTRACT NO. 62805				
Default	PLOT DATE = 10/22/2015	DATE -	REVISED -		SCALE: SHEET OF SHEETS STA. TO STA.				ILLINOIS FED. AID PROJECT				



NOTES:

EXISTING BROKEN FRAMES AND LIDS SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR AND SHALL BE REPLACED AS DIRECTED BY THE ENGINEER. REPLACEMENT FRAMES AND LIDS WILL BE PAID FOR IN ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS UNLESS A SEPARATE PAY ITEM HAS BEEN PROVIDED.

IF THE EXISTING LIDS ARE OPEN, THE FRAME WILL BE ADJUSTED TO THE ELEVATION OF THE MILLED PAVEMENT SURFACE PRIOR TO THE MILLING OPERATION. THE FRAME WILL NOT BE REMOVED AND COVERED BY THE METAL PLATE.

CITY OF CHICAGO CASTINGS ARE THE PROPERTY OF THE CITY AND THE CONTRACTOR SHALL NOTIFY THE CITY FOR REMOVAL AND DISPOSITION OF THE CASTINGS.

THE METAL PLATE USED TO COVER THE STRUCTURE SHALL REMAIN THE PROPERTY OF THE CONTRACTOR.

WHEN STRUCTURES ARE TO BE ADJUSTED OR RECONSTRUCTED, THE LOWERING AND RAISING OF THE FRAMES AND LIDS WILL NOT BE PAID FOR SEPARATELY BUT WILL BE INCLUDED IN THE COST OF THE CORRESPONDING PAY ITEM.

CONSTRUCTION PROCEDURES

STAGE 1 (BEFORE PAVEMENT MILLING)

- A) REMOVE A MINIMUM OF 12 (300) OF THE PAVEMENT FROM AROUND THE STRUCTURE.
- B) REMOVE THE EXISTING FRAME AND LID FROM THE STRUCTURE.
- C) COVER THE STRUCTURE OPENING WITH A 36 (900) DIAMETER METAL PLATE.
- D) BACKFILL WITH CRUSHED STONE AND A MINIMUM 1 1/2 (40) THICK HMA SURFACE MIX APPROVED BY THE ENGINEER.

STAGE 2 (AFTER PAVEMENT MILLING)

- A) REMOVE THE HMA SURFACE MIX AND CRUSHED STONE.
 - B) INSTALL THE FRAME AND LID; ADJUST THE FRAME TO ITS FINAL SURFACE ELEVATION.
 - C) THE SURROUNDING SPACE SHALL BE FILLED WITH CLASS PP-1* CONCRETE TO THE ELEVATION OF THE SURFACE OF THE EXISTING BASE COURSE OR THE BINDER COURSE.
- * UNLESS OTHERWISE SPECIFIED IN THE PLANS.

THE PROCEDURE EXPLAINED ABOVE SHALL CONFORM TO THE APPLICABLE PORTIONS OF SECTIONS 353, 406, 602, AND 603 OF THE STANDARD SPECIFICATIONS EXCEPT THAT "THE CONTRACTOR SHALL ADJUST THE STRUCTURES TO THE FINISHED PAVEMENT ELEVATION NO MORE THAN 5 CALENDAR DAYS PRIOR TO PLACEMENT OF THE FINAL LIFT OF SURFACE UNLESS APPROVED BY THE ENGINEER."

LEGEND

- 1 SUB-BASE GRANULAR MATERIAL
- 2 EXISTING PAVEMENT
- 3 36 (900) DIAMETER METAL PLATE
- 4 PROPOSED CRUSHED STONE AND HMA SURFACE MIX
- 5 EXISTING STRUCTURE
- 6 FRAME AND LID (SEE NOTES)
- 7 CLASS PP-1* CONCRETE
- 8 PROPOSED HMA SURFACE COURSE
- 9 PROPOSED HMA BINDER COURSE

LOCATION OF STRUCTURES:

THE CONTRACTOR WILL BE REQUIRED TO KEEP A RECORD OF THE LOCATIONS OF THE BURIED STRUCTURES ACCORDING TO THE STATION AND DISTANCE LEFT OR RIGHT OF THE CENTERLINE OF PAVEMENT. UPON COMPLETION OF THE WORK, THE CONTRACTOR WILL DELIVER THE RECORD TO THE ENGINEER.

BASIS OF PAYMENT:

REMOVING FRAMES AND LIDS ON DRAINAGE AND UTILITY STRUCTURES IN THE PAVEMENT PRIOR TO MILLING, AND ADJUSTING TO FINAL GRADE PRIOR TO PLACING THE SURFACE COURSE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE EACH FOR "FRAMES AND LIDS TO BE ADJUSTED (SPECIAL)."

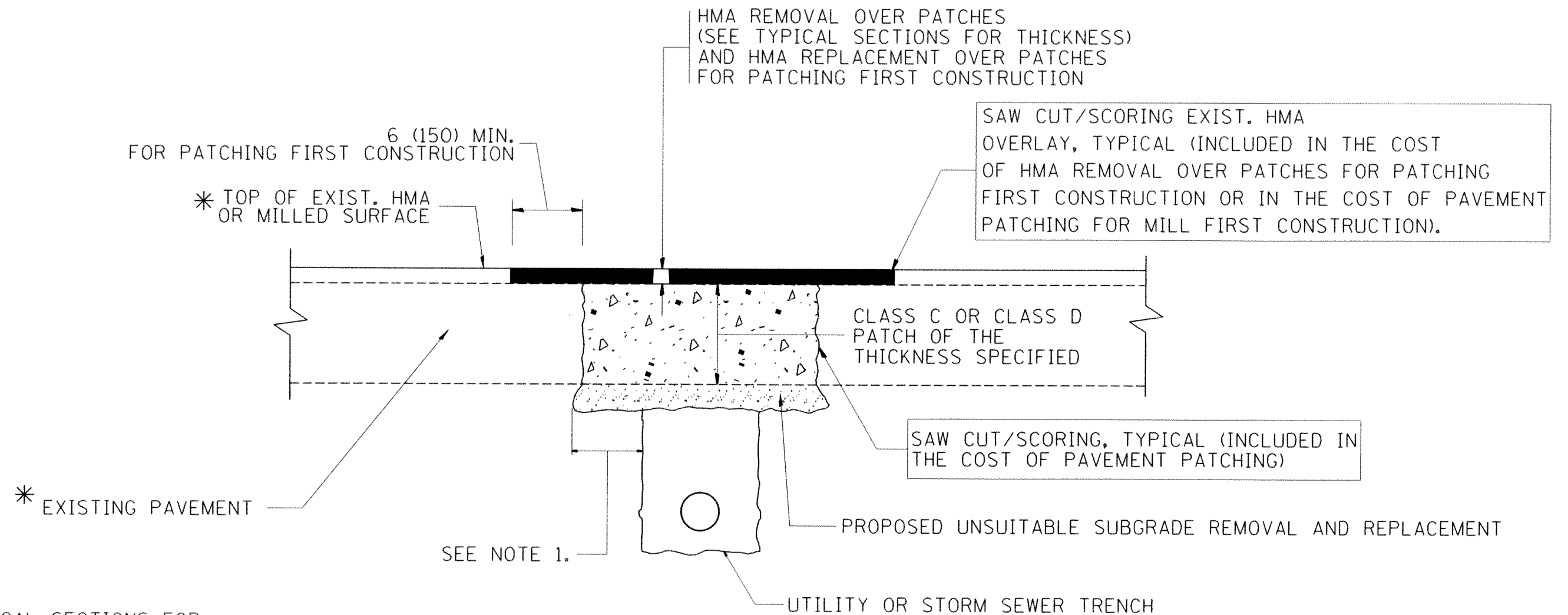
THIS WORK WILL NOT BE PAID FOR WHEN DRAINAGE AND UTILITY STRUCTURES ARE SPECIFIED FOR PAYMENT AS STRUCTURE RECONSTRUCTION.

NEW FRAMES AND LIDS, WHEN SPECIFIED, WILL BE PAID FOR SEPARATELY.

DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN

FILE NAME =	USER NAME = STEEDPA	DESIGNED - R. SHAH	REVISED - R. WIEDEMAN 05-14-04	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION		DETAILS FOR FRAMES AND LIDS ADJUSTMENT WITH MILLING		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
ci:\pw\work\p\dot\steadpa\d0441375\DistStd.dgn		DRAWN -	REVISED - R. BORO 01-01-07					347	2015-045R5	COOK	24	11
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - R. BORO 03-09-11						BD600-03 (BD-8)			
	PLOT DATE = 10/21/2015	DATE - 10-25-94	REVISED - R. BORO 12-06-11									CONTRACT NO. 62805
						SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT		



* SEE TYPICAL SECTIONS FOR THICKNESS AND MATERIALS

NOTES:

1. THE WIDTH OF THE FULL DEPTH PATCH OVER A TRENCH SHALL BE 12 (300) WIDER ON EACH SIDE OF THE TRENCH.
2. FOR METHOD OF MEASUREMENT AND BASIS OF PAYMENT, SEE RECURRING SPECIAL PROVISION "PATCHING WITH HOT-MIX ASPHALT OVERLAY REMOVAL".

SEQUENCE OF CONSTRUCTION (PATCHING FIRST)

1. REMOVE THE EXISTING HMA MATERIAL OVER THE AREA TO BE PATCHED.
2. REMOVE AND REPLACE WITH CLASS C OR D PATCH.
3. REPLACE HMA MATERIAL OVER THE AREA TO BE PATCHED.

SEQUENCE OF CONSTRUCTION (MILLING FIRST)

1. MILL HMA FIRST IF THERE IS AT LEAST 4 1/2 INCHES OR MORE OF HMA MATERIAL ON TOP OF THE EXISTING PAVEMENT OR IF THE PAVEMENT IS FULL DEPTH HMA. A MINIMUM OF 2 INCHES OF HMA MATERIAL SHALL BE IN PLACE AFTER MILLING.
2. REMOVE AND REPLACE WITH FULL DEPTH CLASS D PATCHES TO TOP OF MILLED SURFACE.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = STEEDPA	DESIGNED - R. SHAH	REVISED - A. ABBAS 04-27-98	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT PATCHING FOR HMA SURFACED PAVEMENT		F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\p\work\p\dot\steedpa\0441375\DistStd.dgn		DRAWN -	REVISED - R. BORO 01-01-07				347	2015-045RS	COOK	24	12
PLOT SCALE = 100.0000' / 1"		CHECKED -	REVISED - R. BORO 09-04-07				BD400-04 (BD-22)		CONTRACT NO. 62B05		
PLOT DATE = 10/21/2015		DATE - 10-25-94	REVISED - K. ENG 10-27-08		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	

VARIABLE - TO MEET EXISTING
DIMENSIONS AND FIELD CONDITIONS
(SEE NOTE ②)

PROP. CONC. CURB OR CURB AND GUTTER REPLACEMENT
IN ACCORDANCE WITH STATE STANDARD 606001. (SEE NOTE ②)

SAW CUT FULL DEPTH - INCLUDED IN THE COST OF
SIDEWALK, DRIVEWAY OR MEDIAN SURFACE REMOVAL
PAY ITEM.

SEE STATE STANDARD 606001

18" (450)
MAX.

EXISTING OR PROPOSED HMA SURFACE (IF APPLICABLE)

1/4" (5) **

EXISTING SIDEWALK, DRIVEWAY, MEDIAN SURFACE, SOD OR GROUND.

PROPOSED SIDEWALK, DRIVEWAY PAVEMENT, MEDIAN
SURFACE OR SODDING SALT TOLERANT WITH TOP SOIL, 4" (100)
SOD RESTORATION (SEE NOTE ①).

SUITABLE BACKFILL MATERIAL
(INCLUDED IN THE COST OF CURB OR CURB
AND GUTTER REMOVAL AND REPLACEMENT)

PROPOSED 3/4" (20) PREFORMED EXPANSION JOINT AT CONCRETE
SIDEWALKS, DRIVEWAYS, AND MEDIANS. (INCLUDED IN THE COST
OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.)

UNSUITABLE SUB-BASE MATERIAL TO BE REMOVED, IF DIRECTED BY
THE ENGINEER, SHALL BE REPLACED WITH EITHER SUB-BASE GRANULAR
MATERIAL, TYPE B OR ADDITIONAL THICKNESS OF CONCRETE.

REMOVAL AND REPLACEMENT 4" (100) OR LESS IS INCLUDED IN THE
COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

REMOVAL AND REPLACEMENT IN EXCESS OF 4" (100) WILL BE PAID FOR IN
ACCORDANCE WITH ARTICLE 109.04 OF THE STANDARD SPECIFICATIONS.

PROPOSED #6 (20) EPOXY COATED TIE BARS 24" (600) LONG AT
24" (600) CENTERS WILL NOT BE PAID FOR SEPARATELY. DELETE EPOXY
COATED TIE BARS IF EXISTING TIE BARS ARE USUABLE AS DETERMINED
BY THE ENGINEER. (SEE NOTE ③).

BASIS OF PAYMENT:

THIS WORK WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER
FOOT (METER) FOR "CURB REMOVAL AND REPLACEMENT" OR
"COMBINATION CONCRETE CURB AND GUTTER REMOVAL AND REPLACEMENT".

* 3" (75) MINIMUM FROM TOP AND BOTTOM OF THE CONCRETE PAVEMENT OR BASE COURSE.

* * IF THE FINAL SURFACE OF THE PAVEMENT IS CONCRETE, THE GUTTER IS TO BE FLUSH
WITH THE PAVEMENT.

NOTE: ① SIDEWALK, DRIVEWAY PAVEMENT OR MEDIAN SURFACE SHALL BE SIMILAR TO THE MATERIAL
BEING REMOVED AND WILL BE PAID FOR SEPARATELY.

SODDING, SALT TOLERANT AND TOP SOIL, FURNISH AND PLACE 4" WILL BE PAID FOR SEPARATELY.

② FERTILIZER FOR THE PLACEMENT OF THE SOD IS NOT REQUIRED

③ CURB OR CURB AND GUTTER REPLACEMENT SHALL MATCH THE SHAPE OF THE EXISTING
CURB OR CURB AND GUTTER UNLESS OTHERWISE SPECIFIED.

④ FOR CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT ADJACENT TO FLEXIBLE
PAVEMENT DELETE EPOXY COATED TIE BARS.

⑤ LONGITUDINAL BARS, IF ENCOUNTERED IN THE EXISTING CURB OR CURB AND GUTTER, ARE
NOT TO BE REPLACED. CUTTING AND REMOVING LONGITUDINAL BARS SHALL BE INCLUDED
IN THE COST OF CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT.

⑥ THE COST OF HMA SURFACE REMOVAL IN THE EXISTING GUTTER FLAG SHALL BE
INCLUDED IN THE COST OF THE CURB AND GUTTER REMOVAL AND REPLACEMENT.

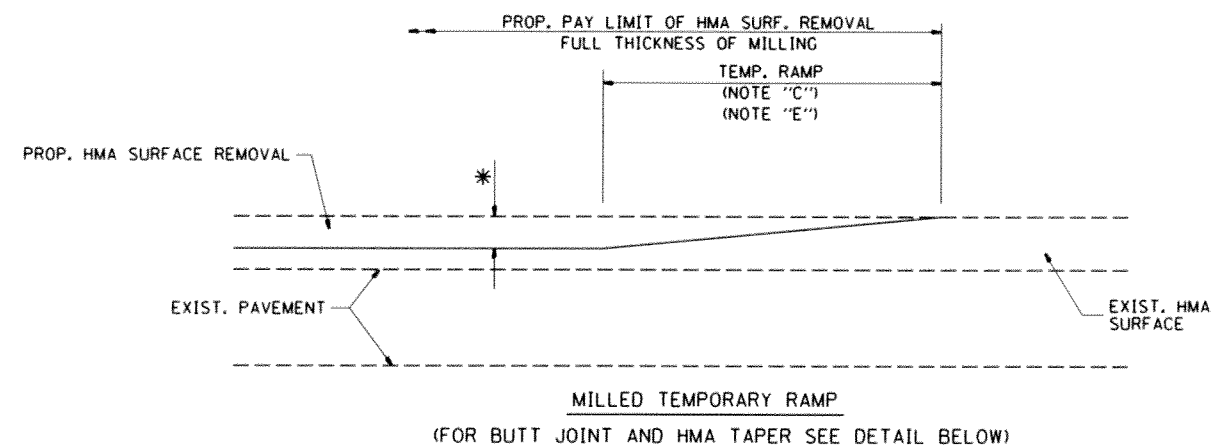
⑦ THE REMOVAL AND REPLACEMENT OF THE EXISTING CURB OR CURB AND GUTTER SHALL
BE DONE IN ACCORDANCE WITH THE APPLICABLE PORTIONS OF SECTION 440 AND 606
OF THE STANDARD SPECIFICATIONS.

⑧ THE LOCATIONS OF REMOVAL AND REPLACEMENT OF EXISTING CURB OR CURB AND GUTTER
SHALL BE DETERMINED BY THE RESIDENT ENGINEER AT THE TIME OF CONSTRUCTION.

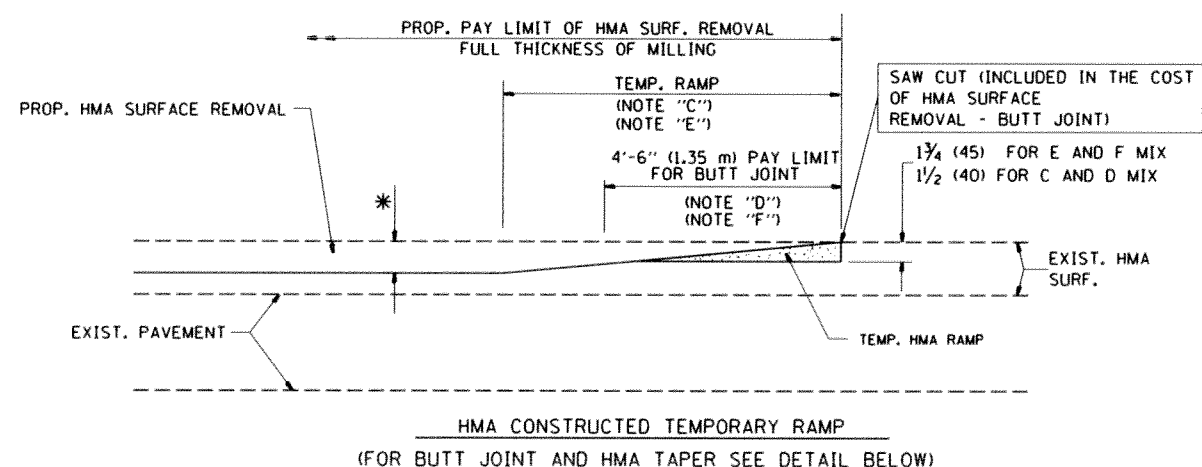
CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = STEEDPA	DESIGNED - A. HOUSEH	REVISED - R. SHAH 10-03-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	CURB OR CURB AND GUTTER REMOVAL AND REPLACEMENT	F.A.P. RTE.		SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
01:\pw_work\p\dot\stedpa\0441375\DistStd.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97			347	2015-045RS	COOK	24	13	
PLOT SCALE = 100.0000' / 1"		CHECKED -	REVISED - M. GOMEZ 01-22-01			BD600-06 (BD-24)		CONTRACT NO. 62B05			
PLOT DATE = 10/21/2015		DATE - 03-11-94	REVISED - R. BORO 12-15-09			FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT					
SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA.		TO STA.					

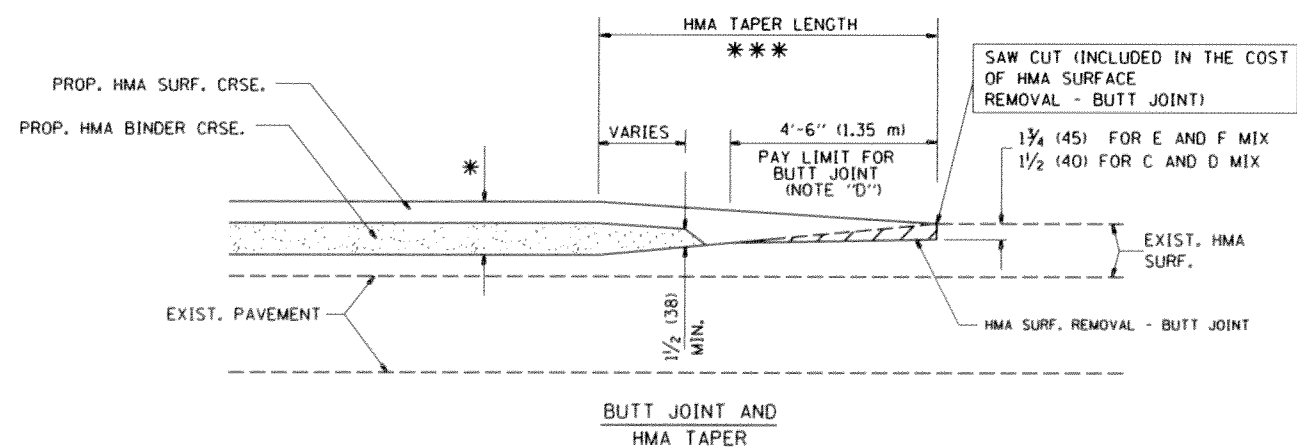


OPTION 1

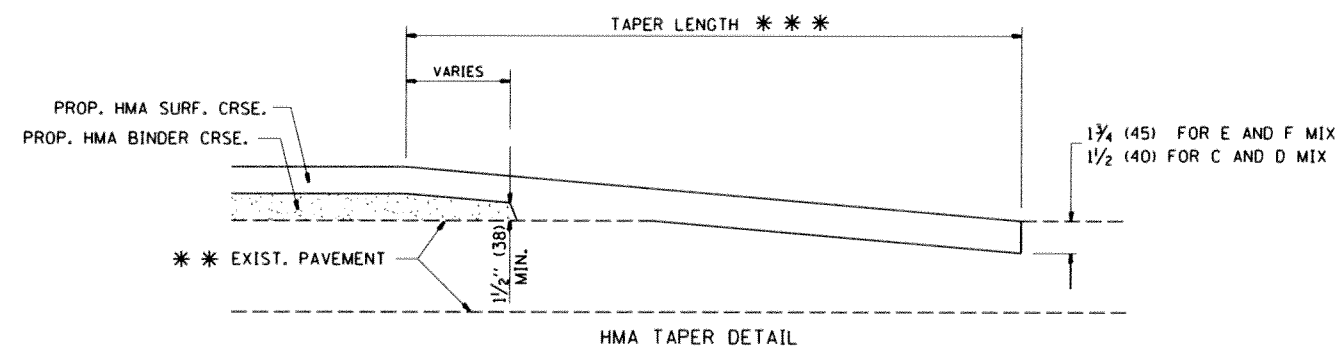
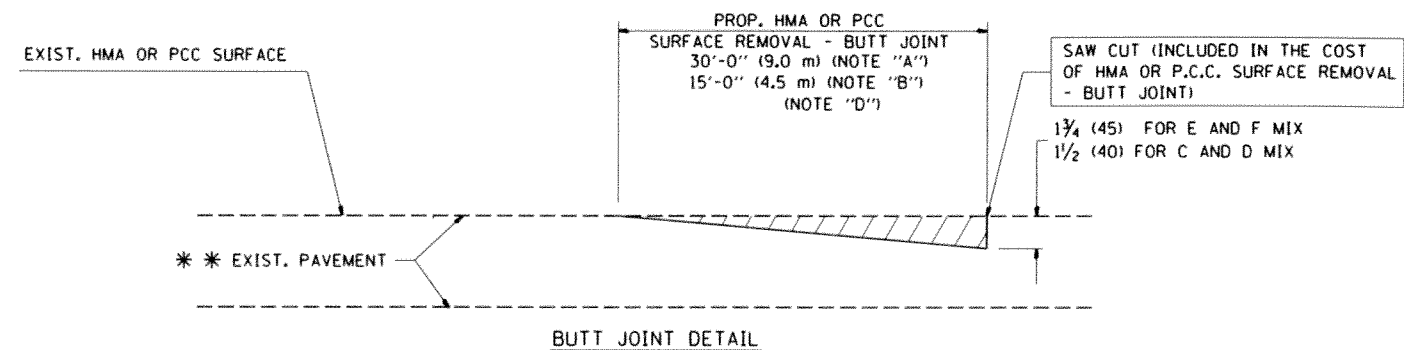


OPTION 2

TYPICAL TEMPORARY RAMP



TYPICAL BUTT JOINT AND HMA TAPER FOR MILLING AND RESURFACING



TYPICAL BUTT JOINT AND HMA TAPER FOR RESURFACING ONLY

** PC CONCRETE, HMA OR HMA RESURFACED PAVEMENT.

NOTES

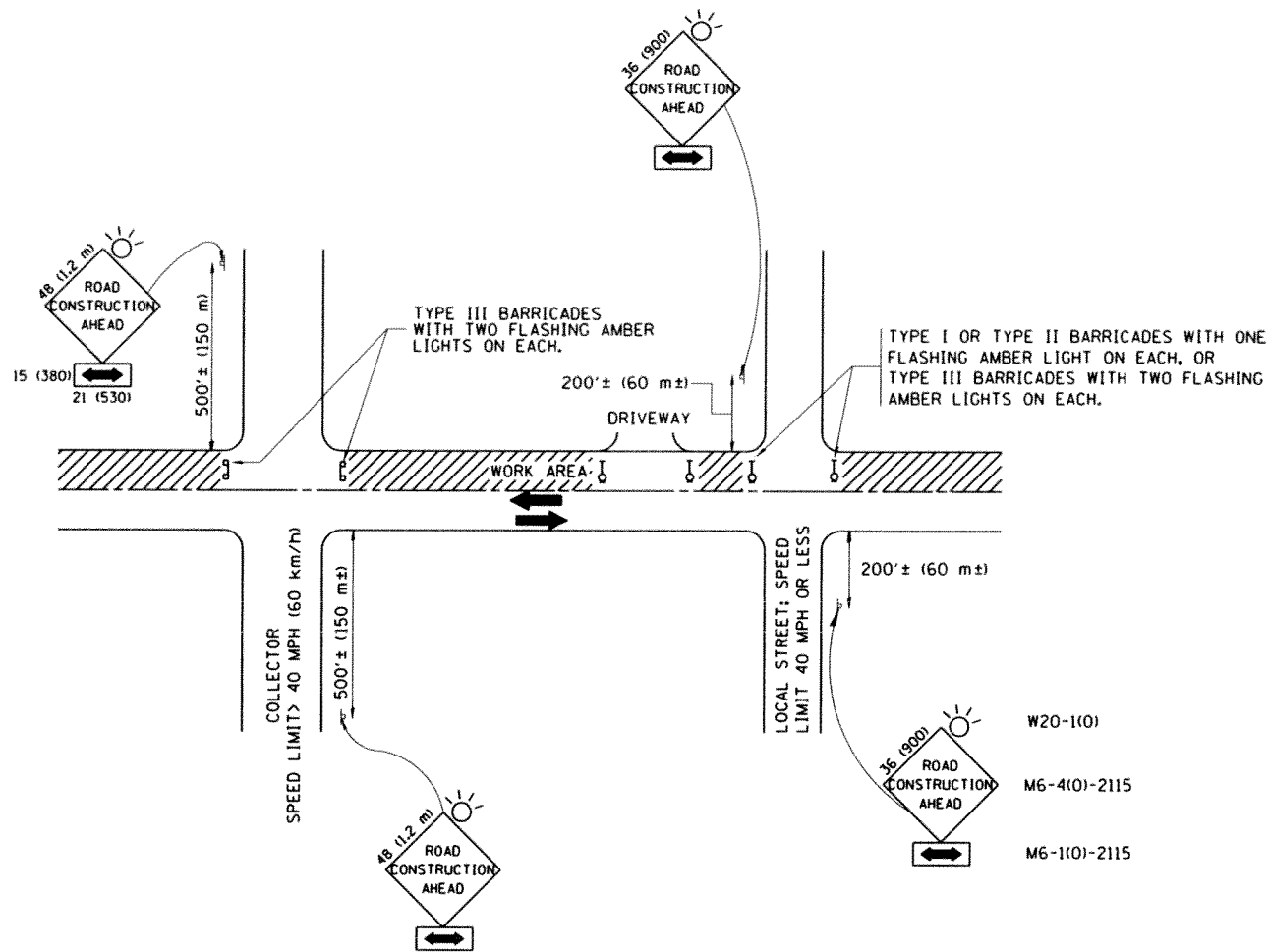
- A: MAINLINE ROADWAYS AND MAJOR SIDE ROADS.
- B: MINOR SIDE ROADS.
- C: THE TEMP. RAMP SHALL BE CONSTRUCTED IMMEDIATELY UPON REMOVAL OF THE EXISTING HMA SURFACE.
- D: THE BUTT JOINT SHALL BE CONSTRUCTED IMMEDIATELY PRIOR TO PLACING THE PROPOSED HMA COURSES.
- E: TAPER THE TEMP. RAMP AT A RATE OF 3'-0" (900 mm) PER 1 INCH (25 mm) OF MILLING THICKNESS.
- F: INSTALLATION AND REMOVAL OF THE 4'-6" (1.35 m) TEMP. RAMP IS INCLUDED IN COST OF HMA SURFACE REMOVAL - BUTT JOINT
- G: SEE ARTICLE 406.08 AND 406.14 OF THE STANDARD SPECIFICATIONS FOR "HMA AND/OR PCC SURFACE REMOVAL, BUTT JOINT".
- * SEE TYPICAL SECTIONS FOR MILLING THICKNESS.
- *** 20'-0" (6.1 m) PER 1 (25) RESURFACING (NOTE "A")
10'-0" (3.0 m) PER 1 (25) RESURFACING (NOTE "B")

BASIS OF PAYMENT:

THE BUTT JOINT WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER SQUARE YARD (SQUARE METER) FOR "HOT-MIX ASPHALT SURFACE REMOVAL - BUTT JOINT" OR FOR "PORTLAND CEMENT CONCRETE SURFACE REMOVAL - BUTT JOINT".

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS) UNLESS OTHERWISE SHOWN.

FILE NAME =	USER NAME = STEEDPA	DESIGNED - M. DE YONG	REVISED - R. SHAH 10-25-94	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	BUTT JOINT AND HMA TAPER DETAILS			F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
c:\p\work\pwsd\steedpa\0441375\Dist\std.dgn		DRAWN -	REVISED - A. ABBAS 03-21-97					347	2015-045RS	COOK	24	14
PLOT SCALE = 100.0000' / 1"		CHECKED -	REVISED - M. GOMEZ 04-06-01					BD400-05 BD32		CONTRACT NO. 62B05		
PLOT DATE = 10/21/2015		DATE - 06-13-90	REVISED - R. BORO 01-01-07					FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				
		SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA.		TO STA.				



TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS

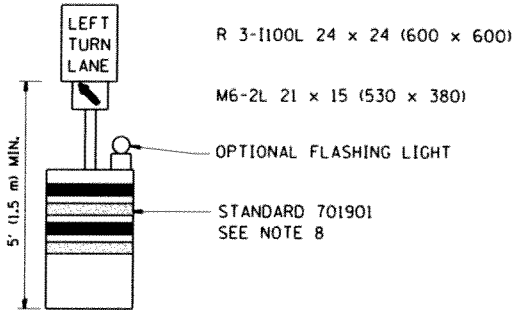
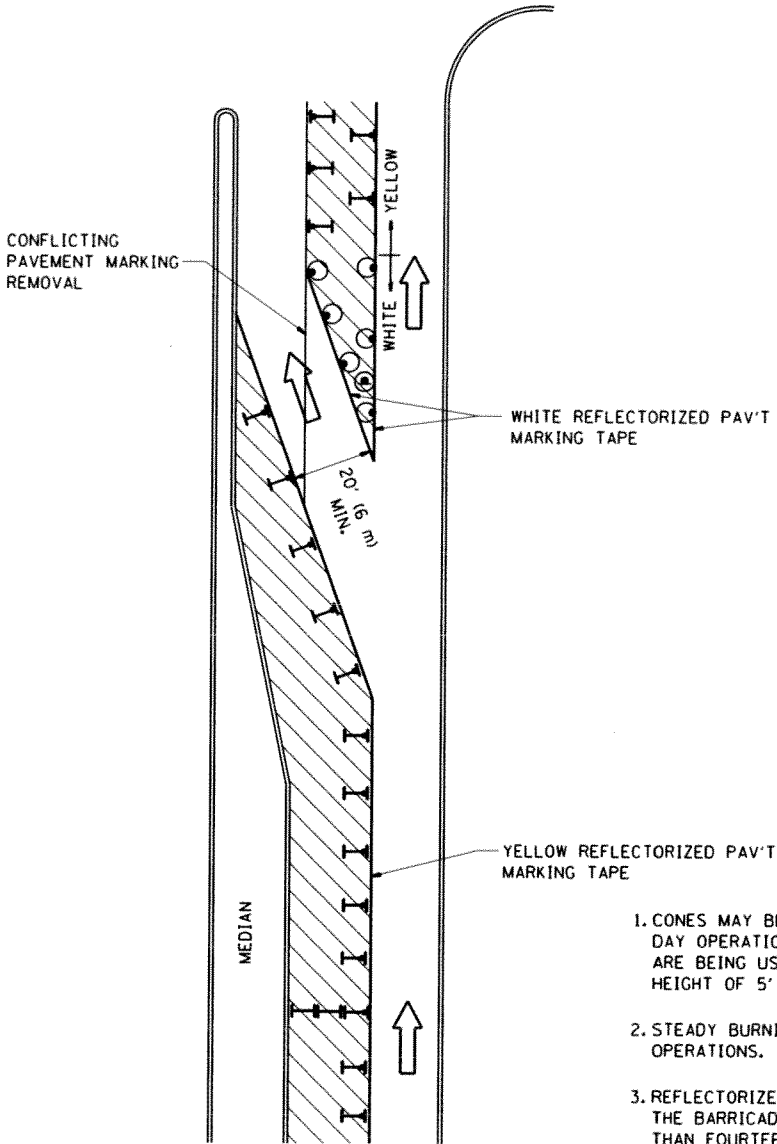
- SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE I, TYPE II OR TYPE III BARRICADES, 1/3 OF THE CROSS SECTION OF THE CLOSED PORTION.
- SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
 - ONE ROAD CONSTRUCTION AHEAD SIGN 48 x 48 (1.2 m x 1.2 m) WITH A FLASHER MOUNTED ON IT APPROXIMATELY 500' (150 m) IN ADVANCE OF THE MAIN ROUTE.
 - THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY BLOCKING WITH TYPE III BARRICADES, 1/2 OF THE CROSS SECTION OF THE CLOSED PORTION.
- WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:

- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME =	USER NAME = STEEDPA	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
cd\pw\work\pwsdot\steadpa\0441375\DistStd.dgn		DRAWN -	REVISED - A. HOUSEH 03-06-96		347	2015-045RS	COOK	24	15				
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - A. HOUSEH 10-15-96			TC-10							
	PLOT DATE = 10/21/2015	DATE - 06-89	REVISED - T. RAMMACHER 01-06-00										
					SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	CONTRACT NO. 62B05				
					FED. ROAD DIST. NO. 1 (ILLINOIS) FED. AID PROJECT								

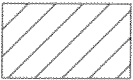







GENERAL NOTES

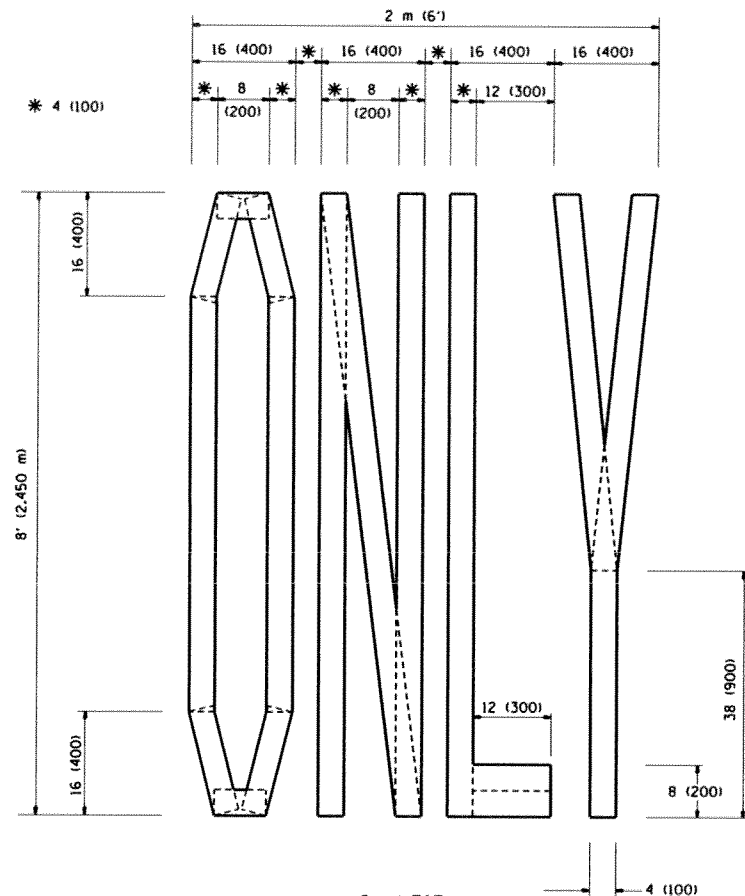
1. CONES MAY BE SUBSTITUTED FOR BARRICADES OR DRUMS AT HALF THE SPACING DURING DAY OPERATIONS. CONES SHALL BE A MINIMUM OF 28 (710) IN HEIGHT. WHEN CONES ARE BEING USED, THE "LEFT TURN LANE" SIGN MAY BE SKID MOUNTED AT A MINIMUM HEIGHT OF 5' (1.5 m).
2. STEADY BURNING LIGHTS WILL NOT BE REQUIRED ON BARRICADES OR DRUMS FOR DAY OPERATIONS. ALL LIGHTS SHALL BE MONODIRECTIONAL.
3. REFLECTORIZED TEMPORARY PAVEMENT MARKING TAPE SHALL BE PLACED THROUGHOUT THE BARRICADED AREA OF EACH TURN BAY WHERE THE CLOSURE TIME IS GREATER THAN FOURTEEN DAYS.
4. THIS APPLICATION ALSO APPLIES WHEN WORK IS BEING PERFORMED IN THE RIGHT LANE(S) AND THE RIGHT TURN BAY IS TO REMAIN OPEN. UNDER THIS CONDITION, "RIGHT TURN LANE" R3-100 24 x 24 (600 x 600) AND M6-2R 21 x 15 (530 x 380) SHALL BE USED.
5. THESE CONTROLS SHALL SUPPLEMENT MAINLINE TRAFFIC CONTROL FOR LANE CLOSURES.
6. LONGITUDINAL DIMENSIONS MAY BE ADJUSTED TO FIT FIELD CONDITIONS.
7. FORM OPER 725 IS REQUIRED.
8. IF A DRUM OR TYPE II BARRICADE WITH AN ATTACHED SIGN PANEL WHICH MEETS NCHRP 350 REQUIREMENTS IS NOT AVAILABLE, THE SIGNS SHALL BE MOUNTED, ABOVE THE BARRICADES, ON SEPARATE SIGNS SUPPORTS THAT MEET NCHRP 350 PREQUIREMENTS.
9. TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC) SHALL BE INCLUDED IN THE COST SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in inches (millimeters) unless otherwise shown.

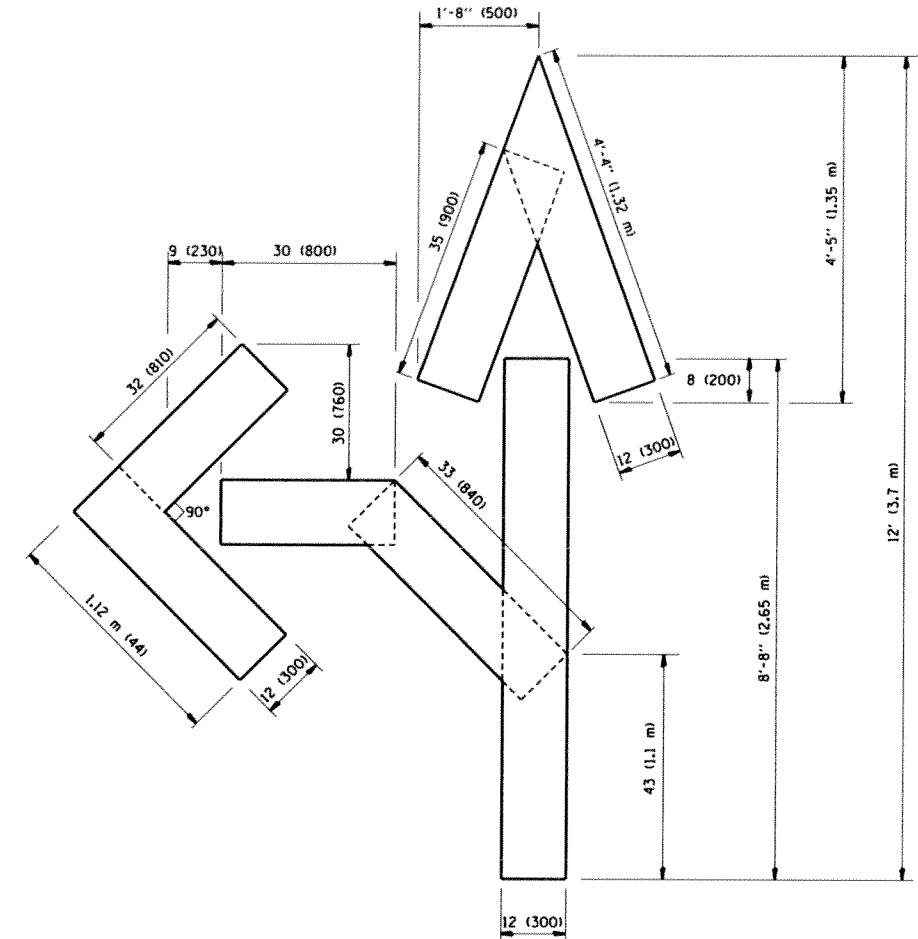
LEGEND

-  WORK AREA
-  LANE OPEN TO TRAFFIC
-  TYPE I OR II BARRICADE WITH STEADY BURN LIGHT
-  DRUM WITH STEADY BURN LIGHT
-  DRUM WITH SIGN (WITH OPTIONAL FLASHING LIGHT) SEE DETAIL
-  TYPE I OR II CHECK BARRICADE WITH FLASHING LIGHT

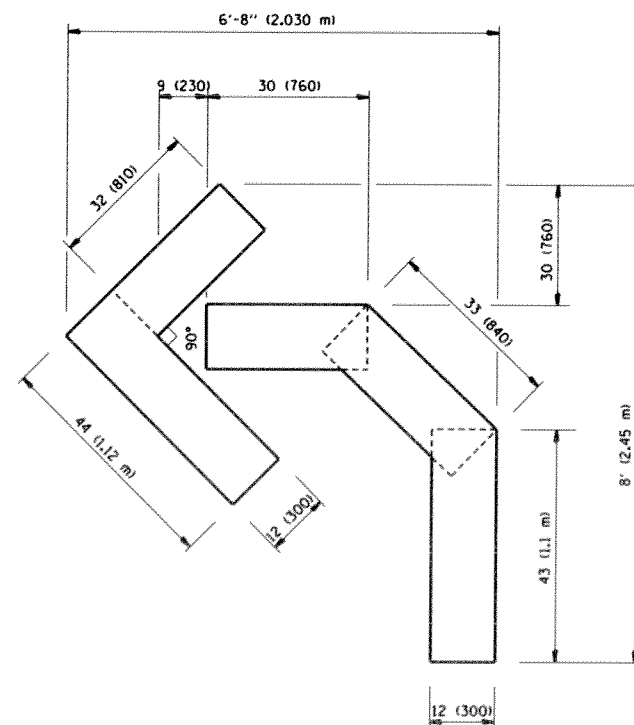
FILE NAME = c:\pw\work\p\std\stedpa\00441375\01std.dgn	USER NAME = STEEDPA	REVISED - T. RAMMACHER 09-08-94	REVISED - R. BORO 09-14-09	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	TRAFFIC CONTROL AND PROTECTION AT TURN BAYS (TO REMAIN OPEN TO TRAFFIC)	F.A.P. RTE. 347	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		REVISED - A. HOUSEH 11-07-95	REVISED -				2015-045RS	COOK	24	16
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	PLOT DATE = 10/21/2015	REVISED - T. RAMMACHER 01-06-00	REVISED -							
PM User=STEEDPA				SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.		FED. ROAD DIST. NO. 1	ILLINOIS FED. AID PROJECT



QUANTITY
4 (100) LINE = 64.1 ft. (19.7 m)
21.1 sq. ft. (1.97 sq. m)



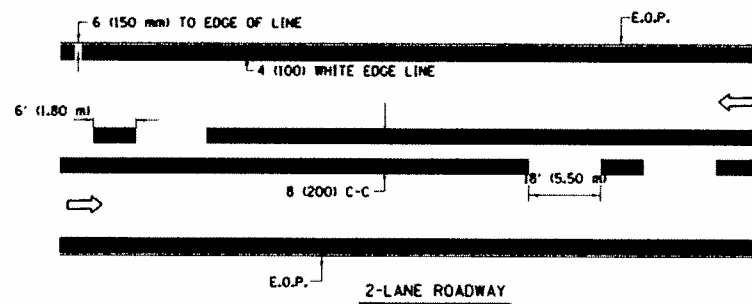
QUANTITY
4 (100) LINE = 82.5 ft. (25.3 m)
27.5 sq. ft. (2.53 sq. m)



QUANTITY
4 (100) LINE = 45.5 ft. (13.9 m)
15.2 sq. ft. (1.39 sq. m)

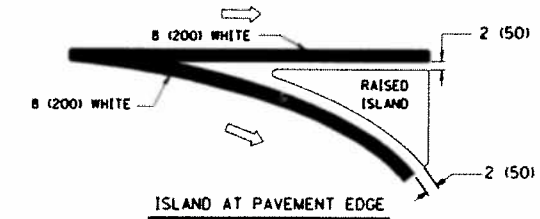
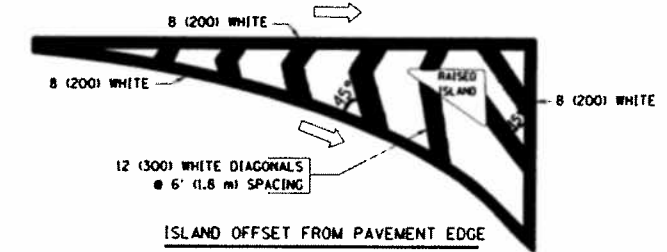
All dimensions are in inches (millimeters)
unless otherwise shown.

FILE NAME =	USER NAME = STEEDPA	DESIGNED -	REVISED -T. RAMMACHER 06-05-96	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	PAVEMENT MARKING LETTERS AND SYMBOLS FOR TRAFFIC STAGING	F.A.P. RTE.		SECTION	COUNTY	TOTAL SHEETS	SHEET NO.		
cs:\pw\work\p\dot\stedpa\d0441375\DistStd.dgn		DRAWN -	REVISED -T. RAMMACHER 11-04-97			347	2015-045RS	COOK	24	17			
PLOT SCALE = 100.0000' / 1"		CHECKED -	REVISED -T. RAMMACHER 03-02-98			SCALE: NONE		SHEET NO. 1 OF 1 SHEETS		STA.		TO STA.	
PLOT DATE = 10/21/2015		DATE - 09-18-94	REVISED -E. GOMEZ 08-28-00					TC-16		CONTRACT NO. 62B05		FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT	

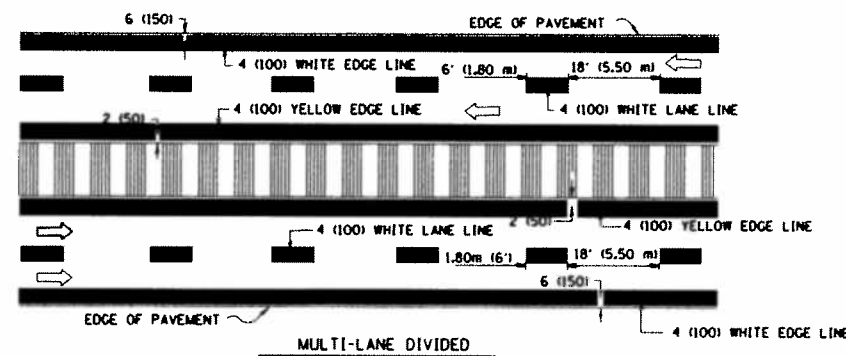
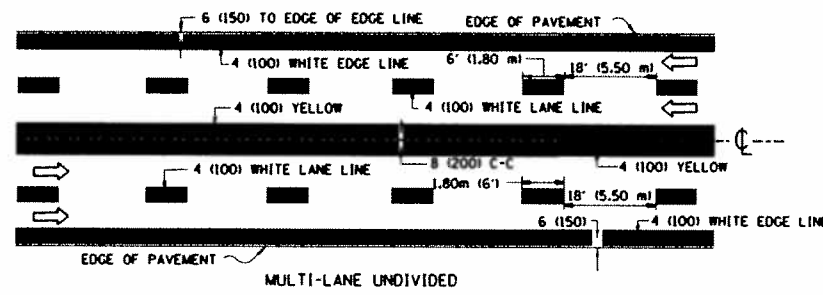


* FOR MEDIAN LENGTHS WHERE DIAGONAL SPACING CANNOT BE ATTAINED, USE 5 (FIVE) EQUALLY SPACED DIAGONAL LINES.

• DIAGONAL LINE SPACING: 20' (6.1 m) C-C

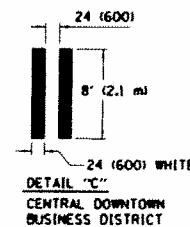
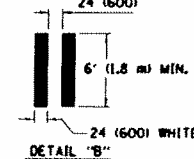
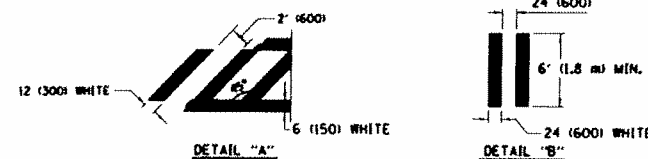
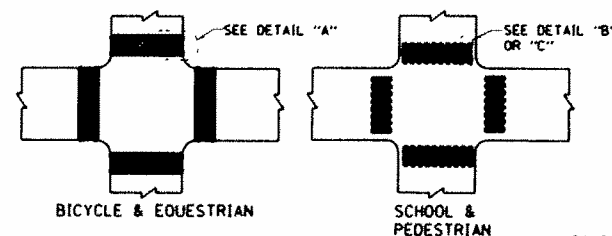


TYPICAL ISLAND MARKING

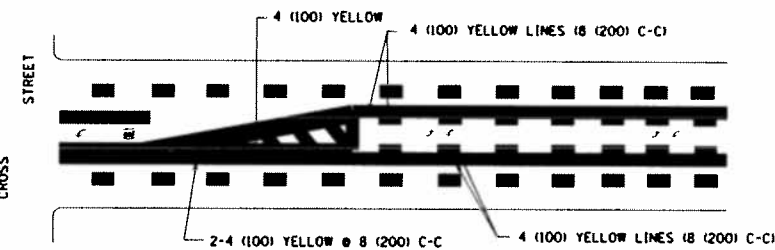


NOTE: MEDIANS WITH BARRIER CURB DO NOT REQUIRE AN EDGE LINE

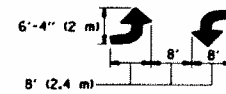
TYPICAL LANE AND EDGE LINE MARKING



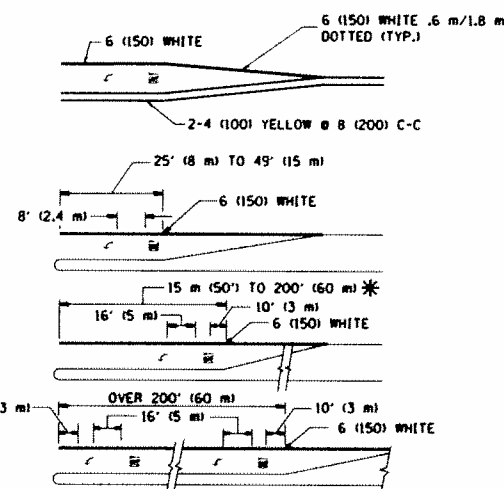
TYPICAL CROSSWALK MARKING



A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR. ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



TYPICAL PAINTED MEDIAN MARKING



FULL SIZE LETTERS 8" (2.4 m) AND ARROWS SHALL BE USED.
 ↶ AREA = 15.8 SQ. FT. (1.47 m²) AREA = 22.9 SQ. FT. (2.13 m²)

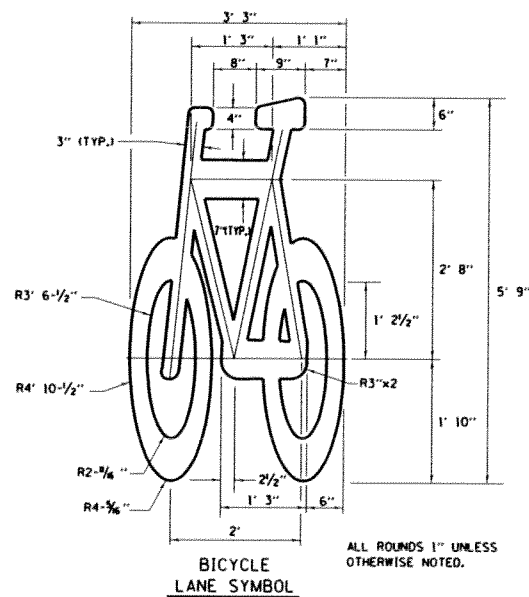
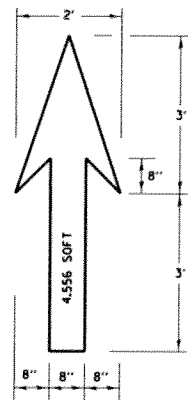
* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" (INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY").

TYPICAL TURN LANE MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 @ 4 (100)	SOLID	YELLOW	8 (200) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	8 (200) C-C
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	6' (1.80 m) LINE WITH 18' (5.50 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW; EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4 m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 @ 4 (100) EACH DIRECTION 8' (2.4 m) LEFT ARROW	SKIP-DASH AND SOLID IN PAIRS	YELLOW WHITE	6' (1.8 m) LINE WITH 18' (5.50 m) SPACE FOR SKIP-DASH; 8 (200) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL & PEDESTRIAN)	12 (300) @ 45° 24 (600) @ 90°	SOLID SOLID	WHITE WHITE	2' (600) APART 2' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT. OTHERWISE, PLACE AT DESIRED STOPPING POINT, PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 @ 4 (100) WITH 12 (300) DIAGONALS @ 45°	SOLID	YELLOW; TWO WAY TRAFFIC WHITE; ONE WAY TRAFFIC	8 (200) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
CORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS; 20' (6.1 m) (LESS THAN 30 MPH (50 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"=3.6 SQ. FT. (0.33m ²) EACH "X"=54.0 SQ. FT. (5.0 m ²)

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO
STREET MARKING STANDARDS.
PRINTED BY CITY OF CHICAGO,
DEPARTMENT OF TRANSPORTATION,
BUREAU OF TRAFFIC.

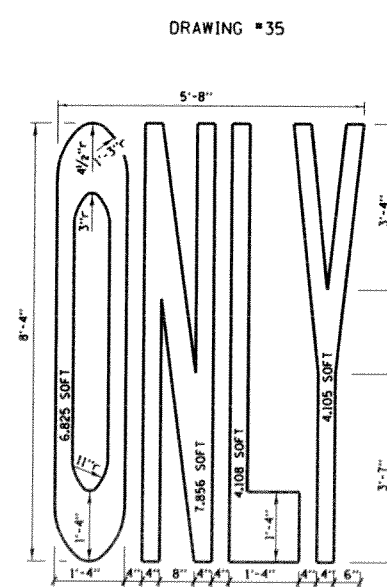
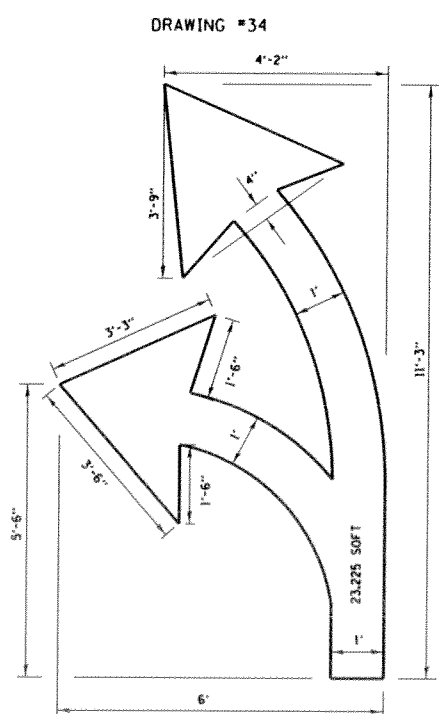
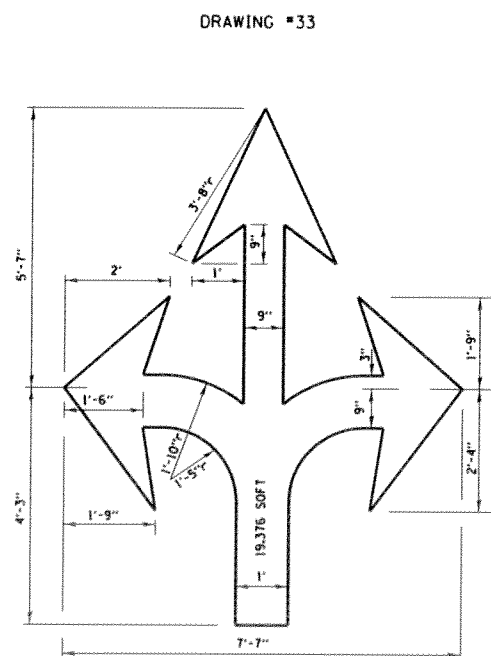
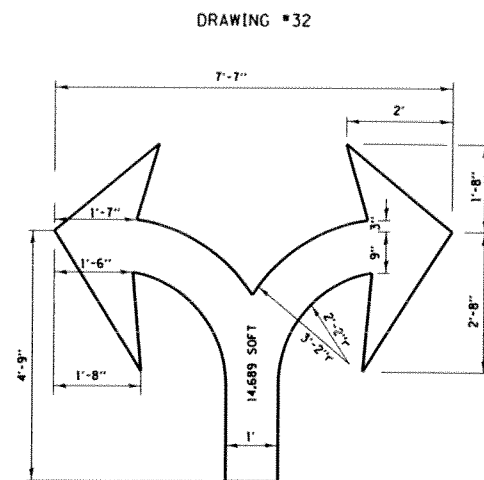
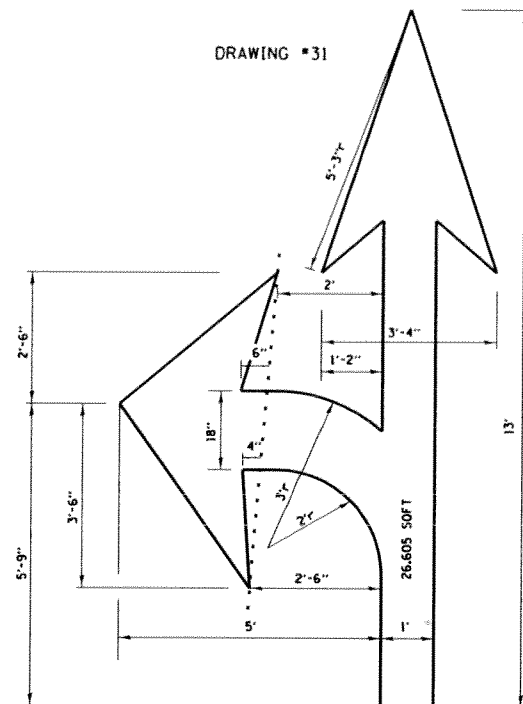
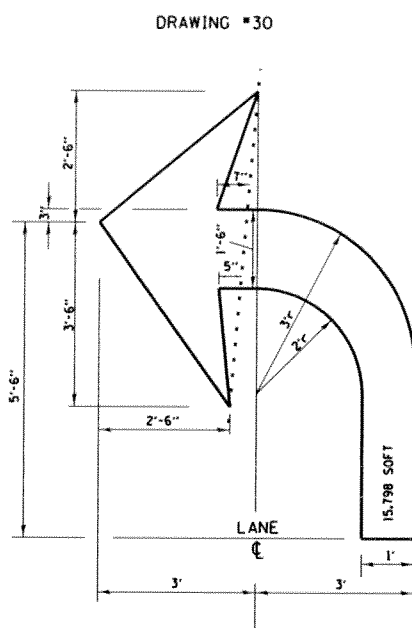
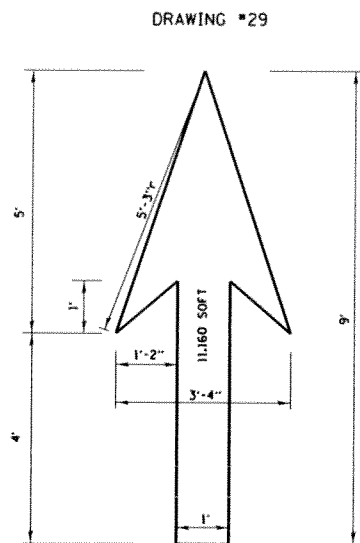
ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.



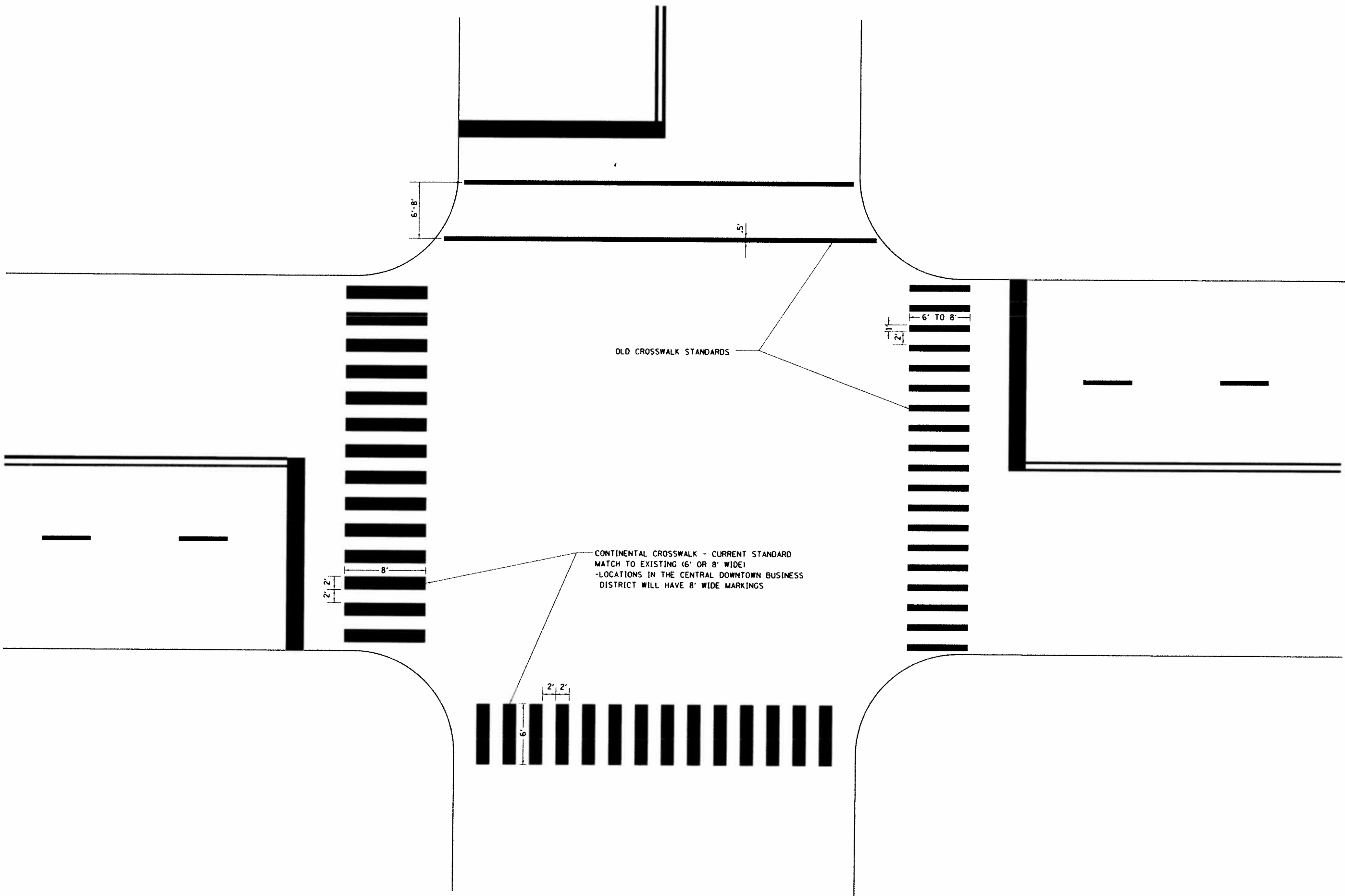
NOTE:

- 1.) FOR BIKE LANE SYMBOLS ONLY, USE PRE-FORMED THERMOPLASTIC WITH A MINIMUM THICKNESS OF 90 MILS, MINIMUM SKID RESISTANCE VALUE OF 60 BPN, & A MINIMUM INDEX OF REFRACTION OF 1.50.
- 2.) THE RESIDENT ENGINEER SHALL CONTACT MR. BEN GOMBERG AT 312-744-8093 AT LEAST ONE CALENDAR WEEK PRIOR TO INSTALLING BIKE LANE SYMBOLS.

TYPICAL BIKE LANE SYMBOLS
DRAWING #28



NOTE:
ALL MARKINGS SHALL BE SOLID WHITE UNLESS OTHERWISE NOTED IN THE PLANS



FILE NAME =	USER NAME = STEEDPA	DESIGNED -	REVISED - T. RAMMACHER 12-07-00
c:\pr\work\p\dot\steadpa\0441375\DistStd.dgn		DRAWN -	REVISED - K. ENG 01-12-12
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED -
	PLOT DATE = 10/21/2015	DATE -	REVISED -

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO			
TYPICAL PAVEMENT MARKINGS			
SCALE: NONE	SHEET NO. 3 OF 3 SHEETS	STA.	TO STA.

F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
347	2015-045RS	COOK	24	20
TC-24		CONTRACT NO. 62805		
FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				

NOTES

--FOREMAN TO CONTACT ALL UTILITIES PRIOR TO EXCAVATION FOR EXACT LOCATION OF THEIR UNDERGROUND FACILITIES

--ALL FOUNDATIONS IN SIDEWALK TO BE INSTALLED AS PER DRAWING # 816 OR 818, 828 & 837

--MOUNT ALL PEDESTRIAN SIGNALS BELOW TRAFFIC SIGNALS WHERE APPLICABLE.

--ALL PEDESTRIAN AND TRAFFIC SIGNALS TO BE L.E.D.

--FOR CODE SHEET, SEE DRAWING NUMBER 826

T --DENOTES D3-2 STREET SIGN 5' OFF CENTER LINE OF POLE

● --DENOTES PEDESTRIAN PUSHBUTTON

--SAWCUT SIDEWALK, REMOVE AND REPLACE AS NOTED, SEAL JOINTS

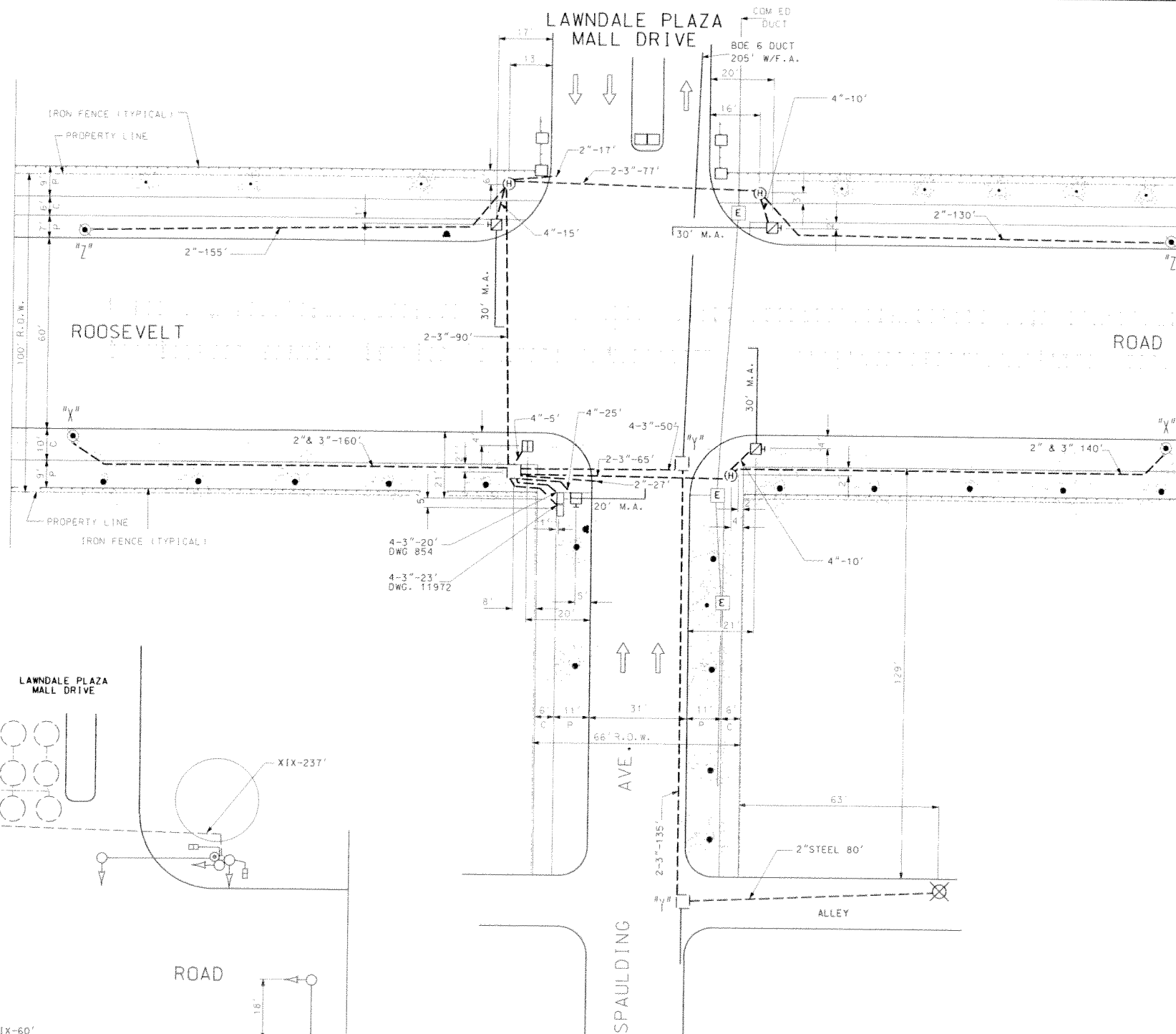
○ --DENOTES VEHICLE LOOP DETECTOR

"Z"--INSTALL 2" ELBOW ADJACENT TO EMBEDDED POLE

"Y"--INSTALL MANHOLE ROOF, & 30" FRAME AND COVER

"X"--INSTALL 2" & 3" ELBOWS ADJACENT TO EMBEDDED POLE

"W"--INSTALL SERVICE EQUIPMENT AS PER DWG. 11925



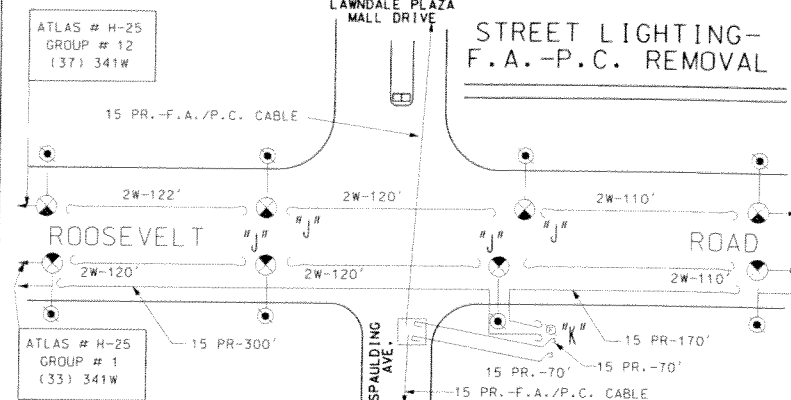
FOUNDATION & CONDUIT PLAN

SCALE 1"=20'

CABLE & SIGNAL PLAN

NOT TO SCALE

STREET LIGHTING-F.A.-P.C. REMOVAL



NOTES

"J"--REMOVE STEEL EMBEDDED POLE, 12' M.A., 341W LUM., & 2 WR

"K"--REMOVE TERMINAL POST MOUNTED CABINET, BREAKDOWN FOUNDATION TYPE "A"

"L"--INSTALL LATERAL UP POLE, & S.E.H.

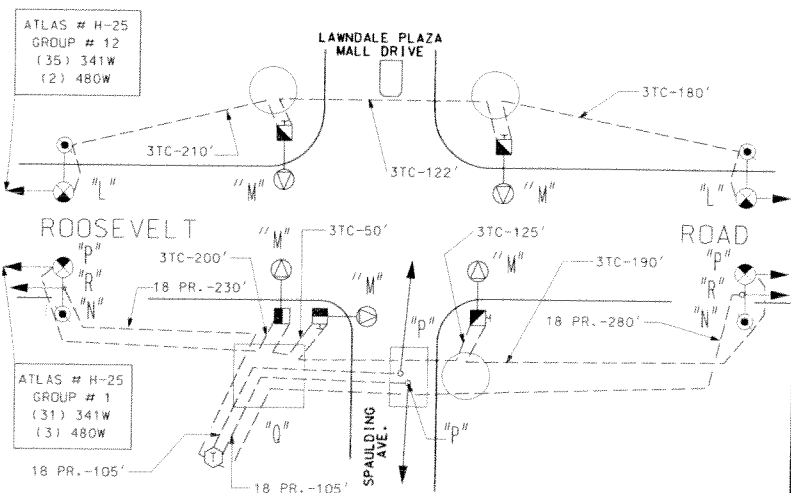
"M"--INSTALL 15' M.A. & 480 W LUMINAIRE

"N"--INSTALL 2" & 3" LATERAL UP POLE, S.E.H. & DEAD END ASSEMBLY WITH STRAND VISE

"P"--SPlice F.A./P.C. CABLE

"Q"--INSTALL TERMINAL CABINET-TYPE "B", BASE MOUNTED, AS PER DWG. #11972

"R"--SPlice MESSENGER THROUGH AND INSTALL STRAND LINK

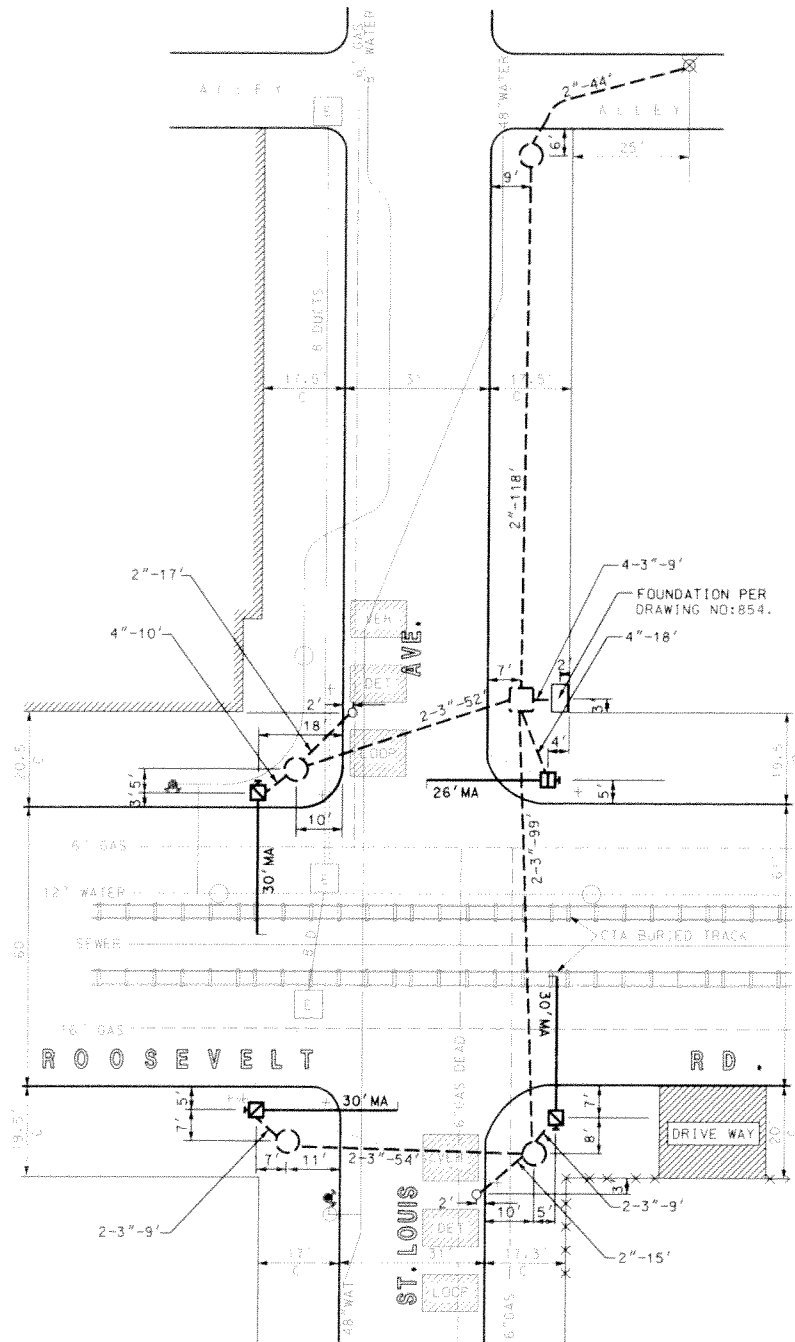


STREET LIGHTING-F.A.-P.C. INSTALLATION

DATE	REVISION
SUPERSEDES DWG. #	
WORK ORDER NO. 1113376	DATE
COST ALLOCATION ACCOUNT	
APPROPRIATION ACCOUNT	MATERIAL
	LABOR
TRAFFIC CONTROL SIGNALS SPAULDING AVENUE & ROOSEVELT ROAD	
CITY OF CHICAGO DEPT. OF STREETS AND SANITATION BUREAU OF ELECTRICITY DIVISION OF ELECTRICAL ENGINEERING	
DRAFTSMAN: O. LETAMENDI	ENGINEER: R. CARTER
SUPERVISING ENGINEER: ELEC. DESIGN ENGR.	ENGINEER-OF ELECTRICITY: GEN'L SFT. OF ELECTRICITY:
DEPUTY COMMISSIONER:	DWG. NO. 23813
SIZE: 22" x 36"	SCALE: 1"=20' DATE: 4-15-02

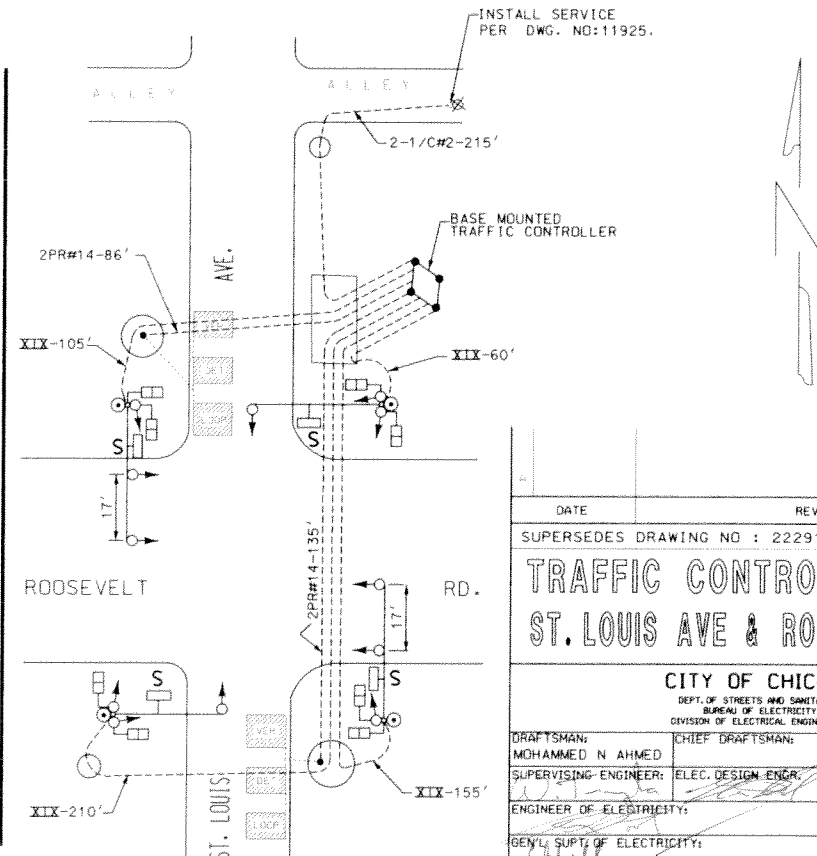
CONSTRUCTION NOTES

- FOREMAN TO CONTACT ALL THE UNDERGROUND UTILITIES PRIOR STARTING ANY EXCAVATION FOR EXACT LOCATION OF THEIR UNDERGROUND FACILITIES.
- FOREMAN TO SAW CUT BETWEEN THE SIDE WALK/PAVEMENT TO BE REMOVED AND THAT TO REMAIN IN PLACE, TO MAKE A CLEAR AND STRAIGHT JOINT.
- ALL FOUNDATIONS TO BE IN ACCORDANCE WITH DWG. NO: 828 & 837.
- ALL SIGNAL SECTIONS TO BE OF STANDARD 12" SIZE.
- ALL VEHICULAR SIGNAL HEADS TO BE OF STANDARD THREE SECTION (RED/YELLOW/GREEN) UNITS.
- ALL PEDESTRIAN SIGNAL HEADS TO BE OF STANDARD TWO SECTION UNITS WITH I.T.E SYMBOLIC (HAND / MAN) LENSES.
- PEDESTRIAN SIGNAL HEADS TO BE INSTALLED BELOW VEHICULAR SIGNAL HEADS WHERE APPLICABLE.
- S — INSTALL THE ROAD NAME SIGN ON THE MAST ARM.



FOUNDATION AND CONDUIT PLAN

SCALE: 1"=20'

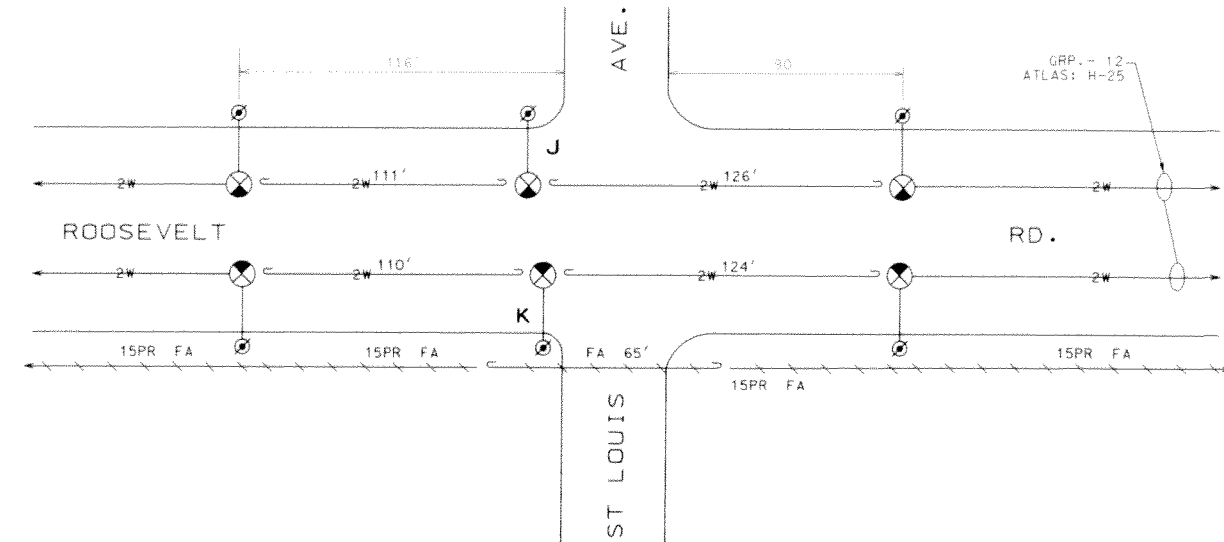


SIGNAL AND CABLE PLAN

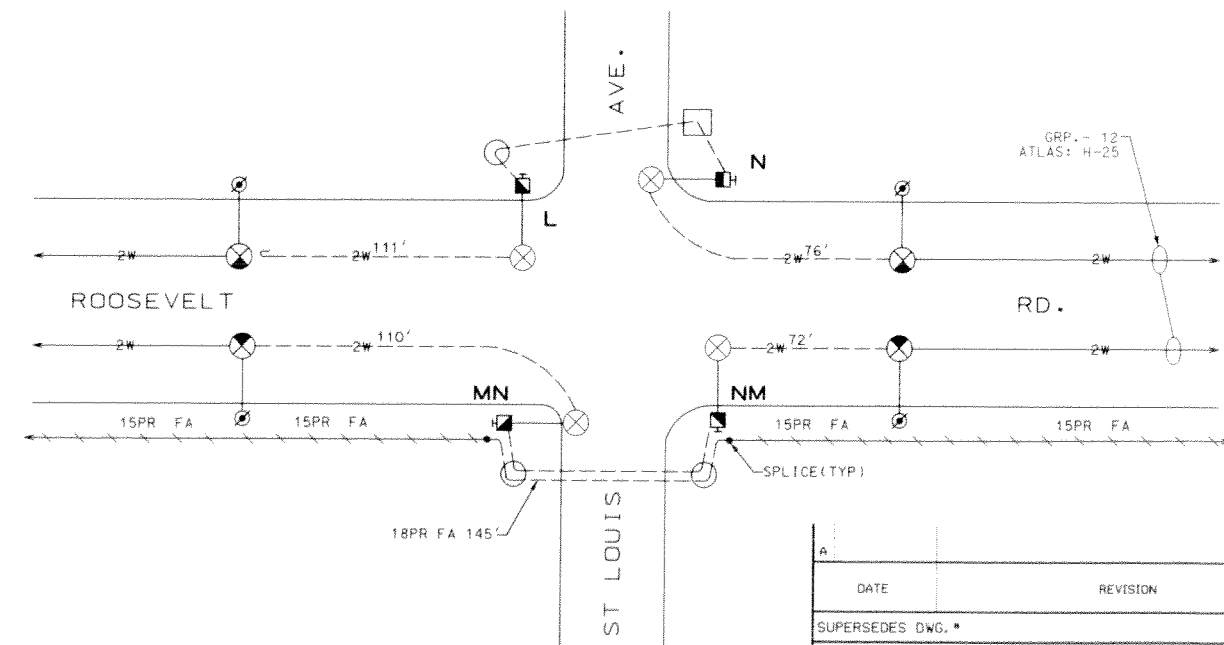
NO SCALE

DATE	REVISION
SUPERSEDES DRAWING NO : 22291 DATED: 7-31-1957	
TRAFFIC CONTROL SIGNALS ST. LOUIS AVE & ROOSEVELT RD.	
CITY OF CHICAGO DEPT. OF STREETS AND SANITATION BUREAU OF ELECTRICITY DIVISION OF ELECTRICAL ENGINEERING	
DRAFTSMAN: MOHAMMED N AHMED	ENGINEER: MOHAMMED N AHMED
SUPERVISING ENGINEER: ELEC. DESIGN ENGR.	DWG. NO. 13898
ENGINEER OF ELECTRICITY:	
GEN'L. SUPT. OF ELECTRICITY:	
DEPUTY COMMISSIONER:	
SIZE: 16" 22"	SCALE: AS NOTED
DATE: 7-15-1999	

ADJUSTMENT TO STREET LIGHTING AND FIRE ALARM EQUIPME



REMOVAL PLAN NO SCALE

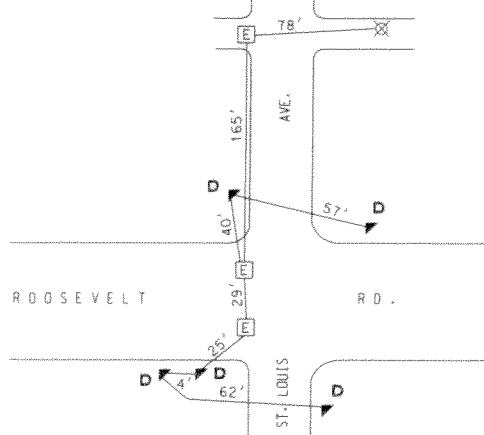


INSTALLATION PLAN

NO SCALE

- J — REMOVE THE LUMINAIRE, 8' MA, 2W AERIAL SECONDARY RACK AND REMOVE EMBEDDED POLE.
- K — REMOVE THE LUMINAIRE, 8' MA, 2W AERIAL SECONDARY RACK FA CABLE CLAMP AND REMOVE THE EMBEDDED POLE.
- L — INSTALL 8' MA, THE LUM, 2W RACK AND SERVICE ENTRANCE HEAD ON POLE CAP.
- M — INSTALL 15' MA, THE LUM, 2W RACK AND SERVICE ENTRANCE HEAD ON POLE CAP.
- N — DRILL POLE AT SUITABLE HEIGHT, INSTALL REVERSED ADAPTOR AND DEAD END VICE.

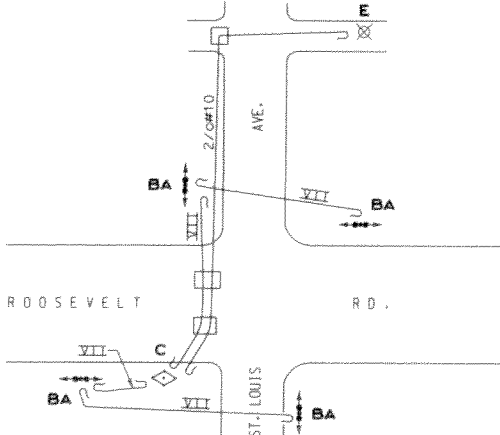
REMOVAL OF OBSOLETE TRAFFIC CONTROL SIGNAL SYSTEM



FOUNDATION REMOVAL PLAN

NO SCALE

- A — REMOVE THE SIGNAL HEAD.
- B — REMOVE THE 15' POST.
- C — REMOVE THE CONTROLLER AND THE POST.
- D — BREAKDOWN THE FOUNDATION.
- E — REMOVE SERVICE EQUIPMENT FROM THE COM ED POLE.
- REMOVE THE CABLES AS SHOWN:
7/C#10-362'
2/C#10-383'



SIGNAL AND CABLE REMOVAL PLAN

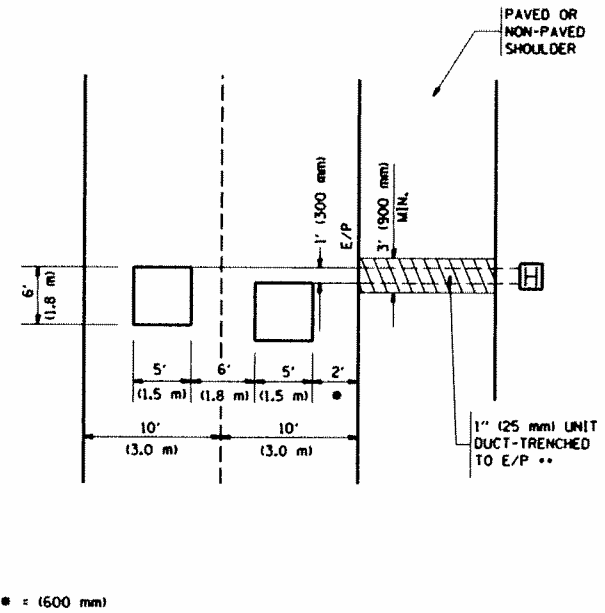
NO SCALE

TRAFFIC CONTROL SIGNAL ST. LOUIS AVE & ROOSEVELT

CITY OF CHICAGO DEPT. OF STREETS AND SANITATION BUREAU OF ELECTRICITY DIVISION OF ELECTRICAL ENGINEERING		
DRAFTSMAN: MOHAMMED N AHMED	CHIEF DRAFTSMAN:	ENGINEER: MOHAMMED N AHMED
SUPERVISING ENGINEER: ELEC. DESIGN ENGR.		DWG. NO. 13898
ENGINEER OF ELECTRICITY:		
GEN'L. SUPT. OF ELECTRICITY:		
DEPUTY COMMISSIONER:		
SIZE: 22" 36"	SCALE: AS NOTED	DATE: 7-

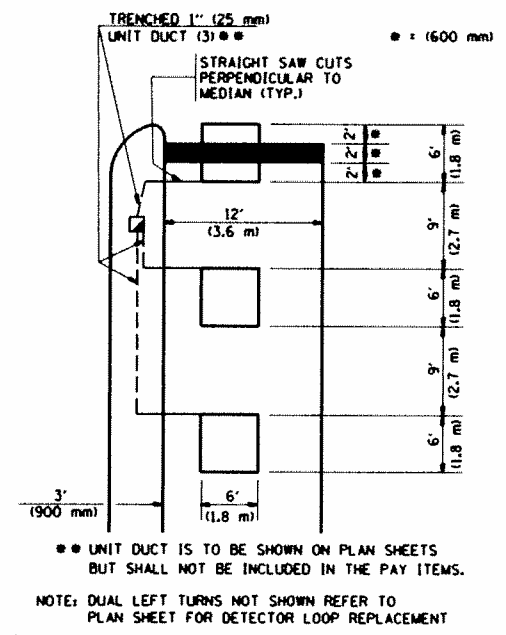
LOOPS NEXT TO SHOULDERS

PROVIDE A PAVEMENT REPLACEMENT
NOTE WHICH SHOULD EQUAL
3' (900 mm) X WIDTH OF
PAVED SHOULDER.

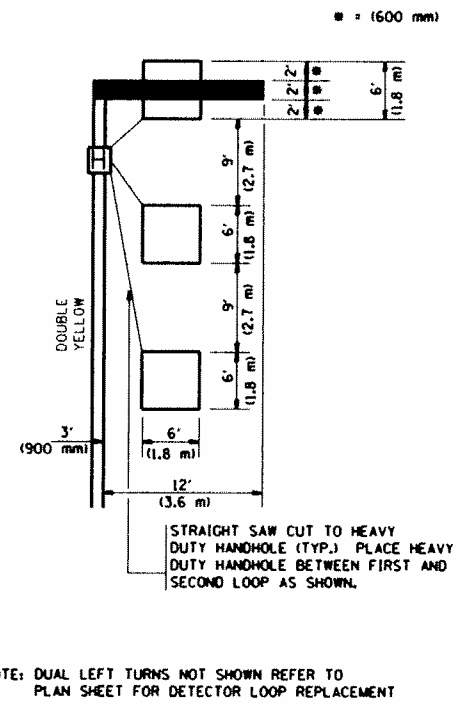


LEFT TURN LANES WITH MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

HANDHOLE LOCATION MAY
VARY DEPENDING ON GEOMETRICS
AND DESIGN OF TRAFFIC SIGNALS.
HEAVY-DUTY HANDHOLES TO BE
USED WHEN THE MEDIAN IS
MOUNTABLE. REFER TO STANDARD
814001 TO ENSURE THAT HANDHOLE
FITS IN MEDIAN.



LEFT TURN LANES WITHOUT MEDIANS
VOLUME DENSITY ("FAR OUT" DETECTION)
ON SAME APPROACH
(PROTECTED / PERMITTED LEFT TURN PHASING)

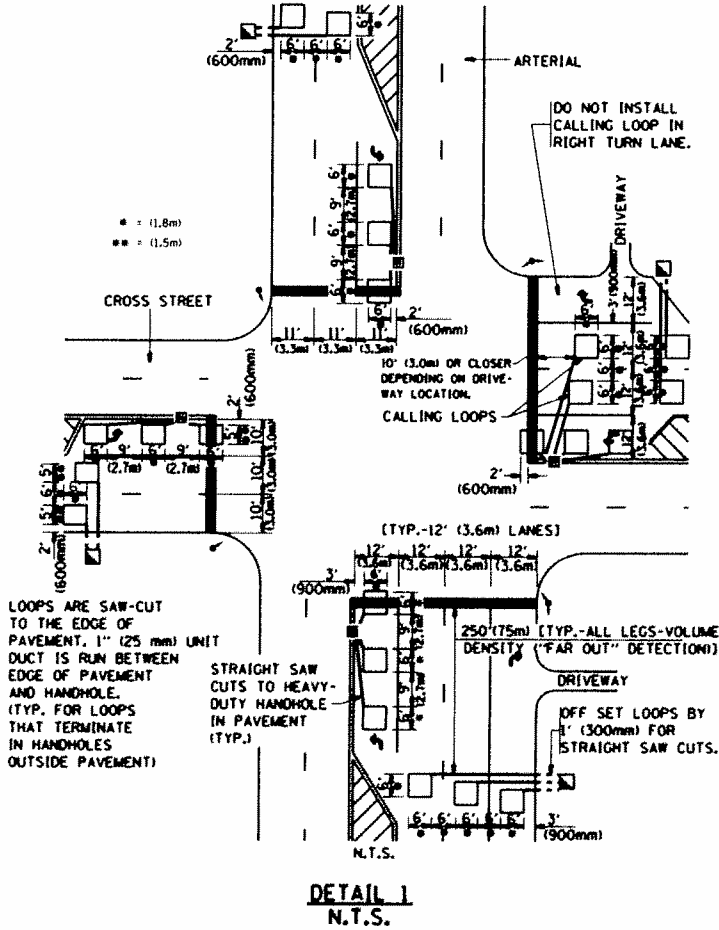


NOTES:

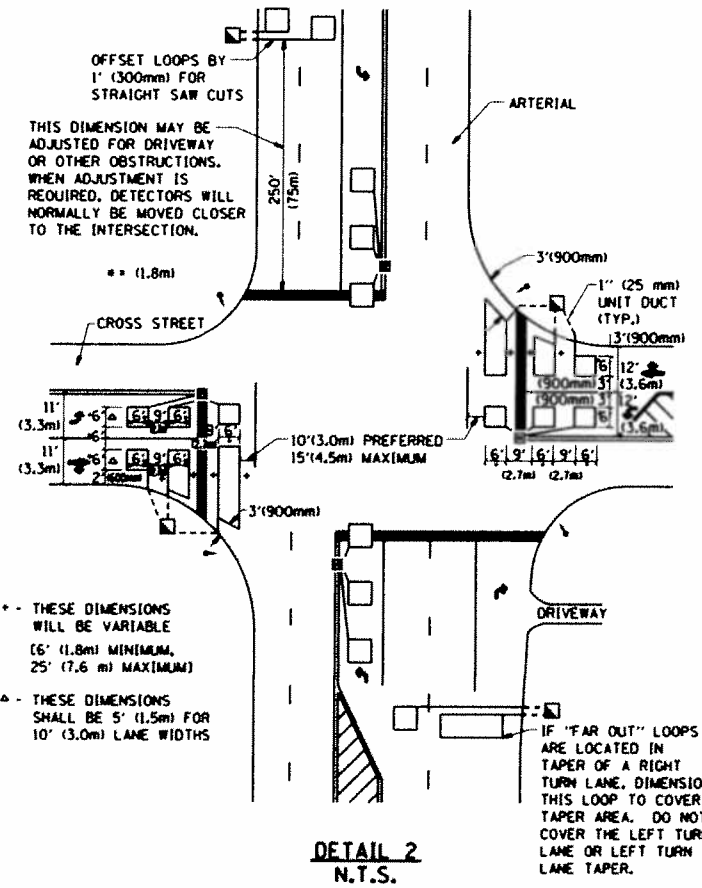
VEHICLES LOOP DETECTORS

- ALL LEAD IN CABLE SHALL BE TWO CONDUCTOR NO. 14 TWISTED, SHIELDED.
- EACH DETECTOR LOOP SHALL HAVE ITS OWN SAW CUT FROM THE LOOP TO THE EDGE OF PAVEMENT OR TO A HANDHOLE IN THE PAVEMENT.
- EACH DETECTOR LOOP SHALL HAVE ITS OWN ONE INCH (25 mm) UNIT DUCT BETWEEN THE EDGE OF PAVEMENT AND THE FIRST HANDHOLE OR JUNCTION BOX. EACH UNIT DUCT RUN SHALL BE SHOWN ON THE PLANS BY THE DESIGNER, BUT SHALL NOT BE PAID FOR SEPARATELY. THIS ITEM IS INCIDENTAL TO THE PAY ITEM FOR DETECTOR LOOPS.
- ONE DIMENSION OF ALL DETECTOR LOOPS SHALL BE SIX FEET (1.8 m)
- EACH LANE OF NON-LOCKING, PRESENCE DETECTION AND EACH LANE OF A DOUBLE LEFT TURN LANE REQUIRES A SEPARATE INDUCTIVE LOOP DETECTOR AND LEAD IN CABLE.
- WHEN NON-LOCKING, PRESENCE DETECTION IS USED, MORE THAN ONE LOOP PER LANE IS REQUIRED BEHIND THE STOP BAR (i.e. 1-1/2, 1-3/4, 2).
- WHEN SYSTEM LOOPS ARE REQUIRED ON AN APPROACH OF AN INTERSECTION, THE LOOPS USED FOR VOLUME DENSITY AND INTERSECTION TIMING SHALL ALSO BE USED AS SYSTEM DETECTORS. EACH ONE OF THESE TYPE OF LOOPS REQUIRES A SEPARATE TWO CONDUCTOR NO. 14 TWISTED SHIELDED CABLE AND A SEPARATE INDUCTIVE LOOP DETECTOR WHEN NEW CONTROLLERS ARE UTILIZED. THE DESIGNER SHALL LABEL THESE TYPES OF LOOPS AS "INTERSECTION AND SAMPLING (SYSTEM) DETECTORS" ON THE SIGNAL LAYOUT, THE INTERCONNECT PLAN AND THE SYSTEM CABLE PLAN. WHEN AN EXISTING CONTROLLER IS UTILIZED FOR THIS TYPE OF DETECTION, THE PAY ITEM "INDUCTIVE LOOP DETECTOR WITH SYSTEM OUTPUT" SHOULD BE USED.

ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-VOLUME DENSITY ("FAR OUT" DETECTION)



ARTERIAL-VOLUME DENSITY ("FAR OUT" DETECTION)
CROSS STREET-NON VOLUME DENSITY ("UPTIGHT" PRESENCE DETECTION)



PLACEMENT OF DETECTORS

THE FOLLOWING FIGURES REPRESENT THE MOST COMMON DETECTOR LOOP LOCATIONS AND SIZES. ADJUSTMENTS WILL BE NECESSARY FOR SPECIFIC GEOMETRIC CONSIDERATIONS.

LOCATIONS AND DEMENSIONS OF DETECTOR LOOPS ARE REQUIRED ON ALL SIGNAL LAYOUT PLAN SHEETS.

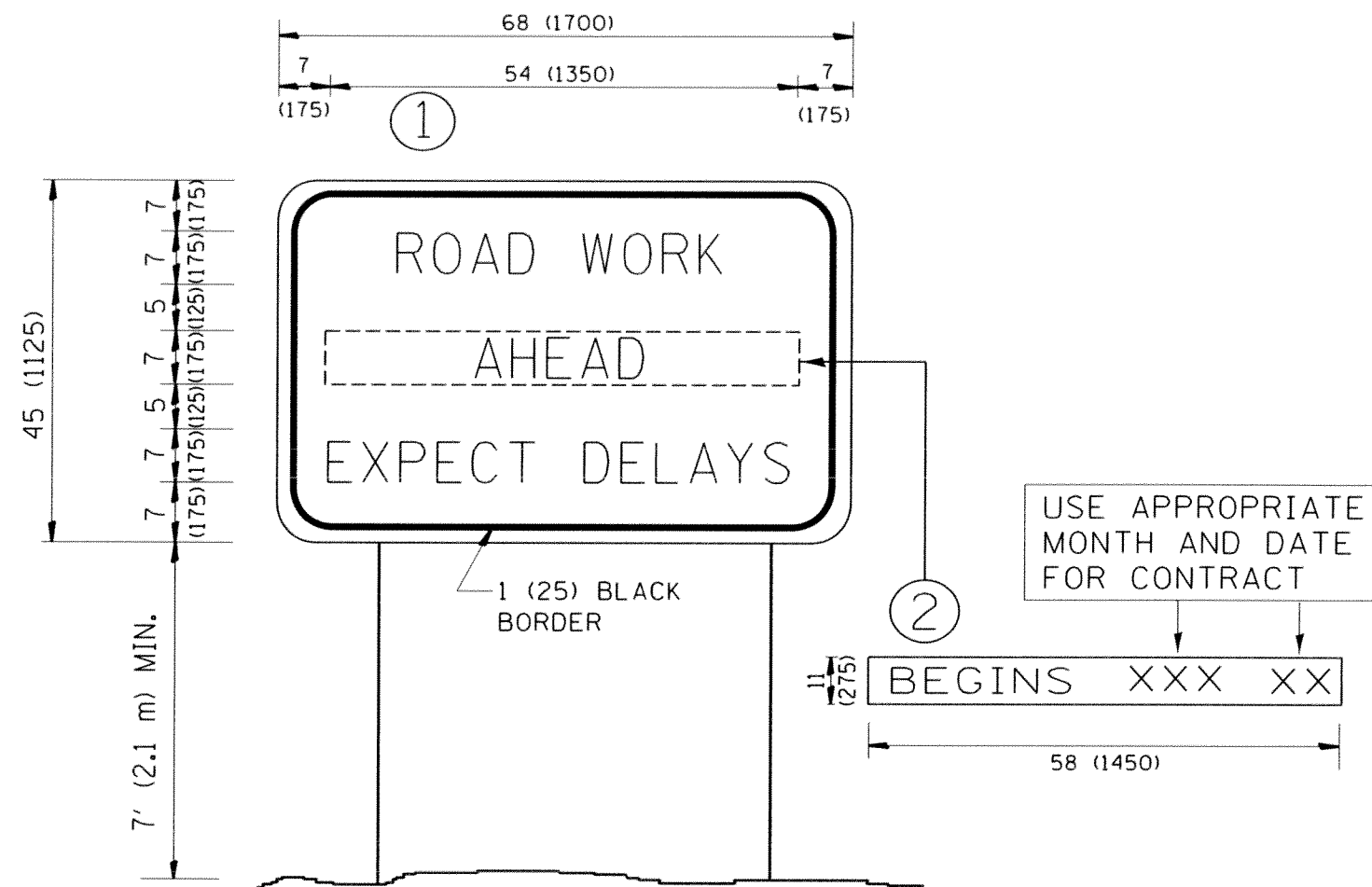
"FAR OUT" DETECTION REFERS TO LOCKING, PRESENCE TYPE DETECTION LOCATED IN THRU LANES, RIGHT TURN LANES, AND RIGHT TURN LANE TAPER AREAS (IF APPLICABLE), USUALLY 250' (75 m) IN ADVANCE OF STOP BARS. "UPTIGHT" DETECTION REFERS TO NON-LOCKING PRESENCE TYPE DETECTION LOCATED IN ALL LANES AND 10'-15' (3.0 m-4.5 m) BEHIND THE CROSSING STREET'S EDGE OF PAVEMENT EXTENDED.

NOTE:

ALL DETAILS AND NOTES SHOWN ARE FROM THE I.D.O.T. DISTRICT 1 TRAFFIC SIGNAL DESIGN GUIDELINES DATED JANUARY 1995

THIS DRAWING HAS BEEN PREPARED TO ASSIST THE RESIDENT ENGINEER FOR ALL ROADWAY RESURFACING OR S.M.A.R.T. PROJECTS WHERE THE DIMENSIONS ARE NOT SHOWN ON THE PLANS AND THE FINAL LOCATIONS FOR CROSSWALKS OR STOP BARS ARE NOT DETERMINED.

FILE NAME: c:\p\work\p\dot\staredpa\0441375\01\std.dgn	USER NAME: STEEDPA	DESIGNED: -	REVISED: -	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	DISTRICT 1 - DETECTOR LOOP INSTALLATION DETAILS FOR ROADWAY RESURFACING	F.A.P. RTE. 347	SECTION 2015-045RS	COUNTY COOK	TOTAL SHEETS 24	SHEET NO. 23
PLOT SCALE: 1/8" = 1'-0"	PLOT DATE: 10/21/2015	CHECKED: R.K.F.	REVISED: -		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA. TO STA.	CONTRACT NO. 62B05		
		DATE: -	REVISED: -					FED. ROAD DIST. NO. 1 ILLINOIS	FED. AID PROJECT	



NOTES:

1. USE BLACK LETTERING ON ORANGE BACKGROUND.
2. ERECT SIGNS IN ADVANCE OF THE LOCATION FOR THE "ROAD CONSTRUCTION AHEAD" SIGN AT LOCATIONS AS DIRECTED BY THE ENGINEER.
3. ERECT SIGN ① WITH INSTALLED PANEL ② ONE WEEK PRIOR TO THE START OF CONSTRUCTION.
4. REMOVE PANEL ② SOON AFTER THE START OF CONSTRUCTION.
5. SEE SPECIAL PROVISION FOR "TEMPORARY INFORMATION SIGNING" FOR ADDITIONAL INFORMATION.
6. ONE SIGN ASSEMBLY EQUALS 25.70 SQ. FT. (2.3 SQ. M.)
7. SHALL BE PAID FOR AS TEMPORARY INFORMATION SIGNING.

ALL DIMENSIONS ARE IN INCHES (MILLIMETERS)
UNLESS OTHERWISE SHOWN.

FILE NAME = c:\pw\work\pwsdot\steadpa\0441375\Dist15std.dgn	USER NAME = STEEDPA	DESIGNED -	REVISED - R. MIRS 09-15-97	STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION	ARTERIAL ROAD INFORMATION SIGN				F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
		DRAWN -	REVISED - R. MIRS 12-11-97						347	2015-045RS	COOK	24	24
	PLOT SCALE = 100.0000' / in.	CHECKED -	REVISED - T. RAMMACHER 02-02-99		TC-22		CONTRACT NO. 62B05						
	PLOT DATE = 10/21/2015	DATE -	REVISED - C. JUCIUS 01-31-07		SCALE: NONE	SHEET NO. 1 OF 1 SHEETS	STA.	TO STA.	FED. ROAD DIST. NO. 1 ILLINOIS FED. AID PROJECT				