

Bench Mark: Chiseled square on top of the N.E. abutment overhang on S.N. 057-0092. Sta.388+26.25, 21.95' Lt., Elevation = 688.79.

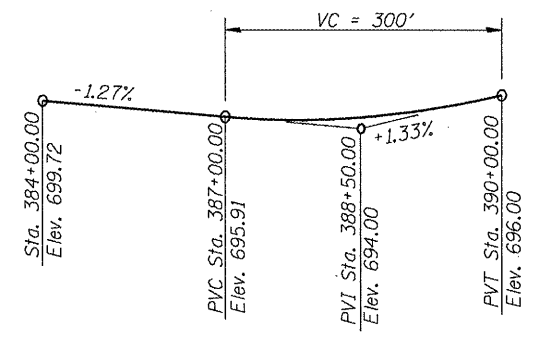
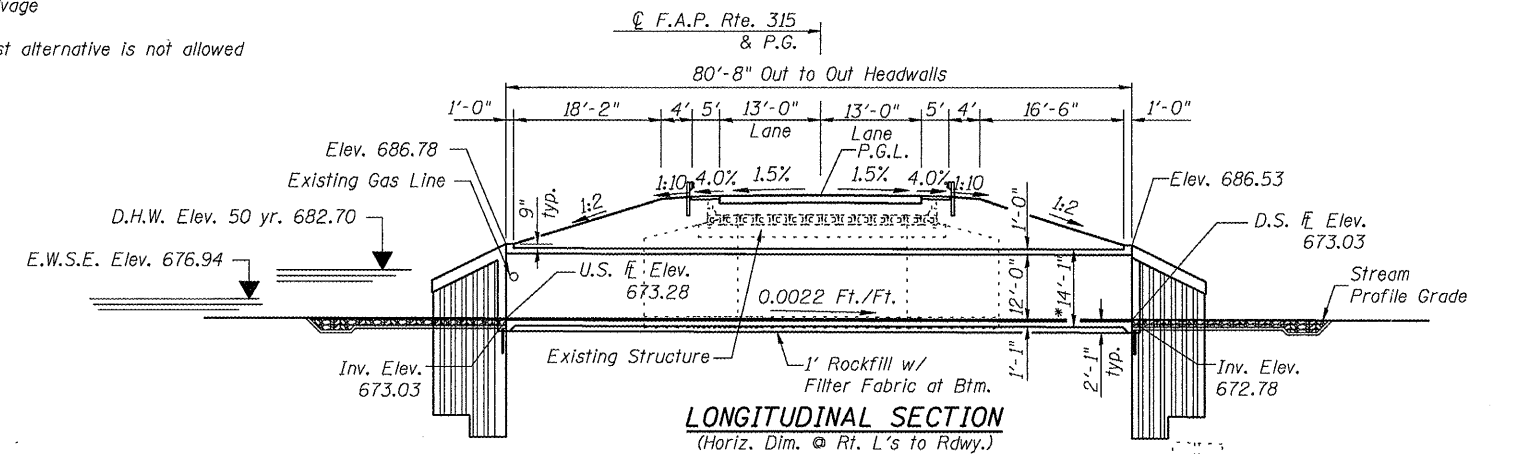
Existing Structure (S.N. 057-0092): The structure was originally built in 1936 as FAP 119, Sec. 121B-WPH. It was a 3 span reinforced concrete deck girder on closed abutments on solid wall concrete piers. In 1982 as FA Route 53, Sec. 121 BR the superstructure was replaced and widened with precast prestressed concrete deck beams. The back-to-back of abutments is 126'-11", 42'-0" out-to-out deck width and 40'-0" face-to-face of parapet.

Structure to be removed and replaced using a temporary runaround. See Roadway Plans.

No Salvage

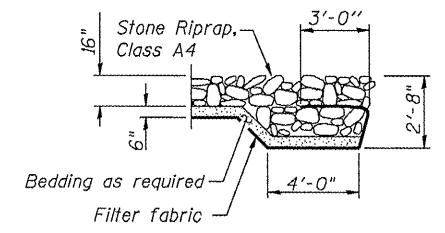
Precast alternative is not allowed

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION



STATION 387+15.00
BUILT 20-- BY
STATE OF ILLINOIS
F.A.P. RT. 315 SEC. 121-BR-1
LOADING HS20-44
STR. NO. 057-2041

NAME PLATE
See Std. 515001

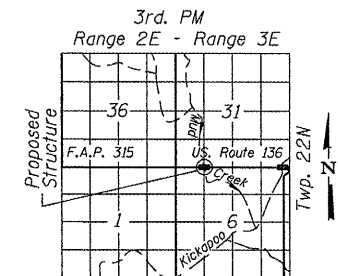


LOADING HS20-44
Allow 50#/sq. ft. for future wearing surface.

DESIGN SPECIFICATIONS
2002 AASHTO

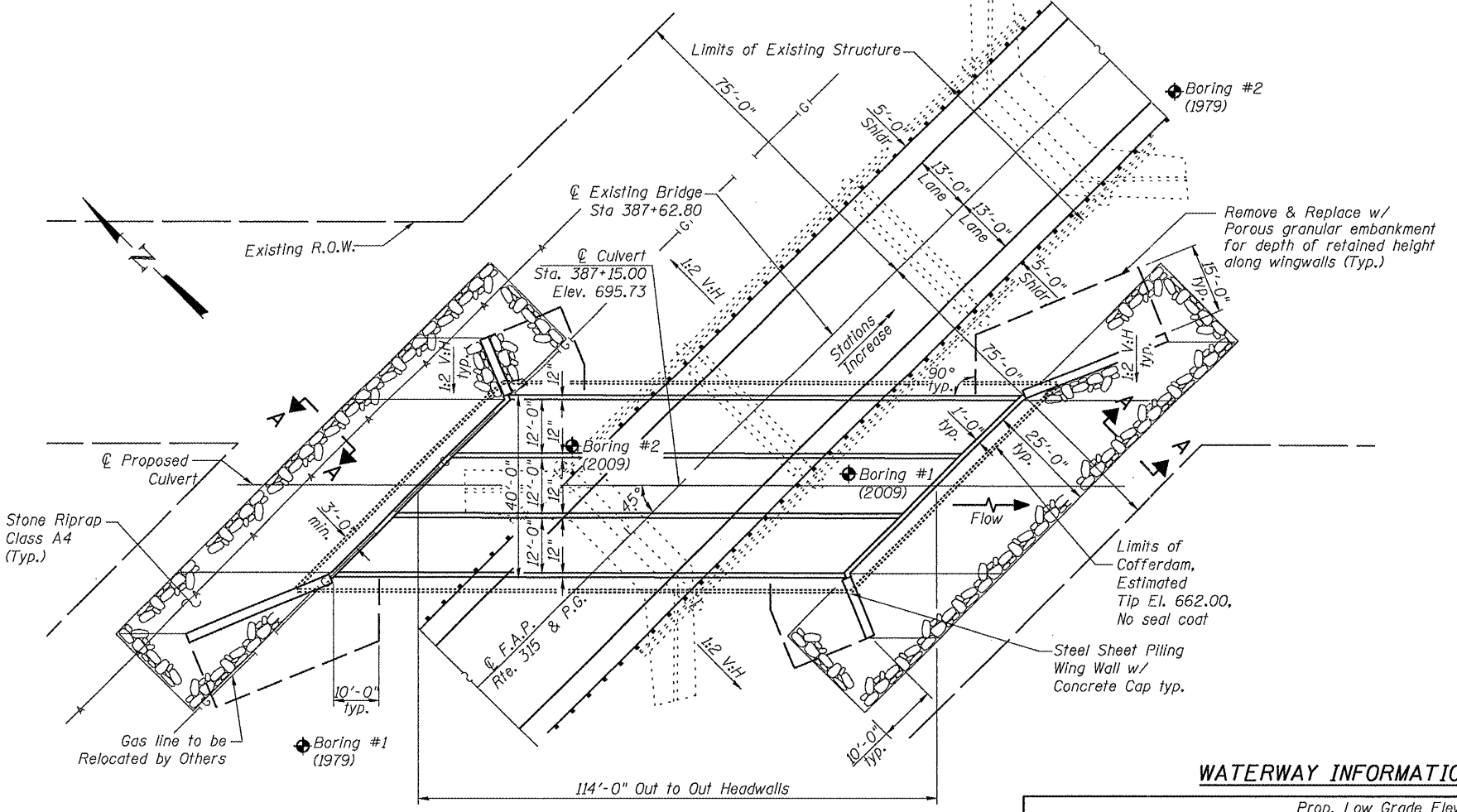
DESIGN STRESSES

FIELD UNITS
f_c = 3,500 psi
f_y = 60,000 psi (reinforcement)
f_y = 38,000 psi (Permanent Sheet Piles)



INDEX OF SHEETS

- 1 General Plan and Elevation
- 2 General Notes and Details
- 3-4 Culvert Details
- 5-7 Soil Boring Logs



WATERWAY INFORMATION

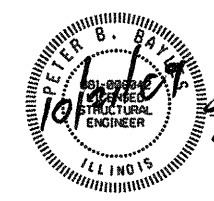
Drainage Area = 4.5 mi ²		Prop. Low Grade Elev. 695.0 @ Sta. 388+46.50		Exist. Low Grade Elev. 691.3 @ Sta. 388+26.02					
Flood	Freq. Yr.	Q C.F.S.	Opening Sq. Ft.	Nat. H.W.E.	Head - Ft.	Headwater El.			
			Exist. Prop.	Exist. Prop.	Exist. Prop.	Exist. Prop.			
Design	50	1760	430	338	682.7	0.3	0.5	683.0	683.2
Base	100	2087	463	352	683.1	0.3	0.8	683.4	683.9
Max. Calc.	500	2901	520	377	683.8	0.5	1.6	684.3	685.4

10 year velocity through existing bridge = 3.1 fps
10 year velocity through prop. bridge = 3.5 fps

DESIGN SCOUR ELEVATION TABLE

Design Scour Elevation (ft.)	U.S. **	D.S. **
	669.03	668.78

**2' min. toewall sheeting embedment req'd



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Expiration Date: 11/30/2010

APPROVE
For Structural Adequacy

Relph E. Anderson (TS)
Engineer of Bridges & Structures

GENERAL PLAN
US 136 OVER MUD CREEK
F.A.P. RTE. 315
SECTION 121-BR-1
MCLEAN COUNTY
STATION 387+15.00
STRUCTURE NO. 057-2041

SHEET NO. 1 7 SHEETS	F.A.P. RTE.	SECTION	COUNTY	TOTAL SHEETS	SHEET NO.
	315	121-BR-1	MCLEAN	67	29
FED. ROAD DIST. NO.			ILLINOIS	FED. AID PROJECT	
CONTRACT NO. 70528					