PLANS FOR PROPOSED FEDERAL AID HIGHWAY

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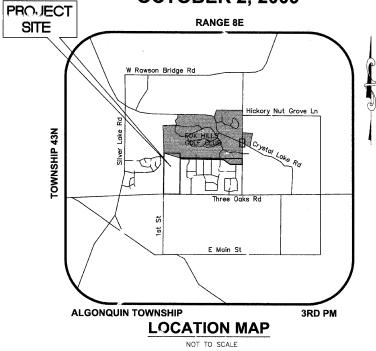
LEGEND

EXISTING	
PROPERTY LINE	
EASEMENT LINE	
EXISTING CONTOUR	
EXISTING SPOT ELEVATION	+xxx.xx
EXISTING FIRE HYDRANT	Q
EXISTING MANHOLE	0
EXISTING CATCH BASIN	0
EXISTING GAS LINE	annessament () annessament
EXISTING POWER LINE	<u> </u>
EXISTING UTILITY POLE	- <u>O</u> -
EXISTING TREE	$\overline{(\cdot)}$
EXISTING TREE LINE	~~~~~
EXISTING FENCE	X
EXISTING LIGHT POLE	¤
EXISTING SIGN	TOT

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DDDDDCED	
PROPOSED	
PROPOSED EASEMENT	
EXISTING GROUND PROFILE	
PROPOSED TRAIL PROFILE	
POINT OF VERTICAL INTERSECTION	♠ PVI
BEGINNING OF VERTICAL CURVE STATION/ELEVATION	O BVCS/BVCE
END OF VERTICAL CURVE STATION/ELEVATION	O EVCS/EVCE
PROPOSED BIKE TRAIL	
PROPOSED SIDEWALK	
PROPOSED 2' AGGREGATE SHOULDER	ERREKERREKEREKERE
PROPOSED 2' AGGREGATE SHOULDER GROUND SLOPE	X.XX%
GROUND SLOPE	x.xx%
GROUND SLOPE SILT FENCE	x.xx%
GROUND SLOPE SILT FENCE PROPOSED SIGN	X.XX% SFSF
GROUND SLOPE SILT FENCE PROPOSED SIGN PROPOSED CONTOUR	SF
GROUND SLOPE SILT FENCE PROPOSED SIGN PROPOSED CONTOUR PROPOSED SPOT ELEVATION	SF
GROUND SLOPE SILT FENCE PROPOSED SIGN PROPOSED CONTOUR PROPOSED SPOT ELEVATION PROPOSED PIPE OUTLET	SF
GROUND SLOPE SILT FENCE PROPOSED SIGN PROPOSED CONTOUR PROPOSED SPOT ELEVATION PROPOSED PIPE OUTLET PROPOSED PERFORATED UNDERDRAIN	SF
GROUND SLOPE SILT FENCE PROPOSED SIGN PROPOSED CONTOUR PROPOSED SPOT ELEVATION PROPOSED PIPE OUTLET PROPOSED PERFORATED UNDERDRAIN PROPOSED STORM PIPE	SF

CARY COMMUNITY TRAIL THREE OAKS ROAD AND SOUTH RAWSON BRIDGE ROAD **CARY PARK DISTRICT** McHENRY COUNTY

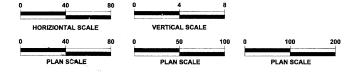
> **SECTION NO. 06-P4001-00-BR** PROJECT NO.ARA-00D1(710) JOB NUMBER C-91-046-07 **COMMUNITY TRAIL OCTOBER 2, 2009**



GROSS LENGTH OF PROJECT = 9,557 LINEAL FEET (1.81 MILES)
NET LENGTH OF PROJECT = 9,557 LINEAL FEET (1.81 MILES)

PEDESTRIAN TRAFFIC DATA

POSTED: 20 MPH
DESIGN: 20 MPH
ADT: 100 PEDESTRIAN USERS PER DAY
FUNCTIONAL CLASSIFICATION: SHARED USE PATH



FULL SIZE PLANS HAVE BEEN PREPARED USING STANDARD ENGINEERING SCALES. REDUCED SIZED PLANS WILL NOT CONFORM TO STANDARD SCALES.



and Associates, Inc.



Date of Signature: 10-02-09

SHEET NO. SECTION 06-P4001-00-BR FED. ROAD DIST. NO. 1 ILLINOIS CONTRACT NO. 63333



10/00 20 09 DAYID J. RAICA DIRECTOR OF PLANNING AND DEVELOPMENT, CARY PARK DISTRICT OctoBER 27 CHECH CHROSOPHEAHOLT DISTRICT 1 ENGINEER OF LOCAL ROADS AND STREETS OCTOBER 29, Dine M. O'Keepe gr

DATUM CONVERSION FOR CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY

NAVD 88 (GEOID 03)-LOWER TOPO 0.11' (WITH AN ACCURACY OF PLUS OR MINUS 0.05') NGVD 29-RAISE TOPO 0.07' (WITH AN ACCURACY OF PLUS OR MINUS 0.05')

J.U.L.I.E JOINT UTILITY LOCATION INFORMATION FOR EXCAVATION 8-1-1 OR 1-800-892-0123

PRINTED BY THE AUTHORITY OF THE STATE OF ILLINO'S

CONTRACT NO 63333

- DIMENSIONS: IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY ALL DIMENSIONS AND CONDITIONS
 EXISTING IN THE FIELD PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.
- THE THICKNESS OF MIXTURES SHOWN IN THE PLANS ARE NOMINAL. DEVIATIONS MAY OCCUR DUE TO IRREGULARITIES IN THE SURFACES OR BASIS ON WHICH THEY ARE TO BE PLACED. PLAN THICKNESS SHOULD BE CONSIDERED THE MINIMUM THICKNESS PERMITTED.
- 4. DEPRESSED CURB: RAMPS OR DEPRESSED CURBS ACCESSIBLE TO THE HANDICAPPED SHALL BE PROVIDED AT ALL CROSSWALKS AND BICYCLE PATHS.
- PAVEMENT GRADES: THE ELEVATIONS INDICATED ON THE PLANS ARE FINISHED GRADES OF PROPOSED PAVEMENT OR SURFACE COURSE, UNLESS OTHERWISE INDICATED.
- 6. ALL BRUSH CLEARING RIGHT OF WAY RESTORATION SHALL BE COMPLETED BY THE CONTRACTOR.
- SUÍTABLE GRADING MATERIAL ON THIS PROJECT SHALL CONSIST OF ALL SOILS ENCOUNTERED WITH THE EXCEPTION
 OF TOPSOIL, DEBRIS, ORGANIC MATERIAL, MUCK, AND OTHER UNSUITABLE MATERIAL DEFINED BY PSI GEOTECHNICAL
 REPORT DATED FEBRUARY 22, 2008.
- SUITABLE GRADING MATERIAL FROM ALL PORTIONS OF THE PROJECT SHALL BE USED IN FILL AREAS, AS REQUIRED, THROUGHOUT THE PROJECT.
- 9. PROVIDE FOR A SAWCUT WHERE PLACING NEW PAVEMENT NEXT TO IN PLACE PAVEMENT.
- 10. THE CONTRACTOR SHALL OBTAIN ALL APPLICABLE PERMITS AND LICENSES AND KEEP COPIES OF THE SAME ON SITE DURING CONSTRUCTION.
- ALL INFORMATION SHOWN ON THESE DRAWINGS RELATIVE TO EXISTING CONDITIONS IS GIVEN AS THE BEST PRESENT KNOWLEDGE, BUT WITHOUT GUARANTEE OF ACCURACY, THE CONTRACTOR SHALL REPORT IMMEDIATELY TO THE ENGINEER AND CARY PARK DISTRICT ANY CONDITIONS CONFLICTING WITH THE DRAWINGS. FIELD MODIFICATIONS TO THE DRAWINGS SHALL NOT BE MADE WITHOUT THE CONSENT OF THE ENGINEER AND CARY PARK DISTRICT.

- 15, ANY DAMAGE TO ADJACENT PROPERTIES AND INFRASTRUCTURE OUTSIDE OF CONSTRUCTION LIMITS SHALL BE RESTORED BY CONTRACTOR AND CONSIDERED INCIDENTAL TO THE PROJECT.
- 16. TRAIL VERTICAL AND HORIZONTAL ALIGNMENT DESIGNED IN ACCORDANCE WITH IDOT BUREAU OF LOCAL ROADS AND STREETS MANUAL, AASHTO GUIDE FOR DEVELOPMENT OF BICYCLE FACILITIES AND CARY PARK DISTRICT STANDARDS.
- 18. CONTRACTOR IS RESPONSIBLE FOR CONSTRUCTION STAKING. CONTRACTOR TO MAINTAIN ALL STAKING AND REPLACE.
 ALL STAKES DISRUPTED BY CONSTRUCTION ACTIVITY. CONTRACTOR SHALL MAKE CORRECTIONS AND PROVIDE
 ADDITIONAL STAKING IF REQUIRED BY CARY PARY DISTRICT OR CONSTRUCTION OSSERVATION ENGINEER.

UTILITY NOTES

- 1. THE PLAN DOES NOT CLARANTEE THE EXISTENCE, NONEXISTENCE SIZE, TYPE, LOCATION, ALICAMENT, OR DEPTHOF ANY OR ALL UNDERGROUND UTILITIES OR OTHER FACULTIES. WHERE SURFACE FEATURES (MANHOLES, CATCH
 BASINS, ML CES, ETC.) ARE UNAVAILABLE OR INCONCLUSIVE, INFORMATION SHOWN MAY BE FROM DIST,
 OWNER'S RECORDS AND OR ELECTRONIC TRACING, THE RELIABILITY OF WHICH IS UNCERTAIN. THE CONTRACTOR
 SHALL PERFORM TEST EXCAVATION OR OTHER REINVESTIGATION AS NECESSARY TO VERIFY LOCATION AND
 CLEARANCES.
- 2. UNLESS OTHERWISE NOTED, UTILITIES ARE NOT TO BE ADJUSTED BY THE RESPECTIVE OWNER.
- 3. EXISTING MANHOLE COVERS, VALVE COVERS, CLEANOUTS AND WATER METER LIDS SHALL BE ADJUSTED TO THE PROPOSED FINISHED GRADE.

CLEARING AND GRUBBING

- THE CONTRACTOR SHALL PERFORM DEMOLITION ACTIVITIES AS NOTED AND SHOWN ON THESE PLANS AND AS DIRECTED BY CARY PARK DISTRICT.
- 2. INSTALL ALL EROSION AND SEDIMENT CONTROL DEVICES AND TREE PROTECTION PRIOR TO BEGINNING DEMOLITION
- DEMOLITION DETAILS AND NOTES ARE INTENDED TO DEPICT GENERAL DEMOLITION AND UTILITY WORK AND ARE NOT INTENDED TO IDENTIFY EACH ELEMENT OF DEMOLITION OR RELOCATION. CONTRACTOR SHALL COORDINATE WITH CARY PARK DISTRICT, MCHENRY COUNTY, COMED, IDOT AND APPROPRIATE UTILITY COMPANIES PRIOR TO COMMENCING WORK.
- 4. THE CONTRACTOR IS RESPONSIBLE FOR THE PROTECTION OF ALL UTILITIES TO REMAIN IN PLACE
- ALL MATERIALS REMOVED UNDER CLEARING WORK, NOT TO BE RELOCATED OR TO BE TURNED OVER TO THE OWNER, SHALL BE REMOVED AS DIRECTED BY CARY PARK DISTRICT.
- THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS TO AVOID UNNECESSARY DAMAGE TO EXISTING ROAD SURFACES AND PUBLIC OR PRIVATE PROPERTY.
- ALL EXISTING ITEMS TO REMAIN WHICH ARE DAMAGED DURING CONSTRUCTION SHALL BE RESTORED TO ITS ORIGINAL CONDITION AT THE EXPENSE OF THE CONTRACTOR.
- 8. THE CONTRACTOR SHALL LEGALLY DISPOSE OF ALL DEBRIS IN A MANNER THAT PROVIDES A SAFE TRAIL ENVIRONMENT AND RESULTS IN LIMITED DISTURBANCE TO EXISTING ON-SITE OR ADJACENT VEGETATION. CARY PARK DISTRICT MLL PROVIDE A STOCKPILE LOCATION AT CARY GROVE SITE FOR ALL CHIPPED MICH MATERIALS. CARY PARK DISTRICT USE. ALL LOGS AND ROOTS SHALL BE LEGALLY REMOVED FROM THE SITE AT THE COST OF THE CONTRACTOR.
- ONLY THOSE TREES DESIGNATED BY THE CARY PARK DISTRICT AND ENGINEER SHALL BE REMOVED. THE CONTRACTOR SHALL PROTECT ALL REMAINING TREES FROM DAMAGE DUE TO CONSTRUCTION OPERATIONS.
- 10. IF ANY ASH TREES REQUIRE REMOVAL, CONTRACTOR SHALL DOUBLE CHIP. NO CHIPPED MATERIALS SHALL BE LARGER THAN 1° IN DIAMETER ON ANY SURFACE. IF LOGS ARE TO REMAIN ON SITE, THE SHALL BE STRIPPED. NO TREE OR STUMP MATERIALS WILL BE REMOVED BY CONTRACTORS TO LOCATIONS OUT OF THE COUNTY OR STATE.

- THE PAVEMENT MATERIALS ON THIS PROJECT WILL BE PAID FOR ON THE TONNAGE BASIS. THE WEIGHT WILL VARY IN ACCORDANCE WITH THE SPECIFIC GRAVITY OF THE AGGREGATES AND THE ASPHALTIC CONTENT OF THE MIX ACTUALLY USED TO SECURE THE DESIGN DEPTH. THE WEIGHT OF THE MSPHALT CONCRETE IS BASED ON 95% OF THE THEORETICAL MAXIMUM DENSITY.
- FOR ALL OPEN CUT TRENCHES IN THE ROADWAY, THE PAVEMENT PATCH SHALL BE IN ACCORDANCE WITH IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (2007).
- ALL PAVING ACTIVITIES WILL BE IN ACCORDANCE WITH CARY PARK DISTRICT, IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND GEOTECHNICAL SPECIFICATIONS PER REPORT BY PSI DATED FEBRUARY 22, 2004.

- 1. THE GRADE LINE DENOTES TOP OF FINISHED PAVEMENT UNLESS SHOWN OTHERWISE ON TYPICAL SECTIONS OR PLANS.
- EARTHWORK QUANTITIES ON THIS PROJECT ARE BASED ON ANTICIPATED SETTLEMENT AND MAY REQUIRE ADJUSTING DURING CONSTRUCTION.
- THE COST OF REMOVAL OF ALL EXISTING CONCRETE ITEMS, ASPHALT PAVEMENT, ETC. LOCATED IN AREA TO BE GRADED, SHALL BE INCLUDED IN PRICE BID FOR REGULAR EXCAVATION.
- 4. WHERE UNSUITABLE MATERIAL IS IDENTIFIED DURING CONSTRUCTION AS VERIFIED BY THE INSPECTOR, THE CONTRACTOR SHALL REMOVE IT AND BACKFILL WITH SUITABLE MATERIAL.
- ALL UNSUITABLE BACKFILL MATERIAL SHALL BE DISPOSED OF BY THE CONTRACTOR AT HIS OWN EXPENSE. SUITABLE BACKFILL MATERIAL MAY BE REUSED ANYWHERE ON THE JOB AND MAY BE STOCKPILED FOR REUSE. STORNG, TRANSPORTATION, LOADING, AND OTHER ASSOCIATED COSTS ARE TO BE INCLUDED IN THE BID PRICE FOR EARTH EXCAVATION.

DRAINAGE

- 3. IF, DURING CONSTRUCTION, THE CULVERT INVEST ELEVATIONS SHOWN ON THE PLANS ARE FOUND TO DIFFER SIGNIFICANTLY FROM THE ELEVATIONS OF THE STREAM OR SWALE IN WHICH THE CULVERT SHALL BE PLACED, THE CONTRACTOR WILL CONFER WITH CARY PARK DISTRICT AND THE ENGINEER BEFORE INSTALLING THE CULVERT.

- IF PRECAST DRAINAGE STRUCTURES ARE USED, SHOP DRAWINGS MUST BE SUBMITTED ALONG WITH PROPER CERTIFICATION, UNLESS PREVIOUSLY APPROVED BY CARY PARK DISTRICT.
- ALL PIPE CULVERTS LOCATED WITHIN RIGHT-OF-WAY EXCAVATION AREAS THAT ARE SUBJECT TO TRAFFIC LOADS SHALL BE BACKFILLED WITH A SELECT OR GRANULAR MATERIAL AND PLACED IN LAYERS AND COMPACTED PER IDOT STANDARDS AND SPECIFICATIONS (2007).
- 8. RIP RAP MUST BE PROVIDED AT ALL ENDWALLS AND FLARED END SECTIONS AS REQUIRED BY THE CARY PARK DISTRICT.
- THE CONTRACTOR SHALL MAINTAIN ALL DRAINAGE, STORMWATER MANAGEMENT, AND BEST MANAGEMENT PRACTICES
 FACILITIES AND SYSTEMS TO ENSURE THAT THEY FUNCTION PROPERLY DURING CONSTRUCTION.
- EXISTING DRAINAGE FACILITIES BEING UTILIZED AS APART OF THE DRAINAGE SYSTEM AND DESIGNATED ON THE PLANS TO BE CLEANED OUT SHALL BE CLEANED AS DIRECTED BY THE ENGINEER. THE COST TO GLEAN OUT PIPES WILL BE CONSIDERED INDICENTAL AND SHALL BE INCLUDED IN THE CONTRACT PIRCE FOR OTHER BID ITEMS.
- 11. THE CONTRACTOR SHALL INSTALL TRAIL IN ACCORDANCE WITH SLOPES, DRAINAGE, AND DETAILS SHOWN IN PLAN SET.

- CONSTRUCTION WILL TAKE PLACE ADJACENT TO ONGOING TRAFFIC OPERATIONS. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES WITH CARY PARK DISTRICT, MCHENRY COUNTY, AND IDOT.
- 2. THE CONTRACTOR SHALL SUBMIT A SCHEDULE FOR CONSTRUCTION TO CARY PARK DISTRICT IN ACCORDANCE WITH PROJECT SPECIFICATIONS.
- 3. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL ARRANGE A MEETING WITH CARY PARK DISTRICT TO DISCUSS COORDINATION OF CONSTRUCTION ACTIVITIES AND RELATED PROJECTS. THE CONTRACTOR SHALL PARTICIPATE IN PERIODIC MEETINGS AND SHALL SUBMIT SCHEDULE UPDATES AT THESE MEETINGS.

- SUBMITTALS ON MATERIALS FOR THE PROJECT SHALL BE PROVIDED TO CARY PARK DISTRICT FOR APPROVAL PRIOR TO ORDERING MATERIALS AND BEGINNING CONSTRUCTION.
- ALL CLEARING, GRUBBING, GRADING, AND PAVING SHALL BE PERFORMED IN ACCORDANCE WITH IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (2007).

- WHEN MATERIALS WHICH ARE UNSUITABLE FOR SUBGRADES OR ROADWAY PURPOSES OCCUR WITHIN THE LIMITS OF CONSTRUCTION, THE CONTRACTOR SHALL BE REQUIRED TO EXCAVATE SUCH MATERIAL BELOW THE GRADE SHOWN ON THE PLANS. THE AREAS SO EXCAVATES SHALL BE BEACHILED WITH APPROVED SUITABLE SELECT FILL MATERIAL. CARY PARK DISTRICT WILL PROVIDE A LOCATION FOR TOP-SOIL, FILL AND ROOKS AT CARY GROVE SITE. TOPSOIL STOCKHELE SHALL NOT BE CONSTRUCTED HIGHER THAN 5' IN HEIGHT. ALL DEBRIS WILL BE SUBJECT TO CARY PARK DISTRICT REVIEW AND APPROVAL ANY OPD AUTHORIZED DEBRIS REMOVAL REQUESTS SHALL BE LEGALLY HAULED AWAY BY CONTRACTOR AND WILL BE MIDDENTAL TO THE PROJECT.
- ANY NECESSARY FILL UNDER PAVED AREAS SHALL BE PLACED IN ACCORDANCE WITH IDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (2007).
- ALL UNPAVED SURFACES SHALL BE GRADED TO PROVIDE POSITIVE DRAINAGE AWAY FROM PAVED AREAS AND TOWARD DRAINAGE STRUCTURES OR EXISTING PERVIOUS AREAS.
- DISTURBED AREAS WITHIN THE PROJECT LIMITS THAT WILL REMAIN INACTIVE FOR A PERIOD OF 14 CALENDAR DAYS OR LONGER SHALL BE TEMPORARILY STABILIZED WITH SEED AND STRAW, MULCH, OR OTHER ACCEPTABLE GROUND COVER.
- 10. THE CONTRACTOR IS REQUIRED TO NOTIFY CARY PARK DISTRICT THREE (3) DAYS PRIOR TO THE BEGINNING OF CONSTRUCTION AND SPECIFICALLY REQUEST INSPECTION BEFORE BEGINNING

 A. INSTALLATION OF SILTATION AND EROSION CONTROL MEASURES

 B. CLEARING AND GRUBBING

 C. EARTHWORK

 D. INSTALLATION OF ANY UNDERGROUND UTILITY, INCLUDING STORM PIPES, CULVERTS, AND INLETS

 E. BACKFILL OF ANY STORM DRAINAGE PIPE, CULVERTS, INLET, AND OTHER UTILITIES

 BEFORE TACKING SUBBASE, BASE, PAWING SURFACE (TRAIL AND SIDEWALK), STONE BRIDGING WORK

 INCLUDING ANALOGA VATION, GEO-TEXTILE AND STONE PLACEMENT.
- 11. THE TOP ELEVATION OF EXISTING MANHOLES SHALL BE ADJUSTED TO MEET THE FINAL PAVEMENT ELEVATION AT THE TIME OF FINAL PAVING OPERATIONS. ALL MANHOLES ARE TO BE PROTECTED FROM THE TRAVELING PUBLIC.
- 12. SHEETING AND SHORING OR OTHER APPROVED METHODS FOR TRENCH BRACING WILL BE REQUIRED ON THIS CONTRACT AS NEEDED TO MEET ALL SAFETY REQUIREMENTS. UNLESS OTHERWISE DIRECTED BY THE ENGINEER, SHEETING AND SHORING WILL BE REMOVED FROM ALL TRENCHES PRIOR TO BAGGETUING OPERATIONS. UNLESS SCIPICALLY DEPARTATIONS. UNLESS SCIPICALLY DEPARTATION WILL BE ALLOWED FOR SHEETING AND SHORING.
- 13. PER A CARY PARK DISTRICT FIELD REVIEW REQUIREMENT, CONTRACTOR TO PROVIDE PRELIMINARY TRAIL STAKING TO ASSIST MTH REVIEW AND PREPARATION FOR TREE WORK, CLEARING AND GRUBBING ACTIVITIES. CONTRACTOR TO PROVIDE STATION STAKING AT 200 INTERVALS OR LESS IN THOSE AREAS AND MARK WITH VERICAL AND HORIZONTAL CONTROLS. INSTRUCTIONS AND MAP LOCATIONS WILL BE PROVIDED AT THE CONSTRUCTION KICK OFF MEETING.

INCIDENTAL CONSTRUCTION NOTES

- ADDITIONAL EXCAVATED MATERIAL NOT USED FOR CONSTRUCTION WILL BE STOCKPILED ON SITE AT A LOCATION DESIGNATED BY THE CARY PARK DISTRICT. MOVEMENT AND PLACEMENT OF THIS MATERIAL SHALL BE INCLUDED IN THE UNIT PRICE FOR PAY CODE 20200100 EARTHWORK AND SHALL BE PERFORMED IN ACCORDANCE WITH SECTION 202 OF THE STANDARD SPECIFICATIONS.
- GENERAL CONTRACTOR SHALL PROVIDE EROSION CONTROL FOR STOCKPILE OF EXCAVATED MATERIAL. THIS ITEM SHALL
 BE INCLUDED IN THE UNIT PRICE FOR PAY CODE 28000400 PERIMETER EROSION BARRIER AND INSTALLED IN
 ACCORDANCE WITH SECTION 280 OF THE STANDARD SPECIFICATIONS.
- CONVEYANCE CULVERTS AND FLARED END SECTIONS SHALL BE INSTALLED PER SECTION 550 OF STANDARD SPECIFICATIONS AND INSTALLED PER MANUFACTURERS REQUIREMENTS.

SEQUENCE OF CONSTRUCTION

- 1. OBTAIN ALL APPLICABLE CONSTRUCTION PERMITS AND NOTICES TO PROCEED FROM ALL JURISDICTIONAL AUTHORITIES.
- 3. DO NOT INITIATE ANY LAND DISTURBING ACTIVITY UNTIL AUTHORIZED TO PROCEED BY CARY PARK DISTRICT AND IDOT
- 4. COORDINATE MOBILIZATION AND JOB SITE ACCESS WITH CARY PARK DISTRICT, MCHENRY COUNTY, AND IDOT. 5. INSTALL ALL EROSION CONTROL DEVICES AND MAINTAIN THROUGHOUT CONSTRUCTION. INSTALL SILT FENCE, SAFETY FENCE, AND DIVERSION DIVES AT DESIGNATED AREAS. ADDITIONAL SILT FENCE SHALL BE INSTALLED AT TEMPORARY LOCATIONS AS DIRECTED BY CARY PARK DISTRICT TO FACILITATE THE CONTROL OF EROSION AND SEDIMENTATION DURING INITIAL DEMOLITION AND GRADING ACTIVITIES.

STANDARD NUMBER 000001-05	<u>DESCRIPTION</u> STANDARD SYMBOLS. ABBREVIATIONS. AND PATTERNS
280001-05	TEMPORARY EROSION CONTROL SYSTEMS
424001-05	CURB RAMPS FOR SIDEWALKS
442201-03	CLASS C AND D PATCHES
601001-03	SUB-SURFACE DRAINS
60200101	CATCH BASIN, TYPE A
604001-03	FRAME AND LIDS, TYPE 1
604006-04	FRAME AND LIDS, TYPE 3
606001-04	CONCRETE CURB TYPE B AND COMBINATION CURB AND GUTTER
70100603	OFF-RD OPERATIONS, 2L, 2W, 15' (4.5 M) TO 24" (600 MM) FROM PAVEMENT EDGE
72000101	SIGN PANEL MOUNTING DETAILS
720006-02	SIGN ERECTION DETAILS
720011-01	METAL POSTS FOR SIGNS, MARKERS, AND DELINEATORS

SEEDING CHART

CLA:	SS - TYPE /HECTAGE)	SEEDS	LB/ACRE
1	LAWN MIXTURE(1)	KY BLUEGRASS PERENNIAL RYEGRASS CREEPING RED FESCUE	100[110] 60[70] 40[50]
18	LOW MAINTENANCE LAWN MIXTURE(1)	FINE LEAF TURF—TYPE FESCUE(2) PERENNIAL RYEGRASS RED TOP CREEPING RED FESCUE	150[170] 20[20] 10[16] 20[20]
4A	LOW PROFILE NATIVE GRASS(3)(4)	ANDROPOGON SCOPARIUS (LITTLE BLUE STEM)(5) BOUTELOUA CHRITPRONLIA (SIDE-OATS GRAMA)(5) ELYMUS CANADENSIS (CANADA WILD RYEX)(5) SPOROBOLUS HETEROLEPSIS (PRAIRIE DROPSEED)(5) ANNUAL RYEGRASS PEREMIALA RYEGRASS	5[5] 5[5] 1[1] 0.5[0.5] 25[25] 25[25] 15[15]

- (1) IN IDOT DISTRICTS 1 THROUGH 6, THE PLANTING TIMES SHALL BE APRIL 1 TO JUNE 15 AND AUGUST 1 TO NOVEMBER 1. SEEDING MAY BE PERFORMED OUTSIDE THESE DATES PROVIDED THE CONTRACTOR GUARANTEES A MINIMUM OF 75 PERCENT UNIFORM GROWTH OVER THE ENTIRE SEEDED AREA(S) AFTER ONE GROWING SEASON. THE GUARANTEE SHALL BE SUBMITTED TO THE ENGINEER IN WRITING PRIOR TO PERFORMENT THE WORK, AFTER ONE GROWING SEASON, AREAS NOT SUSTAINING 75 PERCENT UNIFORM GROWTH SHALL BE INTERSEEDED OR RESEEDED, AS DETERMINED BY THE ENGINEER, AT NO ADDITIONAL COST.
- COST.
 (2) SPECIFIC VARIETY AS SHOWN IN THE PLANS OR APPROVED BY THE ENGINEER.
 (3) FERTILIZER NOT REQUIRED.
 (4) PLANTING TIMES MAY 15 TO JUNE 30 AND OCTOBER 15 TO DECEMBER 1.
 (5) PLS-PURE LIVE SEED TO BE USED.
- PILSEPTORE LIVE SEED TO BE OSED.

 TIME OF YEAR FOR PLANTING ALL TYPES OF SEEDING SHALL BE BASED ON THE IDOT STANDARDS AND SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION SECTION 250.

DATUM CONVERSION FOR CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY

THE DATUM USED FOR THE CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY PERFORMED BY HACGER ENGINEERING, LLC. DATED FEB. 2, 2006 ("THE TOPO") WAS BASED ON MH 46 ON PAGE 25 OF 36 ON THE MANNARD CONSULTING LTD. RECORD DRAWNINGS FOR FOXFORD HIS SUBDIVISION. THE ASSET DETERMANCH STORY WITH SECRET SHOWS THE SECRET SERVEN HAVE SERVED WITH SECRET SHOWS THE SECRET SERVEN HAVE SERVED WITH SECRET SHOWS THE FLAREA (FOXFORD HILLS). THE PUBLISHED RECORD ELEVATION FOR MH 46 WAS B61.45 FEET. THE SELEVATION WAS USED TO ESTABLISH THE ELEVATIONS REPORTED BY "THE FORO." IT IS UNKNOWN TO HAGEOR ENGINEERING, LLC. AS TO THE SOURCE BENCHMARK OR PUBLISHED DATUM THAT WAS ORIGINALLY USED FOR THE RECORD DRAWNINGS OF FOXFORD HILLS SUBDIVISION PROJECT, DUE TO THE FACT HAGEOR ENGINEERING, LLC. WAS NEVER SUPPLIED THIS INFORMATION. HOWEVER, HAGEOR ENGINEERING, LLC. WAS NEVER SUPPLIED THIS INFORMATION. HOWEVER, HAGEOR ENGINEERING, LLC. WAS NEVER SUPPLIED THIS INFORMATION. HOWEVER, HAGEOR ENGINEERING, LLC. BY THE FACT OF THE FACT HAD ALLY SUPPLIED THE SUPPLIED THE SUPPLIED THE TOP OF UNIT THAT IS ASSED ON KARA COMPANY'S RTX CORS REFERENCE NETWORK USING NAVD 66 (GEOID 03) AS ITS REFERENCE DATUM. HAGEOR ENGINEERING, LLC. FOUND MH 48 ELEVATION (661.45) TO BE 0.11' HIGHER THAN THAT MEASURED WITH THE GPS EQUIPMENT (661.34) USING NAVD 86 (GEOID 03) AS THE REFERENCE DATUM. BASED ON THE GPS MEASURED HEIGHT THE CONVERSION TO NOYOU 29 ELEVATION FOR WH 48 USING NOS VERTICAN CONVERSION WOULD BE 661.52 (0.00" HIGHER THAN PUBLISHED MH 48 LEVATION). THE FORE MED OF VERTICAL ACCURACY OF THE GPS RTX MEASUREMENT (PLUS OR MINUS 0.05') IT IS UNKNOWN TO HAGEOR ENGINEERING, LLC. AS TO THE FRUE PUBLISHED DATUM USED FOR "THE TOPO" WITH OUT BEING SUPPLIED THE SOURCE.

BENCHMARK OF A RECORD ENGINEERING. LLC. PERFORD HILLS DEBUT ON MEASURED HILL THE PUBLISH DID HE FOR SURVEY BUILT ON.

BENCHMARK OF THE SURVEY BUILT ON.

BASED ON HAEGER ENGINEERING, LLC. GPS RTK MEASUREMENT OF MH 46 THE DATUM CONVERSION FOR "THE TOPO" IS AS FOLLOWS:

ITEM#	PAY CODE	DESCRIPTION	UNIT	TOTAL PARTICIPATING (Y047)	TOTAL NO PARTICIPAT
1	20100110	TREE REMOVAL (6-15 UNITS DIAMETER)	UNIT	24	
2	20100210	TREE REMOVAL (OVER 15 UNITS DIAMETER)	UNIT	4	
3	20101100	TREE TRUNK PROTECTION	EACH	33	
4	20200100	EARTH EXCAVATION	CU YD	8,330	
5	21001000	GEOTECHNICAL FABRIC FOR GROUND STABILIZATION	SQ YD	150	
6	25000100	SEEDING, CLASS 1	ACRE	0.26	
7	25000115	SEEDING, CLASS 1B	ACRE	0.74	
8	25000312	SEEDING, CLASS 4A	ACRE	1.80	
9	25100115	MULCH METHOD 2	ACRE	1.87	
10	*25100401	EXCELSIOR BLANKET, SPECIAL	SQ YD	7,794	
11	*28000255	TEMPORARY EROSION CONTROL SEEDING	ACRE	2.80	
12	28000400	PERIMETER EROSION BARRIER	FOOT	5,967	
13	28000510	INLET FILTERS	EACH	8	
14	35102000	AGGREGATE BASE COURSE, TYPE B, 8"	SQ YD	13,100	
15	40300100	BITUMINOUS MATERIALS (PRIME COAT)	GALLON	5,106	
16	40603080	HOT MIX ASPHALT BINDER COURSE, IL-19.0, N50	TON	1,287	
17	40603310	HOT MIX ASPHALT SURFACE COURSE, MIX "C", N50	TON	1,144	
18	42400200	PORTLAND CEMENT CONCRETE SIDEWALK, 5 INCH	SQ FT	1,612	
19	42400800	DETECTABLE WARNINGS	SQ FT	174	
20	44000100	PAVEMENT REMOVAL	SQ YD	164	
21	44000500	COMBINATION CURB AND GUTTER REMOVAL	FOOT	45	
22	44000600	SIDEWALK REMOVAL	SQ FT	134	
23	44201789	CLASS D PATCHES, TYPE II, 12 INCH	SQ YD	7	
24	48101200	AGGREGATE SHOULDER, TYPE B	TON	127	
25	550B0050	STORM SEWERS CLASS B, TYPE 1, 12"	FOOT	50	
26	△ 56400100	FIRE HYDRANT TO BE MOVED	EACH	1	
27	∆*56400810	FIRE HYDRANT EXTENSION	FOOT	10	
28	60107600	PIPE UNDERDRAINS, 4"	FOOT	265	
29	60200305	CATCH BASINS, TYPE A, 4'-DIAMETER TYPE 3 FRAME AND GRATE	EACH	1	
30	60250400	CATCH BASINS TO BE ADJUSTED WITH NEW TYPE I FRAME, OPEN LID	EACH	1	
31	*60604800	COMBINATION CONCRETE CURB AND GUTTER. TYPE B6.18 (SPECIAL)	FOOT	45	
32	67100100	MOBILIZATION	L SUM	1	
33	*70101700	TRAFFIC CONTROL AND PROTECTION	L SUM	1	
34	72000100	SIGN PANEL - TYPE 1	SQ FT	129	
35	72900100	METAL POST - TYPE A	FOOT	255	
36	72900100	THERMOPLASTIC PAVEMENT MARKING — LINE 4"	FOOT	3.427	
37	△78000200	THERMOPLASTIC PAVEMENT MARKING - LINE 6"	FOOT	475	
			FOOT		
38	△78000600	THERMOPLASTIC PAVEMENT MARKING - LINE 12"		730	
39	△78000650	THERMOPLASTIC PAVEMENT MARKING — LINE 24"	FOOT	73	
40	78300100	PAVEMENT MARKING REMOVAL	SQ FT	53	
41	*X0322671	STABILIZED CONSTRUCTION ENTRANCE	SQ YD	111	
42	*XX004895	PIPE UNDERDRAIN CLEANOUT, COMPLETE	EACH	3	
43	*X2010510	CLEARING AND GRUBBING	L SUM	1	
44	XX007150	RELOCATE EXISTING SIGN, TYPE 1	EACH	3	
45	*Z0013798	CONSTRUCTION LAYOUT	L SUM	1	

SUMMARY OF QUANTITIES FOR CARY COMMUNITY TRAIL IMPROVEMENTS

A SPECIALTY ITEMS



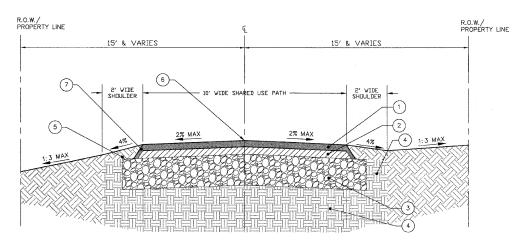
DESIGNED - JWH	REVISED
DRAWN - LES	REVISED
CHECKED - JMM	REVISED
DATE - 10/02/2009	REMSED

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

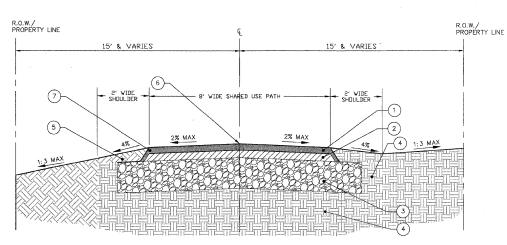
CARY PARK DISTRICT CARY COMMUNITY TRAIL **GENERAL NOTES, SUMMARY OF** QUANTITIES AND HIGHWAY STANDARDS

SECTION	COUNTY		TOTAL SHEETS	SHEE NO.
06-P4001-00-BR	MCHENRY		64	2
FED. ROAD DIST. NO. 1		ILLINOIS	CONTRACT	NO. 63333

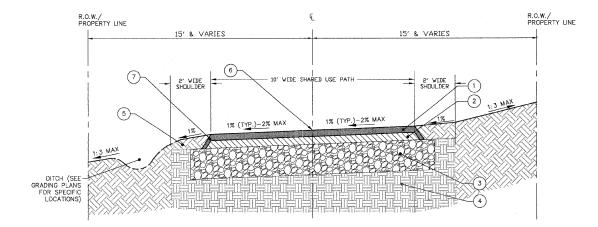
1) TYPICAL 10' SUPERELEVATED CROSS SECTION (SLOPE RIGHT)
NOT TO SCALE
STA. 10+00 TO 44+50, 50+00 TO 52+00, 78+00 TO 90+50,



③TYPICAL 10' CROWNED PATH CROSS SECTION STA. 74+30 TO 76+00, 76+40 TO 77+50



4 TYPICAL 8' CROWNED PATH CROSS SECTION NOT TO SCALE STA. 500+00 TO 503+69.25



(2) TYPICAL 10' SUPERELEVATED CROSS SECTION (SLOPE LEFT) NOT TO SCALE

STA. 45+00 TO 48+89.55, 200+00 TO 202+24.64, 52+50 TO 73+80, 91+00 TO 94+76.57, 100+50 TO 102+21.12

TRAIL CROSS SLOPE TRANSITION SEGEMENTS
44+50 TO 45+00, 52+00 TO 52+50, 73+80 TO 74+30,
77+50 TO 78+00, 90+50 TO 91+00, 100+00 TO 100+50,
111+00 TO 111+82.06

ITEM #	AIR VOIDS	
SHARED USE TRAIL		
HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50, 2"	4% @ 50 Gyr.	
HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2‡"	4% @ 50 Gyr.	
PATCHING		
HOT-MIX ASPHALT PATCH, CLASS D, TYPE III, 12"	4% @ 50 Gyr.	

THE UNIT WEIGHT USED TO CALCULATE ALL HOT-MIX ASPHALT SURFACE MIXTURE QUANTITIES 112 LBS/SY/IN PER IDOT.
FOR "AC TYPE" AND "PERCENT RAP" SEE DISTRICT ONE SPECIAL PROVISIONS.

LEGEND

- 1 PROPOSED HOT-MIX ASPHALT SURFACE COURSE, MIX "C", N50, 2" THICKNESS
- 2 PROPOSED HOT-MIX ASPHALT BINDER COURSE, IL-19.0, N50, 2.25" THICKNESS
- (3) PROPOSED AGGREGATE BASE, TYPE B 8" THICKNESS (12 FT WIDTH)
- (4) COMPACTED SUBGRADE
- 5 PROPOSED SHOULDER: AGGREGATE SHOULDER, TYPE B OR OR SEEDING CLASS 1, OR SEEDING CLASS 1B, OR SEEDING CLASS 4A (NOTE: SEE SHEETS 15-25 FOR SHOULDERS WITH AGGREGATE SHOULDER, TYPE B AND SEE SHEETS 54-58 FOR SHOULDERS WITH SEEDING)
- (6) PROPOSED THERMOPLASTIC PAVEMENT MARKING LINE 4" (NOTE: SEE PLANS FOR SPECIFIC LOCATIONS)
- (7) HAND TAMPED TAPER SEE SHEET 63 FOR CARY PARK DISTRICT DETAIL 13-01

NOTES

- FINAL PAVING CROSS SECTION, MATERIALS AND METHODS WILL BE PER GEOTECHNICAL REPORT BY PSI DATED FEBRUARY 22, 2008, SECTION 406 OF THE IDOT STANDARD SPECIFICATIONS AND CARY PARK DISTRICT SPECIFICATIONS.
- 2. SEE GRADING PLANS ON PAGES 54-58 AND CROSS SECTIONS ON PAGES 26-48 FOR SPECIFIC SLOPES, DITCH INSTALLATION AND TRANSITIONS.
- 3. FINAL AGGREGATE BASE WIDTH TO BE INSTALLED 12 FEET WIDE PER CARY PARK DISTRICT.



 DESIGNED - JWH
 REVISED

 DRAWN - LES
 REVISED

 CHECKED - JMM
 REVISED

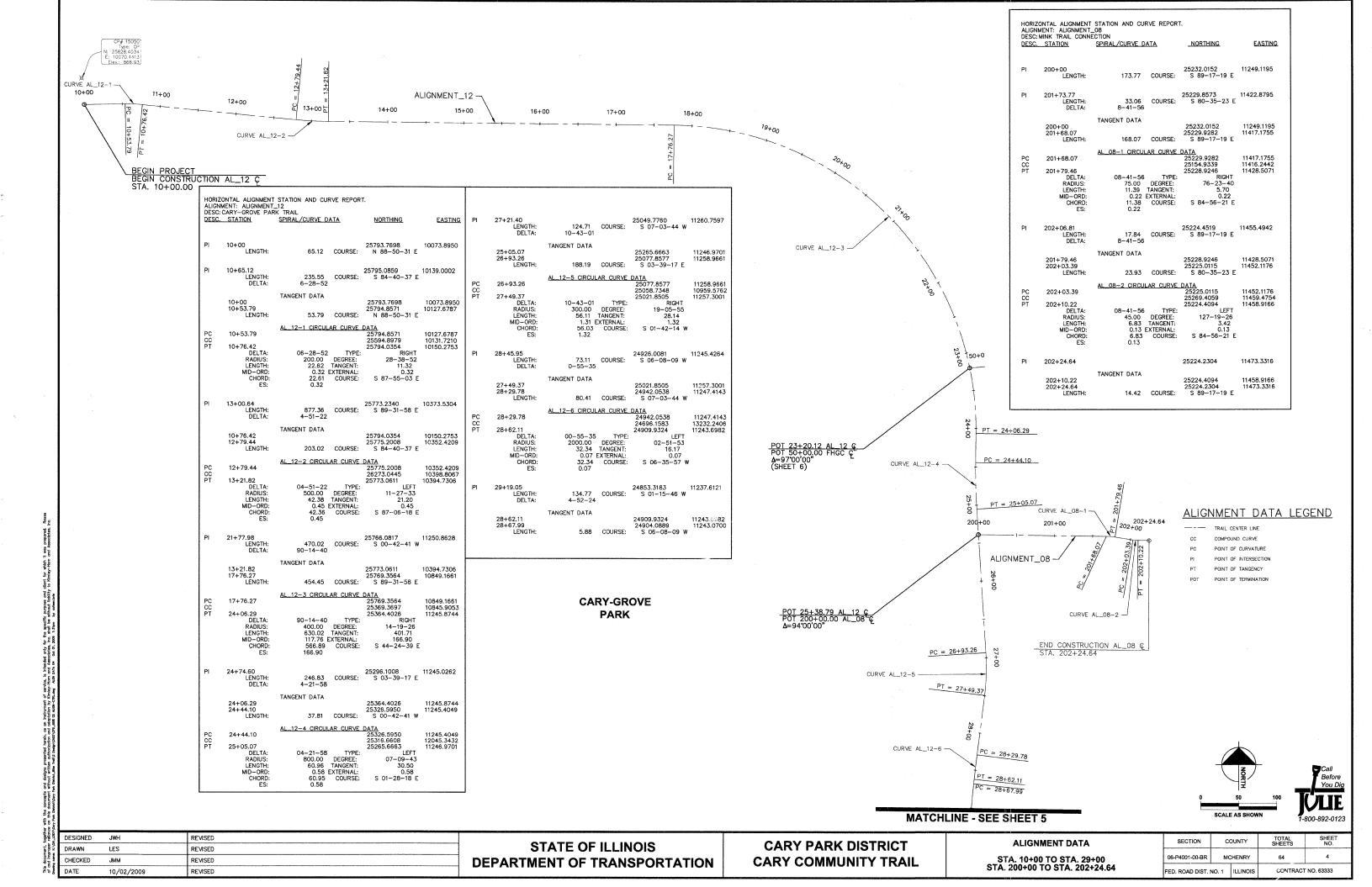
 DATE - 10/02/2009
 REVISED

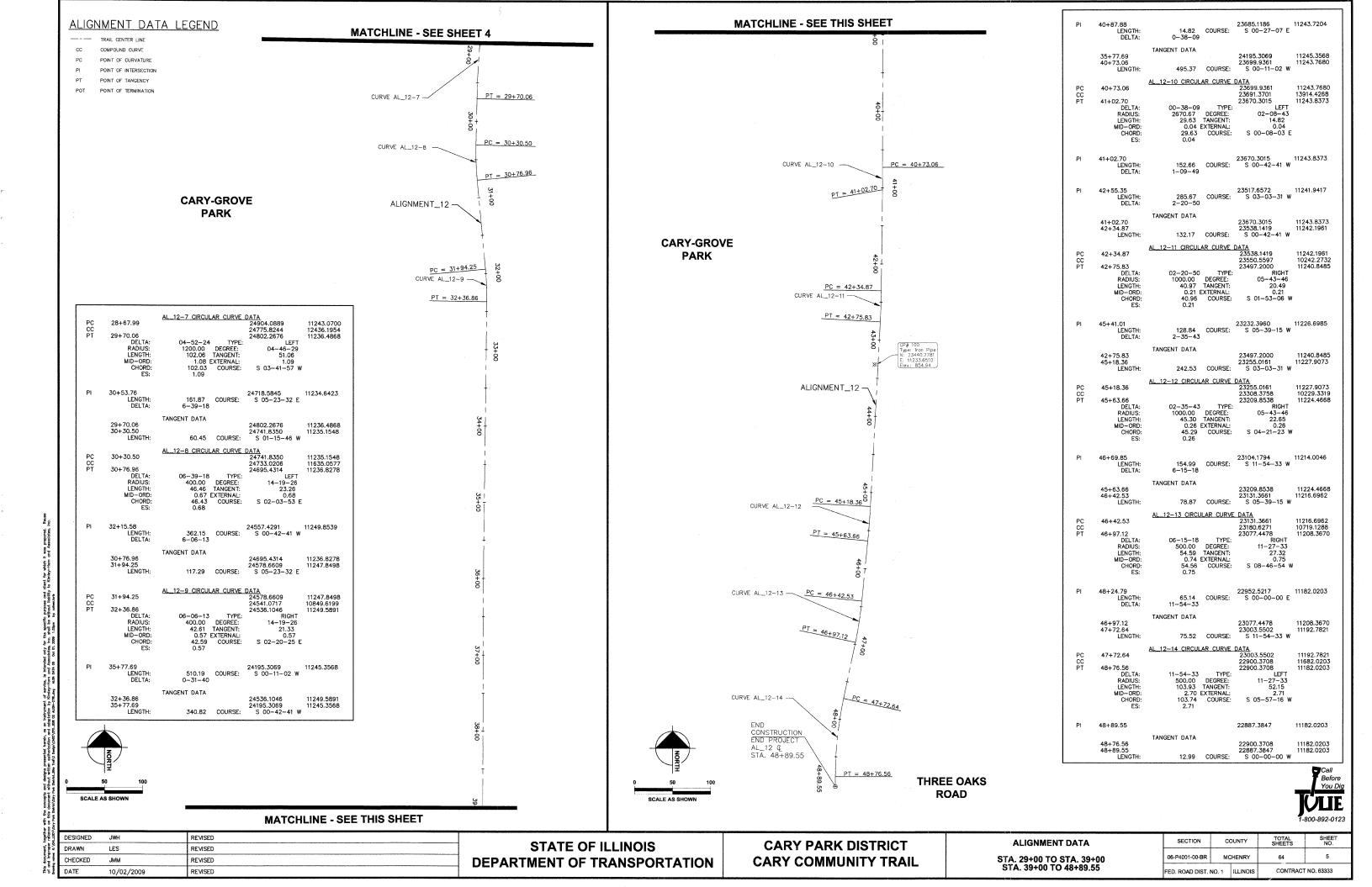
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 REVISED

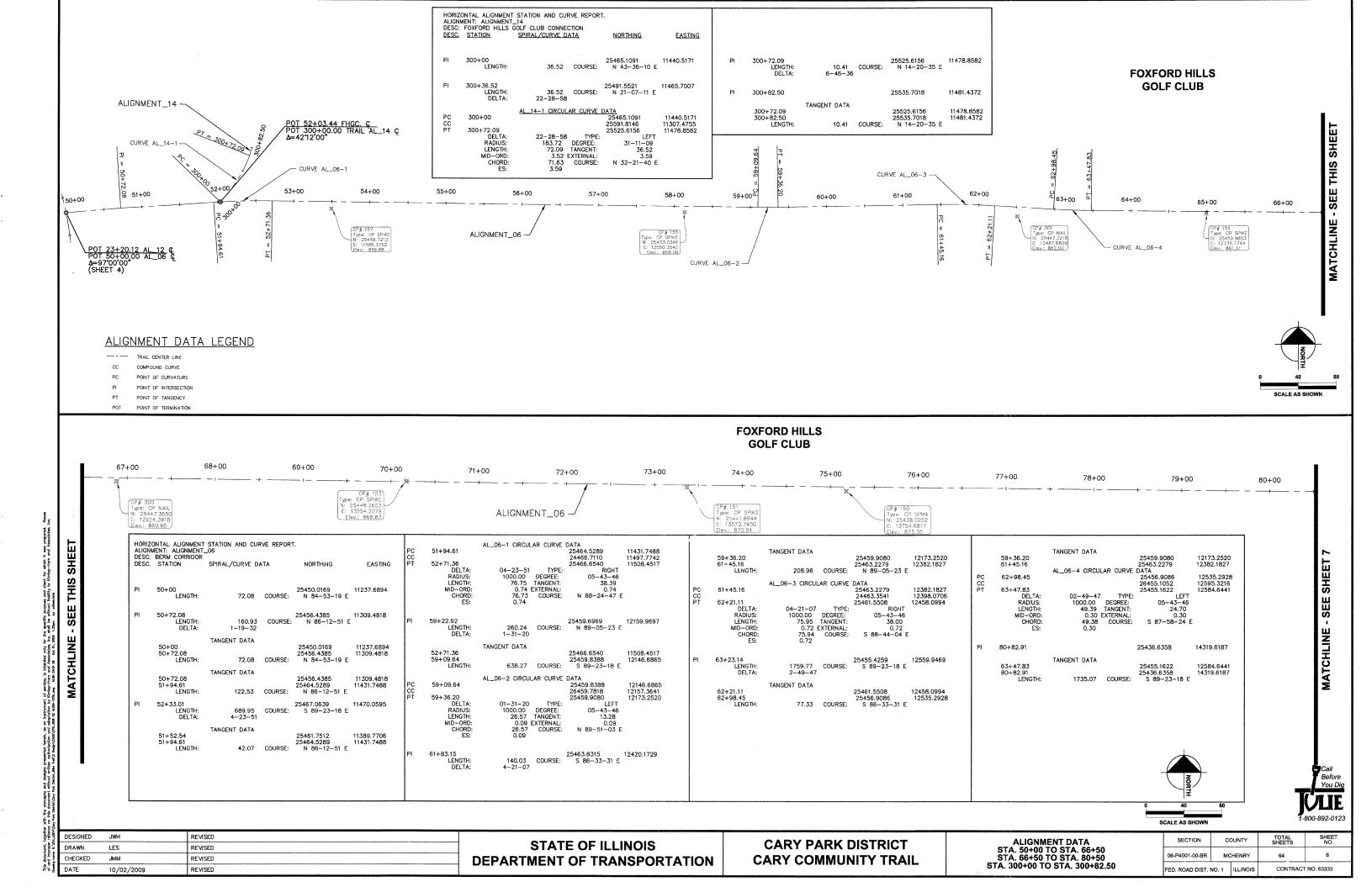
CARY PARK DISTRICT
CARY COMMUNITY TRAIL

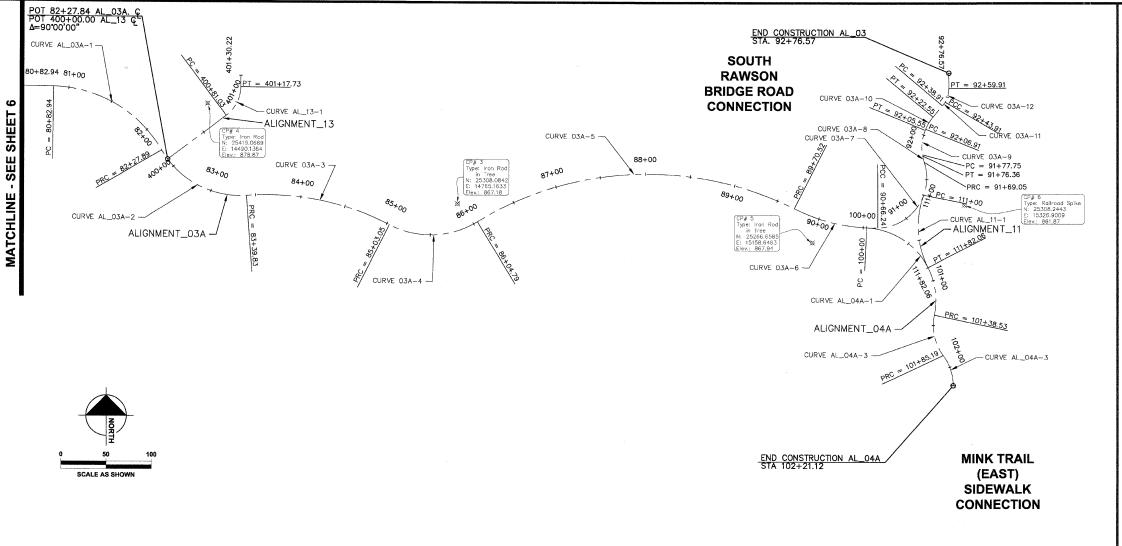
TYPICAL SECTIONS

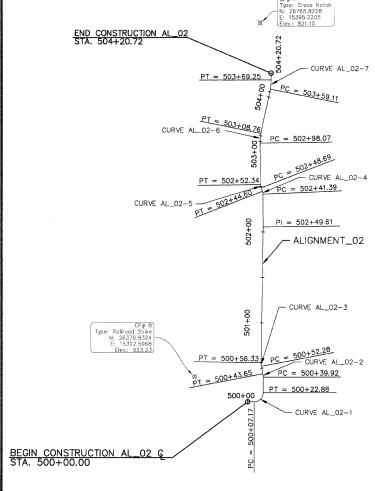
SECTION	C	OUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR M		CHENRY	64	3
FED. ROAD DIST. NO. 1		ILLINOIS	CUNTRACT	NO. 63333











CRYSTAL LAKE AVENUE CONNECTION

HORIZONTAL ALIGNMENT STATION AND CURVE REPORT. ALIGNMENT: ALIGNMENT_13 DESC: NEW CASTLE DRIVE CONNECTION DESC. STATION SPIRAL/CURVE DATA

401+17.73

401+30.22

401+30.22

400+00 LENGTH: 25356.7339 N 53-01-07 E 14445.9449 100.78 COURSE: 401+00.78 LENGTH: DELTA: 25417.3595 N 00-27-00 E 14526.4520 32.25 COURSE: 52-34-06 TANGENT DATA 25356.7339 25405.4755 N 53-01-07 E 400+00 14445.9449 400+81.03 LENGTH 81.03 COURSE: 400+81.03

AL_13-1 CIRCULAR CURVE DATA 25405.4755 25437.4287 25437.1144 14526.6072 52-34-06 TYPE: 40.00 DEGREE: 36.70 TANGENT: 4.14 EXTERNAL: 35.43 COURSE: 25437.1144 LEFT 143-14-22 19.76 4.61 N 26-44-04 E

EASTING

25449.6087 14526.7054 25437.1144 25449.6087 N 00-27-00 E 12.49 COURSE:

HORIZONTAL ALIGNMENT STATION AND CURVE REPORT. ALIGNMENT: ALIGNMENT_11
DESC: ALIGNMENT 03 / 04 CONNECTOR EASTING 25319.4190 S 20-04-51 W 111+00 15281.8516 LENGTH 43.50 COURSE: 111+43.50 LENGTH: PI 25278.5664 15266,9170 43.50 COURSE: 47-00-54 S 26-56-03 E AL_11A-1 CIRCULAR CURVE DATA 25319.4190 25285.0843 25239.7878 111+00 111+82.06 DELTA: RADIUS: LENGTH: 15286.6196 47-00-54 TYPE: 100.00 DEGREE: 82.06 TANGENT: 8.30 EXTERNAL: 79.77 COURSE: 9.05 LEET 57-17-45 43.50 9.05 S 03-25-36 E 111+82.06

SEE SHEET 08 FOR ADDITIONAL ALIGNMENT DATA

ALIGNMENT DATA LEGEND

POINT OF INTERSECTION

POINT OF TERMINATION





9	DESIGNED	JWH	REVISED
K K:\CHI	DRAWN	LES	REMSED
d nom	CHECKED	JMM	REVISED
Drawit	DATE	10/02/2009	REVISED

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

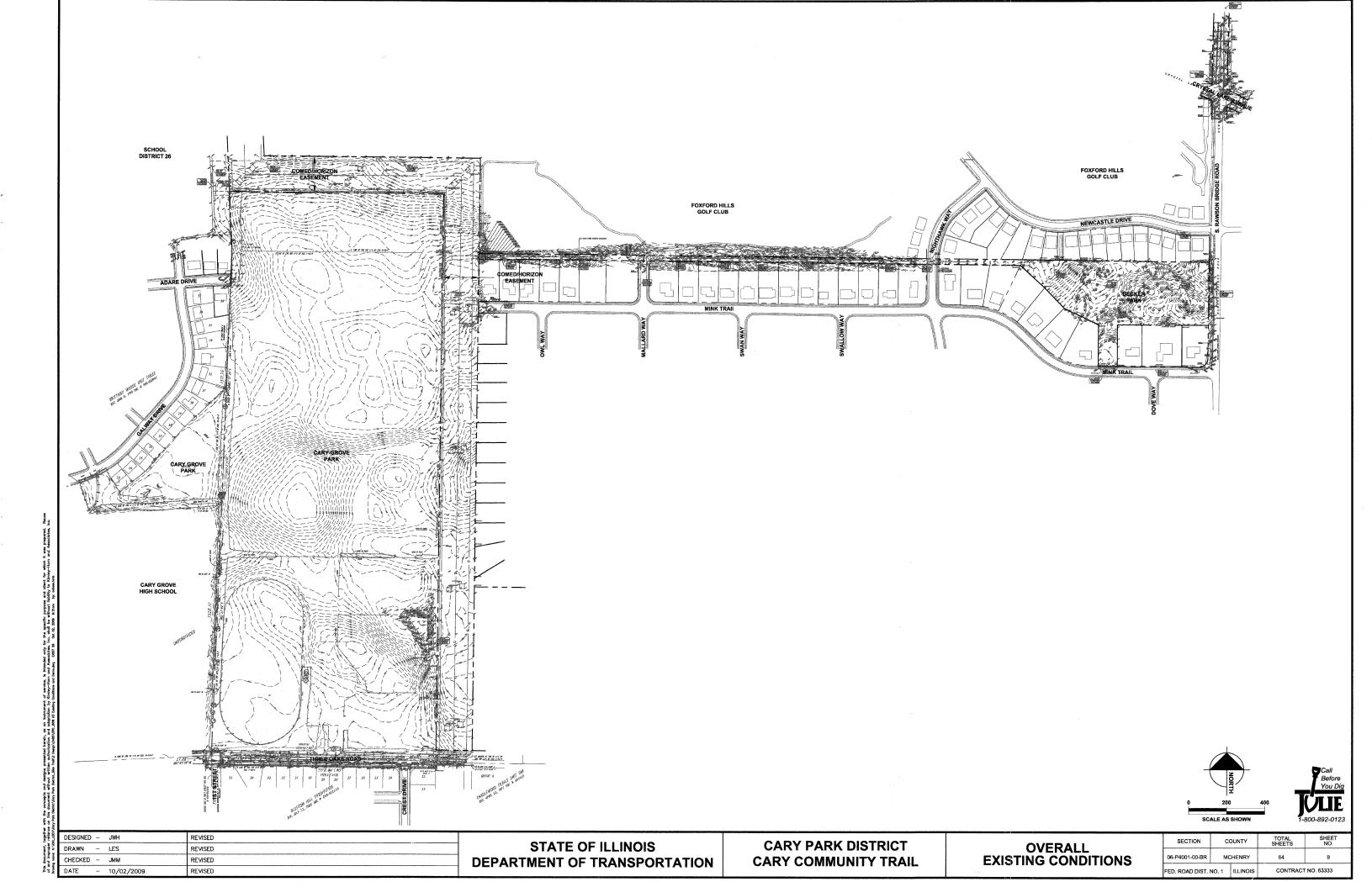
CARY PARK DISTRICT CARY COMMUNITY TRAIL

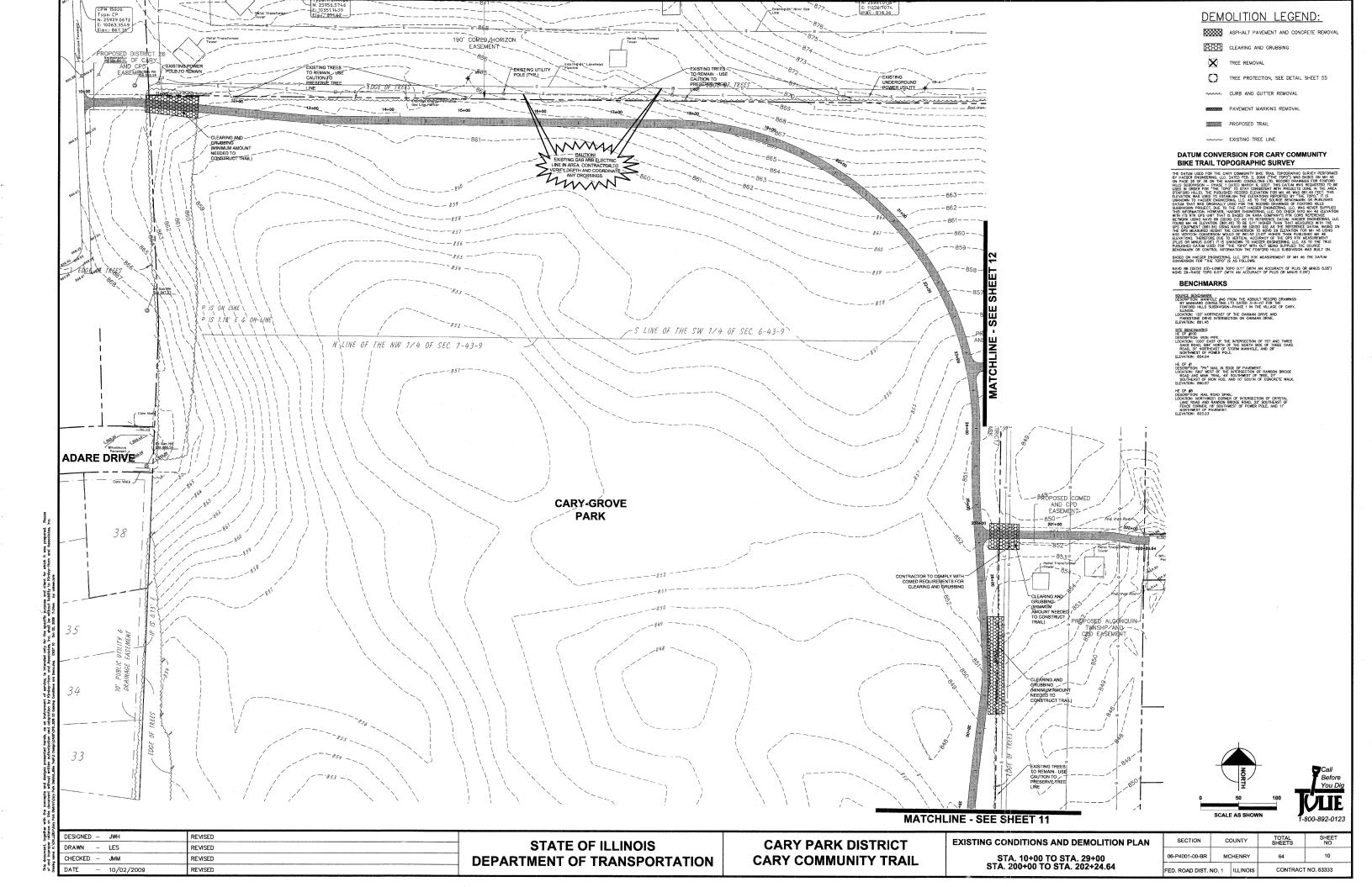
ALIGNMENT DATA STA. 80+50 TO 92+76.57 STA. 100+00 TO 102+21.12

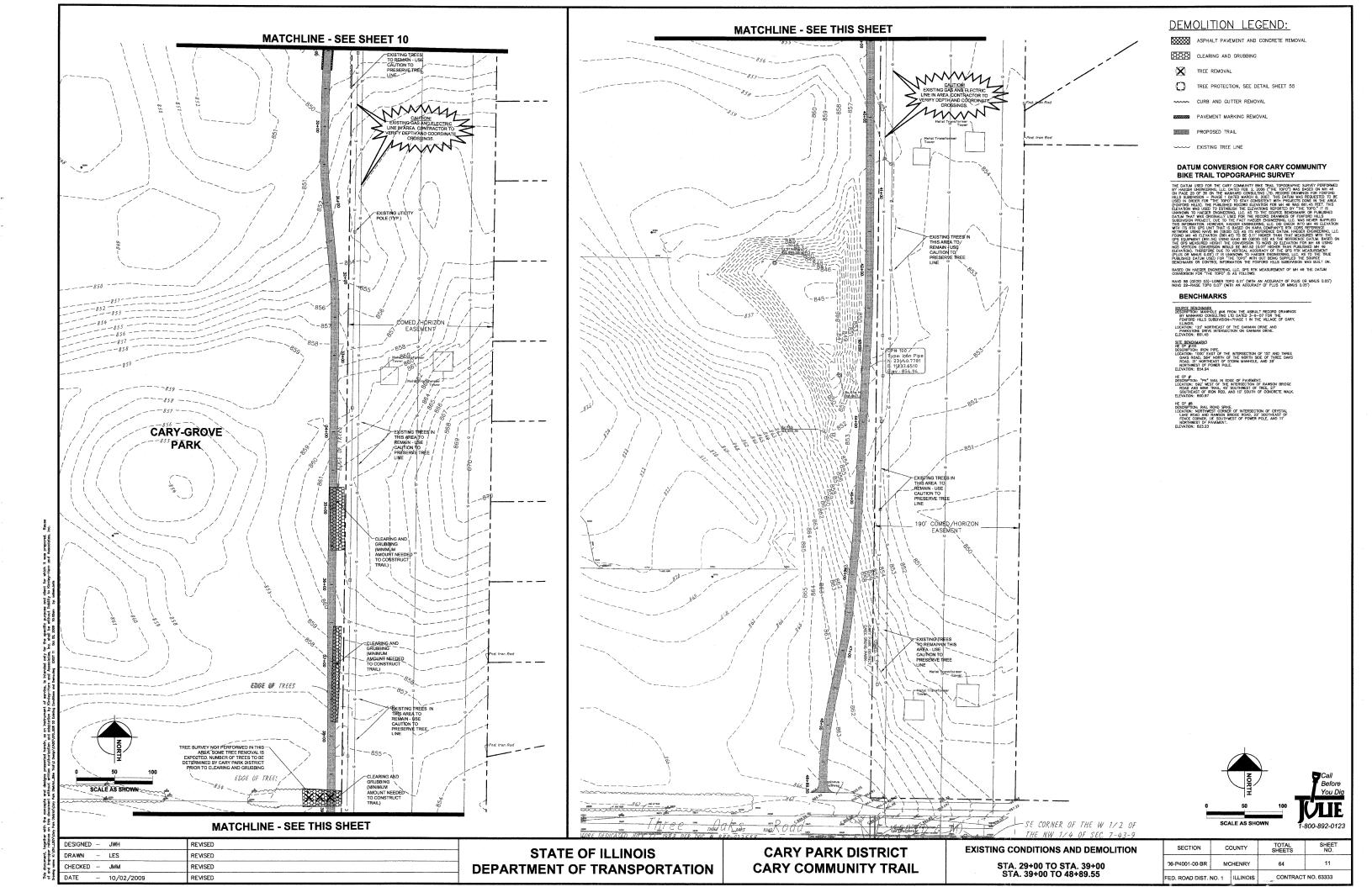
STA. 111+00 TO 111+82.06 STA. 409+00 TO STA. 401+30.22; STA. 500+00 TO 504+20.72 FED. ROAD DIST. NO. 1 ILLINOIS

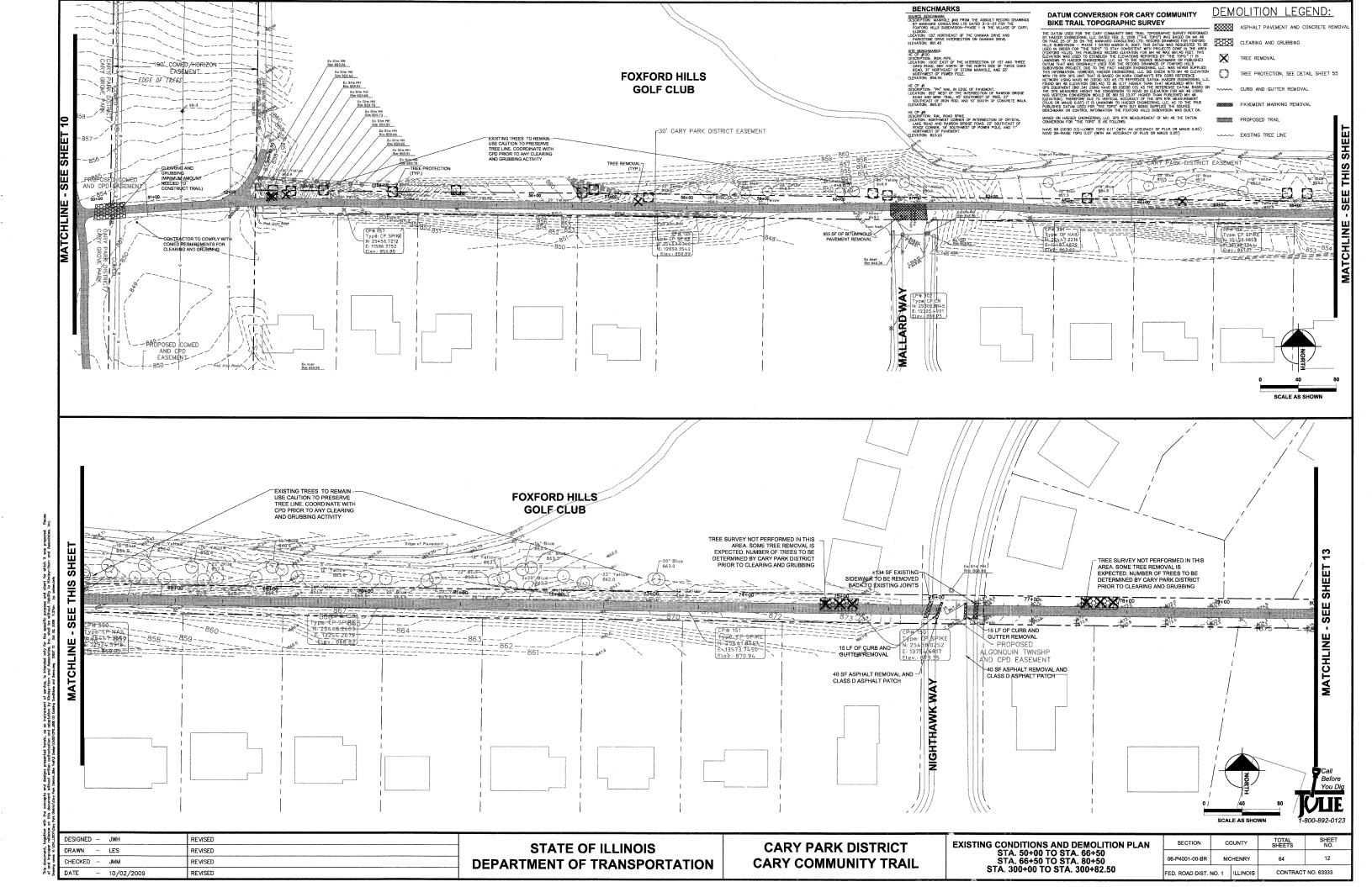
Name and Address of the Owner, where the Owner, which is the Owner, where the Owner, which is the Owner, where the Owner, which is the Owner, w		- Contract (1975)		
SECTION	COUNTY		TOTAL SHEETS	SHEET NO.
-P4001-00-BR	MCHENRY		64	7
			001177107	. 110. 00000

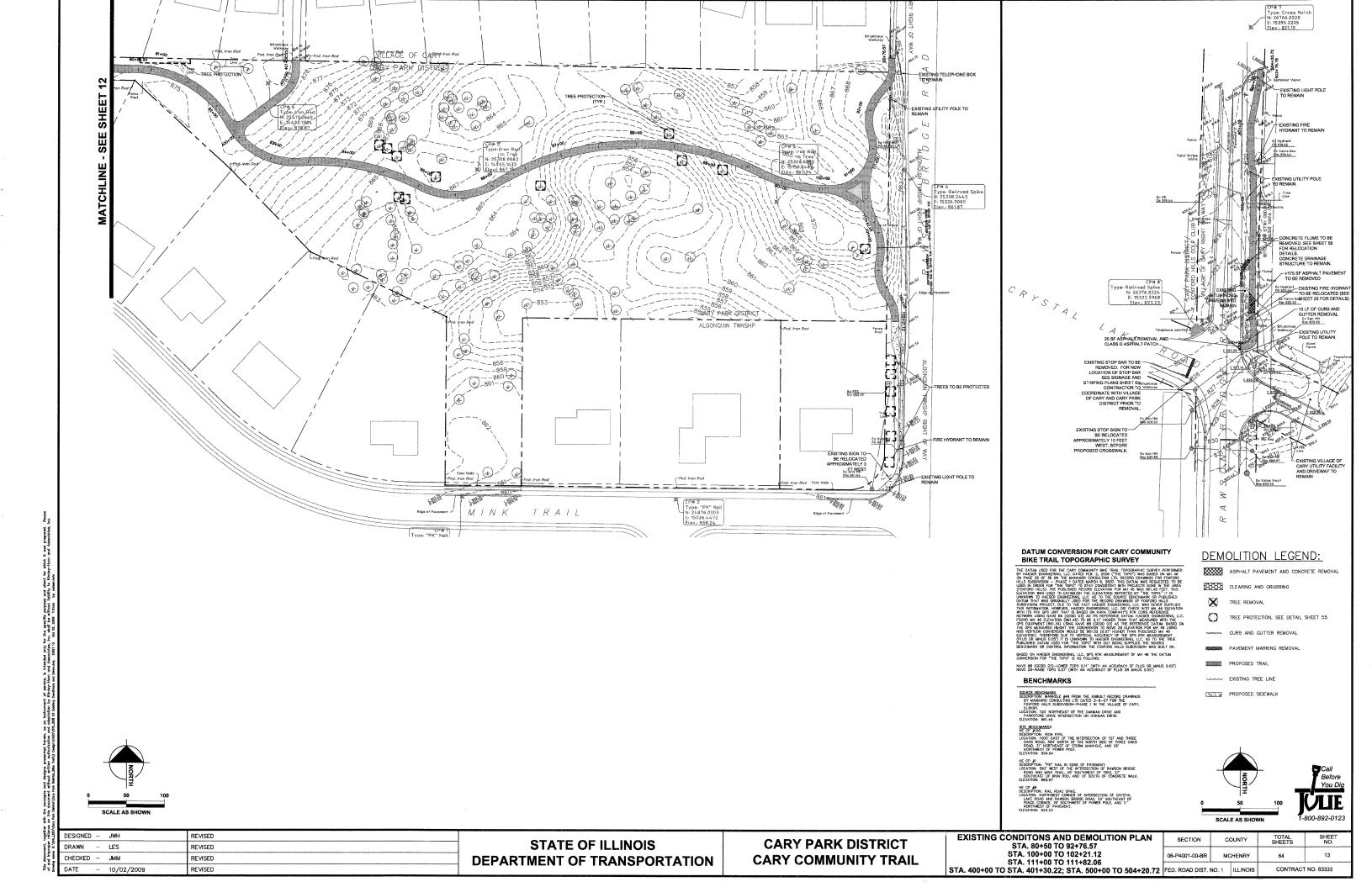
HORIZONTAL ALIGNMENT STATION AND CURVE REPORT. ALIGNMENT: ALIGNMENT_O3A DESC: SOUTH RAWSON BRIDGE ROAD CONNECTION DESC. STATION SPIRAL/CURVE DATA NORTHING EASTING	AL_03A-8 CIRCULAR CURVE DATA PC 91+69.05 25355.4114 15282.4665 CC 25370.9644 15329.9861 PT 91+76.36 25362.4939 15280.7088	AL_04A-1 CIRCULAR CURVE DATA PC 100+00	AL_02-3 CIRCULAR CURVE DATA PC 500+52.28
PI 80+82.94 25436.6358 14319.6187 LENGTH: 79.73 COURSE: S 89-23-18 E	DELTA: 08-22-11 TYPE: RIGHT RADIUS: 50.00 DEGREE: 114-35-30 LENGTH: 7.30 TANGENT: 3.66 MID-ORD: 0.13 EXTERNAL: 0.13 CHORD: 7.30 COURSE: N 13-56-18 W ES: 0.13	DELTA: 99-12-49 TYPE: RIGHT RADIUS: 80.00 DEGREE: 71-37-11 LENGTH: 138.53 TANGENT: 94.02 MID-ORD: 28.16 EXTERNAL: 43.45 CHORD: 121.86 COURSE: \$ 37-52-26 E ES: 43.45	DELTA: 11-35 TYPE: RIGHT RADIUS: 20.00 DEGREE: 286-29 LENGTH: 4.05 TANGENT: 2.03 MID-ORD: 0.10 EXTERNAL: 0.10 CHORD: 4.04 COURSE: N 04-54-11 W ES: 0.10
PI 81+62.67 25435.7846 14399.3400 LENGTH: 142.38 COURSE: S 30-04-04 E DELTA: 59-19-14 AL_03A-1 CIRCULAR CURVE DATA	PI 91+76.36 25362.4939 15280.7088 LENGTH: 1.39 COURSE: N 09-45-13 W DELTA: 0-00-00	PI 101+63.37 25162.9617 15288.5746 LENGTH: 43.47 COURSE: S 36-52-42 E DELTA: 48-36-40	PI 502+05.67 26541.8742 15398.4882 LENGTH: 35.72 COURSE: N 00-20-44 W DELTA: 1-14-12
PC 80+82.94 25.436.6358 14319.6187 CC 25296.6438 14318.1239 PT 82+27.89 25366.7872 14439.2846 DELTA: 59-19-14 TYPE: RIGHT RADIUS: 140.00 DEGREE: 40-55-32 LENGTH: 144.95 TANGENT: 79,73	PI 91+77.75 25363.8655 15280.4730 LENGTH: 14.27 COURSE: N 09-45-13 W DELTA: 0-00-00 TANGENT DATA	AL_04A-2 CIRCULAR CURVE DATA 25187.2826 15293.6258 CC PT 101+85.19 15347.4766 25176.0984 15347.4766 25143.0919 15303.4815 DELTA: 48-36-40 TYPE: LEFT RADIUS: 55.00 DEGREE: 104-10-27	TANGENT DATA 500+56.33 26392.5499 15396.1659 502+05.67 26541.8742 15398.4882 LENGTH: 149.34 COURSE: N 00-53-28 E
MID-ORD: 18.34 EXTERNAL: 21.11 CHORD: 138.56 COURSE: S 59-43-41 E ES: 21.11	91+76.36 25362.4939 15280.7088 91+77.75 25363.8655 15280.4730 LENGTH: 1.39 COURSE: N 09-45-13 W	LENGTH: 46.66 TANGENT: 24.84 MID-ORD: 4.88 EXTERNAL: 5.35 CHORD: 45.28 COURSE: S 12-34-22 E ES: 5.35	PI 502+41.39 26577.5978 15398.2728 LENGTH: 1.62 COURSE: N 02-33-59 W DELTA: 2-13-14 TANGENT DATA
PI 82+90.55 25312.5607 14470.6778 LENGTH: 146.34 COURSE: N 85-47-28 E DELTA: 64-08-28 AL_03A-2 CIRCULAR CURVE DATA	PI 91+92.02 25377.9281 15278.0557 LENGTH: 23.51 COURSE: N 22-06-05 E DELTA: 31-51-18 AL_03A-9 CIRCULAR CURVE DATA	PI 102+03.82 25128.1877 15314.6630 LENGTH: 18.63 COURSE: S 00-33-04 W DELTA: 37-25-46	502+05.67 26541.8742 15398.4882 502+41.39 26577.5978 15398.2728 LENGTH: 35.72 COURSE: N 00-20-44 W
PC 82+27.89 25366.7872 14439,2846 CC 25416.8896 14525.8279 PT 83+39.83 25317.1592 14533.1670 DELTA: 64-08-28 TYPE: LEFT RADIUS: 100.00 DEGREE: 57-17-45 LENGTH: 111.95 TANGENT: 62.66	PC 91+77.75 25363.8655 15280.4730 CC 25372.3360 15329.7503 PT 92+05.55 25372.3360 15329.7503 PELTA: 31-51-18 TYPE: RIGHT RADIUS: 50.00 DEGREE: 114-35-30 LENGTH: 27.80 TANCENT: 14.27	AL_04A-3 CIRCULAR CURVE DATA PC 101+85.19 25143.0919 15303.4815 CC 25110.0854 15259.4863 PT 102+21.12 25109.5563 15314.4838 DELTA: 37-25-46 TYPE: RIGHT RADIUS: 55.00 DEGREE: 104-10-27	PI 502+43.01 26579.2121 15398.2004 LENGTH: 7.55 COURSE: N 20-55-31 W DELTA: 18-21-32 AL_02-4 CIRCULAR CURVE DATA PC 502+41.39 C 502+41.39 PC 502+41.39 PC 502+41.39 PC 15398.2728
MID-ORD: 15.26 EXTERNAL: 18.01 CHORD: 106.19 COURSE: S 62-08-18 E ES: 18.01 PI 84+23.52 25323.3007 14616.6227 LENGTH: 1.39.44 COURSE: S 63-02-14 E	MID—ORD: 1.92 EXTERNAL: 2.00 CHORD: 27.44 COURSE: N 06-10-26 E ES: 2.00	LENGTH: 35.93 TANGENT: 18.63 MID-ORD: 2.91 EXTERNAL: 3.07 CHORD: 35.29 COURSE: S 18-09-49 E ES: 3.07	PT 502+44.60 2057.15 15397.6233
DELTA: 31-10-18 AL_03A-3 CIRCULAR CURVE DATA PC 83+39.83 AL_03A-3 CIRCULAR CURVE DATA 25317.1592 14533.1670	LENGTH: 26.76 COURSE: N 40-01-26 E DELTA: 17-55-21 TANGENT DATA 92+05.55 25391.1485 15283.4243	PI 102+21.12 25109.5563 15314.4838	CHORD: 3.19 COURSE: N 114445 W ES: 0.13 PI 502+50.53 26586.2642 15395.5039
CC 25017.9683 14555.1844 PT 85+03.05 25285.3585 14691.2080 DELTA: 31-10-18 TYPE: RIGHT RADIUS: 300.00 DEGREE: 19-05-55 LENGTH: 163.21 TANGENT: 83.68 MID-ORD: 11.03 EXTERNAL: 11.45 CHORD: 161.21 COURSE: S 78-37-23 E	92+06.91 25392.4078 15283.9358 LENGTH: 1.36 COURSE: N 22-06-05 E AL_03A-10 CIRCULAR CURVE DATA PC 92+06.91 25392.4078 15283.9358 CC 25373.5954 15330.2617 PT 92+22.55 25405.7507 15291.9729	HORIZONTAL ALIGNMENT STATION AND CURVE REPORT. ALIGNMENT: ALIGN_02 DESC: CRYSTAL LAKE AVENUE CONNECTION DESC. STATION SPIRAL/CURVE DATA NORTHING EASTING	LENGTH: 1.85 COURSE: N 00-00-00 E DELTA: 20-55-31 TANGENT DATA 502+44.60 502+48.69 LENGTH: 4.09 COURSE: N 20-55-31 W
ES: 11.45 PI 85+58.81 25260.0748 14740.9098 LENGTH: 255.85 COURSE: N 58-40-19 E DELTA: 58-17-27	DELTA: 17-55-21 TYPE: RIGHT RADIUS: 50.00 DEGREE: 114-35-30 LENGTH: 15.64 TANCENT: 7.88 MID-ORD: 0.61 EXTERNAL: 0.62 CHORD: 15.58 COURSE: N 31-03-46 E ES: 0.62	PI 500+00 LENGTH: 17.17 COURSE: 26349.2940 15381.2894 S 90-00-00 E PI 500+17.17 26349.2940 15398.4605	AL_02-5 CIRCULAR CURVE DATA PC 502+48.69 26584.5393 15396.1635 CC 26588.1108 15405.5039 PT 502+52.34 26588.1108 15395.5039 DELTA: 20-56 TYPE: RIGHT
AL_03A-4 CIRCULAR CURVE DATA PC 85+03.05 25285.3585 14691.2080 CC 25374.4886 14736.5493 PT 86+04.79 25289.0682 14788.5430 DELTA: 58-17-27 TYPE: LEFT	PI 92+41.42 25420.2015 15304.1088 LENGTH: 10.70 COURSE: N 30-28-52 E DELTA: 9-32-34	LENGTH: 28.91 COURSE: N 00-00-00 E DELTA: 90-00-00 TANGENT DATA 500+00 26349.2940 15381.2894 500+07.17 26349.2940 15388.4605	RADIUS: 10.00 DEGREE: 572-57 LENGTH: 3.65 TANGENT: 1.85 MID-ORD: 0.17 EXTERNAL: 0.17 CHORD: 3.63 COURSE: N 10-27-45 W ES: 0.17
RADIUS: 100.00 DEGREE: 57-17-45 LENGTH: 101.74 TANCENT: 55.76 MID-ORD: 12.66 EXTERNAL: 14.50 CHORD: 97.41 COURSE: N 87-49-03 E ES: 14.50	TANGENT DATA 92+22.55 25405.7507 15291.9729 92+38.91 25418.2840 15302.4984 LENGTH: 16.37 COURSE: N 40-01-26 E	LENGTH: 7.17 COURSE: S 90-00-00 E AL_02-1 CIRCULAR CURVE DATA PC 500+07.17 26349.2940 15388.4605 CC 26359.2940 15388.4605	PI 502+52.34 26588.1108 15395.5039 LENGTH: 45.73 COURSE: N 00-32-13 W DELTA: 0-32-13
PI 88+04.87 25393.1010 14959.4584 LENGTH: 248.88 COURSE: S 63-36-02 E DELTA: 57-43-39	AL_03A-11 CIRCULAR CURVE DATA 25418.2840 15302.4984 CC PT 92+43.91 25437.5772 15279.5251 PT 92+43.91 25422.3595 15305.3790 DELTA: 09-32-34 TYPE: LEFT RADIUS: 30.00 DEGREE: 190-59-09 LENGTH: 5.00 TANGENT: 2.50	DELTA: 90-00 TYPE: LEFT RADIUS: 10.00 DEGREE: 572-57 LENGTH: 15.71 TANGENT: 10.00 MID-ORD: 2.93 EXTERNAL: 4.14 CHORD: 14.14 COURSE: N 45-00-00 E	PI 502+98.07 26633.8368 15395.0754 LENGTH: 5.37 COURSE: N 00-38-36 W DELTA: 0-06-22 TANGENT DATA
### AL_03A-5 CIRCULAR CURVE DATA 25289.0682 14788.5430 25289.0682 14788.5430 24978.9919 14977.2802 25304.1388 15138.6797 25304.1388 25304.138	LENGTH: 5.00 TANGENT: 2.50 MID—ORD: 0.10 EXTERNAL: 0.10 CHORD: 4.99 COURSE: N 35-15-09 E ES: 0.10 PI 92+52.11 25429.4243 15309.5374	ES: 4.14 PI 500+41.79 26378.2065 15398.4605	502+52.34 26588.1108 15395.5039 502+98.07 26633.8368 15395.0754 LENGTH: 45.73 COURSE: N 0-32-13 W PI 503+03.44 26639.2061 15395.0151 LENGTH: 60.80 COURSE: N 11-36-58 E
MID-ORD: 45.10 EXTERNAL: 51.49	LENGTH: 24.85 COURSE: N 00-05-09 W DELTA: 30-34-01 AL_03A-12 CIRCULAR CURVE DATA PC 92+43.91 AL_03A-12 CIRCULAR CURVE DATA 25422.3595 15305.3790	TANGENT DATA 500+22.88 26359.2940 15398.4605 500+39.92 26376.3340 15398.4605 LENGTH: 17.04 COURSE: N 00-00-00 E	DELTA: 12-15-33 AL_02-6 CIRCULAR CURVE DATA PC 502+98.07 CC 26534.3981 15395.0754 CC 26634.3981 15445.0722
PI 90+19.32 25282.4403 15182.3881 LENGTH: 123.26 COURSE: N 88-58-36 E DELTA: 27-25-23 AL_03A-6 CIRCULAR CURVE DATA 25304.1368 15138.6797 CC 25483.2800 15227.6051 25283.3119 15231.1775 DELTA: 27-25-23 TYPE: LENGTH: 123.26 COURSE: N 88-58-36 E DELTA: 27-25-23 TYPE: LEFT	CC 25437.5772 15279.5251 PT 92+59.91 25437.6221 15309.5251 DELTA: 30-34-01 TYPE: LEFT RADIUS: 30.00 DEGREE: 190-59-09 LENGTH: 16.00 TANGENT: 8.20 MID-ORD: 1.06 EXTERNAL: 1.10	AL_02-2 CIRCULAR CURVE DATA PC 500+39.92 26376.3340 15378.4605 CC 26376.3340 15378.4605 PT 500+43.65 26380.0464 15398.1129 DELTA: 10-42 TYPE: LEFT RADIUS: 20.00 DEGREE: 286-29	PT 503+08.76 26644.4657 15396.0963 DELTA: 12-16 TYPE: RIGHT RADIUS: 50.00 DEGREE: 114-35 LENGTH: 10.70 TANGENT: 5.37 MID-ORD: 0.29 EXTERNAL: 0.29 CHORD: 10.68 COURSE: N 05-29-11 E
문화를 RADIUS: 200.00 DEGREE: 28-38-52 LENGTH: 95.72 TANGENT: 48.80 BER MID-ORD: 5.70 EXTERNAL: 5.87	CHORD: 15.82 COURSE: N 15-11-52 E ES: 1.10 PI 92+76.57 25454.2747 15309.5002	LENGTH: 3.73 TANGENT: 1.87 MID-ORD: 0.09 EXTERNAL: 0.09 CHORD: 3.73 COURSE: N 05-20-55 W ES: 0.09	ES: 0.29 PI 503+64.20 26698.7609 15407.2573 LENGTH: 12.62 COURSE: N 00-00-00 W DELTA: 11-36-58
15.5.07 15.15 15.15 16.15 16.15 17.1	TANGENT DATA 92+59.91 25437.6221 15309.5251 92+76.57 25454.2747 15309.5002 LENGTH: 16.65 COURSE: N 00-05-09 W	PI 500+54.31 26390.5207 15396.1343 LENGTH: 151.37 COURSE: N 00-53-28 E TANGENT DATA 500+43.65 26380.0464 15398.1129	TANGENT DATA 503+08.76 503+59.11 26644.4657 503+59.11 26693.7792 15406.2332 LENGTH: 50.34 COURSE: N 11–36–58 E
2927 1938 AL_03A-7 CIRCULAR CURVE DATA 5881 PC 90+66 24 25283 3119 15231 1775	HORIZONTAL ALIGNMENT STATION AND CURVE REPORT. ALIGNMENT: ALIGNMENT_04A DESC: MINK TRAIL (EAST) CONNECTION DESC. STATION SPIRAL/CURVE DATA NORTHING EASTING	500+52.28 26388.5265 15396.5110 LENGTH: 8.63 COURSE: N 10-41-50 W	AL_02-7 CIRCULAR CURVE DATA PC 503+59.11 2669.3.7792 15406.2332 CC 26703.8468 15357.2573 PT 503+69.25 26703.8468 15407.2573 DELTA: 11-37 TYPE: LEFT RADIUS: 50.00 DEGREE: 114-35 LENGTH: 10.14 TANGENT: 5.09
## 1	PI 100+00 25283.4733 15218.8138 LENGTH: 94.02 COURSE: S 87-28-50 E	ALIGNMENT DATA LEGEND TRAIL CENTER LINE	MID-ORD: 0.26 EXTERNAL: 0.26 CHORD: 10.12 COURSE: N 05-48-29 E ES: 0.26 CALL Before
THE SET OF	PI 100+94.02 25279.3404 15312.7452 LENGTH: 118.86 COURSE: S 11-43-59 W DELTA: 99-12-49	CC COMPOUND CURVE PC POINT OF CURVATURE PI POINT OF INTERSECTION PT POINT OF TANGENCY	PI 503+76.78 26711.3782 15407.2573 You Dig 503+69.25 503+76.78 26703.8468 15407.2573 15407.2573 LENGTH: 7.53 COURSE: N 00-00-00 E 1-800-892-0123
A COUNTY OF THE	STATE OF ILLINOIS	CARY PARK DISTRICT	ALIGNMENT DATA SECTION COUNTY TOTAL SHEET NO. STA. 80+50 TO 92+76.57
See the see that t	DEPARTMENT OF TRANSPORTATION	CARY COMMUNITY TRAIL st	TA. 100+00 TO 102+21.12 TA. 111+00 TO 111+82.06 TA. 401+30.22; STA. 500+00 TO 504+20.72 O624001-00-BR MCHENRY 64 8 FED. ROAD DIST. NO. 1 ILLINOIS CONTRACT NO. 63333

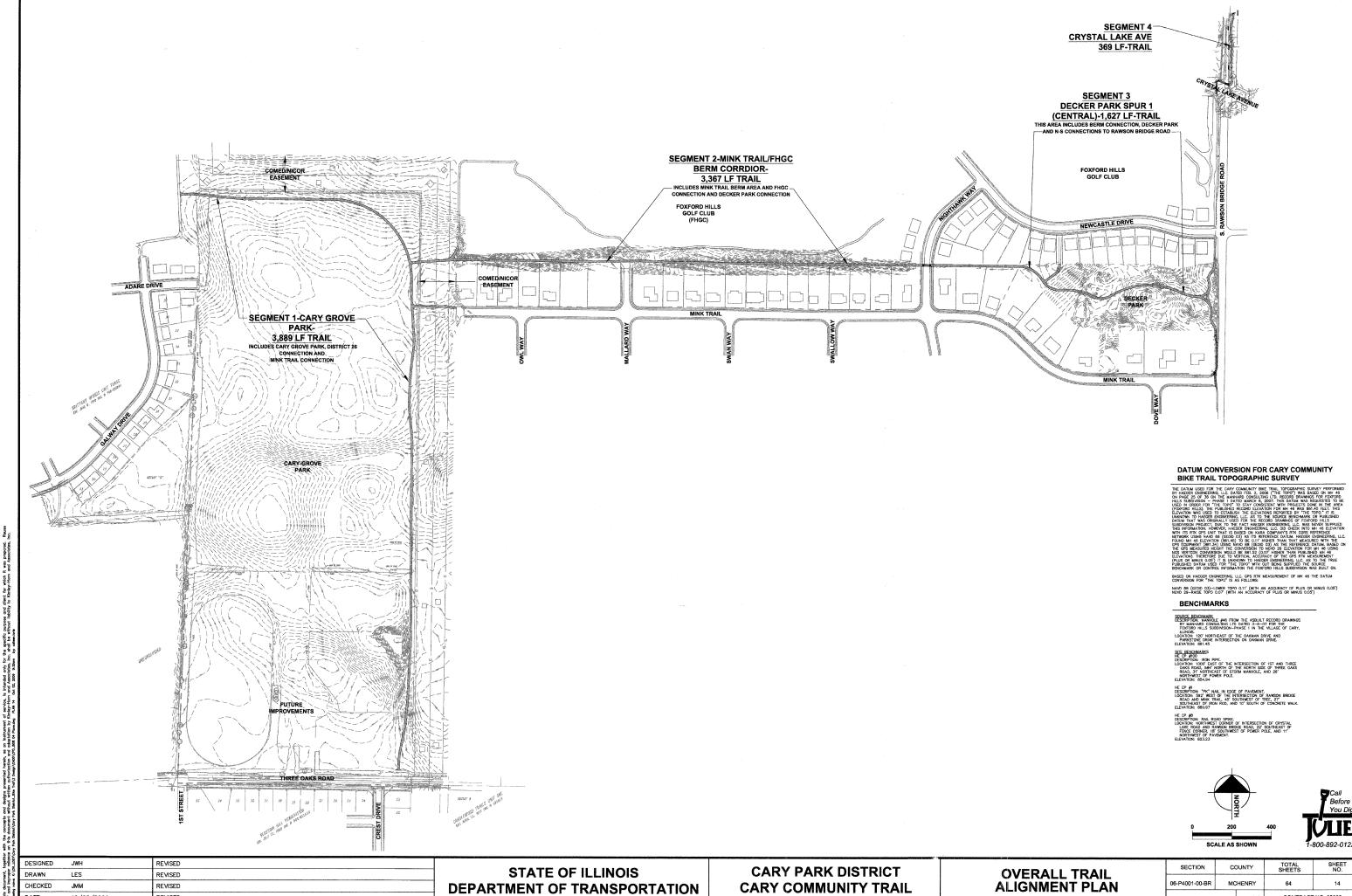




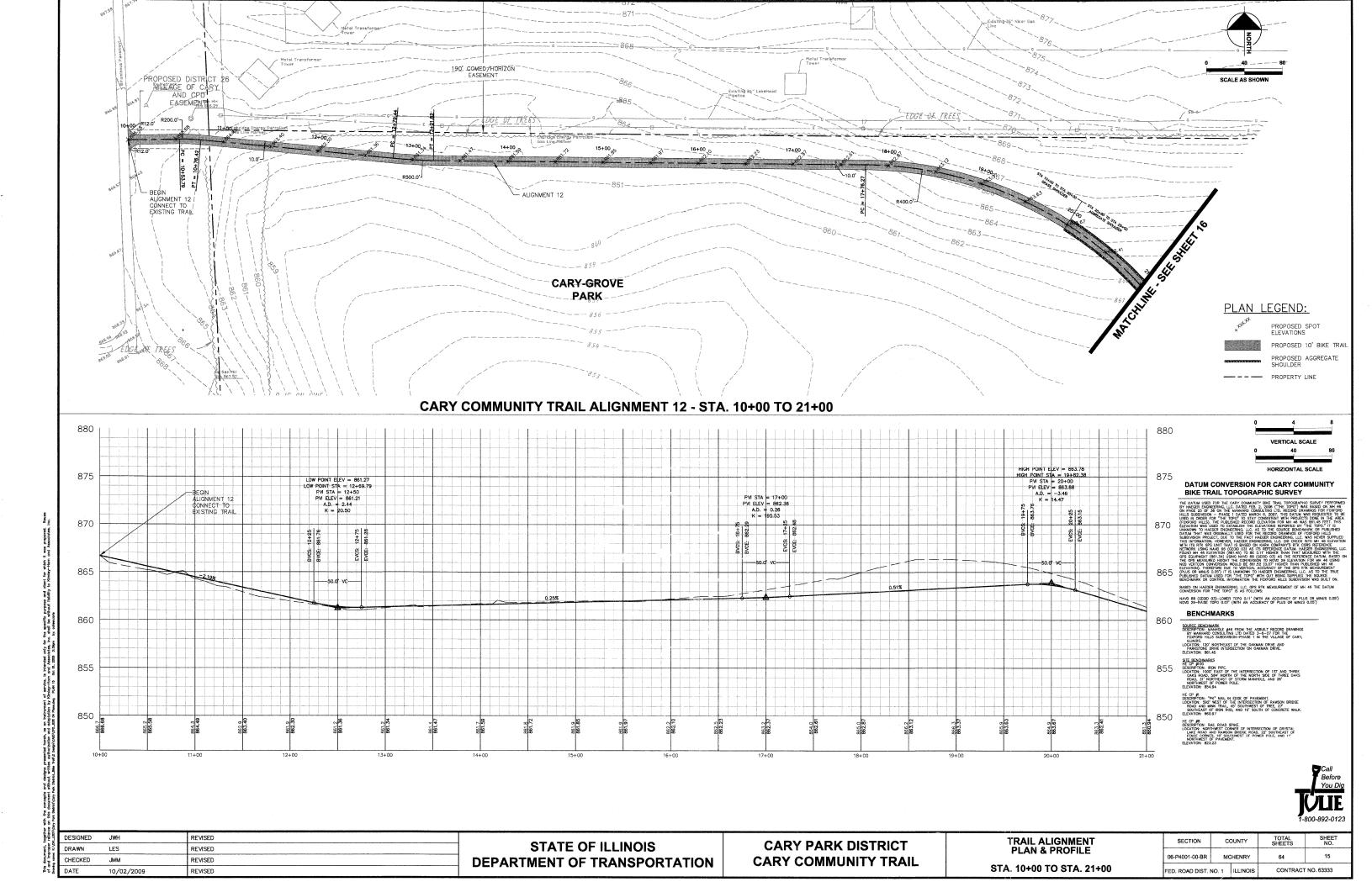


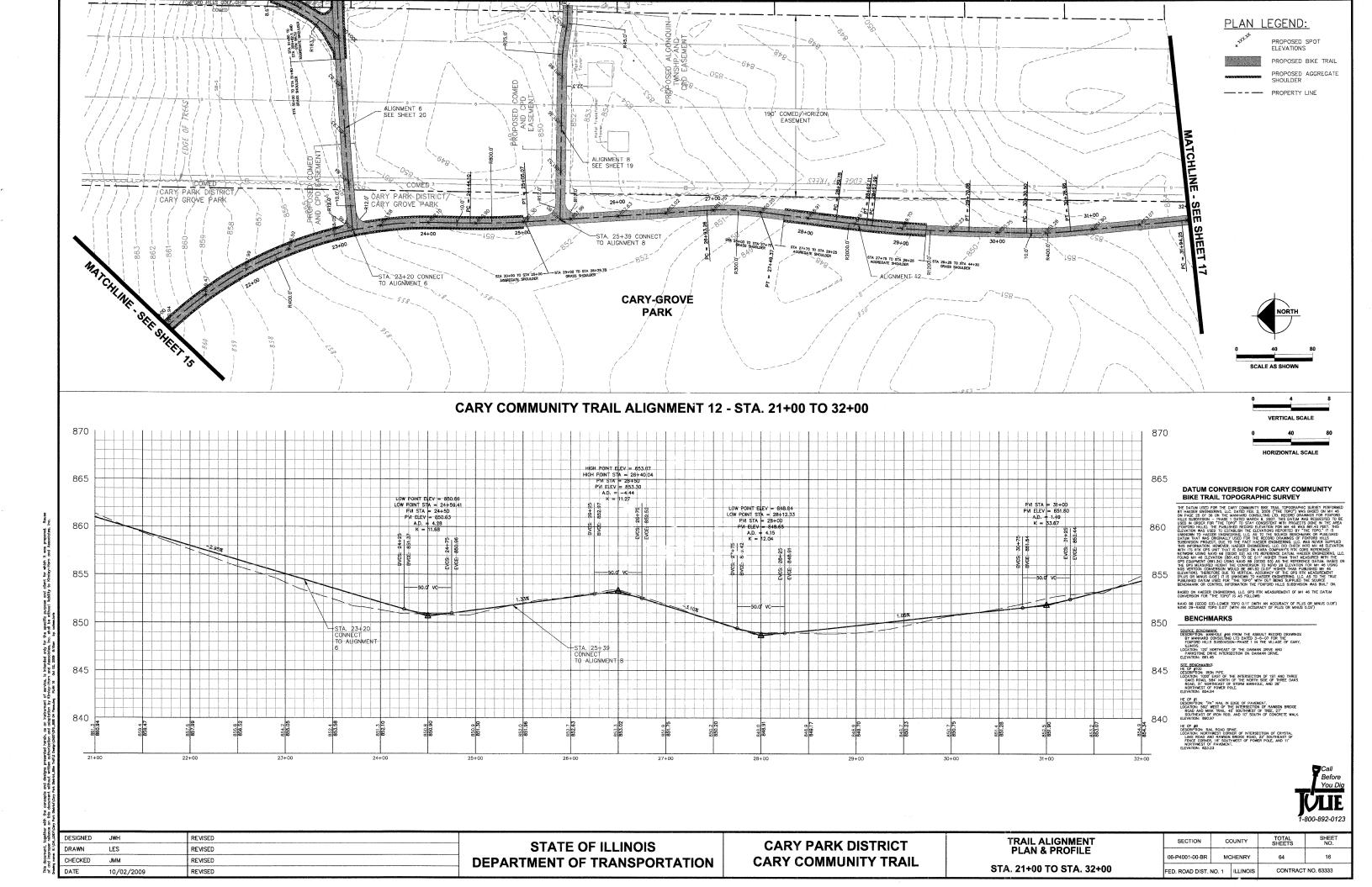


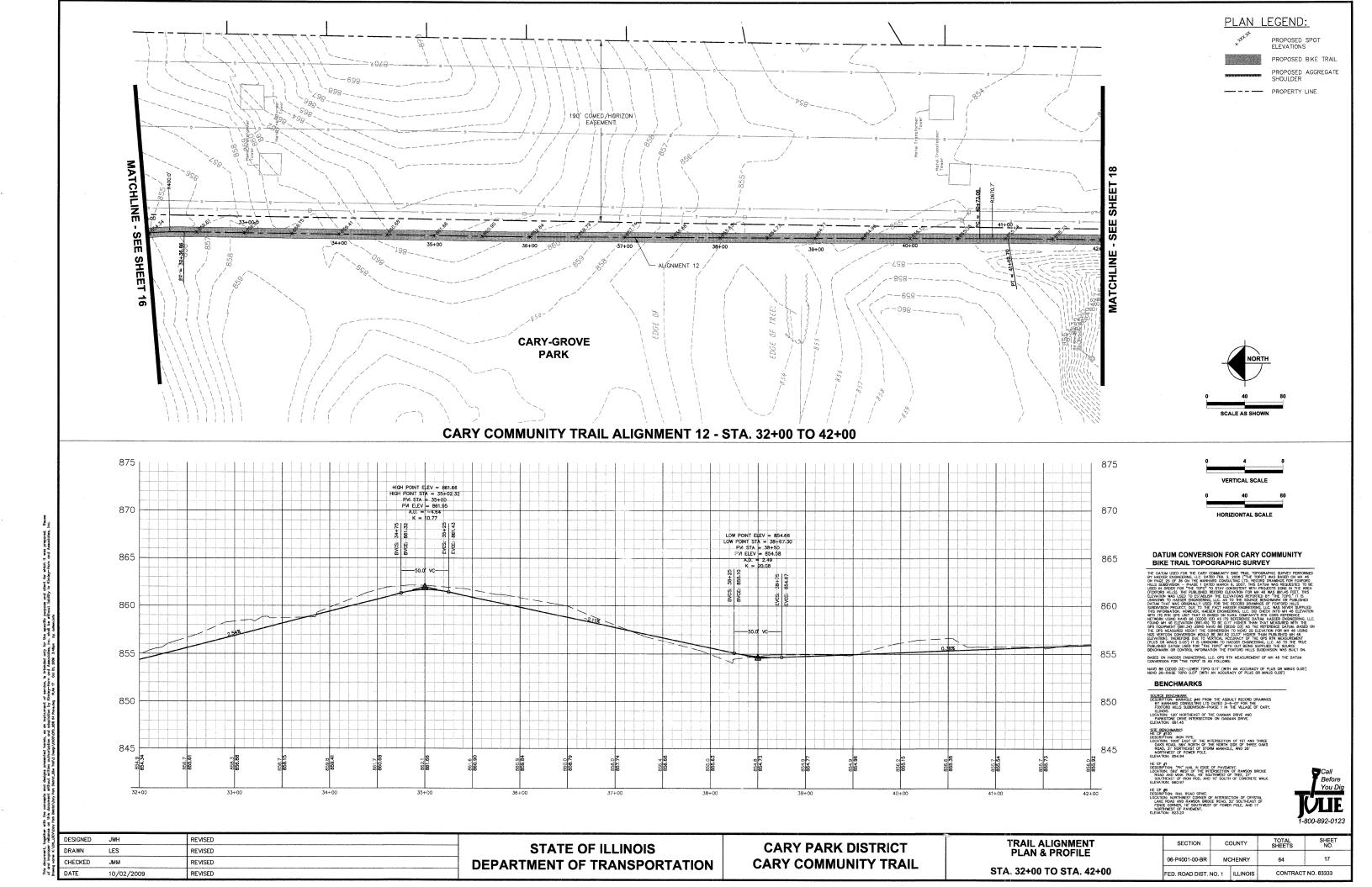


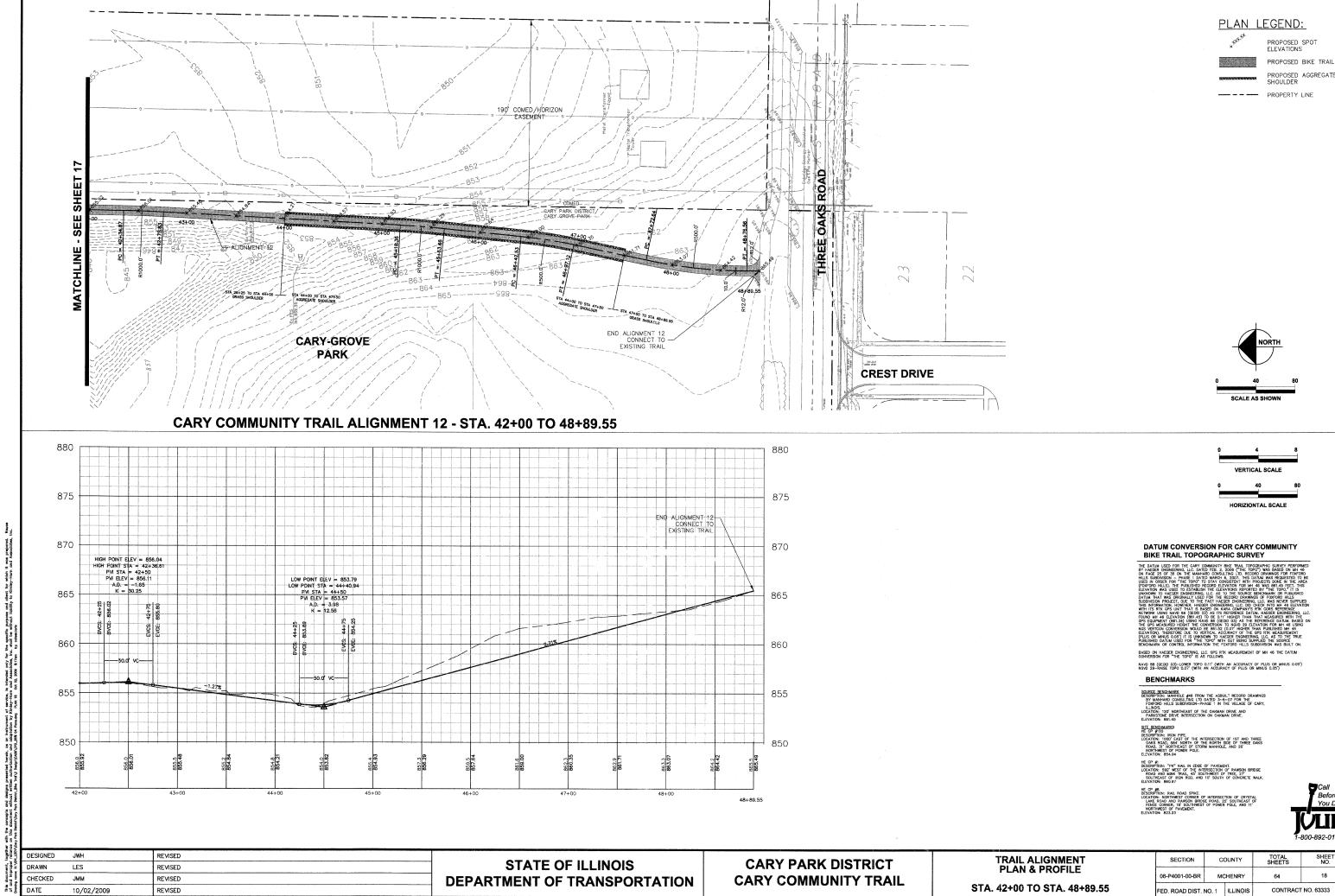


SECTION	COUNTY		TOTAL SHEETS	SHEET NO.
06-P4001-00-BR			64	14
FED. ROAD DIST. NO. 1		ILLINOIS	CONTRACT	NO. 63333



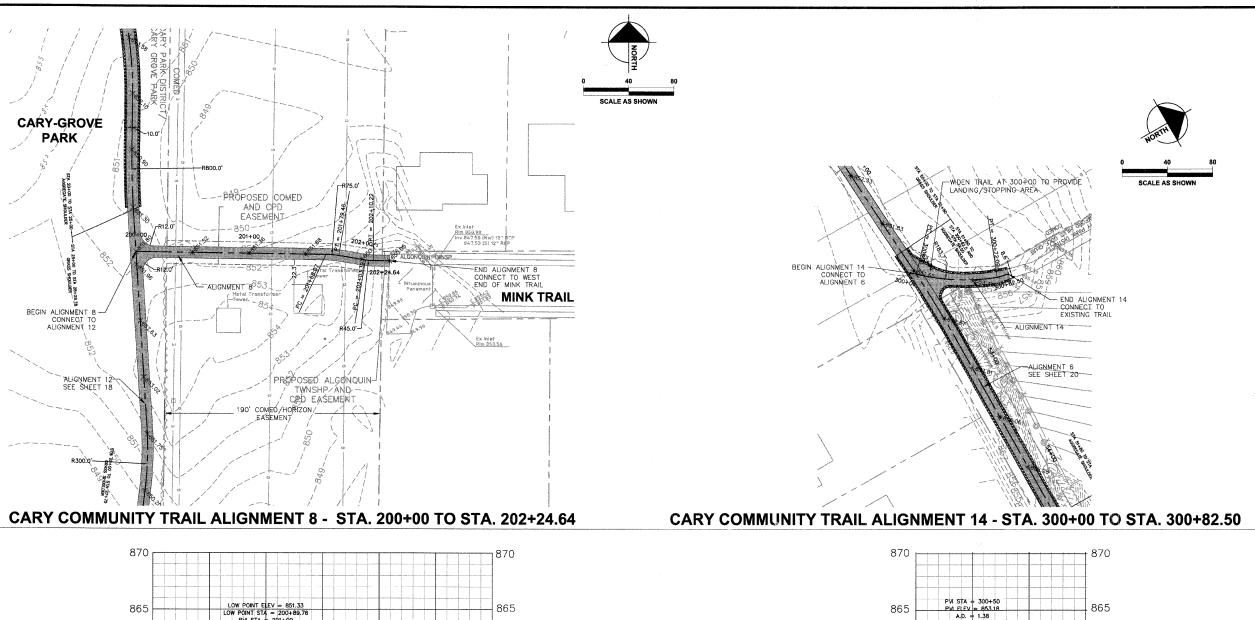


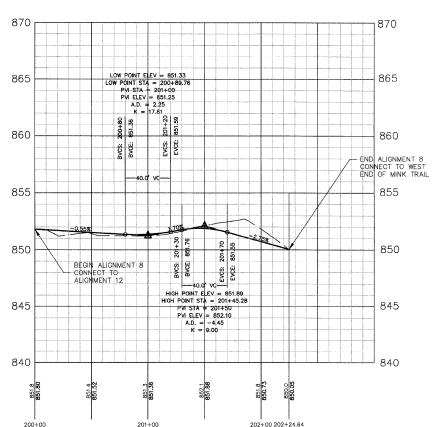


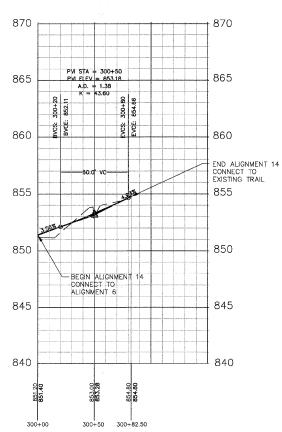


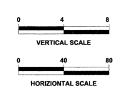
STA. 42+00 TO STA. 48+89.55

CONTRACT NO. 63333









PLAN LEGEND:

PROPERTY LINE

PROPOSED SPOT ELEVATIONS

PROPOSED AGGREGATE SHOULDER

DATUM CONVERSION FOR CARY COMMUNITY BIKE TRAIL TOPOGRAPHIC SURVEY

THE DATUM USED FOR THE CARY COMMUNITY BIEC TRAIL, TOPOGRAPHIC SURVEY PERFORME TO THE CARY COMMUNITY BIEC TRAIL, TOPOGRAPHIC SURVEY PERFORMED THAT THE CARY COMMUNITY BIEC TRAIL SUBJECT SURVEY BY THE CORY. THE TOPO'S THAT TOPO'S THE TOPO'S THAT TOPO'S THE CORY. THE CORY.

CONVERSION FOR "THE TOPO" IS AS FOLLOWS:

NAVD 86 (SCOID 03)-LOWER TOPO 0.11" (WITH AN ACCURACY OF PLUS OR MINUS 0.05")

NGVD 29-RAISE TOPO 0.07" (WITH AN ACCURACY OF PLUS OR MINUS 0.05")

BENCHMARK

SQUIRCE ENLICHMENS

GESCHPION: MANOLE AND FROM THE ASBUILT RECORD DRAWINGS

BY MANHARD CONSULTING LTD DATED 3-6-07 FOR THE
FOXFORD HILLS SUBDIVISION-PHASE 1 IN THE VILLAGE OF CARY,
ILLINOIS.

COCATION: 120" NORTHEAST OF THE OMMAN DRIVE AND
PARKSTONE DRIVE INTERSECTION ON OMMAN DRIVE.

ELEVATION: 801.72" NORTHEAST OF THE OMMAN DRIVE AND
PARKSTONE DRIVE INTERSECTION ON OMMAN DRIVE.

E BENCHMARKS CP #100

DESCRIPTION: IRON PPE.
LOCATION: 1000' EAST OF THE INTERSECTION OF 1ST AND THE
OAKS ROAD, 584' NORTH OF THE NORTH SIDE OF THREE O,
ROAD, 31' NORTHEAST OF STORM MANHOLE, AND 26'
NORTHWEST OF POWER POLE.
LEVATION: 854.94

HE CP #10: "PK" NAIL IN EDGE OF PAVEMENT.
LOCATION: 592 WEST OF THE INTERSECTION OF RAWSON BY
ROAD AND MINK TRAIL, 45 SOUTH-WEST OF TREE, 27'
SOUTH-BAST OF IRON ROD, AND 10' SOUTH OF CONCRETE
ELEVATION: 880.97

HE OF #8
DESCRIPTION: RAIL ROAD SPIKE.
LOCATION: NORTHWEST CORNER OF INTERSECTION OF CRYSTAL
LACE ROAD AND RAWSON BRODE ROAD, 22 SOUTHEAST OF
NORTHWEST OF PAYMENT.
ELEVATION: 82 PAYMENT.

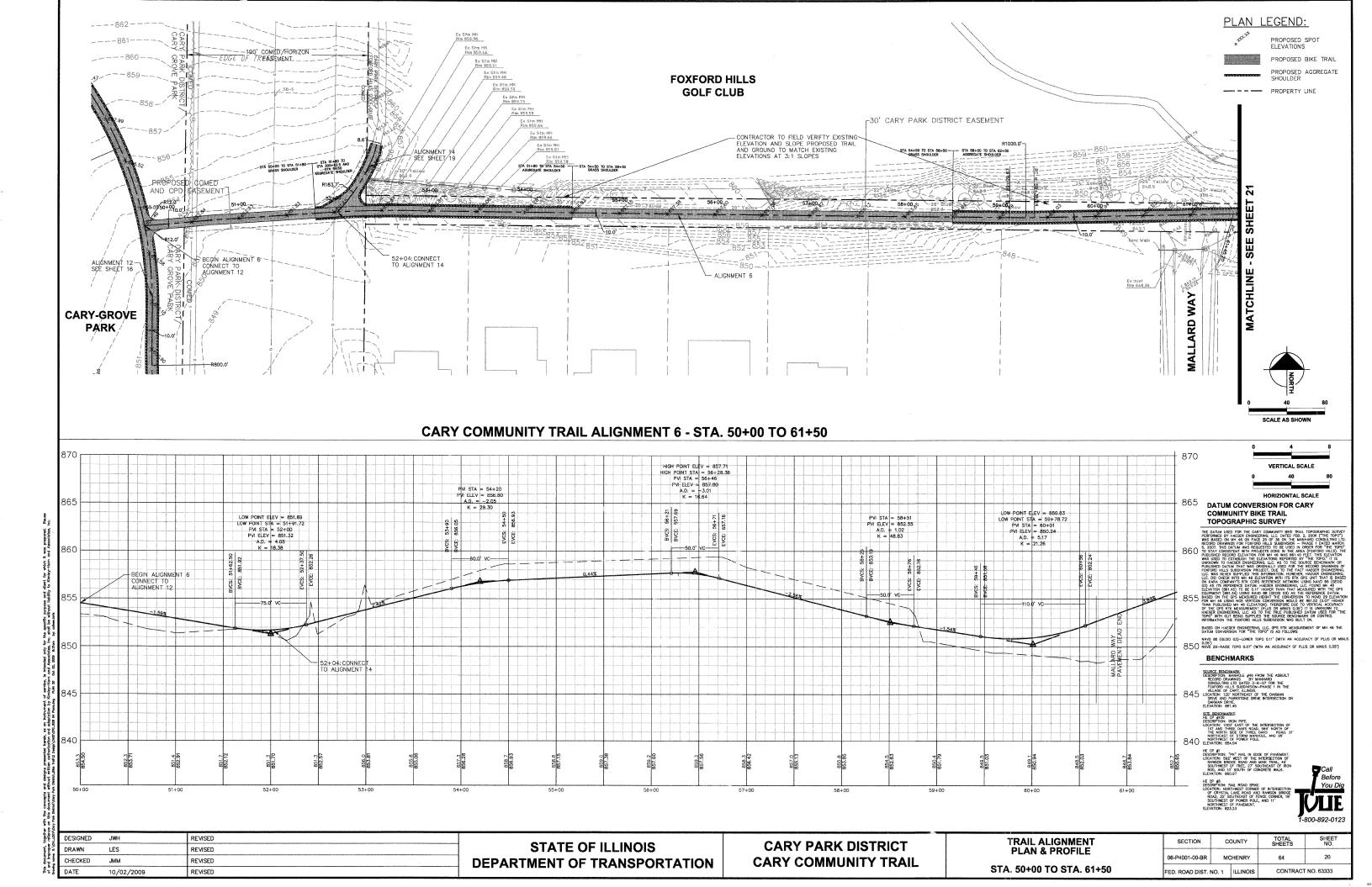


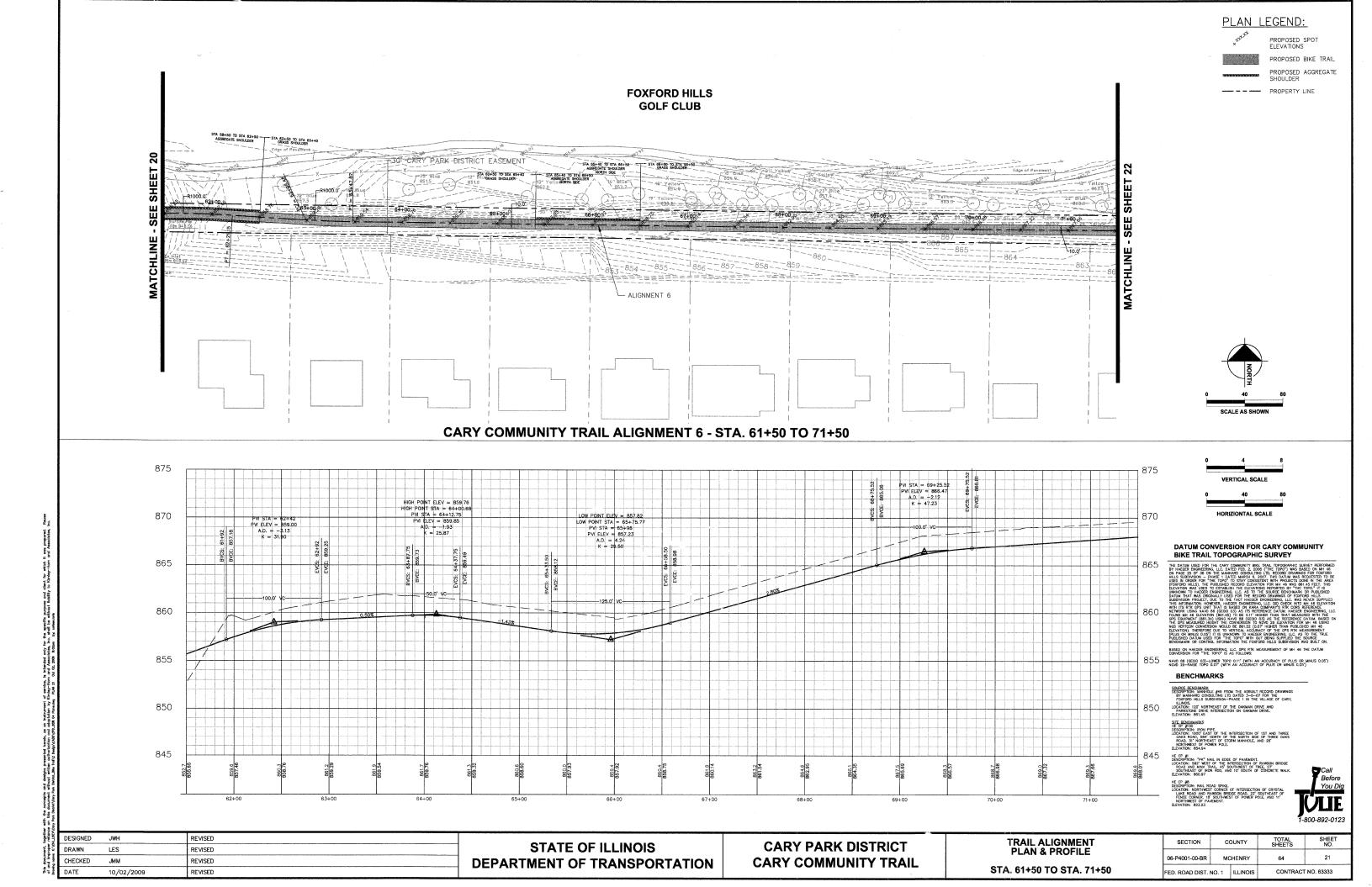
DESIGNED	JWH	REVISED	
DRAWN	LES	REVISED	STATE OF ILLINOIS
CHECKED	JMM	REVISED	DEPARTMENT OF TRANSPORTATION
DATE	10 /02 /2009	REVISED	

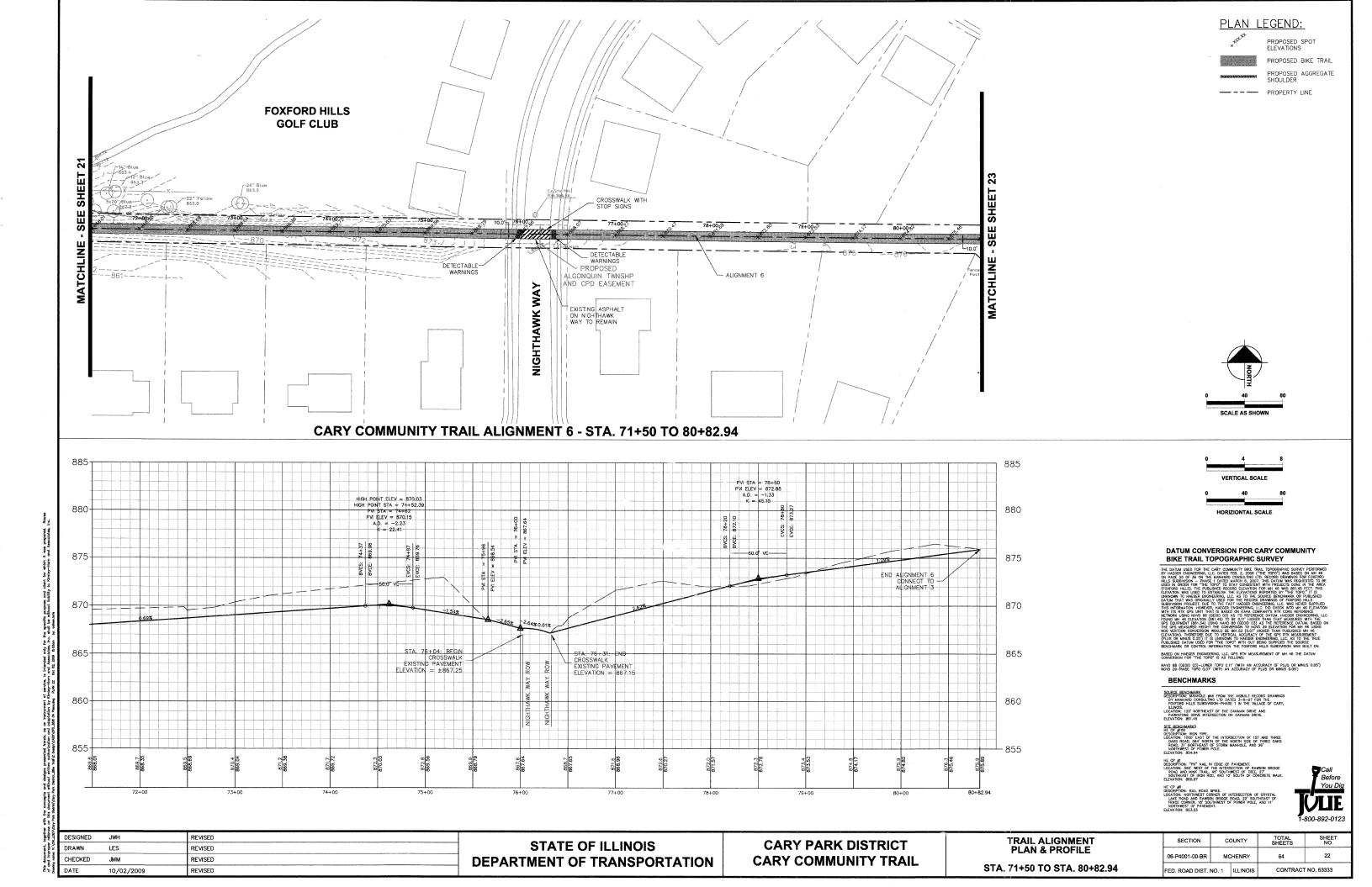
CARY PARK DISTRICT CARY COMMUNITY TRAIL

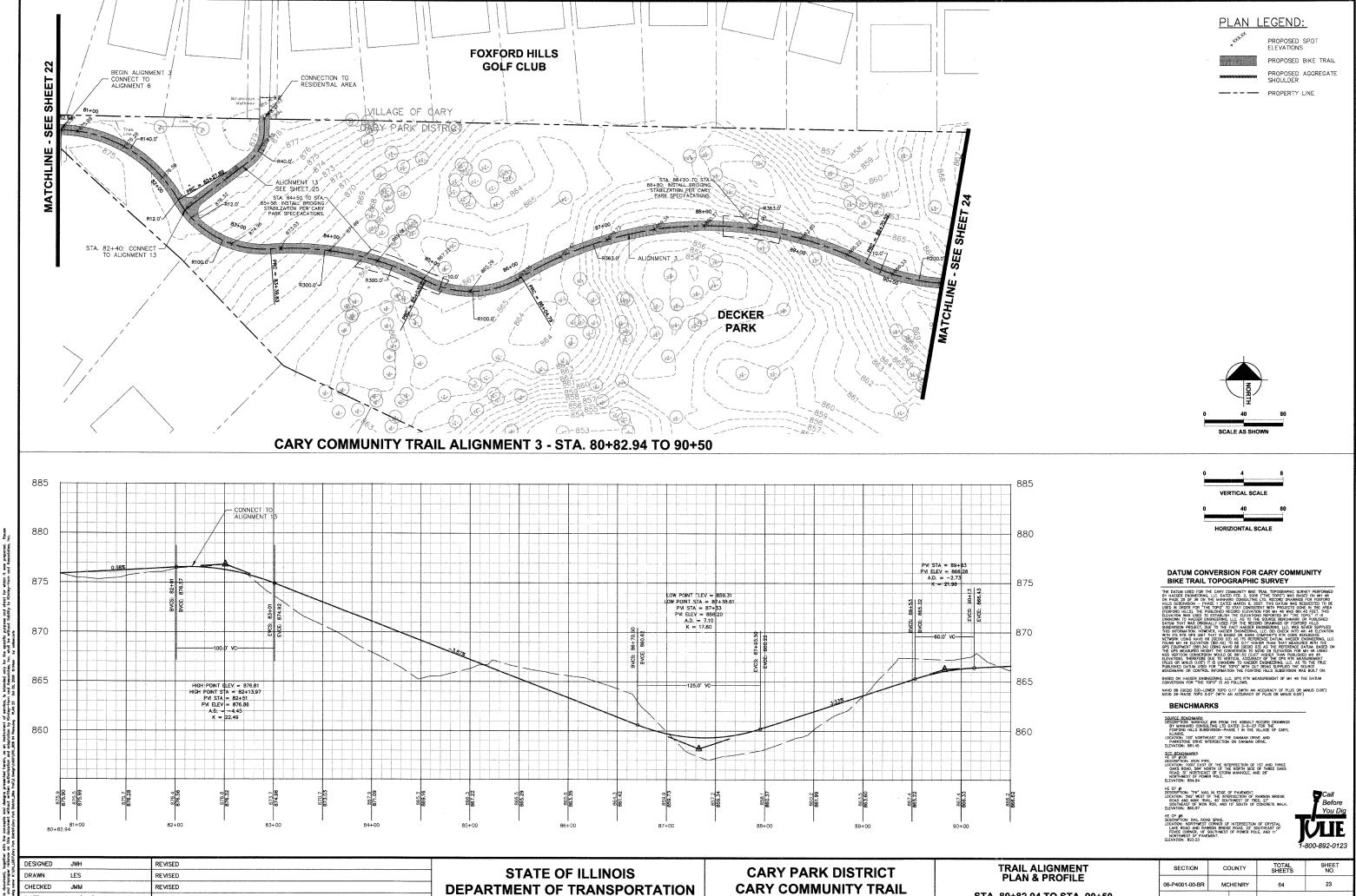
TRAIL ALIGNMENT PLAN & PROFILE STA. 200+00 TO STA. 202+24.64 STA. 300+00 TO STA. 300+82.50

SECTION	C	OUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY		64	19
ED, ROAD DIST, NO. 1		ILLINOIS	CONTRACT	NO. 63333





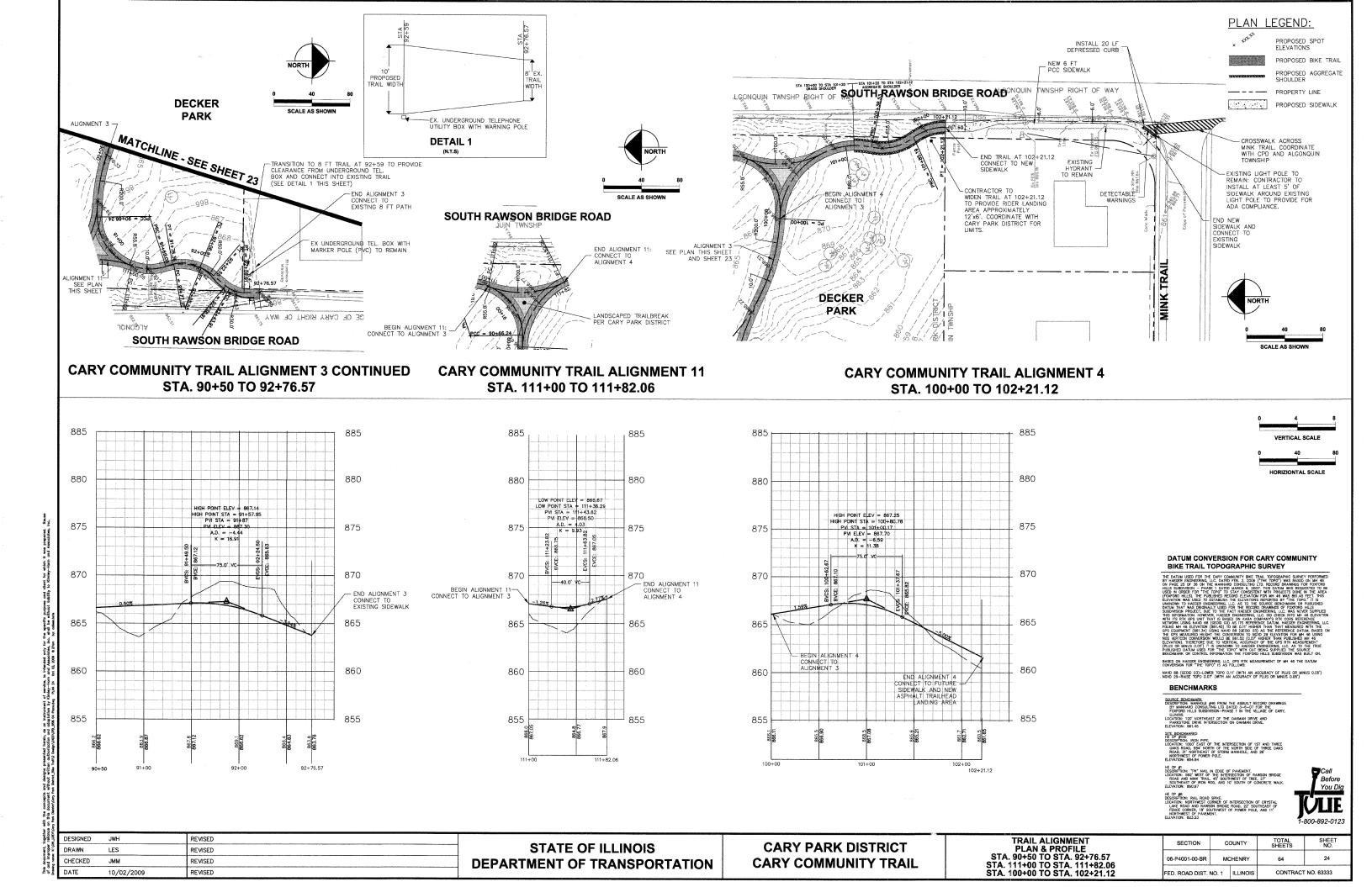


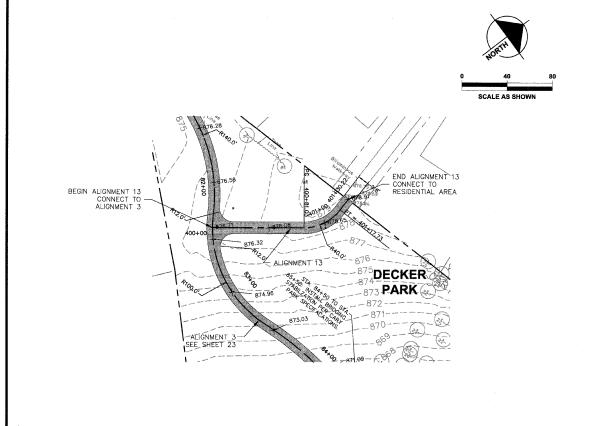


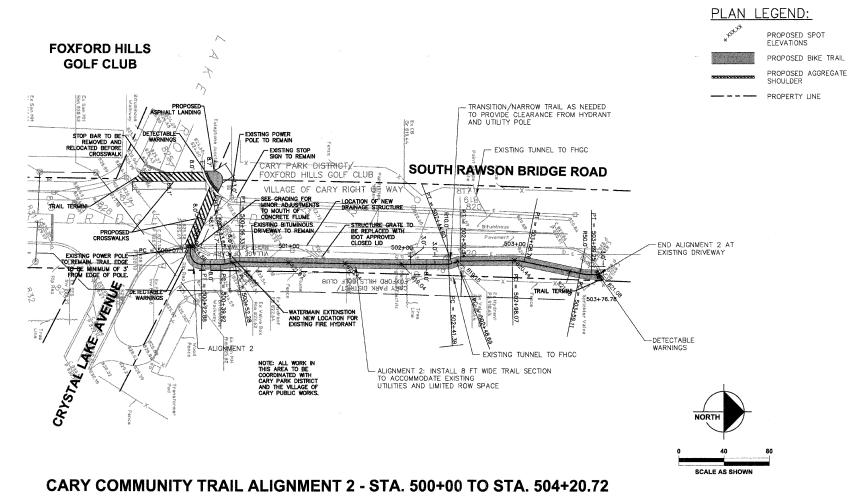
10/02/2009

REVISED

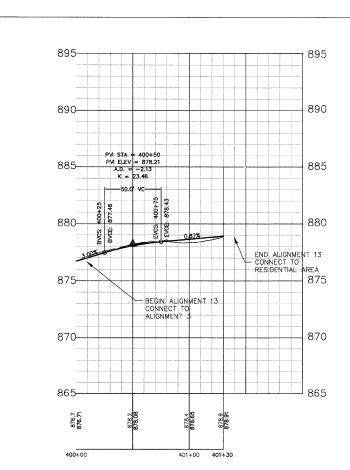
STA. 80+82.94 TO STA. 90+50 FED. ROAD DIST. NO. 1 ILLINOIS CONTRACT NO. 63333

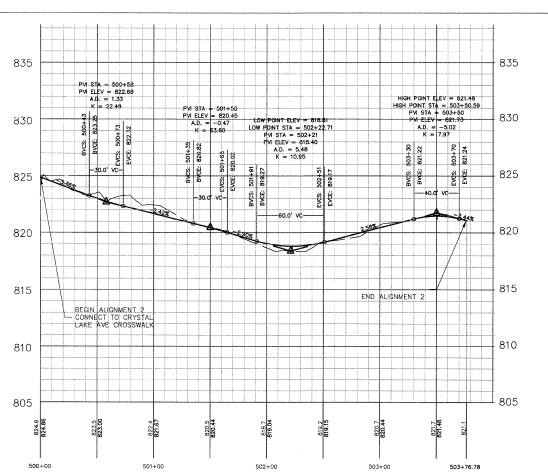


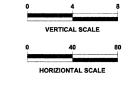




CARY COMMUNITY TRAIL ALIGNMENT 13 - STA. 400+00 TO STA. 401+30.22







DATUM CONVERSION FOR CARY COMMUNITY

THE DATAM USED FOR THE CARY COMMINITY DIKE TRAIL TROPOGRAPHIC SURVEY PERFORMED BY HACERE PAGINETRING, LLC DATED FEB. 2, 200 FTHE TOPO'N AS ASSED ON HIM 46 ON PAGE 20 OF 36 ON THE MANHARD CONSULTING LLT. RECORD DRAWNGS FOR FOOTON DRAWNGS FOOTON FOOTON DRAWNGS FOOTON DRAWNGS FOOTON DRAWNGS FOOTON FOOTON DRAWNGS FOOTON

BENCHMARKS

SQURCE BENCHMARK
DESCRIPTION: MANHOLE \$48 FROM THE ASSUILT RECORD DRAWNG:
BY MANHARD CONSULTING LTD DATED 3-8-07 FOR THE
FOXFORD HILLS SUBDIVISION-PHASE I IN THE VILLAGE OF CARY,
ILLINOIS,
LOCATION, 120' NORTHEAST OF THE OAKMAN DRIVE AND

E BENCHMARKS

DESCRIPTION. IRON PIPE.

LOCATION: 1000' EAST OF THE INTERSECTION OF 1ST AND THREE OAKS ROAD, 584' NORTH OF THE NORTH SIDE OF THREE OAKS ROAD, 31' NORTHEAST OF STORM MANHOLE, AND 26' NORTHWEST OF POWER POLE.

ELEVATION: 854.94

HE CP #1
DESCRIPTION: "PK" NAIL IN EDGE OF PAVEMENT.
LOCATION: 592' WEST OF THE INTERSECTION OF RAWSO!
ROAD AND MINK TRAIL, 45' SOUTHWEST OF TREE, 27'
SOUTH-FAST OF 1900 RDD AND 10' SOUTH OF COMPS.

HE OP #8
DESCRIPTION: RAIL ROAD SPIKE
DESCRIPTION: RAIL ROAD SPIKE
LOCATION: NORTHWEST CORNER OF INTERSECTION OF CR
LAKE ROAD AND RAWSON BRIDGE ROAD, 22' SOUTHEAS
FENCE CORNER, 18' SOUTHWEST OF POWER POLE, AND
NORTHWEST OF PAVENT.



5	DESIGNED	JWH	REVISED
, i		LES	REMSED
amou 6	CHECKED	JMM	REMSED
Dominio	DATE	10/02/2009	REVISED

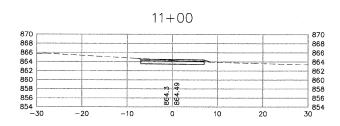
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

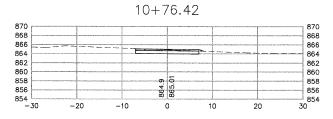
CARY PARK DISTRICT
CARY COMMUNITY TRAIL

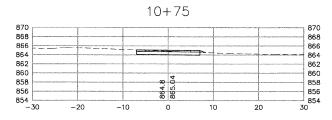
TRAIL ALIGNMENT PLAN & PROFILE STA. 400+00 TO STA. 401+30.22 STA. 500+00 TO STA. 504+20.72

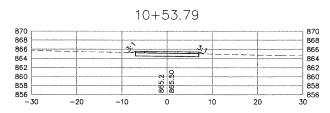
SECTION	C	OUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY		64	25
FED. ROAD DIST. NO. 1		ILLINOIS	CONTRACT	NO. 63333

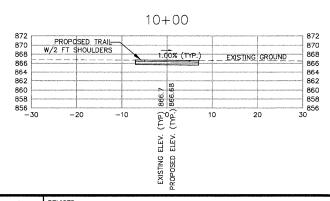
- TIE OUT SLOPES SHALL BE 1V: 3H MAX AND APPLIED AT EACH CROSS SECTION UNLESS LABELED OTHERWISE.
- 2. EXISTING CROUND SUPFACE ON MORTH SUPE OF BERM (ALIGNMENT 6) HAS BEEN MODIFIED FROM SUPEYEY AS DIRECTED BY CARP PARK DISTRICT AT THE FOLLOMING STATIONS. 53+00. 54+00. 55+00. 55+00. 55+00. 55+00. 55+00. 56+00. 65+00. 65+00. 66+00. 65+00. 66+00
- 3. PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SHOULDER SLOPES AND TRANSITIONS.
- 4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.

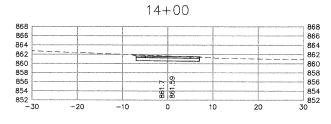


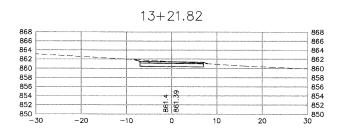


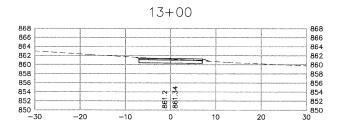


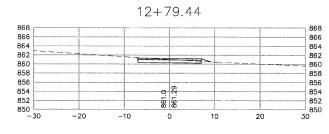


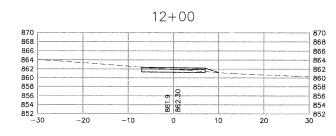












CROSS SECTION LEGEND:

EXISTING GROUND PROFILE

PROPOSED GROUND PROFILE

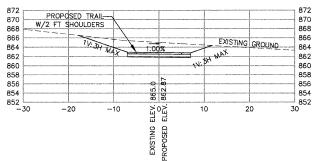
PROPOSED TRAIL W/SHOULDER

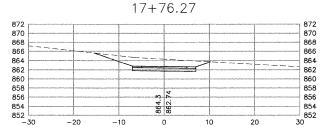
GROUND SLOPE/TIEOUT SLOPE

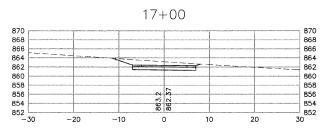
EXISTING/PROPOSED SPOT ELEVATION

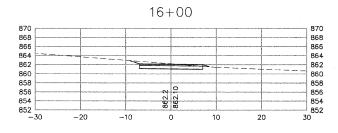


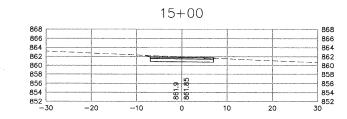
18+00











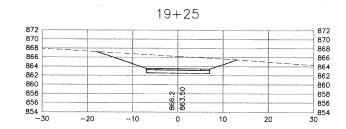


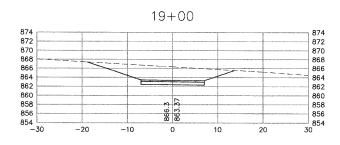


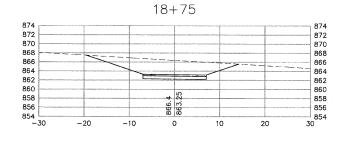
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	CHECKED	JMM	REVISED
	DRAWN	LES	REVISED
	DESIGNED	JWH	REVISED

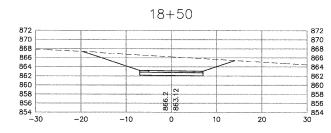
	OF ILLINOIS
DEPARTMENT O	F TRANSPORTATION

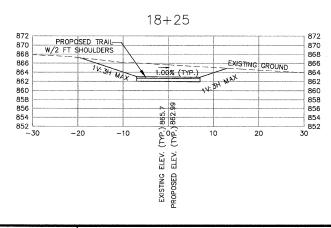
SECTION	COUNTY		TOTAL SHEETS	SHEET NO.
06-P4001-00-BR			64	26
FED. ROAD DIST. NO. 1		ILLINOIS	CONTRACT	NO. 63333

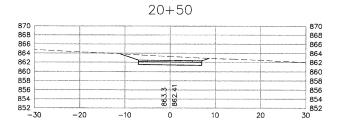


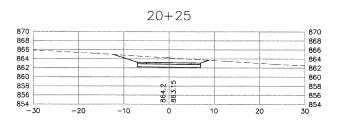


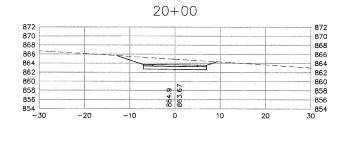


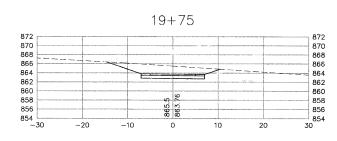


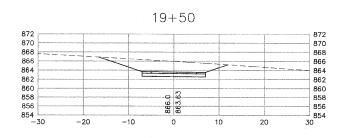




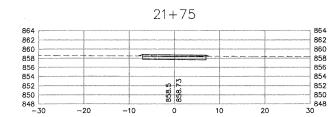


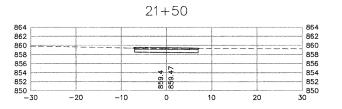


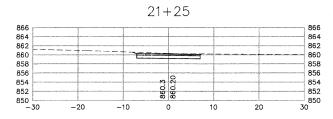


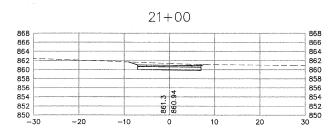


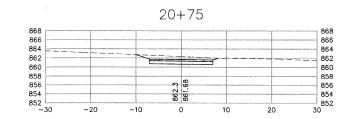
- TIE OUT SLOPES SHALL BE 1V: 3H MAX AND APPLIED AT EACH CROSS SECTION UNLESS LABELED OTHERWISE.
- 2. EXISTING GROUND SURFACE ON NORTH SIDE OF BERM (ALIGAMENT 6) HAS BEEN MODIFIE FROM SURVEY AS DIRECTED BY CARY PARK DISTRICT AT THE FOLLOWING STATIONS. 53+00, 54+00, 55+00, 55+00, 57+00, 59+00, 63+00, 65+00,
- PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SHOULDER SLOPES AND TRANSITIONS.
- 4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.













CROSS SECTION LEGEND:

<u>1%</u> 3:1

PROPOSED GROUND PROFILE PROPOSED TRAIL W/SHOULDER

GROUND SLOPE/TIEOUT SLOPE
EXISTING/PROPOSED SPOT ELEVATION



DESIGNED	JWH	REVISED
DRAWN	LES	REVISED
CHECKED	JMM	REVISED
DATE	10 /02 /2000	DEWCED

SECTION	COUNTY		TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY		64	27
FED. ROAD DIST. NO. 1		ILLINOIS	CONTRACT	NO. 63333

TIE OUT SLOPES SHALL BE 19:3H MAX AND APPLIED AT EACH CROSS SECTION UNLESS LABELED OTHERWISE.

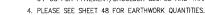
PROPOSED GROUND PROFILE GROUND SLOPE/TIEOUT SLOPE

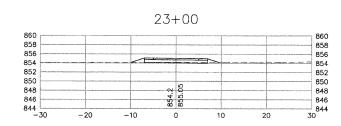
CROSS SECTION LEGEND:

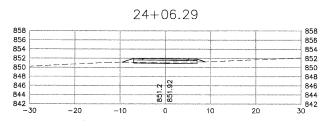
1%_ 3:1

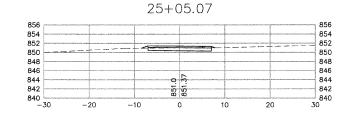
3. PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SHOULDER SLOPES AND TRANSITIONS.

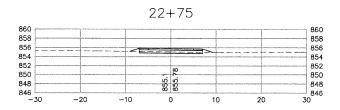


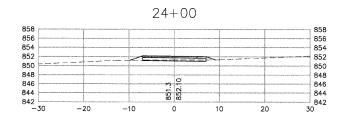


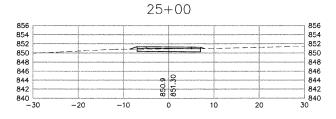


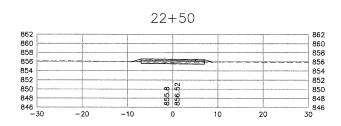


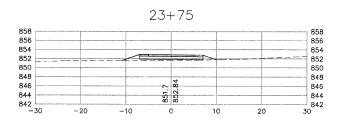


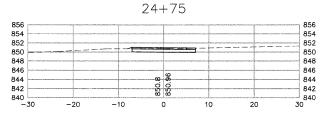


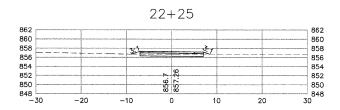


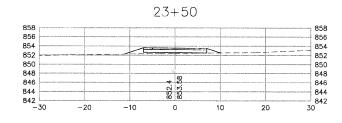


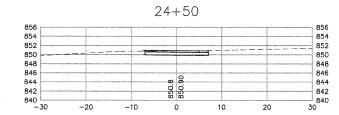


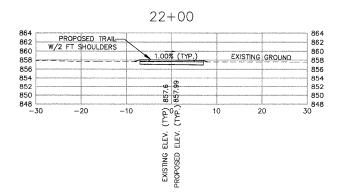


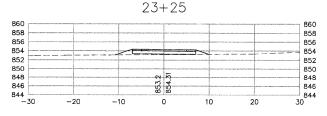


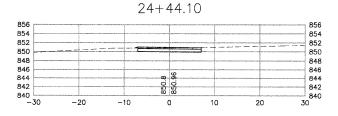


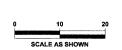














DESIGNED	JWH	REVISED	
DRAWN	LES	REVISED	
CHECKED	JMM	REVISED	
DATE	10/02/2009	REVISED	

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

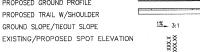
CARY PARK DISTRICT CARY COMMUNITY TRAIL CROSS SECTIONS STA. 22+00 TO STA 25+05.07

SECTION	COUNTY		TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY		64	28
FED. ROAD DIST.	NO. 1	ILLINOIS	CONTRACT NO. 63333	

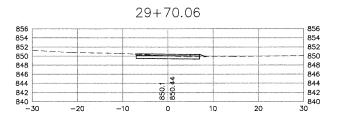
- TIE OUT SLOPES SHALL BE 1V:3H MAX AND APPLIED AT EACH CROSS SECTION UNLESS LABELED OTHERWISE.
- 3. PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SHOULDER SLOPES AND TRANSITIONS.
- 4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.

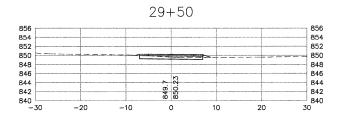


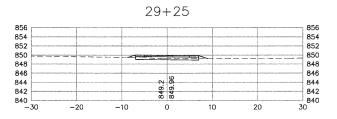
GROUND SLOPE/TIEOUT SLOPE

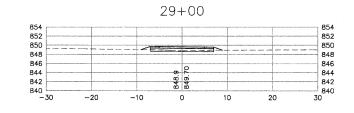


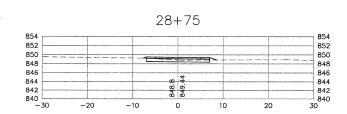




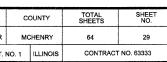


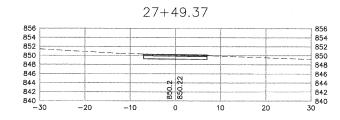


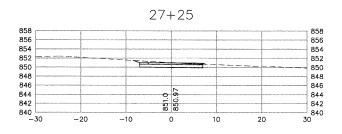


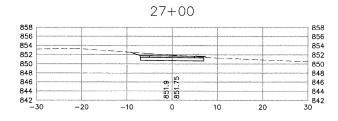


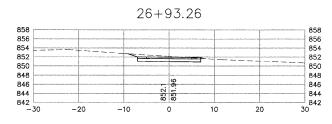


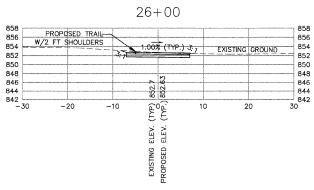












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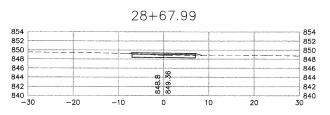
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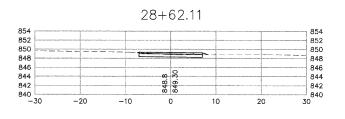
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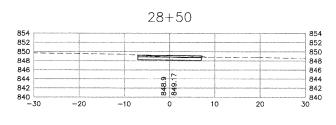
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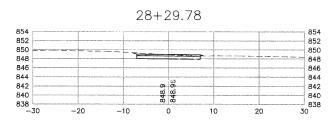
REVISED

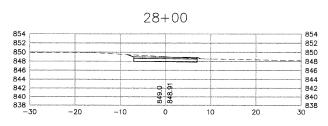
REVISED







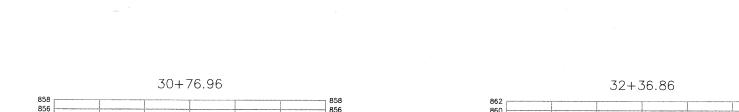


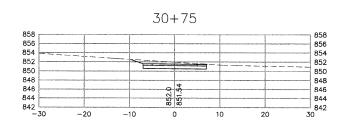


DEPARTMENT OF TRANSPORTATION



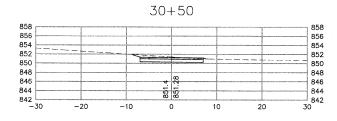
SECTION	MCHENRY NO. 1 ILLINOIS		SHEETS	NO.
06-P4001-00-BR			64	29
FED. ROAD DIST.			CONTRACT	NO. 63333

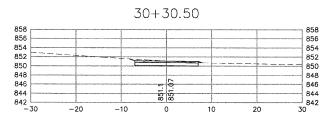


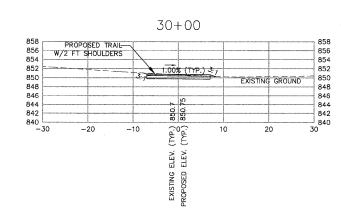


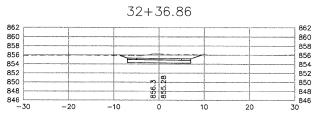
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852 850

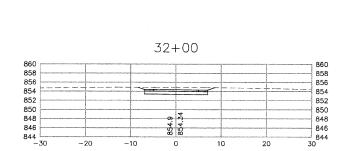


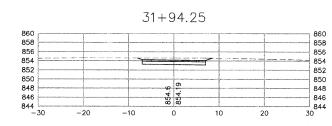


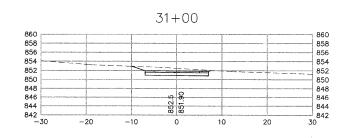




32 + 25







GENERAL NOTES: 1. THE OUT SLOPES SHALL BE 11'3H MAX AND APPLIED AT EACH CROSS SECTION UNLESS LABELED OTHERWISE. CROSS SECTION LEGEND:

EXISTING GROUND PROFILE
PROPOSED GROUND PROFILE
PROPOSED TRAIL W/SHOULDER
GROUND SLOPE/TIEOUT SLOPE

GROUND SLOPE/TIEOUT SLOPE
EXISTING/PROPOSED SPOT ELEVATION

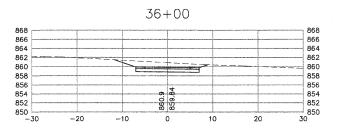
K QUAN	TITIES.			37-	+00		
864							 86
862							 86
860	***************************************						 86
858						-	 85
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852					<u>-</u>		 85
850				22	857		85
848 L -3	^	20	10				 84

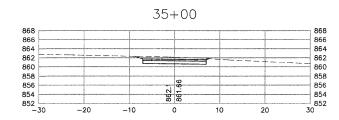
3. PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SHOULDER SLOPES AND TRANSITIONS.

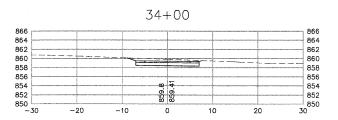
4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.

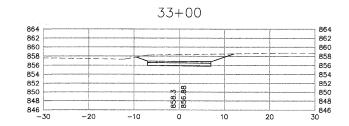
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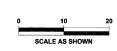
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DESIGNED	JWH	REVISED
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CHECKED	JMM	REVISED
DATE	10 /02 /2000	PEMSED

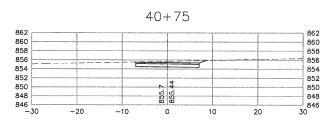
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

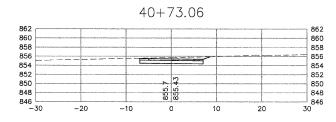
CARY PARK DISTRICT
CARY COMMUNITY TRAIL

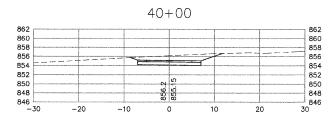
CROSS SECTIONS STA. 30+00 TO STA 37+00

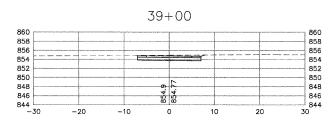
SECTION	COUNTY		TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY		64	30
FED. ROAD DIST. NO. 1		ILLINOIS	CONTRACT	NO. 63333

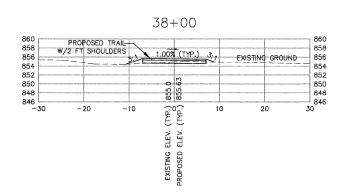
- 1. TIE OUT SLOPES SHALL BE 1V:3H MAX AND APPLIED AT EACH CROSS SECTION UNLESS LABELED OTHERWISE.
- 2. EXISTING GROUND SURFACE ON NORTH SIDE OF BERM (ALIGNMENT 6) HAS BEEN MODIFIED FROM SURVEY AS DIRECTED BY CARY PARK DISTRICT AT THE FOLLOWING STATIONS. 53400, 54400, 55400, 56400, 567400, 56400, 567400, 66400, 67400, 68400, 69400, 70400, AND 71400, CHANGES ARE PER FIELD VERIFICATIONS BY CARY PARK DISTRICT STAFF, CONTRACTOR TO VERIFY EXISTING GRADES, WITH CARY PARK DISTRICT BEFORE BEGINNING
- PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SHOULDER SLOPES AND TRANSITIONS.
- 4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.

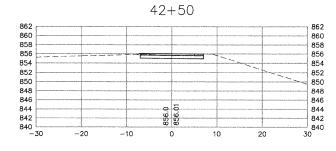


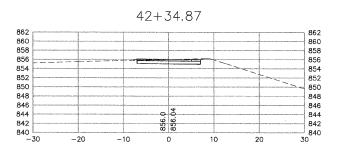


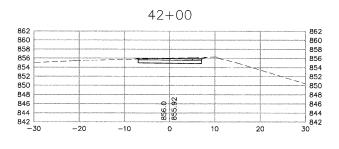


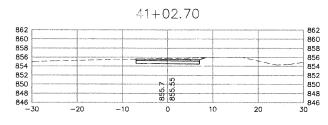


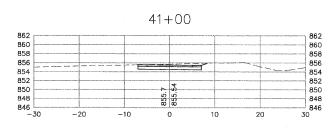


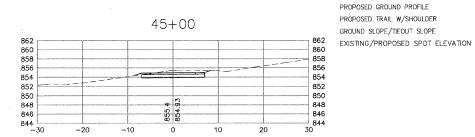


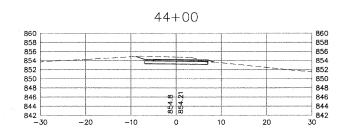


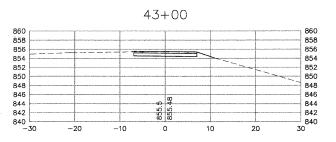


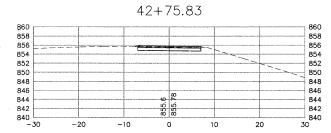


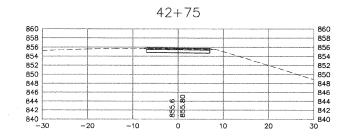
















DESIGNED	JWH	REVISED
DRAWN	LES	REVISED
CHECKED	JMM	REVISED
DATE	10/02/2009	REVISÉD

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CARY PARK DISTRICT CARY COMMUNITY TRAIL

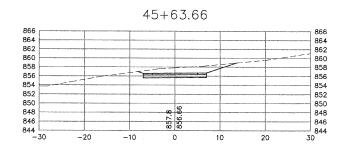
CROSS SECTIONS STA. 38+00 TO STA 45+00

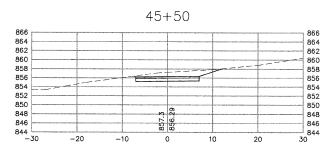
SECTION	C	DUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY		64	31
FED. ROAD DIST. NO. 1		ILLINOIS	CONTRACT	NO. 63333

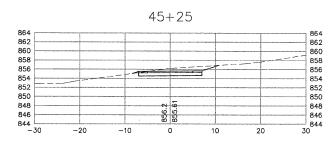
CROSS SECTION LEGEND:

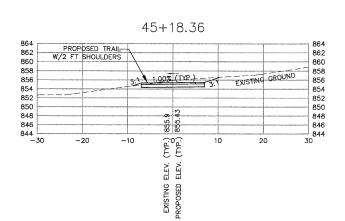
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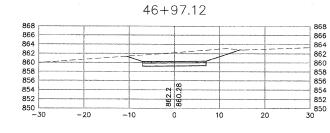
- TIE OUT SLOPES SHALL BE 17:3H MAX AND APPLIED AT EACH CROSS SECTION UNLESS LABELED OTHERWISE.
- 2. EXISTING GROUND SURFACE ON NORTH SIDE OF BERM (ALIGNMENT 6) HAS BEEN MODIFIED FROM SURVEY AS DIRECTED BY CARY PARK DISTRICT AT THE FOLLOWING STATIONS. 53+00, 54+00, 55+00, 55+00, 55+00, 63+00, 63+00, 64+00, 65+00, 65+00, 66+00, 67+00: 68+00, 69+00, 70+00, AND 71+00. CHANGES ARE PER FIELD VERIFICATIONS BY CARY PARK DISTRICT STAFF. CONTRACTOR TO VERIFY EXISTING GRADES WITH CARY PARK DISTRICT BEFORE BEGINNING WORK AT THEFS STATIONS.
- 3. PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SHOULDER SLOPES AND TRANSITIONS.
- 4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.

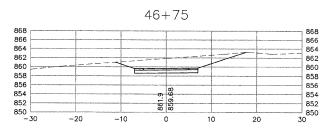


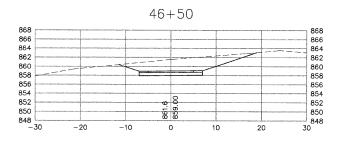


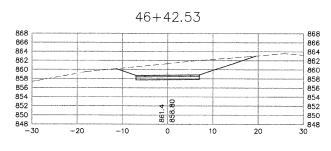


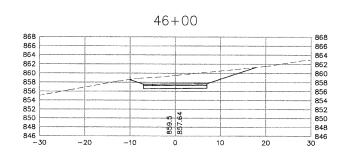








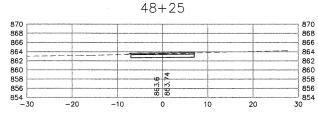


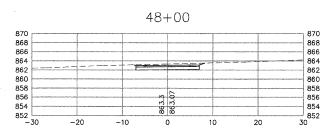


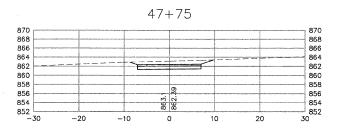
CROSS SECTION LEGEND:

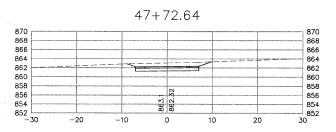
EXISTING GROUND PROFILE
PROPOSED GROUND PROFILE
PROPOSED TRAIL W/SHOULDER
GROUND SLOPE/TIEOUT SLOPE
EXISTING/PROPOSED SPOT ELEVATION

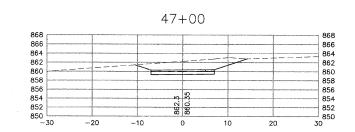
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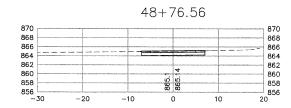


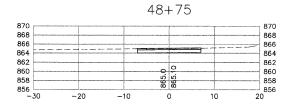


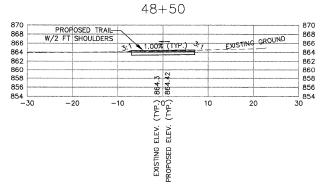




П	DESIGNED	JWH	REVISED
	DRAWN	LES	REMSED
	CHECKED	JMM	REVISED
	DATE	10/02/2009	REVISED







TIE OUT SLOPES SHALL BE 1V: 3H MAX AND APPLIED AT EACH CROSS SECTION UNLESS LABELED OTHERWISE.

3. PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SHOULDER SLOPES AND TRANSITIONS.

4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.

CROSS SECTION LEGEND:

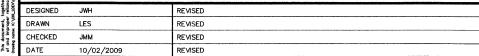
EXISTING/PROPOSED SPOT ELEVATION

EXISTING GROUND PROFILE PROPOSED GROUND PROFILE PROPOSED TRAIL W/SHOULDER GROUND SLOPE/TIEOUT SLOPE

1% 3:1

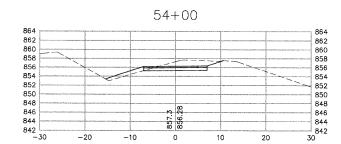
1	0	20			
SCALE AS SHOWN					

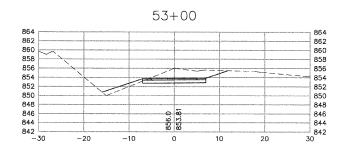
SECTION	C	YTNUC	TOTAL SHEETS	SHEET NO.
P4001-00-BR	МС	CHENRY	64	33
. ROAD DIST. NO. 1 ILLINOIS		ILLINOIS	CONTRACT	NO. 63333

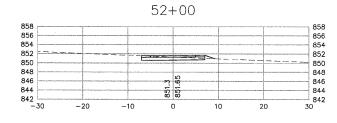


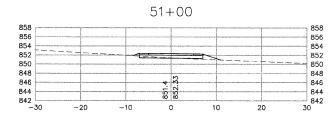
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

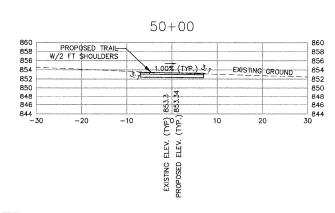
CARY PARK DISTRICT CARY COMMUNITY TRAIL CROSS SECTIONS STA. 48+50 TO STA 48+76.56



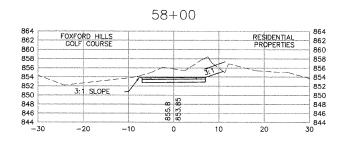


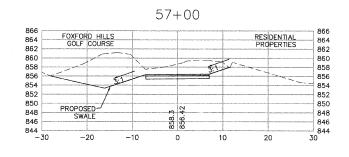


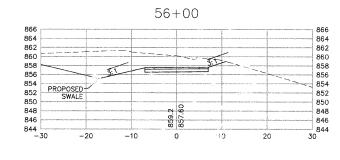


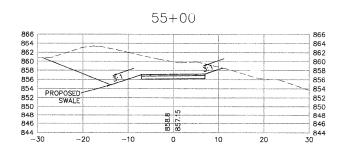


- TIE OUT SLOPES SHALL BE 1V: 3H MAX AND APPLIED AT EACH CROSS SECTION UNLESS LABELED OTHERWISE.
- 3. PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SHOULDER SLOPES AND TRANSITIONS.
- 4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.
- 5. EXISTING TOPO BETWEEN 54+00 AND 59+00 IS APPROXIMATE. CONTRACTOR TO CONFIRM EXISTING GRADES IN THIS AREA AND COORDINATE WITH CARY PARK DISTRICT PRIOR TO STAPTING WORK





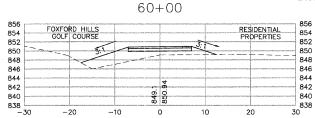


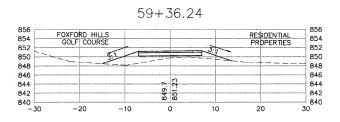


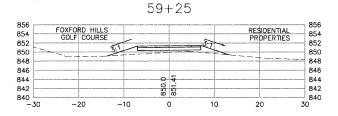
CROSS SECTION LEGEND:

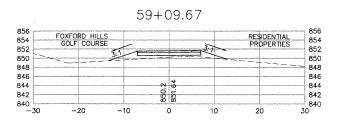
GROUND SLOPE/TIEOUT SLOPE 1%_ 3:1 EXISTING/PROPOSED SPOT ELEVATION

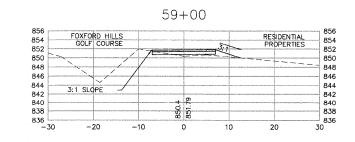


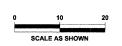








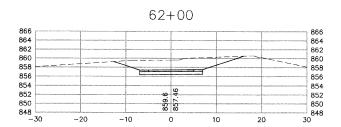


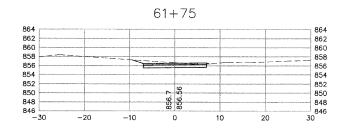


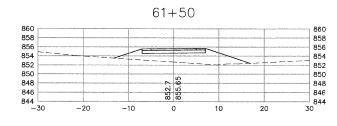


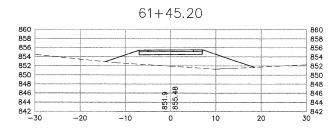
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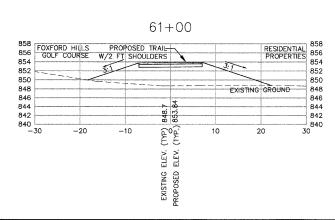
SECTION	C	YTNUC	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY		64	34
ED. ROAD DIST. NO. 1		ILLINOIS	CONTRACT	NO. 63333



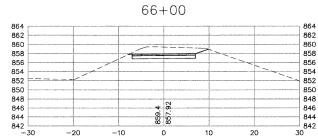


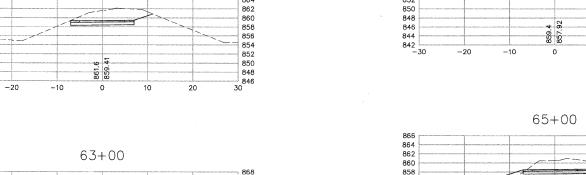


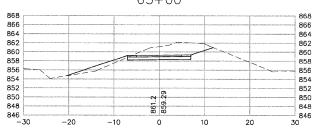




- TIE OUT SLOPES SHALL BE 1V: 3H MAX AND APPLIED AT EACH CROSS SECTION UNLESS LABELED OTHERWISE.
- 2. EXISTING GROUND SURFACE ON NORTH SIDE OF BERN (ALIGNMENT 6) HAS BEEN MODIFIED FROM SURVEY AS DIRECTED BY CARY PARK DISTRICT AT THE FOLLOWING STATIONS. 53+00, 54+00, 55+00, 56+00, 57+00, 59+00, 69+00,
- PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SHOULDER SLOPES AND TRANSITIONS.
- 4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.

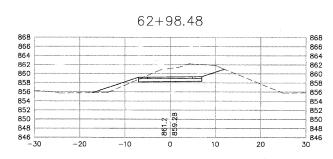


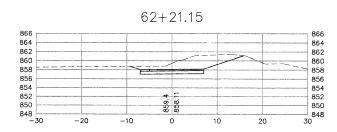


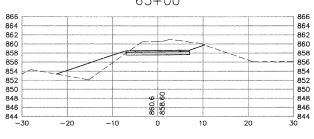


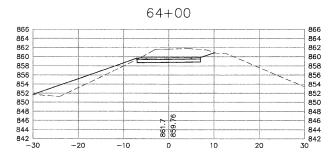
63 + 25

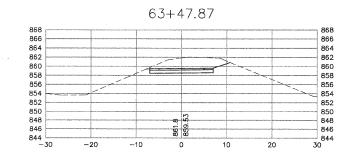
850

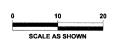












CROSS SECTION LEGEND:

1%__ 3:1

XXXX

EXISTING GROUND PROFILE PROPOSED GROUND PROFILE PROPOSED TRAIL W/SHOULDER

GROUND SLOPE/TIEOUT SLOPE

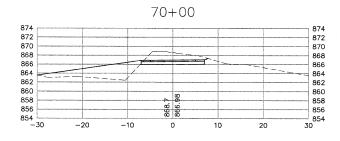
EXISTING/PROPOSED SPOT ELEVATION

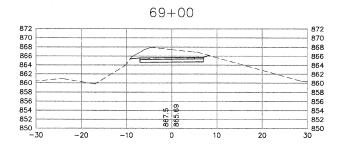


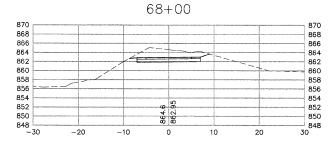
j	DESIGNED	JWH	REVISED
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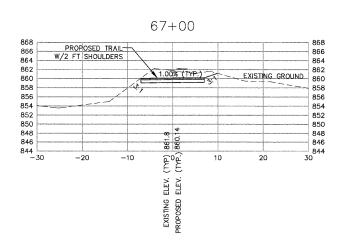
SECTION	COUNTY		TOTAL SHEETS	SHEET NO.
06-P4001-00-BR MC		CHENRY	64	35
FED. ROAD DIST.	NO. 1	ILLINOIS	CONTRACT	NO. 63333

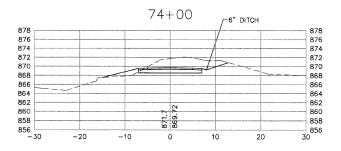
- 1. TIE OUT SLOPES SHALL BE 1V:3H MAX AND APPLIED AT EACH CROSS SECTION UNLESS LABELED OTHERWISE.
- 3. PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SHOULDER SLOPES AND TRANSITIONS.
- 4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.

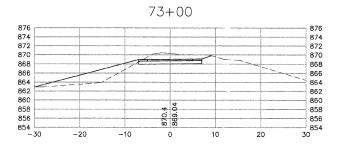


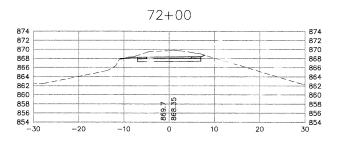


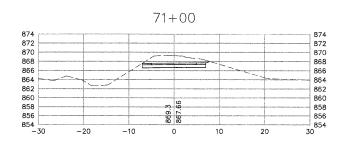




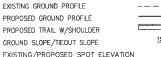


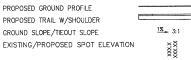


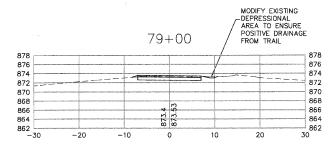


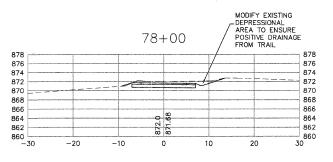


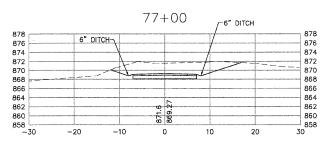
CROSS SECTION LEGEND:

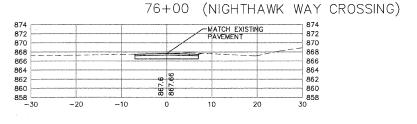


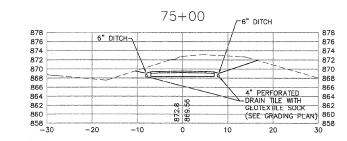


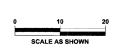








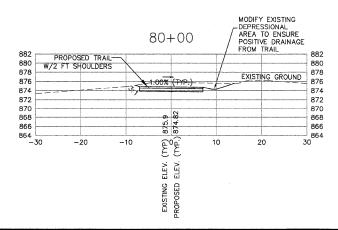






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DATE	10 /02 /2000	PEMSED

SECTION	COUNTY		TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY		64	36
ED. ROAD DIST.	NO. 1	ILLINOIS	CONTRACT NO. 63333	



STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CARY PARK DISTRICT CARY COMMUNITY TRAIL

CROSS SECTIONS STA 80+00 TO STA 80+82.94

SECTION	COUNTY		TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY		64	37
FED. ROAD DIST. NO. 1		ILLINOIS	CONTRACT	NO. 63333

GENERAL NOTES:

1. TIE OUT SLOPES SHALL BE 1V:3H MAX AND APPLIED AT EACH CROSS SECTION UNLESS LABELED OTHERWISE.

 EXISTING GROUND SURFACE ON NORTH SIDE OF BERM (ALIGNMENT 6) HAS BEEN MODIFIE FROM SURVEY AS DIRECTED BY CARY PARK DISTRICT AT THE FOLLOWING STATIONS. 53+00, 54+00, 55+00, 55+00, 57+00, 59+00, 63+00, 64+00, 65+00, 66+00, 67+00. 68+00, 69+00, 70+00, AND 71+00. CHANGES ARE PER FIELD VERIFICATIONS BY CARY PARK DISTRICT STAFF. CONTRACTOR TO VERIFY EXISTING GRADES WITH CARY PARK DISTRICT BEFORE BEGINNING WORK AT THESE STATIONS.

3. PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SHOULDER SLOPES AND TRANSITIONS.

4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.

CROSS SECTION LEGEND:

EXISTING GROUND PROFILE

PROPOSED GROUND PROFILE

PROPOSED TRAIL W/SHOULDER

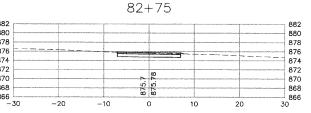
GROUND SLOPE/TIEOUT SLOPE

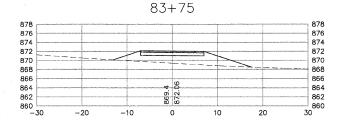
EXISTING/PROPOSED SPOT ELEVATION

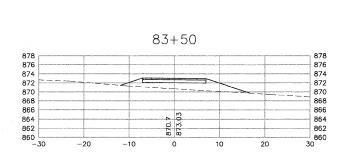
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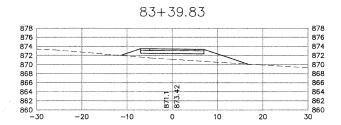
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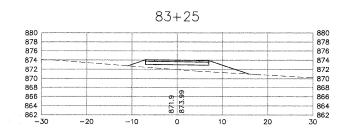
- TIE OUT SLOPES SHALL BE 1V: 3H MAX AND APPLIED AT EACH CROSS SECTION UNLESS LABELED OTHERWISE.
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- 4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.

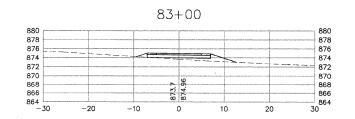


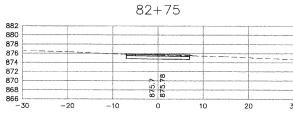


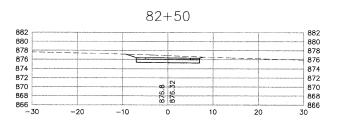


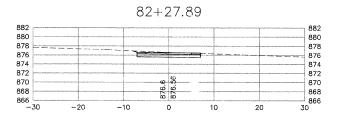


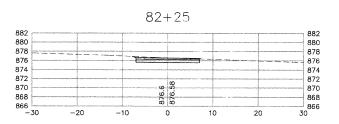


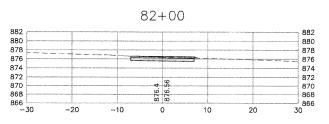








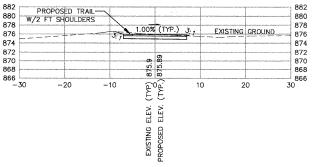




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10/02/2009





STATE OF ILLINOIS

CARY PARK DISTRICT CARY COMMUNITY TRAIL

CROSS SECTIONS

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SECTION	C	YTNUC	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	мс	HENRY	64	38
ED. ROAD DIST. NO. 1		ILLINOIS	CONTRACT	NO. 63333

SCALE AS SHOW

Call Before You Dig

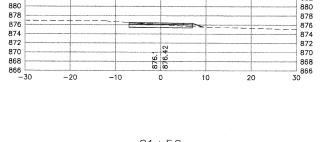
CROSS SECTION LEGEND:

1%_ 3:1

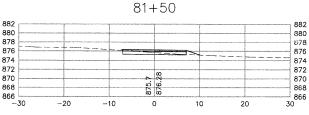
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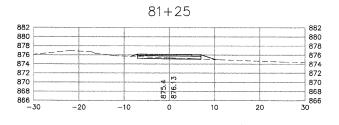
EXISTING GROUND PROFILE PROPOSED GROUND PROFILE PROPOSED TRAIL W/SHOULDER

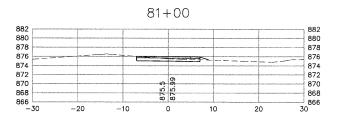
GROUND SLOPE/TIEOUT SLOPE EXISTING/PROPOSED SPOT ELEVATION

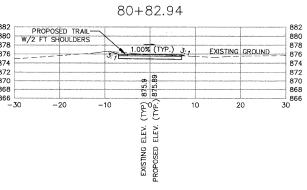


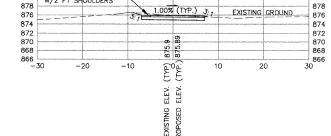
81 + 75

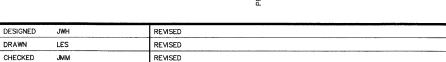








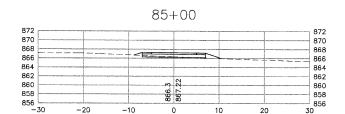


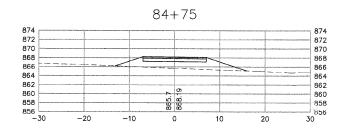


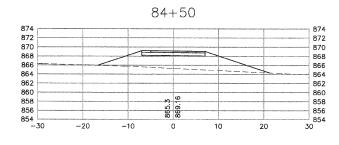
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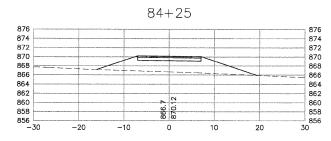
DEPARTMENT OF TRANSPORTATION

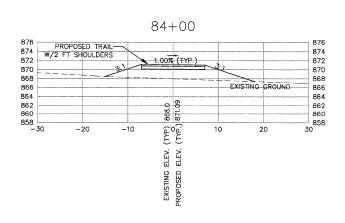
STA. 80+82.94 TO STA. 83+75



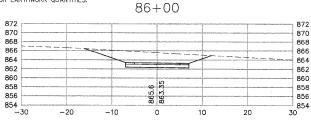


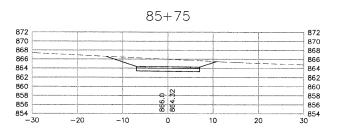


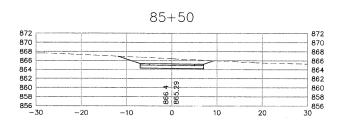


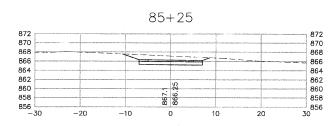


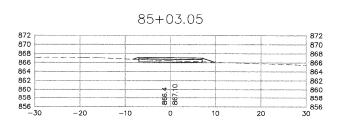
- TIE OUT SLOPES SHALL BE 1V: 3H MAX AND APPLIED AT EACH CROSS SECTION UNLESS LABELED OTHERWISE.
- 3. PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SHOULDER SLOPES AND TRANSITIONS.
- 4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.







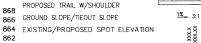


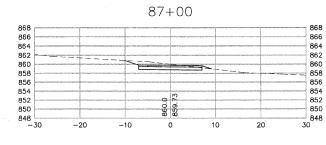


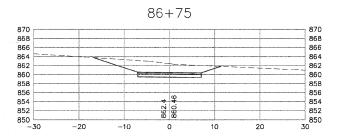
CROSS SECTION LEGEND:

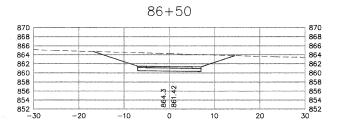
EXISTING GROUND PROFILE PROPOSED GROUND PROFILE PROPOSED TRAIL W/SHOULDER 866 GROUND SLOPE/TIEOUT SLOPE

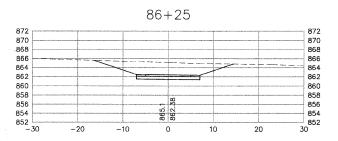
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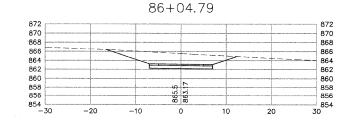
















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CHECKED	JMM	REVISED
DATE	10/02/2009	REVISED

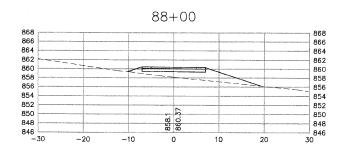
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

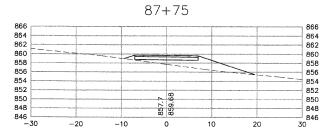
CARY PARK DISTRICT CARY COMMUNITY TRAIL

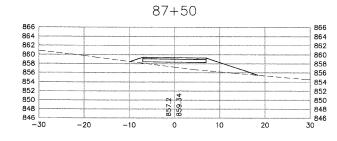
CROSS SECTIONS STA. 84+00 TO STA. 87+00

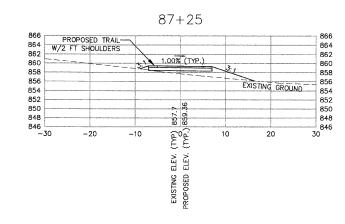
SECTION	COUNTY		TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY		64	39
FED. ROAD DIST, NO. 1		ILLINOIS	CONTRACT	NO. 63333

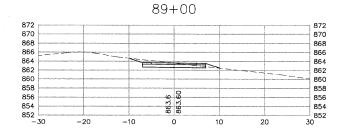
- TIE OUT SLOPES SHALL BE 1V: 3H MAX AND APPLIED AT EACH CROSS SECTION UNLESS LABELED OTHERWISE.
- PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SHOULDER SLOPES AND TRANSITIONS.
- 4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.

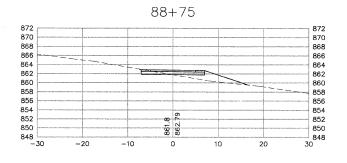


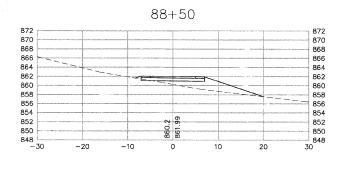


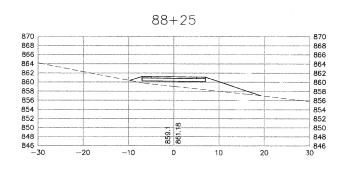








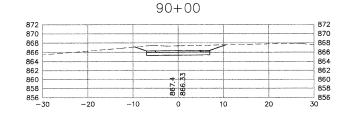


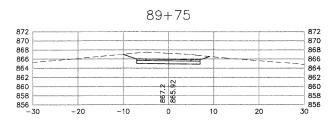


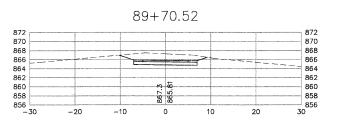
CROSS SECTION LEGEND:

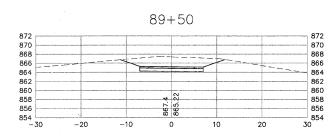
EXISTING GROUND PROFILE 1%_ 3:1

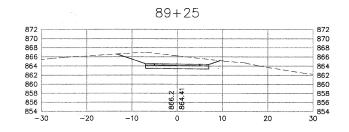










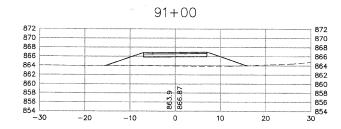


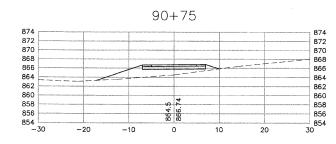


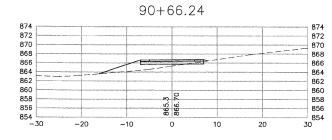


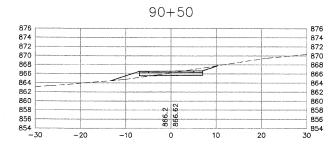
9	DESIGNED	JWH	REVISED
K:\OH	DRAWN	LES	REVISED
nan gr	CHECKED	JMM	REVISED
1	DATE	10 /02 /2000	DEVISED

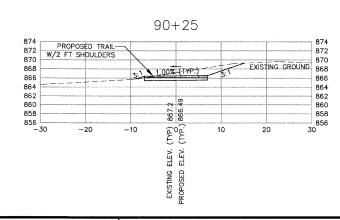
SECTION	C	YTMUC	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY		64	40
D. ROAD DIST. NO. 1		ILLINOIS	CONTRACT	NO. 63333

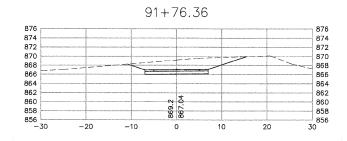


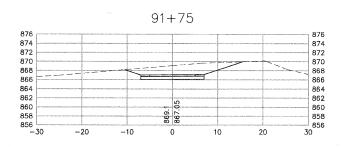


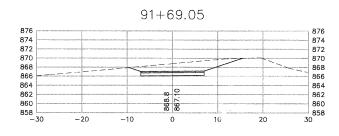


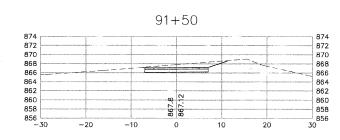


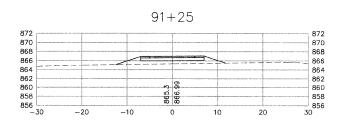


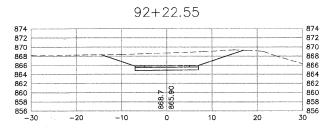


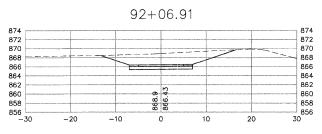


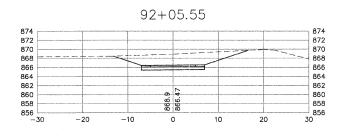


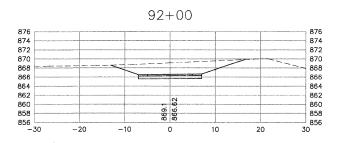


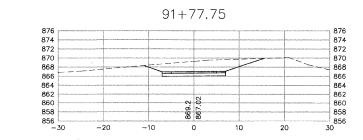












CROSS SECTION LEGEND:

EXISTING GROUND PROFILE PROPOSED GROUND PROFILE PROPOSED TRAIL W/SHOULDER 1%_ 3:1 GROUND SLOPE/TIEOUT SLOPE EXISTING/PROPOSED SPOT ELEVATION

GENERAL NOTES:

SURFACE ON NORTH SIDE OF BERM (ALIGNMENT 6)

3. PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SHOULDER SLOPES AND TRANSITIONS.

4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.

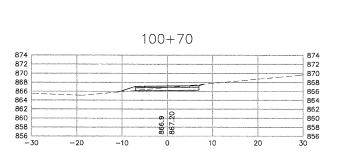


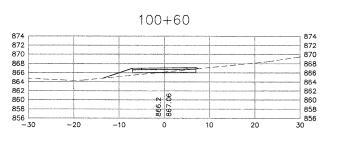


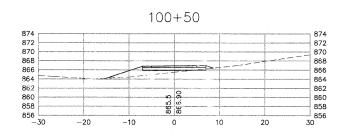
DESIGNED	JWI	REVISED
DRAWN	LES	REVISED
CHECKED	JMM	REVISED
DATE	10/02/2009	REVISED

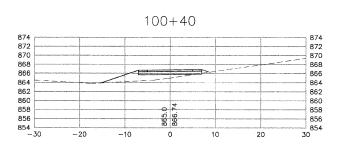
SECTION	COUNTY		TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY		64	41
FED. ROAD DIST. NO. 1		ILLINOIS	CONTRACT	NO. 63333

100+80 872 870 868 866 864 862 860 858 856









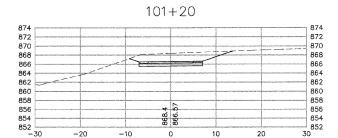
GENERAL NOTES:

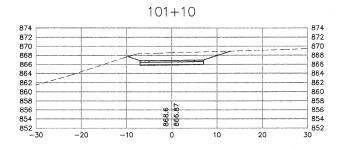
- TIE OUT SLOPES SHALL BE 1V:3H MAX AND APPLIED AT EACH CROSS SECTION UNLESS LABELED OTHERWISE.
- 3. PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SHOULDER SLOPES AND TRANSITIONS.
- 4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.

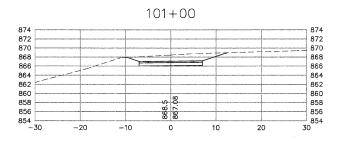
CROSS SECTION LEGEND:

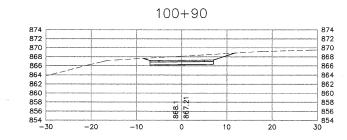
GROUND SLOPE/TIEOUT SLOPE 1%_ 3:1 EXISTING/PROPOSED SPOT ELEVATION

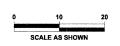
XXXX XXXX













重量	DESIGNED	JWH	REVISED
and improper	DRAWN	LES	REVISED
	CHECKED	JMM	REVISED
of o Drawk	DATE	10/02/2009	REVISED

100 + 30

100 + 20

100+10

100+00

870

- 862 - 860 - 858 - 856 - 854 30

866 864

- 862 - 860 - 858 - 856 - 854 30

874 872

870

872 870

876 874 872

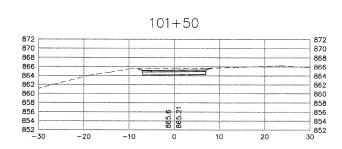
870 866

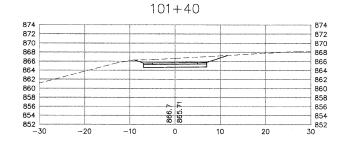
860

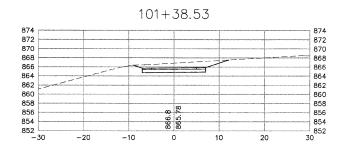
-20

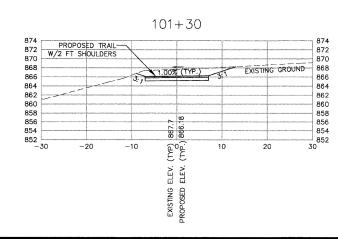
-10

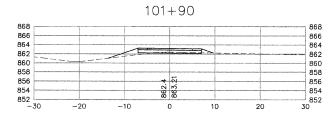
SECTION	COUNTY		TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY		64	42
FED. ROAD DIST. NO. 1		ILLINOIS	CONTRACT	i NO. 63333

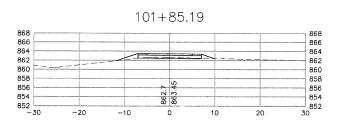


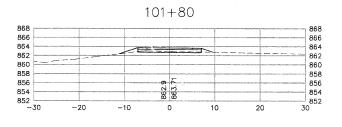


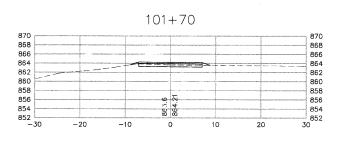


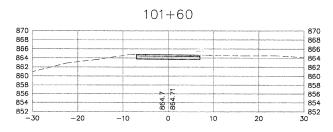












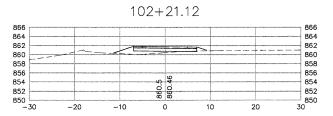
- TIE OUT SLOPES SHALL BE 1V:3H MAX AND APPLIED AT EACH CROSS SECTION UNLESS LABELED OTHERWISE.

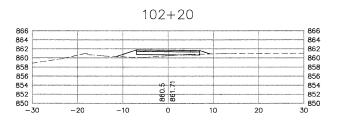
- 4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.

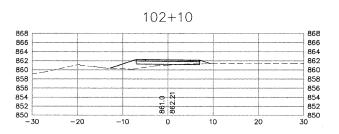
CROSS SECTION LEGEND:

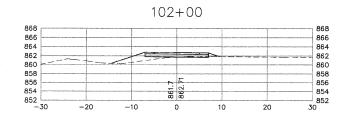
EXISTING GROUND PROFILE PROPOSED TRAIL W/SHOULDER GROUND SLOPE/TIEOUT SLOPE 1%_ 3:1 EXISTING/PROPOSED SPOT ELEVATION

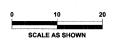








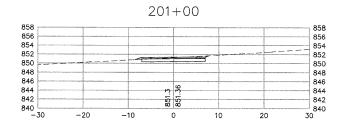


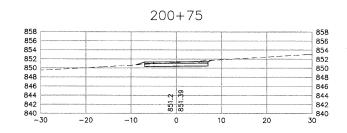


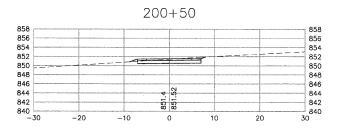


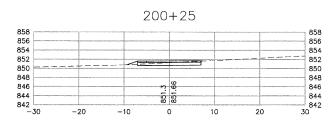
3	DESIGNED	JWH	REVISED
2	DRAWN	LES	REVISED
nou d	CHECKED	JMM	REVISED
ŧ	DATE	10 /02 /2000	DEMSED

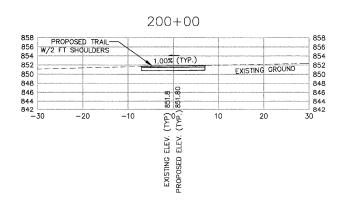
SECTION	C	OUNTY	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	МС	HENRY	64	43
FED. ROAD DIST.	NO. 1	ILLINOIS	CONTRACT	NO. 63333



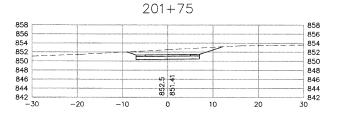


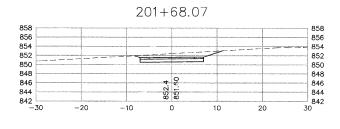


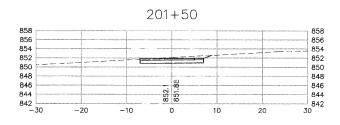


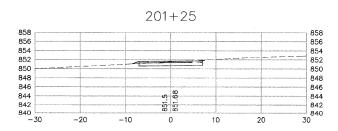


201 + 79.46858 856 854 852 850 848 846 844 842 30 856 854 852 850 848 846 844 842 -30









GENERAL NOTES:

- TIE OUT SLOPES SHALL BE 1V: 3H MAX AND APPLIED AT EACH CROSS SECTION UNLESS LABELED OTHERWISE.

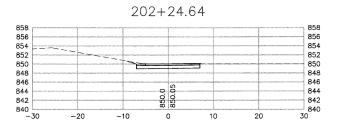
CROSS SECTION LEGEND:

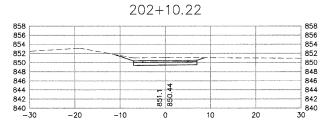
PROPOSED TRAIL W/SHOULDER GROUND SLOPE/TIEOUT SLOPE EXISTING/PROPOSED SPOT ELEVATION

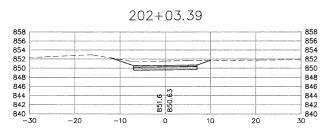


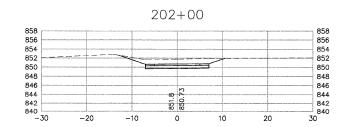
3. PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SHOULDER SLOPES AND TRANSITIONS.

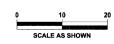
4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.











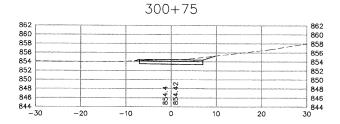


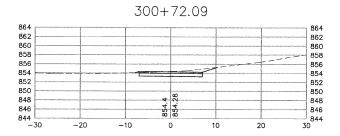
REVISED DRAWN LES REVISED CHECKED JMM REVISED DATE 10/02/2009 REVISED

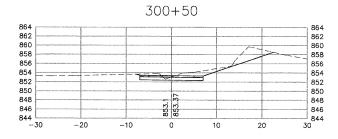
STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

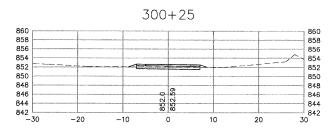
CARY PARK DISTRICT CARY COMMUNITY TRAIL CROSS SECTIONS STA. 200+00 TO STA. 202+24.64

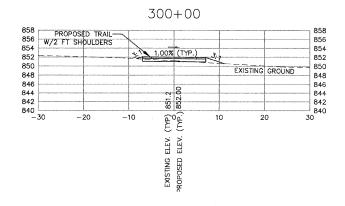
SECTION	C	YTAUC	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	МС	CHENRY	64	44
FED. ROAD DIST.	NO. 1	ILLINOIS	CONTRACT	NO. 63333

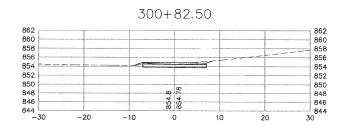












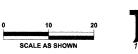
- 1. TIE OUT SLOPES SHALL BE 1V:3H MAX AND APPLIED AT EACH CROSS SECTION UNLESS LABELED OTHERWISE.
- PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SHOULDER SLOPES AND TRANSITIONS.
- 4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.

CROSS SECTION LEGEND:

EXISTING GROUND PROFILE PROPOSED GROUND PROFILE GROUND SLOPE/TIEOUT SLOPE 1%__ 3:1 EXISTING/PROPOSED SPOT ELEVATION

XXX XXX

××	



SECTION

06-P4001-00-BR

10 E AS SHOWN	20	VLIE 800-892-0123
COUNTY	TOTAL SHEETS	SHEET NO.
MCHENRY	64	45

CONTRACT NO. 63333

9	DESIGNED	JWH	REVISED		
, K	DRAWN	LES	REVISED		
g name	CHECKED	JMM	REVISED		
100	DATE	10/02/2009	REVISED	 	

 PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SHOULDER SLOPES AND TRANSITIONS.

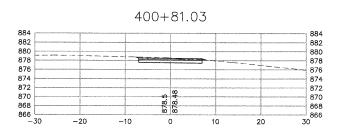
4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.

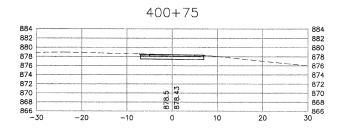
CROSS SECTION LEGEND:

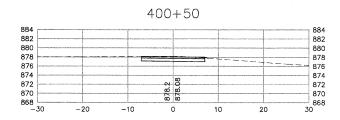
EXISTING GROUND PROFILE
PROPOSED GROUND PROFILE
PROPOSED TRAIL W/SHOULDER
GROUND SLOPE/TIEOUT SLOPE
EXISTING/PROPOSED SPOT ELEVATION

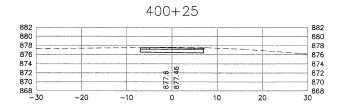
1%_ 3:1

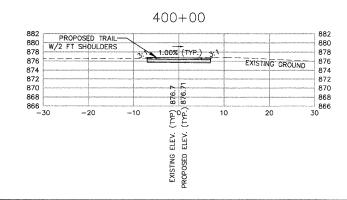
123__ 3:1 XX XX XX











DESIGNED

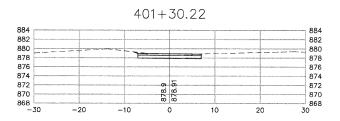
CHECKED

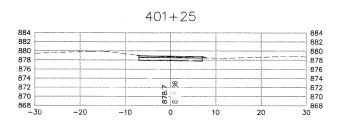
LES

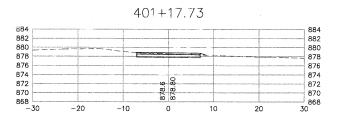
JMM

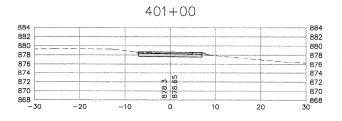
10/02/2009

DRAWN













REVISED	
REVISED	STATE
REVISED	DEPARTMENT
REVISED	<i></i>

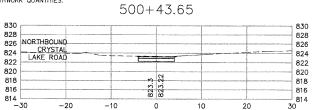
STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

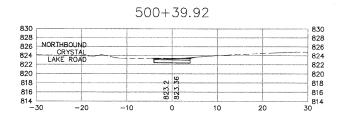
CARY PARK DISTRICT
CARY COMMUNITY TRAIL

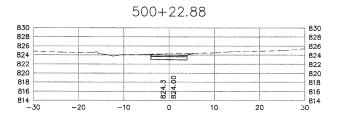
CROSS SECTIONS STA. 400+00 TO STA. 401+30.22

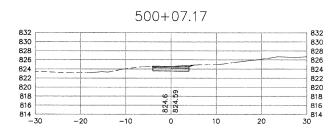
		1 1 1 1		
SECTION	C	YTNUC	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	МС	CHENRY	64	46
FED. ROAD DIST.	NO. 1	ILLINOIS	CONTRACT	NO. 63333

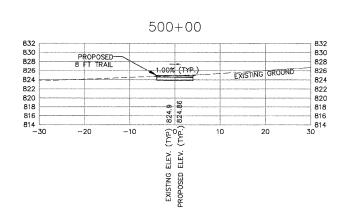
- 1. THE OUT SLOPES SHALL BE 1V: 3H MAX AND APPLIED AT EACH
- 2. EXISTING GROUND SURFACE ON NORTH SIDE OF BERM (ALIGNMENT 6) HAS BEEN MODIFIED FROM SURVEY AS DIRECTED BY CARY PARK DISTRICT AT THE FOLLOWING STATIONS. 53+00, 54+00, 55+00, 56+00, 56+00, 65+00, 66+00, 66+00, 66+00, 66+00, 66+00, 66+00, 70+00, AND 71+00. CHANGES ARE PER FIELD VERIFICATIONS BY CARY PARK DISTRICT STAFF, CONTRACTOR TO VERIFY EMSTING GRADES, WITH CARY PARK DISTRICT SEFORE BEGINNING
- 3. PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SHOULDER SLOPES AND TRANSITIONS.
- 4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.

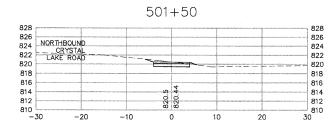


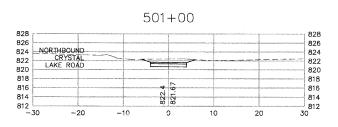


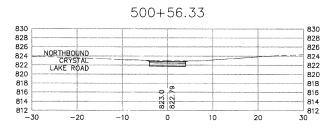


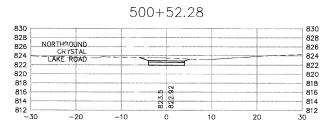


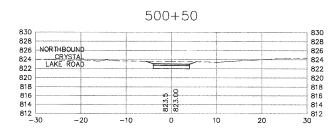












CROSS SECTION LEGEND:

EXISTING GROUND PROFILE

PROPOSED GROUND PROFILE

PROPOSED TRAIL W/SHOULDER

GROUND SLOPE/TIEOUT SLOPE

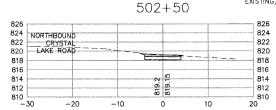
EXISTING/PROPOSED SPOT ELEVATION

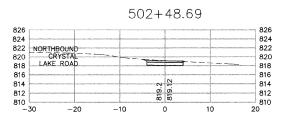
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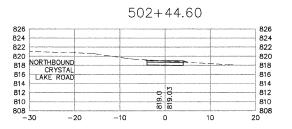
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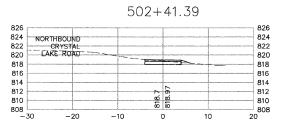
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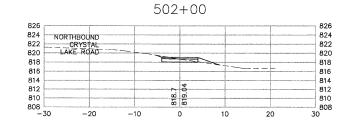
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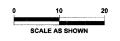














DESIGNED	JWH	REVISED
DRAWN	LES	REVISED
CHECKED	JMM	REVISED
DATE	10 /02 /2009	PEMSED

SECTION	C	YTNUC	TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	мс	CHENRY	64	47
FED. ROAD DIST.	NO. 1	ILLINOIS	CONTRACT	NO. 63333

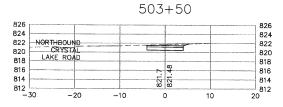
- 1. TIE OUT SLOPES SHALL BE 1V:3H MAX AND APPLIED AT EACH CROSS SECTION UNLESS LABELED OTHERWISE.
- 2. EXISTING GROUND SURFACE ON NORTH SIDE OF BERM (ALICNMENT 6) HAS BEEN MODIF FROM SURVEY AS DIRECTED BY CARY PARK DISTRICT AT THE FOLLOWING STATIONS. 33+00, 54+00, 55+00, 56+00, 57+00, 59+00, 63+00, 64+00, 68+00, 69+00, 66+00, 67+00 GBHOW, 69+00, 70+00, AND 71+00. CHANGES ARE PER FILED VERIFICATIONS BY CARY PARK DISTRICT STAFF. CONTRACTOR TO VERIFY EXISTING GRADES WITH CARY PARK DISTRICT BEFORE BEGINNING WORK AT THEFE STATIONS.
- 3. PLEASE SEE TYPICAL SECTIONS ON SHEET 3 AND GRADING SHEETS 54-58 FOR PAVEMENT/SHOULDER SLOPES AND TRANSITIONS.
- 4. PLEASE SEE SHEET 48 FOR EARTHWORK QUANTITIES.

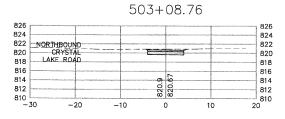
CROSS SECTION LEGEND:

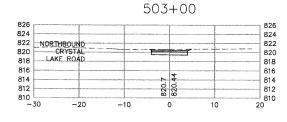
EXISTING GROUND PROFILE
PROPOSED GROUND PROFILE
PROPOSED TRAIL W/SHOULDER
GROUND SLOPE/TIEOUT SLOPE
EXISTING/PROPOSED SPOT ELEVATION

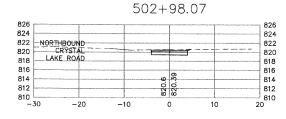
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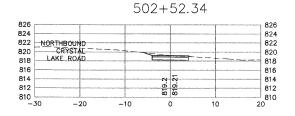
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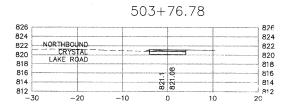


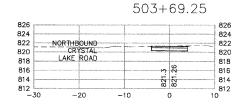












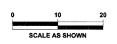
		50	03 + 59.	11
826 824 822 820 818 816 814	NORTHBOUND CRYSTAL LAKE ROAD		821.44	826 824 822 820 818 816 814 812

STATION	AK Squar			UMES Yards	CUMULATIV Cubic	
10+00	CUT 13.94	FILL 0.00	CUT	FILL	CUT	FILL
10+00	9.07	0.00	22.92	0.38	22.92	0.38
10+75	11.13	0.26	7.94	0.24	30.86 31.48	0.62
11+00	12.16	0.17	10.32	0.21	41.80	0.85
12+00	8.99	1.19	37.88	2.79	79.67	3.64
12+79.44	10.62	0.59	28.86 8.68	2.60 0.37	108.53	6,24
13+00 13+21.82	12.21	0.37	10.89	0.21	128.10	6.82
14+00	15.14	0.03	43.33	0.24	171.43	7.06
15+00	15.41	0.03	56.58	0.10	228.00	7.17
16+00 17+00	15.82 28.02	0.26	81.18	0.49	367.01	8.19
17+76.27	46.15	0.00	104.75	0.00	471.76	8.19
18+00	61.61	0.00	47.56 57.31	0.00	519.33 576.64	8.19 8.19
18+25	61.61	0.00	57.31	0.00	633.95	8.19
18+50 18+75	61.61 61.61	0.00	57.31	0.00	691.26	8.19
19+00	83.92	0.00	67.67 78.04	0.00	758.93 836.97	8.19 8.19
19+25	83.92	0.00	78.04 78.04	0.00	915.00	8.19
19+75	83.92 83.92	0.00	76.04	0.00	993,04	8.19
20+00	37.06	0.00	56.22 34.42	0.00	1049.26	8.19 8.19
20+25	37.06	0.00	34.42	0.00	1118.11	8.19
20+50	37.06 37.06	0.00	34.42	0.00	1152.53	8.19
21+00	20.33	0.00	28.63	0.00	1179.16	8.19
21+25	20.33	0.00	18.85	0.00	1198.01 1216.86	8.19 8.19
21+50	20.33	0.00	18.85	0.00	1235.71	8.19
21+75	20.33 8.44	0.00	13.32	0.22	1249.04	8.41
22+25	8.44	0.48	7.81	0.44	1256.84	8.86
22+50	8.44	0.48	7.81	0.44	1264.65 1272.46	9.30 9.75
22+75	8.44	0.48	4.60	1.34	1277.05	11.09
23+00 23+25	1.52	2.39	1.40	2.23	1278.45	13.31
23+50	1.52	2.39	1.40	2.23	1279.85	15.54
23+75	1.52	2.39	1.40	2.23	1281.25	17.77
24+00	2.64 3.67	2.01 1.67	0.73	0.43	1283.90	20.24
24+06.29	12.40	0.08	11.25	1.23	1295.15	21.47
24+50	12.40	0.08	2.71	0.02	1297.86	21.49
24+75	12.40	0.08	9.60	0.08	1309.35	21.57
25+00 25+05.07	8.32 9.06	0.58	1.63	0.10	1320.58	21.97
26+00	9.06 15.20	0.47	42.65	0.86	1363.23	22.83
6+93.26	17.15	0.06	55.87	0.14	1419.10	22.95
27+00	17.23	0.06	16.00	0.01	1423.41	23.03
27+25	17.23	0.06	13.81	0.04	1453.22	23.08
28+00	15.59	0.09	27.10	0.12	1480.32	23.20
8+29.78	13.63	0.09	16.11	0.10	1496.43 1506.63	23.30
28+50	13.63	0.09	4.97	0.09	1511.60	23.47
28+62.11	8.52 7.72	0.32	1.77	0.08	1513.36	23.54
28+75	7.72	0.40	2.00	0.10	1515.37	23.65
29+00	3.71	1.40	5.29	1.30	1520.66 1524.10	24.48
29+25 29+50	3.71	1.40	3.44	1.30	1527.54	27.08
9+70.06	8.97	0.40	4.71	0.67	1532.25	27.76
30+00	12.86	0.24	12.11	0.35	1544.35 1559.83	28.11 28.31
0+30.50	14.53	0.12	10.47	0.09	1570.30	28.40
30+50 30+75	14.53	0.12	13.43	0.11	1583.73	28.52
0+76.96	21.31	0.00	1.30	0.00	1585.03	28.52
31+00	23.85	0.00	19.27 77.14	0.00	1604.30 1681.44	28.52 28.52
32+00	20.35	0.00	4.52	0.00	1685.96	28.52
32+00 32+25	22.17 22.17	0.00	20.52	0.00	1706.48	28.52
2+36.86	27.02	0.00	10.80 78.65	0.00	1717.28 1795.93	28.52 28.52
33+00	40.24	0.00	78.65 112.03	0.00	1795.93	28.52
34+00 35+00	20.25	0.00	75.84	0.00	1983.80	28.52
36+00	32.05	0.00	97.69	0.00	2081.49	28.52
37+00	18.29	0.00	93.23	0.00 4.83	2174.72 2215.51	28.52 33.35
38+00	3.73	2.61	37.68	4.83	2215.51	38.18
39+00 40+00	16.62 31.93	0.00	89.91	0.00	2343.09	38.18
0+73.06	18.39	0.00	68.09	0.00	2411.18	38.18
40+75	18.39	0.00	1.32	0.00	2412.50 2428.87	38.18 38.18
41+00	16.98	0.00	1.59	0.00	2428.87	38.18
1+02.70 42+00	16,82	0.00	57.87	0.09	2488.43	38.27
2+34.87	13.56	0.05	18.63	0.11	2507.06	38.38
42+50	13,49	0.08	7.58	0.06	2514.63 2527.12	38.43
42+75	13.49	0.08	0.39	0.00	2527.12	38.51
2+75.83 43+00	11.61 13.83	0.08	11.38	0.09	2538.89	38.60
44+00	20.84	0.11	64.20	0.84	2603.09	39.43
45+00	18.40	0.28	72.67	0.23	2675.76 2689.08	40.57
5+18.36	20.79	0.41	5.32	0.23	2694.40	40.86
45+25 45+50	22.54	0.20	24.27	0.09	2718.67	40.97
5+63.66	35.43	0.00	16.51	0.00	2735.18	40.97
46+00	56.32	0.00	61.75	0.00	2796.93 2899.25	40.97
6+42.53	73.59	0.00	20.25	0.00	2899.25	40.97
46+50 46+75	73.59 73.59	0.00	67.76	0.00	2987.26	40.97
6+97.12	53.85	0.00	51.96	0.00	3039.22	40.97
47+00	52.64	0.00	5.69 89.58	0.00	3044.91	40.97
7+72.64	13,96	0.00	1.22	0.00	3135.71	40.97
47+75 48+00	13.96 13.96	0.00	12.92	0.00	3148.64	40.97
48+25	13.96	0.00	12.92	0.00	3161.56	40.97
48+50	13.96	0.00	12.92	0.00	3174.49	40.97
48+75	13.96	0.00	0.81	0.00	3187.41	40.97
8+76.56	13.96	0.00				

		EAS e Feet	VOL: Cubic	VMES Yards	CUMULATIV Cubic	E VOLUME Yards
STATION	CUT	FILL	CUT	FILL	CUT	FILL
50+00 51+00	14.03	0.03 3.19	29.12	5.96	29.12	5.96
52+00	9.36	0.73	20.48	7.25	49.59	13.21
■53+00	36.90	6.93	85.68 114.38	14.18 24.62	135.27 249.65	27.39 52.01
■54+00	24.86	6.37	111.50	35.00	361.16	87.01
■55+00 ■56+00	35.35 32.92	12.53	126.42	48.85	487,57	135.86
≈57+00	49,89	2.63	153.34	30.52	640.91	166.38
●58+00	52.25	0.00	189.14	4.88	830.05	171.25 182.52
59+00	1.84	6.09	0.33	11.27 3.99	930.21 930.54	186.51
59+09.67	0.00	16.17 17.74	0.00	9.61	930.54	196.13
59+25 59+36.24	0.00	27.19	0.00	9.33	930.54	205.46
60+00	0.00	46.09	0.00	86.53	930.54	291.99
61+00	0.00	124.06	0.00	315.09 164.45	930.54 930.54	607.08 771.53
61+45.20	0.00	72.42	0.00	11.14	930.54	782.67
61+50 61+75	0.00	53.10 0.08	7.56	24.50	938.10	807.17
62+00	64.79	0.00	37.55 45.60	0.04	975.65 1021.25	807.21
62+21.15	51.90	0.00	139.95	10.26	1021.25	807.21 817.47
62+98.48	45.83	7.17 8.81	2.58	0.44	1163.78	817.91
63+25	45.94 46.79	0.00	43.02	4.05	1206.81	821.96
63+47.87	46.54	0.00	39.59	0.00	1246.40	821.96
64+00	39.17	31.33	82.74 148.73	30.24	1329.14	852.21 966.34
65+00	41.14	30.30	140.71	114.13 56.11	1618.58	1022.45
66+00	34.84	0.00	131.41	0.00	1749.99	1022.45
67+00 68+00	36.12 37.57	0.00	136.46	0.00	1886.44	1022.45
69+00	39.36	0.00	142.46	0.00	2028.91	1022.45
70+00	32.51	20.12	133.10	37.26 37.26	2162.01 2280.75	1059.70
71+00	31.61	0.00	114.77	0.00	2395.53	1096.96
72+00 73+00	30.37 28.43	0,00 21.98	108.87	40.70	2504.40	1137.66
74+00	28.43 45.68	21.98 5.15	137.22	50,24	2641.63	1187.90
75+00	81.14	0.00	234.84	9.54	2876,46	1197.44
76+00	16.71	0.00	181.20 171.43	0.00	3057.67 3229.09	1197.44
77+00	75.86	0.00	181.62	0.56	3410.72	1198.00
78+00 79+00	22.22 12.83	0.30	64.91	0.98	3475.63	1198.98
80+00	34.02	0.00	36.76	0.42	3562.39	1199.40
80+82.94	15.42	0.08	75.93	0.12	3638.32 3638.32	1199.52 1199.52
	AR.	PAG		TMRS	CUMULATTY	
STATION	Squar			Yards	Cubic	Yards
	CUT	FILL	CUT	FILL	CUT	PILL
80+82.94	15.42	0.0B	7.56	0.22	7.56	0.22
81+00 81+25	8.31 3.27	0.67 1.98	5.42	1.17	12.98	1.39
81+50	5.94	1.48	4.32	1.53	17.30	2.93
81+75	8.96	0.84	6.9B 9.74	0.42	24.28 34.02	3.94 4.36
82+00	11.95	0.13	12.35	0.07	46.37	4.43
82+25 82+27.89	14.63	0.03	1.59	0.00	47.96	4.43
82+50	22.05	0.02	15.12	0.01	63.08	4.44
82+75	12.37	0.07	15.87 5.54	0.03 4.95	78,95 84.50	4.47 9.43
83+00	0.03	10.05	0.01	18.69	84.51	28.11
83+25 83+39.83	0.00	28.81 34.72	0.00	18.01	84.51	48.13
83+50	0.00	36.57	0.00	13.29	84.51	59.41
83+75			0.00	38.41	84.51	97.83
84+00	0.00	47.14	0.00			
	0.00	60.30	0.00	49.42	84.51 84.51	147.25
84+25	0.00 0.00 0.00	60.30 72.07	0.00 0.00		84.51 84.51 84.51	147.25 208.25 281.28
84+25 84+50	0.00 0.00 0.00 0.00	60.30 72.07 86.60	0.00 0.00 0.00	49.42 61.00 73.03 58.30	84.51 84.51 84.51	208.25 281.28 339.58
84+25	0.00 0.00 0.00	60.30 72.07	0.00 0.00 0.00 0.75	49.42 61.00 73.03 58.30 19.59	84.51 84.51 84.51 85.27	208.25 281.28 339.58 359.17
84+25 84+50 84+75 85+00 85+03.05	0.00 0.00 0.00 0.00 0.00 0.00 1.62 4.53	60.30 72.07 86.60 40.12 2.63 1.48	0.00 0.00 0.00 0.75 0.35	49.42 61.00 73.03 58.30 19.59 0.23	84.51 84.51 84.51 85.27 85.82	208.25 281.28 339.58 359.17 359.40
84+25 84+50 84+75 85+00 85+03.05 85+25	0.00 0.00 0.00 0.00 0.00 1.62 4.53 28.38	60.30 72.07 86.60 40.12 2.63 1.48 0.00	0.00 0.00 0.00 0.75	49.42 61.00 73.03 58.30 19.59	84.51 84.51 84.51 85.27 85.82 98.87	208.25 281.28 339.58 359.17
84+25 84+50 84+75 85+00 85+03.05 85+25 85+50	0.00 0.00 0.00 0.00 0.00 1.62 4.53 28.38 34.62	60.30 72.07 86.60 40.12 2.63 1.48 0.00 0.00	0.00 0.00 0.00 0.75 0.35 13.25	49.42 61.00 73.03 58.30 19.59 0.23 0.61	84.51 84.51 84.51 85.27 85.62 98.87 127.81 165.06	208.25 281.26 339.58 359.17 359.40 360.01
84+25 84+50 84+75 85+00 85+03.05 85+25 85+50 85+75 86+00	0.00 0.00 0.00 0.00 0.00 1.62 4.53 28.38	60.30 72.07 86.60 40.12 2.63 1.48 0.00	0.00 0.00 0.00 0.75 0.35 13.25 28.94 37.25 49.39	49.42 61.00 73.03 58.30 19.59 0.23 0.61 0.00 0.00	84.51 84.51 84.51 85.27 85.62 98.87 127.81 165.06 214.45	208.25 281.28 339.58 359.17 359.40 360.01 360.01 360.01
84+25 84+50 84+75 85+00 85+03.05 85+25 85+50 85+75 86+00	0.00 0.00 0.00 0.00 0.00 1.62 4.53 28.38 34.62 46.61	60.30 72.07 86.60 40.12 2.63 1.48 0.00 0.00	0.00 0.00 0.00 0.75 0.35 13.25 28.94 37.25 49.39	49.42 61.00 73.03 58.30 19.59 0.23 0.61 0.00 0.00 0.00	84.51 84.51 84.51 85.27 85.62 98.87 127.81 165.06 214.45 225.52	208.25 281.28 339.58 359.17 359.40 360.01 360.01 360.01 360.01
84+25 84+50 84+75 85+00 85+03.05 85+25 85+50 85+75 86+00 86+04.79 86+25	0.00 0.00 0.00 0.00 0.00 1.62 4.63 28.38 34.62 46.61 61.35 65.33 75.33	60.30 72.07 86.60 40.12 2.63 1.48 0.00 0.00 0.00 0.00 0.00	0.00 0.00 0.00 0.75 0.35 13.25 28.94 37.25 49.39	49.42 61.00 73.03 58.30 19.59 0.23 0.61 0.00 0.00 0.00 0.00	84.51 84.51 84.51 85.27 85.62 98.87 127.81 165.06 214.45	208.25 281.28 339.58 359.17 359.40 360.01 360.01 360.01
84+25 84+50 84+75 85+00 85+03.05 85+25 85+50 85+75 86+00 86+04.79 86+25 86+50	0.00 0.00 0.00 0.00 0.00 1.62 4.53 28.38 34.62 46.61 61.35 65.33 75.33 78.85	60.30 72.07 86.60 40.12 2.63 1.48 0.00 0.00 0.00 0.00 0.00 0.00	0.00 0.00 0.00 0.75 0.35 13.25 28.94 37.25 49.39 11.07 52.81 71.52 64.24	49.42 61.00 73.03 58.30 19.59 0.23 0.61 0.00 0.00 0.00 0.00 0.00 0.00	84.51 84.51 84.51 85.27 85.82 98.87 127.81 163.06 214.45 225.52 278.33 349.84	208.25 281.28 339.58 359.17 359.40 360.01 360.01 360.01 360.01 360.01 360.01
84+25 84+50 84+75 85+00 85+03.05 85+25 85+25 85+75 86+00 86+04.79 86+25 86+50 86+75	0.00 0.00 0.00 0.00 0.00 1.62 4.53 28.38 34.62 46.61 61.35 65.33 75.33 78.85 59.39	60.30 72.07 86.60 40.12 2.63 1.48 0.00 0.00 0.00 0.00 0.00 0.00 0.00 0	0.00 0.00 0.75 0.35 13.25 28.94 37.25 49.39 11.07 52.81 71.52 64.24 36.38	49.42 61.00 73.03 58.30 19.59 0.23 0.61 0.00 0.00 0.00 0.00 0.00 0.00 0.00	84.51 84.51 84.51 85.27 85.62 98.87 127.61 165.06 214.45 225.52 278.33 349.84 414.08	208.25 281.28 339.58 359.17 359.40 360.01 360.01 360.01 360.01 360.01 360.01 360.01
84+25 84+50 94+75 85+00 85+03 85+25 85+25 85+75 86+00 86+04.79 86+25 88+50 86+75 87+00 87+25	0.00 0.00 0.00 0.00 0.00 1.62 4.53 28.38 34.62 46.61 61.35 65.33 75.33 78.85	60.30 72.07 86.60 40.12 2.63 1.48 0.00 0.00 0.00 0.00 0.00 0.00	0.00 0.00 0.00 0.00 0.75 0.35 13.25 28.94 37.25 49.39 11.07 52.81 71.52 64.24 36.38 8.61	49.42 61.00 73.03 58.30 19.59 0.23 0.61 0.00 0.00 0.00 0.00 0.00 0.00 0.00	84.51 84.51 84.51 85.27 85.62 98.87 127.81 165.06 214.45 225.52 278.33 349.84 44.08 459.27	208.25 281.28 339.58 359.17 359.40 360.01 360.01 360.01 360.01 360.01 360.01 360.01 360.01
84+25 84+50 84+75 85+00 85+03.05 85+25 85+25 85+50 85+75 86+00 86+04.79 86+25 86+50 86+75 87+05 87+25	0.00 0.00 0.00 0.00 0.00 1.62 4.63 34.62 46.61 61.35 65.33 78.85 59.39 18.78 0.03 0.00	60.30 72.07 86.60 40.12 2.63 1.48 0.00 0.00 0.00 0.00 0.00 0.00 0.00 0	0.00 0.00 0.00 0.75 13.25 28.94 37.25 49.39 11.07 52.81 71.52 64.24 36.38 8.61	49.42 61.00 73.03 58.30 19.59 0.23 0.61 0.00 0.00 0.00 0.00 0.00 0.00 0.00	84.51 84.51 84.51 85.27 85.62 98.87 127.81 165.06 214.45 225.52 278.33 349.84 414.08 450.46 459.27	208.25 281.28 339.58 359.17 359.40 360.01 360.01 360.01 360.01 360.01 360.01 360.01 360.01 360.01 360.01
84+25 84+50 84+75 85+03 85+03 85+03 85+25 85+50 86+04 86+04 86+04 86+55 86+75 87+00 87+25 87+50 87+75	0.00 0.00 0.00 0.00 0.00 1.62 4.53 28.38 34.62 46.61.35 65.33 75.33 78.85 59.39 18.78 0.03	60.30 72.07 88.60 40.12 2.63 1.48 0.00 0.00 0.00 0.00 0.00 0.00 0.00 0	0.00 0.00 0.00 0.00 0.75 0.35 13.25 28.94 37.25 49.39 11.07 52.81 71.52 64.24 36.38 8.61	49.42 61.00 73.03 58.30 19.59 0.23 0.61 0.00 0.00 0.00 0.00 0.00 0.00 0.00	84.51 84.51 84.51 85.27 85.62 98.87 127.81 165.06 214.45 225.52 278.33 349.84 44.08 459.27	208.25 281.28 339.58 359.17 359.40 360.01 360.01 360.01 360.01 360.01 360.01 360.01 360.01
84+25 84+50 84+75 85+00 85+03.05 85+25 85+25 85+50 85+75 86+00 86+04.79 86+25 86+50 86+75 87+05 87+25	0.00 0.00 0.00 0.00 0.00 1.62 4.63 34.62 46.61 61.35 65.33 78.85 59.39 18.78 0.03 0.00	60.30 72.07 88.60 40.12 2.63 1.48 0.00 0.00 0.00 0.00 0.00 0.00 0.00 0	0.00 0.00 0.00 0.75 0.35 13,25 28,94 37,25 49,39 11,07 52,31 71,52 64,24 36,38 8,61 0.01 0.00	49.42 61.00 73.03 58.30 19.59 0.23 0.61 0.00 0.00 0.00 0.00 0.00 0.00 0.00	84.51 84.51 84.51 85.27 85.82 98.87 127.81 165.06 214.45 225.52 278.33 348.84 414.08 450.46 459.27 459.28	208.25 281.28 339.58 359.17 359.40 360.01 36
84+25 84+50 84+75 85+00 85+03.05 85+25 85+25 85+75 86+00 86+04.79 86+25 86+75 87+00 87+25 87+50 87+50 87+50 87+75 88+00	0.00 0.00 0.00 0.00 0.00 1.62 4.63 28.38 34.62 61.35 65.33 75.33 78.85 59.39 10.00 0.00 0.00	60.30 72.07 88.60 40.12 2.63 1.48 0.00 0.00 0.00 0.00 0.00 0.00 0.00 0	0.00 0.00 0.00 0.75 0.35 13,25 28,94 37.25 49,39 11.07 52,81 71,52 64,24 36,38 8,61 0.01	49.42 61.00 73.03 58.30 19.59 0.23 0.61 0.00 0.00 0.00 0.00 0.00 0.00 0.00	84.51 84.51 84.51 85.27 65.62 98.87 127.81 165.06 214.45 2278.33 349.84 414.08 450.46 459.27 459.28	208.25 281.28 339.58 359.17 359.40 360.01 360.01 360.01 360.01 360.01 360.01 360.01 360.01 360.01 360.01 360.01 360.01 360.01 360.01 360.01 360.01 360.01
84+25 84+50 84+75 85+00 85+03.05 85+25 85+50 85+75 86+00.79 86+25 86+50 86+75 87+00 87+25 87+50 87+75 88+00 88+75 88+00 88+75	0.00 0.00 0.00 0.00 0.00 1.62 4.53 34.62 46.61 61.35 65.33 75.33 78.85 59.39 18.78 0.03 0.00	60.30 72.07 88.60 40.12 2.63 1.48 0.00 0.00 0.00 0.00 0.00 0.00 0.00 0	0.00 0.00 0.00 0.75 0.35 13.25 28.94 37.25 49.39 11.07 52.91 71.52 64.24 36.38 8.61 0.01 0.00 0.00	49.42 61.00 73.03 58.30 19.59 0.23 0.61 0.00 0.00 0.00 0.00 0.00 0.00 0.00	84.51 84.51 84.51 85.27 85.62 98.87 127.81 165.06 214.45 225.52 276.33 349.84 414.08 450.26 459.27 459.28 459.28 459.28	208.25 281.28 339.58 359.17 359.40 360.01 36
84+25 84+50 94+75 85+00 85+03.05 85+03.05 85+25 85+25 85+75 86+04.79 86+25 86+75 87+00 87+25 87+00 87+25 87+00 88+50 88+50 88+50 88+50 88+50 88+75	0.00 0.00 0.00 0.00 0.00 1.62 4.53 28.38 34.62 46.61 61.35 65.33 75.33 75.33 78.85 9.00 0.00	60.30 72.07 86.60 40.12 2.63 1.48 0.00 0.00 0.00 0.00 0.00 0.00 0.00 0	0.00 0.00 0.00 0.75 0.35 13,25 28,94 37,25 49,39 11,07 52,31 71,52 64,24 36,38 8,61 0.01 0.00	49.42 61.00 73.03 58.30 19.59 0.23 0.61 0.00 0.00 0.00 0.00 0.00 0.00 0.00	84.51 84.51 84.51 85.27 85.82 98.87 127.81 165.06 214.45 225.52 278.33 348.84 414.08 450.46 459.27 459.28	208.25 281.28 339.58 359.17 359.40 360.01 36
84+25 84+50 84+75 85+00 85+03.05 85+25 85+25 85+50 85+75 86+04.79 86+25 86+75 87+00 87+25 87+75 88+00 88+25 88+59 88+59 88+25 88+59 88+59 88+59	0.00 0.00 0.00 0.00 0.00 0.00 1.62 4.53 34.62 46.61 61.35 65.33 75.33 78.35 69.39 18.78 0.03 0.00	60.30 72.07 88.60 40.12 2.63 1.48 0.00 0.00 0.00 0.00 0.00 0.00 0.00 0	0.00 0.00 0.00 0.75 0.35 13.25 28.94 37.25 49.39 11.07 92.81 71.52 64.24 36.38 8.61 0.01 0.00 0.00 0.00 0.00 0.00 0.00 0	49.42 61.00 73.03 58.30 19.59 0.23 0.61 0.00 0.00 0.00 0.00 0.00 0.00 0.00	84.51 84.51 84.51 85.27 98.87 127.81 165.06 214.45 225.53 349.84 414.08 450.46 450.27 459.28 459.28 459.28 459.28 459.28 459.28 459.28 459.28 459.28 559.47 461.38	208.25 281.28 339.58 359.17 359.40 360.01 36
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84+25 84+50 84+75 55+00 85+03.08 85+25 85+55 85+05 86+04.79 86+04.79 86+04.79 86+75 87+00 87+25 87+75 88+00 87+25 88+50 88+50 87+55 87+50 87+75 88+00 88+25 88+50 88+75	0.00 0.00 0.00 0.00 0.00 1.62 4.63 34.62 46.61 61.35 65.33 78.85 95.33 78.85 0.00 0.00 0.00 0.00 0.00 0.00 0.00	60.30 72.07 86.60 40.12 86.60 40.12 60.00 0.00 0.00 0.00 0.00 0.00 0.00 0.	0.00 0.00 0.00 0.75 0.35 13.25 28.94 37.25 49.39 11.07 52.81 71.52 64.24 36.38 8.61 0.01 0.00 0.00 0.00 0.19 1.91 8.48 9.33 9.33 9.33 9.33 9.33 9.33 9.33 9.3	49.42 61.00 73.03 58.30 19.59 0.23 0.61 0.00 0	84.51 84.51 84.51 85.27 98.87 127.81 165.06 214.45 225.52 276.33 349.84 414.09 450.46 459.27 459.28 459.28 459.28 459.28 459.28 459.28 459.28 459.28 459.28 459.28 616.52	208.25 281.28 339.58 359.17 359.40 360.01 36
84+25 84+50 84+75 85+00 85+03.08 85+03.08 85+03.08 85+03.08 85+05.08 86+07 86+07 86+07 86+07 86+75 86+75 87+70 87+25 87+75 88+08 88+75 88+75 88+75 88+75 88+75 88+75 89+70 89+75 89+75 89+75	0.00 0.00 0.00 0.00 0.00 0.00 1.62 4.63 28.38 34.62 46.61 61.35 65.33 75.33 75.33 75.83 59.39 18.78 0.03 0.00 0.00 0.00 0.00 0.00 0.00 0.0	60.30 72.07 86.60 40.12 86.60 0.00 0.00 0.00 0.00 0.00 0.00 0.0	0.00 0.00 0.00 0.75 0.35 13.25 28.94 37.25 49.39 11.07 52.81 77.52 64.24 36.38 8.81 0.01 0.00 0.00 0.00 0.00 0.00 0.0	49.42 61.00 73.03 58.30 19.59 0.23 0.61 0.00 0.00 0.00 0.00 0.00 0.00 0.00	84.51 84.51 84.51 85.27 98.87 127.81 155.06 214.45 225.52 278.33 349.84 414.08 450.46 450.28 450.28 450.28 450.28 450.28 450.28 50.28 450.28 450.28 450.28 450.28 450.28 450.28	208.25 281.28 339.58 359.17 359.40 360.01 360.01 360.01 360.01 360.01 360.01 360.01 360.01 360.10 36
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84+25 84+75 85+00 85+03.05 85+03.05 85+25 85+05.05 85+75 86+04.79 86+25 86+04.79 86+25 87+25 87+25 87+25 87+25 87+25 88+25 88+25 88+50 88+75 88+25 88+50 88+75 88+25 88+50 88+75 88+25 88+50 88+75 88+25 88+75 88+25 88+	0.00 0.00 0.00 0.00 0.00 0.00 1.62 4.53 28.38 34.62 46.61 61.35 65.33 75.33 78.85 59.39 18.78 0.03 0.00 0.00 0.00 0.00 0.00 0.00 0.0	60.30 72.07 86.60 40.12 86.60 0.00 0.00 0.00 0.00 0.00 0.00 0.0	0.00 0.00 0.00 0.75 13.25 13.25 149.39 11.07 52.81 11.07 52.81 11.07 52.81 11.07 52.81 10.00 0.00 0.00 0.00 0.00 0.00 0.00	49.42 61.00 73.03 58.30 19.59 0.23 0.61 0.00 0.00 0.00 0.00 0.00 0.00 0.23 9.56 30.52 32.53 29.28 20.30 7.27 0.00 0.00 0.00 0.00 0.00 0.00 0.0	84.51 84.51 84.51 85.27 85.62 98.87 127.81 165.06 274.45 225.52 276.33 349.84 450.46 459.27 459.28 4	208.25 28 33.9.58 28 33.9.58 28 33.9.58 28 35.9.17 28 3
84+25 84+75 85+00 85+03.08 85+03.09 85+25 85+55 86+00 86+04.79 86+25 86+75 87+00 87+25 87+75 88+00 88+75 88+00 88+75 88+00 88+75 89+00 88+75 89+75 89+75 99+75 90+00 90+25	0.00 0.00 0.00 0.00 0.00 1.62 4.63 28.38 34.62 46.61 61.35 75.33 75.33 78.85 59.39 18.78 0.03 0.00 0.00 0.00 0.00 0.00 0.00 0.0	60.30 72.07 86.80 40.12 .63 1.48 0.00 0.00 0.00 0.00 0.00 0.00 0.00 0	0.00 0.00 0.00 0.75 0.35 13.25 49.39 11.07 52.81 71.52 64.24 36.38 8.61 0.01 0.00 0.00 0.00 0.00 0.00 0.00 0	49.42 61.00 73.03 58.30 19.59 0.23 0.61 0.00 0.00 0.00 0.00 0.00 0.00 0.00	84.51 84.51 84.51 85.27 98.87 127.81 165.06 214.45 225.52 278.33 349.94 414.08 450.26 450.27 450.28	208.25 233.98 339.17 1339.58 339.17 1339.58 339.17 135
84+25 84+50 84+75 85+00 85+03.05 85+03.05 85+53.05 86+04.05 86+04.79 86+25 86+30 86+50 87+20 87+25 87+70 88+25 88+	0.00 0.00 0.00 0.00 1.62 4.63 28.38 46.61 61.35 75.33 78.85 65.33 78.85 69.33 18.78 0.03 0.00 0.00 0.00 0.00 0.00 0.00 0.0	60.30 72.07 86.60 40.12 2.63 1.48 0.00 0.00 0.00 0.00 0.00 0.00 0.00 0	0.00 0.00 0.00 0.75 0.75 13.25 13.25 149.39 11.07 77.52 64.24 36.38 8.81 0.01 0.00 0.00 0.00 0.19 1.91 8.48 29.39 33.90 5.97 34.97 35.97	49.42 61.00 73.03 58.30 19.59 0.23 0.61 0.00 0	8-6.51 8-6.51 8-6.51 85.27 85.27 85.62 96.87 127.81 155.06 214.45 225.52 278.33 349.84 414.06 450.46 459.27 459.28 459.28 459.28 459.28 459.28 669.33 669.33 669.33 669.33	208.25 28 339.38 38 38 39.17 339.17 39.40 360.01 360.01 360.01
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844 25 45 47 5 45 47 5 47 5 47 5 47 5 47	0.00 0.00 0.00 0.00 1.02 4.63 3.462 46.61 65.33 18.78 85.33 0.00 0.00 0.00 0.00 0.00 0.00 0.00	60.30 60.30 60.60 60	0.00 0.00 0.00 0.75 0.35 13.25 28.94 37.25 28.94 37.25 64.24 36.38 64.24 36.38 0.00 0.00 0.00 0.19 1.91 8.48 9.33,90 0.00 0.19 1.91 3.93 3.3,90 0.00 0.19 1.91 3.93 3.94 3.94 3.94 3.95 3.95 3.95 3.95 3.95 3.95 3.95 3.95	49.42 61.00 73.01 18.59 0.23 0.21 0.00 0.00 0.00 0.00 0.00 0.00 0.00	8-6.51 8-6.51 8-6.51 85.27 85.82 96.87 127.61 165.06 2114.45 220.53 348.64 414.08 450.27 450.28 450.28 450.28 450.27	208.25 2 261.28 2 339.58 3 339.17 2 339.59 4 360.01 3 360.01 3 360
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84+25 84+50 84+75 84+50 84+75	0.00 0.00 0.00 0.00 0.00 0.00 0.00 0.0	69.30 69.30 88.60 40.12 2.63 1.46 1.00 0.	0.00 0.00 0.00 0.75 0.35 13.25 28.94 37.25 28.94 37.25 64.24 36.38 64.24 36.38 0.00 0.00 0.00 0.19 1.91 8.49 29.39 33.39 0.00 0.19 1.91 3.40 0.00 0.19 1.91 3.40 0.19 0.19 0.19 0.19 0.19 0.19 0.19 0.1	49.42 61.00 73.01 18.59 0.23 0.21 0.00 0.00 0.00 0.00 0.00 0.00 0.00	8-6.51 8-6.51 8-6.51 85.27 85.82 96.87 127.61 165.06 214.45 225.33 346.27 414.08 450.28 450.28 450.28 450.28 450.28 450.28 450.28 450.28 660.33 660.33 660.33 660.33 660.33 660.33 720.18 720.18 720.93	208.25 25 281.28 25 339.58 359.17 25 359.40
844 50 84	0.00 0.00 0.00 0.00 1.62 2.8.38 3.4.62 46.61 65.33 18.78 85.39 18.78 18.78 18.72 18.93 18.72 19.94 19.	69.30 69.30 88.60 2.63 2.6	0.00 0.00 0.00 0.75 0.75 0.35 13,25 49,39 11.07 77.52 64,24 36,38 8.38 0.01 0.00 0.00 0.00 0.19 1.91 8.48 29,39 31,04 29,39 10,07 10,00 10	49.42 19.00 19.59 19.59 10.00 10	851 851 851 85.27 85.27 85.26 98.87 127.81 165.06 214.45 225.52 278.33 349.84 414.06 450.46 450.28 459.27 459.28 459.28 459.28 459.28 669.33 669.33 669.33 669.33 681.63 7709.08 720.18	208.22 28 208.22 28 339.35 38 339.17 339.58 339.17 38 340.01 38 340.01 38 340.01 38 340.01 38 340.01 38 350.01
844 52 844 50 84	0.00 0.00 0.00 0.00 0.00 0.00 0.00 0.0	69.30 69.30 88.60 40.12 2.45 1.46 1.02 0.00 0.00 0.00 0.00 0.00 0.00 0.00 0.00 1.	0.00 0.00 0.00 0.75 0.35 13.25 13.25 49.39 11.07 77.52 64.24 36.38 8.61 0.01 0.00 0.00 0.19 1.91 8.48 29.39 33.90 5.97 1.10 1.91 1.91 1.91 1.91 1.93 1.93 1.93 1.93	49.42 1.00 1.00 1.00 1.00 1.00 1.00 1.00 1.0	851 851 851 85.27 85.27 85.27 85.62 96.87 127.81 165.06 214.45 225.52 278.33 349.84 414.08 450.46 450.28 459.28 459.28 459.28 459.28 660.67 660.67 660.67 660.67 660.14 660.33 660.33 660.33 660.33 709.08	208.22 25 251.28
844-25 844-50 85-4	0.00 0.00 0.00 0.00 0.00 1.62 4.53 4.53 1.62 4.66,61 1.75 1.87 1.87 1.87 1.87 1.87 1.87 1.87 1.87	69.30 69.30 88.60 2.65 2.6	0.00 0.00 0.00 0.75 0.35 13.25 28.94 17.25 28.94 11.07 71.52 64.24 36.38 64.24 36.38 0.00 0.00 0.00 0.19 1.91 8.49 29.39 19.34 3.39 19.34 3.39 19.34 3.40 0.00 0.00 0.00 0.00 0.00 0.00 0	49.42 1.00 1.00 1.00 1.00 1.00 1.00 1.00 1.0	8-6.51 8-6.51 8-6.51 8-6.51 85.27 85.82 96.87 127.61 165.06 214.45 225.52 276.33 348.62 414.09 450.27 450.28 450.28 450.28 450.28 450.28 450.28 450.28 450.28 60.87 460.87 60	208.22 92 281.28
844-25 844-50 85-4	0.00 0.00 0.00 0.00 0.00 0.00 0.00 0.0	69.30 69.30 88.60 40.12 2.45 1.46 1.02 0.00 0.00 0.00 0.00 0.00 0.00 0.00 0.00 1.	0.00 0.00 0.00 0.00 0.75 0.35 13,25 49,39 11,07 92,81 77,52 64,24 36,38 8,38 0.01 0.00 0.00 0.00 0.00 0.00 0.00 0.19 1.91 3,59 3,59 3,59 3,59 3,59 3,59 3,59 3,59	49.42 19.10 19.59 19.59 19.59 19.59 19.59 19.59 19.59 19.59 19.50	851 851 851 85.27 85.27 85.22 98.67 127.81 155.06 214.45 225.52 278.33 349.84 415.08 450.46 450.27 459.28 459.28 459.28 459.28 459.28 459.28 655.74 665.74 665.74 665.74 665.74 665.74 665.74 679.77 779.81 810.82 81	208.22 92 281.28
84+25 84+50 85+00 85+00 85+00 85+00 85+00 85+00 85+00 85+00 85+00 86+00 85+00 86+00	0.00 0.00 0.00 0.00 0.00 0.00 0.00 0.0	69.30 69.30 88.60 40.12 2.63 1.44 0.20 0.20 0.00 0.	0.00 0.00 0.00 0.75 0.35 13.25 28.94 37.25 28.94 11.07 12.21 171.52 64.24 36.38 64.24 36.38 0.00 0.00 0.00 0.19 1.91 29.39 33.90 29.38 19.34 3.49 0.00 0.00 0.19 1.91 1.91 1.91 1.91 1.9	49.42 49.12	8-6.51 8-6.51 8-6.51 8-6.51 85.27 85.82 96.87 127.61 165.06 214.45 225.33 348.24 414.08 450.27 450.28 450.28 450.28 450.28 450.28 450.28 450.28 450.28 60.87 450.28 60.87 60.	208.22 92 281.28
84+25 84+50 85+50	0.00 0.00 0.00 0.00 1.62 4.53 3.4.02 4.66 ft. 61.35 65.33 18.78 85.33 18.78 18.73 18.75 18	69.30 69.30 88.60 2.65 2.6	0.00 0.00 0.00 0.00 0.75 0.35 13,25 49,39 11,07 92,81 77,52 64,24 36,38 8,38 0.01 0.00 0.00 0.00 0.00 0.00 0.00 0.19 1.91 3,59 3,59 3,59 3,59 3,59 3,59 3,59 3,59	49.42 19.10 19.59 19.59 19.59 19.59 19.59 19.59 19.59 19.59 19.50	851 851 851 85.27 85.27 85.22 98.67 127.81 155.06 214.45 225.52 278.33 349.84 415.08 450.46 450.27 459.28 459.28 459.28 459.28 459.28 459.28 655.74 665.74 665.74 665.74 665.74 665.74 665.74 679.77 779.81 810.82 81	288,222 281 281,282 281 281,2
844-25 844-50 85-403-6 85-403-	0.00 0.00 0.00 0.00 0.00 0.00 0.00 0.0	69.30 69.30 88.60 40.12 2.63 1.44 0.20 0.20 0.00 0.	0.00 0.00 0.00 0.75 0.35 13,25 28,94 49,39 11,07 92,91 71,52 64,24 36,38 8,38 0.01 0.00 0.00 0.00 0.00 0.00 0.19 1.91 33,39 33,90 33,90 34,90 35,97 31,04 29,39 31,04 29,39 31,04 31	49.42 49.12 49.12 49.12 49.12 49.12 49.11 49.12 49.11 49.12 49.11 49.12 49.12 49.11 49.12	851 851 851 851 851 851 851 852 98.67 127.81 155.06 214.45 225.52 278.33 349.84 415.08 450.46 450.46 450.47 459.27 459.28 459.28 459.28 459.37 459.28 665.74 665.13 665.33 666.33 769.08 720.18 772.93 779.91	208.22 98 281.2

	Squar	EAS o Foot	Cubic	UMIS Yards	CUMULATIV	Yard
STATION	CUT	FILL	cur	FILL	CUT	P
100+00	16.77	2.55	4.16	2.10	4.16	2.
100+10	6.84	7.70				
100+20	2.48	14.07	1.62	4.44	5.79	6.
100+30	0.96	18.15	0.59	6.51	6.38	13
100+40	0.13	20.25	0.19	7.66	6.57	20
100+50	0.39	15.32	0.09	7.09	6.66	27
100+60	2.96	6.54	0.58	4.41	7.24	32
100+70	9.33	1.77	2.18	1.69	9.42	33
100+80	19.63	0.09	5.24	0.38	14.66	34
100+90	30.53	0.00	9.14	0.02	23.80	34
101+00	39.19	0.00	12.74	0.00	36.54	34
101+10	45.35	0.00	15.48	0.00	52.02	34
101+10	47.24	0.00	16.94	0.00	68.96	34
101+30	41.08		16.13	0.00	85.09	34
101+36,53	32.93	0.00	11.53	0.00	96.62	34
101+40			1.77	0.00	98.40	34
101+40	30.88	0.00	9.52	0.00	107.92	34
	20.10 14.05	0.00	6.32	0.00	114.24	34
101+60		0.00	3.75	0.16	117.99	34
101+70	6.27	0.86	1.71	0.70	119.70	35
101+80	3.04	2.92	0.53	0.62	120.23	35
101+85.19	2.48	3.87	0.37	0.98	120.61	36
101+90	1.81	6.27	0.47	3.68	121.08	40
102+00	0.87	11.50	0.23	4.69	121.31	45
102+10	0.51	11.14	0.17	3.94	121.48	49
102+20	0.49	8.28	0.01	0.18	121.49	49
102+21.12	0.00	0.00	0.00	0.00	121.49	49
	7.0	F49		mares	CUMULATTY	
	Squar	e Feet	Cubic	Yards	Cubic	Yard
STATION	CUT	FILL	CUT	FILL.	CUT	Tara
200+00	13.96	0.01				
200+00	8.28	0.01	10.30	0.40	10.30	0.
200+25	12.79	0.43	9.76	0.60	20.05	1.
200+50	12.79	0.43	11.48	0.42	31.53	1.
200+75	12.00	0.48	11.36	0.37	42.90	1.
	12.54	0.32	11.38	0.31	54.28	2.
201+25		0.34	13.72	0.16	68.00	2
201+50	17.58		15.34	0.00	83.34	2
201+68.07	28.25	0.00	7.87	0.00	91.21	2
			5.87	0.00	97.08	2
201+79.46	38.18	0.00	27.49	0.00	124.57	2
202+00	34.09	0.00	4.07	0.00	128.64	2
202+03.39	30.75	0.00	7.00	0.00	135.64	2
202+10.22	24.91	0.00	10.52	0.00	146.16	2
202+24.64	14.50	0.01	0.00	0.00	146.16	2
	1P	EAS	unr	UMES	CUMULATTY	E VO
	Squar	e Feet	Cubic	Yards	Cubic	Yard
STATION	CUT	FILL	CUT	FILL	cur	7
300+00	2.68	2.91				
300+25	6.15	0.94	4.05	1.85	4.05	1
300+50	37.35	0.00	20.60	0.43	24.66	2.
300+72.09	16.86	0.10	22.76	0.04	47.42	2
300+75	15.52	0.20	1.74	0.02	49.16	2
300+82.50		0.45	4.15	0.09	53.31	2
			0.00	0.00	53.31	_ 2
				UMES	CUMULATTY	
		EAS	NOL			
CT4TON	AR Squar	e Feet	VOL. Cubic	Yards	Cubic	Yard
STATION	AR Squar CUT	e Foet FILL		Yards FILL	Cubic	Yard F
400+00	Squar CUT 13.99	Fiet FILL 0.03	Cubic CUT	FILL	CUT	P
400+00 400+25	Squar CUT 13.99 15.54	FILL 0.03 0.00	Cubic CUT 13.67	PILL 0.02	13.67	0
400+00 400+25 400+50	Squar CUT 13.99 15.54 14.65	9 Feet FILL 0.03 0.00 0.04	Cubic CUT 13.67 13.98	0.02 0.02	13.67 27.65	0
400+00 400+25 400+50 400+75	Squar CUT 13.99 15.54 14.65 14.32	9 Feet FILL 0.03 0.00 0.04 0.02	Cubic CUT 13.67 13.98 13.41	FILL 0.02 0.02 0.03	13.67 27.65 41.07	0 0
400+00 400+25 400+50 400+75 400+81.03	Squar CUT 13.99 15.54 14.65 14.32	9 Feet FILL 0.03 0.00 0.04 0.02 0.11	Cubic CUT 13.67 13.98 13.41 3.20	0.02 0.02 0.03 0.03	27.65 41.07 44.26	0 0
400+00 400+25 400+50 400+75 400+81.03	Squar CUT 13.99 15.54 14.65 14.32 14.33 9.55	FILL 0.03 0.00 0.04 0.02 0.11 0.48	Cubic CUT 13.67 13.98 13.41 3.20 8.25	0.02 0.02 0.03 0.01 0.25	27.65 41.07 44.26 52.51	0 0
400+00 400+25 400+50 400+75 400+81.03 401+00 401+17.73	Squar CUT 13.99 15.54 14.65 14.32 14.33 9.55 10.54	Feet FILL 0.03 0.00 0.04 0.02 0.11 0.48 0.29	Cubic CUT 13.67 13.98 13.41 3.20 8.25 6.49	0.02 0.02 0.03 0.01 0.25 0.30	CUT 13.67 27.65 41.07 44.26 52.51 59.00	0 0 0 0 0
400+00 400+25 400+50 400+75 400+81.03 401+00	Squar CUT 13.99 15.54 14.65 14.32 14.33 9.55 10.54	FILL 0.03 0.00 0.04 0.02 0.11 0.48	Cubic CUT 13.67 13.98 13.41 3.20 8.25 6.49 2.84	97LL 0.02 0.02 0.03 0.01 0.25 0.30 0.08	CUT 13.67 27.65 41.07 44.26 52.51 59.00 61.84	0 0 0 0 0
400+00 400+25 400+50 400+75 400+81.03 401+00 401+17.73 401+25	Squar CUT 13.99 15.54 14.65 14.32 14.33 9.55	Feet FILL 0.03 0.00 0.04 0.02 0.11 0.48 0.29	Cubic CUT 13.67 13.98 13.41 3.20 8.25 6.49 2.84 2.41	PILL 0.02 0.02 0.03 0.01 0.25 0.30 0.08 0.03	CUT 13.67 27.65 41.07 44.26 52.51 59.00 61.84 64.26	0 0 0 0 0 0
400+00 400+25 400+50 400+75 400+81.03 401+00 401+17.73 401+25	Squar CUT 13.99 15.54 14.65 14.32 14.33 9.55 10.54 10.54	0.03 0.00 0.04 0.02 0.11 0.48 0.29 0.29	Cubic CUT 13.67 13.98 13.41 3.20 8.25 6.49 2.84 2.41 0.00	PILL 0.02 0.02 0.03 0.01 0.25 0.30 0.08 0.03 0.00	CUT 13.67 27.65 41.07 44.26 52.51 59.00 61.84 64.26 64.26	0 0 0 0 0 0
400+00 400+25 400+50 400+75 400+81.03 401+00 401+17.73 401+25 401+30.22	AR Squar CUT 13.99 15.54 14.65 14.32 14.33 9.55 10.54 10.54 14.40	e Feet FILL 0.03 0.00 0.04 0.02 0.11 0.48 0.29 0.29 0.00	Cubio CUT 13.67 13.98 13.41 3.20 8.25 6.49 2.84 2.41 0.00	PILL 0.02 0.02 0.03 0.01 0.25 0.30 0.08 0.03 0.00	CUT 13.67 27.65 41.07 44.26 52.51 59.00 61.84 64.26 64.26 CUMULATIV	0 0 0 0 0 0 0
400+00 400+25 400+50 400+75 400+81.03 401+00 401+17.73	AR Squar CUT 13.99 15.54 14.65 14.32 14.33 9.55 10.54 10.54 14.40	e Feet FILL 0.03 0.00 0.04 0.02 0.11 0.48 0.29 0.29 0.00 EAS	Cubio CUT 13.67 13.98 13.41 3.20 8.25 6.49 2.84 2.41 0.00 VOL Cubic	PILL 0.02 0.02 0.03 0.01 0.25 0.30 0.08 0.03 0.00 UMES Yards	CUT 13.67 27.65 41.07 44.28 52.51 59.00 81.84 64.26 64.26 CUMULATIV. Cubic	0 0 0 0 0 0 0 0 0 7
400+00 400+25 400+50 400+75 400+81.03 401+00 401+17.73 401+25 401+30.22	AR Squar CUT 13.99 15.54 14.65 14.32 14.33 9.55 10.54 10.54 14.40 AR Squar CUT	e Feet FILL 0.03 0.00 0.04 0.02 0.11 0.48 0.29 0.29 0.00 EAS e Feet FILL	Cubic CUT 13.67 13.98 13.41 3.20 8.25 6.49 2.84 2.41 0.00 VOL Cubic CUT	#ILL 0.02 0.02 0.03 0.01 0.25 0.30 0.08 0.08 0.03 0.00 UMES Yards	CUT 13.67 27.65 41.07 44.26 52.51 59.00 61.84 64.26 CUMULATIV Cubic CUT	0 0 0 0 0 0 0 0 0 7 8 7
400+00 400+25 400+50 400+75 400+81.03 401+00 401+17.73 401+25 401+30.22 STATION 500+00	AR Squar CUT 13.99 15.54 14.65 14.32 14.33 9.55 10.54 10.54 14.40 AR Squar CUT 8.08	e Feet FIL 0.03 0.00 0.04 0.02 0.11 0.48 0.29 0.29 0.00 ELS e Feet FIL 0.06	Cubio CUT 13.67 13.98 13.41 3.20 8.25 6.49 2.84 2.41 0.00 VOL Cubic	PILL 0.02 0.02 0.03 0.01 0.25 0.30 0.08 0.03 0.00 UMES Yards	CUT 13.67 27.65 41.07 44.28 52.51 59.00 81.84 64.26 64.26 CUMULATIV. Cubic	0 0 0 0 0 0 0 0 0 7
400+00 400+25 400+55 400+75 400+81.03 401+00 401+17.73 401+25 401+30.22 <i>STATTON</i> 500+00 500+07.17	AR Squar CUT 13.99 15.54 14.65 14.32 9.55 10.54 10.54 14.40 CUT 8.06 6.50 8.50	e Feet FILL 0.03 0.00 0.04 0.02 0.11 0.48 0.29 0.29 0.00 EAS e Feet FILL 0.06 0.01	Cubic CUT 13.67 13.98 13.41 3.20 8.25 6.49 2.84 2.41 0.00 VOL Cubic CUT	#ILL 0.02 0.02 0.03 0.01 0.25 0.30 0.08 0.08 0.03 0.00 UMES Yards	CUT 13.67 27.65 41.07 44.26 52.51 59.00 61.84 64.26 CUMULATIV Cubic CUT	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0
400+00 400+25 400+50 400+75 400+81.03 401+00 401+17.73 401+25 401+30.22 STATTON 500+00 500+07.17 500+22.88	AR Squar CUT 13.99 15.54 14.65 14.32 14.33 10.54 10.54 14.40 AR Squar CUT 8.08 8.50 10.52	e Feet FILL 0.03 0.00 0.04 0.02 0.11 0.48 0.29 0.29 0.00 EAS e Feet FILL 0.08 0.01	Cubic CUT 13.67 13.98 13.41 3.20 8.25 6.49 2.84 2.41 0.00 VDL Cubic CUT 2.20 5.69	### F### 0.02 0.03 0.01 0.05	CUT 13.67 27.65 41.07 44.26 52.51 59.00 61.84 64.26 64.26 CUMULATIV Cubic CUT 2.20 7.89	0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0
400+00 400+25 400+50 400+75 400+81.03 401+02 401+02 401+17.73 401+25 401+30.22 STATTON 500+00 500+07.17 500+22.88 500+22.88	ARE Squar CUT 13.99 15.54 14.65 14.32 9.55 10.54 10.54 10.54 10.54 10.54 10.55	e Feet FILL 0.03 0.00 0.04 0.02 0.11 0.48 0.29 0.29 0.00 ELS FELL 0.06 0.01 0.00	Cubic CUT 13.67 13.98 13.41 3.20 8.25 6.49 2.84 2.41 0.00 VOL Cubic CUT	######################################	CUT 13.67 27.65 41.07 44.26 52.51 59.00 61.84 64.26 64.26 CUMULATY Cubic CUT 2.20	00 00 00 00 00 00 00 00 00 00 00 00 00
400+00 400+25 400+50 400+75 400+81.03 401+00 401+17.73 401+25 401+30.22 STATTON 500+00 500+07.17 500+22.88 500+38.92 500+43.65	AR Squar CUT 13.99 15.54 14.65 14.32 14.33 9.55 10.54 10.54 14.40 AR Squar CUT 8.05 6.50 10.52 7.78 8.67	e Feet FILL 0.00 0.00 0.04 0.02 0.11 0.48 0.29 0.00 ELS e Feet FILL 0.06 0.01 0.00 0.00 0.00	Cubic CUT 13.67 13.98 13.41 3.20 8.25 6.49 2.84 2.41 0.00 VOL Cubic CUT 2.20 5.69 5.77 1.16	0.02 0.02 0.03 0.01 0.25 0.30 0.08 0.03 0.00 0.00 0.00 0.00 0.00	CUT 13.67 27.65 41.07 44.26 52.51 59.00 61.84 64.26 CUMULATIV. Cubic CUT 2.20 7.89 13.66	00 00 00 00 00 00 00 00 00 00 00 00 00
400+00 400+25 400+50 400+75 400+810,3 401+00 401+17,73 401+25 401+30,22 STATTON 500+00 500+07,17 500+22,88 500+30,5 500+30,5 500+50	AR Squar CUT 10.52 10.52 10.52 7.78 8.87 12.99	e Feet FILL 0.03 0.00 0.04 0.02 0.11 0.48 0.29 0.29 0.00 EAS FELL 0.06 0.01 0.00 0.00 0.00	Cubic CUT 13.69 13.41 3.20 8.25 6.49 2.84 2.41 0.00 VOL Cubic CUT 1.69 5.69 5.77 1.16	0.02 0.02 0.03 0.01 0.25 0.30 0.00 0.03 0.00 0.00 0.00 0.00 0.0	CUT 13.67 27.65 41.07 44.26 52.51 59.00 61.84 64.26 64.26 CUMULATY Cubic CUT 7.89 13.66 14.82	P 0 0 0 0 0 0 0 0 0
400+00 400+25 400+50 400+75 400+81.03 401+00 401+17.73 401+25 401+30.22 ***********************************	AR Squar CUT 13.99 15.54 14.52 14.32 10.54 10.54 10.54 14.40 AR Squar CUT 8.05 8.50 10.52 7.78 8.67 12.99 13.56	e Feet FILL 0.03 0.00 0.04 0.02 0.11 0.48 0.29 0.00 0.00 EAS e Feet FILL 0.06 0.01 0.00 0.02 0.00 0.00	Cubic CUT 13.67 13.98 13.41 3.20 8.25 6.49 2.84 0.00 WOL Cubic CUT 2.20 5.69 5.77 1.16 2.57	FILL 0.02 0.02 0.02 0.03 0.01 0.05	CUT 13.67 27.65 41.07 44.26 52.51 55.00 61.84 64.26 CUMULATIV Cubic CUT 2.20 7.89 13.66 14.82 17.39 18.51	# 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0
400+00 400+25 400+50 400+75 400+810,3 401+00 401+17,73 401+25 401+30,22 STATTON 500+00 500+07,17 500+22,88 500+30,5 500+30,5 500+50	AR Squar CUT 10.52 10.52 10.52 7.78 8.87 12.99	e Feet FILL 0.03 0.00 0.04 0.02 0.11 0.48 0.29 0.29 0.00 EAS FELL 0.06 0.01 0.00 0.00 0.00	Cubic CUT 13.69 13.41 3.20 8.25 6.49 2.84 2.41 0.00 WDL Cubic CUT 2.20 5.69 5.77 1.16 2.57	PILL 0.02 0.02 0.02 0.03 0.01 0.25 0.30 0.03 0.00 0.00 0.00 0.00 0.00 0.0	CUT 13.67 27.65 41.07 44.26 52.51 59.00 61.84 64.26 64.26 CUMULATY Cubic CUT 2.20 7.89 13.66 14.82 17.39 18.51	
400+00 400+25 400+50 400+75 400+81.03 401+00 401+17.73 401+25 401+30.22 ***********************************	AR Squar CUT 13.99 15.54 14.52 14.32 10.54 10.54 10.54 14.40 AR Squar CUT 8.05 8.50 10.52 7.78 8.67 12.99 13.56	e Feet FILL 0.03 0.00 0.04 0.02 0.11 0.48 0.29 0.00 0.00 EAS e Feet FILL 0.06 0.01 0.00 0.02 0.00 0.00	Cubic CUT 13.67 13.98 13.41 3.20 8.25 6.49 2.41 0.00 CUT 2.20 VUL Cubic CUT 2.20 1.16 2.569 5.77 1.16 2.57 1.12 1.74 20.24	0.02 0.02 0.03 0.01 0.25 0.03 0.06 0.03 0.00 0.00 0.00 0.00 0.00	CUT 13.67 12.7.85 41.07 44.26 52.91 59.00 61.84 64.26 64.26 CUMULATIV. Cubic CUT 2.20 7.89 13.66 14.82 17.39 18.51 20.26	
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400+00 400+25 400+35 400+75 400+75 401+10 401+10 401+10 401+10 401+10 401+10 500+00 50	Square 1.15.54 14.40 Square 1.15.54 14.52 10.54 14.40 Square 1.15.54 14.40 Square 1.15.54 15.54 15.55	**Pert** **Part** **Part* **Part** **Part* **Part** **Part** **Part** **Part** **Part** **Part** **Part* **Part** **Part** **Part** **Part* **P	Cubic CUT 13.67 13.69 13.67 13.69 14.62 14	PRL CO CO CO CO CO CO CO C	CUT 13.67 27.85 41.07 27.85 44.26 42.26 52.51 59.00 61.84 64.26 64.26 64.26 64.26 13.66 13.86 14.82 17.39 18.51 12.026 40.50 62.85 75.71 86.39 86.82 87.61 103.01 103.01 103.01 103.01 103.01 103.01 103.01 103.01 103.01 103.01 103.01 103.01	## 100 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0 0
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*EXISTING TOPO AND EARTHWORK VOLUME AT THESE STATIONS IS APPROXIMATE AND SUBJECT TO CHANGE. CONTRACTOR TO CORDINATE WITH CARY PARK DISTRICT PRIOR TO STARTING WORK.





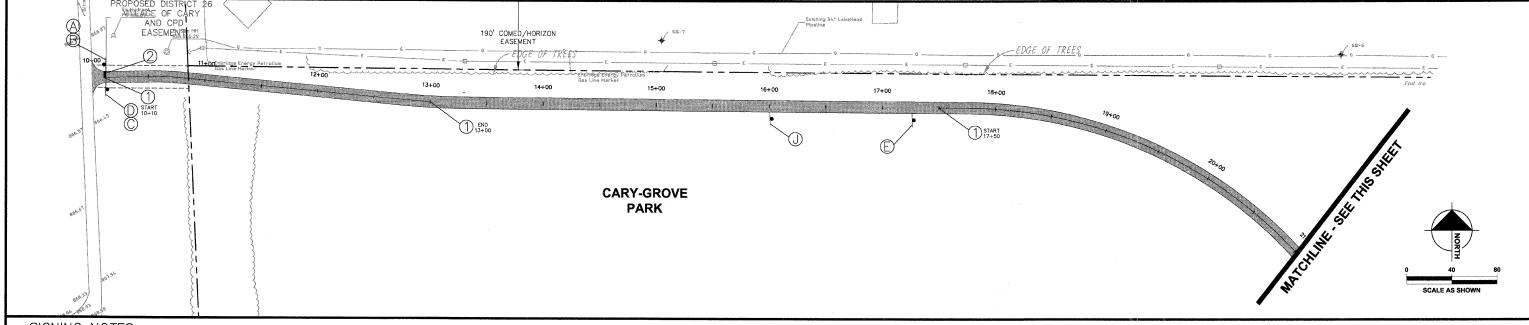
DESIGNED	JWH	REVISED
DRAWN	LES	REVISED
CHECKED	JMM	REMSED
DATE	10/02/2009	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CARY PARK DISTRICT
CARY COMMUNITY TRAIL

CROSS SECTIONS STA. 502+52.34 TO STA. 503+76.78

SECTION	COUNTY	TOTAL SHEETS	SHEE NO.	
06-P4001-00-BR	MCHENRY	64	48	
FED. DO 4 D DIOT.	11 11 11	CONTRAC	T NO 62222	



SIGNING NOTES:

- ALL PROPOSED SIGNING AND PAVEMENT MARKING SHALL BE IN ACCORDANCE WITH THE MOST CURRENT EDITION OF EACH OF THE FOLLOWING DOCUMENTS AND ANY REVISION THERETO:
 - MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES
 ILLINOIS DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE
 CONSTRUCTION
- PROPOSED SIGN LOCATIONS ARE APPROXIMATE AND SHALL BE MODIFIED IN THE FIELD TO AVOID
 CONFLICT WITH UNDERGROUND UTILITIES OR OTHER OBSTRUCTIONS AND TO COMPLY WITH THE
 STANDARDS REFERENCED IN NOTE 1 ABOVE. SIGNS SHALL BE INSTALLED ONLY AFTER APPROVAL OF
 FINAL LOCATIONS BY CARY PARK DISTRICT.
- 3. CROSSWALK LONGITUDINAL LINES TO BE 6" WIDE. CROSSWALK DIAGONAL LINES TO BE 12" WIDE WITH 2' SPACING BETWEEN LINES. STOP BARS SHALL BE PLACED A MINIMUM OF 4' BEHIND CROSSWALKS.
- SIGNS LISTED AND LABELED IN THE SIGN LEGEND ARE UNIQUE TO THE ALIGNMENTS SHOWN ON THIS SHEET.
- 5. PLACE SIGN A MINIMUM OF 3 FEET AND MAXIMUM OF 6 FEET FROM EDGE OF TRAIL.

SIGNING LEGEND

PLACE NEW SIGN DETECTABLE WARNINGS

SIGN LEGEND

SIGN LETTER	(A)	B	0	D	E	(F)	\oplus		J
SIGN	STOP	CARY JUNIOR HS ADARE DRIVE	BIKE ROUTE	BEGIN	(YIELD	MINK TRAIL	MINK TRAIL ↔	CARY COMMUNITY TRAIL
NO.	R1-1	D1 — 1b	D11-1	M4-11	W1-2	R1-2	D1 — 1b	D1 — 1b	D1-1c
SIGN DIM.	18" x 18"	24" x 12" D4 letter size	24" x 18"	12" x 4"	18" x 18"	18" x 18" x 18"	24" x 6" D4 letter size	24" x 6"	24" x 18"

PAVEMENT MARKING LEGEND

- THERMOPLASTIC PAVEMENT MARKING, 4" (100 mm) WHITE



THERMOPLASTIC PAVEMENT MARKING, 24" (600 mm) WHITE

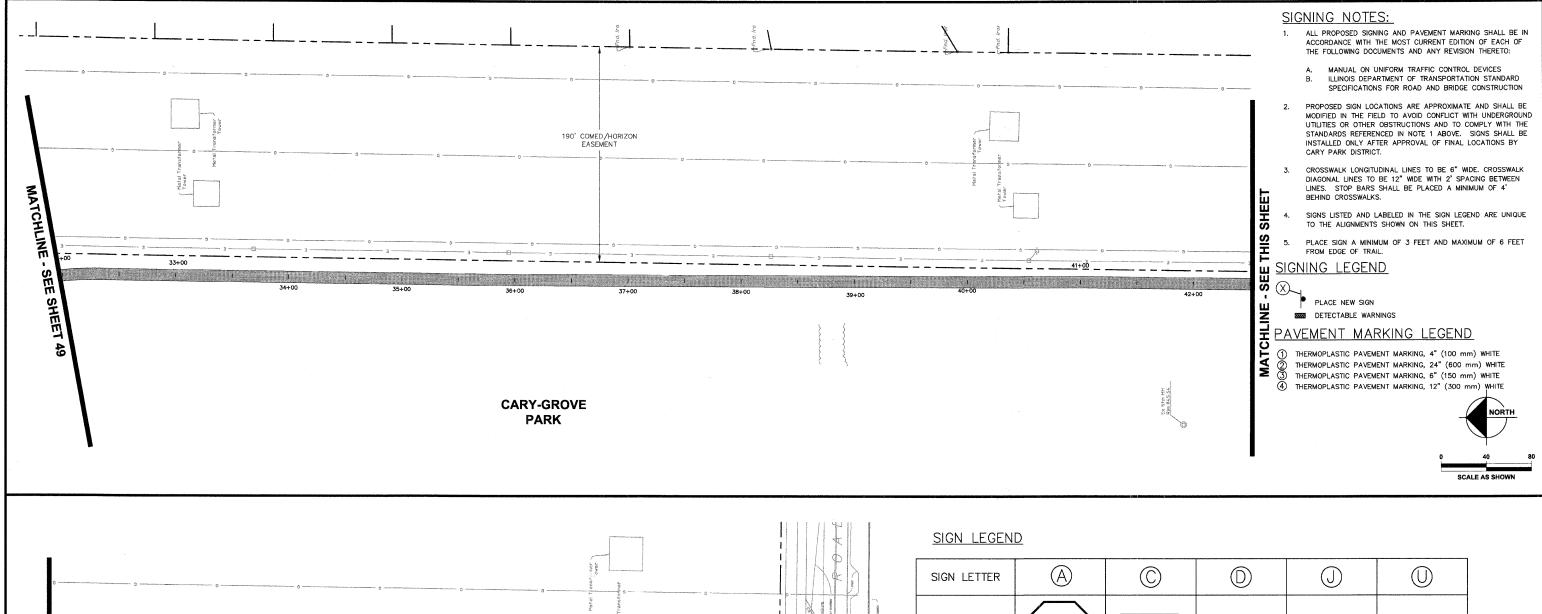
THERMOPLASTIC PAVEMENT MARKING, 6" (150 mm) WHITE
THERMOPLASTIC PAVEMENT MARKING, 12" (300 mm) WHITE COMFO CARY PARK DISTRICT CARY GROVE PARK CARY PARK DISTRICT, CARY GROVE PARK

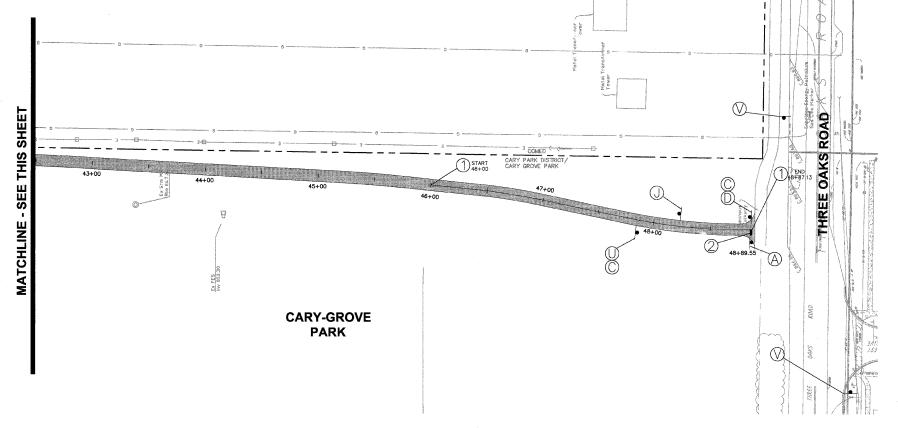
10	DESIGNED - JWH	REVISED
improper one K:\Q	DRAWN - LES	REVISED
and im	CHECKED — JMM	REVISED
Drong	DATE - 10/02/2009	REVISED

STATE OF ILLINOIS DEPARTMENT OF TRANSPORTATION

CARY PARK DISTRICT CARY COMMUNITY TRAIL SIGNAGE AND STRIPING PLAN STA. 10+00 TO STA. 21+00 STA. 21+00 TO 32+00

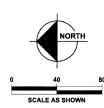
SHEET NO. SECTION COUNTY CONTRACT NO. 63333 FED. ROAD DIST. NO. 1 | ILLINOIS





SIGN LETTER	A	©	0	\bigcirc	0
SIGN	STOP	BIKE ROUTE	BEGIN	CARY COMMUNITY TRAIL	END
NO.	R1-1	D11-1	M4-11	D1-1c	M4-12
SIGN DIM.	18" x 18"	24" x 18"	12" x 4"	24" x 18"	12" x 4"

SIGN LETTER	\bigcirc
SIGN	TRUCKS ENTERING HIGHWAY
NO.	W21-I104
SIGN DIM.	18" x 18"





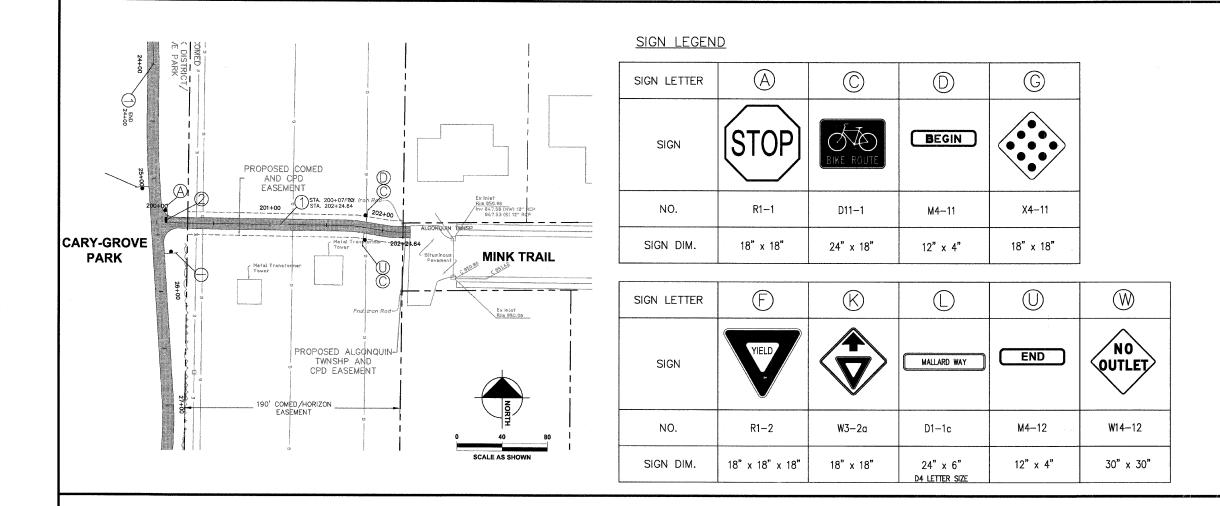
relici LDE	DESIGNED — JWH	REVISED
ment, iproper ne n:∖G	DRAWN - LES	REVISED
nd in	CHECKED — JMM	REVISED
This of o	DATE - 10/02/2009	REVISED

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

CARY PARK DISTRICT CARY COMMUNITY TRAIL

SIGNAGE AND STRIPING PLAN STA. 32+00 TO STA. 42+50 STA. 42+50 TO 48+89.55

SECTION	COUNTY		TOTAL SHEETS	SHEET NO.		
P4001-00-BR	мс	HENRY	64	50		
. ROAD DIST, NO. 1 ILLINOIS		CONTRACT	NO. 63333			



SIGNING NOTES:

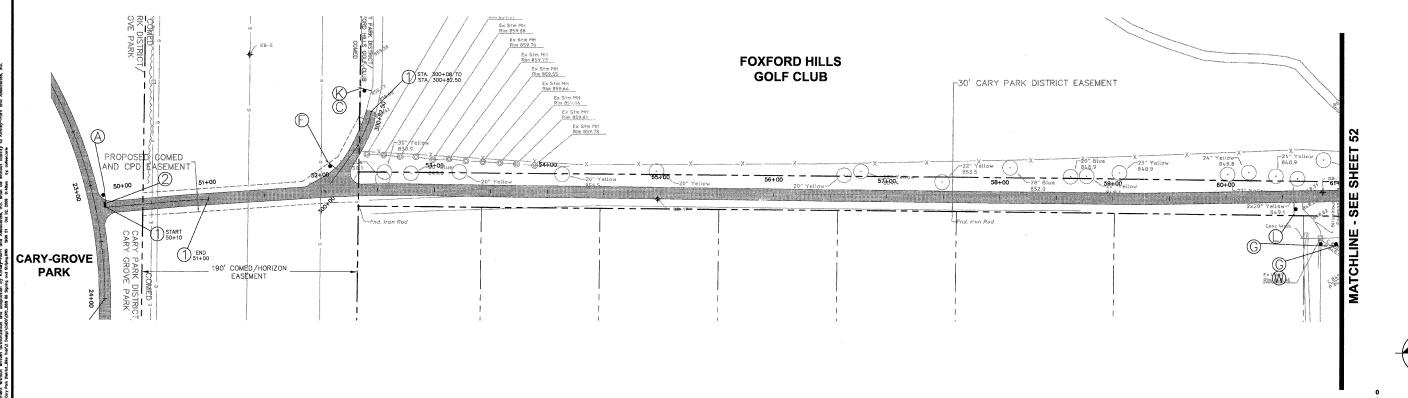
- ALL PROPOSED SIGNING AND PAVEMENT MARKING SHALL BE IN ACCORDANCE WITH THE MOST CURRENT EDITION OF EACH OF THE FOLLOWING DOCUMENTS AND ANY REVISION THERETO:
 - A. MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES
 B. ILLINOIS DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS
 FOR ROAD AND BRIDGE CONSTRUCTION
- PROPOSED SIGN LOCATIONS ARE APPROXIMATE AND SHALL BE MODIFIED IN THE FIELD TO AVOID CONFLICT WITH UNDERGROUND UTILITIES OR OTHER OBSTRUCTIONS AND TO COMPLY WITH THE STANDARDS REFERENCED IN NOTE 1 ABOVE. SIGNS SHALL BE INSTALLED ONLY AFTER APPROVAL OF FINAL LOCATIONS BY CARY BARK DISTRICT.
- CROSSWALK LONGITUDINAL LINES TO BE 6" WIDE. CROSSWALK DIAGONAL LINES
 TO BE 12" WIDE WITH 2' SPACING BETWEEN LINES. STOP BARS SHALL BE
 PLACED A MINIMUM OF 4' BEHIND CROSSWALKS.
- SIGNS LISTED AND LABELED IN THE SIGN LEGEND ARE UNIQUE TO THE ALIGNMENTS SHOWN ON THIS SHEET.
- PLACE SIGN A MINIMUM OF 3 FEET AND MAXIMUM OF 6 FEET FROM EDGE OF TRAIL.

SIGNING LEGEND

PLACE NEW SIGN
DETECTABLE WARNINGS

PAVEMENT MARKING LEGEND

- 1 THERMOPLASTIC PAVEMENT MARKING, 4" (100 mm) WHITE
- THERMOPLASTIC PAVEMENT MARKING, 24" (600 mm) WHITE
- THERMOPLASTIC PAVEMENT MARKING, 6" (150 mm) WHITE
- 4 THERMOPLASTIC PAVEMENT MARKING, 12" (300 mm) WHITE







 DESIGNED - JWH
 REVISED

 DRAWN - LES
 REVISED

 CHECKED - JMM
 REVISED

 DATE - 10/02/2009
 REVISED

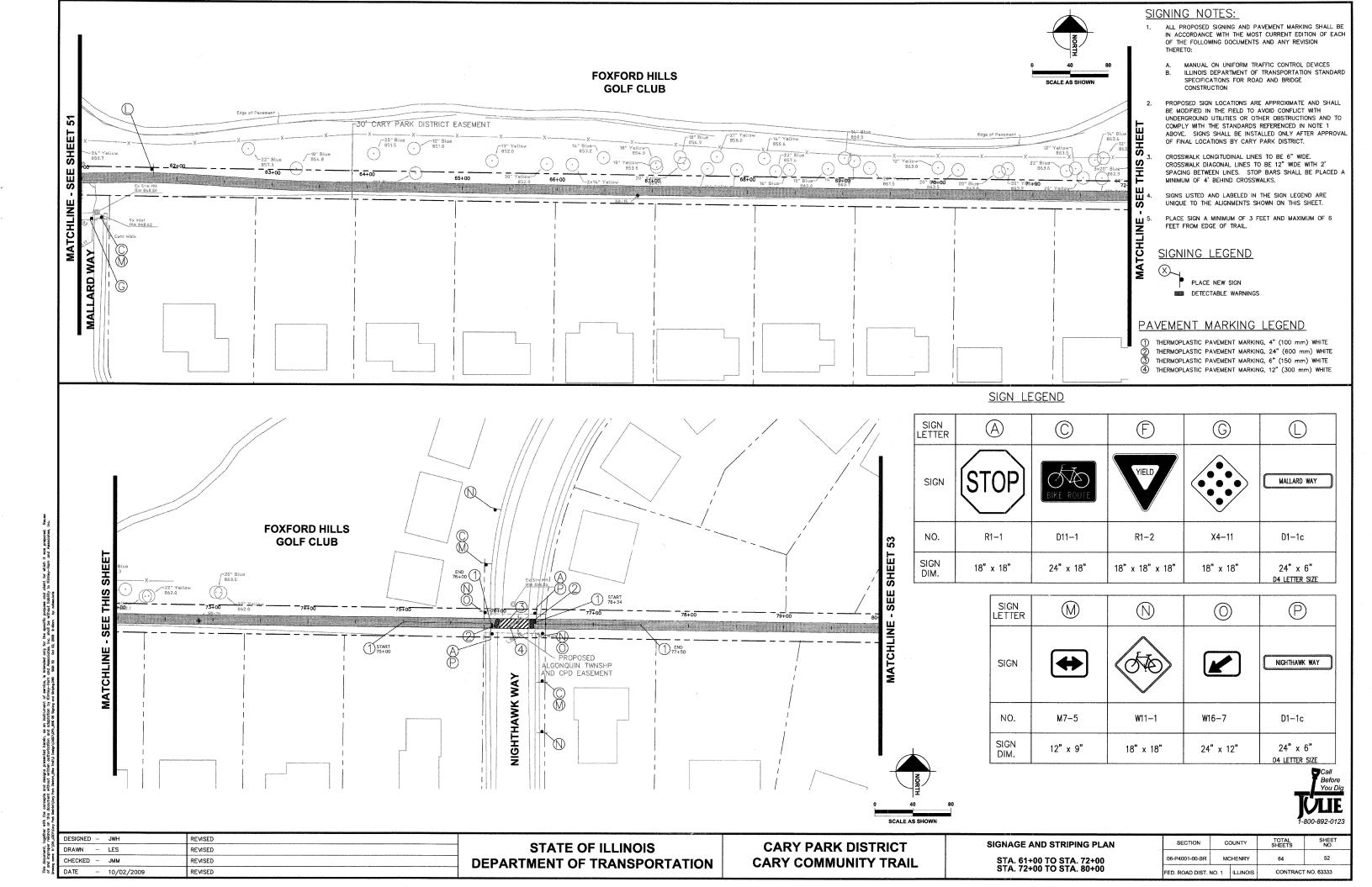
**DEPARTMENT OF TRANSPORTATION*

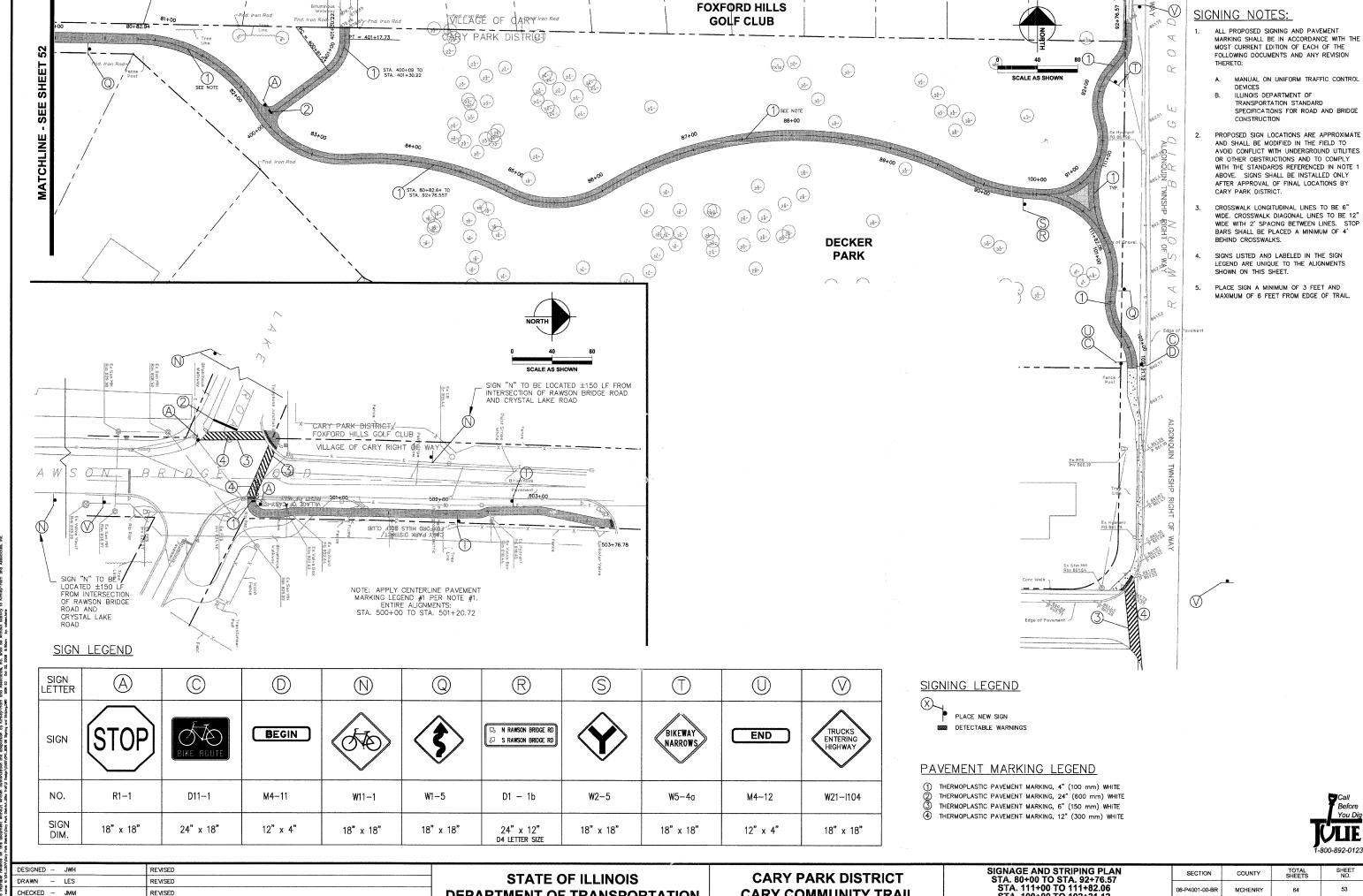
CARY PARK DISTRICT
CARY COMMUNITY TRAIL

SIGNAGE AND STRIPING PLAN STA. 200+00 TO STA. 202+24.64 STA. 300+00 TO STA. 300+82.50 STA. 50+00 TO STA. 61+00
 SECTION
 COUNTY
 TOTAL SHEET NO.

 06-P4001-00-BR
 MCHENRY
 64
 51

 FED. ROAD DIST. NO. 1
 ILLINOIS
 CONTRACT NO. 63333



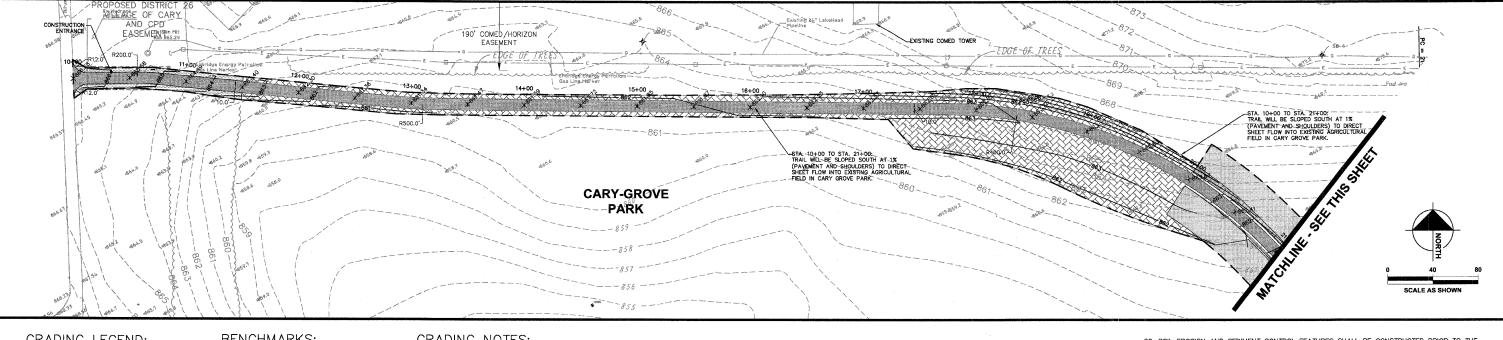


DEPARTMENT OF TRANSPORTATION

CARY COMMUNITY TRAIL

STA. 111+00 TO 111+82.06 STA. 100+00 TO 102+21.12 STA. 500+00 TO 503+76.78

CONTRACT NO. 63333



GRADING LEGEND:

PROPOSED SPOT ELEVATIONS

EXISTING CONTOUR

EXISTING STORM SEWER PIPE OUTLET/CULVERT

4" PERFORATED EDGE DRAIN

EDGE DRAIN CLEANOUT

IDOT CLASS 1 SEEDING*

IDOT CLASS 4A SEEDING EXCELSIOR/WOOD BLANKET

INLET PROTECTION

RIP RAF

5.8 TREE PROTECTION

---- PROPERTY LINE

--- LIMITS OF DISTURBANCE (8.96 ACRES) NOTE: STRAW MULCH STABILIZATION TO BE PLACED OVER SEEDING WHERE THERE IS NO EXCELSIOR BLANKET

* SEE SHEET 2 FOR SEEDING CHARTS

CHECKED - JMM

- 10/02/2009

REVISED

DATE

BENCHMARKS:

SOURCE BENCHMARK
DESCRIPTION: MANHOLE #46 FROM THE ASBUILT RECORD DRAWINGS BY MANHARD CONSULTING LTD DATED 3-6-07 FOR THE FOXFORD HILLS SUBDIVISION-PHASE 1 IN THE VILLAGE OF CARY, ILLINOIS.

LOCATION: 120' NORTHEAST OF THE OAKMAN DIVE AND PARKSTONE DRIVE INTERSECTION ON OAKMAN DRIVE. ELEVATION: 861.45

SITE BENCHMARK
HE CP#100
DESCRIPTION: IRON PIPE

LOCATION: 1000' EAST OF INTERSECTION OF 1ST AND THREE OAKS ROAD, 584' NORTH OF THE NORTH SIDE OF THREE OAKS ROAD, 31' NORTHEAST OF STORM MANHOLE, AND 26' NORTHWEST OF POWER POLE. ELEVATION: 854.94

GRADING NOTES:

- PRIOR TO INSTALLATION OF STORM DRAINS OR ANY OTHER UTILITIES, THE CONTRACTOR SHALL EXCAVATE, VERIFY, AND CALCULATE ALL POINTS OF CONNECTION AND ALL UTILITY CROSSINGS AND INFORM THE OWNER AND THE ENGINEER OF ANY CONFLICTS OR REQUIRED DEVATIONS FROM THE PLAN. NOTIFICATION SHALL BE MADE A MINIMUM OF 72 HOURS PRIOR TO CONSTRUCTION. THE ENGINEER AND ITS CLIENTS SHALL BE HELD HARMLESS IN THE EVENT THE CONTRACTOR FAILS TO MAKE SUCH NOTIFICATION.
- ALL UNPAVED SLOPES AND AREAS DISTURBED BY CONSTRUCTION SHALL BE GRADED SMOOTH AND FOUR INCHES OF TOPSOIL APPLIED. IF ADEQUATE TOPSOIL IS NOT AVAILABLE ON SITE, THE CONTRACTOR SHALL PROVIDE TOPSOIL, APPROVED BY THE OWNER, AS NEEDED. THE AREA SHALL THEN BE SEEDED/SODDED, FERTILIZED, MULCHED, WATERED AND MAINTAINED UNTIL HARDY GRASS GROWTH IS ESTABLISHED IN ALL AREAS. ANY RELOCATED TREES SHALL BE MAINTAINED UNTIL SUCH POINT AS TREE IS RE-ESTABLISHED. ANY AREAS DISTURBED FOR ANY REASON PRIOR TO FINAL ACCEPTANCE OF THE PROJECT SHALL BE CORRECTED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.
- THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES, AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. CONTRACTOR MUST CALL THE APPROPRIATE UTILITY COMPANY AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES.
- 4. CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE EXISTING GRADES SHOWN ON THESE PLANS ARE BASED OFF A TOPOGRAPHIC SURVEY BY HAEGER ENGINEERING DATED 02/07/2008, A TOPOGRAPHIC SURVEY AND FUTURE GRADING IMPROVEMENT PLAN PERFORMED BY HITCHCOCK ENGINEERING DATED 10/04/2004 AND A FOXFORD HILLS SUBDIVISION AS BUILT SURVEY BY MANHARD DATED 03/28/07.
- CONTRACTOR TO FIELD VERIFY ELEVATIONS OF PROPOSED DRAINAGE STRUCTURES AND ADJACENT PAVEMENT PRIOR TO STARTING CONSTRUCTION.
- 6. REFER TO GEOTECHNICAL REPORT FOR SPECIFIC SITE SOIL CONDITIONS AND CONSIDERATIONS.
- 7. CONTRACTOR SHALL COMPLY COMPLETELY WITH THE LATEST STANDARDS OF OSHA DIRECTIVES OR ANY OTHER AGENCY HAVING JURISDICTION FOR EXCAVATION AND TRENCHING PROCEDURES. THE CONTRACTOR SHALL "SES SUPPORT SYSTEMS, SLOPING, BENCHING, AND OTHER MEANS OF PROTECTION. THIS IS TO INCLUDE, BUT NOT LIMITED TO ACCESS AND ECRESS FROM ALL EXCAVATION AND TRENCHING. CONTRACTOR IS RESPONSIBLE FOR COMPLYING WITH PERFORMANCE CRITERIA AS REQUIRED BY OSHA.
- MULCH METHOD TO BE USED TO PROTECT ALL PERMANENT SEEDING WHEREVER THE EXCELSIOR BLANKET HAS NOT BEEN SPCIFIED.

DEPARTMENT OF TRANSPORTATION

- WHERE APPLICABLE, CONTRACTOR SHALL TAKE ALL APPROPRIATE MEASURES TO KEEP SEDIMENT FROM ENTERING ADJACENT RESIDENTIAL PROPERTIES.
- CONTRACTOR SHALL TAKE ALL APPROPRIATE MEASURES TO AVOID DISTURBING AREA OUTSIDE OF EXISTING CARY PARK DISTRICT PROPERTY, EASEMENTS, AND APPROVED CONSTRUCTION $% \left(1\right) =\left(1\right) \left(1\right)$ EASEMENTS WITH ADJACENT STAKE HOLDERS.
- 11. CONTRACTOR SHALL RESTORE ANY DISTURBED AREAS OUTSIDE OF CONSTRUCTION LIMITS TO PRE-EXISTING CONDITIONS AT NO ADDITIONAL COST TO THE OWNER.
- 12. ALL GRADING AND DRAINAGE IMPROVEMENTS WILL BE PERFORMED IN ACCORDANCE WITH MCHENRY COUNTY STORMWATER ORDINANCE AND IDOT STANDARD SPECIFICATIONS FOR ROAD AND
- 13. ALL EROSION AND SEDIMENT CONTROL MEASURES WILL BE IN ACCORDANCE WITH THE APPROVED SWPPP, MCHENRY COUNTY, AND THE MCHENRY COUNTY SOIL CONSERVATION DISTRICT.
- 14. DRAIN TILE SYSTEMS DISTURBED DURING DEVELOPMENT MUST BE RECONNECTED BY THOSE RESPONSIBLE FOR THEIR DISTURBANCE UNLESS THE APPROVED ENGINEERING PLANS INDICATE HOW THE DRAIN TILE SYSTEM IS TO BE CONNECTED TO THE PROPOSED STORMWATER MANAGEMENT SYSTEM. ALL ABANDONED DRAIN TILES SHALL BE REMOVED IN THEIR ENTIRETY.
- 15. PLEASE REFER TO STANDARDS AND DETAILS LOCATED ON SHEET 54-55 FOR CONSTRUCTION
- 16. SEE PLAN AND PROFILES ON PAGES 10-20 AND CROSS SECTIONS ON PAGES 21-43 FOR SPECIFIC SLOPES, DITCH INSTALLATION AND TRANSITIONS.
- 17. PRIOR TO INSTALLING SOIL STABILIZATION MEASURES, CONTRACTOR SHALL COORDINATE WITH CARY PARK DISTRICT TO VERIFY STABILIZATION TYPE (STRAW MULCH OR EXCELSIOR BLANKET) TO BE USED.
- 18. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. AREAS OF THE DEVELOPMENT SITE THAT ARE NOT TO BE GRADED SHALL BE PROTECTED FROM CONSTRUCTION TRAFFIC OR OTHER DISTURBANCE UNTIL FINAL SEEDING IS PERFORMED.
- 19. PROPERTIES AND CHANNELS ADJOINING THE DEVELOPMENT SITE SHALL BE PROTECTED FROM

CARY COMMUNITY TRAIL

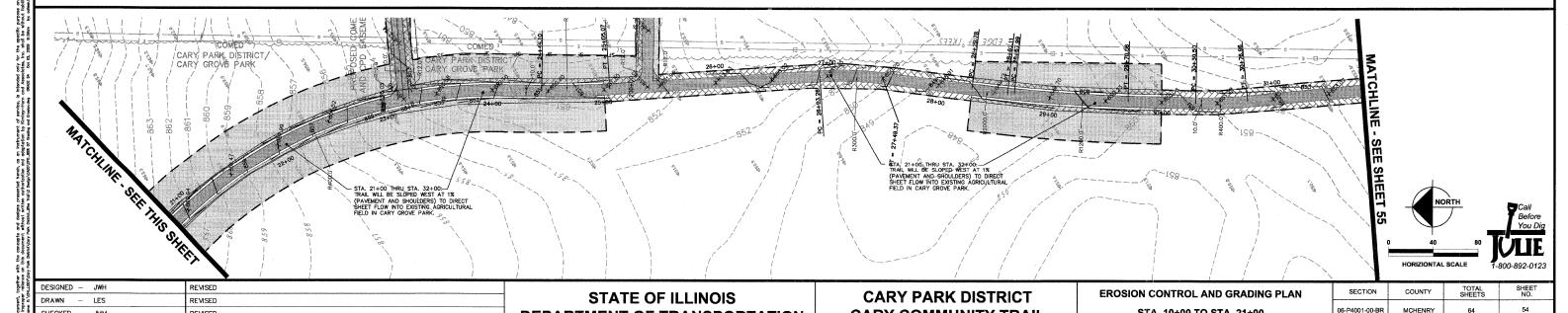
- 20. SOIL EROSION AND SEDIMENT CONTROL FEATURES SHALL BE CONSTRUCTED PRIOR TO THE COMMENCEMENT OF HYDROLOGIC DISTURBANCE OF UPLAND AREAS.
- 21. DISTURBED AREAS SHALL BE STABILIZED WITH TEMPORARY OR PERMANENT MEASURES WITHIN SEVEN (7) CALENDAR DAYS FOLLOWING THE END OF ACTIVE HYDROLOGIC DISTURBANCE.
- 22. ALL STORM SEWERS THAT ARE OR WILL BE FUNCTIONING DURING CONSTRUCTION SHALL BE PROTECTED BY AN APPROPRIATE SEDIMENT CONTROL MEASURE.
- 23. IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION. DISCHARGES SHALL BE ROUTED THROUGH AN EFFECTIVE SEDIMENT CONTROL MEASURE (E.G., SEDIMENT TRAP, SEDIMENT BASIN, OR OTHER APPROPRIATE MEASURES).
- 24. ALL TEMPORARY SOIL FROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN THIRTY (3) DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED, TRAPPED SEDIMENT AND OTHER DISTURBED SOIL AREAS SHALL BE PERMANENTLY STABILIZED.
- 25. A STABILIZED MAT OF AGGREGATE UNDERLAIN WITH FILTER CLOTH (OR OTHER APPROPRIATE MEASURES) SHALL BE LOCATED AT ANY POINT WHERE TRAFFIC WILL BE ENTERING OR LEAVING CONSTRUCTION SITE OF A MAJOR DEVELOPMENT TO OR FROM A PUBLIC RIGHT-OF-WAY, STREET, ALLEY OR PARKING AREA. ANY SEDIMENT OR SOIL REACHING AN IMPROVED PUBLIC RIGHT-OF-WAY, STREET, ALLEY OR PARKING AREA SHALL BE REMOVED BY SCRAPING OR STREET CLEANING AS ACCUMULATIONS WARRANT AND TRANSPORTED TO A CONTROLLED SEDIMENT DISPOSAL AREA.
- 26. SOIL STOCKPILES SHALL NOT BE LOCATED IN A FLOOD-PRONE AREA OR A DESIGNATED BUFFER PROTECTING WATERS OF THE UNITED STATES OR ISOLATED WATERS OF MCHENRY COUNTY.
- 27. THE CONTRACTOR SHALL PROVIDE ADEQUATE RECEPTACLES FOR THE DEPOSITION OF ALL CONSTRUCTION MATERIAL DEBRIS GENERATED DURING THE DEVELOPMENT PROCESS. THE CONTRACTOR SHALL NOT CAUSE OR PERMIT THE DUMPING, DEPOSITING, DROPPING, THROWING DISCARDING OR LEAVING OF CONSTRUCTION MATERIAL DEBRIS UPON OR INTO ANY DEVELOPMENT SITE, CHANNEL, WATERS OF THE U.S. OR ISOLATED WATERS OF MCHENRY COUNTY. THE CONTRACTOR SHALL MAINTAIN THE DEVELOPMENT SITE FREE OF CONSTRUCTION MATERIAL DEBRIS.
- 28. ALL TEMPORARY AND PERMANENT EROSION AND SEDIMENT CONTROL MEASURES SHALL BE MAINTAINED IN AN EFFECTIVE WORKING CONDITION.

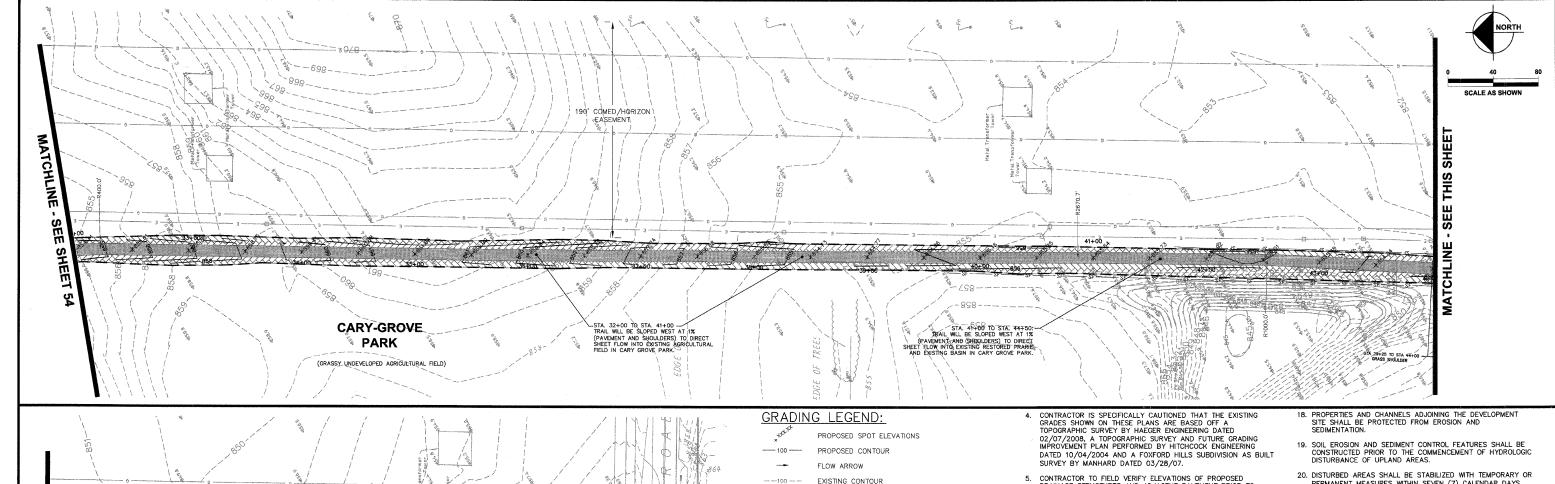
STA. 10+00 TO STA. 21+00 STA. 21+00 TO 32+00

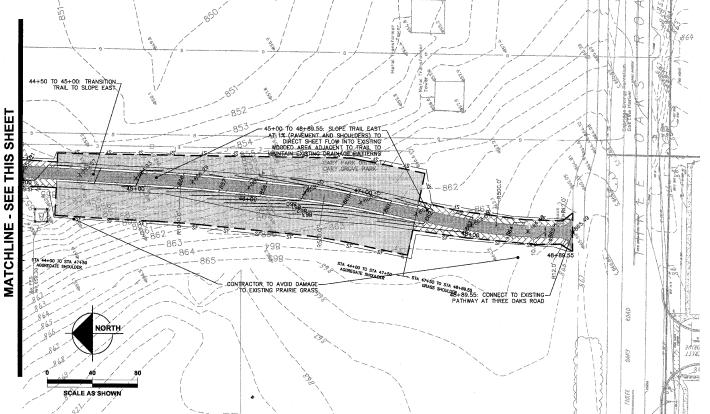
29. PRIOR TO SILT FENCE AND TREE PROTECTION INSTALLATION, 72 HOURS NOTICE TO CARY PARK DISTRICT IS REQUIRED FOR APPROVAL AND INSPECTION OF MATERIALS.

FED. ROAD DIST. NO. 1 ILLINOIS

CONTRACT NO. 63333







LIMITS OF DISTURBANCE (8.96 ACRES) NOTE: STRAW MULCH STABILIZATION TO BE PLACED OVER

* SEE SHEET 2 FOR SEEDING CHARTS

GRADING NOTES:

PRIOR TO INSTALLATION OF STORM DRAINS OR ANY OTHER UTILITIES,
THE CONTRACTOR SHALL EXCAVATE, VERIFY, AND CALCULATE ALL
POINTS OF CONNECTION AND ALL UTILITY CROSSINGS AND INFORM THE
OWNER AND THE ENGINEER OF ANY CONFLICTS OR REQUIRED
OWNER AND THE ENGINEER OF ANY CONFLICTS OR REQUIRED
OF 72 HOURS PRIOR TO CONSTRUCTION. THE ENGINEER AND ITS
CLIENTS SHALL BE HELD HARMLESS IN THE EVENT THE CONTRACTOR
FAILS TO MAKE SUCH NOTIFICATION. FAILS TO MAKE SUCH NOTIFICATION.

PIPE OUTLET/CULVER

EDGE DRAIN CLEANOUT

IDOT CLASS 1 SEEDING*

IDOT CLASS 4A SEEDING

EXCELSIOR/WOOD BLANKE

INLET PROTECTION

TREE PROTECTION

SEEDING WHERE THERE IS NO EXCELSIOR BLANKET

SILT FENCE

RIP RAP

- ALL UNPAVED SLOPES AND AREAS DISTURBED BY CONSTRUCTION SHALL BE GRADED SMOOTH AND FOUR INCHES OF TOPSOIL APPLIED. IF ADEQUATE TOPSOIL IS NOT AVAILABLE ON SITE, THE CONTRACTOR SHALL PROVIDE TOPSOIL, APPROVED BY THE OWNER, AS NEEDED. THE SHALL PROVIDE IOPSOIL, APPROVED BY THE OWNER, AS NEEDED. THE AREA SHALL THEN BE SEEDED/SODDED, FERTILIZED, MULCHED, WATERED AND MAINTAINED UNTIL HARDY GRASS GROWTH IS ESTABLISHED IN ALL AREAS. ANY RELOCATED TREES SHALL BE MAINTAINED UNTIL SUCH POINT AS TREE IS RE-ESTABLISHED. ANY AREAS DISTURBED FOR ANY REASON PRIOR TO FINAL ACCEPTANCE OF THE PROJECT SHALL BE CORRECTED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.
- 3. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES, AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE APPROPRIATE UTILITY COMPANY AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO REQUEST EXACT FIELD LOCATION OF UTILITIES.

- 5. CONTRACTOR TO FIELD VERIFY ELEVATIONS OF PROPOSED DRAINAGE STRUCTURES AND ADJACENT PAVEMENT PRIOR TO STARTING CONSTRUCTION.
- 6. REFER TO GEOTECHNICAL REPORT FOR SPECIFIC SITE SOIL CONDITIONS AND CONSIDERATIONS.
- CONTRACTOR SHALL COMPLY COMPLETELY WITH THE LATEST STANDARDS OF OSHA DIRECTIVES OR ANY OTHER AGENCY HAVING JURISDICTION FOR EXCAVATION AND TRENCHING PROCEDURES. THE CONTRACTOR SHALL USE SUPPORT SYSTEMS, SLOPING, BENCHING, AND OTHER MEANS OF PROTECTION. THIS IS TO INCLUDE, BUT NOT LIMITED TO ACCESS AND EGRESS FROM ALL EXCAVATION AND TRENCHING. CONTRACTOR IS RESPONSIBLE FOR COMPLYING WITH PERFORMANCE CRITERIA AS REQUIRED BY OSHA.
- 8. WHERE APPLICABLE, CONTRACTOR SHALL TAKE ALL APPROPRIATE MEASURES TO KEEP SEDIMENT FROM ENTERING ADJACENT RESIDENTIAL PROPERTIES.
- CONTRACTOR SHALL TAKE ALL APPROPRIATE MEASURES TO AVOID DISTURBING AREA OUTSIDE OF EXISTING CARY PARK DISTRICT PROPERTY, EASEMENTS, AND APPROVED CONSTRUCTION EASEMENTS WITH ADJACENT STAKE HOLDERS.
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- 11. ALL GRADING AND DRAINAGE IMPROVEMENTS WILL BE PERFORMED IN ACCORDANCE WITH MCHENRY COUNTY STORMWATER ORDINANCE AND IDOT STANDARDS AND SPECIFICATIONS FOR ROAD AND REIDER CONSTRUCTION.
- 12. ALL EROSION AND SEDIMENT CONTROL MEASURES WILL BE IN ACCORDANCE WITH THE APPROVED SWPPP, MCHENRY COUNTY, AND THE MCHENRY COUNTY SOIL CONSERVATION DISTRICT.
- 13. DRAIN TILE SYSTEMS DISTURBED DURING DEVELOPMENT MUST BE RECONNECTED BY THOSE RESPONSIBLE FOR THEIR DISTURBANCE UNLESS THE APPROVED ENGINEERING PLANS INDICATE HOW THE DRAIN TILE SYSTEM IS TO BE CONNECTED TO THE PROPOSED STORMWATER MANAGEMENT SYSTEM ALL ABANDONED DRAIN TILES SHALL BE REMOVED IN THEIR ENTIRETY.
- PLEASE REFER TO STANDARDS AND DETAILS LOCATED ON SHEET 59-63 FOR CONSTRUCTION DETAILS.
- 15. SEE PLAN AND PROFILES ON PAGES 15-25 AND CROSS SECTIONS ON PAGES 26-48 FOR SPECIFIC SLOPES, DITCH INSTALLATION AND TRANSITIONS.
- 16. PRIOR TO INSTALLING SOIL STABILIZATION MEASURES, CONTRACTOR SHALL COORDINATE WITH CARY PARK DISTRICT TO VERIFY STABILIZATION TYPE (STRAW MULCH OR EXCELSIOR BLANKET) TO BE USED.
- 17. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION. AREAS OF THE DEVELOPMENT SITE THAT ARE NOT TO BE GRADED SHALL BE PROTECTED FROM CONSTRUCTION TRAFFIC OR OTHER DISTURBANCE UNTIL FINAL SEEDING IS DEPENDED. SEEDING IS PERFORMED.

- PERMANENT MEASURES WITHIN SEVEN (7) CALENDAR DAYS FOLLOWING THE END OF ACTIVE HYDROLOGIC DISTURBANCE.
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- 27. ALL TEMPORARY AND PERMANENT EROSION AND SEDIMENT CONTROL MEASURES SHALL BE MAINTAINED IN AN EFFECTIVE WORKING CONDITION.
- 28. PRIOR TO SILT FENCE AND TREE PROTECTION INSTALLATION, 72 HOURS NOTICE TO CARY PARK DISTRICT IS REQUIRED FOR APPROVAL AND INSPECTION OF MATERIALS.
- 29. MULCH METHOD TO BE USED TO PROTECT ALL PERMANENT SEEDING WHEREVER THE EXCELSIOR BLANKET HAS NOT BEEN SPCIFIED.

You Di

REVISED DESIGNED - JWH DRAWN - LES REVISED CHECKED -- JMM REVISED DATE - 10/02/2009 REVISED

SITE BENCHMARK
HE CP#100
DESCRIPTION: IRON PIPE

ELEVATION: 854.94

LOCATION: 1000' EAST OF INTERSECTION OF 1ST AND THREE OAKS ROAD, 584' NORTH OF

THE NORTH SIDE OF THREE OAKS ROAD, 31'

NORTHEAST OF STORM MANHOLE, AND 26' NORTHWEST OF POWER POLE.

BENCHMARKS:

SOURCE BENCHMARK
DESCRIPTION: MANHOLE #46 FROM THE ASBUILT
RECORD DRAWINGS BY MANHARD CONSULTING
LTD DATED 3-6-07 FOR THE FOXFORD
HILLS SUBDIVISION-PHASE 1 IN THE VILLAGE
OF CARY, ILLINOIS.

LOCATION: 120' NORTHEAST OF THE OAKMAN DIVE AND PARKSTONE DRIVE INTERSECTION ON OAKMAN DRIVE.

DEPARTMENT OF TRANSPORTATION

CARY PARK DISTRICT CARY COMMUNITY TRAIL

SHEET NO. SECTION COUNTY 06-P4001-00-RE CONTRACT NO. 63333 FED. ROAD DIST. NO. 1 ILLINOIS

STATE OF ILLINOIS

EROSION CONTROL AND GRADING PLAN STA, 32+00 TO STA, 44+00 STA. 44+00 TO 48+89.55

GRADING LEGEND:

PROPOSED SPOT ELEVATIONS

EXISTING CONTOUR

EXISTING STORM SEWER

PIPE OUTLET/CULVERT 4" PERFORATED EDGE DRAIN

12" PVC STORM PIPE

IDOT CLASS 1 SEEDING* IDOT CLASS 1B SEEDING*

EXCELSIOR/WOOD BLANKE

INLET PROTECTION

CHECKED - JMM

DATE - 10/02/2009

SILT FENCE

TREE PROTECTION PROPERTY LINE

-- LIMITS OF DISTURBANCE (8.96 ACRES)

REVISED

REVISED

NOTE: STRAW MULCH STABILIZATION TO BE PLACED OVER * SEE SHEET 2 FOR SEEDING CHARTS

GRADING NOTES:

- PRIOR TO INSTALLATION OF STORM DRAINS OR ANY OTHER UTILITIES, THE CONTRACTOR SHALL EXCAVATE, VERIFY, AND CALCULATE ALL POINTS OF CONNECTION AND ALL UTILITY CROSSINGS AND INFORM THE OWNER AND THE ENGINEER OF ANY CONFLICTS OR REQUIRED DEVATIONS FROM THE PLAN. NOTHICATION SHALL BE MADE A MINIMUM OF 72 HOURS PRIOR TO CONSTRUCTION. THE ENGINEER AND ITS CLIENTS SHALL BE HELD HARMLESS IN THE EVENT THE CONTRACTOR FAILS TO MAKE SUCH NOTIFICATION.
- 2. ALL UNPAVED SLOPES AND AREAS DISTURBED BY CONSTRUCTION SHALL BE GRADED SMOOTH AND FOUR INCHES OF TOPSOIL APPLIED. IF ADEQUATE TOPSOIL IS NOT AVAILABLE ON SITE, THE CONTRACTOR SHALL PROVIDE TOPSOIL, APPROVED BY THE OWNER, AS NEEDED. THE AREA SHALL THEN BE SEEDED/SODDED, FERTILIZED, MULCHED, WATERED AND MAINTAINED UNTIL HARDY GRASS GROWTH IS ESTABLISHED IN ALL AREAS. ANY RELOCATED TREES SHALL BE MAINTAINED UNTIL SUCH POINT AS TREE IS RE-ESTABLISHED. ANY AREAS DISTURBED FOR ANY REASON PRIOR TO FINAL ACCEPTANCE OF THE PROPERTY SHALL BE CORPORTED ANY THE CONTRACTOR AT NO. ACCEPTANCE OF THE PROJECT SHALL BE CORRECTED BY THE CONTRACTOR AT NO
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- 5. CONTRACTOR TO FIELD VERIFY ELEVATIONS OF PROPOSED DRAINAGE STRUCTURES AND ADJACENT PAVEMENT PRIOR TO STARTING CONSTRUCTION.
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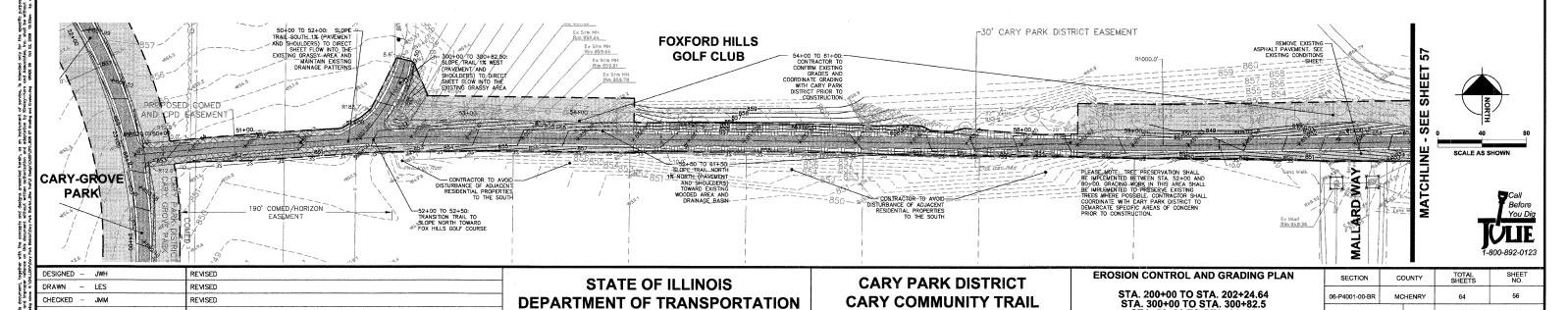
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- 13. DRAIN TILE SYSTEMS DISTURBED DURING DEVELOPMENT MUST BE RECONNECTED BY THOSE RESPONSIBLE FOR THEIR DISTURBANCE UNLESS THE APPROVED ENGINEERING PLANS INDICATE HOW THE DRAIN TILE SYSTEM IS TO BE CONNECTED TO THE PROPOSED STORMWATER MANAGEMENT SYSTEM. ALL ABANDONED DRAIN TILES SHALL BE REMOVED IN THEIR ENTIRETY
- 14. PLEASE REFER TO STANDARDS AND DETAILS LOCATED ON SHEET 54-55 FOR CONSTRUCTION DETAILS.
- 15. SEE PLAN AND PROFILES ON PAGES 10-20 AND CROSS SECTIONS ON PAGES 21-43 FOR SPECIFIC SLOPES, DITCH INSTALLATION AND TRANSITIONS.
- 16 PRIOR TO INSTALLING SOIL STABILIZATION MEASURES, CONTRACTOR SHALL COORDINATE WITH CARY PARK DISTRICT TO VERIFY STABILIZATION TYPE (STRAW MULCH OR EXCELSIOR BLANKET) TO
- 17. SOIL DISTURBANCE SHALL BE CONDUCTED IN SUCH A MANNER AS TO MINIMIZE EROSION, AREAS OF THE DEVELOPMENT SITE THAT ARE NOT TO BE GRADED SHALL BE PROTECTED FROM CONSTRUCTION TRAFFIC OR OTHER DISTURBANCE UNTIL FINAL SEEDING IS PERFORMED.
- 18. PROPERTIES AND CHANNELS ADJOINING THE DEVELOPMENT SITE SHALL BE PROTECTED FROM EROSION AND SEDIMENTATION
- 19. MULCH METHOD TO BE USED TO PROTECT ALL PERMANENT SEEDING WHEREVER THE EXCELSIOR BLANKET HAS NOT BEEN SPCIFIED.

- 20. SOIL EROSION AND SEDIMENT CONTROL FEATURES SHALL BE CONSTRUCTED PRIOR TO THE COMMENCEMENT OF HYDROLOGIC DISTURBANCE OF UPLAND AREAS.
- 21. DISTURBED AREAS SHALL BE STABILIZED WITH TEMPORARY OR PERMANENT MEASURES WITHIN SEVEN (7) CALENDAR DAYS FOLLOWING THE END OF ACTIVE HYDROLOGIC DISTURBANCE.
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- 23. IF DEWATERING SERVICES ARE USED, ADJOINING PROPERTIES AND DISCHARGE LOCATIONS SHALL BE PROTECTED FROM EROSION. DISCHARGES SHALL BE ROUTED THROUGH AN EFFECTIVE SEDIMENT CONTROL MEASURE (E.G., SEDIMENT TRAP, SEDIMENT BASIN, OR OTHER APPROPRIATE MEASURES).
- 24. ALL TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES SHALL BE REMOVED WITHIN THIRTY (3) DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY MEASURES ARE NO LONGER NEEDED. TRAPPED SEDIMENT AND OTHER DISTURBED SOIL AREAS SHALL BE PERMANENTLY STABILIZED.
- 25. A STABILIZED MAT OF AGGREGATE UNDERLAIN WITH FILTER CLOTH (OR OTHER APPROPRIATE MEASURES) SHALL BE LOCATED AT ANY POINT WHERE TRAFFIC WILL BE ENTERING OR LEAVING A CONSTRUCTION SITE OF A MAJOR DEVELOPMENT TO OR FROM A PUBLIC RIGHT-OF-WAY, STREET, ALLEY OR PARKING AREA. ANY SEDIMENT OR SOIL REACHING AN IMPROVED PUBLIC RIGHT-OF-WAY, STREET, ALLEY OR PARKING AREA SHALL BE REMOVED BY SCRAPING OR STREET CLEANING AS ACCUMULATIONS WARRANT AND TRANSPORTED TO A CONTROLLED SEDIMENT DISPOSAL AREA.
- 26. SOIL STOCKPILES SHALL NOT BE LOCATED IN A FLOOD-PRONE AREA OR A DESIGNATED BUFFER PROTECTING WATERS OF THE UNITED STATES OR ISOLATED WATERS OF MCHENRY COUNTY.
- 27. THE CONTRACTOR SHALL PROVIDE ADEQUATE RECEPTACLES FOR THE DEPOSITION OF ALL CONSTRUCTION MATERIAL DEBRIS GENERATED DURING THE DEVELOPMENT PROCESS. THE CONTRACTOR SHALL NOT CAUSE OR PERMIT THE DUMPING, DEPOSITING, DROPPING, THROWING DISCARDING OR LEAVING OF CONSTRUCTION MATERIAL DEBRIS UPON OR INTO ANY DEVELOPMENT SITE, CHANNEL, WATERS OF THE U.S. OR ISOLATED WATERS OF MCHENRY COUNTY. THE CONTRACTOR SHALL MAINTAIN THE DEVELOPMENT SITE FREE OF CONSTRUCTION MATERIAL DEBRIS.
- 28. ALL TEMPORARY AND PERMANENT EROSION AND SEDIMENT CONTROL MEASURES SHALL BE MAINTAINED IN AN EFFECTIVE WORKING CONDITIONS.
- 29. PRIOR TO SILT FENCE AND TREE PROTECTION INSTALLATION, 72 HOURS NOTICE TO CARY PARK DISTRICT IS REQUIRED FOR APPROVAL AND INSPECTION OF MATERIALS.

STA. 50+00 TO STA. 61+50

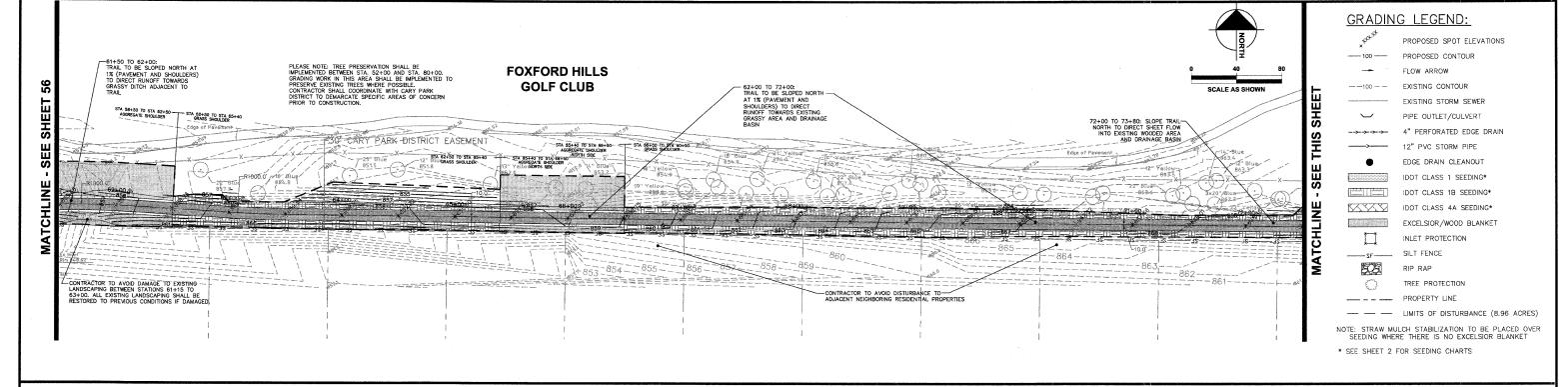
FED. ROAD DIST. NO. 1 ILLINOIS

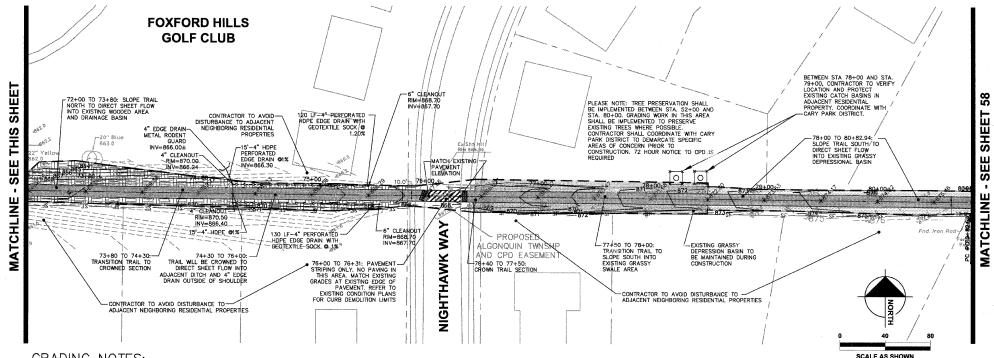
CONTRACT NO. 63333



CARY COMMUNITY TRAIL

DEPARTMENT OF TRANSPORTATION





GRADING NOTES:

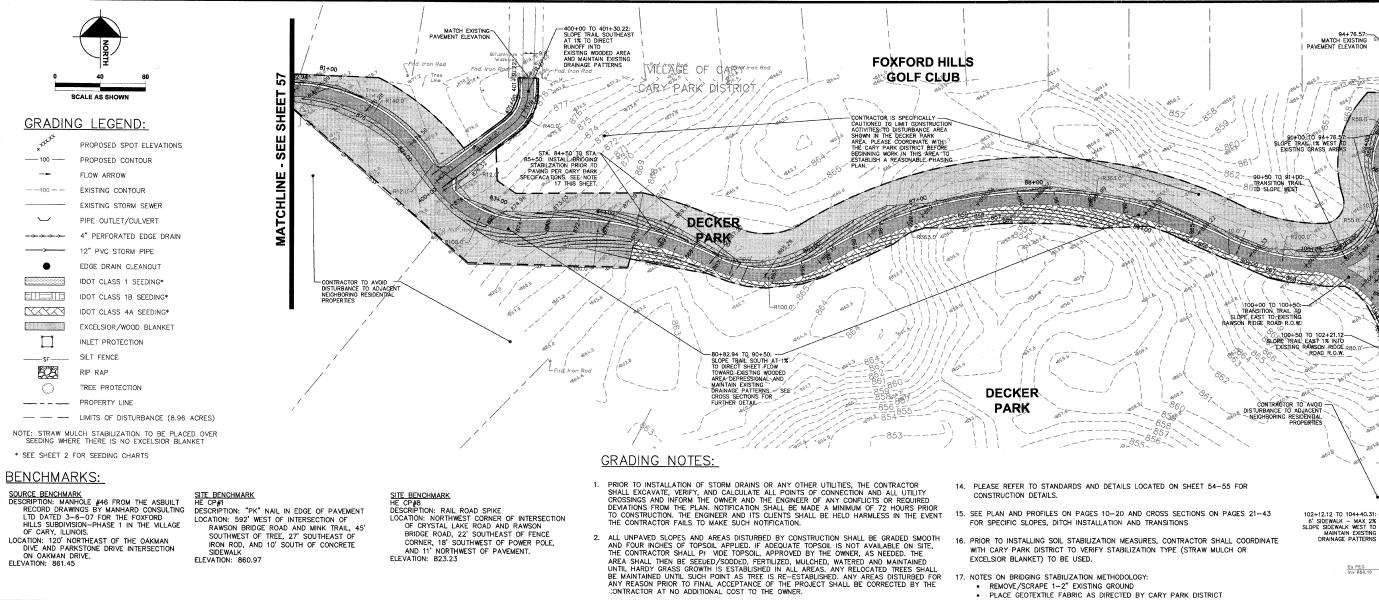
- PRIOR TO INSTALLATION OF STORM DRAINS OR ANY OTHER UTILITIES, THE CONTRACTOR SHALL EXCAVATE, VERIFY, AND CALCULATE ALL POINTS OF CONNECTION AND ALL UTILITY CROSSINGS AND INFORM THE OWNER AND THE ENGINEER OF ANY CONFLICTS OR REQUIRED DEVIATIONS FROM THE PLAN. NOTIFICATION SHALL BE MADE A MINIMUM OF 72 HOURS PRIOR TO CONSTRUCTION. THE ENCINEER AND ITS CLIENTS SHALL BE HELD HARMLESS IN THE EVENT THE CONTRACTOR FAILS TO MAKE SUCH NOTIFICATION.
- 2. ALL UNPAVED SLOPES AND AREAS DISTURBED BY CONSTRUCTION SHALL BE GRADED SMOOTH AND FOUR INCHES OF TOPSOIL APPLIED. IF ADEQUATE TOPSOIL IS NOT AVAILABLE ON SITE, THE CONTRACTOR SHALL PROVIDE TOPSOIL, APPROVED BY THE OWNER, AS NEEDED. THE AREA SHALL THEN BE SEEDED/SODDED, FERTILIZED, MULCHED, WATERED AND MAINTAINED UNTIL HARDY GRASS GROWITH IS ESTABLISHED IN ALL AREAS. ANY RELOCATED TREES SHALL BE MAINTAINED UNTIL SUCH POINT AS TREE IS RE-ESTABLISHED. ANY AREAS DISTURBED FOR ANY REASON PRIOR TO FINAL ACCEPTANCE OF THE PROJECT SHALL BE CORRECTED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.
- 3. THE CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE LOCATION AND/OR ELEVATION OF EXISTING UTILITIES AS SHOWN ON THESE PLANS IS BASED ON RECORDS OF THE VARIOUS UTILITY COMPANIES, AND WHERE POSSIBLE, MEASUREMENTS TAKEN IN THE FIELD. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR MUST CALL THE APPROPRIATE UTILITY COMPANY AT LEAST 72 HOURS BEFORE ANY EXCAVATION TO
- 4. CONTRACTOR IS SPECIFICALLY CAUTIONED THAT THE EXISTING GRADES SHOWN ON THESE PLANS ARE BASED OFF A TOPOGRAPHIC SURVEY BY HAEGER ENGINEERING DATED 02/07/2008, A TOPOGRAPHIC SURVEY AND FUTURE GRADING IMPROVEMENT PLAN PERFORMED BY HITCHCOCK ENGINEERING DATED 10/04/2004 AND A FOXFORD HILLS SUBDIVISION AS BUILT SURVEY BY MANHARD DATED 03/28/07.

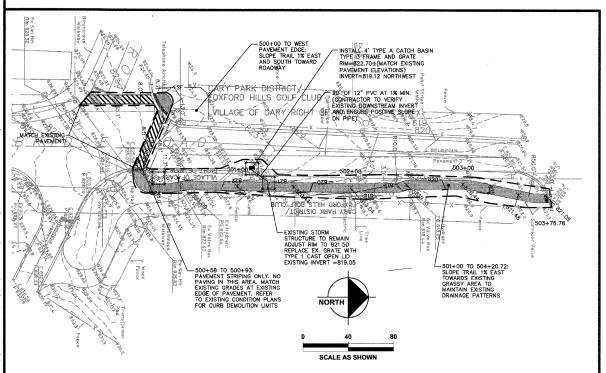
- CONTRACTOR TO FIELD VERIFY ELEVATIONS OF PROPOSED DRAINAGE STRUCTURES AND ADJACENT PAVEMENT PRIOR TO STARTING CONSTRUCTION.
- 6. REFER TO GEOTECHNICAL REPORT FOR SPECIFIC SITE SOIL CONDITIONS AND CONSIDERATIONS.
- 7. CONTRACTOR SHALL COMPLY COMPLETELY WITH THE LATEST STANDARDS OF OSHA DIRECTIVES OR ANY OTHER AGENCY HAVING JURISDICTION FOR EXCAVATION AND TRENCHING PROCEDURES. THE CONTRACTOR SHALL USE SUPPORT SYSTEMS, SLOPING, BENCHING, AND OTHER MEANS OF PROTECTION. THIS IS TO INCLUDE, BUT NOT LIMITED TO ACCESS AND EGRESS FROM ALL EXCAVATION AND TRENCHING. CONTRACTOR IS RESPONSIBLE FOR COMPLYING WITH PERFORMANCE CRITERIA AS REQUIRED BY OSHA.
- 8. WHERE APPLICABLE, CONTRACTOR SHALL TAKE ALL APPROPRIATE MEASURES TO KEEP SEDIMENT FROM ENTERING ADJACENT RESIDENTIAL PROPERTIES.
- CONTRACTOR SHALL TAKE ALL APPROPRIATE MEASURES TO AVOID DISTURBING AREA OUTSIDE OF EXISTING CARY PARK DISTRICT PROPERTY, EASEMENTS, AND APPROVED CONSTRUCTION EASEMENTS WITH ADJACENT STAKE HOLDERS.
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- 29. ALL EDGE DRAIN PIPE USED SHALL MEET IDOT SECTION 1040



REVISED DESIGNED - JWH SHEET NO. TOTAL SECTION COUNTY **EROSION CONTROL AND GRADING PLAN** STATE OF ILLINOIS **CARY PARK DISTRICT** REVISED STA. 60+50 TO STA. 72+50 STA. 71+50 TO STA. 80+82.94 CHECKED - JMM REVISED **DEPARTMENT OF TRANSPORTATION CARY COMMUNITY TRAIL** DATE - 10/02/2009 REVISED FED. ROAD DIST. NO. 1 ILLINOIS CONTRACT NO. 63333





DESIGNED - JWH

DATE - 10/02/2009

DRAWN CHECKED - JMM REVISED

REVISED

REVISED

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 INSTALL STONE IN APPROVED LIFTS TO BRING SECTION UP TO PROPOSED

SUBGRADE ELEVATION.

. INSTALL PAVEMENT SECTION UP TO FINAL GRADE. SEE PLAN/PROFILE FOR FLEVATIONS CONTRACTOR IS TO COORDINATE WITH CARY PARK DISTRICT PRIOR TO

INSTALLATION OF THESE MATERIALS.

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Ex FES Inv 860,19

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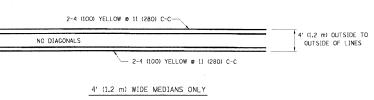
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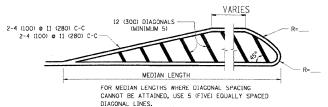
SLOPE EAST

STATE OF ILLINOIS **DEPARTMENT OF TRANSPORTATION**

CARY PARK DISTRICT CARY COMMUNITY TRAIL **EROSION CONTROL AND GRADING PLAN** STA. 80+82.94 TO STA. 92+76.57 STA. 111+00 TO 111+82.06 STA. 500+00 TO 504+20.72

SECTION	COUNTY		TOTAL SHEETS	SHEET NO.
06-P4001-00-BR	MCHENRY		64	58
FED. ROAD DIST. NO. 1		ILLINOIS	CONTRACT	NO. 63333



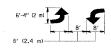


MEDIANS OVER 4' (1.2 m) WIDE

DIAGONAL LINE SPACING: 50' (15 m) C-C (LESS THAN 30MPH (50 km/h))
75' (25 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h))
150' (45 m) C-C (MORE THAN 45MPH (70 km/h))

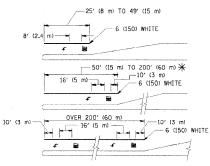
4 (100) YELLOW LINES (5½ (140) C-C) 2-4 (100) YELLOW P 11 (280) C-C 4 (100) YELLOW LINES (5½ (140) C-C)

A MINIMUM OF TWO PAIRS OF TURN ARROWS SHALL BE USED, WHITE IN COLOR.
ADDITIONAL PAIRS SHALL BE PLACED AT 200' (60 m) TO 300' (90 m) INTERVALS.



MEDIAN WITH TWO-WAY LEFT TURN LANE

TYPICAL PAINTED MEDIAN MARKING

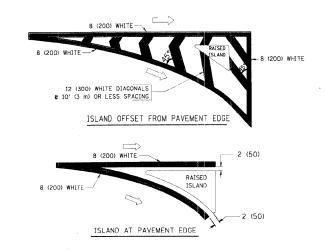


FULL SIZE LETTERS 8' (2.4 m) AND ARROWS SHALL BE USED. \P AREA = 15.6 SQ. FT. (1.5 m²) \P AREA = 20.8 SQ. FT. (1.9 m²)

* TURN LANES IN EXCESS OF 400' (120 m) IN LENGTH MAY HAVE AN ADDITIONAL SET OF ARROW - "ONLY" INSTALLED MIDWAY BETWEEN THE OTHER TWO SETS OF ARROW - "ONLY".

TYPICAL LEFT (OR RIGHT) TURN LANE

TYPICAL TURN LANE MARKING



TYPICAL ISLAND MARKING

TYPE OF MARKING	WIDTH OF LINE	PATTERN	COLOR	SPACING / REMARKS
CENTERLINE ON 2 LANE PAVEMENT	4 (100)	SKIP-DASH	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE
CENTERLINE ON MULTI-LANE UNDIVIDED PAVEMENT	2 2 4 (100)	SOLID	YELLOW	11 (280) C-C
NO PASSING ZONE LINES: FOR ONE DIRECTION FOR BOTH DIRECTIONS	4 (100) 2 @ 4 (100)	SOLID SOLID	YELLOW YELLOW	5½ (140) C-C FROM SKIP-DASH CENTERLINE 11 (280) C-C OMIT SKIP-DASH CENTERLINE BETWEEN
LANE LINES	4 (100) 5 (125) ON FREEWAYS	SKIP-DASH SKIP-DASH	WHITE WHITE	10' (3 m) LINE WITH 30' (9 m) SPACE
DOTTED LINES (EXTENSIONS OF CENTER, LANE OR TURN LANE MARKINGS)	SAME AS LINE BEING EXTENDED	SKIP-DASH	SAME AS LINE BEING EXTENDED	2' (600) LINE WITH 6' (1.8 m) SPACE
EDGE LINES	4 (100)	SOLID	YELLOW-LEFT WHITE-RIGHT	OUTLINE MOUNTABLE MEDIANS IN YELLOW: EDGE LINES ARE NOT USED NEXT TO BARRIER CURB
TURN LANE MARKINGS :	6 (150) LINE; FULL SIZE LETTERS & SYMBOLS (8' (2.4m))	SOLID	WHITE	SEE TYPICAL TURN LANE MARKING DETAIL
TWO WAY LEFT TURN MARKING	2 & 4 (100) EACH DIRECTION	SKIP-DASH AND SOLID	YELLOW	10' (3 m) LINE WITH 30' (9 m) SPACE FOR SKIP-DASH; 5½ (140) C-C BETWEEN SOLID LINE AND SKIP-DASH LINE
	8' (2.4m) LEFT ARROW	IN PAIRS	WHITE	SEE TYPICAL TWO-WAY LEFT TURN MARKING DETAIL
CROSSWALK LINES (PEDESTRIAN) A. DIAGONALS (BIKE & EQUESTRIAN) B. LONGITUDINAL BARS (SCHOOL)	2 & 6 (150) 12 (300) & 45° 12 (300) & 90°	SOLID SOLID SOLID	WHITE WHITE WHITE	NOT LESS THAN 6' (1.8 m) APART 2' (600) APART 2' (600) APART 5' (600) APART SEE TYPICAL CROSSWALK MARKING DETAILS.
STOP LINES	24 (600)	SOLID	WHITE	PLACE 4' (1.2 m) IN ADVANCE OF AND PARALLEL TO CROSSWALK, IF PRESENT OTHERWISE, PLACE AT DESIRED STOPPING POINT. PARALLEL TO CROSSROAD CENTERLINE, WHERE POSSIBLE
PAINTED MEDIANS	2 2 4 (100) WITH 12 (300) DIAGONALS 10 45°	SOLID	YELLOW: TWO WAY TRAFFIC	11 (280) C-C FOR THE DOUBLE LINE SEE TYPICAL PAINTED MEDIAN MARKING.
	NO DIAGONALS USED FOR 4' (1.2 m) WIDE MEDIANS		WHITE: ONE WAY TRAFFIC	SEE THIORE FAIRTED MEDIAN MANNETON
GORE MARKING AND CHANNELIZING LINES	8 (200) WITH 12 (300) DIAGONALS @ 45°	SOLID	WHITE	DIAGONALS: 15' (4.5 m) C-C (LESS THAN 30MPH (50 km/h)) 20' (6 m) C-C 30MPH (50 km/h) TO 45MPH (70 km/h)) 30' (9 m) C-C (0YER 45MPH (70 km/h))
RAILROAD CROSSING	24 (600) TRANSVERSE LINES; "RR" IS 6' (1.8 m) LETTERS; 16 (400) LINE FOR "X"	SOLID	WHITE	SEE STATE STANDARD 780001 AREA OF: "R"*3.6 S0. FT. (0.33 m ²) EACH "X"=54.0 S0. FT. (5.0 m ²)
SHOULDER DIAGONALS	12 (300) c 45°	SOLID	WHITE - RIGHT YELLOW - LEFT	50' (15 m) C-C (LESS THAN 30MPH (50 km/h)) 75' (25 m) C-C (30 MPH (50 km/h)) TO 45MPH (70 km/h)) 150' (45 m) C-C (0VER 45MPH (70 km/h))

FOR FURTHER DETAILS ON PAVEMENT MARKING REFER TO STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION AND STATE STANDARD 780001.

All dimensions are in inches (millimeters) unless otherwise shown.

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TYPICAL PAVEMENT MARKINGS TC-13 CONTRACT	
	NO.
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	REVISED	DEPARTMENT OF TRANSPORTATION	CARY COMMUNITY TRAIL	DETAILO	06-P4001-00-BR	MCHENRY	64	
/2009	REVISED				FED. ROAD DIST, N	IO. 1 ILLINOIS	CONTRACT	NO. 63333

Before You Dig VLIE -800-892-0123

TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS

NOTES:

- A. FOR NO LANE RESTRICTION ON THE SIDE ROAD OR DRIVEWAYS
- 1. SIDE ROAD WITH A SPEED LIMIT OF 40 MPH (60 km/h) OR LESS AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
- O) ONE ROAD CONSTRUCTION AHEAD SIGN 36 x 36 (900x900) WITH A FLASHER AND FLAG MOUNTED ON IT APPROXIMATELY 200' (60 m) IN ADVANCE
- b) the closed portion of the main route shall be protected by blocking with type 1, type 11 or type 111 barricades, 1/3 of the cross section of the closed portion.
- 2. SIDE ROAD WITH A SPEED LIMIT GREATER THAN 40 MPH (60 km/h) AS SHOWN ON THE DRAWING AND AS DIRECTED BY THE ENGINEER:
- of one road construction ahead Sign 48 x 48 (1.2 m x 1.2 m) with a Flasher Mounted on it approximately 500' (150 m) in advance of the Main Route.
- b) THE CLOSED PORTION OF THE MAIN ROUTE SHALL BE PROTECTED BY
 BLOCKING WITH TYPE 111 BARRICADES, 1/2 OF THE CROSS SECTION
 OF THE CLOSED PORTION.
- 3. WHEN THE SIDE ROAD LIES BETWEEN THE BEGINNING OF THE MAINLINE SIGNING AND THE WORK ZONE, A SINGLE HEADED ARROW (M6-1) SHALL BE USED IN LIEU OF THE DOUBLE HEADED ARROW (M6-4).

- B. FOR A LANE CLOSURE ON A SIDE ROAD OR DRIVEWAY:
- USE APPLICABLE PORTIONS OF THE TYPICAL APPLICATION OF TRAFFIC CONTROL DEVICES (STD. 701501, STD. 701606 OR THE APPROPRIATE STANDARD). THE SPACING OF SIGNS AND BARRICADES SHALL BE ADJUSTED FOR FIELD CONDITIONS AS DIRECTED BY THE ENGINEER. THE DIRECTIONAL ARROW SHALL BE COVERED OR REMOVED WHEN NO LONGER CONSISTENT WITH THE SIDE ROAD LANE CLOSURE.
- C. ADVANCE WARNING SIGNS ARE TO BE OMITTED ON DRIVEWAY UNLESS OTHERWISE NOTED.
- D. THE TRAFFIC CONTROL AND PROTECTION FOR SIDE ROADS, INTERSECTIONS, AND DRIVEWAYS SHALL BE INCIDENTAL TO THE COST OF SPECIFIED TRAFFIC CONTROL STANDARDS OR ITEMS.

All dimensions are in millimeters (inches) unless otherwise shown.

FILE NAME =	USER NAME = gaglianobt	DESIGNED - LHA	REVISED - J. OBERLE 10-18-95
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	PLOT DATE = 1/4/2008	DATE ~ 06-89	REVISED -T. RAMMACHER 01-06-00

STATE OF ILLINOIS
DEPARTMENT OF TRANSPORTATION

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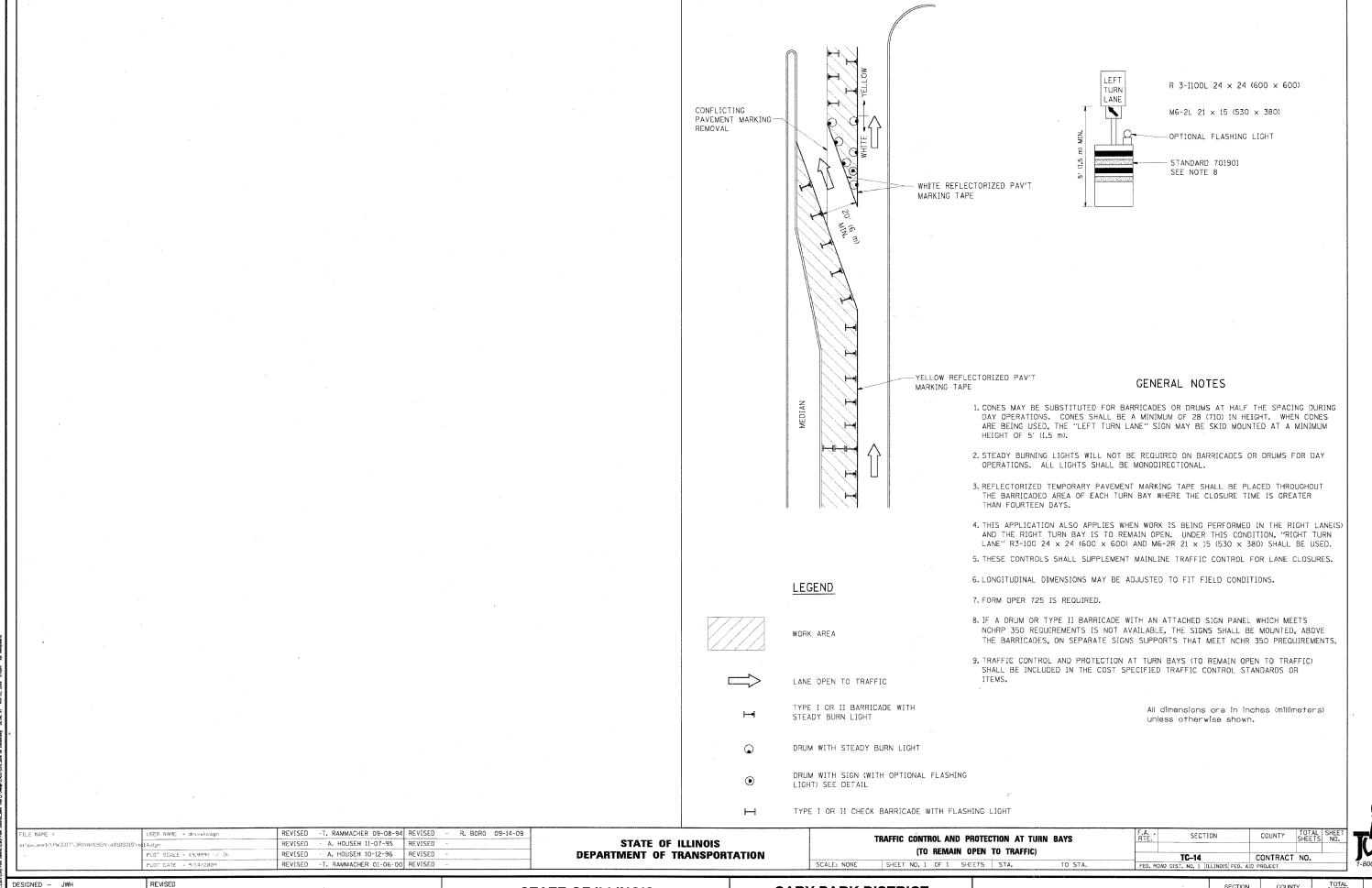
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This document, together with the concepts and designs presented herein, on an instrument of service, is intended only for the specific purpose of and improve inferiors on this document without without other documents of Month-Vinter and Association, and Association, and all the without is because ones KNA to KNA LEVICE of Detach. And therefore the Internal Purpose are KNA SERVICE OF ALC 2000 BYTON. In Conservat



Before You Dig

STATE OF ILLINOIS CARY PARK DISTRICT
DEPARTMENT OF TRANSPORTATION CARY COMMUNITY TRAIL

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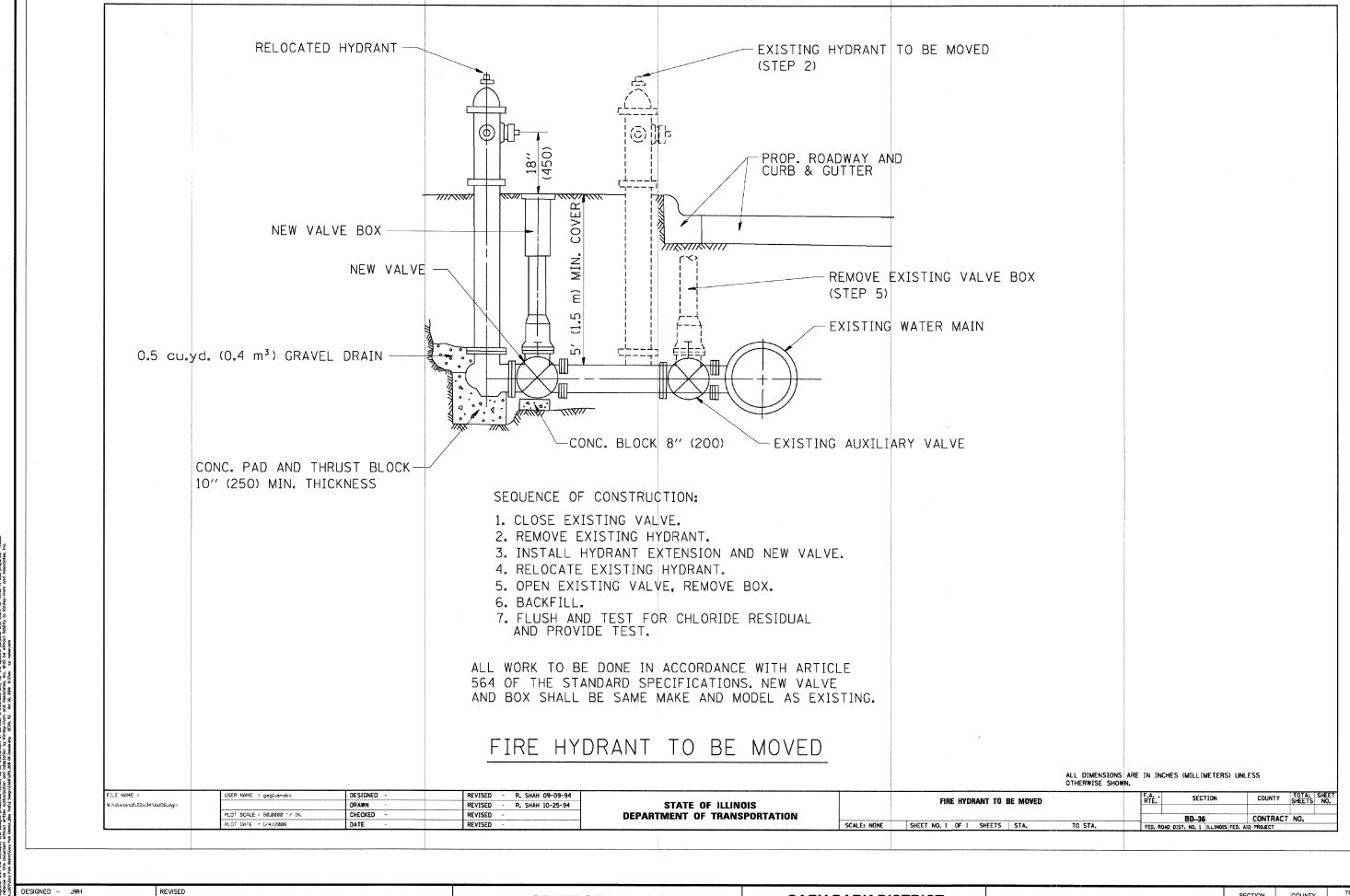
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 FED. ROAD DIST. NO. 1
 ILLINOIS
 CONTRACT NO. 63333



Before You Dig **JULIE** 1-800-892-0123

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DATE - 10/02/2009 REVISED

STATE OF ILLINOIS

DEPARTMENT OF TRANSPORTATION

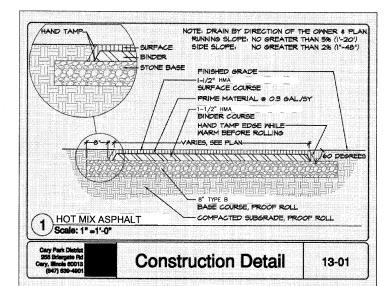
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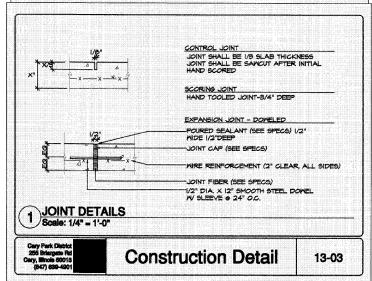
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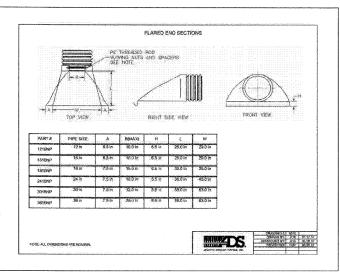
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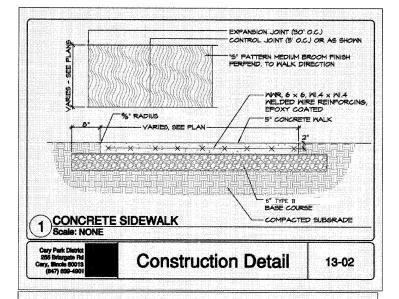
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 CONTRACT NO. 63333

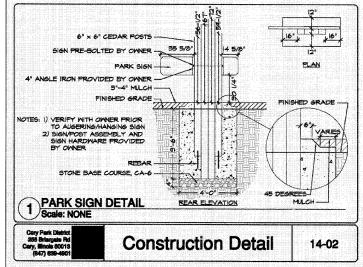


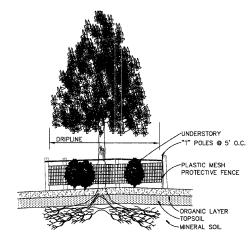




FLARED END SECTION



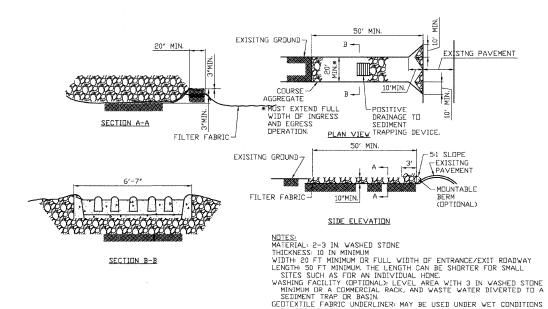




- 1. EITHER PLASTIC OR WOOD ORANGE SNOW FENCING SHALL BE INSTALLED AT OR BEYOND THE DRIPLINE, UNLESS MORE SUBSTANTIAL FENCING IS REQUIRED.
- 2. STAKES SHALL BE METAL "T" POLES SPACED NO FURTHER APART THAN 5' ON CENTER.
- 3. FENCING SHALL NOT BE INSTALLED CLOSER TO THE TREE THAN THE DRIPLINE OF THOSE TREES TO BE SAVED. SPECIAL CIRCUMSTANCES SHALL BE REVEWED BY THE CARY "JARK DISTRICT.
- 4. FENCING SHALL BE ERECTED PRIOR TO CONSTRUCTION. THE CARY PARK DISTRICT SHALL 3E NOTIFIED ONCE THE FENCING IS INSTALLED FOR INSPECTION.
- 6. NO PERSON SHALL CONDUCT ANY ACTIVITY WITHIN THE AREAS PROPOSED TO REMAIN. THIS SHALL INCLUDE, BUT NOT LIMITED TO:
- A. NO SOLVENTS OR CHEMICALS WITHIN THE PROTECTED AREAS.
- $\ensuremath{\mathsf{B}}.$ NO BUILDING MATERIALS OR CONSTRUCTION EQUIPMENT WITHIN THE PROTECTED AREAS.
- C. NO GRADE CHANGES, INCLUDING FILL, WITHIN THE PROJECTED AREAS.
- D. NO REMOVAL OR VEGETATION FROM THE GROUND, WITHOUT PERMISSION FROM THE PROPER REVIEWING AUTHORITY.

C. ANY REQUIRED SWALE NEEDS TO BE DIRECTED AROUND THE PROTECTED AREAS. IN INSTANCES WHERE SWALES ARE APPROVED THROUGH A PROTECTED AREA, THE SWALES NEED TO BE <u>HAND DUG</u>, MACHINERY OF ANY KIND IS PROHIBITED.

TREE PROTECTION DETAIL



CONSTRUCTION ENTRANCE



DESIGNED - JWH STATE OF ILLINOIS DRAWN - LES REVISED CHECKED - JMM REVISED DEPARTMENT OF TRANSPORTATION DATE - 10/02/2009

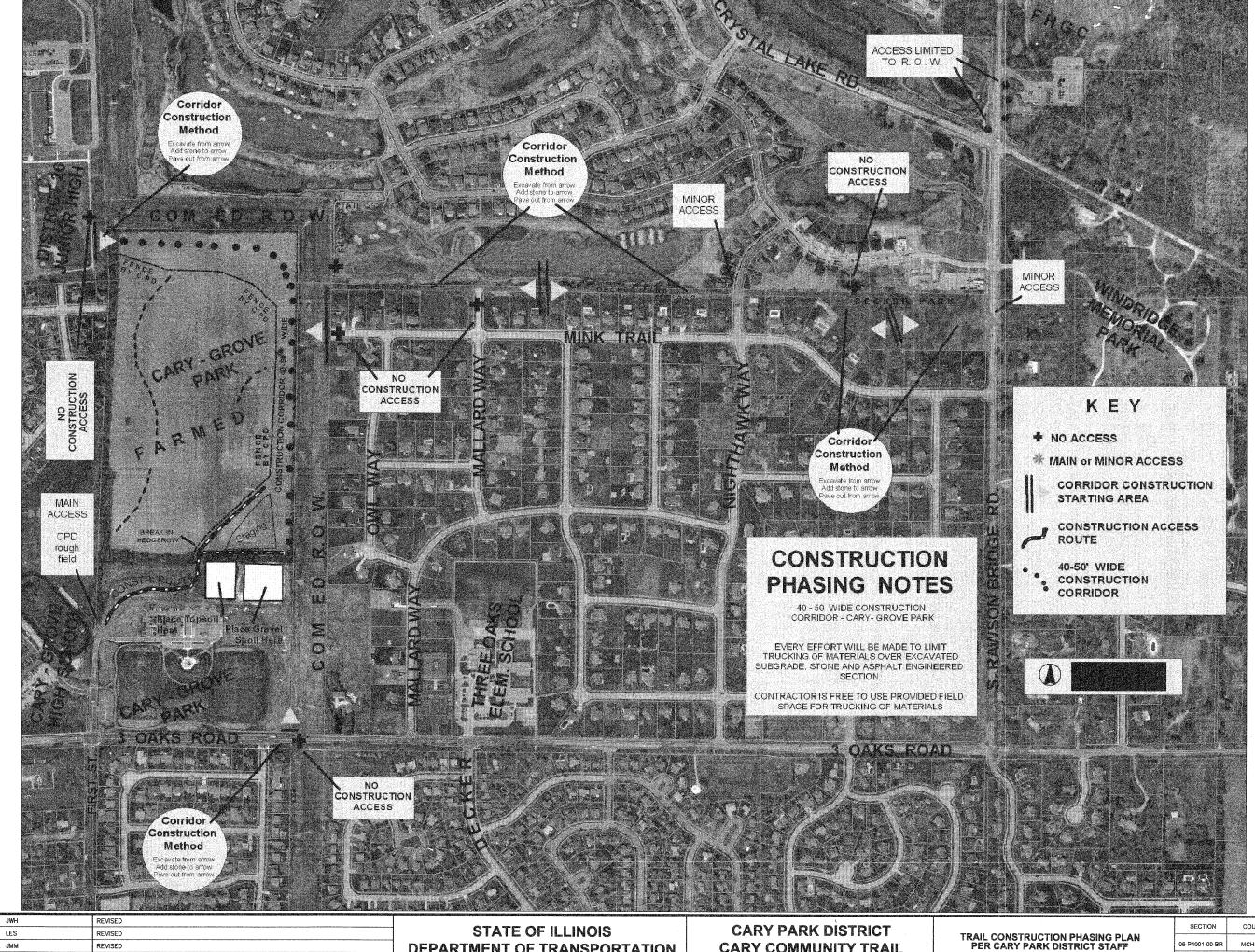
CARY PARK DISTRICT CARY COMMUNITY TRAIL

GEOTEXTILE FABRIC UNDERLINER: MAY BE USED UNDER WET CONDITIONS OR FOR SOLIDS WITHIN A HIGH SEASONAL WATER TABLE TO PROVIDE GREATER BEARING STRENGTH.

SECTION COUNTY 6-P4001-00-RF CONTRACT NO. 63333 FED. ROAD DIST, NO. 1 ILLINOIS

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DETAILS





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CARY COMMUNITY TRAIL

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	FED. ROAD DIST. NO. 1		ILLINOIS	CONTRACT	NO. 63333