

KANKAKEE VALLEY AIRPORT AUTHORITY KANKAKEE, ILLINOIS

CONSTRUCTION PLANS FOR GREATER KANKAKEE AIRPORT

RECONSTRUCT SOUTHWEST QUADRANT APRON; TERMINAL AND SOUTHEAST HANGAR APRON PAVEMENT REPAIRS

811 Know what's below.
 Call before you dig.

J.U.L.I.E.
 JOINT UTILITY LOCATING
 INFORMATION FOR EXCAVATORS
 www.illinois1call.com

THE LOCATION, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ON THE PLANS IS NOT REPRESENTED AS BEING ACCURATE, SUFFICIENT OR COMPLETE IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE ACTUAL LOCATIONS OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF HIS OPERATIONAL PLANS, OBTAIN FROM RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION AND THE ONE-CALL NOTICE SYSTEM. THE ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH UTILITY OR SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

CALL J.U.L.I.E. FOR UTILITY INFORMATION AT 811.

ILLINOIS PROJECT: IKK-5084
 S.B.G. PROJECT: 3-17-SBGP-TBD

JUNE 6, 2025

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DAVID K. PEABODY
 LICENSED PROFESSIONAL ENGINEER
 062-048437
 STATE OF ILLINOIS

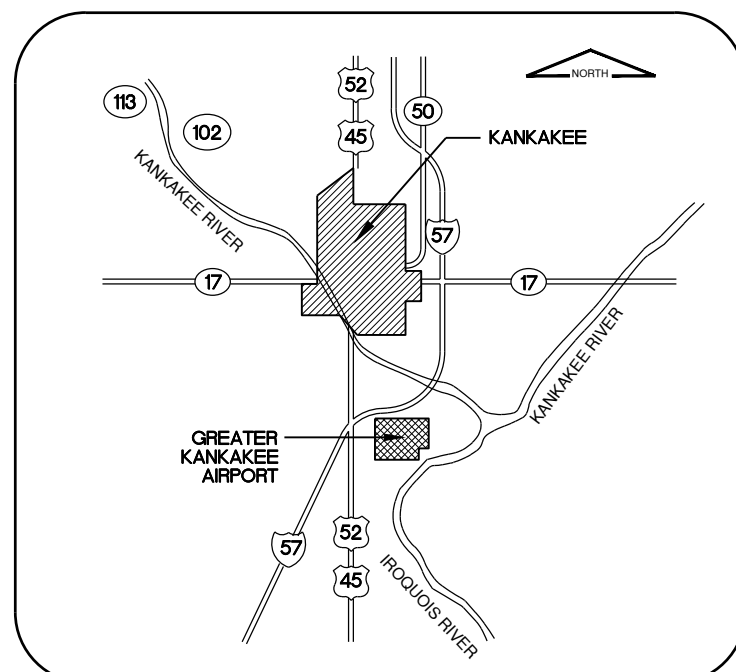
SUBMITTED BY *D. Kyle Peabody*
 D. KYLE PEABODY, P.E.

DATE JUNE 6, 2025

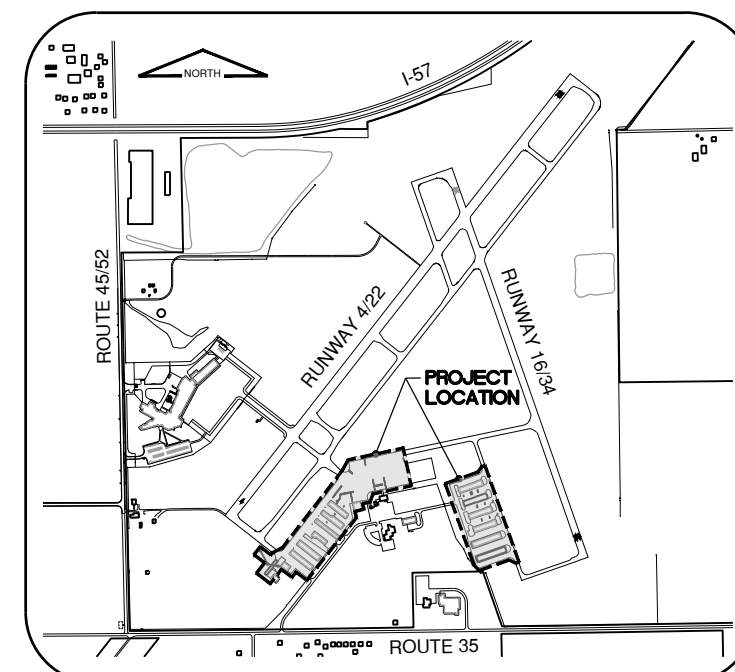
KANKAKEE VALLEY
KVAA
 AIRPORT AUTHORITY

APPROVED BY *[Signature]*
 JEFF BENOIT - AIRPORT MANAGER

DATE 6-4-25



LOCATION MAP



SITE PLAN

| |
|--|
| RUNWAY 4/22 |
| DESIGN AIRCRAFT APPROACH CATEGORY D |
| RUNWAY 16/34 |
| DESIGN AIRCRAFT APPROACH CATEGORY B |
| TAXIWAY A, A2, B, A3, A4, A5, D, H |
| TAXIWAY DESIGN GROUP 2A AND 2B |
| TAXILANE - SOUTHWEST QUADRANT |
| TAXIWAY DESIGN GROUP 1A AND 1B |
| KANKAKEE VALLEY AIRPORT AUTHORITY GREATER KANKAKEE AIRPORT |
| SECTION: 21 COUNTY: KANKAKEE RANGE: R 12 E TOWNSHIP: T 30 N |
| UNICOM RADIO FREQUENCY - 123.0 |



CONSULTANTS

IL PROJECT: IKK-5084
 IL LETTING ITEM: 04A
 IL CONTRACT NO.: KA055

RECONSTRUCT SW
 QUADRANT APRON;
 TERMINAL AND SE HANGAR
 APRON PAVEMENT REPAIRS

JUNE 6, 2025

OWNER



GREATER KANKAKEE AIRPORT
 KANKAKEE, ILLINOIS

| MARK | DATE | DESCRIPTION |
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SHEET TITLE
**SUMMARY OF
 QUANTITIES/INDEX
 TO SHEETS**

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- 2 SUMMARY OF QUANTITIES/INDEX TO SHEETS
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- 4 CONSTRUCTION SAFETY AND PHASING PLAN - 1
- 5 CONSTRUCTION SAFETY AND PHASING PLAN - 2
- 6 CONSTRUCTION SAFETY AND PHASING PLAN - 3
- 7 CONSTRUCTION SAFETY AND PHASING PLAN - 4
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STOCKPILE NOTES

1. THE GENERAL LAYOUT OF THE EARTH STOCKPILE TO ACCOMMODATE THE ESTIMATED QUANTITY OF EXCESS EARTH MATERIAL IS SHOWN. THE DIMENSIONS MAY BE MODIFIED BY THE RESIDENT ENGINEER.
2. THE HAULING, PLACEMENT, GRADING OF THE UNCLASSIFIED EXCAVATION, TOPSOIL PLACEMENT, EARTH STORM SEWER SPOILS, TOPSOIL STRIPPING AND VOLUME CONTROL SITE EXCAVATION SPOILS SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE PAY ITEM ASSOCIATED WITH SAID WORK. SILT FENCE, SEEDING AND MULCHING ARE THE ONLY PAY ITEMS FOR THE EARTH STOCKPILE WORK.
3. THE FILL HEIGHT OF THE EARTH STOCKPILE SHALL BE A MAXIMUM OF 3'. THE PERIMETER SIDE SLOPES SHALL BE 7H:1V MAXIMUM. THE TOP OF THE BERM SHALL BE PEAKED IN THE CENTER AND DRAIN TO THE PERIMETER AT A 2.0% SLOPE.
4. EARTH MATERIALS SHALL BE SEPARATED BY TOPSOIL AND CLAY MATERIALS AS DIRECTED BY THE RESIDENT ENGINEER.
5. CONTRACTOR SHALL PLACE A 4" MINIMUM OF TOPSOIL AS NECESSARY TO ESTABLISH TURF.
6. CONTRACTOR'S HAUL ROAD RESTORATION TO ORIGINAL CONDITION SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

GENERAL NOTES

1. THE EXISTING PAVEMENT STRUCTURE WAS DESIGNED FOR EXCLUSIVE USE BY SMALL AIRCRAFT. THE CONTRACTOR SHALL TAKE THE NECESSARY PRECAUTIONS TO PROTECT THE PAVEMENT STRUCTURE AND SUBGRADE FROM DAMAGE, WHICH MAY INCLUDE BUT NOT BE LIMITED TO USE OF TRACKED EQUIPMENT, SHORT HAUL TRUCKS OR TRACKED PAVERS, AT NO ADDITIONAL COST TO CONTRACT.
2. AT ALL TIMES THE CONTRACTOR SHALL PERFORM ALL MAINTENANCE WORK NECESSARY TO KEEP EACH NEWLY CONSTRUCTED PAVEMENT SECTION LAYER IN A SATISFACTORY CONDITION.
3. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL DAMAGE DONE BY HIS HAULING AND CONSTRUCTION EQUIPMENT. ANY WORK NECESSARY TO CORRECT DAMAGED WORK AND EXISTING PAVEMENT SHALL BE PERFORMED BY THE CONTRACTOR AND AT THE EXPENSE OF THE CONTRACTOR.
4. THE RESIDENT ENGINEER IN CONSULTATION WITH THE AIRPORT SHALL IDENTIFY ALL CRACKS TO BE REPAIRED. CONTRACTOR SHALL ADJUST THEIR OPERATIONS AS NECESSARY FOR THE IDENTIFIED WORK. NO ADDITIONAL COMPENSATION SHALL BE MADE.
5. CONTRACTOR SHALL DISPOSE OF ALL PAVEMENT REMOVAL AND OTHER MISCELLANEOUS CONSTRUCTION DEBRIS OFF OF AIRPORT PROPERTY.
6. PAVEMENT REMOVAL AND REPLACEMENT AREAS SHALL BE LAID OUT BY THE RESIDENT ENGINEER IN THE FIELD DURING CONSTRUCTION. RESIDENT ENGINEER MAY REVISE LAYOUT AREAS AND/OR ADD NEW AREAS AS NECESSARY TO USE AVAILABLE PAVEMENT REMOVAL AND REPLACEMENT QUANTITIES.

SUMMARY OF QUANTITIES

| ITEM | DESCRIPTION | FEDERAL/STATE/LOCAL ESTIMATED QUANTITY | NPE FUNDS ONLY ESTIMATED QUANTITY | TOTAL ESTIMATED QUANTITY | UNIT |
|----------|------------------------------------|--|-----------------------------------|--------------------------|-------|
| AR125100 | ELEVATED RETROREFLECTIVE MARKER | 8 | 0 | 8 | EACH |
| AR150510 | ENGINEER'S FIELD OFFICE | 1 | 0 | 1 | L SUM |
| AR150520 | MOBILIZATION | 1 | 0 | 1 | L SUM |
| AR152410 | UNCLASSIFIED EXCAVATION | 1140 | 30 | 1,170 | CU YD |
| AR152540 | SOIL STABILIZATION FABRIC | 610 | 30 | 640 | SQ YD |
| AR156510 | SILT FENCE | 1090 | 0 | 1,090 | FOOT |
| AR156520 | INLET PROTECTION | 3 | 0 | 3 | EACH |
| AR156531 | EROSION CONTROL BLANKET | 490 | 0 | 490 | SQ YD |
| AR156540 | RIPRAP | 50 | 0 | 50 | SQ YD |
| AR208515 | POROUS GRANULAR EMBANKMENT | 250 | 30 | 280 | CU YD |
| AR209611 | CRUSHED AGG. BASE COURSE - 11" | 680 | 30 | 710 | SQ YD |
| AR401610 | BITUMINOUS SURFACE COURSE | 190 | 0 | 190 | TON |
| AR401900 | REMOVE BITUMINOUS PAVEMENT | 650 | 0 | 650 | SQ YD |
| AR401915 | REM & REP BIT PAVEMENT - TYPE A | 0 | 300 | 300 | SQ YD |
| AR401916 | REM & REP BIT PAVEMENT - TYPE B | 0 | 530 | 530 | SQ YD |
| AR403673 | REFLECTIVE CRACK CONTROL TREATMENT | 0 | 300 | 300 | SQ YD |
| AR510510 | TIE DOWN | 0 | 5 | 5 | EACH |
| AR510900 | REMOVE TIE DOWN | 0 | 5 | 5 | EACH |
| AR602510 | BITUMINOUS PRIME COAT | 195 | 0 | 195 | GAL |
| AR603510 | BITUMINOUS TACK COAT | 50 | 0 | 50 | GAL |
| AR605510 | JOINT SEALING FILLER | 0 | 300 | 300 | LF |
| AR620520 | PAVEMENT MARKING - WATERBORNE | 310 | 100 | 410 | SQ FT |
| AR620525 | PAVEMENT MARKING - BLACK BORDER | 310 | 100 | 410 | SQ FT |
| AR701512 | 12" RCP, CLASS IV | 240 | 0 | 240 | FOOT |
| AR705504 | 4" PERFORATED UNDERDRAIN | 279 | 0 | 279 | FOOT |
| AR752412 | PRECAST REINFORCED CONC. FES 12" | 4 | 0 | 4 | EACH |
| AR901510 | SEEDING | 0.75 | 0 | 0.75 | ACRE |
| AR908515 | HEAVY-DUTY HYDRAULIC MULCH | 0.75 | 0 | 0.75 | ACRE |

HMA MIXTURE REQUIREMENT TABLE

| ITEM | RUNWAY/TAXIWAY (60K + LBS.) NDES | PG BINDER | AGGREGATE QUALITY | MAX RAP | DENSITY ACCEPTANCE |
|--|----------------------------------|--------------|-------------------|---------|--|
| 401: HMA SURFACE | N50 @ 3.0% | SBS PG 70-28 | A | 0 (N/A) | MAINLINE: NUCLEAR GAUGE JOINTS: CORING |
| 401: HMA SURFACE (REM & REP PAV'T) | N50 @ 3.0% | SBS PG 70-28 | A | 0 (N/A) | PATCHING: NUCLEAR GAUGE |
| 401: HMA SURFACE (APRON REM & REP PAV'T) | N50 @ 3.0% | SBS PG 76-28 | A | 0 (N/A) | PATCHING: NUCLEAR GAUGE |
| 401: HMA SURFACE (LOWER LIFTS) | N50 @ 3.0% | SBS PG 64-22 | A | 0 (N/A) | MAINLINE: NUCLEAR GAUGE JOINTS: CORING PATCHING: NUCLEAR GAUGE |



CONSULTANTS

IL PROJECT: IKK-5084
IL LETTING ITEM: 04A
IL CONTRACT NO.: KA055

RECONSTRUCT SW
QUADRANT APRON;
TERMINAL AND SE HANGAR
APRON PAVEMENT REPAIRS

JUNE 6, 2025

OWNER



GREATER KANKAKEE AIRPORT
KANKAKEE, ILLINOIS

| MARK | DATE | DESCRIPTION |
|---------------|---|-------------|
| PROJECT NO: | 23005642.00 | |
| CAD DWG FILE: | IKK5084_23005642.00_GI_4_SITE PLAN_01.DWG | |
| DESIGNED BY: | CMJ | |
| DRAWN BY: | JRO | |
| CHECKED BY: | DKP | |
| APPROVED BY: | DKP | |
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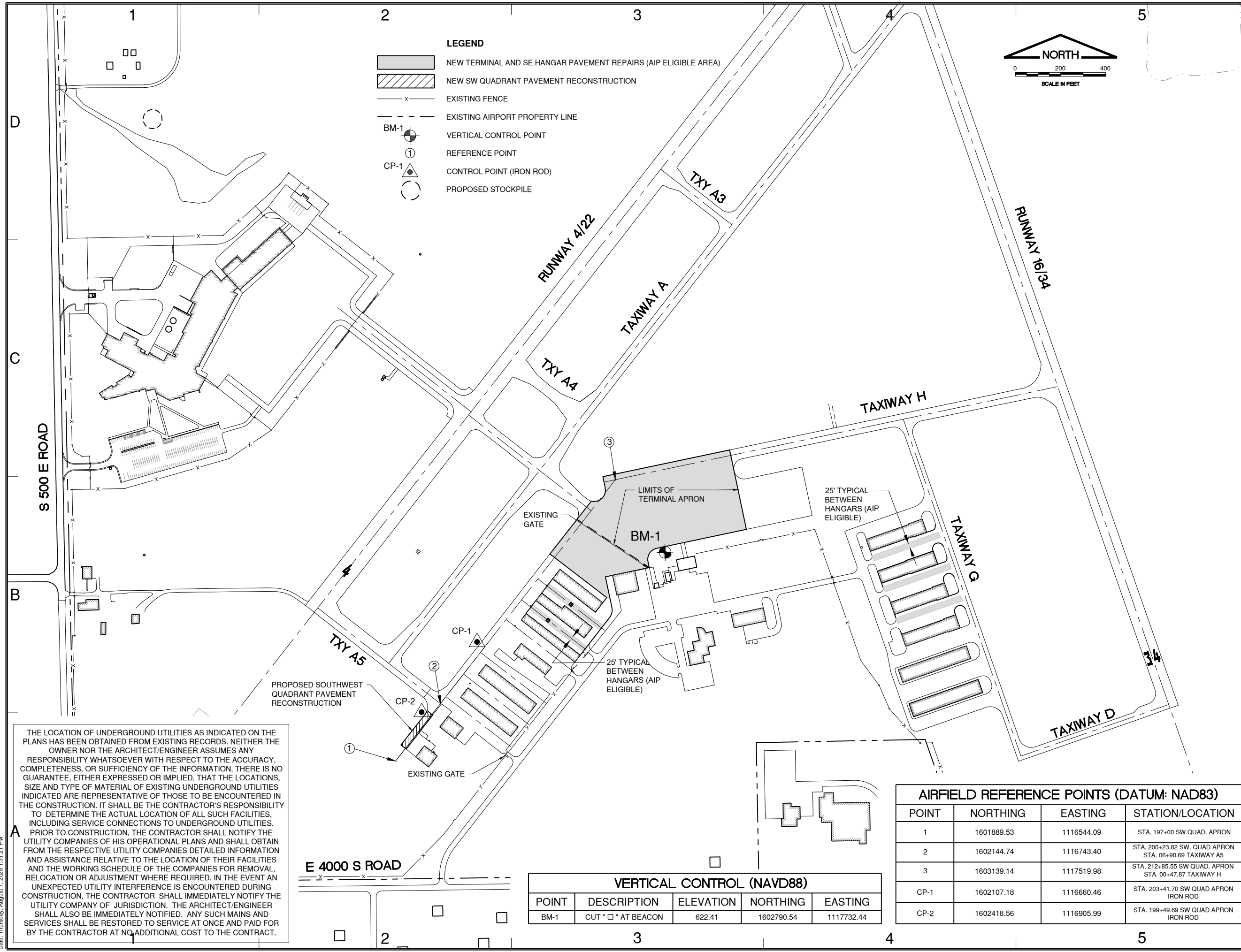
SHEET TITLE
**SITE PLAN/PROJECT
CONTROL PLAN**

SHEET 3 OF 26



LEGEND

- NEW TERMINAL AND SE HANGAR PAVEMENT REPAIRS (AIP ELIGIBLE AREA)
- NEW SW QUADRANT PAVEMENT RECONSTRUCTION
- EXISTING FENCE
- EXISTING AIRPORT PROPERTY LINE
- BM-1 VERTICAL CONTROL POINT
- REFERENCE POINT
- CP-1 CONTROL POINT (IRON ROD)
- PROPOSED STOCKPILE



THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ARCHITECT/ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER WITH RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED IN THE CONSTRUCTION. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANIES OF HIS OPERATIONAL PLANS AND SHALL OBTAIN FROM THE RESPECTIVE UTILITY COMPANIES DETAILED INFORMATION AND ASSISTANCE RELATIVE TO THE LOCATION OF THEIR FACILITIES AND THE WORKING SCHEDULE OF THE COMPANIES FOR REMOVAL, RELOCATION OR ADJUSTMENT WHERE REQUIRED. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY OF JURISDICTION. THE ARCHITECT/ENGINEER SHALL ALSO BE IMMEDIATELY NOTIFIED. ANY SUCH MAINS AND SERVICES SHALL BE RESTORED TO SERVICE AT ONCE AND PAID FOR BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.

| POINT | DESCRIPTION | ELEVATION | NORTHING | EASTING |
|-------|---------------------|-----------|------------|------------|
| BM-1 | CUT " □ " AT BEACON | 622.41 | 1602790.54 | 1117732.44 |

| POINT | NORTHING | EASTING | STATION/LOCATION |
|-------|------------|------------|---|
| 1 | 1601889.53 | 1116544.09 | STA. 197+00 SW QUAD. APRON |
| 2 | 1602144.74 | 1116743.40 | STA. 200+23.82 SW. QUAD APRON STA. 06+90.69 TAXIWAY A5 |
| 3 | 1603139.14 | 1117519.98 | STA. 212+85.55 SW QUAD. APRON STA. 00+47.87 TAXIWAY H |
| CP-1 | 1602107.18 | 1116660.46 | STA. 203+41.70 SW QUAD APRON IRON ROD |
| CP-2 | 1602418.56 | 1116905.99 | STA. 199+49.69 SW QUAD APRON IRON ROD |

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 Date: Thursday, August 7, 2025 1:31:21 PM

PHASING NOTES

1. NOTIFY RESIDENT ENGINEER/AIRPORT MANAGER A MINIMUM OF 14 DAYS PRIOR TO START OF CONSTRUCTION TO ISSUE APPROPRIATE NOTAMS.
2. CONTRACTOR TO INSTALL TEMPORARY STOP SIGN. CONTRACTOR TO STOP AND YIELD TO TRAVELING AIRCRAFT AT ALL TIMES.
3. THE CONTRACTOR SHALL ENTER AND EXIT THE FIELD AT THE LOCATIONS SHOWN DEPENDING ON THE WORK AREA. AT NO TIME SHALL THE CONTRACTOR USE RUNWAYS OR TAXIWAYS AS A HAUL ROUTE.
4. ANY DAMAGE CAUSED BY CONTRACTOR HAUL OPERATIONS OVER EXISTING AIRFIELD PAVEMENTS SHALL BE REPAIRED BY THE CONTRACTOR AT NO COST TO THE CONTRACT.
5. SEE THE SEQUENCE OF CONSTRUCTION GENERAL NOTES AND DETAILS SHEETS FOR ADDITIONAL REQUIREMENTS.
6. ALL WORK IN THIS PHASE WILL REQUIRE ONE (1) ESCORT WHEN CONSTRUCTION TRAFFIC IS ENTERING THE AIRFIELD.

PHASE 1: SUGGESTED SEQUENCE OF CONSTRUCTION

- COORDINATE APRON AND TAXIWAY CLOSURE WITH THE RESIDENT ENGINEER.
- COVER TAXIWAY A5 LIGHTS/SIGNS WITHIN CLOSED TAXIWAY AREA.
- PLACE BARRICADES FOR TAXIWAY CLOSURES.
- CONSTRUCT IMPROVEMENTS INCLUDING PAVEMENT REHABILITATION, REFLECTOR INSTALLATION DRAINAGE INSTALLATION, GRADING, PAVEMENT MARKING AND LANDSCAPING.
- CONTRACTOR MUST WAIT 30 DAYS AFTER PAVEMENT CONSTRUCTION TO APPLY SECOND COAT OF PAVEMENT MARKING.
- CLEAN AIRFIELD PAVEMENTS/REMOVE BARRICADES.
- OPEN PAVEMENTS.

LEGEND

- PROPOSED PHASE 1 WORK AREA
- CONTRACTOR'S STAGING AND STORAGE AREA
- CONTRACTOR'S EMPLOYEE PARKING
- RSA RUNWAY SAFETY AREA
- TOFA TAXIWAY OBJECT FREE AREA
- LOW PROFILE BARRICADES
- FAA CRITICAL POINT
- AIRCRAFT MOVEMENT AREA
- CONTRACTOR'S ACCESS/HAUL ROUTE
- CROSSING ESCORT AND TEMPORARY "STOP" AND "AIRCRAFT MOVEMENT AREA" SIGN



CONSULTANTS

IL PROJECT: IKK-5084
IL LETTING ITEM: 04A
IL CONTRACT NO.: KA055

RECONSTRUCT SW QUADRANT APRON; TERMINAL AND SE HANGAR APRON PAVEMENT REPAIRS

JUNE 6, 2025

OWNER



**GREATER KANKAKEE AIRPORT
KANKAKEE, ILLINOIS**

MARK DATE DESCRIPTION

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|---------------|---------------------------------------|
| PROJECT NO: | 23005642.00 |
| CAD DWG FILE: | IKK5084_23005642.00_CD_1_CSPP - 1.DWG |
| DESIGNED BY: | CMJ |
| DRAWN BY: | JRO |
| CHECKED BY: | DKP |
| APPROVED BY: | DKP |
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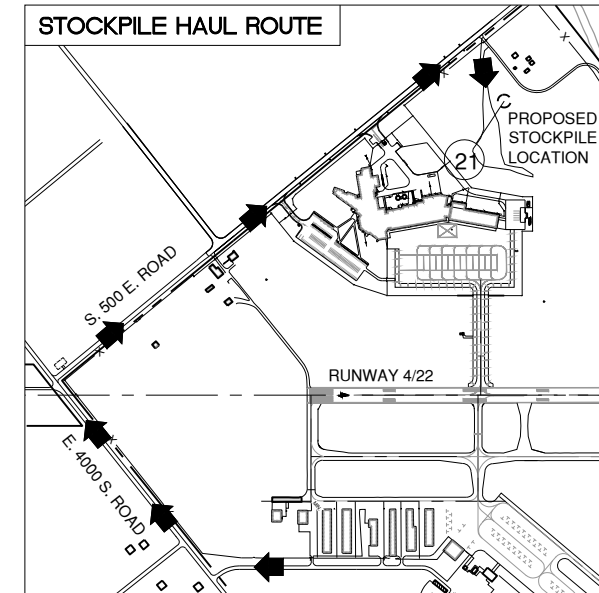
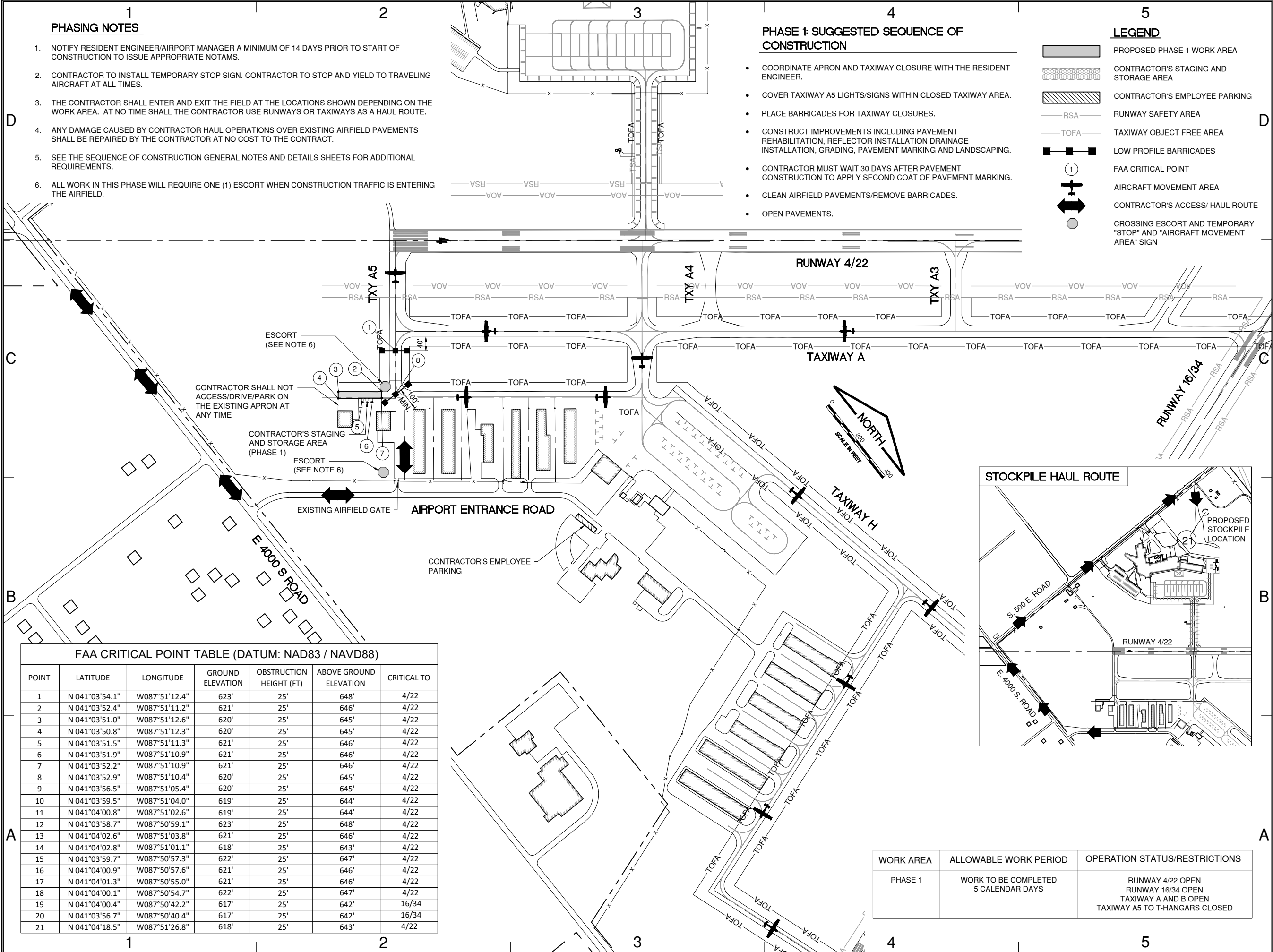
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CONSTRUCTION SAFETY AND PHASING PLAN - 1

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Date: Thursday, August 7, 2025 1:31:22 PM

FAA CRITICAL POINT TABLE (DATUM: NAD83 / NAVD88)

| POINT | LATITUDE | LONGITUDE | GROUND ELEVATION | OBSTRUCTION HEIGHT (FT) | ABOVE GROUND ELEVATION | CRITICAL TO |
|-------|----------------|---------------|------------------|-------------------------|------------------------|-------------|
| 1 | N 041°03'54.1" | W087°51'12.4" | 623' | 25' | 648' | 4/22 |
| 2 | N 041°03'52.4" | W087°51'11.2" | 621' | 25' | 646' | 4/22 |
| 3 | N 041°03'51.0" | W087°51'12.6" | 620' | 25' | 645' | 4/22 |
| 4 | N 041°03'50.8" | W087°51'12.3" | 620' | 25' | 645' | 4/22 |
| 5 | N 041°03'51.5" | W087°51'11.3" | 621' | 25' | 646' | 4/22 |
| 6 | N 041°03'51.9" | W087°51'10.9" | 621' | 25' | 646' | 4/22 |
| 7 | N 041°03'52.2" | W087°51'10.9" | 621' | 25' | 646' | 4/22 |
| 8 | N 041°03'52.9" | W087°51'10.4" | 620' | 25' | 645' | 4/22 |
| 9 | N 041°03'56.5" | W087°51'05.4" | 620' | 25' | 645' | 4/22 |
| 10 | N 041°03'59.5" | W087°51'04.0" | 619' | 25' | 644' | 4/22 |
| 11 | N 041°04'00.8" | W087°51'02.6" | 619' | 25' | 644' | 4/22 |
| 12 | N 041°03'58.7" | W087°50'59.1" | 623' | 25' | 648' | 4/22 |
| 13 | N 041°04'02.6" | W087°51'03.8" | 621' | 25' | 646' | 4/22 |
| 14 | N 041°04'02.8" | W087°51'01.1" | 618' | 25' | 643' | 4/22 |
| 15 | N 041°03'59.7" | W087°50'57.3" | 622' | 25' | 647' | 4/22 |
| 16 | N 041°04'00.9" | W087°50'57.6" | 621' | 25' | 646' | 4/22 |
| 17 | N 041°04'01.3" | W087°50'55.0" | 621' | 25' | 646' | 4/22 |
| 18 | N 041°04'00.1" | W087°50'54.7" | 622' | 25' | 647' | 4/22 |
| 19 | N 041°04'00.4" | W087°50'42.2" | 617' | 25' | 642' | 16/34 |
| 20 | N 041°03'56.7" | W087°50'40.4" | 617' | 25' | 642' | 16/34 |
| 21 | N 041°04'18.5" | W087°51'26.8" | 618' | 25' | 643' | 4/22 |



| WORK AREA | ALLOWABLE WORK PERIOD | OPERATION STATUS/RESTRICTIONS |
|-----------|--------------------------------------|---|
| PHASE 1 | WORK TO BE COMPLETED 5 CALENDAR DAYS | RUNWAY 4/22 OPEN RUNWAY 16/34 OPEN TAXIWAY A AND B OPEN TAXIWAY A5 TO T-HANGARS CLOSED |

| WORK AREA | ALLOWABLE WORK PERIOD | OPERATION STATUS/RESTRICTIONS |
|-----------|---|---|
| PHASE 2A | WORK TO BE COMPLETED 5 CALENDAR DAYS | RUNWAY 4/22 OPEN RUNWAY 16/34 OPEN TAXIWAY A, B, AND H OPEN T-HANGAR TAXILANES CLOSED AS SHOWN |
| PHASE 2B | WORK TO BE COMPLETED 5 CALENDAR DAYS | RUNWAY 4/22 OPEN RUNWAY 16/34 OPEN TAXIWAY A AND B OPEN T-HANGAR TAXILANE TO TERMINAL CLOSED |



CONSULTANTS

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GREATER KANKAKEE AIRPORT
KANKAKEE, ILLINOIS

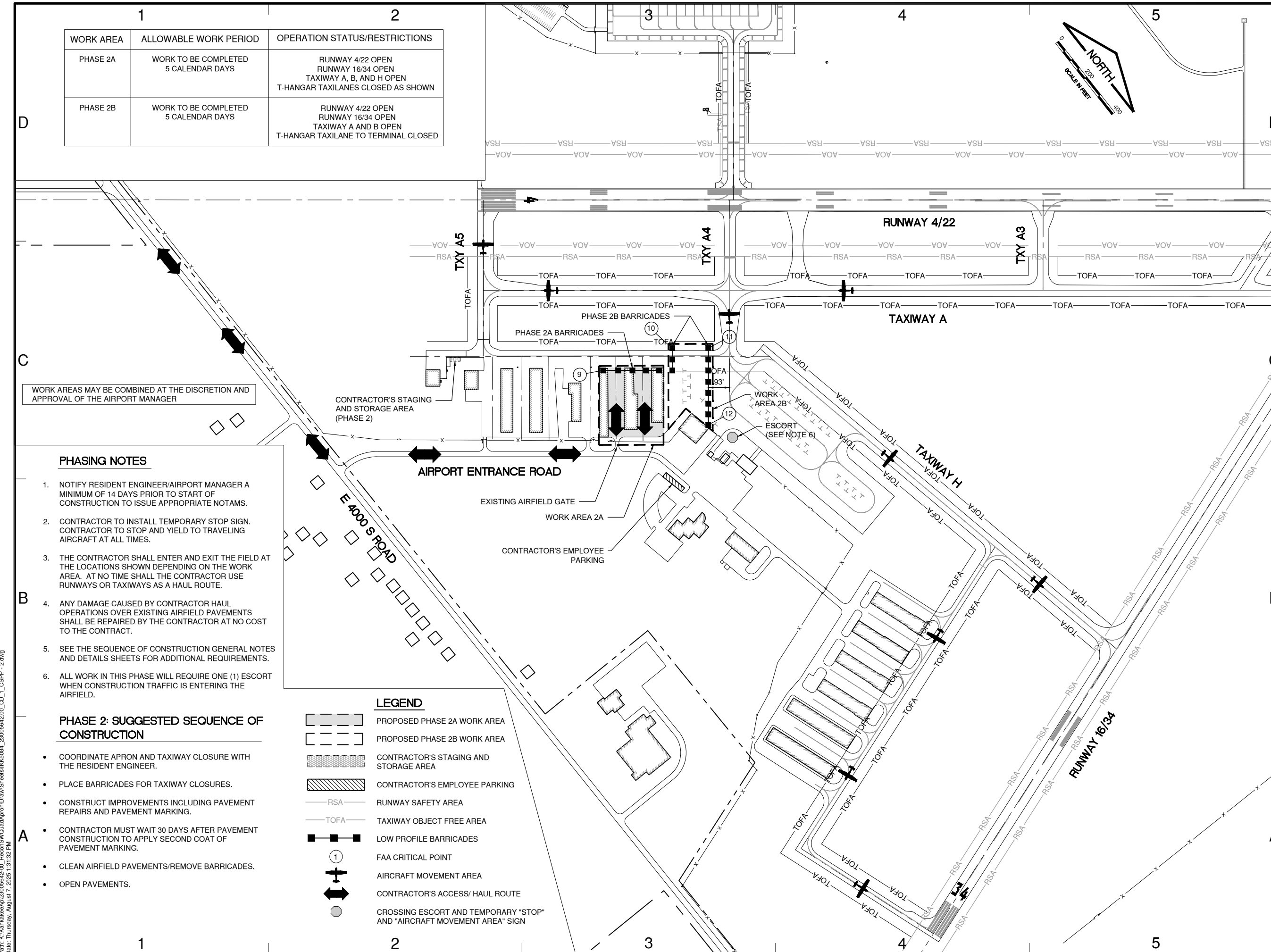
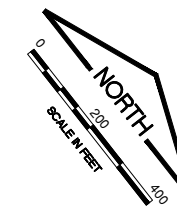
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|---------------|---------------------------------------|
| PROJECT NO: | 23005642.00 |
| CAD DWG FILE: | IKK5084_23005642.00_CD_1_CSPP - 2.DWG |
| DESIGNED BY: | CMJ |
| DRAWN BY: | JRO |
| CHECKED BY: | DKP |
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SHEET TITLE

CONSTRUCTION SAFETY AND PHASING PLAN - 2

SHEET 5 OF 26



WORK AREAS MAY BE COMBINED AT THE DISCRETION AND APPROVAL OF THE AIRPORT MANAGER

CONTRACTOR'S STAGING AND STORAGE AREA (PHASE 2)

WORK AREA 2B
ESCORT (SEE NOTE 6)

PHASING NOTES

1. NOTIFY RESIDENT ENGINEER/AIRPORT MANAGER A MINIMUM OF 14 DAYS PRIOR TO START OF CONSTRUCTION TO ISSUE APPROPRIATE NOTAMS.
2. CONTRACTOR TO INSTALL TEMPORARY STOP SIGN. CONTRACTOR TO STOP AND YIELD TO TRAVELING AIRCRAFT AT ALL TIMES.
3. THE CONTRACTOR SHALL ENTER AND EXIT THE FIELD AT THE LOCATIONS SHOWN DEPENDING ON THE WORK AREA. AT NO TIME SHALL THE CONTRACTOR USE RUNWAYS OR TAXIWAYS AS A HAUL ROUTE.
4. ANY DAMAGE CAUSED BY CONTRACTOR HAUL OPERATIONS OVER EXISTING AIRFIELD PAVEMENTS SHALL BE REPAIRED BY THE CONTRACTOR AT NO COST TO THE CONTRACT.
5. SEE THE SEQUENCE OF CONSTRUCTION GENERAL NOTES AND DETAILS SHEETS FOR ADDITIONAL REQUIREMENTS.
6. ALL WORK IN THIS PHASE WILL REQUIRE ONE (1) ESCORT WHEN CONSTRUCTION TRAFFIC IS ENTERING THE AIRFIELD.

PHASE 2: SUGGESTED SEQUENCE OF CONSTRUCTION

- COORDINATE APRON AND TAXIWAY CLOSURE WITH THE RESIDENT ENGINEER.
- PLACE BARRICADES FOR TAXIWAY CLOSURES.
- CONSTRUCT IMPROVEMENTS INCLUDING PAVEMENT REPAIRS AND PAVEMENT MARKING.
- CONTRACTOR MUST WAIT 30 DAYS AFTER PAVEMENT CONSTRUCTION TO APPLY SECOND COAT OF PAVEMENT MARKING.
- CLEAN AIRFIELD PAVEMENTS/REMOVE BARRICADES.
- OPEN PAVEMENTS.

LEGEND

- PROPOSED PHASE 2A WORK AREA
- PROPOSED PHASE 2B WORK AREA
- CONTRACTOR'S STAGING AND STORAGE AREA
- CONTRACTOR'S EMPLOYEE PARKING
- RSA RUNWAY SAFETY AREA
- TOFA TAXIWAY OBJECT FREE AREA
- LOW PROFILE BARRICADES
- FAA CRITICAL POINT
- AIRCRAFT MOVEMENT AREA
- CONTRACTOR'S ACCESS/ HAUL ROUTE
- CROSSING ESCORT AND TEMPORARY "STOP" AND "AIRCRAFT MOVEMENT AREA" SIGN

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Date: Thursday, August 7, 2025 1:31:32 PM

CONSULTANTS

IL PROJECT: IKK-5084
 IL LETTING ITEM: 04A
 IL CONTRACT NO.: KA055

RECONSTRUCT SW QUADRANT APRON; TERMINAL AND SE HANGAR APRON PAVEMENT REPAIRS

JUNE 6, 2025

OWNER



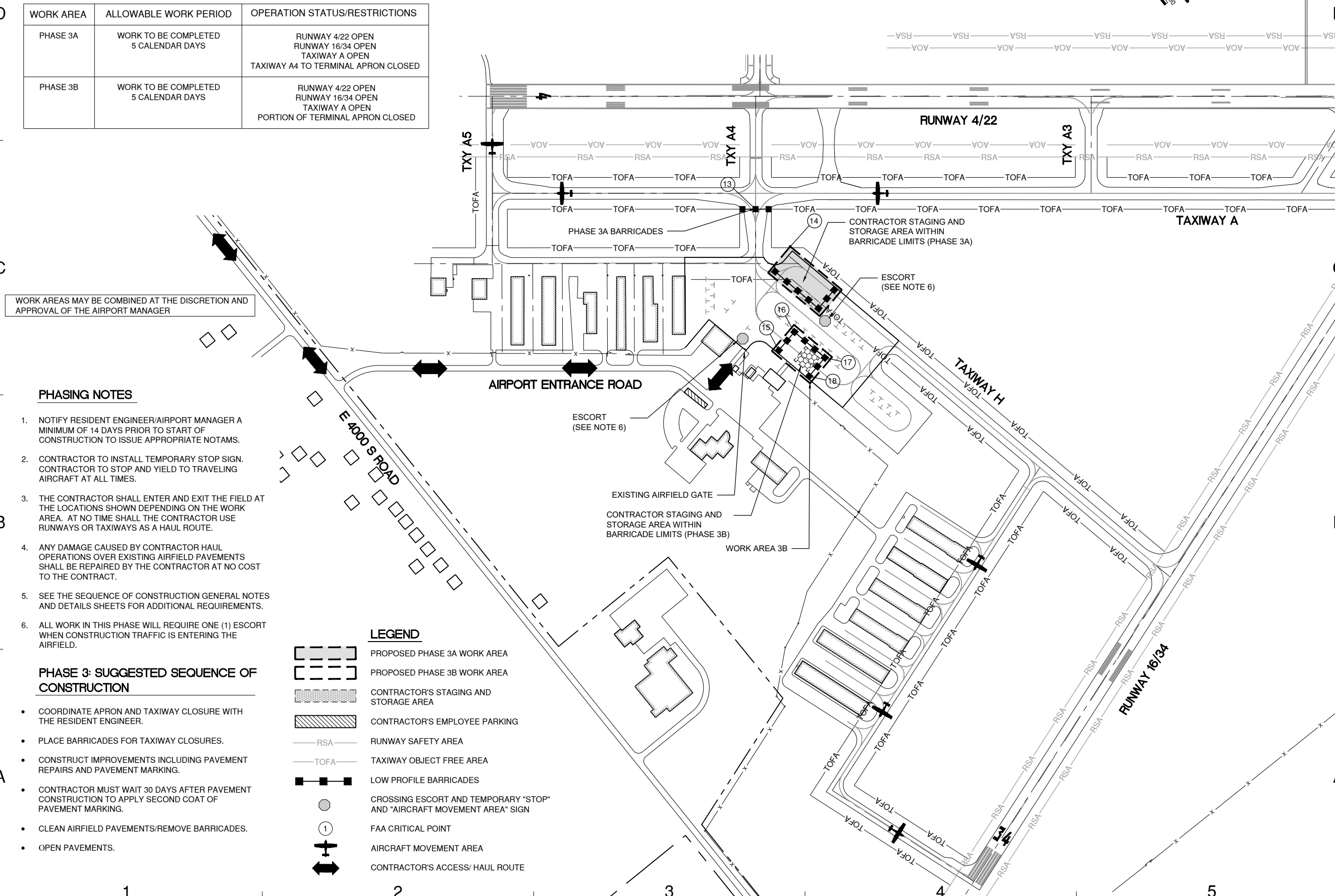
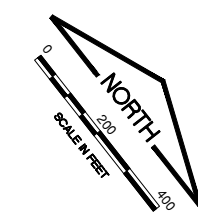
GREATER KANKAKEE AIRPORT
 KANKAKEE, ILLINOIS

MARK DATE DESCRIPTION

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| PROJECT NO: | 23005642.00 |
| CAD DWG FILE: | IKK5084_23005642.00_CD_1_CSPP - 3.DWG |
| DESIGNED BY: | CMJ |
| DRAWN BY: | JRO |
| CHECKED BY: | -- |
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SHEET TITLE
CONSTRUCTION SAFETY AND PHASING PLAN - 3

| WORK AREA | ALLOWABLE WORK PERIOD | OPERATION STATUS/RESTRICTIONS |
|-----------|--------------------------------------|--|
| PHASE 3A | WORK TO BE COMPLETED 5 CALENDAR DAYS | RUNWAY 4/22 OPEN RUNWAY 16/34 OPEN TAXIWAY A OPEN TAXIWAY A4 TO TERMINAL APRON CLOSED |
| PHASE 3B | WORK TO BE COMPLETED 5 CALENDAR DAYS | RUNWAY 4/22 OPEN RUNWAY 16/34 OPEN TAXIWAY A OPEN PORTION OF TERMINAL APRON CLOSED |



WORK AREAS MAY BE COMBINED AT THE DISCRETION AND APPROVAL OF THE AIRPORT MANAGER

PHASING NOTES

1. NOTIFY RESIDENT ENGINEER/AIRPORT MANAGER A MINIMUM OF 14 DAYS PRIOR TO START OF CONSTRUCTION TO ISSUE APPROPRIATE NOTAMS.
2. CONTRACTOR TO INSTALL TEMPORARY STOP SIGN. CONTRACTOR TO STOP AND YIELD TO TRAVELING AIRCRAFT AT ALL TIMES.
3. THE CONTRACTOR SHALL ENTER AND EXIT THE FIELD AT THE LOCATIONS SHOWN DEPENDING ON THE WORK AREA. AT NO TIME SHALL THE CONTRACTOR USE RUNWAYS OR TAXIWAYS AS A HAUL ROUTE.
4. ANY DAMAGE CAUSED BY CONTRACTOR HAUL OPERATIONS OVER EXISTING AIRFIELD PAVEMENTS SHALL BE REPAIRED BY THE CONTRACTOR AT NO COST TO THE CONTRACT.
5. SEE THE SEQUENCE OF CONSTRUCTION GENERAL NOTES AND DETAILS SHEETS FOR ADDITIONAL REQUIREMENTS.
6. ALL WORK IN THIS PHASE WILL REQUIRE ONE (1) ESCORT WHEN CONSTRUCTION TRAFFIC IS ENTERING THE AIRFIELD.

PHASE 3: SUGGESTED SEQUENCE OF CONSTRUCTION

- COORDINATE APRON AND TAXIWAY CLOSURE WITH THE RESIDENT ENGINEER.
- PLACE BARRICADES FOR TAXIWAY CLOSURES.
- CONSTRUCT IMPROVEMENTS INCLUDING PAVEMENT REPAIRS AND PAVEMENT MARKING.
- CONTRACTOR MUST WAIT 30 DAYS AFTER PAVEMENT CONSTRUCTION TO APPLY SECOND COAT OF PAVEMENT MARKING.
- CLEAN AIRFIELD PAVEMENTS/REMOVE BARRICADES.
- OPEN PAVEMENTS.

LEGEND

- PROPOSED PHASE 3A WORK AREA
- PROPOSED PHASE 3B WORK AREA
- CONTRACTOR'S STAGING AND STORAGE AREA
- CONTRACTOR'S EMPLOYEE PARKING
- RUNWAY SAFETY AREA
- TAXIWAY OBJECT FREE AREA
- LOW PROFILE BARRICADES
- CROSSING ESCORT AND TEMPORARY "STOP" AND "AIRCRAFT MOVEMENT AREA" SIGN
- FAA CRITICAL POINT
- AIRCRAFT MOVEMENT AREA
- CONTRACTOR'S ACCESS/HAUL ROUTE

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CONSULTANTS

IL PROJECT: IKK-5084
IL LETTING ITEM: 04A
IL CONTRACT NO.: KA055

RECONSTRUCT SW
QUADRANT APRON;
TERMINAL AND SE HANGAR
APRON PAVEMENT REPAIRS

JUNE 6, 2025

OWNER



GREATER KANKAKEE AIRPORT
KANKAKEE, ILLINOIS

MARK DATE DESCRIPTION

PROJECT NO: 23005642.00
CAD DWG FILE: IKK5084_23005642.00_CD_1_CSPP - 4.DWG
DESIGNED BY: CMJ
DRAWN BY: JRO
CHECKED BY: DKP
APPROVED BY: DKP
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SHEET TITLE

CONSTRUCTION SAFETY
AND PHASING PLAN - 4

SHEET 7 OF 26

PHASE 4 NOTES

1. NOTIFY RESIDENT ENGINEER/AIRPORT MANAGER A MINIMUM OF 14 DAYS PRIOR TO START OF CONSTRUCTION TO ISSUE APPROPRIATE NOTAMS.

PHASE 4: SUGGESTED SEQUENCE OF CONSTRUCTION

- COORDINATE APRON AND TAXIWAY CLOSURE WITH THE RESIDENT ENGINEER
- PLACE BARRICADES FOR TAXIWAY CLOSURES
- CONSTRUCT IMPROVEMENTS INCLUDING PAVEMENT REHABILITATION AND EDGE LIGHT INSTALLATION.

WORK AREAS MAY BE COMBINED AT THE DISCRETION AND APPROVAL OF THE AIRPORT MANAGER

LEGEND

- PROPOSED PHASE 4 WORK AREA
- CONTRACTOR'S STAGING AND STORAGE AREA
- CONTRACTOR'S EMPLOYEE PARKING
- RSA RUNWAY SAFETY AREA
- TOFA TAXIWAY OBJECT FREE AREA
- LOW PROFILE BARRICADES
- FAA CRITICAL POINT
- AIRCRAFT MOVEMENT AREA
- CONTRACTOR'S ACCESS/HAUL ROUTE

PHASING NOTES

1. NOTIFY RESIDENT ENGINEER/AIRPORT MANAGER A MINIMUM OF 14 DAYS PRIOR TO START OF CONSTRUCTION TO ISSUE APPROPRIATE NOTAMS.
2. CONTRACTOR TO INSTALL TEMPORARY STOP SIGN. CONTRACTOR TO STOP AND YIELD TO TRAVELING AIRCRAFT AT ALL TIMES.
3. THE CONTRACTOR SHALL ENTER AND EXIT THE FIELD AT THE LOCATIONS SHOWN DEPENDING ON THE WORK AREA. AT NO TIME SHALL THE CONTRACTOR USE RUNWAYS OR TAXIWAYS AS A HAUL ROUTE.
4. ANY DAMAGE CAUSED BY CONTRACTOR HAUL OPERATIONS OVER EXISTING AIRFIELD PAVEMENTS SHALL BE REPAIRED BY THE CONTRACTOR AT NO COST TO THE CONTRACT.
5. SEE THE SEQUENCE OF CONSTRUCTION GENERAL NOTES AND DETAILS SHEETS FOR ADDITIONAL REQUIREMENTS.
6. ALL WORK IN THIS PHASE WILL REQUIRE ONE (1) ESCORT WHEN CONSTRUCTION TRAFFIC IS ENTERING THE AIRFIELD.
7. PHASE 4 CAN BE PERFORMED CONCURRENTLY WITH PHASES 1, 2A/2B, OR 3A/3B

PHASE 4: SUGGESTED SEQUENCE OF CONSTRUCTION

- COORDINATE APRON AND TAXIWAY CLOSURE WITH THE RESIDENT ENGINEER.
- PLACE BARRICADES FOR TAXIWAY CLOSURES.
- CONSTRUCT IMPROVEMENTS INCLUDING PAVEMENT REPAIRS AND PAVEMENT MARKING.
- CONTRACTOR MUST WAIT 30 DAYS AFTER PAVEMENT CONSTRUCTION TO APPLY SECOND COAT OF PAVEMENT MARKING.
- CLEAN AIRFIELD PAVEMENTS/REMOVE BARRICADES.
- OPEN PAVEMENTS.

| WORK AREA | ALLOWABLE WORK PERIOD | OPERATION STATUS/RESTRICTIONS |
|-----------|--------------------------------------|---|
| PHASE 4 | WORK TO BE COMPLETED 5 CALENDAR DAYS | RUNWAY 4/22 OPEN RUNWAY 16/34 OPEN TAXIWAY A AND B OPEN TAXIWAY G PARTIALLY CLOSED |

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Date: Thursday, August 7, 2025 1:31:42 PM



CONSULTANTS

IL PROJECT: IKK-5084
IL LETTING ITEM: 04A
IL CONTRACT NO.: KA055

**RECONSTRUCT SW
QUADRANT APRON;
TERMINAL AND SE HANGAR
APRON PAVEMENT REPAIRS**

JUNE 6, 2025

OWNER



**GREATER KANKAKEE AIRPORT
KANKAKEE, ILLINOIS**

MARK DATE DESCRIPTION

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| PROJECT NO: | 23005642.00 |
| CAD DWG FILE: | IKK5084_23005642.00_CD_1_CSPP NOTES_01.DWG |
| DESIGNED BY: | CMJ |
| DRAWN BY: | JRO |
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**CONSTRUCTION SAFETY
AND PHASING PLAN
GENERAL NOTES - 1**

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FOR ANY EQUIPMENT USED BY THE CONTRACTOR WITH A HEIGHT GREATER THAN 25', THE CONTRACTOR SHALL PROVIDE TO THE AIRPORT THE TYPE OF EQUIPMENT, TOTAL HEIGHT, AND LOCATION WHERE THE EQUIPMENT WILL BE USED. THE AIRPORT WILL SUBMIT FAA FORM 7460-1 TO THE FAA FOR AN AIRSPACE STUDY. NO EQUIPMENT WITH A HEIGHT GREATER THAN 25' SHALL BE USED UNTIL A DETERMINATION FROM FAA IS RECEIVED.

5. IN THE EVENT OF AN EMERGENCY, THE CONTRACTOR SHALL CALL 911.

10. INSPECTION REQUIREMENTS

1. THE CONTRACTOR SHALL INSPECT THE JOBSITE DAILY TO ENSURE COMPLIANCE WITH THE CSPP. THE CHECKLIST FOUND IN APPENDIX 3 OF FAA AC 150/5370-2G OR LATEST EDITION MAY BE USED TO AID IN THE INSPECTIONS.

2. THE CONTRACTOR SHALL REQUEST OPERATIONAL INSPECTION OF EACH PHASE WORK AREA PRIOR THE AREA BEING REOPENED. THE AIRPORT WILL DETERMINE IF THE WORK AREA IS ALLOWED TO BE OPENED.

11. UNDERGROUND UTILITIES

1. COORDINATION BY THE CONTRACTOR WITH THE EXISTING UTILITIES SHALL BE COMPLETED BEFORE CONSTRUCTION IS STARTED. SEE SPECIAL PROVISIONS FOR SPECIFIC REQUIREMENTS. THE LOCATION OF UNDERGROUND UTILITIES AS INDICATED ON THE PLANS HAS BEEN OBTAINED FROM EXISTING RECORDS. NEITHER THE OWNER NOR THE ENGINEER ASSUMES ANY RESPONSIBILITY WHATSOEVER IN RESPECT TO THE ACCURACY, COMPLETENESS, OR SUFFICIENCY OF THE INFORMATION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION. THERE IS NO GUARANTEE, EITHER EXPRESSED OR IMPLIED, THAT THE LOCATIONS, SIZE AND TYPE OF MATERIAL OF EXISTING UNDERGROUND UTILITIES AS INDICATED ARE REPRESENTATIVE OF THOSE TO BE ENCOUNTERED DURING CONSTRUCTION.

2. SHOULD A UTILITY COMPANY OR GOVERNMENT AGENCY BE UNABLE TO LOCATE FACILITIES, THE CONTRACTOR SHALL LOCATE THESE FACILITIES. PAYMENT FOR THIS LOCATION SHALL BE INCIDENTAL TO THE IMPROVEMENTS REQUIRING THE LOCATIONS.

3. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE ACTUAL LOCATION OF ALL SUCH FACILITIES, INCLUDING SERVICE CONNECTIONS TO UNDERGROUND UTILITIES. PRIOR TO CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE UTILITY COMPANY/OWNER OF HIS OPERATIONAL PLANS. THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR DETAILED INFORMATION AND ASSISTANCE IN LOCATING UTILITIES. IN THE EVENT AN UNEXPECTED UTILITY INTERFERENCE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE UTILITY COMPANY, THE OWNER AND THE ENGINEER. ANY SUCH MAINS AND/OR SERVICES DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED IMMEDIATELY AT THEIR EXPENSE TO THE SATISFACTION OF THE OWNER AND THE ENGINEER.

LIMITATIONS ON CONSTRUCTION WITHIN AIRCRAFT OPERATIONS AREA (AOA) AND TAXIWAY/TAXILANE OBJECT FREE AREA (TOFA)

TAXIWAYS / TAXILANES:

ANY WORK WITHIN TAXIWAY / TAXILANE OBJECT FREE AREA (TOFA) WILL REQUIRE A TAXIWAY / TAXILANE CLOSURE. WORK WITHIN THE TOFA SHALL BE EXPEDITED. ANY DROP OFF SHALL BE ADEQUATELY LIGHTED, SIGNED AND BARRICADED. NO MATERIAL SHALL BE STOCKPILED WITHIN THE TOFA. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT TO ALLOW AIRCRAFT TO PASS, THEY SHALL DO SO AT NO EXTRA COST TO THE PROJECT. THE CONTRACTOR SHALL NOTIFY THE RESIDENT ENGINEER AND AIRPORT MANAGER FIVE (5) WORKING DAYS IN ADVANCE OF ANY PLANNED CONSTRUCTION WITHIN THESE LIMITS. REFERENCE TABLE ON PREVIOUS SHEET FOR OBJECT FREE AREA WIDTHS. NO DROP-OFFS OR OPEN EXCAVATIONS WILL BE ALLOWED WITHIN THE TAXIWAY / TAXILANE SAFETY AREAS OF OPEN TAXIWAYS / TAXILANES.

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10. ALL EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED AS A STAGING AREA BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE ENGINEER AND AIRPORT MANAGER. THE COST OF MAINTAINING, REPAIRING SEEDING /MULCHING OR CONSTRUCTING THESE PAVEMENTS / AREAS SHALL BE INCIDENTAL TO THE CONTRACT.

11. ALL VEHICLE AND EQUIPMENT OPERATORS USED BY THE CONTRACTOR SHALL BE PROPERLY TRAINED BY THE CONTRACTOR.

12. THE CONTRACTOR SHALL NOTIFY THE AIRPORT IF CONSTRUCTION ACTIVITY WILL REQUIRE THE BLOCKAGE OF EMERGENCY ACCESS TO THE AIRPORT.

13. THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE DUST CONTROL AT ALL TIMES DURING THE PROJECT DURATION. A WATER TRUCK SHALL BE REQUIRED TO BE ON SITE DURING ALL CONSTRUCTION OPERATION WORKING HOURS. PAYMENT FOR DUST CONTROL SHALL BE INCIDENTAL TO THE CONTRACT.

6. WILDLIFE MANAGEMENT

1. THE CONTRACTOR SHALL NOTIFY THE AIRPORT OR THE RESIDENT ENGINEER IF ANY WILDLIFE IS SEEN ENTERING THE AIRPORT.

2. CONTRACTOR ACCESS GATES SHALL REMAIN CLOSED WHEN THE CONTRACTOR IS NOT WORKING.

3. THE CONTRACTOR SHALL DISPOSE OF ALL TRASH INCLUDING FOOD SCRAPS IN APPROVED CONTRACTOR PROVIDED CONTAINERS.

7. FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT

1. THE CONTRACTOR SHALL PICK UP ANY FOREIGN OBJECT DEBRIS (FOD) SEEN ON THE AIRFIELD PAVEMENTS.

2. THE CONTRACTOR SHALL SECURE ALL LOOSE ITEMS FROM VEHICLES PRIOR TO DRIVING ON AIRFIELD PAVEMENTS.

3. THE CONTRACTOR SHALL KEEP ALL TRUCKS, EQUIPMENT AND MATERIALS OFF OF THE EXISTING PAVEMENT OUTSIDE OF THE PROJECT LIMITS EXCEPT AS SHOWN OR WITH THE PRIOR PERMISSION OF THE ENGINEER. SHOULD THE CONTRACTOR TRACK ANY DEBRIS ONTO EXISTING PAVEMENTS, THIS DEBRIS SHALL BE REMOVED IMMEDIATELY WITH A PICK UP SWEEPER. A PICK UP SWEEPER SHALL BE REQUIRED TO BE ON SITE AND OPERATE DURING ALL CONSTRUCTION OPERATION WORKING HOURS, UNLESS WAIVED BY THE DIRECTOR OF OPERATIONS. THE CONTRACTOR SHALL PROVIDE WASTE RECEPTACLES THROUGHOUT THE WORK ZONE AND MAINTAIN SANITARY FACILITIES FOR EMPLOYEES TO USE. FACILITIES WITHIN THE HANGARS/AIRPORT BUILDINGS SHALL NOT BE USED.

8. HAZARDOUS MATERIALS (HAZMAT) MANAGEMENT

1. THE CONTRACTOR SHALL DEVELOP A HAZMAT MANAGEMENT PLAN AND KEEP COPIES ON THE JOBSITE OF MATERIAL SAFETY DATA SHEETS (MSDS) FOR ALL MATERIALS HANDLED ON THE JOBSITE.

9. NOTIFICATION OF CONSTRUCTION ACTIVITIES

1. THE CONTRACTOR SHALL PROVIDE A 24 HOUR EMERGENCY CONTACT PERSON AND PHONE NUMBER. PERSONNEL SHALL BE ON CALL 24 HOURS PER DAY FOR MAINTAINING AIRPORT HAZARD LIGHTING AND BARRICADES.

2. THE CONTRACTOR SHALL GIVE A MINIMUM OF 10 DAYS NOTICE TO THE FAA AND AIRPORT PRIOR TO THE CLOSURE OF ANY RUNWAY SO THAT THE FAA MAY DEACTIVATE THE FAA - OWNED NAVAIDS.

3. THE CONTRACTOR SHALL GIVE A MINIMUM 30 DAYS NOTICE TO THE AIRPORT, AND PRIOR TO THE PRE-CONSTRUCTION CONFERENCE, PRIOR TO CLOSING ANY RUNWAY OR TAXIWAY PAVEMENT SO THAT THE PROPER NOTAMS MAY BE ISSUED BY THE AIRPORT FOR COORDINATION WITH THE AIRPORT TENANTS.

CONTRACTOR SHALL PLAN AND PERFORM HIS WORK SO AS NOT TO INTERFERE OR HINDER THE PROGRESS, WORK OR HAUL ROAD ACCESS OF OTHER CONTRACTORS (SEE STANDARD SPECIFICATIONS SECTION 50-05). THE PRIME CONTRACTOR WILL BE RESPONSIBLE TO COORDINATE CONSTRUCTION ACTIVITIES AND ACCESS BETWEEN ALL ON-SITE CONTRACTORS SUBCONTRACTORS.

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**3. AREAS AND OPERATIONS AFFECTED BY THE
CONSTRUCTION ACTIVITY**

1. ALL RUNWAYS, TAXIWAYS AND APRONS SHALL BE KEPT OPEN TO AIRCRAFT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED ON THE PHASING PLAN.

2. THE ENGINEER AND AIRPORT MANAGER OR HIS DESIGNATED REPRESENTATIVE SHALL HAVE FINAL SAY IN THE APPROVAL OF THE CONSTRUCTION OPERATING SEQUENCE AS IT RELATES TO PEDESTRIAN, VEHICULAR AND AIRCRAFT OPERATIONS. AIRCRAFT OPERATIONS HAVE THE RIGHT-OF-WAY ON THE AIRFIELD. VEHICULAR TRAFFIC AND CONTRACTOR ACTIVITIES SHALL YIELD TO AIRCRAFT OPERATIONS. SHOULD IT BE NECESSARY FOR THE CONTRACTOR TO TEMPORARILY RELOCATE EQUIPMENT AT ANY TIME TO ALLOW AN AIRCRAFT TO PASS, THE CONTRACTOR SHALL DO SO IMMEDIATELY AT NO EXTRA COST TO THE OWNER.

4. NAVAIDS THAT COULD BE AFFECTED

1. THE CONTRACTOR SHALL REMAIN CLEAR OF THE ILS CRITICAL AREAS AND OTHER NAVAIDS FACILITIES AT ALL TIMES.

5. CONTRACTOR ACCESS

1. THE CONTRACTOR ACCESS ROAD AND STAGING AREAS SHALL BE AS SHOWN ON THE REFERENCED PLAN. THE CONTRACTOR SHALL MAINTAIN AND REPAIR THE CONSTRUCTION ACCESS ROAD AND STAGING AREA IN ITS ORIGINAL CONDITION AT NO ADDITIONAL COST TO THE CONTRACT. ALTERNATE STAGING AREAS AND ACCESS FOR THIS AREA WILL NOT BE ALLOWED.

2. THE CONTRACTOR SHALL ACCESS THE SITE USING THE ROUTES AND GATES SHOWN. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING THE ACCESS GATE(S) CLOSED WHEN NOT IN USE.

3. CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS AND TEMPORARY EASEMENTS FOR THE PUBLIC ACCESS ROAD(S) SHOWN AND SHALL COMPLY WITH ALL REQUIREMENTS, LOAD RESTRICTIONS, & TRAFFIC CONTROL SIGNAGE REQUIRED BY THE CITY, COUNTY, TOWNSHIP, OR I.D.O.T.

4. THE CONTRACTOR WILL BE REQUIRED TO PUT AIRPORT FLAGS AND HAVE BEACON (FLASHING YELLOW) LIGHTS ON ALL EQUIPMENT AT ALL TIMES DURING CONSTRUCTION.

5. THE CONTRACTOR'S MATERIAL AND EQUIPMENT, WHEN NOT IN USE, SHALL BE STORED IN THE CONTRACTOR'S STAGING AREA. ALL DELIVERIES, EQUIPMENT REFUELING, EQUIPMENT MAINTENANCE AND EQUIPMENT TRANSFERS SHALL TAKE PLACE WITHIN THE CONTRACTOR'S STAGING AREA.

6. DURING ADVERSE WEATHER THE CONTRACTOR SHALL MAINTAIN ACCESS TO THE WORK AT NO ADDITIONAL COST TO THE CONTRACT. NO EXTENSION OF THE CONTRACT TIME WILL BE CONSIDERED FOR DELAYS DUE TO LACK OF ADEQUATE ACCESS TO THE WORK SITE.

7. THE CONTRACTOR WILL BE PERMITTED TO STORE EQUIPMENT AND MATERIALS ONLY AT THE LOCATIONS SHOWN. PARKED EQUIPMENT AND MATERIAL STOCKPILES SHALL NOT PENETRATE SURFACES DEFINED BY F.A.R. TITLE 14 PART 77 - OBJECTS AFFECTING NAVIGABLE AIRSPACE. EXISTING TURF AREAS DISTURBED BY THE CONTRACTOR'S OPERATIONS SHALL BE RESTORED BY HIM/HER AT HIS/HER EXPENSE TO THE SATISFACTION OF THE RESIDENT ENGINEER AND THE AIRPORT.

8. ALL CONSTRUCTION TRAFFIC OPERATING ON, OR CROSSING RUNWAYS, TAXIWAYS AND APRONS OPEN TO AIRCRAFT TRAFFIC SHALL BE UNDER CONTROL BY A FLAGMAN OR ESCORT IN RADIO CONTACT WITH THE CONTROL TOWER. THE CONTRACTOR SHALL PROVIDE HIS OWN FLAGMEN.

9. IF NECESSARY, THE CONTRACTOR SHALL CONSTRUCT A HAUL ROUTE TO THE STAGING AREA WITHIN THE PROJECT LIMITS. HAUL ROUTE(S) SHALL BE INCIDENTAL TO THE COST OF MAINTENANCE OF TRAFFIC. ALL HAUL ROUTE(S) INCLUDING EXISTING PAVEMENTS, DRIVES OR ANY OTHER AREAS USED BY THE CONTRACTOR SHALL BE RESTORED IN KIND TO THEIR PRE-CONSTRUCTION CONDITION OR TO THE SATISFACTION OF THE ENGINEER AND AIRPORT MANAGER. THE COST OF MAINTAINING, REPAIRING SEEDING /MULCHING OR CONSTRUCTING THESE HAUL ROUTE(S) SHALL BE INCIDENTAL TO THE COST OF THE CONTRACT.

DESIGN AIRCRAFT APPROACH CATEGORY: B AND D

AIRPLANE DESIGN GROUP: III
RUNWAY 4/22 SAFETY AREA WIDTH: 250'
RUNWAY 16/34 SAFETY AREA WIDTH: 250'
TAXIWAY CENTERLINE TO OBJECT SEPARATION: 85.5'
TAXILANE CENTERLINE TO OBJECT SEPARATION: 79'

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GENERAL

1. THE CONTRACTOR AND ALL SUBCONTRACTORS SHALL FOLLOW THE REQUIREMENTS OF THE AIRPORT'S APPROVED CONSTRUCTION SAFETY AND PHASING PLAN (CSPP), FAA AC 150/5370-2G OR LATEST EDITION, AND ALL AIRPORT SAFETY AND SECURITY REQUIREMENTS.

2. PRIOR TO THE NOTICE TO PROCEED, THE CONTRACTOR SHALL SUBMIT TO THE AIRPORT THROUGH THE RESIDENT ENGINEER, FOR APPROVAL A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) IN ACCORDANCE WITH FAA AC 150/5370-2G OR LATEST EDITION. NO CONSTRUCTION ACTIVITY SHALL BEGIN UNTIL THE AIRPORT HAS APPROVED THE SPCD.

3. THE CSPP COVERS OPERATIONAL SAFETY. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE INDIVIDUAL SAFETY OF HIS/HER PERSONNEL AND MEETING OSHA REQUIREMENTS.

4. A MINIMUM OF 10 DAYS PRIOR TO THE NOTICE TO PROCEED THE CONTRACTOR SHALL PROVIDE A LIST OF SUBCONTRACTORS AND MATERIAL SUPPLIERS.

5. ALL CONTRACTOR COSTS ASSOCIATED WITH THE REQUIREMENTS LISTED ON THIS SHEET SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

6. THE SUGGESTED SEQUENCE OF CONSTRUCTION SHOWN IS INTENDED TO ALLOW FOR THE ORDERLY CONSTRUCTION OF THE NEW IMPROVEMENTS WHILE MAINTAINING TENANT/AIRCRAFT ACCESS AT ALL TIMES. THE PHASING SHOWN IS A SUGGESTED SEQUENCE OF CONSTRUCTION ONLY. THIS SEQUENCE MAY BE MODIFIED WITH THE APPROVAL OF THE ENGINEER IN CONSULTATION WITH THE AIRPORT MANAGER. HOWEVER, ALTERNATE STAGING PLANS MUST MAINTAIN AIRPORT OPERATIONS TO THE SATISFACTION OF THE AIRPORT DIRECTOR OF OPERATIONS.

1. COORDINATION

1. PRIOR TO THE START OF CONSTRUCTION THE CONTRACTOR SHALL ATTEND A PRE-CONSTRUCTION CONFERENCE WITH THE AIRPORT, RESIDENT ENGINEER, AND ILLINOIS DIVISION OF AERONAUTICS (IDA). THE COST OF PREPARING FOR AND ATTENDING THE PRE-CONSTRUCTION CONFERENCE SHALL BE INCIDENTAL TO THE CONTRACT.

2. PRIOR TO THE START OF CONSTRUCTION, THE CONTRACTOR SHALL SUBMIT A PROGRESS SCHEDULE TO BE APPROVED BY THE ENGINEER. THIS SCHEDULE SHALL SHOW START/ STOP DATES OF ALL PHASES. THE APPROVED PROGRESS SCHEDULE SHALL BE DISTRIBUTED TO ALL PARTIES 10 WORKING DAYS PRIOR TO START OF CONSTRUCTION. THE CONTRACTOR MAY BE REQUIRED TO SUBMIT A REVISED PROGRESS SCHEDULE TO ACCOMMODATE AIRPORT EVENTS OR CONTRACTOR'S LACK OF PROGRESS. SHOULD A REVISED SCHEDULE BE REQUIRED, THE REVISION SHALL BE COMPLETED AT NO ADDITIONAL COST TO THE CONTRACT.

3. DURING CONSTRUCTION THE CONTRACTOR SHALL ATTEND A WEEKLY COORDINATION MEETING WITH AIRPORT STAFF, FAA, RESIDENT ENGINEER AND OTHER APPROPRIATE STAKE HOLDERS TO DISCUSS PROJECT PROGRESS. AT A MINIMUM, PROJECT SCHEDULE AND GATE VISITOR LOGS SHALL BE DISCUSSED. REPRESENTATION BY THE PRIME CONTRACTOR IS MANDATORY. ALL COSTS ASSOCIATED WITH ATTENDING THE WEEKLY MEETING SHALL BE INCIDENTAL TO THE CONTRACT.

2. PHASING

1. TOTAL CONTRACT TIME FOR THE PROJECT SHALL BE 21 CALENDAR DAYS.

2. PHASING SHALL BE AS NOTED BELOW AND AS SHOWN ON THE CONSTRUCTION SAFETY AND PHASING PLANS.

3. TO CLOSE A RUNWAY OR TAXIWAY, THE CONTRACTOR SHALL PLACE RUNWAY AND TAXIWAY CLOSURE MARKERS AND BARRICADES AT THE LOCATIONS SPECIFIED. TO RE-OPEN THE RUNWAY OR TAXIWAY, THE CONTRACTOR SHALL CLEAN ANY DEBRIS OFF OF THE PAVEMENT AND REMOVE THE RUNWAY/TAXIWAY CLOSURE MARKERS. ALL WORK ASSOCIATED WITH CLOSING AND OPENING AIRFIELD PAVEMENTS SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

4. CONTRACTOR SHALL COVER ALL AIRFIELD SIGNS AND LIGHTS ON CLOSED TAXIWAYS UNTIL THE TAXIWAY IS REOPENED FOR AIRCRAFT USE. THE METHOD AND MATERIALS USED TO COVER THE SIGNS AND LIGHTS SHALL MEET THE ENGINEER'S AND AIRPORT'S APPROVAL.

5. SEE CONTRACTOR ACCESS NOTES ON THIS SHEET FOR SITE ACCESS AND HAULING GUIDELINES.

6. PRIOR TO REOPENING A CLOSED RUNWAY OR TAXIWAY, THE ENTIRE RUNWAY SAFETY AREA (RSA), MEASURED 250 FEET FROM THE RUNWAY CENTERLINE, INCLUDING BEYOND THE RUNWAY END WITHIN THE EXTENDED RSA, AND THE ENTIRE TAXIWAY OBJECT FREE AREA (TOFA), MEASURED 40.5 FEET FROM PAVEMENT EDGE FOR GROUP II AIRCRAFT MUST MEET FAA CRITERIA. FAA CRITERIA REQUIRES THAT THERE BE NO OPEN EXCAVATIONS OR TRENCHES IN THESE AREAS. THE MAXIMUM PAVEMENT DROP OFF SHALL BE 3 INCHES, AND ALL GRADES IN ANY DIRECTION BE LESS THAN 5 PERCENT. STEEL PLATES, TEMPORARY GRADING OR TEMPORARY WEDGING OF BASE COURSE FOR NEW PAVEMENT MAY BE REQUIRED TO MEET CRITERIA. ALL NECESSARY TEMPORARY MEASURES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

7. NO MATERIAL OR EQUIPMENT SHALL BE STOCKPILED WITHIN AN RSA, ROFA OR OBSTACLE FREE ZONE (OFZ) OF AN ACTIVE RUNWAY, OR WITHIN THE TOFA OF AN ACTIVE TAXIWAY.

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12. PENALTIES

- NONCOMPLIANCE BY THE CONTRACTOR WITH AIRPORT RULES AND REGULATIONS OR FAILURE TO COMPLY WITH THE AIRPORT'S APPROVED CSPP AND THE CONTRACTOR'S APPROVED SPCD MAY RESULT IN FINES AS ALLOWED BY LAW.
- THE CONTRACTOR SHALL RESTRICT ALL CONSTRUCTION ACTIVITIES TO THE CONSTRUCTION AREA DETAILED IN THE CONSTRUCTION SAFETY AND PHASING PLAN. ANY UNAUTHORIZED MOVEMENTS, PEDESTRIAN OR VEHICULAR, BEYOND THE CONSTRUCTION LIMITS SHOWN SHALL BE CONSIDERED AND AIRFIELD INCURSION. AIRFIELD INCURSIONS, AT THE DISCRETION OF THE AIRPORT DIRECTOR OF AVIATION, MAY BE FINED UP TO \$1,000 PER INCIDENT. INCURSION FINES WILL BE ASSESSED IMMEDIATELY AND TAKEN FROM MONIES DUE THE CONTRACTOR ON THE NEXT CONSTRUCTION PAYMENT.

13. SPECIAL CONDITIONS

- ADJACENT CONSTRUCTION MAY IMPACT THE OPERATIONS OF THE CONTRACTOR. SEE THE COORDINATION NOTES FOR ADDITIONAL INFORMATION.

14. RUNWAY AND TAXIWAY VISUAL AIDS

- ALL RUNWAYS, TAXIWAYS, AND APRONS SHALL BE KEPT OPEN TO AIRPORT TRAFFIC DURING CONSTRUCTION EXCEPT AS NOTED IN THE CONSTRUCTION SAFETY AND PHASING PLAN.
- IF ANY RUNWAY OR TAXIWAY CLOSURES ARE REQUESTED BY THE CONTRACTOR AND APPROVED BY THE AIRPORT, THE CONTRACTOR SHALL USE MARKING, LIGHTING AND SIGNS THAT FOLLOWING THE REQUIREMENTS OF FAA AC 150/5370-2G OR LATEST EDITION.

15. MARKING AND SIGNS FOR ACCESS ROUTES

- MARKING AND SIGNAGE FOR THE ACCESS ROUTE SHALL BE MARKED WITH LATHE OR AS DIRECTED BY THE RESIDENT ENGINEER. ACCESS ROUTES ARE AS SHOWN ON THE CONSTRUCTION SAFETY AND PHASING PLAN OR AS DIRECTED BY THE RESIDENT ENGINEER.

16. HAZARD MARKING AND LIGHTING

- THE CONTRACTOR SHALL FURNISH, ERECT, AND MAINTAIN MARKINGS AND ASSOCIATED LIGHTING OF OPEN TRENCHES, EXCAVATIONS, TEMPORARY STOCKPILES, AND HIS/HER CONSTRUCTION EQUIPMENT.
- ALL CONSTRUCTION EQUIPMENT SHALL BE FLAGGED AND/OR LIGHTED IN ACCORDANCE WITH FAA ADVISORY CIRCULAR 150/5370-2G AND 150/5210-5C OR LATEST EDITION AT ALL TIMES WHILE OPERATING ON AIRPORT PROPERTY. THE MAXIMUM EQUIPMENT HEIGHT IS 25'.
- BARRICADES SHALL BE PLACED AT THE LOCATIONS SHOWN ON THE CONSTRUCTION SAFETY AND PHASING PLAN SHEET OR AS DIRECTED BY THE ENGINEER.
- THE CONTRACTOR SHALL INSPECT THE BARRICADES ONCE DURING EACH WORK DAY TO INSURE PROPER PLACEMENT AND PROPER OPERATION OF THE RED LIGHTS AND FLAG PLACEMENT.
- PAYMENT FOR ALL AIRSIDE AND ROADWAY TRAFFIC CONTROL INCLUDING, BUT NOT LIMITED TO, TEMPORARY CONSTRUCTION FENCING, BARRICADES, SIGNING, AIR OPERATIONS AREA (A.O.A.) LATH AND RIBBON, ETC. SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT. BARRICADES USED ON THE AIRFIELD MUST BE LOW PROFILE BARRICADES. LOW PROFILE BARRICADES WITH STEADY BURN RED LIGHTS SHALL BE OF NO GAPS BETWEEN BARRICADES. BARRICADES SHALL BE PLACED AS SHOWN ON THIS PLAN AND AS DIRECTED BY THE ENGINEER FOR WORK ADJACENT TO THE EXPEDITED WORK AREA. WHEN NOT IN USE, THESE BARRICADES SHALL BE STORED AT THE CONTRACTOR'S STAGING AREA OR OFF SITE. ACCESS TO THE ACTIVE RUNWAY AND TAXIWAY PAVEMENTS (TOWER CONTROLLED AREAS) SHALL BE SIGNED WITH STOP SIGNS MOUNTED ON THE CLOSEST BARRICADES (2 EACH, RIGHT AND LEFT) AT THE ENTRANCE. IN ADDITION TO THE STOP SIGNS, WARNING SIGNS (2 EACH, RIGHT AND LEFT) SHALL BE MOUNTED. WARNING SIGNS SHALL STATE "TOWER CONTROL AREA / UNAUTHORIZED ACCESS SUBJECT TO FINE." ALL NON AIRFIELD LOCATIONS REQUIRING BARRICADES SHALL BE TYPE II OR TYPE III BARRICADES AND SHALL CONFORM TO IDOT STANDARD DETAIL 701901-04.

17. WORK ZONE LIGHTING FOR NIGHTTIME CONSTRUCTION

- WORK PERFORMED BY THE CONTRACTOR OUTSIDE OF DAYLIGHT HOURS SHALL BE DONE UNDER SUFFICIENT ARTIFICIAL AREA LIGHTING TO ALLOW FOR PROPER CONSTRUCTION METHODS AND INSPECTION.
- LIGHTS SHALL CONSIST OF VEHICLE OR MOVABLE POLE MOUNTED FLOODLIGHTS AND/OR SPOTLIGHTS OF SUFFICIENT NUMBER TO ILLUMINATE THE WORK AREA. VEHICLE HEADLIGHTS WILL BE ALLOWED ONLY IN ADDITION TO OTHER LIGHTS MENTIONED ABOVE. LIGHTING SHALL NOT INTERFERE WITH AIR OPERATIONS. ANY WORK BEING PERFORMED UNDER INSUFFICIENT ARTIFICIAL LIGHTING, IN THE RESIDENT ENGINEER'S JUDGEMENT, SHALL BE STOPPED UNTIL SUCH TIME AS ADDITIONAL LIGHTING IS PROVIDED. ALL WORK PERFORMED DURING THAT TIME WILL NOT BE ACCEPTABLE UNTIL PROPER INSPECTION AND TESTING CAN BE MADE.

18. PROTECTION

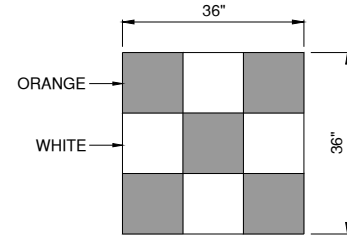
- ALL WORK REQUIRED INSIDE OF A RUNWAY SAFETY AREA WILL REQUIRE THE RUNWAY TO BE CLOSED.
- ALL WORK REQUIRED ON AN ACTIVE TAXIWAY OR INSIDE OF AN ACTIVE TAXIWAY OBJECT FREE AREA, WILL REQUIRE THE TAXIWAY TO BE CLOSED.

19. OTHER LIMITATIONS ON CONSTRUCTION

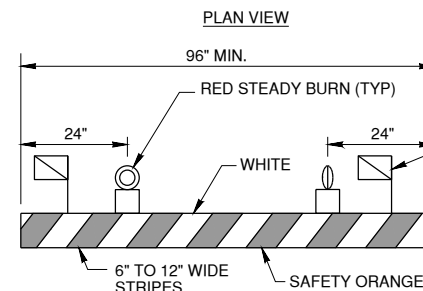
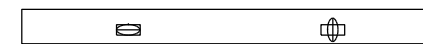
- IF, DURING CONSTRUCTION, AN EMERGENCY IS DECLARED BY THE AIRPORT, THE CONTRACTOR SHALL IMMEDIATELY CLEAR THE PAVEMENT OF ALL VEHICLES, PERSONNEL AND EQUIPMENT.
- BROKEN CONCRETE, BROKEN ASPHALT, RUBBISH FROM DEMO, AND OTHER MISCELLANEOUS DEBRIS SHALL BE DISPOSED OF OFF AIRPORT PROPERTY, UNLESS OTHERWISE SPECIFIED.
- THE CONTRACTOR WILL BE RESPONSIBLE FOR COORDINATING THE AIRSPACE FOR THE CONSTRUCTION EQUIPMENT THAT IS TALLER THAN THAT SPECIFIED ON THE PLANS WITH THE FAA. THIS PROCESS MAY TAKE UP TO 12 WEEKS TO COMPLETE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR MEGGAR TESTING ALL EXISTING CIRCUITS PRIOR TO CONSTRUCTION AND FOLLOWING CONSTRUCTION AS SPECIFIED IN THE CONTRACT DOCUMENTS.

AIRFIELD LIGHTS AND SIGN NOTES

- CONTRACTOR SHALL COVER ALL AIRFIELD SIGNS AND TAXIWAY LIGHTS ON CLOSED TAXIWAYS UNTIL THE TAXIWAY IS RE-OPENED FOR AIRCRAFT USE. THE METHOD AND MATERIALS USED TO COVER THE SIGNS AND LIGHTS SHALL MEET THE ENGINEER'S AND AIRPORT'S APPROVAL. COST INCIDENTAL TO THE CONTRACT. REMOVING LAMPS FROM ENERGIZED FIXTURES AS A MEANS TO REMOVE THE LIGHTS OR FIXTURES FROM SERVICE SHALL NOT BE ACCEPTABLE.



CONSTRUCTION EQUIPMENT AND TRUCK SIGNAL FLAG
NOT TO SCALE

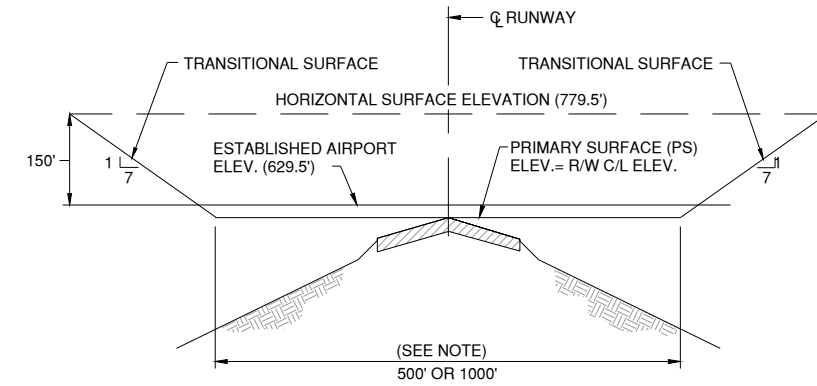


AIRSIDE LOW PROFILE LIGHTED BARRICADE
NOT TO SCALE

BARRICADE NOTES

- STEADY BURN LIGHTS SHALL BE BATTERY OPERATED. LENS SHALL BE RED AND BE ABLE TO ROTATE 90°.
- FACING OF BARRICADE SHALL BE COVERED WITH REFLECTIVE TAPE OR PAINT.
- BARRICADES TO BE PLACED WITH A MAXIMUM OF 4' SPACING END TO END UP TO THE EDGE OF PAVEMENT ALONG OPERATIONAL PAVEMENT ADJACENT TO CONSTRUCTION AS DIRECTED BY THE RESIDENT ENGINEER. ALTERNATE STEADY BURN LENSES SO THAT EVERY OTHER LENS IS ROTATED 90°.
- STEADY BURN LIGHTS SHALL BE SECURED TO THE BARRICADES, AS APPROVED BY THE RESIDENT ENGINEER.
- BARRICADES SHALL BE OF LOW MASS, EASILY COLLAPSIBLE UPON CONTACT WITH AN AIRCRAFT OR ANY OF IT COMPONENTS, AND WEIGHTED TO AVOID BEING BLOWN OVER.
- BARRICADES SHALL BE OF A COMMERCIAL DESIGN AND SHALL MEET CURRENT FAA REQUIREMENTS.
- PLACE ALL BARRICADES OUTSIDE RUNWAY SAFETY AREAS AND OUTSIDE TAXIWAY OBJECT FREE AREAS.
- ALL COST ASSOCIATED WITH THE LOW PROFILE BARRICADES SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.

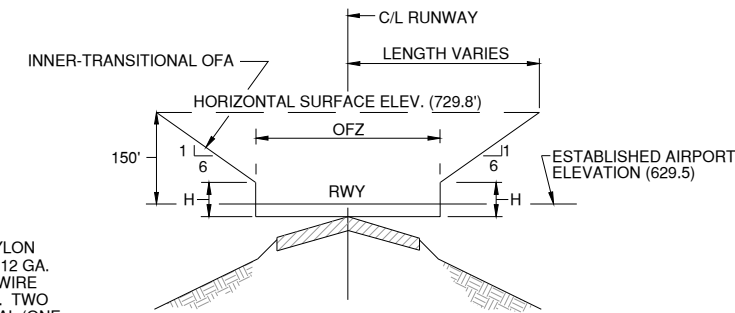
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TYPICAL SECTION
F.A.R. PART 77 IMAGINARY SURFACES

NO SCALE

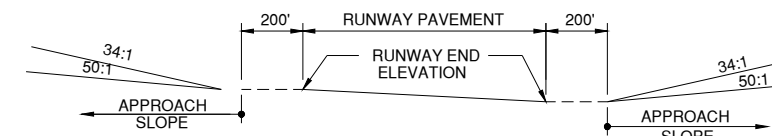
NOTE:
IMAGINARY SURFACE REQUIREMENTS FOR EXISTING ACTIVE RUNWAYS (R/W) ARE SIMILAR EXCEPT PRIMARY SURFACE (PS) DIMENSIONS VARY



TYPICAL SECTION
OBSTACLE FREE ZONE (OFZ)

NO SCALE

| RUNWAY | TYPE OF RUNWAY | H (FEET) |
|--------|----------------|----------|
| 16-34 | NON-PRECISION | 150 |
| 4-22 | PRECISION | 150 |



TYPICAL PROFILE
F.A.R. PART 77 IMAGINARY SURFACES

NO SCALE

| RUNWAY END | ELEVATION | APPROACH SLOPE |
|------------|-----------|----------------|
| 16 | 621 | 34:1 |
| 34 | 617 | 34:1 |
| 4 | 624 | 50:1 |
| 22 | 630 | 34:1 |



CONSULTANTS

IL PROJECT: IKK-5084
IL LETTING ITEM: 04A
IL CONTRACT NO.: KA055

RECONSTRUCT SW QUADRANT APRON;
TERMINAL AND SE HANGAR
APRON PAVEMENT REPAIRS

JUNE 6, 2025

OWNER



GREATER KANKAKEE AIRPORT
KANKAKEE, ILLINOIS

MARK DATE DESCRIPTION

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DRAWN BY: JRO
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SHEET TITLE

CONSTRUCTION SAFETY
AND PHASING PLAN
GENERAL NOTES - 2



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IL LETTING ITEM: 04A
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RECONSTRUCT SW QUADRANT APRON; TERMINAL AND SE HANGAR APRON PAVEMENT REPAIRS

JUNE 6, 2025

OWNER



GREATER KANKAKEE AIRPORT
KANKAKEE, ILLINOIS

MARK DATE DESCRIPTION

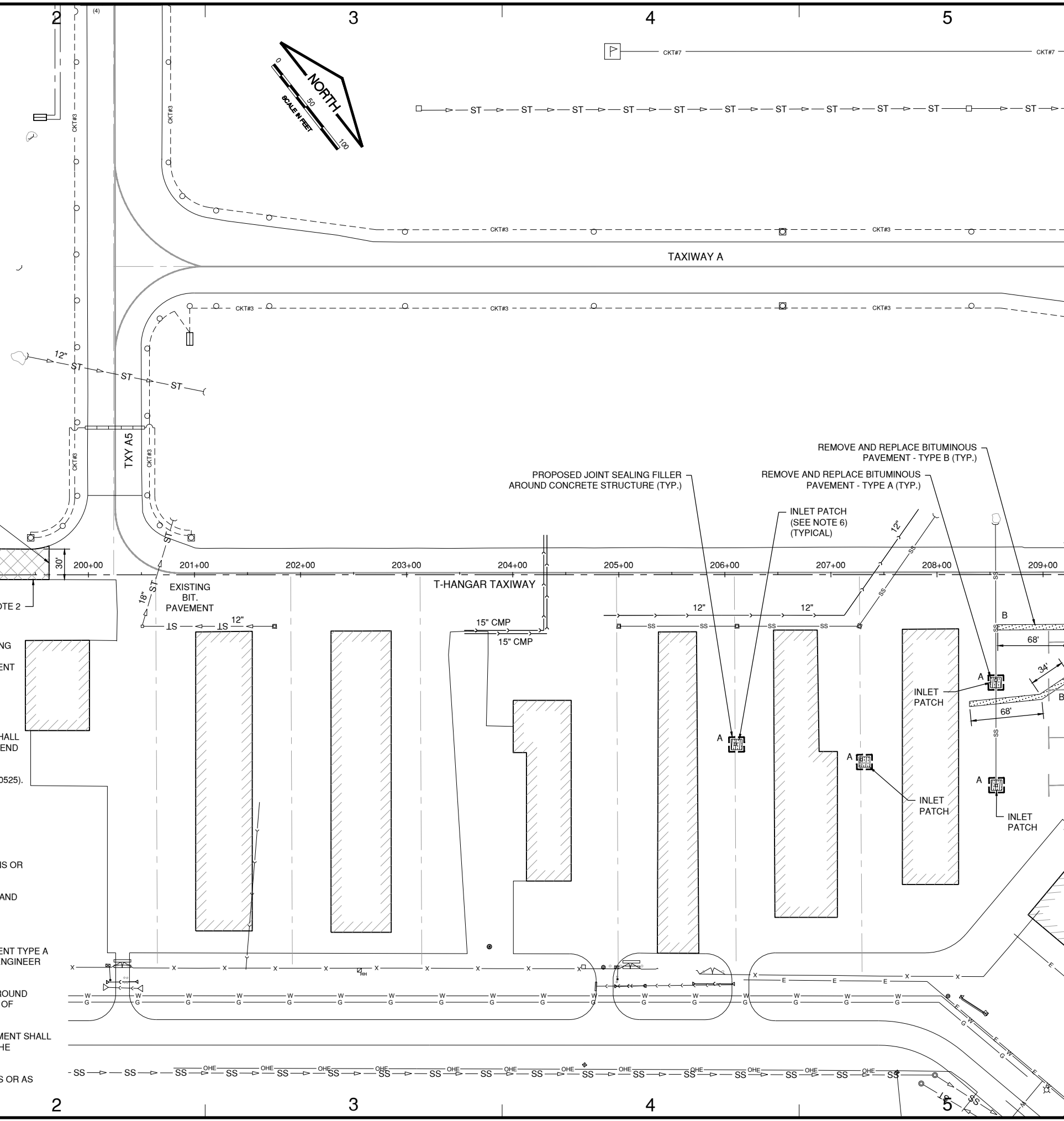
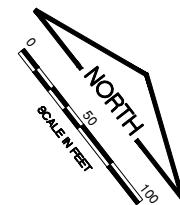
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EXISTING CONDITIONS/PROPOSED REMOVALS - 1

SHEET 10 OF 26

LEGEND

- EXISTING BITUMINOUS PAVEMENT (TO BE REMOVED)
- EXISTING BUILDING
- A** REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE A
- B** REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE B
- EXISTING STORM SEWER
- EXISTING FLARED END SECTION
- EXISTING BASE MOUNTED TAXIWAY EDGE LIGHT
- EXISTING STAKE MOUNTED TAXIWAY EDGE LIGHT
- EXISTING ELECTRICAL DUCT BANK
- EXISTING FENCE
- EXISTING TAXIWAY A, A2, A3, A4 AND A5 CIRCUIT
- EXISTING ELECTRIC CIRCUIT 7
- EXISTING TAXIWAY GUIDANCE SIGN
- EXISTING SANITARY SEWER
- EXISTING GAS LINE
- EXISTING WATER MAIN
- EXISTING ELECTRICAL CABLE
- EXISTING 12" YELLOW CENTERLINE MARKING
- PROPOSED JOINT SEALING FILLER (REFER TO PAVEMENT REPAIR DETAILS)



- #### PAVEMENT PATCHING GENERAL NOTES
- ALL AIRFIELD PAVEMENT MARKINGS WITHIN PAVEMENT PATCHING LIMITS SHALL BE REMARKED BY THE CONTRACTOR. NEW PAVEMENT MARKING SHALL EXTEND 5' BEYOND THE PATCH LIMITS (AR620520).
 - NEW AIRFIELD PAVEMENT MARKING SHALL HAVE A 6" BLACK BORDER (AR620525).
 - THE PAVEMENT SURFACE SHALL BE CLEAN AND DRY PRIOR TO MARKING.
 - ALL PAVEMENTS SHALL BE MARKED TO THE EXISTING CONDITION PER FAA STANDARDS AC 150/5340-1 (LATEST EDITION) OR AS DIRECTED BY AIRPORT/ENGINEER.
 - PATCHES SHALL BE CONSTRUCTED AT LOCATIONS AS SHOWN ON THE PLANS OR AS DESIGNATED BY THE RESIDENT ENGINEER.
 - INLET PATCHES SHALL BE 4' WIDE, 22' PERIMETER AROUND EXISTING INLET AND CONCRETE FRAME.
- #### NOTES
- THE LOCATIONS AND LIMITS OF REMOVE AND REPLACE BITUMINOUS PAVEMENT TYPE A AND TYPE B ARE APPROXIMATE AND SHALL BE VERIFIED BY THE RESIDENT ENGINEER PRIOR TO SAWCUTTING.
 - THE EXISTING PAVEMENT TO BE REMOVED SHALL BE SAWED FULL DEPTH AROUND PERIMETER OF THE REMOVAL LIMITS. COST OF SAWCUTTING AND DISPOSAL OF PAVEMENT SHALL BE CONSIDERED INCIDENTAL TO THE ITEM.
 - ANY DAMAGE TO EXISTING EDGE LIGHTS, GUIDANCE SIGNS, AND FAA EQUIPMENT SHALL BE FIXED IMMEDIATELY BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.
 - PATCHES SHALL BE CONSTRUCTED AT LOCATIONS AS SHOWN ON THE PLANS OR AS DESIGNATED BY THE RESIDENT ENGINEER.

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CONSULTANTS

IL PROJECT: IKK-5084
IL LETTING ITEM: 04A
IL CONTRACT NO.: KA055

RECONSTRUCT SW QUADRANT APRON; TERMINAL AND SE HANGAR APRON PAVEMENT REPAIRS

JUNE 6, 2025

OWNER



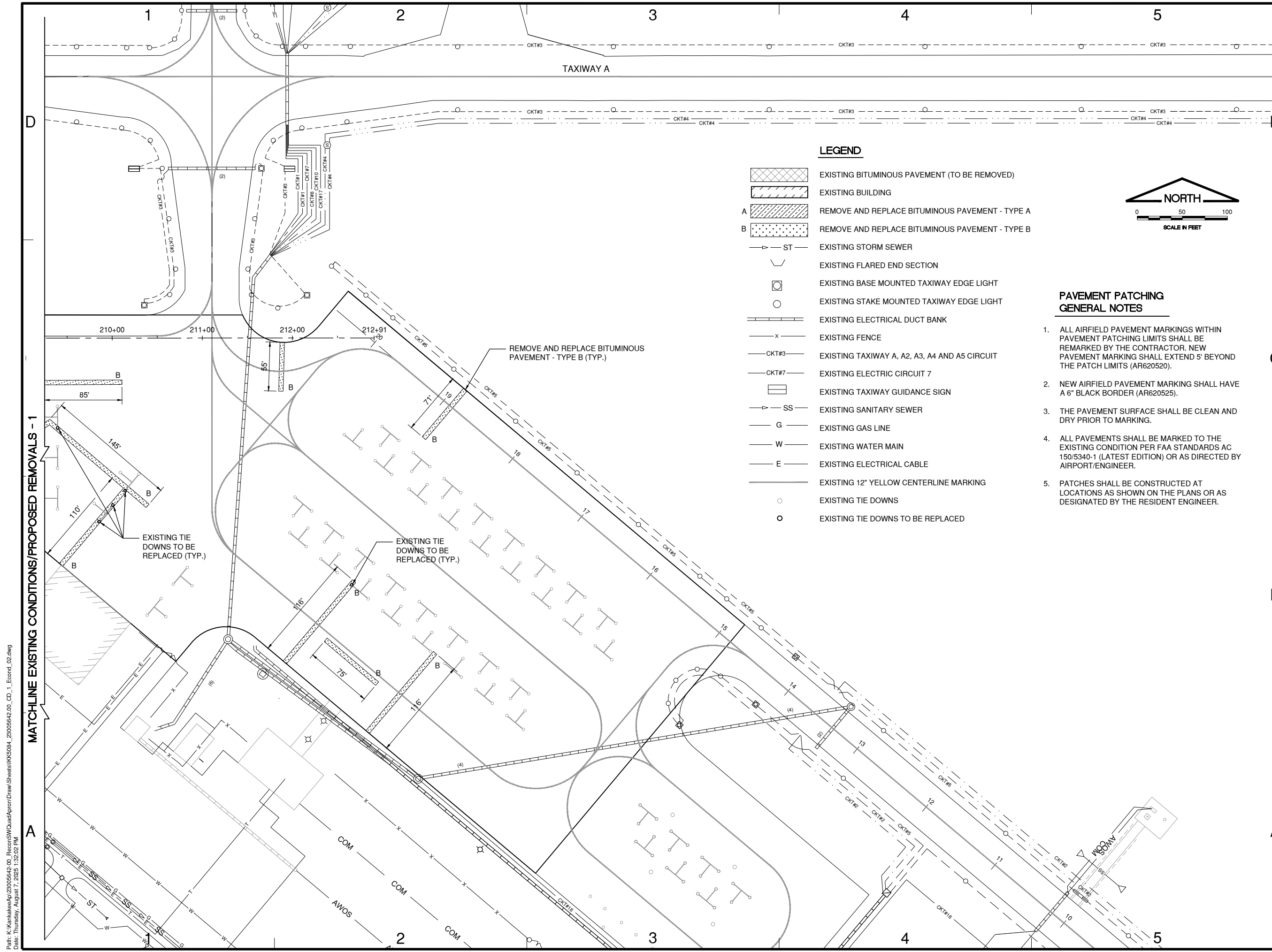
GREATER KANKAKEE AIRPORT
KANKAKEE, ILLINOIS

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EXISTING CONDITIONS/PROPOSED REMOVALS - 2

SHEET 11 OF 26



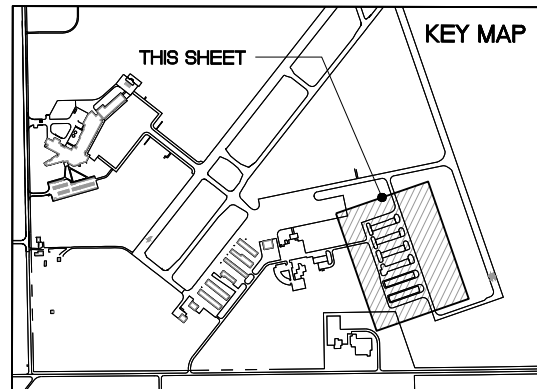
LEGEND

- EXISTING BITUMINOUS PAVEMENT (TO BE REMOVED)
- EXISTING BUILDING
- REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE A
- REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE B
- ST EXISTING STORM SEWER
- EXISTING FLARED END SECTION
- EXISTING BASE MOUNTED TAXIWAY EDGE LIGHT
- EXISTING STAKE MOUNTED TAXIWAY EDGE LIGHT
- EXISTING ELECTRICAL DUCT BANK
- EXISTING FENCE
- EXISTING TAXIWAY A, A2, A3, A4 AND A5 CIRCUIT
- EXISTING ELECTRIC CIRCUIT 7
- EXISTING TAXIWAY GUIDANCE SIGN
- SS EXISTING SANITARY SEWER
- EXISTING GAS LINE
- EXISTING WATER MAIN
- EXISTING ELECTRICAL CABLE
- EXISTING 12" YELLOW CENTERLINE MARKING
- EXISTING TIE DOWNS
- EXISTING TIE DOWNS TO BE REPLACED

PAVEMENT PATCHING GENERAL NOTES

1. ALL AIRFIELD PAVEMENT MARKINGS WITHIN PAVEMENT PATCHING LIMITS SHALL BE REMARKED BY THE CONTRACTOR. NEW PAVEMENT MARKING SHALL EXTEND 5' BEYOND THE PATCH LIMITS (AR620520).
2. NEW AIRFIELD PAVEMENT MARKING SHALL HAVE A 6" BLACK BORDER (AR620525).
3. THE PAVEMENT SURFACE SHALL BE CLEAN AND DRY PRIOR TO MARKING.
4. ALL PAVEMENTS SHALL BE MARKED TO THE EXISTING CONDITION PER FAA STANDARDS AC 150/5340-1 (LATEST EDITION) OR AS DIRECTED BY AIRPORT/ENGINEER.
5. PATCHES SHALL BE CONSTRUCTED AT LOCATIONS AS SHOWN ON THE PLANS OR AS DESIGNATED BY THE RESIDENT ENGINEER.

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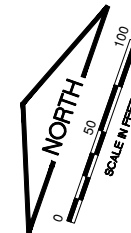


LEGEND

| | | | |
|--|---|--|--|
| | EXISTING BITUMINOUS PAVEMENT (TO BE REMOVED) | | EXISTING FENCE |
| | EXISTING BUILDING | | EXISTING SPLICE CAN |
| | REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE A | | EXISTING ELECTRIC CIRCUIT 5 |
| | REMOVE AND REPLACE BITUMINOUS PAVEMENT - TYPE B | | EXISTING TAXIWAY GUIDANCE SIGN |
| | EXISTING STORM SEWER | | EXISTING SANITARY SEWER |
| | EXISTING BASE MOUNTED TAXIWAY EDGE LIGHT | | EXISTING UNDERDRAIN |
| | EXISTING STAKE MOUNTED TAXIWAY EDGE LIGHT | | EXISTING WATER MAIN |
| | EXISTING ELECTRICAL DUCT BANK | | EXISTING ELECTRICAL CABLE |
| | EXISTING 12" YELLOW CENTERLINE MARKING | | PROPOSED JOINT SEALING FILLER (REFER TO PAVEMENT REPAIR DETAILS) |

PAVEMENT PATCHING GENERAL NOTES

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- NEW AIRFIELD PAVEMENT MARKING SHALL HAVE A 6" BLACK BORDER (AR620525).
- THE PAVEMENT SURFACE SHALL BE CLEAN AND DRY PRIOR TO MARKING.
- ALL PAVEMENTS SHALL BE MARKED TO THE EXISTING CONDITION PER FAA STANDARDS AC 150/5340-1 (LATEST EDITION) OR AS DIRECTED BY AIRPORT/ENGINEER.
- PATCHES SHALL BE CONSTRUCTED AT LOCATIONS AS SHOWN ON THE PLANS OR AS DESIGNATED BY THE RESIDENT ENGINEER.
- INLET PATCHES SHALL BE 4' WIDE, 22' PERIMETER AROUND EXISTING INLET AND CONCRETE FRAME.



CONSULTANTS

IL PROJECT: IKK-5084
IL LETTING ITEM: 04A
IL CONTRACT NO.: KA055

RECONSTRUCT SW QUADRANT APRON; TERMINAL AND SE HANGAR APRON PAVEMENT REPAIRS

JUNE 6, 2025

OWNER



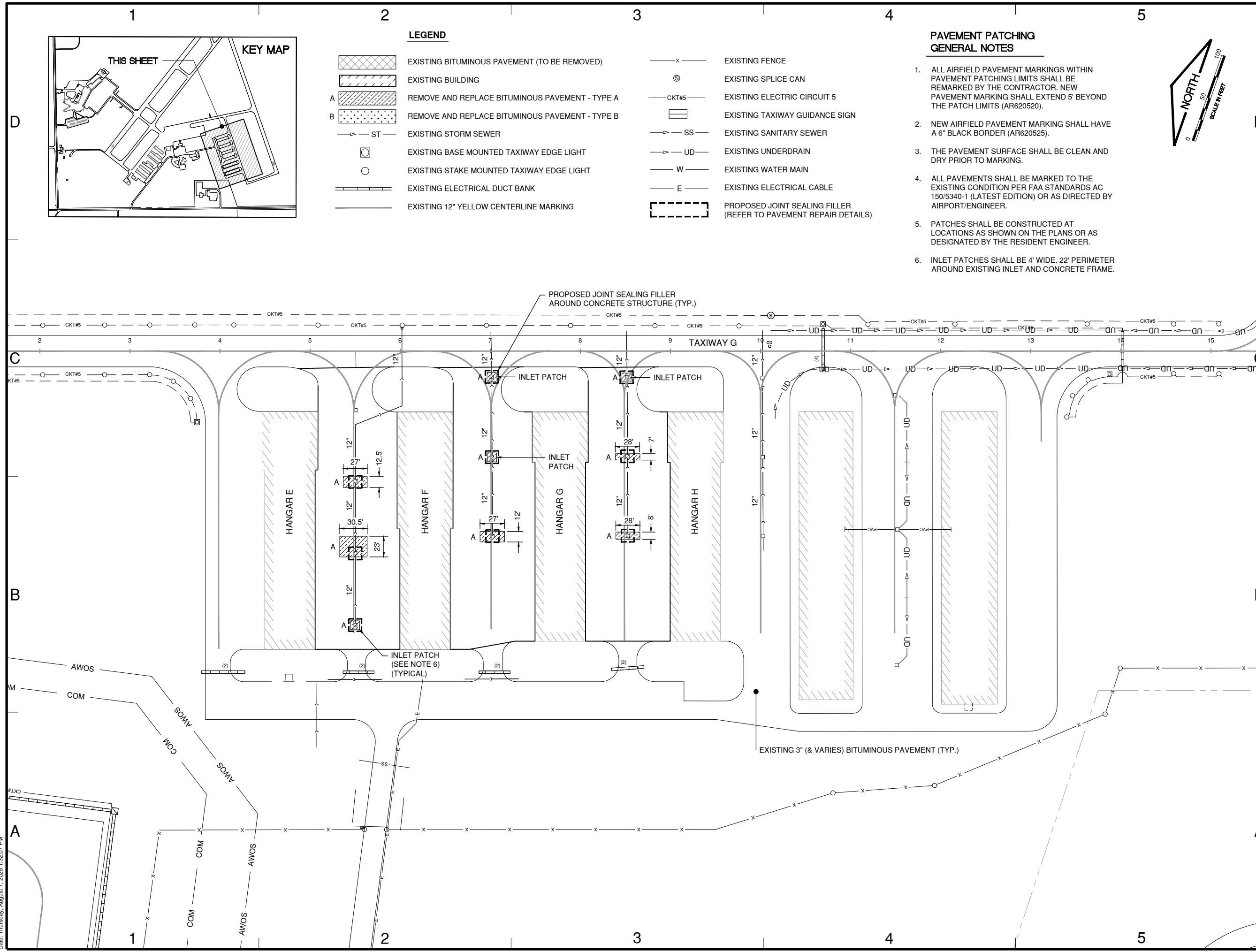
GREATER KANKAKEE AIRPORT
KANKAKEE, ILLINOIS

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SHEET TITLE
EXISTING CONDITIONS/PROPOSED REMOVALS - 3

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CONSULTANTS

IL PROJECT: IKK-5084
IL LETTING ITEM: 04A
IL CONTRACT NO.: KA055

RECONSTRUCT SW
QUADRANT APRON;
TERMINAL AND SE HANGAR
APRON PAVEMENT REPAIRS

JUNE 6, 2025

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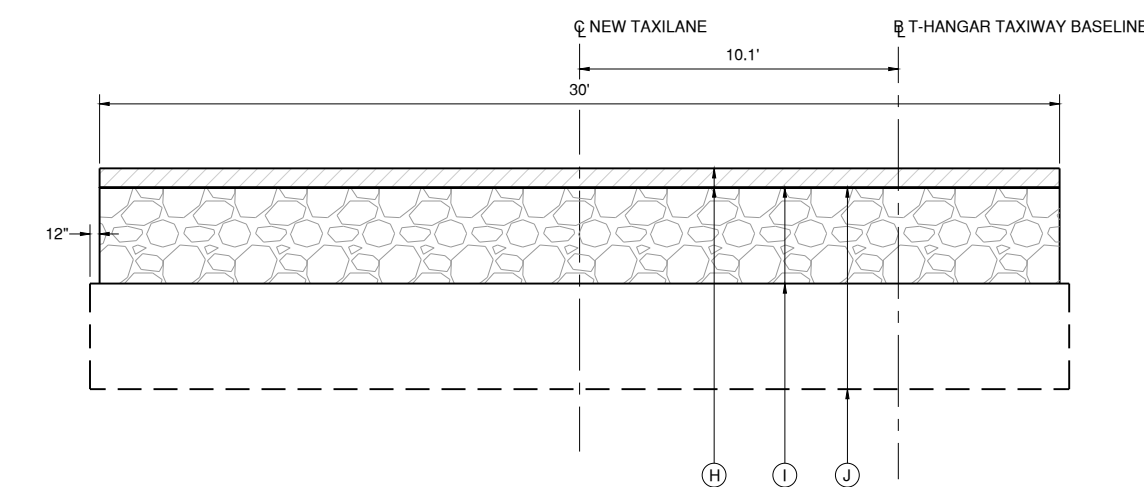


GREATER KANKAKEE AIRPORT
KANKAKEE, ILLINOIS

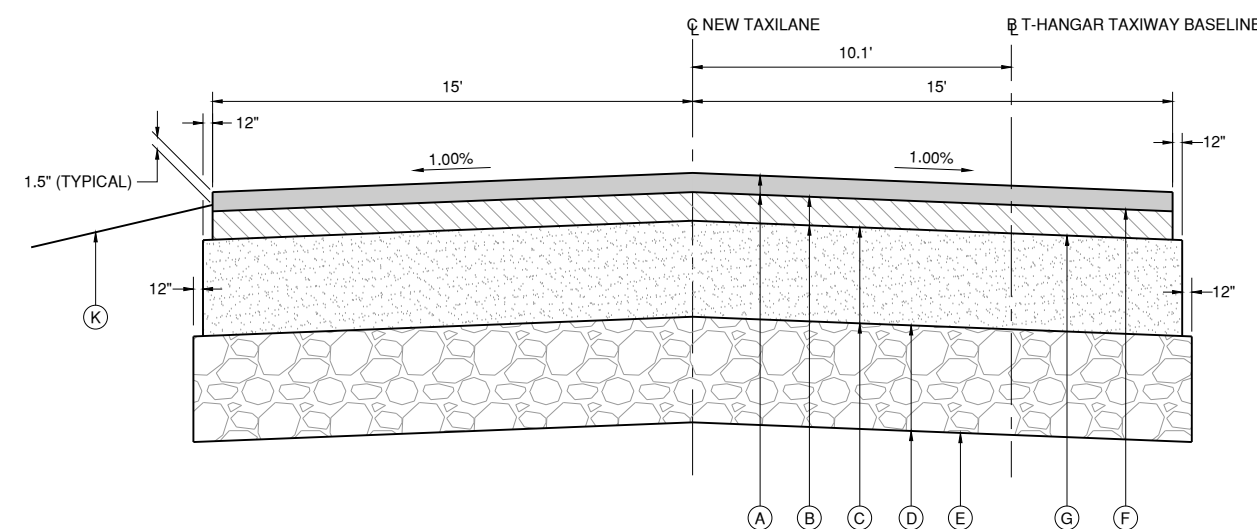
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TYPICAL SECTIONS

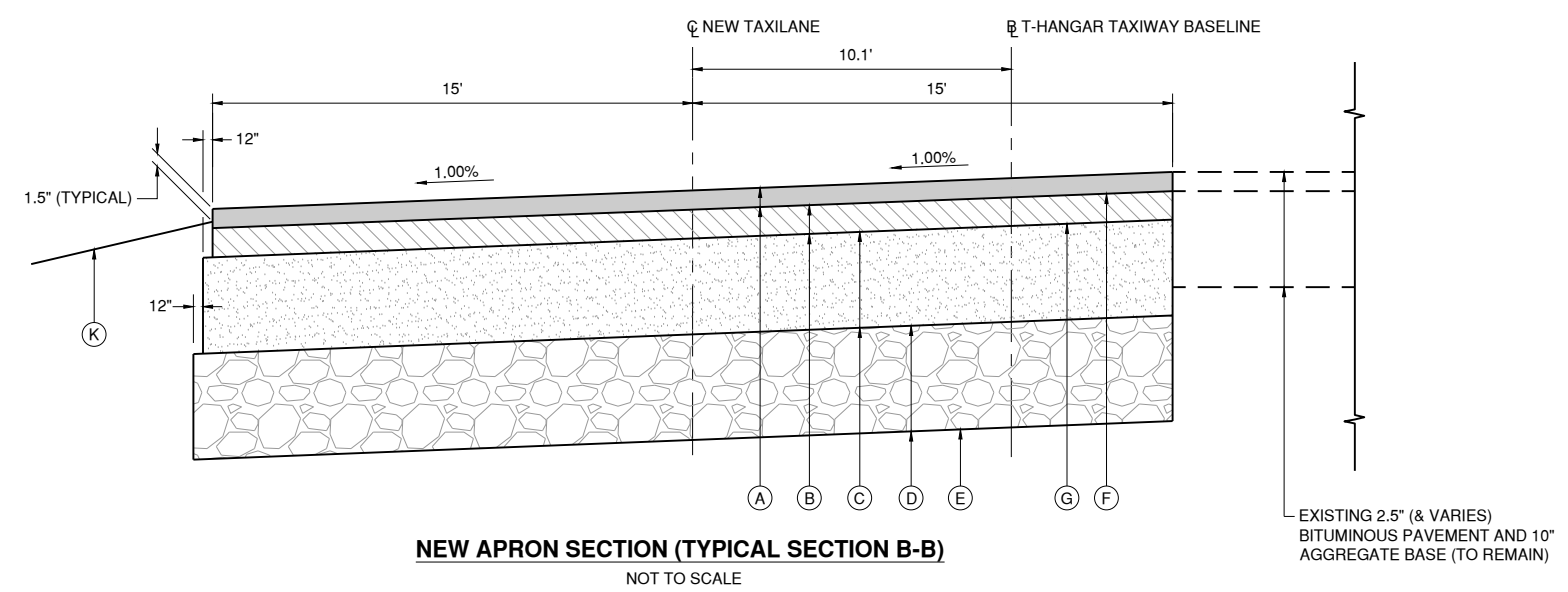
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EXISTING TAXILANE (TYPICAL SECTION A-A)
NOT TO SCALE



NEW TAXILANE (TYPICAL SECTION A-A)
NOT TO SCALE

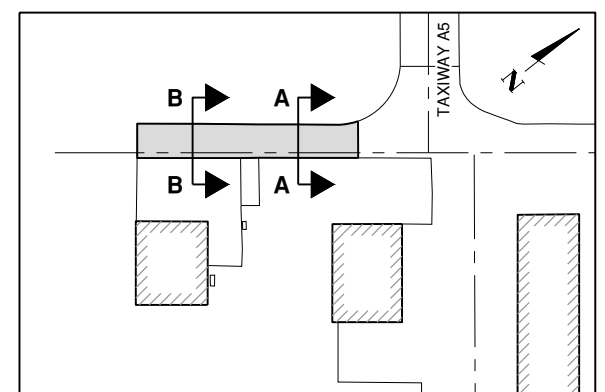


NEW APRON SECTION (TYPICAL SECTION B-B)
NOT TO SCALE

EXISTING 2.5" (& VARIES)
BITUMINOUS PAVEMENT AND 10"
AGGREGATE BASE (TO REMAIN)

LEGEND

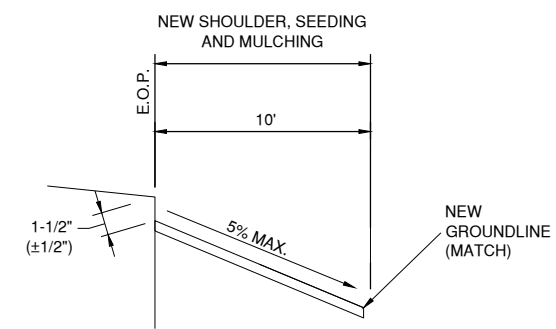
- (A) NEW 2" BITUMINOUS SURFACE COURSE (401)
- (B) NEW 3" BITUMINOUS SURFACE COURSE (401)
- (C) NEW 11" CRUSHED AGGREGATE BASE COURSE (209)
- (D) NEW 12" POROUS GRANULAR EMBANKMENT (208)
- (E) NEW SOIL STABILIZATION FABRIC (156)
- (F) NEW TACK COAT (603)
- (G) NEW PRIME COAT (602)
- (H) EXISTING 2.5" (& VARIES) BITUMINOUS SURFACE COURSE (TO BE REMOVED)
- (I) EXISTING 10" (& VARIES) AGGREGATE BASE COURSE (TO BE REMOVED)
- (J) UNCLASSIFIED EXCAVATION (152)
- (K) NEW GROUNDLINE



KEY MAP

NOTE:

1. SEE CROSS SECTIONS - 1 FOR SHOULDER, TAXILANE SAFETY AREA AND DRAINAGE SWALE SLOPES AND DIMENSIONS.



TURF SHOULDER ADJUSTMENT DETAIL
NOT TO SCALE

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CONSULTANTS

IL PROJECT: IKK-5084
 IL LETTING ITEM: 04A
 IL CONTRACT NO.: KA055

RECONSTRUCT SW
 QUADRANT APRON;
 TERMINAL AND SE HANGAR
 APRON PAVEMENT REPAIRS

JUNE 6, 2025

OWNER



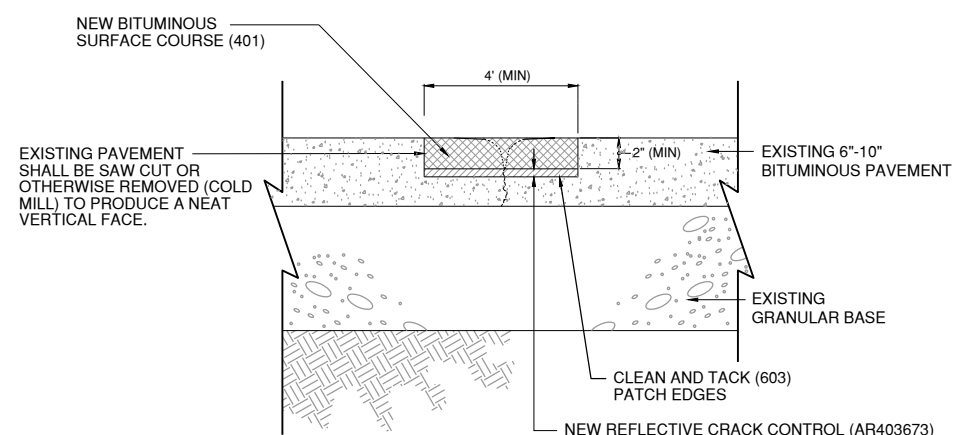
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 KANKAKEE, ILLINOIS

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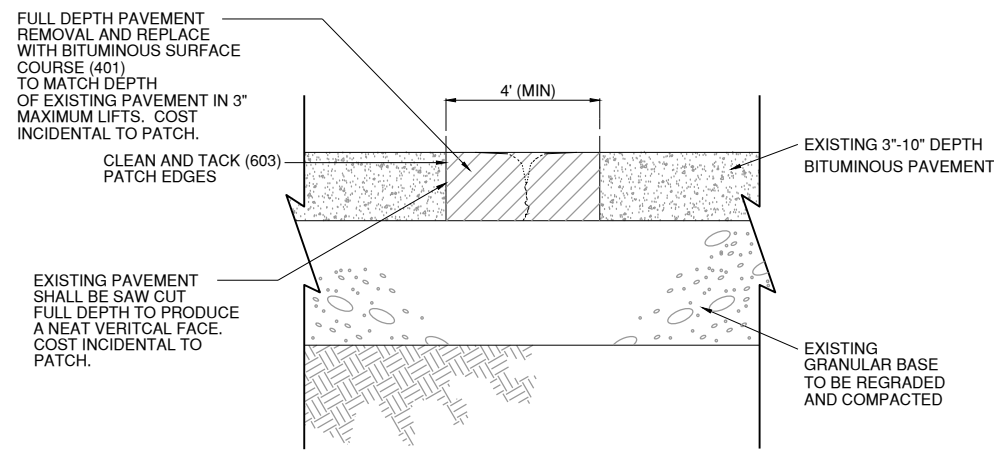
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**PAVEMENT REPAIR
 DETAILS**



**REMOVE AND REPLACE
 BITUMINOUS PAVEMENT - TYPE A**

NOT TO SCALE

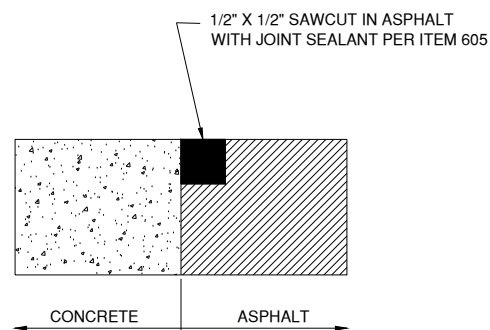


**REMOVE AND REPLACE
 BITUMINOUS PAVEMENT - TYPE B**

NOT TO SCALE

PAVEMENT REMOVAL AND REPLACEMENT NOTES

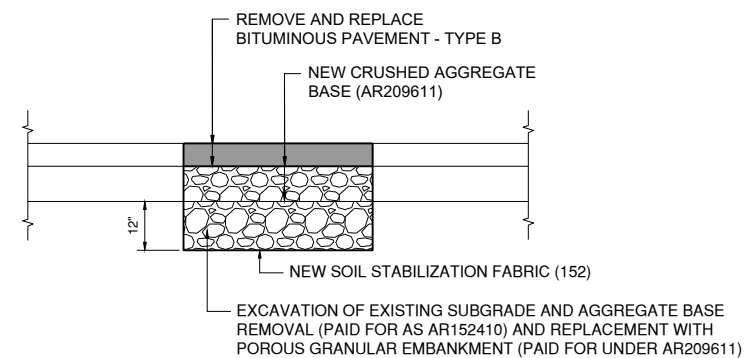
1. DEPTHS OF EXISTING PAVEMENTS SECTIONS ARE APPROXIMATE BASED ON DATA SUPPLIED BY RECORD DRAWINGS. THE CONTRACTOR SHALL VERIFY THE TYPE AND THICKNESS OF MATERIAL TO REMOVE. NO EXTRA COMPENSATION WILL BE ALLOWED FOR ANY VARIATION IN THE PAVEMENT SECTIONS ACTUALLY ENCOUNTERED.
2. PAVEMENT REMOVAL AND REPLACEMENT QUANTITIES ARE ESTIMATED. THE RESIDENT ENGINEER SHALL LAY OUT PAVEMENT REMOVAL AND REPLACEMENT AREAS IN THE FIELD DURING CONSTRUCTION.



**JOINT SEALING AT CONCRETE
 ASPHALT INTERFACE**

NOT TO SCALE

NOTE: JOINT DIMENSIONS SHALL MEET
 MANUFACTURER'S RECOMMENDATION



SUBGRADE STABILIZATION DETAIL

NOT TO SCALE

NOTES

1. LOCATION OF SUBGRADE STABILIZATION SHALL BE DETERMINED AT THE TIME OF CONSTRUCTION AND APPROVED BY THE RESIDENT ENGINEER.

1







2

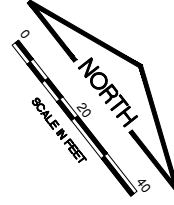
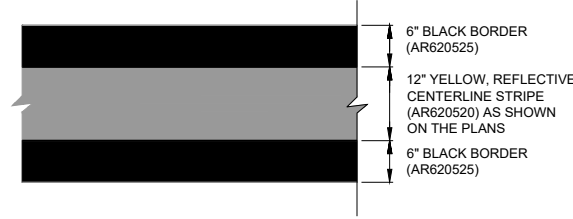
3

4

5

LEGEND

-  EXISTING BASE MOUNTED TAXIWAY EDGE LIGHT
-  EXISTING STAKE MOUNTED TAXIWAY EDGE LIGHT
-  EXISTING PAVEMENT MARKING
-  NEW PAVEMENT MARKING
-  NEW RETROREFLECTIVE MARKER
-  NEW APRON PAVEMENT
 - 2" BITUMINOUS SURFACE COURSE (401)
 - 3" BITUMINOUS SURFACE COURSE (401)
 - 11" CRUSHED AGGREGATE BASE COURSE (209)
 - 12" POROUS GRANULAR EMBANKMENT (208)
 - SOIL STABILIZATION FABRIC (156)



CONSULTANTS

IL PROJECT: IKK-5084
IL LETTING ITEM: 04A
IL CONTRACT NO.: KA055

RECONSTRUCT SW QUADRANT APRON;
TERMINAL AND SE HANGAR
APRON PAVEMENT REPAIRS

JUNE 6, 2025

OWNER



GREATER KANKAKEE AIRPORT
KANKAKEE, ILLINOIS

MARK DATE DESCRIPTION

| | |
|---------------|---|
| PROJECT NO: | 23005642.00 |
| CAD DWG FILE: | IKK5084_23005642.00_CD_1_ELEC AND TURFING |
| DESIGNED BY: | PLAN/DWG CMJ |
| DRAWN BY: | JRO |
| CHECKED BY: | -- |
| APPROVED BY: | -- |
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SHEET TITLE

**PROPOSED
IMPROVEMENT
PLAN**

D

D

C

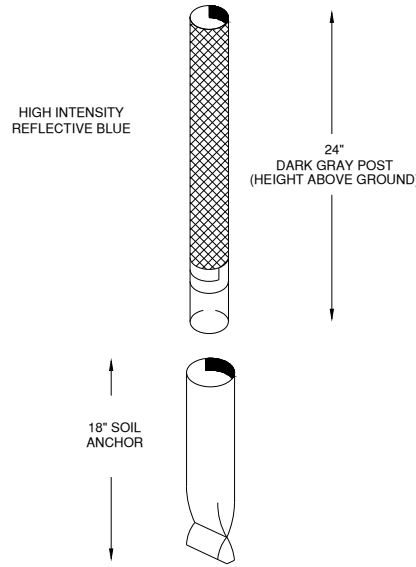
C

B

B

A

A



NOTE: RETROREFLECTIVE MARKER SHALL MET FAA AC 150/5345-39 (LATEST EDITION) AIRFIELD LIGHTING EQUIPMENT CERTIFICATION PROGRAM

NEW RETROREFLECTIVE MARKER (TYP.)
STA. = 197+62.59

197+00
STA. = 197+71.56
N: 1116568.1928
E: 1601961.2950

STA. = 197+71.56
N: 1116591.6394
E: 1601942.9688

198+00
STA. = 198+76.54
N: 1116644.9153
E: 1602034.7957

199+00
STA. = 199+62.87
N: 1116684.3366
E: 1602113.3366

200+00
STA. = 199+70.29
N: 1116659.3556
E: 1602142.5390

201+00
STA. = 200+85.35
N: 1116779.9594
E: 1602194.2631

PCC
PVMT

STA. = 199+62.87
N: 1116709.4071
E: 1602093.9616

NEW 12" YELLOW TAXIWAY
CENTERLINE MARKING
(TYP.)

BITUMINOUS
PAVEMENT

BITUMINOUS
PAVEMENT

TIE IN AT EXISTING TAXIWAY A5 CENTERLINE

CKT #3

CKT #3

CKT #3

TYX A5

55' R

NOT TO SCALE

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DRAINAGE NOTES

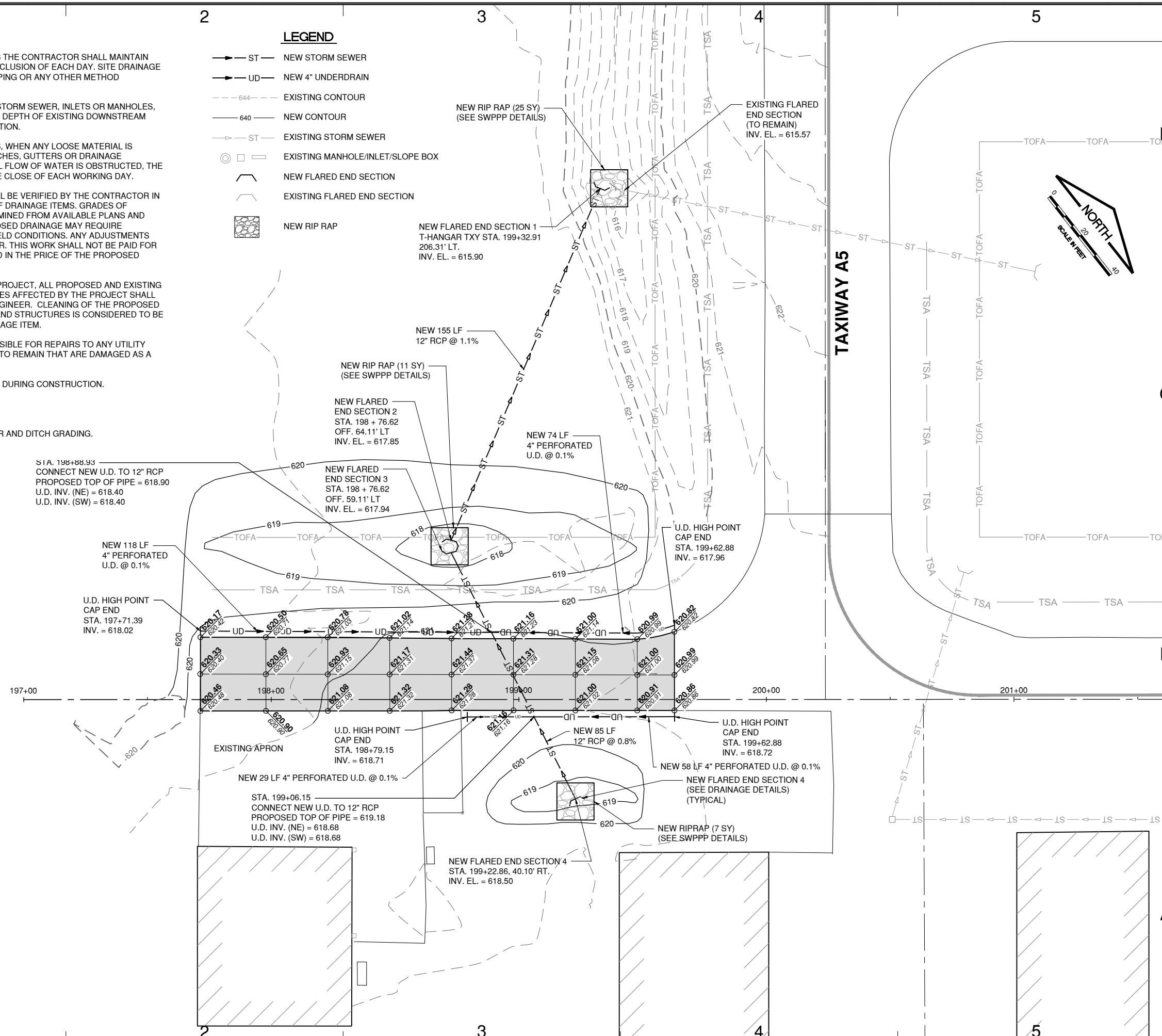
- DURING CONSTRUCTION OPERATIONS THE CONTRACTOR SHALL MAINTAIN POSITIVE SITE DRAINAGE AT THE CONCLUSION OF EACH DAY. SITE DRAINAGE MAY BE ACHIEVED BY DITCHING, PUMPING OR ANY OTHER METHOD ACCEPTABLE TO THE ENGINEER.
- BEFORE ORDERING PIPE CULVERTS, STORM SEWER, INLETS OR MANHOLES, THE CONTRACTOR SHALL VERIFY THE DEPTH OF EXISTING DOWNSTREAM STRUCTURES OR PIPES FOR CONNECTION.
- DURING CONSTRUCTION OPERATIONS, WHEN ANY LOOSE MATERIAL IS DEPOSITED IN THE FLOW LINE OF DITCHES, GUTTERS OR DRAINAGE STRUCTURES IMPEDING THE NATURAL FLOW OF WATER IS OBSTRUCTED, THE MATERIAL SHALL BE REMOVED AT THE CLOSE OF EACH WORKING DAY.
- DRAINAGE STRUCTURE GRADES SHALL BE VERIFIED BY THE CONTRACTOR IN THE FIELD PRIOR TO INSTALLATION OF DRAINAGE ITEMS. GRADES OF EXISTING SEWER LINES WERE DETERMINED FROM AVAILABLE PLANS AND SURVEY. THE INVERTS OF THE PROPOSED DRAINAGE MAY REQUIRE REVISIONS TO MEET THE EXISTING FIELD CONDITIONS. ANY ADJUSTMENTS SHALL BE DIRECTED BY THE ENGINEER. THIS WORK SHALL NOT BE PAID FOR SEPARATELY BUT SHALL BE INCLUDED IN THE PRICE OF THE PROPOSED STORM SEWER AND STRUCTURES.
- BEFORE FINAL ACCEPTANCE OF THE PROJECT, ALL PROPOSED AND EXISTING STORM SEWER LINES AND STRUCTURES AFFECTED BY THE PROJECT SHALL BE CLEANED AS DIRECTED BY THE ENGINEER. CLEANING OF THE PROPOSED AND EXISTING STORM SEWER LINES AND STRUCTURES IS CONSIDERED TO BE INCLUDED IN THE COST OF THE DRAINAGE ITEM.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRS TO ANY UTILITY LINES AND EXISTING IMPROVEMENTS TO REMAIN THAT ARE DAMAGED AS A RESULT OF THE WORK.
- CONTRACTOR TO PROTECT NEW PIPE DURING CONSTRUCTION.

GRADING NOTES

- SEE CROSS SECTIONS FOR SHOULDER AND DITCH GRADING.

LEGEND

- ST — NEW STORM SEWER
- UD — NEW 4" UNDERDRAIN
- - - 644 - - - EXISTING CONTOUR
- 640 — NEW CONTOUR
- ST — EXISTING STORM SEWER
- ⊙ □ ⊠ EXISTING MANHOLE/INLET/SLOPE BOX
- ⌒ NEW FLARED END SECTION
- ⌒ EXISTING FLARED END SECTION
- ▨ NEW RIP RAP



CONSULTANTS
 IL PROJECT: IKK-5084
 IL LETTING ITEM: 04A
 IL CONTRACT NO.: KA055

RECONSTRUCT SW QUADRANT APRON; TERMINAL AND SE HANGAR APRON PAVEMENT REPAIRS

JUNE 6, 2025



GREATER KANKAKEE AIRPORT
 KANKAKEE, ILLINOIS

| MARK | DATE | DESCRIPTION |
|------|------|-------------|
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PROJECT NO: 23005642.00
 CAD DWG FILE: IKK5084_23005642.00_CD_1_MARKING AND DRAINAGE PLAN.DWG
 DESIGNED BY: CMJ
 DRAWN BY: JRO
 CHECKED BY: DKP
 APPROVED BY: DKP
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GRADING AND DRAINAGE PLAN

Path: K:\Kankakee\23005642.00_ReconSWQuadrApron\Draw\Sheets\IKK5084_23005642.00_CD_1_Marking and Drainage Plan.dwg
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CONSULTANTS

IL PROJECT: IKK-5084
IL LETTING ITEM: 04A
IL CONTRACT NO.: KA055

RECONSTRUCT SW
QUADRANT APRON;
TERMINAL AND SE HANGAR
APRON PAVEMENT REPAIRS

JUNE 6, 2025

OWNER



GREATER KANKAKEE AIRPORT
KANKAKEE, ILLINOIS

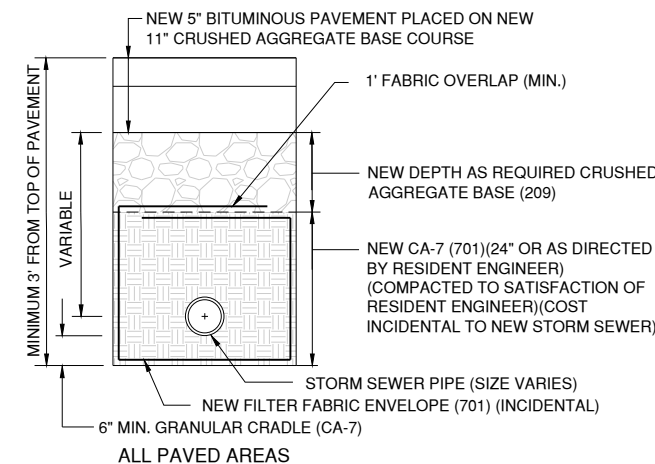
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DRAWN BY: JRO
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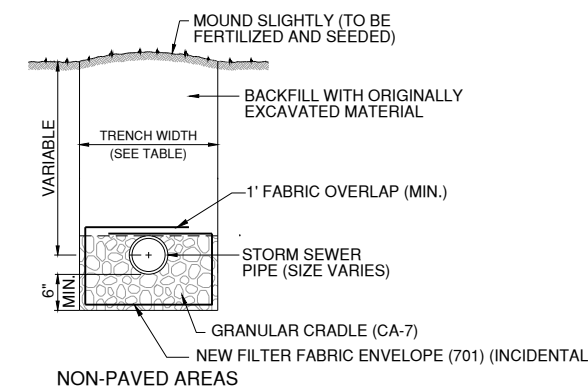
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SHEET TITLE

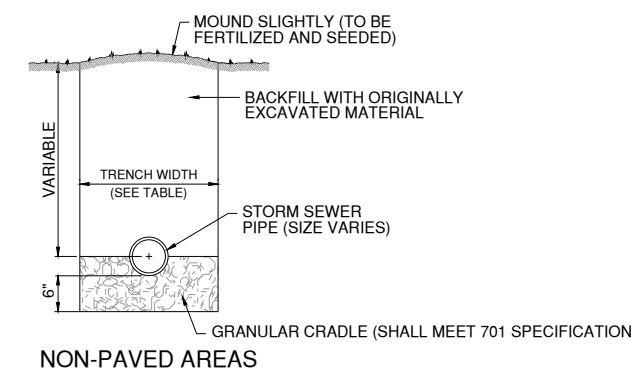
DRAINAGE DETAILS



| INSIDE DIAMETER OF STORM SEWER (INCHES) | MAXIMUM TRENCH WIDTH |
|---|----------------------|
| 6 | 3'-7" |
| 8 | 3'-9" |
| 12 | 4'-2" |
| 15 | 4'-6" |
| 18 | 4'-9" |
| 21 | 5'-0" |
| 24 | 5'-4" |
| 27 | 5'-7" |
| 30 | 5'-11" |
| 36 | 6'-6" |

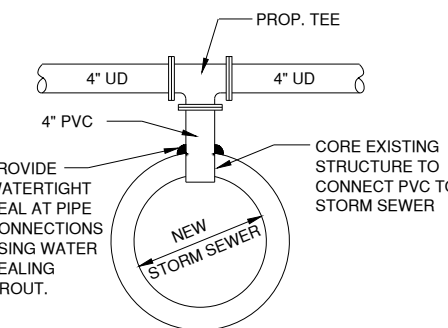


TRENCH DETAILS STORM SEWER FOR FES #3 TO FES #4
NOT TO SCALE



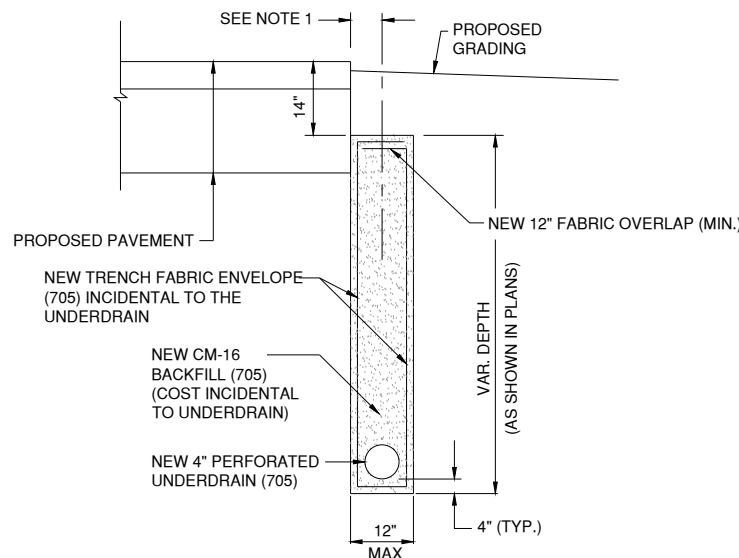
TRENCH DETAILS STORM SEWER FOR FES #1 TO FES #2
NOT TO SCALE

| INSIDE DIAMETER OF STORM SEWER (INCHES) | MAXIMUM TRENCH WIDTH |
|---|----------------------|
| 6 | 3'-7" |
| 8 | 3'-9" |
| 12 | 4'-2" |
| 15 | 4'-6" |
| 18 | 4'-9" |
| 21 | 5'-0" |
| 24 | 5'-4" |
| 27 | 5'-7" |
| 30 | 5'-11" |
| 36 | 6'-6" |



UNDERDRAIN CONNECTION DETAILS
NOT TO SCALE

UNDERDRAIN CONNECTIONS AND FITTINGS, TEES AND ELBOWS USED FOR CONNECTIONS TO PROPOSED STRUCTURES AND STORM SEWERS / EXISTING STRUCTURES AND STORM SEWERS, SHALL BE CONSIDERED INCIDENTAL TO THE PROPOSED UNDERDRAIN.



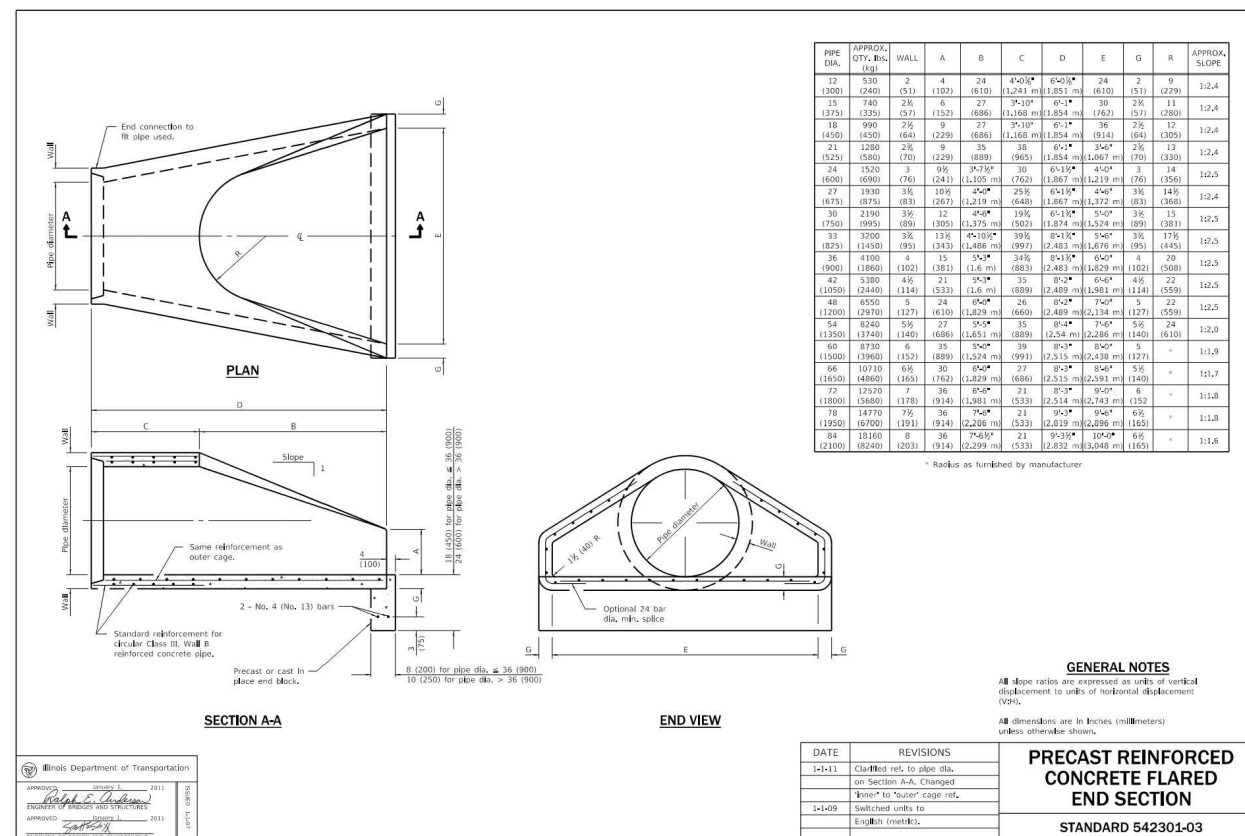
UNDERDRAIN DETAIL - PAVED AREAS
NOT TO SCALE

NOTES

- THE 4" UNDERDRAIN SHALL BE INSTALLED AFTER THE SUBGRADE IS COMPACTED.

STORM SEWER/UNDERDRAIN NOTES

- CONTRACTOR SHALL FIELD VERIFY EXISTING STORM SEWER/UNDERDRAIN INVERTS BEFORE INSTALLING PROPOSED PIPE, CONNECTIONS AND ORDERING MATERIALS.
- ALL UNDERDRAIN CONNECTIONS, CORING INTO STRUCTURES, CAPS, TEES, BENDS, STORM SEWER ETC. SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE UNDERDRAIN.
- UNDERDRAIN SLOPES FOLLOW EDGE OF PAVEMENT SLOPE UNLESS OTHERWISE NOTED.
- UNDERDRAIN CONFLICTS WITH EXISTING CONDITIONS SHALL BE RESOLVED AND COST SHALL BE INCIDENTAL TO UNDERDRAIN.



| PIPE DIA. | APPROX. QTY. (ft. / sq. ft.) | WALL | A | B | C | D | E | G | R | APPROX. SLOPE |
|-----------|------------------------------|---------|------------|-------------|-------------------|-------------------|----------------|---------|----------|---------------|
| 12 | 530 (240) | 2 (51) | 4 (102) | 24 (610) | 4'0.5" (1,028 mm) | 6'0.5" (1,851 mm) | 24 (610) | 3 (76) | 9 (229) | 1:2.4 |
| 15 | 740 (335) | 2 (51) | 6 (152) | 27 (686) | 4'10" (1,067 mm) | 6'1" (1,854 mm) | 30 (762) | 3 (76) | 11 (280) | 1:2.4 |
| 18 | 990 (450) | 2 (51) | 9 (229) | 33 (838) | 4'10" (1,067 mm) | 6'1" (1,854 mm) | 36 (914) | 3 (76) | 12 (305) | 1:2.4 |
| 21 | 1280 (580) | 2 (51) | 12 (305) | 39 (991) | 4'10" (1,067 mm) | 6'1" (1,854 mm) | 42 (1,067 mm) | 3 (76) | 13 (330) | 1:2.4 |
| 24 | 1520 (690) | 2 (51) | 15 (381) | 45 (1,143) | 4'10" (1,067 mm) | 6'1" (1,854 mm) | 48 (1,219 mm) | 3 (76) | 14 (355) | 1:2.5 |
| 27 | 1930 (875) | 3 (76) | 18 (457) | 54 (1,371) | 4'10" (1,067 mm) | 6'1" (1,854 mm) | 54 (1,371 mm) | 3 (76) | 15 (381) | 1:2.4 |
| 30 | 2190 (995) | 3 (76) | 21 (533) | 63 (1,615) | 4'10" (1,067 mm) | 6'1" (1,854 mm) | 60 (1,524 mm) | 3 (76) | 15 (381) | 1:2.5 |
| 33 | 2500 (1145) | 3 (76) | 24 (610) | 72 (1,829) | 4'10" (1,067 mm) | 6'1" (1,854 mm) | 66 (1,676 mm) | 3 (76) | 15 (381) | 1:2.5 |
| 36 | 3100 (1450) | 4 (102) | 30 (762) | 84 (2,130) | 4'10" (1,067 mm) | 6'1" (1,854 mm) | 72 (1,829 mm) | 4 (102) | 15 (381) | 1:2.5 |
| 42 | 3580 (1650) | 4 (102) | 36 (914) | 96 (2,438) | 4'10" (1,067 mm) | 6'1" (1,854 mm) | 84 (2,130 mm) | 4 (102) | 22 (559) | 1:2.5 |
| 48 | 4150 (1920) | 4 (102) | 42 (1,067) | 108 (2,743) | 4'10" (1,067 mm) | 6'1" (1,854 mm) | 96 (2,438 mm) | 4 (102) | 22 (559) | 1:2.5 |
| 54 | 4850 (2250) | 5 (127) | 48 (1,219) | 120 (3,048) | 4'10" (1,067 mm) | 6'1" (1,854 mm) | 108 (2,743 mm) | 5 (127) | 22 (559) | 1:2.5 |
| 60 | 5580 (2580) | 5 (127) | 54 (1,371) | 132 (3,353) | 4'10" (1,067 mm) | 6'1" (1,854 mm) | 120 (3,048 mm) | 5 (127) | 22 (559) | 1:2.5 |
| 66 | 6350 (2950) | 6 (152) | 60 (1,524) | 144 (3,658) | 4'10" (1,067 mm) | 6'1" (1,854 mm) | 132 (3,353 mm) | 5 (127) | 22 (559) | 1:1.7 |
| 72 | 7180 (3350) | 6 (152) | 72 (1,829) | 168 (4,267) | 4'10" (1,067 mm) | 6'1" (1,854 mm) | 156 (3,992 mm) | 5 (127) | 22 (559) | 1:1.8 |
| 78 | 8080 (3780) | 7 (178) | 84 (2,130) | 180 (4,572) | 4'10" (1,067 mm) | 6'1" (1,854 mm) | 180 (4,572 mm) | 6 (152) | 22 (559) | 1:1.8 |
| 84 | 9050 (4250) | 8 (203) | 96 (2,438) | 216 (5,486) | 4'10" (1,067 mm) | 6'1" (1,854 mm) | 216 (5,486 mm) | 6 (152) | 22 (559) | 1:1.6 |

GENERAL NOTES
Slope ratios are expressed as units of vertical displacement to units of horizontal displacement (V:H).
Dimensions are in inches (millimeters) unless otherwise shown.

| DATE | REVISIONS |
|--------|---|
| 1-1-11 | Clarified ref. to pipe dia. |
| 1-1-11 | Rev Section A-A, changed "inner" to "outer" cage ref. |
| 1-1-09 | Switched units to English (metric). |

PRECAST REINFORCED CONCRETE FLARED END SECTION
STANDARD 542301-03

CONSULTANTS

IL PROJECT: IKK-5084
 IL LETTING ITEM: 04A
 IL CONTRACT NO.: KA055

RECONSTRUCT SW
 QUADRANT APRON;
 TERMINAL AND SE HANGAR
 APRON PAVEMENT REPAIRS

JUNE 6, 2025

OWNER



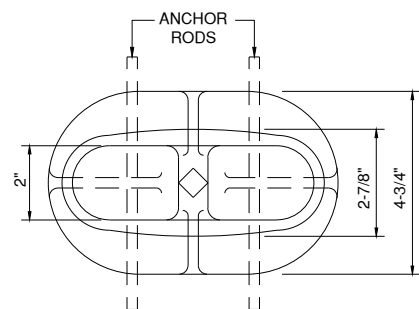
GREATER KANKAKEE AIRPORT
 KANKAKEE, ILLINOIS

MARK DATE DESCRIPTION

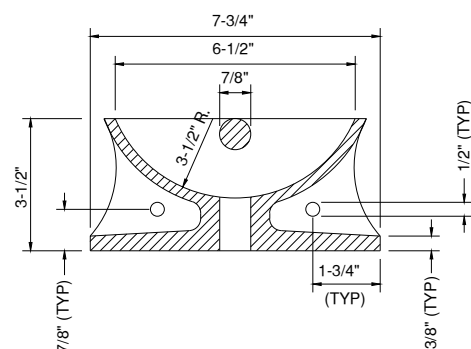
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| CAD DWG FILE: | IKK5084_23005642.00_CD_T_MISCELLANEOUS DETAILS.DWG |
| DESIGNED BY: | -- |
| DRAWN BY: | -- |
| CHECKED BY: | -- |
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SHEET TITLE

MISCELLANEOUS
 DETAILS



PLAN



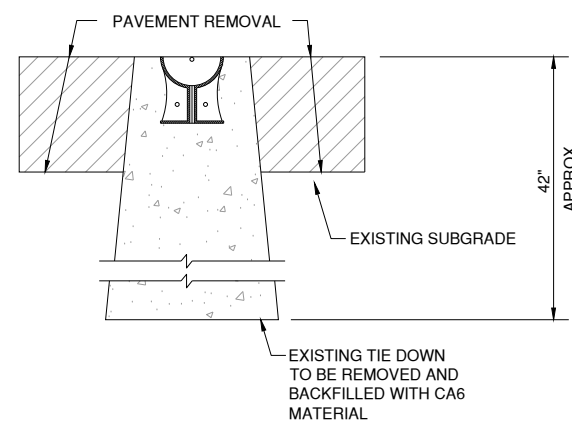
SECTION

MOORING EYE DETAIL

NOT TO SCALE

NOTES

1. MOORING CASTINGS SHALL BE NEENAH SEMI-STEEL AIRPORT MOORING EYE CATALOG NO. R3490 OR APPROVED EQUIVALENT.
2. ANCHOR RODS TO BE NO. 3 DEFORMED REINFORCING STEEL 15" LONG AND SHALL BE BENT DOWNWARD AT 45°.

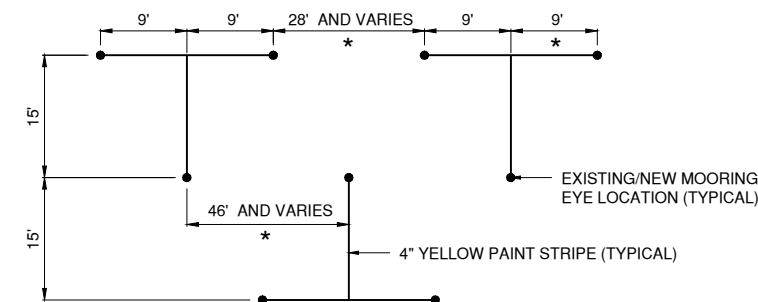


TIE DOWN REMOVAL DETAIL

NOT TO SCALE

NOTES:

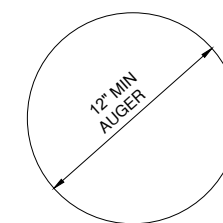
1. THE VOID LEFT BY THE REMOVAL SHALL BE BACKFILLED AND COMPACTED TO THE BOTTOM OF THE NEW HMA PAVEMENT WITH CA6 BACKFILL MATERIAL - PER SPECIAL PROVISIONS 701-2.12 AND COMPACTED PER SPECIAL PROVISION 701(COST INCIDENTAL).
2. SURROUNDING AGGREGATE BASE DISTURBED SHALL BE REMOVED AND REPLACED WITH 9 INCHES OF CRUSHED AGGREGATE BASE. REMOVAL OF THE EXISTING AGGREGATE BASE SHALL BE INCIDENTAL TO REMOVE TIEDOWN PAY ITEM.
3. EXISTING TIE DOWN SHALL BE DISPOSED OFF OFFSITE BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.



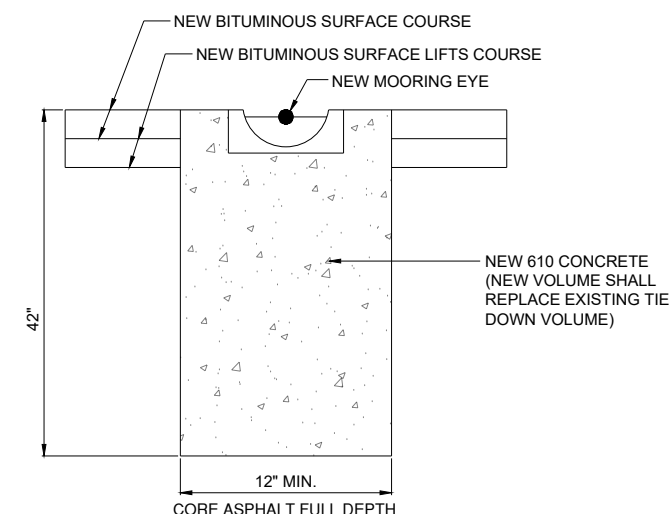
TIEDOWN DETAIL

NOT TO SCALE

* NOTE: CONTRACTOR SHALL ADJUST SPACING TO MATCH EXISTING FIELD CONDITIONS



PLAN



SECTION

MOORING EYE INSTALLATION DETAIL

NOT TO SCALE

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CONSULTANTS

IL PROJECT: IKK-5084
IL LETTING ITEM: 04A
IL CONTRACT NO.: KA055

RECONSTRUCT SW
QUADRANT APRON;
TERMINAL AND SE HANGAR
APRON PAVEMENT REPAIRS

JUNE 6, 2025

OWNER



GREATER KANKAKEE AIRPORT
KANKAKEE, ILLINOIS

| MARK | DATE | DESCRIPTION |
|---------------|---|-------------|
| PROJECT NO: | 23005642.00 | |
| CAD DWG FILE: | IKK5084_23005642.00_CD_1_SWPPP PLAN.DWG | |
| DESIGNED BY: | CMJ | |
| DRAWN BY: | JRO | |
| CHECKED BY: | -- | |
| APPROVED BY: | -- | |
| COPYRIGHT: | CRAWFORD, MURPHY & TILLY, INC. 2025 | |

SHEET TITLE
**STORM WATER
POLLUTION
PREVENTION PLAN**

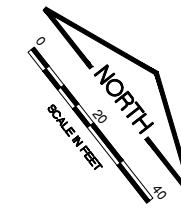
LEGEND

- ST — EXISTING STORM SEWER
- ST — NEW STORM SEWER
- NEW FLARED END SECTION
- EXISTING FLARED END SECTION
- NEW SHOULDER ADJUSTMENT, SEEDING AND MULCHING
- SF — NEW SILT FENCE
- NEW GRADING LIMITS
- NEW INLET PROTECTION
- NEW RIP RAP
- NEW APRON PAVEMENT
- 640 — EXISTING CONTOURS
- 650 — PROPOSED CONTOURS
- EROSION CONTROL BLANKET

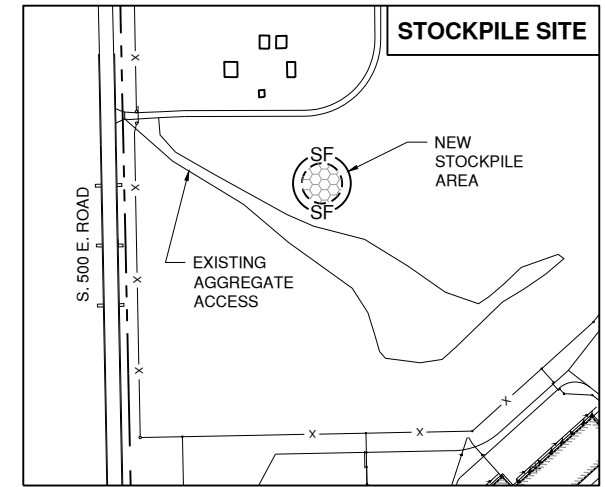
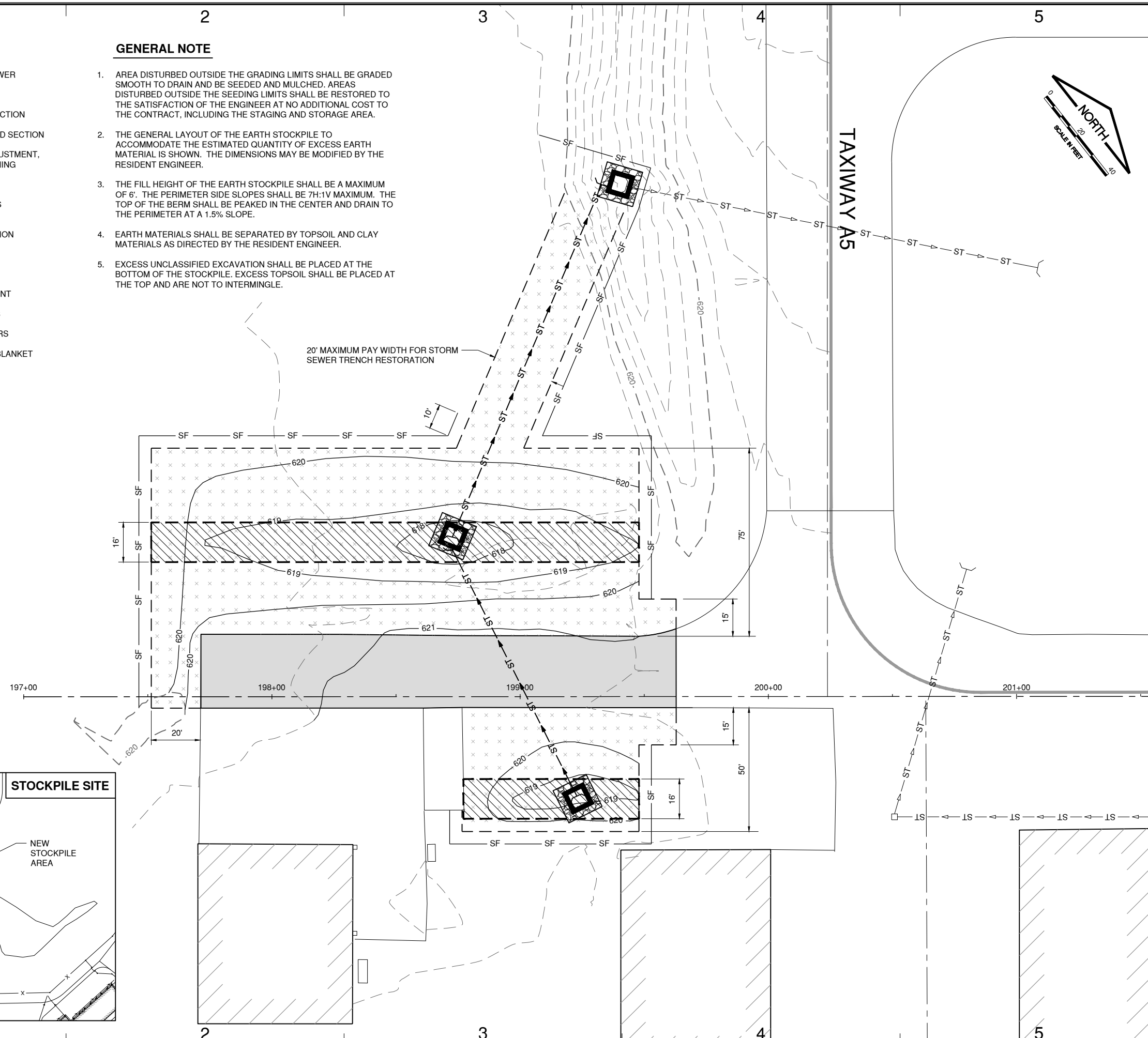
GENERAL NOTE

1. AREA DISTURBED OUTSIDE THE GRADING LIMITS SHALL BE GRADED SMOOTH TO DRAIN AND BE SEEDED AND MULCHED. AREAS DISTURBED OUTSIDE THE SEEDING LIMITS SHALL BE RESTORED TO THE SATISFACTION OF THE ENGINEER AT NO ADDITIONAL COST TO THE CONTRACT, INCLUDING THE STAGING AND STORAGE AREA.
2. THE GENERAL LAYOUT OF THE EARTH STOCKPILE TO ACCOMMODATE THE ESTIMATED QUANTITY OF EXCESS EARTH MATERIAL IS SHOWN. THE DIMENSIONS MAY BE MODIFIED BY THE RESIDENT ENGINEER.
3. THE FILL HEIGHT OF THE EARTH STOCKPILE SHALL BE A MAXIMUM OF 6'. THE PERIMETER SIDE SLOPES SHALL BE 7H:1V MAXIMUM. THE TOP OF THE BERM SHALL BE PEAKED IN THE CENTER AND DRAIN TO THE PERIMETER AT A 1.5% SLOPE.
4. EARTH MATERIALS SHALL BE SEPARATED BY TOPSOIL AND CLAY MATERIALS AS DIRECTED BY THE RESIDENT ENGINEER.
5. EXCESS UNCLASSIFIED EXCAVATION SHALL BE PLACED AT THE BOTTOM OF THE STOCKPILE. EXCESS TOPSOIL SHALL BE PLACED AT THE TOP AND ARE NOT TO INTERMINGLE.

20' MAXIMUM PAY WIDTH FOR STORM SEWER TRENCH RESTORATION



TAXIWAY A5



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CONSULTANTS

IL PROJECT: IKK-5084
IL LETTING ITEM: 04A
IL CONTRACT NO.: KA055

RECONSTRUCT SW
QUADRANT APRON;
TERMINAL AND SE HANGAR
APRON PAVEMENT REPAIRS

JUNE 6, 2025

OWNER



GREATER KANKAKEE AIRPORT
KANKAKEE, ILLINOIS

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| PROJECT NO: | 23005642.00 | |
| CAD DWG FILE: | IKK5084_23005642.00_CD_1_SWPPP NOTES AND DETAILS.DWG | |
| DESIGNED BY: | CMJ | |
| DRAWN BY: | JRO | |
| CHECKED BY: | -- | |
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SHEET TITLE
**STORM WATER POLLUTION
PREVENTION PLAN NOTES**

STORM WATER POLLUTION PREVENTION PLAN

THE FOLLOWING PLAN IS ESTABLISHED AND INCORPORATED IN THE PROJECT TO DIRECT THE CONTRACTOR IN THE PLACEMENT OF TEMPORARY EROSION CONTROL SYSTEMS AND TO PROVIDE A STORM WATER POLLUTION PREVENTION PLAN FOR COMPLIANCE WITH NPDES.

THE PURPOSE OF THIS PLAN IS TO MINIMIZE EROSION WITHIN THE CONSTRUCTION SITE AND TO LIMIT SEDIMENTS FROM LEAVING THE SITE BY UTILIZING PROPER TEMPORARY EROSION CONTROL SYSTEMS AND PROVIDING GROUND COVER WITHIN A REASONABLE AMOUNT OF TIME.

CERTAIN EROSION CONTROL FACILITIES SHALL BE INSTALLED BY THE CONTRACTOR AT THE BEGINNING OF CONSTRUCTION. OTHER ITEMS SHALL BE INSTALLED BY THE CONTRACTOR AS DIRECTED BY THE ENGINEER ON A CASE BY CASE SITUATION DEPENDING ON THE CONTRACTOR'S SEQUENCE OF ACTIVITIES, TIME OF YEAR, AND EXPECTED WEATHER CONDITIONS.

THE CONTRACTOR SHALL INSTALL PERMANENT EROSION CONTROL SYSTEMS AND SEEDING WITHIN A TIMEFRAME SPECIFIED HEREIN AND AS DIRECTED BY THE ENGINEER, THEREFORE MINIMIZING THE AMOUNT OF AREA SUSCEPTIBLE TO EROSION AND REDUCING THE AMOUNT OF TEMPORARY SEEDING, WHICH WILL BE AT THE CONTRACTOR'S COST. THE ENGINEER WILL DETERMINE IF ANY TEMPORARY EROSION CONTROL SYSTEMS SHOWN IN THE PLAN CAN BE DELETED AND IF ANY ADDITIONAL TEMPORARY EROSION CONTROL SYSTEMS, WHICH ARE NOT INCLUDED IN THIS PLAN, SHALL BE ADDED. THE CONTRACTOR SHALL PERFORM ALL WORK AS DIRECTED BY THE ENGINEER AND AS SHOWN ON THE PLANS.

SITE DESCRIPTION:

THE FOLLOWING IS A DESCRIPTION OF THE CONSTRUCTION ACTIVITY WHICH IS THE SUBJECT OF THIS PLAN:

THIS PROJECT CONSISTS OF THE RECONSTRUCTION OF AN EXISTING BITUMINOUS PAVEMENT AT THE GREATER KANKAKEE AIRPORT. THE PROJECT INCLUDES STORM SEWER INSTALLATION, BITUMINOUS PAVING, PAVEMENT MARKING AND OTHER MISCELLANEOUS CONSTRUCTION WORK.

DESCRIPTION OF CONSTRUCTION ACTIVITY:

THE FOLLOWING IS A DESCRIPTION OF THE INTENDED SEQUENCE OF MAJOR ACTIVITIES WHICH WILL DISTURB SOILS FOR MAJOR PORTIONS OF THE CONSTRUCTION SITE, SUCH AS GRUBBING, EXCAVATION AND GRADING:

- PLACEMENT, MAINTENANCE, REMOVAL AND PROPER CLEAN-UP OF TEMPORARY EROSION CONTROL.
- REMOVAL OF EXISTING BITUMINOUS PAVEMENT. CONSTRUCTION OF EARTHWORK AND GRADING.
- DRAINAGE INFRASTRUCTURE INSTALLATION.
- CONSTRUCT NEW PAVEMENT.
- SEEDING AND MULCHING DISTURBED AREAS.
- AIRFIELD REFLECTOR INSTALLATION.
- INSTALLATION OF NEW PAVEMENT MARKING.
- REMOVAL AND DISPOSAL OF TEMPORARY SOIL EROSION AND SEDIMENT CONTROL MEASURES.

AREA OF CONSTRUCTION SITE:

THE TOTAL AREA OF THE CONSTRUCTION SITE THAT WILL BE DISTURBED BY EXCAVATION, GRADING AND OTHER ACTIVITIES IS LESS THAN ONE (1) ACRE.

OTHER REPORTS, STUDIES AND PLANS WHICH AID IN THE DEVELOPMENT OF THE STORM WATER POLLUTION PREVENTION PLAN AS REFERENCED DOCUMENTS:

1. INFORMATION OF THE SOILS AND TERRAIN WITHIN THE SITE WAS OBTAINED FROM TOPOGRAPHIC SURVEYS AND SOIL BORINGS THAT WERE UTILIZED FOR THE DEVELOPMENT OF THE PROPOSED TEMPORARY EROSION CONTROL SYSTEMS.
2. PROJECT PLAN DOCUMENTS, SPECIFICATION AND SPECIAL PROVISIONS, AND PLAN DRAWINGS INDICATING DRAINAGE PATTERNS AND APPROXIMATE SLOPES ANTICIPATED AFTER GRADING ACTIVITIES WERE UTILIZED FOR THE PROPOSED PLACEMENT OF THE TEMPORARY EROSION CONTROL SYSTEMS.

DRAINAGE TRIBUTARIES AND SENSITIVE AREAS RECEIVING RUNOFF FROM THIS CONSTRUCTION SITE:

THE CONSTRUCTION SITE DRAINS INTO THE KANKAKEE RIVER THROUGH A STORM SEWER SYSTEM.

EROSION AND SEDIMENT CONTROL:

DESCRIPTION OF STABILIZATION PRACTICES AT THE BEGINNING OF CONSTRUCTION:

THE DRAWINGS SPECIFICATIONS AND SPECIAL PROVISIONS WILL ENSURE THAT EXISTING VEGETATION IS PRESERVED WHERE ATTAINABLE AND DISTURBED PORTIONS OF THE SITE WILL BE STABILIZED. STABILIZATION PRACTICES INCLUDE: TEMPORARY SEEDING, PERMANENT SEEDING, MULCHING, SOD, PROTECTION OF TREES, PRESERVATION OF NATURAL VEGETATION, AND ALL OTHER APPROPRIATE MEASURES AS DIRECTED BY THE ENGINEER. STABILIZATION MEASURES SHALL BE INITIATED AS SOON AS PRACTICABLE IN PORTIONS OF THE SITE WHERE CONSTRUCTION ACTIVITIES HAVE TEMPORARILY OR PERMANENTLY CEASED, BUT IN NO CASE MORE THAN 7 DAYS AFTER THE CONSTRUCTION ACTIVITY IN THAT PORTION OF THE SITE HAS TEMPORARILY OR PERMANENTLY CEASED.

AREAS OF EXISTING VEGETATION (WOOD AND GRASSLANDS) OUTSIDE THE PROPOSED CONSTRUCTION LIMITS SHALL BE IDENTIFIED BY THE ENGINEER FOR PRESERVING AND SHALL BE PROTECTED FROM CONSTRUCTION ACTIVITIES.

DEAD, DISEASED, OR UNSUITABLE VEGETATION WITHIN THE SITE SHALL BE REMOVED AS DIRECTED BY THE ENGINEER.

THIS PLAN HAS BEEN PREPARED TO COMPLY WITH THE PROVISIONS OF THE NPDES PERMIT NUMBER ILR10, ISSUED BY THE ILLINOIS ENVIRONMENTAL PROTECTION AGENCY FOR STORM WATER DISCHARGES FROM CONSTRUCTION SITE ACTIVITIES.

EROSION AND SEDIMENT CONTROL:

DESCRIPTION OF STABILIZATION PRACTICES DURING CONSTRUCTION:

DURING CONSTRUCTION, AREAS OUTSIDE THE CONSTRUCTION LIMITS AS OUTLINED PREVIOUSLY HEREIN SHALL BE PROTECTED. THE CONTRACTOR SHALL NOT USE THIS AREA FOR STAGING (EXCEPT AS DESCRIBED ON THE PLANS AND DIRECTED BY THE ENGINEER), PARKING OF VEHICLES OR CONSTRUCTION EQUIPMENT, STORAGE OF MATERIALS, OR OTHER CONSTRUCTION RELATED ACTIVITIES.

WITHIN THE CONSTRUCTION LIMITS, AREAS WHICH MAY BE SUSCEPTIBLE TO EROSION AS DETERMINED BY THE ENGINEER SHALL REMAIN UNDISTURBED UNTIL FULL SCALE CONSTRUCTION IS UNDERWAY TO PREVENT UNNECESSARY SOIL EROSION.

EARTH STOCKPILES SHALL BE TEMPORARILY SEEDED, AT THE CONTRACTOR'S EXPENSE, IF THEY ARE TO REMAIN UNUSED FOR MORE THAN SEVEN (7) DAYS.

THE DOWN STREAM SIDE OF ALL STOCKPILES SHALL BE ENCOMPASSED WITH EROSION CONTROL BARRIER.

AS CONSTRUCTION PROCEEDS, THE CONTRACTOR SHALL INSTITUTE THE FOLLOWING AS DIRECTED BY THE ENGINEER:

A. PLACE TEMPORARY EROSION CONTROL FACILITIES AT LOCATIONS SHOWN ON THE PLANS.

CONSTRUCTION EQUIPMENT SHALL BE STORED AND FUELED ONLY AT DESIGNATED LOCATIONS WITHIN THE STAGING AREA. ALL NECESSARY MEASURES SHALL BE TAKEN TO CONTAIN ANY FUEL OR POLLUTANT IN ACCORDANCE WITH EPA WATER QUALITY REGULATIONS. LEAKING EQUIPMENT OR SUPPLIES SHALL BE IMMEDIATELY REPAIRED OR REMOVED FROM THE SITE.

THE RESIDENT ENGINEER SHALL INSPECT THE PROJECT PERIODICALLY DURING CONSTRUCTION ACTIVITIES. INSPECTION SHALL ALSO BE DONE WEEKLY AND AFTER RAINS OF 1/2" OR GREATER OR EQUIVALENT SNOWFALL AND DURING WINTER SHUTDOWN PERIOD. THE PROJECT SHALL ADDITIONALLY BE INSPECTED BY THE RESIDENT ENGINEER ON A BI-WEEKLY BASIS TO DETERMINE THAT THE EROSION AND SEDIMENT CONTROL EFFORTS ARE IN PLACE AND EFFECTIVE AND IF OTHER EROSION CONTROL WORK IS NECESSARY.

SEDIMENT COLLECTED DURING CONSTRUCTION OF THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON SITE ON A REGULAR BASIS AS DIRECTED BY THE ENGINEER. THE COST OF THIS MAINTENANCE SHALL BE INCIDENTAL TO THE CONTRACT.

THE TEMPORARY EROSION CONTROL SYSTEMS SHALL BE REMOVED AS DIRECTED BY THE ENGINEER AFTER USE IS NO LONGER NEEDED OR NO LONGER FUNCTIONING. THE COST OF THIS REMOVAL SHALL BE INCIDENTAL TO THE CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREVENTING SOIL CONTAMINATION FROM BUILDING MATERIALS, FERTILIZERS, CHEMICALS, PAVEMENT MARKING, WASTE PILES, FUEL CONTAINMENT, AND ANY OTHER POTENTIAL HAZARDOUS MATERIALS THAT MAY EXIST ONSITE.

NO DEDICATED CONCRETE OR ASPHALT BATCH PLANTS SHALL BE LOCATED ON THIS SITE.

DESCRIPTION OF STRUCTURAL PRACTICES AFTER FINAL GRADING:

TEMPORARY EROSION CONTROL SYSTEMS SHALL BE LEFT IN PLACE WITH PROPER MAINTENANCE UNTIL PERMANENT EROSION CONTROL IS IN PLACE AND WORKING PROPERLY AND ALL PROPOSED TURF AREAS ARE SEEDED AND ESTABLISHED.

COST OF MAINTAINING THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE INCIDENTAL TO THE CONTRACT.

ONCE PERMANENT EROSION CONTROL SYSTEMS AS PROPOSED IN THE PLANS ARE FUNCTIONAL AND ESTABLISHED, TEMPORARY ITEMS SHALL BE REMOVED, CLEANED UP, AND DISTURBED TURF RE-SEEDED AND/OR SODDED.

MAINTENANCE AFTER CONSTRUCTION:

CONSTRUCTION IS COMPLETE AFTER FINAL ACCEPTANCE BY THE ILLINOIS DIVISION OF AERONAUTICS. MAINTENANCE OF TEMPORARY AND PERMANENT EROSION CONTROL SYSTEMS UP TO THIS DATE WILL BE REQUIRED BY THE CONTRACTOR.

GENERAL NOTES FOR SOIL EROSION AND SEDIMENT CONTROL:

1. ALL TREE PROTECTION, SEDIMENT CONTROL MEASURES, AND PERMANENT AND TEMPORARY STORM WATER PRACTICES SHALL BE IN PLACE PRIOR TO STARTING CONSTRUCTION.
2. NO WORK SHALL BE PERFORMED IN FLOWING WATER. WORK IN AND NEAR FLOWING WATER SHALL BE ISOLATED FROM CONCENTRATED FLOWS OR STREAM FLOWS AT ALL TIMES. THE USE OF EARTHEN MATERIAL FOR ISOLATION WILL NOT BE ACCEPTABLE.
3. CONSTRUCTION MATERIALS AND/OR OTHER STOCKPILES SHALL NOT BE LOCATED ON STREAM BANKS NOR IN THE PATH OF STREAM FLOW.
4. TEMPORARY EROSION CONTROL DEVICES SHALL BE CONSTRUCTED AS SHOWN ON THE PLANS OR AS DIRECTED BY THE ENGINEER.
5. PERMANENT SEEDING SHALL BE USED WHENEVER POSSIBLE. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR PROLONG GRADING OR SHAPING SO THAT THE ENTIRE PROJECT CAN BE PERMANENTLY SEEDED AT ONE TIME.
6. THE CONTRACTOR SHALL INSPECT ADJACENT STREETS DAILY AND CLEAN ADJACENT STREETS WHEN NECESSARY. ADJACENT STREETS SHALL BE KEPT FREE OF SOIL AND DEBRIS.
7. SHOULD IT BE NECESSARY TO REMOVE ANY EROSION CONTROL DEVICES FOR CONSTRUCTION REASONS, THE CONTRACTOR SHALL FIRST OBTAIN PERMISSION AND SHALL REPLACE AND/OR REPAIR THE REMOVED DEVICES THE SAME DAY. THE COST OF REMOVING AND REPLACING THE DEVICE SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT.
8. ALL OTHER SOIL EROSION AND SEDIMENT CONTROL DEVICES AND MEASURES DEEMED NECESSARY BY THE RESIDENT ENGINEER, KANKAKEE COUNTY, GREATER KANKAKEE AIRPORT AIRPORT, IDOT DIVISION OF AERONAUTICS, AND THE IEPA SHALL BE IMPLEMENTED IMMEDIATELY UPON NOTIFICATION OF THE CONTRACTOR.
9. THE CONTRACTOR SHALL PROVIDE LOCATIONS FOR CONCRETE TRUCK WASHOUT, AS APPROVED BY THE ENGINEER, PRIOR TO ANY CONCRETE POURS. THESE LOCATIONS SHALL NOT BE NEAR ANY STREAM OR BODY OF WATER. LOCATIONS SHALL BE APPROVED BY THE ENGINEER PRIOR TO ANY CONCRETE POURS. ADDITIONALLY THE CONTRACTOR SHALL PROVIDE ADEQUATE FACILITIES TO WASH OUT PAVING EQUIPMENT AND FINISHING TOOLS. ALL WASTE WATER AND EXCESS CONCRETE MATERIALS SHALL BE CONTAINED BY AN APPROVED CONCRETE WASHOUT FACILITY.
10. THE CONTRACTOR SHALL COORDINATE CONSTRUCTION ACTIVITIES TO ENSURE THAT EROSION CONTROL MEASURES ARE CONSISTENT BETWEEN ALL PROJECT PHASES AND ALL SUB-CONTRACTORS.
11. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS TO PROTECT WETLANDS TO REMAIN FROM DAMAGE BY SEDIMENT, CONSTRUCTION EQUIPMENT, OR BY HIS PERSONNEL. THE CONTRACTOR SHALL ASSURE THAT DEBRIS OR ANY CONSTRUCTION MATERIAL IS NOT DISPOSED OF IN THE WETLANDS.
12. WATER PUMPED OR OTHERWISE DISCHARGED FROM THE SITE DURING CONSTRUCTION DEWATERING SHALL BE FILTERED BY AN APPROVED MEANS.
13. SEDIMENT COLLECTED DURING CONSTRUCTION BY THE VARIOUS TEMPORARY EROSION CONTROL SYSTEMS SHALL BE DISPOSED OF ON A REGULAR BASIS. SEDIMENT SHALL BE REMOVED FROM EROSION CONTROL SYSTEMS WHEN THE HEIGHT OF THE SEDIMENT EXCEEDS ONE-HALF OF THE HEIGHT OF THE DEVICE OR AS RECOMMENDED BY THE MANUFACTURER, WHICHEVER IS LESS.
14. ALL EROSION CONTROL MEASURES SHALL BE KEPT OPERATIONAL AND MAINTAINED CONTINUOUSLY THROUGHOUT THE PERIOD OF LAND DISTURBANCE UNTIL PERMANENT SOIL EROSION AND SEDIMENT CONTROL MEASURES ARE OPERATIONAL.
15. THE CONDITION OF THE CONSTRUCTION SITE FOR WINTER SHUTDOWN SHALL BE ADDRESSED EARLY IN THE FALL GROWING SEASON SO THAT SLOPES AND OTHER BARE EARTH AREAS MAY BE STABILIZED WITH TEMPORARY AND/OR PERMANENT VEGETATIVE COVER. ALL OPEN AREAS THAT ARE TO REMAIN IDLE THROUGHOUT THE WINTER SHALL RECEIVE TEMPORARY EROSION CONTROL MEASURES INCLUDING TEMPORARY SEEDING, MULCHING AND/OR EROSION CONTROL BLANKET PRIOR TO THE END OF THE FALL GROWING SEASON. THE AREAS TO BE WORKED BEYOND THE END OF THE GROWING SEASON MUST INCORPORATE SOIL STABILIZATION MEASURES THAT DO NOT RELY ON VEGETATIVE COVER SUCH AS EROSION CONTROL BLANKET AND HEAVY MULCHING.
16. PERMANENT STABILIZATION SHALL BE COMPLETED WITHIN 7 DAYS FOR AREAS WHERE WORK IS COMPLETED.

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CONSULTANTS

IL PROJECT: IKK-5084
IL LETTING ITEM: 04A
IL CONTRACT NO.: KA055

RECONSTRUCT SW
QUADRANT APRON;
TERMINAL AND SE HANGAR
APRON PAVEMENT REPAIRS

JUNE 6, 2025

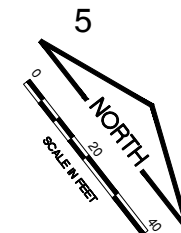
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GREATER KANKAKEE AIRPORT
KANKAKEE, ILLINOIS

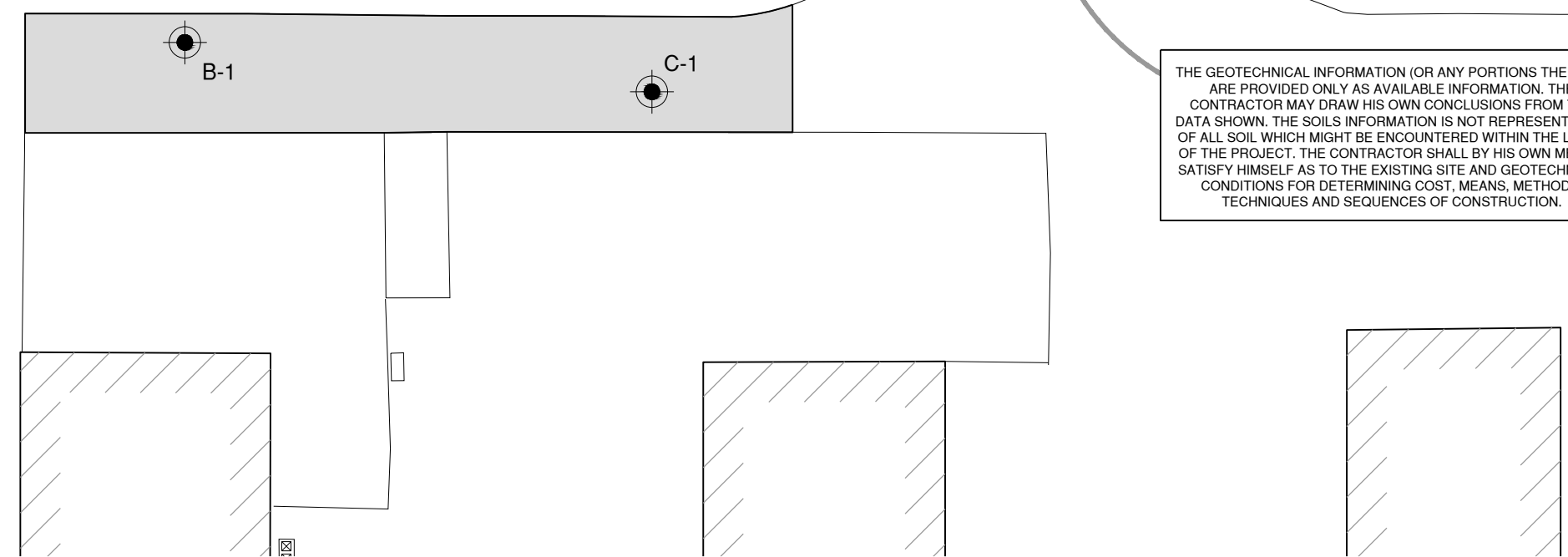
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| DESIGNED BY: | CMJ | |
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SHEET TITLE
**GEOTECHNICAL
INFORMATION**



THE GEOTECHNICAL INFORMATION (OR ANY PORTIONS THEREOF) ARE PROVIDED ONLY AS AVAILABLE INFORMATION. THE CONTRACTOR MAY DRAW HIS OWN CONCLUSIONS FROM THE DATA SHOWN. THE SOILS INFORMATION IS NOT REPRESENTATIVE OF ALL SOIL WHICH MIGHT BE ENCOUNTERED WITHIN THE LIMITS OF THE PROJECT. THE CONTRACTOR SHALL BY HIS OWN MEANS, SATISFY HIMSELF AS TO THE EXISTING SITE AND GEOTECHNICAL CONDITIONS FOR DETERMINING COST, MEANS, METHODS, TECHNIQUES AND SEQUENCES OF CONSTRUCTION.

- LEGEND**
- PROPOSED IMPROVEMENTS
 - B-1 BORING LOCATION
 - C-1 CORING LOCATION



Geo Services, Inc. GEO Job No. 24133
1235 East Duval Street, Arlington, Tennessee, 37005 (847) 253-3845

SOIL BORING LOG Page 1 of 1 Date 1/23/25

PROJECT: Reconstruct Southwest Quadrant Apron, Terminal and Southeast Hangar
LOCATION: Greater Kankakee Airport, Kankakee, IL
COUNTY: Kankakee DRILLING METHOD: Hollow Stem Auger HAMMER TYPE: CME Automatic
CLIENT: CMT

| DEPTH (ft) | DESCRIPTION | UNIT WEIGHT (pcf) | MOISTURE (%) | COMPRESSION (%) | TESTS |
|------------|--|-------------------|--------------|-----------------|-------|
| 0.0 - 0.5 | 2.5" ASPHALT GRAVEL | | | | |
| 0.5 - 1.0 | SANDY SILT-dark black-medium dense (ML) | 111 | 21 | | |
| 1.0 - 1.5 | SAND-brown & gray-loose to medium dense (SC) | 116 | 16 | | |
| 1.5 - 2.0 | | | | | |
| 2.0 - 2.5 | | | | | |
| 2.5 - 3.0 | | | | | |
| 3.0 - 3.5 | | | | | |
| 3.5 - 4.0 | | | | | |
| 4.0 - 4.5 | | | | | |
| 4.5 - 5.0 | | | | | |
| 5.0 - 5.5 | | | | | |
| 5.5 - 6.0 | | | | | |
| 6.0 - 6.5 | | | | | |
| 6.5 - 7.0 | | | | | |
| 7.0 - 7.5 | LEAN CLAY-gray-very stiff (CL) | 119 | 14 | 119 | |
| 7.5 - 8.0 | | | | | |
| 8.0 - 8.5 | | | | | |
| 8.5 - 9.0 | | | | | |
| 9.0 - 9.5 | | | | | |
| 9.5 - 10.0 | | | | | |

End Of Boring @ -10.0'. Boring backfilled with cuttings and pavement patched.

The Unconfined Compressive Strength (UCS) Failure Mode is indicated by (B-Bulge, S-Shear, P-Penetrometer)
The SPT (N value) is the sum of the last two blow values in each sampling zone (AASHTO T206), GP-Geoprobe Hand Auger BBS, from 137 (Rev. 8-99)

Geo Services, Inc. PAVEMENT CORE SUMMARY Page: 1 of 1
1235 East Duval Street, Arlington, Tennessee, 37005 (847) 253-3845

Project: Reconstruct Southwest Quadrant Apron, Terminal & SE Hangar Apron Pavement Repairs GSI Job No.: 24133
Location: Greater Kankakee Airport, Kankakee, IL Date: 1/23/2025
County: Kankakee Cored By: TZ
Client: CMT Checked By: SC

| CORE NO. | THICKNESS (in.) | MATERIAL DESCRIPTION |
|----------|-----------------|--|
| C-01 | 2.5 2.5+ | Asphalt-well consolidated, fine to medium aggregate. Surface mix. Subbase-Gravel |

Geo Services, Inc. PAVEMENT CORE SUMMARY Page: 1 of 1
1235 East Duval Street, Arlington, Tennessee, 37005 (847) 253-3845

Project: Reconstruct Southwest Quadrant Apron, Terminal & SE Hangar Apron Pavement Repairs GSI Job No.: 24133
Location: Greater Kankakee Airport, Kankakee, IL Date: 1/23/2025
County: Kankakee Cored By: TZ
Client: CMT Checked By: SC

Core No.: C-01 Core Location: Northing: 1602072.50 Easting: 1116679.84 Elevation: 621.04

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CONSULTANTS

IL PROJECT: IKK-5084
IL LETTING ITEM: 04A
IL CONTRACT NO.: KA055

RECONSTRUCT SW
QUADRANT APRON;
TERMINAL AND SE HANGAR
APRON PAVEMENT REPAIRS

JUNE 6, 2025

OWNER



GREATER KANKAKEE AIRPORT
KANKAKEE, ILLINOIS

| MARK | DATE | DESCRIPTION |
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| PROJECT NO: | 23005642.00 | |
| CAD DWG FILE: | IKK5084_23005642.00_CD_1_INDEX TO CROSS SECTIONS.DWG | |
| DESIGNED BY: | CMJ | |
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SHEET TITLE
INDEX TO CROSS SECTIONS

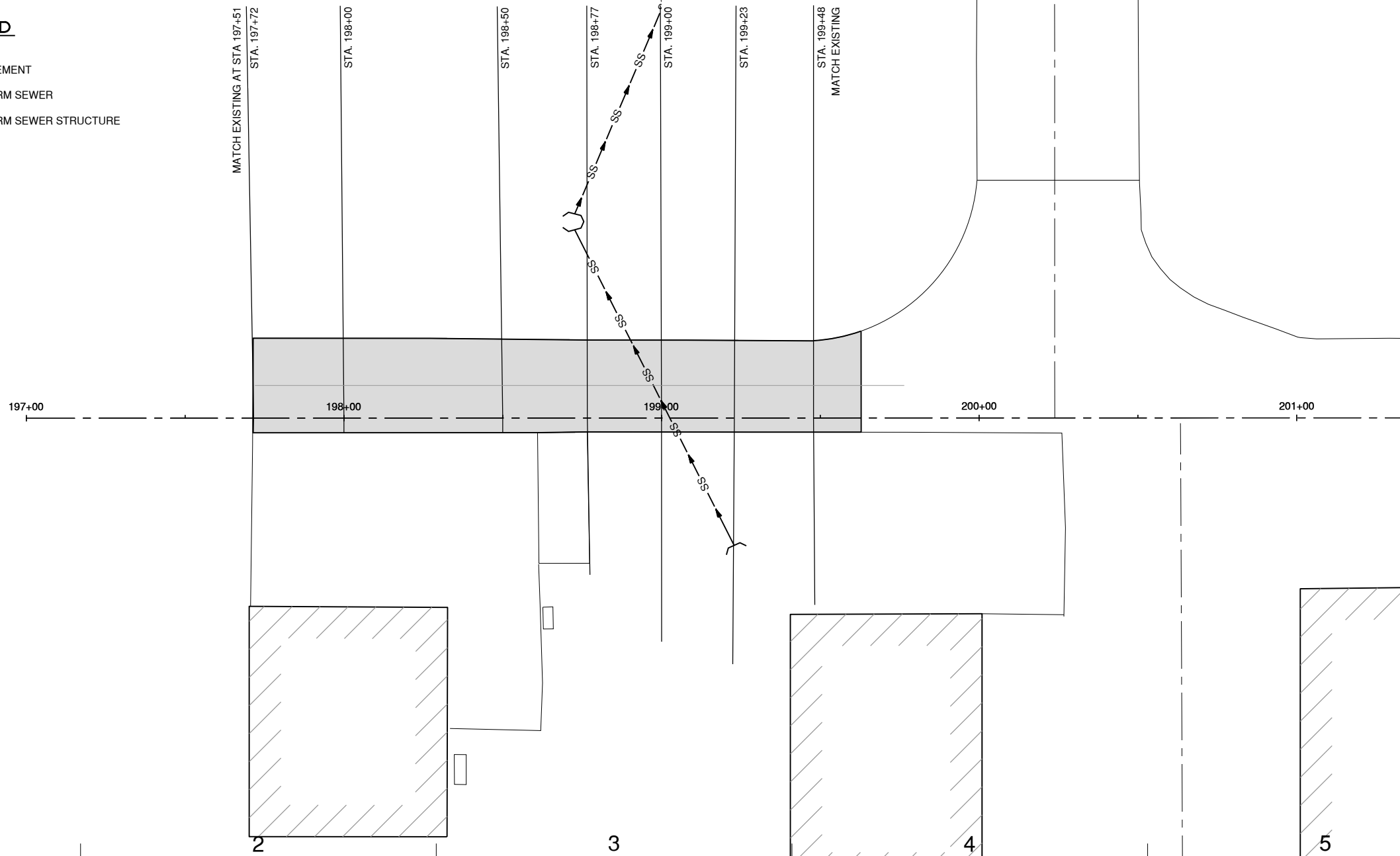
SHEET 23 OF 26

GENERAL EARTHWORK NOTES:

- ALL EARTHWORK QUANTITIES ARE CALCULATED BASED ON THE MATERIAL IN ITS INITIAL OR FINAL POSITION AS SHOWN IN THE PLANS AND QUANTIFIED BY THE METHOD OF AVERAGE END AREAS.
- AREAS OF UNSUITABLE MATERIAL (UNCLASSIFIED EXCAVATION) SHALL BE AS DESIGNATED BY THE ENGINEER. THE QUANTITY OF UNSUITABLE MATERIAL SHALL NOT BE USED AS EMBANKMENT FILL MATERIAL, UNLESS AUTHORIZED BY THE ENGINEER.
- PAYMENT FOR UNCLASSIFIED EXCAVATION IS THE SUM OF TOPSOIL STRIPPING AND UNCLASSIFIED EXCAVATION AREAS IN ITS INITIAL POSITION. UNCLASSIFIED EXCAVATION AREA ALSO INCLUDES PGE SUBGRADE STABILIZATION AREA AS DIRECTED BY THE RESIDENT ENGINEER.
- THE CONTRACTOR SHALL ENSURE THAT 4 INCHES OF TOPSOIL CAN BE SPREAD OVER THE LIMITS OF THE GRADED AREA. IN SOME CASES, CONTRACTOR MAY BE REQUIRED TO OVER-EXCAVATE TO PROVIDE THE REQUIRED 4-INCH TOPSOIL LAYER. THE EARTHWORK QUANTITIES SHOWN INCLUDE THE REQUIRED OVER-EXCAVATION AND ARE SHOWN IN THE CROSS SECTIONS.
- TOPSOIL PLACEMENT, EMBANKMENT FILL AND SHOULDER FILL SHALL BE CONSIDERED INCLUDED IN THE CONTRACT UNIT PRICE FOR UNCLASSIFIED EXCAVATION. NO SEPARATE PAYMENT WILL BE MADE FOR TOPSOIL PLACEMENT, EMBANKMENT FILL AND SHOULDER FILL.
- ANY EXCESS MATERIAL INCLUDING CLAY, UNSUITABLE MATERIAL AND TOPSOIL SHALL BE HAULED OFF AIRPORT PROPERTY AND DISPOSED OF BY THE CONTRACTOR (COST INCIDENTAL UNCLASSIFIED EXCAVATION). CCDD TESTING REQUIREMENTS SHALL PER THE SPECIAL PROVISIONS SECTION 152 (COST INCIDENTAL TO UNCLASSIFIED EXCAVATION).
- AVERAGE 6 INCHES OF TOPSOIL IS PRESENT AND USED IN QUANTITY ESTIMATION. TOPSOIL STRIPPED SHALL BE INCORPORATED INTO THE REQUIRED TOPSOIL PLACEMENT ON DISTURBED AREAS OUTSIDE OF NEW PAVEMENT.

LEGEND

- NEW PAVEMENT
- NEW STORM SEWER
- NEW STORM SEWER STRUCTURE



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CONSULTANTS

IL PROJECT: IKK-5084
IL LETTING ITEM: 04A
IL CONTRACT NO.: KA055

RECONSTRUCT SW
QUADRANT APRON;
TERMINAL AND SE HANGAR
APRON PAVEMENT REPAIRS

JUNE 6, 2025

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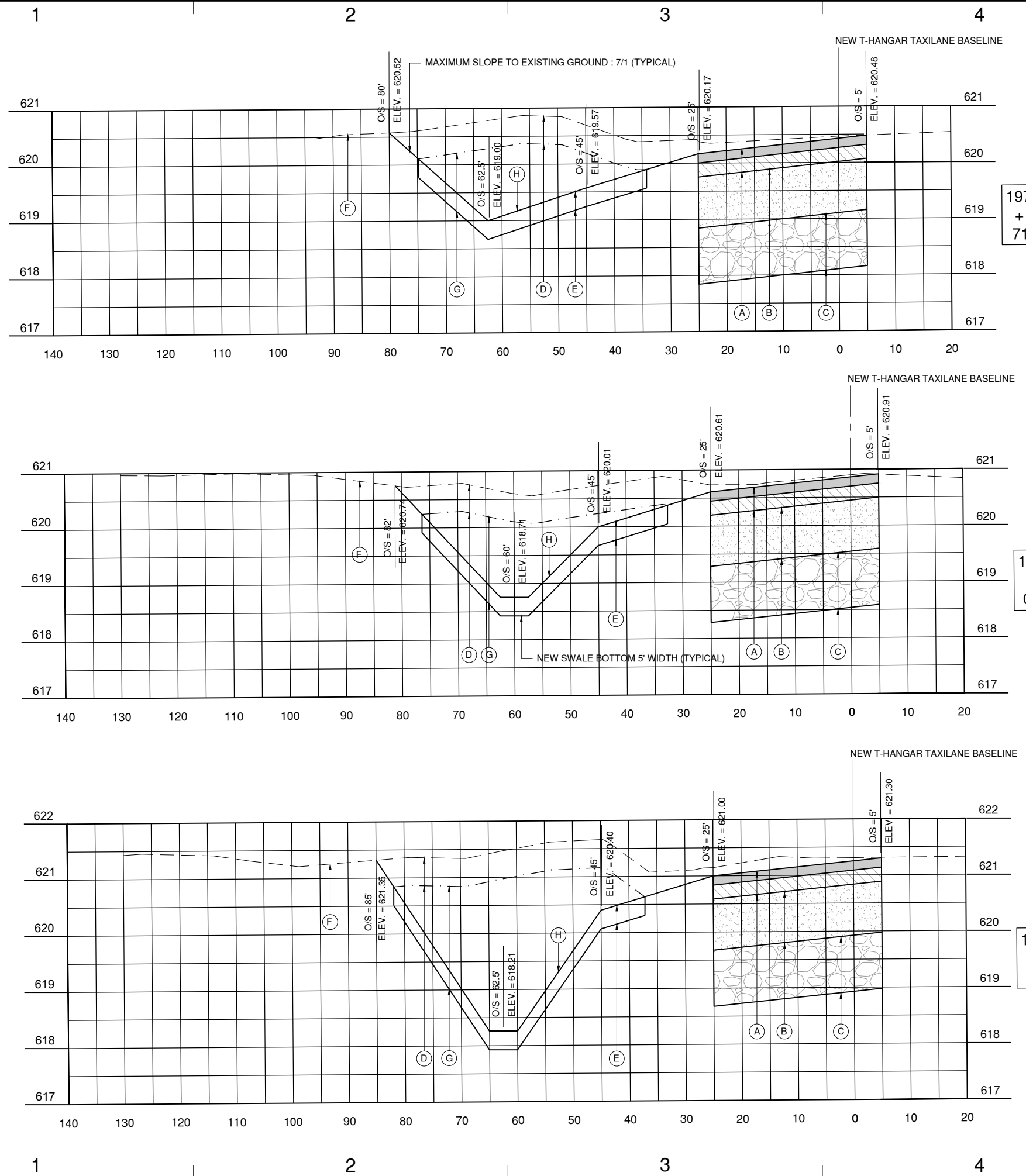
GREATER KANKAKEE AIRPORT
KANKAKEE, ILLINOIS

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SHEET TITLE

CROSS
SECTIONS - 1

SHEET 24 OF 26



NOTE: CONTRACTOR TO MATCH
EXISTING GRADES AT STA. 197+51.

LEGEND

- EXISTING GROUND
- PROPOSED GROUND
- [Solid Grey Box] NEW BITUMINOUS SURFACE COURSE
- [Hatched Box] NEW BITUMINOUS BASE COURSE
- [Dotted Box] NEW CRUSHED AGGREGATE BASE COURSE
- [Stippled Box] NEW POROUS GRANULAR EMBANKMENT
- (A) NEW 5" HMA PAVEMENT (2 LIFTS)(401)
- (B) NEW 11" CRUSHED AGGREGATE BASE COURSE (209)
- (C) NEW 12" POROUS GRANULAR EMBANKMENT (208)
- (D) AVERAGE 6" TOPSOIL STRIPPING (152)
- (E) MINIMUM 4" TOPSOIL PLACEMENT (905)
- (F) EXISTING GROUND LINE
- (G) UNCLASSIFIED EXCAVATION (152)
- (H) NEW GROUND LINE

CROSS SECTION NOTES

- SEE GRADING PLAN FOR ELEVATIONS.
- EXISTING AND NEW UTILITIES/UNDERDRAIN ARE NOT SHOWN FOR CLARITY. SEE EXISTING CONDITIONS AND REMOVALS FOR APPROXIMATE UTILITY LOCATIONS.



CROSS SECTION NOTES

1. SEE GRADING PLAN FOR ELEVATIONS.
2. EXISTING AND NEW UTILITIES/UNDERDRAIN ARE NOT SHOWN FOR CLARITY. SEE EXISTING CONDITIONS AND REMOVALS FOR APPROXIMATE UTILITY LOCATIONS.
3. SEE CROSS SECTION - 1 FOR LEGEND.

CONSULTANTS

IL PROJECT: IKK-5084
 IL LETTING ITEM: 04A
 IL CONTRACT NO.: KA055

**RECONSTRUCT SW QUADRANT APRON;
 TERMINAL AND SE HANGAR
 APRON PAVEMENT REPAIRS**

JUNE 6, 2025

OWNER



**GREATER KANKAKEE AIRPORT
 KANKAKEE, ILLINOIS**

MARK DATE DESCRIPTION

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**CROSS
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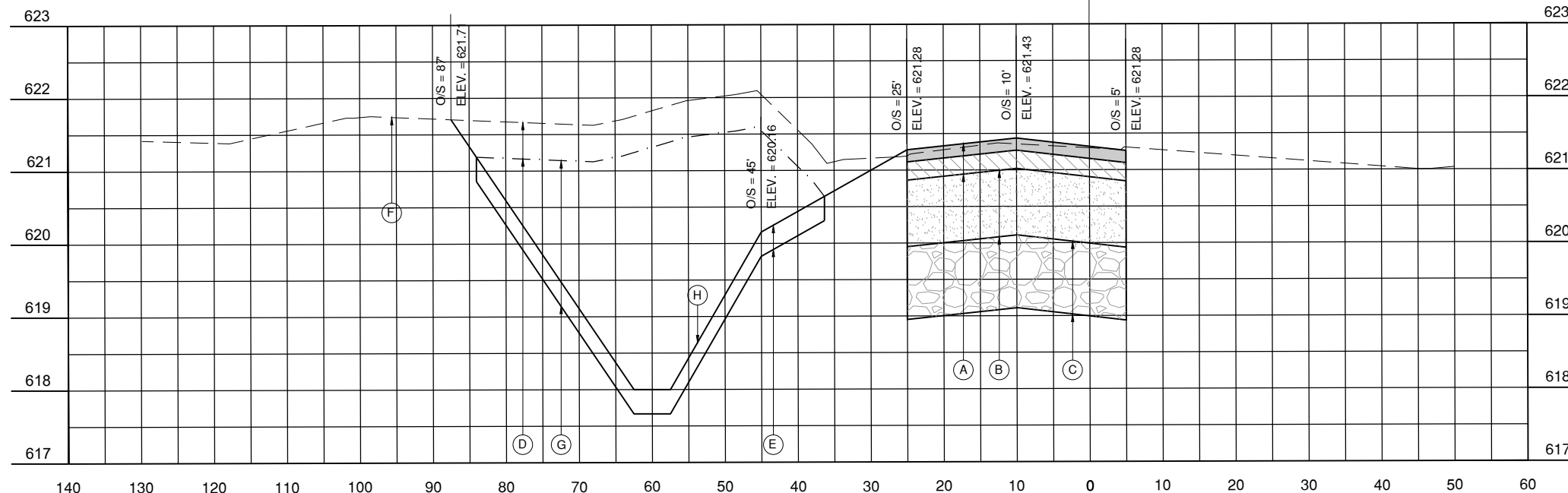
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NEW T-HANGAR TAXILANE BASELINE



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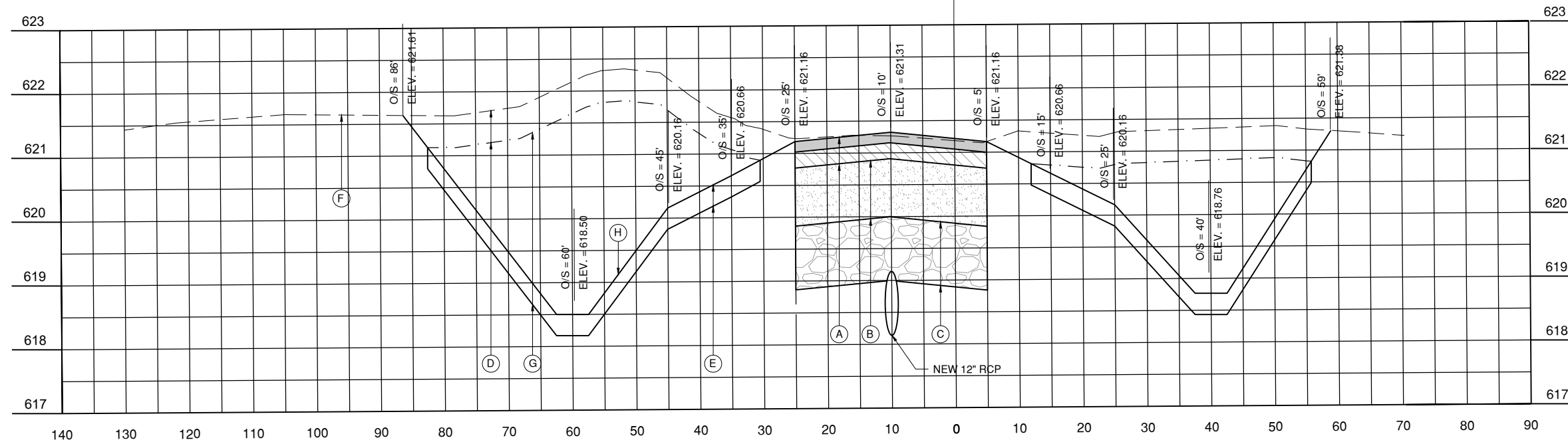
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NEW T-HANGAR TAXILANE BASELINE



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 Date: Thursday, August 7, 2025 1:33:10 PM



CROSS SECTION NOTES

1. SEE GRADING PLAN FOR ELEVATIONS.
2. EXISTING AND NEW UTILITIES/UNDERDRAIN ARE NOT SHOWN FOR CLARITY. SEE EXISTING CONDITIONS AND REMOVALS FOR APPROXIMATE UTILITY LOCATIONS.
3. SEE CROSS SECTION - 1 FOR LEGEND.

CONSULTANTS

IL PROJECT: IKK-5084
 IL LETTING ITEM: 04A
 IL CONTRACT NO.: KA055

RECONSTRUCT SW QUADRANT APRON; TERMINAL AND SE HANGAR APRON PAVEMENT REPAIRS

JUNE 6, 2025

OWNER



GREATER KANKAKEE AIRPORT
 KANKAKEE, ILLINOIS

MARK DATE DESCRIPTION

| | |
|---------------|---|
| PROJECT NO: | 23005642.00 |
| CAD DWG FILE: | IKK5084_23005642.00_CD_1_CROSS SECTIONS.DWG |
| DESIGNED BY: | CMJ |
| DRAWN BY: | JRO |
| CHECKED BY: | -- |
| APPROVED BY: | -- |
| COPYRIGHT: | CRAWFORD, MURPHY & TILLY, INC. 2025 |

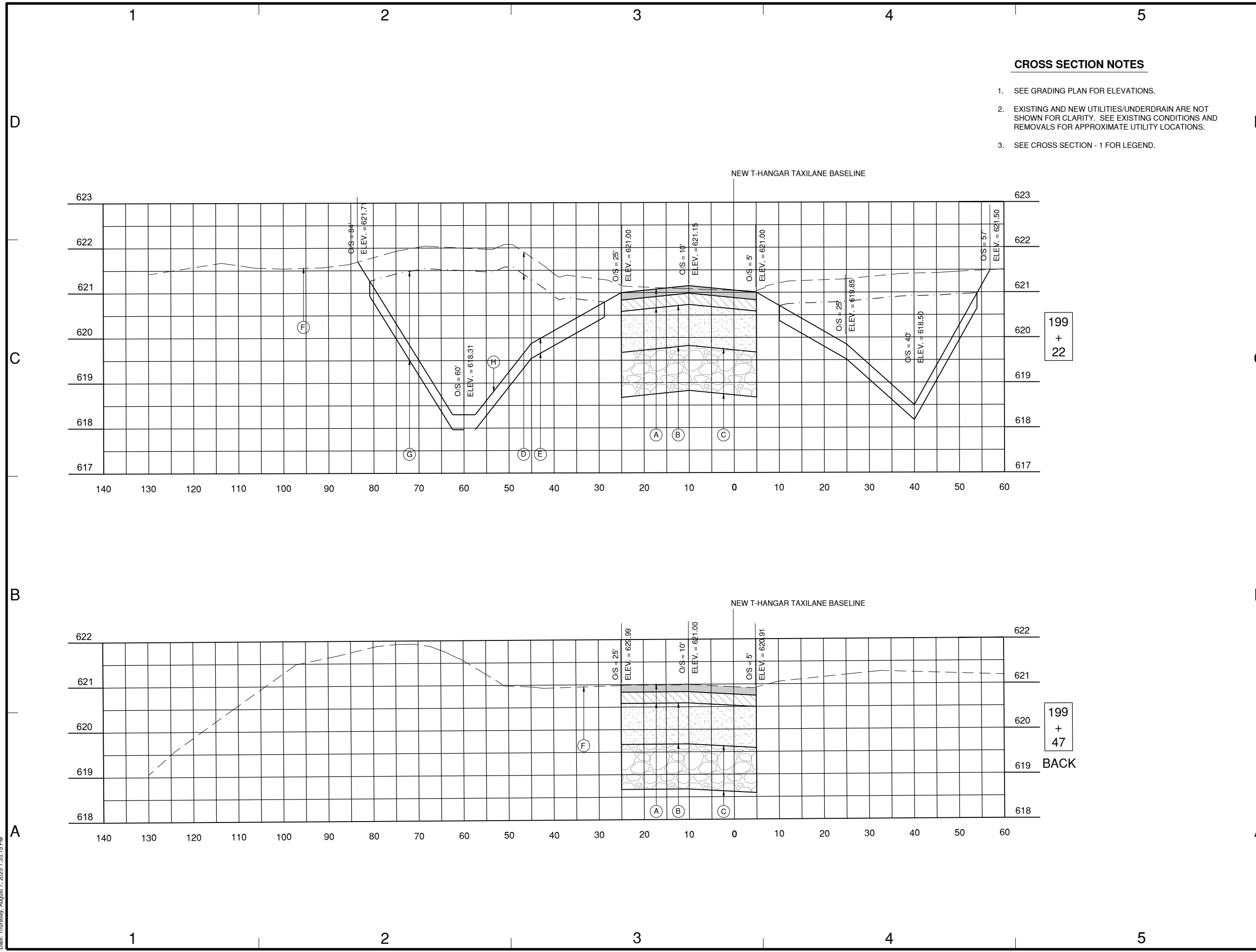
SHEET TITLE

CROSS
 SECTIONS - 3

199
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BACK



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